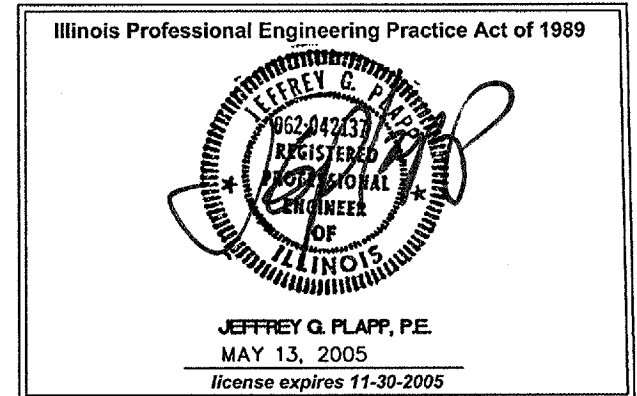




ROCKFORD, ILLINOIS CONSTRUCTION PLANS FOR GREATER ROCKFORD AIRPORT



REHABILITATE N. TERMINAL AUTO PARKING EXPAND W. TERMINAL AUTO PARKING

DESIGN INFORMATION

TOWNSHIP: 43 NORTH ROCKFORD TOWNSHIP
RANGE: 1 EAST (SECTIONS: 11, 14, 15, 16,
WINNEBAGO COUNTY 21, 22, & 23)

MAXIMUM EQUIPMENT HEIGHT = 25 FEET

FINAL SUBMITTAL

**CALL JULIE
BEFORE EXCAVATING**
1-800-892-0123

ILLINOIS PROJECT: RFD-2820

MAY 13, 2005

CMT 05258-04-00
http://www.cmtengr.com
Springfield, Illinois
Aurora, Illinois
Rockford, Illinois
St. Louis, Mo.

SUBMITTED BY: *Jeffrey G. Plapp*
JEFFREY G. PLAPP, P.E.
DATE: MAY 13, 2005

ROCKFORD RFD
Northwest Chicagoland International Airport

NORTHWEST CHICAGOLAND
INTERNATIONAL AIRPORT
ROCKFORD, ILLINOIS

APPROVED BY: *Robert W. O'Brien, Jr.*
ROBERT W. O'BRIEN, JR., AAE
EXECUTIVE DIRECTOR
DATE: MAY 13, 2005

INDEX TO SHEETS

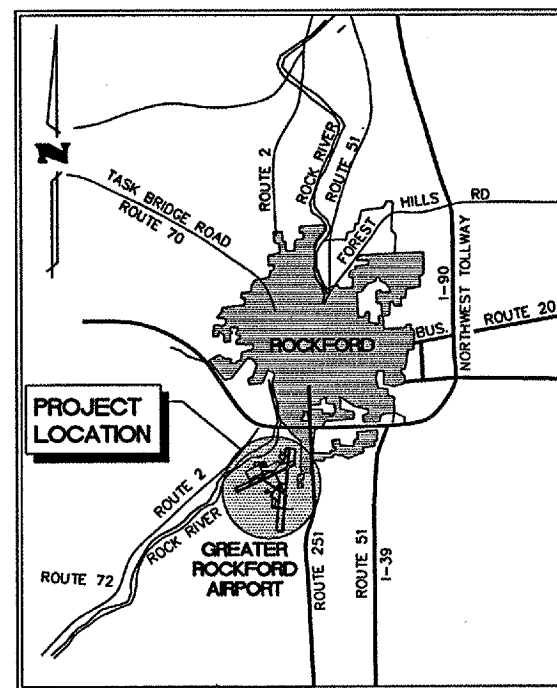
1. COVER SHEET
2. SUMMARY OF QUANTITIES

NORTH AUTO PARKING

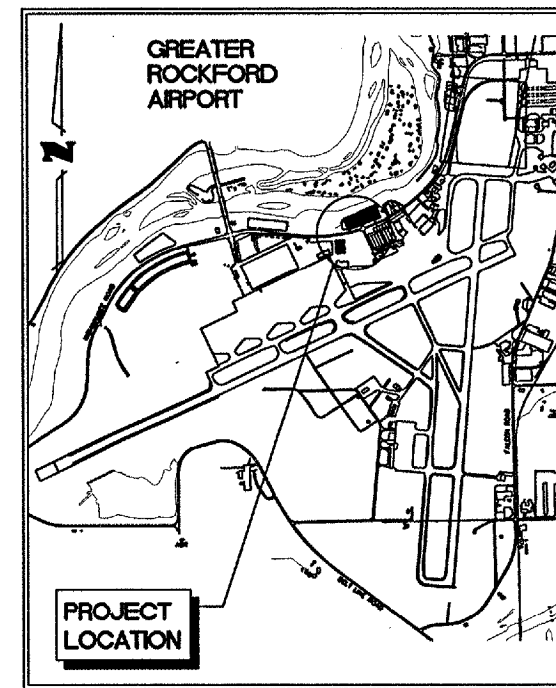
3. SITE PLAN
4. TYPICAL SECTIONS
5. CONSTRUCTION PHASING PLAN
6. CONSTRUCTION PHASING PLAN
GENERAL NOTES
7. EXIST. CONDITIONS / REMOVALS
8. GRADING PLAN
9. ELECTRICAL PLAN
10. DRAINAGE / MARKING /
LANDSCAPING / PLAN
11. DRAINAGE DETAILS - 1
12. EROSION CONTROL DETAILS - 1
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- 17-18. CROSS SECTIONS 1-2

WEST AUTO PARKING

19. SITE PLAN
20. TYPICAL SECTIONS
21. EXIST. CONDITIONS /
REMOVALS
22. GRADING PLAN
23. DRAINAGE / MARKING /
LANDSCAPING / ELECTR. PLAN
24. CROSS SECTION 1



LOCATION MAP



SITE PLAN

SUMMARY OF QUANTITIES


**RO010
TOTAL SHEETS = 24**

REVISIONS		
NUMBER	BY	DATE


0 1
THIS BAR IS EQUAL TO 1"
AT FULL SCALE (17 X 11).

SEQ NO	ITEM NO	DESCRIPTION	UNIT	REHABILITATE NORTH LOT EST. QUANTITY	EXPAND WEST LOT EST. QUANTITY	TOTAL EST. QUANTITY	RECORD TOTAL QUANTITY
1	AR106511	TYPE A AREA LIGHT POLE W/1 FIXTURE	EACH	5.00	0.00	5.00	
2	AR106513	TYPE A AREA LIGHT POLE W/3 FIXTURES	EACH	6.00	0.00	6.00	
3	AR106514	TYPE A AREA LIGHT POLE W/4 FIXTURES	EACH	6.00	2.00	8.00	
4	AR106905	REMOVE LIGHT POLE & FIXTURE	EACH	15.00	0.00	15.00	
5	AR108084	1/C #4 XLP-USE	L.F.	5,840.00	820.00	6,660.00	
6	AR108758	1/C #8 GROUND	L.F.	1,760.00	410.00	2,170.00	
7	AR109620	LIGHTING CONTROL SYSTEM	L.S.	1.00	0.00	1.00	
8	AR110212	2" STEEL DUCT, DIRECT BURY	L.F.	1,630.00	365.00	1,995.00	
9	AR110214	4" STEEL DUCT, DIRECT BURY	L.F.	90.00	0.00	90.00	
10	AR110610	ELECTRICAL HANDHOLE	EACH	6.00	0.00	6.00	
11	AR151420	CLEARING TREES 0-2.5' BUTT. DIA.	EACH	8.00	0.00	8.00	
12	AR152410	UNCLASSIFIED EXCAVATION	C.Y.	0.00	1,840.00	1,840.00	
13	AR152540	SOIL STABILIZATION FABRIC	S.Y.	14,060.00	4,140.00	18,200.00	
14	AR162408	CLASS E FENCE, VINYL-8'	L.F.	1,220.00	0.00	1,220.00	
15	AR162620	CLASS E GATE-20'	EACH	3.00	0.00	3.00	
16	AR163910	FENCE REMOVAL, WOOD	L.F.	1,115.00	0.00	1,115.00	
17	AR208515	POROUS GRANULAR EMBANKMENT	C.Y.	2,425.00	200.00	2,625.00	
18	AR209608	CRUSHED AGG. BASE COURSE - 8"	S.Y.	14,060.00	4,140.00	18,200.00	
19	AR401610	BITUMINOUS SURFACE COURSE	TON	3,515.00	980.00	4,495.00	
20	AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1.00	1.00	2.00	
21	AR501604	4" PCC SIDEWALK	S.F.	0.00	1,470.00	1,470.00	
22	AR602510	BITUMINOUS PRIME COAT	GAL.	4,450.00	1,240.00	5,690.00	
23	AR603510	BITUMINOUS TACK COAT	GAL.	1,113.00	310.00	1,423.00	
24	AR620520	PAVEMENT MARKING-WATERBORNE	S.F.	3,430.00	800.00	4,230.00	
25	AR701518	18" RCP, CLASS IV	L.F.	690.00	0.00	690.00	
26	AR751560	MANHOLE 6'	EACH	8.00	0.00	8.00	
27	AR751927	REPLACE FRAME & GRATE	EACH	0.00	2.00	2.00	
28	AR754410	COMB. CONCRETE CURB & GUTTER	L.F.	1,585.00	740.00	2,325.00	
29	AR754904	REMOVE COMB CURB & GUTTER	L.F.	0.00	60.00	60.00	
30	AR800859	CROSSWALK LIGHTING SYSTEM	L.S.	1.00	0.00	1.00	
31	AR800860	WALKWAY LIGHTING	L.S.	1.00	0.00	1.00	
32	AR800861	BIT. PAVMENT PULVERIZATION	S.Y.	15,470.00	0.00	15,470.00	
33	AR800868	SOIL GUARD	S.Y.	4,840.00	480.00	5,320.00	
34	AR901510	SEEDING	ACRE	1.00	0.10	1.10	
35	AR904510	SODDING	S.Y.	416.00	0.00	416.00	
36	AR905520	TOPSOILING (FROM OFF SITE)	C.Y.	200.00	0.00	200.00	
37							
38							
39							
40							


**GREATER ROCKFORD AIRPORT AUTHORITY
ROCKFORD, ILLINOIS
ILLINOIS PROJECT: RFD-2820
NORTH AND WEST AUTO PARKING
SUMMARY OF QUANTITIES**



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Airport

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CHECKED BY: TAS
APPROVED BY: 
DATE: 5.13.005
JOB No: 05258-04-00

NEW TREE CLEARING (AR151)
BRUSH CLEARING INC. TO UNCL. EXC. (AR152)

NEW LANDSCAPING LIMITS
(AR901 / 800868)

ROO10
TOTAL SHEETS = 24



NEW 4' BITUMINOUS SURFACE (AR401)/
8' CRUSHED AGG. BASE (AR209)/
EXIST. PAVEMENT TO BE
PULVERIZED (AR800861)

NEW FGE (AR209)

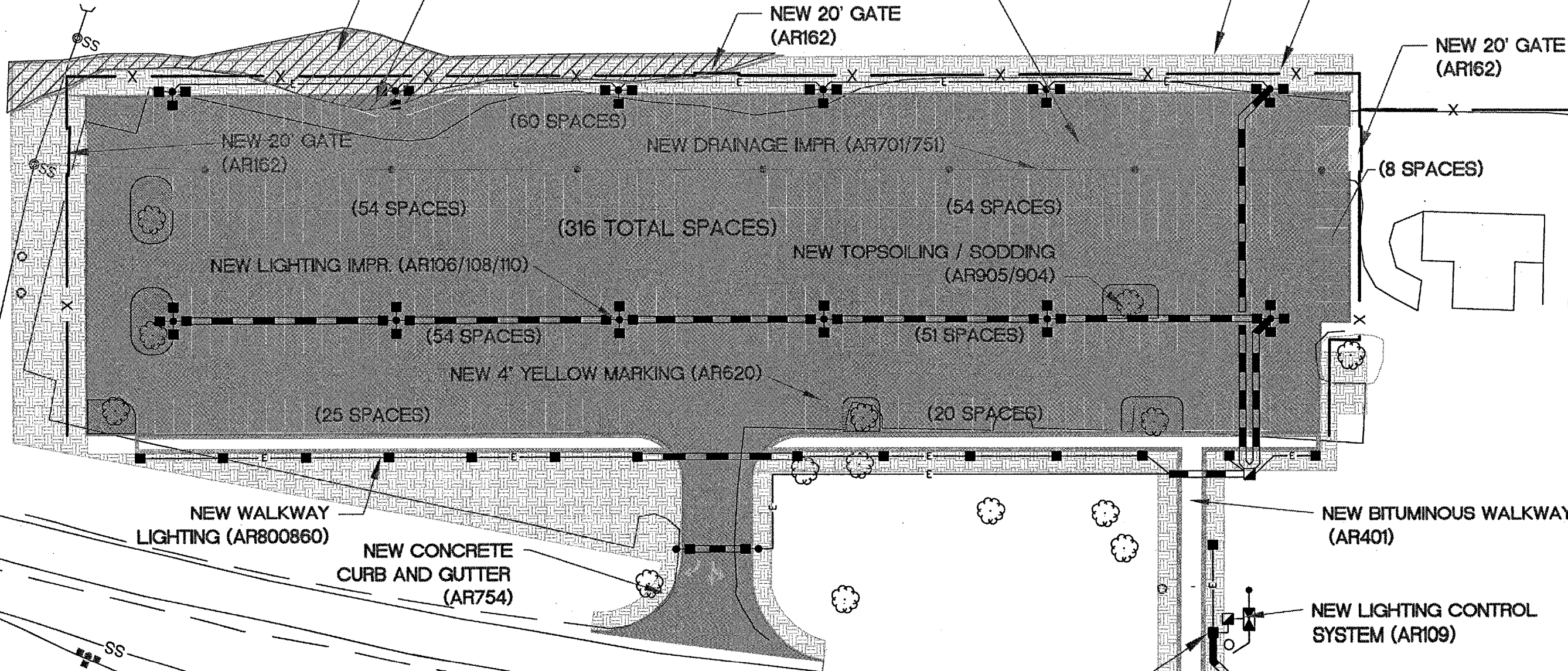
NEW 8' PERIMETER
FENCING (AR162)

NEW 20' GATE
(AR162)

NEW 20' GATE
(AR162)

REVISIONS		
NUMBER	BY	DATE

0 1
THIS BAR IS EQUAL TO 1"
AT FULL SCALE (17 X 11).



GREATER ROCKFORD AIRPORT AUTHORITY
ROCKFORD, ILLINOIS
ILLINOIS PROJECT: RFD-2820
NORTH AND WEST AUTO PARKING
SITE PLAN

PARKING SUMMARY

NORTH ROW:	50 Spaces
NORTHWEST ROW:	54 Spaces
NORTHEAST ROW:	54 Spaces
SOUTHWEST ROW:	54 Spaces
SOUTHWEST ROW:	51 Spaces
SOUTH ROW (WEST):	25 Spaces
SOUTH ROW (EAST):	20 Spaces
EAST ROW:	8 Spaces
Total =	316 Spaces

AIRPORT DRIVE

NEW CROSSWALK
LIGHTING SYSTEM
(AR800859)

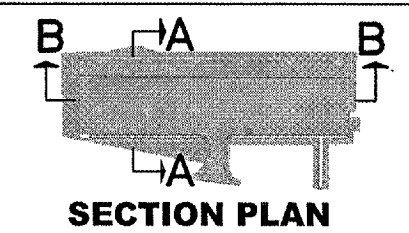
AIRPORT TERMINAL
AUTO PARKING LOT

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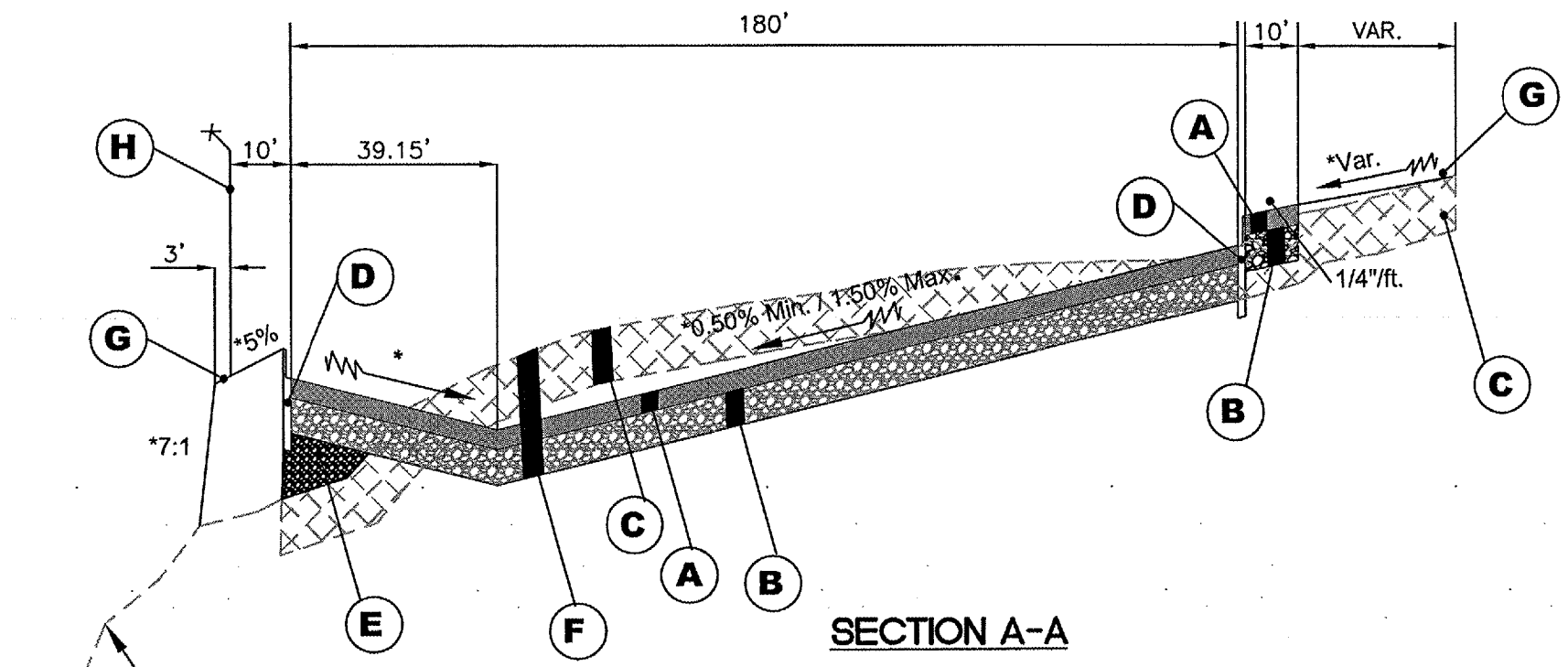
Northwest
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DRAWN BY: CMT
CHECKED BY: JGP
APPROVED BY: *[Signature]*
DATE: 5.13.2005
JOB No: 05258-04-00

3 (SP1)
SHEET 3 OF 24 SHEETS

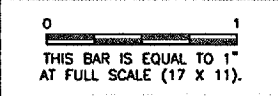


**R0010
TOTAL SHEETS = 24**

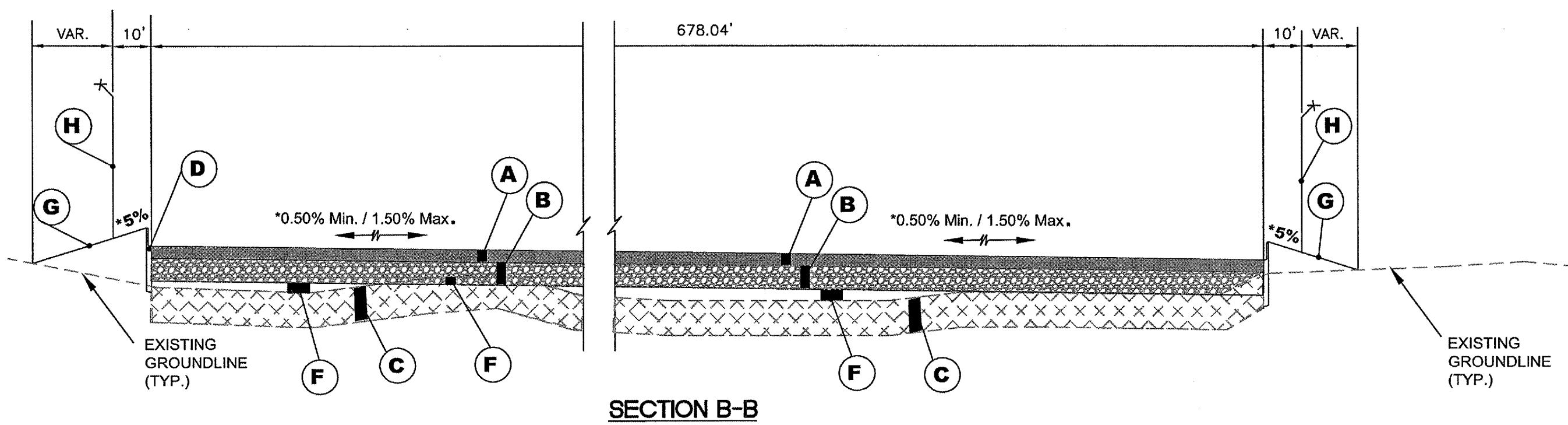


- A** NEW 4" (2-2" LIFTS) BITUMINOUS SURFACE COURSE (BITUMINOUS TACK COAT BETWEEN LIFTS) (AR401/AR603)
- B** NEW 8" CRUSHED AGGREGATE BASE COURSE (AR209) (BITUMINOUS PRIME COAT APPLIED PRIOR TO PAVING) (AR602)
- C** PULVERIZE AND INCORPORATE EXISTING PARKING PAVEMENT STRUCTURE (APPROX. 1" BIT. PVMT. / 10" AGGR. BASE) SURFACE WITH EXISTING AGG. BASE (AR401). GRADE EXISTING PULVERIZED MATERIAL AS SUBBASE FOR PROP. AGGR. BASE (AR152).
- D** NEW COMB. CONC. CURB AND GUTTER M-6.12 (AR754)
- E** NEW POROUS GRANULAR EMBANKMENT TO PROVIDE STRUCTURAL FILL IN NORTH TREE CLEARING AREA (AR208)
- F** NEW UNCLASSIFIED EXCAVATION (AR152)
- G** NEW SEEDING/SOIL GUARD (AR901-AR910)
- H** NEW 8' CLASS E FENCE (AR162)

REVISIONS		
NUMBER	BY	DATE



**GREATER ROCKFORD AIRPORT AUTHORITY
ROCKFORD, ILLINOIS
ILLINOIS PROJECT, RFD-2820
NORTH AND WEST AUTO PARKING
TYPICAL SECTIONS**



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CONSTRUCTION PHASING PLAN - GENERAL NOTES

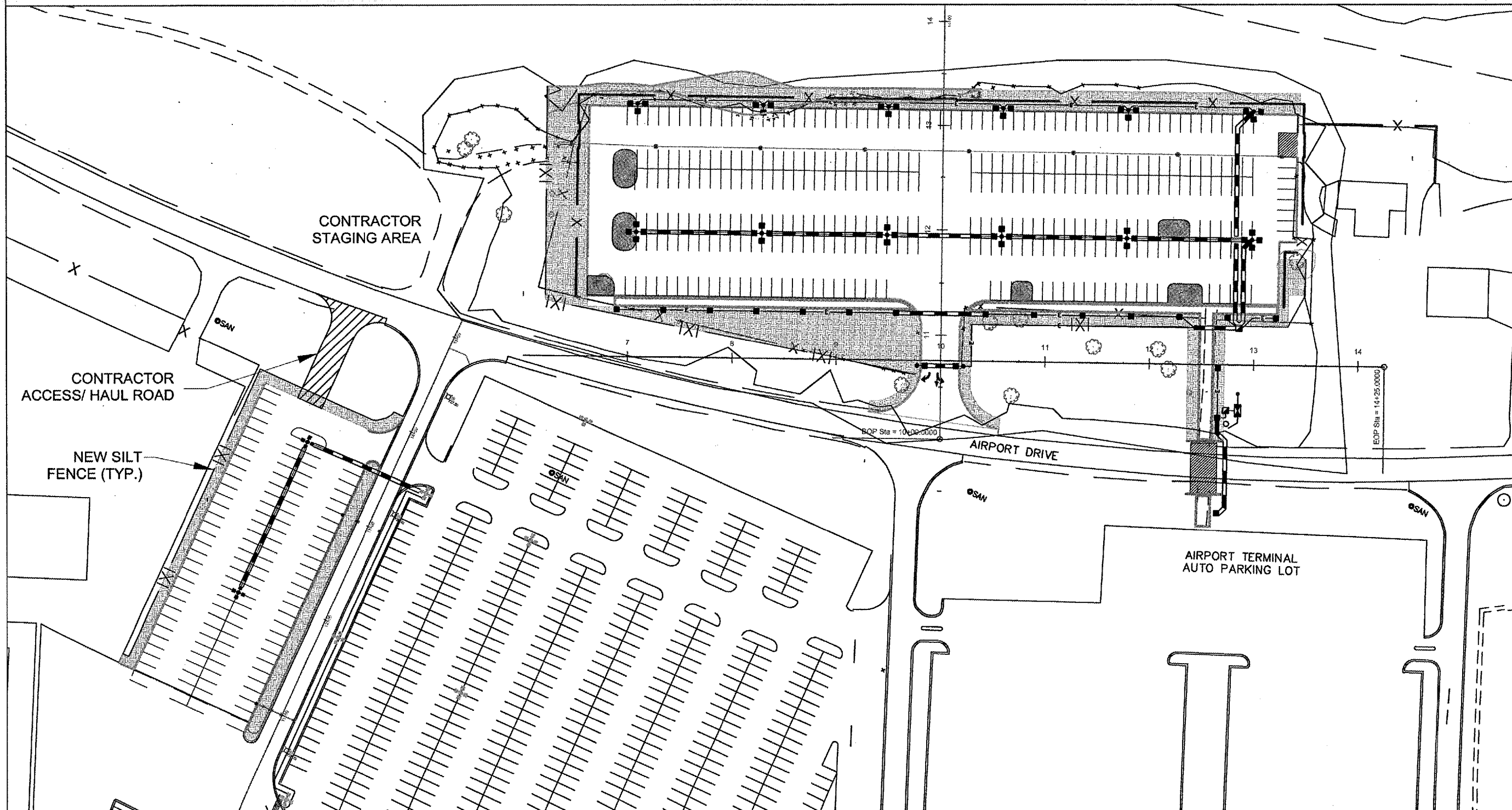
- 1) CONTRACTOR STAGING AREA SHALL BE LOCATED AS SHOWN ON THIS SHEET.
- 2) CONTRACTOR SHALL PROVIDE RESIDENT ENGINEER WITH APPROVED PROGRESS SCHEDULE SHOWING START/ STOP DATES OF PROPOSED CONSTRUCTION. APPROVED PROGRESS SCHEDULE SHALL BE SUBMITTED 5 WORKING DAYS PRIOR TO START OF CONSTRUCTION.
- 3) RESIDENT ENGINEER SHALL COORDINATE NOTAM AND FAA FACILITY COORDINATION WITH AIRPORT / FAA PERSONNEL.
- 4) THE CONTRACTOR SHALL MAINTAIN AND REPAIR THE THE CONSTRUCTION ACCESS ROAD AND STAGING AREA IN ITS ORIGINAL CONDITION AT NO ADDITIONAL COST THE CONTRACT.
- 5) ACCESS TO EXISTING PARKING AREAS AND PERIMETER ROADWAYS MUST BE MAINTAINED AT ALL TIMES.
- 6.) THE CONTRACTOR SHALL INSTALL SILT FENCE AS SHOWN ON THIS SHEET OR AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

R00010
TOTAL SHEETS = 24




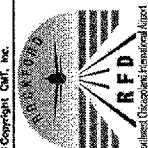
REVISIONS		
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THIS BAR IS EQUAL TO 1"
AT FULL SCALE (17 X 11).



GREATER ROCKFORD AIRPORT AUTHORITY
ROCKFORD, ILLINOIS
ILLINOIS PROJECT: RFD-2820
NORTH AND WEST AUTO PARKING
CONSTRUCTION PHASING PLAN


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Northwest Chicago International Airport
RFD
Rockford, Chicago International Airport

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APPROVED BY:	
DATE:	5.13.2005
JOB No:	05258-04-00
5 (CPP)	
SHEET 5 OF 24 SHEETS	

GENERAL NOTES:

1. SUGGESTED SEQUENCE OF CONSTRUCTION:

THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE ENGINEER. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DIRECTOR OF OPERATIONS.

2. HAUL ROAD / STAGING AREA RESTORATION:

ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STAGING AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE ENGINEER AND AIRPORT DIRECTOR OF OPERATIONS. THE COST OF MAINTAINING, REPAIRING, SEEDING / MULCHING OR CONSTRUCTING THESE PAVEMENTS / AREAS SHALL BE INCIDENTAL TO THE CONTRACT.

3. AIRPORT APPROVAL OF PHASING:

THE ENGINEER AND AIRPORT DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT OPERATIONS. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. VEHICULAR TRAFFIC AND CONTRACTOR ACTIVITIES SHALL YIELD TO AIRCRAFT OPERATIONS. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.

4. AIRFIELD PAVEMENT / SITE DEBRIS REMOVAL:

THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER MAY BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.

5. PROJECT LIGHTING OUTSIDE OF DAYLIGHT HOURS:

WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.

6. EXISTING UTILITY COORDINATION:

COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

7. TRAFFIC CONTROL PAYMENT:

PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, AIR OPERATIONS AREA (A.O.A.) LATH AND RIBBON, ETC. SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. TYPE 2 BARRICADES WITH STEADY BURN RED LIGHTS SHALL BE PLACED ON 15' CENTERS AND HAVE ORANGE CONSTRUCTION FENCING BETWEEN EACH SET OF BARRICADES. TYPE 2 BARRICADES SHALL BE PLACED AS SHOWN ON THIS PLAN AND AS DIRECTED BY THE ENGINEER FOR WORK ADJACENT TO THE EXPEDITED WORK AREA. WHEN NOT IN USE, THESE BARRICADES SHALL BE STORED AT THE CONTRACTOR'S STAGING AREA OR OFF SITE. ACCESS TO THE ACTIVE RUNWAY AND TAXIWAY PAVEMENTS (TOWER CONTROLLED AREAS) SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON THE CLOSEST BARRICADES (2 EACH, RIGHT AND LEFT) AT THE ENTRANCE. IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "TOWER CONTROL AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE." ALL TYPE II AND TYPE III BARRICADES SHALL CONFORM TO IDOT STANDARD DETAIL 702001. ALL PAVEMENT DROP-OFFS GREATER THAN 24" REQUIRE TYPE II BARRICADES WITH EXTENDED LEGS.

8. DUST CONTROL REQUIREMENTS:

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ON SITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS. PAYMENT FOR DUST CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.

9. OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION (AC 150/5370-2 (Latest Edition)):

ALL WORK SHALL CONFORM TO AC 150/5370-2 (Latest Edition) OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION. THIS AC IS AVAILABLE AT www.faa.gov/arp/pdf/5370-2 (Latest Edition).pdf.

10. STAGING AREA:

THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT RE-FUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFERS SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.

11. AIRFIELD LIGHTING COORDINATION:

THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD LIGHTING CIRCUITS AT THE START AND END OF EACH CONSTRUCTION DAY.

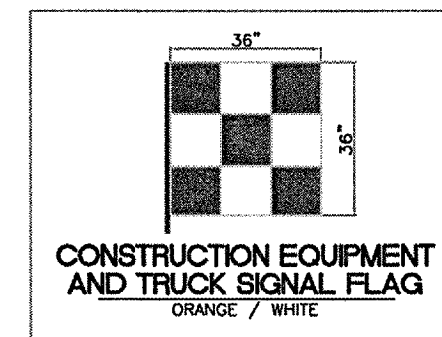
12. WEEKLY COORDINATION MEETINGS:

WEEKLY COORDINATION MEETINGS SHALL BE REQUIRED TO DISCUSS PROJECT PROGRESS. AS A MINIMUM, PROJECT SCHEDULE AND GATE VISITOR LOGS SHALL BE DISCUSSED. REPRESENTATION BY THE PRIME CONTRACTOR IS MANDATORY.

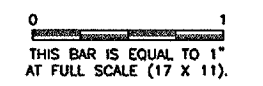
PROJECT NOTES:

1. CONSTRUCTION SITE ACCESS:

THE CONTRACTOR SHALL INSTALL THE HAUL ROAD AS SHOWN ON THE CONSTRUCTION PHASING PLAN. COST OF THE INSTALLATION, REMOVAL AND RESTORATION TO PRE-CONSTRUCTION CONDITIONS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE ENTRANCE SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLE ACCESS AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO THE CITY OF ROCKFORD AND IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.



REVISIONS		
NUMBER	BY	DATE



**GREATER ROCKFORD AIRPORT AUTHORITY
ROCKFORD, ILLINOIS**
ILLINOIS PROJECT: RFD-2820
**NORTH AND WEST AUTO PARKING
CONSTRUCTION PHASING PLAN
GENERAL NOTES**

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CONSULTING ENGINEERS

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Northwest
Chicago
International
Airport

DESIGN BY:	TAS
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APPROVED BY:	
DATE:	5.13.2005
JOB No:	05258-04-00

6 (CGN)

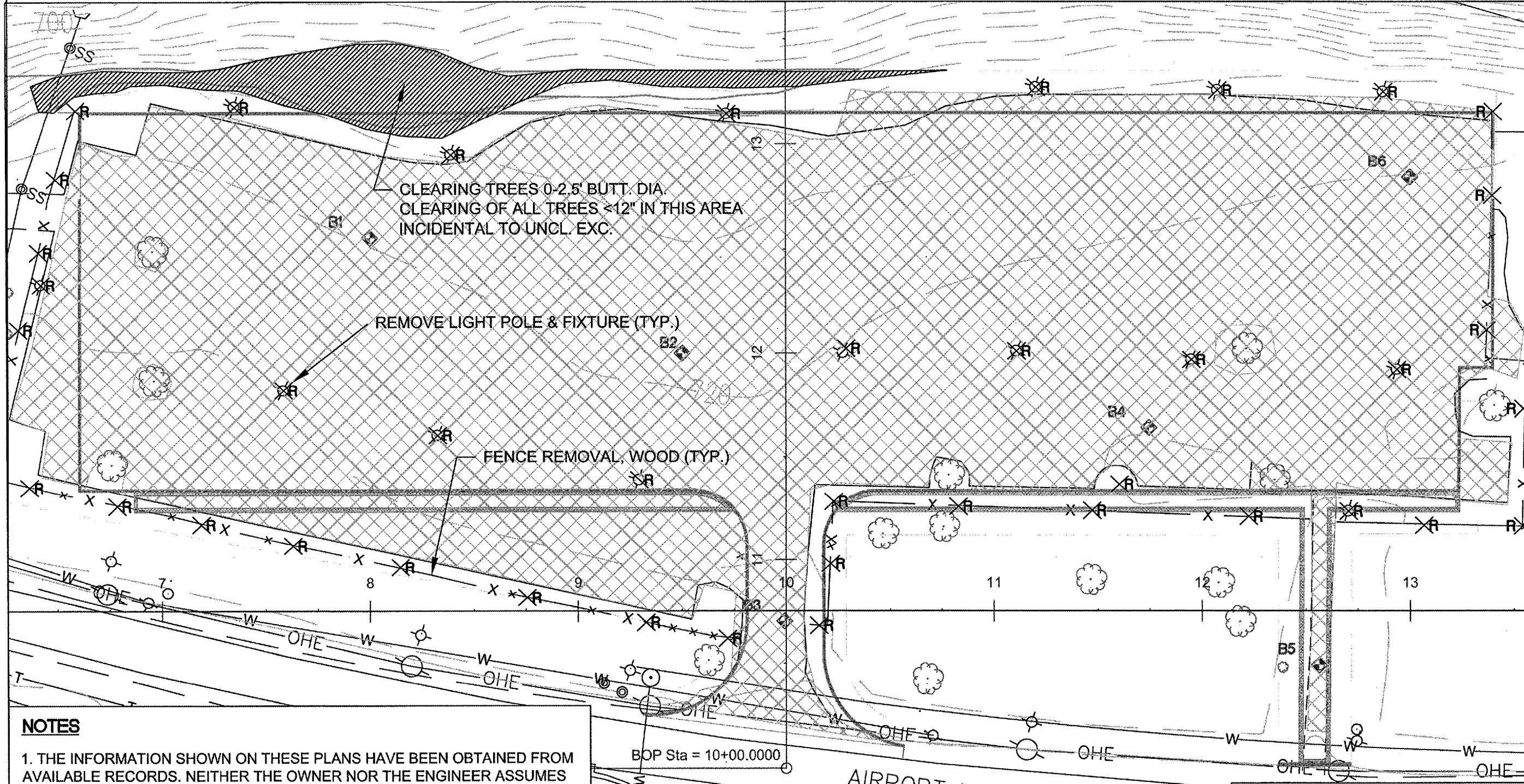
- NEW PARKING LIMITS
- ▨ BIT. PVMT. PULVERIZATION
- ▤ EXISTING BUILDING
- SS— EXISTING STORM SEWER
- UD— EXISTING UNDERDRAIN
- X,R DENOTES ITEM TO BE REMOVED
- ⊙ EXISTING TREE
- ⊕ EXISTING FIRE HYDRANT
- X— EXISTING FENCE
- ⊙ EXISTING POWER/LIGHT POLE
- ⊥ EXISTING SIGN
- ◆ SOIL BORING
- OHE— EXISTING OVERHEAD ELECTR.
- G— EXISTING GASLINE
- T— EXISTING TELEPHONE
- E— EXISTING UNDERGROUND ELECTR.

RO010
TOTAL SHEETS = 24



REVISIONS		
NUMBER	BY	DATE

0 1
THIS BAR IS EQUAL TO 1" AT FULL SCALE (17 X 11).



NOTES

1. THE INFORMATION SHOWN ON THESE PLANS HAVE BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

2. LIGHT POLE REMOVAL SHALL CONSIST OF REMOVAL OF THE POLE AND FIXTURE AS WELL AS THE PEDESTAL/JUNCTION BOX AT EACH POLE..

PROJECT CONTROL

#1 PK NAIL IN LOT
N: 2,018,310.511 E: 2,586,042.574

#2 CHISELED "X" SIDEWALK
N: 2,018,152.638 E: 2,585,031.251

BENCHMARK
LIGHT POLE BASE "O" = 727.08'

GREATER ROCKFORD AIRPORT AUTHORITY
ROCKFORD, ILLINOIS
ILLINOIS PROJECT: RFD-2620
NORTH AND WEST AUTO PARKING
EXIST. CONDITIONS/ REMOVALS

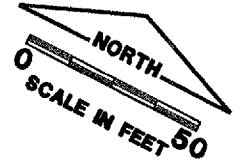
CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS

Northwest
Chicagoland
International
Airport

RFD

DESIGN BY:	AGJ
DRAWN BY:	CMT
CHECKED BY:	TAS
APPROVED BY:	<i>[Signature]</i>
DATE:	5.13.2005
JOB No:	05258-04-00
7 (RE1)	
SHEET 7 OF 24 SHEETS	

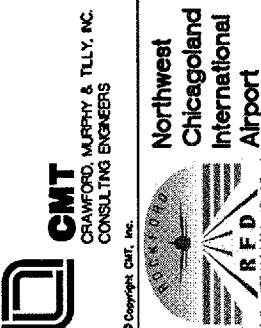
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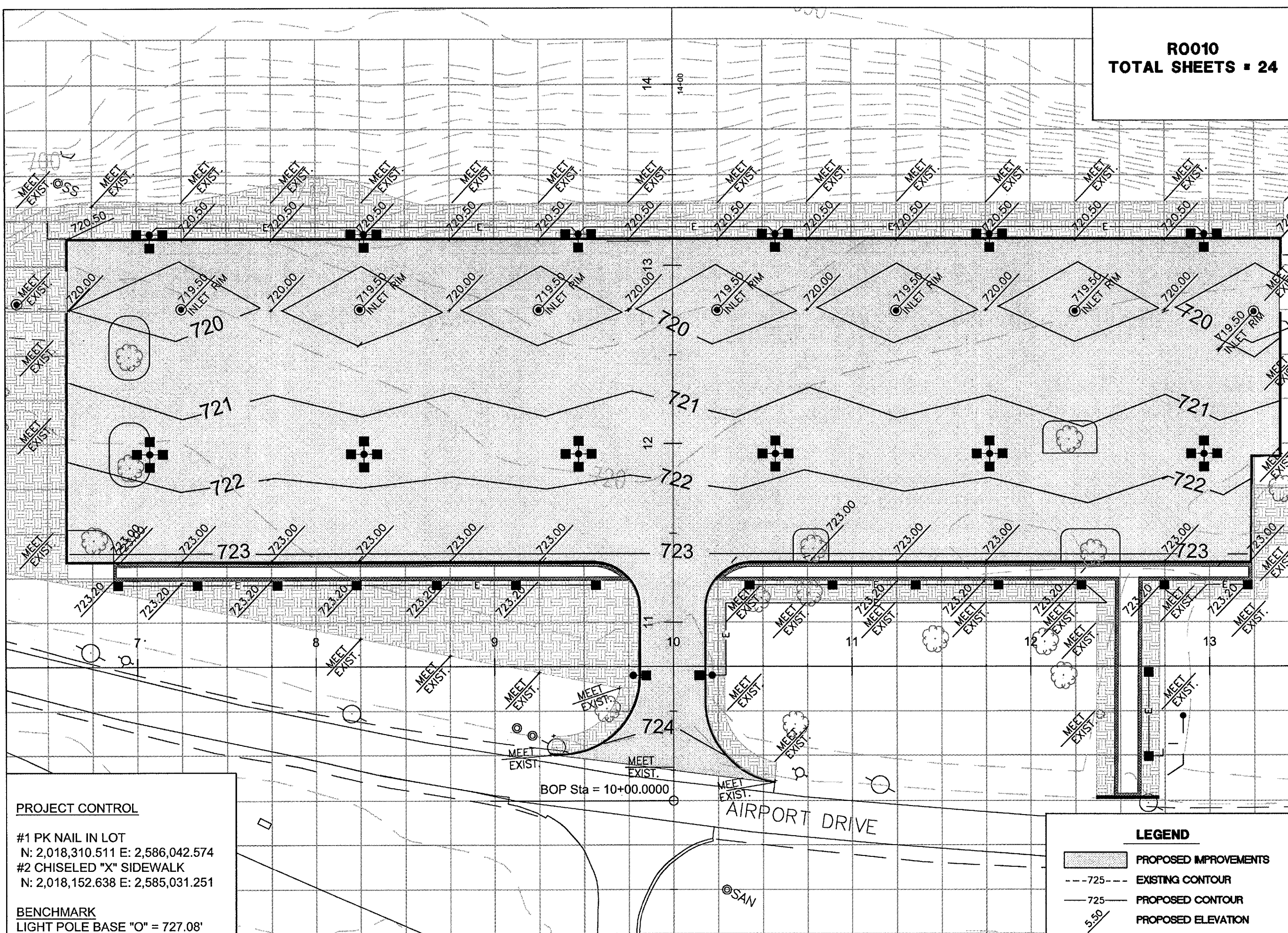
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AT FULL SCALE (17 X 11).

GREATER ROCKFORD AIRPORT AUTHORITY
ROCKFORD, ILLINOIS
ILLINOIS PROJECT, RFD-2820
NORTH AUTO PARKING
GRADING PLAN



DESIGN BY: JGP
DRAWN BY: CMT
CHECKED BY: JGP
APPROVED BY: *JGP*
DATE: 5.13.2005
JOB No: 05258-04-00

8 (GR1)
SHEET 8 OF 24 SHEETS



PROJECT CONTROL

#1 PK NAIL IN LOT
N: 2,018,310.511 E: 2,586,042.574
#2 CHISELED "X" SIDEWALK
N: 2,018,152.638 E: 2,585,031.251

BENCHMARK
LIGHT POLE BASE "O" = 727.08'


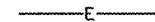






LEGEND

	PROPOSED IMPROVEMENTS
	EXISTING CONTOUR
	PROPOSED CONTOUR
	PROPOSED ELEVATION

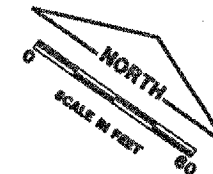
NOTES

- 1) THE ROUTING OF THE NEW CABLES AND CONDUITS SHOWN IS FOR INFORMATION ONLY. THE EXACT ROUTING SHALL BE COORDINATED WITH THE RESIDENT ENGINEER. ALL CONDUITS SHALL BE BURIED A MIN 30" BELOW FINISHED GRADE.
- 2) IT IS CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
- 3) ANY SPLICES TO THE NEW CABLES SHALL BE MADE INSIDE THE HANDHOLES OR POLE BASES. NO DIRECT BURIED SPLICES WILL BE ALLOWED. ALL SPLICES SHALL BE WATERPROOF AND SHALL HAVE HEAT SHRINK TUBING OVER ENTIRE SPLICE.
- 4) THE COST OF PAVEMENT REMOVAL AND REPLACEMENT OF PAVEMENT SHALL BE INCIDENTAL TO THE INSTALLATION OF THE DIRECT BURIED CONDUIT.
- 5) GROUND ROD SHALL BE 3/4" DIA. x 10' LONG. CONTRACTOR SHALL ROUTE 1 #8 GND. WIRE FROM GROUND ROD TO POWER PANEL. GROUND ROD SHALL BE BURIED 1'-0" BELOW FINISHED GRADE. CONNECTIONS TO GROUND ROD SHALL BE EXOTHERMIC TYPE.
- 6) CONTRACTOR SHALL COORDINATE ALL SERVICE CONNECTIONS WITH UTILITY. CONTRACTOR SHALL PROVIDE 10'-0" RISER AT POLE AND COIL UP SUFFICIENT CABLES AT TRANSFORMER FOR UTILITY CONNECTIONS.
- 7) THE LOCATION OF NEW ELECTRICAL TRANSCLASURE IS APPROXIMATE AND SHALL BE FIELD ADJUSTED TO THE SATISFACTION OF THE ENGINEER. SEE TRANSCLASURE DETAIL SHEET.

LEGEND

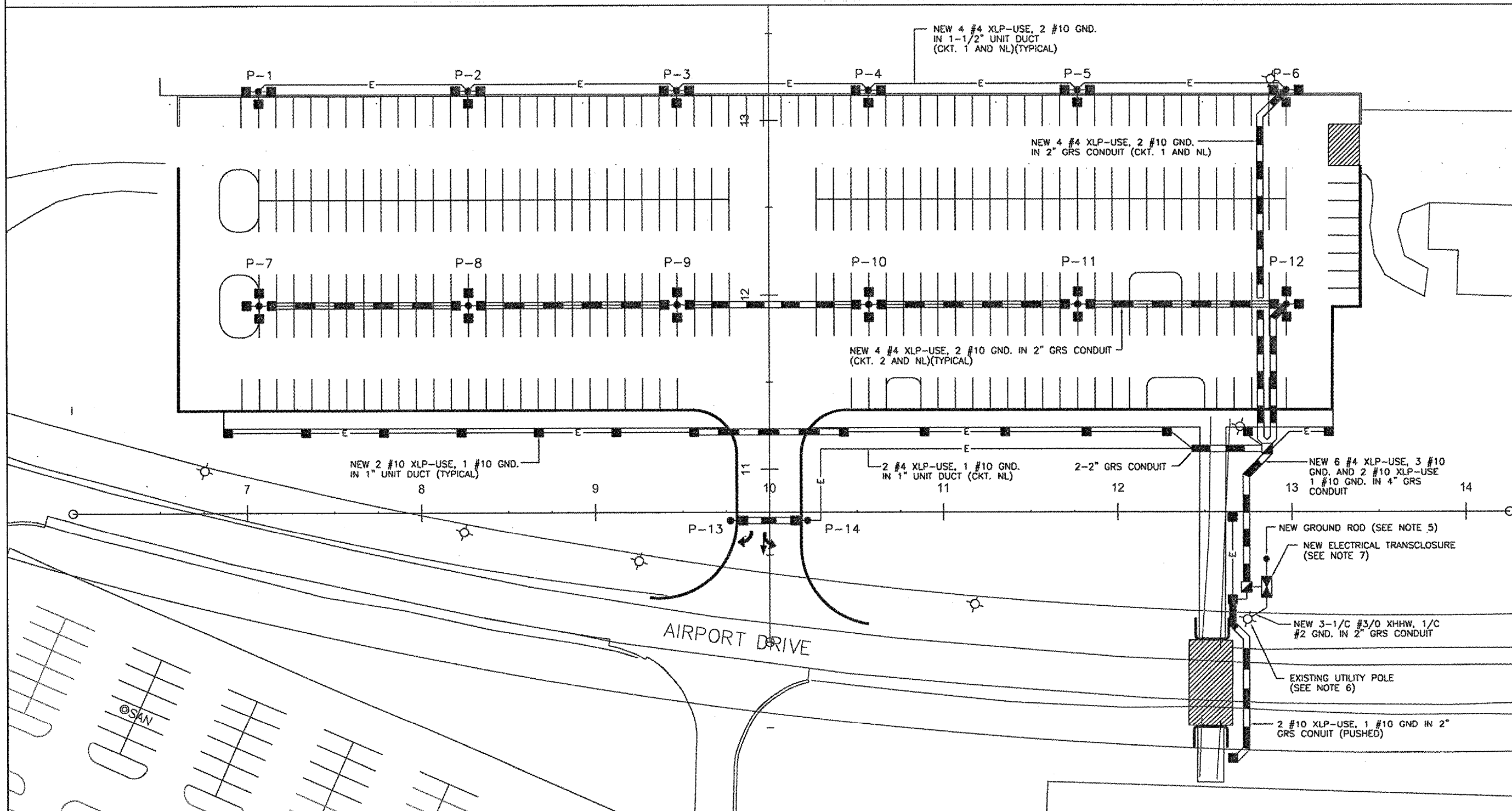
-  NEW 2" GRS CONDUIT
-  NEW ELECTRIC CABLE IN UNIT DUCT
-  NEW ELECTRICAL HANDHOLE
-  NEW WALKWAY BOLLARD LIGHTING
-  NEW TYPE A AREA LIGHT POLE W/1 FIXTURE
-  NEW TYPE A AREA LIGHT POLE W/3 FIXTURES
-  NEW TYPE A AREA LIGHT POLE W/4 FIXTURES
-  NEW ELECTRICAL TRANSCLASURE

RO010
TOTAL SHEETS = 24




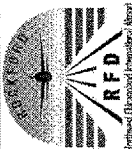
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
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GREATER ROCKFORD AIRPORT AUTHORITY
ROCKFORD, ILLINOIS
ILLINOIS PROJECT: RFD-2820
NORTH AND WEST AUTO PARKING
ELECTRICAL PLAN

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 CHECKED BY: AB
 APPROVED BY: 
 DATE: 5.13.2005
 JOB No: 05258-04-00

NOTES

- 1) IT IS CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
- 2) CONNECTION TO EXISTING STORM SEWER AND ALL OTHER MISCELLANEOUS ITEMS REQUIRED FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO PAY ITEM "AR751560 MANHOLE 6" ".
- 3) SEE DRAINAGE DETAIL SHEET FOR INLET INFORMATION.
- 4) TOPSOIL AT ALL SODDED AREAS SHALL BE PAID FOR UNDER PAY ITEM "AR905520, TOPSOILING (FROM OFF SITE)".
- 5) TOPSOIL SHALL BE PLACED A MINIMUM OF 4" AT ALL LANDSCAPING AREAS. UNCLASSIFIED EXCAVATION SHALL BE USED TO BALANCE THE TOPSOIL QUANTITY. PLACEMENT OF THE TOPSOIL SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

**R0010
TOTAL SHEETS = 24**



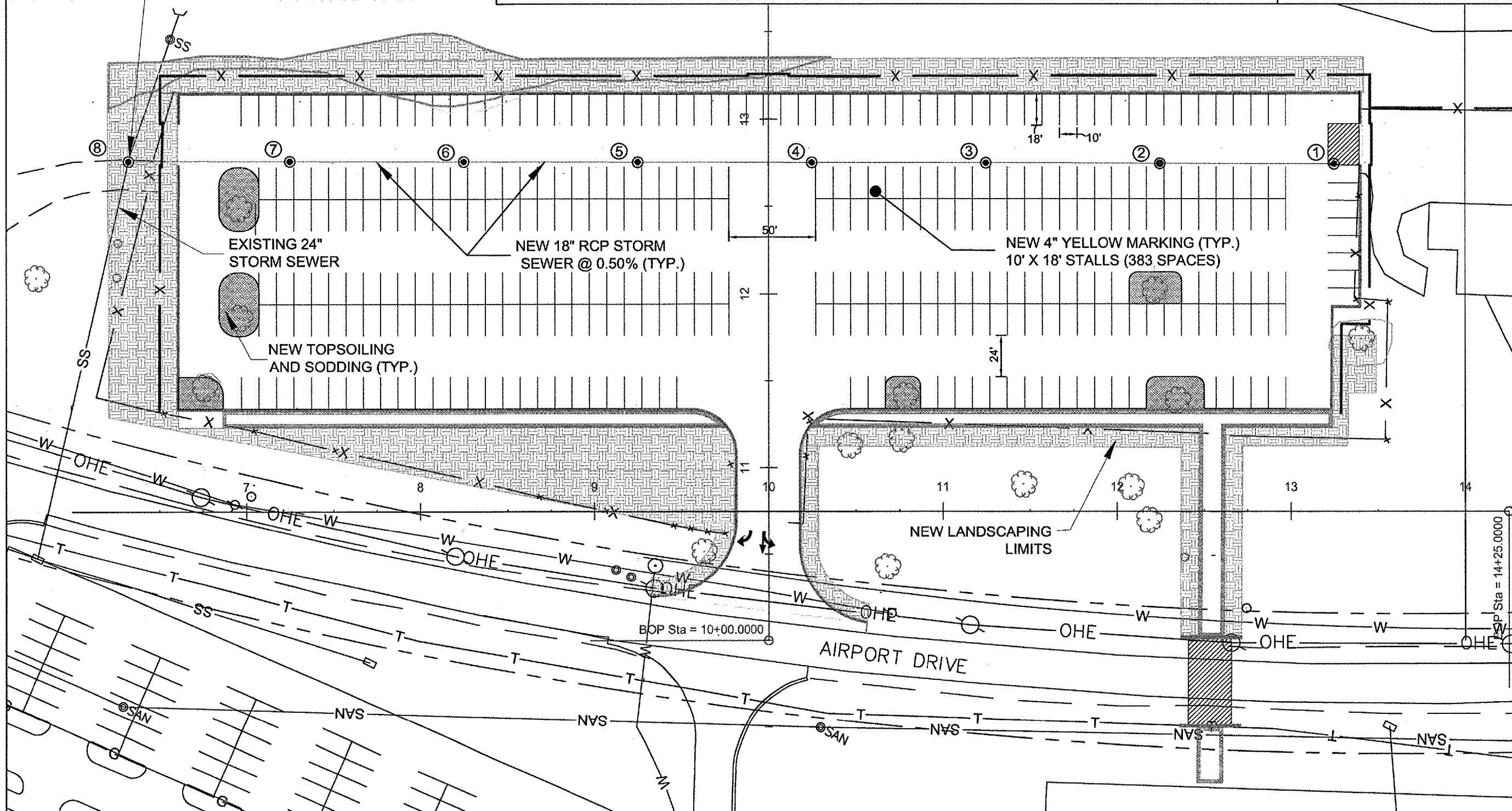
LEGEND

- LANDSCAPING LIMITS
- TOPSOILING / SODDING
- NEW A-6 MANHOLE
- NEW 18" STORM SEWER

REVISIONS		
NUMBER	BY	DATE

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THIS BAR IS EQUAL TO 1" AT FULL SCALE (17 X 11).

SEE NOTE 2



**GREATER ROCKFORD AIRPORT AUTHORITY
ROCKFORD, ILLINOIS**
ILLINOIS PROJECT, RFD-2820
**NORTH AND WEST AUTO PARKING
PROPOSED DRAINAGE / MARKING
AND LANDSCAPING PLAN**

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS

**Northwest
Chicago
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Airport**
RFD

DESIGN BY: TAS
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 APPROVED BY:
 DATE: 5.13.2005
 JOB No: 05258-04-00

STRUCTURE SCHEDULE

NO.	STATION/OFFSET	STRUCTURE TYPE	PROPOSED RIM ELEV.	PROPOSED INV. ELEV.
1	STA. 12+75 325.00' RT	NEW TYPE A-6 MANHOLE W/ FLATTOP, TYPE 1 FRAME AND LID	719.50	W. = 717.00
2	STA. 12+75 225.00' RT	NEW TYPE A-6 MANHOLE W/ FLATTOP, TYPE 1 FRAME AND LID	719.50	E. = 716.50 W. = 716.40
3	STA. 12+75 125.00' RT	NEW TYPE A-6 MANHOLE W/ FLATTOP, TYPE 1 FRAME AND LID	719.50	E. = 715.90 W. = 715.80
4	STA. 12+75 25.00' RT	NEW TYPE A-6 MANHOLE W/ FLATTOP, TYPE 1 FRAME AND LID	719.50	E. = 715.30 W. = 715.20
5	STA. 12+75 75.00' LT	NEW TYPE A-6 MANHOLE W/ FLATTOP, TYPE 1 FRAME AND LID	719.50	E. = 714.70 W. = 714.60
6	STA. 12+75 175.00' LT	NEW TYPE A-6 MANHOLE W/ FLATTOP, TYPE 1 FRAME AND LID	719.50	E. = 716.50 W. = 716.40
7	STA. 12+75 275.00' LT	NEW TYPE A-6 MANHOLE W/ FLATTOP, TYPE 1 FRAME AND LID	719.50	E. = 714.10 W. = 714.00
8	STA. 12+75 368' LT	NEW TYPE A-6 MANHOLE W/ FLATTOP, TYPE 1 FRAME AND LID	MATCH EXISTING GRADE	E = 713.54 N = FIELD VERIFY S = FIELD VERIFY

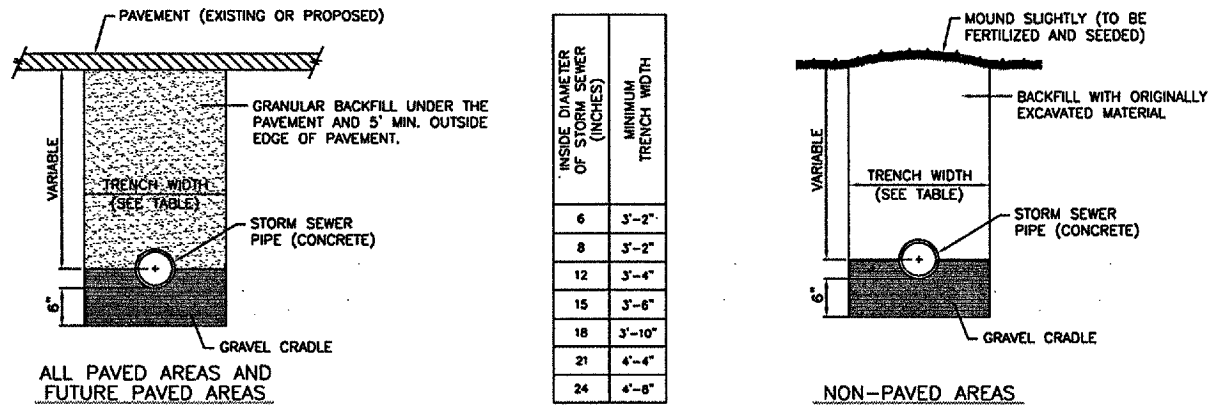
GENERAL NOTES

- STRUCTURAL CONCRETE (ITEM 610) SHALL BE USED.
- EXPOSED EDGES SHALL BE BEVELED 1".
- SHOP DRAWINGS WILL BE REQUIRED FOR EACH STRUCTURE AND FRAME AND FOR GRATES.
- A 6" GRANULAR BEDDING CONFORMING TO SECTION 751 SHALL BE PROVIDED UNDER THE FULL LENGTH AND WIDTH OF THE PRECAST UNIT. ALL VOIDS AROUND THE PIPE ENTRANCE, BOTH INSIDE AND OUTSIDE, SHALL BE SEALED WITH A HYDRAULIC MORTAR.
- THE GRATING SHALL SEAT FIRMLY IN THE FRAME AND SHALL BE SECURED TO THE FRAME EXCEPT AS SHOWN AND NOTED. THE LENGTH AND WIDTH OF GRATING SHALL BE SUCH AS TO LEAVE NO MORE THAN 5/8" CLEARANCE ON EITHER SIDE WHEN PLACED IN THE FRAME. THE STEEL GRATING SHALL BE CUT IN SUCH A MANNER THAT ALL RIVETED OR WELDED CONNECTIONS ARE LEFT IN TACT.
- ALL CASTINGS SHALL BE NEENAH TYPE C OPEN LID OR EQUAL. THESE CASTINGS SHALL BE BICYCLE SAFE.

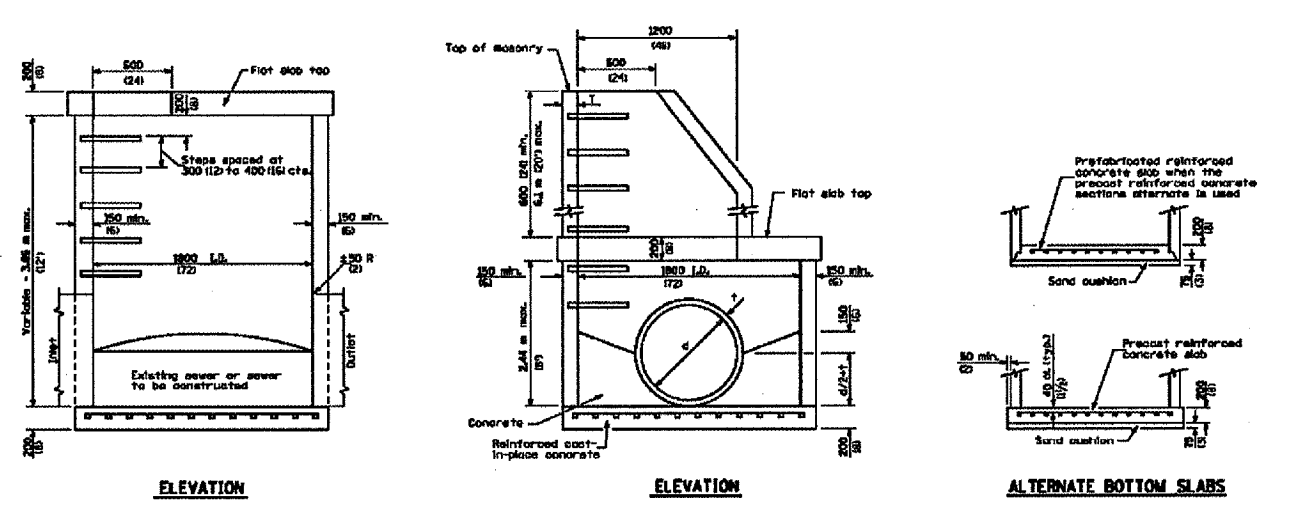
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TOTAL SHEETS = 24

REVISIONS		
NUMBER	BY	DATE

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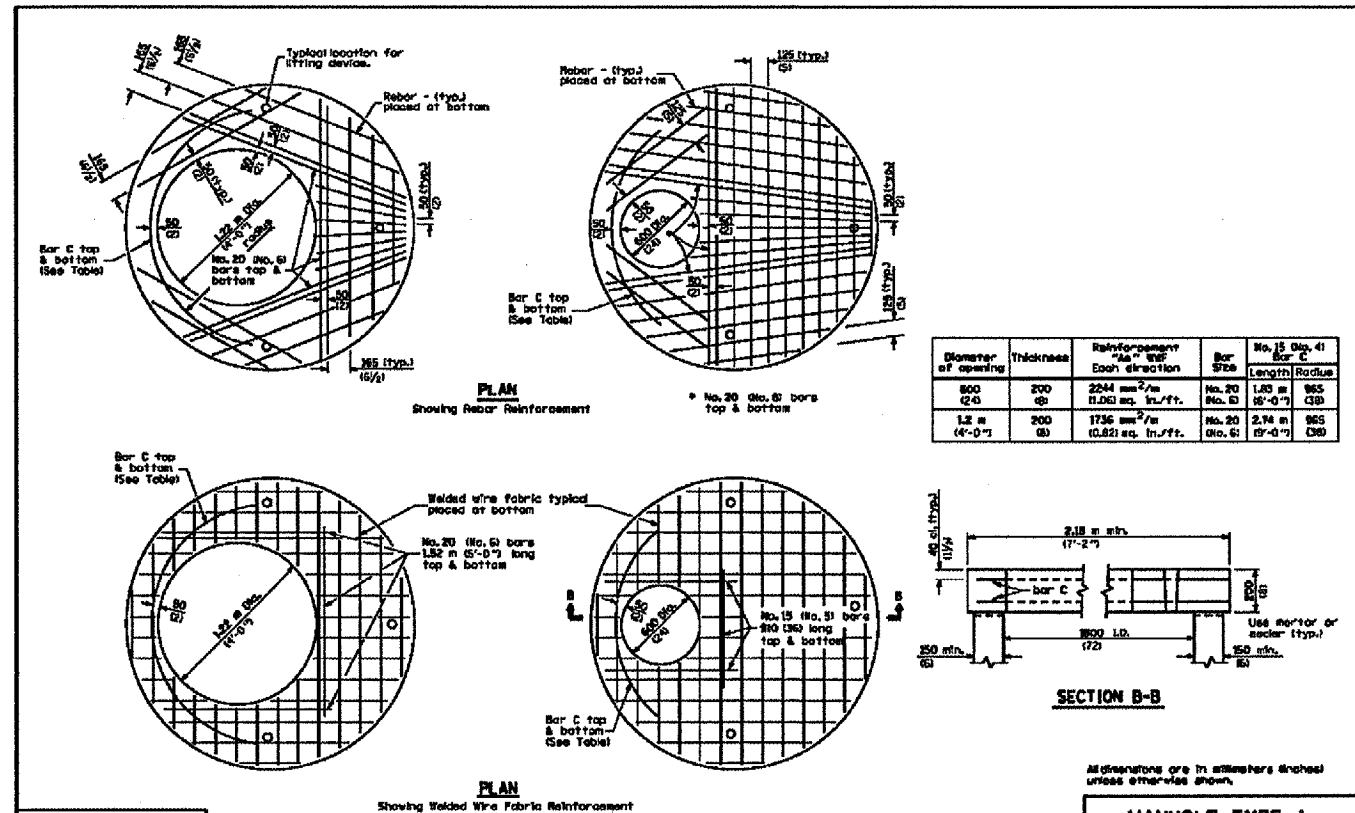
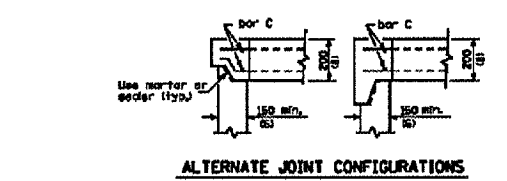
TRENCH DETAILS - STORM SEWER



GENERAL NOTES

Joint configuration and dimensions of flat slab top shall match and fit the riser joint detail.
Lifting devices shall be approved by the Engineer.
Bottom slabs shall be reinforced with a minimum of 315 mm²/m (24.4 sq. in./ft.) in both directions.
See Standard 602401 for details of cast-in-place concrete.
All dimensions are in millimeters unless otherwise shown.

ALTERNATE MATERIALS FOR WALLS	Y
Concrete Masonry Units	125 (5)
Precast Reinforced Concrete Sections	100 (4)
Cast-in-Place Concrete	150 (6)



Diameter of opening	Thickness	Reinforcement "As" "Typ" Each direction	Bar Size	No. 15 Dia. 41 Length/Radius
800 (24)	200 (8)	2244 mm ² /m (17.02 sq. in./ft.)	No. 20 (No. 1) 180 (7.1)	865 (33)
1.2 m (4'-0")	200 (8)	1736 mm ² /m (13.42 sq. in./ft.)	No. 20 (No. 1) 180 (7.1)	865 (33)

MANHOLE TYPE A
1800 mm (72") DIAMETER
STANDARD 602406-01

Illinois Department of Transportation
Standard 602406-01

DATE	REVISIONS
1-1-03	Added general note for reinforcement.
1-1-01	Revised Standard 2335-1. Deleted Dit Symbol. Rev. bars add to opening.

MANHOLE TYPE A
1800 mm (72") DIAMETER
STANDARD 602406-01
Sheet 1 of 2

Illinois Department of Transportation
Standard 602406-01

GREATER ROCKFORD AIRPORT AUTHORITY
ROCKFORD, ILLINOIS
ILLINOIS PROJECT: RFD-2820
NORTH AND WEST AUTO PARKING
DRAINAGE DETAILS

CMT
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CONSULTING ENGINEERS
Northwest Chicago International Airport

DESIGN BY: TAS
DRAWN BY: CMT
CHECKED BY: TAS
APPROVED BY: *TAS*
DATE: 5.13.2005
JOB No: 05258-04-00
11 (DRD)
SHEET 11 OF 24 SHEETS

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS EXPANDING A PARKING LOT AT THE GREATER ROCKFORD AIRPORT. THE PROJECT INCLUDES PAVEMENT REMOVAL, EXCAVATION, EMBANKMENT, DRAINAGE, VARIOUS PAVEMENT ITEMS, ELECTRICAL IMPROVEMENTS AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

1. EXCAVATION AND EMBANKMENT WILL BE COMPLETED WITHIN THE PROJECT LIMITS TO GRADE OUT FOR THE PROPOSED DRAINAGE AND PAVEMENT IMPROVEMENTS.
2. STORM SEWER, MANHOLE AND INLET INSTALLATION.
3. PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL, SUCH AS PERIMETER SILT FENCE, TEMPORARY DITCH CHECKS AND INLET PROTECTION.
4. PAVEMENT CONSTRUCTION.
5. ELECTRICAL IMPROVEMENTS.
6. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
7. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCHING.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 3.8 ACRES OF WHICH 3.8 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE ROCK RIVER THROUGH A STORM SEWER SYSTEM.

CONTROLS-EROSION CONTROLS AND SEDIMENT CONTROL

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION

1. THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS POSSIBLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, TEMPORARY DITCH CHECKS, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
 - A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
 - B. CONSTRUCT DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.
 - C. BUILD NECESSARY EMBANKMENT AT CULVERT/STORM SEWER LOCATIONS AND THEN EXCAVATE AND PLACE PIPE.
 - D. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.

4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

5. THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2 INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD.

6. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

7. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

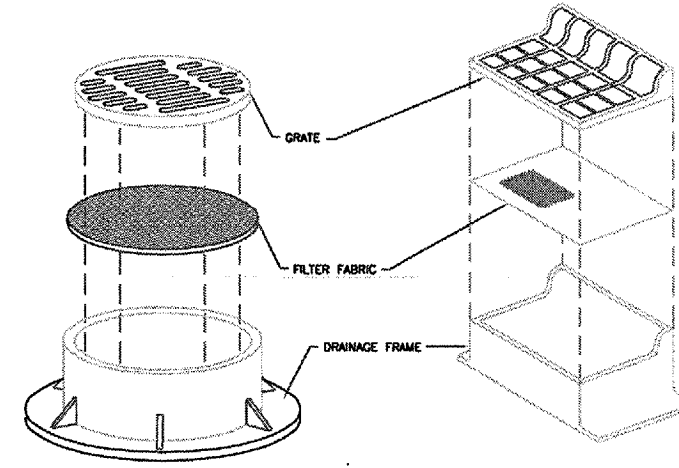
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TOTAL SHEETS = 24



REVISIONS

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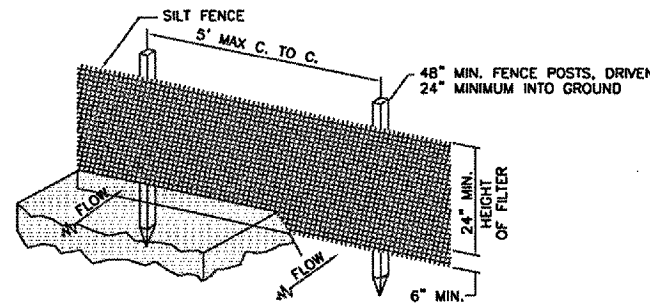


DRAINAGE STRUCTURE FILTER WRAP

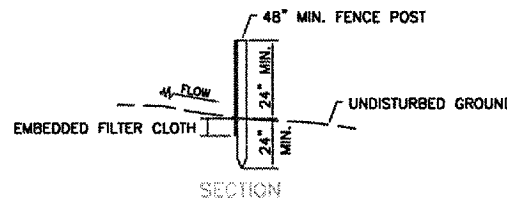
N.T.S.

NOTES

1. FILTER WRAP TO BE PLACED IN ALL INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
2. FABRIC SHALL BE IN CONFORMANCE WITH MATERIALS SPECIFIED FOR SILT FENCE.
3. FABRIC SHALL OVERLAY FRAME BY 2" (MIN.).
4. CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
5. FABRIC SHALL REMAIN IN PLACE UNTIL TURFED AREAS HAVE DEVELOPED A MIN. OF 80% OF COVERAGE.
6. COST OF FILTER WRAP SHALL BE CONSIDERED INCIDENTAL TO BALES.



PERSPECTIVE VIEW



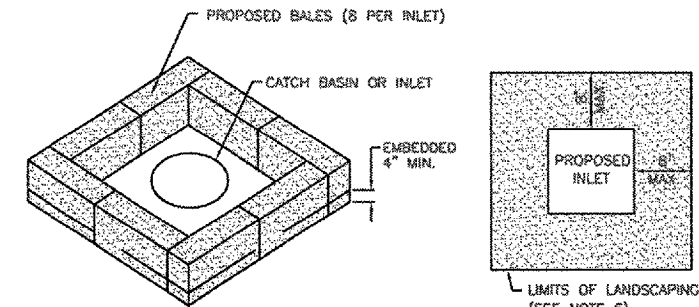
SECTION

EROSION CONTROL FABRIC FENCE DETAIL

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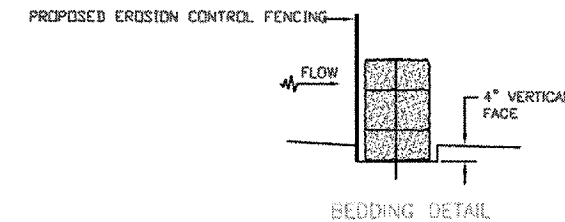
CONSTRUCTION NOTES FOR SILT FENCE

1. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVER-LAPPED BY 6" MIN. AND FOLDED.
2. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE, SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.
3. SILT FENCE SHALL BE INSTALLED PER STORMWATER POLLUTION PREVENTION PLAN OR AS DIRECTED BY THE ENGINEER.



INLET PLACEMENT

INLET PLACEMENT



INLET PROTECTION - TURF AREAS

N.T.S.

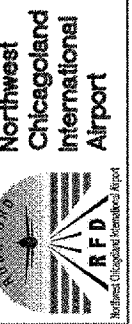
NOTES

1. BALES SHALL BE PLACED AT THE TOE OF SLOPE OR ON A CONTOUR AND IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES.
2. EACH BALE SHALL BE EMBEDDED IN THE SOIL A MINIMUM OF 4 INCHES, AND PLACED SO THE BINDINGS ARE HORIZONTAL.
3. BALES SHALL BE SECURELY ANCHORED IN PLACE BY EITHER TWO STAKES OR REBARS DRIVEN THROUGH THE BALE. THE FIRST STAKE IN EACH BALE SHALL BE DRIVEN TOWARD THE PREVIOUSLY LAID BALE AT AN ANGLE TO FORCE THE BALES TOGETHER. STAKES SHALL BE DRIVEN FLUSH WITH THE BALE.
4. INSPECTION SHALL BE FREQUENT AND REPAIR / REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
5. BALES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. COST OF REMOVAL / REPLACEMENT TO BE INCLUDED IN UNIT PRICE FOR BALES.
6. AFTER FINAL APPROVAL OF THE ENGINEER, STRAW BALES MAY BE REMOVED. CONTRACTOR SHALL PLACE SEED AND MULCH OVER THE DISTURBED AREAS, COST INCIDENTAL TO BALES.

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ILLINOIS PROJECT: RFD-2820

NORTH AND WEST AUTO PARKING
EROSION CONTROL DETAILS - 1

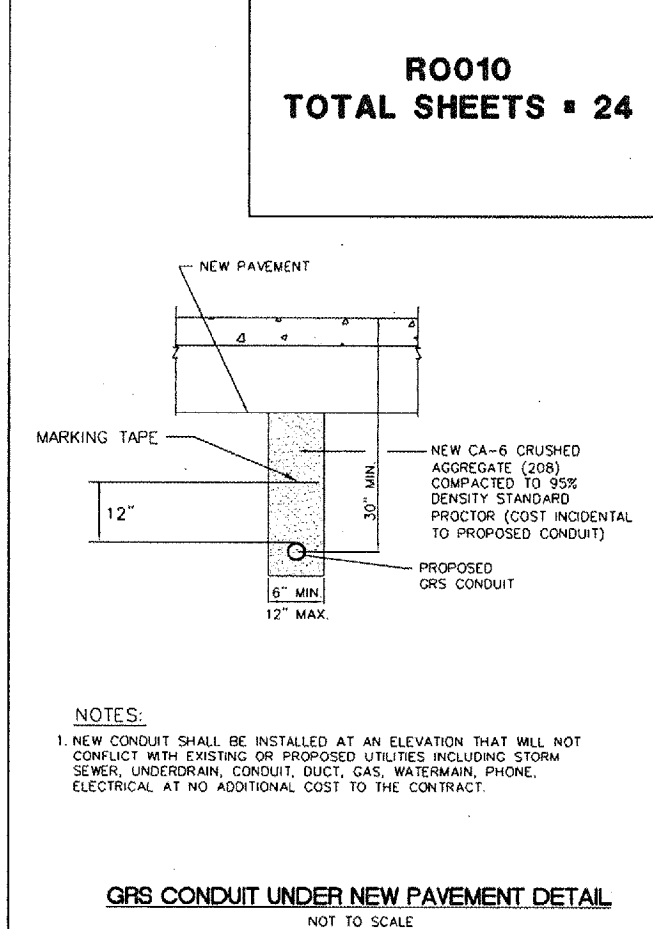
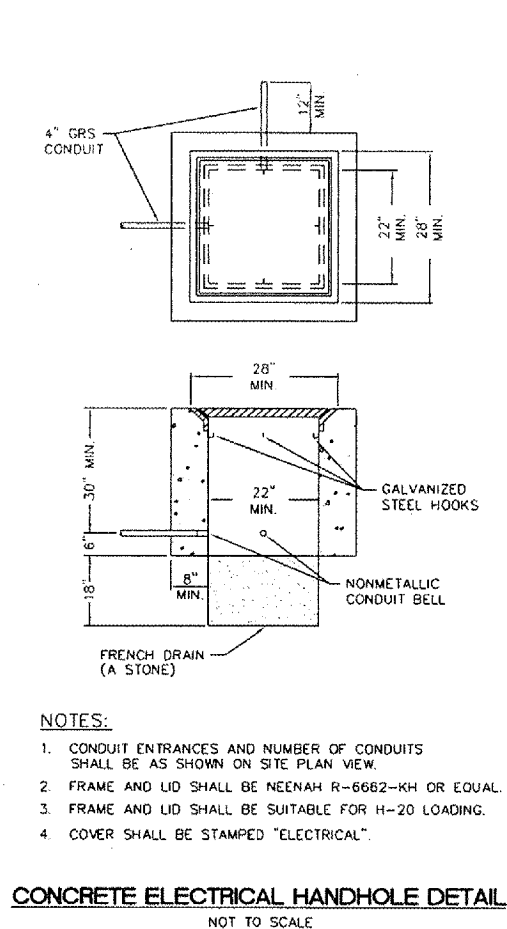
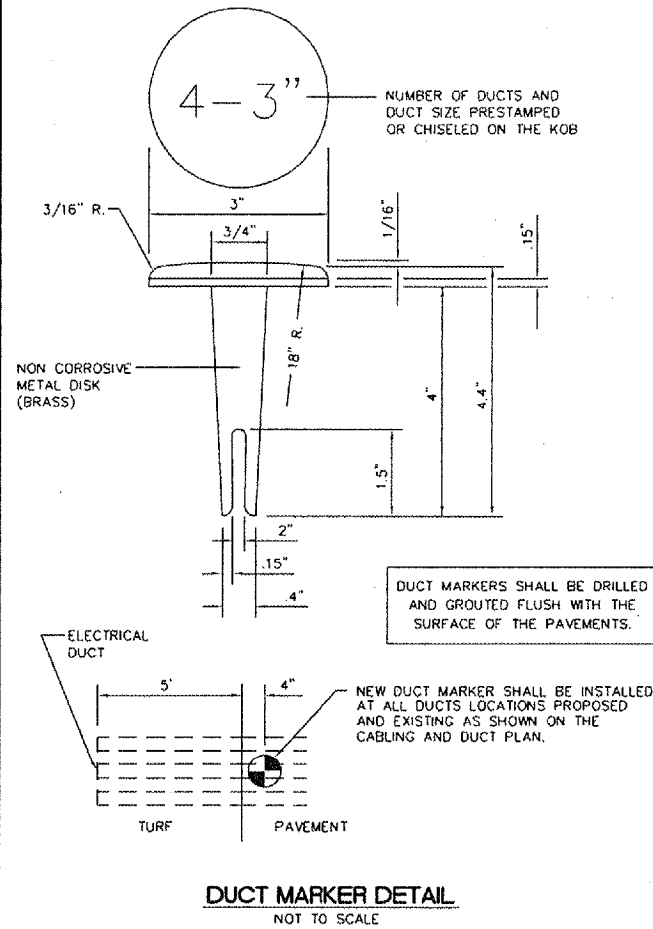


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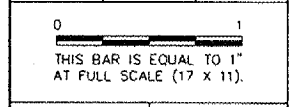
12 (ERD)

SHEET 12 OF 24 SHEETS

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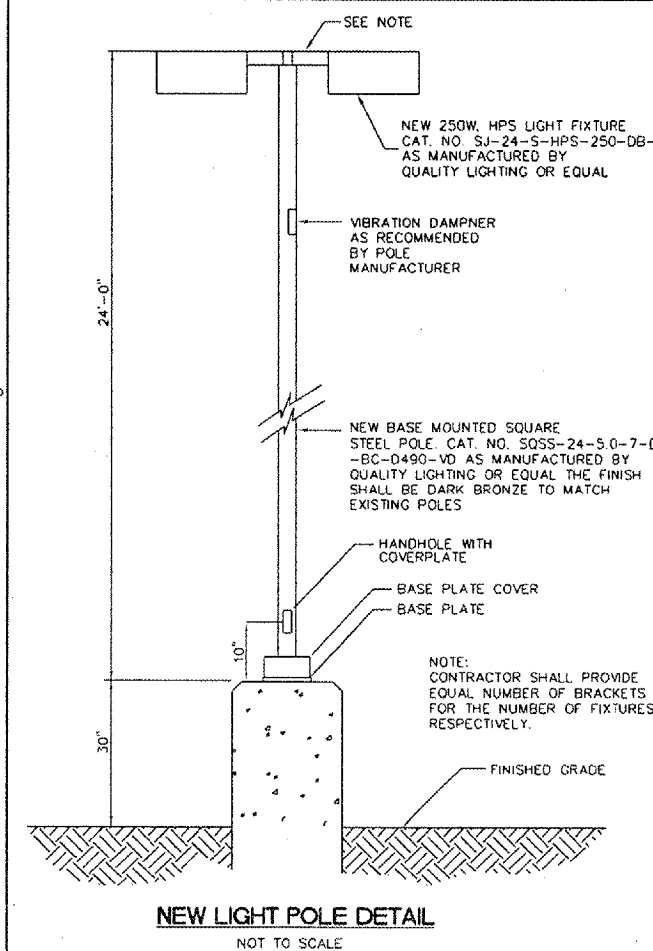
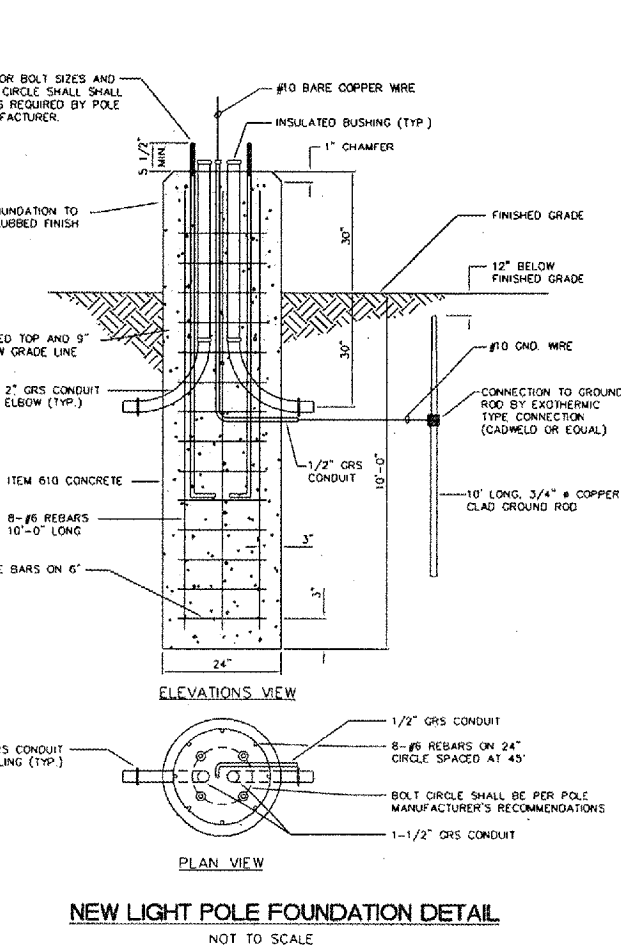
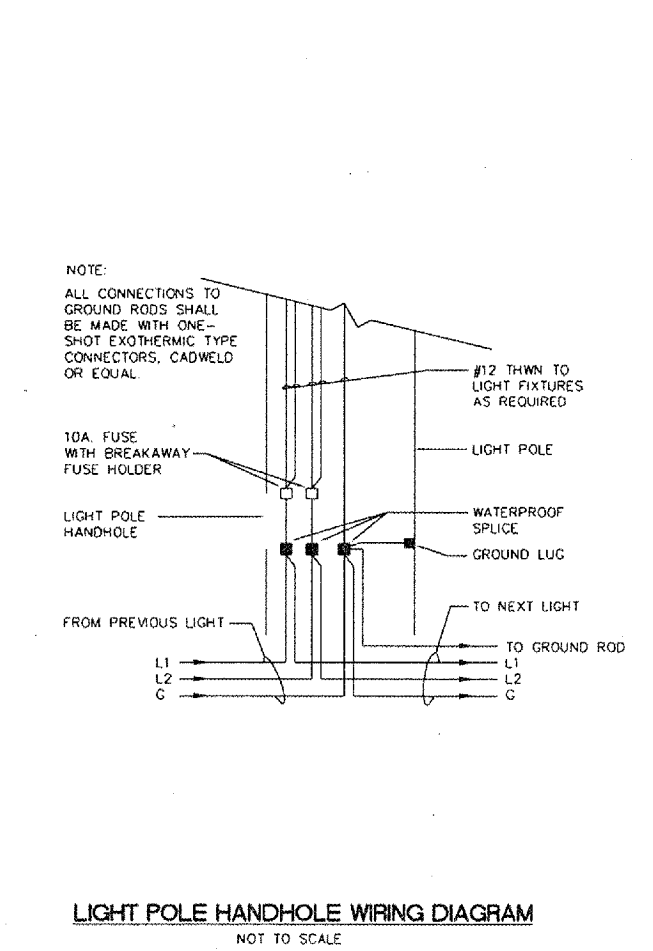


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**GREATER ROCKFORD AIRPORT AUTHORITY
ROCKFORD, ILLINOIS
ILLINOIS PROJECT: RFD-2820
NORTH AND WEST AUTO PARKING
ELECTRICAL DETAILS - 1**

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13 (ED1)

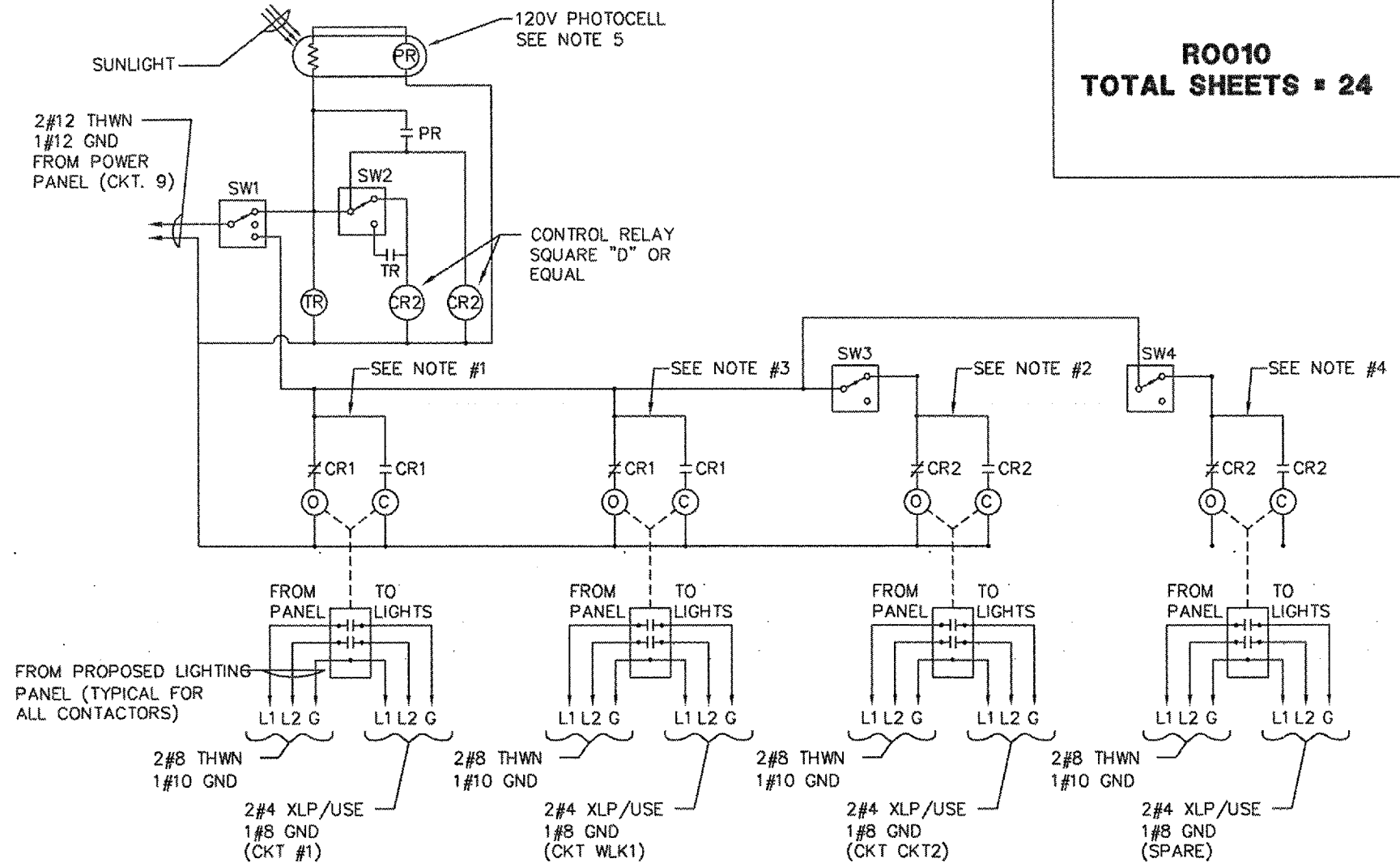
SHEET 13 OF 24 SHEETS

SWITCH SCHEDULE	
SWITCH	LABEL
SW1	HAND-OFF-AUTO
SW2	PHOTOCELL-TIME
SW3	(ON-OFF) CKT2
SW4	(ON-OFF) SPARE

PROPOSED LIGHT POLE SCHEDULE			
POLE #	NO. OF FIXTURES	CURCUIT # @ 240V	STATION & OFFSET
P-1	1	1,NL	9+40, 18.5' RT. BL. A
P-2	1	1	8+11.5, 18.5' RT. BL. A
P-3	3	1,NL	6+30, 18.5' RT. BL. A
P-4	3	1	22+36, 65' LT. BL. B
P-5	3	1,NL	24+26, 65' LT. BL. B

NOTES:

1. THE ROUTING OF THE PROPOSED CABLES AND CONDUIT SHOWN IS FOR INFORMATION ONLY, THE EXACT ROUTING SHALL BE COORDINATED WITH THE ENGINEER. THE MINIMUM BURIAL DEPTH OF THE CABLES SHALL BE 30" BELOW FINISHED GRADE.
2. IT IS THE CONTRACTORS RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
3. ANY SPLICES TO THE PROPOSED CABLES SHALL BE MADE INSIDE THE HANDHOLE OR POLE BASES. NO DIRECT BURIED SPLICES WILL BE ALLOWED. ALL SPLICES SHALL BE WATER PROOF AND SHALL HAVE HEAT SHRINK TUBING OVER THE ENTIRE SPLICE.
4. ALL AREAS DISTURBED AS A RESULT OF THE WORK UNDER THIS CONTRACT SHALL BE RESTORED, THE COST OF RESTORATION SHALL BE INCIDENTAL TO THE RESPECTIVE ITEMS.
5. WHEN CROSSING UNDERGROUND UTILITIES, CONTRACTOR SHALL HAND DIG THE TRENCH TO AVOID ANY DAMAGE. CONTRACTOR MAY BE REQUIRED TO ROUTE THE PROPOSED CABLES UNDER THE EXISTING UTILITIES AS DIRECTED BY THE RESIDENT ENGINEER. NO ADDITIONAL COMPENSATION WILL BE MADE FOR WORK REQUIRED TO PLACE CABLES BENEATH EXISTING UTILITIES.
6. GROUND ROD SHALL BE 3/4" DIA. 10' LONG, CONTRACTOR SHALL ROUTE 1-#4 GND. WIRE FROM GROUND ROD POWER PANEL GROUND LUG IN LIGHTING CONTROLLER. GROUND ROD SHALL BE BURIED 1'-0" BELOW FINISH GRADE. CONNECTIONS TO GROUND ROD SHALL BE ONE-SHOT EXOTHERMIC TYPE.
7. THE LOCATION OF THE LIGHTING CONTROLLER SHOWN IS FOR INFORMATION PURPOSES ONLY AND SHALL BE FIELD ADJUSTED TO THE SATISFACTION OF THE ENGINEER.
8. CONTRACTOR SHALL PROVIDE 2" RISER AT UTILITY POLE. CONTRACTOR SHALL LEAVE MINIMUM 10'-0" SLACK OF CABLES FOR TERMINATION BY THE UTILITY COMPANY. THE SERVICE SHALL BE 60 AMP 120/240 VAC 1-PHASE.
9. CONTRACTOR SHALL INSTALL PHOTOCELL ON PEDESTAL MOUNTED CABINET.



LIGHTING CONTACTORS WIRING SCHEMATIC

NOT TO SCALE

NOTES:

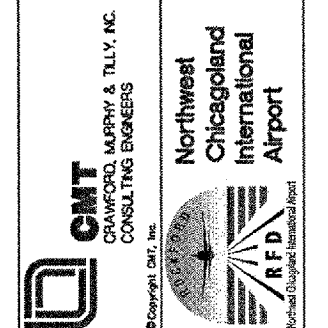
1. 2 POLE , 50 AMP, MECHANICALLY HELD LIGHTING CONTACTOR FOR CKT #1.
2. 2 POLE, 50 AMP, MECHANICALLY HELD LIGHTING CONTACTOR FOR CKT #2.
3. 2 POLE, 50 AMP, MECHANICALLY HELD LIGHTING CONTACTOR FOR NIGHT LIGHT CIRCUIT. (WLK1)
4. 2 POLE, 50 AMP MECHANICALLY HELD LIGHTING CONTACTOR FOR SPARE.
5. PHOTOCELL SHALL BE MOUNTED ON TOP OF CONTROLLER. CONTRACTOR SHALL SHIELD PHOTOCELL FROM SURROUNDING LIGHTS AT NO ADDITIONAL COST.

**R0010
TOTAL SHEETS = 24**

REVISIONS		
NUMBER	BY	DATE

0 1
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ROCKFORD, ILLINOIS
ILLINOIS PROJECT. RFD-2820
NORTH AND WEST AUTO PARKING
ELECTRICAL DETAILS - 2**



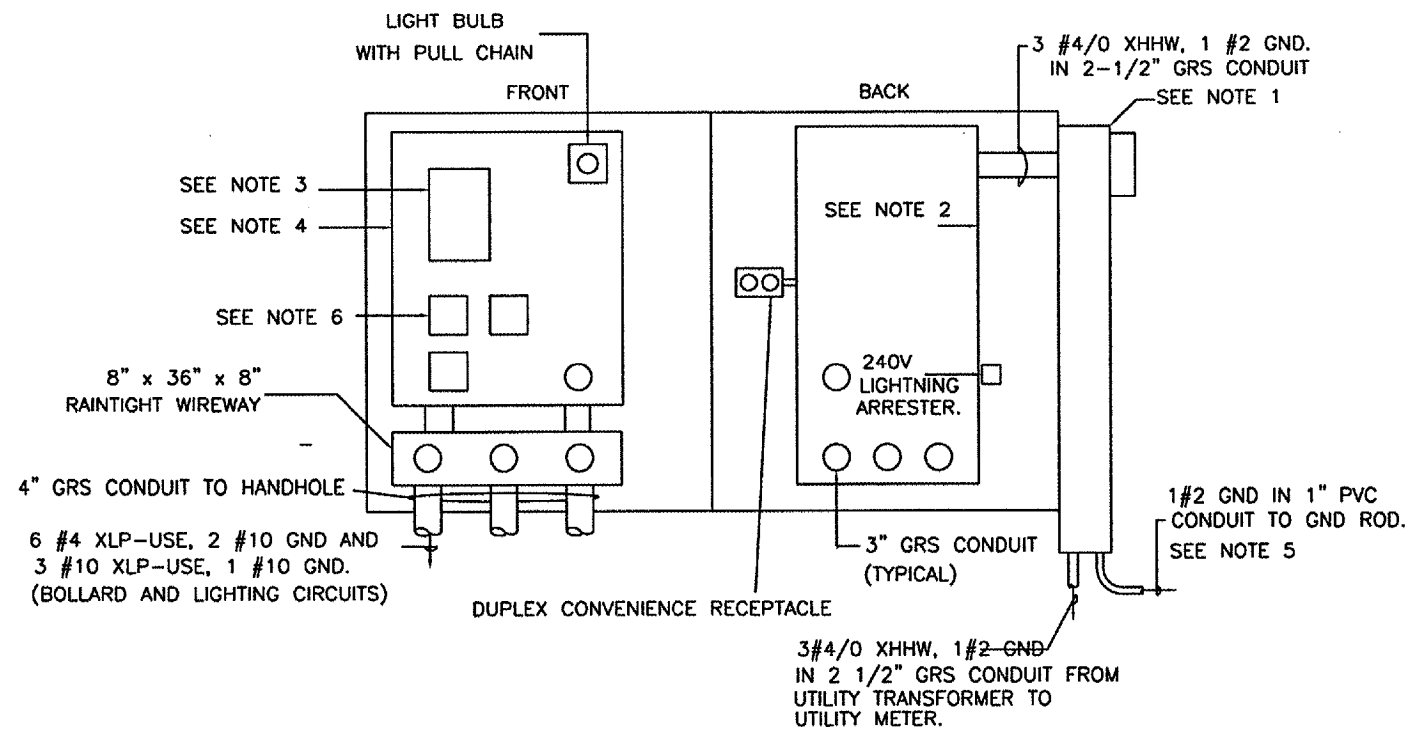
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14 (ED2)

LIGHTING PANEL SCHEDULE			
CIRCUIT NO.	POLE NO.	CIRCUIT BREAKER SIZE	USAGE
A-1	1,3	200A	MAIN CIRCUIT BREAKER
A-2	2,4	30A	PARKING LOT LIGHTING CKT. 1
A-3	5,7	30A	PARKING LOT LIGHTING CKT. 2
A-4	8	15A	NIGHT LIGHT (NL)
A-5	9	15A	LIGHT INSIDE CABINET
A-6	10,12	30A	TIMECLOCK
			BOLLARD LIGHTS

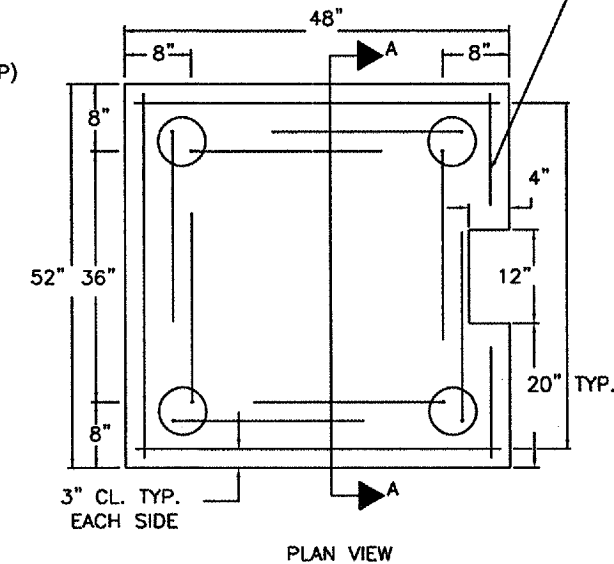
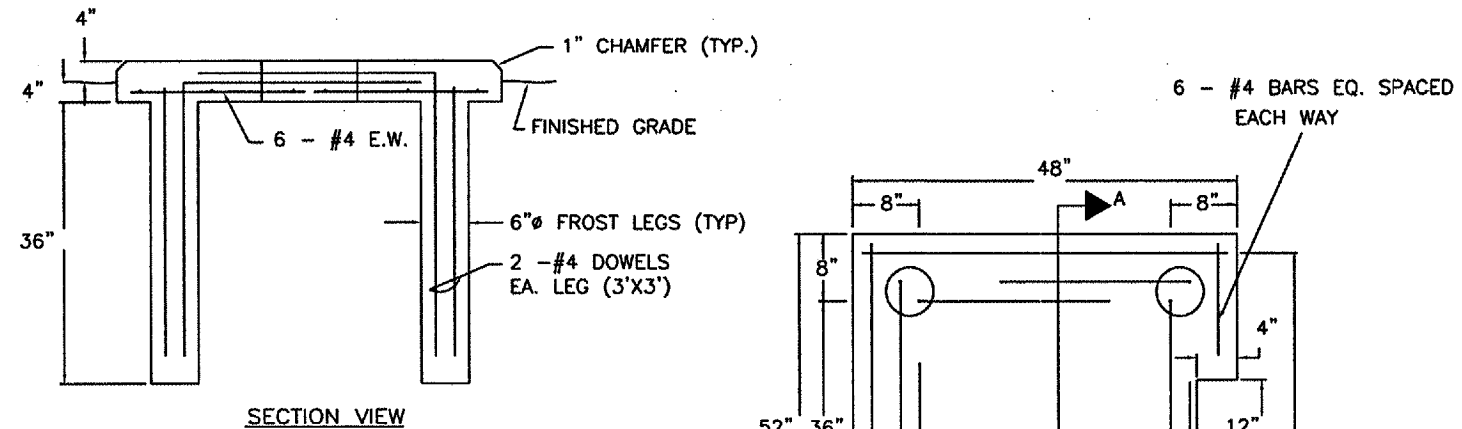
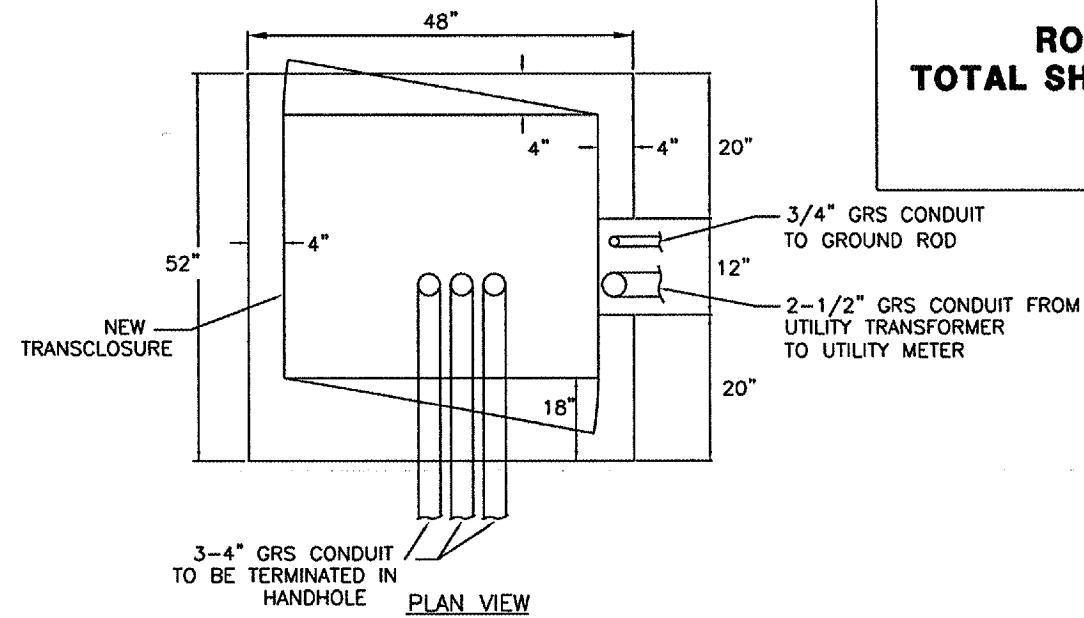
NOTES

- UTILITY METER. THE NEW ELECTRIC SERVICE SHALL BE 200 AMP, 120/240 VOLT, 1 PHASE, 3-WIRE 60 HZ SERVICE. UTILITY METER SHALL BE INSTALLED ON THE SIDE OF TRANSCLOSURE. CONTRACTOR SHALL SUPPLY THE BASE FOR METER AND COORDINATE SERVICE CONNECTION WITH UTILITY COMPANY.
- 20 CIRCUIT LIGHTING PANEL WITH 200 AMP 2-POLE MAIN CIRCUIT BREAKER.
- 8 CIRCUIT PROGRAMMABLE TIMECLOCK, MODEL NO. ET70815CR AS MANUFACTURED BY INTERMATIC OR EQUAL.
- 36" x 36" x 8" JUNCTION BOX HOUSING FOR TIMECLOCK AND CONTACTORS.
- GROUND ROD SHALL BE 3/4" DIA. x 10'-0" COPPER CLAD. ALL CONNECTIONS TO GROUND ROD SHALL BE ONE-SHOT EXOTHERMIC TYPE.
- 30A, 2-POLE LIGHTING CONTACTOR. (TYP. OF 4)



ELECTRICAL TRANSCLOSURE DETAIL

NOT TO SCALE



- CONCRETE SHALL HAVE A COMPRESSIVE STRENGTH AT 14 DAYS OF 3500 PSI
- REINFORCING STEEL SHALL BE A-615 GRADE 60
- ALL EXPOSED EDGES AND EQUIPMENT PADS SHALL BE CHAMFERED 1"
- CONTRACTOR SHALL INSTALL CONDUITS THROUGH PAD AS REQUIRED. CONDUITS NOT SHOWN FOR CLARITY.
- DIMENSIONS ARE APPROXIMATE AND SHALL BE FIELD VERIFIED.

CONCRETE PAD FOR ELECTRICAL TRANSCLOSURE

NOT TO SCALE

R0010
TOTAL SHEETS = 24



REVISIONS

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0 1
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GREATER ROCKFORD AIRPORT AUTHORITY
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ILLINOIS PROJECT: RFD-2820
NORTH AND WEST AUTO PARKING
ELECTRICAL DETAILS - 3

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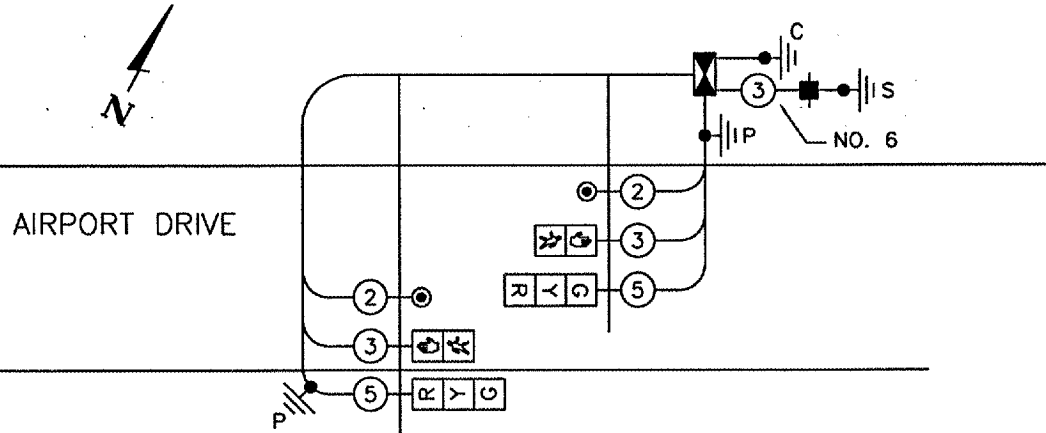
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15 (ED3)

NOTES

1. TRAFFIC CONTROL EQUIPMENT SHALL BE MANUFACTURED BY EAGLE.
2. A BATTERY BACKUP SYSTEM NOVUS XT 1000P-700 WATT (MANUFACTURED BY ALPHA TECHNOLOGIES) SHALL BE INSTALLED IN THE TYPE V CONTROLLER CABINET WITH A BMS-4 EXTERNAL BATTERY ENCLOSURE. THE EXTERNAL BATTERY ENCLOSURE SHOULD MOUNT TO THE POWER PANEL SIDE OF THE TYPE V CABINET WITH 4 SHELVES TO STORE THE BATTERIES. THE SYSTEM SHALL BE CAPABLE OF 3-HOUR MINIMUM BATTERY LIFE TO PROVIDE ONE HOUR OF FULL FUNCTIONING SIGNAL AND 2 HOURS OF FLASHING RED.
3. ALL TRAFFIC SIGNAL HEADS SHALL BE L.E.D. (LIGHT EMITTING DIODE) AND MANUFACTURED BY DIALIGHT OR APPROVED EQUIVALENT.
4. ALL PEDESTRIAN TRAFFIC SIGNAL HEADS SHALL BE L.E.D. (LIGHT EMITTING DIODE) WITH COUNTDOWN FEATURE AND MANUFACTURED BY DIALIGHT OR APPROVED EQUIVALENT.
5. CURB RAMP FOR SIDEWALKS SHALL BE CONSTRUCTED AT ALL CROSS-WALKS. (PER IDOT STD. 424001-03)
6. ALL TRAFFIC SIGNAL EQUIPMENT IS TO BE POWDER COATED "DARK BRONZE" EXCEPT FOR THE SIGNAL HEADS, WHICH SHALL BE BLACK.



CABLE PLAN

NOT TO SCALE

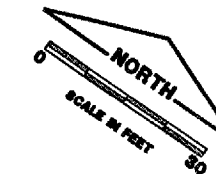
LEGEND

- PEDESTRIAN SIGNAL SECTION
- CONTROLLER CABINET
- SERVICE INSTALLATION
- DENOTES NUMBER OF CONDUCTORS
ALL CABLE NO. 14 EXCEPT AS INDICATED
ALL LOOP DETECTOR CABLE TO BE SHIELDED
- PEDESTRIAN PUSHBUTTON DETECTOR
- GROUND ROD AT HANDHOLE (H)
DOUBLE HANDHOLE (H)
OR CONTROLLER (C)
- GROUND ROD AT POST (P)
OR MAST ARM POLE (MA)
- GROUND ROD AT ELECTRIC SERVICE INSTALLATION

TRAFFIC SIGNAL LEGEND

- CONTROLLER
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD PEDESTRIAN
- SIGNAL POST
- PEDESTRIAN PUSHBUTTON DETECTOR
- HANDHOLE
- GRS CONDUIT IN TRENCH OR PUSHED

R0010
TOTAL SHEETS = 24



REVISIONS

NUMBER	BY	DATE

0 1
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GREATER ROCKFORD AIRPORT AUTHORITY
ROCKFORD, ILLINOIS

ILLINOIS PROJECT, RFD-2820

NORTH AND WEST AUTO PARKING
ELECTRICAL DETAILS - 4

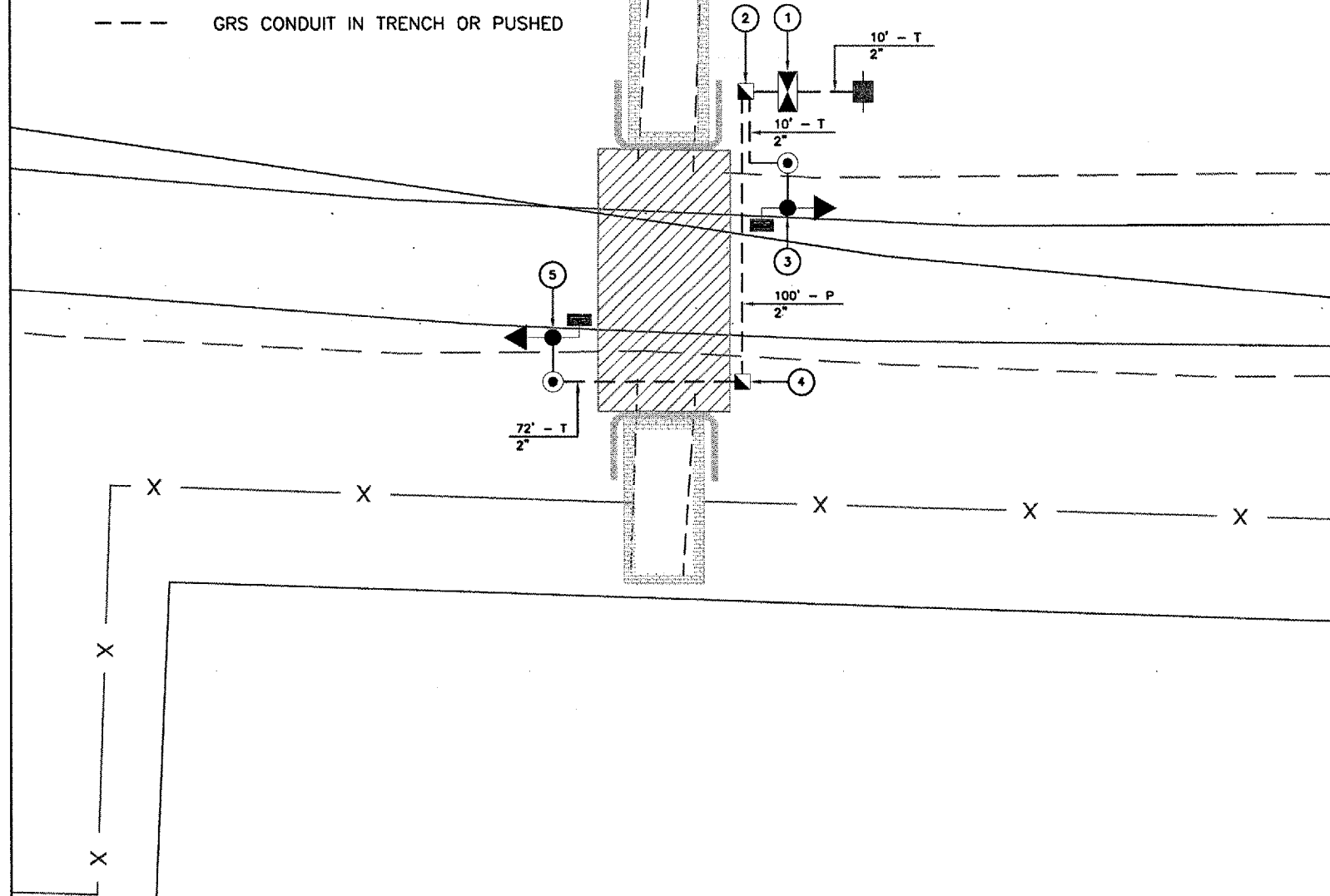
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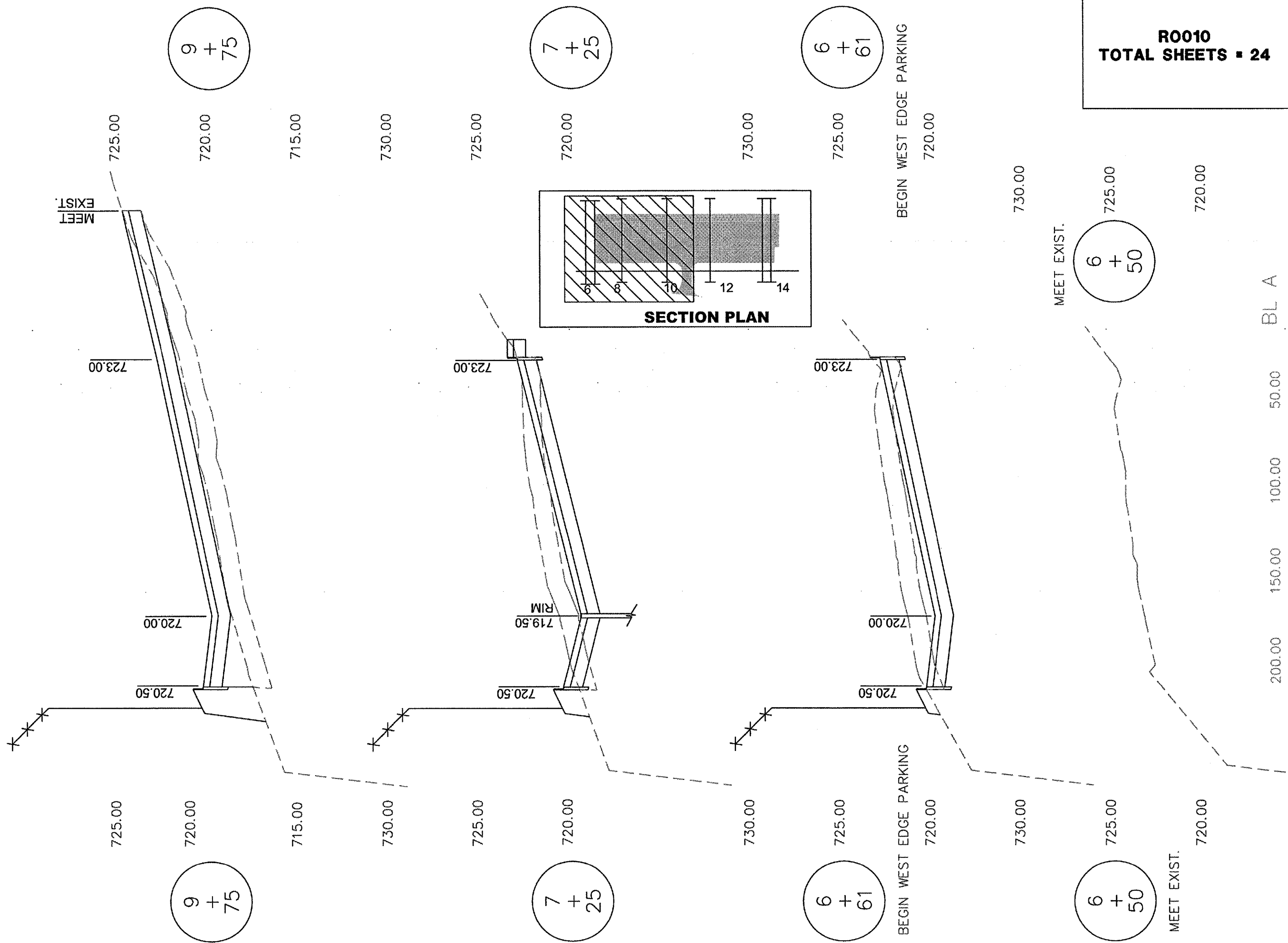
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16 (ED4)

SHEET 16 OF 24 SHEETS

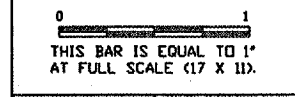


STRUCTURE NO.	STRUCTURE DESCRIPTION	STATION/OFFSET
1	CONTROLLER	-
2	HANDHOLE	-
3	TRAFFIC SIGNAL POST, 16'	-
4	HANDHOLE	-
5	TRAFFIC SIGNAL POST, 16'	-



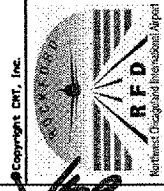
R0010
TOTAL SHEETS = 24

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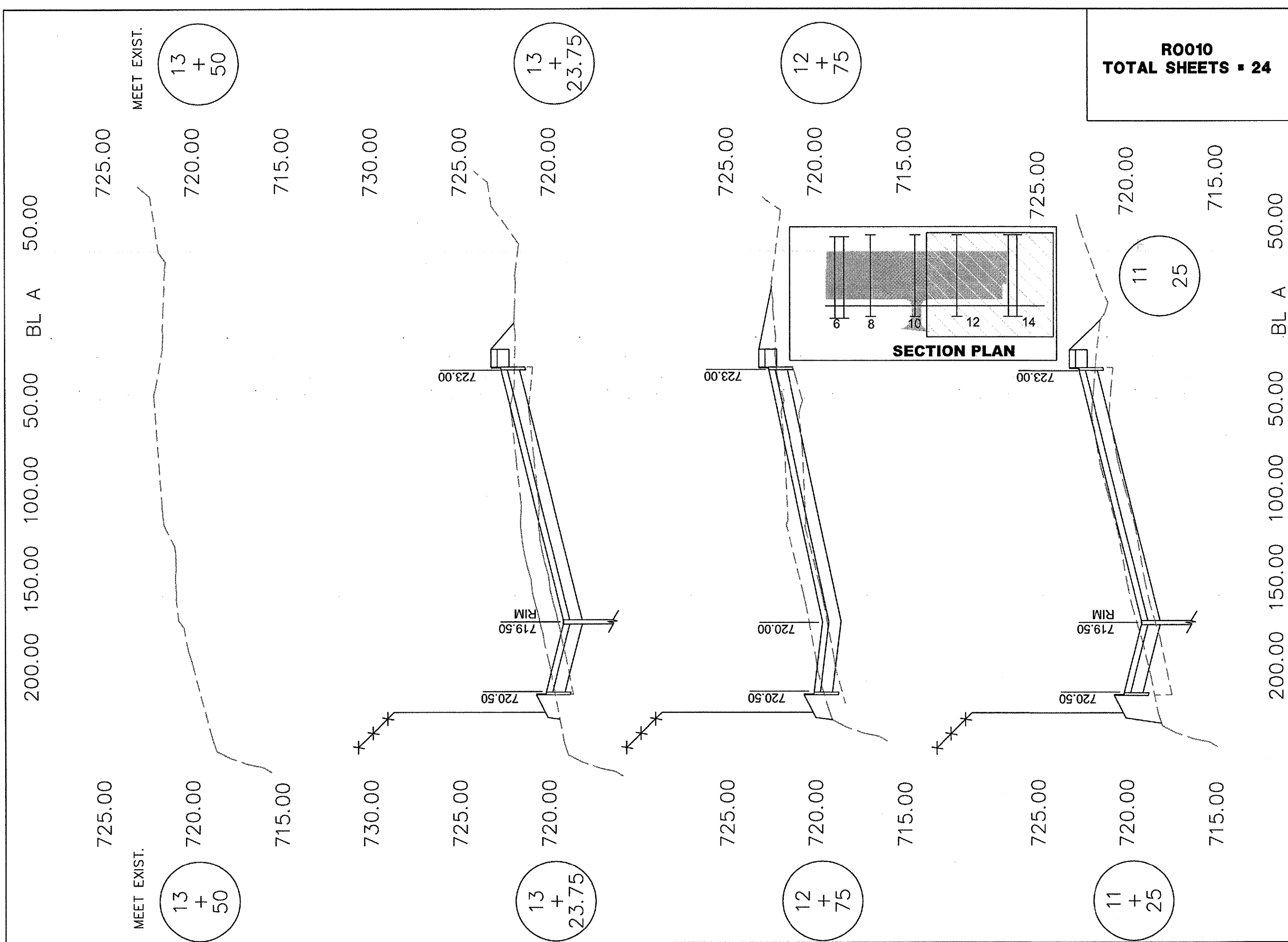
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ROCKFORD, ILLINOIS
ILLINOIS PROJECT, RFD-2820
NORTH AUTO PARKING
CROSS SECTIONS - 1


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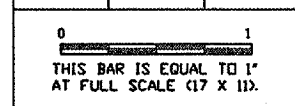

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17 (CR1)	
SHEET 17 OF 24 SHEETS	


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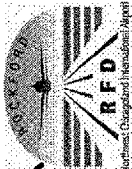



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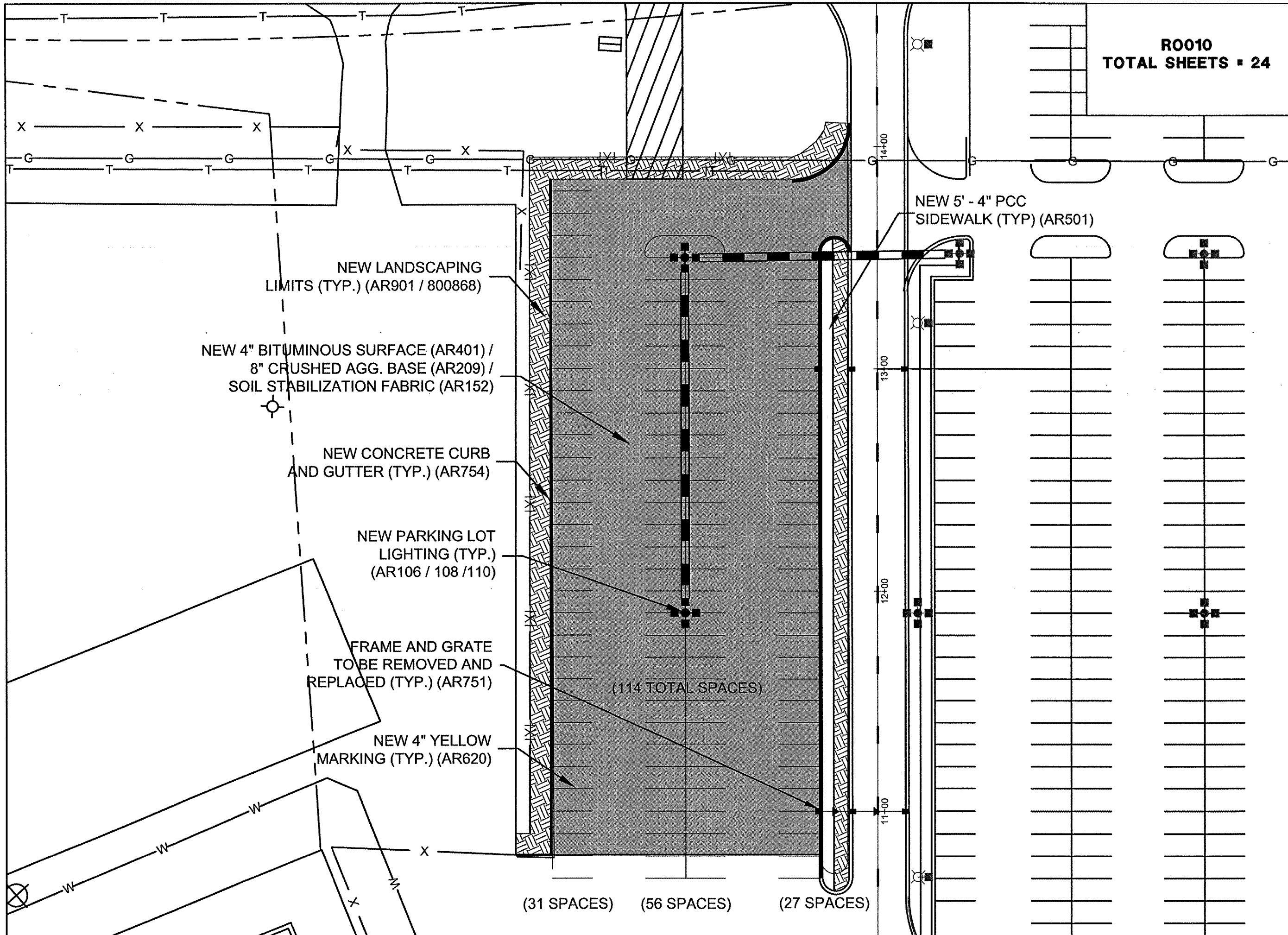


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ILLINOIS PROJECT: RFD-2820
NORTH AUTO PARKING
CROSS SECTIONS - 2

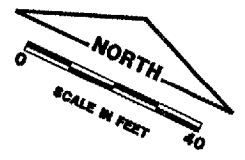

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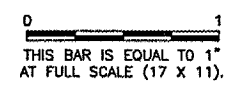
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R0010
TOTAL SHEETS = 24



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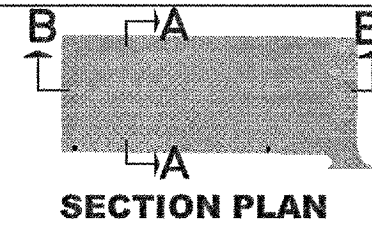
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ILLINOIS PROJECT: RFD-2820

NORTH AND WEST AUTO PARKING
SITE PLAN

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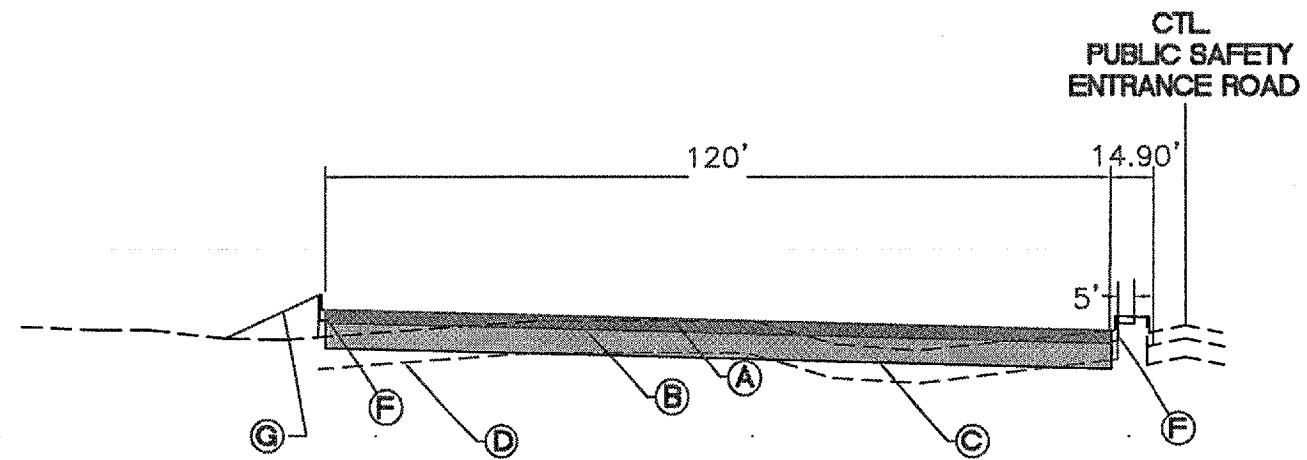
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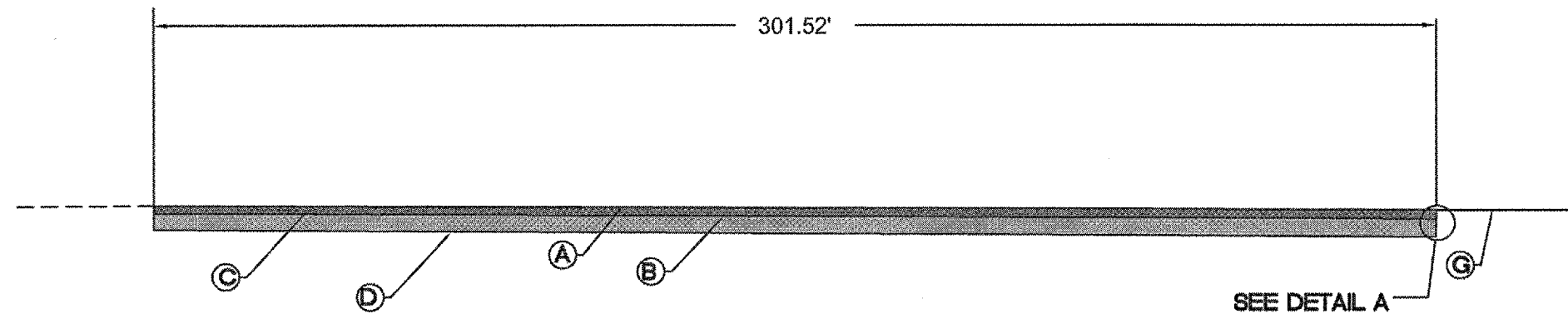
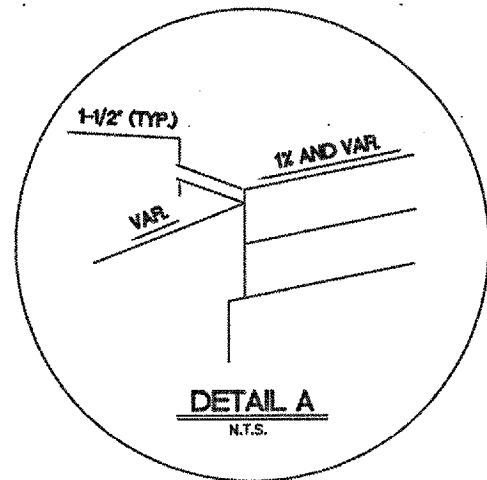
**R0010
TOTAL SHEETS = 24**

REVISIONS		
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0 1
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SECTION A-A
N.T.S.



SECTION B-B
N.T.S.

CTL LEGEND

- (A) NEW 4" (2-2" LIFTS) BITUMINOUS SURFACE COURSE (BITUMINOUS TACK COAT BETWEEN LIFTS) (AR401/AR603)
- (B) NEW 8" CRUSHED AGGREGATE BASE COURSE (AR209) (BITUMINOUS PRIME COAT APPLIED PRIOR TO PAVING) (AR602)
- (C) NEW 12" AVG. TOPSOIL STRIPPING (AR152)
- (D) NEW UNCLASSIFIED EXCAVATION (AR152)
- (E) NEW DRAINAGE SWALE (AR152)
- (F) NEW COMB. CONC. CURB AND GUTTER M-6.12 (AR754)
- (G) NEW SEEDING/SOIL GUARD (AR901-AR910)

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ROCKFORD, ILLINOIS
ILLINOIS PROJECT: RFD-2820
NORTH AND WEST AUTO PARKING
TYPICAL SECTIONS**



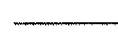
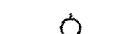
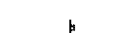
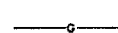
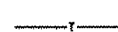








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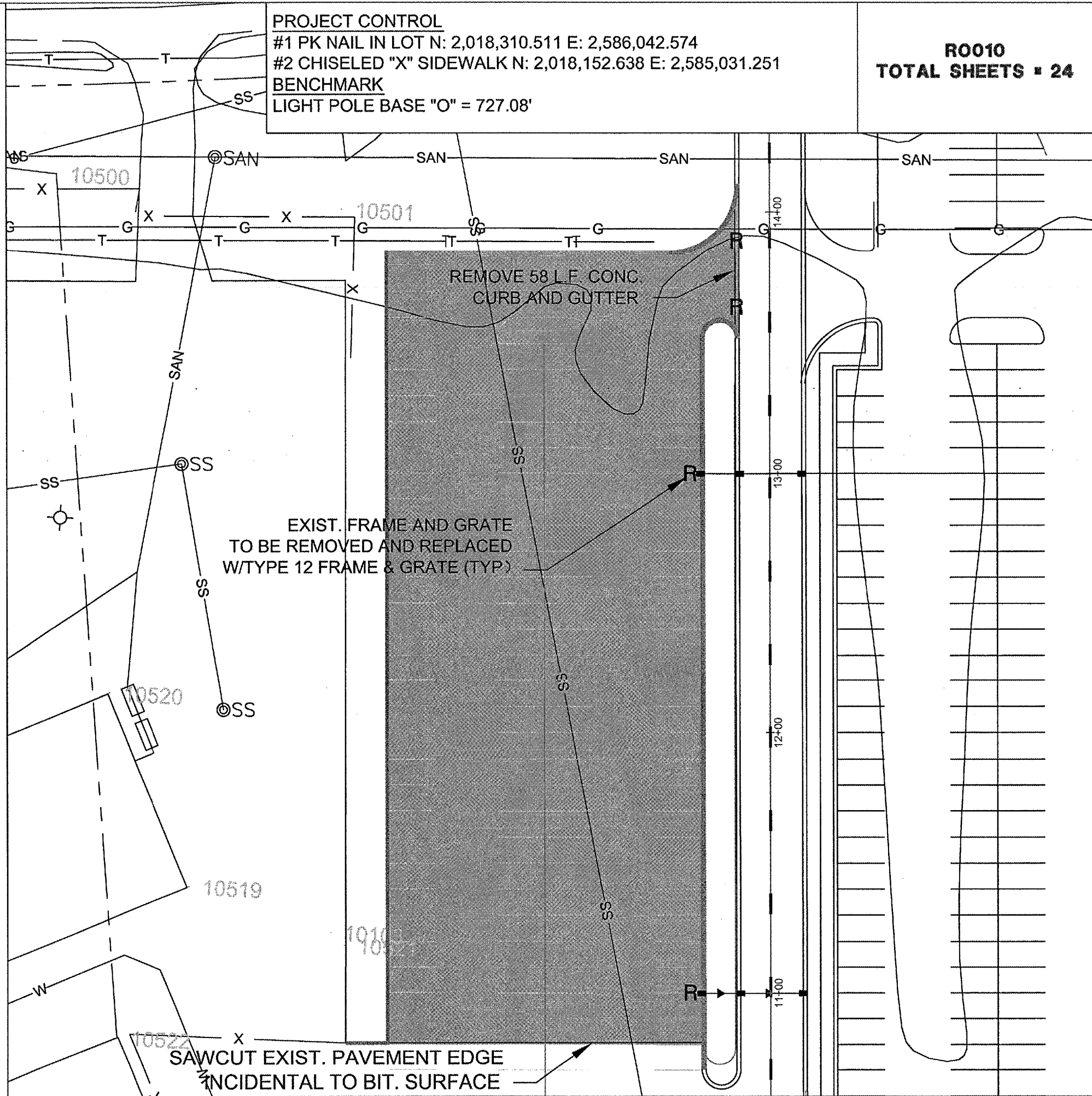
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JOB No:	05258-04-00

LEGEND

-  BASE BID
-  EXISTING STORM SEWER
-  EXISTING FENCE
-  EXISTING POWER POLE
-  EXISTING SIGN
-  EXISTING GAS LINE
-  EXISTING TELEPHONE CABLE
-  EXISTING ELECTRIC CABLE
-  EXISTING OVERHEAD ELECTRIC
-  EXISTING UNDERDRAIN
-  DENOTES ITEM TO BE REMOVED
-  EXISTING TREE
-  EXISTING LIGHT POLE
-  EXISTING FIRE HYDRANT
-  SOIL BORING

NOTES

1. THE INFORMATION SHOWN ON THESE PLANS HAVE BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.



PROJECT CONTROL
 #1 PK NAIL IN LOT N: 2,018,310.511 E: 2,586,042.574
 #2 CHISELED "X" SIDEWALK N: 2,018,152.638 E: 2,585,031.251
BENCHMARK
 LIGHT POLE BASE "O" = 727.08'

R0010
TOTAL SHEETS = 24



REVISIONS		
NUMBER	BY	DATE

0 1
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ILLINOIS PROJECT, RFD-2820

NORTH AND WEST AUTO PARKING

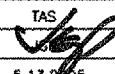
EXISTING CONDITIONS/NEW REMOVALS



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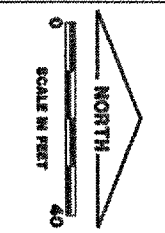


Northwest
 Chicago
 International
 Airport

DESIGN BY: AGJ
 DRAWN BY: CMT
 CHECKED BY: TAS
 APPROVED BY: 
 DATE: 5.13.05
 JOB No: 05258-04-00

21 (REM2)

R0010
TOTAL SHEETS = 24



REVISIONS		
NUMBER	BY	DATE

0 1
THIS BAR IS EQUAL TO 1"
AT FULL SCALE (17 X 11).

GREATER ROCKFORD AIRPORT AUTHORITY
ROCKFORD, ILLINOIS
ILLINOIS PROJECT, RFD-2820
NORTH AND WEST AUTO PARKING
GRADING PLAN

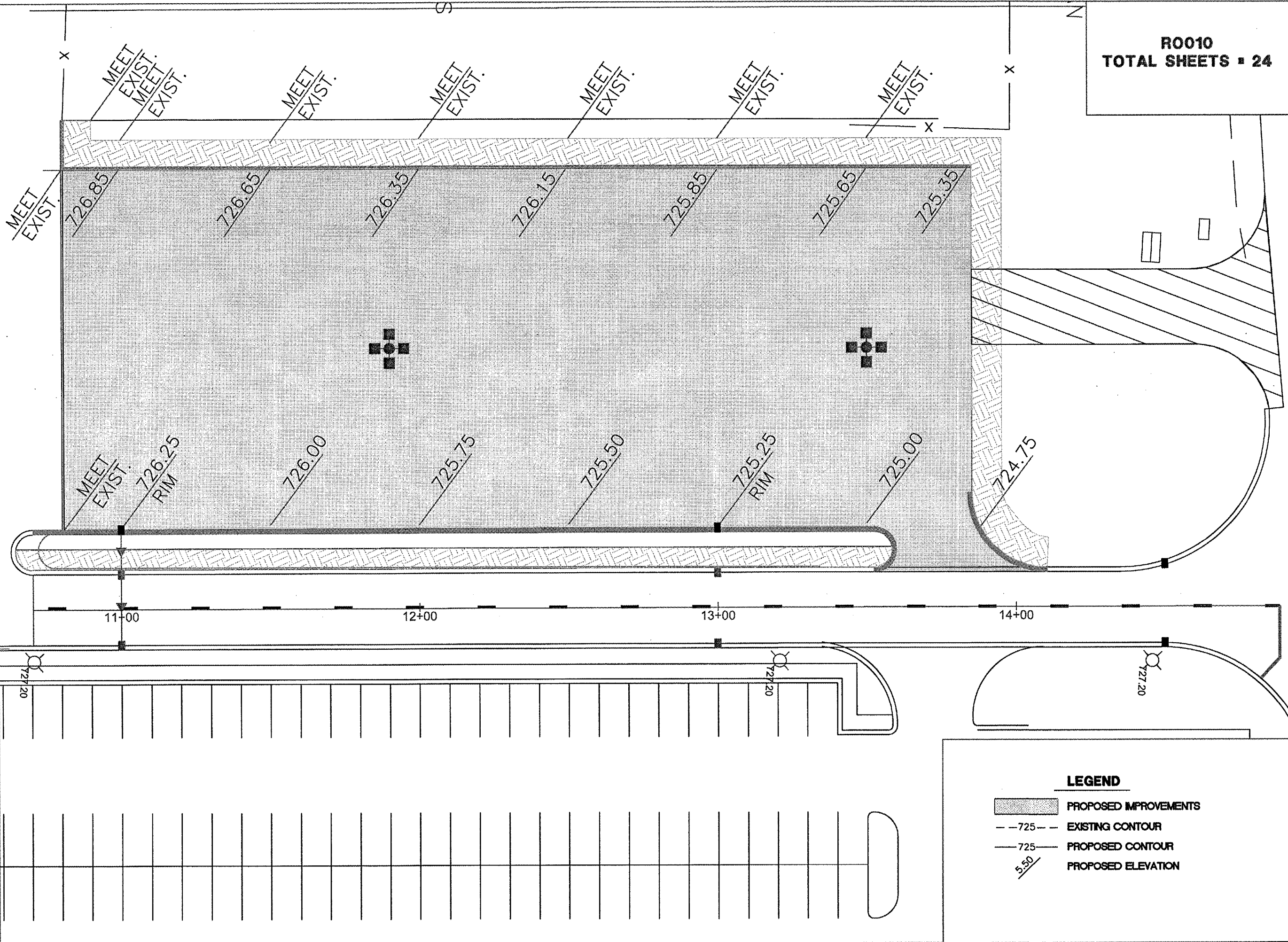
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DATE: 5.13.2005
JOB No: 05258-04-00

22 (GR2)








LEGEND

- PROPOSED IMPROVEMENTS
- 725-- EXISTING CONTOUR
- 725— PROPOSED CONTOUR
- 5.30 PROPOSED ELEVATION

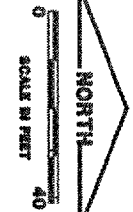
NOTES

- 1) THE ROUTING OF THE NEW CABLES AND CONDUITS SHOWN IS FOR INFORMATION ONLY. THE EXACT ROUTING SHALL BE COORDINATED WITH THE RESIDENT ENGINEER. ALL CONDUITS SHALL BE BURIED A MIN 30" BELOW FINISHED GRADE.
- 2) IT IS CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
- 3) ANY SPLICES TO THE NEW CABLES SHALL BE MADE INSIDE THE HANDHOLES OR POLE BASES. NO DIRECT BURIED SPLICES WILL BE ALLOWED. ALL SPLICES SHALL BE WATERPROOF AND SHALL HAVE HEAT SHRINK TUBING OVER ENTIRE SPLICE.
- 4) THE COST OF PAVEMENT REMOVAL AND REPLACEMENT OF PAVEMENT SHALL BE INCIDENTAL TO THE INSTALLATION OF THE DIRECT BURIED CONDUIT.
- 5) GROUND ROD SHALL BE 3/4" DIA. x 10' LONG. CONTRACTOR SHALL ROUTE 1 #8 GND. WIRE FROM GROUND ROD TO POWER PANEL. GROUND ROD SHALL BE BURIED 1'-0" BELOW FINISHED GRADE. CONNECTIONS TO GROUND ROD SHALL BE EXOTHERMIC TYPE.
- 6) CONTRACTOR SHALL COORDINATE ALL SERVICE CONNECTIONS WITH UTILITY. CONTRACTOR SHALL PROVIDE 10'-0" RISER AT POLE AND COIL UP SUFFICIENT CABLES AT TRANSFORMER FOR UTILITY CONNECTIONS.
- 7) 2" GRS CONDUIT SHALL BE JACKED WHEN CROSSING EXISTING PAVEMENT. COST ASSOCIATED WITH THIS WORK SHALL BE INCIDENTAL TO PAY ITEM "AR110212 2" STEEL DUCT, DIRECT BURY."
- 8) CONNECT NEW CIRCUIT #1 AND NL CIRCUIT TO THE EXISTING PARKING LOT LIGHTING CIRCUIT AND NL CIRCUIT. ANY SPLICES TO THE NEW CABLES SHALL BE MADE INSIDE THE POLE BASES.

LEGEND

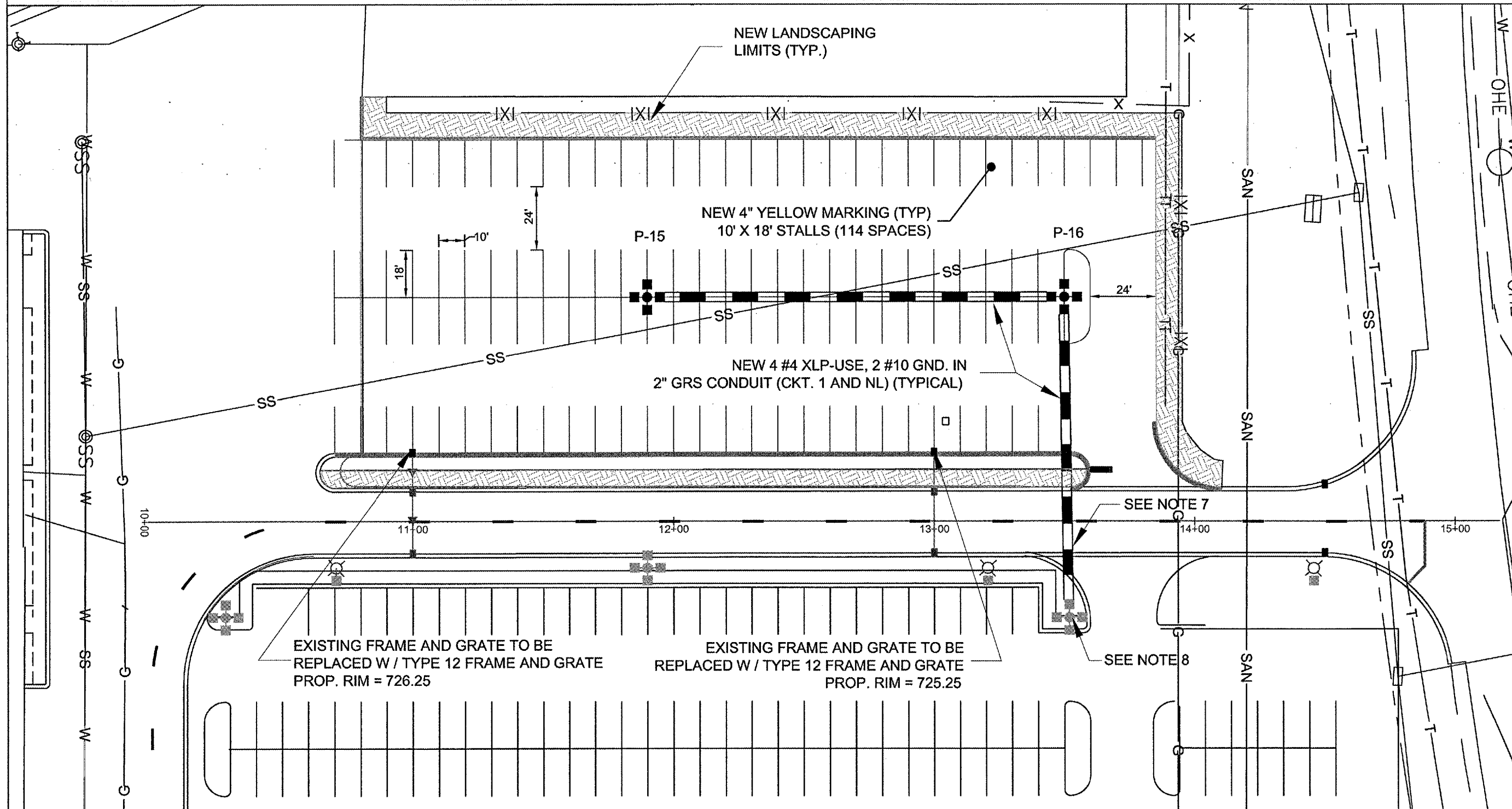
-  NEW 2" GRS CONDUIT
-  NEW TYPE A AREA LIGHT POLE W/4 FIXTURES
-  NEW FRAME AND GRATE
-  NEW LANDSCAPING LIMITS
-  EXISTING TYPE A AREA LIGHT POLE W/4 FIXTURES

R00010
TOTAL SHEETS = 24



REVISIONS		
NUMBER	BY	DATE

0 1
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ILLINOIS PROJECT: RFD-2820

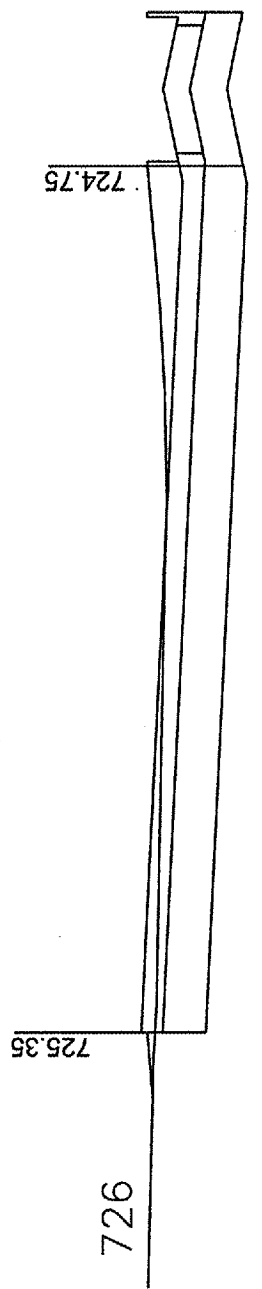
NORTH AND WEST AUTO PARKING
**PROPOSED DRAINAGE / MARKING
LANDSCAPING / ELECTRICAL PLAN**

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JOB No: 05258-04-00

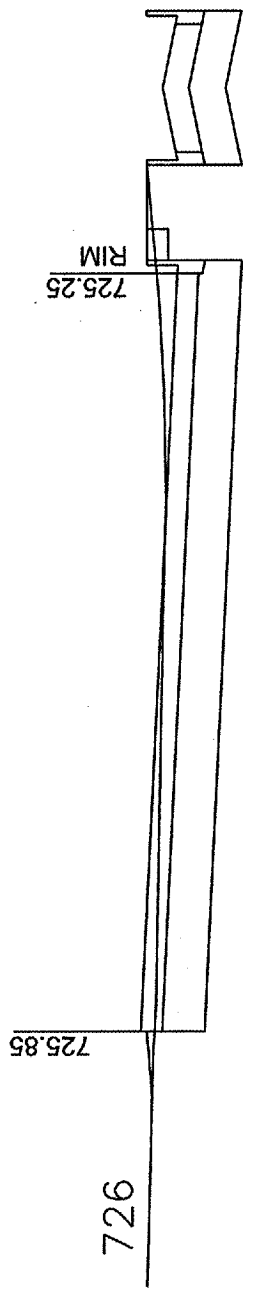
23 (LME)



13
+
85.35

726

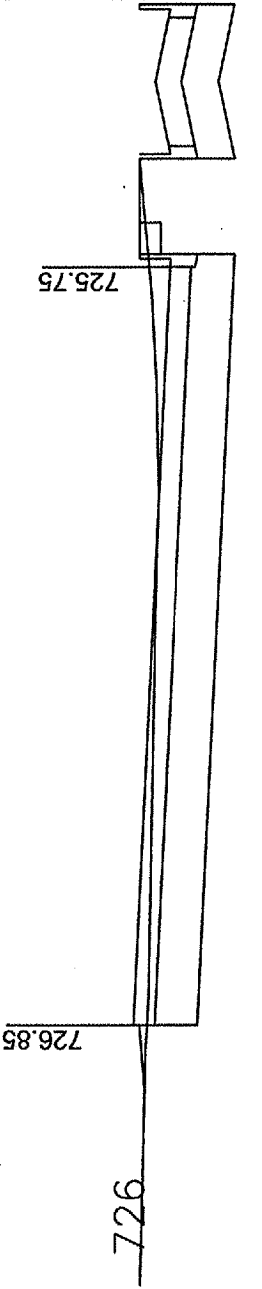
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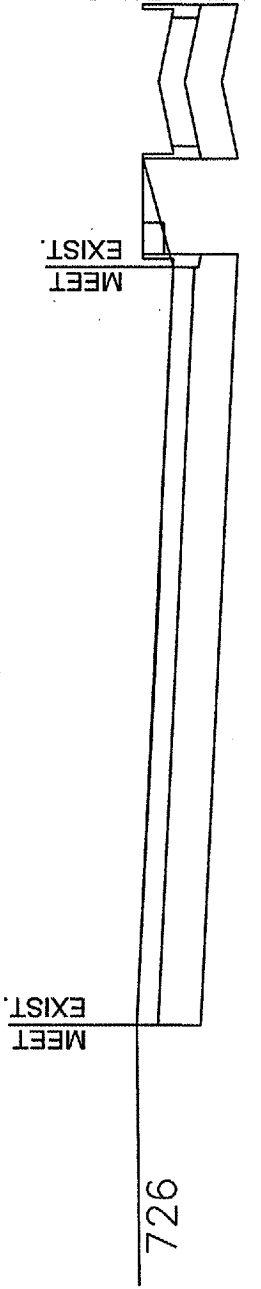
723



12
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00

726

723



10
+
81

726

723

**RO010
TOTAL SHEETS = 24**

REVISIONS		
NUMBER	BY	DATE

0 1
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AT FULL SCALE (17 X 11).

**GREATER ROCKFORD AIRPORT AUTHORITY
ROCKFORD, ILLINOIS
ILLINOIS PROJECT: RFD-2820
NORTH AUTO PARKING
CROSS SECTIONS - 1**

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05258-04-00

24 (CR3)