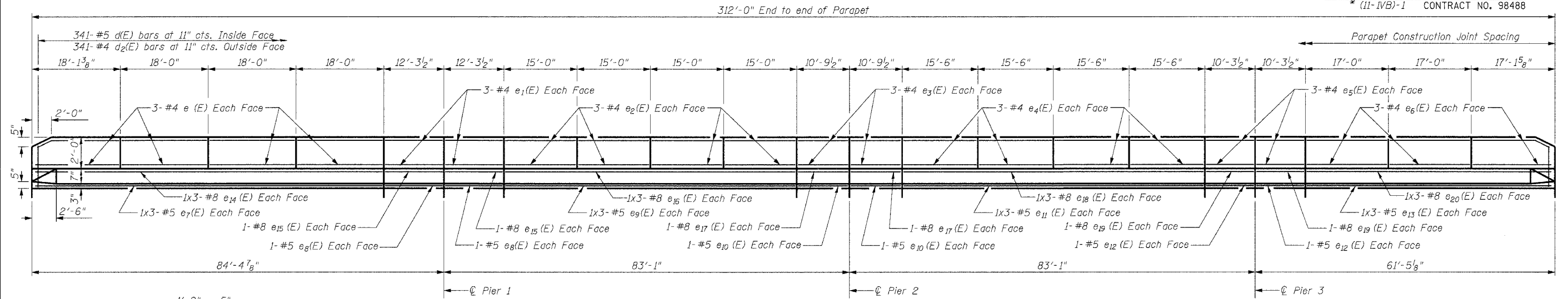
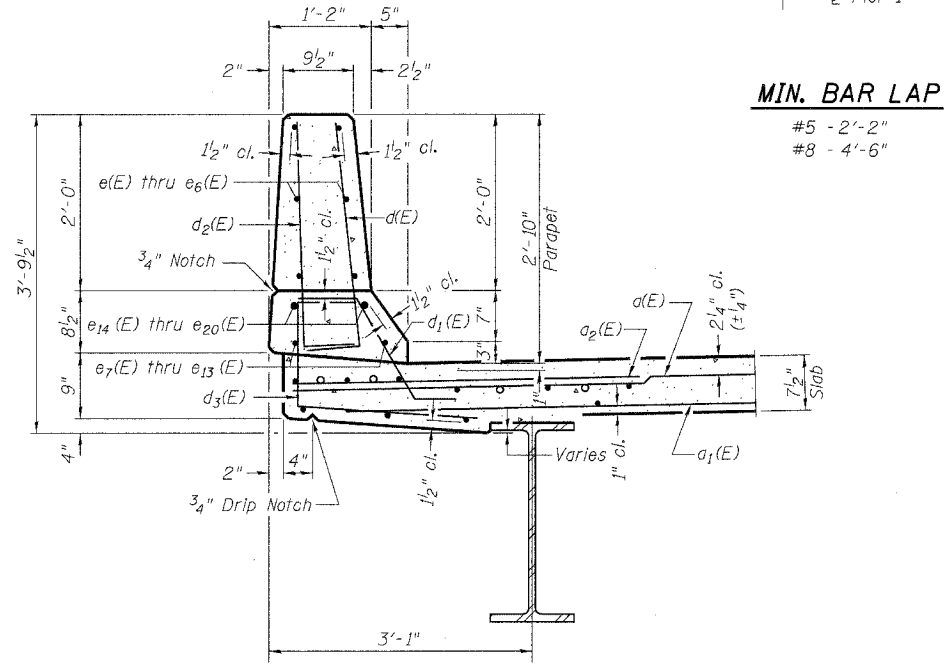


STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

|                       |         |                  |                                |           |              |
|-----------------------|---------|------------------|--------------------------------|-----------|--------------|
| ROUTE NO.             | SECTION | COUNTY           | TOTAL SHEETS                   | SHEET NO. | SHEET NO. 10 |
| F.A.P. 322            | *       | UNION            | 39                             | 27        | 22 SHEETS    |
| FED. ROAD DIST. NO. 7 | BALANCE | FED. AID PROJECT | *(11-IVB)-1 CONTRACT NO. 98488 |           |              |



**PARAPET ELEVATION**  
(Looking East at Inside Face)



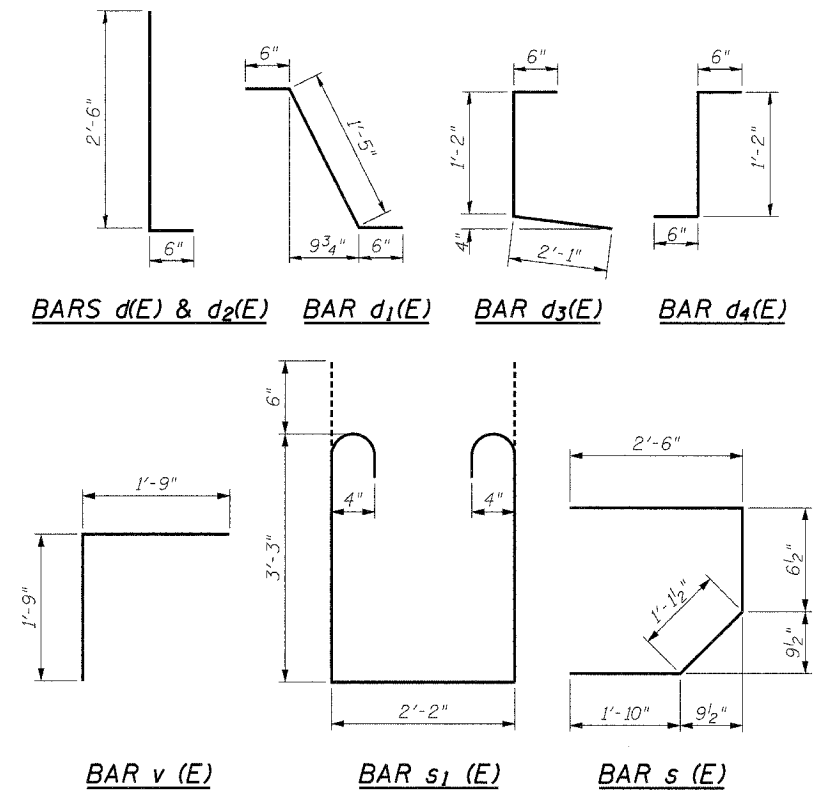
**SECTION THRU PARAPET**

**MIN. BAR LAP**  
#5 - 2'-2"  
#8 - 4'-6"

**SUPERSTRUCTURE  
BILL OF MATERIAL**

| Bar                              | No. | Size     | Length  | Shape |
|----------------------------------|-----|----------|---------|-------|
| a (E)                            | 623 | #5       | 19'-7"  | —     |
| a <sub>1</sub> (E)               | 623 | #5       | 23'-5"  | —     |
| a <sub>2</sub> (E)               | 624 | #6       | 4'-0"   | —     |
| a <sub>3</sub> (E)               | 440 | #5       | 19'-0"  | —     |
| a <sub>4</sub> (E)               | 440 | #5       | 22'-0"  | —     |
| a <sub>5</sub> (E)               | 4   | #5       | 19'-0"  | —     |
| a <sub>6</sub> (E)               | 4   | #5       | 23'-0"  | —     |
| b(E)                             | 564 | #5       | 28'-0"  | —     |
| b <sub>1</sub> (E)               | 44  | #6       | 45'-3"  | —     |
| b <sub>2</sub> (E)               | 44  | #6       | 39'-9"  | —     |
| b <sub>3</sub> (E)               | 44  | #6       | 38'-9"  | —     |
| b <sub>4</sub> (E)               | 468 | #5       | 26'-11" | —     |
| d(E)                             | 682 | #5       | 3'-0"   | —     |
| d <sub>1</sub> (E)               | 670 | #5       | 2'-5"   | —     |
| d <sub>2</sub> (E)               | 682 | #4       | 3'-0"   | —     |
| d <sub>3</sub> (E)               | 624 | #4       | 3'-9"   | —     |
| d <sub>4</sub> (E)               | 12  | #5       | 2'-2"   | —     |
| e(E)                             | 48  | #4       | 25'-2"  | —     |
| e <sub>1</sub> (E)               | 24  | #4       | 11'-8"  | —     |
| e <sub>2</sub> (E)               | 48  | #4       | 21'-3"  | —     |
| e <sub>3</sub> (E)               | 24  | #4       | 10'-2"  | —     |
| e <sub>4</sub> (E)               | 48  | #4       | 21'-11" | —     |
| e <sub>5</sub> (E)               | 24  | #4       | 9'-8"   | —     |
| e <sub>6</sub> (E)               | 36  | #4       | 18'-2"  | —     |
| e <sub>7</sub> (E)               | 12  | #5       | 25'-6"  | —     |
| e <sub>8</sub> (E)               | 8   | #5       | 11'-8"  | —     |
| e <sub>9</sub> (E)               | 12  | #5       | 21'-8"  | —     |
| e <sub>10</sub> (E)              | 8   | #5       | 10'-2"  | —     |
| e <sub>11</sub> (E)              | 12  | #5       | 22'-4"  | —     |
| e <sub>12</sub> (E)              | 8   | #5       | 9'-8"   | —     |
| e <sub>13</sub> (E)              | 12  | #8       | 27'-0"  | —     |
| e <sub>14</sub> (E)              | 12  | #8       | 11'-8"  | —     |
| e <sub>15</sub> (E)              | 8   | #8       | 11'-8"  | —     |
| e <sub>16</sub> (E)              | 12  | #8       | 23'-1"  | —     |
| e <sub>17</sub> (E)              | 8   | #8       | 10'-2"  | —     |
| e <sub>18</sub> (E)              | 12  | #8       | 23'-9"  | —     |
| e <sub>19</sub> (E)              | 8   | #8       | 9'-8"   | —     |
| e <sub>20</sub> (E)              | 12  | #8       | 20'-0"  | —     |
| m (E)                            | 4   | #6       | 19'-0"  | —     |
| m <sub>1</sub> (E)               | 6   | #6       | 19'-9"  | —     |
| m <sub>2</sub> (E)               | 8   | #6       | 11'-3"  | —     |
| m <sub>3</sub> (E)               | 8   | #6       | 7'-6"   | —     |
| m <sub>4</sub> (E)               | 4   | #6       | 3'-0"   | —     |
| m <sub>5</sub> (E)               | 2   | #6       | 1'-6"   | —     |
| m <sub>6</sub> (E)               | 4   | #6       | 24'-0"  | —     |
| m <sub>7</sub> (E)               | 6   | #6       | 24'-9"  | —     |
| m <sub>8</sub> (E)               | 8   | #6       | 14'-0"  | —     |
| m <sub>9</sub> (E)               | 2   | #6       | 5'-10"  | —     |
| s (E)                            | 92  | #4       | 6'-0"   | —     |
| s <sub>1</sub> (E)               | 84  | #4       | 9'-8"   | —     |
| v (E)                            | 86  | #5       | 3'-6"   | —     |
| Reinforcement Bars, Epoxy Coated |     | Pound    | 105070  |       |
| Concrete Superstructure          |     | Cu. Yds. | 409.2   |       |
| Bar Splicers                     |     | Each     | 1161    |       |

Reinforcement bars designated (E) shall be epoxy coated.  
Bars Indicated thus 1x3 - #5 etc. indicates 1 line of bars with 3 lengths per line.



**SUPERSTRUCTURE DETAILS**  
U.S. ROUTE 51 OVER TRIBUTARY TO  
DRURY CREEK AND CN/IC RAILROAD  
FAP 322 - SECTION (11-IVB)-1  
UNION COUNTY  
STATION 583+74.84  
STRUCTURE NO. 091-0073

|          |         |
|----------|---------|
| DESIGNED | GLH     |
| CHECKED  | TML     |
| DRAWN    | RJN     |
| CHECKED  | TML     |
| S-1-D    | 9-01-03 |

**PARAPET JOINT DETAILS**

