

62846

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1576	2121 RS-2	COOK	18	1

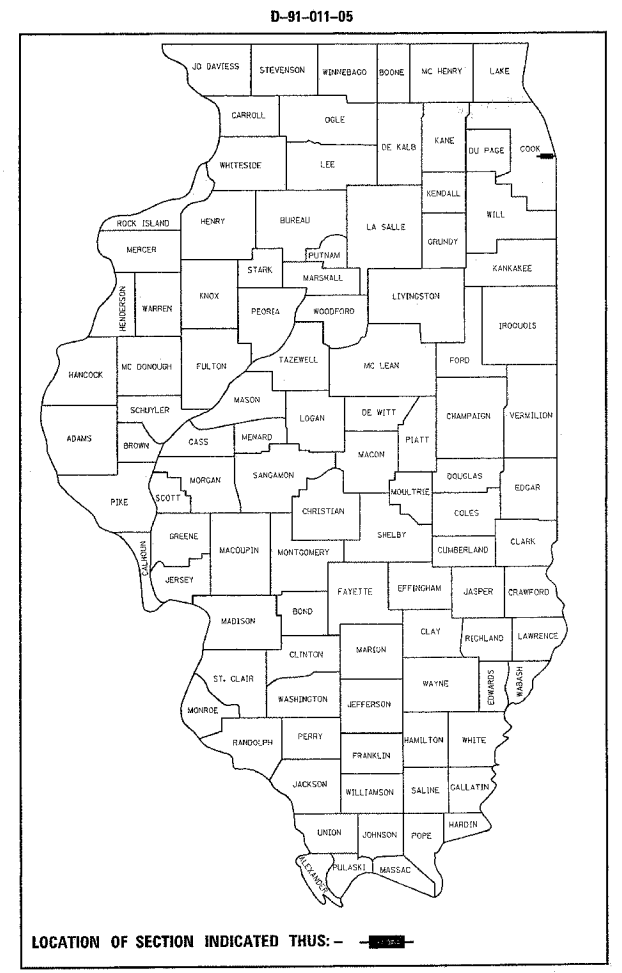
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED
HIGHWAY**

**F.A.U. ROUTE: 1576 (106 TH STREET)
SECTION: 2121 RS-2
TORRENCE AVENUE TO INDIANAPOLIS BOULEVARD
PAVEMENT RESURFACING (MAINTENANCE)
COOK COUNTY
C-91-011-05**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

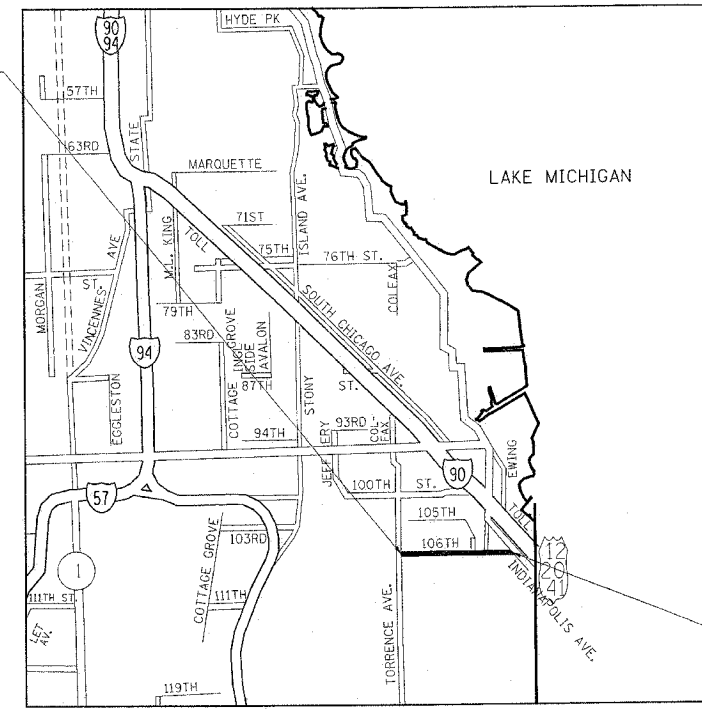
THE IMPROVEMENT IS LOCATED IN THE CITY OF CHICAGO



IMPROVEMENT BEGINS
STA. 0+40

OMISSION
~~STA. 2+46.2 TO
STA. 3+46.2~~

OMISSION
STA. 32+69.6 TO
STA. 38+36.6



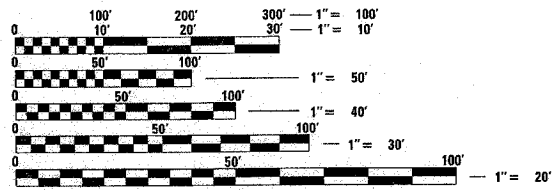
TRAFFIC DATA

1986 ADT = 12,900
POSTED SPEED LIMIT = 35 MPH

IMPROVEMENT ENDS
STA. 95+60

THORNTON TOWNSHIP

GROSS LENGTH OF IMPROVEMENT = 9,520 LINEAL FEET = 1.803 MILES
NET LENGTH OF IMPROVEMENT = 8,953 LINEAL FEET = 1.696 MILES



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

FOR UTILITY LOCATION INFORMATION FOR EXCAVATION
CALL CHICAGO UTILITY ALERT NETWORK
(C.U.A.N.) 312-744-7000

CONTRACT NO. 62846

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED March 31, 2006

Diane M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 12, 2006
Mike Hine
ENGINEER OF DESIGN AND ENVIRONMENT

May 12, 2006
Milton R. See
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

Rev.

DISTRICT ONE DESIGN PLAN PREPARATION ENGINEER:
KEN ENG/ROBERT BORO (847)705-4178

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1576	2121 RS-2	COOK	18	3

FED. ROAD DIST. NO. 1 ILLINOIS HIGHWAY PROJECT
CONTRACT 62846

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	URBAN	PARKING		
				100% STATE	100% CITY		
				1000-2A	1025		
40300200	BITUMINOUS MATERIALS (PRIME COAT)	TON	19.5	14.5	5		
40800030	AGGREGATE (PRIME COAT)	TON	98	69.9	28.1		
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	15	13.5	1.5		
40600895	CONSTRUCTING TEST STRIP	EACH	1	1			
40600980	BITUMINOUS SURFACE REMOVAL - BUTT JOINT	SQ YD	1000	1000			
40601000	BITUMINOUS REPLACEMENT OVER PATCHES	TON	20	20			
44000119	BITUMINOUS REMOVAL OVER PATCHES 4 3/4"	SQ YD	600	600			
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	900	900			
44201341	CLASS C PATCHES, TYPE II, 9 INCH	SQ YD	730	730			
44201345	CLASS C PATCHES, TYPE III, 9 INCH	SQ YD	730	730			
44201347	CLASS C PATCHES, TYPE IV, 9 INCH	SQ YD	973	973			
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	102	102			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	4	4			
67100100	MOBILIZATION	L SUM	1	1			
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1			
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	6000	6000			
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	550	550			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	22000	22000			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1000	1000			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	300	300			
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	550	550			
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	22000	22000			
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1000	1000			
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	300	300			
X4066426	BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70	TON	4138	3112	1026		

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	URBAN	PARKING		
				100% STATE	100% CITY		
				1000-2A	1025		
X4067100	POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50	TON	2075	1562	513		
X4409410	BITUMINOUS SURFACE REMOVAL 2 1/4"	SQ YD	48668	36568	12100		
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4			
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1			

* SPECIALTY ITEMS

REVISIONS	
NAME	DATE

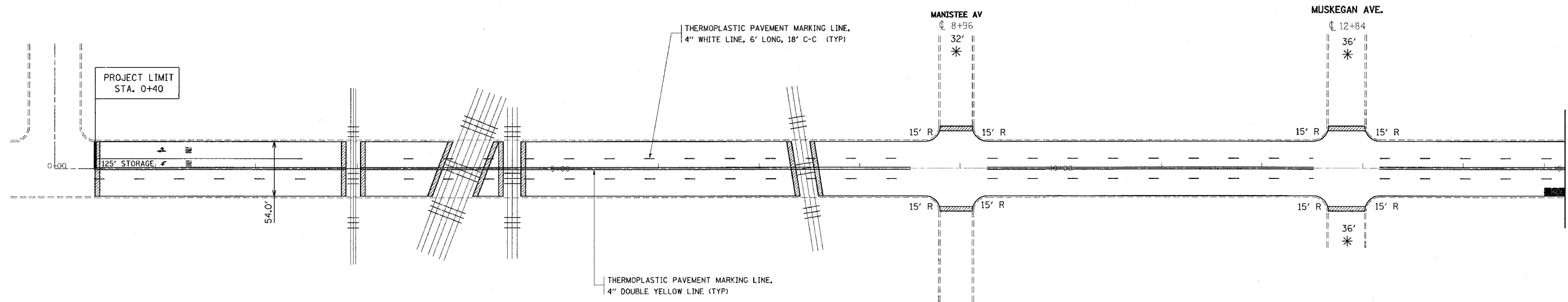
ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES



PLOT DATE: 3/31/2006

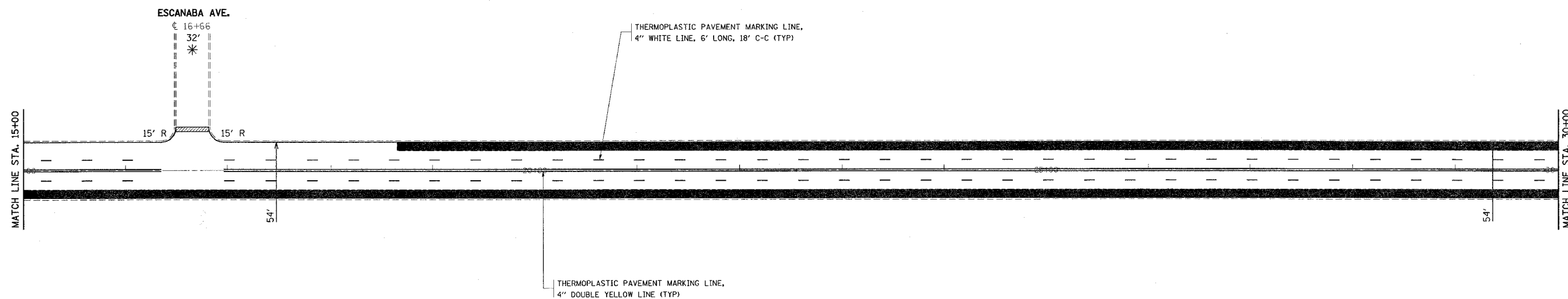
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F. A. S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1576	2121 RS-2	COOK	18	5
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

62846



-  BITUMINOUS SURFACE REMOVAL (BUTT JOINT)
SEE DETAILS IN SHEET 12 FOR TAPER LENGTH
-  8' PARKING LANE



-  BITUMINOUS SURFACE REMOVAL (BUTT JOINT)
SEE DETAILS IN SHEET 12 FOR TAPER LENGTH
-  8' PARKING LANE

REVISIONS	
NAME	DATE

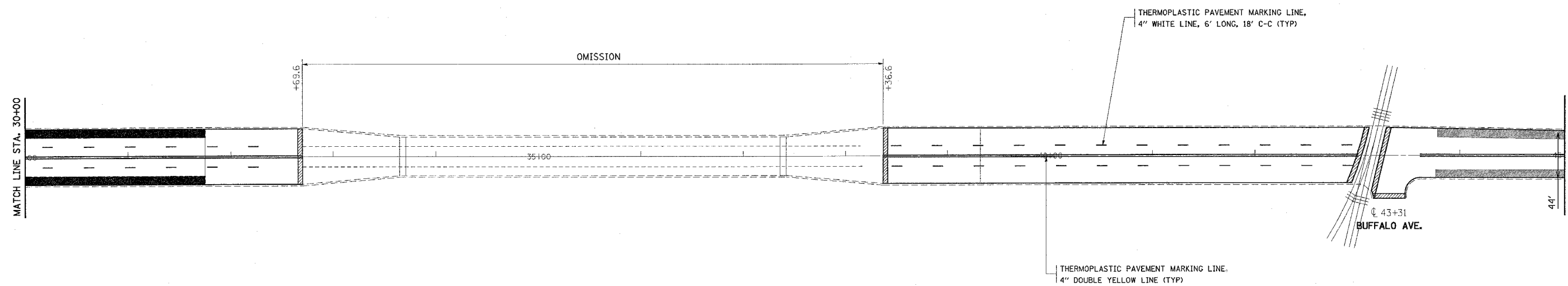
ILLINOIS DEPARTMENT OF TRANSPORTATION
**106 TH STREET
EXISTING AND PROPOSED
ROADWAY PLAN**
SCALE: VERT. 1"=50'
HORIZ. 1"=50'
DATE 5/4/2006
DRAWN BY SHB
CHECKED BY

5/4/2006
c:\p\o\ec\88d101105\88d101105.dwg
*REF-
*REF-

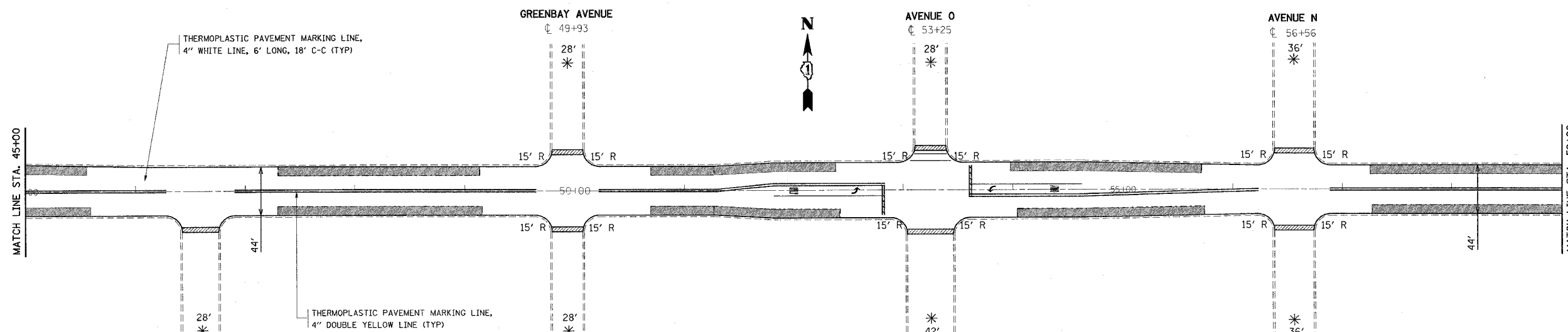
VI-

F. A. U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1576	2121 RS-2	COOK	18	6
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

62846



- BITUMINOUS SURFACE REMOVAL (BUTT JOINT)
SEE DETAILS IN SHEET 12 FOR TAPER LENGTH
- 8' PARKING LANE



- BITUMINOUS SURFACE REMOVAL (BUTT JOINT)
SEE DETAILS IN SHEET 12 FOR TAPER LENGTH
- 8' PARKING LANE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**106 TH STREET
EXISTING AND PROPOSED
ROADWAY PLAN**

SCALE: VERT. 1"=50'
HORIZ. 1"=50'

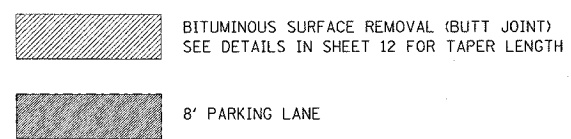
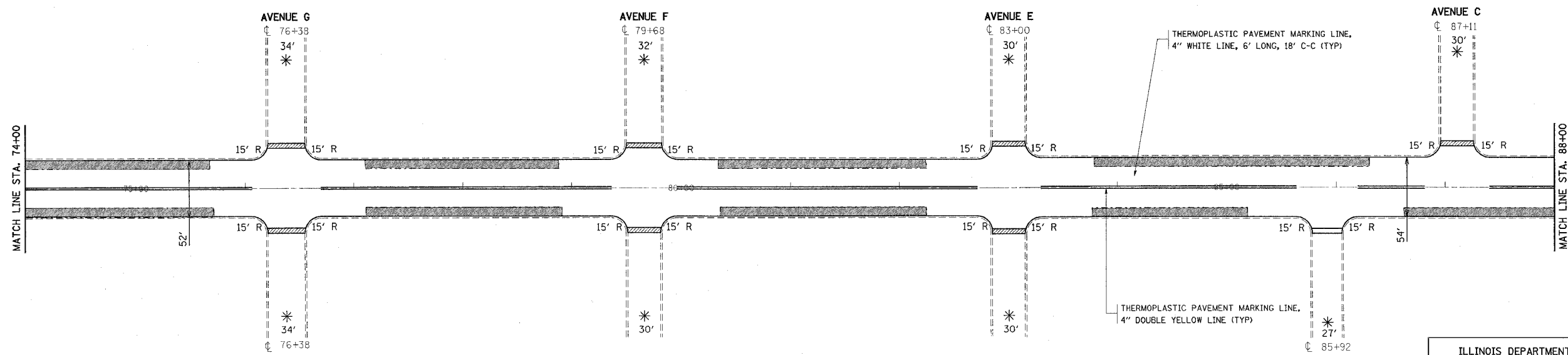
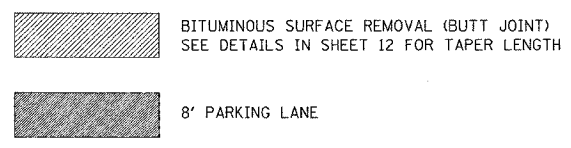
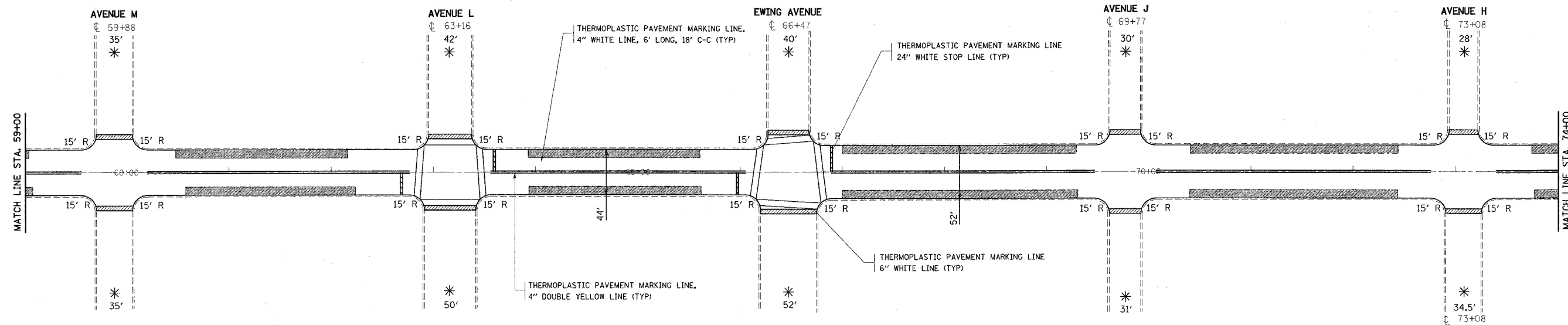
DATE 5/4/2006

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F. A. U. ETC.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1576	2121 RS-2	COOK	18	7
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

62846



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**106 TH STREET
EXISTING AND PROPOSED
ROADWAY PLAN**

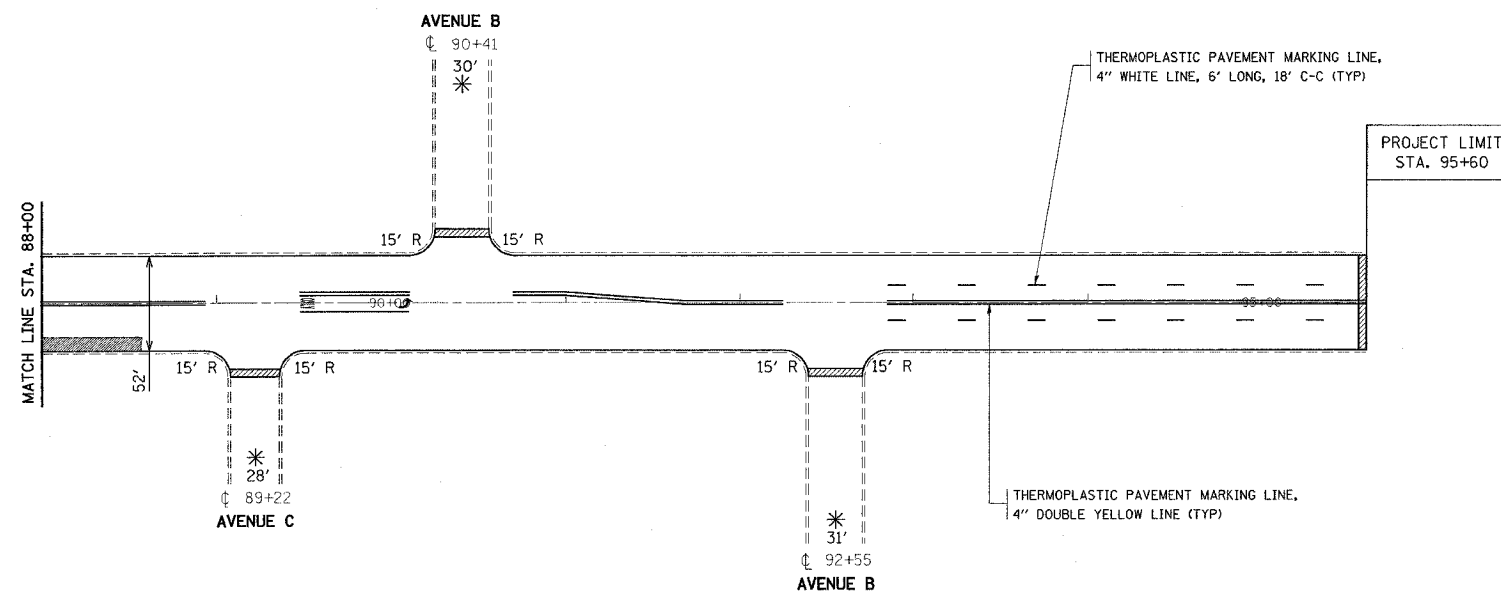
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HORIZ. 1"=50'
DATE 5/4/2006



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F. A. U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1576	2121 RS-2	COOK	18	8
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

62846



-  BITUMINOUS SURFACE REMOVAL (BUTT JOINT)
SEE DETAILS IN SHEET 12 FOR TAPER LENGTH
-  8' PARKING LANE

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REVISIONS	
NAME	DATE

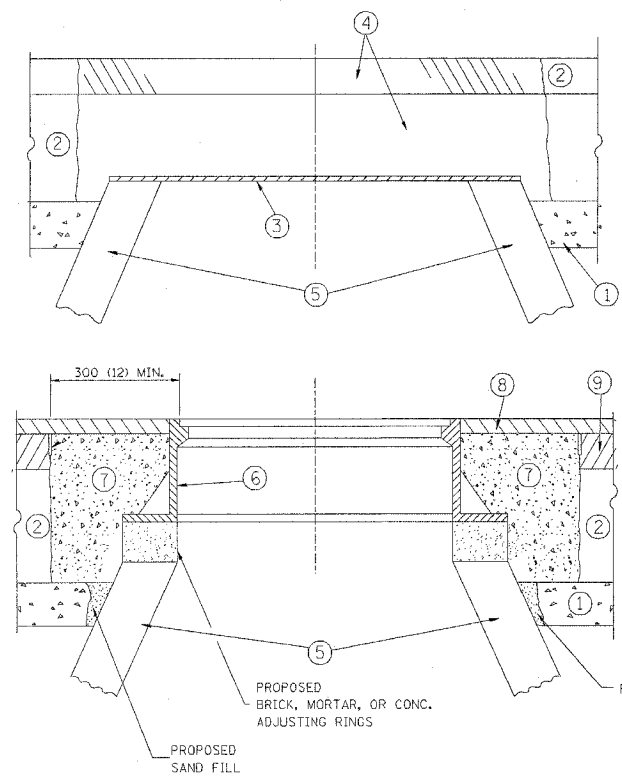
ILLINOIS DEPARTMENT OF TRANSPORTATION

**106 TH STREET
 EXISTING AND PROPOSED
 ROADWAY PLAN**

SCALE: VERT. 1"=50'
 HORIZ. 1"=50'
 DATE 5/4/2006

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K.A. SHE.	SECTION	COUNTY	TOTAL SHEETS
			18 9
STA.	TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 300 (12) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 900 (36) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 40 (1 1/2) THICK BITUMINOUS MATERIAL APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE BITUMINOUS MATERIAL AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 900 (36) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND BITUMINOUS MATERIAL
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL
- ⑧ PROPOSED BITUMINOUS CONCRETE SURFACE COURSE
- ⑨ PROPOSED BITUMINOUS CONCRETE BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: FRAMES AND LIDS TO BE ADJUSTED, SPECIAL EACH

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN

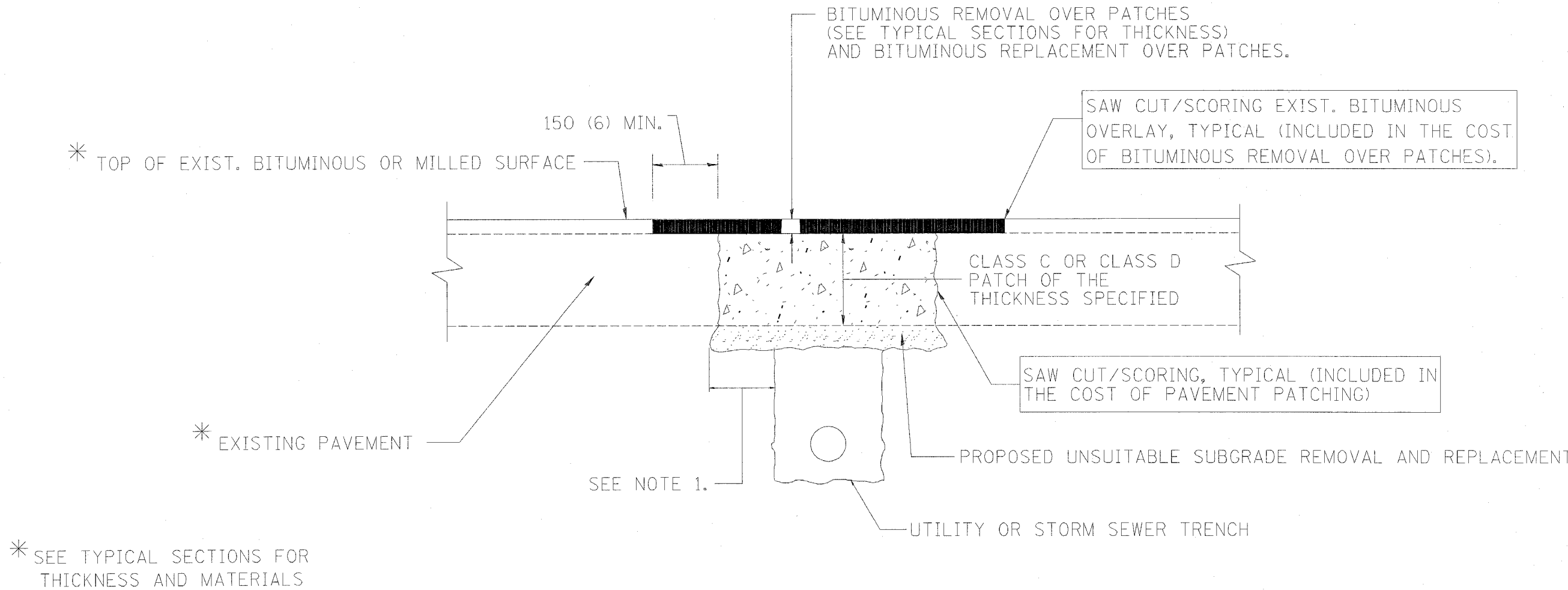
ILLINOIS DEPARTMENT OF TRANSPORTATION
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04

SCALE: NONE
DATE: 05/17/2004

DRAWN BY
CHECKED BY
BD600-03
REVISION DATE: 0

P. A. NO.	SECTION	COUNTY	TOTAL SHEETS
			18 10
STA.	TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	



NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 300 (12) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE SPECIAL PROVISION "PATCHING WITH BITUMINOUS OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

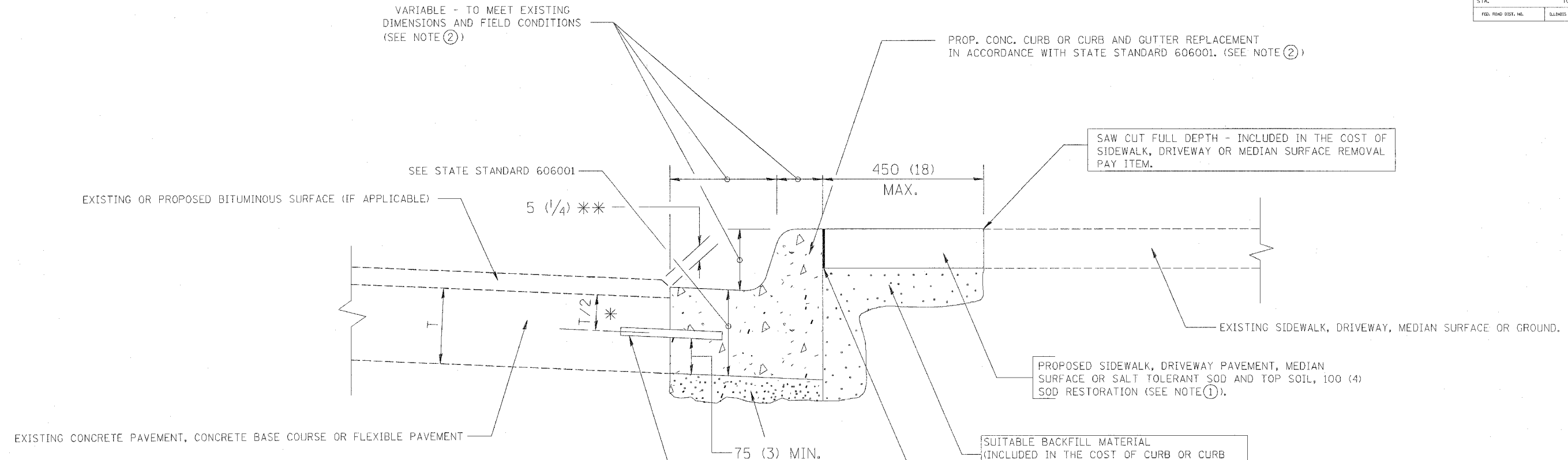
PAVEMENT PATCHING FOR BITUMINOUS SURFACED PAVEMENT

REVISIONS		REVISIONS	
NAME	DATE	NAME	DATE
R. SHAH	10/25/94	ART ABBAS	04/27/98
R. SHAH	01/14/95		
R. SHAH	03/23/95		
R. SHAH	04/24/95		
A. HOUSEH	03/15/96		
A. ABBAS	03/21/97		
A. ABBAS	01/20/98		

SCALE: NONE
DATE 10/18/2002

DRAWN BY
CHECKED BY
BD400-04 (BD-2)
REVISION DATE: D-

FILE NO.	SECTION	COUNTY	TOTAL SHEETS
			18 11
STA.	TO STA.		
FED. RDW DIST. NO.	ILLINOIS	FED. AID PROJECT	



- * 75 (3) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SALT TOLERANT SOD AND TOP SOIL, 100 (4) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF BITUMINOUS SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER METER (FOOT) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

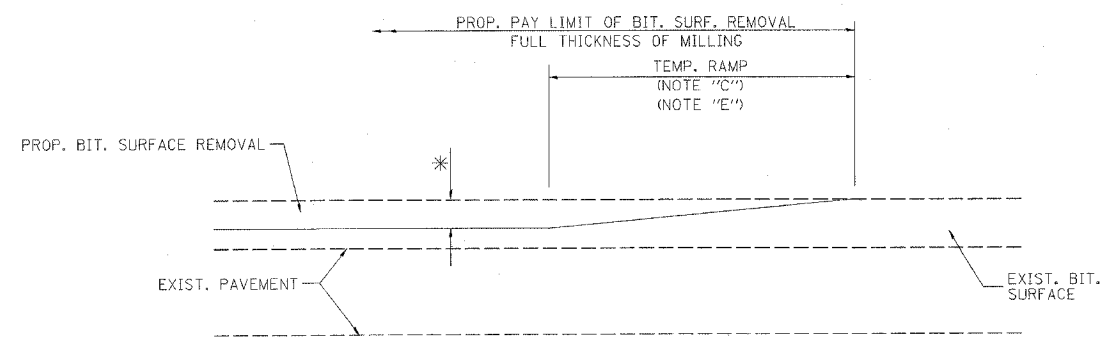
REVISIONS	
NAME	DATE
M. DE YONG	05/28/91
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01

ILLINOIS DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

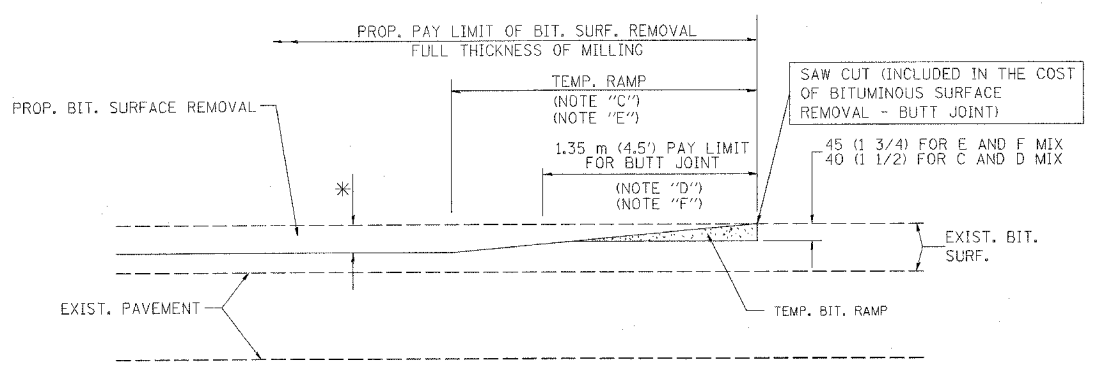
SCALE: NONE
 DATE: 10/18/2002
 DRAWN BY:
 CHECKED BY:
 REVISION DATE:

10/18/2002
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 V4-BD24



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND BIT. TAPER SEE DETAIL BELOW)

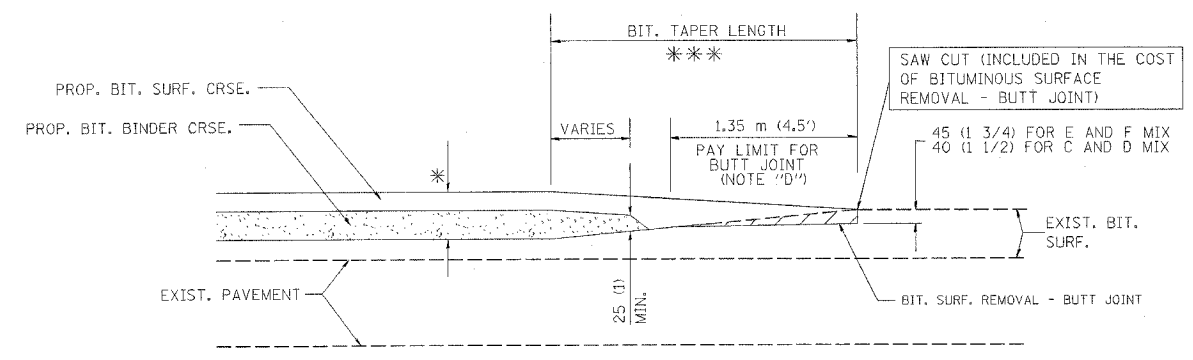
OPTION 1



BITUMINOUS CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND BIT. TAPER SEE DETAIL BELOW)

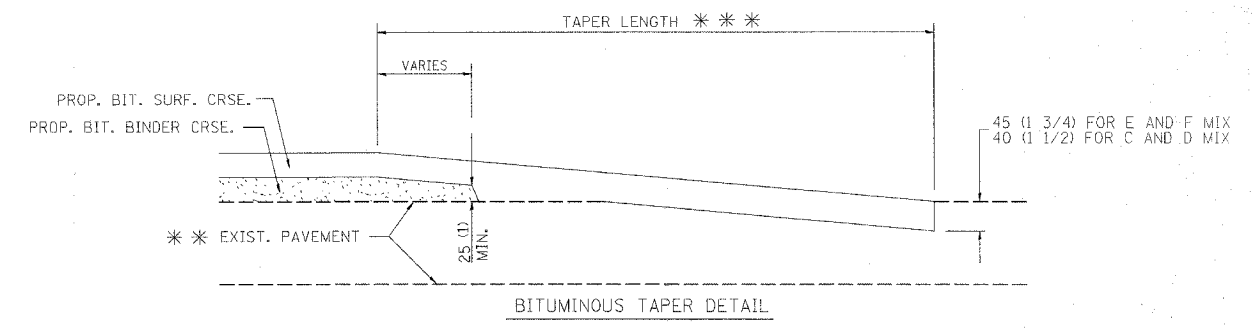
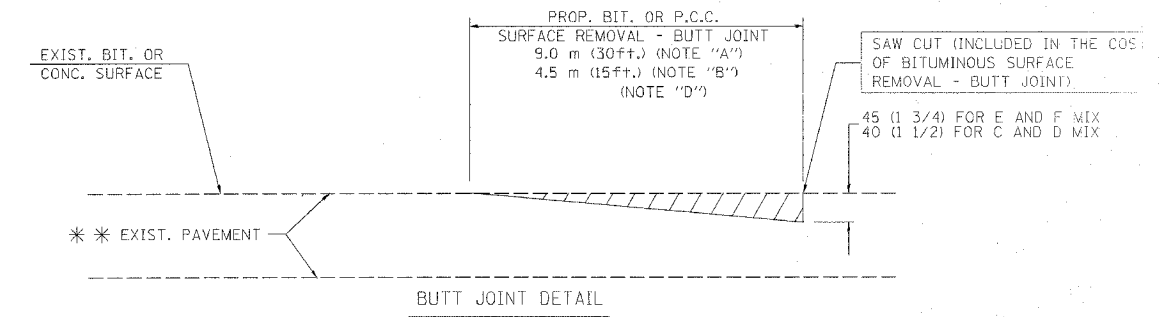
OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT AND BITUMINOUS TAPER

TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR RESURFACING ONLY

*** PC CONCRETE, BITUMINOUS OR BITUMINOUS RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING BITUMINOUS SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED BITUMINOUS COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 900 (3 ft.) PER INCH OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 1.35 m (4.5') TEMP. BIT. RAMP WILL BE PAID AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT".
 - G: SEE ARTICLE 406.18 AND 406.24 OF THE STANDARD SPECIFICATIONS FOR "BITUMINOUS AND PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 6.1 m (20') PER 25 (1) RESURFACING (NOTE "A")
3.0 m (10') PER 25 (1) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND BITUMINOUS TAPER DETAILS

BASIS OF PAYMENT:

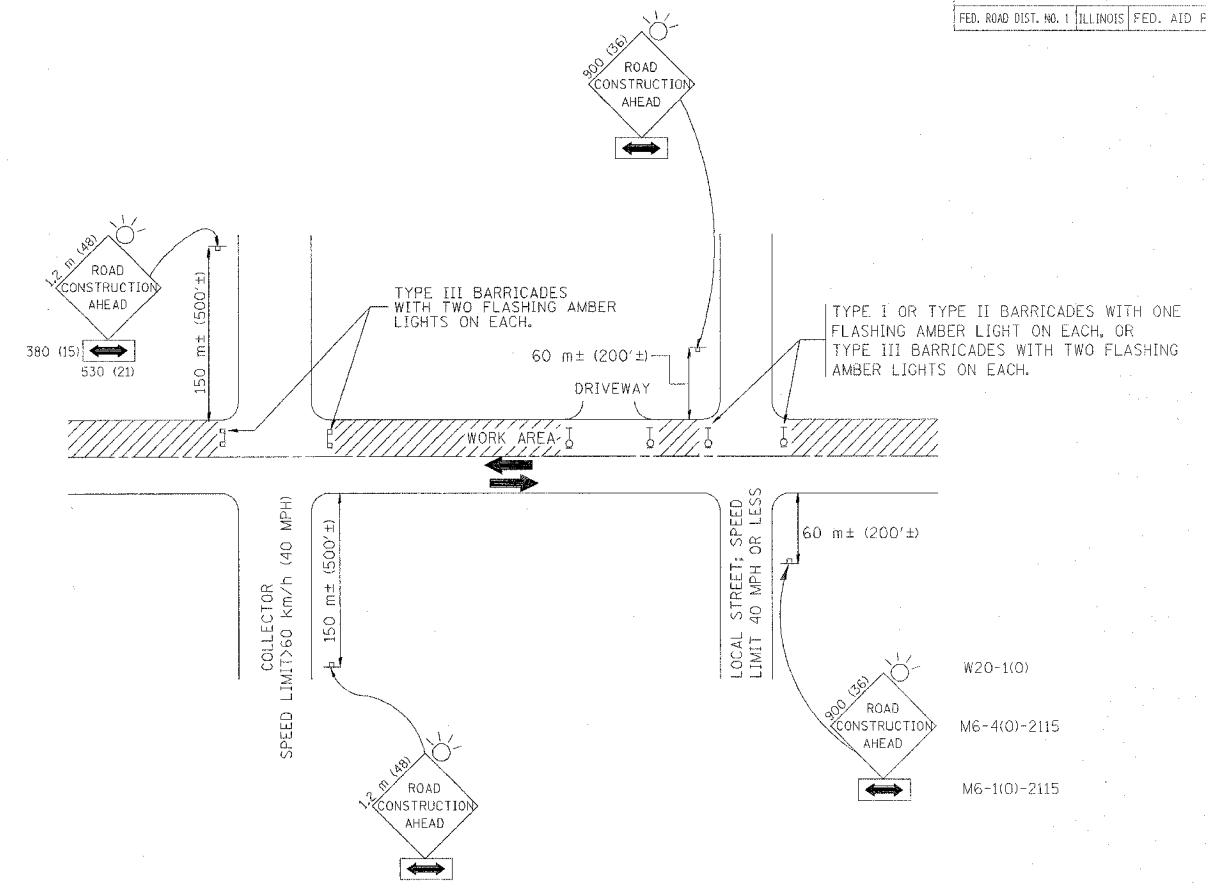
THE BUTT JOINT WILL BE PAID FOR PER SQUARE METER (SQUARE YARD.) AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT" OR AS "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01

SCALE: NONE
DATE PLOTTED: 10/18/2002

DRAWN BY
CHECKED BY
REVISION DATE: 0

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS
			18 of 14
STA.		TO STA.	
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

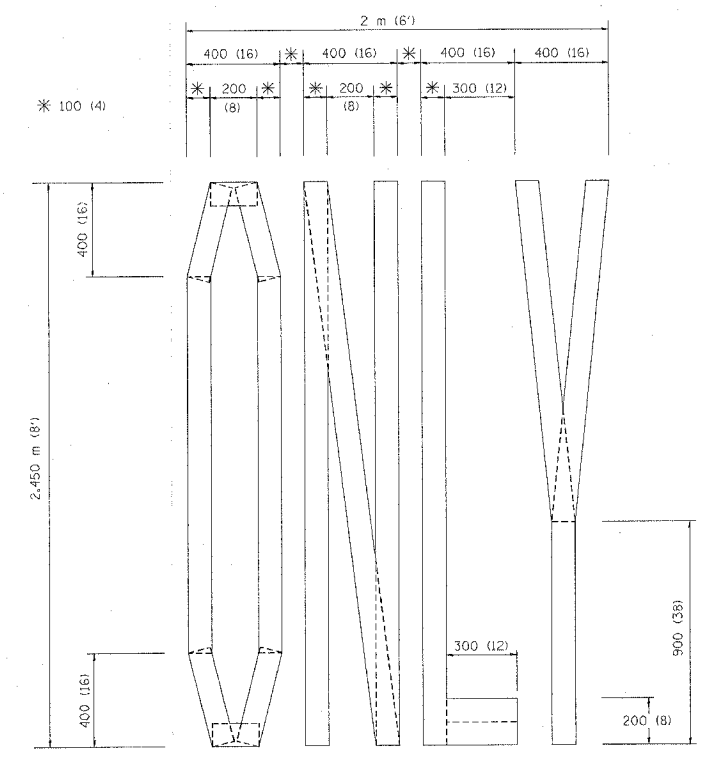
- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

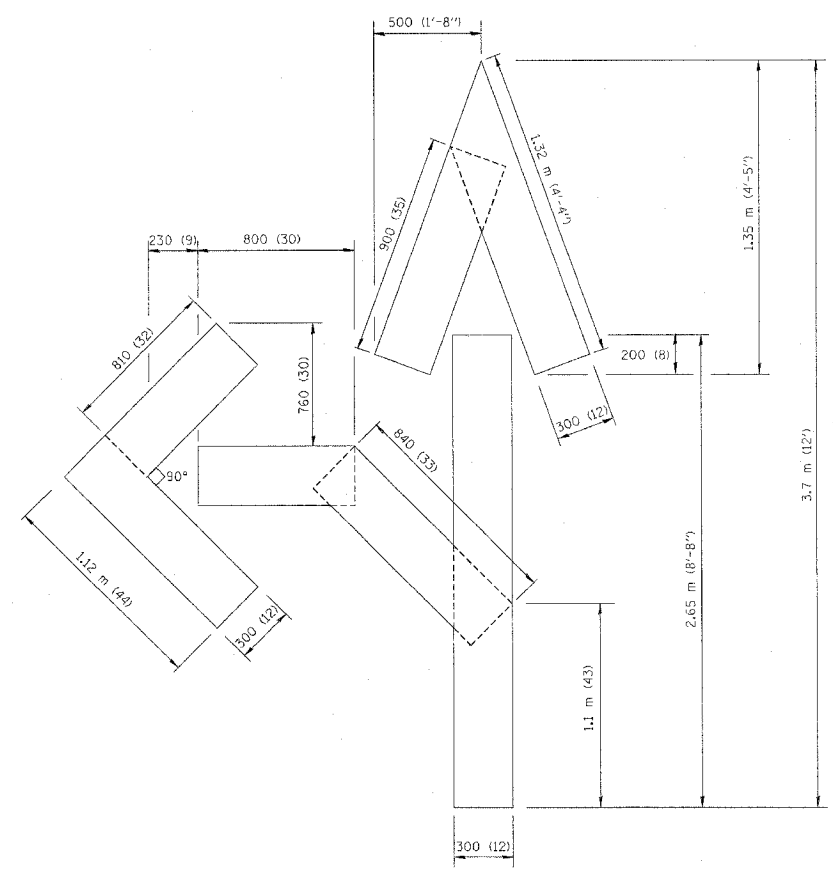
ILLINOIS DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL AND PROTECTION
 FOR
 SIDE ROADS, INTERSECTIONS, AND
 DRIVEWAYS

SCALE: VERT. _____
 HORIZ. _____
 DATE 10/18/2002

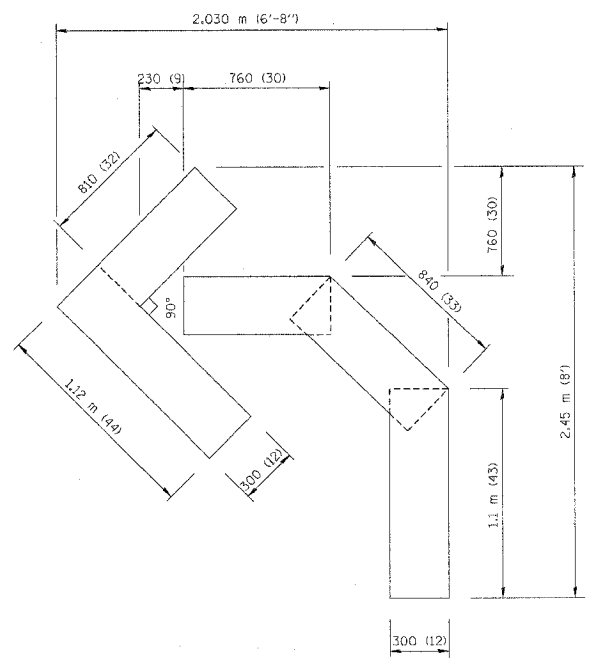
DRAWN BY _____
 CHECKED BY _____
 TC-10
 REVISION DATE: 0/



QUANTITY
 100 (4) LINE = 19.7 m (64.1 ft.)
 1.97 sq. m (21.1 sq. ft.)



QUANTITY
 100 (4) LINE = 25.3 m (82.5 ft.)
 2.53 sq. m (27.5 sq. ft.)



QUANTITY
 100 (4) LINE = 13.9 m (45.5 ft.)
 1.39 sq. m (15.2 sq. ft.)

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

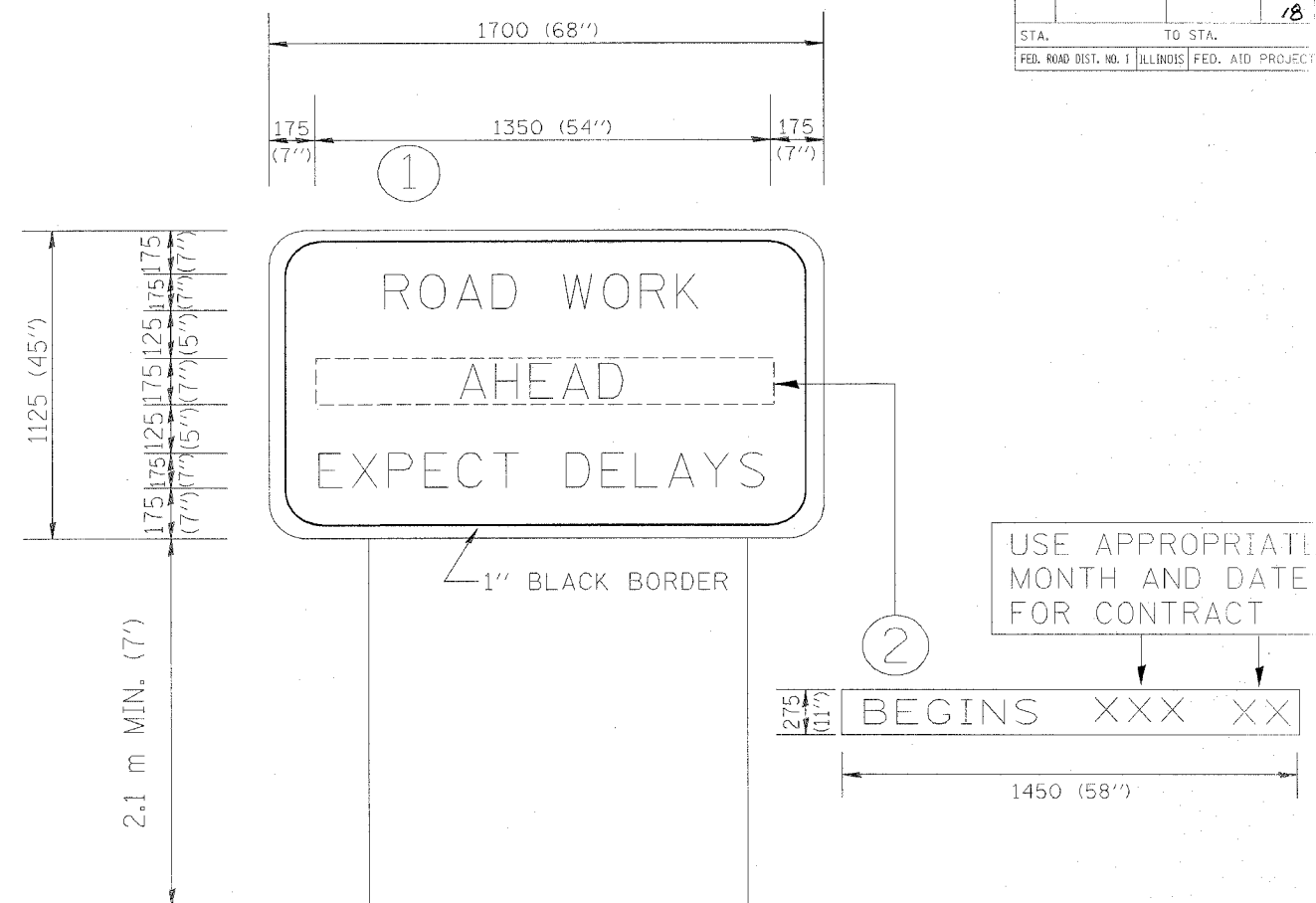
PAVEMENT MARKING
 LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

REVISIONS		
NAME	DATE	
T. RAMMACHER	09/18/94	
J. OBERLE	08/01/96	
T. RAMMACHER	06/05/96	
T. RAMMACHER	11/04/97	
T. RAMMACHER	03/02/98	
E. GOMEZ	08/28/00	

SCALE: NONE
 DATE 10/18/2002

DRAWN BY CADD
 CHECKED BY TC-16

REVISION DATE: G



NOTES:

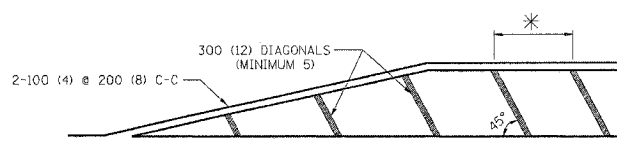
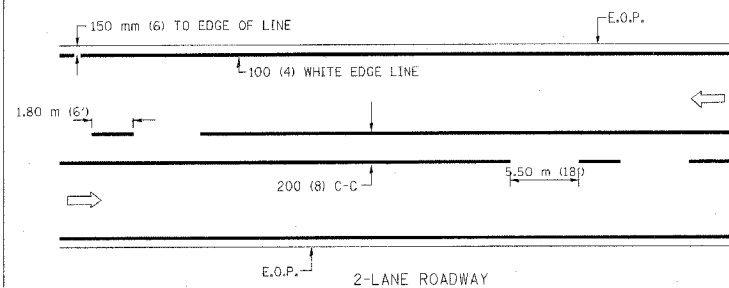
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 2.3 SQ. M. (25.70 SQ. FT.)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. MIRS	9-15-97
R. MIRS	12-11-97
T. RAMMACHER	2-2-99

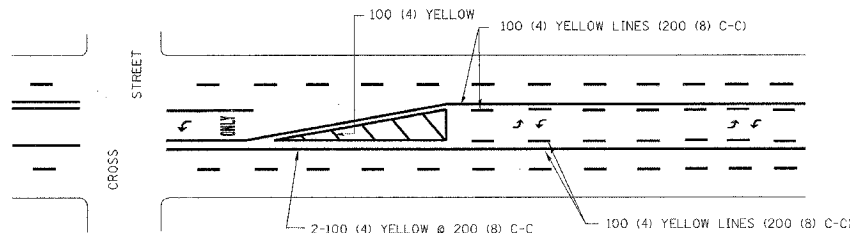
ILLINOIS DEPARTMENT OF TRANSPORTATION
TEMPORARY INFORMATION SIGNING

SCALE: DATE 10/18/2002
DRAWN BY: BUR. OF D.
CHECKED BY
TC22
REVISION DATE: 02/02/99

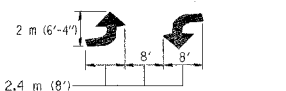


* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
* DIAGONAL LINE SPACING: 6.1 m (20') C-C

PAINTED MEDIANS

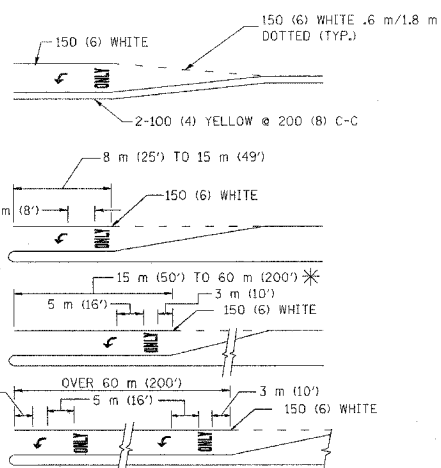


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 60 m (200') TO 90 m (300') INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

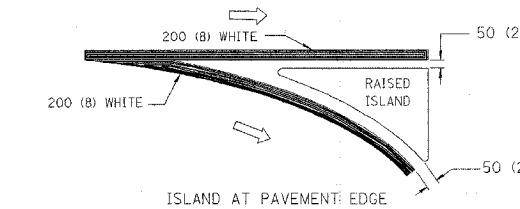
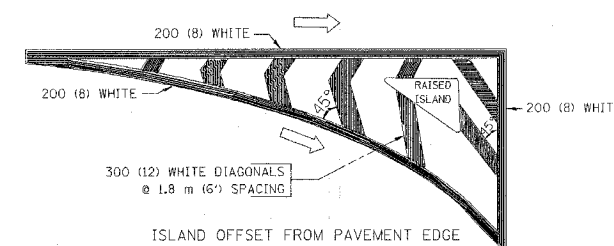


FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED.
AREA = 1.47 m² (15.8 SQ. FT.) ONLY AREA = 2.13 m² (22.9 SQ. FT.)

* TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	1.80 m (6') LINE WITH 5.50 m (18') SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	200 (8) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	100 (4) 2 @ 100 (4)	SOLID SOLID	YELLOW YELLOW	200 (8) C-C
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	1.80 m (6') LINE WITH 5.50 m (18') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	1.8 m (6') LINE WITH 5.50 m (18') SPACE FOR SKIP-DASH; 200 (8) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 200 (8) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 600 (2') APART 700 (2'-4") APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT; OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	200 (8) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 6.1 m (20') (LESS THAN 50 km/h (30 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=0.33m ² (3.6 SQ. FT.) EACH "X"=5.0 m ² (54.0 SQ. FT.)

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

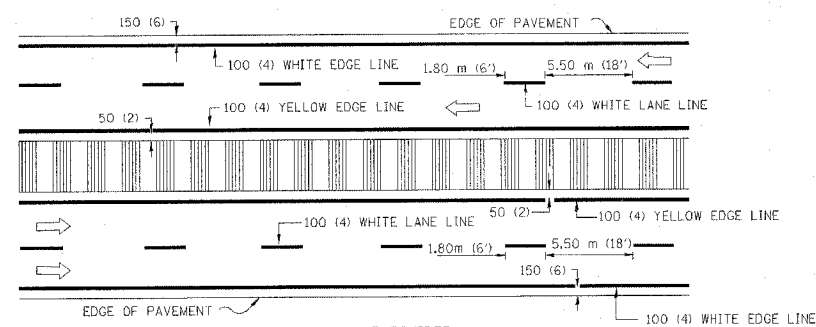
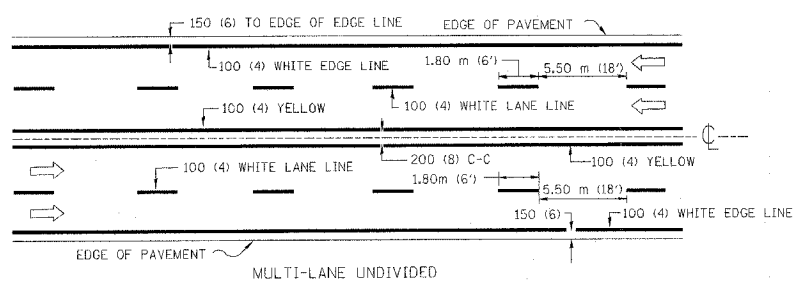
All dimensions are in millimeters (inches) unless otherwise shown. 1 OF

ILLINOIS DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT
MARKINGS

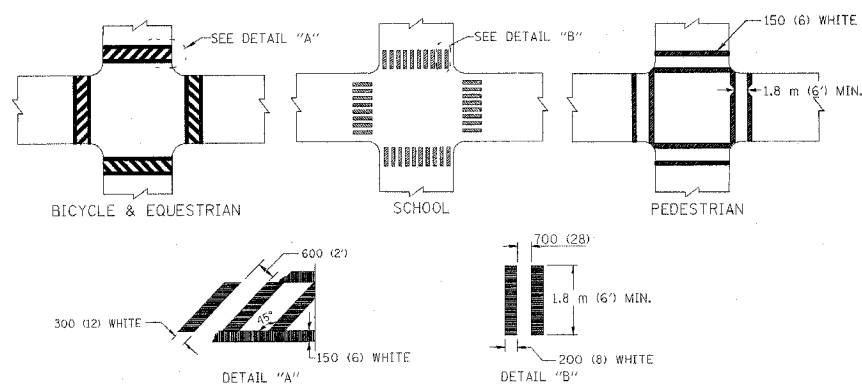
SCALE: NONE
DATE: 10/18/2002
DRAWN BY: CAD
CHECKED BY: TC-24
REVISION DATE:

REVISIONS	
NAME	DATE
T. RAMMACHER	12/07/00

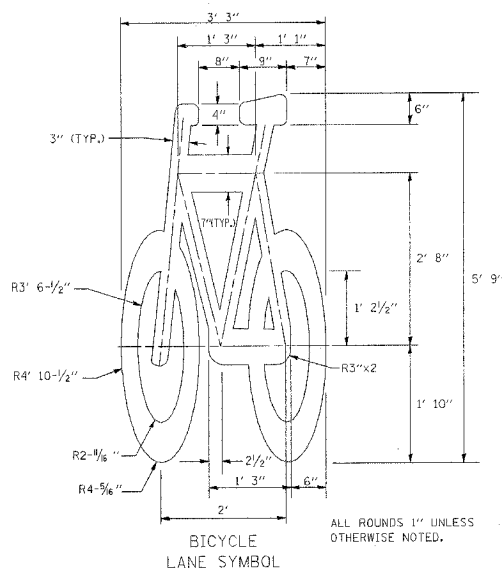
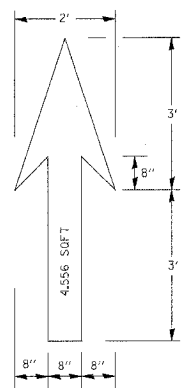


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

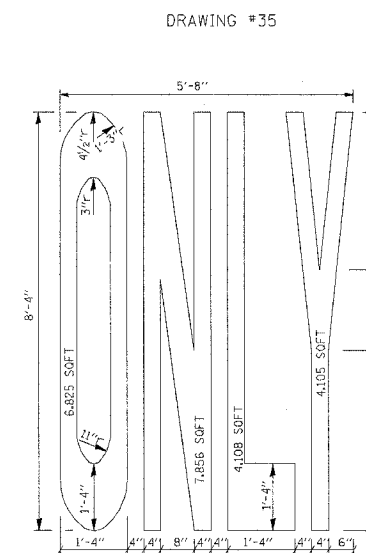
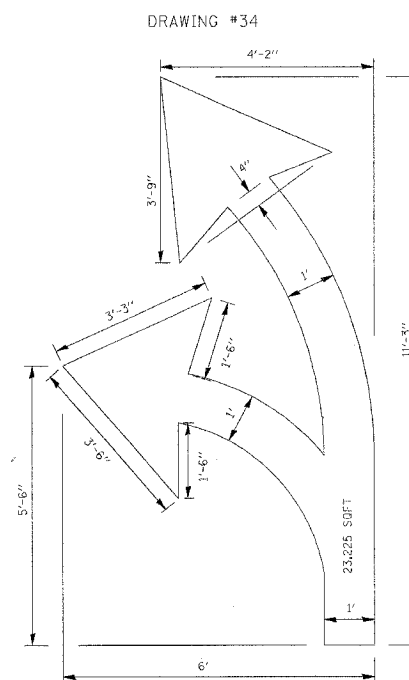
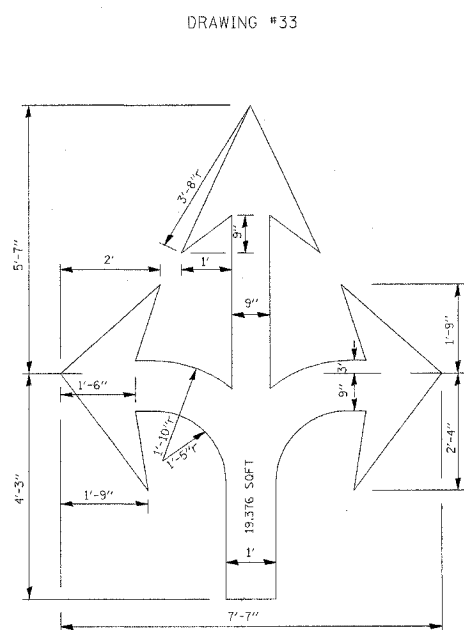
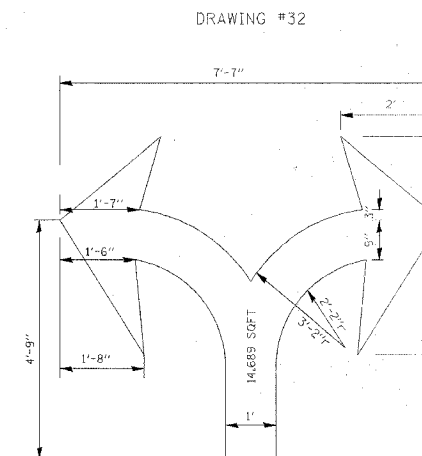
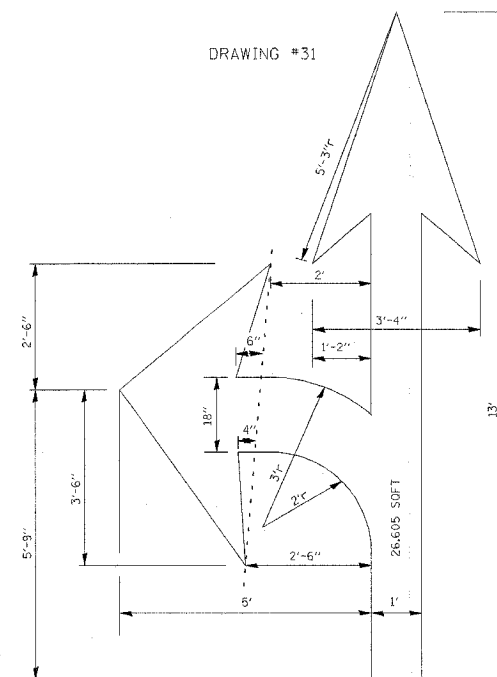
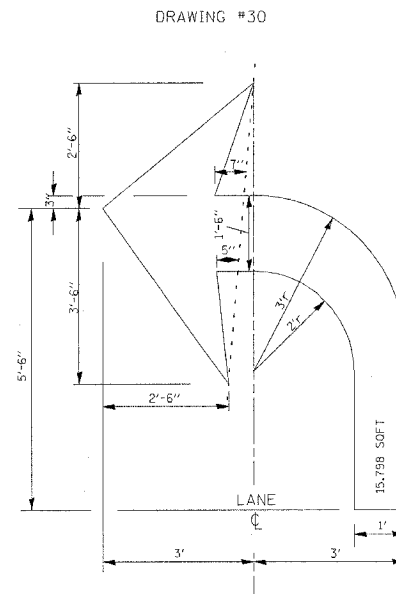
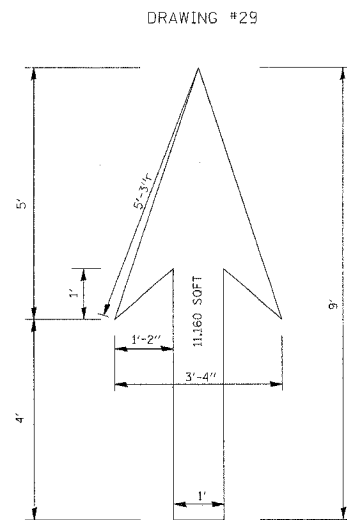


TYPICAL CROSSWALK MARKING



NOTE:
 1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
 2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN COMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28



NOTE:
 ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

REVISIONS	
NAME	DATE
T. RAMMACHER	12/07/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CITY OF CHICAGO
 TYPICAL PAVEMENT MARKINGS.

SCALE: NONE
 DATE 03/09/2004

DRAWN BY
 CHECKED BY
 TC-24

REVISION DATE: 0