

| F.A.D. REC. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------------|----------------|------------------------------|--------------|-----------|
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 1 |
| COVER SHEET | | | | |
| FED. ROAD DISTRICT NO. 7 | ILLINOIS | FED. AID PROJECT F-0021(068) | | |
| CONTRACT NO. 83852 | | | | |

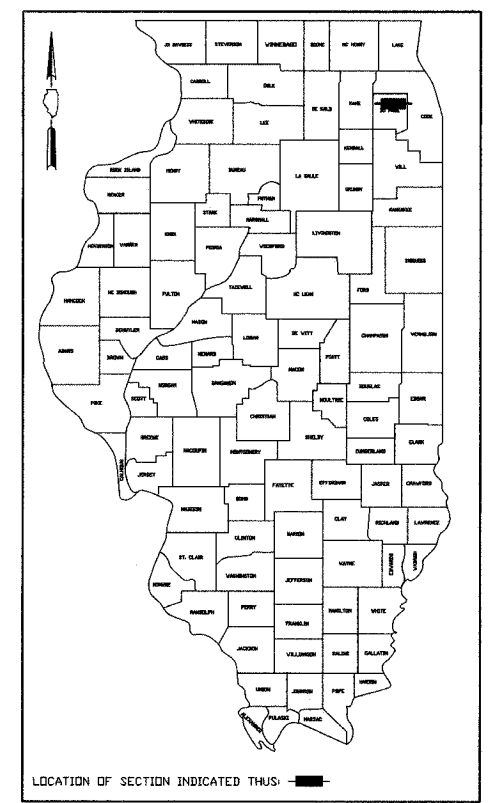
INDEX OF SHEETS

| SHEET NO. | DESCRIPTION |
|-----------|---|
| 1 | COVER SHEET |
| 2 | GENERAL NOTES |
| 3 | SUMMARY OF QUANTITIES |
| 4 | TYPICAL SECTIONS |
| 5 | ALIGNMENT & BENCHMARKS |
| 6-7 | ROADWAY IMPROVEMENT PLANS |
| 8 | MAINTENANCE OF TRAFFIC |
| 9 | PAVEMENT MARKING AND SIGNING PLAN |
| 10 | EROSION CONTROL AND LANDSCAPING PLANS |
| 11-14 | CROSS SECTIONS |
| 15-21 | ROADWAY DETAILS |
| 22-23 | TEMPORARY TRAFFIC SIGNAL PLANS |
| 24-25 | TEMPORARY TRAFFIC SIGNAL INTERCONNECT PLANS |
| 26-29 | PERMANENT TRAFFIC SIGNAL PLANS |
| 30-31 | TRAFFIC SIGNAL INTERCONNECT PLANS |
| 32 | MAST ARM MOUNTED STREET NAME SIGNS |
| 33-36 | TRAFFIC SIGNAL DETAILS |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED FEDERAL AID PROJECT**

**F.A.P. ROUTE 21 (U.S. ROUTE 20/LAKE STREET)
AT F.A.U. ROUTE 3809 (SPRINGFIELD DRIVE)
SECTION 05-00050-00-CH**

**PROJECT NO. F-0021(068)
WIDENING, RESURFACING,
AND TRAFFIC SIGNAL MODERNIZATION
DU PAGE COUNTY
C-91-226-05**



VILLAGE OF BLOOMINGDALE

APPROVED April 4 2006

Michael J. Mansueti
DIRECTOR OF VILLAGE STREETS

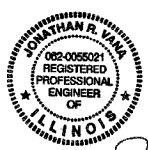
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PASSED APRIL 6 2006

Charles Holt
BUREAU CHIEF OF LOCAL ROADS AND STREETS

SIGNED April 10 2006

Diane M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER



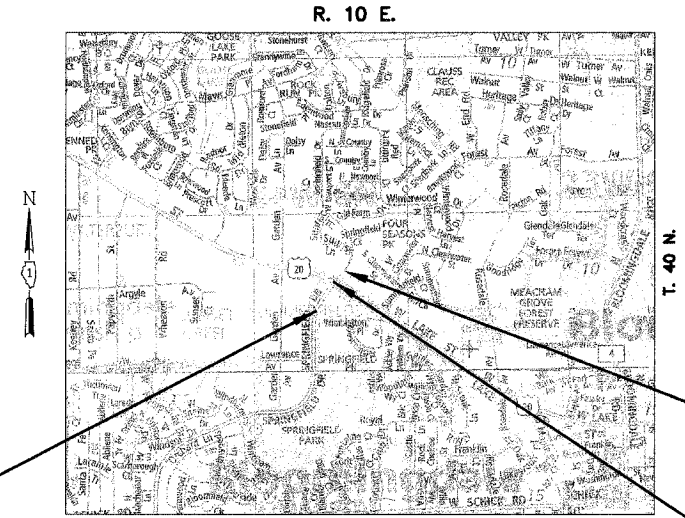
REGISTERED P.E., STATE OF ILLINOIS EXPIRES 30 Nov 2007

PLANS PREPARED BY:
CIVILTECH
450 E. Devon Ave., Suite 300 - Itasca, Illinois 60143
Tel: 630.773.3900 - Fax: 630.773.3975
www.civiltechinc.com

PRINTED BY AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF STATE STANDARDS

| NO. | DESCRIPTION |
|-----------|---|
| 000001-04 | STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS |
| 280001-02 | TEMPORARY EROSION CONTROL SYSTEMS |
| 424001-04 | SIDEWALK RAMPS ACCESSIBLE TO THE DISABLED |
| 442201-01 | CLASS C AND D PATCHES |
| 542606 | REINFORCED CONCRETE PIPE TEE |
| 602001 | CATCH BASIN TYPE A |
| 602011 | CATCH BASIN TYPE C |
| 602401-01 | MANHOLE TYPE A |
| 604001-02 | FRAME AND LIDS, TYPE 1 |
| 604006-02 | FRAME AND GRATE, TYPE 3 |
| 604036-01 | GRATE, TYPE B |
| 606001-02 | CONCRETE CURB AND COMBINATION CONCRETE CURB AND GUTTER |
| 701101-01 | OFF-RD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE |
| 701106-01 | OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' AWAY |
| 701501-03 | URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED |
| 701606-04 | URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN |
| 701701-04 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 701801-03 | LANE CLOSURE, MULTILANE, 1W OR 2W, CROSSWALK OR SIDEWALK CLOSURE, FOR SPEEDS < 45 MPH |
| 702001-06 | TRAFFIC CONTROL DEVICES |
| 720001 | SIGN PANEL MOUNTING DETAILS |
| 720006 | SIGN PANEL ERECTION DETAILS |
| 780001-01 | TYPICAL PAVEMENT MARKINGS |
| 805001 | ELECTRICAL SERVICE INSTALLATION DETAILS |
| 814001 | CONCRETE HANDHOLES |
| 814006 | DOUBLE HANDHOLES |
| 857001 | STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES |
| 877001-02 | STEEL MAST ARM ASSEMBLY AND POLE |
| 878001-04 | CONCRETE FOUNDATION DETAILS |
| 880001 | SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION |
| 880006 | TRAFFIC SIGNAL MOUNTING DETAILS |
| 886001 | DETECTOR LOOP INSTALLATIONS |
| 886006 | TYPICAL LAYOUT FOR DETECTION LOOPS |



IMPROVEMENT IS LOCATED IN THE VILLAGE OF BLOOMINGDALE AND THE VILLAGE OF ROSELLE.

PROJECT BEGINS STATION 24+65.46

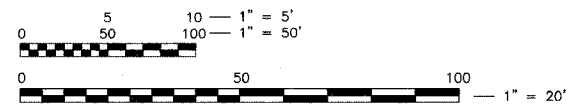
PROJECT ENDS STATION 52+32.00

PROJECT OMISSION STATION 27+94.81 TO 48+51.99

PROJECT LENGTH:
SPRINGFIELD DRIVE = 768 FT. (0.145 MILE) (GROSS)
709 FT. (0.134 MILE) (NET)

DESIGN DESIGNATIONS:
SPRINGFIELD DRIVE - N. LEG 4,000(2030) • LOCAL COLLECTOR • 0.085(FD-20)
SPRINGFIELD DRIVE - S. LEG 9,000(2030) • LOCAL COLLECTOR • 0.11(FD-20)

DESIGN SPEEDS:
LAKE STREET - 45 MPH (POSTED SPEED = 40 MPH)
SPRINGFIELD DRIVE (N. LEG) - 30 MPH (POSTED SPEED = 25 MPH)
SPRINGFIELD DRIVE (S. LEG) - 40 MPH (POSTED SPEED = 35 MPH)



Call Before You Dig
JULIE
ILLINOIS ONE CALL SYSTEM
1-800-892-0123

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

CONTRACT NO. 83852

THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2002 BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, SHALL GOVERN THIS WORK.

FEDERAL AID DESIGN ENGINEER: ABIGAIL BRINKS 847-705-4233 SCHAMBURG, IL
CONSULTANT ENGINEER: JON VANA, P.E. CIVILTECH ENGINEERING, INC.

| FAH FILE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|----------------|--------|-----------------|--------------|
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 2 |
| GENERAL NOTES | | | | |
| FED. ROAD DISTRICT NO. 7 ILLINOIS FED. AID PROJECT F-0021(068) | | | | |

CONTRACT NO. 83852

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2002 AND THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED MARCH 1, 2005.
- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2002; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED MARCH 1, 2005; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (MUTCD); THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", SSTDG), "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY 1996 FIFTH EDITION, THE DETAILS IN THE PLANS, AND THE SPECIAL PROVISIONS AND IDOT STANDARD DRAWINGS INCLUDED IN THE CONTRACT DOCUMENTS.
- ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700, 701, AND 702 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION."
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED AS THE RESIDENT ENGINEER.
- NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET AND APPROPRIATE PERMITS HAVE BEEN OBTAINED FROM THE VILLAGE OF BLOOMINGDALE AND THE VILLAGE OF ROSELLE.
- ALL UTILITY COMPANIES, SCHOOL DISTRICTS, AND LOCAL POLICE AND FIRE DEPARTMENTS SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 72 HOURS PRIOR TO THE START OF CONSTRUCTION.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

STAKING

- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE VILLAGE, ITS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- THE STATION/OFFSET/ELEVATIONS NOTED FOR ALL DRAINAGE STRUCTURES LOCATED IN THE CURB LINE REFER TO THE POSITION OF THE ADJACENT PROPOSED EDGE OF PAVEMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE OFFSET NECESSARY FOR EACH STRUCTURE TO SET THE FRAME AND GRATE IN THE PROPER LOCATION. ALL OTHER STRUCTURES ARE DIMENSIONED TO THE CENTER OF STRUCTURE.
- PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT, UNLESS OTHERWISE INDICATED.
- ESTIMATED LOCATIONS OF SIDEWALK REMOVAL AND REPLACEMENT HAVE BEEN SHOWN ON THE PLANS. THE ENGINEER WILL DETERMINE THE EXACT LIMITS IN THE FIELD DURING CONSTRUCTION.
- ALL ELEVATIONS SHOWN ON THESE PLANS ARE ON U.S.G.S. DATUM.
- THE CONSTRUCTION BASELINE HAS BEEN ESTABLISHED FOR STAKING PURPOSES ONLY AND IS NOT INTENDED TO BE A CENTERLINE.

TREE REMOVAL, CLEARING AND HEDGE REMOVAL

- ALL TREES ARE DESIGNATED TO BE SAVED UNLESS OTHERWISE NOTED ON THE PLANS, AND SHALL BE PROTECTED IN ACCORDANCE WITH THE PROVISIONS OF ARTICLE 201.05 OF THE STANDARD SPECIFICATIONS.
- ALL CLEARING AND REMOVAL OF TREES UNDER 6" IN DIAMETER SHALL BE INCIDENTAL TO THE COST OF EARTH EXCAVATION.
- ALL CLEARING AND THE REMOVAL AND/OR RELOCATION OF BUSHES, AS DIRECTED BY THE ENGINEER, SHALL BE INCIDENTAL TO THE COST OF EARTH EXCAVATION.
- ALL LIMBS, BRANCHES AND OTHER DEBRIS RESULTING FROM THIS WORK SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN EXPENSE IN ACCORDANCE WITH ARTICLE 202.03.

PAVING AND CURB & GUTTER

- THE CONTRACTOR SHALL SAW CUT PAVEMENT, CURB & GUTTER, AND SIDEWALK AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING MATERIAL TO BE REMOVED BY MEANS OF AN APPROVED CONCRETE SAW TO A DEPTH AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE ITEM BEING REMOVED.
- BASE COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY CURED AND BACKFILLED TO THE SATISFACTION OF THE ENGINEER.
- BITUMINOUS CONCRETE SURFACE COURSE SHALL NOT BE PLACED UNTIL ALL EARTH EXCAVATION, TOP SOIL PLACEMENT, AND BITUMINOUS CONCRETE BINDER COURSE HAVE BEEN COMPLETED TO THE SATISFACTION OF THE ENGINEER.
- THE THICKNESSES OF BITUMINOUS MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACE OR BASE UPON WHICH THE BITUMINOUS MATERIALS ARE PLACED.
- THE MAXIMUM COMPACTED THICKNESS OF A LIFT OF BITUMINOUS BASE COURSE SHALL BE 4" UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.

UTILITIES

- THE CONTRACTOR SHALL COOPERATE WITH THE VILLAGE IN ANY UNDERGROUND UTILITY CONSTRUCTION WHICH THE VILLAGE MAY WANT TO PLACE DURING THE CONTRACTOR'S OPERATIONS.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES. THE LOCATION OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING HIS CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 107.31 OF THE "STANDARD SPECIFICATIONS." THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ABOVE AND BELOW GROUND UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE VILLAGE. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.
- COORDINATION OF ANY UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS, WATER, SEWER, AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)
- WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT.
- ANY EXISTING OR PROPOSED SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO COST TO THE VILLAGE.
- ALL AUXILIARY VALVES, FRAMES, GRATES, LIDS AND BOXES REMOVED FROM EXISTING WATER SERVICE OR SEWER STRUCTURES WHICH ARE TO BE ABANDONED OR ADJUSTED WITH A NEW OR DIFFERENT FRAME AND LID SHALL BECOME THE PROPERTY OF THE CONTRACTOR. ALL HYDRANTS TO BE REMOVED SHALL BE REMOVED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND THE CONTRACTOR SHALL CONTACT THE VILLAGE TO DETERMINE IF THE VILLAGE WANTS THEM RETURNED TO PUBLIC WORKS OR DISPOSED OF BY THE CONTRACTOR.
- THE CONTRACTOR SHALL RECEIVE NO ADDITIONAL COMPENSATION FOR CONSTRUCTION STAGING NECESSARY TO ACCOMMODATE UTILITY RELOCATION OR ADJUSTMENT AND/OR FOR DELAYS CAUSED BY UTILITY RELOCATION OR ADJUSTMENT.
- THE CONTRACTOR SHALL FURNISH ALL LABOR, EQUIPMENT AND MATERIAL NECESSARY FOR DEWATERING TRENCH EXCAVATIONS AS WELL AS SHORING TRENCH WALLS DURING UTILITY OPERATIONS. COMPLIANCE WITH THE ABOVE WILL BE INCIDENTAL TO THE UTILITY INSTALLATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING LOCAL AGENCIES MAINTAINING SANITARY SEWERS AND WATER MAINS TO VERIFY THE MATERIALS AND METHODS ALLOWED FOR THE ADJUSTMENT OR RELOCATION OF THEIR FACILITIES, IF NECESSARY.
- WHERE TRENCH BACKFILL IS REQUIRED, THE MATERIAL USED SHALL BE COMPACTED AS SPECIFIED IN ARTICLE 550.07 OF THE "STANDARD SPECIFICATIONS" USING METHOD ONE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING LAWN SPRINKLER SYSTEMS PRIOR TO REMOVAL AND/OR EXCAVATION OPERATIONS. ANY DAMAGE TO THE SYSTEM SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. IF A SPRINKLER SYSTEM IS ENCOUNTERED THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY SO THAT THE OWNER CAN BE CONTACTED TO HAVE IT RELOCATED. NO EXTRA COMPENSATION SHALL BE ALLOWED FOR DELAYS CAUSED BY THIS WORK.

STORM & SANITARY SEWER

- THE COST OF MAKING SEWER CONNECTIONS TO EXISTING OR PROPOSED SEWER OR DRAINAGE STRUCTURES SHALL BE INCIDENTAL TO THE COST OF THE SEWER OR STRUCTURE BEING CONSTRUCTED.
- WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE THE SAME. HE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET. HE SHALL BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWERS ARE BUILT AND IN SERVICE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT.
- ALL ABANDONED PIPE AND STRUCTURE INVERTS SHALL BE PLUGGED WITH BRICK AND MORTAR TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE STORM OR SANITARY SEWER ITEMS BEING REMOVED.
- THE CONTRACTOR SHALL DETERMINE WHEN FLAT SLAB TOPS ARE REQUIRED ON MANHOLES. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR THE USE OF FLAT SLAB TOPS.
- TOP OF FRAME ("RIM") ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF EACH STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATIONS OF THE AREAS IN WHICH THEY ARE LOCATED, AS PART OF THE STRUCTURE COST.
- DRAINAGE STRUCTURE FLAT-TOPS AND CONES SHALL BE TURNED SO THAT THE FRAMES ARE CLOSEST TO THE CENTERLINE OF THE ROAD. ALL FLAT-TOPS AND CONES ARE ASSUMED TO BE ECCENTRIC.
- BITUMINOUS PAVEMENT CROSSINGS REMOVED DUE TO STORM SEWER OR CULVERT WORK SHALL NOT BE LEFT IN GRAVEL OVERNIGHT. TEMPORARY BITUMINOUS PATCHING (AT THE CONTRACTOR'S EXPENSE) MAY BE USED IN LIEU OF IMMEDIATE PAVEMENT REPLACEMENT.
- ALL SEWER AND WATER SERVICES CROSSED BY NEW STORM SEWERS SHALL BE PROPERLY LOCATED AND PROTECTED DURING CONSTRUCTION. ANY DAMAGE TO SAID SERVICES NOT CONSIDERED TO BE IN CONFLICT WITH THE PROPOSED STORM SEWER SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.

ROADWAY EXCAVATION

- ALL EMBANKMENTS AND SUB-GRADE SHALL BE COMPACTED TO THE SATISFACTION OF THE ENGINEER PRIOR TO THE PLACEMENT OF GRANULAR SUB-BASE OR EMBANKMENT.
 - ALL EXCESS MATERIAL (BROKEN CONCRETE, SEWER PIPE, WASTE ROADWAY EXCAVATION AND SURPLUS MATERIAL FROM SEWER TRENCHES) SHALL BE LEGALLY DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SELECT DUMP SITES AND OBTAIN PERMISSION AND ALL NECESSARY PERMITS TO USE SUCH DUMP SITES. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN "EARTH EXCAVATION."
 - POROUS GRANULAR EMBANKMENT, SUBGRADE HAS BEEN PROVIDED TO REPLACE SOILS WHICH TEND TO BE UNSTABLE WHEN WET. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. IF UNSUITABLE SOILS ARE ENCOUNTERED THE SOILS SHALL BE REMOVED AND REPLACED WITH PGES. THE REMOVAL AND REPLACEMENT AREA SHALL EXTEND TO 12 INCHES BEYOND THE CURB AND GUTTER AND COME UP AT A 1:1 SLOPE TO EXISTING GROUND SURFACE. THESE LIMITS MAY BE ALTERED BY THE ENGINEER IF FIELD CONDITIONS SO WARRANT. REMOVAL OF THESE UNSUITABLE SOILS SHALL BE PAID FOR AS "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL."
- IN ALL OTHER AREAS, THE REMEDIAL TREATMENT SHALL CONSIST OF DISKING, DRYING AND RECOMPACTING THE SUBGRADE FOR THE FULL WIDTH OF THE EMBANKMENT BASE, IN ACCORDANCE WITH ARTICLE 212.03. IF UNSUITABLE SOIL IS NOT ENCOUNTERED, THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION SHALL BE DUE THE CONTRACTOR.

SIGNING, STRIPING & LANDSCAPING

- WHEN DIRECTED BY THE ENGINEER, SUPPLEMENTAL WATERING SHALL BE APPLIED TO ALL SODDED AREAS PRIOR TO FINAL ACCEPTANCE AT A RATE SPECIFIED BY THE ENGINEER AND IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS" AND SPECIAL PROVISIONS.
- THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR, SHALL BE RESTORED BY THE CONTRACTOR AT HIS EXPENSE, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ALL EXISTING TRAFFIC SIGNS WHICH INTERFERE WITH THE CONTRACTOR'S WORK, SHALL BE REMOVED, A RECORD MADE OF THEIR CONDITION, AND SAFELY STORED AND SAFEGUARDED BY THE CONTRACTOR UNTIL THE ENGINEER DETERMINES THAT THEY BE REINSTALLED IN THE PERMANENT LOCATIONS.
- IMMEDIATELY AFTER EACH SIGN IS REMOVED, A TEMPORARY SIGN OF THE SAME TYPE SHALL BE INSTALLED ON A SIGN SUPPORT APPROVED BY, AND AT A LOCATION DETERMINED BY, THE ENGINEER (UNLESS THE SIGN IS DESIGNATED FOR REMOVAL). THESE SIGNS SHALL BE MAINTAINED STRAIGHT AND CLEAN UNTIL THE PERMANENT SIGNS ARE REINSTALLED.
- ANY SIGN WHICH IS DAMAGED DURING THE TIME IT IS STORED SHALL BE REPAIRED OR REPLACED IN KIND BY THE CONTRACTOR AT HIS OWN EXPENSE PRIOR TO PERMANENT REINSTALLATION.
- ALL UNUSED SIGNS AND POSTS SHALL BE RETURNED TO EITHER THE VILLAGE OF BLOOMINGDALE'S PUBLIC WORKS FACILITY OR THE VILLAGE OF ROSELLE'S PUBLIC WORKS FACILITY, DEPENDING ON THE SIGN'S OWNER.
- THE COST OF STORING AND SAFEGUARDING THE PERMANENT SIGNS AND POSTS, OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING THE TEMPORARY SIGNS, AND REINSTALLING THE PERMANENT SIGNS SHALL BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR "TRAFFIC CONTROL AND PROTECTION". NEW SIGN SUPPORTS SHALL BE USED FOR REINSTALLED SIGNS. THE SUPPORTS SHALL BE PAID FOR AS "TELESCOPING STEEL SIGN SUPPORT."

MISCELLANEOUS

- DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
- THE CONTRACTOR SHALL LIMIT ANY DROP-OFF BETWEEN LANES TO 2 INCHES DURING ANY OVERNIGHT PERIOD.
- SITE OBJECTS: REMOVAL OF MISCELLANEOUS PARKWAY IMPROVEMENTS INCLUDING, BUT NOT LIMITED TO, BLOCK RETAINING WALLS, CONCRETE RETAINING WALLS, LANDSCAPE TIMBERS, FENCES, FENCE POSTS, PLANTERS, VEGETATION, BRICK OR BRICKPAVER WALKWAYS WITHIN R.O.W. LIMITS SHALL BE INCIDENTAL TO THE PAY ITEM FOR "EARTH EXCAVATION."
- PAY ITEMS IN THE SUMMARY OF QUANTITIES HAVE BEEN ESTIMATED. IF, IN THE ENGINEER'S OPINION, THE WORK IS NOT REQUIRED, THE ITEM WILL BE DEDUCTED FROM THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL ADHERE TO IDOT STANDARD DRAWING NO. 701801-03 WHEN CLOSING ANY SIDEWALK TO PERMIT CONSTRUCTION OF THE IMPROVEMENTS.
- ALL DRIVEWAY APRONS SHALL BE REPLACED WITH MATERIAL OF THE SAME KIND AS THE EXISTING APRON.
- ALL UNBALLASTED TYPE I AND II BARRICADES SHALL HAVE TWO SANDBAGS ON THE BOTTOM RAILS.
- UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER, ALL EXISTING ACCESS POINTS SHALL BE MAINTAINED AT ALL TIMES BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOT CROSS COMPLETED BASE COURSE, OR EXISTING PAVEMENT NOT SCHEDULED TO BE REMOVED, WITH CONSTRUCTION EQUIPMENT WHICH MAY DAMAGE THE PAVEMENT.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
GENERAL NOTES
SPRINGFIELD ROAD
 DRAWN BY: DJK
 CHECKED BY: JRV
 DATE: 04/04/06

| FAA/ ALE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------------|----------------|-----------------------------|--------------|-----------|
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 3 |
| SUMMARY OF QUANTITIES | | | | |
| FED. ROAD DISTRICT NO. 7 | ILLINOIS | FED. AID PROJECT F-002(068) | | |
| CONTRACT NO. 83852 | | | | |

| CODED PAY ITEM NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION TYPE CODE | | |
|--------------------|---|--------|----------------|------------------------|---------|---------|
| | | | | I000-1A | Y030-1E | Y031-1F |
| 20100110 | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | UNIT | 17 | 17 | | |
| 20101000 | TEMPORARY FENCE | FOOT | 780 | 780 | | |
| 20101100 | TREE TRUNK PROTECTION | EACH | 26 | 26 | | |
| 20101200 | TREE ROOT PRUNING | EACH | 26 | 26 | | |
| 20200100 | EARTH EXCAVATION | CU YD | 383 | 383 | | |
| 20201200 | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | CU YD | 50 | 50 | | |
| 20400800 | FURNISHED EXCAVATION | CU YD | 140 | 140 | | |
| 20700420 | POROUS GRANULAR EMBANKMENT, SUBGRADE | CU YD | 50 | 50 | | |
| 20800150 | TRENCH BACKFILL | CU YD | 74 | 74 | | |
| 21001000 | GEOTECHNICAL FABRIC FOR GROUND STABILIZATION | SQ YD | 150 | 150 | | |
| 21101615 | TOPSOIL FURNISH AND PLACE, 4" | SQ YD | 1588 | 1588 | | |
| 21300010 | EXPLORATION TRENCH, SPECIAL | FOOT | 25 | 25 | | |
| 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 30 | 30 | | |
| 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 30 | 30 | | |
| 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 30 | 30 | | |
| 25200110 | SODDING, SALT TOLERANT | SQ YD | 1588 | 1588 | | |
| 25200200 | SUPPLEMENTAL WATERING | UNIT | 10 | 10 | | |
| 28000250 | TEMPORARY EROSION CONTROL SEEDING | POUND | 33 | 33 | | |
| 28000510 | INLET FILTERS | EACH | 11 | 11 | | |
| 31101200 | SUB-BASE GRANULAR MATERIAL, TYPE B 4" | SQ YD | 1234 | 1234 | | |
| 40600100 | BITUMINOUS MATERIALS (PRIME COAT) | GALLON | 1276 | 1276 | | |
| 40600300 | AGGREGATE (PRIME COAT) | TON | 26 | 26 | | |
| 40600980 | BITUMINOUS SURFACE REMOVAL - BUTT JOINT | SQ YD | 156 | 156 | | |
| 42300600 | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 10 INCH | SQ YD | 168 | 168 | | |
| 42400430 | PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL | SQ FT | 954 | 954 | | |
| 42400800 | DETECTABLE WARNING | SQ FT | 250 | 250 | | |
| 44000025 | BITUMINOUS SURFACE REMOVAL, SPECIAL | SQ YD | 4128 | 4128 | | |
| 44000100 | PAVEMENT REMOVAL | SQ YD | 314 | 314 | | |
| 44000200 | DRIVEWAY PAVEMENT REMOVAL | SQ YD | 282 | 282 | | |
| 44000300 | CURB REMOVAL | FOOT | 78 | 78 | | |
| 44000500 | COMBINATION CURB AND GUTTER REMOVAL | FOOT | 1976 | 1976 | | |
| 44000600 | SIDEWALK REMOVAL | SQ FT | 1232 | 1232 | | |
| 44003100 | MEDIAN REMOVAL | SQ FT | 1579 | 1579 | | |
| 44300100 | AREA REFLECTIVE CRACK CONTROL TREATMENT | SQ YD | 4128 | 4128 | | |
| 48301000 | PROTECTIVE COAT | SQ YD | 744 | 744 | | |
| 550A0050 | STORM SEWERS, CLASS A, TYPE 1 12" | FOOT | 87 | 87 | | |
| 550A0340 | STORM SEWERS, CLASS A, TYPE 2 12" | FOOT | 44 | 44 | | |
| 55100500 | STORM SEWER REMOVAL 12" | FOOT | 29 | 29 | | |
| 56106500 | ADJUSTING WATER MAIN 10" | FOOT | 60 | 60 | | |
| 60109510 | PIPE UNDERDRAINS, FABRIC LINED TRENCH 4" | FOOT | 50 | 50 | | |
| 60200105 | CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID | EACH | 2 | 2 | | |
| 60200305 | CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 3 FRAME AND GRATE | EACH | 2 | 2 | | |
| 60200805 | CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 8 GRATE | EACH | 1 | 1 | | |
| 60206905 | CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID | EACH | 1 | 1 | | |
| 60207605 | CATCH BASINS, TYPE C, TYPE 8 GRATE | EACH | 1 | 1 | | |
| 60218400 | MANHOLES, TYPE A, 4'- DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH | 1 | 1 | | |
| 60236200 | INLETS, TYPE A, TYPE 8 GRATE | EACH | 3 | 3 | | |
| 60250200 | CATCH BASINS TO BE ADJUSTED | EACH | 1 | 1 | | |
| 60255500 | MANHOLES TO BE ADJUSTED | EACH | 1 | 1 | | |
| 60257900 | MANHOLES TO BE RECONSTRUCTED | EACH | 1 | 1 | | |
| 60285700 | VALVE VAULTS TO BE ADJUSTED | EACH | 1 | 1 | | |
| 60286100 | VALVE VAULTS TO BE RECONSTRUCTED | EACH | 1 | 1 | | |
| 60500060 | REMOVING INLETS | EACH | 2 | 2 | | |
| 60600605 | CONCRETE CURB, TYPE B | FOOT | 63 | 63 | | |
| 60603800 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 | FOOT | 686 | 686 | | |
| 60604200 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL) | FOOT | 244 | 244 | | |
| 60604400 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 | FOOT | 616 | 616 | | |
| 60604800 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (SPECIAL) | FOOT | 108 | 108 | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | | |
| 70101700 | TRAFFIC CONTROL AND PROTECTION | L SUM | 1 | 1 | | |
| 70300100 | SHORT-TERM PAVEMENT MARKING | FOOT | 200 | 200 | | |
| 70300610 | TEMPORARY PAINT PAVEMENT MARKING, LETTERS AND SYMBOLS | SQ FT | 327 | 327 | | |
| 70300625 | TEMPORARY PAINT PAVEMENT MARKING LINE 4" | FOOT | 4882 | 4882 | | |
| 70300635 | TEMPORARY PAINT PAVEMENT MARKING LINE 6" | FOOT | 1916 | 1916 | | |
| 70300640 | TEMPORARY PAINT PAVEMENT MARKING LINE 8" | FOOT | 277 | 277 | | |
| 70300645 | TEMPORARY PAINT PAVEMENT MARKING LINE 12" | FOOT | 288 | 288 | | |
| 70300660 | TEMPORARY PAINT PAVEMENT MARKING LINE 24" | FOOT | 187 | 187 | | |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 392 | 392 | | |
| 72000100 | SIGN PANEL - TYPE 1 | SQ FT | 18 | | | 18 |
| 72000200 | SIGN PANEL - TYPE 2 | SQ FT | 44 | 19 | | 25 |
| 72400100 | REMOVE SIGN PANEL ASSEMBLY - TYPE A | EACH | 2 | 2 | | |
| 72800100 | TELESCOPING STEEL SIGN SUPPORT | FOOT | 58 | 58 | | |
| 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 256 | 256 | | |
| 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 2762 | 2762 | | |
| 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 1612 | 1612 | | |
| 78000500 | THERMOPLASTIC PAVEMENT MARKING - LINE 8" | FOOT | 277 | 277 | | |

| CODED PAY ITEM NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION TYPE CODE | | |
|--------------------|---|-------|----------------|------------------------|---------|---------|
| | | | | I000-1A | Y030-1E | Y031-1F |
| * 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 288 | 288 | | |
| * 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 135 | 135 | | |
| 78300100 | PAVEMENT MARKING REMOVAL | SQ FT | 400 | 400 | | |
| * 81000600 | CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL | FOOT | 784 | | | 784 |
| * 81000700 | CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL | FOOT | 84 | | | 84 |
| * 81001100 | CONDUIT IN TRENCH, 5" DIA., GALVANIZED STEEL | FOOT | 16 | | | 16 |
| * 81018500 | CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL | FOOT | 428 | | | 428 |
| * 81018600 | CONDUIT PUSHED, 2 1/2" DIA., GALVANIZED STEEL | FOOT | 113 | | | 113 |
| * 81018900 | CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL | FOOT | 474 | | | 474 |
| * 81400100 | HANDHOLE | EACH | 4 | | | 4 |
| * 81400200 | HEAVY-DUTY HANDHOLE | EACH | 4 | | | 4 |
| * 81400300 | DOUBLE HANDHOLE | EACH | 2 | | | 2 |
| * 81500200 | TRENCH AND BACKFILL FOR ELECTRICAL WORK | FOOT | 876 | | | 876 |
| * 84200500 | REMOVAL OF EXISTING LIGHTING UNIT, SALVAGE | EACH | 2 | | 2 | |
| * 84200700 | LIGHTING FOUNDATION REMOVAL | EACH | 1 | | 1 | |
| * 85000200 | MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION | EACH | 1 | | | 1 |
| * 85700205 | FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL | EACH | 1 | | | 1 |
| * 86400100 | TRANSCIEVER-FIBER OPTIC | EACH | 1 | | | 1 |
| * 87301215 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C | FOOT | 660 | | | 660 |
| * 87301225 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C | FOOT | 1730 | | | 1730 |
| * 87301245 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | FOOT | 459 | | | 459 |
| * 87301255 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | FOOT | 2398 | | | 2398 |
| * 87301305 | ELECTRIC CABLE IN CONDUIT, LEAD-IN NO. 14 1PAIR | FOOT | 2368 | | | 2368 |
| * 87301805 | ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C | FOOT | 61 | | | 61 |
| * 87502500 | TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT. | EACH | 4 | | | 4 |
| * 87700150 | STEEL MAST ARM ASSEMBLY AND POLE, 22 FT. | EACH | 1 | | | 1 |
| * 87700170 | STEEL MAST ARM ASSEMBLY AND POLE, 26 FT. | EACH | 1 | | | 1 |
| * 87700200 | STEEL MAST ARM ASSEMBLY AND POLE, 32 FT. | EACH | 1 | | | 1 |
| * 87700210 | STEEL MAST ARM ASSEMBLY AND POLE, 34 FT. | EACH | 1 | | | 1 |
| * 87800100 | CONCRETE FOUNDATION, TYPE A | FOOT | 16 | | | 16 |
| * 87800200 | CONCRETE FOUNDATION, TYPE D | FOOT | 4 | | | 4 |
| * 87800400 | CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER | FOOT | 60 | | | 60 |
| * 87900200 | DRILL EXISTING HANDHOLE | EACH | 1 | | | 1 |
| * 88200210 | TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM | EACH | 8 | | | 8 |
| * 88500100 | INDUCTIVE LOOP DETECTOR | EACH | 11 | | | 11 |
| * 88600100 | DETECTOR LOOP, TYPE 1 | FOOT | 1079 | | | 1079 |
| * 88700200 | LIGHT DETECTOR | EACH | 2 | | | 2 |
| * 88700300 | LIGHT DETECTOR AMPLIFIER | EACH | 1 | | | 1 |
| * 88800100 | PEDESTRIAN PUSH-BUTTON | EACH | 4 | | | 4 |
| * 89000100 | TEMPORARY TRAFFIC SIGNAL INSTALLATION | EACH | 1 | | | 1 |
| * 89502300 | REMOVE ELECTRIC CABLE FROM CONDUIT | FOOT | 2223 | | | 2223 |
| * 89502375 | REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT | EACH | 1 | | | 1 |
| * 89502380 | REMOVE EXISTING HANDHOLE | EACH | 9 | | | 9 |
| * 89502385 | REMOVE EXISTING CONCRETE FOUNDATION | EACH | 9 | | | 9 |
| * Z0004600 | BITUMINOUS DRIVEWAY PAVEMENT 9" | SQ YD | 19 | 19 | | |
| * XX002856 | RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM | L SUM | 1 | | | 1 |
| * XX004801 | BITUMINOUS BIKE PATH REMOVAL | SQ YD | 39 | 39 | | |
| * XX004904 | BITUMINOUS DRIVEWAY PAVEMENT 10" | SQ YD | 90 | 90 | | |
| * XX004913 | REMOVE FIBER OPTIC CABLE FROM CONDUIT | FOOT | 1778 | | | 1778 |
| * XX005378 | CLASS D PATCHES, SUPERPAVE, TYPE 3, 7 INCHES | SQ YD | 23 | 23 | | |
| * X0321556 | SANITARY MANHOLES TO BE ADJUSTED | EACH | 3 | 3 | | |
| * X0322256 | TEMPORARY INFORMATION SIGNING | SQ FT | 194 | 194 | | |
| * X0322925 | ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C | FOOT | 2330 | | | 2330 |
| * X0323381 | STORM SEWER, (WATER MAIN REQUIREMENTS) TYPE 1, 12 INCH | FOOT | 185 | 185 | | |
| * X0323863 | STORM SEWER, (WATER MAIN REQUIREMENTS) TYPE 2, 12 INCH | FOOT | 76 | 76 | | |
| * X0323426 | SEDIMENT CONTROL, DRAINAGE STRUCTURE INLET FILTER CLEANING | EACH | 22 | 22 | | |
| * X3550500 | BITUMINOUS BASE COURSE SUPERPAVE 8" | SQ YD | 613 | 613 | | |
| * X4021000 | TEMPORARY ACCESS (PRIVATE ENTRANCE) | EACH | 1 | 1 | | |
| * X4022000 | TEMPORARY ACCESS (COMMERCIAL ENTRANCE) | EACH | 5 | 5 | | |
| * X4066424 | BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N50 | TON | 365 | 365 | | |
| * X4066614 | BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N50 | TON | 1063 | 1063 | | |
| * X7030104 | WET TEMPORARY PAVEMENT MARKING TAPE, TYPE III 4" | FOOT | 917 | 917 | | |
| * X7030106 | WET TEMPORARY PAVEMENT MARKING TAPE, TYPE III 6" | FOOT | 53 | 53 | | |
| * X8050015 | SERVICE INSTALLATION, POLE MOUNTED | EACH | 1 | | | 1 |
| * X8710020 | FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F & SM12F | FOOT | 2354 | | | 2354 |
| * X8730027 | ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C | FOOT | 767 | | | 767 |
| * X8730250 | ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED | FOOT | 368 | | | 368 |
| * X8800020 | SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, MAST ARM MOUNTED | EACH | 2 | | | 2 |
| * X8800040 | SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 2 | | | 2 |
| * X8800045 | SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, MAST ARM MOUNTED | EACH | 6 | | | 6 |
| * X8800070 | SIGNAL HEAD, L.E.D., 2-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 2 | | | 2 |
| * X8810620 | PEDESTRIAN SIGNAL HEAD, L.E.D., 2-FACE, BRACKET MOUNTED | EACH | 4 | | | 4 |
| * K1005481 | SHREDDED BARK MULCH 3" | SQ YD | 77 | 77 | | |

* INDICATES SPECIALTY ITEMS

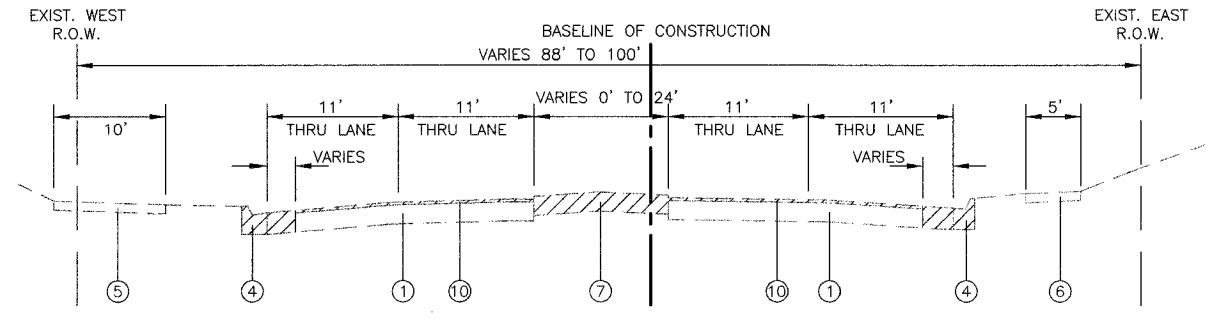
ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
 SPRINGFIELD ROAD

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

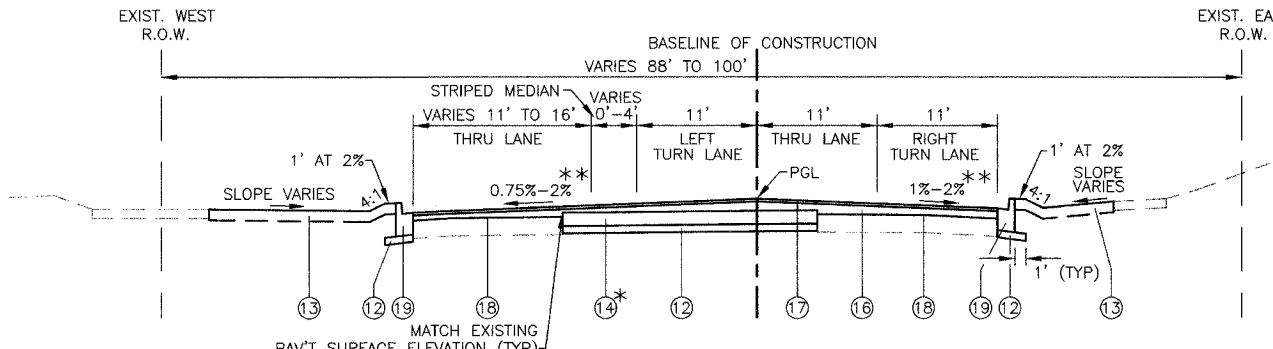
DRAWN BY: KRK
 CHECKED BY: JRV
 DATE: 04/04/06

| | | | |
|--------------------------|----------|------------------------------|-----------|
| SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 05-00050-00-CH | DUPAGE | 36 | 4 |
| TYPICAL SECTIONS | | | |
| FED. ROAD DISTRICT NO. 7 | ILLINOIS | FED. AID PROJECT F-0021(068) | |

CONTRACT NO. 83852

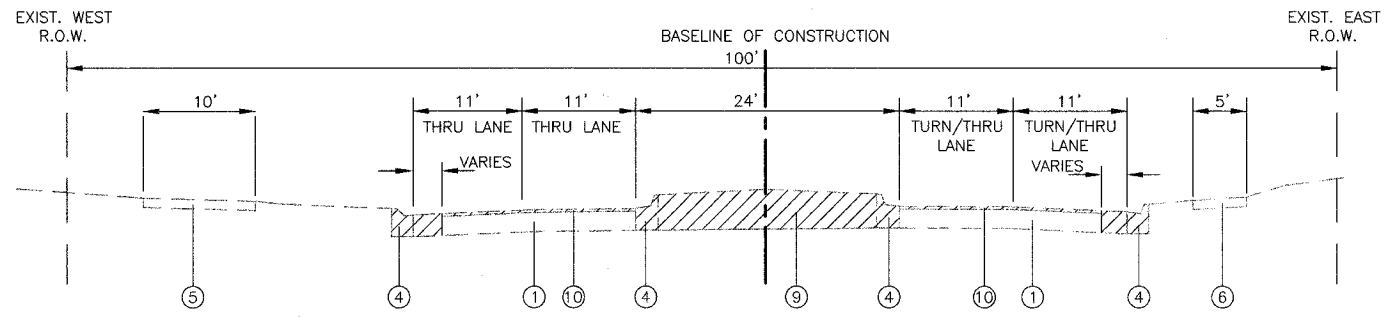


EXISTING TYPICAL SECTION
STA. 24+65.46 TO STA. 27+03

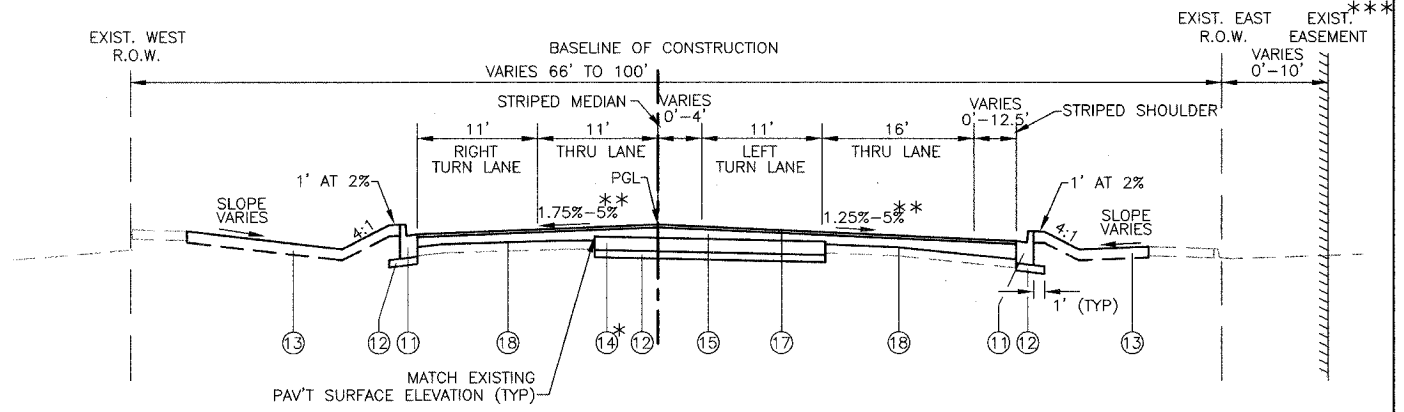


PROPOSED TYPICAL SECTION
STA. 24+65.46 TO STA. 27+94.81

* PROPOSED BIT. BASE COURSE TO BE MILLED AT SAME TIME AS EXISTING PAVEMENT (PAID FOR AS BITUMINOUS SURFACE REMOVAL, SPECIAL)
** SEE SHEET NO. 15 FOR ADDITIONAL INFORMATION



EXISTING TYPICAL SECTION
STA. 27+03 TO STA. 27+50.25



PROPOSED TYPICAL SECTION
STA. 48+52 TO STA. 52+32

* PROPOSED BIT. BASE COURSE TO BE MILLED AT SAME TIME AS EXISTING PAVEMENT (PAID FOR AS BITUMINOUS SURFACE REMOVAL, SPECIAL)
** SEE SHEET NO. 15 FOR ADDITIONAL INFORMATION
*** STA. 50+31.08 TO STA. 52+32

EARTHWORK SCHEDULE
(ASSUME 15% SHRINKAGE)

| ITEM | UNIT | SPRINGFIELD DRIVE | | TOTALS |
|---|------|-------------------|-------------|--------|
| | | (NORTH LEG) | (SOUTH LEG) | |
| EARTH EXCAVATION (SUITABLE MATL.) | C.Y. | 206 | 177 | 383 * |
| EARTH EXCAVATION ADJ. FOR SHRINKAGE | C.Y. | 175 | 150 | 325 |
| EMBANKMENT | C.Y. | 58 | 82 | 140** |
| EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) | C.Y. | +117 | +68 | +185 |

* PLAN QUANTITY
*** PAY ITEM FOR "FURNISHED EXCAVATION" HAS BEEN INCLUDED ON THE ASSUMPTION THAT, DUE TO LIMITED WORKING SPACE, ALL EMBANKMENT MAY HAVE TO BE BROUGHT IN FROM OUTSIDE THE PROJECT LIMITS.

* 21' - STA. 48+73.5 TO STA. 49+46
0' - STA. 48+52 TO STA. 48+73.5 AND STA. 49+46 TO STA. 52+32
** STA. 50+31.08 TO STA. 52+32

BITUMINOUS MIXES

| PAY ITEM | AC TYPE | VOIDS | MAX. RAP |
|---|----------|--------------|----------|
| BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIXTURE "D", N50 | PG 64-22 | 4% @ 50 GYR. | 15% |
| BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N50 | PG 58-22 | 4% @ 50 GYR. | 25% |
| BITUMINOUS BASE COURSE, SUPERPAVE, 8" | PG 58-22 | 2% @ 50 GYR. | 50% |
| BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIXTURE "C", N50 (FOR BITUMINOUS DRIVEWAY PAVEMENT 9" AND 10") | PG 64-22 | 4% @ 50 GYR. | 15% |
| CLASS D PATCH, SUPERPAVE, 7" (IL-19mm) | PG 64-22 | 4% @ 70 GYR. | 15% |

NOTE: THE UNIT WEIGHT USED TO CALCULATE ALL BITUMINOUS SURFACE MIXTURES IS 112 LB/SY-IN.

LEGEND

- ① EXISTING BITUMINOUS CONCRETE PAVEMENT, 12" & VARIES
- ② EXISTING BITUMINOUS CONCRETE PAVEMENT, 7" & VARIES
- ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.12
- ④ EXISTING COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.18
- ⑤ EXISTING BITUMINOUS BIKE PATH
- ⑥ EXISTING CONCRETE SIDEWALK
- ⑦ EXISTING CORRUGATED CONCRETE MEDIAN
- ⑧ NOT USED
- ⑨ EXISTING LANDSCAPED MEDIAN
- ⑩ BITUMINOUS SURFACE REMOVAL, SPECIAL, 1 1/2"
- ⑪ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (9" GUTTER FLAG)
- ⑫ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B 4"
- ⑬ PROPOSED TOPSOIL FURNISH AND PLACE, 4" AND SODDING, SALT TOLERANT
- ⑭ PROPOSED BITUMINOUS BASE COURSE, SUPERPAVE, 8"
- ⑮ PROPOSED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N50 3.5" MIN & VARIES
- ⑯ PROPOSED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N50 2.25" MIN & VARIES
- ⑰ PROPOSED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N50 1.5"
- ⑱ AREA REFLECTIVE CRACK CONTROL TREATMENT
- ⑲ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (9" GUTTER FLAG)



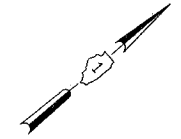
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
SPRINGFIELD ROAD

NOT TO SCALE
DRAWN BY: DJK
CHECKED BY: JRV
DATE: 04/04/06

| FILE NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------------|----------------|------------------------------|--------------|-----------|
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 5 |
| ALIGNMENT & BENCHMARKS | | | | |
| FED. ROAD DISTRICT NO. 7 | ILLINOIS | FED. AID PROJECT F-0021(068) | | |

CONTRACT NO. 83852

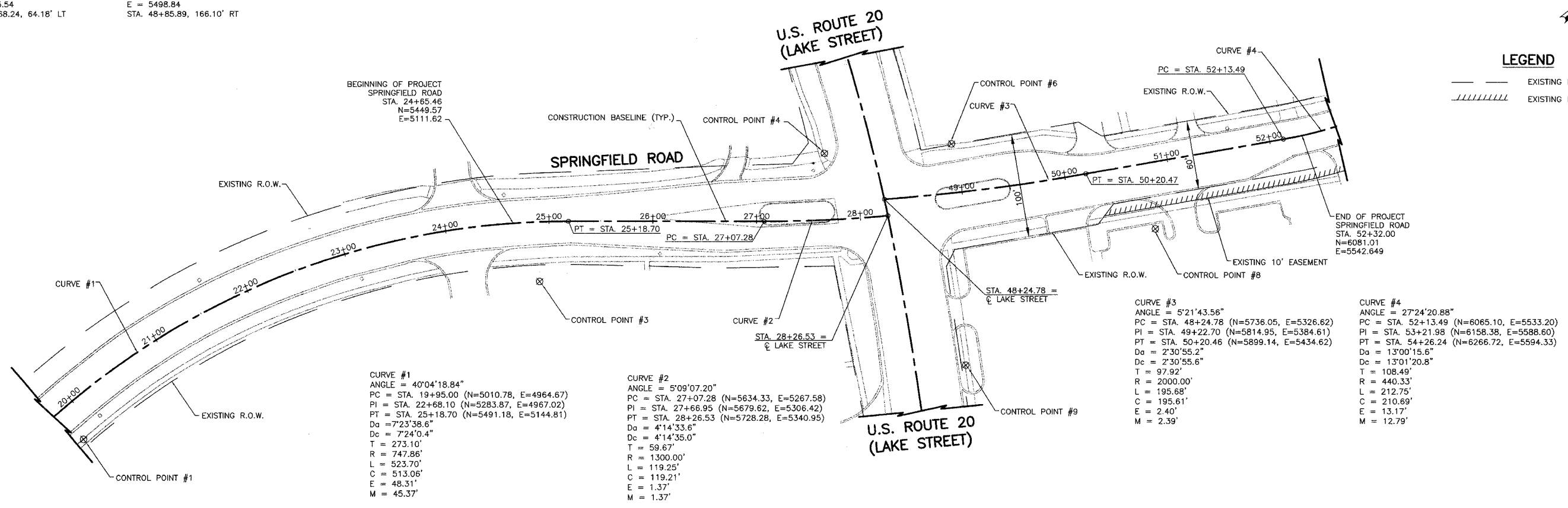


LEGEND

- — — — — EXISTING RIGHT-OF-WAY LINE
- /////// EXISTING EASEMENT LINE

- CONTROL POINT #1**
CUT IN CONC. SIDEWALK
N = 5000.00
E = 5000.00
STA. 19+84.22, 35.33' RT
- CONTROL POINT #3**
SET IRON ROD IN BERM
N = 5432.36
E = 5170.87
STA. 24+88.70, 57.52' RT
- CONTROL POINT #4**
CUT ON HANDHOLE
N = 5720.97
E = 5255.54
STA. 27+68.24, 64.18' LT
- CONTROL POINT #6**
CUT IN CONC. SIDEWALK
N = 5819.70
E = 5328.40
STA. 48+94.90, 46.91' LT
- CONTROL POINT #8**
CUT IN CONC. SIDEWALK
N = 5915.26
E = 5518.48
STA. 50+77.15, 63.87' RT
- CONTROL POINT #9**
CUT IN CONC. SIDEWALK
N = 5691.61
E = 5498.84
STA. 48+85.89, 166.10' RT

- DPC BENCH MARK #117001**
BRASS DISK SET IN CONCRETE
SW CORNER OF LAWRENCE AND GARY AVENUE
ELEV = 798.86
- TEMPORARY BENCH MARK #12**
SE FLANGE (NOT BOLT) OF FIRE HYDRANT
SW CORNER OF LAKE STREET AND SPRINGFIELD DRIVE
ELEV = 787.48



CURVE #1
ANGLE = 40°04'18.84"
PC = STA. 19+95.00 (N=5010.78, E=4964.67)
PI = STA. 22+68.10 (N=5283.87, E=4967.02)
PT = STA. 25+18.70 (N=5491.18, E=5144.81)
Da = 7°23'38.6"
Dc = 7°24'0.4"
T = 273.10'
R = 747.86'
L = 523.70'
C = 513.06'
E = 48.31'
M = 45.37'

CURVE #2
ANGLE = 5°09'07.20"
PC = STA. 27+07.28 (N=5634.33, E=5267.58)
PI = STA. 27+66.95 (N=5679.62, E=5306.42)
PT = STA. 28+26.53 (N=5728.28, E=5340.95)
Da = 4°14'33.6"
Dc = 4°14'35.0"
T = 59.67'
R = 1300.00'
L = 119.25'
C = 119.21'
E = 1.37'
M = 1.37'

CURVE #3
ANGLE = 5°21'43.56"
PC = STA. 48+24.78 (N=5736.05, E=5326.62)
PI = STA. 49+22.70 (N=5814.95, E=5384.61)
PT = STA. 50+20.46 (N=5899.14, E=5434.62)
Da = 2°30'55.2"
Dc = 2°30'55.6"
T = 97.92'
R = 2000.00'
L = 195.68'
C = 195.61'
E = 2.40'
M = 2.39'

CURVE #4
ANGLE = 27°24'20.88"
PC = STA. 52+13.49 (N=6065.10, E=5533.20)
PI = STA. 53+21.98 (N=6158.38, E=5588.60)
PT = STA. 54+26.24 (N=6266.72, E=5594.33)
Da = 13°00'15.6"
Dc = 13°01'20.8"
T = 108.49'
R = 440.33'
L = 212.75'
C = 210.69'
E = 13.17'
M = 12.79'





| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
ALIGNMENT & BENCHMARKS
SPRINGFIELD ROAD

SCALE IN FEET

DRAWN BY: KRK
CHECKED BY: JRV
DATE: 04/04/06

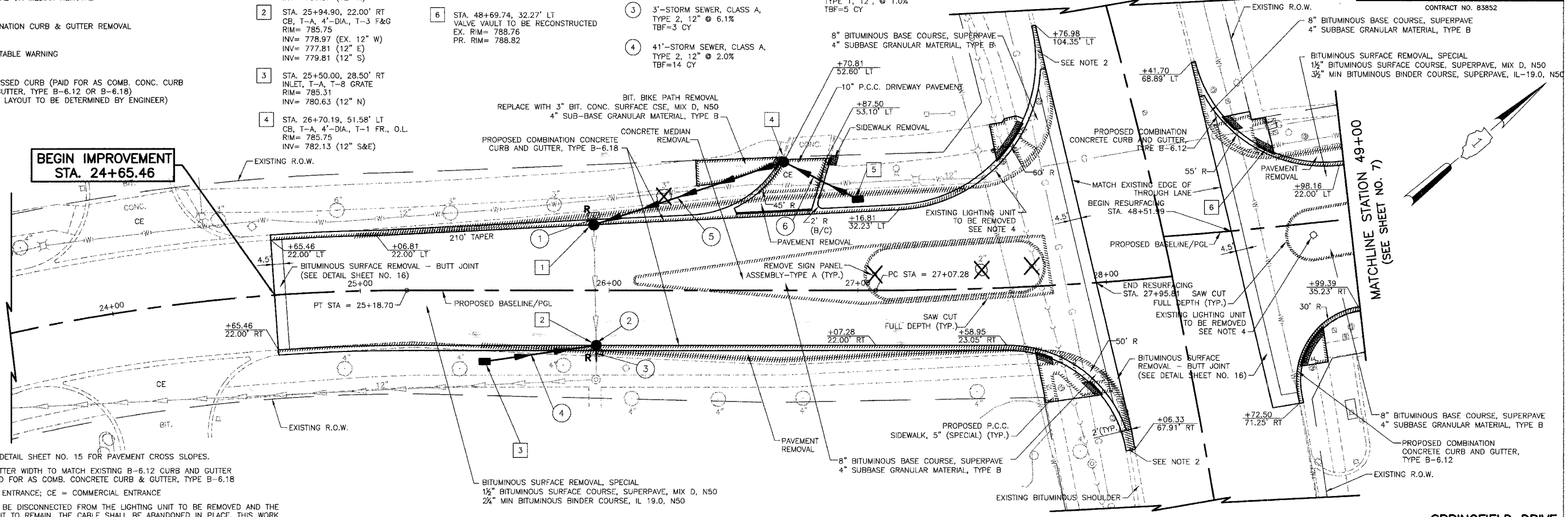
LEGEND

-  PAVEMENT REMOVAL, DRIVEWAY PAVEMENT REMOVAL, SIDEWALK REMOVAL OR MEDIAN REMOVAL
-  COMBINATION CURB & GUTTER REMOVAL
-  DETECTABLE WARNING
-  DEPRESSED CURB (PAID FOR AS COMB. CONC. CURB AND GUTTER, TYPE B-6.12 OR B-6.18) (FINAL LAYOUT TO BE DETERMINED BY ENGINEER)

- 1 STA. 25+93.89, 26.13' LT
CE, T-A, 4'-DIA., T-3 F&G
RIM= 785.67
INV= 781.37 (EX. 12" E)
INV= 781.37 (12" N)
- 2 STA. 25+94.90, 22.00' RT
CB, T-A, 4'-DIA., T-3 F&G
RIM= 785.75
INV= 778.97 (EX. 12" W)
INV= 777.81 (12" E)
INV= 779.81 (12" S)
- 3 STA. 25+50.00, 28.50' RT
INLET, T-A, T-8 GRATE
RIM= 785.31
INV= 780.63 (12" N)
- 4 STA. 26+70.19, 51.58' LT
CB, T-A, 4'-DIA., T-1 FR., O.L.
RIM= 785.75
INV= 782.13 (12" S&E)
- 5 STA. 27+00.00, 36.60' LT
INLET, T-A, T-8 GRATE
RIM= 786.02
INV= 782.45 (12" W)
- 6 STA. 48+69.74, 32.27' LT
VALVE VAULT TO BE RECONSTRUCTED
EX. RIM= 788.76
PR. RIM= 788.82
- 1 3'-STORM SEWER REMOVAL, 12"
- 2 5'-STORM SEWER REMOVAL, 12"
- 3 3'-STORM SEWER, CLASS A, TYPE 2, 12" @ 6.1% TBF=3 CY
- 4 41'-STORM SEWER, CLASS A, TYPE 2, 12" @ 2.0% TBF=14 CY
- 5 76'-STORM SEWER, WATER MAIN QUALITY, TYPE 2, 12", @ 1.0% TBF=18 CY
- 6 32'-STORM SEWER, WATERMAIN QUALITY, TYPE 1, 12", @ 1.0% TBF=5 CY

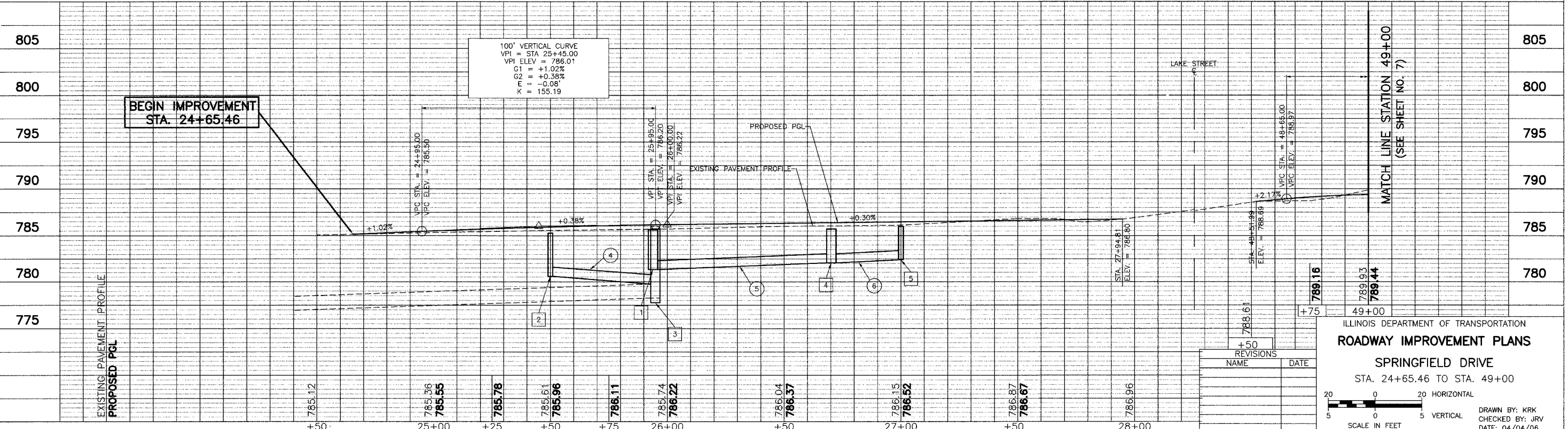
| FAIR | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|----------------------------|----------------|------------------------------|--------------|-----------|
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 6 |
| ROADWAY IMPROVEMENTS PLANS | | | | |
| FED. ROAD DISTRICT NO. 7 | ILLINOIS | FED. AID PROJECT F-0021(068) | | |

U.S. ROUTE 20 (LAKE STREET)



- NOTES:
- SEE ROADWAY DETAIL SHEET NO. 15 FOR PAVEMENT CROSS SLOPES.
 - TRANSITION GUTTER WIDTH TO MATCH EXISTING B-6.12 CURB AND GUTTER OVER 5 FT - PAID FOR AS COMB. CONCRETE CURB & GUTTER, TYPE B-6.18
 - PE = PRIVATE ENTRANCE; CE = COMMERCIAL ENTRANCE
 - CABLES SHALL BE DISCONNECTED FROM THE LIGHTING UNIT TO BE REMOVED AND THE NEXT LIGHTING UNIT TO REMAIN. THE CABLE SHALL BE ABANDONED IN PLACE. THIS WORK SHALL BE INCLUDED IN THE COST OF "REMOVAL OF EXISTING LIGHTING UNIT, SALVAGE."

SPRINGFIELD DRIVE



ILLINOIS DEPARTMENT OF TRANSPORTATION
ROADWAY IMPROVEMENT PLANS
 SPRINGFIELD DRIVE
 STA. 24+65.46 TO STA. 49+00

SCALE IN FEET
 20 0 20 HORIZONTAL
 5 0 5 VERTICAL

DRAWN BY: KRK
 CHECKED BY: JRV
 DATE: 04/04/06

| | | | | |
|---------------------------|----------------|----------|------------------------------|-----------|
| FILE NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 7 |
| ROADWAY IMPROVEMENT PLANS | | | | |
| FED. ROAD DISTRICT NO. 7 | | ILLINOIS | FED. AID PROJECT F-0021(098) | |
| CONTRACT NO. 83852 | | | | |

LEGEND

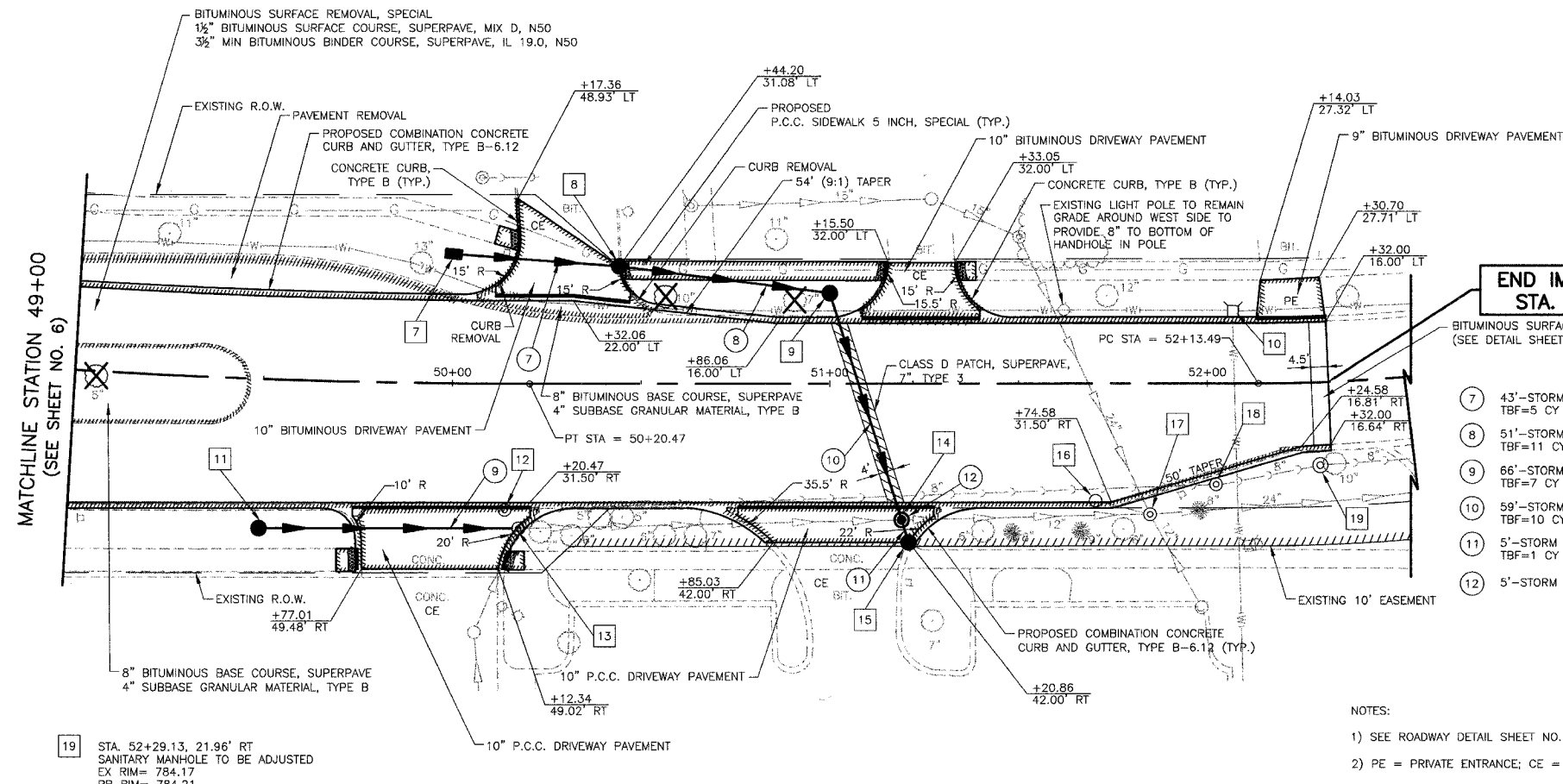
PAVEMENT REMOVAL, DRIVEWAY PAVEMENT REMOVAL, BITUMINOUS DRIVEWAY REMOVAL OR MEDIAN REMOVAL

COMBINATION CURB & GUTTER REMOVAL

DETECTABLE WARNING (FINAL LAYOUT TO BE DETERMINED BY ENGINEER)

DEPRESSED CURB (PAID FOR AS COMB. CONC. CURB AND GUTTER, TYPE B-6.12 OR B-6.18) (FINAL LAYOUT TO BE DETERMINED BY ENGINEER)

- 7 STA. 50+00.00, 34.22' LT INLET, T-A, T-B GRATE
RIM= 787.08
INV= 783.39 (12" N)
- 8 STA. 50+44.20, 31.08' LT CB, T-A, 4'-DIA., T-1 FR., O.L.
RIM= 785.88
INV= 783.17 (12" S)
INV= 782.40 (12" N)
- 9 STA. 51+00.00, 23.90' LT CB, T-A, 4'-DIA., T-8 GRATE
RIM= 784.85
INV= 782.14 (12" S)
INV= 782.04 (12" E)
- 10 STA. 52+06.74, 19.15' LT VALVE VAULT TO BE ADJUSTED
EX RIM= 784.36
PR RIM= 784.61
- 11 STA. 49+50.00, 39.50' RT CB, T-C, T-8 GRATE
RIM= 787.84
INV= 784.42 (12" N)
- 12 STA. 50+13.90, 33.50' RT SANITARY MANHOLE TO BE ADJUSTED
EX RIM= 786.74
PR RIM= 786.88
- 13 STA. 50+17.48, 38.33' RT MANHOLE TO BE ADJUSTED
EX RIM= 786.73
PR RIM= 786.80
INV= 782.18 (EX 12" N&E)
INV= 784.09 (12" S)
- 14 STA. 51+18.96, 36.00' RT MH, T-A, 4'-DIA., T-1 FR., C.L.
RIM= 784.46
INV= 781.75 (12" E&W)
INV= 781.65 (EX 12" S)
INV= 781.63 (EX 12" N)
- 15 STA. 51+20.86, 42.00' RT CB, T-C, T-1 FR., O.L.
RIM= 784.21
INV= 781.78 (12" W)
- 16 STA. 51+70.38, 31.10' RT CATCH BASIN TO BE ADJUSTED
EX RIM= 783.46
PR RIM= 784.03
- 17 STA. 51+84.81, 34.73' RT MANHOLE TO BE RECONSTRUCTED
EX RIM= 784.82
PR RIM= 784.57
- 18 STA. 52+02.28, 26.74' RT SANITARY MANHOLE TO BE ADJUSTED
EX RIM= 784.13
PR RIM= 784.58

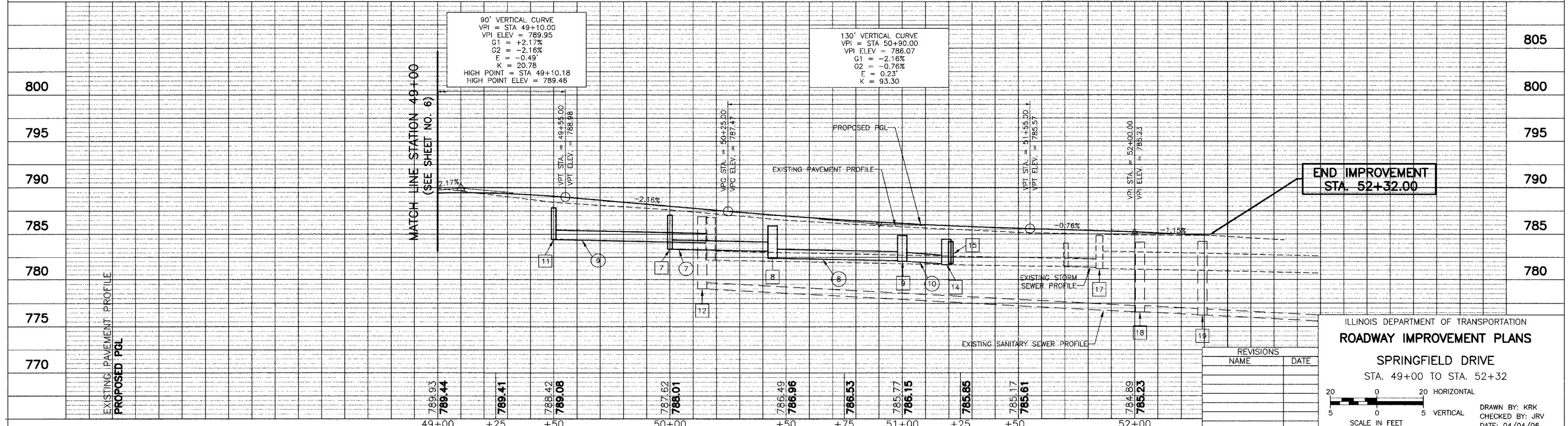


**END IMPROVEMENT
STA. 52+32.00**

- 7 43'-STORM SEWER, WATERMAIN QUALITY, TYPE 1, 12" @ 0.5% TBF=5 CY
- 8 51'-STORM SEWER, WATERMAIN QUALITY, TYPE 1, 12" @ 0.5% TBF=11 CY
- 9 66'-STORM SEWER, CLASS A, TYPE 1, 12" @ 0.5% TBF=7 CY
- 10 59'-STORM SEWER, WATERMAIN QUALITY, TYPE 1, 12" @ 0.5% TBF=10 CY
- 11 5'-STORM SEWER, CLASS A, TYPE 1, 12", @ 0.5% TBF=1 CY
- 12 5'-STORM SEWER REMOVAL, 12"

NOTES:
1) SEE ROADWAY DETAIL SHEET NO. 15 FOR PAVEMENT CROSS SLOPES.
2) PE = PRIVATE ENTRANCE; CE = COMMERCIAL ENTRANCE

SPRINGFIELD DRIVE



**END IMPROVEMENT
STA. 52+32.00**

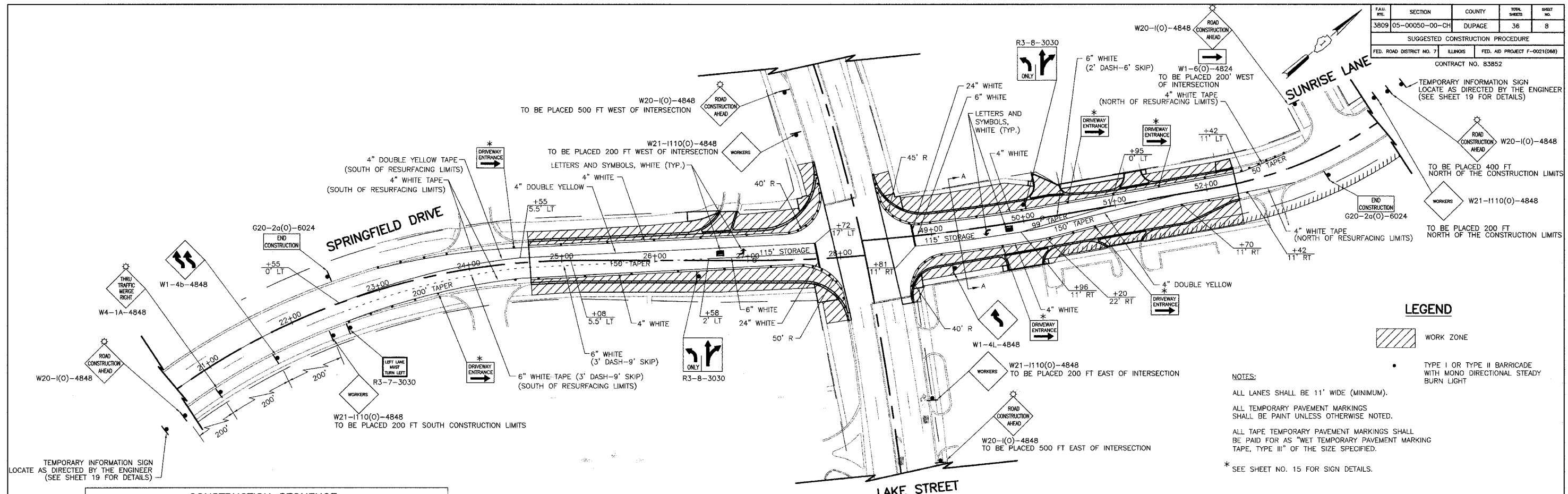
ILLINOIS DEPARTMENT OF TRANSPORTATION
ROADWAY IMPROVEMENT PLANS
SPRINGFIELD DRIVE
STA. 49+00 TO STA. 52+32

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |

SCALE IN FEET
20 0 20 HORIZONTAL
5 0 5 VERTICAL

DRAWN BY: KRK
CHECKED BY: JRV
DATE: 04/04/06

| | | | |
|----------------------------------|----------|-----------------------------|-----------|
| SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3809 05-00050-00-CH | DUPAGE | 36 | 8 |
| SUGGESTED CONSTRUCTION PROCEDURE | | | |
| FED. ROAD DISTRICT NO. 7 | ILLINOIS | FED. AID PROJECT F-021(066) | |
| CONTRACT NO. 83852 | | | |



LEGEND

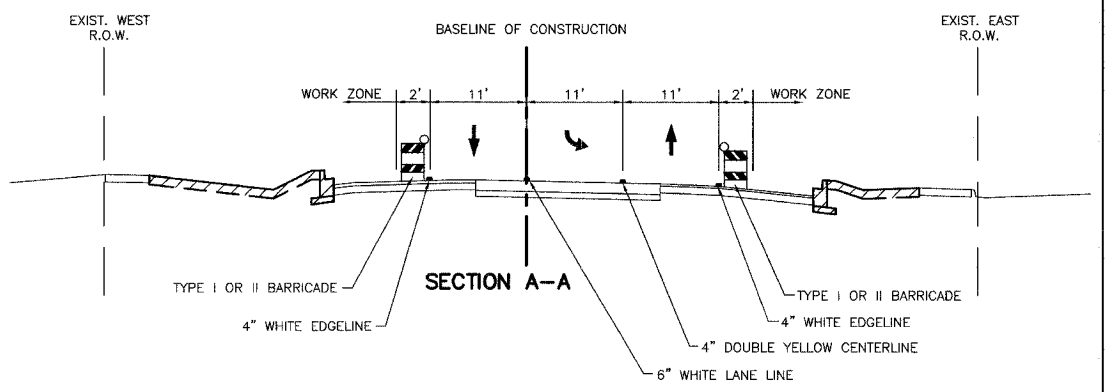
- WORK ZONE
- TYPE I OR TYPE II BARRICADE WITH MONO DIRECTIONAL STEADY BURN LIGHT

NOTES:

- ALL LANES SHALL BE 11' WIDE (MINIMUM).
- ALL TEMPORARY PAVEMENT MARKINGS SHALL BE PAINT UNLESS OTHERWISE NOTED.
- ALL TAPE TEMPORARY PAVEMENT MARKINGS SHALL BE PAID FOR AS "WET TEMPORARY PAVEMENT MARKING TAPE, TYPE III" OF THE SIZE SPECIFIED.
- * SEE SHEET NO. 15 FOR SIGN DETAILS.

| CONSTRUCTION SEQUENCE | |
|--------------------------------------|--|
| PRE-STAGE 1 (NOT ILLUSTRATED) | |
| 1. | REMOVE TREES WITHIN MEDIANS ON SPRINGFIELD DRIVE. |
| 2. | INSTALL AND ACTIVATE TEMPORARY TRAFFIC SIGNALS AND TEMPORARY TRAFFIC SIGNAL INTERCONNECT AT THE INTERSECTION OF LAKE STREET AND SPRINGFIELD DRIVE. (NOTE: THE EXISTING SIGNALS SHALL BE TURNED OFF AT THE SAME TIME THE TEMPORARY SIGNALS ARE ACTIVATED. SEE SHEETS 20 AND 21 FOR SIGNAL DETAILS.) |
| 3. | REMOVE EXISTING SIGNAL POLES AND EQUIPMENT. |
| STAGE 1 (NOT ILLUSTRATED) | |
| 1. | REMOVE EXISTING MEDIANS AND REPLACE WITH SUB-BASE GRANULAR MATERIAL, TYPE B, 4" AND BITUMINOUS BASE COURSE, SUPERPAVE, 8". DAILY LANE CLOSURES IN ACCORDANCE WITH IDOT STANDARD 701501 OR 701606 SHALL BE USED. AT THE END OF EACH WORK DAY, THE CONTRACTOR SHALL PLACE TYPE 2 BARRICADES AT 10 FOOT SPACING WITHIN EACH EXCAVATION. |
| STAGE 2 | |
| 1. | REMOVE ALL CONFLICTING PAVEMENT MARKINGS. |
| 2. | PLACE ALL CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS AND BARRICADES AS SHOWN ON THIS SHEET. SHIFT TRAFFIC AS INDICATED. |
| 3. | CLOSE EXISTING SIDEWALKS AND BIKEPATHS IN ACCORDANCE WITH IDOT STANDARD 701801. |
| 4. | CONSTRUCT STORM SEWERS AND DRAINAGE STRUCTURES OUTSIDE EXISTING EDGES OF PAVEMENT. CONSTRUCT STORM SEWER CROSSINGS USING IDOT STANDARD 701501 AND 701606 (SEE NOTE #7 UNDER "STORM & SANITARY SEWER" ON SHEET NO. 2). |
| 5. | CONSTRUCT ALL PAVEMENT WIDENING, CURB AND GUTTER, SIDEWALKS, ENTRANCES, ETC., OUTSIDE EXISTING PAVEMENT. |
| 6. | THE CONSTRUCTION OF THE CONCRETE DRIVEWAYS AT THE NORTHEAST CORNER OF THE INTERSECTION SHALL BE ALTERNATED SO THAT AT LEAST ONE DRIVEWAY IS OPEN TO TRAFFIC AT ALL TIMES. |
| STAGE 3 (NOT ILLUSTRATED) | |
| 1. | REMOVE ALL BARRICADES AND PLACE BINDER COURSE MAINTAINING TRAFFIC PER STANDARD 701501 OR 701606. IF SURFACE WILL NOT BE PLACED WITHIN 14 DAYS, PLACE TEMPORARY PAVEMENT MARKINGS MATCHING THE PERMANENT MARKINGS USING PAINT. |
| 2. | INSTALL AND ACTIVATE PERMANENT TRAFFIC SIGNALS. (NOTE: THE TEMPORARY SIGNALS SHALL BE TURNED OFF AT THE SAME TIME THE PERMANENT SIGNALS ARE ACTIVATED.) |
| 3. | COMPLETE ALL LANDSCAPING. |
| 4. | PLACE SURFACE COURSE MAINTAINING TRAFFIC PER STANDARD 701501 OR 701606. |
| 5. | PLACE PERMANENT PAVEMENT MARKINGS AND COMPLETE SIGNING WORK. |

| MAINTENANCE OF TRAFFIC GENERAL NOTES | |
|--------------------------------------|--|
| 1. | TRAFFIC CONTROL DEPICTED ON THIS PLAN AND THE APPLICABLE I.D.O.T. DETAILS ARE THE MINIMUM REQUIREMENT. OTHER WORK OR SIGNING MAY BE REQUIRED BY THE ENGINEER. |
| A. | SIGN SPACING FOR STANDARDS 701101, 701106, 701501 AND 701606 MAY BE REDUCED TO THE REQUIREMENTS OF STANDARD 701501 WHEN THE POSTED SPEED LIMIT IS LESS THAN 45 MPH. |
| B. | THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND TRAFFIC CONTROL DEVICES SHALL FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. |
| 2. | BARRICADES WILL BE REQUIRED ADJACENT TO PAVEMENT EDGES WHERE WIDENING, CURB AND GUTTER OR OVERLAYING WORK IS BEING DONE, AS SPECIFIED IN SECTIONS 701 AND 702 OF THE "STANDARD SPECIFICATIONS" EXCEPT THAT THE BARRICADES SHALL BE TYPE II (OR DRUMS), NON-METALLIC WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS. SPACING SHALL BE AS SHOWN ON THE CONSTRUCTION STAGING PLANS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOPS OF THE BARRICADES ARE IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF STANDARD 702001. |
| 3. | ALL TYPE II BARRICADES AT LANE DIVERSIONS WITHIN TAPER SECTIONS SHALL HAVE DIRECTION INDICATOR PANELS ON THE TOP RAILS. |
| 4. | TYPE I OR II BARRICADES (OR DRUMS) EQUIPPED WITH ONE-WAY FLASHING LIGHTS WILL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER STRUCTURES, AND AT ANY OTHER LOCATIONS DESIGNATED BY THE ENGINEER OR LOCAL LAW ENFORCEMENT AGENCIES. BARRICADES SHALL BE PLACED AT 50' CENTERS ALONG TANGENTS, 25' ALONG TAPERS AND 10' AROUND RADII. |
| 5. | ALL CONSTRUCTION SIGNS SHALL HAVE FLUORESCENT ORANGE BACKGROUNDS. |
| 6. | DRUMS SHALL HAVE ALTERNATING REFLECTORIZED TYPE AA OR TYPE AP FLUORESCENT ORANGE AND REFLECTORIZED WHITE HORIZONTAL, CIRCUMFERENTIAL STRIPES. |
| 7. | VERTICAL BARRICADES SHALL MEET THE REQUIREMENTS OF THE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) REPORT 350 AND THE SPECIAL PROVISION "WORK ZONE TRAFFIC CONTROL DEVICES". VERTICAL BARRICADES MAY BE USED IN LIEU OF CONES, DRUMS OR TYPE I AND TYPE II BARRICADES TO CHANNELIZE TRAFFIC. VERTICAL BARRICADES SHALL NOT BE USED IN LANE CLOSURE TAPERS. |
| 8. | THE CONTRACTOR SHALL INFORM THE ENGINEER OF ANY STAGE CHANGE AT LEAST TWO WORKING DAYS IN ADVANCE OF THE CHANGE. |
| 9. | ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION EXCEPT FOR PERIODS OF SHORT DURATION WHEN APPROVED BY THE ENGINEER. TEMPORARY ACCESS SHALL BE PROVIDED IN ACCORDANCE WITH THE SPECIAL PROVISION "AGGREGATE SURFACE COURSE FOR TEMPORARY ACCESS." |
| 10. | THE FIRST TWO WARNING SIGNS IN EACH DIRECTION OF TRAVEL SHALL BE EQUIPPED WITH MONO-DIRECTIONAL AMBER FLASHING LIGHTS DURING HOURS OF DARKNESS. FLAGS ARE OPTIONAL. |
| 11. | EXISTING TRAFFIC CONTROL DEVICES ARE TO BE PROTECTED FROM DAMAGE BY THE CONTRACTOR. ANY DAMAGE CAUSED BY HIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR. |
| 12. | WHEN REQUIRED, TEMPORARY LANE CLOSURES WILL BE ALLOWED ONLY BETWEEN THE HOURS OF 9:00 A.M. AND 3:00 P.M., WITH TRAFFIC MAINTAINED IN ACCORDANCE WITH STANDARD 701501 OR 701606 UNLESS OTHERWISE DIRECTED BY THE ENGINEER. |
| 13. | "WORKERS" SIGN SHALL ONLY BE ERECTED WHEN WORKERS ARE PRESENT. SIGN MUST BE COVERED OR REMOVED WHEN NO WORKERS ARE PRESENT. |



| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUGGESTED CONSTRUCTION PROCEDURE & MAINTENANCE OF TRAFFIC
 SPRINGFIELD ROAD

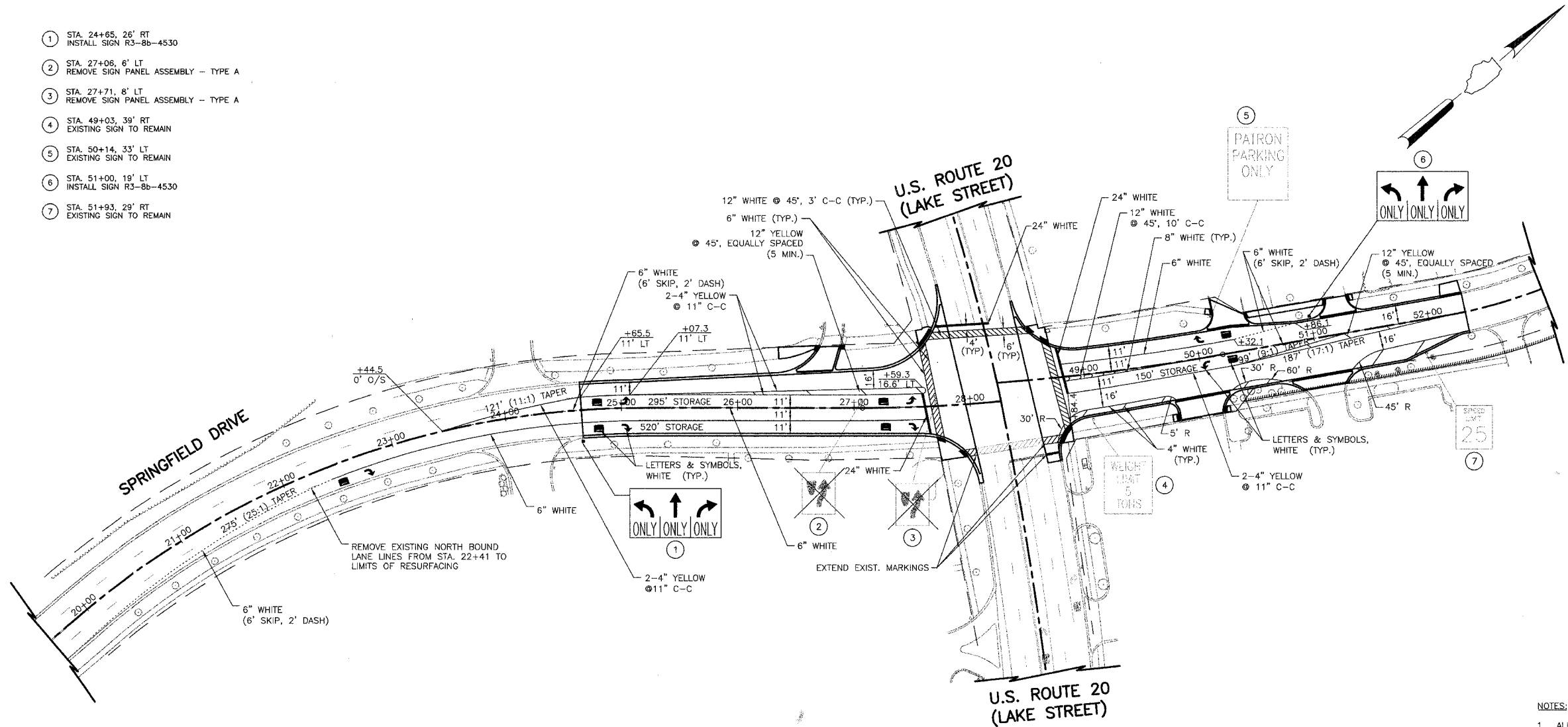
SCALE IN FEET

DRAWN BY: DJK
 CHECKED BY: JRV
 DATE: 04/04/06

| | | | | |
|-----------------------------------|----------------|------------------------------|--------------|-----------|
| FILE NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 9 |
| PAVEMENT MARKING AND SIGNING PLAN | | | | |
| FED. ROAD DISTRICT 7 | ILLINOIS | FED. AID PROJECT F-0021(068) | | |

CONTRACT NO. 83852

- ① STA. 24+65, 26' RT
INSTALL SIGN R3-Bb-4530
- ② STA. 27+06, 6' LT
REMOVE SIGN PANEL ASSEMBLY - TYPE A
- ③ STA. 27+71, 8' LT
REMOVE SIGN PANEL ASSEMBLY - TYPE A
- ④ STA. 49+03, 39' RT
EXISTING SIGN TO REMAIN
- ⑤ STA. 50+14, 33' LT
EXISTING SIGN TO REMAIN
- ⑥ STA. 51+00, 19' LT
INSTALL SIGN R3-Bb-4530
- ⑦ STA. 51+93, 29' RT
EXISTING SIGN TO REMAIN



- NOTES:
- 1. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC.
 - 2. LARGE LETTERS AND SYMBOLS SHALL BE USED FOR LEFT AND RIGHT TURN ONLY MARKINGS.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND SIGNING PLAN

SPRINGFIELD DRIVE

SCALE IN FEET

50 0 50

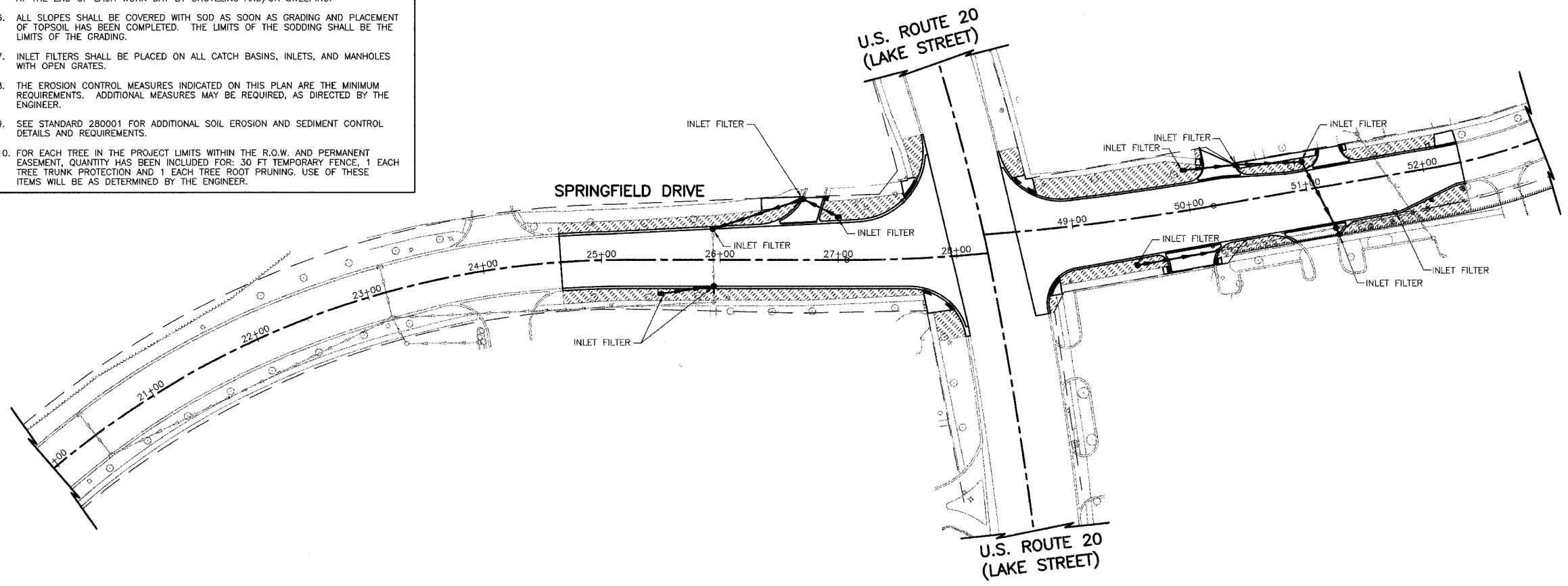
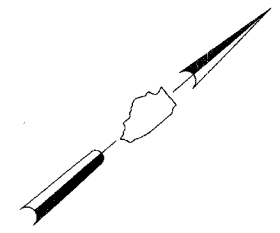
DRAWN BY: KRK
CHECKED BY: JRV
DATE: 04/04/06

| | | | | |
|--------------------------------------|----------------|------------------------------|-----------------|-------------|
| FAH SITE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO |
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 10 |
| EROSION CONTROL AND LANDSCAPING PLAN | | | | |
| FED. ROAD DISTRICT NO. 7 | ILLINOIS | FED. AID PROJECT F-0021(068) | | |

CONTRACT NO. 83852

GENERAL NOTES

1. ALL VEGETATIVE AND STRUCTURAL EROSION CONTROL PRACTICES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE "ILLINOIS PROCEDURES AND STANDARDS FOR URBAN SOIL EROSION AND SEDIMENTATION CONTROL", AND THE "STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL" OF THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.
2. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
3. THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES PERIODICALLY AND AFTER EACH RUNOFF-PRODUCING RAINFALL EVENT. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF SAID MEASURES SHALL BE MADE IMMEDIATELY.
4. THE MAINTENANCE AND REPAIR OR REPLACEMENT OF EROSION CONTROL ITEMS, WHEN DIRECTED BY THE ENGINEER, WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE ASSOCIATED PAY ITEMS.
5. ALL STORM SEWER FACILITIES THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED, FILTERED OR OTHERWISE TREATED TO REMOVE SEDIMENT. MUD AND SEDIMENT DEPOSITS SHALL BE REMOVED FROM THE ROADWAY AT THE END OF EACH WORK DAY BY SHOVELING AND/OR SWEEPING.
6. ALL SLOPES SHALL BE COVERED WITH SOD AS SOON AS GRADING AND PLACEMENT OF TOPSOIL HAS BEEN COMPLETED. THE LIMITS OF THE SODDING SHALL BE THE LIMITS OF THE GRADING.
7. INLET FILTERS SHALL BE PLACED ON ALL CATCH BASINS, INLETS, AND MANHOLES WITH OPEN GRATES.
8. THE EROSION CONTROL MEASURES INDICATED ON THIS PLAN ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER.
9. SEE STANDARD 280001 FOR ADDITIONAL SOIL EROSION AND SEDIMENT CONTROL DETAILS AND REQUIREMENTS.
10. FOR EACH TREE IN THE PROJECT LIMITS WITHIN THE R.O.W. AND PERMANENT EASEMENT, QUANTITY HAS BEEN INCLUDED FOR: 30 FT TEMPORARY FENCE, 1 EACH TREE TRUNK PROTECTION AND 1 EACH TREE ROOT PRUNING. USE OF THESE ITEMS WILL BE AS DETERMINED BY THE ENGINEER.



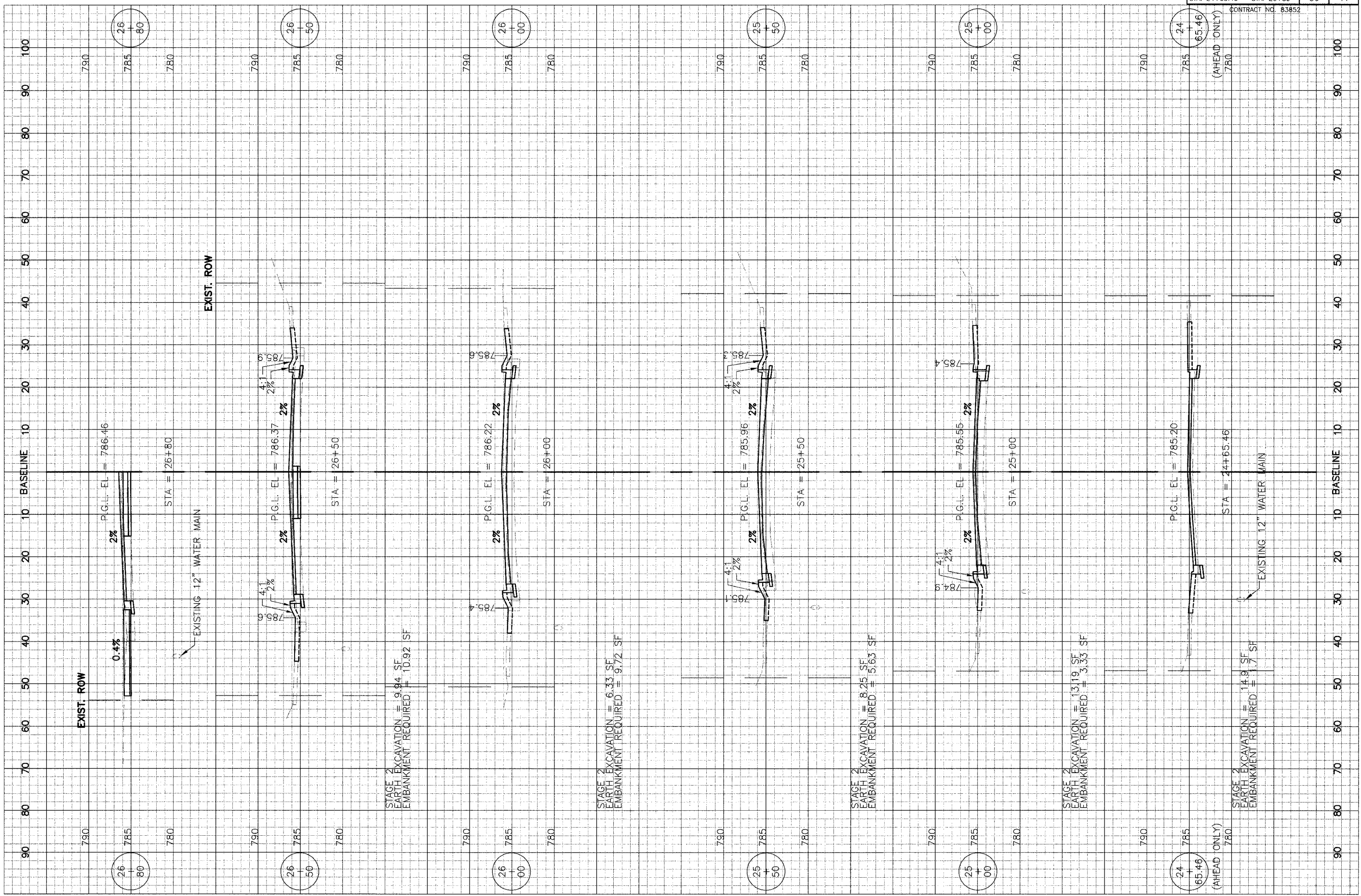
LEGEND
 TOPSOIL FURNISH AND PLACE, 4", AND SODDING, SALT TOLERANT

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
EROSION CONTROL AND LANDSCAPING PLAN
SPRINGFIELD DRIVE

SCALE IN FEET

DRAWN BY: KRK
CHECKED BY: JRV
DATE: 04/04/06



EXIST. ROW

P.G.L. EL = 786.46

26
+
80

STA = 26+80

EXIST. ROW

P.G.L. EL = 786.37

26
+
50

STA = 26+50

STAGE 2
EARTH EXCAVATION = 9.94 SF
EMBANKMENT REQUIRED = 10.92 SF

P.G.L. EL = 786.22

26
+
00

STA = 26+00

STAGE 2
EARTH EXCAVATION = 6.33 SF
EMBANKMENT REQUIRED = 9.72 SF

P.G.L. EL = 785.96

25
+
50

STA = 25+50

STAGE 2
EARTH EXCAVATION = 8.25 SF
EMBANKMENT REQUIRED = 5.63 SF

P.G.L. EL = 785.55

25
+
00

STA = 25+00

STAGE 2
EARTH EXCAVATION = 13.19 SF
EMBANKMENT REQUIRED = 3.33 SF

P.G.L. EL = 785.20

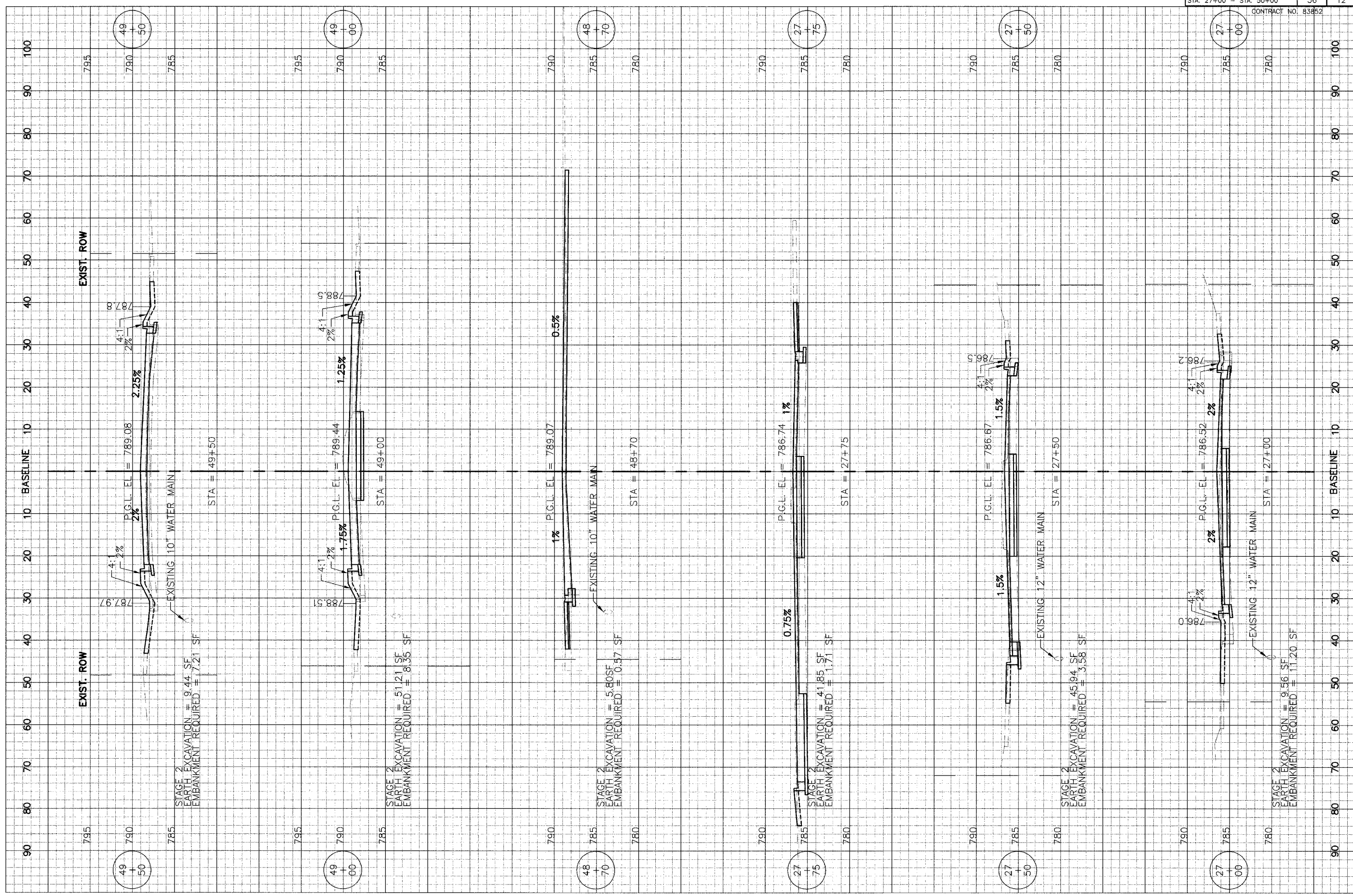
24
+
65.46

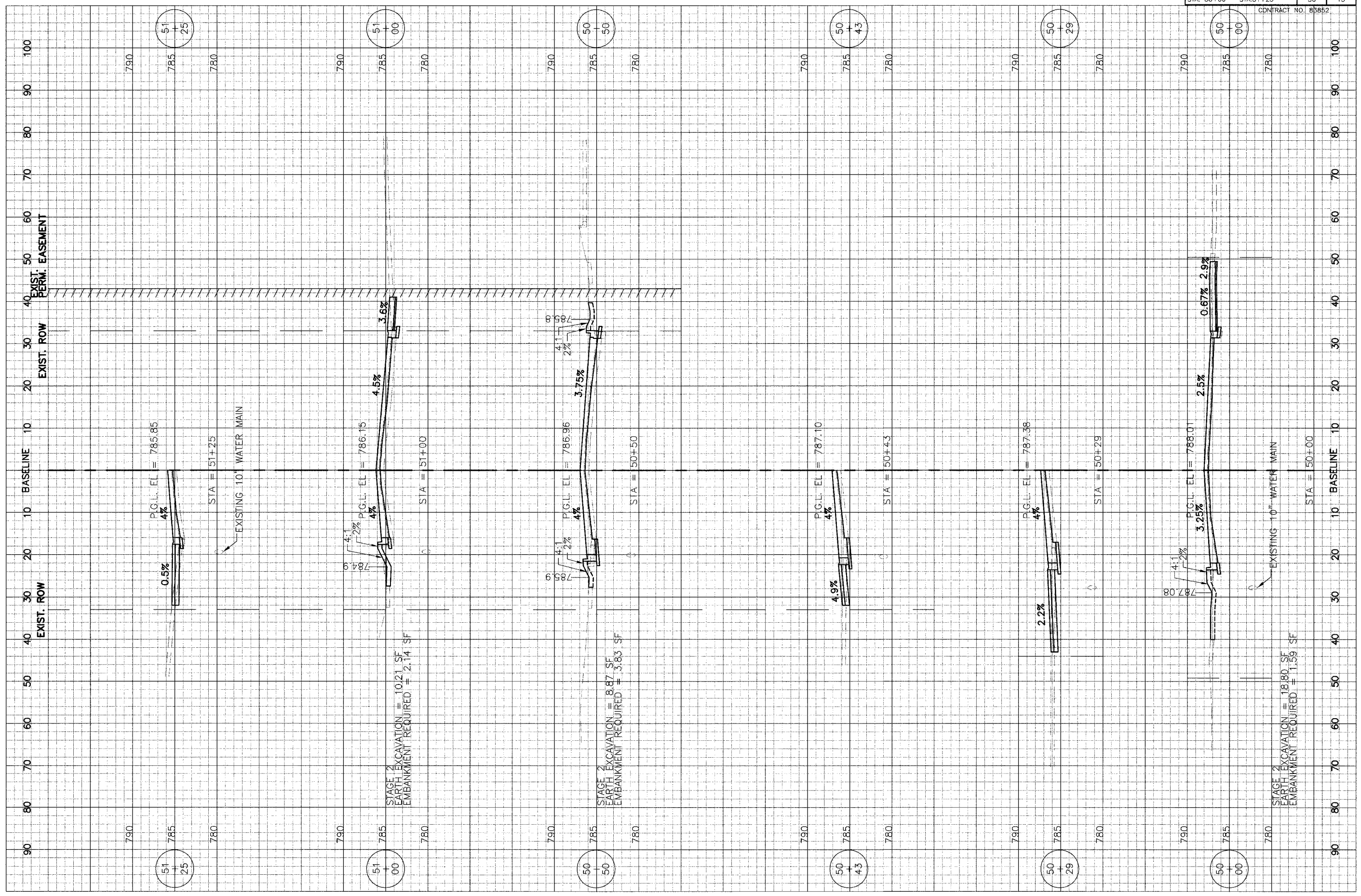
STA = 24+65.46

STAGE 2
EARTH EXCAVATION = 14.9 SF
EMBANKMENT REQUIRED = 1.7 SF

(AHEAD ONLY)
780

EXISTING 12" WATER MAIN





51
+
25

51
+
00

50
+
50

50
+
43

50
+
29

50
+
00

STAGE 2
EARTH EXCAVATION = 10.21 SF
EMBANKMENT REQUIRED = 2.14 SF

STAGE 2
EARTH EXCAVATION = 8.87 SF
EMBANKMENT REQUIRED = 3.83 SF

STAGE 2
EARTH EXCAVATION = 18.80 SF
EMBANKMENT REQUIRED = 1.59 SF

EXIST. ROW

BASELINE

EXIST. PERM. EASEMENT

P.C.L. EL = 785.85

STA = 51+25

EXISTING 10" WATER MAIN

P.C.L. EL = 786.15

SIA = 51+00

P.C.L. EL = 786.96

SIA = 50+50

P.C.L. EL = 787.10

SIA = 50+43

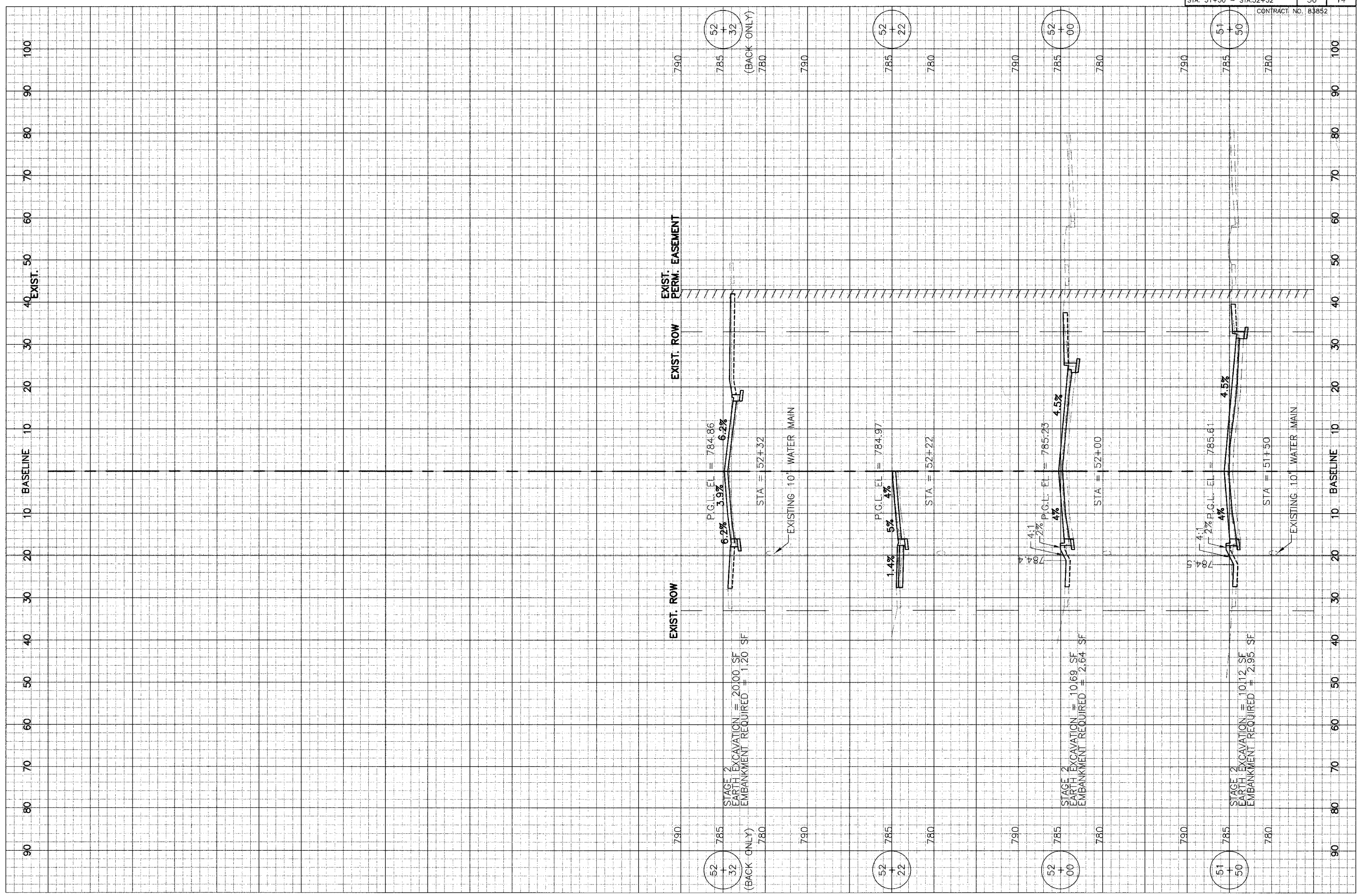
P.C.L. EL = 787.36

STA = 50+29

P.C.L. EL = 788.01

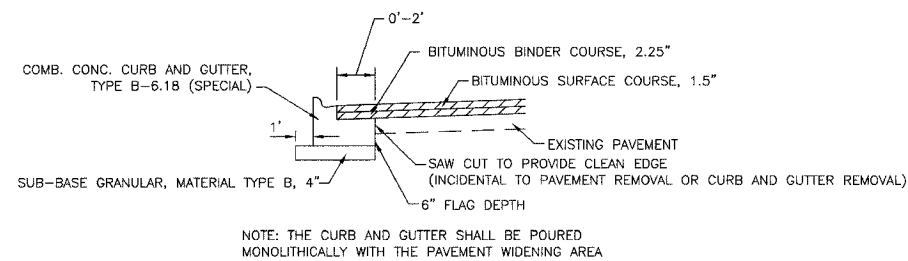
SIA = 50+00

EXISTING 10" WATER MAIN

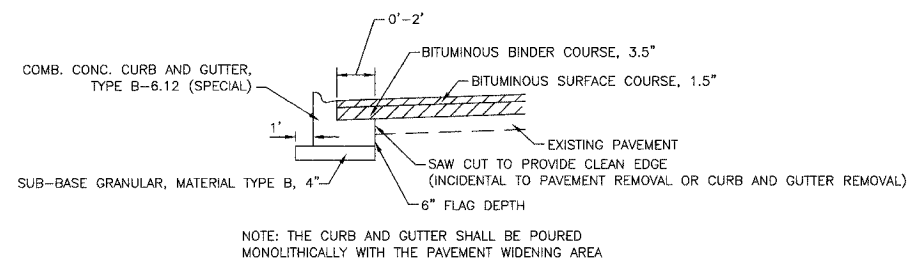


| F&E NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------------|----------------|----------|------------------------------|-----------|
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 15 |
| ROADWAY DETAILS | | | | |
| FED. ROAD DISTRICT NO. 7 | | ILLINOIS | FED. AID PROJECT F-0021(068) | |

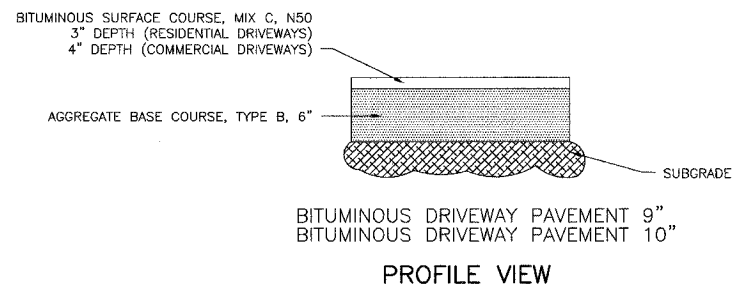
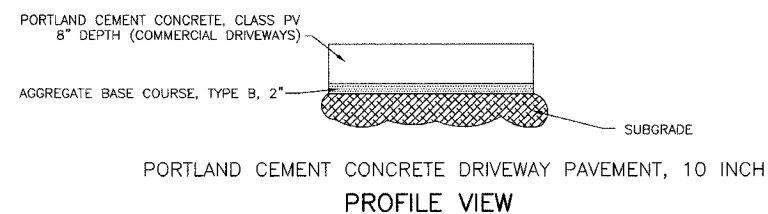
CONTRACT NO. 83852



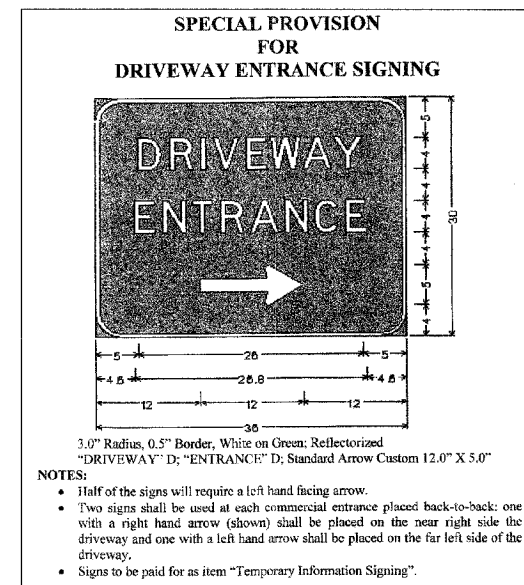
DETAIL FOR
COMB. CONC. CURB AND GUTTER, TYPE B-6.18 (SPECIAL)



DETAIL FOR
COMB. CONC. CURB AND GUTTER, TYPE B-6.12 (SPECIAL)



PROPOSED DRIVEWAY SECTIONS



- NOTES:
- Half of the signs will require a left hand facing arrow.
 - Two signs shall be used at each commercial entrance placed back-to-back: one with a right hand arrow (shown) shall be placed on the near right side the driveway and one with a left hand arrow shall be placed on the far left side of the driveway.
 - Signs to be paid for as item "Temporary Information Signing".

DRIVEWAY ENTRANCE SIGNING

PROPOSED EDGE OF PAVEMENT GRADE TABLES

| SOUTHBOUND | | | | | NORTHBOUND | | | | |
|------------|-----------|---------------------------|-----------------|---------------|-----------------|----------------------------|-----------|----------|--|
| STATION | ELEVATION | DISTANCE LEFT OF CL (EOP) | CROSS SLOPE (%) | PGL ELEVATION | CROSS SLOPE (%) | DISTANCE RIGHT OF CL (EOP) | ELEVATION | STATION | |
| 24+65.48 | 784.76 | 22.00 | 2.00 | 785.20 | 2.00 | 22.00 | 784.76 | 24+65.48 | |
| 24+75 | 784.85 | 22.00 | 2.00 | 785.29 | 2.00 | 22.00 | 784.85 | 24+75 | |
| 25+00 | 785.11 | 22.00 | 2.00 | 785.55 | 2.00 | 22.00 | 785.11 | 25+00 | |
| 25+25 | 785.33 | 22.89 | 2.00 | 785.78 | 2.00 | 22.00 | 785.34 | 25+25 | |
| 25+50 | 785.48 | 23.94 | 2.00 | 785.96 | 2.00 | 22.00 | 785.52 | 25+50 | |
| 25+75 | 785.61 | 25.19 | 2.00 | 786.11 | 2.00 | 22.00 | 785.67 | 25+75 | |
| 26+00 | 785.69 | 26.44 | 2.00 | 786.22 | 2.00 | 22.00 | 785.78 | 26+00 | |
| 26+25 | 785.74 | 27.69 | 2.00 | 786.29 | 2.00 | 22.00 | 785.85 | 26+25 | |
| 26+50 | 785.79 | 28.94 | 2.00 | 786.37 | 2.00 | 22.00 | 785.93 | 26+50 | |
| 26+75 | 785.84 | 30.19 | 2.00 | 786.44 | 2.00 | 22.00 | 786.00 | 26+75 | |
| 26+80 | 785.85 | 30.44 | 2.00 | 786.46 | 2.00 | 22.00 | 786.02 | 26+80 | |
| 27+00 | 785.89 | 31.44 | 2.00 | 786.52 | 2.00 | 22.00 | 786.08 | 27+00 | |
| 27+25 | 786.01 | 33.09 | 1.75 | 786.59 | 1.75 | 22.12 | 786.20 | 27+25 | |
| 27+50 | 786.03 | 42.79 | 1.50 | 786.67 | 1.50 | 22.71 | 786.33 | 27+50 | |
| 27+75 | 786.19 | 73.60 | 0.75 | 786.74 | 1.00 | 26.38 | 786.48 | 27+75 | |

| SOUTHBOUND | | | | | | | NORTHBOUND | | | | | | |
|------------|-----------|---------------------|-----------------|-----------|---------------------|-----------------|---------------|-----------------|----------------------------|-----------|---------|--|--|
| STATION | ELEVATION | DISTANCE LEFT OF CL | CROSS SLOPE (%) | ELEVATION | DISTANCE LEFT OF CL | CROSS SLOPE (%) | PGL ELEVATION | CROSS SLOPE (%) | DISTANCE RIGHT OF CL (EOP) | ELEVATION | STATION | | |
| 49+00 | - | - | - | 789.06 | 22.00 (EOP) | 1.75 | 789.44 | 1.25 | 35.19 | 789.00 | 49+00 | | |
| 49+25 | - | - | - | 789.00 | 22.00 (EOP) | 1.88 | 789.41 | 1.75 | 33.82 | 788.82 | 49+25 | | |
| 49+50 | - | - | - | 788.64 | 22.00 (EOP) | 2.00 | 789.08 | 2.25 | 32.76 | 788.34 | 49+50 | | |
| 49+75 | - | - | - | 787.97 | 22.00 (EOP) | 2.63 | 788.55 | 2.38 | 32.03 | 787.79 | 49+75 | | |
| 50+00 | - | - | - | 787.30 | 22.00 (EOP) | 3.25 | 788.01 | 2.50 | 31.61 | 787.22 | 50+00 | | |
| 50+25 | - | - | - | 786.61 | 22.00 (EOP) | 3.90 | 787.47 | 3.13 | 31.50 | 786.48 | 50+25 | | |
| 50+29 | - | - | - | 786.50 | 22.00 (EOP) | 4.00 | 787.38 | 3.23 | 31.50 | 786.36 | 50+29 | | |
| 50+43 | - | - | - | 786.27 | 20.78 (EOP) | 4.00 | 787.10 | 3.58 | 31.50 | 785.97 | 50+43 | | |
| 50+50 | - | - | - | 786.16 | 20.01 (EOP) | 4.00 | 786.96 | 3.75 | 31.50 | 785.78 | 50+50 | | |
| 50+75 | - | - | - | 785.84 | 17.23 (EOP) | 4.00 | 786.53 | 4.13 | 31.50 | 785.23 | 50+75 | | |
| 51+00 | - | - | - | 785.51 | 16.00 (EOP) | 4.00 | 786.15 | 4.50 | 31.50 | 784.73 | 51+00 | | |
| 51+25 | - | - | - | 785.21 | 16.00 (EOP) | 4.00 | 785.85 | 4.50 | 31.50 | 784.43 | 51+25 | | |
| 51+50 | - | - | - | 784.97 | 16.00 (EOP) | 4.00 | 785.61 | 4.50 | 31.50 | 784.19 | 51+50 | | |
| 51+75 | - | - | - | 784.78 | 16.00 (EOP) | 4.00 | 785.42 | 4.50 | 31.38 | 784.01 | 51+75 | | |
| 52+00 | - | - | - | 784.59 | 16.00 (EOP) | 4.00 | 785.23 | 4.50 | 24.02 | 784.15 | 52+00 | | |
| 52+22 | 784.27 | 16.00 (EOP) | 5.00 | 784.57 | 10.00 | 4.00 | 784.97 | 5.53 | 17.63 | 784.00 | 52+22 | | |
| 52+25 | 784.22 | 16.00 (EOP) | 5.34 | 784.54 | 10.00 | 3.98 | 784.94 | 5.67 | 16.64 | 784.00 | 52+25 | | |
| 52+32 | 784.07 | 16.41 (EOP) | 6.15 | 784.44 | 10.41 | 4.00 | 784.86 | 6.19 | 16.64 | 783.83 | 52+32 | | |

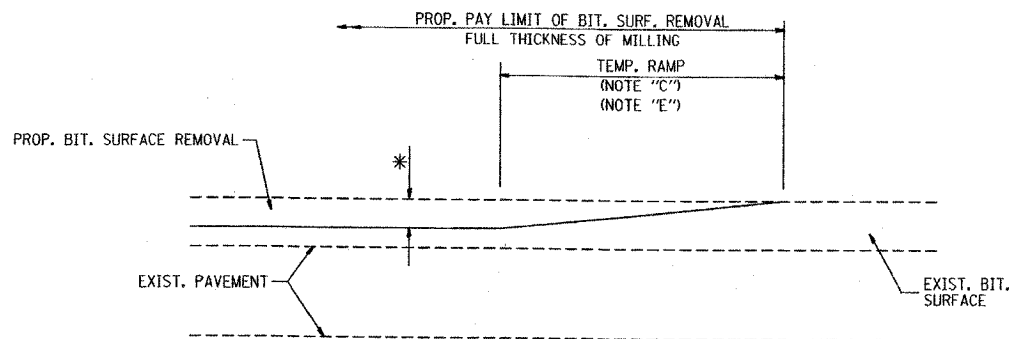
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
ROADWAY DETAILS
SPRINGFIELD DRIVE

NOT TO SCALE
DRAWN BY: DJK
CHECKED BY: JRV
DATE: 04/04/06

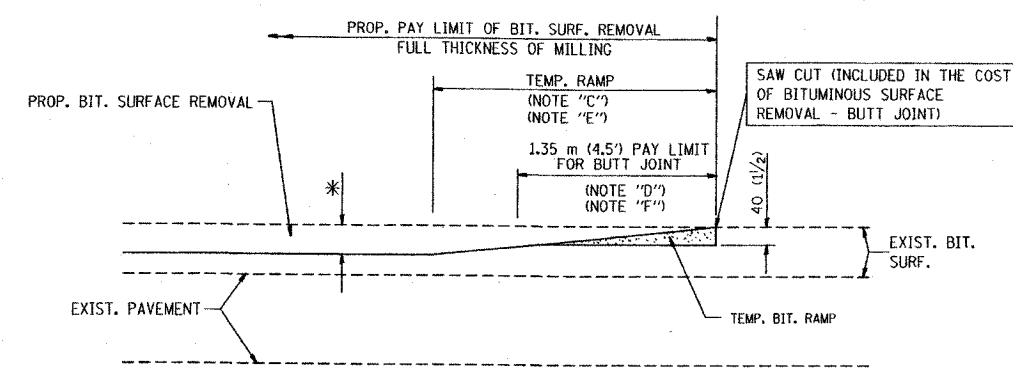
| P.A.L. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------------|----------|------------------------------|-----------|
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 16 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 7 | | ILLINOIS | FED. AID PROJECT F-002(0098) | |

CONTRACT NO. B3852



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND BIT. TAPER SEE DETAIL BELOW)

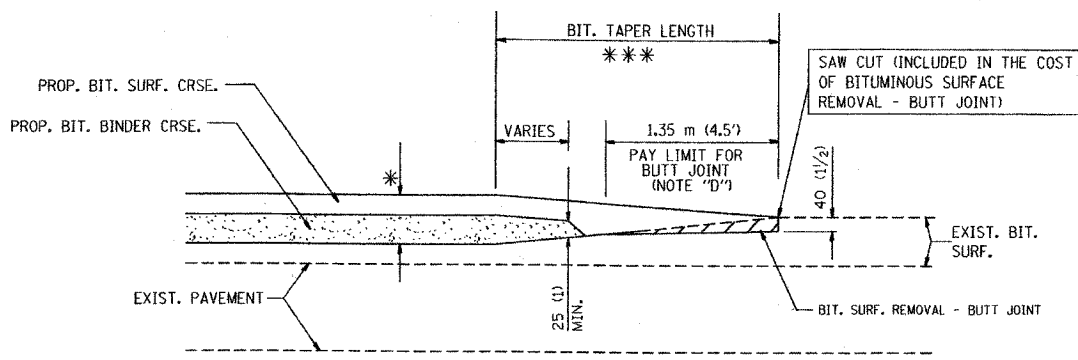
OPTION 1



BITUMINOUS CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND BIT. TAPER SEE DETAIL BELOW)

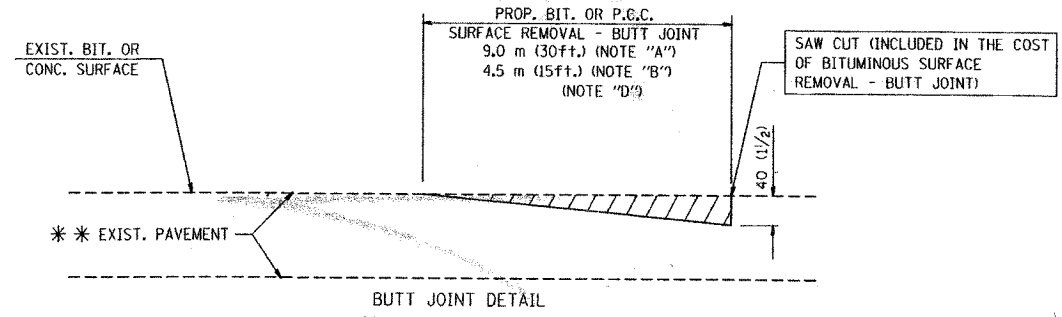
OPTION 2

TYPICAL TEMPORARY RAMP

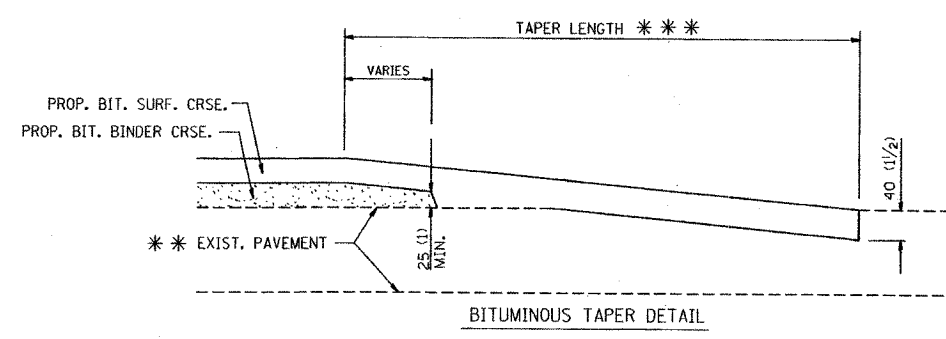


BUTT JOINT AND BITUMINOUS TAPER

TYPICAL BUTT JOINT AND BITUMINOUS TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



BITUMINOUS TAPER DETAIL

TYPICAL BUTT JOINT AND BITUMINOUS TAPER
FOR RESURFACING ONLY

** PC CONCRETE, BITUMINOUS OR BITUMINOUS RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING BITUMINOUS SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED BITUMINOUS COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 900 (3 ft.) PER INCH OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 1.35 m (4.5') TEMP. BIT. RAMP WILL BE PAID AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT".
 - G: SEE ARTICLE 406.18 AND 406.24 OF THE STANDARD SPECIFICATIONS FOR "BITUMINOUS AND PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 6.1 m (20') PER 25 (1) RESURFACING (NOTE "A")
3.0 m (10') PER 25 (1) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR PER SQUARE METER (SQUARE YARD.) AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT" OR AS "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

| REVISIONS | |
|------------|----------|
| NAME | DATE |
| M. DE YONG | 6-13-90 |
| M. DE YONG | 7-3-90 |
| M. DE YONG | 3-27-92 |
| R. SHAH | 09/09/94 |
| R. SHAH | 10/25/94 |
| A. ABBAS | 03/21/97 |

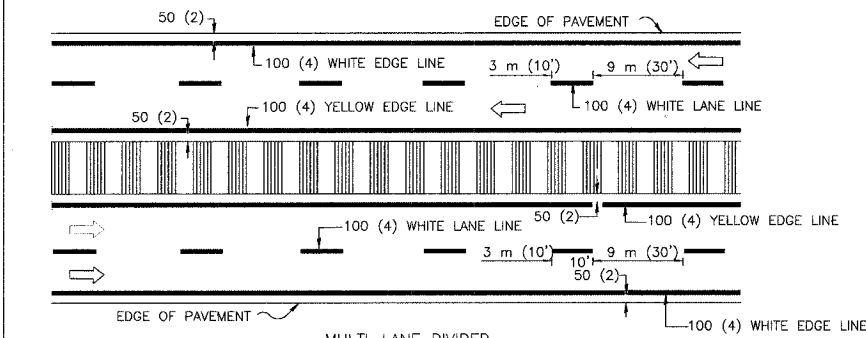
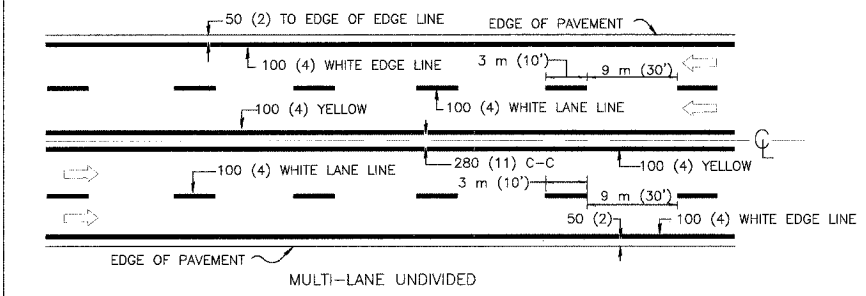
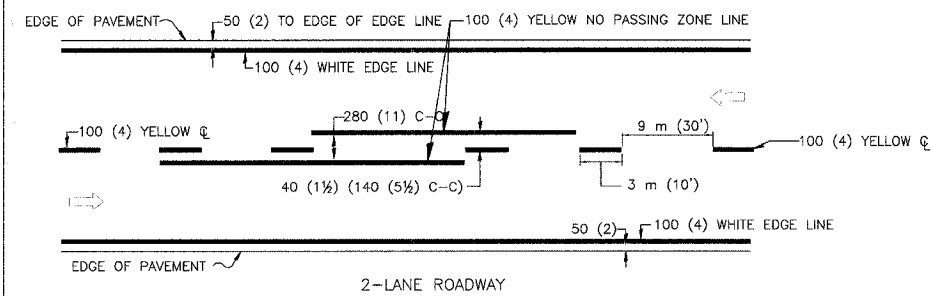
ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND BITUMINOUS TAPER DETAILS

SCALE: NONE
DATE: 03/13/00
DRAWN BY
CHECKED BY

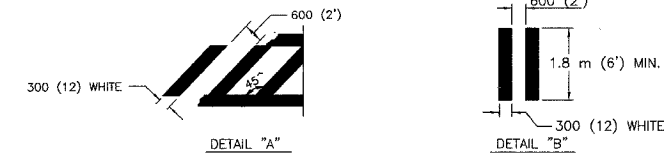
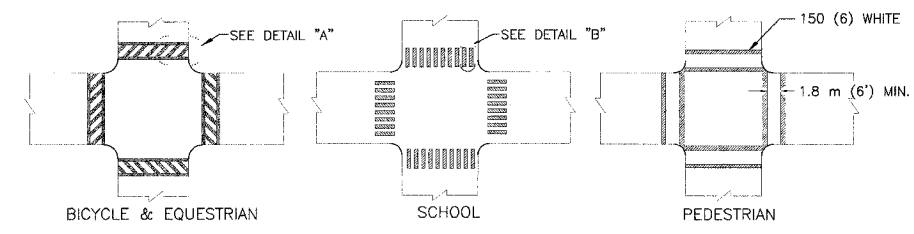
Mon_Mar_13_13:44:49_2000...v:\p\k\metrie\bd32\m\prk

| | | | | |
|-------------------------------|----------------|------------------------------|--------------|-----------|
| FALL PRE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 17 |
| IDOT PAVEMENT MARKING DETAILS | | | | |
| FED. ROAD DISTRICT NO. 7 | ILLINOIS | FED. AID PROJECT F-0021(088) | | |
| CONTRACT NO. 83852 | | | | |

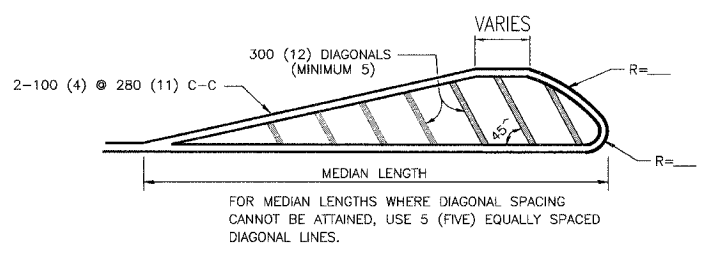
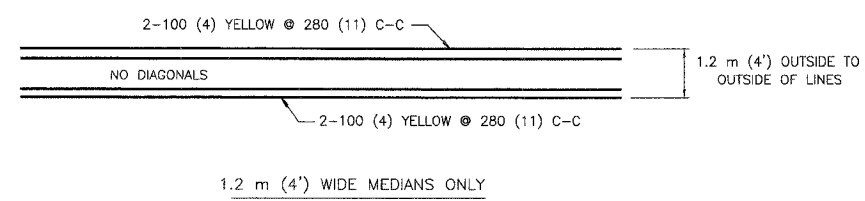


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

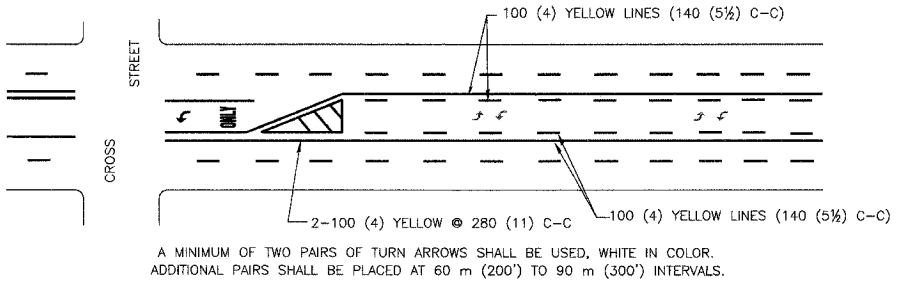


TYPICAL CROSSWALK MARKING

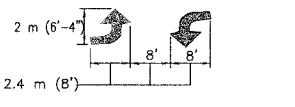


DIAGONAL LINE SPACING: 15 m (50') C-C (LESS THAN 50 km/h (30 MPH))
25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH))
45 m (150') C-C (MORE THAN 70 km/h (45 MPH))

MEDIANS OVER 1.2 m (4') WIDE

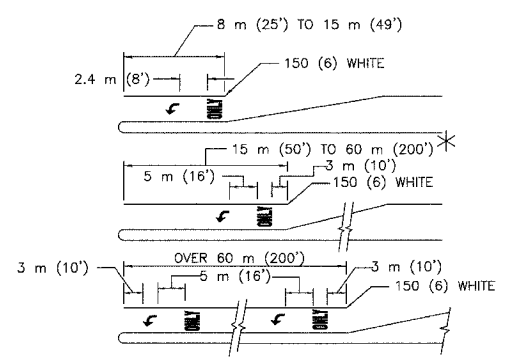


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 60 m (200') TO 90 m (300') INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

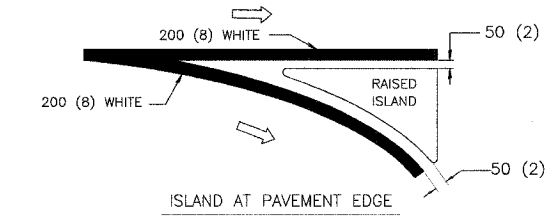
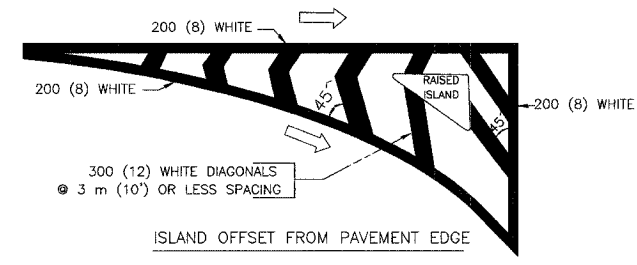
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED.
AREA = 1.5 m² (15.6 SQ. FT.) ONLY AREA = 1.9 m² (20.8 SQ. FT.)
* TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|--|-------------------------|---|---|
| CENTERLINE ON 2 LANE PAVEMENT | 100 (4) | SKIP-DASH | YELLOW | 3 m (10') LINE WITH 9 m (30') SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 100 (4) | SOLID | YELLOW | 280 (11) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS | 100 (4) 2 @ 100 (4) | SOLID SOLID | YELLOW YELLOW | 140 (5?) C-C FROM SKIP-DASH CENTERLINE 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 100 (4) 125 (5) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 3 m (10') LINE WITH 9 m (30') SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 600 (2') LINE WITH 1.8 m (6') SPACE |
| EDGE LINES | 100 (4) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8')) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 100 (4) EACH DIRECTION | SKIP-DASH AND SOLID | YELLOW | 3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5 1/2) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE |
| | 2.4 m (8') LEFT ARROW | IN PAIRS | WHITE | SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 1.8 m (6') APART 600 (2') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 600 (24) | SOLID | WHITE | PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. |
| PAINTED MEDIANS | 2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° | SOLID | YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC | 280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 200 (8) WITH 300 (12) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH)) |
| RAILROAD CROSSING | 600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=0.40 m ² (4.3 SQ. FT.) EACH "X"=5.0 m ² (54.0 SQ. FT.) |
| SHOULDER DIAGONALS | 300 (12) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JANUARY 1, 1997 AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

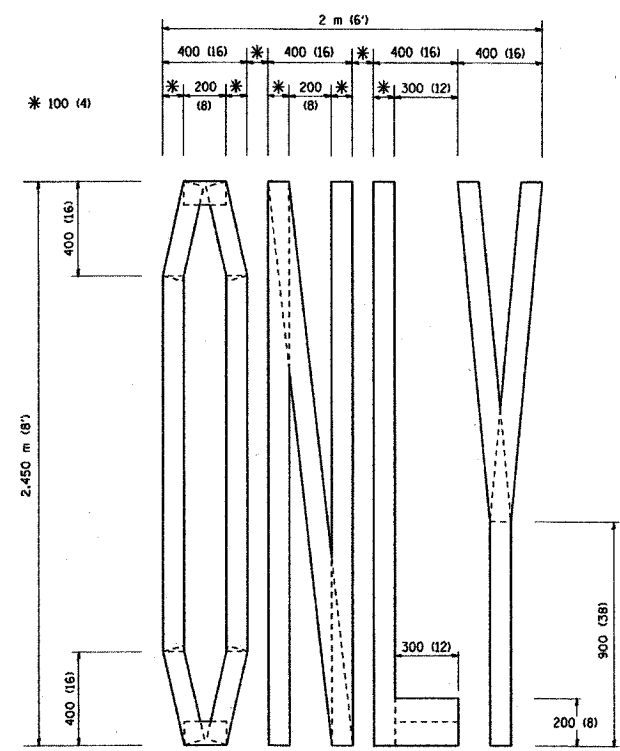
DISTRICT ONE
TYPICAL PAVEMENT
MARKINGS

| REVISIONS | |
|-------------|----------|
| NAME | DATE |
| EVERS | 03-19-90 |
| T. RAMMACH | 10-27-94 |
| ALEX HOUSEH | 10-09-96 |
| ALEX HOUSEH | 10-17-96 |

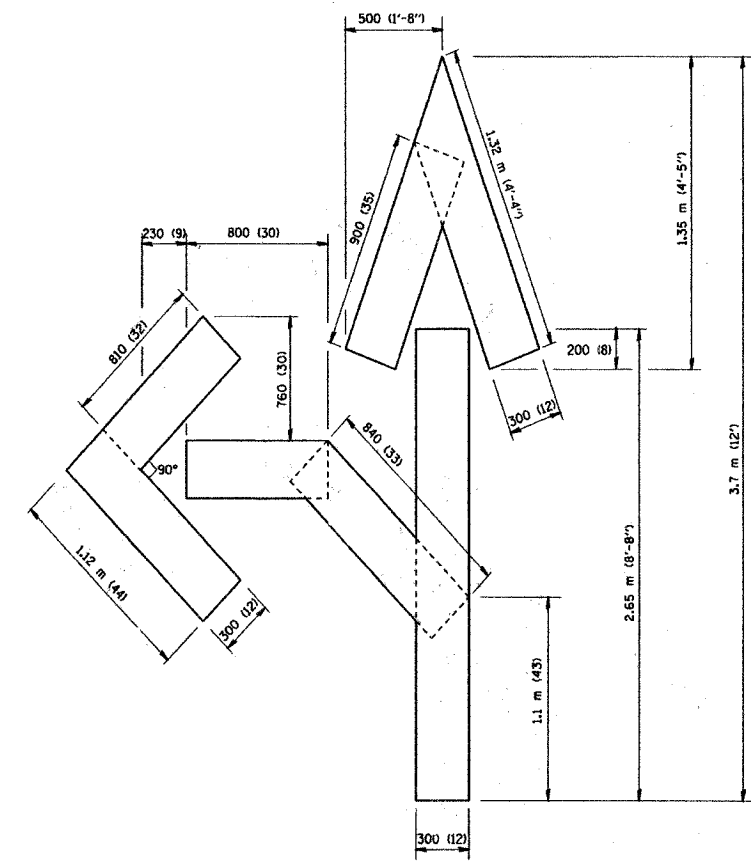
SCALE: NONE
DATE
DRAWN BY CADD
CHECKED BY

| | | | | |
|---------------------|----------------|-----------------|--------------|-----------|
| P.A.U. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 18 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | ALIGNED | FED. RD PROJECT | | |

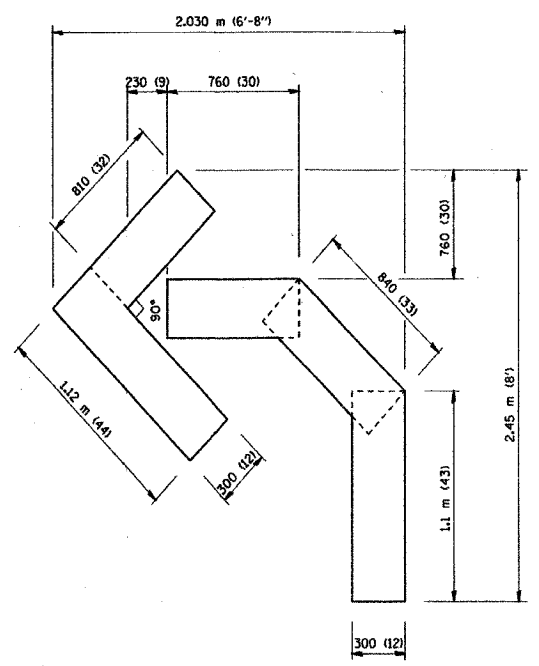
CONTRACT NO. 83852



QUANTITY
100 (4) LINE = 19.7 m (64.1 ft.)
1.97 sq. m (21.1 sq. ft.)



QUANTITY
100 (4) LINE = 25.3 m (82.5 ft.)
2.53 sq. m (27.5 sq. ft.)



QUANTITY
100 (4) LINE = 13.9 m (45.5 ft.)
1.39 sq. m (15.2 sq. ft.)

All dimensions are in millimeters (Inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION
TEMPORARY PAVEMENT MARKING
LETTERS AND SYMBOLS

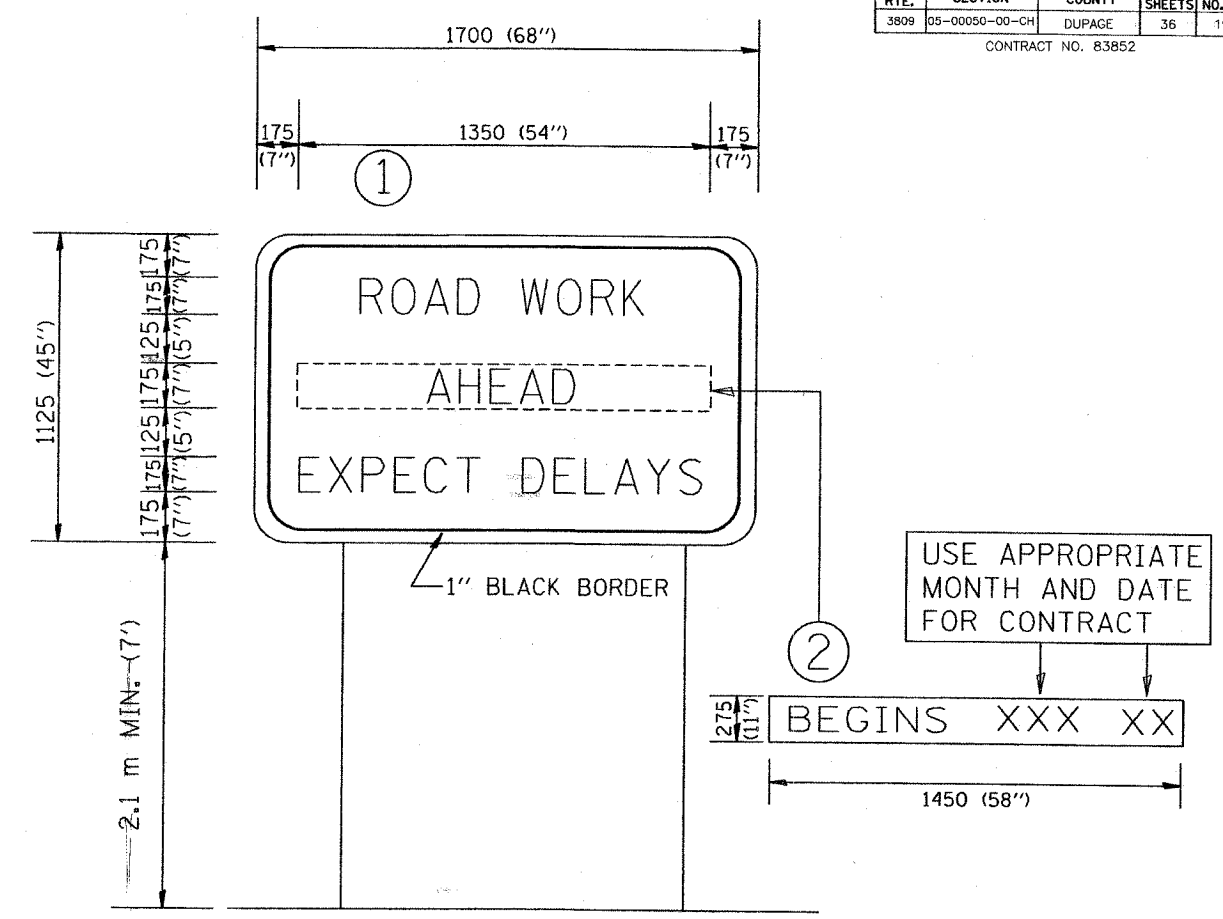
| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| T. RAMMACHER | 09/18/94 |
| J. OBERLE | 06/01/95 |
| T. RAMMACHER | 06/05/96 |
| T. RAMMACHER | 11/04/97 |
| T. RAMMACHER | 03/02/98 |

SCALE: NONE
DATE 03/13/00
DRAWN BY CADD
CHECKED BY

Mon_Mar_13_13:54:41_2000...v:\cadd\files\CAD\qcf...w:\prk\metric\tc16m.pct

| F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------------|--------|--------------|-----------|
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 19 |

CONTRACT NO. 83852



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 2.3 SQ. M. (25.70 SQ. FT.)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| R. MIRS | 9-15-97 |
| R. MIRS | 12-11-97 |
| T. RAMMACHER | 2-2-99 |
| | |
| | |
| | |
| | |

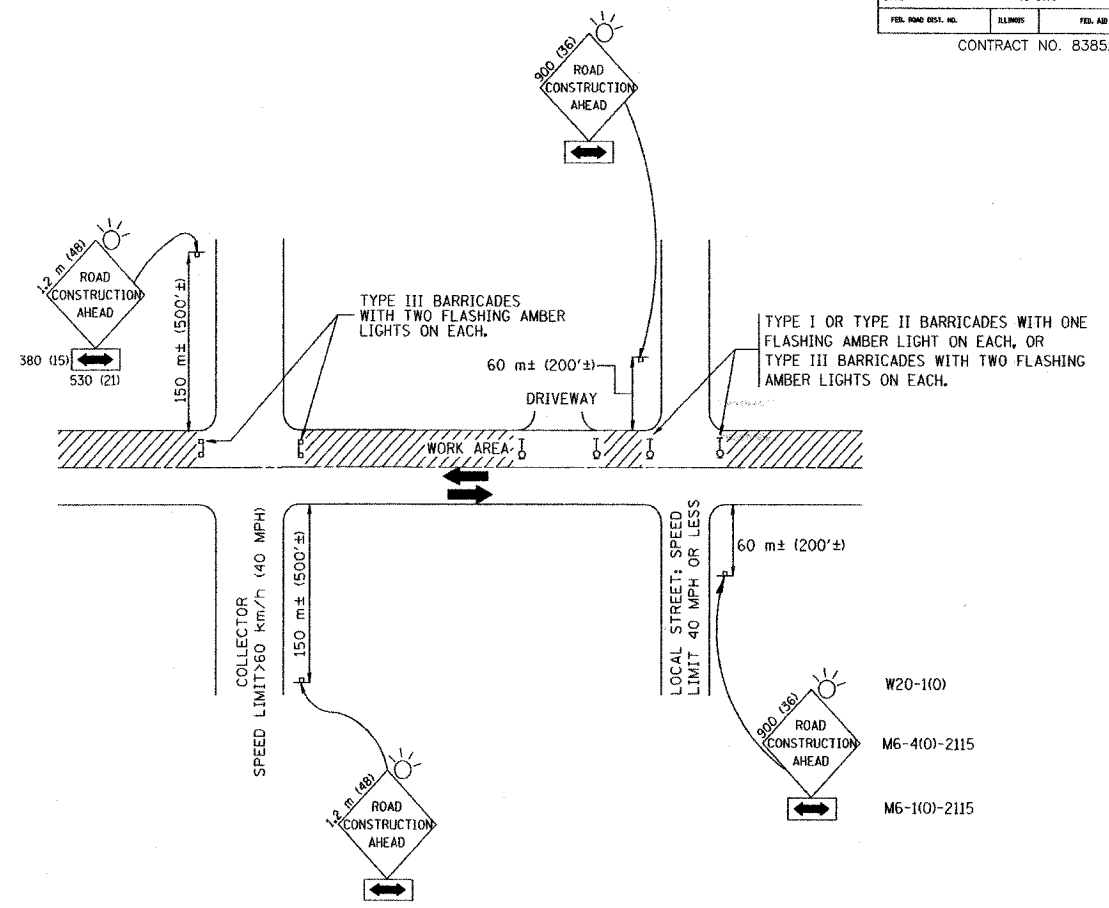
ILLINOIS DEPARTMENT OF TRANSPORTATION
TEMPORARY INFORMATION SIGNING

03/13/00
DRAWN BY: BUR. OF DESIGN
CHECKED BY:

Mon_Mar_13_13:55:53_2000...x:\efgfiles\CAD\ocf...wi\prk\metric\tc22.mprk

| | | | | |
|---------------------|----------------|------------------|--------------|----------|
| F. A. U. FILE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO |
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 20 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

CONTRACT NO. 83852



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS:
- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL AND PROTECTION
 FOR
 SIDE ROADS, INTERSECTIONS, AND
 DRIVEWAYS

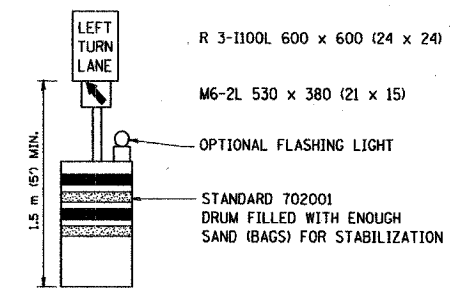
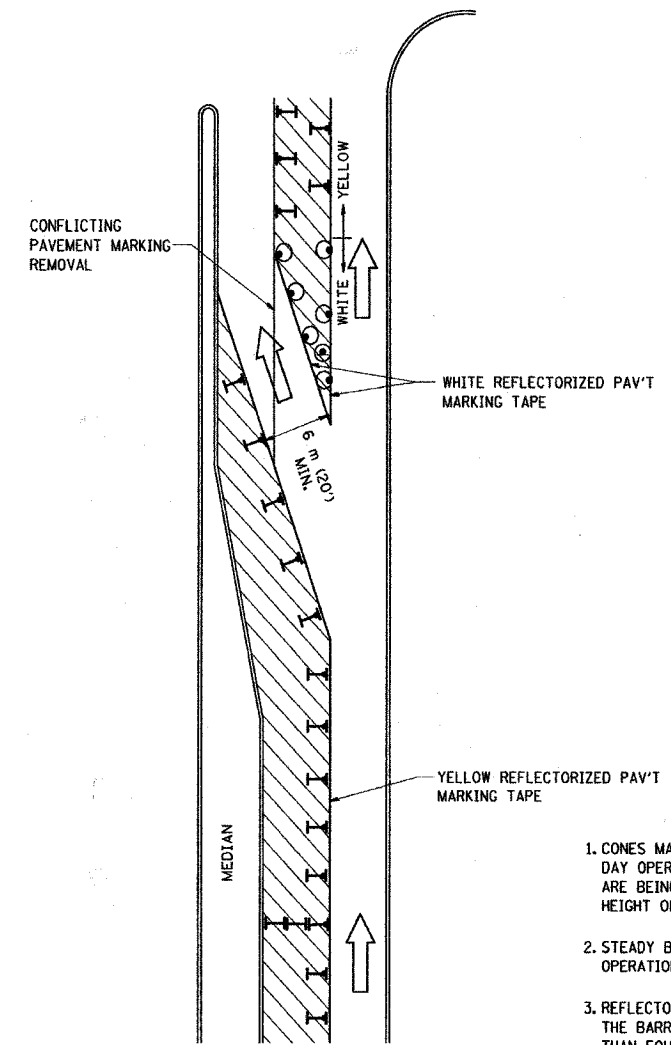
| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| LHA | 6/89 |
| T. RAMMACHER | 09/08/94 |
| J. OBERLE | 10/18/95 |
| A. HOUSEH | 03/06/96 |
| A. HOUSEH | 10/15/96 |
| T. RAMMACHER | 01/06/00 |

SCALE: NONE
 DATE: 03/13/00
 DRAWN BY
 CHECKED BY

Mon_Mar_13_13:50:52_2000...v:\cf\files\CAD\gef...w\prk\metric\td\m.prk

| | | | | |
|---------------------|----------------|---------|--------------|------------------|
| S. A. C. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 21 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | SHEETS | | FED. AID PROJECT |

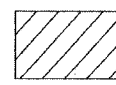
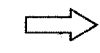




CONTRACT NO. 83852



GENERAL NOTES

- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 710 (28) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 1.5 m (5').
- STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
- THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 600 x 600 (24 x 24) AND M6-2R 530 x 380 (21 x 15) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
- FORM BT 725 IS REQUIRED.
- TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)

| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| T. RAMMACHER | 09/08/94 |
| A. HOUSEH | 11/07/95 |
| A. HOUSEH | 10/12/96 |
| T. RAMMACHER | 01/06/00 |

SCALE: NONE
DATE: 03/13/00
DRAWN BY
CHECKED BY LHA

Mon Mar 13 13:53:45 2000...w:\cadd\projects\diststd\tel14.dgn

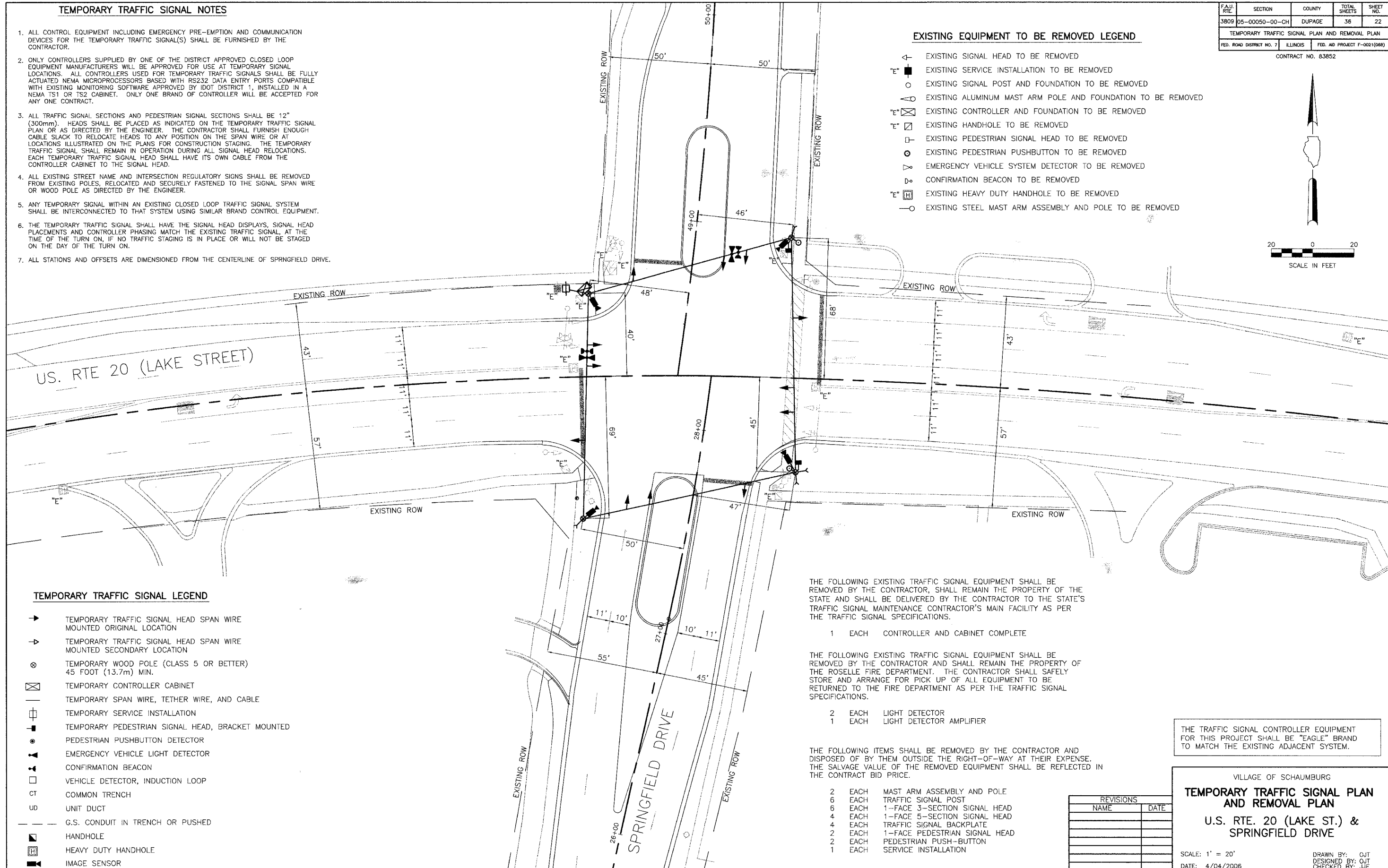
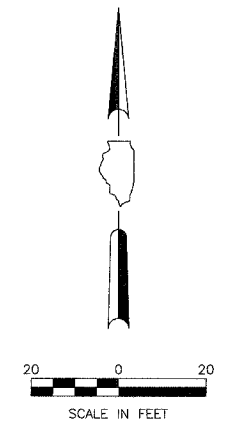
TEMPORARY TRAFFIC SIGNAL NOTES

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSORS BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12" (300mm). HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. ALL STATIONS AND OFFSETS ARE DIMENSIONED FROM THE CENTERLINE OF SPRNGFIELD DRIVE.

| F&J RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|----------------|------------------------------|--------------------|-----------|
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 22 |
| TEMPORARY TRAFFIC SIGNAL PLAN AND REMOVAL PLAN | | | | |
| FED. ROAD DISTRICT NO. 7 | ILLINOIS | FED. AID PROJECT F-0021(068) | CONTRACT NO. 83852 | |

EXISTING EQUIPMENT TO BE REMOVED LEGEND

- ⬆ EXISTING SIGNAL HEAD TO BE REMOVED
- EXISTING SERVICE INSTALLATION TO BE REMOVED
- EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED
- ⊖ EXISTING ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED
- ⊗ EXISTING CONTROLLER AND FOUNDATION TO BE REMOVED
- ⊠ EXISTING HANDHOLE TO BE REMOVED
- ⊡ EXISTING PEDESTRIAN SIGNAL HEAD TO BE REMOVED
- ⊙ EXISTING PEDESTRIAN PUSHBUTTON TO BE REMOVED
- ⚡ EXISTING EMERGENCY VEHICLE SYSTEM DETECTOR TO BE REMOVED
- ⊞ EXISTING CONFIRMATION BEACON TO BE REMOVED
- ⊠ EXISTING HEAVY DUTY HANDHOLE TO BE REMOVED
- ⊖ EXISTING STEEL MAST ARM ASSEMBLY AND POLE TO BE REMOVED



TEMPORARY TRAFFIC SIGNAL LEGEND

- ➔ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- ➡ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- ⊗ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MIN.
- ⊗ TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- ⊡ TEMPORARY SERVICE INSTALLATION
- ⊡ TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- PEDESTRIAN PUSHBUTTON DETECTOR
- ⚡ EMERGENCY VEHICLE LIGHT DETECTOR
- ⊞ CONFIRMATION BEACON
- VEHICLE DETECTOR, INDUCTION LOOP
- CT COMMON TRENCH
- UD UNIT DUCT
- G.S. CONDUIT IN TRENCH OR PUSHED
- ⊠ HANDHOLE
- ⊠ HEAVY DUTY HANDHOLE
- ⊠ IMAGE SENSOR

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE STATE AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE STATE'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- 1 EACH CONTROLLER AND CABINET COMPLETE

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE ROSELLE FIRE DEPARTMENT. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE FIRE DEPARTMENT AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- 2 EACH LIGHT DETECTOR
- 1 EACH LIGHT DETECTOR AMPLIFIER

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 2 EACH MAST ARM ASSEMBLY AND POLE
- 6 EACH TRAFFIC SIGNAL POST
- 6 EACH 1-FACE 3-SECTION SIGNAL HEAD
- 4 EACH 1-FACE 5-SECTION SIGNAL HEAD
- 4 EACH TRAFFIC SIGNAL BACKPLATE
- 2 EACH 1-FACE PEDESTRIAN SIGNAL HEAD
- 2 EACH PEDESTRIAN PUSH-BUTTON
- 1 EACH SERVICE INSTALLATION

THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" BRAND TO MATCH THE EXISTING ADJACENT SYSTEM.

VILLAGE OF SCHAUMBURG
TEMPORARY TRAFFIC SIGNAL PLAN AND REMOVAL PLAN
 U.S. RTE. 20 (LAKE ST.) & SPRINGFIELD DRIVE

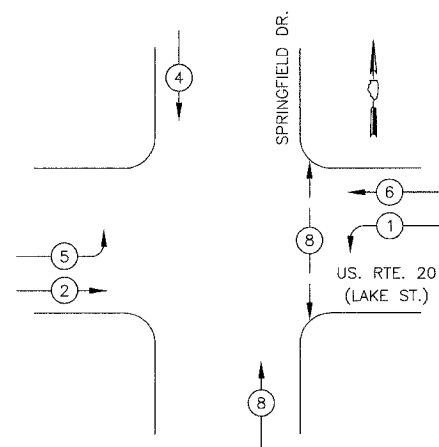
SCALE: 1" = 20'
 DATE: 4/04/2006

DRAWN BY: OJT
 DESIGNED BY: OJT
 CHECKED BY: JJE

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |

CONTRACT NO. 83852

TEMPORARY CONTROLLER SEQUENCE

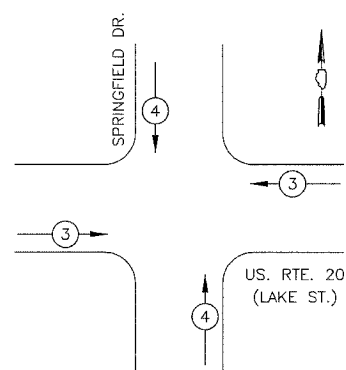


LEGEND

- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- OVERLAP
- PEDESTRIAN MOVEMENT
- * NUMBER REFERS TO ASSOCIATED PHASE

PHASE DESIGNATION DIAGRAM

EMERGENCY VEHICLE PREEMPTION SEQUENCE

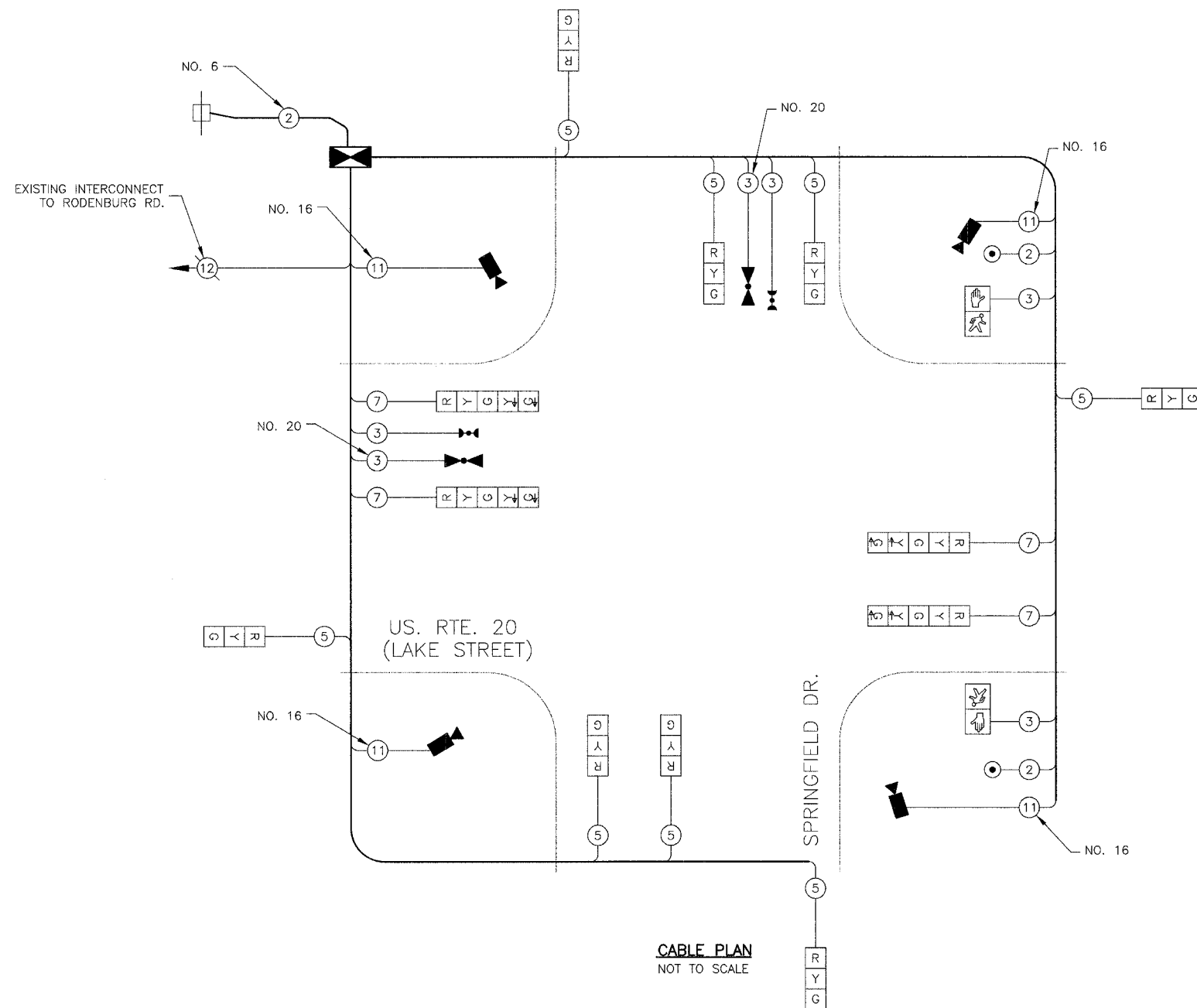


| PROPOSED EMERGENCY VEHICLE PREEMPTORS | | |
|---------------------------------------|---|---|
| EMERGENCY VEHICLE PREEMPTOR | 3 | 4 |
| MOVEMENT | | |

TEMPORARY CABLE PLAN LEGEND

- TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12" (300mm)
- CONTROLLER CABINET
- SERVICE INSTALLATION
- INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED
- CONFIRMATION BEACON
- EMERGENCY VEHICLE LIGHT DETECTOR
- VEHICLE DETECTOR, INDUCTION LOOP
- PEDESTRIAN PUSHBUTTON DETECTOR
- 12" (300mm) PEDESTRIAN SIGNAL SECTION
- IMAGE SENSOR

NOTE:
THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND SHALL EXTEND INTO THE CONTROLLER CABINET



CABLE PLAN
NOT TO SCALE

| I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS | | | | | TOTAL WATTAGE |
|--|-----------|--------------------|-----|-------------|---------------|
| | NO. LAMPS | WATTAGE INCAND. | LED | % OPERATION | |
| SIGNAL (RED) | 12 | 135 | | 0.50 | 810 |
| (YELLOW) | 12 | 135 | | 0.25 | 405 |
| (GREEN) | 12 | 135 | | 0.25 | 405 |
| ARROW | 8 | 135 | | 0.10 | 108 |
| PED. SIGNAL | 2 | 90 | | 1.00 | 180 |
| CONTROLLER | 1 | 100 | | 1.00 | 100 |
| FLASHER | | | | 0.50 | |
| TOTAL = | | | | | 2008 |

ENERGY COSTS TO: ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, IL 60196-1096

ENERGY SUPPLY: CONTACT: MICHAEL BELL
PHONE: 630-691-4529
COMPANY: COM ED

THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" BRAND TO MATCH THE EXISTING ADJACENT SYSTEM.

VILLAGE OF SCHAUMBURG TEMPORARY CABLE PLAN AND TEMPORARY PHASE DESIGNATION DIAGRAM U.S. RTE. 20 (LAKE ST.) & SPRINGFIELD DRIVE

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

NOT TO SCALE
DATE: 4/04/2006

DRAWN BY: OJT
DESIGNED BY: OJT
CHECKED BY: JUE

INTERCONNECT PLAN LEGEND

| | EXISTING | PROPOSED |
|----------------------------------|----------|----------|
| CONTROLLER | | |
| HANDHOLE | | |
| HEAVY DUTY HANDHOLE | | |
| DOUBLE HANDHOLE | | |
| G.S. CONDUIT IN TRENCH OR PUSHED | | |
| DETECTOR LOOP | | |
| COMMON TRENCH | | |
| UNIT DUCT | | |
| SYSTEM | | |
| INTERSECTION | | |

| F.A.L. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------------|----------------|------------------------------|--------------|-----------|
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 24 |
| TEMPORARY INTERCONNECT PLAN | | | | |
| FED. ROAD DISTRICT NO. 7 | ILLINOIS | FED. AID PROJECT F-0021(098) | | |
| CONTRACT NO. 83852 | | | | |



THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" BRAND TO MATCH THE EXISTING ADJACENT SYSTEM.

VILLAGE OF SCHAUMBURG

TEMPORARY INTERCONNECT PLAN

U.S. RTE. 20 (LAKE ST.)
RODENBURG RD. TO SPRINGFIELD DR.

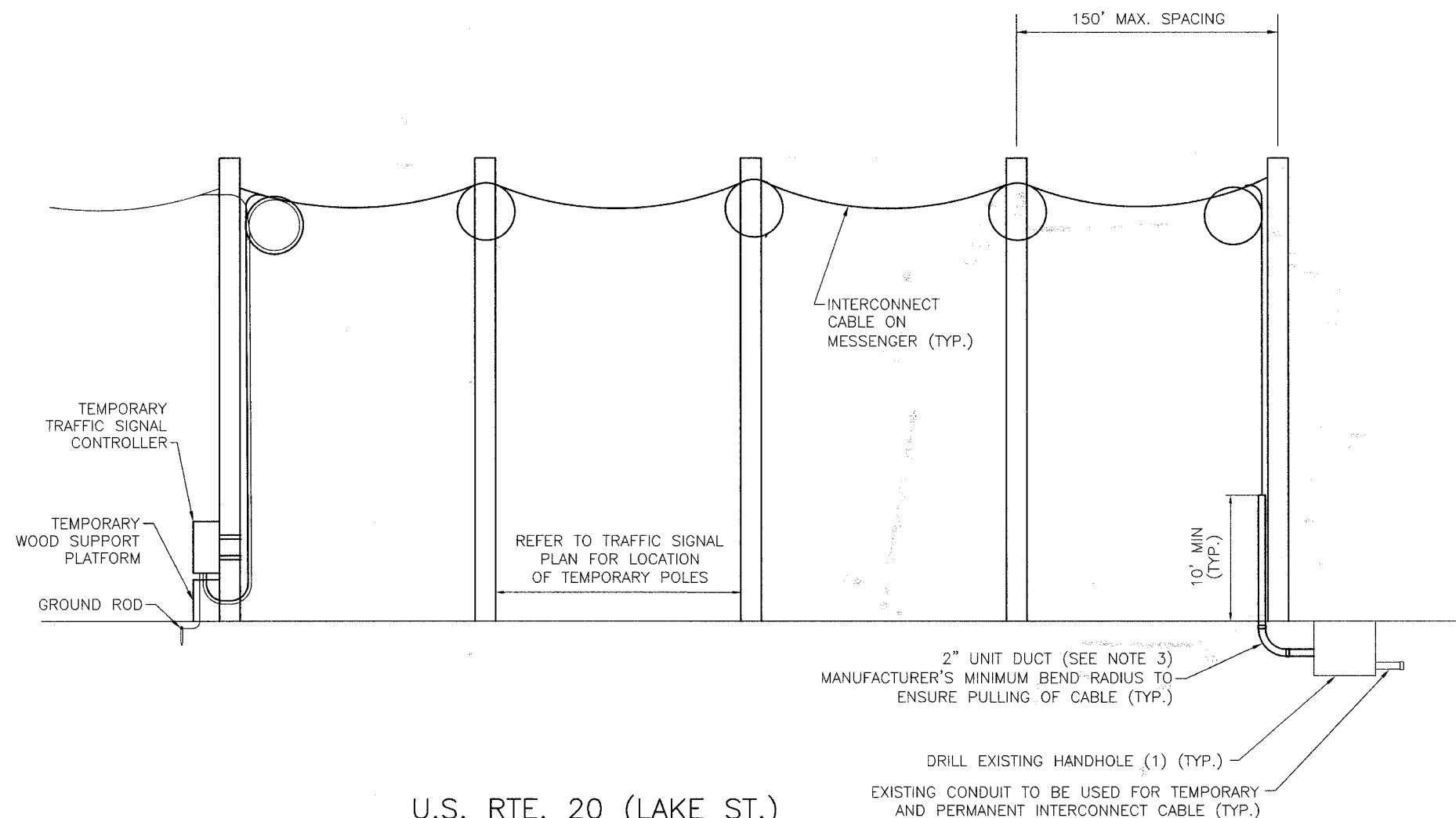
SCALE: 1"=50'
DATE: 4/04/2006

DRAWN BY: OJT
DESIGNED BY: OJT
CHECKED BY: JJE

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |

4/4/2006 10:58 AM
 C:\PROJECTS\3809\3809-05-00050-00-CH\3809-05-00050-00-CH-24.dwg
 2/18/06 10:58 AM
 JJE

| | | | | |
|--|----------------|------------------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 25 |
| TEMPORARY TRAFFIC SIGNAL INTERCONNECT DETAIL | | | | |
| FED. ROAD DISTRICT NO. 7 | ILLINOIS | FED. AID PROJECT F-0021(068) | | |
| CONTRACT NO. 83852 | | | | |



U.S. RTE. 20 (LAKE ST.)
TEMPORARY INTERCONNECT DETAIL
 (NOT TO SCALE)

NOTES :

- 1) TRANSFER OF EXISTING TO TEMPORARY INTERCONNECT CABLE AND TEMPORARY INTERCONNECT CABLE TO PROPOSED MUST BE COMPLETED IN ONE (1) WORKING DAY DURING NON-PEAK HOURS OR AS DIRECTED BY THE ENGINEER.
- 2) AFTER PROPOSED INTERCONNECT IS TURNED ON, THE EXISTING INTERCONNECT CABLE SHALL BE REMOVED.
- 3) THE 2" UNIT DUCT USED FOR THE TEMPORARY INTERCONNECT CABLE SHALL BE REMOVED TO BELOW THE GROUND LEVEL AND CAPPED AT THE TIME THE TEMPORARY TRAFFIC SIGNAL IS REMOVED. THE UNIT DUCT, AS WELL AS ALL WORK ASSOCIATED WITH THE INSTALLATION AND REMOVAL OF SAME, SHALL BE CONSIDERED INCIDENTAL TO THE PAY ITEM, "TEMPORARY TRAFFIC SIGNAL INSTALLATION".
- 4) CONTRACTOR MUST NOTIFY IDOT SIGNAL SYSTEM ENGINEER A MINIMUM OF SEVEN (7) WORKING DAYS PRIOR TO THE START OF ANY WORK ON THE INTERCONNECT SIGNAL SYSTEM.
- 5) THE EXISTING FIBER OPTIC INTERCONNECT CABLE MAY BE SPLICED AND USED AS NECESSARY FOR A SPAN WIRE SUPPORTED TEMPORARY CONDITION, BUT MAY NOT BE REUSED FOR THE PERMANENT INSTALLATION.
- 6) ALL ADDITIONAL WOOD POLES, CABLES, ETC. USED IN THE INSTALLATION OF THE TEMPORARY INTERCONNECT SHALL BE CONSIDERED INCIDENTAL TO THE PAY ITEM, "TEMPORARY TRAFFIC SIGNAL INSTALLATION".

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

VILLAGE OF SCHAUMBURG
**TEMPORARY TRAFFIC SIGNAL
 INTERCONNECT DETAIL**
 U.S. RTE. 20 (LAKE ST.) &
 SPRINGFIELD DRIVE

SCALE: NOT TO SCALE
 DATE: 4/04/2006

DRAWN BY: OJT
 DESIGNED BY: OJT
 CHECKED BY: JUE

| | | | | |
|----------------------------------|----------------|----------|------------------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 26 |
| TRAFFIC SIGNAL INSTALLATION PLAN | | | | |
| FED. ROAD DISTRICT NO. 7 | | ILLINOIS | FED. AID PROJECT F-0021(068) | |
| CONTRACT NO. 83852 | | | | |

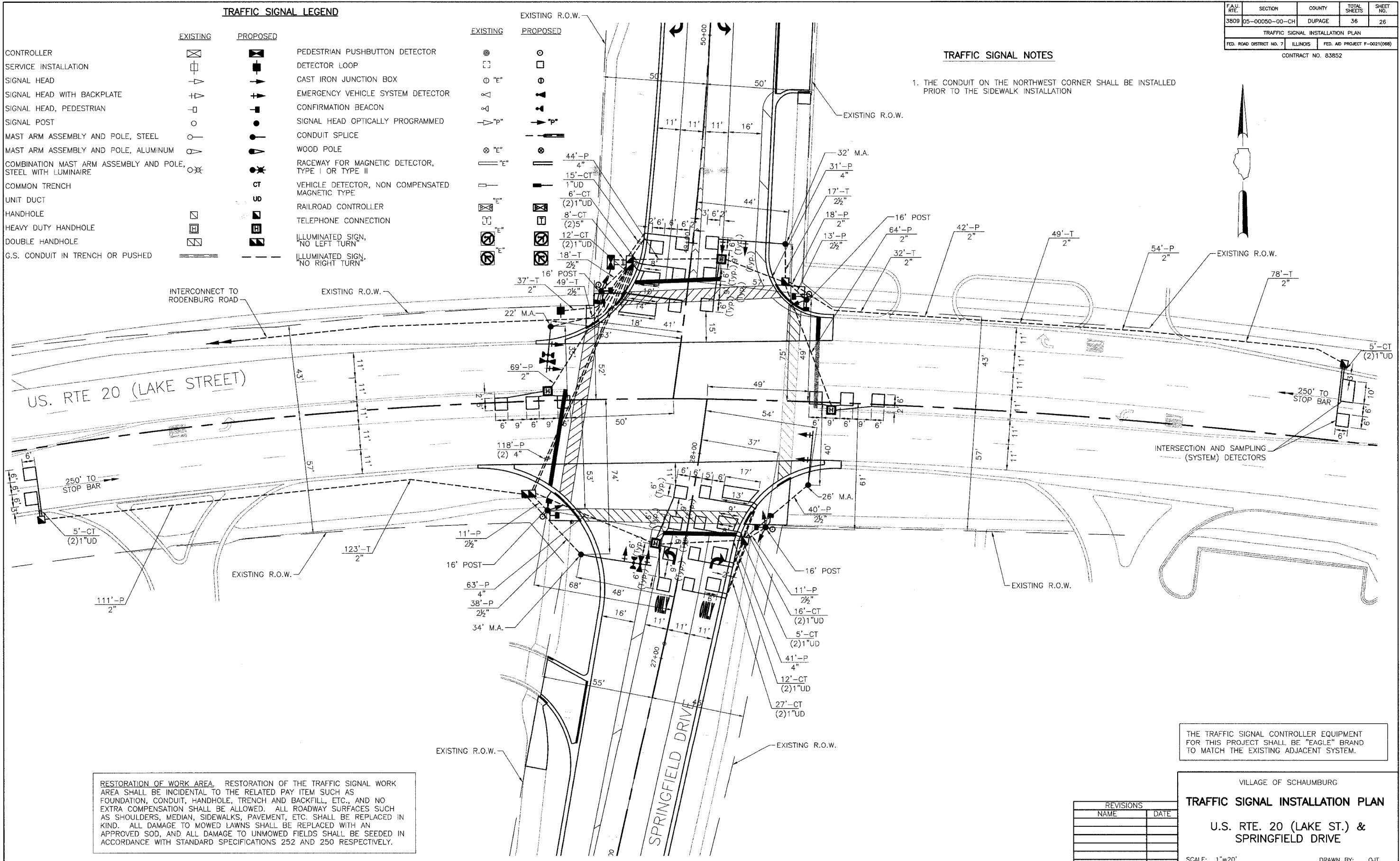
TRAFFIC SIGNAL NOTES

1. THE CONDUIT ON THE NORTHWEST CORNER SHALL BE INSTALLED PRIOR TO THE SIDEWALK INSTALLATION



TRAFFIC SIGNAL LEGEND

| EXISTING | PROPOSED | EXISTING | PROPOSED |
|----------|----------|----------|----------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |



RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" BRAND TO MATCH THE EXISTING ADJACENT SYSTEM.

VILLAGE OF SCHAUMBURG
TRAFFIC SIGNAL INSTALLATION PLAN
 U.S. RTE. 20 (LAKE ST.) & SPRINGFIELD DRIVE

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

SCALE: 1"=20'
 DATE: 4/04/2006

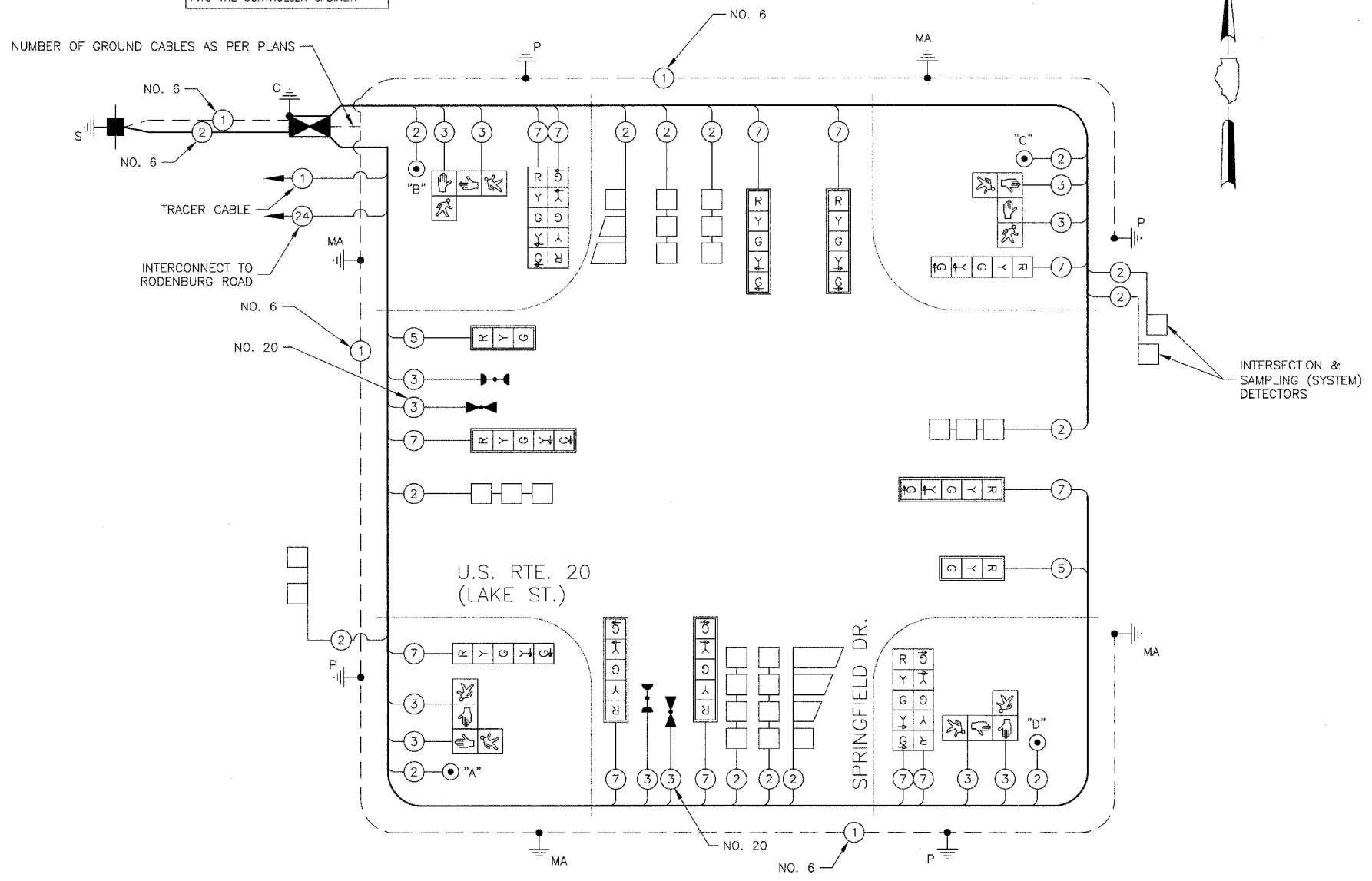
DRAWN BY: OJT
 DESIGNED BY: OJT
 CHECKED BY: JJE

ALL DIMENSIONS UNLESS OTHERWISE NOTED ARE IN FEET AND INCHES. DIMENSIONS SHALL BE TO FACE UNLESS OTHERWISE NOTED.

CABLE PLAN LEGEND

| EXISTING | PROPOSED | |
|----------|----------|--|
| | | 8" (200mm) TRAFFIC SIGNAL SECTION |
| | | 12" (300mm) TRAFFIC SIGNAL SECTION |
| | | 12" (300mm) PEDESTRIAN SIGNAL SECTION |
| | | CONTROLLER CABINET |
| | | SERVICE INSTALLATION |
| | | TELEPHONE CONNECTION |
| | | VEHICLE DETECTOR, INDUCTION LOOP |
| | | MAGNETIC DETECTOR |
| | | EMERGENCY VEHICLE LIGHT DETECTOR |
| | | CONFIRMATION BEACON |
| | | PUSHBUTTON DETECTOR |
| | | DENOTES NUMBER OF CONDUCTORS ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED. |
| | | GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN) |
| | | FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 2-MM12F SM12F |
| | | SIGNAL FACE WITH BACKPLATE "P" INDICATES PROGRAMMED HEAD. |
| | | RAILROAD CONTROL CABINET |
| | | ILLUMINATED SIGN, "NO LEFT TURN" |
| | | ILLUMINATED SIGN, "NO RIGHT TURN" |
| | | GROUND ROD AT HANDHOLE (H), DOUBLE HANDHOLE (H), OR CONTROLLER (C) |
| | | GROUND ROD AT POST (P) OR MAST ARM POLE (MA) |
| | | GROUND ROD AT ELECTRIC SERVICE INSTALLATION |

NOTE:
THE END OF THE TRACER CABLE
SHALL BE CONTINUOUS AND EXTEND
INTO THE CONTROLLER CABINET.



CABLE PLAN
NOT TO SCALE

NOTE:
PUSHBUTTON A SHALL PLACE A CALL TO PHASES 2 & 4
PUSHBUTTON B SHALL PLACE A CALL TO PHASES 4 & 6
PUSHBUTTON C SHALL PLACE A CALL TO PHASES 6 & 8
PUSHBUTTON D SHALL PLACE A CALL TO PHASES 8 & 2

THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT
FOR THIS PROJECT SHALL BE "EAGLE" BRAND
TO MATCH THE EXISTING ADJACENT SYSTEM.

| I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS | | | | | TOTAL WATTAGE |
|--|-----------|--------------------|-----|-------------|------------------|
| TYPE | NO. LAMPS | WATTAGE INCAND. | LED | % OPERATION | |
| SIGNAL (RED) | 14 | | 17 | 0.50 | 119 |
| (YELLOW) | 14 | | 25 | 0.25 | 88 |
| (GREEN) | 14 | | 15 | 0.25 | 53 |
| ARROW | 24 | | 12 | 0.10 | 29 |
| PED. SIGNAL | 8 | | 25 | 1.00 | 200 |
| CONTROLLER | 1 | | 100 | 1.00 | 100 |
| FLASHER | | | | 0.50 | |
| TOTAL = | | | | | 589 |

| FOUNDATION (DEPTH) | FT. (m) | CABLE SLACK | FT. (m) | VERTICAL | FT. (m) |
|--------------------|----------|------------------|-----------|-------------------|-----------------------------------|
| TYPE A - POST | 4 (1.2) | HANDHOLE | 6.5 (2.0) | ALL FOUNDATIONS | 3.5 (1.0) |
| D - CONTROLLER | 4 (1.2) | DOUBLE HANDHOLE | 13 (4.0) | MAST ARM (L) POLE | 20 + L - 2 = (6m + L - 0.6m) = |
| E - M. ARM POLE | | SIGNAL POST | 2 (1.0) | BRACKET MOUNTED | 13 (4.0) |
| 24" (600mm) | 10 (3.0) | CONTROLLER CAB. | 1 (0.5) | PED. PUSHBUTTON | 4 (1.2) |
| 30" (750mm) | 15 (4.6) | FIBER OPTIC | 13 (4.0) | ELECTRIC SERVICE | 13.5 (4.1) |
| 36" (900mm) | 15 (4.6) | ELECTRIC SERVICE | 1 (0.5) | SERVICE TO GROUND | 13.5 (4.1) |
| | | GROUND CABLE | 1 (0.5) | POST MOUNTED | 6 (1.8) |

ENERGY COSTS TO: ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, IL 60196-1096
CONTACT: MICHAEL BELL
PHONE: 630-691-4529
COMPANY: COM ED

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |

VILLAGE OF SCHAUMBURG

CABLE PLAN

U.S. RTE. 20 (LAKE ST.) &
SPRINGFIELD DRIVE

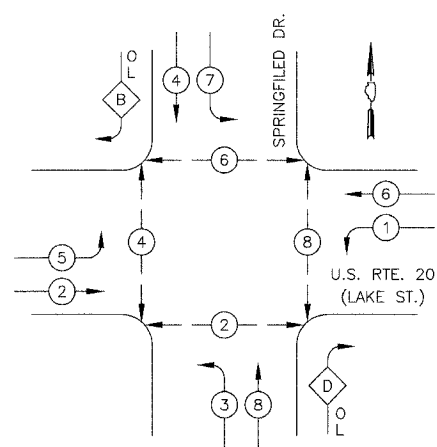
NOT TO SCALE
DATE: 4/04/2006

DRAWN BY: OJT
DESIGNED BY: OJT
CHECKED BY: JUE

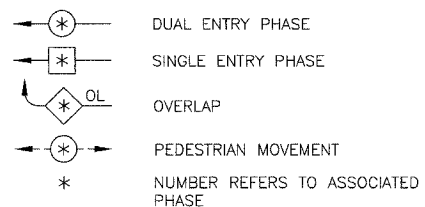
| | | | | |
|--|----------------|------------------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 28 |
| SCHEDULE OF QUANTITIES AND PHASE DESIGNATION DIAGRAM | | | | |
| FED. ROAD DISTRICT NO. 7 | ILLINOIS | FED. AID PROJECT F-0021(068) | | |

CONTRACT NO. 83852

CONTROLLER SEQUENCE



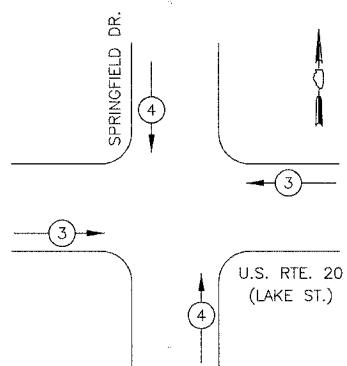
LEGEND



PHASE DESIGNATION DIAGRAM

| RIGHT TURN OVERLAP PHASE DESIGNATION | | | | |
|--------------------------------------|---|------------------|---|-----------------|
| OVERLAP LETTER | = | PERMISSIVE PHASE | + | PROTECTED PHASE |
| B | = | 4 | + | 5 |
| D | = | 8 | + | 1 |

EMERGENCY VEHICLE PREEMPTION SEQUENCE



| PROPOSED EMERGENCY VEHICLE PREEMPTORS | | |
|---------------------------------------|---|---|
| EMERGENCY VEHICLE PREEMPTOR | 3 | 4 |
| MOVEMENT | | |

TRAFFIC SIGNAL SCHEDULE OF QUANTITIES

| DESCRIPTION | UNIT | QNTY. |
|--|-------|-------|
| SIGN PANEL- TYPE 1 | SQ FT | 18 |
| SIGN PANEL- TYPE 2 | SQ FT | 25 |
| CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL | FOOT | 319 |
| CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL | FOOT | 84 |
| CONDUIT IN TRENCH, 5" DIA., GALVANIZED STEEL | FOOT | 16 |
| CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL | FOOT | 358 |
| CONDUIT PUSHED, 2 1/2" DIA., GALVANIZED STEEL | FOOT | 113 |
| CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL | FOOT | 474 |
| HANDHOLE | EACH | 4 |
| HEAVY-DUTY HANDHOLE | EACH | 4 |
| DOUBLE HANDHOLE | EACH | 2 |
| TRENCH AND BACKFILL FOR ELECTRICAL WORK | FOOT | 411 |
| FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL | EACH | 1 |
| TRANSCEIVER-FIBER OPTIC | EACH | 1 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C | FOOT | 660 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C | FOOT | 1730 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | FOOT | 459 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | FOOT | 2398 |
| ELECTRIC CABLE IN CONDUIT, LEAD-IN NO. 14 1PAIR | FOOT | 2368 |
| ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C | FOOT | 61 |
| TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT. | EACH | 4 |
| STEEL MAST ARM ASSEMBLY AND POLE, 22 FT. | EACH | 1 |
| STEEL MAST ARM ASSEMBLY AND POLE, 26 FT. | EACH | 1 |
| STEEL MAST ARM ASSEMBLY AND POLE, 32 FT. | EACH | 1 |
| STEEL MAST ARM ASSEMBLY AND POLE, 34 FT. | EACH | 1 |
| CONCRETE FOUNDATION, TYPE A | FOOT | 16 |
| CONCRETE FOUNDATION, TYPE D | FOOT | 4 |
| CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER | FOOT | 60 |
| TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM | EACH | 8 |
| INDUCTIVE LOOP DETECTOR | EACH | 11 |
| DETECTOR LOOP, TYPE 1 | FOOT | 1079 |
| LIGHT DETECTOR | EACH | 2 |
| LIGHT DETECTOR AMPLIFIER | EACH | 1 |
| PEDESTRIAN PUSH-BUTTON | EACH | 4 |
| TEMPORARY TRAFFIC SIGNAL INSTALLATION | EACH | 1 |
| REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT | EACH | 1 |
| REMOVE EXISTING HANDHOLE | EACH | 9 |
| REMOVE EXISTING CONCRETE FOUNDATION | EACH | 9 |
| SERVICE INSTALLATION - POLE MOUNTED | EACH | 1 |
| ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C | FOOT | 767 |
| ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED | FOOT | 368 |
| SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, MAST ARM MOUNTED | EACH | 2 |
| SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, MAST ARM MOUNTED | EACH | 6 |
| SIGNAL HEAD, L.E.D., 2-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 2 |
| SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 2 |
| PEDESTRIAN SIGNAL HEAD, L.E.D., 2-FACE, BRACKET MOUNTED | EACH | 4 |

THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" BRAND TO MATCH THE EXISTING ADJACENT SYSTEM.

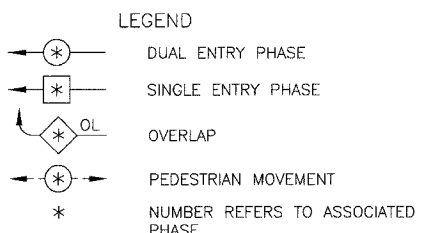
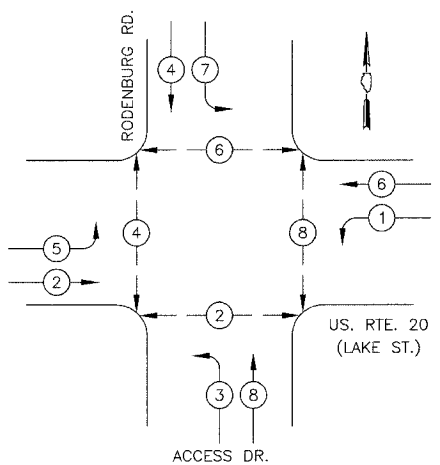
VILLAGE OF SCHAUMBURG SCHEDULE OF QUANTITIES AND PHASE DESIGNATION DIAGRAM U.S. RTE. 20 (LAKE ST.) & SPRINGFIELD DRIVE

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

NOT TO SCALE
DATE: 4/04/2006

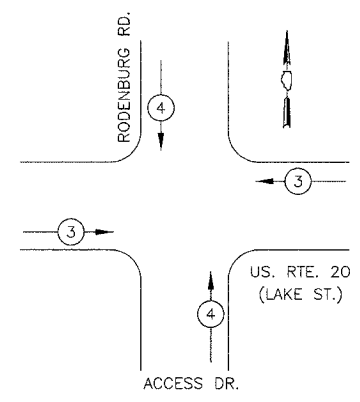
DRAWN BY: OJT
DESIGNED BY: OJT
CHECKED BY: JUE

CONTROLLER SEQUENCE



PHASE DESIGNATION DIAGRAM

EMERGENCY VEHICLE PREEMPTION SEQUENCE

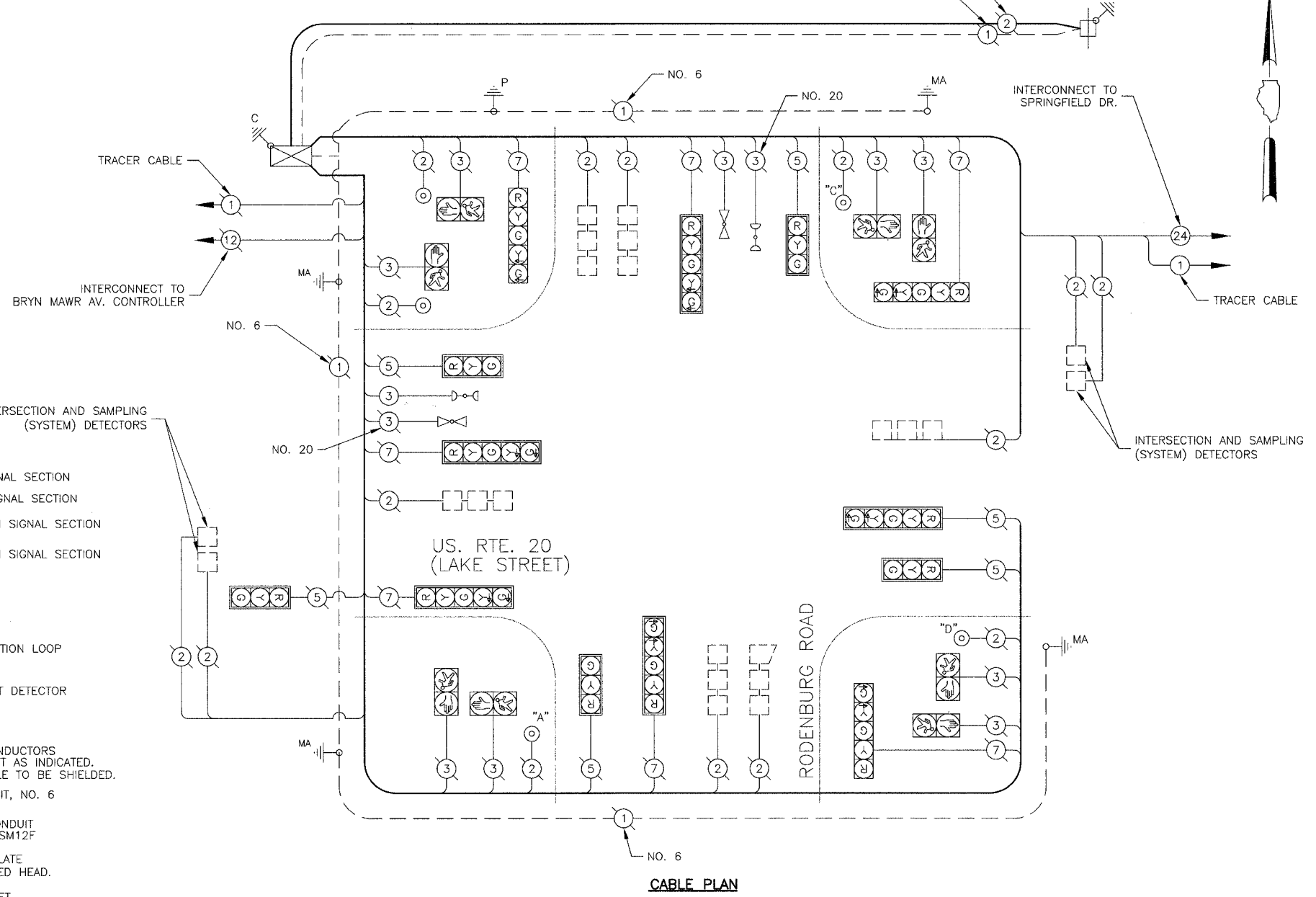


| PROPOSED EMERGENCY VEHICLE PREEMPTORS | | |
|---------------------------------------|---|---|
| EMERGENCY VEHICLE PREEMPTOR | 3 | 4 |
| MOVEMENT | | |

CABLE PLAN LEGEND

| EXISTING | PROPOSED | DESCRIPTION |
|----------|----------|---|
| | | 8" (200mm) TRAFFIC SIGNAL SECTION |
| | | 12" (300mm) TRAFFIC SIGNAL SECTION |
| | | 12" (300mm) PEDESTRIAN SIGNAL SECTION |
| | | 12" (300mm) PEDESTRIAN SIGNAL SECTION |
| | | CONTROLLER CABINET |
| | | SERVICE INSTALLATION |
| | | TELEPHONE CONNECTION |
| | | VEHICLE DETECTOR, INDUCTION LOOP |
| | | MAGNETIC DETECTOR |
| | | EMERGENCY VEHICLE LIGHT DETECTOR |
| | | CONFIRMATION BEACON |
| | | PUSHBUTTON DETECTOR |
| | | DENOTES NUMBER OF CONDUCTORS. ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED. |
| | | GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN) |
| | | FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 2-MM12F SM12F |
| | | SIGNAL FACE WITH BACKPLATE |
| | | "P" INDICATES PROGRAMMED HEAD. |
| | | RAILROAD CONTROL CABINET |
| | | ILLUMINATED SIGN, "NO LEFT TURN" |
| | | ILLUMINATED SIGN, "NO RIGHT TURN" |
| | | GROUND ROD AT HANDHOLE (H), DOUBLE HANDHOLE (H), OR CONTROLLER (C) |
| | | GROUND ROD AT POST (P) OR MAST ARM POLE (MA) |
| | | GROUND ROD AT ELECTRIC SERVICE INSTALLATION |

NOTE:
THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.



CABLE PLAN
NOT TO SCALE

NOTE:
PUSHBUTTON A SHALL PLACE A CALL TO PHASES 2 & 4
PUSHBUTTON C SHALL PLACE A CALL TO PHASES 6 & 8
PUSHBUTTON D SHALL PLACE A CALL TO PHASES 8 & 2

TRAFFIC SIGNAL SCHEDULE OF QUANTITIES

| DESCRIPTION | UNIT | QNTY. |
|---|------|-------|
| MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION | EACH | 1 |

| I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS | | | | | TOTAL WATTAGE |
|--|-----------|-----------------|-------------|-------------|---------------|
| TYPE | NO. LAMPS | WATTAGE INCAND. | WATTAGE LED | % OPERATION | |
| SIGNAL (RED) | 13 | 135 | 17 | 0.50 | 110.5 |
| (YELLOW) | 13 | 135 | 25 | 0.25 | 81.25 |
| (GREEN) | 13 | 135 | 15 | 0.25 | 48.75 |
| ARROW | 16 | 135 | 12 | 0.10 | 19.2 |
| PED. SIGNAL | 8 | 90 | 25 | 1.00 | 72.0 |
| CONTROLLER | 1 | 100 | 100 | 1.00 | 100 |
| ILLUM. SIGN | | | | | |
| FLASHER | | | | 0.50 | |
| TOTAL = | | | | | 431.7 |

ENERGY COSTS TO: ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, IL 60196-1096

ENERGY SUPPLY: CONTACT: MICHAEL BELL
PHONE: 630-691-4529
COMPANY: COM ED

| FOUNDATION (DEPTH) | FT. (m) | CABLE SLACK | FT. (m) | VERTICAL | FT. (m) |
|--------------------|----------|------------------|-----------|-------------------|----------------------|
| TYPE A - POST | 4 (1.2) | HANDHOLE | 6.5 (2.0) | ALL FOUNDATIONS | 3.5 (1.0) |
| D - CONTROLLER | 4 (1.2) | DOUBLE HANDHOLE | 13 (4.0) | MAST ARM (L) POLE | 20+L-2= (6m+L-0.6m)= |
| E - M. ARM POLE | | SIGNAL POST | 2 (1.0) | BRACKET MOUNTED | 13 (4.0) |
| 24" (600mm) | 10 (3.0) | CONTROLLER CAB. | 1 (0.5) | PED. PUSHBUTTON | 4 (1.2) |
| 30" (750mm) | 15 (4.6) | FIBER OPTIC | 13 (4.0) | ELECTRIC SERVICE | 13.5 (4.1) |
| 36" (900mm) | 15 (4.6) | ELECTRIC SERVICE | 1 (0.5) | SERVICE TO GROUND | 13.5 (4.1) |
| | | GROUND CABLE | 1 (0.5) | POST MOUNTED | 6 (1.8) |

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |

VILLAGE OF SCHAUMBURG

EXISTING CABLE PLAN AND PHASE DESIGNATION DIAGRAM

U.S. RTE. 20 (LAKE ST.) & RODENBURG ROAD

NOT TO SCALE

DATE: 4/04/2006

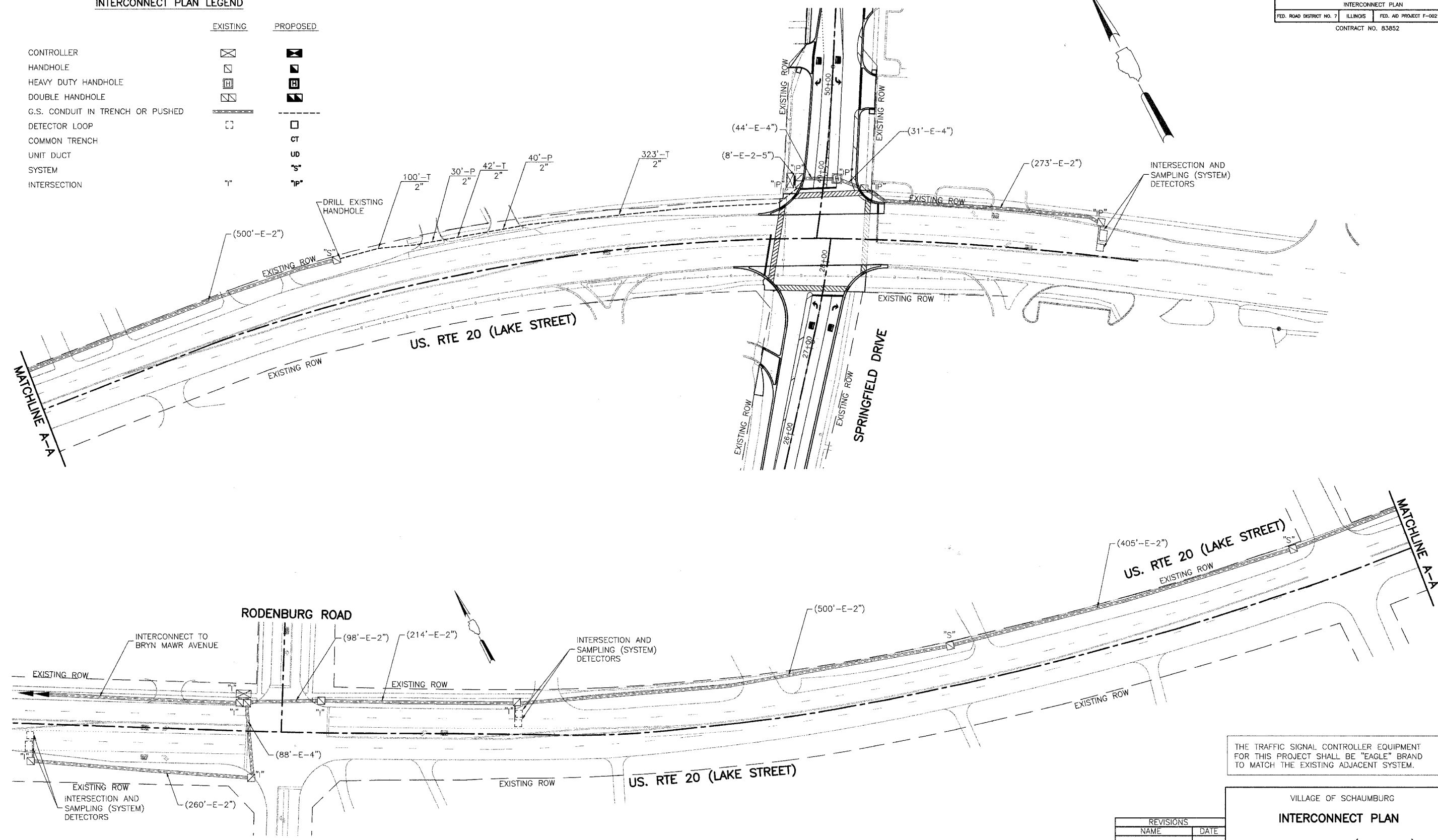
DRAWN BY: OJT
DESIGNED BY: OJT
CHECKED BY: JUE

ALL DIMENSIONS UNLESS OTHERWISE SPECIFIED ARE IN METERS AND MILLIMETERS.

| | | | | |
|--------------------------|----------------|------------------------------|--------------|-----------|
| FAU. RYE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 30 |
| INTERCONNECT PLAN | | | | |
| FED. ROAD DISTRICT NO. 7 | ILLINOIS | FED. AID PROJECT F-0021(068) | | |
| CONTRACT NO. 83852 | | | | |

INTERCONNECT PLAN LEGEND

| | EXISTING | PROPOSED |
|----------------------------------|----------|----------|
| CONTROLLER | | |
| HANDHOLE | | |
| HEAVY DUTY HANDHOLE | | |
| DOUBLE HANDHOLE | | |
| G.S. CONDUIT IN TRENCH OR PUSHED | | |
| DETECTOR LOOP | | |
| COMMON TRENCH | | |
| UNIT DUCT | | |
| SYSTEM | | |
| INTERSECTION | | |



THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" BRAND TO MATCH THE EXISTING ADJACENT SYSTEM.

VILLAGE OF SCHAUMBURG

INTERCONNECT PLAN

U.S. RTE. 20 (LAKE ST.)
RODENBURG RD. TO SPRINGFIELD DR.

SCALE: 1"=50'
DATE: 4/04/2006

DRAWN BY: QJT
DESIGNED BY: QJT
CHECKED BY: JJE

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

4/04/2006 11:58 AM
 3809 05-00050-00-CH
 30 30
 QJT

| | | | | |
|---|----------------|-----------------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 31 |
| INTERCONNECT SCHEMATIC AND SCHEDULE OF QUANTITIES | | | | |
| FED. ROAD DISTRICT NO. 7 | ILLINOIS | FED. AID PROJECT F-021(046) | | |

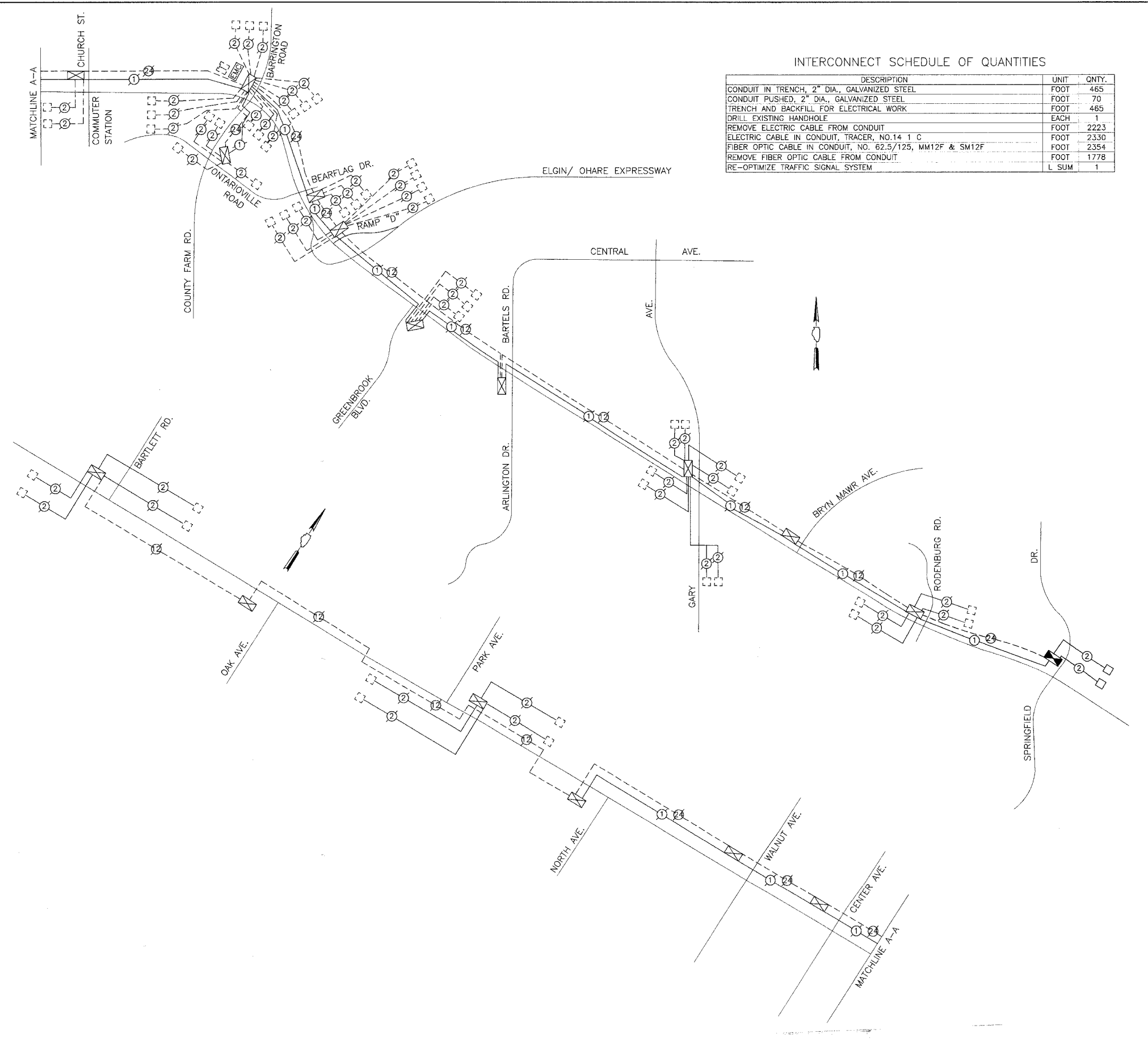
CONTRACT NO. 83852

INTERCONNECT SCHEDULE OF QUANTITIES

| DESCRIPTION | UNIT | QNTY. |
|---|-------|-------|
| CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL | FOOT | 465 |
| CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL | FOOT | 70 |
| TRENCH AND BACKFILL FOR ELECTRICAL WORK | FOOT | 465 |
| DRILL EXISTING HANDHOLE | EACH | 1 |
| REMOVE ELECTRIC CABLE FROM CONDUIT | FOOT | 2223 |
| ELECTRIC CABLE IN CONDUIT, TRACER, NO.14 1 C | FOOT | 2330 |
| FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F & SM12F | FOOT | 2354 |
| REMOVE FIBER OPTIC CABLE FROM CONDUIT | FOOT | 1778 |
| RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM | L SUM | 1 |

INTERCONNECT SCHEMATIC LEGEND

- EXISTING INTERSECTION CONTROLLER
- PROPOSED INTERSECTION CONTROLLER
- EXISTING MASTER CONTROLLER
- PROPOSED MASTER CONTROLLER
- MASTER MASTER CONTROLLER
- EXISTING INTERSECTION & SAMPLING (SYSTEM) DETECTORS
- PROPOSED INTERSECTION & SAMPLING (SYSTEM) DETECTORS
- EXISTING INTERSECTION DETECTORS
- PROPOSED SAMPLING (SYSTEM) DETECTORS
- EXISTING SAMPLING (SYSTEM) DETECTORS
- PROPOSED SAMPLING (SYSTEM) DETECTORS
- EXISTING SAMPLING (SYSTEM) DETECTORS PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTORS
- EXISTING SAMPLING (SYSTEM) DETECTORS PROPOSED SAMPLING (SYSTEM) DETECTORS
- EXISTING PREFORMED INTERSECTION & SAMPLING (SYSTEM) DETECTORS
- PROPOSED PREFORMED INTERSECTION & SAMPLING (SYSTEM) DETECTORS
- EXISTING SAMPLING (SYSTEM) PREFORMED DETECTORS
- PROPOSED SAMPLING (SYSTEM) PREFORMED DETECTORS
- EXISTING FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F
- PROPOSED FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F
- EXISTING INTERCONNECT CABLE - NO. 62.5/125 12F FIBER OPTIC CABLE
- PROPOSED INTERCONNECT CABLE - NO. 62.5/125 12F FIBER OPTIC CABLE
- EXISTING INTERCONNECT CABLE - NO. 18 3 PAIR TWISTED, SHIELDED
- PROPOSED INTERCONNECT CABLE - NO. 18 3 PAIR TWISTED, SHIELDED
- EXISTING LOOP DETECTOR CABLE - 2/C TWISTED, SHIELDED
- PROPOSED LOOP DETECTOR CABLE - 2/C TWISTED, SHIELDED
- EXISTING ELECTRIC CABLE 1/C (AS SPECIFIED)
- PROPOSED ELECTRIC CABLE 1/C (AS SPECIFIED)
- EXISTING TELEPHONE CONNECTION
- PROPOSED TELEPHONE CONNECTION



THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" BRAND TO MATCH THE EXISTING ADJACENT SYSTEM.

VILLAGE OF SCHAUMBURG
INTERCONNECT SCHEMATIC SCHEDULE OF QUANTITIES
 U.S. RTE. 20 (LAKE ST.)
 BARTLETT ROAD TO SPRINGFIELD DRIVE

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

NOT TO SCALE
 DATE: 4/04/2006

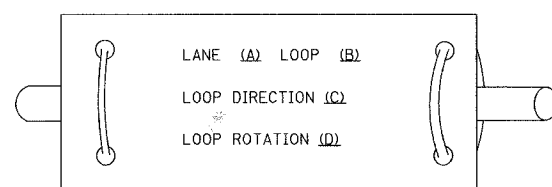
DRAWN BY: OJT
 DESIGNED BY: OJT
 CHECKED BY: JUE

ALL DIMENSIONS UNLESS OTHERWISE SPECIFIED SHALL BE IN FEET AND INCHES. DIMENSIONS SHALL BE TO FACE UNLESS OTHERWISE SPECIFIED.

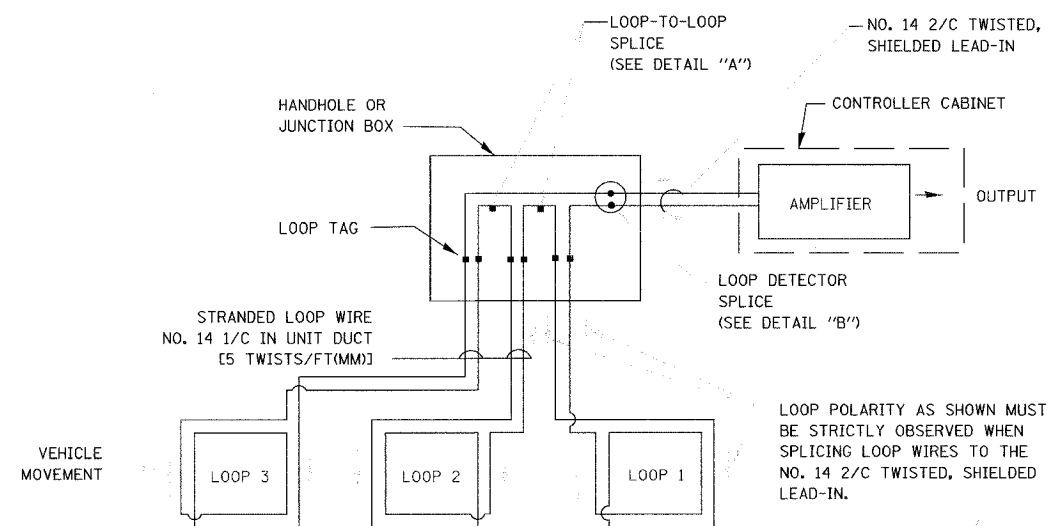
LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

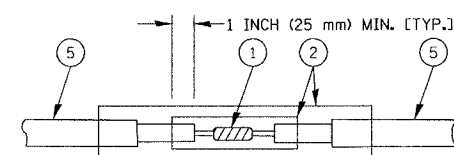


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

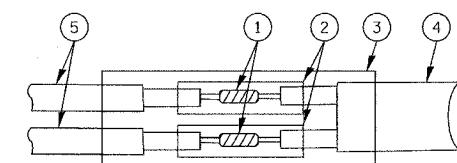


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT ONE
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

SCALE: VERT. NONE
HORIZ. DATE 1-01-02

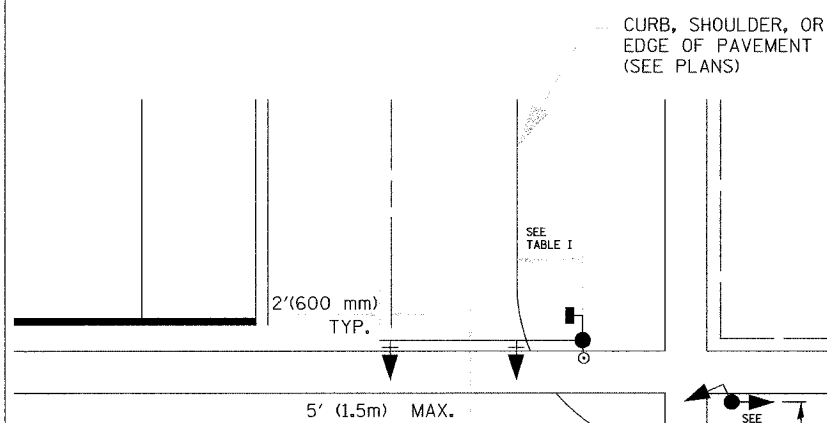
DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 1 OF 4

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 34 |
| STA. _____ TO STA. _____ | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT F-00210681 | | | | |

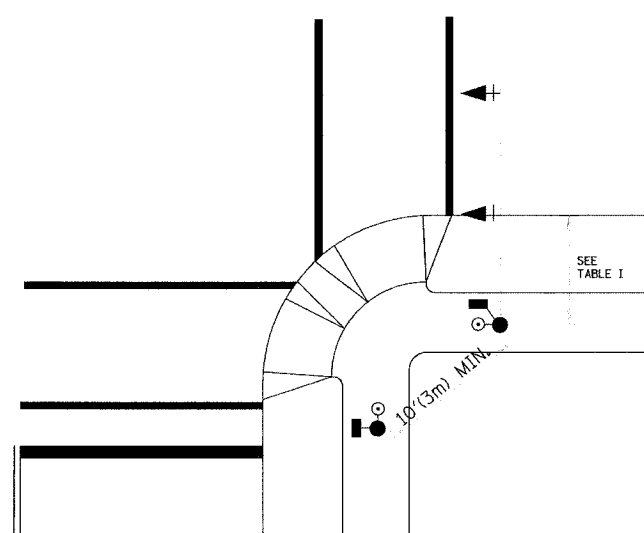
CONTRACT NO. 83852

TRAFFIC SIGNAL MAST ARM AND POST

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



PEDESTRIAN SIGNAL PUSHBUTTON



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

NOTES:

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.

AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.

PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:
 - A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.
 - B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.
 - C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
 - D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).
 - E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

PEDESTRIAN SIGNAL POST

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

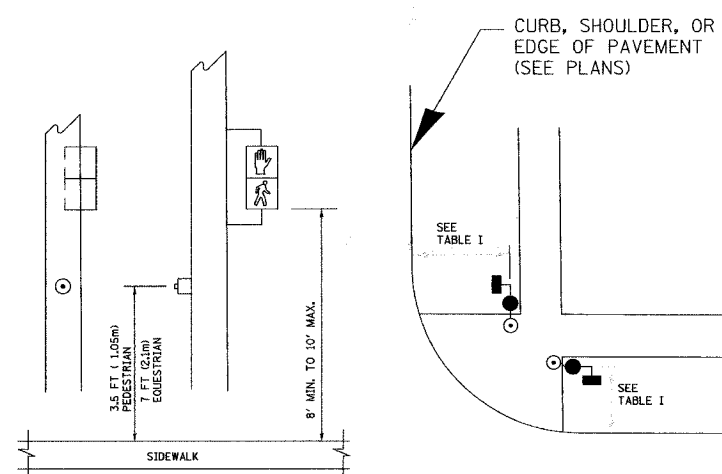


TABLE I

| TRAFFIC SIGNAL EQUIPMENT | COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB) | SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT) |
|------------------------------|---|---|
| TRAFFIC SIGNAL MAST ARM POLE | 6 FT (1.8m) | SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m) |
| TRAFFIC SIGNAL POST | 4 FT (1.2m) | SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m) |
| PEDESTRIAN SIGNAL POST | 4 FT (1.2m) | SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m) |
| PEDESTRIAN PUSHBUTTON | SEE NOTE 1 | SEE NOTE 1 |

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

VERT. NONE
SCALE: HORIZ.
DATE 1-01-02

DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 2 OF 4

FILE

DATE-TIME
DGN-SPEC

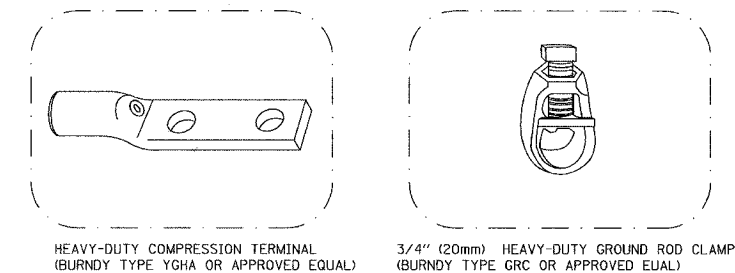
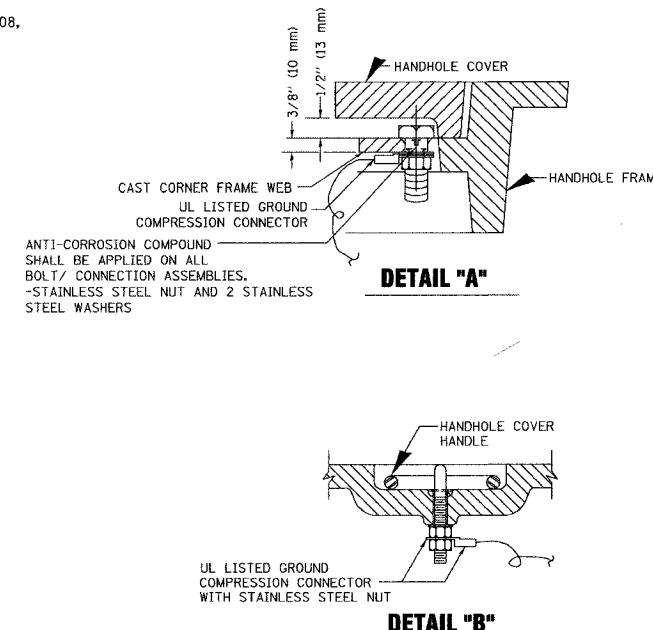
| | | | | |
|-----------------------|----------------|----------------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 35 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. 7 | ILLINOIS | FED. AID PROJECT F-0021068 | | |

CONTRACT NO. 83852

NOTES:

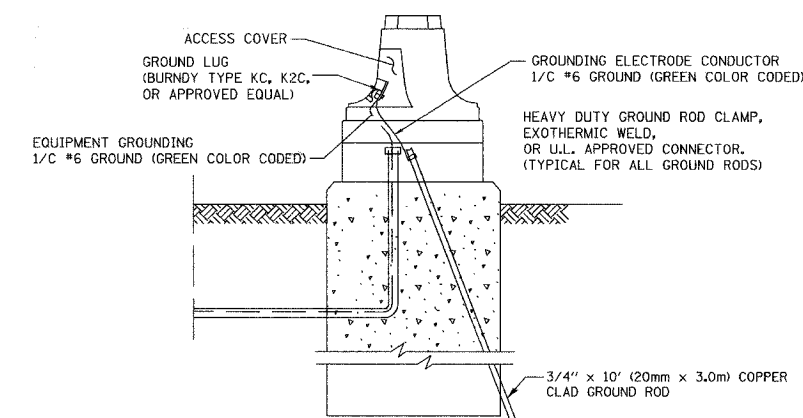
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.), GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



REVISIONS

| NAME | DATE |
|------|------|
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

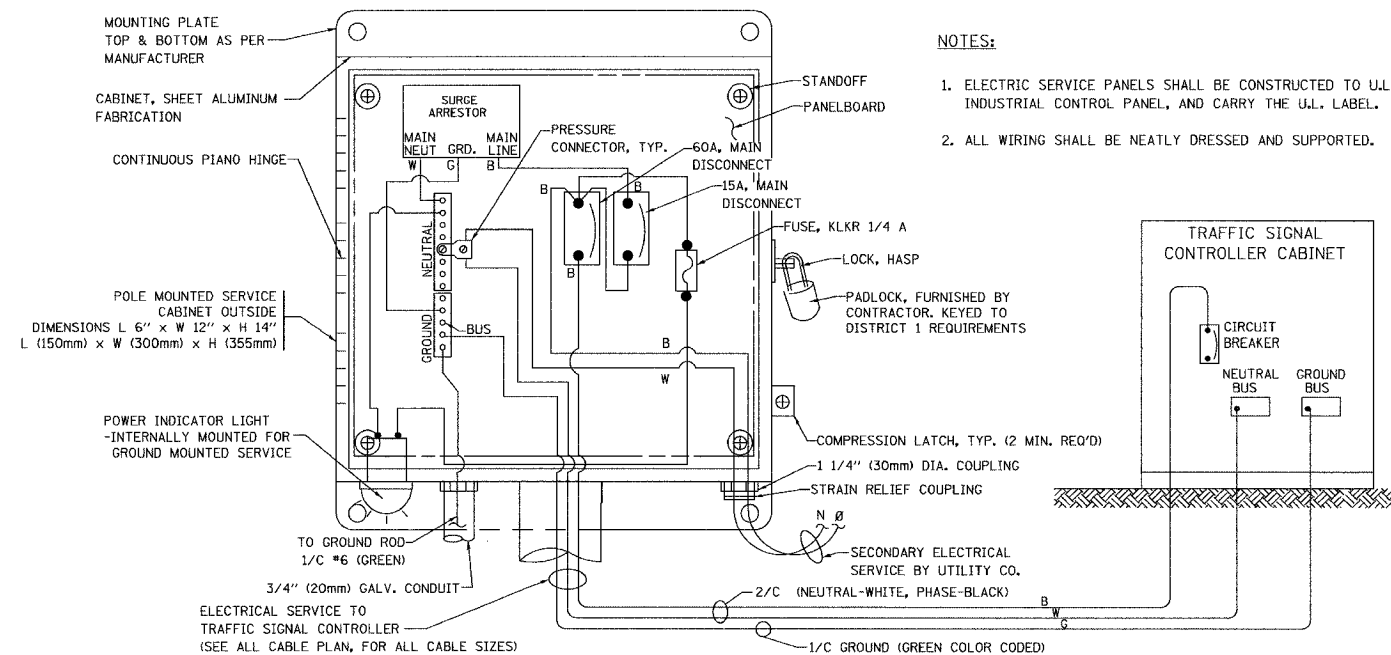
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

SCALE: VERT. NONE
 HORIZ. 1"=10'
 DATE 1-01-02

DRAWN BY: RWP
 DESIGNED BY: DAD
 CHECKED BY: DAZ
 SHEET 3 OF 4

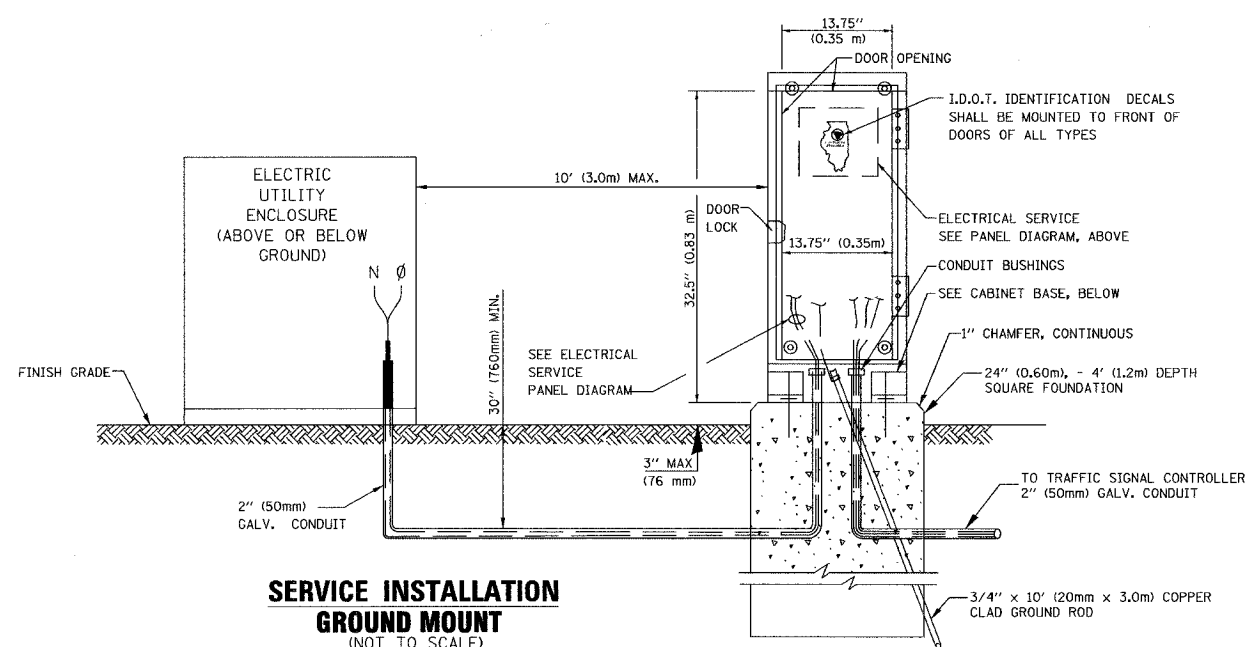
NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

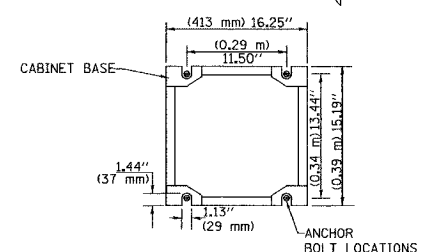


ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)

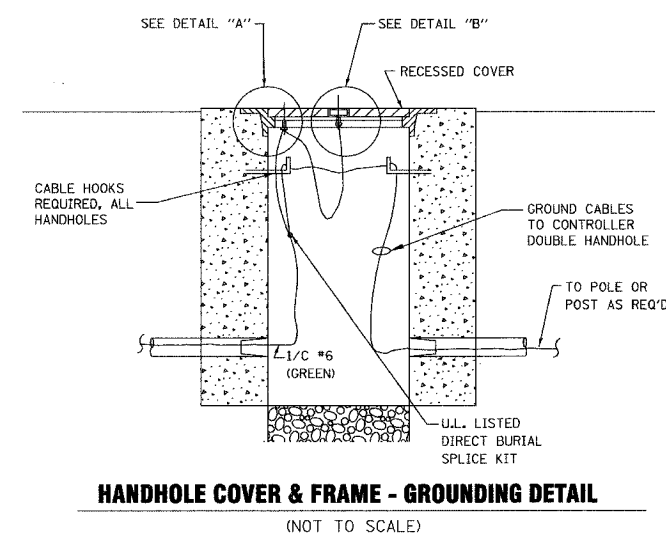
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



SERVICE INSTALLATION GROUND MOUNT
 (NOT TO SCALE)

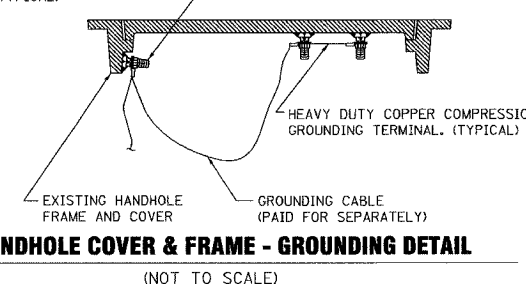


CABINET - BASE BOLT PATTERN
 (NOT TO SCALE)



HANDHOLE COVER & FRAME - GROUNDING DETAIL
 (NOT TO SCALE)

(2) 1/2" x 1 1/4" STAINLESS STEEL BOLT WITH SPLIT LOCK WASHER AND NYLON INSERT LOCKOUT WELDED TO FRAME AND TO COVER. (TYPICAL)

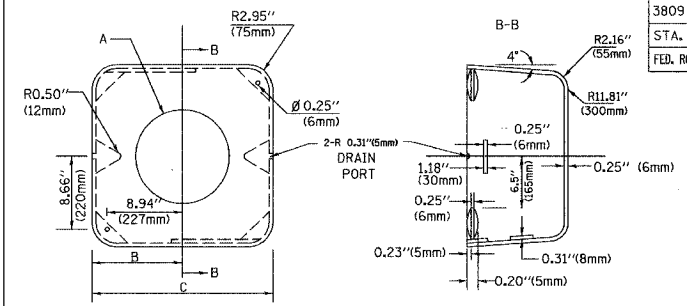


EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL
 (NOT TO SCALE)

| | | | | |
|-----------------------|----------------|----------|-----------------------------|-----------|
| F.A.U. R.T.C. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3809 | 05-00050-00-CH | DUPAGE | 36 | 36 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. 7 | | ILLINOIS | FED. AID PROJECT F-002(068) | |

CONTRACT NO. 83852

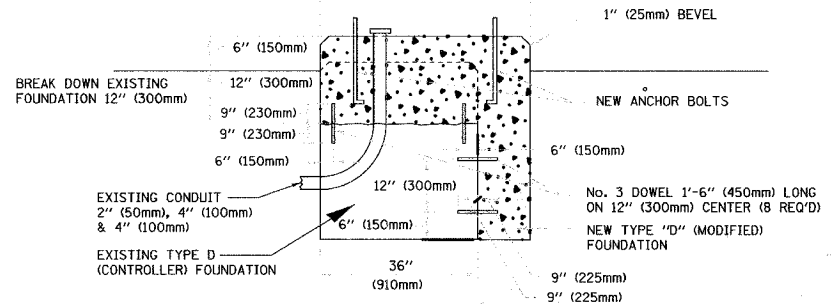
MATERIAL:
 - ASTM A48 CLASS 30 GREY IRON
 - ASTM A123 HOT DIPPED GALVANIZED



| TYPE | A | B | C | HEIGHT | WEIGHT |
|------|-------------------|----------------|---------------|-------------|--------|
| I | Ø 10.125" (257mm) | 9.5" (241mm) | 19" (483mm) | 12" (300mm) | 24kg |
| II | Ø 11.125" (283mm) | 10.75" (273mm) | 21.5" (546mm) | 12" (300mm) | 26kg |

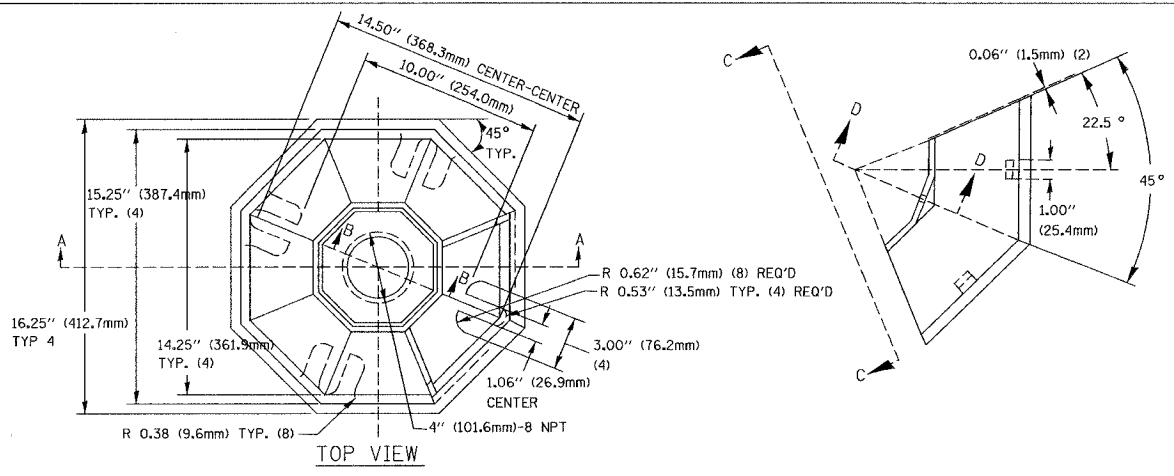
SHROUD DETAIL

NOTE:
 SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION

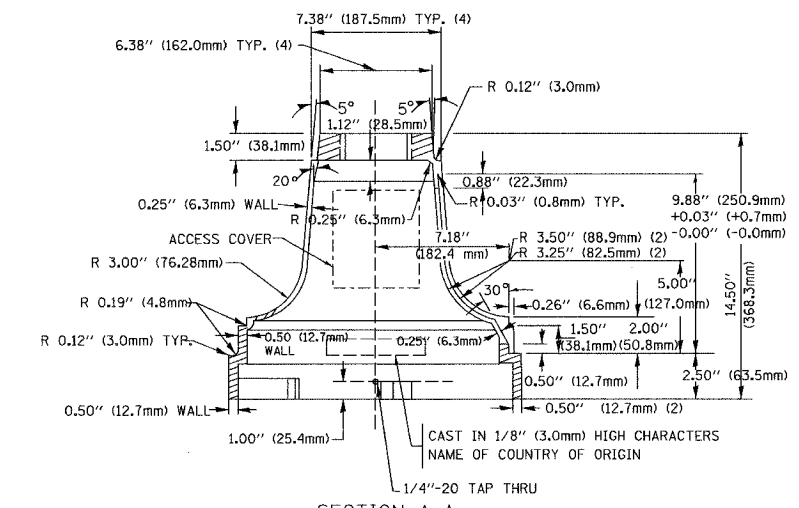
(NOT TO SCALE)



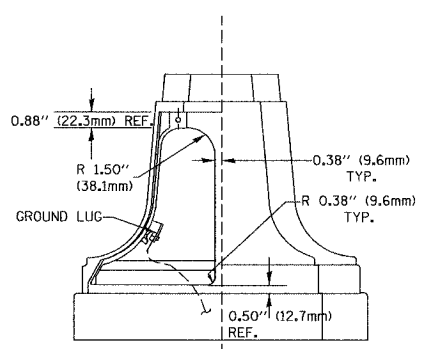
TOP VIEW

SECTION B-B

SECTION D-D

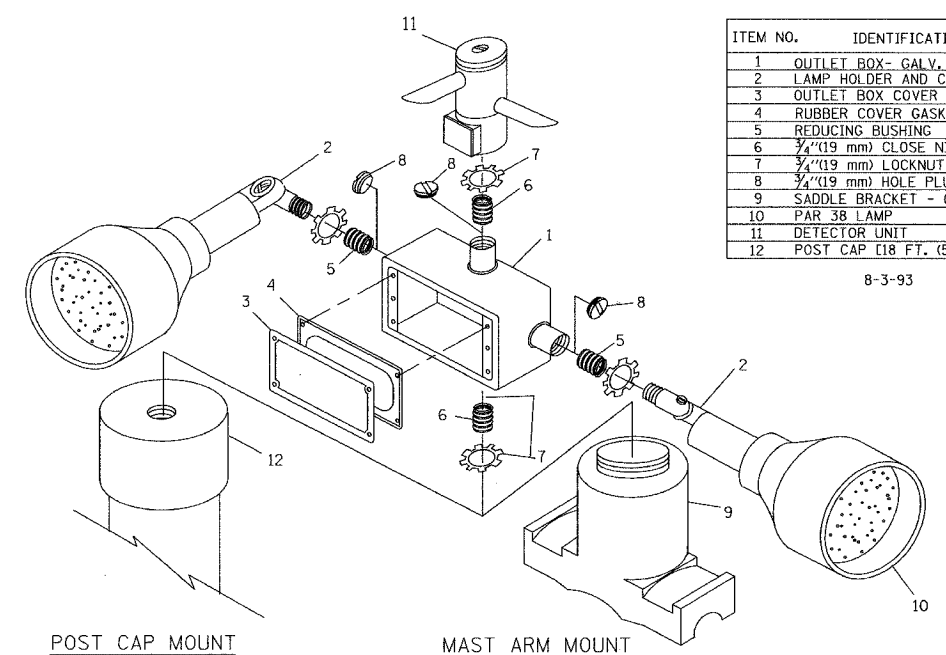


SECTION A-A



VIEW C-C

TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



POST CAP MOUNT

MAST ARM MOUNT

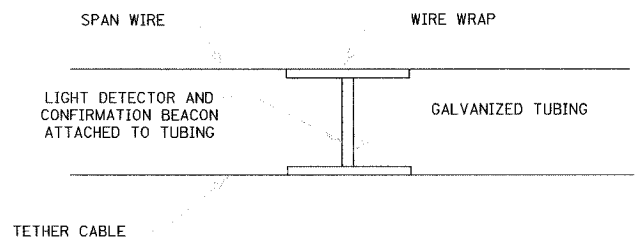
EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

| ITEM NO. | IDENTIFICATION |
|----------|---|
| 1 | OUTLET BOX - GALV. 21 CU. IN. (0.000344 CU. M.) |
| 2 | LAMP HOLDER AND COVER |
| 3 | OUTLET BOX COVER |
| 4 | RUBBER COVER GASKET |
| 5 | REDUCING BUSHING |
| 6 | 3/4" (19 mm) CLOSE NIPPLE |
| 7 | 3/4" (19 mm) LOCKNUT |
| 8 | 3/4" (19 mm) HOLE PLUG |
| 9 | SADDLE BRACKET - GALV. |
| 10 | PAR 38 LAMP |
| 11 | DETECTOR UNIT |
| 12 | POST CAP [18 FT. (5.4 m) POST MIN.] |

8-3-93

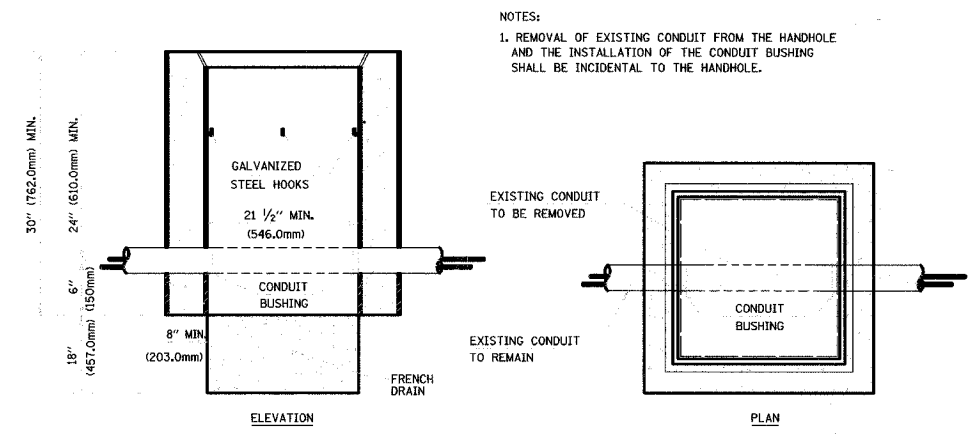
NOTES:

- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
 ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
 ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/8" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



LIGHT DETECTOR AND CONFIRMATION BEACON MOUNTING FOR TEMPORARY TRAFFIC SIGNALS

(NOT TO SCALE)



DETAIL
 HANDHOLE TO INTERCEPT EXISTING CONDUIT
 N.T.S.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT 1
 STANDARD TRAFFIC SIGNAL
 DESIGN DETAILS

SCALE: VERT. NONE
 HORIZ. DATE 1-01-02

DRAWN BY: RWP
 DESIGNED BY: DAD
 CHECKED BY: DAZ
 SHEET 4 OF 4