06-15-2018 LETTING ITEM 006

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	1
	ILLINOIS	CONTRACT NO. 62B61		

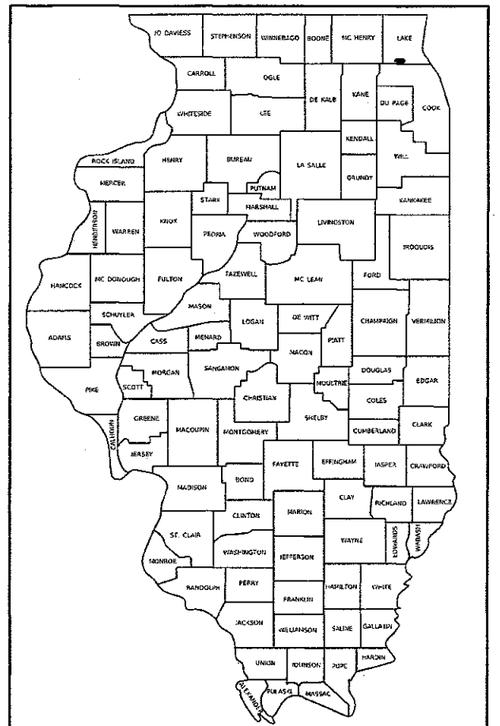
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED HIGHWAY PLANS

F.A.U. ROUTE 1261 (IL 53)
AT OLD HICKS RD.
SECTION: 530N-3
PROJECT: STP-AXG7(828)
CHANNELIZATION
LAKE COUNTY

C-91-074-16

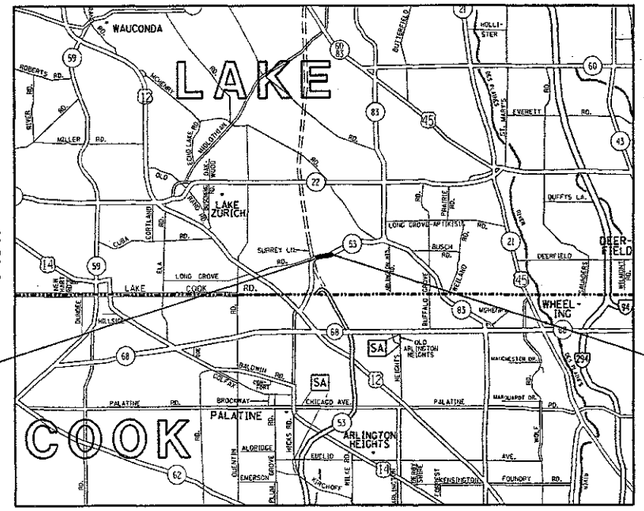
D-91-074-16



LOCATION OF SECTION INDICATED THUS: -

R 10 E

T 43 N



ELA TOWNSHIP

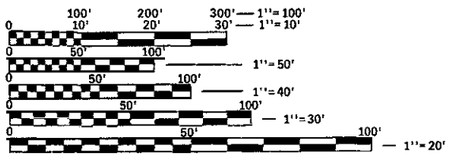
PROJECT BEGINS
STA. 483+75

PROJECT ENDS
STA. 512+68

N

LOCATION MAP
(NOT TO SCALE)

GROSS & NET LENGTH = 2,983 FT. = 0.548 MILE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE VILLAGE OF LONG GROVE

TRAFFIC DATA:
IL 53
2017 ADT = 23,700
POSTED AND DESIGN SPEED LIMIT = 45 MPH

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: DAN WILGREEN (847)705-4240
PROJECT MANAGER: FAWAD AQUEEL (847)705-4247

CONTRACT NO. 62B61

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED March 15 20 18
REGIONAL ENGINEER
May 11 20 18
ENGINEER OF DESIGN AND ENVIRONMENT
May 10 20 18
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
406201-01	MAILBOX TURNOUT
442201-03	CLASS C AND D PATCHES
482001-02	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
482011-03	HMA SHLD. STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
542311-07	TRAVERSABLE PIPE GRATE FOR CONCRETE END SECTIONS
542401-03	METAL FLARED END SECTION FOR PIPE CULVERTS
601001-05	PIPE UNDERDRAINS
601101-02	CONCRETE HEADWALL FOR PIPE UNDERDRAINS
602001-02	CATCH BASIN TYPE A
602402	PRECAST MANHOLE TYPE A 5' (1.52 m) DIAMETER
602601-05	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-04	FRAME AND LIDS TYPE 1
604036-03	GRATE TYPE 8
630001-12	STEEL PLATE BEAM GUARDRAIL
630201-07	PCC/HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
635001-02	DELINEATORS
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5 m) AWAY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701201-04	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS >= 45 MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TME OPERATIONS
701306-04	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >= 45 MPH
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701502-08	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-07	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
725001-01	OBJECT AND TERMINAL MARKERS
728001-01	TELESCOPING STEEL SIGN SUPPORT
731001-01	BASE FOR TELESCOPING STEEL SIGN SUPPORT
780001-05	TYPICAL PAVEMENT MARKINGS
782006	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTNG DETAILS
814001-03	HANDHOLES
886001-01	DETECTOR LOOP INSTALLATIONS

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)
2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF LONG GROVE.
3. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
4. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
5. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
6. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
7. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
8. THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, ARTERIAL TRAFFIC FIELD ENGINEER, AT WALTER.CZARNY@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS.
9. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
10. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
11. DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.
12. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
13. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
14. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
15. UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
16. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
17. ALL ABANDONED SEWERS SHALL BE PLUGGED AT BOTH ENDS WITH TWO (2) FOOT LONG NON-SHRINK CONCRETE OR MORTAR GROUT OR AS SPECIFIED BY THE ENGINEER.
18. TOP OF GRATE ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF EACH STRUCTURE. FRAMES/GRATES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATIONS OF THE AREAS IN WHICH THEY ARE LOCATED, AS PART OF THE STRUCTURE COST. PROPOSED STORM SEWER AND PIPE CULVERT LENGTHS PROVIDED IN THE QUANTITIES ARE FROM THE CENTER OF THE STRUCTURES.
19. THE SUBGRADE STABILITY SHALL BE VERIFIED BY PROOF ROLLING WITH A FULLY LOADED TANDEM-AXLE TRUCK.
20. AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ASI WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND THE IDOT SUBGRADE STABILITY MANUAL. GEOTECHNICAL FABRIC FOR GROUND STABILIZATION SHALL BE INSTALLED WHERE ASI IS USED. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
21. ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTOR'S VEHICLES AND/OR EQUIPMENT IS TO BE REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
22. PIPE UNDERDRAINS SHALL BE INSTALLED ACCORDING TO SECTION 601 OF THE SSRBC AND STANDARD 601001-05. TOP OF PIPE UNDERDRAINS SHALL BE PLACED A MINIMUM OF 6" BELOW THE AGGREGATE SUBGRADE IMPROVEMENT LAYER. THE COST OF MAKING PIPE UNDERDRAIN CONNECTIONS TO DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE COST OF THE PIPE UNDERDRAINS.
23. THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) and USE/WASTE REVIEW(BDE 2290) SUBMITTALS, THE CONTRACTOR SHALL SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION II.G.1 AND 2 OF THE SWPPP. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
24. THE ENGINEER SHALL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171 AT LEAST 7 DAYS PRIOR TO LAYOUT TREE PLANTING LOCATIONS.

USER NAME = tarkqfm	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 3/20/2018	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, HIGHWAY STANDARDS, AND GENERAL NOTES
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	2
		CONTRACT NO. 62B61		
		ILLINOIS	FED. AID PROJECT	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE: 80% FED 20% STATE ROADWAY 0004				
CODE NO	ITEM	UNIT						
X0800007	SUBBASE GRANULAR MATERIAL, TYPE B 3"	SQ YD	3346	3346				
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	1436	1436				
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	911	911				
20101000	TEMPORARY FENCE	FOOT	1121	1121				
20101200	TREE ROOT PRUNING	EACH	38	38				
20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	25	25				
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	13	13				
20200100	EARTH EXCAVATION	CU YD	3471	3471				
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	1006	1006				
20800150	TRENCH BACKFILL	CU YD	69	69				
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	1241	1241				
21101505	TOPSOIL EXCAVATION AND PLACEMENT	CU YD	1532	1532				
* 25000210	SEEDING, CLASS 2A	ACRE	1.61	1.61				
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	145	145				
* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	145	145				

* SPECIALTY ITEMS

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE: 80% FED 20% STATE ROADWAY 0004					
CODE NO	ITEM	UNIT							
* 25100115	MULCH, METHOD 2	ACRE	1.61	1.61					
* 25100630	EROSION CONTROL BLANKET	SQ YD	6689	6689					
* 25100635	HEAVY DUTY EROSION CONTROL BLANKET	SQ YD	1104	1104					
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	161	161					
28000305	TEMPORARY DITCH CHECKS	FOOT	761	761					
28000400	PERIMETER EROSION BARRIER	FOOT	4317	4317					
28000510	INLET FILTERS	EACH	2	2					
28100105	STONE RIPRAP, CLASS A3	SQ YD	120	120					
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	414	414					
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	4963	4963					
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	258	258					
35600710	HOT-MIX ASPHALT BASE COURSE WIDENING, 8 1/2"	SQ YD	1617	1617					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	10558	10558					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	19	19					
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	526	526					

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PLOT DATE = 3/20/2018 | DATE - | REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL 53 AT OLD HICKS RD.

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	3

CONTRACT NO. 62B61

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE 80% FED 20% STATE ROADWAY 0004				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	94	94				
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	29	29				
40603565	POLYMERIZED HOT-MIX ASPHALT SURFACE	TON	1395	1395				
	COURSE, MIX "E", N70							
40800029	BITUMINOUS MATERIALS (TACK COAT)	POUND	116	116				
44000100	PAVEMENT REMOVAL	SQ YD	109	109				
44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"	SQ YD	1494	1494				
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	11120	11120				
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	333	333				
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	413	413				
44004250	PAVED SHOULDER REMOVAL	SQ YD	1388	1388				
44201839	CLASS D PATCHES, TYPE II, 16 INCH	SQ YD	83	83				
44201843	CLASS D PATCHES, TYPE III, 16 INCH	SQ YD	51	51				
44201845	CLASS D PATCHES, TYPE IV, 16 INCH	SQ YD	61	61				
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	2814	2814				
50105220	PIPE CULVERT REMOVAL	FOOT	142	142				

* SPECIALTY ITEMS

CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE 80% FED 20% STATE ROADWAY 0004					
50300225	CONCRETE STRUCTURES	CU YD	38	38					
50800105	REINFORCEMENT BARS	POUND	1792.9	1792.9					
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	60	60					
52200015	PERMANENT SHEET PILING	SQ FT	2415	2415					
54002020	EXPANSION BOLTS 3/4 INCH	EACH	4	4					
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	4	4					
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	6	6					
54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"	EACH	2	2					
54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	1	1					
54213675	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 30"	EACH	1	1					
54248510	CONCRETE COLLAR	CU YD	1.1	1.1					
54260316	TRAVERSABLE PIPE GRATE FOR CONCRETE END SECTION	FOOT	1	1					
54262736	METAL FLARED END SECTIONS 36"	EACH	1	1					
542A0220	PIPE CULVERTS, CLASS A, TYPE 1 15"	FOOT	84	84					
542A0223	PIPE CULVERTS, CLASS A, TYPE 1 18"	FOOT	56	56					
542A0229	PIPE CULVERTS, CLASS A, TYPE 1 24"	FOOT	222	222					

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE: 80% FED 20% STATE ROADWAY 0004				
542A1069	PIPE CULVERTS, CLASS A, TYPE 2 24"	FOOT	68	68				
54390230	INSERTION CULVERT LINER 36"	FOOT	73	73				
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	20	20				
550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	5	5				
550A0140	STORM SEWERS, CLASS A, TYPE 1 30"	FOOT	6	6				
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	77	77				
55100500	STORM SEWER REMOVAL 12"	FOOT	66	66				
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	50	50				
60100060	CONCRETE HEADWALLS FOR PIPE DRAINS	EACH	5	5				
60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	60	60				
60108204	PIPE UNDERDRAINS, TYPE 2, 4"	FOOT	1520	1520				
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	1				
60200805	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	1	1				
60203905	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1				

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE: 80% FED 20% STATE ROADWAY 0004					
60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	3	3					
60255500	MANHOLES TO BE ADJUSTED	EACH	3	3					
60500050	REMOVING CATCH BASINS	EACH	4	4					
60500060	REMOVING INLETS	EACH	4	4					
* 63000003	STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS	FOOT	425	425					
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	6	6					
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	3805	3805					
* 66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1					
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	7	7					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6					
67100100	MOBILIZATION	LSUM	1	1					
70300100	SHORT TERM PAVEMENT MARKING	FOOT	8335	8335					
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	3385	3385					
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	338	338					

* SPECIALTY ITEMS

FILE NAME =	USER NAME =	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES IL 53 AT OLD HICKS RD.	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000 ' / in.	DRAWN -	REVISED -			1261	530N-3	LAKE	80	5
	PLOT DATE = 3/20/2018	CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.	CONTRACT NO. 62B61				
		DATE -	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE 80% FED 20% STATE ROADWAY 0004					
CODE NO	ITEM	UNIT							
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	13765	13765					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	929	929					
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	235	235					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	453	453					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	77	77					
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	2084	2084					
70300904	PAVEMENT MARKING TAPE, TYPE IV 4"	FOOT	14506	14506					
70300912	PAVEMENT MARKING TAPE, TYPE IV 12"	FOOT	65	65					
70300924	PAVEMENT MARKING TAPE, TYPE IV 24"	FOOT	68	68					
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1762.5	1762.5					
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1612.5	1612.5					
70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	6	6					
70600332	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	6	6					
* 72000100	SIGN PANEL - TYPE 1	SQ FT	159.2	159.2					
* 72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	21	21					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE 80% FED 20% STATE ROADWAY 0004					
CODE NO	ITEM	UNIT							
* 72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	7	7					
* 72400310	REMOVE SIGN PANEL - TYPE I	SQ FT	6.25	6.25					
* 72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	5	5					
* 72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	EACH	3	3					
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	6	6					
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	294.25	294.25					
* 73100100	BASE FOR TELESCOPING STEEL SIGN SUPPORT	EACH	19	19					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	338	338					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	13765	13765					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	929	929					
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	235	235					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	453	453					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	77	77					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	179	179					
* 78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	26	26					

* SPECIALTY ITEMS

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL 53 AT OLD HICKS RD.

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	6
		CONTRACT NO. 62B61		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

SUMMARY OF QUANTITIES

	CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE: 80% FED 20% STATE ROADWAY 0004
	78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	142	142
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	154	154
*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1	1
*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	72	72
*	89502376	REBUILD EXISTING HANDHOLE	EACH	1	1
*	A2001720	TREE, ACER SACCHARUM (SUGAR MAPLE), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	9	9
*	A2005116	TREE, JUGLANS NIGRA (BLACK WALNUT), 2" CALIPER, BALLED AND BURLAPPED	EACH	4	4
*	A2005820	TREE, PLATANUS OCCIDENTALIS (SYCAMORE), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	7	7
*	A2006716	TREE, QUERCUS MACROCARPA (BUR OAK), 2" CALIPER, BALLED AND BURLAPPED	EACH	5	5
*	A2007116	TREE, QUERCUS RUBRA (RED OAK), 2" CALIPER, BALLED AND BURLAPPED	EACH	8	8
*	A2007820	TREE, TILIA AMERICANA (AMERICAN LINDEN/ BASSWOOD), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	6	6
*	A2008470	TREE, ULMUS AMERICANA PRINCETON (PRINCETON AMERICAN ELM), 2-1/2" CALIPER , BALLED AND BURLAPPED	EACH	9	9

* SPECIALTY ITEMS

SUMMARY OF QUANTITIES

	CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE: 80% FED 20% STATE ROADWAY 0004
	X0327301	RELOCATE EXISTING MAILBOX	EACH	5	5
	X0327979	PAVEMENT MARKING REMOVAL - GRINDING	SQ FT	2517	2517
	X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	134	134
	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	7	7
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	1	1
	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	LSUM	1	1
	X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	11887	11887
	X7040125	PINNING TEMPORARY CONCRETE BARRIER	EACH	381	381
	Z0012755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	25	25
	Z0013798	CONSTRUCTION LAYOUT	LSUM	1	1
	Z0022800	FENCE REMOVAL	FOOT	65	65
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	63.9	63.9
	Z0062458	TEMPORARY PAVEMENT (VARIABLE DEPTH)	TON	8	8
*	Z0064800	SELECTIVE CLEARING	UNIT	17.5	17.5
Ø	Z0076600	TRAINEES	HOUR	500	500
Ø	Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500	500

Ø 0042

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		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

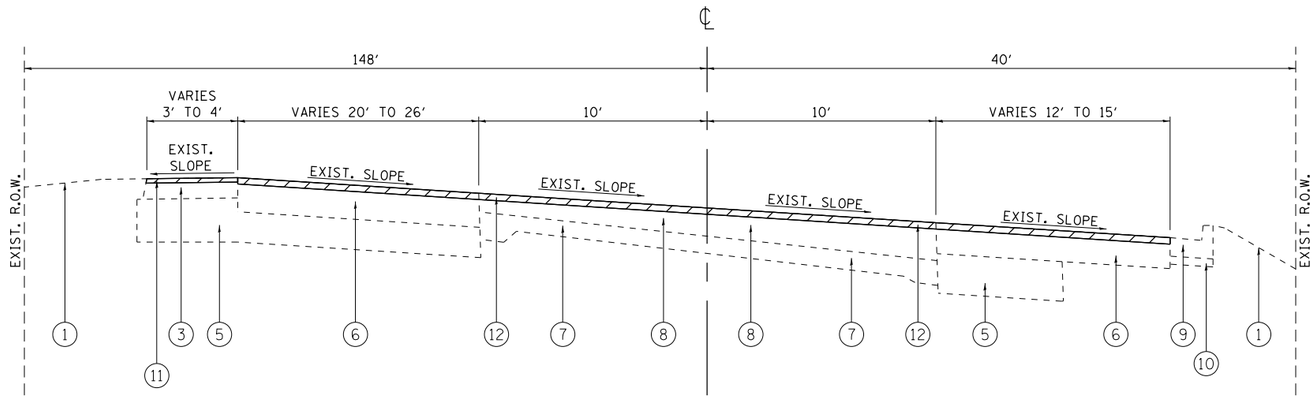
SUMMARY OF QUANTITIES
IL 53 AT OLD HICKS RD.

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	7

CONTRACT NO. 62B61

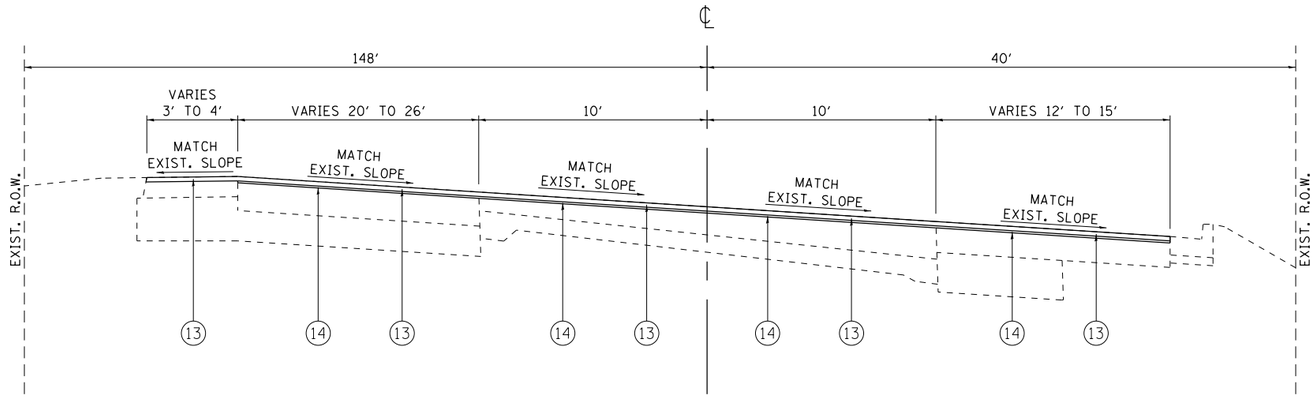
FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT



IL 53

EXISTING TYPICAL SECTION

STA. 483+75 TO STA. 484+11



IL 53

PROPOSED TYPICAL SECTION

STA. 483+75 TO STA. 484+11

HOT-MIX ASPHALT MIXTURE REQUIREMENTS			
MIXTURE USES	MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT WIDENING	POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, (IL 9.5 mm); 1¾"	4% @ 70 GYR.	QC/QA
	POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50; ¾"	3.5% @ 50 GYR.	QC/QA
	HMA BASE COURSE WIDENING, (HMA BINDER IL-19 mm); 8½"	4% @ 90 GYR.	QC/QA
PAVEMENT RESURFACING	POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, (IL 9.5 mm); 1¾"	4% @ 70 GYR.	QC/QA
	POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50; ¾"	3.5% @ 50 GYR.	QC/QA
SHOULDER RECONSTRUCTION AND WIDENING (HMA SHOULDERS, 8")	POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, (IL 9.5 mm); 1¾"	4% @ 70 GYR.	QC/QA
	(HMA BINDER IL-19 mm); 6¼"	4% @ 70 GYR	QC/QA
SHOULDER RESURFACING	POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, (IL 9.5 mm); 1¾"	4% @ 70 GYR.	QC/QA
DRIVEWAYS	HMA SURFACE COURSE, MIX "D", N50, (IL-9.5 mm); 2"	4% @ 50 GYR.	QC/QA
	HMA BASE COURSE, (HMA BINDER IL-19 mm); PE - 6"	4% @ 50 GYR.	QC/QA
PATCHING	CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR	QC/QA
TEMPORARY PAVEMENT (VARIABLE DEPTH)	HMA SURFACE COURSE, MIX "D", N70, (IL-9.5 mm); VARIABLE DEPTH	4% @ 70 GYR	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP)			

NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

LEGEND:

1. EXISTING SOD
2. EXISTING PCC SHOULDERS, 8"
3. EXISTING HMA SHOULDERS, 8" OR 12"
4. EXISTING AGGREGATE SHOULDER, 8"
5. EXISTING AGGREGATE SUBGRADE, 12"
6. EXISTING HMA BINDER/BASE COURSE, ± 13" TO ± 14¼"
7. EXISTING CONCRETE BASE COURSE, ± 7" TO ± 9"
8. EXISTING HMA SURFACE/WIDENING, ± 7½" TO ± 21"
9. EXISTING COMB. CONC. CURB AND GUTTER
10. EXISTING AGGREGATE BASE COURSE, 6"
11. PROPOSED HMA SURFACE REMOVAL, 1¾"
12. PROPOSED HMA SURFACE REMOVAL, 2½"
13. PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1¾"
14. PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾"
15. PROPOSED HMA BASE COURSE WIDENING, 8½"
16. PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"
17. PROPOSED HMA SHOULDERS, 8" (INCLUDES POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1¾" AND HMA BINDER IL-19 mm, 6¼")
18. PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 3"
19. PROPOSED TOPSOIL EXCAVATION AND PLACEMENT (PLACED AT A DEPTH OF 4")
20. PROPOSED SEEDING, CLASS 2A

R = TO BE REMOVED
(SEE ROADWAY PLANS FOR LOCATIONS OF REMOVAL)

NOTES:

1. THE CONTRACTOR SHALL MILL FIRST PRIOR TO PATCHING BETWEEN STA. 483+75 AND STA. 484+11.

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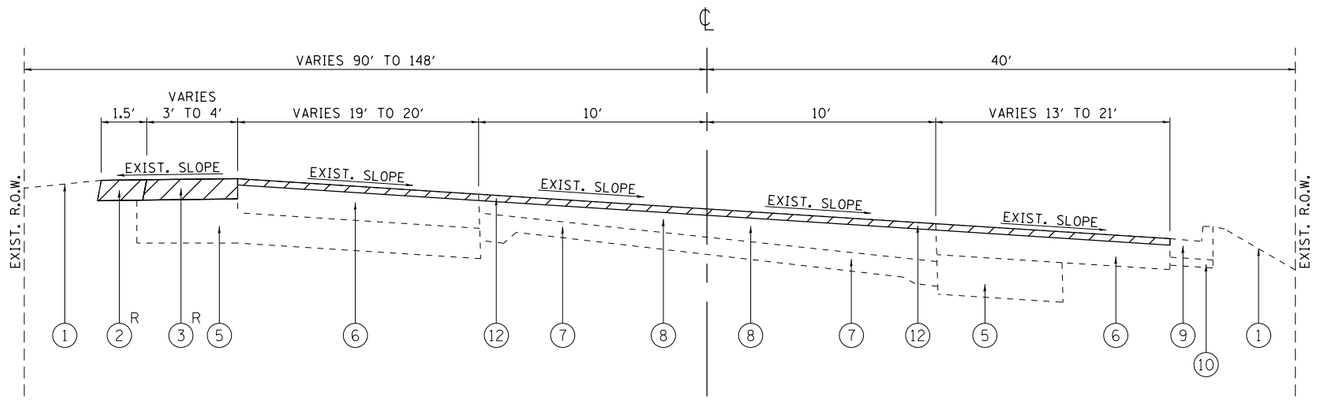
	PLOT DATE = 3/20/2018	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.

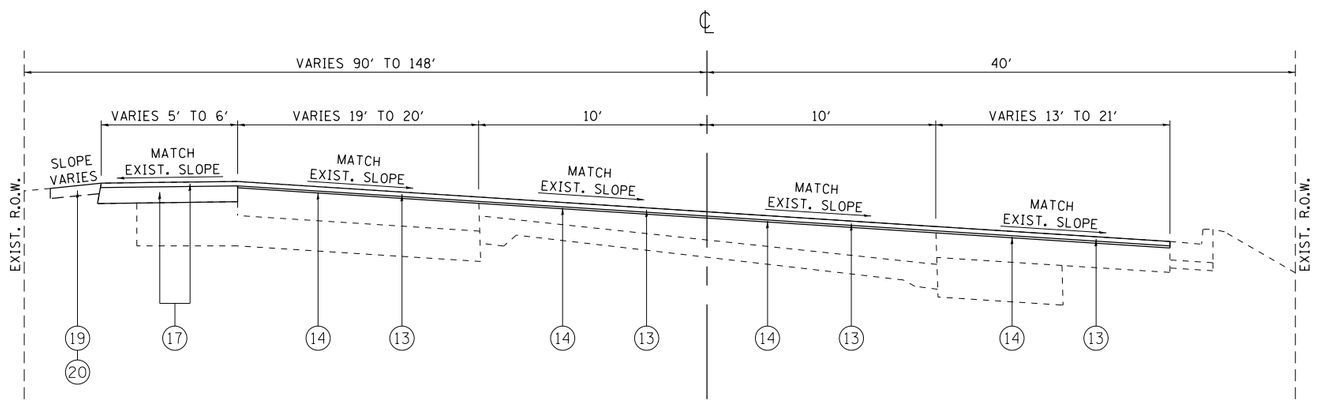
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	8
		CONTRACT NO. 62B61		
		ILLINOIS FED. AID PROJECT		



IL 53

EXISTING TYPICAL SECTION

STA. 484+11 TO STA. 485+91



IL 53

PROPOSED TYPICAL SECTION

STA. 484+11 TO STA. 485+91

LEGEND:

1. EXISTING SOD
2. EXISTING PCC SHOULDERS, 8''
3. EXISTING HMA SHOULDERS, 8'' OR 12''
4. EXISTING AGGREGATE SHOULDER, 8''
5. EXISTING AGGREGATE SUBGRADE, 12''
6. EXISTING HMA BINDER/BASE COURSE, ± 13'' TO ± 14¼''
7. EXISTING CONCRETE BASE COURSE, ± 7'' TO ± 9''
8. EXISTING HMA SURFACE/WIDENING, ± 7½'' TO ± 21''
9. EXISTING COMB. CONC. CURB AND GUTTER
10. EXISTING AGGREGATE BASE COURSE, 6''
11. PROPOSED HMA SURFACE REMOVAL, 1¾''
12. PROPOSED HMA SURFACE REMOVAL, 2½''
13. PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX ''E'', N70, 1¾''
14. PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾''
15. PROPOSED HMA BASE COURSE WIDENING, 8½''
16. PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12''
17. PROPOSED HMA SHOULDERS, 8''
    (INCLUDES POLYMERIZED HMA SURFACE COURSE, MIX ''E'', N70, 1¾''
    AND HMA BINDER IL-19 mm, 6¼'')
18. PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 3''
19. PROPOSED TOPSOIL EXCAVATION AND PLACEMENT
    (PLACED AT A DEPTH OF 4'')
20. PROPOSED SEEDING, CLASS 2A

R = TO BE REMOVED
(SEE ROADWAY PLANS FOR LOCATIONS OF REMOVAL)

NOTES:

1. THE CONTRACTOR SHALL MILL FIRST PRIOR TO PATCHING BETWEEN STA. 484+11 AND STA. 485+91.

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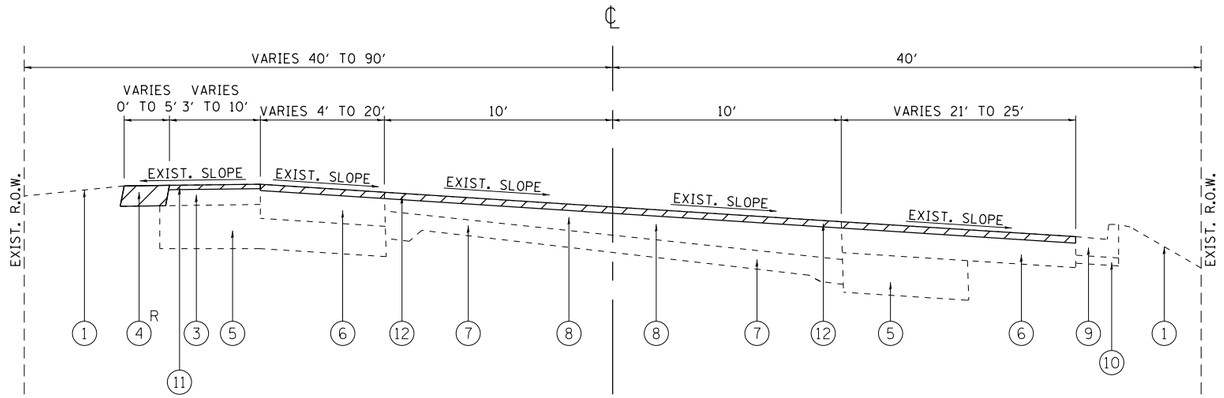
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED TYPICAL SECTIONS
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. TO STA.

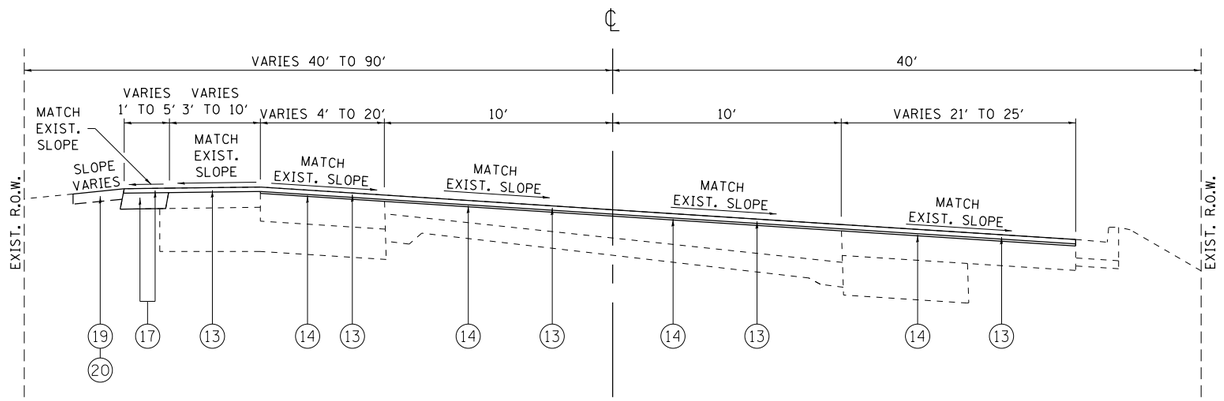
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	9
		CONTRACT NO. 62B61		
		ILLINOIS FED. AID PROJECT		



IL 53

EXISTING TYPICAL SECTION

STA. 485+91 TO STA. 488+79



IL 53

PROPOSED TYPICAL SECTION

STA. 485+91 TO STA. 488+79

LEGEND:

1. EXISTING SOD
2. EXISTING PCC SHOULDERS, 8''
3. EXISTING HMA SHOULDERS, 8'' OR 12''
4. EXISTING AGGREGATE SHOULDER, 8''
5. EXISTING AGGREGATE SUBGRADE, 12''
6. EXISTING HMA BINDER/BASE COURSE, ± 13'' TO ± 14¼''
7. EXISTING CONCRETE BASE COURSE, ± 7'' TO ± 9''
8. EXISTING HMA SURFACE/WIDENING, ± 7½'' TO ± 21''
9. EXISTING COMB. CONC. CURB AND GUTTER
10. EXISTING AGGREGATE BASE COURSE, 6''
11. PROPOSED HMA SURFACE REMOVAL, 1¾''
12. PROPOSED HMA SURFACE REMOVAL, 2½''
13. PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX ''E'', N70, 1¾''
14. PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾''
15. PROPOSED HMA BASE COURSE WIDENING, 8½''
16. PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12''
17. PROPOSED HMA SHOULDERS, 8''
    (INCLUDES POLYMERIZED HMA SURFACE COURSE, MIX ''E'', N70, 1¾'' AND HMA BINDER IL-19 mm, 6¼'')
18. PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 3''
19. PROPOSED TOPSOIL EXCAVATION AND PLACEMENT
    (PLACED AT A DEPTH OF 4'')
20. PROPOSED SEEDING, CLASS 2A

R = TO BE REMOVED
(SEE ROADWAY PLANS FOR LOCATIONS OF REMOVAL)

NOTES:

1. THE CONTRACTOR SHALL MILL FIRST PRIOR TO PATCHING BETWEEN STA. 485+91 AND STA. 488+79.

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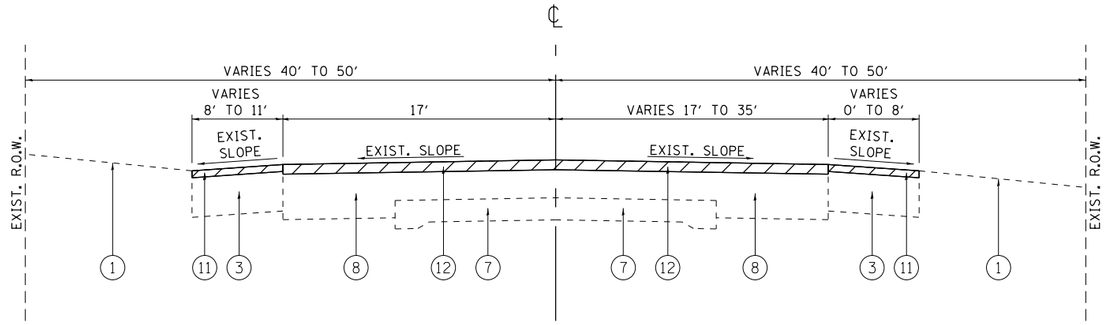
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED TYPICAL SECTIONS
IL 53 AT OLD HICKS RD.

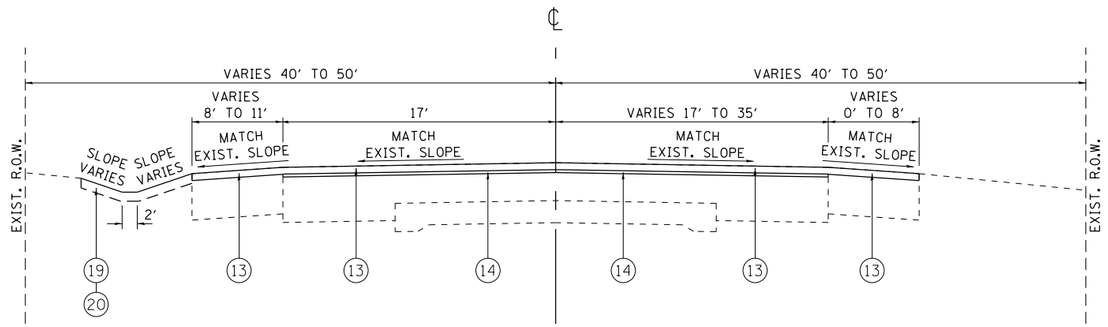
SCALE: | SHEET OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	10
		CONTRACT NO. 62B61		
		ILLINOIS FED. AID PROJECT		



IL 53
EXISTING TYPICAL SECTION

STA. 488+79 TO STA. 495+55



IL 53
PROPOSED TYPICAL SECTION

STA. 488+79 TO STA. 495+55

LEGEND:

(1) EXISTING SOD

(2) EXISTING PCC SHOULDERS, 8″

(3) EXISTING HMA SHOULDERS, 8″ OR 12″

(4) EXISTING AGGREGATE SHOULDER, 8″

(5) EXISTING AGGREGATE SUBGRADE, 12″

(6) EXISTING HMA BINDER/BASE COURSE, ± 13″ TO ± 14¼″

(7) EXISTING CONCRETE BASE COURSE, ± 7″ TO ± 9″

(8) EXISTING HMA SURFACE/WIDENING, ± 7½″ TO ± 21″

(9) EXISTING COMB. CONC. CURB AND GUTTER

(10) EXISTING AGGREGATE BASE COURSE, 6″

(11) PROPOSED HMA SURFACE REMOVAL, 1¾″

(12) PROPOSED HMA SURFACE REMOVAL, 2½″

(13) PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX ″E″, N70, 1¾″

(14) PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾″

(15) PROPOSED HMA BASE COURSE WIDENING, 8½″

(16) PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12″

(17) PROPOSED HMA SHOULDERS, 8″
(INCLUDES POLYMERIZED HMA SURFACE COURSE, MIX ″E″, N70, 1¾″ AND HMA BINDER IL-19 mm, 6¼″)

(18) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 3″

(19) PROPOSED TOPSOIL EXCAVATION AND PLACEMENT
(PLACED AT A DEPTH OF 4″)

(20) PROPOSED SEEDING, CLASS 2A

R = TO BE REMOVED
(SEE ROADWAY PLANS FOR LOCATIONS OF REMOVAL)

NOTES:

1. THE CONTRACTOR SHALL MILL FIRST PRIOR TO PATCHING BETWEEN STA. 488+79 AND STA. 495+55.

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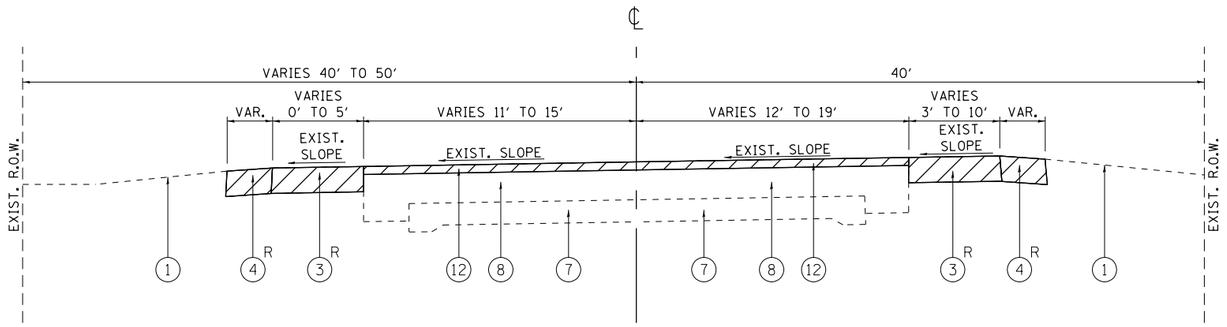
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

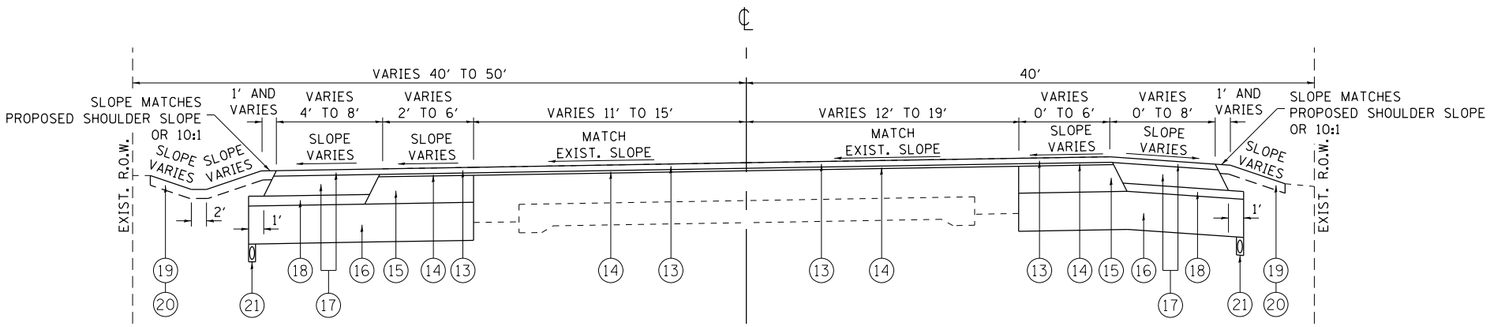
EXISTING AND PROPOSED TYPICAL SECTIONS
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	11
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		



IL 53
EXISTING TYPICAL SECTION
STA. 495+55 TO STA. 512+68



IL 53
PROPOSED TYPICAL SECTION
STA. 495+55 TO STA. 512+68

LEGEND:

(1) EXISTING SOD
(2) EXISTING PCC SHOULDERS, 8″
(3) EXISTING HMA SHOULDERS, 8″ OR 12″
(4) EXISTING AGGREGATE SHOULDER, 8″
(5) EXISTING AGGREGATE SUBGRADE, 12″
(6) EXISTING HMA BINDER/BASE COURSE, ± 13″ TO ± 14¼″
(7) EXISTING CONCRETE BASE COURSE, ± 7″ TO ± 9″
(8) EXISTING HMA SURFACE/WIDENING, ± 7½″ TO ± 21″
(9) EXISTING COMB. CONC. CURB AND GUTTER
(10) EXISTING AGGREGATE BASE COURSE, 6″
(11) PROPOSED HMA SURFACE REMOVAL, 1¾″
(12) PROPOSED HMA SURFACE REMOVAL, 2½″
(13) PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX ″E″, N70, 1¾″
(14) PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾″
(15) PROPOSED HMA BASE COURSE WIDENING, 8½″
(16) PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12″
(17) PROPOSED HMA SHOULDERS, 8″
(INCLUDES POLYMERIZED HMA SURFACE COURSE, MIX ″E″, N70, 1¾″ AND HMA BINDER IL-19 mm, 6¼″)
(18) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 3″
(19) PROPOSED TOPSOIL EXCAVATION AND PLACEMENT
(PLACED AT A DEPTH OF 4″)
(20) PROPOSED SEEDING, CLASS 2A

R = TO BE REMOVED
(SEE ROADWAY PLANS FOR LOCATIONS OF REMOVAL)

NOTES:

1. THE CONTRACTOR SHALL MILL FIRST PRIOR TO PATCHING BETWEEN STA. 495+55 AND STA. 512+68.

2. SEE DRAINAGE PLANS FOR LOCATIONS OF PROPOSED PIPE UNDERDRAINS, TYPE 2, 4″.

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Default	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED TYPICAL SECTIONS
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	12
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

EARTHWORK SCHEDULE

1	2	3	4	5	6	7	8
IL 53	EARTH EXCAVATION (CU. YD.)	EMBANKMENT (CU. YD.)	EARTH EXCAVATION TO BE USED AS EMBANKMENT (ADJUSTED FOR SHRINKAGE (15%)) (CU. YD.)	EARTHWORK BALANCE SURPLUS (+) OR SHORTAGE (-) (CU. YD.)	TOPSOIL EXCAVATION AND PLACEMENT (CU. YD.)	TOPSOIL REQUIRED (CU. YD.)	TOPSOIL BALANCE SURPLUS (+) OR SHORTAGE (-) (CU. YD.)
STAGE I (RT)	823	516	700	184	512	280	232
STAGE II (LT)	2648	355	2251	1896	1020	660	360
TOTAL =	3471	871	2951	2080	1532	940	592

COLUMN 1: LOCATION FROM PLANS

COLUMN 2: CUT QUANTITIES FROM CROSS SECTIONS, WHICH DOES NOT INCLUDE UNSUITABLE MATERIAL OR TOPSOIL

COLUMN 3: FILL QUANTITIES FROM CROSS SECTIONS

COLUMN 4: EARTH EXCAVATION THAT IS TO BE USED AS EMBANKMENT, ADJUSTED FOR SHRINKAGE (SHRINKAGE FACTOR = 15%)

COLUMN 5: COLUMN 4 - COLUMN 3
POSITIVE QUANTITY = EXTRA EXCAVATION
NEGATIVE QUANTITY = FURNISHED EXCAVATION NEEDED

COLUMN 6: TOPSOIL EXCAVATED TO A DEPTH OF 6" IN AREAS THAT SHALL BE DISTURBED BY PROPOSED WORK AND PLACED AT AREAS OF PROPOSED SEEDING AT A DEPTH OF 4".

COLUMN 7: TOPSOIL REQUIRED BASED ON AREA OF PROPOSED SEEDING.

COLUMN 8: COLUMN 6 - COLUMN 7
POSITIVE QUANTITY = EXTRA TOPSOIL
NEGATIVE QUANTITY = TOPSOIL FURNISH AND PLACE NEEDED

PAVED SHOULDER REMOVAL

STATION	TO	STATION	SIDE	PAVED SHOULDER REMOVAL (SQ YD)
484+11	TO	485+91	LEFT	98
495+55	TO	507+98	LEFT	383
497+48	TO	499+85	RIGHT	213
500+08	TO	512+68	RIGHT	486
508+80	TO	512+68	LEFT	208
TOTAL =				1388

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	DRAWN -	REVISED -	
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PLOT DATE = 3/20/2018	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	13
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			

TREE REMOVAL

STATION	OFFSET	TREE REMOVAL (6 TO 15 UNITS DIAMETER) (UNIT)	TREE REMOVAL (OVER 15 UNITS DIAMETER) (UNIT)
486+04	60.2' LT	6	
486+51	50.2' LT	8	
486+70	48.2' LT	6	
486+70	48.2' LT	6	
487+18	50.0' LT	6	
487+19	49.4' LT	8	
487+23	51.4' LT	8	
487+30	47.9' LT		32
487+82	47.0' LT		18
487+83	43.9' LT		40
487+85	42.3' LT		16
488+43	37.4' LT	10	
488+61	38.9' LT	10	
488+76	39.6' LT	10	
489+34	35.0' LT	6	
489+64	33.7' LT	10	
489+76	38.0' LT	10	
489+81	28.2' LT	10	
489+94	29.9' LT		60
490+16	35.9' LT	8	
490+26	28.7' LT	8	
490+29	39.6' LT	8	
490+36	29.7' LT		50
490+49	44.9' LT	10	
490+52	39.0' LT	10	
490+53	46.1' LT	12	
490+71	44.9' LT		16
490+78	43.0' LT	6	
490+80	33.3' LT	8	
490+81	32.5' LT		20
490+87	40.4' LT	12	
491+16	34.9' LT	10	
491+18	49.6' LT	6	
491+39	38.8' LT	10	
491+73	39.0' LT	12	
491+79	43.5' LT	6	
491+80	39.8' LT	10	
491+80	37.1' LT	6	
492+00	46.9' LT	12	
492+01	30.5' LT	6	
492+25	37.5' LT	12	
492+33	35.5' LT	14	
492+43	44.5' LT	10	
492+75	39.1' LT	6	
492+79	44.1' LT	6	
492+95	40.0' LT	6	
492+97	43.2' LT	6	
493+02	46.4' LT	8	

TREE REMOVAL

STATION	OFFSET	TREE REMOVAL (6 TO 15 UNITS DIAMETER) (UNIT)	TREE REMOVAL (OVER 15 UNITS DIAMETER) (UNIT)
493+06	43.2' LT	14	
493+08	47.3' LT		24
493+33	39.0' LT	6	
493+42	46.8' LT	6	
493+49	39.4' LT	6	
493+60	42.5' LT	8	
493+75	37.1' LT		40
493+86	39.2' LT		36
493+98	39.8' LT	6	
494+03	39.4' LT		20
494+23	38.0' LT	10	
494+31	38.2' LT		24
494+41	38.6' LT	8	
494+51	46.2' LT	6	
494+52	43.7' LT	8	
494+53	38.6' LT	10	
494+64	36.0' LT	8	
494+68	38.1' LT		20
494+68	35.9' LT	6	
494+76	39.4' LT	6	
494+93	49.1' LT	6	
494+97	41.6' LT	8	
495+04	43.0' LT	10	
495+11	43.8' LT	12	
495+12	47.5' LT	6	
495+15	59.6' LT	10	
495+16	56.3' LT	6	
495+17	64.0' LT	6	
495+71	42.4' LT	12	
495+71	46.2' LT	10	
495+73	43.3' LT	6	
495+93	43.3' LT		16
496+23	41.2' LT		16
496+26	46.4' LT	6	
496+57	40.8' LT	10	
496+65	42.0' LT		16
496+81	42.0' LT	14	
497+25	33.1' LT	14	
497+36	40.3' LT		16
497+47	42.3' LT		32
497+75	42.2' LT		40
497+85	34.0' LT	10	
497+91	41.9' LT		18
497+92	34.6' LT	12	
497+94	40.8' LT	8	
498+23	43.1' LT		22
498+53	42.8' LT	8	
498+64	40.3' LT	8	

TREE REMOVAL

STATION	OFFSET	TREE REMOVAL (6 TO 15 UNITS DIAMETER) (UNIT)	TREE REMOVAL (OVER 15 UNITS DIAMETER) (UNIT)
498+69	43.0' LT		30
498+78	40.5' LT	6	
498+81	42.9' LT	12	
498+82	39.2' LT	10	
498+88	41.4' LT	8	
499+02	43.7' LT		24
499+48	41.9' LT	8	
499+66	51.8' RT	10	
499+67	39.3' LT	6	
499+70	45.0' RT	7	
499+70	50.0' RT	15	
499+71	42.0' LT	9	
500+25	45.0' RT	12	
501+52	30.0' LT	11	
501+96	28.0' LT	8	
502+07	38.0' RT	15	
502+15	40.0' LT		32
502+15	31.0' RT	14	
502+16	25.0' LT	9	
502+25	39.0' RT	7	
502+35	27.0' RT	8	
502+40	28.0' LT	10	
502+45	35.0' RT	14	
502+50	30.0' RT	8	
502+55	38.0' LT	11	
502+62	30.0' RT	12	
502+62	30.0' RT	11	
502+75	30.0' RT	11	
502+85	29.8' RT	12	
502+90	27.3' LT	7	
502+97	27.7' RT	10	
503+08	26.2' RT	12	
503+10	27.7' RT	12	
503+10	27.7' RT	8	
503+12	38.3' LT	12	
503+23	26.7' RT	13	
503+34	35.2' LT	9	
503+34	35.2' LT	8	
503+35	23.8' RT	8	
503+54	37.0' LT		18
503+59	38.9' RT	8	
503+65	28.2' LT	6	
503+73	24.8' RT	8	
503+73	24.8' RT	8	
503+76	28.3' LT		20
503+81	36.5' LT	14	
503+86	48.3' LT	8	
503+91	38.2' RT	8	

TREE REMOVAL

STATION	OFFSET	TREE REMOVAL (6 TO 15 UNITS DIAMETER) (UNIT)	TREE REMOVAL (OVER 15 UNITS DIAMETER) (UNIT)
503+95	45.2' LT	10	
503+95	37.7' RT		38
504+02	37.7' RT	12	
504+02	37.7' RT	10	
504+03	30.3' LT	9	
504+10	34.0' RT	12	
504+11	35.2' LT	12	
504+12	37.8' RT	6	
504+19	37.9' RT	6	
504+23	37.7' RT	10	
504+25	26.8' RT	8	
504+25	32.2' LT	7	
504+31	46.1' LT	6	
504+42	38.0' LT	15	
504+42	38.0' LT	8	
504+42	38.0' LT	8	
504+44	46.3' LT		18
504+50	40.1' LT	8	
504+50	33.2' LT	8	
504+75	37.3' LT	8	
504+97	35.3' LT		17
505+02	31.3' LT	8	
505+03	37.2' LT		18
505+13	38.2' LT	14	
505+44	38.2' LT	13	
505+44	38.2' LT	10	
505+44	38.2' LT	9	
505+50	32.3' LT	14	
505+50	32.3' LT	11	
505+50	32.3' LT	10	
506+16	27.8' LT		48
506+98	35.0' LT	6	
507+04	36.9' RT	12	
507+15	37.8' RT	11	
507+49	24.7' RT	6	
507+80	32.1' LT	14	
508+01	38.2' LT	7	
508+02	32.0' RT		36
508+12	50.8' LT	8	
509+07	38.9' LT		20
509+48	37.1' LT	9	
509+48	37.1' LT	9	
509+48	37.1' LT	8	
509+85	39.4' LT	10	
509+98	38.3' LT	12	
509+98	38.3' LT	10	
510+99	28.0' RT	7	
TOTAL =		1436	911

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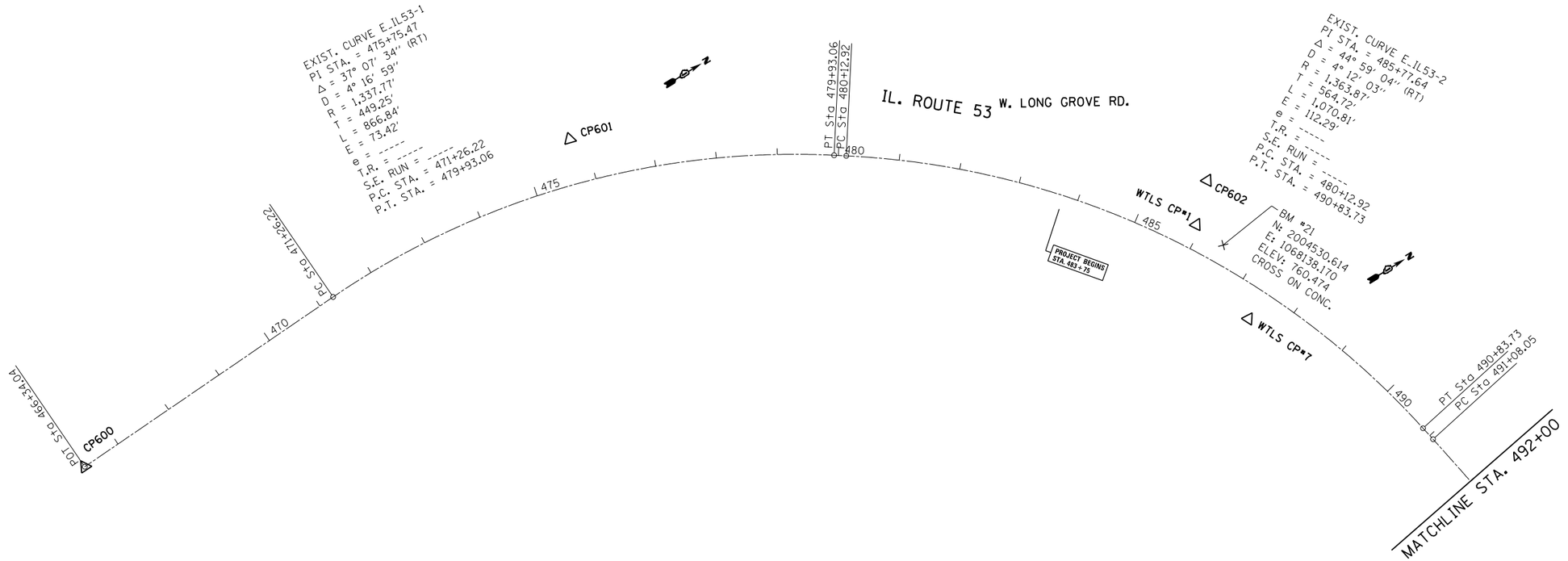
PLOT DATE = 3/20/2018	DATE -	REVISED -	

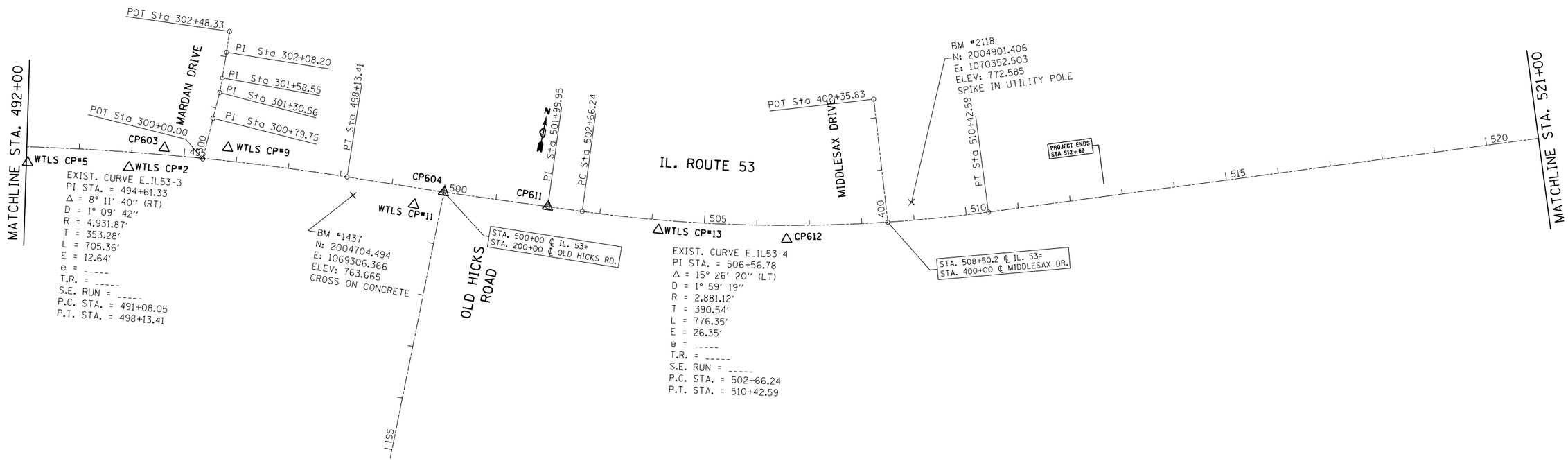
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	14
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		





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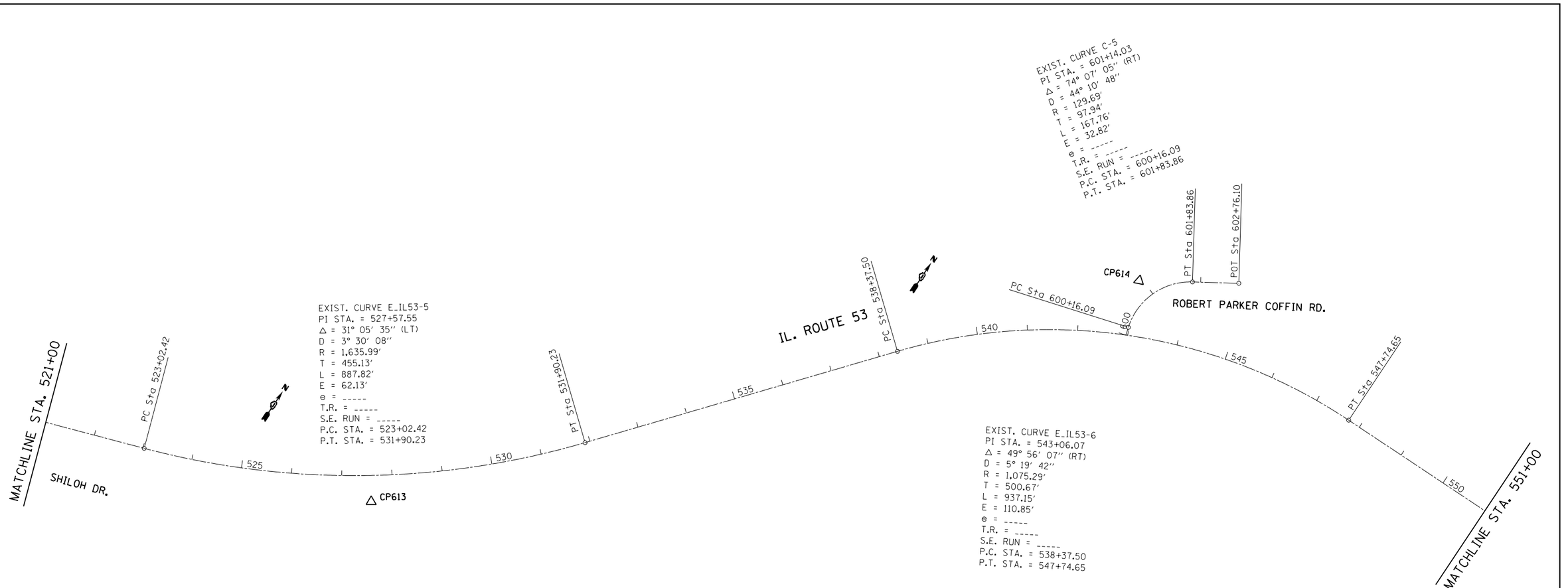
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS PLAN
IL. 53 AT OLD HICKS RD.

SCALE: 1"=100' | SHEET OF SHEETS | STA. 485+00 | TO STA. 499+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	53ON-3	LAKE	80	15
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			



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	PLOT SCALE = 200.0000 ' / in.	CHECKED -	REVISED -			1261	530N-3	LAKE	80	16
	PLOT DATE = 3/20/2018	DATE -	REVISED -					CONTRACT NO. 62B61		
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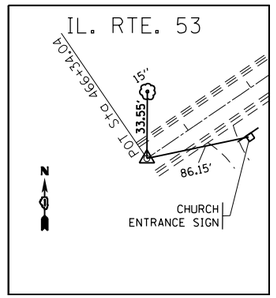
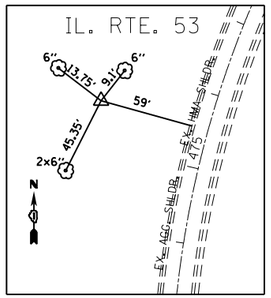


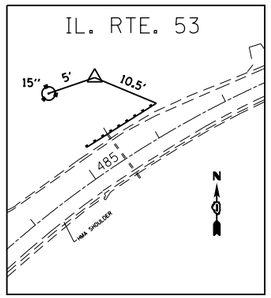
CONTROL POINT #600

MAGNETIC NAIL AT P.O.T. ℄ OF IL. RTE. 53
STA. 466+34.04,
N=2002754.5000
E=1067470.4904



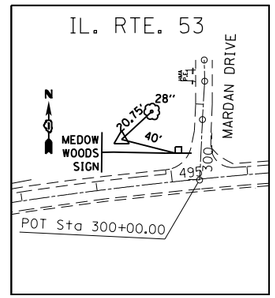
CONTROL POINT #601

IRON ROD WITH CAP P.I. (CURVE #1) OF IL RTE. 53
STA. 475+75.47, 73.42' LT.
N=2003246.1181
E=1067447.0292



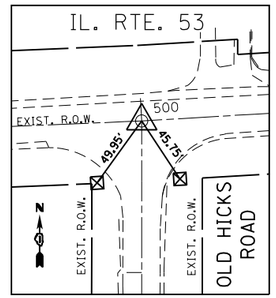
CONTROL POINT #602

IRON ROD WITH CAP P.I. (CURVE #2) OF IL RTE. 53
STA. 485+75.64, 112.29' LT.
N=2003694.8581
E=1067425.6086



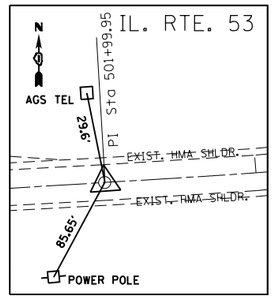
CONTROL POINT #603

MAGNETIC NAIL AT P.I. (CURVE #3) OF IL. RTE. 53
STA. 494+61.33, 12.64' LT.
N=2004065.5692
E=107679.3794



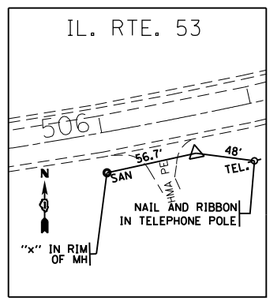
CONTROL POINT #604

MAGNETIC NAIL AT P.O.T. ℄ OF IL. RTE. 53
AND ℄ OF OLD HICKS RD.
STA. 500+00.00
N=2004744.5070
E=1069475.1528



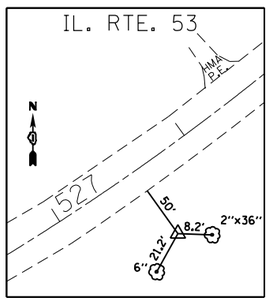
CONTROL POINT #611

MAGNETIC NAIL AT P.O.T. ℄ OF IL. RTE. 53
STA. 502+00.00
N=2004756.5400
E=1069674.7905



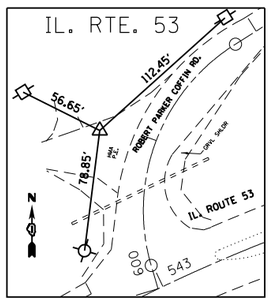
CONTROL POINT #612

IRON ROD WITH CAP P.I. (CURVE #4) OF IL RTE. 53
STA. 506+56.79, 26.35' RT.
N=2004784.9328
E=1070130.6920



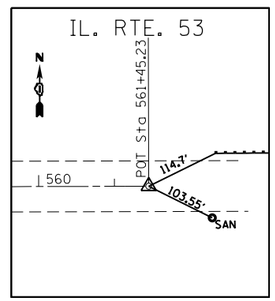
CONTROL POINT #613

IRON ROD WITH CAP P.I. (CURVE #5) OF IL RTE. 53
STA. 527+57.55, 52.13' RT.
N=2005470.5080
E=1072121.4482



CONTROL POINT #614

MAGNETIC NAIL AT P.I. (CURVE #6) OF IL. RTE. 53
NEAR ROBERT PARKER COFFIN RD.
STA. 543+06.07, 110.85. LT.
N=2006700.2548
E=1073149.8288



CONTROL POINT #615

MAGNETIC NAIL AT P.O.T. ℄ OF IL. RTE. 53
STA. 561+45.23
N=2006705.5018
E=1075021.0733

BENCH MARKS

BENCH MARK #1 — 6" SPIKE IN POWER POLE ON EAST SIDE OF ROUTE 53 ±200' SOUTH OF OLD HICKS RD. ELEVATION 764.84

BENCH MARK #2 — 6" SPIKE IN POWER POLE AT SOUTHEAST CORNER OF OLD HICKS RD. AND SHILOH RD. ELEVATION 765.682

BENCH MARK #3 — RAILROAD SPIKE IN POWER POLE ±400' EAST OF ROBERT PARKER COFFIN RD. ON NORTH SIDE OF IL. ROUTE 53 ELEVATION 731.363

BENCH MARK #4 — RAILROAD SPIKE IN POWER POLE, STATION 482+10/ 46' RT. ±120' SOUTH OF LONG GROVE RD., EAST SIDE OF IL. RTE. 53 ELEVATION 753.86

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Default	PLOT DATE = 3/20/2018	DATE -	REVISED -

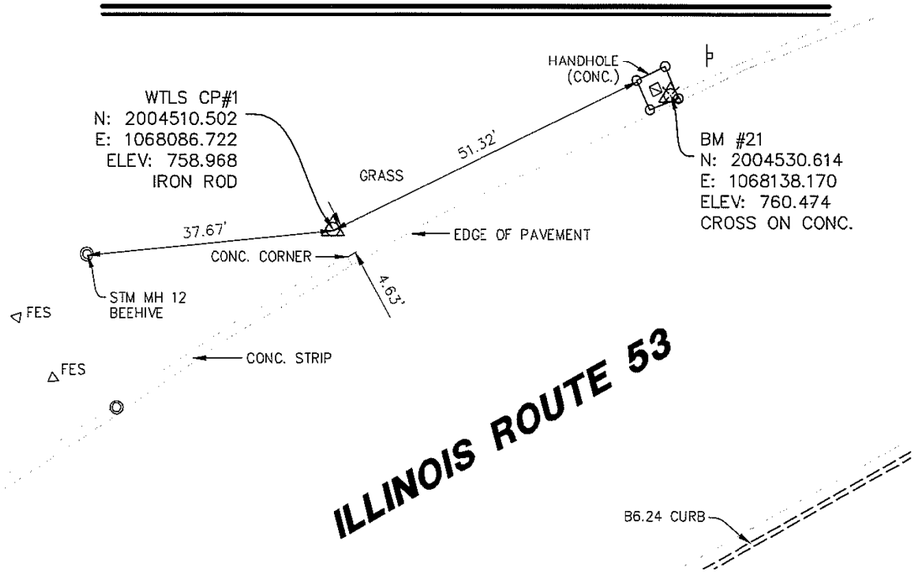
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

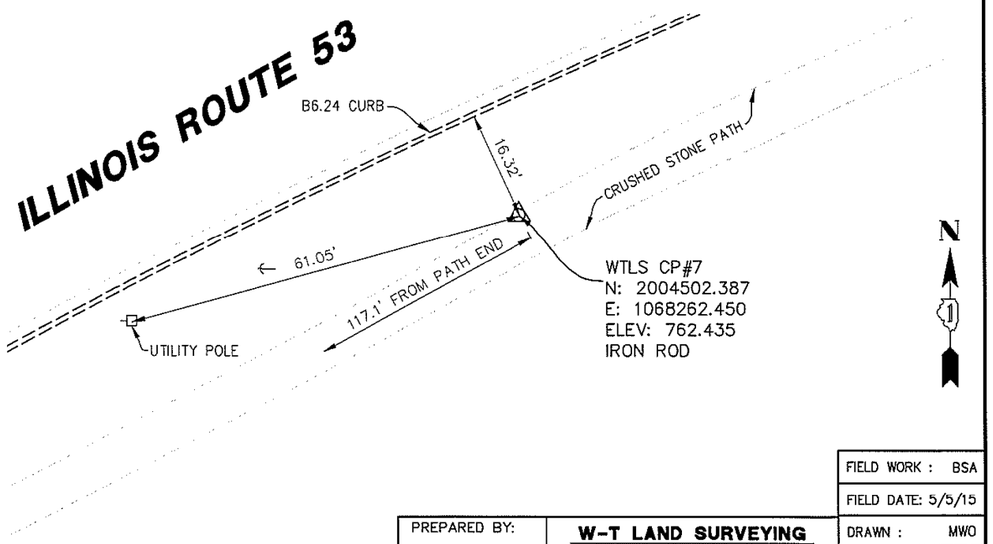
ALIGNMENT, TIES AND BENCHMARKS PLAN
IL. 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. 485+00 | TO STA. 499+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	53ON-3	LAKE	80	17
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

CONTROL POINT TIE SHEET





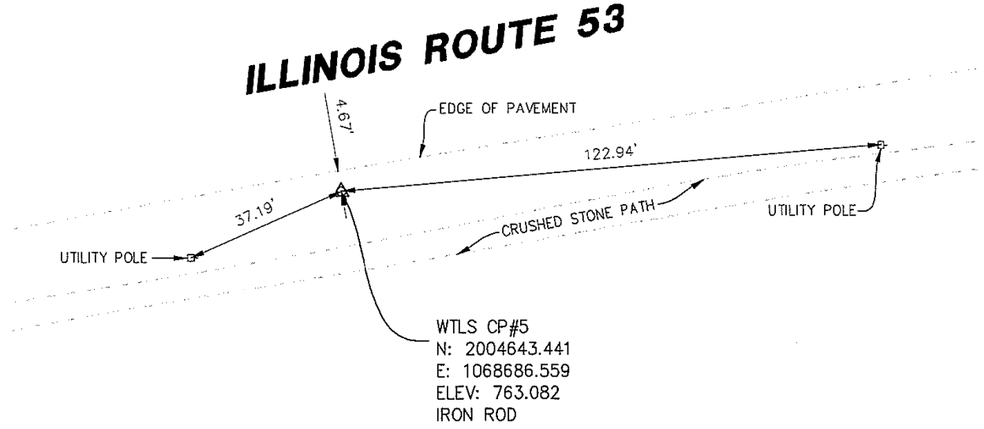
PREPARED FOR: ILLINOIS DEPARTMENT OF TRANSPORTATION
ILLINOIS ROUTE 53 @ OLD HICKS (LAKE COUNTY)
JOB NUMBER: P-91-451-09

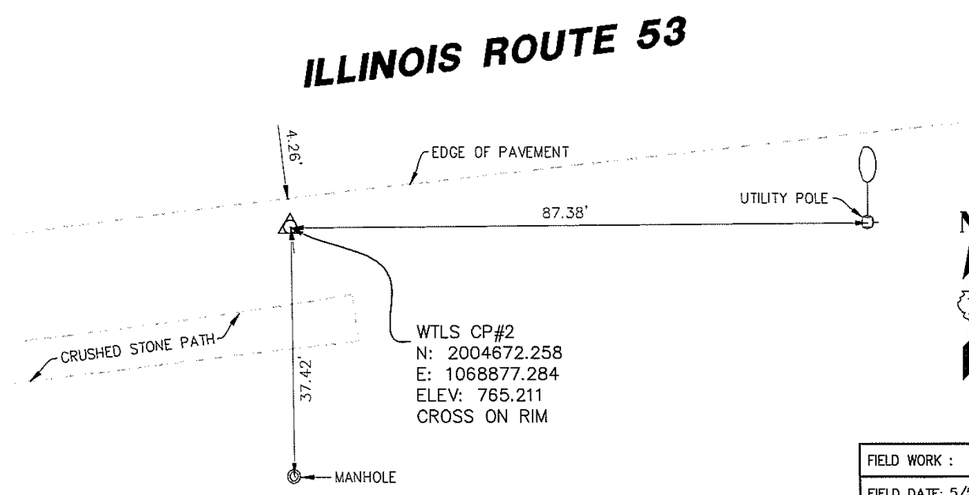
PREPARED BY:

W-T LAND SURVEYING
LAND AND CONSTRUCTION SURVEYORS
2675 Pratum Avenue
Hoffman Estates, Illinois 60192
PH: (224) 293-6333 FAX: (224) 293-6444
www.wtengineering.com
IL. LICENSE NO.: 184.007570-0015 EXP: 04/30/19

FIELD WORK :	BSA
FIELD DATE:	5/5/15
DRAWN :	MWO
JOB :	S17096
SHEET	TIE-1
OF	FOUR SHEETS

CONTROL POINT TIE SHEET





PREPARED FOR: ILLINOIS DEPARTMENT OF TRANSPORTATION
ILLINOIS ROUTE 53 @ OLD HICKS (LAKE COUNTY)
JOB NUMBER: P-91-451-09

PREPARED BY:

W-T LAND SURVEYING
LAND AND CONSTRUCTION SURVEYORS
2675 Pratum Avenue
Hoffman Estates, Illinois 60192
PH: (224) 293-6333 FAX: (224) 293-6444
www.wtengineering.com
IL. LICENSE NO.: 184.007570-0015 EXP: 04/30/19

FIELD WORK :	BSA
FIELD DATE:	5/5/15
DRAWN :	MWO
JOB :	S17096
SHEET	TIE-2
OF	FOUR SHEETS

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Default	PLOT DATE = 3/20/2018	DATE -	REVISED -

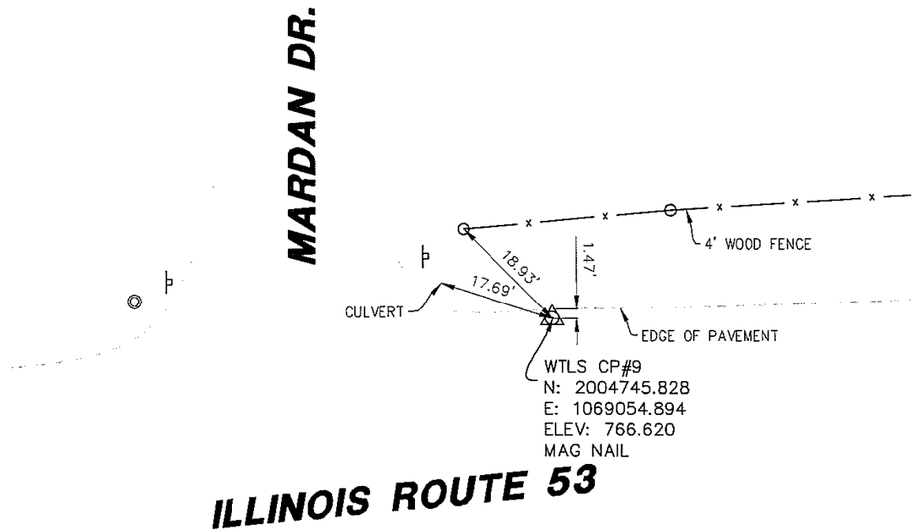
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

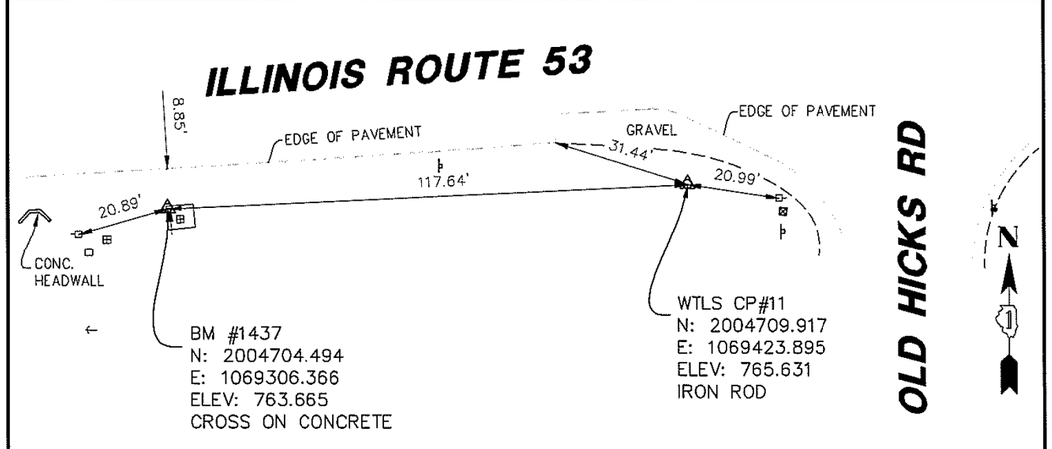
ALIGNMENT, TIES AND BENCHMARKS PLAN
IL. 53 AT OLD HICKS RD.

SCALE: NONE | SHEET OF SHEETS | STA. 485+00 | TO STA. 499+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	53ON-3	LAKE	80	18
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

CONTROL POINT TIE SHEET





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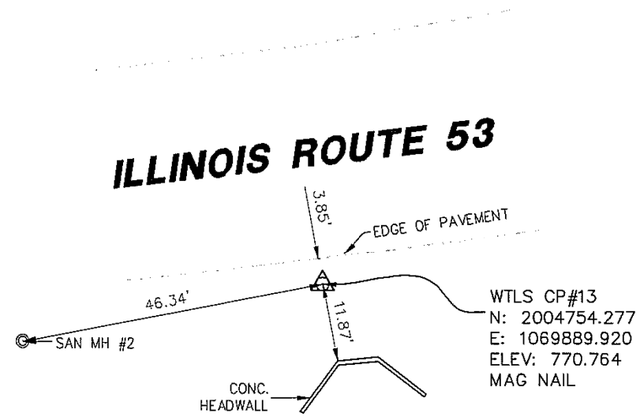
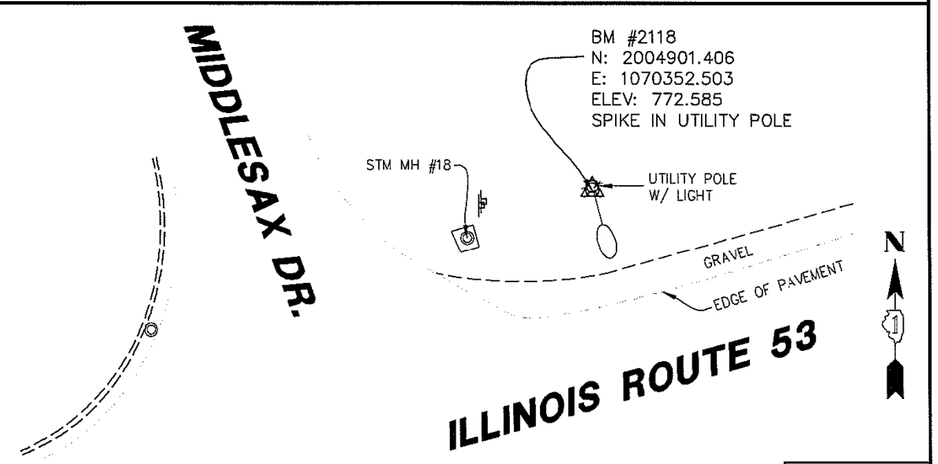
DRAWN :	MWO
JOB :	S17096
SHEET	TIE-3
OF	FOUR SHEETS

CONTROL POINT TIE SHEET





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ILLINOIS ROUTE 53 @ OLD HICKS (LAKE COUNTY)
JOB NUMBER: P-91-451-09

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FIELD WORK :	BSA
FIELD DATE:	5/5/15
DRAWN :	MWO
JOB :	S17096
SHEET	TIE-4
OF	FOUR SHEETS

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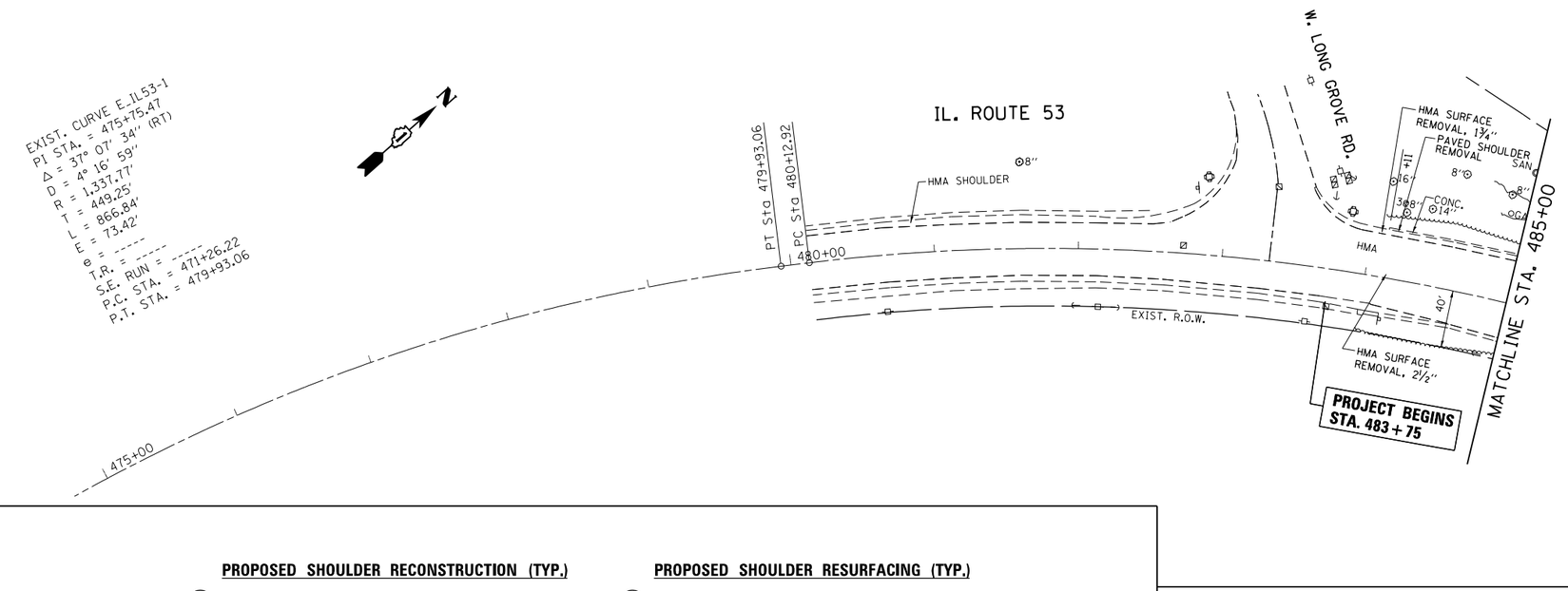
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS PLAN
IL. 53 AT OLD HICKS RD.

SCALE: NONE | SHEET OF SHEETS | STA. 485+00 | TO STA. 499+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	53ON-3	LAKE	80	19
		CONTRACT NO. 62B61		
		ILLINOIS	FED. AID PROJECT	



EXISTING

LEGEND

PROPOSED PAVEMENT WIDENING (TYP.)

(1) PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1¾"
PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾"
PROPOSED HMA BASE COURSE WIDENING, 8½"
PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"

PROPOSED PAVEMENT RESURFACING (TYP.)

(2) PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1¾"
PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾"

PROPOSED SHOULDER RECONSTRUCTION (TYP.)

(3) PROPOSED HMA SHOULDERS, 8"
(INCLUDES POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1¾" AND HMA BINDER IL-19 mm, 6¼")
PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 3"
PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"

PROPOSED SHOULDER WIDENING / REPLACEMENT (TYP.)

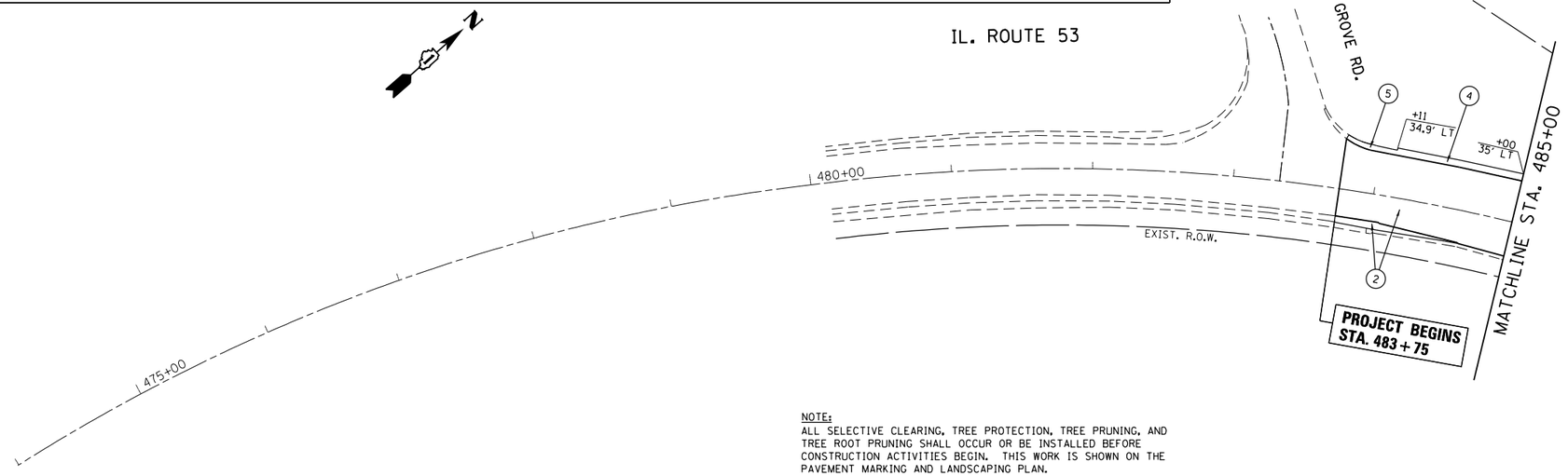
(4) PROPOSED HMA SHOULDERS, 8"
(INCLUDES POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1¾" AND HMA BINDER IL-19 mm, 6¼")

PROPOSED SHOULDER RESURFACING (TYP.)

(5) PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1¾"

PROPOSED DRIVEWAYS (TYP.)

(6) PROPOSED HMA SURFACE COURSE, MIX "D", N50, 2"
PROPOSED HMA BASE COURSE, 6"



PROPOSED

NOTE:
ALL SELECTIVE CLEARING, TREE PROTECTION, TREE PRUNING, AND TREE ROOT PRUNING SHALL OCCUR OR BE INSTALLED BEFORE CONSTRUCTION ACTIVITIES BEGIN. THIS WORK IS SHOWN ON THE PAVEMENT MARKING AND LANDSCAPING PLAN.

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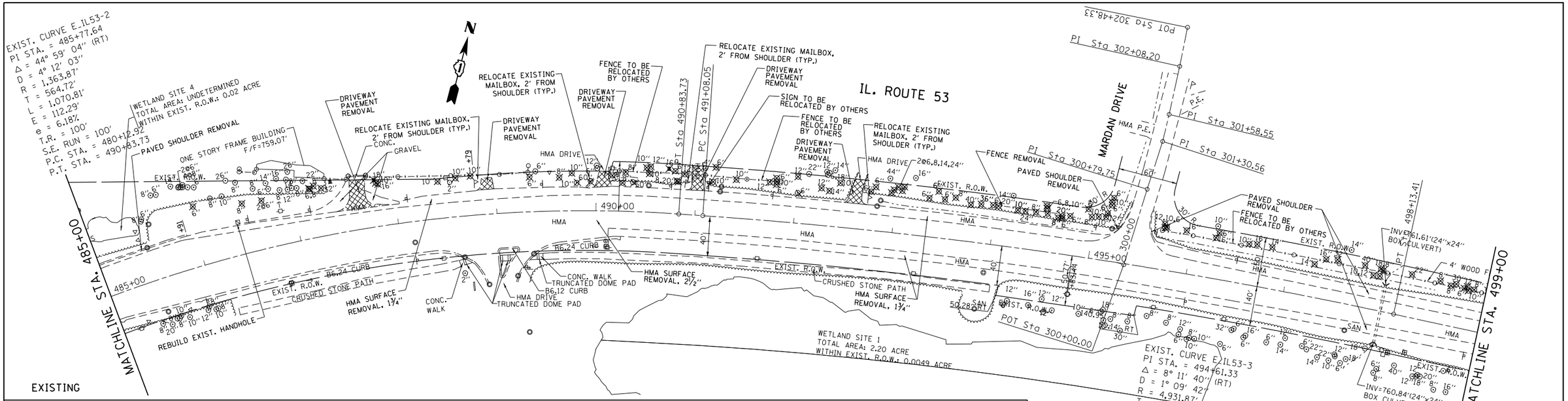
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PLAN
IL. 53 AT OLD HICKS RD.

SCALE: 1"=50' | SHEET OF SHEETS | STA. 485+00 TO STA. 499+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	53ON-3	LAKE	80	20
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			



LEGEND

PROPOSED PAVEMENT WIDENING (TYP.)

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PROPOSED HMA BASE COURSE WIDENING, 8½"
PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"

PROPOSED PAVEMENT RESURFACING (TYP.)

(2) PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1¾"
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PROPOSED SHOULDER RECONSTRUCTION (TYP.)

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(INCLUDES POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1¾" AND HMA BINDER IL-19 mm, 6¼")
PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 3"
PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"

PROPOSED SHOULDER WIDENING / REPLACEMENT (TYP.)

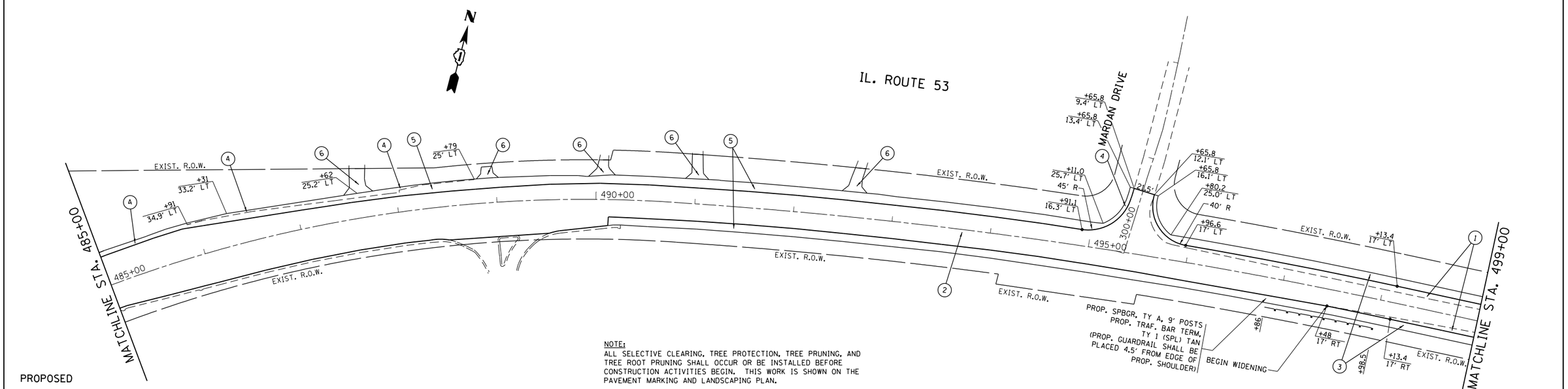
(4) PROPOSED HMA SHOULDERS, 8"
(INCLUDES POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1¾" AND HMA BINDER IL-19 mm, 6¼")

PROPOSED SHOULDER RESURFACING (TYP.)

(5) PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1¾"

PROPOSED DRIVEWAYS (TYP.)

(6) PROPOSED HMA SURFACE COURSE, MIX "D", N50, 2"
PROPOSED HMA BASE COURSE, 6"



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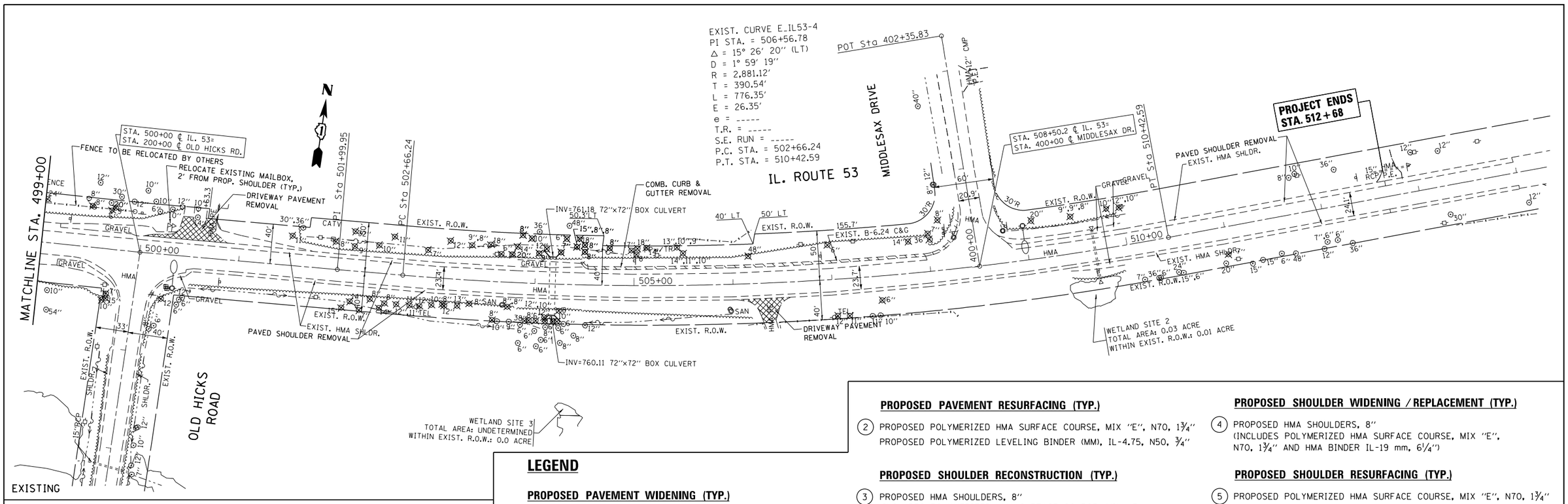
Default	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PLAN
IL. 53 AT OLD HICKS RD.

SCALE: 1"=50' | SHEET OF SHEETS | STA. 485+00 TO STA. 499+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	53ON-3	LAKE	80	21
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			



LEGEND

PROPOSED PAVEMENT WIDENING (TYP.)

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PROPOSED HMA BASE COURSE WIDENING, 8½"
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(2) PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1¾"
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PROPOSED SHOULDER RECONSTRUCTION (TYP.)

(3) PROPOSED HMA SHOULDERS, 8"
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PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 3"
PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"

PROPOSED SHOULDER WIDENING / REPLACEMENT (TYP.)

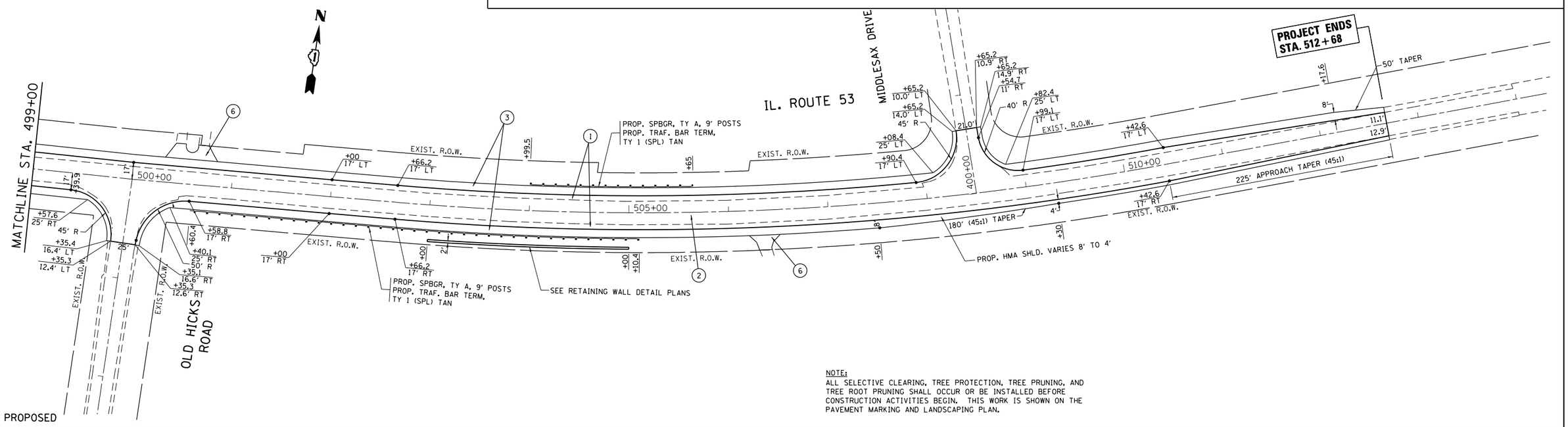
(4) PROPOSED HMA SHOULDERS, 8"
(INCLUDES POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1¾" AND HMA BINDER IL-19 mm, 6¼")

PROPOSED SHOULDER RESURFACING (TYP.)

(5) PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1¾"

PROPOSED DRIVEWAYS (TYP.)

(6) PROPOSED HMA SURFACE COURSE, MIX "D", N50, 2"
PROPOSED HMA BASE COURSE, 6"



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PLAN
IL. 53 AT OLD HICKS RD.

SCALE: 1"=50' | SHEET OF SHEETS | STA. 499+00 TO STA. 512+67.69

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	53ON-3	LAKE	80	22
		CONTRACT NO. 62B61		
		ILLINOIS FED. AID PROJECT		

PLAN

DATE	BY

SURVEYED
PLOTTED
DRAWN
CHECKED
R/W NOTED
STRUCTURE NO.

NOTE BOOK
NO.

PROJECT BEGINS
STA. 483 + 75

EXISTING AND PROPOSED ℄ PROFILE IL. RTE. 53

765
760
755
750
745
740
735
730

750.94 752.15 753.72 755.44 757.18

481+00 482+00 483+00 484+00 485+00

PROFILE

DATE	BY

SURVEYED
PLOTTED
DRAWN
CHECKED
R/W NOTED
STRUCTURE NO.

NOTE BOOK
NO.

EXISTING AND PROPOSED ℄ PROFILE IL. RTE. 53

780
775
770
765
760
755
750
745

757.18 758.80 760.23 761.56 762.62 763.56 764.41 765.13 765.73 766.16 766.46 766.51 766.46 766.41 766.48

485+00 486+00 487+00 488+00 489+00 490+00 491+00 492+00 493+00 494+00 495+00 496+00 497+00 498+00 499+00

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PROFILE
IL. 53 AT OLD HICKS RD.

1"=50' H
SCALE: 1"=10' V | SHEET OF SHEETS | STA. 485+00 TO STA. 512+67.69

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	23
		CONTRACT NO. 62B61		
		ILLINOIS FED. AID PROJECT		

PLAN

DATE	BY

SURVEYED
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ALIGNMENT CHECKED
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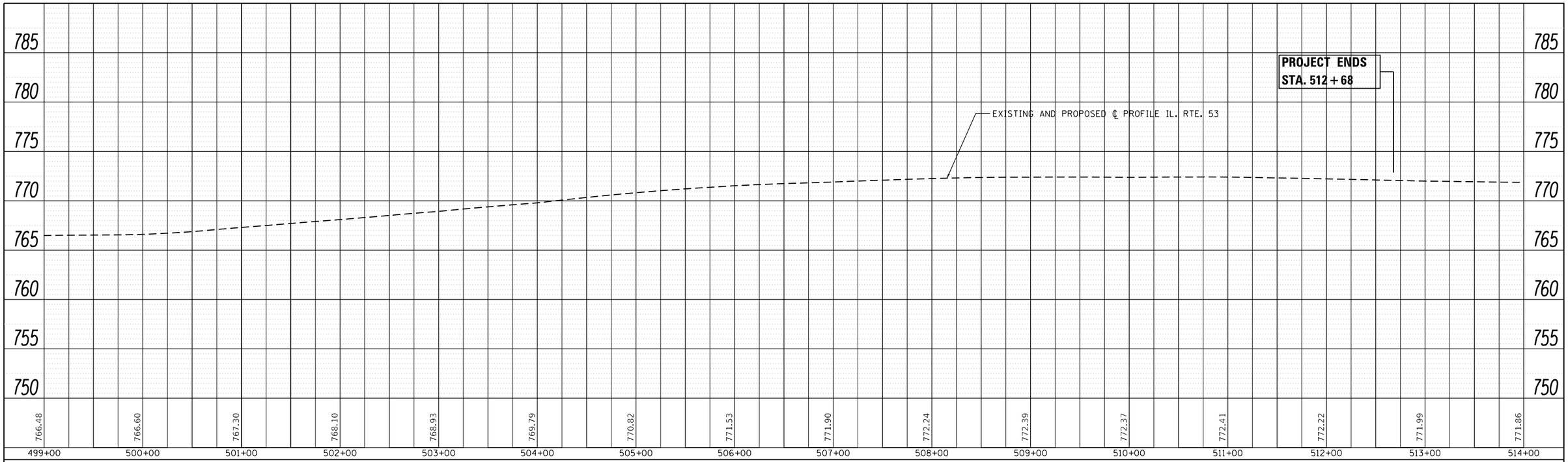
CADD FILE NAME

NOTE BOOK
NO.



PROFILE

DATE	BY

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NOTE BOOK
NO.

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STATE OF ILLINOIS
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EXISTING AND PROPOSED ROADWAY PROFILE
IL. 53 AT OLD HICKS RD.

1"=50' H
SCALE: 1"=10' V | SHEET OF SHEETS | STA. 485+00 TO STA. 512+67.69

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	24
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

MAINTENANCE OF TRAFFIC NOTES

- THE STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS SHALL SERVE AS A GUIDE FOR THE SAFE DIVERSION OF TRAFFIC DURING THE EXECUTION OF THIS CONTRACT. ANY CHANGES TO THE PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
- THE CONTRACTOR SHALL MAINTAIN TRAFFIC IN ACCORDANCE WITH THE SPECIAL PROVISIONS, STATE STANDARDS, STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.
- THE FURNISHING, INSTALLING, AND RELOCATION OF ALL TRAFFIC SIGNS SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND THE STANDARD SPECIFICATIONS.
- ACCESS TO PROPERTIES SHALL BE MAINTAINED AT ALL TIMES. WHEN A DRIVEWAY MUST BE CLOSED TEMPORARILY FOR CONSTRUCTION OF THE DRIVEWAY APRON, PROPERTIES WITH MULTIPLE ENTRANCES SHALL HAVE ONLY ONE ENTRANCE CLOSED AT A TIME.
- ALL EXISTING PAVEMENT MARKINGS IN CONFLICT WITH THE PAVEMENT MARKING TAPE, TYPE IV USED FOR STAGING SHALL BE REMOVED. THIS WORK SHALL BE PAID FOR AS "PAVEMENT MARKING REMOVAL - GRINDING" WITHIN THE PROJECT LIMITS AND "PAVEMENT MARKING REMOVAL - WATER BLASTING" OUTSIDE THE PROJECT LIMITS.
- ALL EXISTING SIGNS WITHIN THE LIMITS OF THE MAINTENANCE OF TRAFFIC WHICH ARE OBSCURED BY OR OTHERWISE INTERFERED WITH BY THE CONSTRUCTION OPERATIONS AND MAINTENANCE OF TRAFFIC, SHALL BE COVERED OR REMOVED BY THE CONTRACTOR UNLESS SPECIFIED IN THE PLANS OR WHEN DIRECTED BY THE ENGINEER. THIS WORK SHALL BE IN ACCORDANCE WITH ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

CONSTRUCTION SEQUENCE

PRE-STAGE:

APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL AND PROTECTION FOR DAY-TIME LANE CLOSURES SHALL BE UTILIZED TO PERFORM THE WORK LISTED BELOW. LANE CLOSURES REQUIRED TO COMPLETE PRE-STAGE WORK SHALL ONLY BE UTILIZED DURING ALLOWABLE HOURS AS SPECIFIED IN THE SPECIAL PROVISION 'KEEPING ARTERIAL ROADWAYS OPEN TO TRAFFIC (LANE CLOSURES ONLY)'.

- CONSTRUCTION OF SECTIONS OF PROPOSED PIPE CULVERT, DRAINAGE STRUCTURES AND ASSOCIATED PAVEMENT PATCHING (SEE PRE-STAGE PLAN FOR LOCATIONS)
- PROPOSED PAVEMENT PATCHING FOR SHOULDER REHABILITATION (SEE PRE-STAGE PLAN FOR LOCATION)
- RELOCATION OF UTILITIES

STAGE I:

TRAVEL LANES EAST OF MARDAN DRIVE SHALL BE SHIFTED TO THE NORTH WHILE MAINTAINING ONE LANE IN EACH DIRECTION. TRAVEL LANES WEST OF MARDAN DRIVE SHALL FOLLOW THE EXISTING TRAVEL LANE CONFIGURATION.

11 FOOT WIDE TRAVEL LANES SHALL BE MAINTAINED AT ALL TIMES. EXISTING PAVEMENT MARKINGS CONFLICTING WITH THE PAVEMENT MARKING TAPE, TYPE IV FOR STAGE I SHALL BE REMOVED. THIS WORK SHALL BE PAID FOR AS "PAVEMENT MARKING REMOVAL - GRINDING" OR "PAVEMENT MARKING REMOVAL - WATER BLASTING". PROPOSED PAVEMENT MARKING TAPE, TYPE IV SHALL BE PLACED ACCORDING TO THE SUGGESTED STAGE I MAINTENANCE OF TRAFFIC PLAN OR AS DIRECTED BY THE ENGINEER.

ALL PROPOSED WORK SHALL BE EXECUTED WITHIN THE WORK AREA. THIS INCLUDES, BUT IS NOT LIMITED TO:

- REMOVAL OF PAVED SHOULDER AND THE DRIVEWAY AT STA. 506+35 ALONG THE SOUTH SIDE OF IL 53
- INSTALLATION OF THE PIPE CULVERT AND FLARED END SECTIONS AT THE DRIVEWAY AT STA. 506+35 AND CULVERT/RETAINING WALL WORK AT STA. 504+18 ALONG THE SOUTH SIDE OF IL 53
- CONSTRUCTION OF PAVEMENT, SHOULDER, AND THE DRIVEWAY AT STA. 506+35 ALONG THE SOUTH SIDE OF IL 53 (ALL HMA SHALL BE INSTALLED TO THE BASE/BINDER COURSE ONLY; THE SURFACE COURSE AND LEVELING BINDER (PAVEMENT ONLY) IS TO BE INSTALLED DURING STAGE III)
- INSTALLATION OF GUARDRAIL ALONG THE SOUTH SIDE OF IL 53

STAGE II:

TRAVEL LANES EAST OF MARDAN DRIVE SHALL BE SHIFTED TO THE SOUTH WHILE MAINTAINING ONE LANE IN EACH DIRECTION. TRAVEL LANES WEST OF MARDAN DRIVE SHALL FOLLOW THE EXISTING TRAVEL LANE CONFIGURATION. DAY-TIME LANE CLOSURES SHALL BE UTILIZED WEST OF MARDAN DRIVE TO COMPLETE PROPOSED WORK.

10 FOOT WIDE TRAVEL LANES SHALL BE MAINTAINED AT ALL TIMES. PAVEMENT MARKING TAPE, TYPE IV FOR STAGE I CONFLICTING WITH THE PAVEMENT MARKING TAPE, TYPE IV FOR STAGE II SHALL BE REMOVED. THIS WORK SHALL BE PAID FOR AS "TEMPORARY PAVEMENT MARKING REMOVAL". PROPOSED PAVEMENT MARKING TAPE, TYPE IV SHALL BE PLACED ACCORDING TO THE SUGGESTED STAGE II MAINTENANCE OF TRAFFIC PLAN OR AS DIRECTED BY THE ENGINEER.

ALL PROPOSED WORK SHALL BE EXECUTED WITHIN THE WORK AREA. THIS INCLUDES, BUT IS NOT LIMITED TO:

- REMOVAL OF PAVED SHOULDER, CURB & GUTTER, DRIVEWAYS, PIPE CULVERTS, AND DRAINAGE STRUCTURES ALONG THE NORTH SIDE OF IL 53
- INSTALLATION OF PIPE CULVERTS, STORM SEWERS, DRAINAGE STRUCTURES, FLARED END SECTIONS ALONG THE NORTH SIDE OF IL 53
- CULVERT WORK AT STA. 498+00 AND STA. 504+18 ALONG THE NORTH SIDE OF IL 53
- CONSTRUCTION OF PAVEMENT, SHOULDER, AND DRIVEWAYS ALONG THE NORTH SIDE OF IL 53 (ALL HMA SHALL BE INSTALLED TO THE BASE/BINDER COURSE ONLY; THE SURFACE COURSE AND LEVELING BINDER (PAVEMENT ONLY) IS TO BE INSTALLED DURING STAGE III)
- INSTALLATION OF GUARDRAIL ALONG THE NORTH SIDE OF IL 53

STAGE III (NOT SHOWN ON MAINTENANCE OF TRAFFIC PLAN SHEETS):

PAVEMENT MARKING TAPE, TYPE IV FOR STAGE II SHALL BE REMOVED PRIOR TO THE MILLING OF THE EXISTING PAVEMENT. THIS WORK SHALL BE PAID FOR AS "TEMPORARY PAVEMENT MARKING REMOVAL". THEREAFTER, SHORT TERM PAVEMENT MARKINGS SHALL BE USED AND PLACED ACCORDING TO THE FINAL PAVEMENT MARKING PLAN OR AS DIRECTED BY THE ENGINEER.

ALL REMAINING PROPOSED WORK SHALL BE COMPLETED DURING THIS STAGE. THIS INCLUDES, BUT IS NOT LIMITED TO:

- MILLING OF HMA SURFACE COURSE ON EXISTING PAVEMENT WITHIN THE ENTIRE PROJECT LIMITS
- PLACEMENT OF HMA SURFACE COURSE AND LEVELING BINDER (PAVEMENT ONLY) WITHIN THE ENTIRE PROJECT LIMITS
- INSTALLATION OF PROPOSED LANDSCAPING THROUGHOUT PROJECT LIMITS
- INSTALLATION OF PROPOSED ROADWAY SIGNS
- INSTALLATION OF PROPOSED THERMOPLASTIC PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS

USER NAME = tarikjfm	DESIGNED -	REVISED -	
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PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 3/20/2018	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINT. OF TRAFFIC NOTES AND SUGGESTED CONSTRUCTION SEQUENCE
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

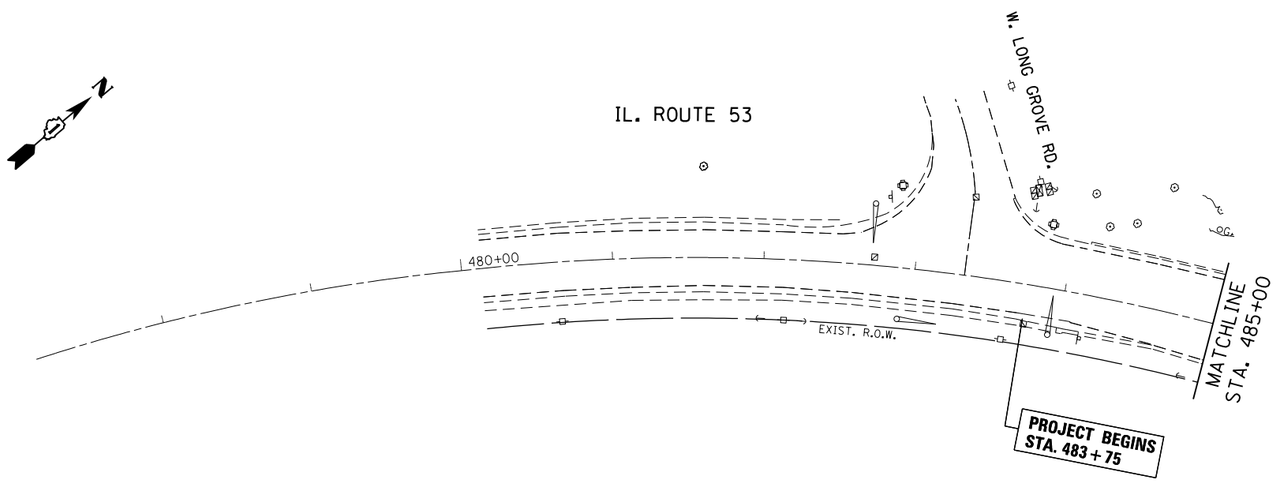
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	25
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

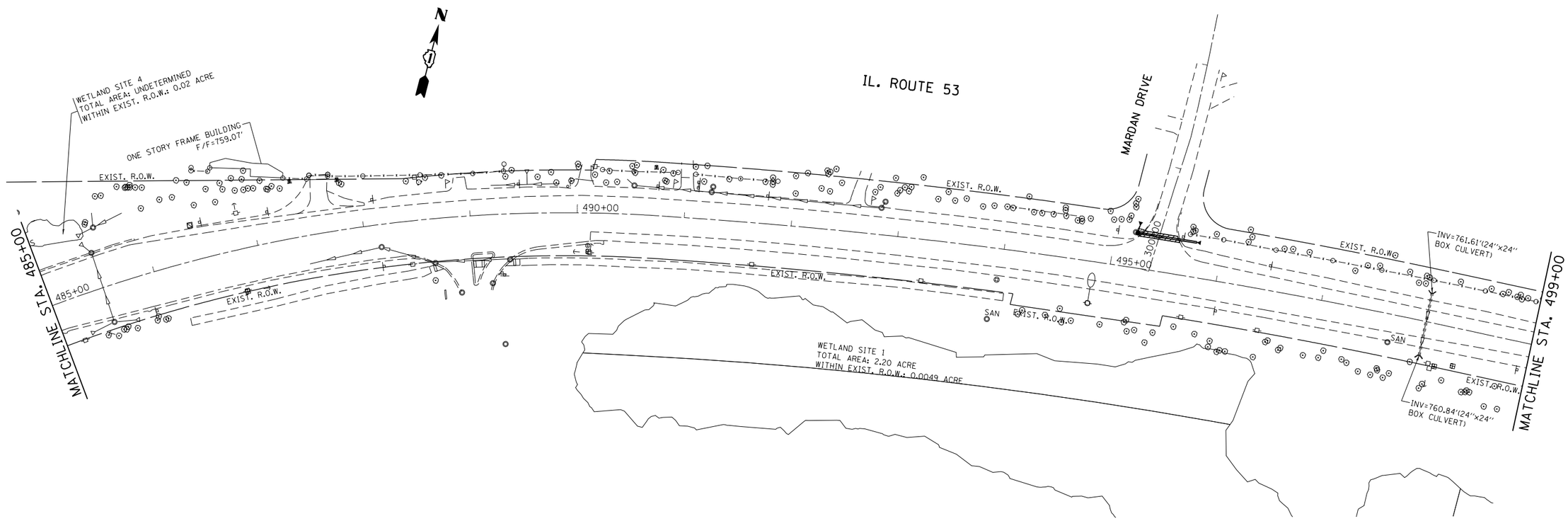
LEGEND

- CLASS D PATCHES, TYPE II, 16 INCH
- CLASS D PATCHES, TYPE IV, 16 INCH
- ● / ⊙ PROP. DRAINAGE STRUCTURE (SEE DRAINAGE & UTILITIES PLANS FOR DETAILS)
- ▸ PROP. FLARED END SECTION (SEE DRAINAGE & UTILITIES PLANS FOR DETAILS)
- PROP. STORM SEWER (SEE DRAINAGE & UTILITIES PLANS FOR DETAILS)
- PROP. PIPE CULVERT (SEE DRAINAGE & UTILITIES PLANS FOR DETAILS)

NOTE:

THE CONTRACTOR SHALL UTILIZE HIGHWAY STANDARDS 701001, 701006, 701011, AND 701201 FOR TRAFFIC CONTROL AND PROTECTION IN ORDER TO COMPLETE ALL PRE-STAGE WORK. LANE CLOSURES REQUIRED TO COMPLETE PRE-STAGE WORK SHALL ONLY BE UTILIZED DURING ALLOWABLE HOURS AS SPECIFIED IN THE SPECIAL PROVISION 'KEEPING ARTERIAL ROADWAYS OPEN TO TRAFFIC (LANE CLOSURES ONLY)'.





USER NAME = tarkjfm	DESIGNED -	REVISED -	**STATE OF ILLINOIS**
	DRAWN -	REVISED -	**DEPARTMENT OF TRANSPORTATION**
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PLOT DATE = 3/20/2018	DATE -	REVISED -	

SUGGESTED PRE-STAGE PLAN
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	26
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

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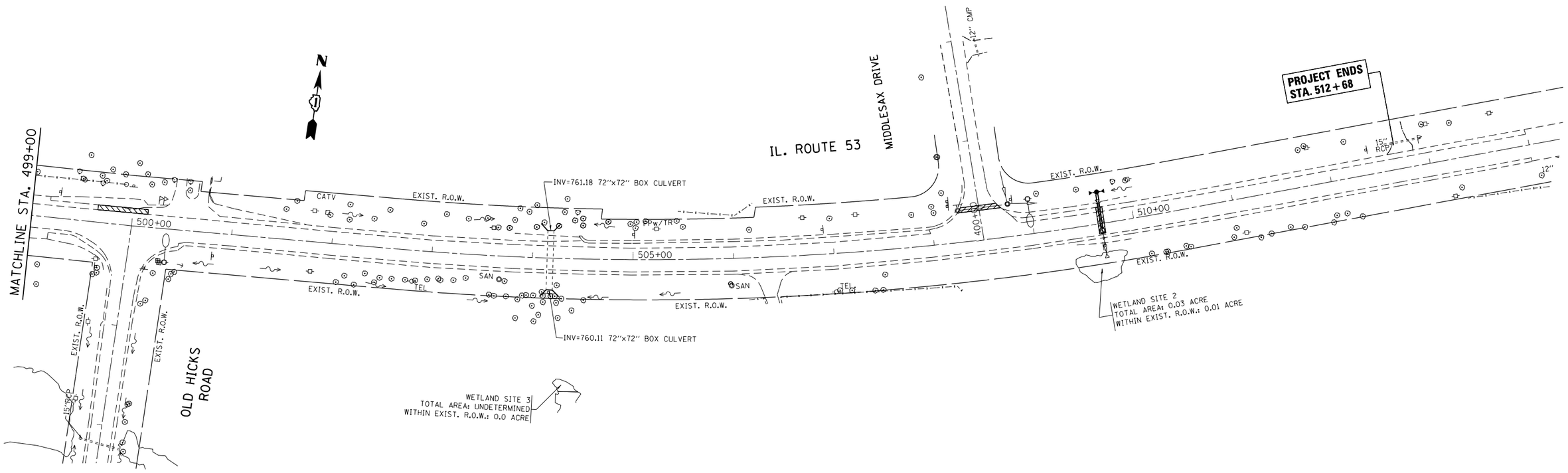

LEGEND

- CLASS D PATCHES, TYPE II, 16 INCH
- CLASS D PATCHES, TYPE IV, 16 INCH
- PROP. DRAINAGE STRUCTURE (SEE DRAINAGE & UTILITIES PLANS FOR DETAILS)
- PROP. FLARED END SECTION (SEE DRAINAGE & UTILITIES PLANS FOR DETAILS)
- PROP. STORM SEWER (SEE DRAINAGE & UTILITIES PLANS FOR DETAILS)
- PROP. PIPE CULVERT (SEE DRAINAGE & UTILITIES PLANS FOR DETAILS)

NOTE:

THE CONTRACTOR SHALL UTILIZE HIGHWAY STANDARDS 701001, 701006, 701011, AND 701201 FOR TRAFFIC CONTROL AND PROTECTION IN ORDER TO COMPLETE ALL PRE-STAGE WORK. LANE CLOSURES REQUIRED TO COMPLETE PRE-STAGE WORK SHALL ONLY BE UTILIZED DURING ALLOWABLE HOURS AS SPECIFIED IN THE SPECIAL PROVISION 'KEEPING ARTERIAL ROADWAYS OPEN TO TRAFFIC (LANE CLOSURES ONLY)'.

USER NAME = tarkqfm	DESIGNED -	REVISED -	**STATE OF ILLINOIS** **DEPARTMENT OF TRANSPORTATION**
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PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 3/20/2018	DATE -	REVISED -	

SUGGESTED PRE-STAGE PLAN
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	27
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

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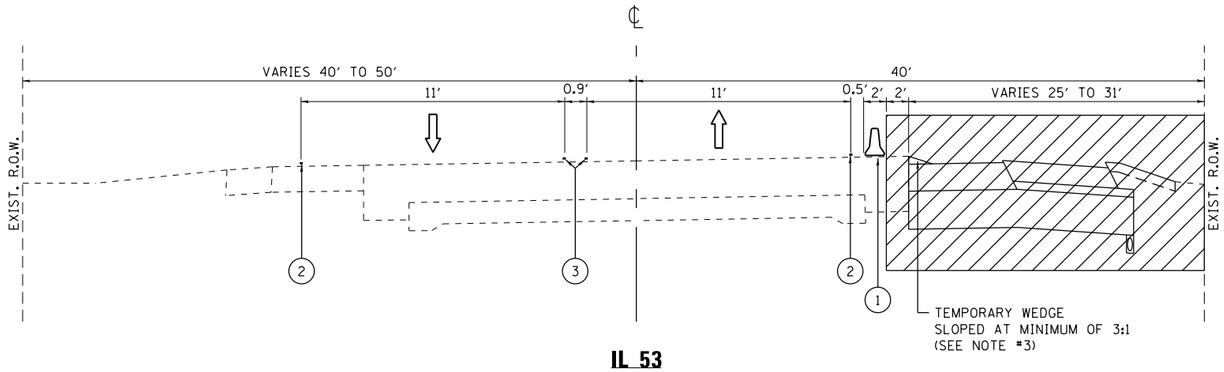
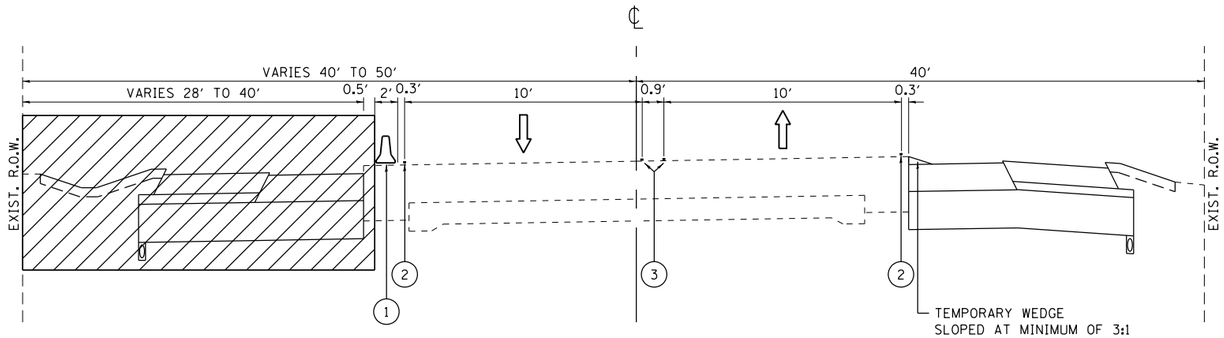


IL 53

STAGE I TYPICAL SECTION

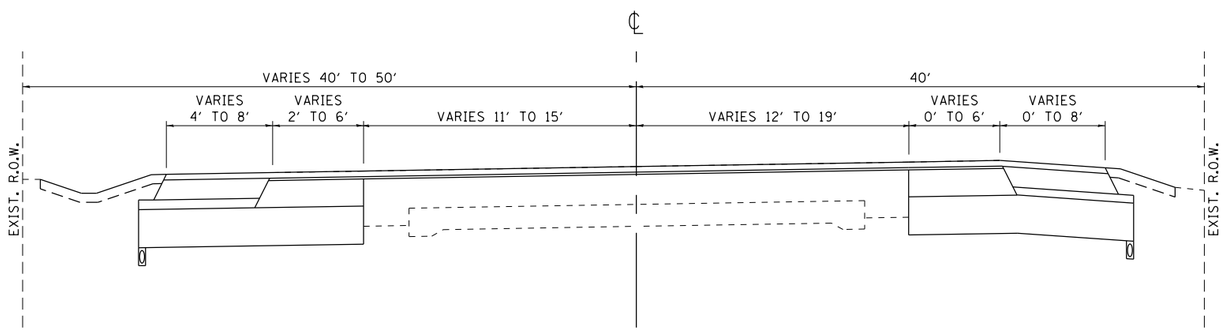
STA. 495+55 TO STA. 512+68



IL 53

STAGE II TYPICAL SECTION

STA. 495+55 TO STA. 512+68



IL 53

STAGE III TYPICAL SECTION

STA. 495+55 TO STA. 512+68

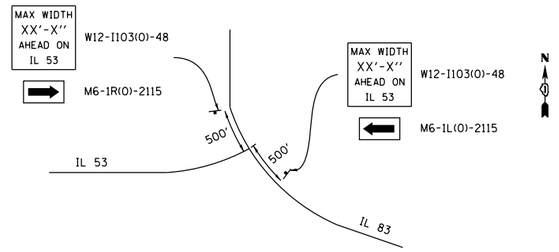
LEGEND

- DIRECTION OF TRAFFIC
- WORK AREA
- (1) TEMPORARY CONCRETE BARRIER
- (2) PAVEMENT MARKING TAPE TYPE IV, 4″, SOLID WHITE EDGE LINE
- (3) PAVEMENT MARKING TAPE TYPE IV, 4″, SOLID DOUBLE YELLOW LINES

NOTES

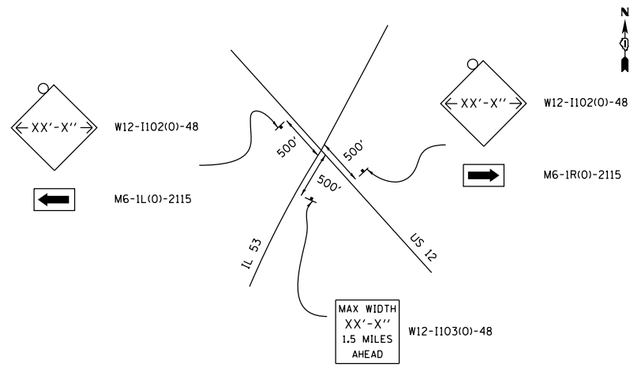
1. DURING STAGE II DAY-TIME LANE CLOSURES (HIGHWAY STANDARDS 701502 & 701701) SHALL BE UTILIZED WEST OF MARDAN DRIVE (STA. 484+11 TO STA. 495+55) TO COMPLETE PROPOSED WORK.
2. DURING STAGE II, TEMPORARY CONCRETE BARRIER SHALL BE PINNED. PINNING IS REQUIRED AT ALL LOCATIONS WHERE TEMPORARY CONCRETE BARRIER IS PROPOSED TO BE INSTALLED.
3. TEMPORARY WEDGE SHALL BE PAID FOR AS TEMPORARY PAVEMENT (VARIABLE DEPTH). ITS REMOVAL SHALL BE PAID FOR AS PAVEMENT REMOVAL.

WIDTH RESTRICTION FOR WESTBOUND TRAFFIC



NOTE: THE DIMENSION ON THE SIGNS SHALL BE 11′-6″ DURING STAGE I AND 8′-6″ DURING STAGE II.

WIDTH RESTRICTION FOR EASTBOUND TRAFFIC



NOTE: THE DIMENSION ON THE SIGNS SHALL BE 10′-0″ DURING STAGE I AND 8′-6″ DURING STAGE II.

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PLOT DATE = 3/20/2018	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

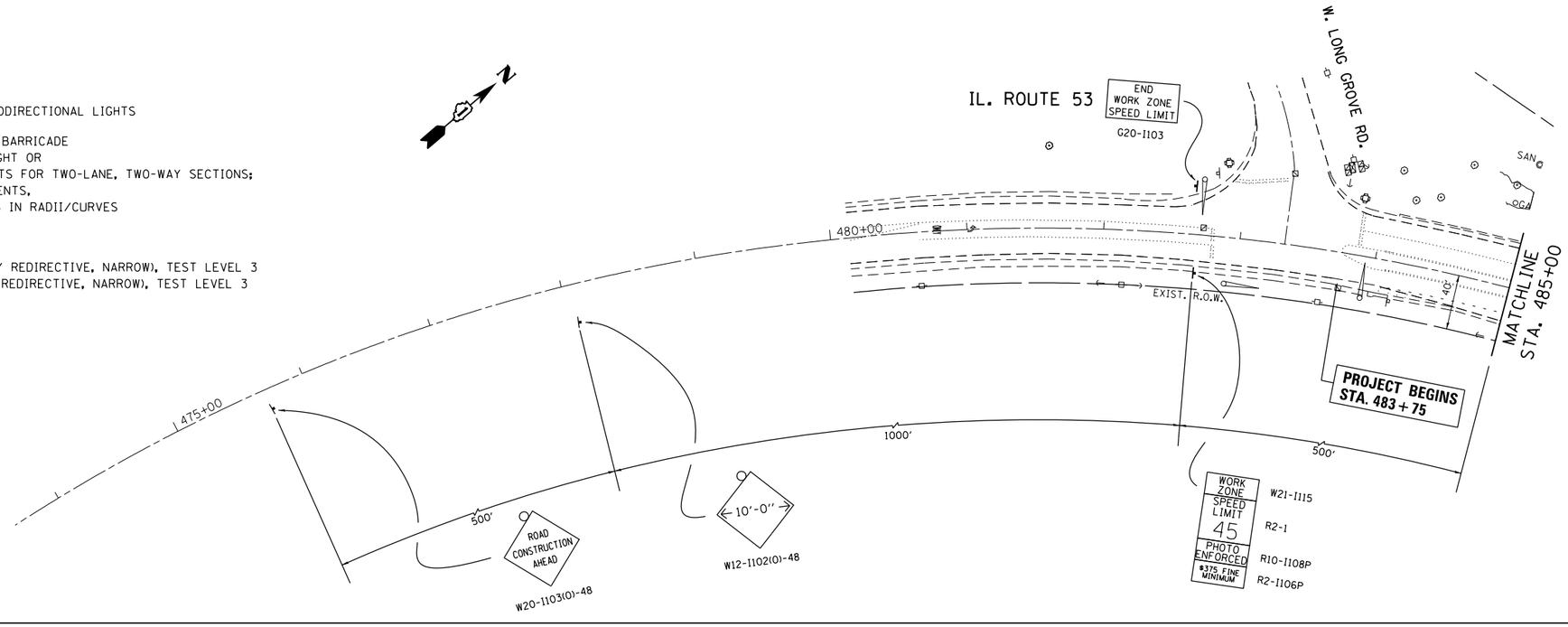
SUGGESTED STAGING TYPICAL SECTIONS
IL 53 AT OLD HICKS RD.

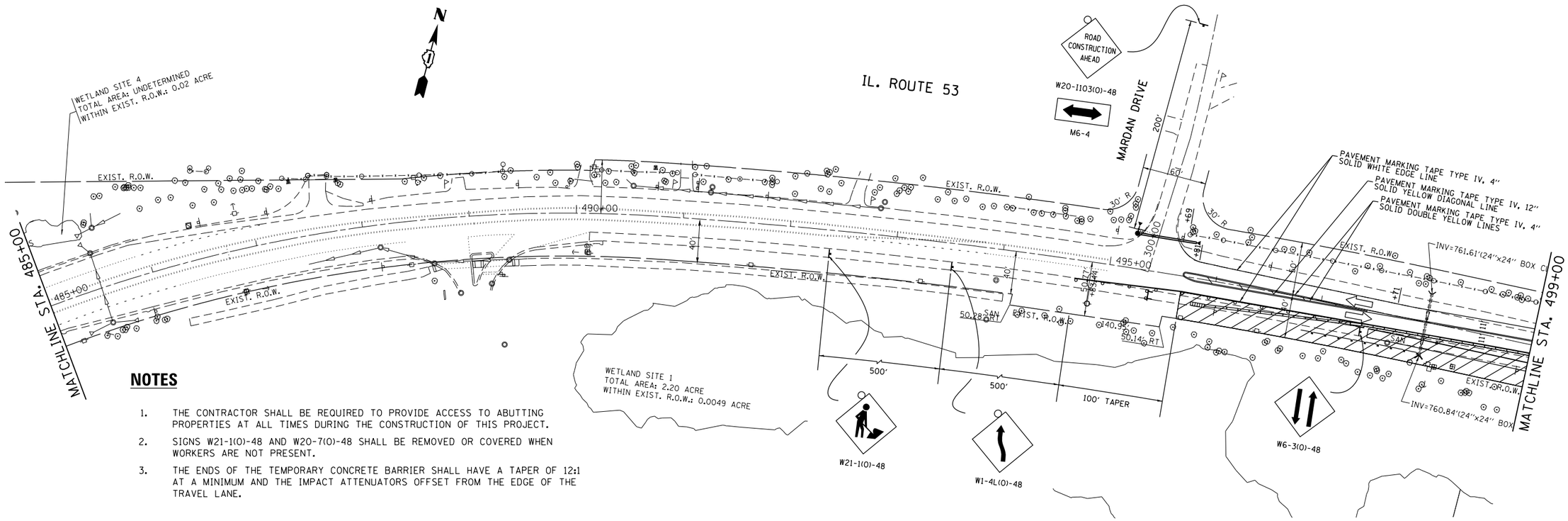
SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	28
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			

LEGEND

- WORK AREA
- DIRECTION OF TRAFFIC
- SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT OR WITH STEADY BURN BI-DIRECTIONAL LIGHTS FOR TWO-LANE, TWO-WAY SECTIONS; TO BE PLACED AT 50' CENTERS IN TANGENTS, 20' CENTERS IN TAPER, AND 10' CENTERS IN RADII/CURVES
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3
  IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3





NOTES

1. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTIES AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
2. SIGNS W21-1(0)-48 AND W20-7(0)-48 SHALL BE REMOVED OR COVERED WHEN WORKERS ARE NOT PRESENT.
3. THE ENDS OF THE TEMPORARY CONCRETE BARRIER SHALL HAVE A TAPER OF 12:1 AT A MINIMUM AND THE IMPACT ATTENUATORS OFFSET FROM THE EDGE OF THE TRAVEL LANE.

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PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGE I MAINTENANCE OF TRAFFIC
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	29
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			

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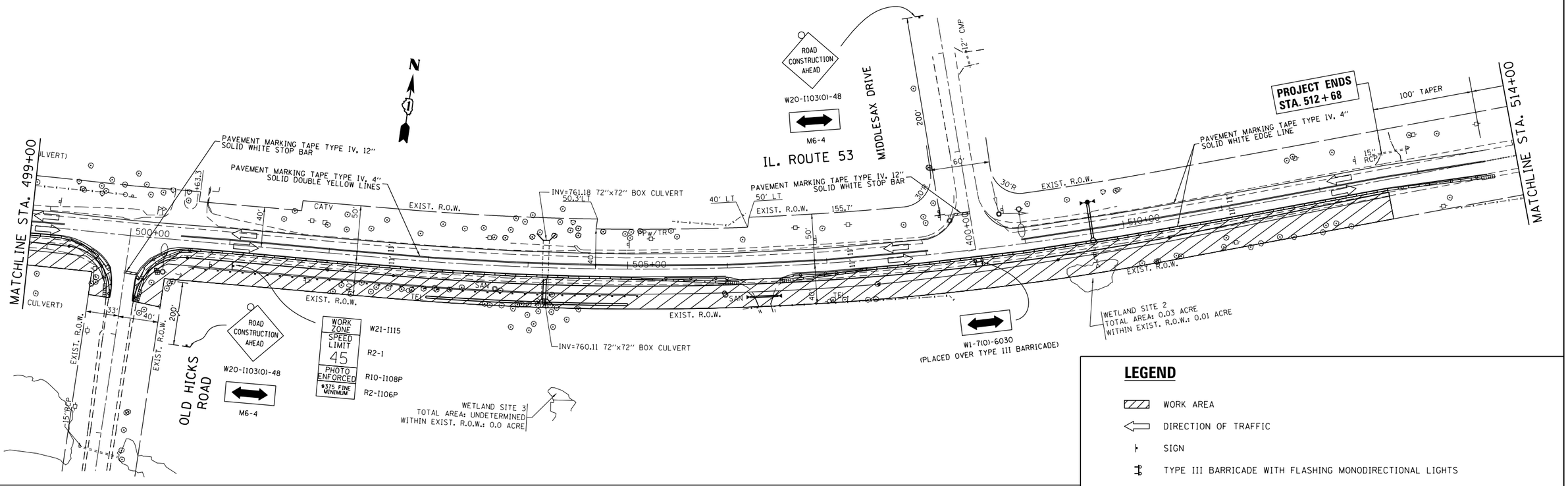

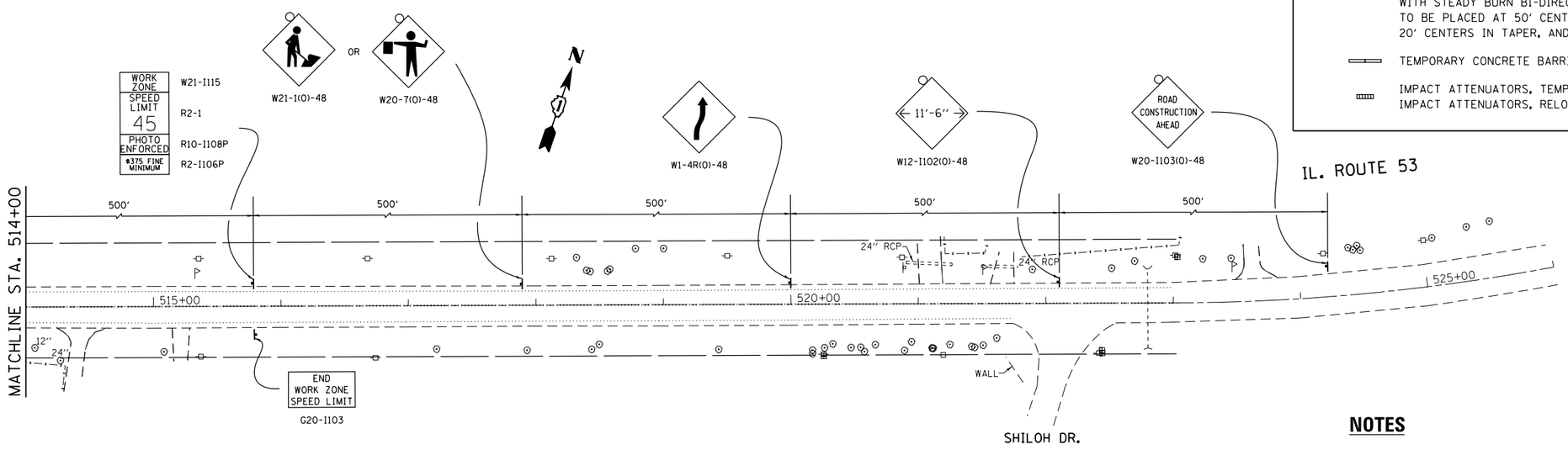

LEGEND

- WORK AREA
- DIRECTION OF TRAFFIC
- SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT OR WITH STEADY BURN BI-DIRECTIONAL LIGHTS FOR TWO-LANE, TWO-WAY SECTIONS; TO BE PLACED AT 50' CENTERS IN TANGENTS, 20' CENTERS IN TAPER, AND 10' CENTERS IN RADII/CURVES
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3
  IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

NOTES

1. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTIES AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
2. SIGNS W21-1(0)-48 AND W20-7(0)-48 SHALL BE REMOVED OR COVERED WHEN WORKERS ARE NOT PRESENT.
3. THE ENDS OF THE TEMPORARY CONCRETE BARRIER SHALL HAVE A TAPER OF 12:1 AT A MINIMUM AND THE IMPACT ATTENUATORS OFFSET FROM THE EDGE OF THE TRAVEL LANE.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

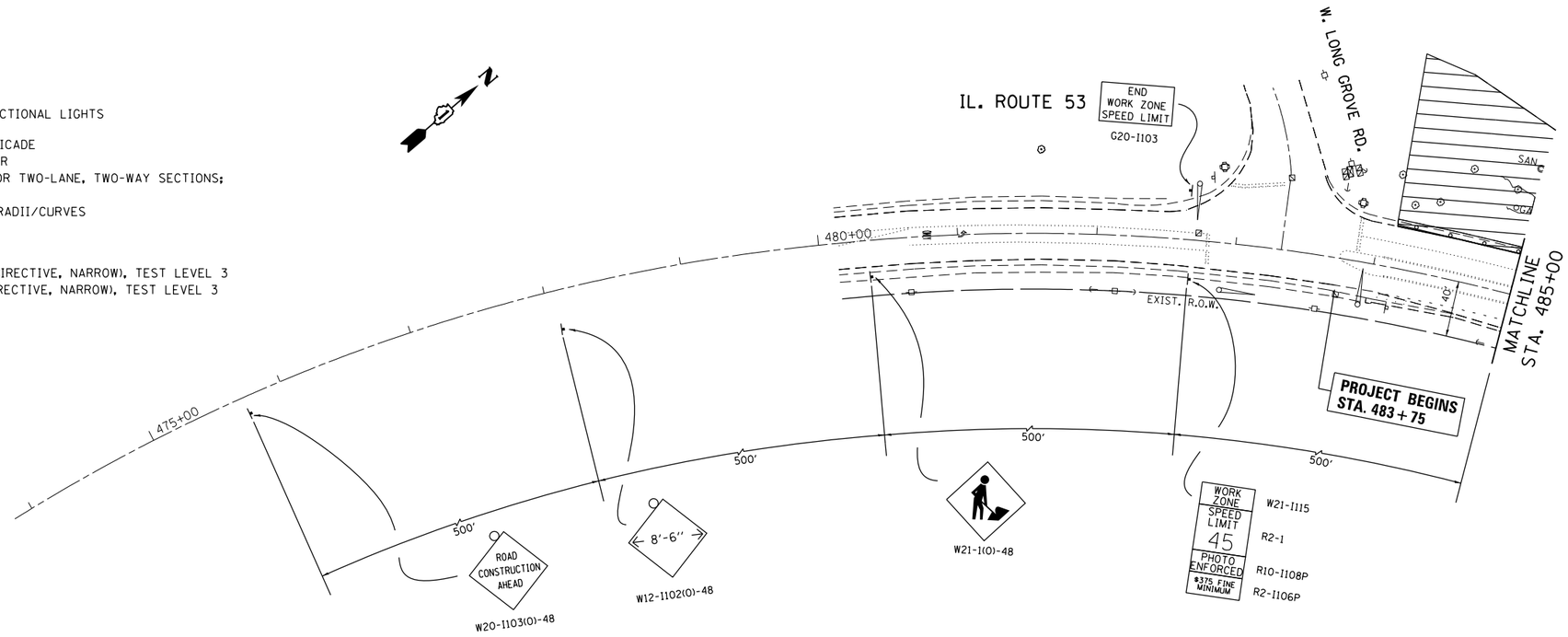
SUGGESTED STAGE I MAINTENANCE OF TRAFFIC
IL 53 AT OLD HICKS RD.

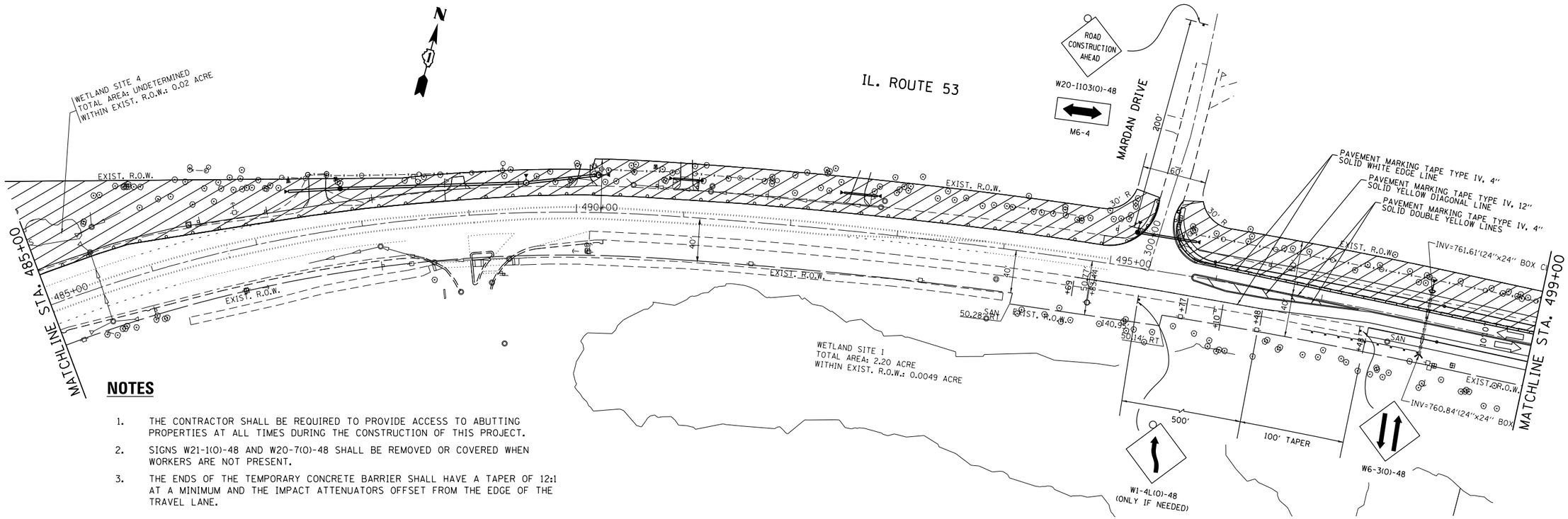
SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	30
		CONTRACT NO. 62B61		
		ILLINOIS	FED. AID PROJECT	

LEGEND

- WORK AREA
- DIRECTION OF TRAFFIC
- SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE
  WITH STEADY BURN MONODIRECTIONAL LIGHT OR
  WITH STEADY BURN BI-DIRECTIONAL LIGHTS FOR TWO-LANE, TWO-WAY SECTIONS;
  TO BE PLACED AT 50' CENTERS IN TANGENTS,
  20' CENTERS IN TAPER, AND 10' CENTERS IN RADII/CURVES
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3
  IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3





NOTES

1. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTIES AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
2. SIGNS W21-1(0)-48 AND W20-7(0)-48 SHALL BE REMOVED OR COVERED WHEN WORKERS ARE NOT PRESENT.
3. THE ENDS OF THE TEMPORARY CONCRETE BARRIER SHALL HAVE A TAPER OF 12:1 AT A MINIMUM AND THE IMPACT ATTENUATORS OFFSET FROM THE EDGE OF THE TRAVEL LANE.

USER NAME = tarlqfm	DESIGNED -	REVISED -
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PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGE II MAINTENANCE OF TRAFFIC
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	31
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

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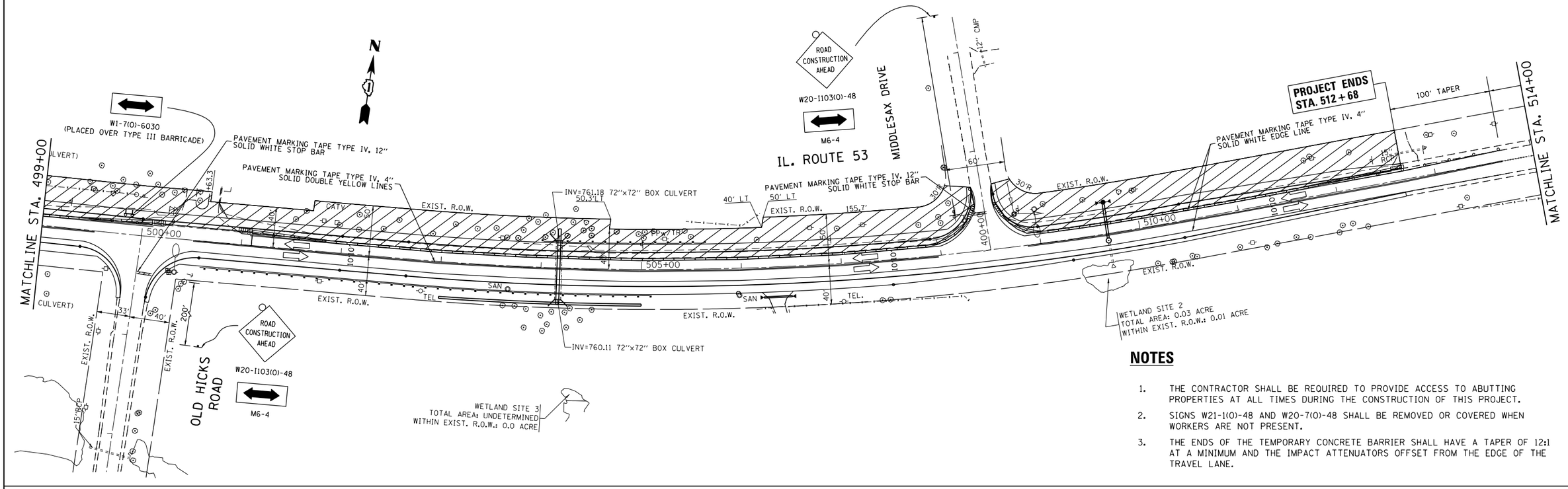
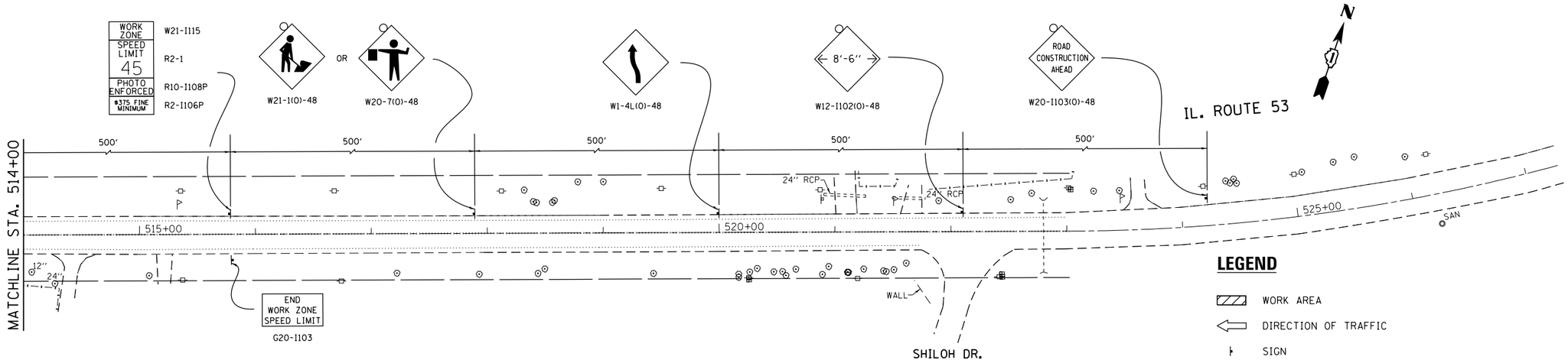


NOTES

1. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTIES AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
2. SIGNS W21-1(0)-48 AND W20-7(0)-48 SHALL BE REMOVED OR COVERED WHEN WORKERS ARE NOT PRESENT.
3. THE ENDS OF THE TEMPORARY CONCRETE BARRIER SHALL HAVE A TAPER OF 12:1 AT A MINIMUM AND THE IMPACT ATTENUATORS OFFSET FROM THE EDGE OF THE TRAVEL LANE.



LEGEND

- WORK AREA
- DIRECTION OF TRAFFIC
- SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE
  WITH STEADY BURN MONODIRECTIONAL LIGHT OR
  WITH STEADY BURN BI-DIRECTIONAL LIGHTS FOR TWO-LANE, TWO-WAY SECTIONS;
  TO BE PLACED AT 50' CENTERS IN TANGENTS,
  20' CENTERS IN TAPER, AND 10' CENTERS IN RADII/CURVES
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3
  IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

USER NAME = tarkpfm	DESIGNED -	REVISED -
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PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGE II MAINTENANCE OF TRAFFIC
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	32
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

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EROSION AND SEDIMENT CONTROL NOTES:

1. ALL EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE STATE STANDARD FOR THE ENTIRE DURATION OF THE CONTRACT OR UNTIL SUCH TIME AS DIRECTED BY THE ENGINEER.

2. EROSION CONTROL ITEMS ARE CONSIDERED TO BE HIGH PRIORITY ITEMS ON THIS CONTRACT. THE ENGINEER WILL IMPLEMENT ALL PROVISIONS OF THE SPECIFICATION NECESSARY TO ASSURE THAT EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY WAY. ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE BEGINNING OF ANY CONSTRUCTION ACTIVITIES WHICH POTENTIALLY CREATE ERODIBLE CONDITIONS.

3. THE EROSION CONTROL MEASURES SHOWN ARE BUT A GRAPHICAL REPRESENTATION OF SUGGESTED MEASURES. DEVIATIONS FROM THE PLANS ARE TO BE EXPECTED PENDING A JOBSITE INSPECTION BETWEEN THE CONTRACTOR AND THE DEPARTMENT.

4. AT A MINIMUM, THE CONTRACTOR SHALL INSPECT ALL OF THE EROSION AND SEDIMENT CONTROL WEEKLY OR AFTER EACH ONE-HALF INCH OR GREATER RAINFALL EVENT. ANY REQUIRED REPAIRS SHALL BE MADE BY THE CONTRACTOR TO KEEP ALL EROSION AND SEDIMENT CONTROL FUNCTIONING AS DESIGNED.

5. PERIMETER EROSION BARRIER SHALL BE PLACED 1' FROM THE EXISTING R.O.W. LINE OR AS DIRECTED OTHERWISE BY THE ENGINEER.

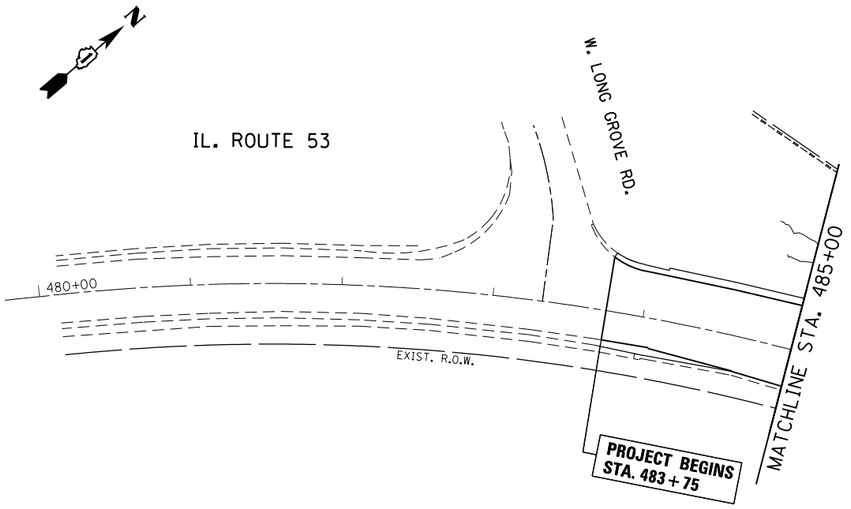
6. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN INLET PROTECTION AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. INLET PROTECTION SHALL BE PLACED IMMEDIATELY AFTER THE AREA INVOLVED HAS BEEN DISTURBED.

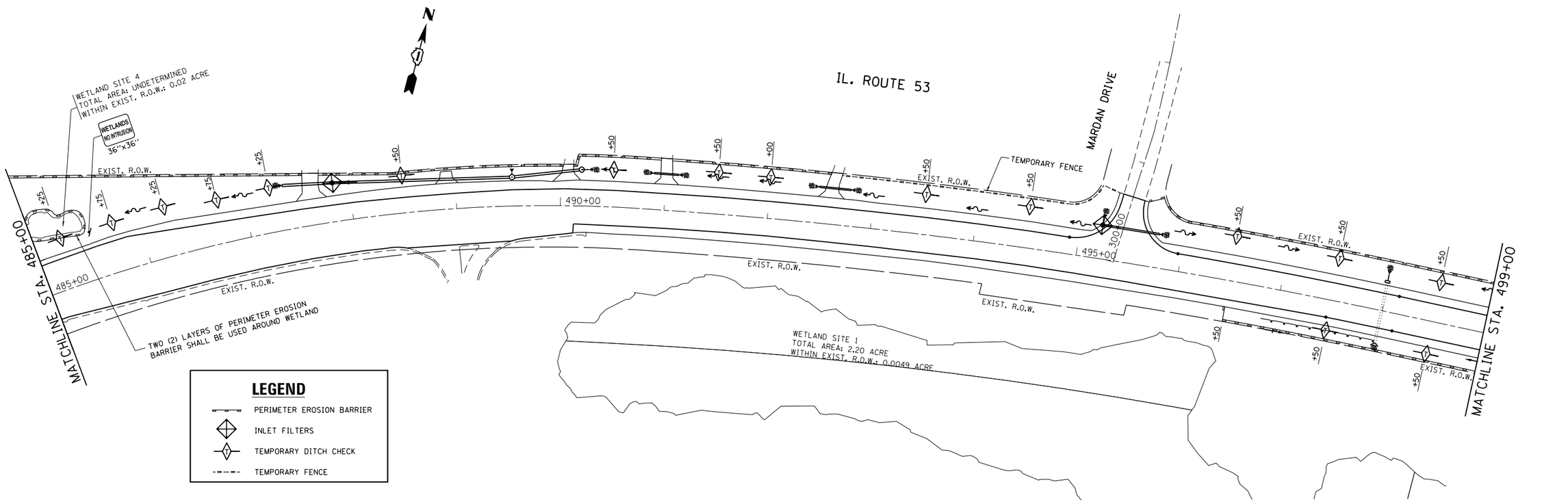
7. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL.

8. TEMPORARY OR PERMANENT STABILIZATION SHALL BE INITIATED IMMEDIATELY UPON COMPLETION OF DISTURBANCE OR IF THE WORK AREA IS TO BE LEFT UNDISTURBED FOR 14 DAYS OR MORE.

9. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG FINAL GRADING AND SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.

10. "WETLANDS NO INTRUSION" SIGNAGE SHOULD ALSO BE PROVIDED AT THE BOUNDARY OF ALL UN-IMPACTED WETLANDS AND/OR WOUS. THE CONTRACTOR CAN BORROW THE SIGNS FROM THE BUREAU OF MAINTENANCE. INCLUDE TEMPORARY FENCING AND WETLAND SIGNAGE WITHIN THE EROSION AND SEDIMENT CONTROL STRATEGY.





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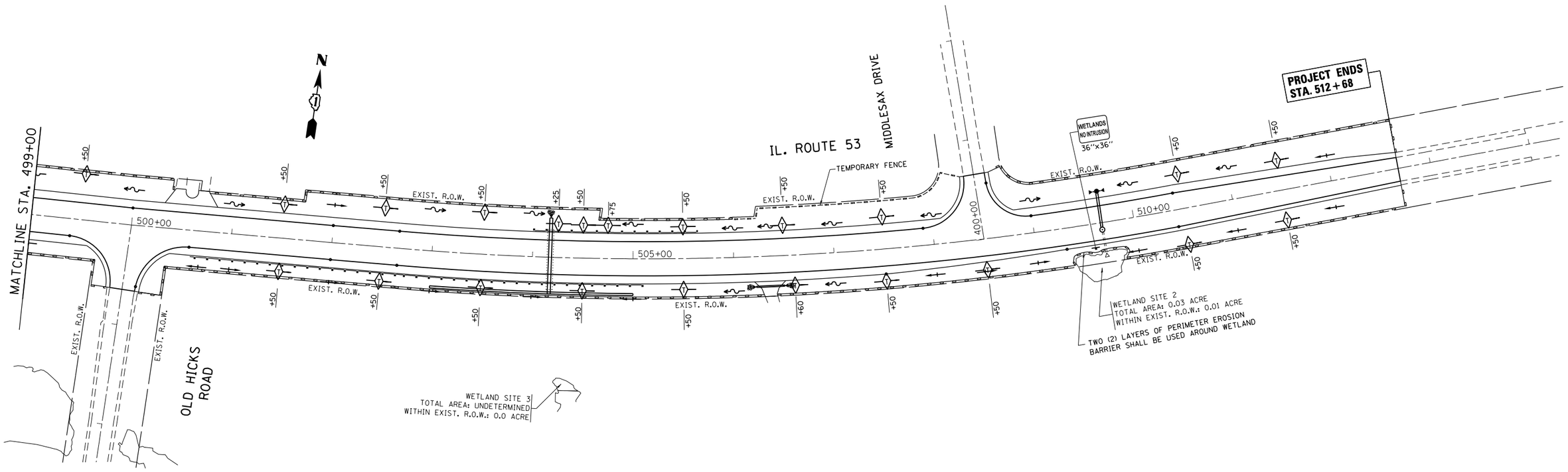
Default	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION AND SEDIMENT CONTROL PLAN
IL. 53 AT OLD HICKS RD.

SCALE: 1"=50' | SHEET OF SHEETS | STA. 485+00 | TO STA. 499+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	33
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		



EROSION AND SEDIMENT CONTROL NOTES:

1. ALL EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE STATE STANDARD FOR THE ENTIRE DURATION OF THE CONTRACT OR UNTIL SUCH TIME AS DIRECTED BY THE ENGINEER.

2. EROSION CONTROL ITEMS ARE CONSIDERED TO BE HIGH PRIORITY ITEMS ON THIS CONTRACT. THE ENGINEER WILL IMPLEMENT ALL PROVISIONS OF THE SPECIFICATION NECESSARY TO ASSURE THAT EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY WAY. ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE BEGINNING OF ANY CONSTRUCTION ACTIVITIES WHICH POTENTIALLY CREATE ERODIBLE CONDITIONS.

3. THE EROSION CONTROL MEASURES SHOWN ARE BUT A GRAPHICAL REPRESENTATION OF SUGGESTED MEASURES. DEVIATIONS FROM THE PLANS ARE TO BE EXPECTED PENDING A JOBSITE INSPECTION BETWEEN THE CONTRACTOR AND THE DEPARTMENT.

4. AT A MINIMUM, THE CONTRACTOR SHALL INSPECT ALL OF THE EROSION AND SEDIMENT CONTROL WEEKLY OR AFTER EACH ONE-HALF INCH OR GREATER RAINFALL EVENT. ANY REQUIRED REPAIRS SHALL BE MADE BY THE CONTRACTOR TO KEEP ALL EROSION AND SEDIMENT CONTROL FUNCTIONING AS DESIGNED.

5. PERIMETER EROSION BARRIER SHALL BE PLACED 1' FROM THE EXISTING R.O.W. LINE OR AS DIRECTED OTHERWISE BY THE ENGINEER.

6. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN INLET PROTECTION AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. INLET PROTECTION SHALL BE PLACED IMMEDIATELY AFTER THE AREA INVOLVED HAS BEEN DISTURBED.

7. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY, PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL.

8. TEMPORARY OR PERMANENT STABILIZATION SHALL BE INITIATED IMMEDIATELY UPON COMPLETION OF DISTURBANCE OR IF THE WORK AREA IS TO BE LEFT UNDISTURBED FOR 14 DAYS OR MORE.

9. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG FINAL GRADING AND SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.

10. "WETLANDS NO INTRUSION" SIGNAGE SHOULD ALSO BE PROVIDED AT THE BOUNDARY OF ALL UN-IMPACTED WETLANDS AND/OR WOUS. THE CONTRACTOR CAN BORROW THE SIGNS FROM THE BUREAU OF MAINTENANCE. INCLUDE TEMPORARY FENCING AND WETLAND SIGNAGE WITHIN THE EROSION AND SEDIMENT CONTROL STRATEGY.

LEGEND

- PERIMETER EROSION BARRIER
- INLET FILTERS
- TEMPORARY DITCH CHECK
- TEMPORARY FENCE

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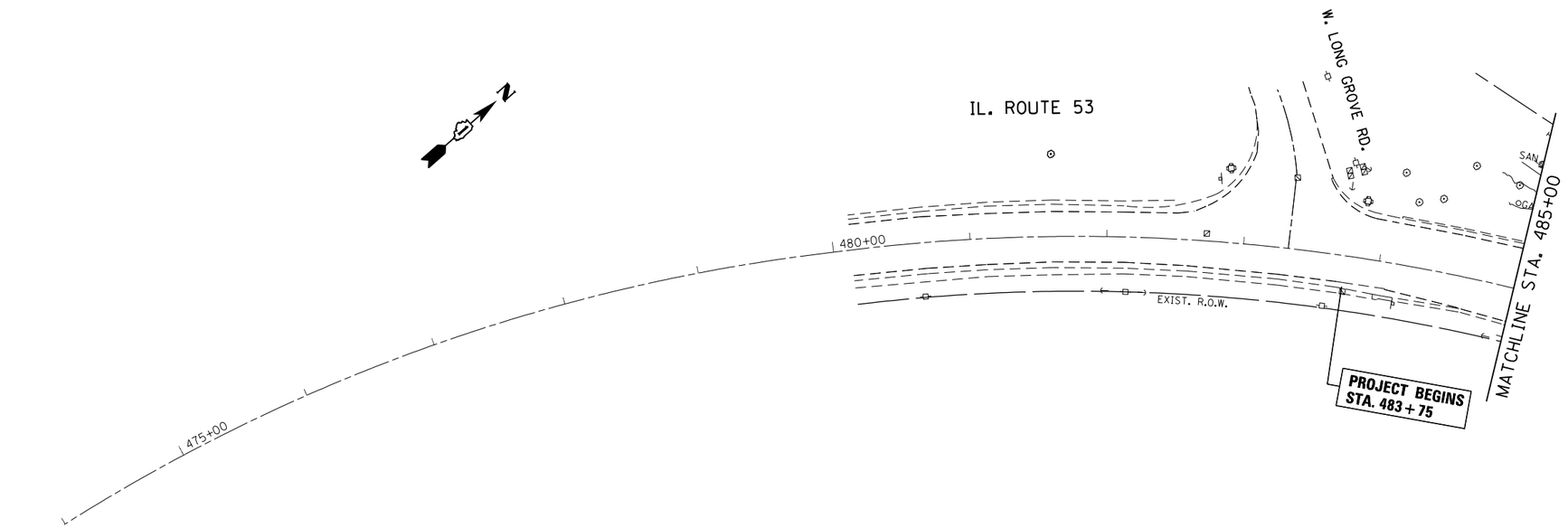
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION AND SEDIMENT CONTROL PLAN
IL. 53 AT OLD HICKS RD.

SCALE: 1"=50' | SHEET OF SHEETS | STA. 485+00 TO STA. 499+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	34
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			



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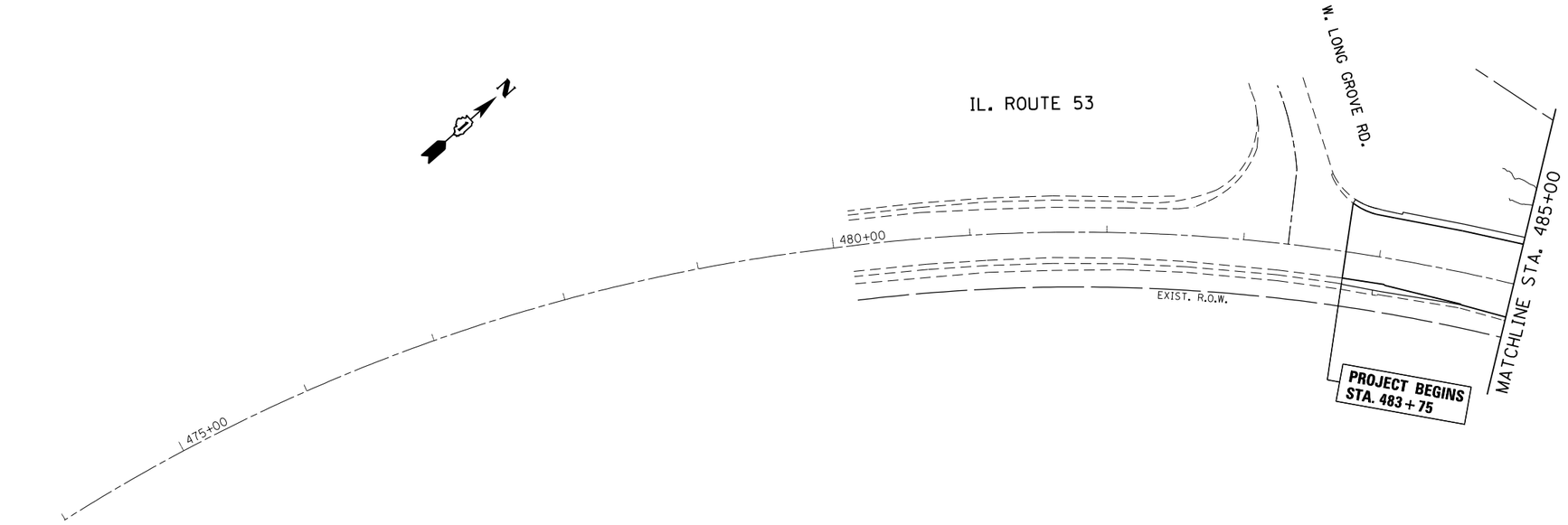

PROPOSED

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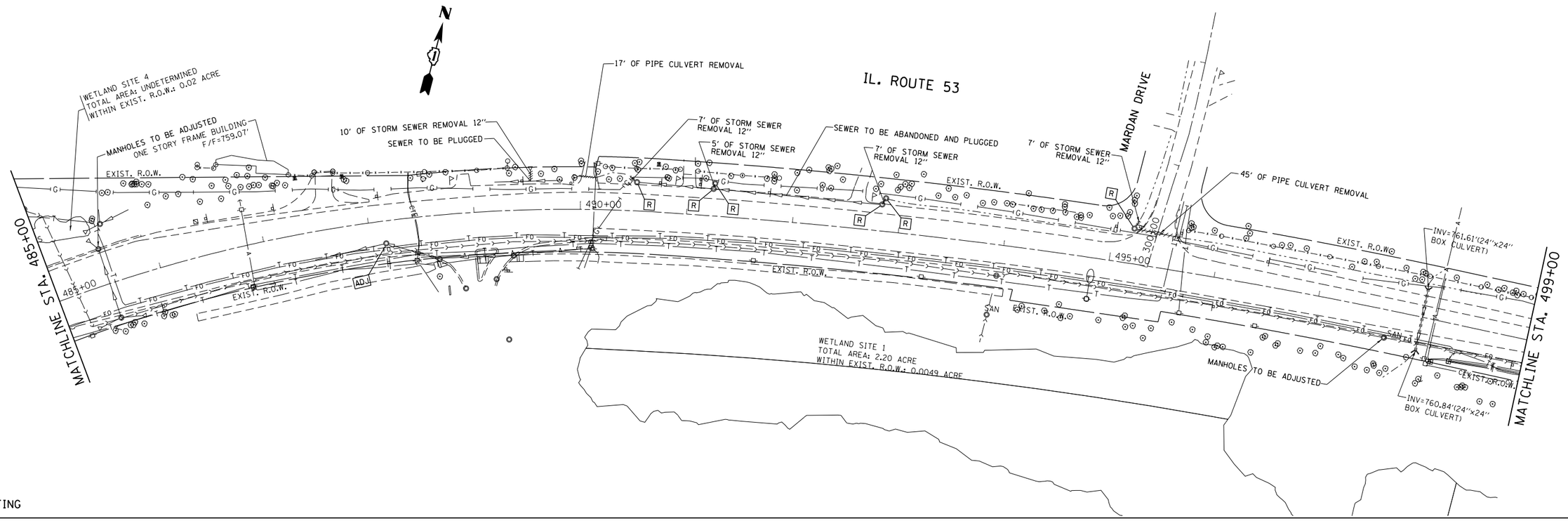
Default	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED DRAINAGE PLAN
IL. 53 AT OLD HICKS RD.

SCALE: 1"=50' | SHEET OF SHEETS | STA. 485+00 | TO STA. 499+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	35
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		



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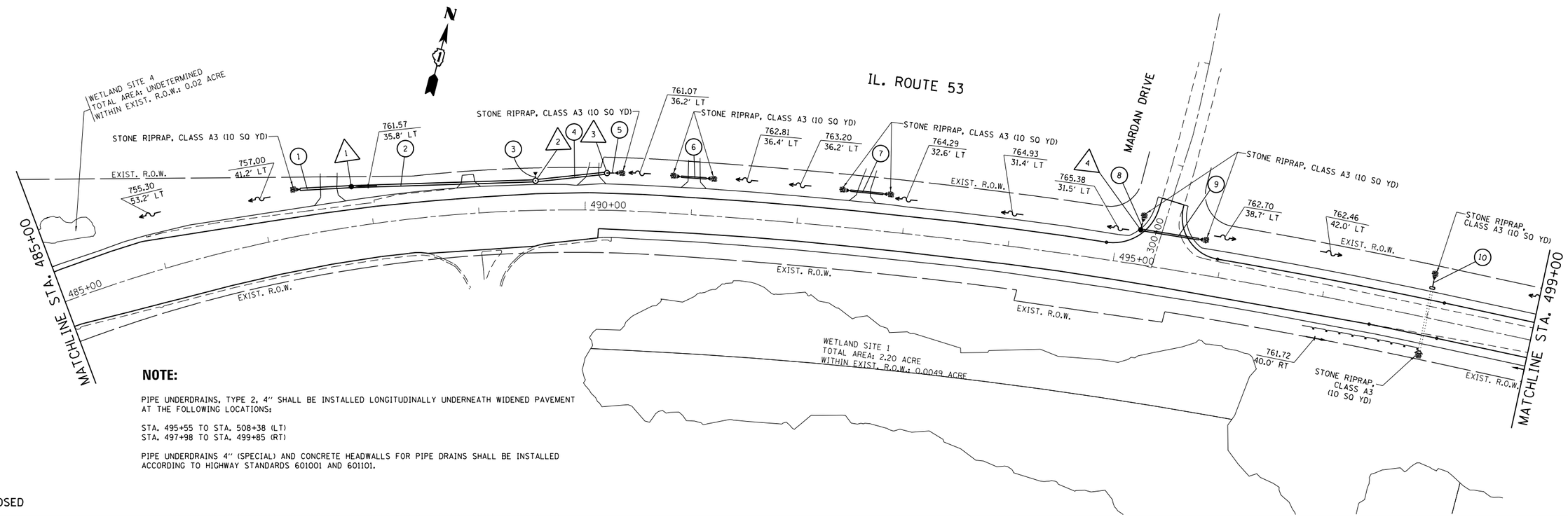

NOTE:

PIPE UNDERDRAINS, TYPE 2, 4" SHALL BE INSTALLED LONGITUDINALLY UNDERNEATH WIDENED PAVEMENT AT THE FOLLOWING LOCATIONS:

STA. 495+55 TO STA. 508+38 (LT)
STA. 497+98 TO STA. 499+85 (RT)

PIPE UNDERDRAINS 4" (SPECIAL) AND CONCRETE HEADWALLS FOR PIPE DRAINS SHALL BE INSTALLED ACCORDING TO HIGHWAY STANDARDS 601001 AND 601101.

PROPOSED

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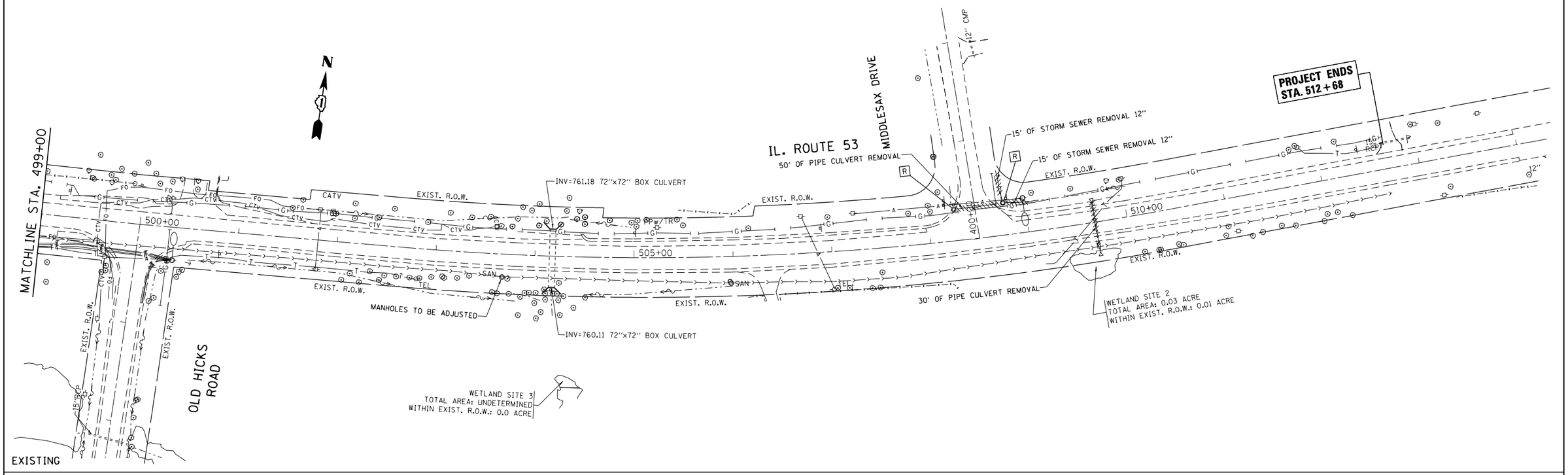
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED DRAINAGE PLAN
IL. 53 AT OLD HICKS RD.

SCALE: 1"=50' | SHEET OF SHEETS | STA. 485+00 TO STA. 499+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	36
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			



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PIPE UNDERDRAINS, TYPE 2, 4" SHALL BE INSTALLED LONGITUDINALLY UNDERNEATH WIDENED PAVEMENT AT THE FOLLOWING LOCATIONS:

STA. 495+55 TO STA. 508+38 (LT)
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED DRAINAGE PLAN
IL. 53 AT OLD HICKS RD.

SCALE: 1"=50' | SHEET OF SHEETS | STA. 499+00 | TO STA. 512+67.69

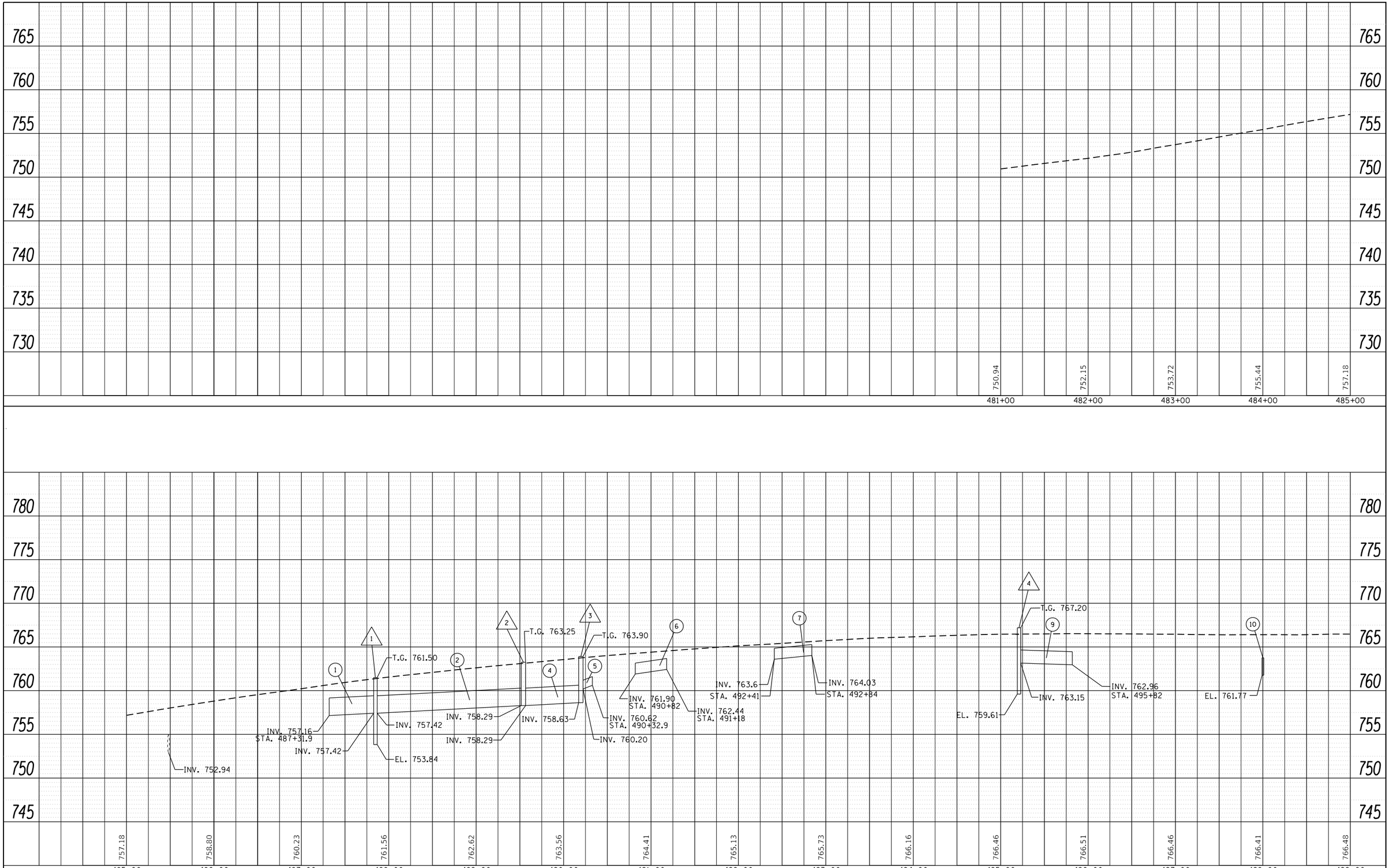
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	53ON-3	LAKE	80	37
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			

PLAN

	DATE
SURVEYED	
PLOTTED	
ALIGNMENT CHECKED	
RT. OF WAY CHECKED	
CADD FILE NAME	
NOTE BOOK	
NO.	

PROFILE

	DATE
SURVEYED	
PLOTTED	
GRADES CHECKED	
BLM. NOTED	
STRUCTURE NOTATNS CHKD	
NOTE BOOK	
NO.	



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED DRAINAGE PROFILE
IL. 53 AT OLD HICKS RD.

1"=50' H
SCALE: 1"=10' V | SHEET OF SHEETS | STA. 485+00 TO STA. 512+67.69

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	38
		CONTRACT NO. 62B61		
		ILLINOIS FED. AID PROJECT		

PLAN
SURVEYED
PLOTTED
DRAWN
CHECKED
R/T. OF WAY CHECKED
CADD FILE NAME
NOTE BOOK
NO.
BY
DATE

PROFILE
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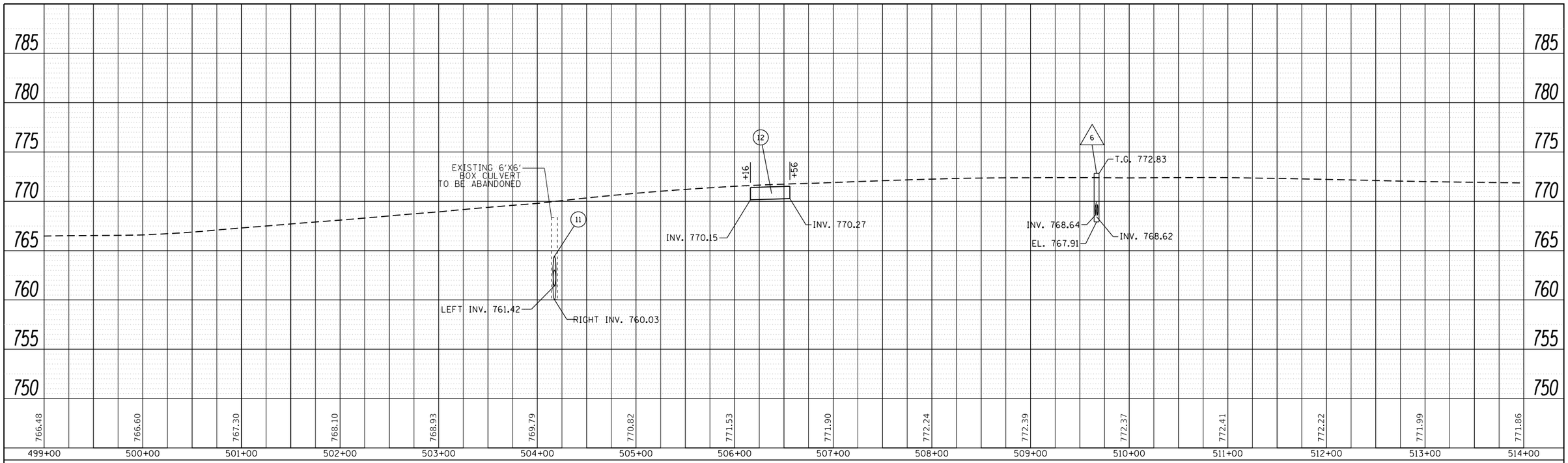
STRUCTURE NOTATNS CHKD
NOTE BOOK
NO.
BY
DATE



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED DRAINAGE PROFILE
IL. 53 AT OLD HICKS RD.

1"=50' H
SCALE: 1"=10' V | SHEET OF SHEETS | STA. 485+00 TO STA. 512+67.69

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	53ON-3	LAKE	80	39
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

PIPES

PIPE NO.	PIPE TYPE	DIA (IN.)	LENGTH (FT.)	SLOPE	TRENCH BACKFILL (CU. YD.)
1	PIPE CULVERT, CLASS A, TYPE 1 W/ PRECAST REINFORCED CONCRETE FLARED END SECTION 24", INV = 757.16	24	48.4	0.5%	0
2	PIPE CULVERT, CLASS A, TYPE 1	24	173.5	0.5%	47
3	STORM SEWER, CLASS A, TYPE 1 W/ PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12", INV = 759.59	12	4.4	1.0%	0
4	PIPE CULVERT, CLASS A, TYPE 2	24	67.7	0.5%	0
5	STORM SEWER, CLASS A, TYPE 1 W/ PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12", INV = 760.62	12	7.0	4.0%	0
6	PIPE CULVERT, CLASS A, TYPE 1 W/ PRECAST REINFORCED CONCRETE FLARED END SECTION 15", INV = 761.90 (W) AND INV = 762.44 (E)	15	24.0	1.5%	0
7	PIPE CULVERT, CLASS A, TYPE 1 W/ PRECAST REINFORCED CONCRETE FLARED END SECTION 15", INV = 763.6 (W) AND INV = 764.03 (E)	15	31.2	1.0%	0
8	STORM SEWER, CLASS A, TYPE 1 W/ PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18", INV = 763.52	18	5.0	2.8%	0
9	PIPE CULVERT, CLASS A, TYPE 1 W/ PRECAST REINFORCED CONCRETE FLARED END SECTION 18", INV = 762.96	18	55.6	0.3%	14
10	SEE CULVERT DETAIL PLANS				
11	SEE CULVERT DETAIL PLANS				
12	PIPE CULVERT, CLASS A, TYPE 1 W/ PRECAST REINFORCED CONCRETE FLARED END SECTION 15", INV = 770.15 (W) AND INV = 770.27 (E)	15	28.3	0.3%	0
13	STORM SEWER, CLASS A, TYPE 1 W/ PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12", INV = 768.86	12	4.0	1.0%	0
14	STORM SEWER, CLASS A, TYPE 1 W/ PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12", INV = 768.86	12	4.0	1.0%	0
15	TWO (2) STORM SEWERS, CLASS A, TYPE 2	12	38.4	0.3%	8

DRAINAGE STRUCTURES

FLAT TOP
1
CB, TY-A, 4' DIA W/ TY-8 G
STA. 487+84.7, 37.5' LT
T.G. 761.50
INV. 757.42 (E)
INV. 757.42 (W)

FLAT TOP
2
MH, TY-A, 5' DIA W/ TY-1 FR & CLID
STA. 489+54.0, 27.9' LT
T.G. 763.25
INV. 758.29 (W)
INV. 758.29 (E)
INV. 759.49 (N)

FLAT TOP
3
MH, TY-A, 5' DIA W/ TY-1 FR & CLID
STA. 490+19.8, 35.1' LT
T.G. 763.90
INV. 758.63 (W)
INV. 760.20 (E)

FLAT TOP
4
CB, TY-A, 4' DIA W/ TY-1 FR & OLID
STA. 495+21.1, 32.7' LT
T.G. 767.20
INV. 763.15 (N)
INV. 763.15 (E)

FLAT TOP
5
CB, TY-A, 5' DIA W/ TY-1 FR & CLID
STA. 509+67.5, 31.6' LT
T.G. 772.43
INV. 768.76 (S)
INV. 768.76 (E)
INV. 768.76 (W)

FLAT TOP
6
MH, TY-A, 5' DIA W/ TY-1 FR & CLID
STA. 509+67.5, 7.0' RT
T.G. 772.83
INV. 768.64 (N)
INV. MATCH EX. 768.62 (S) (EX. 18'' SS)

USER NAME = tarikjfm	DESIGNED -	REVISED -	
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED DRAINAGE STRUCTURES AND PIPES
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	40
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

A	AERIAL
T	CABLE TV
E	ELECTRIC
FO	FIBER OPTIC
G	GAS
O	OIL
	SANITARY SEWER
T	TELEPHONE
	UNKNOWN
W	WATER
	SO-DEEP TEST HOLE

ILLINOIS
SCOTT ALLEN WECHTER
REG. 62-04788
REGISTERED PROFESSIONAL ENGINEER

I, SCOTT ALLEN WECHTER, CERTIFY TO THE ILLINOIS DEPARTMENT OF TRANSPORTATION THAT TO THE BEST OF MY KNOWLEDGE, INFORMATION, AND BELIEF, THE UTILITY INFORMATION DEPICTED BY SO-DEEP, INC. ON THIS PLAN SHEET WAS OBTAINED AND COMPILED UNDER MY DIRECT SUPERVISION USING ACCEPTED PRACTICES AND PROCEDURES.

DATE: 3-20-18

PROJECT MANAGER
SAM, LLC

N

SO-DEEP INC.
The Subsurface Utility Engineering Company
8397 Eucla Avenue
Manassas Park, VA 20111
703-361-6005

SIXF898.DGN	Designated By: P.R.
Scale: 1"=100'	Drafted By: J.S.
Sheet: COVER	Checked By: S.W.
REV: 3-7-2018 6-20-2017	SIXF898

0 50 100 150
SCALE IN FEET

UTILITY OWNERS

NICOR GAS
ATTN: BRUCE KOPPANG
1844 FERRY ROAD
NAPERVILLE, IL 60563
630-388-3046
bkoppang@southernco.com

AT&T
ATTN: HECTOR GARCIA
1000 COMMERCE DRIVE
OAK BROOK, IL 60523
630-573-5465
hg2929@att.com

COMED
ATTN: ANGELA HARRELL
COMED PUBLIC RELOCATION
LINCOLN CENTER ONE
630-576-6185
angela.harrell@comed.com

COMCAST CABLE COMMUNICATIONS
ATTN: ROBERT SCHULTER
688 INSUSTRIAL DRIVE
ELMHURST, IL 60126
224-229-5861
bob.schulter@cable.comcast.com

(LCPW) LAKE COUNTY PUBLIC WORKS
ATTN: JAVIER SALAZAR
650 W WINCHESTER ROAD
LIBERTYVILLE, IL 60048
847-377-7500

LEGEND

ALL UTILITY INFORMATION HEREON IS DEPICTED TO QUALITY LEVEL "B" (QL-B) UNLESS OTHERWISE NOTED. QL-B INFORMATION IS OBTAINED THROUGH THE APPLICATION OF APPROPRIATE SURFACE GEOPHYSICAL METHODS TO IDENTIFY THE EXISTENCE AND APPROXIMATE HORIZONTAL POSITION OF SUBSURFACE UTILITIES. QL-B DATA ARE REPRODUCIBLE BY SURFACE GEOPHYSICS AT ANY POINT OF THEIR DEPICTION. THIS INFORMATION IS SURVEYED TO APPLICABLE TOLERANCES AND REDUCED ONTO PLAN DOCUMENTS.

SIZE INFORMATION SHOWN HEREON IS TAKEN FROM AVAILABLE UTILITY RECORDS.

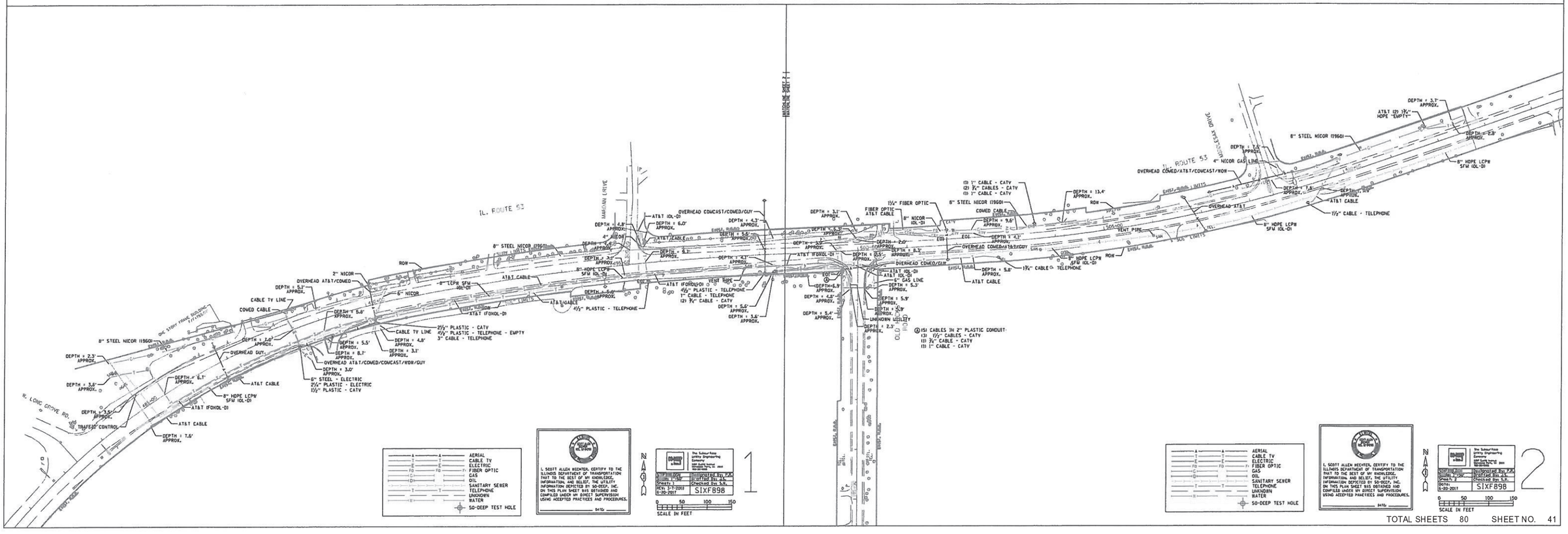
ABBREVIATIONS:

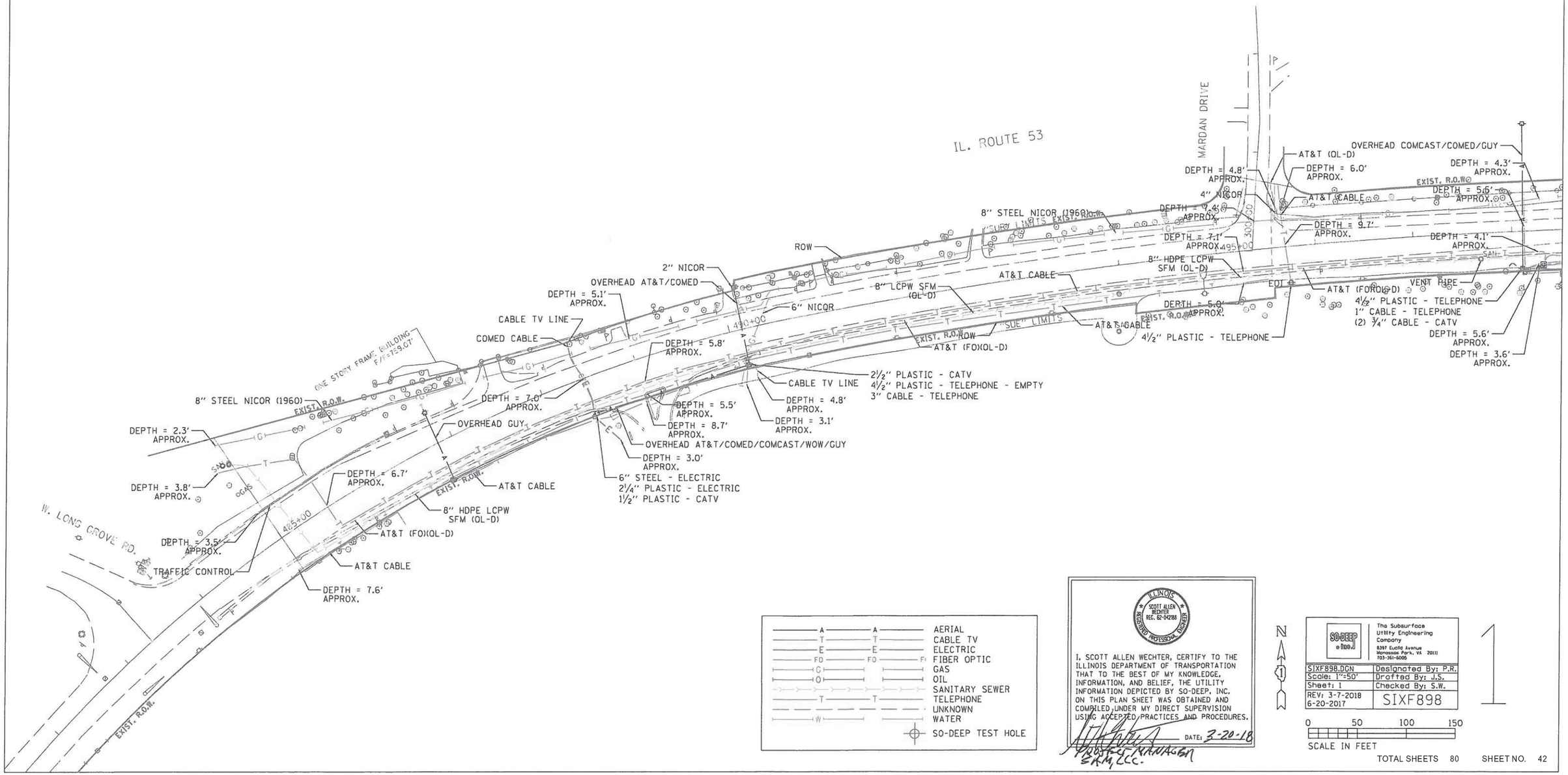
(QL-C)	DEPICTED ACCORDING TO RECORD INFORMATION AND EXISTING ASSOCIATED UTILITY STRUCTURES. NO ELECTRONIC INFORMATION WAS OBTAINED.
(QL-D)	DEPICTED ACCORDING TO RECORD INFORMATION. NO ELECTRONIC INFORMATION WAS OBTAINED.
(DATFI)	DEPICTED ACCORDING TO FIELD INSPECTION
(FO)	FIBER OPTIC
(AATUR)	ABANDONED ACCORDING TO UTILITY RECORDS
(AATFI)	ABANDONED ACCORDING TO FIELD INSPECTION
(EATFI)	EMPTY ACCORDING TO FIELD INSPECTION
EOI	END OF ELECTRONIC DESIGNATING INFORMATION
EORI	END OF UTILITY RECORD INFORMATION
(NAC)	NO ASSOCIATED CABLE FOUND FROM UTILITY STRUCTURE
(NAP)	NO ASSOCIATED PIPING FOUND FROM UTILITY STRUCTURE
⊢——	UTILITY ENDPOINT

UNLESS OTHERWISE NOTED, UTILITY LINE LIMITS DEPICTED REPRESENT FIELD DESIGNATING LIMITS AND NOT ENDPOINTS OF UTILITIES.

UTILITY INFORMATION LABELED "QL-C" OR "QL-D" IS DERIVED FROM FURNISHED RECORDS. SUCH INFORMATION MAY NOT BE ACCURATE OR RELIABLE. SO-DEEP, INC. EXPRESSLY DISCLAIMS RESPONSIBILITY FOR THE ACCURACY OR RELIABILITY OF UTILITY INFORMATION DEPICTED ACCORDING TO RECORDS.

ELECTRONIC DEPTH READINGS WERE TAKEN DIRECTLY FROM ELECTRONIC DESIGNATING INSTRUMENTS AND HAVE NOT BEEN VERIFIED BY ANY OTHER MEANS. EQUIPMENT MANUFACTURERS WILL NOT GUARANTEE AND ACCURACY FOR THIS DATA . THEREFORE, THE DEPTH READINGS ARE NOT TO BE CONSIDERED SUITABLE FOR DESIGN DECISIONS. SO-DEEP INC. DOES NOT WARRANT OR GUARANTEE THE ACCURACY OF RELIABILITY OF ANY ELECTRONIC DEPTH READINGS.





ILLINOIS
SCOTT ALLEN WECHTER
REG. 62-042188
REGISTERED PROFESSIONAL ENGINEER

I, SCOTT ALLEN WECHTER, CERTIFY TO THE ILLINOIS DEPARTMENT OF TRANSPORTATION THAT TO THE BEST OF MY KNOWLEDGE, INFORMATION, AND BELIEF, THE UTILITY INFORMATION DEPICTED BY SO-DEEP, INC. ON THIS PLAN SHEET WAS OBTAINED AND COMPILED UNDER MY DIRECT SUPERVISION USING ACCEPTED PRACTICES AND PROCEDURES.

DATE: 3-20-18

PROJECT MANAGER
S&M, LLC.

N

SO-DEEP INC.

The Subsurface Utility Engineering Company
8397 Euclid Avenue
Manassas Park, VA 20111
703-361-6005

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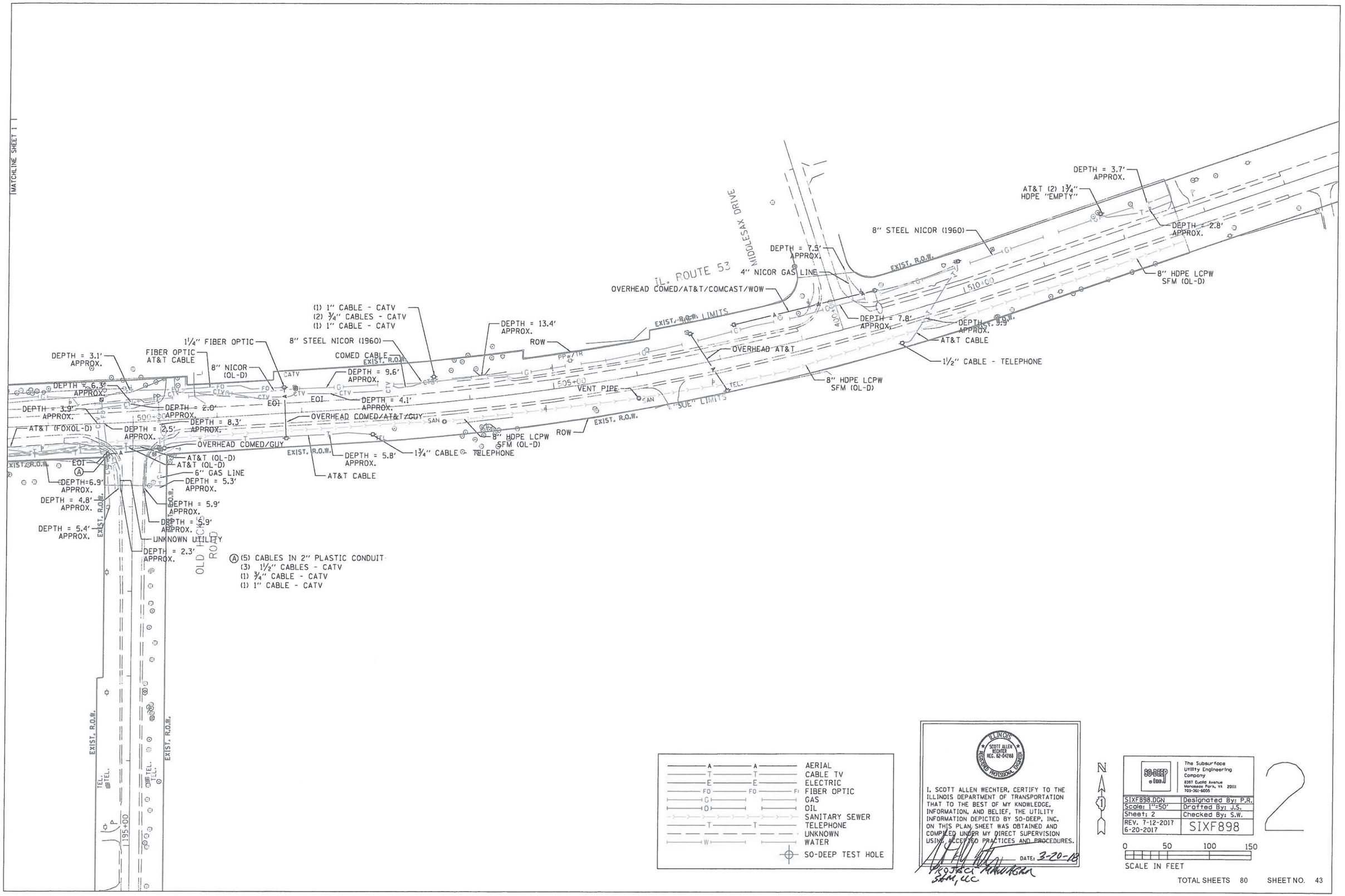
Sheet: 1	Checked By: S.W.
REV: 3-7-2018 6-20-2017	SIXF898

0 50 100 150
SCALE IN FEET

TOTAL SHEETS 80 SHEET NO. 42



A	AERIAL
T	CABLE TV
E	ELECTRIC
FO	FIBER OPTIC
G	GAS
O	OIL
	SANITARY SEWER
T	TELEPHONE
	UNKNOWN
W	WATER
	SO-DEEP TEST HOLE

ILLINOIS
SCOTT ALLEN WECHTER
REG. 62-04788
REGISTERED PROFESSIONAL ENGINEER

I, SCOTT ALLEN WECHTER, CERTIFY TO THE ILLINOIS DEPARTMENT OF TRANSPORTATION THAT TO THE BEST OF MY KNOWLEDGE, INFORMATION, AND BELIEF, THE UTILITY INFORMATION DEPICTED BY SO-DEEP, INC. ON THIS PLAN SHEET WAS OBTAINED AND COMPILED UNDER MY DIRECT SUPERVISION USING ACCEPTED PRACTICES AND PROCEDURES.

DATE: 3-20-18

Scott Wechter
S&H, LLC

N

SO-DEEP
One Call

The Subsurface Utility Engineering Company
8397 Euclid Avenue
Manassas Park, VA 20111
703-361-6005

SIXF898.DGN	Designated By: P.R.
Scale: 1"=50'	Drafted By: J.S.
Sheet: 2	Checked By: S.W.
REV. 7-12-2017 6-20-2017	SIXF898

0 50 100 150
SCALE IN FEET

2

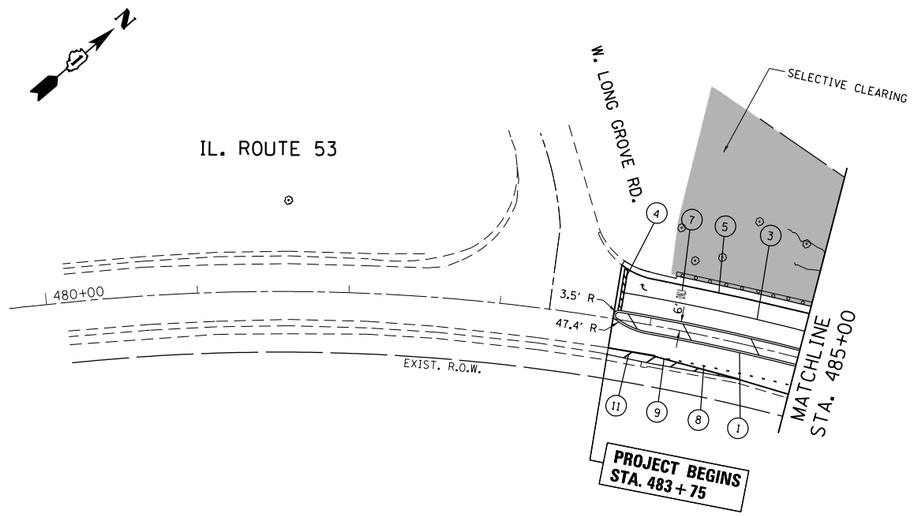
TOTAL SHEETS 80 SHEET NO. 43

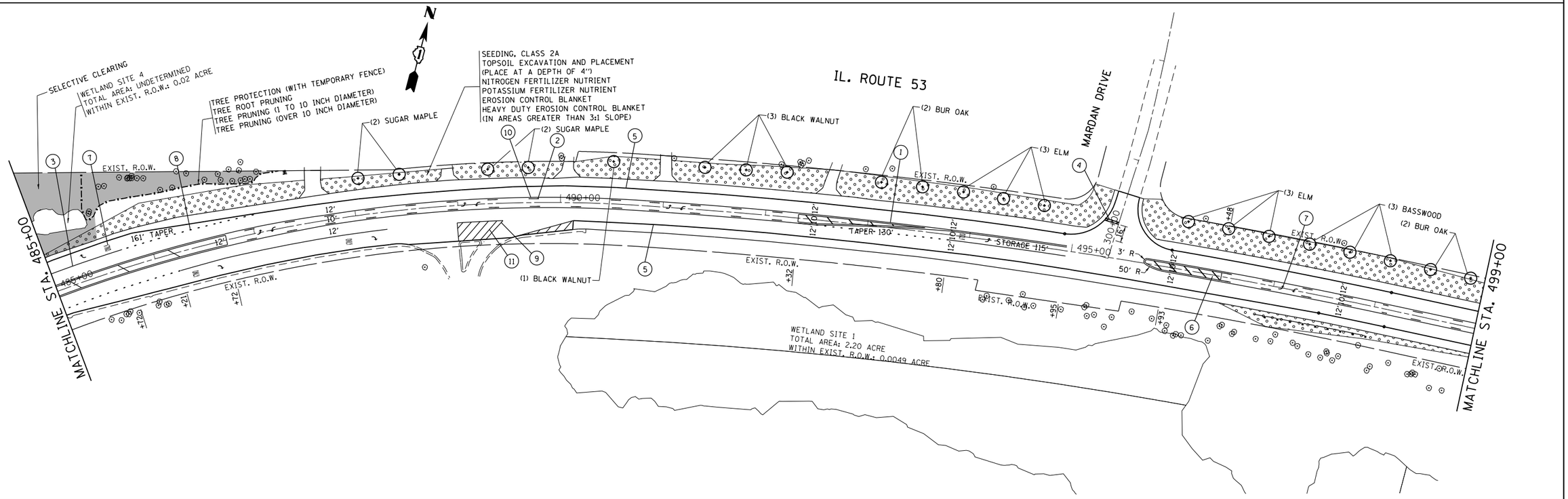
LEGEND

1. THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW LINE, 2 @ 11" C-C (TYP.)
2. THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW LINE, 10' DASH - 30' SKIP (TYP.)
3. THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE LINE (TYP.)
4. THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)
5. THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE EDGE LINE (TYP.)
6. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID YELLOW DIAGONAL LINE (TYP.)
7. THERMOPLASTIC PAVEMENT MARKING, LETTERS & SYMBOLS, SOLID WHITE (TYP.)
8. THERMOPLASTIC PAVEMENT MARKING, 6" SKIP DASH WHITE DOTTED LINE, 2' DASH - 6' SKIP (TYP.)
9. THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE GORE LINE (TYP.)
10. THERMOPLASTIC PAVEMENT MARKING, 4" SOLID YELLOW LINE, 5 1/2 " C-C FROM SKIP-DASH LINE (TYP.)
11. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE DIAGONAL LINE (TYP.)

PLANT SCHEDULE

CODE NO.	QUANTITY (EACH)	BOTANICAL NAME	COMMON NAME	SIZE
A2001720	9	ACER SACCHARUM	SUGAR MAPLE	2-1/2" CALIPER
A2005116	4	JUGLANS NIGRA	BLACK WALNUT	2" CALIPER
A2006716	5	QUERCUS MACROCARPA	BUR OAK	2" CALIPER
A2007116	8	QUERCUS RUBRA	RED OAK	2" CALIPER
A2005820	7	PLATANUS OCCIDENTALIS	SYCAMORE	2-1/2" CALIPER
A2007820	6	TILIA AMERICANA	BASSWOOD	2-1/2" CALIPER
A2008470	9	ULMUS AMERICANA PRINCETON	PRINCETON AMERICAN ELM	2-1/2" CALIPER





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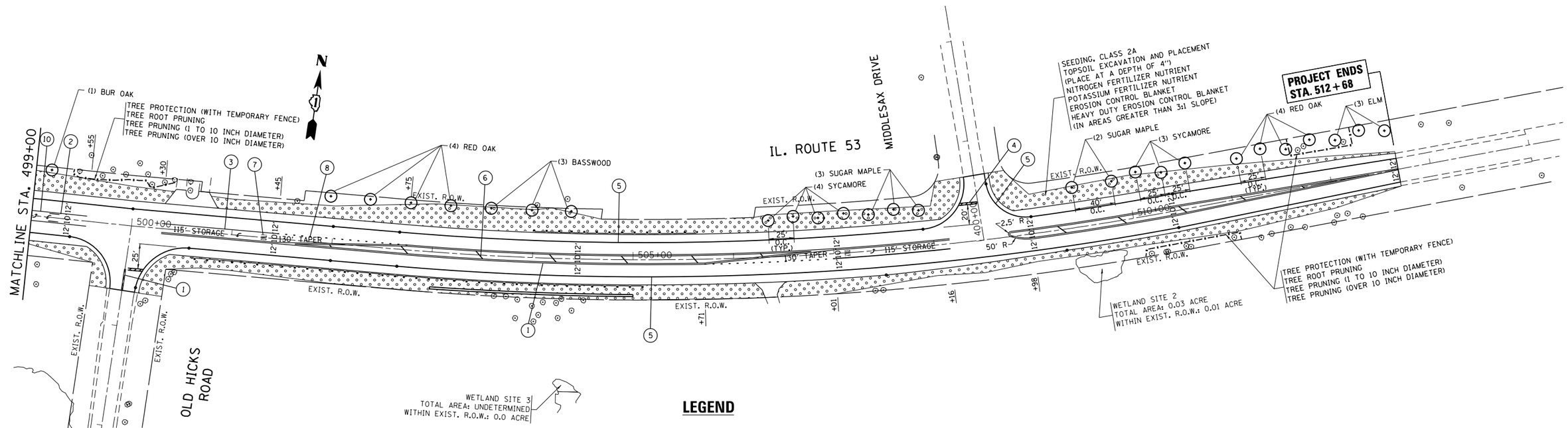
	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND LANDSCAPING PLAN
IL. 53 AT OLD HICKS RD.

SCALE: 1"=50' | SHEET OF SHEETS | STA. 485+00 | TO STA. 499+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	44
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			



LEGEND

1. THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW LINE, 2 @ 11" C-C (TYP.)
2. THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW LINE, 10' DASH - 30' SKIP (TYP.)
3. THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE LINE (TYP.)
4. THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND LANDSCAPING PLAN
IL. 53 AT OLD HICKS RD.

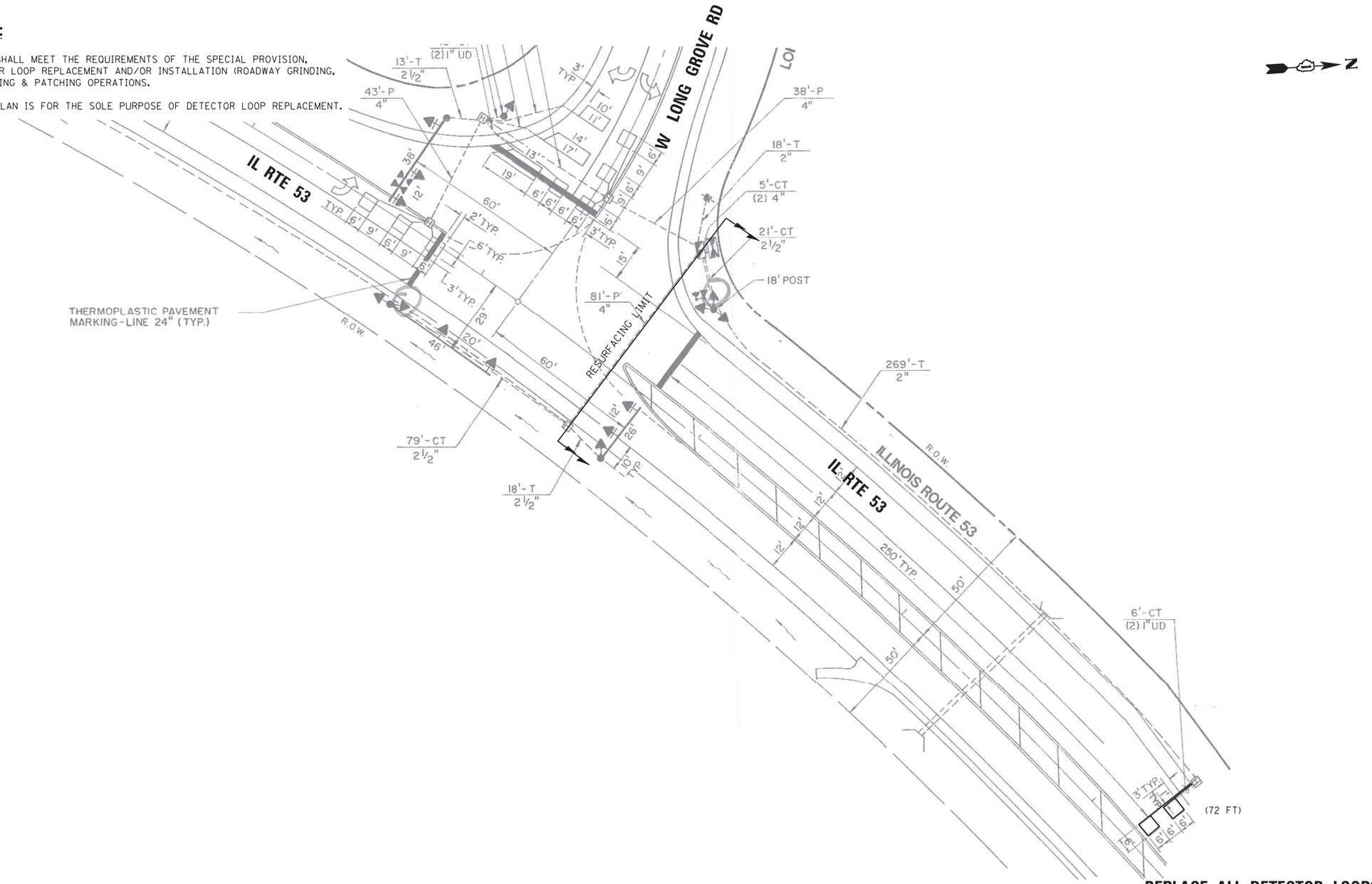
SCALE: 1"=50' | SHEET OF SHEETS | STA. 485+00 TO STA. 499+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	45
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS.

2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	72	FOOT

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DETECTOR LOOP REPLACEMENT PLAN
IL RTE 53 AT LONG GROVE RD

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	46
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			

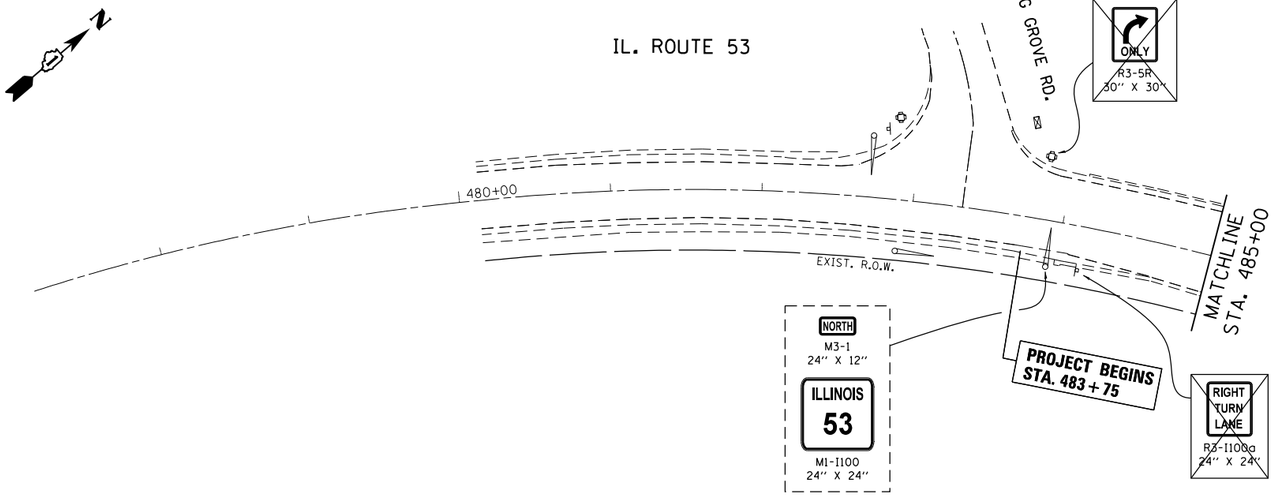
LEGEND:

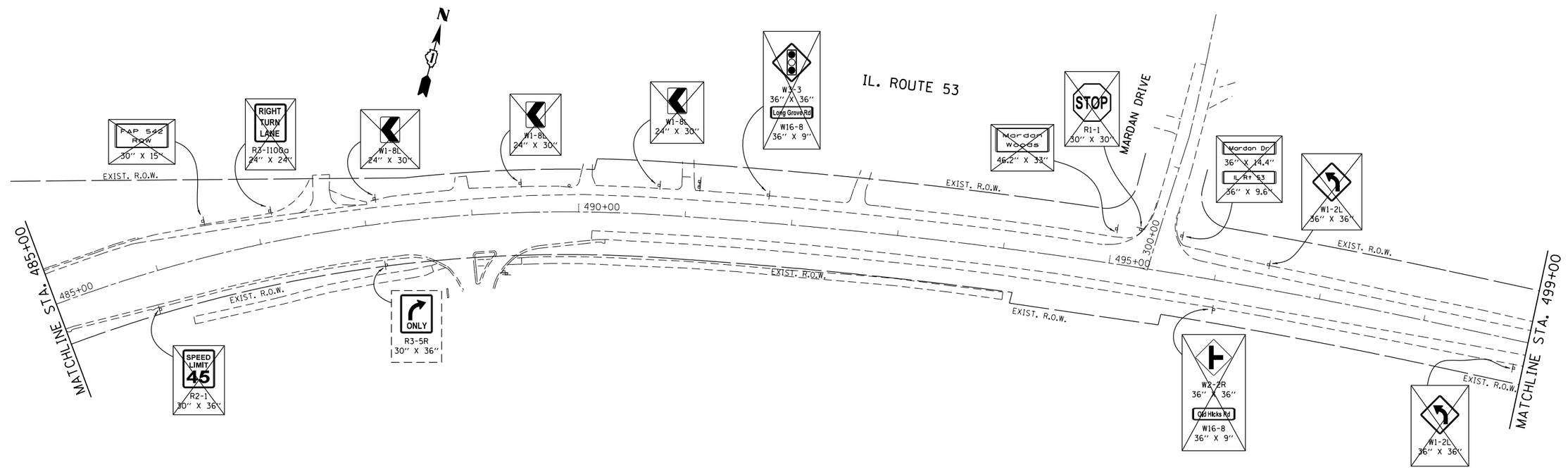
EXISTING SIGN PANEL OR SIGN PANEL ASSEMBLY TO REMAIN

EXISTING SIGN PANEL OR SIGN PANEL ASSEMBLY TO BE REMOVED OR RELOCATED

NOTE:

SEE "EXISTING IL 53 SIGNS" SCHEDULE FOR ADDITIONAL DETAILS.





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	DRAWN -	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 3/20/2018	DATE -	REVISED -	

EXISTING SIGNING PLAN
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	47
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			

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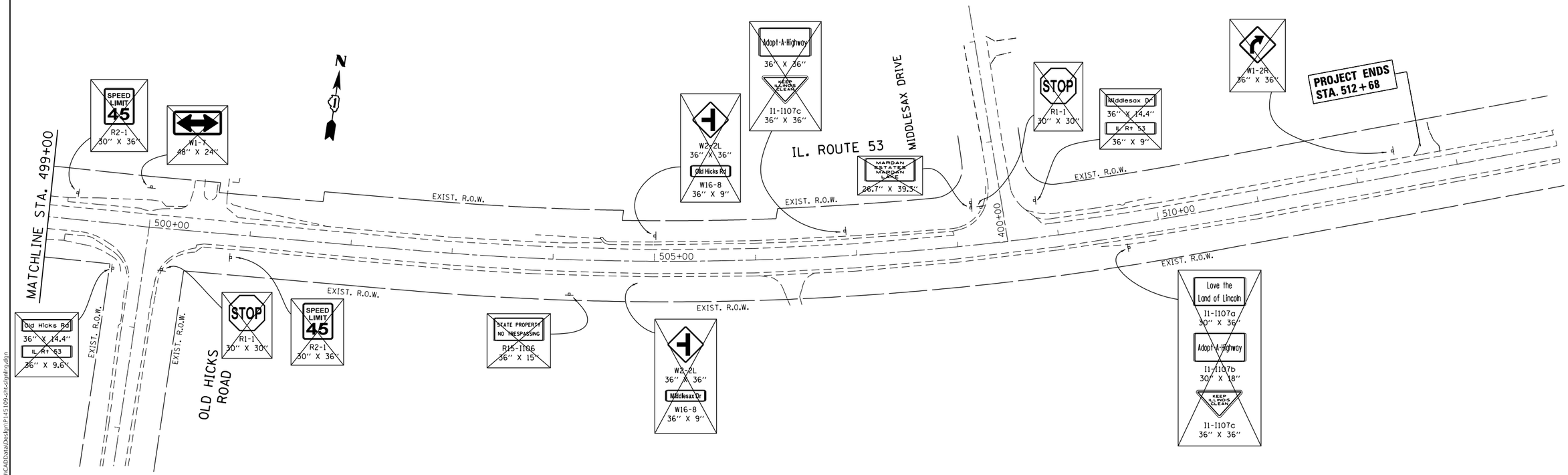

LEGEND:

ILLINOIS 53
M1-1100
24" X 24"

EXISTING SIGN PANEL OR SIGN PANEL ASSEMBLY TO REMAIN

ILLINOIS 53
M1-1100
24" X 24"

EXISTING SIGN PANEL OR SIGN PANEL ASSEMBLY TO BE REMOVED OR RELOCATED

NOTE:

SEE "EXISTING IL 53 SIGNS" SCHEDULE FOR ADDITIONAL DETAILS.

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PLOT DATE = 3/20/2018	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING SIGNING PLAN
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	48
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			

EXISTING IL 53 SIGNS:

STATION	OFFSET	FACING TRAFFIC	PANEL DESCRIPTION	SIZE (W x H)	SIGN CODE	NOTES	ACTION	REMOVE SIGN PANEL - TYPE 1 (SQ FT)	REMOVE SIGN PANEL ASSEMBLY - TYPE A (EACH)	REMOVE SIGN PANEL ASSEMBLY - TYPE B (EACH)
483+85	41.9' LT	WB	MANDATORY MOVEMENT LANE CONTROL	30" x 30"	R3-5R	ON SIGNAL POST	SIGN PANEL TO BE REMOVED	6.25	-	-
483+94	30.5' RT	EB	CARDINAL DIRECTION (NORTH)	24" x 12"	M3-1	ON MAST ARM POLE	SIGN TO REMAIN	-	-	-
483+94	30.5' RT	EB	ILLINOIS ROUTE 53	24" x 24"	M1-I100	ON MAST ARM POLE	SIGN TO REMAIN	-	-	-
* 484+15	29.0' RT	EB	RIGHT TURN LANE	24" x 24"	R3-I100a		SIGN & POST TO BE REMOVED	-	1	-
485+92	35.8' RT	EB	SPEED LIMIT (45)	30" x 36"	R2-1		SIGN & POST TO BE REMOVED	-	1	-
486+52	33.5' LT	WB	FAP 542 ROW	30" x 15"	-		SIGN & POST TO BE REMOVED	-	1	-
487+14	28.8' LT	WB	RIGHT TURN LANE	24" x 24"	R3-I100a		SIGN & POST TO BE REMOVED	-	1	-
488+11	24.0' LT	WB	CHEVRON ALIGNMENT	24" x 30"	W1-8L		SIGN & POST TO BE REMOVED	-	1	-
488+14	39.1' RT	EB	MANDATORY MOVEMENT LANE CONTROL	30" x 36"	R3-5R		SIGN AND POST TO REMAIN	-	-	-
489+46	27.4' LT	WB	CHEVRON ALIGNMENT	24" x 30"	W1-8L		SIGN & POST TO BE REMOVED	-	1	-
490+74	27.6' LT	WB	CHEVRON ALIGNMENT	24" x 30"	W1-8L		SIGN & POST TO BE REMOVED	-	1	-
491+76	26.0' LT	WB	SIGNAL AHEAD	36" x 36"	W3-3		SIGNS AND POST TO BE REMOVED	-	-	1
			ADVANCED STREET NAME PLAQUE (LONG GROVE RD)	36" x 9"	W16-8					
* 495+02	32.7' LT	EB/WB	MARDAN WOODS	46.2" x 33"	-		SIGN & POST TO BE REMOVED	-	-	1
495+24	35.6' LT	SB	STOP	30" x 30"	R1-1		SIGN & POST TO BE REMOVED	-	1	-
* 495+63	35.6' LT	-	MARDAN DR	36" x 14.4"	-	EXISTING STREET SIGNS	SIGNS AND POST TO BE REMOVED	-	1	-
			IL RT 53	36" x 9.6"	-					
496+04	27.4' RT	EB	SIDE ROAD	36" x 36"	W2-2R		SIGNS AND POST TO BE REMOVED	-	-	1
			ADVANCED STREET NAME PLAQUE (OLD HICKS RD)	36" x 9"	W16-8					
496+47	22.9' LT	WB	CURVE	36" x 36"	W1-2L		SIGN & POST TO BE REMOVED	-	1	-
498+92	26.1' RT	EB	CURVE	36" x 36"	W1-2L		SIGN & POST TO BE REMOVED	-	1	-
499+26	26.4' LT	WB	SPEED LIMIT (45)	30" x 36"	R2-1		SIGN & POST TO BE REMOVED	-	1	-
* 499+68	43.9' RT	-	OLD HICKS RD	36" x 14.4"	-	EXISTING STREET SIGNS	SIGNS AND POST TO BE REMOVED	-	1	-
			IL RT 53	36" x 9.6"	-					
499+98	39.7' LT	NB	TWO DIRECTION LARGE ARROW	48" x 24"	W1-7		SIGN & POST TO BE REMOVED	-	1	-
500+16	40.8' RT	NB	STOP	30" x 30"	R1-1		SIGN & POST TO BE REMOVED	-	1	-
500+83	22.7' RT	EB	SPEED LIMIT (45)	30" x 36"	R2-1		SIGN & POST TO BE REMOVED	-	1	-
504+17	34.0' RT	NB	STATE PROPERTY NO TRESPASSING	36" x 15"	R15-I106		SIGN & POST TO BE REMOVED	-	1	-
504+86	21.9' RT	EB	SIDE ROAD	36" x 36"	W2-2L		SIGNS AND POST TO BE REMOVED	-	-	1
			ADVANCED STREET NAME PLAQUE (MIDDLESAX DR)	36" x 9"	W16-8					
505+00	24.9' LT	WB	SIDE ROAD	36" x 36"	W2-2L		SIGNS AND POST TO BE REMOVED	-	-	1
			ADVANCED STREET NAME PLAQUE (OLD HICKS RD)	36" x 9"	W16-8					
* 506+90	24.4' LT	WB	ADOPT-A-HIGHWAY	36" x 36"	-		SIGNS AND POST TO BE REMOVED	-	-	1
			KEEP ILLINOIS CLEAN / CLEANUP CREW WORKING	36" x 36"	I1-I107c					
* 508+17	39.4' LT	EB/WB	MARDAN ESTATES, MARDAN LAKE	26.7" x 39.3"	-		SIGN & POST TO BE REMOVED	-	1	-
508+27	36.3' LT	SB	STOP	30" x 30"	R1-1		SIGN & POST TO BE REMOVED	-	1	-
* 508+81	36.7' LT	-	MIDDLESAX DR	36" x 14.4"	-	EXISTING STREET SIGNS	SIGNS AND POST TO BE REMOVED	-	1	-
			IL RT 53	36" x 9"	-					
* 509+67	22.4' RT	EB	LOVE THE LAND OF LINCOLN	30" x 36"	I1-I107a		SIGNS AND POST TO BE REMOVED	-	-	1
			ADOPT-A-HIGHWAY	30" x 18"	I1-I107b					
			KEEP ILLINOIS CLEAN / CLEANUP CREW WORKING	36" x 36"	I1-I107c					
512+39	24.7' LT	WB	CURVE	36" x 36"	W1-2R		SIGN & POST TO BE REMOVED	-	1	-

* THIS SIGN PANEL ASSEMBLY SHALL BE RELOCATED. SEE THE "PROPOSED IL 53 SIGNS" SCHEDULE FOR LOCATION.

NOTE: THE CONTRACTOR SHALL TAKE EXTRA CARE WHILE REMOVING AND RELOCATING THE SIGNS AT STA. 495+02, 32.7' LT AND AT STA. 508+17, 39.4' LT.

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	DRAWN -	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 3/20/2018	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING SIGNING SCHEDULE
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	49
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

LEGEND:

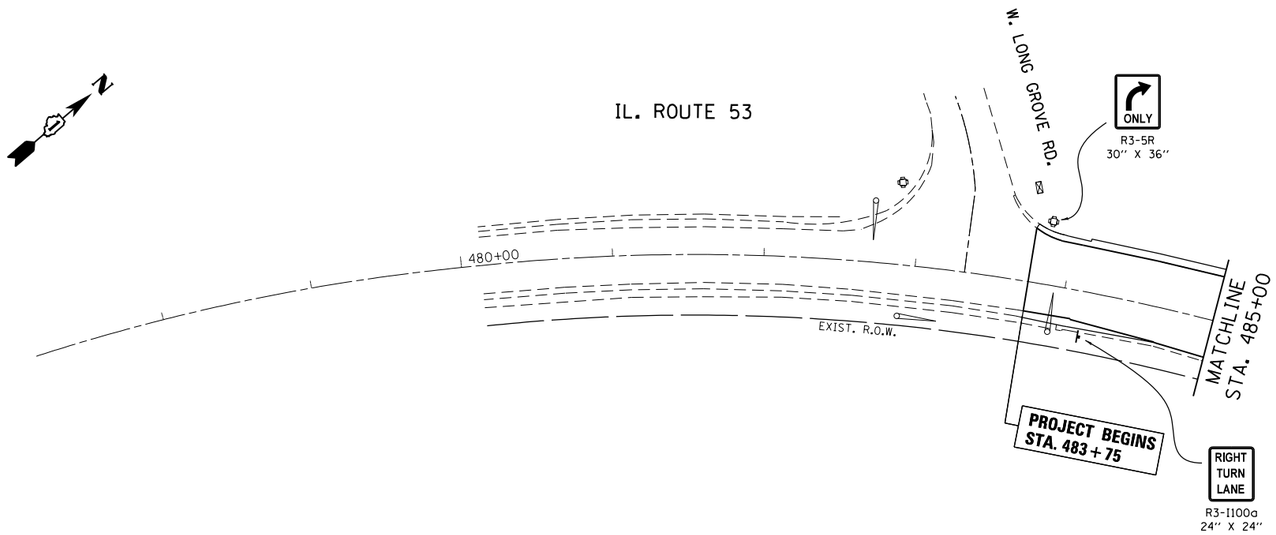
ILLINOIS 53
M1-1100
24" X 24"

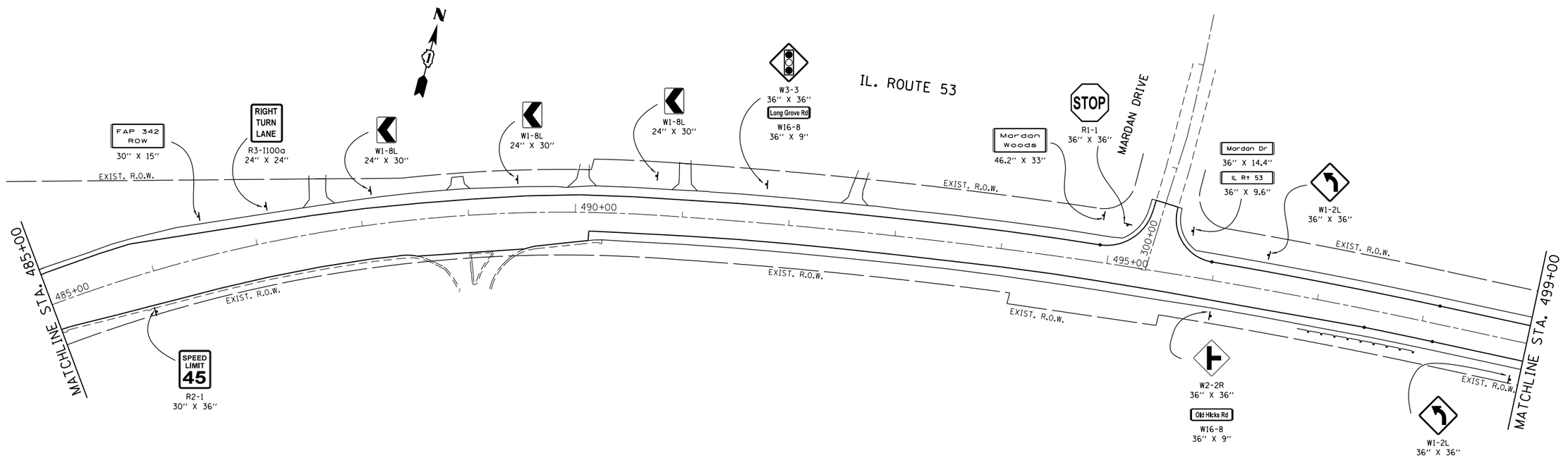
PROPOSED SIGN PANEL
OR SIGN PANEL ASSEMBLY
TO BE RELOCATED

NOTES:

1. SEE "PROPOSED IL 53 SIGNS" SCHEDULE FOR ADDITIONAL DETAILS.

2. FINAL PROPOSED LOCATIONS SHALL BE ESTABLISHED IN THE FIELD PER ENGINEER DIRECTION.





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	DRAWN -	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 3/20/2018	DATE -	REVISED -	

PROPOSED SIGNING PLAN
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	50
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			

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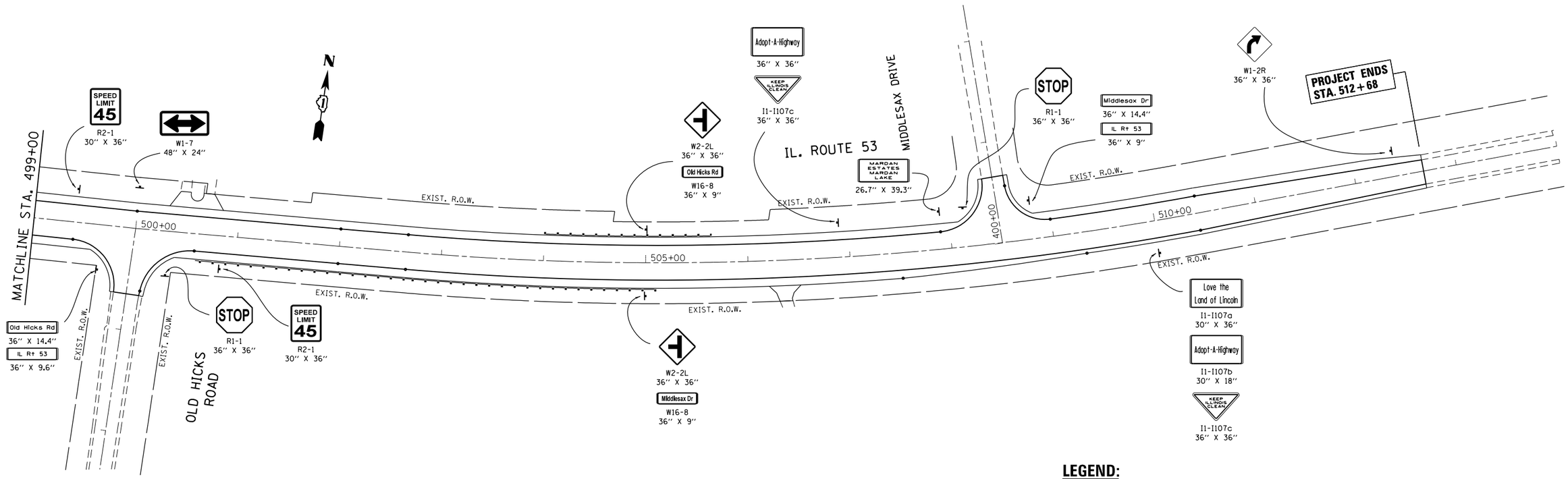

LEGEND:

ILLINOIS 53
M1-1100
24" X 24"

PROPOSED SIGN PANEL
OR SIGN PANEL ASSEMBLY
TO BE RELOCATED

NOTES:

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2. FINAL PROPOSED LOCATIONS SHALL BE ESTABLISHED IN THE FIELD PER ENGINEER DIRECTION.

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PLOT DATE = 3/20/2018	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED SIGNING PLAN
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	51
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

PROPOSED IL 53 SIGNS:

STATION	OFFSET	FACING TRAFFIC	SIGN CODE	SIZE (W X H)	PANEL DESCRIPTION	RELOCATE SIGN PANEL ASSEMBLY - TYPE A (EACH)	RELOCATE SIGN PANEL ASSEMBLY - TYPE B (EACH)	SIGN PANEL - TYPE 1 (SQ FT)	TELESCOPING STEEL SIGN SUPPORT (FT)	BASE FOR TELESCOPING STEEL SIGN SUPPORT (EA)	NOTES
483+85	41.9' LT	WB	R3-5R	30" x 36"	MANDATORY MOVEMENT LANE CONTROL	-	-	7.50	-	-	TO BE INSTALLED ON EXISTING SIGNAL POST
484+15	30.0' RT	EB	R3-I100a	24" x 24"	RIGHT TURN LANE	1	-	-	-	-	
485+92	37.2' RT	EB	R2-1	30" x 36"	SPEED LIMIT (45)	-	-	7.50	16.00	1	
486+53	38.3' LT	WB	-	30" x 15"	FAP 342 ROW	-	-	3.13	14.25	1	
487+15	33.6' LT	WB	R3-I100a	24" x 24"	RIGHT TURN LANE	-	-	4.00	15.50	1	
488+11	32.0' LT	WB	W1-8L	24" x 30"	CHEVRON ALIGNMENT	-	-	5.00	14.00	1	
489+46	31.3' LT	WB	W1-8L	24" x 30"	CHEVRON ALIGNMENT	-	-	5.00	15.00	1	
490+74	36.7' LT	WB	W1-8L	24" x 30"	CHEVRON ALIGNMENT		-	5.00	13.50	1	
491+76	36.5' LT	WB	W3-3	36" x 36"	SIGNAL AHEAD	-	-	9.00	17.25	1	
			W16-8	36" x 9"	ADVANCED STREET NAME PLAQUE (LONG GROVE RD)			2.25			
494+90	44.0' LT	EB/WB	-	46.2" x 33"	MARDAN WOODS	-	1	-	-	-	
495+14	38.4' LT	SB	R1-1	36" x 36"	STOP	-	-	9.00	14.00	1	
495+74	42.6' LT	-	-	36" x 14.4"	MARDAN DR	1	-	-	-	-	EXISTING STREET SIGNS
			-	36" x 9.6"	IL RT 53						
496+04	33.1' RT	EB	W2-2R	36" x 36"	SIDE ROAD	-	-	9.00	17.75	1	
			W16-8	36" x 9"	ADVANCED STREET NAME PLAQUE (OLD HICKS RD)			2.25			
496+47	32.5' LT	WB	W1-2L	36" x 36"	CURVE	-	-	9.00	16.25	1	
498+92	36.5' RT	EB	W1-2L	36" x 36"	CURVE	-	-	9.00	16.00	1	
499+40	32.5' LT	WB	R2-1	30" x 36"	SPEED LIMIT (45)	-	-	7.50	16.00	1	
499+66	44.0' RT	-	-	36" x 14.4"	OLD HICKS RD	1	-	-	-	-	EXISTING STREET SIGNS
			-	36" x 9.6"	IL RT 53						
500+00	41.0' LT	NB	W1-7	48" x 24"	TWO DIRECTION LARGE ARROW	-	-	8.00	14.25	1	
500+32	43.0 ' RT	NB	R1-1	36" x 36"	STOP	-	-	9.00	14.00	1	
500+85	32.5' RT	EB	R2-1	30" x 36"	SPEED LIMIT (45)	-	-	7.50	16.00	1	
505+00	32.5' RT	EB	W2-2L	36" x 36"	SIDE ROAD	-	-	9.00	17.75	1	
			W16-8	36" x 9"	ADVANCED STREET NAME PLAQUE (MIDDLESAX DR)			2.25			
505+00	32.5' LT	EB	W2-2L	36" x 36"	SIDE ROAD	-	-	9.00	17.25	1	
			W16-8	36" x 9"	ADVANCED STREET NAME PLAQUE (OLD HICKS RD)			2.25			
506+90	34.0' LT	WB	-	36" x 36"	ADOPT-A-HIGHWAY	-	1	-	-	-	
			I1-I107c	36" x 36"	KEEP ILLINOIS CLEAN / CLEANUP CREW WORKING						
507+90	37.0' LT	EB/WB	-	26.7" x 39.3"	MARDAN ESTATES, MARDAN LAKE	1	-	-	-	-	
508+14	37.9' LT	SB	R1-1	36" x 36"	STOP	-	-	9.00	14.00	1	
508+80	38.0' LT	-	-	36" x 14.4"	MIDDLESAX DR	1	-	-	-	-	EXISTING STREET SIGNS
			-	36" x 9"	IL RT 53						
510+00	32.0' RT	EB	I1-I107a	30" x 36"	LOVE THE LAND OF LINCOLN		1				
			I1-I107b	30" x 18"	ADOPT-A-HIGHWAY						
			I1-I107c	36" x 36"	KEEP ILLINOIS CLEAN / CLEANUP CREW WORKING						
512+39	25.3' LT	WB	W1-2R	36" x 36"	CURVE	-	-	9.00	15.50	1	

USER NAME = tarkqfm	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 3/20/2018	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED SIGNING SCHEDULE
IL 53 AT OLD HICKS RD.

SCALE: | SHEET OF SHEETS | STA. | TO STA.

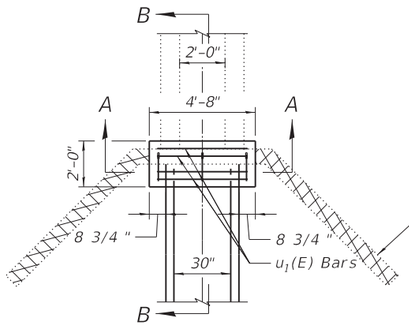
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	52
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

Benchmarks:

BM #1437
N: 2004704.494
E: 1069306.366
ELEV: 763.665
CROSS ON CONCRETE

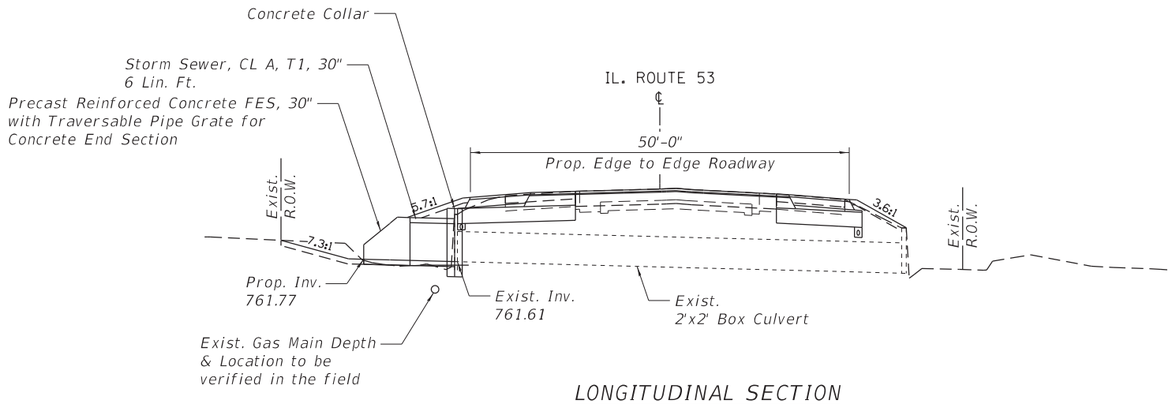
BM #2118
N: 2004901.406
E: 1070352.503
ELEV: 772.585
SPIKE IN UTILITY POLE

Existing Structure:
Existing structure is a 2'x2' box culvert.

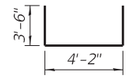


PLAN

Partial Removal of existing Concrete End Section. Remove only as needed for installation of proposed Concrete Collar and proposed embankment grading.



LONGITUDINAL SECTION



BAR u_1(E)

DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications, 7th Edition with 2015 Interims

DESIGN STRESSES

FIELD UNITS

f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)

LOADING HL-93

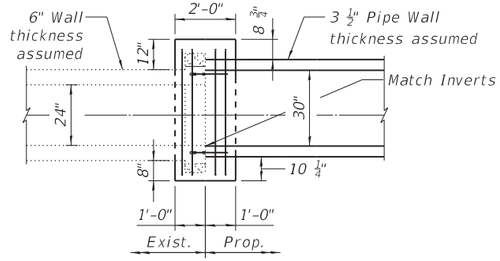
Allow 50#/sq. ft. for future wearing surface.

BILL OF MATERIALS

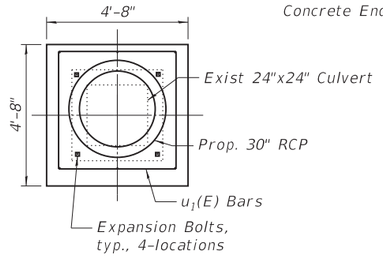
Bar	No.	Size	Length	Shape
u_1(E)	8	#4	11'- 6"	⌴
Reinforcement Bars, Epoxy Coated			Pound	60
Expansion Bolts 3/4 Inch			Each	4
Concrete Collar			Cu. Yd.	1.1

Notes:

1. See Roadway Plans for Construction Staging and Maintenance of Traffic.
2. Flow Diversion, as needed for completion of the culvert extension is the Contractor's responsibility to design and install.
3. All work, materials and labor required for completing Flow Diversionsare to be included in contract unit price for Concrete Collar.
4. Storm Sewer Pipe and Precast Reinforced Concrete FES shall be paid for per the Standard Specifications, including all work associatedwith their installation.
5. Partial Removal of existing Concrete End Section shall be included in cost of Concrete Collar.

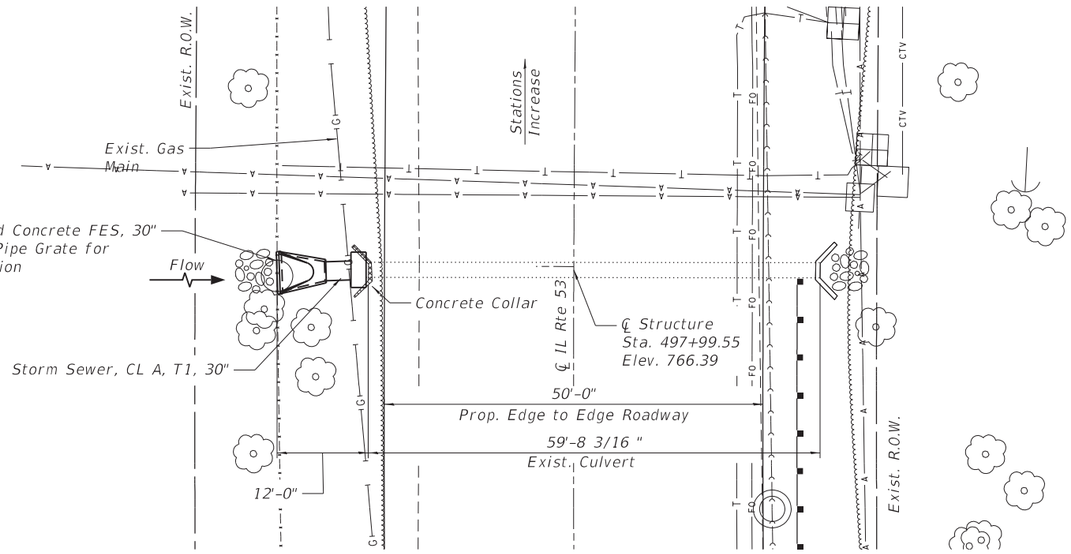


SECT. B-B

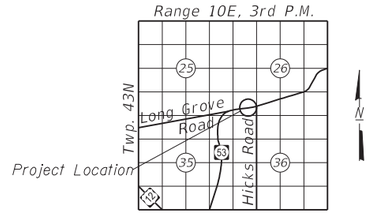


SECT. A-A

COLLAR DETAILS



PLAN



LOCATION SKETCH

GENERAL PLAN AND ELEVATION
PROPOSED CULVERT EXTENSION
IL 53
SECTION 530N-3
LAKE COUNTY
STATION 497+99.55

HRG PROJECT NO.: 8600096.63
HRG PROJ. CONTACT:
FILE NAME: 8600096.63_RCP_GPE_00.dgn
PLOT DRIVER: IL.pdf.bw.pltcfg
PEN TABLE: plotTable.tbl

HRGreen
HRGreen.com
Illinois Professional Design Firm # 184-001322

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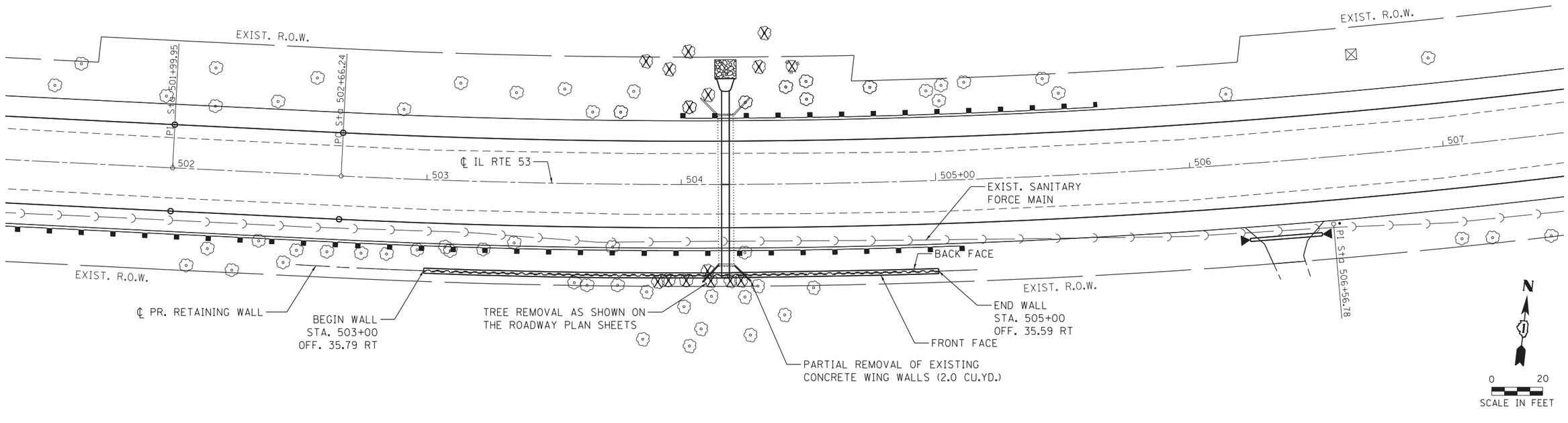
	CHECKED - TEH	REVISED -
PLOT SCALE =	DRAWN - WJH	REVISED -
PLOT DATE = 3/12/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

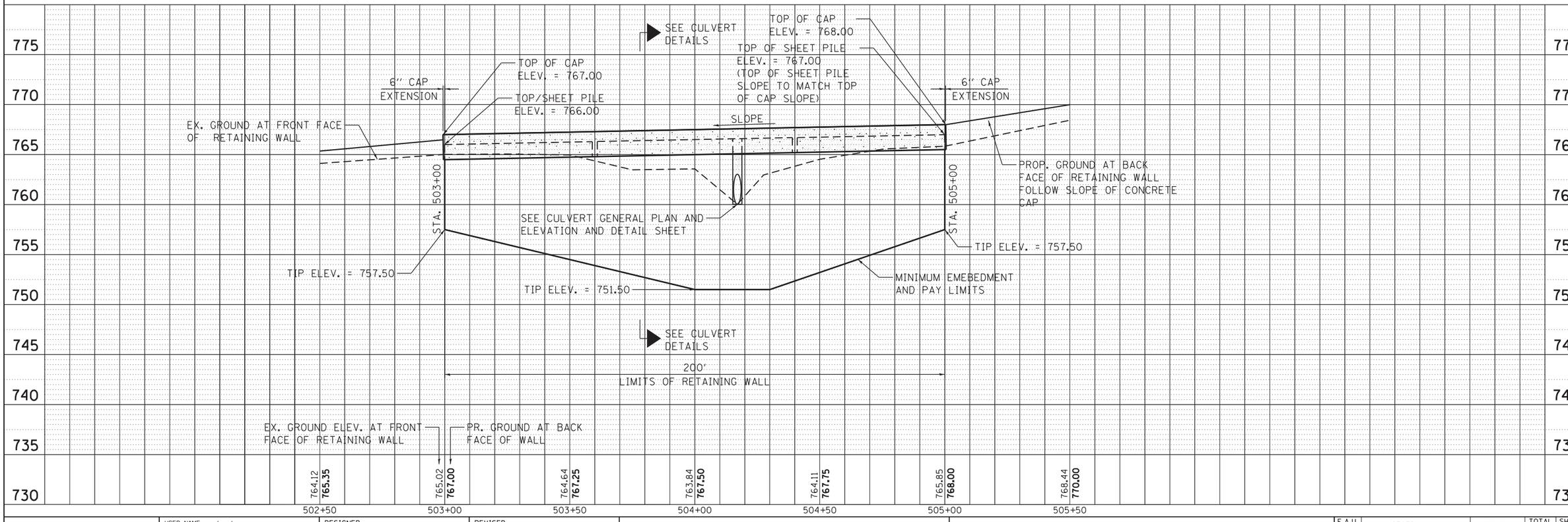
CULVERT DETAIL PLANS (STA. 497+99.55)
IL 53 AT OLD HICKS RD.
SHEET NO. OF SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	53
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

PLAN
SURVEYED
PLOTTED
ALIGNMENT CHECKED
RT. OF WAY CHECKED
CADD FILE NAME
NOTE BOOK
NO.
BY
DATE



PROFILE
SURVEYED
PLOTTED
DRAWN
CHECKED
BAK. NOTED
STRUCTURE NOTATNS CHKD
NOTE BOOK
NO.
BY
DATE



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PEN TABLE:

HRGreen
HRGreen.com
Illinois Professional Design Firm
184-001322

USER NAME = whood	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE =	CHECKED -	REVISED -
PLOT DATE = 3/14/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET PILE RETAINING WALL PLAN AND PROFILE
IL 53 AT OLD HICKS RD.

SCALE: 1"=20' | SHEET OF SHEETS | STA. 502+50 TO STA. 505+50

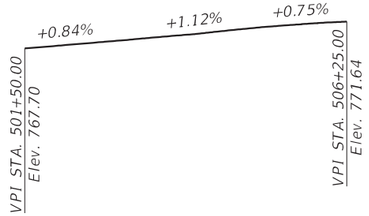
F.A.U RTE.	SECTION NO.	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	54
		CONTRACT NO. 62B61		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

Benchmarks:

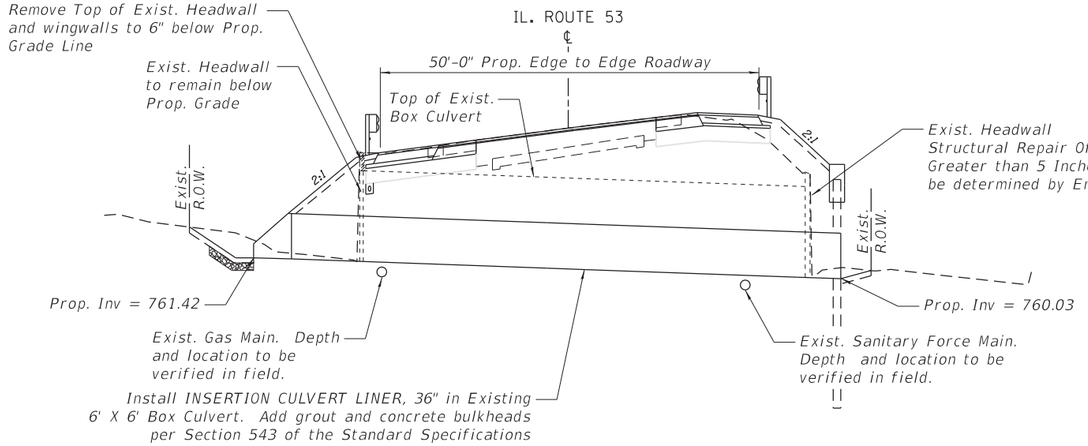
BM #1437
N: 2004704.494
E: 1069306.366
ELEV: 763.665
CROSS ON CONCRETE

BM #2118
N: 2004901.406
E: 1070352.503
ELEV: 772.585
SPIKE IN UTILITY POLE

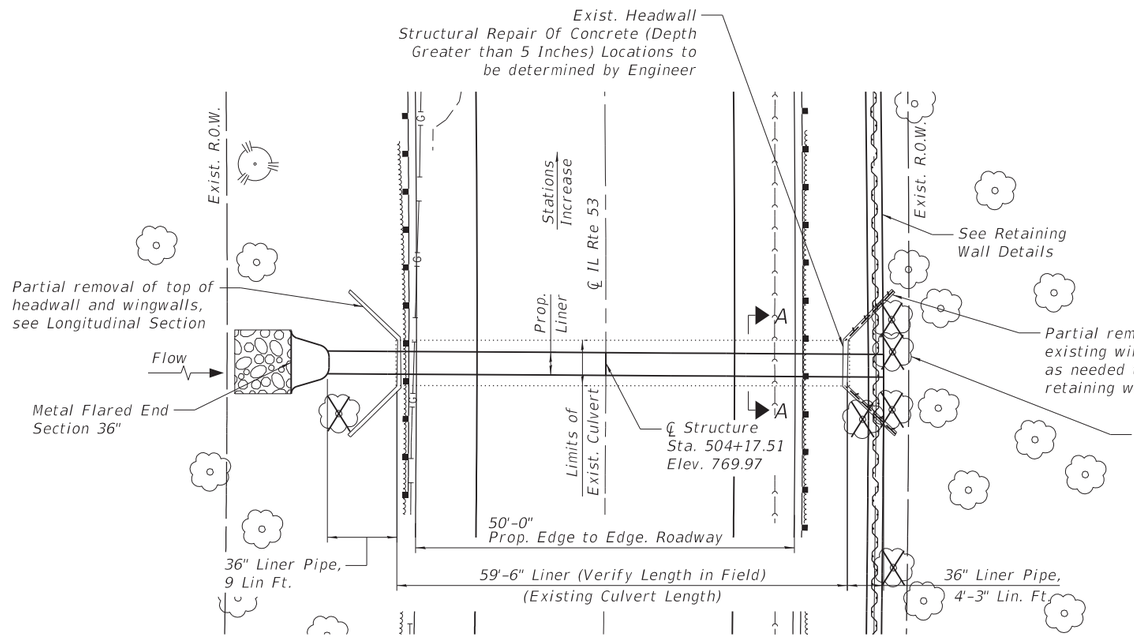
Existing Structure:
Existing structure is approximately a 72"x72" box culvert.



PROFILE GRADE



LONGITUDINAL SECTION



PLAN

DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications, 7th Edition with 2015 Interims

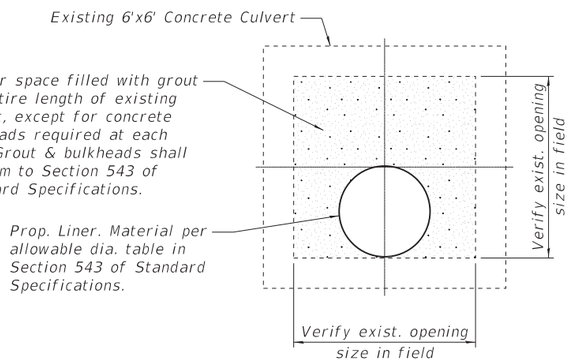
DESIGN STRESSES

FIELD UNITS

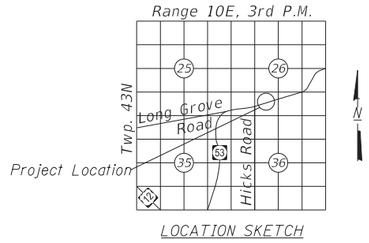
f'_c = 3,500 psi

LOADING HL-93

Allow 50#/sq. ft. for future wearing surface.



SECTION A-A



LOCATION SKETCH

Notes:

1. Existing Concrete Box Culvert size to be field verified prior to ordering Liner.
2. See Roadway Plans for Construction Staging and Maintenance of Traffic.
3. Flow Diversion, as needed for completion of the culvert lining, are the Contractor's responsibility to design and install.
4. All work, materials and labor required for completing Flow Diversionsare to be included in contract unit price for Insertion Culvert Liner.
5. Concrete Bulkheads shall be installed at each end of Culvert per Section 543 of the Standard Specifications and included in the cost of Insertion Culvert Liner.
6. Insertion Culvert Liner, 36" shall conform to Section 543 of the Standard Specifications with the following exceptions:
   a. The Liner Pipe size shall be as specified on these plans in place of the sizing requirements stated in Article 543.02 of the Standard Specifications.
   b. The list of pre-approved Liner Pipe material listed in Section 543 shall apply, except that the use of Corrugated Steel and/or Steel Casing will not be permitted without prior approval by IDOT.
7. Costs for the portion of the 36" Culvert Liner that extends past the exist. culvert end section (per plan) and the partial removal of the exist. Headwalls and/or wingwalls shall be included in the cost of Insertion Culvert Liner, 36".

GENERAL PLAN AND ELEVATION
PROPOSED CULVERT EXTENSION
IL 53
SECTION 530N-3
LAKE COUNTY
STATION 504+17.51

HRG PROJECT NO.: 8650046.8
HRG PROJ.CONTACT: 8610096.63.RCP_GPE_002.dgn
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PLOT DRIVER: plotlabs.tbl
PEN TABLE:

HRGreen
HRGreen.com
Illinois Professional Design Firm # 184-001322

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	CHECKED -	REVISED -
PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE = 3/15/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CULVERT DETAIL PLANS (STA. 504+17.51)
IL 53 AT OLD HICKS RD.
SHEET NO. OF SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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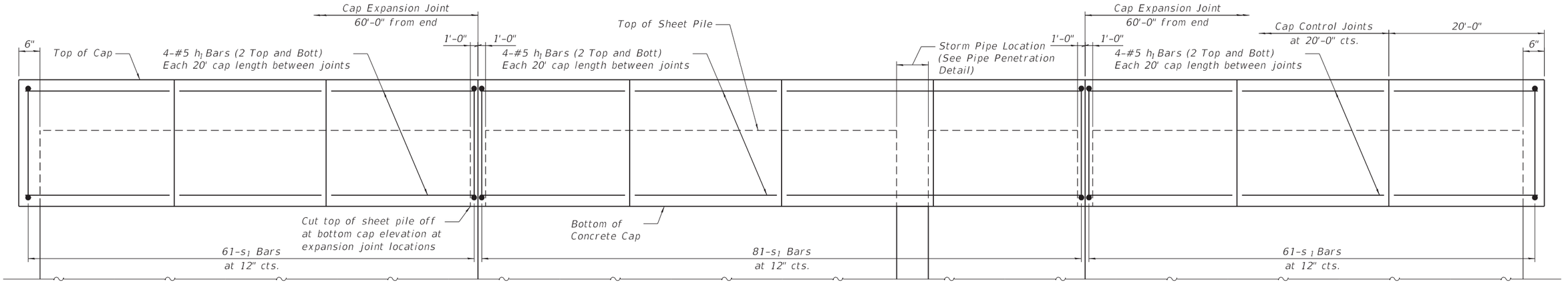
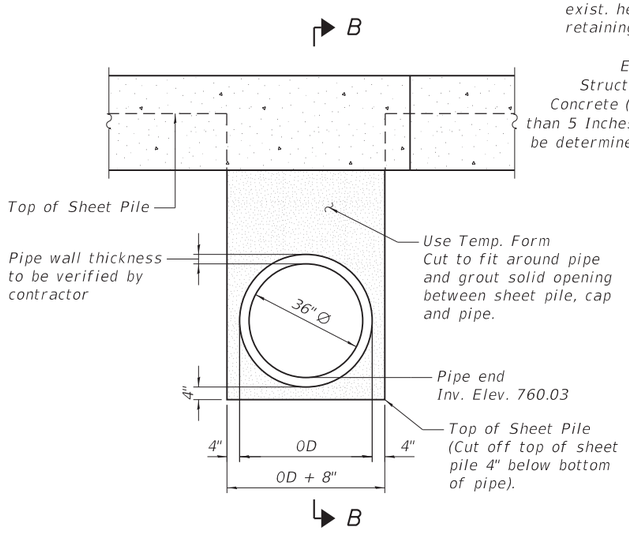
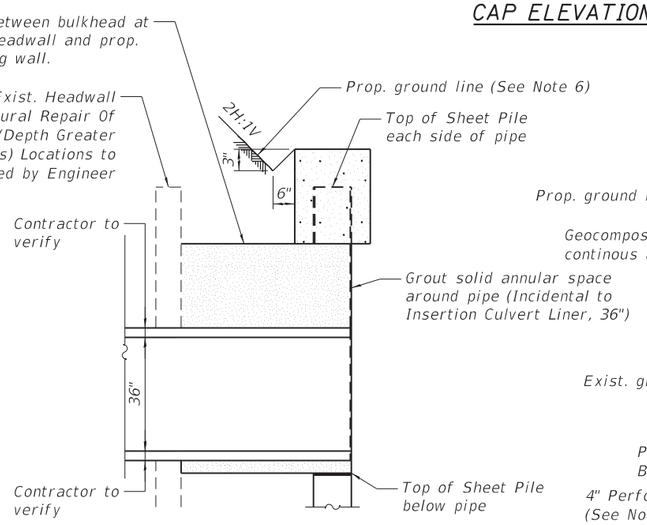
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			



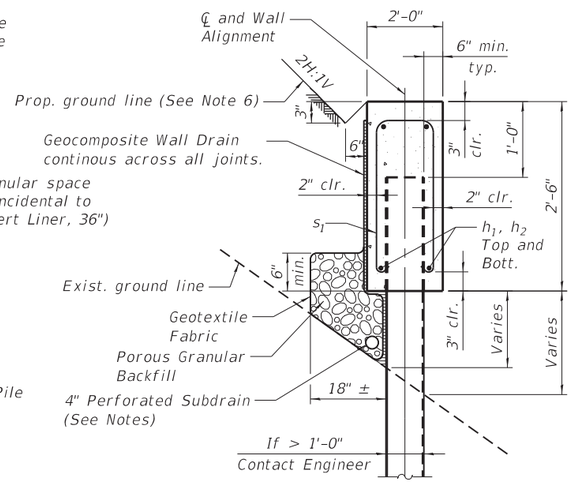
CAP ELEVATION



PIPE PENETRATION



SECTION B-B



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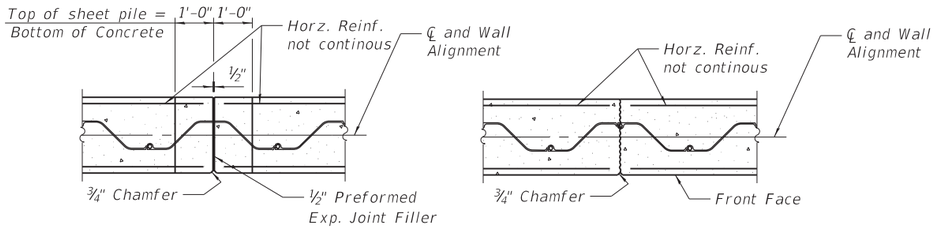
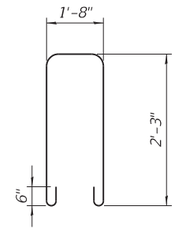


EXPANSION JOINT

CONTROL JOINT



BAR s_1

GENERAL NOTES:

1. Bars indicated thus 12 x 4 – #5 etc., indicates 12 lines of bars with 4 lengths per line.
2. The cost of furnishing and placing subdrain, geotextile, porous granular backfill and subdrain outlet beyond the wall limits shall be included in the unit price for PERMANENT SHEET PILING.
3. The minimum effective section modulus of the permanent steel sheet pile wall shall be 15 in.3/ft, with a min. thickness of 5/16".
4. Contractor shall coordinate with the Department a construction sequence that is compatible with the roadway construction sequence, and prevents conflicts between the proposed sheet pile installation and the partial removal of existing concrete wingwalls and/or the installation of the proposed Insertion Culvert Liner and/or flow diversion as needed.
5. Pitch Subdrain to match slope of concrete cap and outlet at west end of wall to existing ditch.
6. Low ground swale behind wall cap can be gradually eliminated as grading approaches east end of wall. No swale required from Station 504+17.51 to Station 505+00

BILL OF BARS

BAR	NO.	SIZE	LENGTH	SHAPE
h_1	40	#5	19'- 8"	
s_1	203	#4	7'- 2"	
CONCRETE STRUCTURES			CU.YD.	38
REINFORCEMENT BARS			POUND	1,793

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Structures	Cu.Yd.	38
Reinforcement Bars	Pound	1792.9
Permanent Sheet Piling	Sq.Ft.	2415
Metal Flared End Section 36"	Each	1
Insertion Culvert Liner 36"	Foot	73
Geocomposite Wall Drain	Sq. Yd.	50
Structural Repair of Concrete (Depth Greater Than 5 Inches)	Sq.Ft.	25

HRG PROJECT NO.: 8650046.18
HRG PROJ. CONTACT:
FILE NAME: D162B61-Str_Details.dgn
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PEN TABLE: plotTable1.tbl

HRGreen
HRGreen.com
Inside Professional Design Firm
184-001322

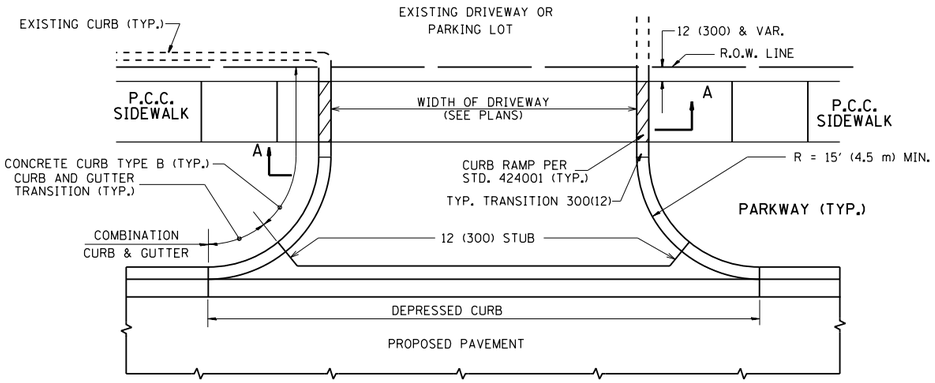
USER NAME = whood	DESIGNED -	REVISED -
	CHECKED -	REVISED -
PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE = 3/14/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

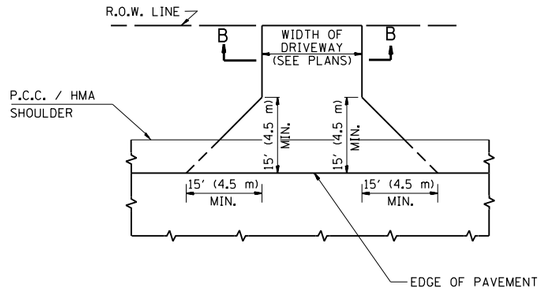
SHEET PILE RETAINING WALL DETAILS
IL 53 AT OLD HICKS RD.

SHEET NO. OF SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	56
		CONTRACT NO. 62B61		
		ILLINOIS FED. AID PROJECT		



WITH CONCRETE CURB, TYPE B



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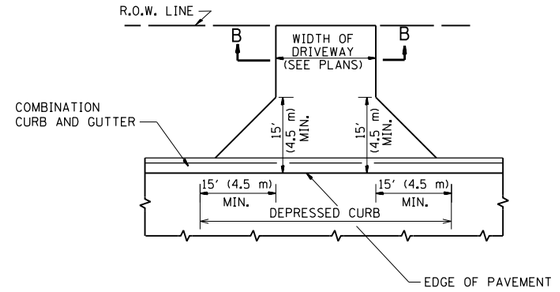

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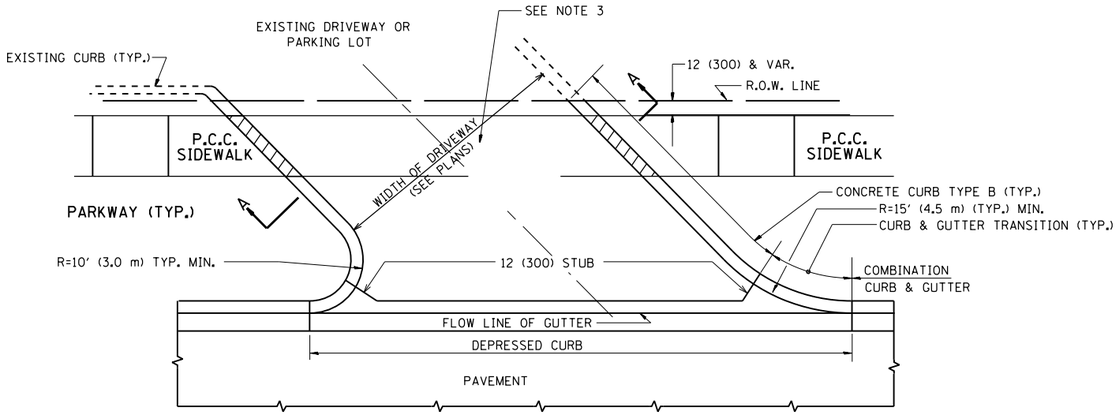

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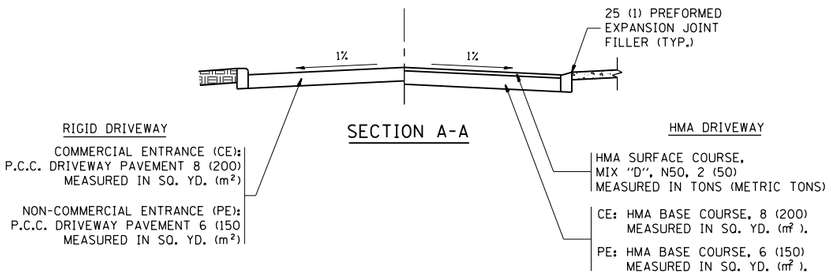
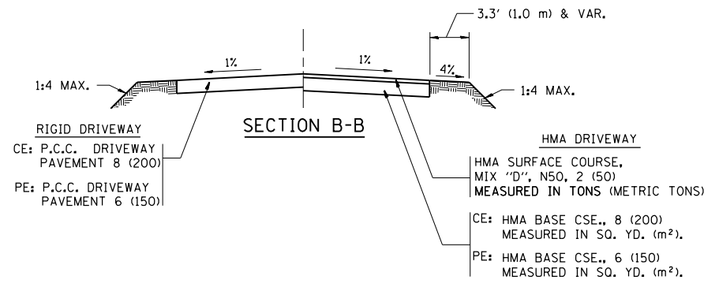




RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE,
MIX "D", N50, 2 (50)
MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200)
MEASURED IN SQ. YD. (m^2).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

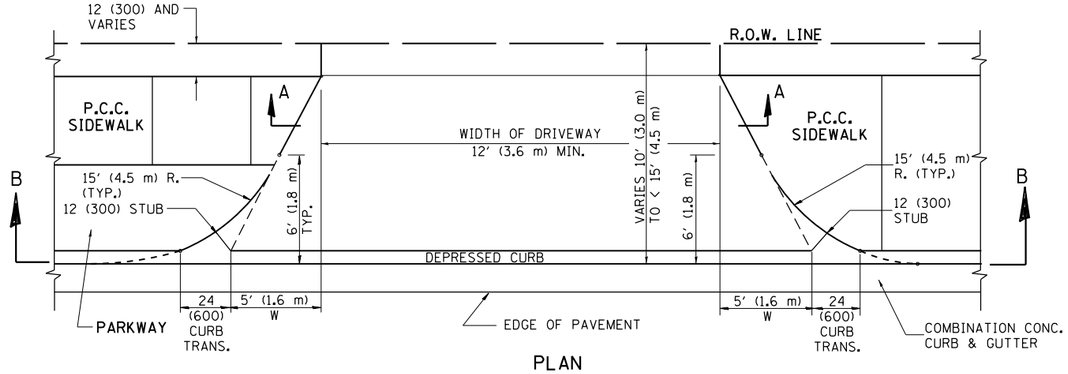
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P145\DRAWN\Data\Design\DistStd.dgn	USER NAME = tariqfm	DESIGNED - R. SHAH	REVISED - P. LaFLUER 04-15-03
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED - R. BORO 01-01-07
	PLOT DATE = 3/20/2018	DATE - 11-04-95	REVISED - R. BORO 06-11-08
			REVISED - R. BORO 09-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS – DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER > = 15' (4.5 m)

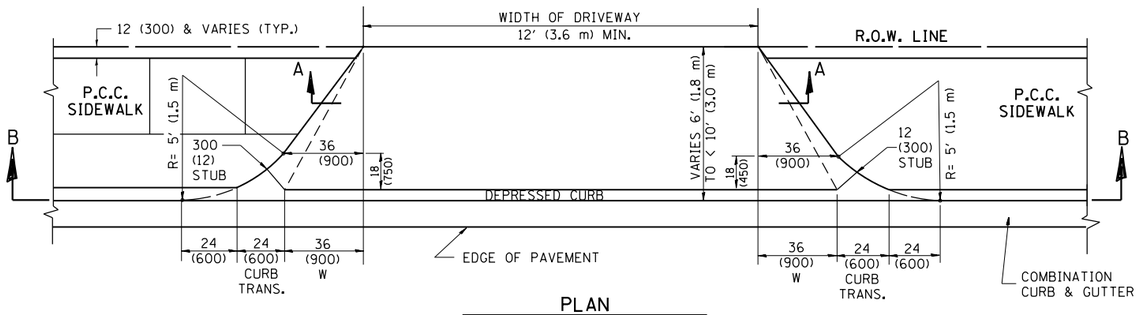
SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	57
	BD0156-07 (BD-01)	CONTRACT NO. 62B61		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



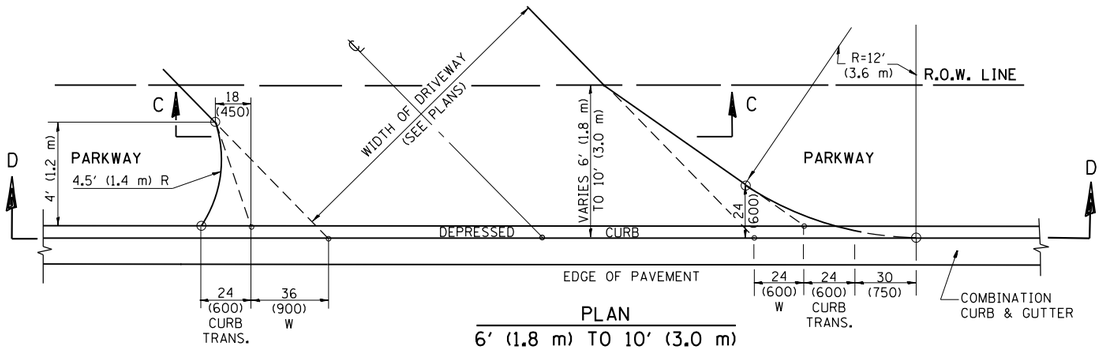
PLAN

10' (3.0 m) TO < 15' (4.5 m)



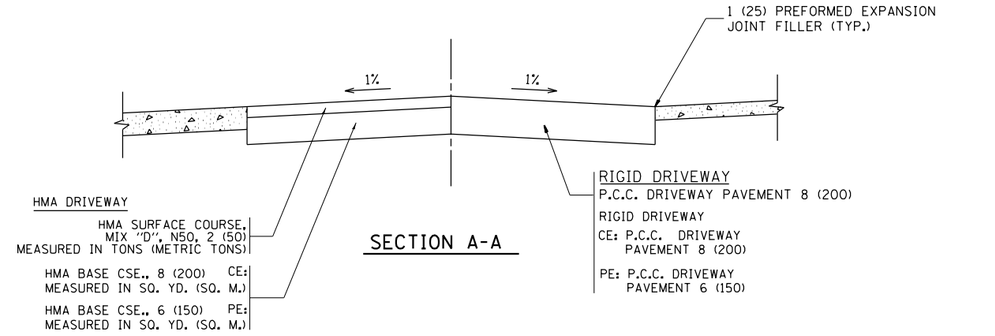
PLAN

6' (1.8 m) TO < 10' (3.0 m)

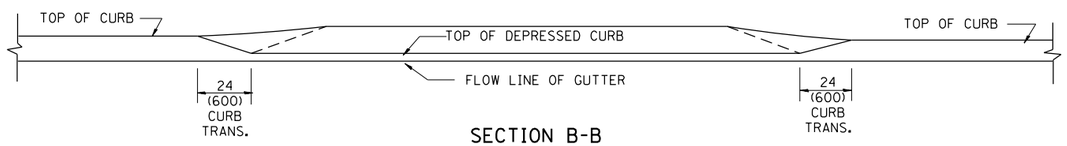


PLAN

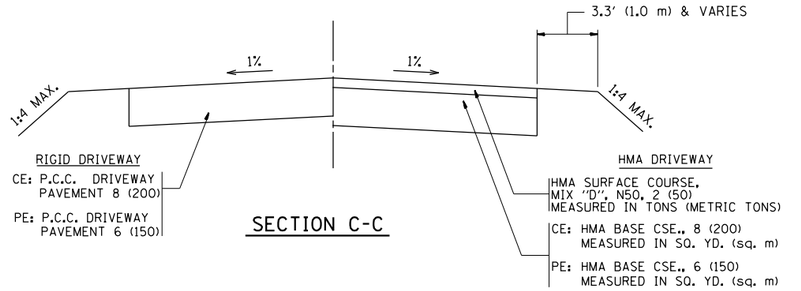
6' (1.8 m) TO 10' (3.0 m)



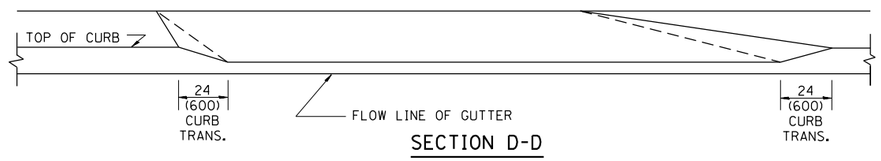
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

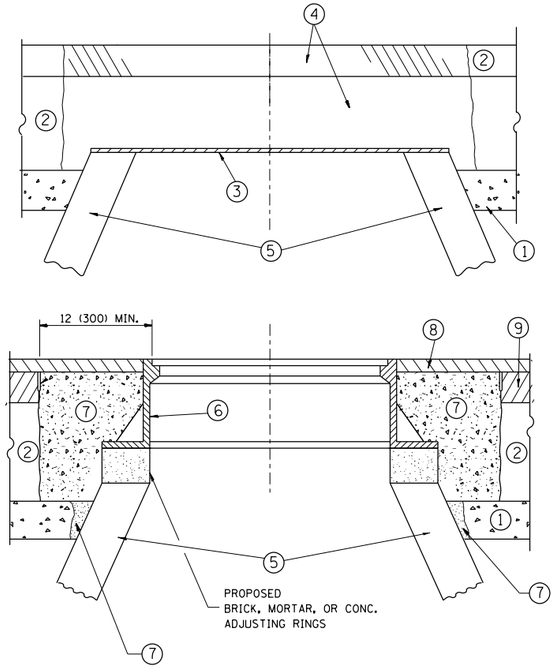
COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P145[illegible]\DrawingData\Design\DistStd.dgn	USER NAME = tariqfm	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY DETAILS DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED - P. LaFLEUR 04-15-03			1261	530N-3	LAKE	80	58
	PLOT DATE = 3/20/2018	DATE - 11-06-95	REVISED - R. BORO 01-01-07			BD400-02 (BD-02)		CONTRACT NO. 62B61		
			REVISED - R. BORO 01-01-07		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

(1) SUB-BASE GRANULAR MATERIAL
(2) EXISTING PAVEMENT
(3) 36 (900) DIAMETER METAL PLATE
(4) PROPOSED CRUSHED STONE AND HMA SURFACE MIX
(5) EXISTING STRUCTURE
(6) FRAME AND LID (SEE NOTES)
(7) CLASS PP-1* CONCRETE
(8) PROPOSED HMA SURFACE COURSE
(9) PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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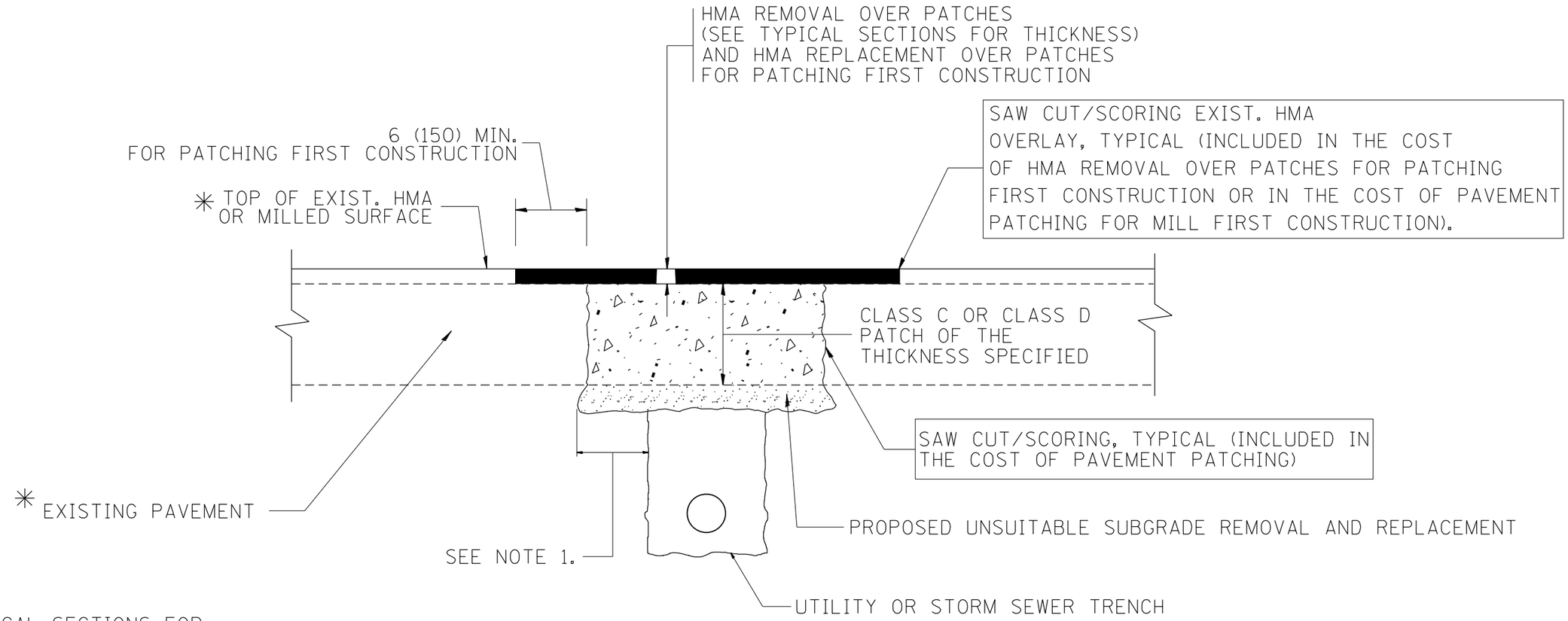
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 3/20/2018	DATE - 10-25-94	REVISED - R. BORO 12-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	59
	BD600-03 (BD-8)	CONTRACT NO. 62B61		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



✳ SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST $4\frac{1}{2}$ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 3/20/2018	DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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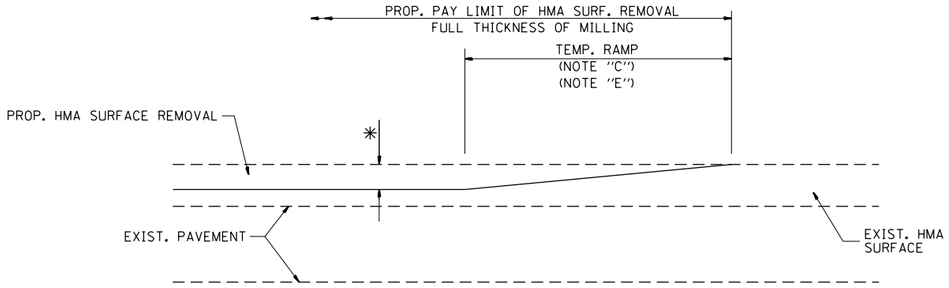
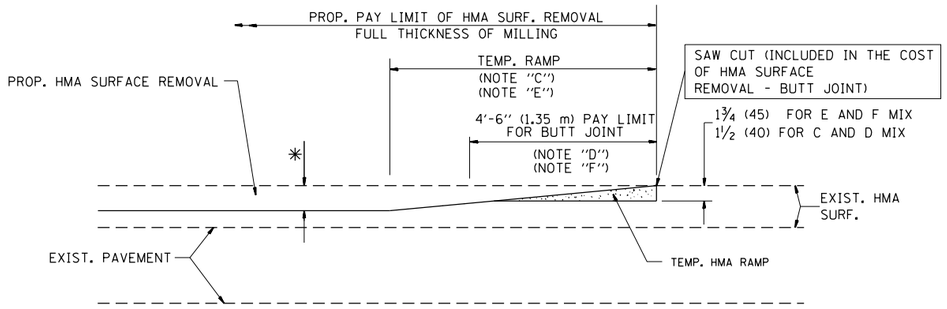
	BD400-04 (BD-22)	CONTRACT NO. 62B61		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

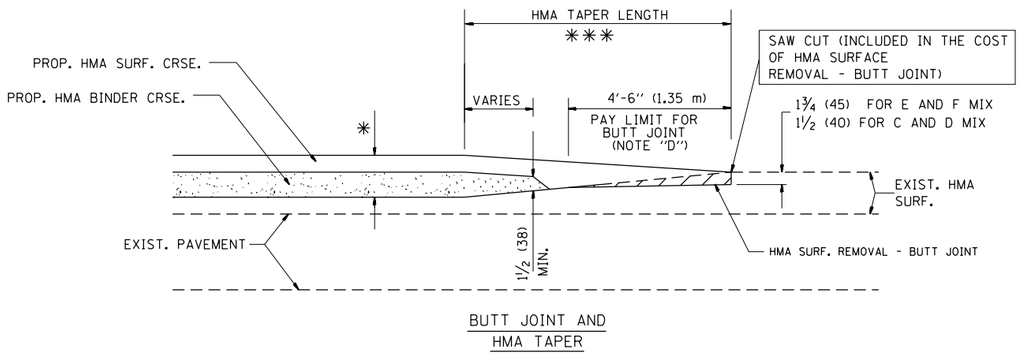


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

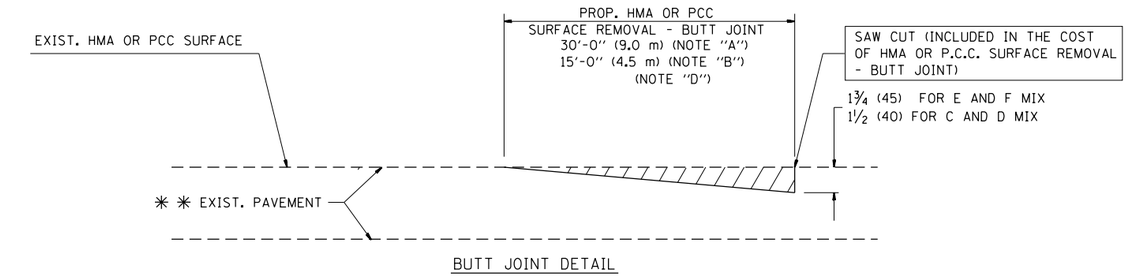
OPTION 2

TYPICAL TEMPORARY RAMP

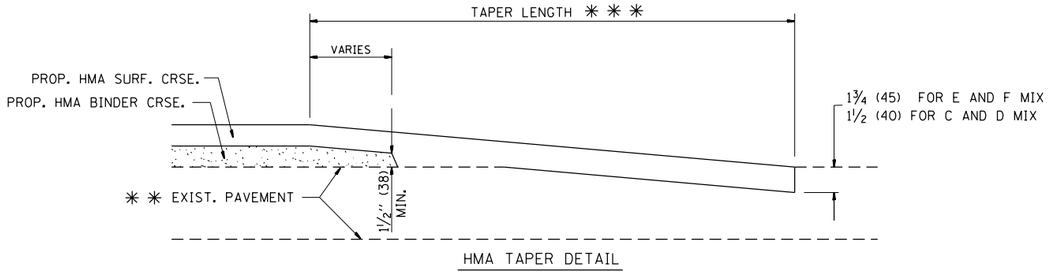


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

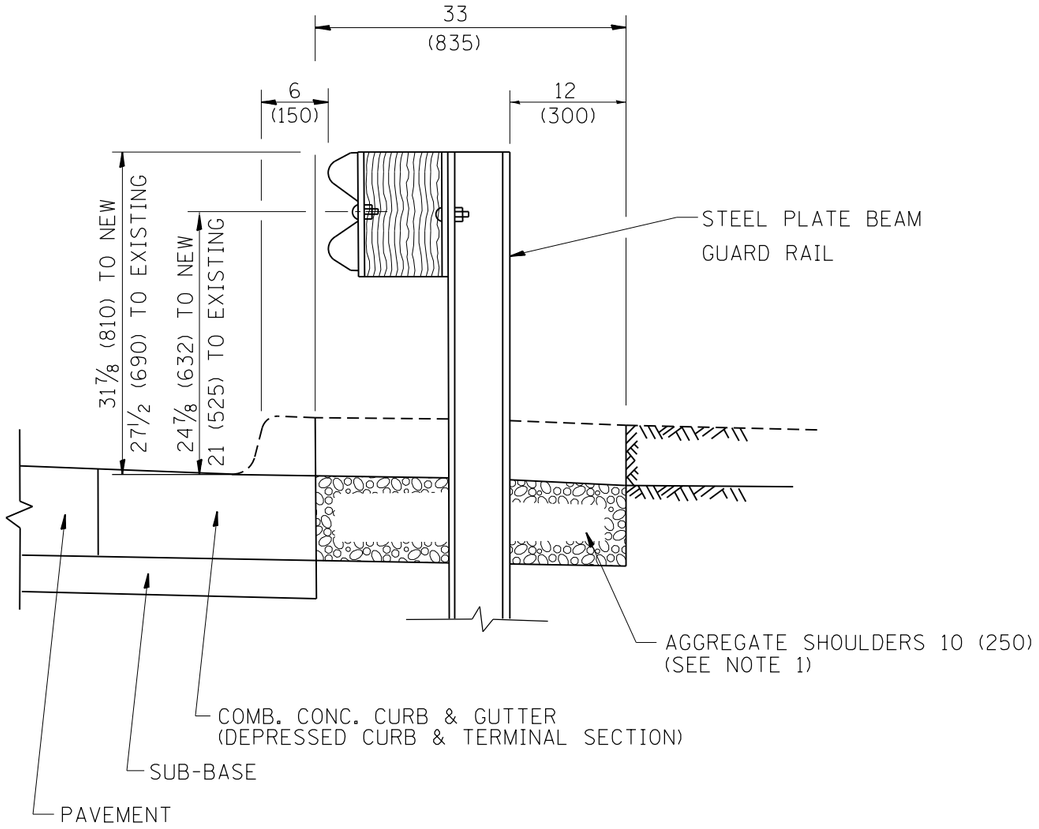
BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P145\DRAWINGS\Design\DistStd.dgn	USER NAME = tariqfm	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS
	PLOT SCALE = 100.0002 '/ in.	CHECKED -	REVISED - A. ABBAS 03-21-97		
	PLOT DATE = 3/20/2018	DATE - 06-13-90	REVISED - M. GOMEZ 04-06-01		
			REVISED - R. BORO 01-01-07		SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	61
	BD400-05 BD32	CONTRACT NO. 62B61		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

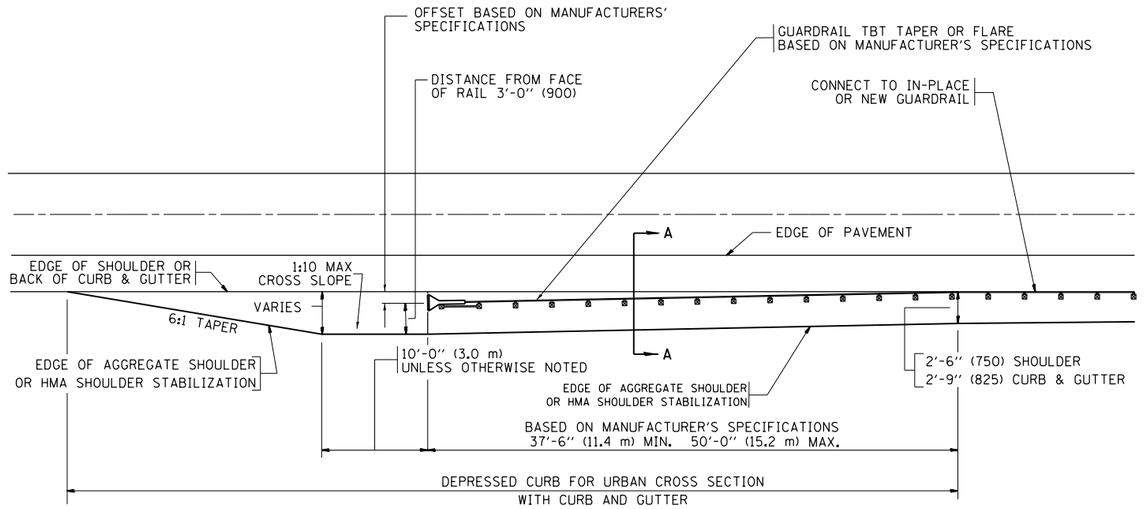


SECTION A-A

NOTES:
1. THE AGGREGATE SHOULDER, 10" OR HMA SHOULDER, 6" (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER

[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]



DEPRESSED CURB AND GUTTER AND SHOULDER TREATMENT AT TBT TY. 1 SPL.

BASIS OF PAYMENT: HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDERS 6" (150 mm)".

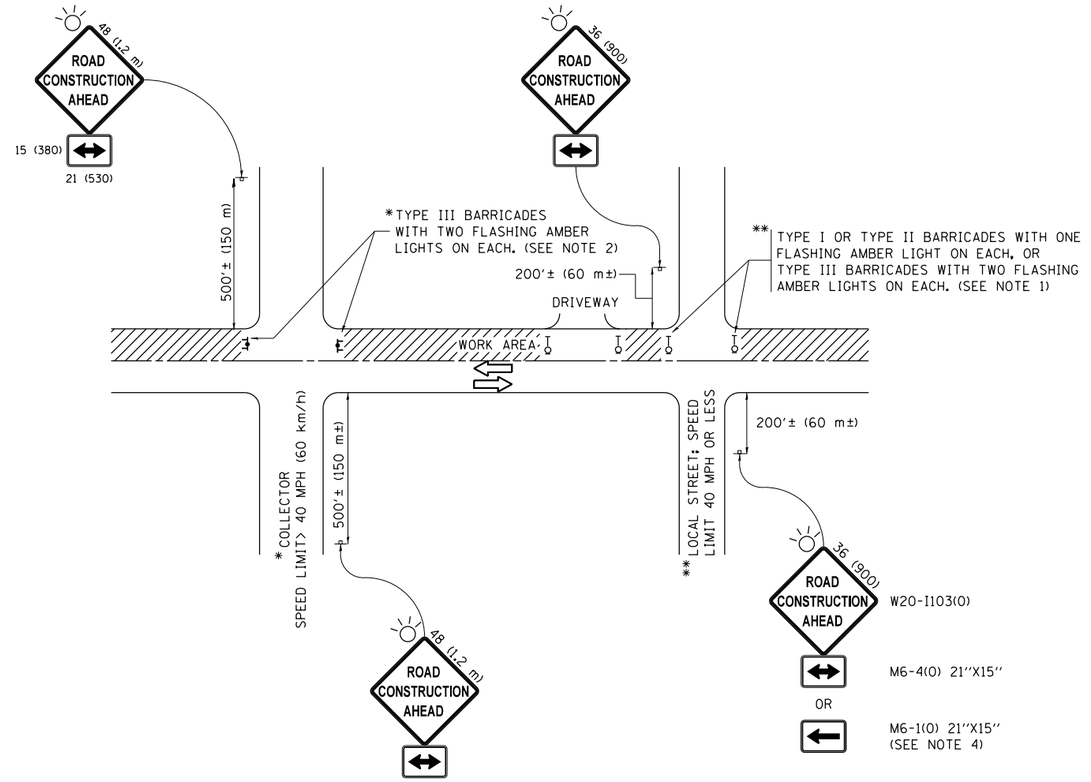
STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P145\DRAWN\Data\Design\DistStd.dgn	USER NAME = tariqfm	DESIGNED - M. DE YONG	REVISED - E. GOMEZ 08-28-00	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR DEPRESSED CURB & GUTTER AND SHOULDER TREATMENT AT TBT TY 1 SPL.
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED - R. BORO 01-01-07		
	PLOT DATE = 3/20/2018	DATE - 09-22-90	REVISED - R. BORO 12-08-2008		
			REVISED - R. BORO 09-14-2009		SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	62
	BD600-10 (BD 34)	CONTRACT NO. 62B61		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE ''ROAD CONSTRUCTION AHEAD'' SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE ''ROAD CONSTRUCTION AHEAD'' SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

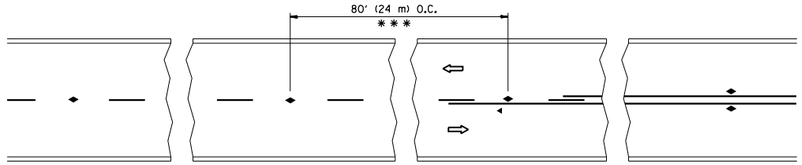
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P145\DRAWN Default	USER NAME = tariqfm	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
		DRAWN Data\Design\DistStd.dgn	REVISED -T. RAMMACHER 01-06-00
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 3/20/2018	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

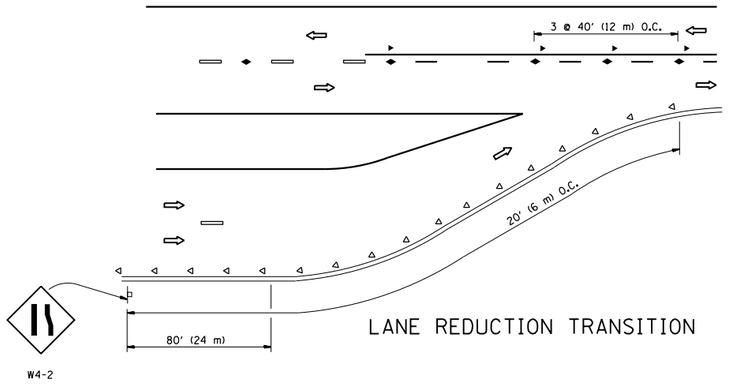
SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	63
	TC-10	CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			

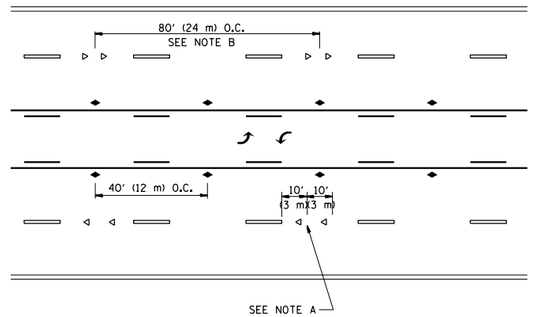


* * * REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

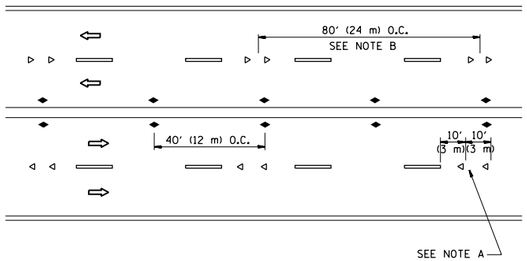
TWO-LANE/TWO-WAY



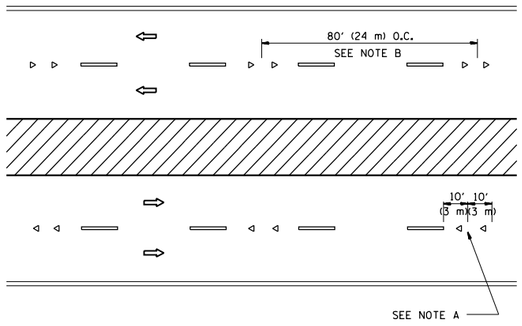
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

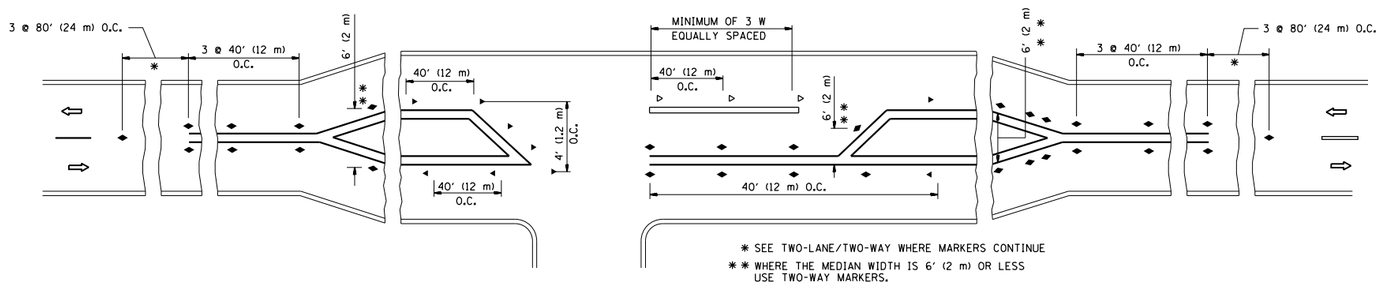
LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



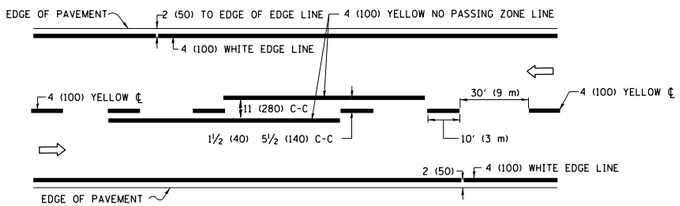
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

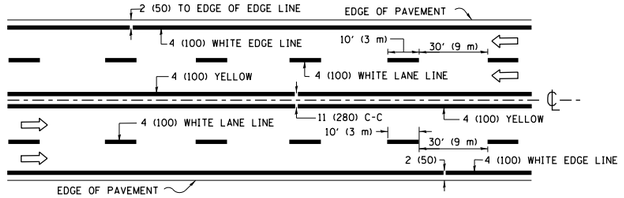
LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

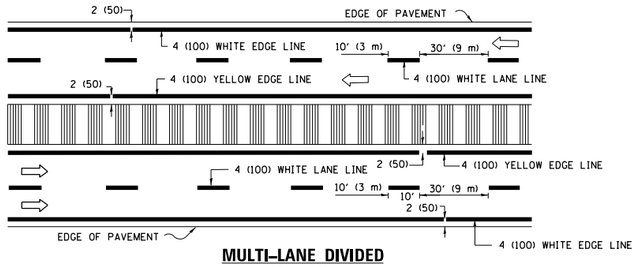
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P145\DRAWINGS\Design\DistStd.dgn	USER NAME = tariqfm	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - T. RAMMACHER 03-12-99					1261	530N-3	LAKE	80	64
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00						TC-11		CONTRACT NO. 62B61	
	PLOT DATE = 3/20/2018	DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.		FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	



2–LANE ROADWAY

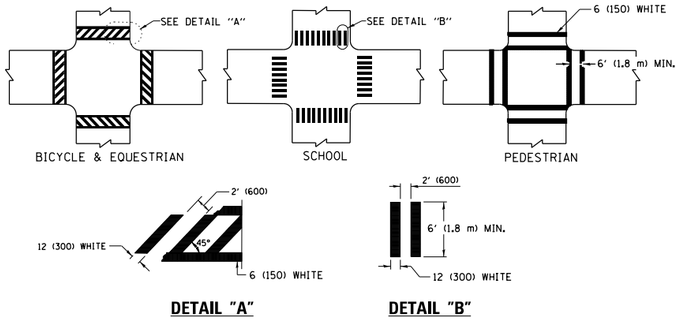


MULTI–LANE UNDIVIDED



MULTI–LANE DIVIDED
WITH MEDIAN

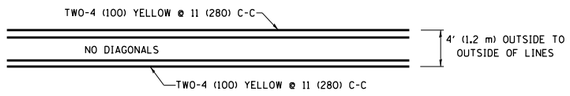
TYPICAL LANE AND EDGE LINE MARKING



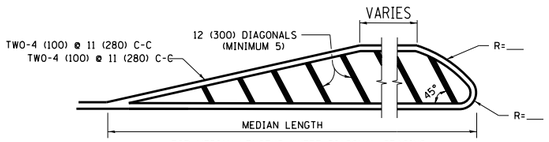
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

✳ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



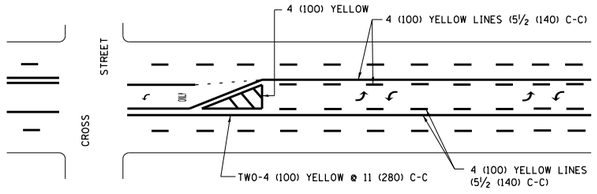
4' (1.2 m) WIDE MEDIANS ONLY



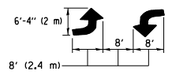
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

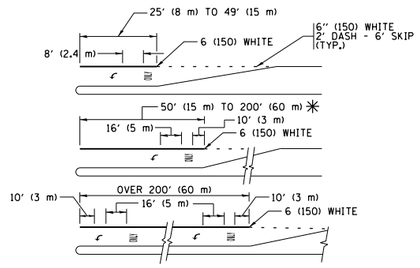


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

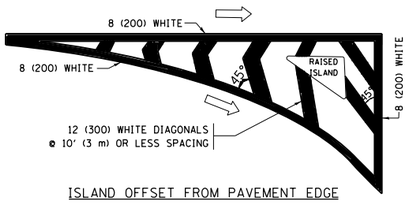


FULL SIZE LETTERS 8' (2,4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1.9 m^2)

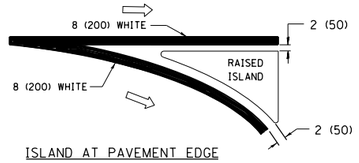
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

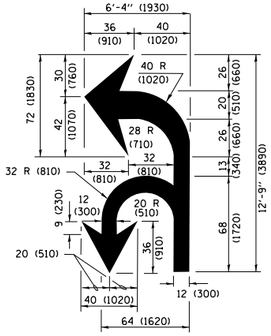


ISLAND OFFSET FROM PAVEMENT EDGE

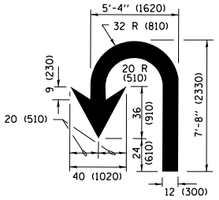


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

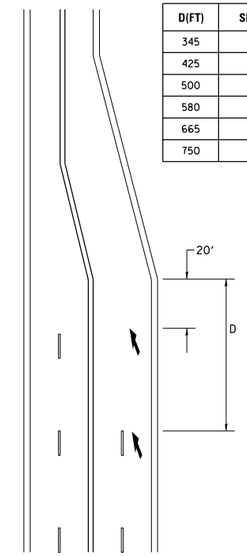


COMBINATION LEFT AND U–TURN



U–TURN

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55



LANE REDUCTION TRANSITION

✳ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 ⌀ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 ⌀ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 ⌀ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 ⌀ 6 (150) 12 (300) ⌀ 45° 12 (300) ⌀ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 ⌀ 4 (100) WITH 12 (300) DIAGONALS ⌀ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS ⌀ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) ⌀ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1450DRAWN\Data\Design\DistStd.dgn Default	USER NAME = tariqfm	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
			REVISED - C. JUCIUS 07-01-13
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 3/20/2018	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	65
	TC-13	CONTRACT NO. 62B61		
		ILLINOIS FED. AID PROJECT		

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

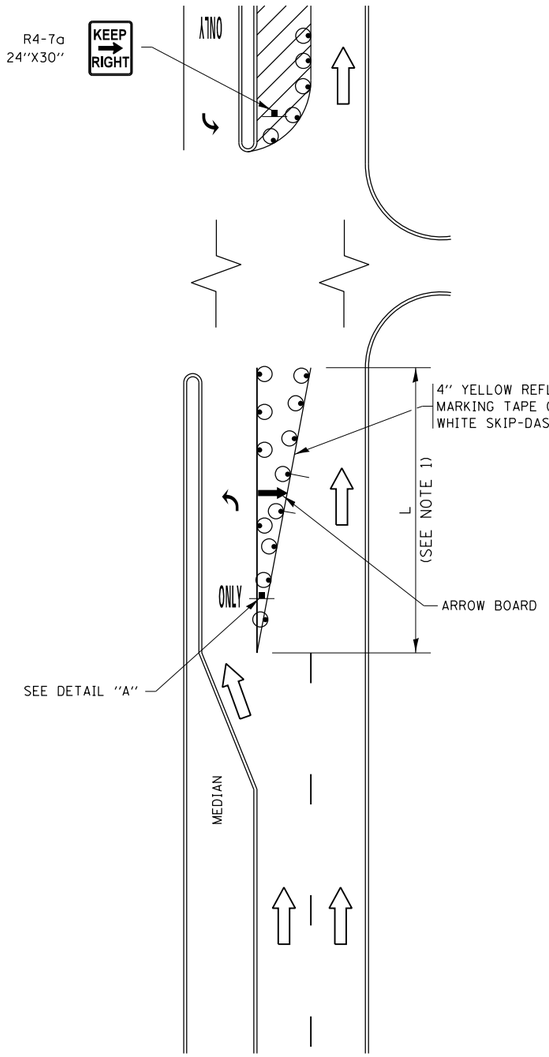


FIGURE 1

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.

   B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.

2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.

3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.

4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.

5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-110OR 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.

6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.

7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.

8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

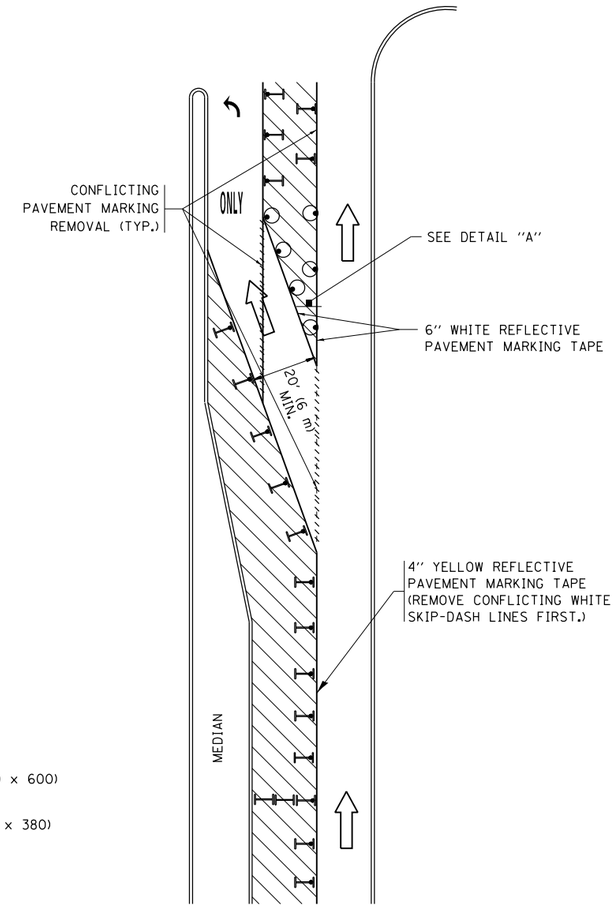
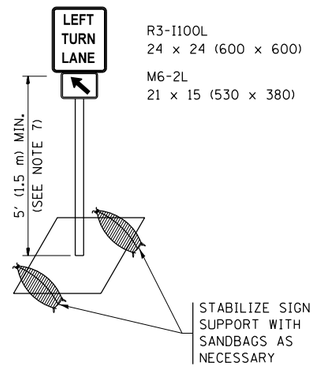


FIGURE 2

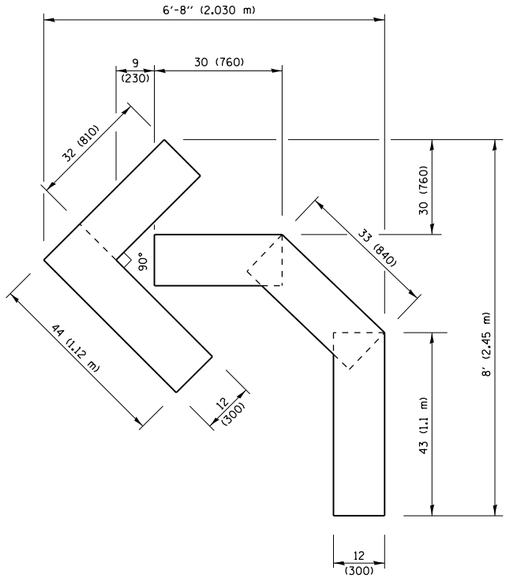


DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

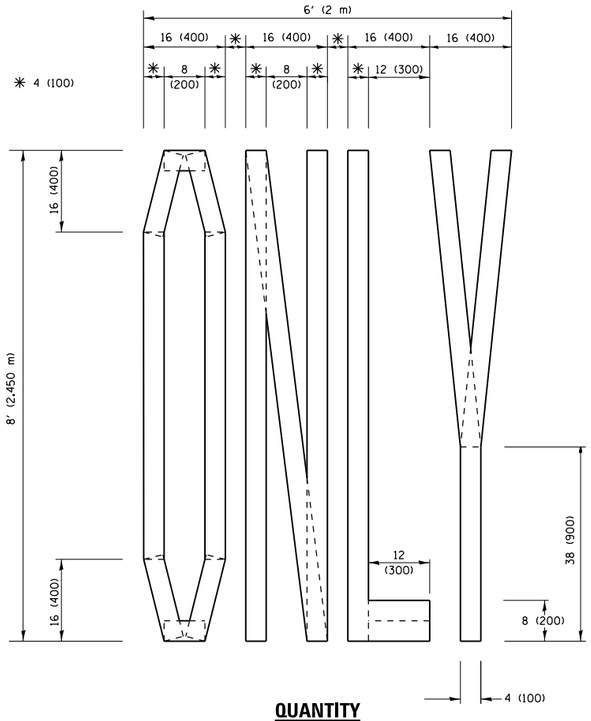
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P145\Revised\Design\Dushoth\Default	USER NAME = tariqfm	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)				
	PLOT SCALE = 100.0002 ' / in.	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13						
	PLOT DATE = 3/20/2018	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16						
		REVISED - T. RAMMACHER 01-06-00	REVISED -		SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.	

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	66
	TC-14	CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			



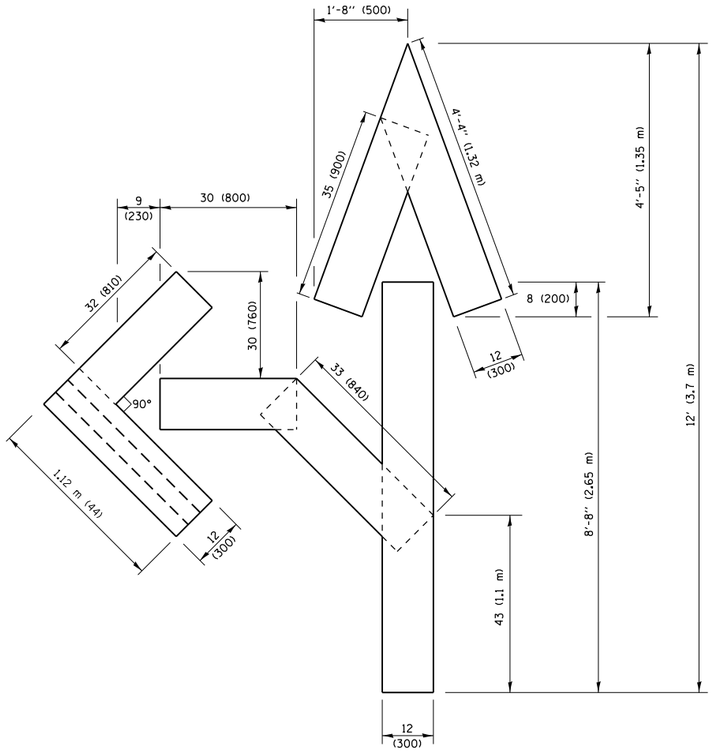
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

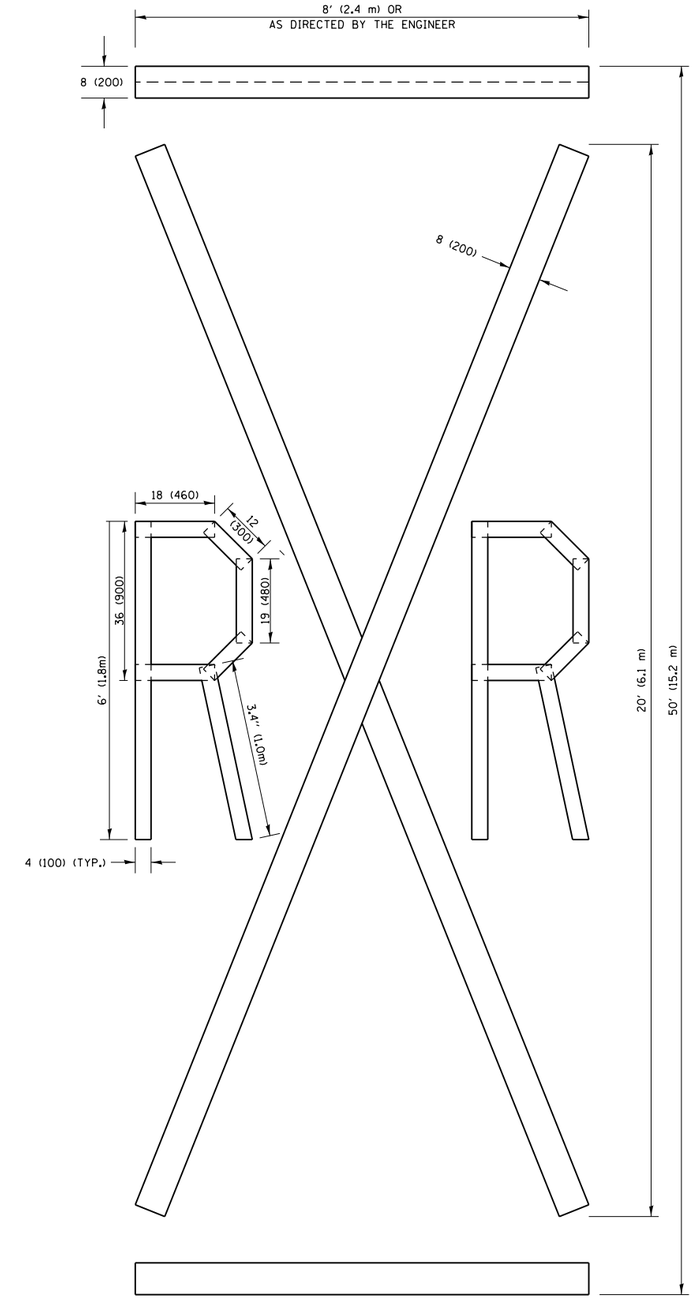


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

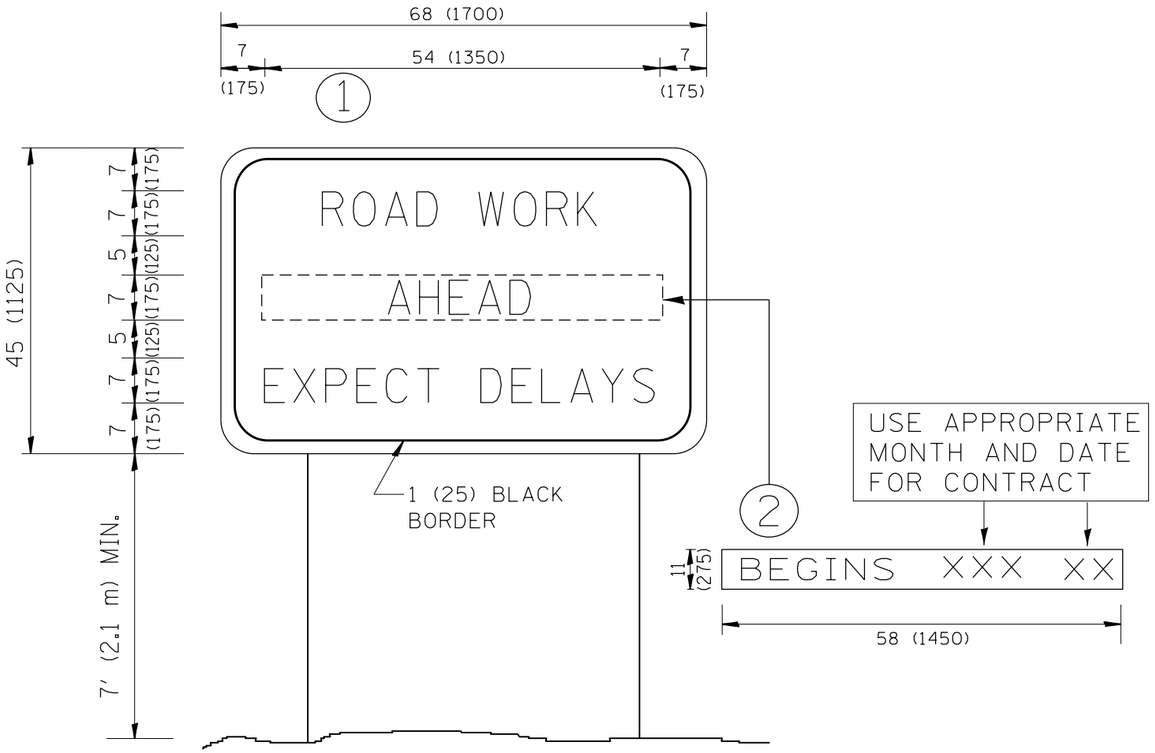
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P145\DRAWN\Data\Design\DistStd.dgn	USER NAME = tariqfm	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
		DRAWN -	REVISED -E. GOMEZ 08-28-00
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED -E. GOMEZ 08-28-00
	PLOT DATE = 3/20/2018	DATE - 09-18-94	REVISED -A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	67
	TC-16	CONTRACT NO. 62B61		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE ''ROAD CONSTRUCTION AHEAD'' SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR ''TEMPORARY INFORMATION SIGNING'' FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

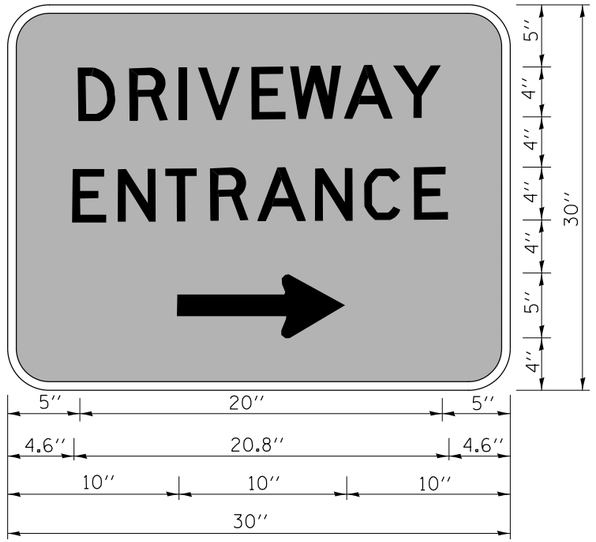
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P145[illegible]\Data\Design\DistStd.dgn	USER NAME = tariqfm	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0002 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 3/20/2018	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	68
	TC-22	CONTRACT NO. 62B61		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



3.0'' RADIUS, 0.5'' BORDER, WHITE ON GREEN; REFLECTORIZED
''DRIVEWAY'' D; ''ENTRANCE'' D; STANDARD ARROW CUSTOM 12.0'' x 5.0''

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM ''TEMPORARY INFORMATION SIGNING''.

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		DRAWN -	REVISED -
	PLOT SCALE = 100.0000 '/ in.	CHECKED -	REVISED -
	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	69
	TC-26	CONTRACT NO. 62B61		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

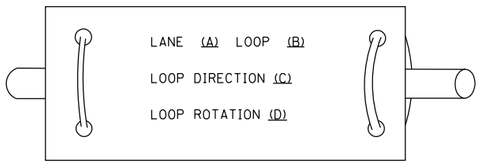
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

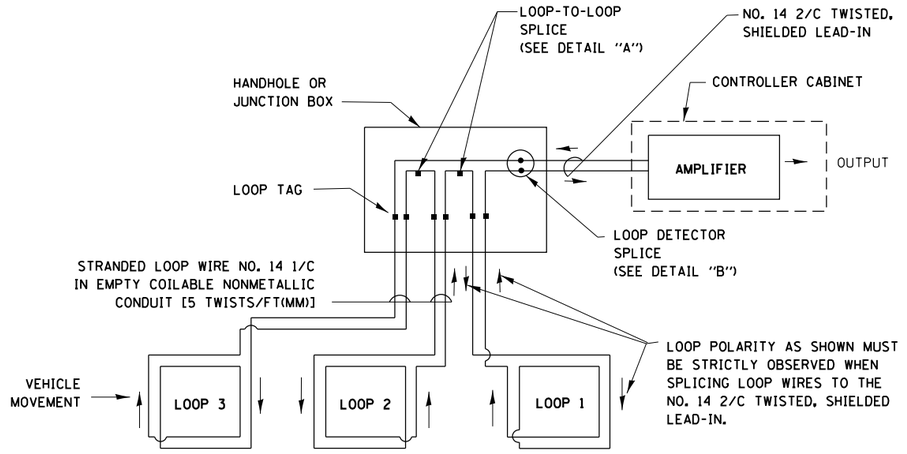


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY

B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.

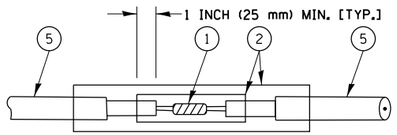
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

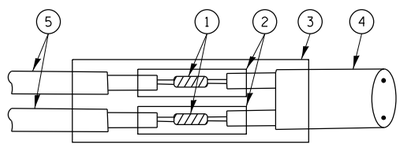


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

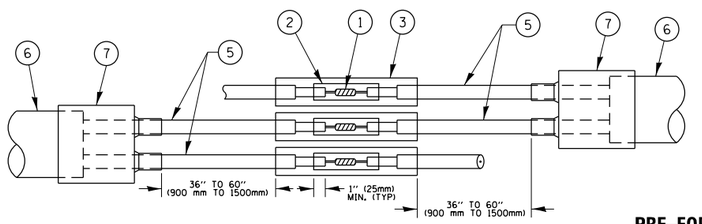


DETAIL "A"
LOOP-TO-LOOP SPLICE



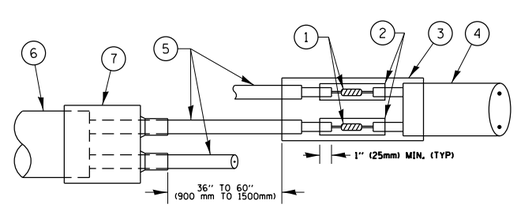
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE

PRE-FORMED LOOP



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

(1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.

(2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.

(3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

(5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

(6) PRE-FORMED LOOP

(7) XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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Default	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

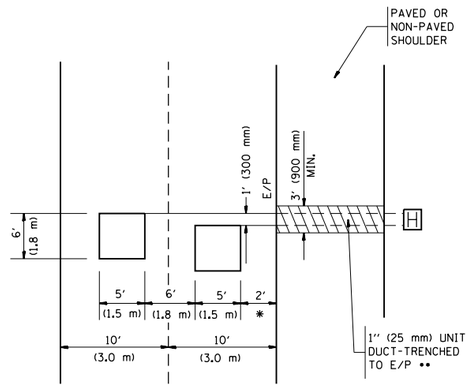
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 2 OF 7 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	70
	TS-05	CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

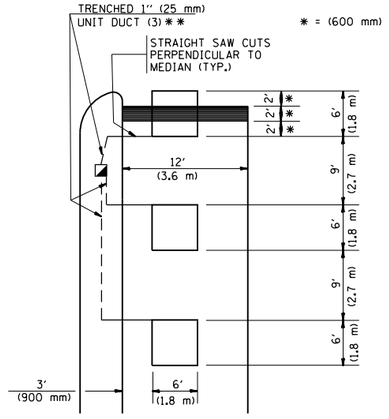


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

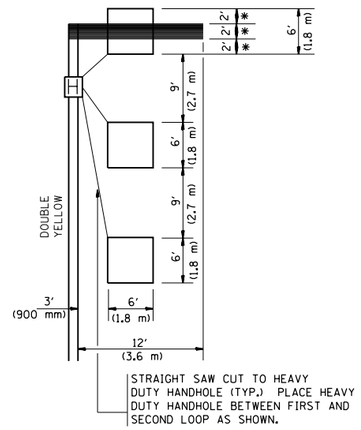


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

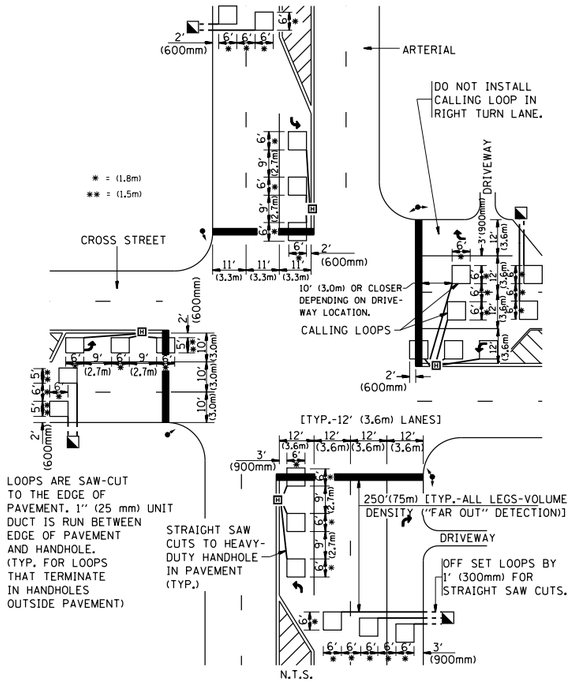
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

* = (600 mm)



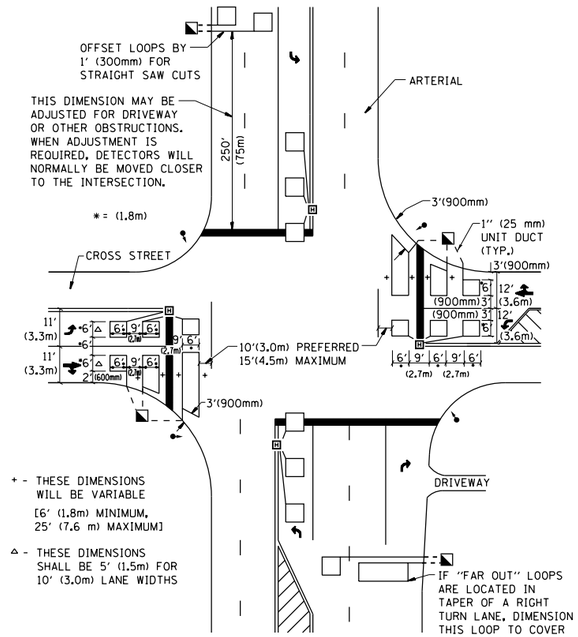
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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		DRAWN -	REVISED -
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	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

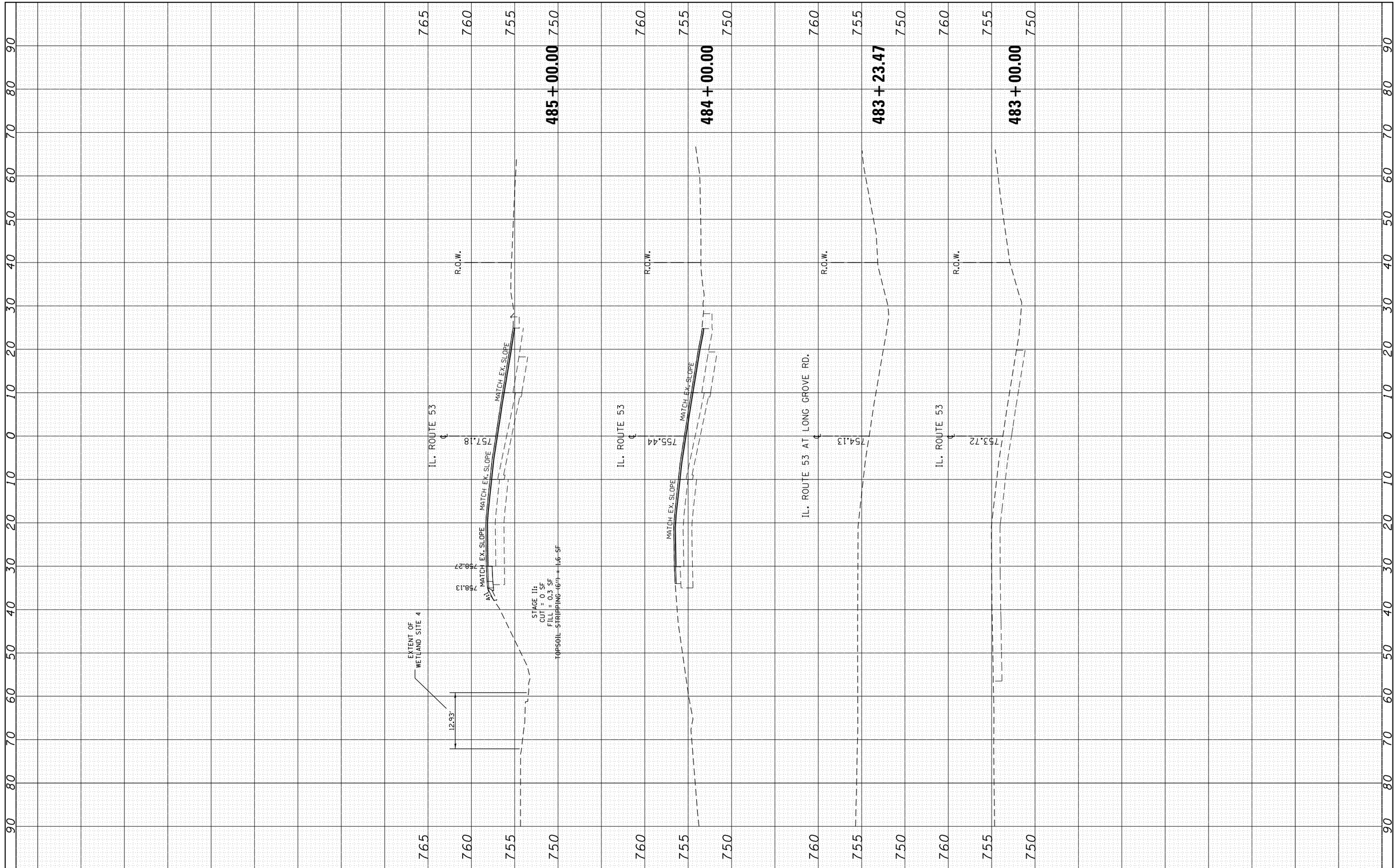
DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	71
	TS-07	CONTRACT NO. 62B61		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
NO.	PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
NO.	PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		



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	PLOT SCALE = 20.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED CROSS SECTIONS
IL 53

SCALE: | SHEET OF SHEETS | STA. 483+00.00 TO STA. 485+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	53ON-3	LAKE	80	72
		CONTRACT NO. 62B61		
		ILLINOIS FED. AID PROJECT		

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
NO.	PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		

FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
NO.	PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		

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USER NAME = tarrdfm

PLOT SCALE = 20.0000 / in.

PLOT DATE = 3/20/2018

Default

DESIGNED - BRAUN

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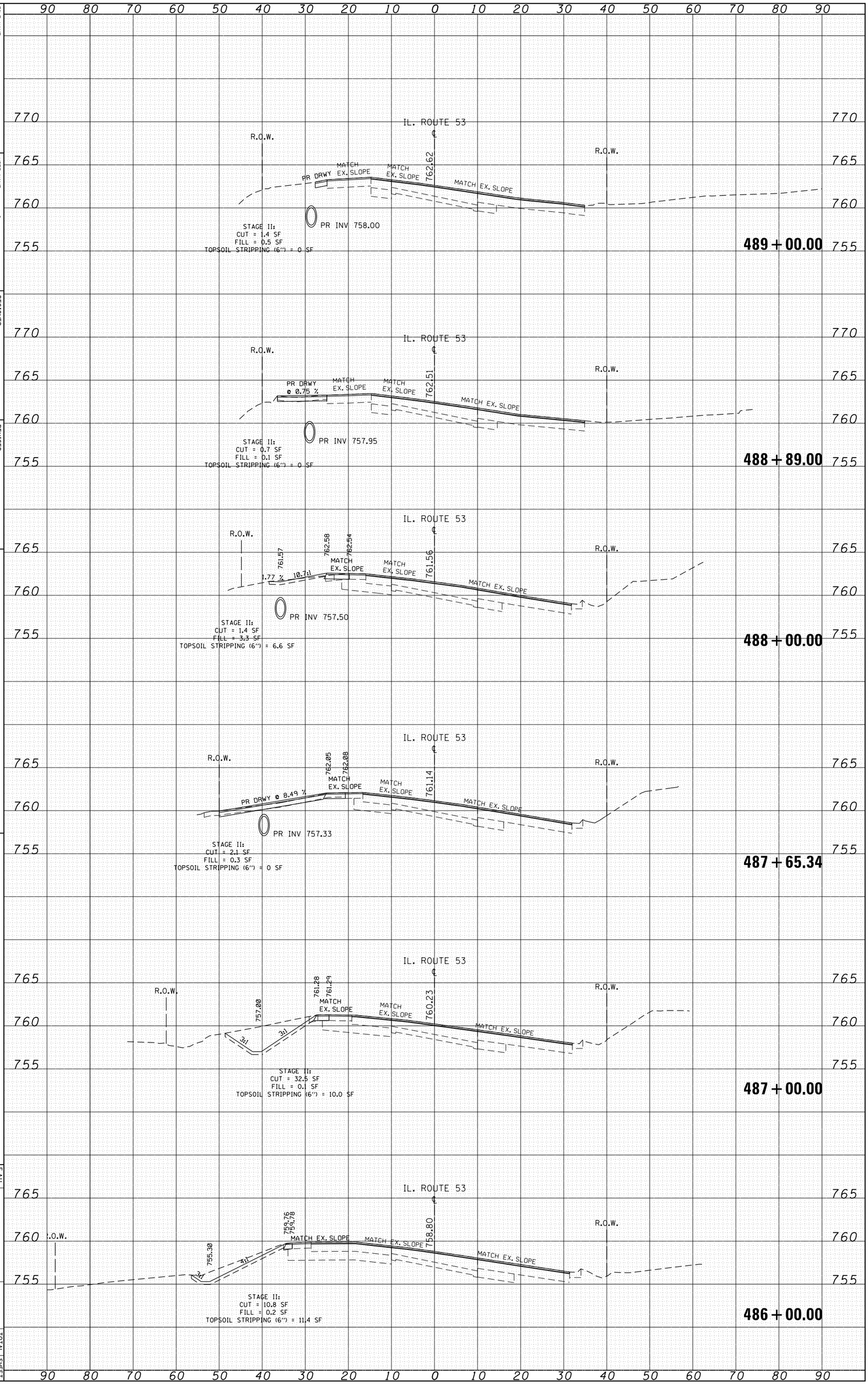
REVISED - REVISED - REVISED -

REVISED - REVISED - REVISED -



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED CROSS SECTIONS
IL 53

SCALE: 1

SHEET OF SHEETS STA. 486+00.00 TO STA. 489+00.00

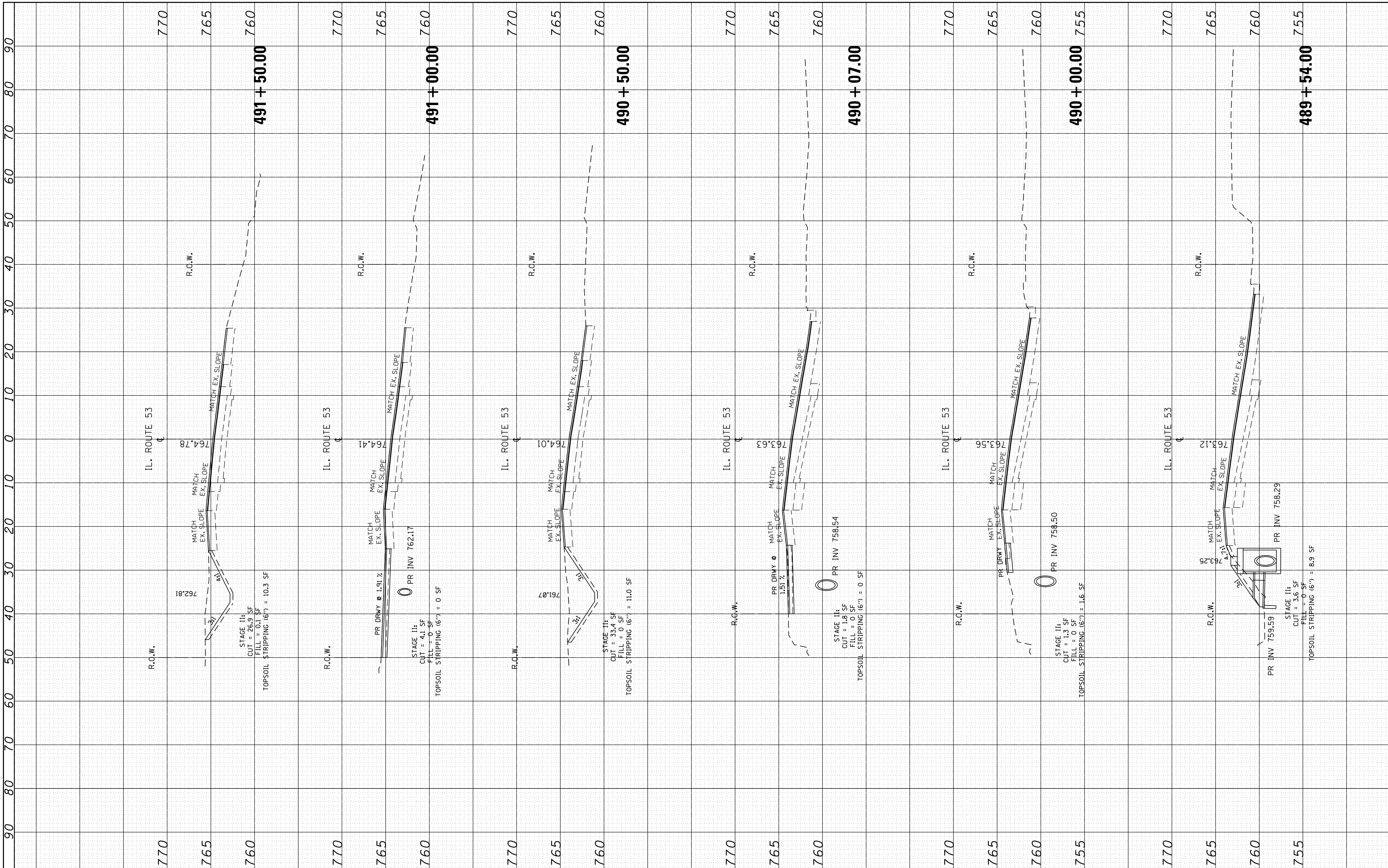
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	73

ILLINOIS FED. AID PROJECT

CONTRACT NO. 62B61

ORIGINAL SURVEY	NOTE BOOK NO.	SURVEYED	PLOTTED	TEMPLATE	AREAS	AREAS CHECKED	BY	DATE

FINAL SURVEY	NOTE BOOK NO.	SURVEYED	PLOTTED	TEMPLATE	AREAS	AREAS CHECKED	BY	DATE



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	PLOT SCALE = 20.0000 ' / in.	DRAWN -	REVISED -
		CHECKED -	REVISED -
Default	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED CROSS SECTIONS
IL 53

SCALE: | SHEET OF SHEETS | STA. 489+54.00 TO STA. 491+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	74

CONTRACT NO. 62B61

ILLINOIS FED. AID PROJECT

FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
NO.	AREAS		
	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
NO.	AREAS		
	AREAS CHECKED		

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	PLOT SCALE = 20.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED CROSS SECTIONS
IL 53

SCALE: | SHEET OF SHEETS | STA. 492+00.00 TO STA. 495+36.63

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	53ON-3	LAKE	80	75
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		

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	PLOT SCALE = 20.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

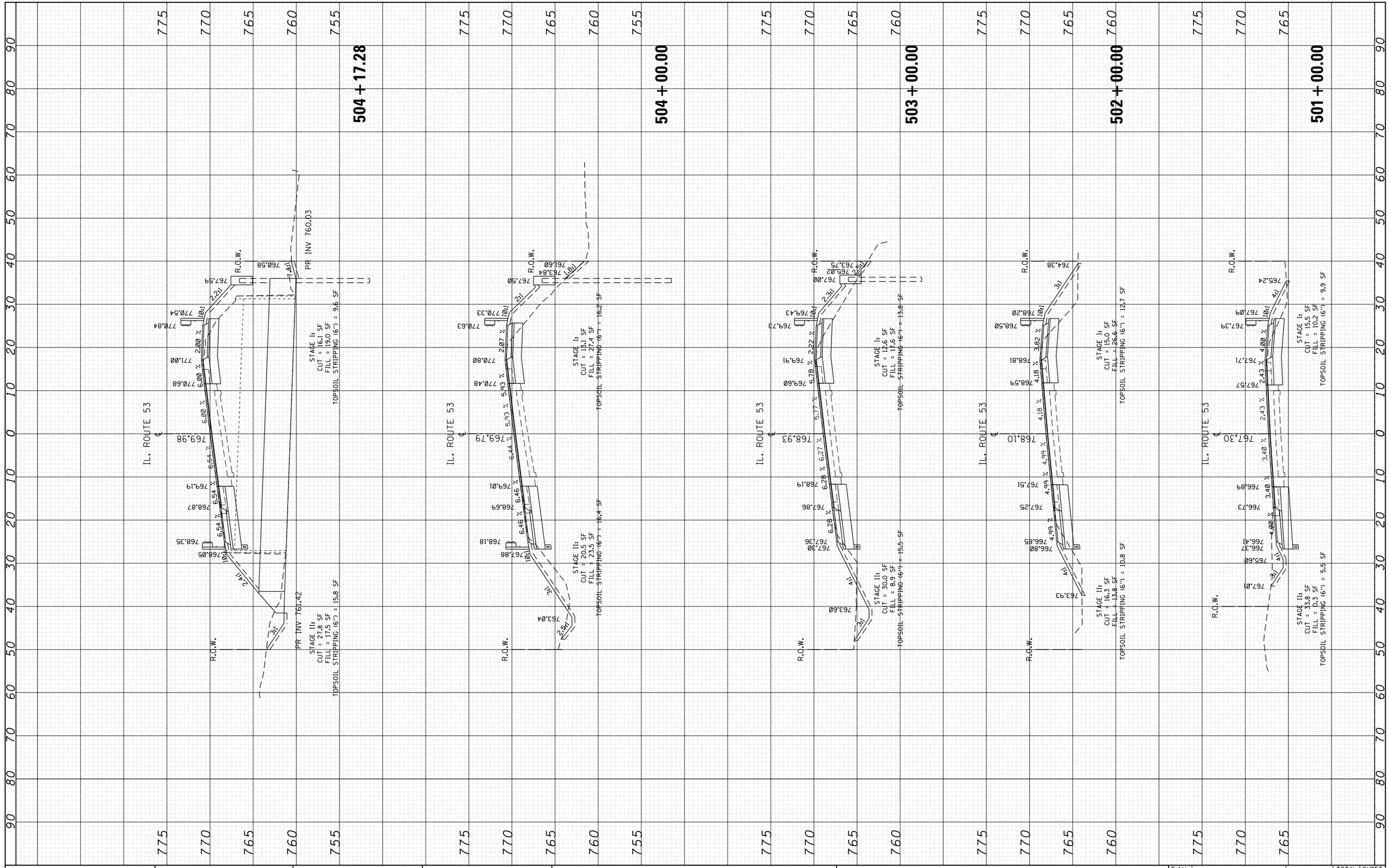
EXISTING AND PROPOSED CROSS SECTIONS
IL 53

SCALE: | SHEET OF SHEETS | STA. 496+00.00 TO STA.500+69.32

F.A.I.. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	53ON-3	LAKE	80	76
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		



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Default	PLOT DATE = 3/20/2018	DRAWN -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

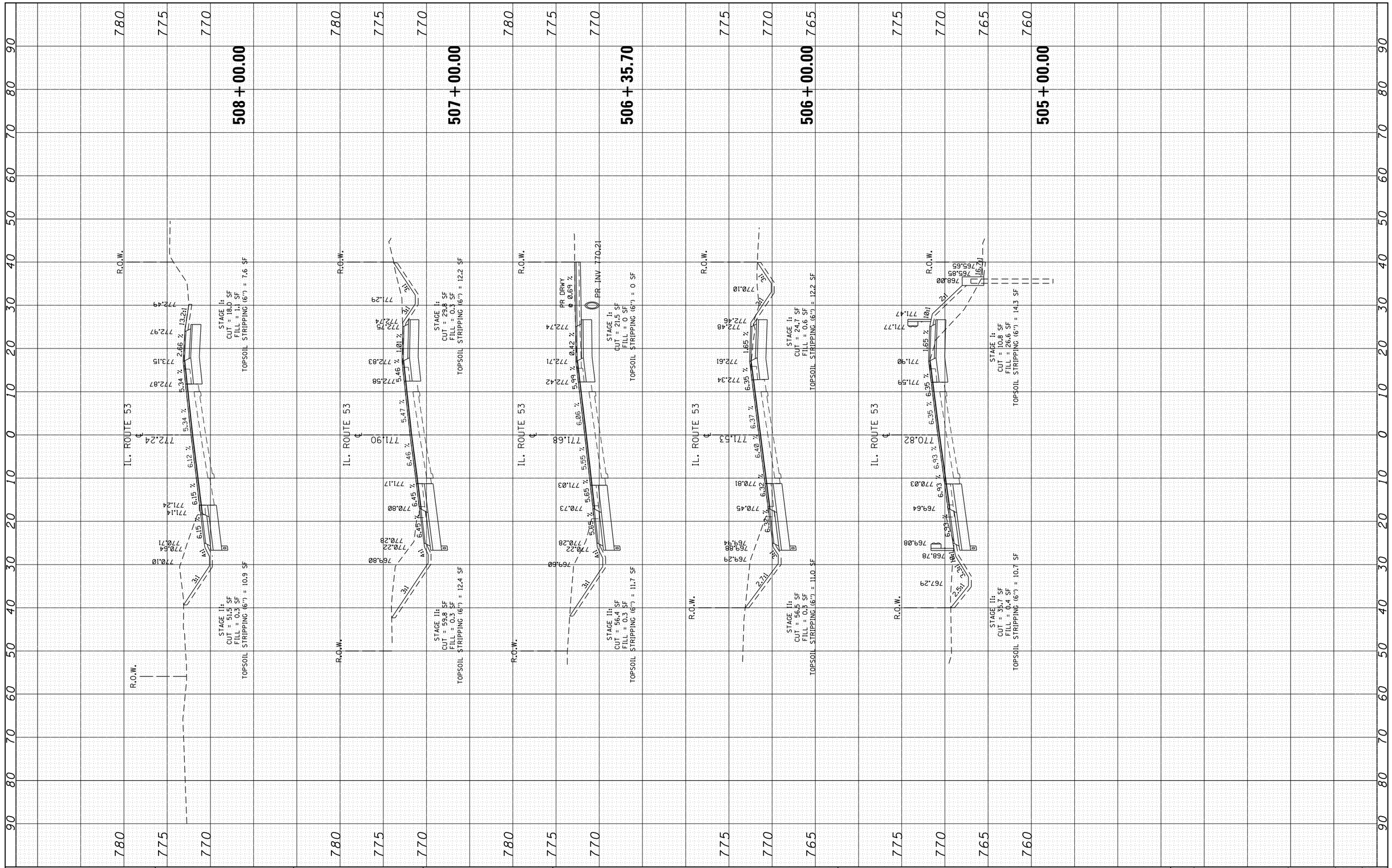
EXISTING AND PROPOSED CROSS SECTIONS
IL 53

SCALE: | SHEET OF SHEETS | STA. 501+00.00 TO STA. 504+17.28

F.A.I.. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	53ON-3	LAKE	80	77
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	AREAS CHECKED		



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	PLOT SCALE = 20.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

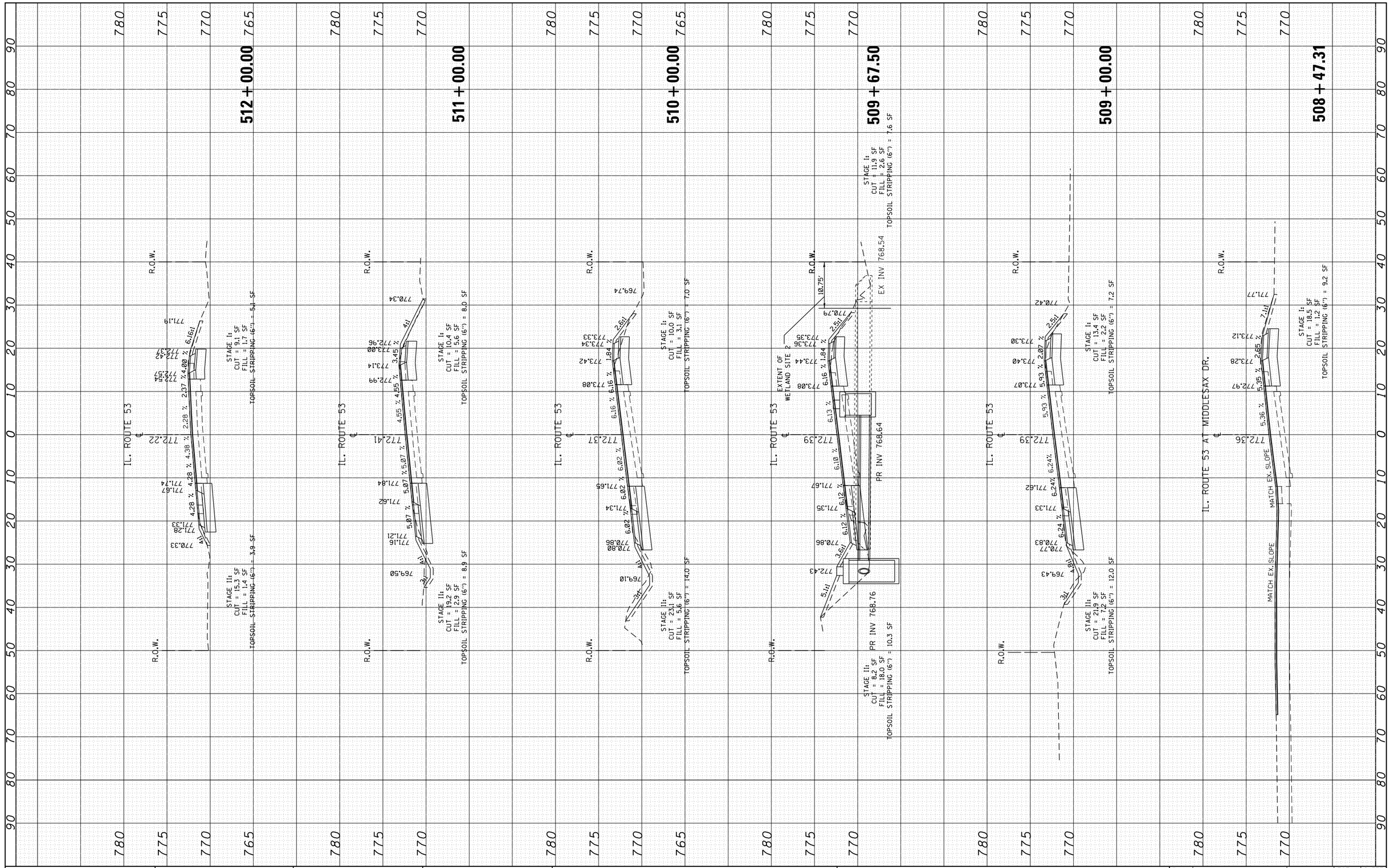
EXISTING AND PROPOSED CROSS SECTIONS
IL 53

SCALE: | SHEET OF SHEETS | STA. 505+00.00 TO STA. 508+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	53ON-3	LAKE	80	78
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		

FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
NO.	PLOTTED		
	TEMPLATE		
	ANGLES		
	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
NO.	PLOTTED		
	TEMPLATE		
	ANGLES		
	AREAS CHECKED		



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		CHECKED -	REVISED -
	PLOT SCALE = 20.0000 ' / in.	DRAWN -	REVISED -
Default	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

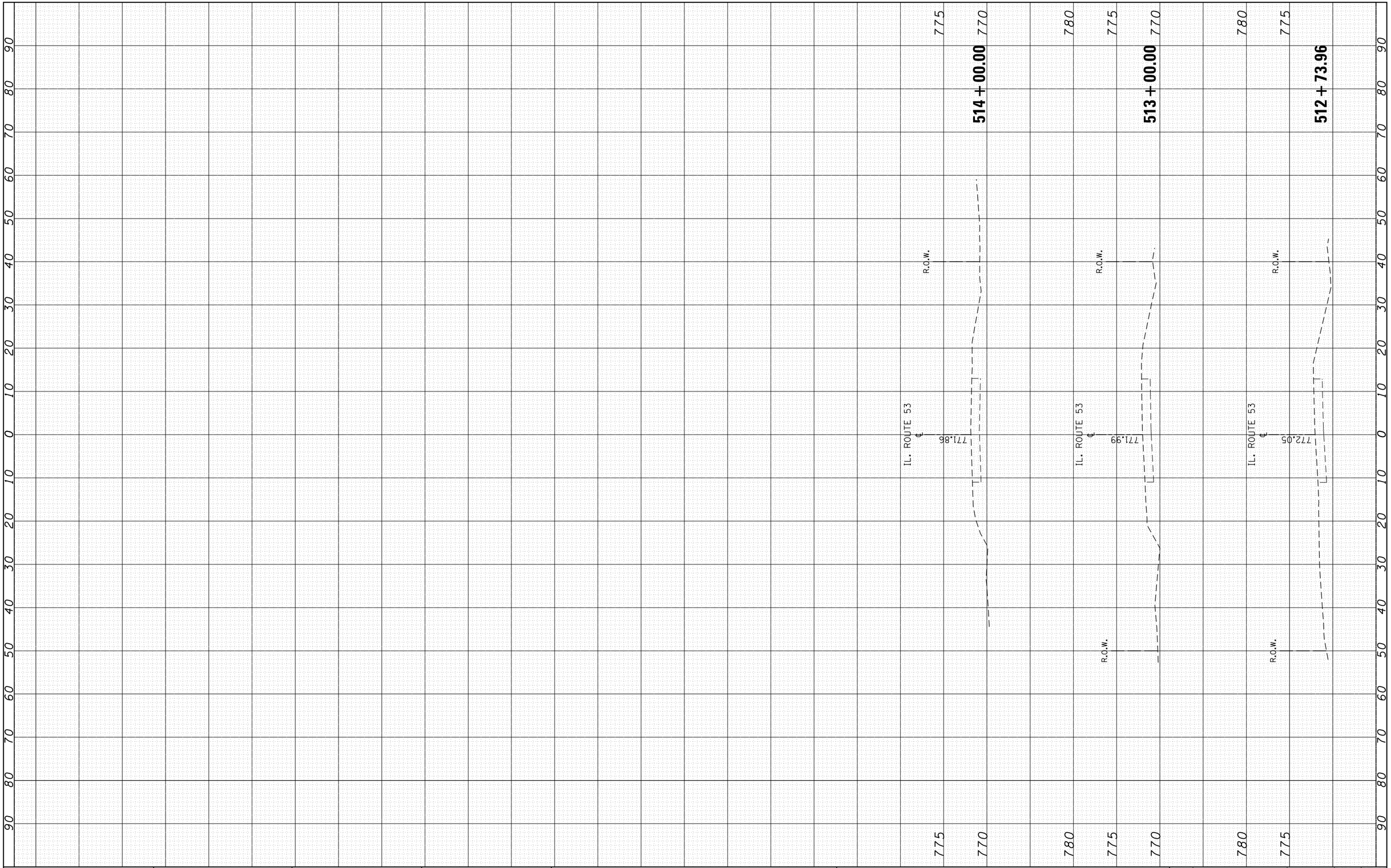
EXISTING AND PROPOSED CROSS SECTIONS
IL 53

SCALE: | SHEET OF SHEETS | STA. 508+47.31 TO STA. 512+00.00

F.A.I.. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	53ON-3	LAKE	80	79
		CONTRACT NO. 62B61		
	ILLINOIS FED. AID PROJECT			

FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
NO.	PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
NO.	PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		



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Default	PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED CROSS SECTIONS
IL 53

SCALE: | SHEET OF SHEETS | STA. 512+73.96 | TO STA. 514+00.00

F.A.I.. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1261	530N-3	LAKE	80	80
		CONTRACT NO. 62B61		
	ILLINOIS	FED. AID PROJECT		