

| F.A.U. RTE. | SECTION    | COUNTY   | TOTAL SHEETS       | SHEET NO. |
|-------------|------------|----------|--------------------|-----------|
| 1321        | 2010-134-T | DUPAGE   | 40                 | 1         |
|             |            | ILLINOIS | CONTRACT NO. 60N06 |           |

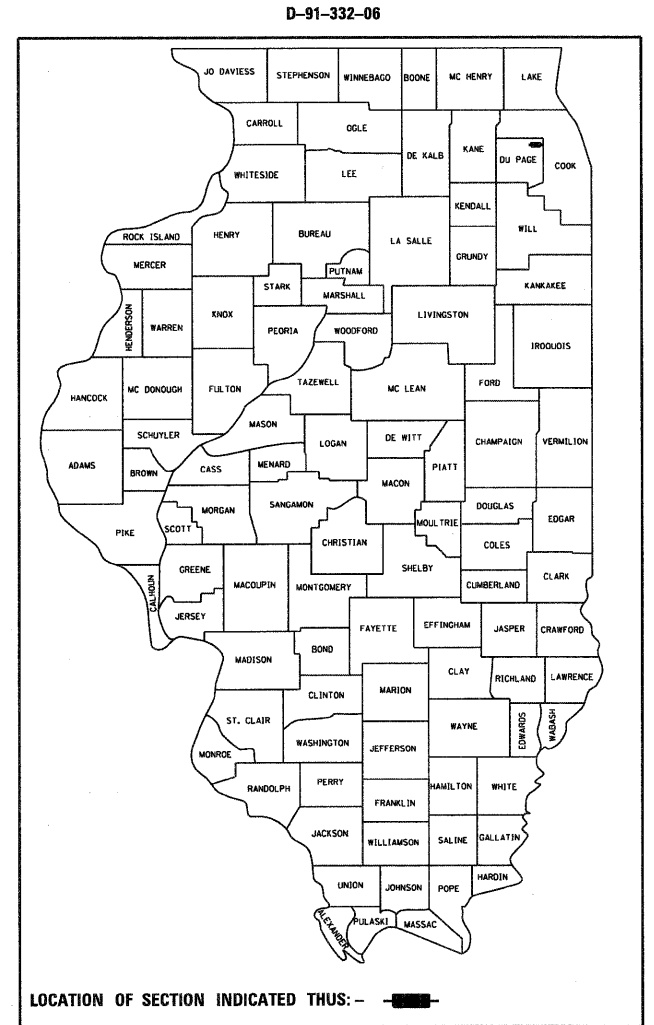
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

# PROPOSED HIGHWAY PLANS

**FAU ROUTE 1321: IL 19 (IRVING PARK RD.) AT YORK RD.  
SECTION 2010-134-T**

**ADVANCE CONTRACT  
C-91-223-11**

**DUPAGE COUNTY**



**FOR INDEX OF SHEETS, SEE SHEET NO. 2**

**FOR LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2**

**TRAFFIC DATA**

**DESIGN DESIGNATION:**

IRVING PARK ROAD =  
3230 (30) PRINCIPAL ARTERIAL 20.98 (PCC-20)

YORK ROAD =  
2880 (30) MINOR ARTERIAL 20.38 (PCC-20)

**ADT:**

IL ROUTE 19 (EAST) = 37,000  
IL ROUTE 19 (WEST) = 30,000

YORK ROAD (NORTH) = 28,000  
YORK ROAD (SOUTH) = 23,000

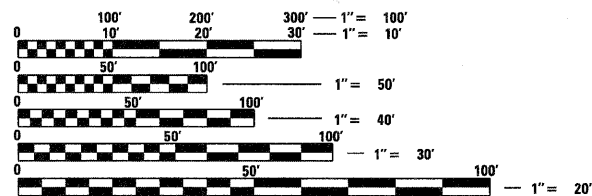
**DESIGN SPEED:**

IL ROUTE 19 = 40 MPH  
YORK ROAD (NORTH) = 45 MPH  
YORK ROAD (SOUTH) = 40 MPH

PROJECT LOCATED IN  
THE CITY OF CHICAGO

**DESCRIPTION OF PROJECT**

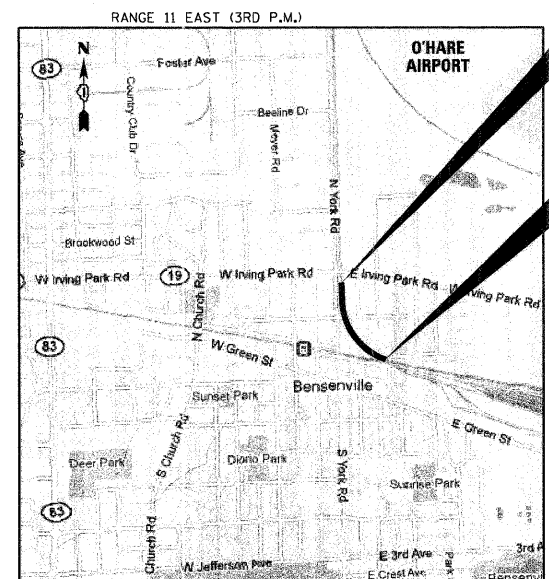
THIS PROJECT CONSISTS OF RAILROAD EMBANKMENT, TEMPORARY SOIL RETENTION, AND ALL INCIDENTAL AND COLLATERAL WORK AS NECESSARY TO COMPLETE THE IMPROVEMENT SHOWN HEREIN AND AS DESCRIBED IN THE SPECIAL PROVISIONS.



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

**J.U.L.I.E. (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION)**  
1-800-892-0123 OR 811  
**C.U.A.N. (CHICAGO UTILITY ALERT NETWORK)**  
312-744-7000  
**RAILROAD UTILITY CONTACT TELEPHONE NUMBERS**  
1-888-625-8702 (CPR "ONE CALL" FIBER/SIGNAL LOCATE)  
1-312-322-6900 (METRA SIGNAL AND COMMUNICATIONS DEPARTMENTS)

PROJECT MANAGER - CATHERINE KIBBLE, PE (847) 705-4269  
PROJECT ENGINEER - TIM SCHMIDT, PE (847) 705-4556



**LOCATION MAP**  
N.T.S.

GROSS LENGTH = 2035 FT. = 0.385 MILES  
NET LENGTH = 2035 FT. = 0.385 MILES

**END IMPROVEMENTS  
CPR  
STA 10296 + 20**

**BEGIN IMPROVEMENTS  
CPR  
STA 10275 + 85**

BY: *[Signature]* DATE: 3/17/2011  
HDR ENGINEERING, INC.  
DWGS. 5-11, 28-40  
LICENSE EXPIRES NOV. 30, 2011

BY: *[Signature]* DATE: 3/17/2011  
HDR ENGINEERING, INC.  
DWGS. 1-4, 12-15, 24  
LICENSE EXPIRES NOV. 30, 2011

BY: *[Signature]* DATE: 3/17/2011  
GEOSERVICES, INC.  
DWGS. 25-27  
LICENSE EXPIRES NOV. 30, 2011

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED MARCH 21, 2011

*Diane M. O'Keefe*  
DISTRICT ENGINEER

May 13 2011  
*Scott E. Sitt* P.E.  
ENGINEER OF DESIGN AND ENVIRONMENT

May 13 2011  
*Christine M. Rood*  
DIRECTOR, DIVISION OF HIGHWAYS

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

**CONTRACT NO. 60N06**

**HDR** 8550 W. Bryn Mawr Ave., Suite 900  
Chicago, IL 60631  
312-443-4900  
HDR Engineering, Inc.

**INDEX OF SHEETS**

|   |   |
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**LIST OF HIGHWAY STANDARDS**

|           |   |
|-----------|---|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS                |
| 001001-02 | AREAS OF REINFORCEMENT BARS                                 |
| 001006    | DECIMAL OF AN INCH AND OF A FOOT                            |
| 280001-05 | TEMPORARY EROSION CONTROL SYSTEMS                           |
| 664001-02 | CHAIN LINK FENCE  |
| 701101-02 | OFF-RD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE |
| 701106-02 | OFF-RD OPERATIONS, MULTILANE MORE THAN 15' AWAY             |
| 701901-01 | TRAFFIC CONTROL DEVICES                                     |

**IDOT DISTRICT ONE STANDARD DETAILS (INCLUDED AS PLAN SHEETS)**

|   |  |
|---|--|
| <b>BUREAU OF DESIGN DETAIL STANDARDS</b>  |  |
| BD51                                      | BENCHING CONSTRUCTION DETAIL   |
| <b>BUREAU OF TRAFFIC DETAIL STANDARDS</b> |  |
| TC10                                      | TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS |
| TC22                                      | ARTERIAL ROAD INFORMATION SIGN   |

**GENERAL NOTES**

1. ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2007; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2011, "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JULY 2009 SIXTH EDITION, THE "DETAILS" IN THE PLANS, AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS. ANY REFERENCE TO "STANDARDS" THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT STANDARD. SHOULD A REVISED STANDARD EXIST THAT SUPERCEDES STANDARDS REFERENCED IN THE CONTRACT DOCUMENTS, THE CONTRACTOR IS RESPONSIBLE FOR SEEKING CLARIFICATION FROM THE ENGINEER BEFORE PROCEEDING WITH THE ORDERING OF MATERIALS, SCHEDULING OF PERSONNEL, PERFORMING THE WORK OR ANY OTHER ACTIVITY RELATED TO THE WORK. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING THE CORRECT STANDARD BEFORE PERFORMING WORK. WHEN WORKING ON CANADIAN PACIFIC RAILWAY PROPERTY, CANADIAN PACIFIC RAILWAY STANDARD SPECIFICATIONS, POLICIES AND DETAILS SHALL BE FOLLOWED IN ADDITION TO THE DOCUMENTS LISTED ABOVE.
2. THE CONTRACTOR SHALL NOTIFY THE ENGINEER AT LEAST 72 HOURS IN ADVANCE PRIOR TO BEGINNING WORK AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER. IN ADDITION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER AT LEAST 48 HOURS IN ADVANCE FOR ANY TREE REMOVAL.
3. THE CONTRACTOR SHALL ENSURE ALL PERMITS HAVE BEEN OBTAINED PRIOR TO COMMENCEMENT OF WORK.
4. THE CONTRACTOR SHALL NOTIFY AS NECESSARY, ALL TESTING AGENCIES SUFFICIENTLY IN ADVANCE OF CONSTRUCTION. IF THE CONTRACTOR FAILS TO ALLOW PROPER NOTIFICATION TIME RESULTING IN THE TESTING AGENCIES BEING UNABLE TO VISIT THE SITE AND PERFORM THE NECESSARY TESTING, THE CONTRACTOR MUST SUSPEND THE OPERATION FOR WHICH TESTING IS NECESSARY UNTIL THE TESTING AGENCY CAN SCHEDULE TESTING OPERATIONS. THE COST OF SUSPENDING WORK SHALL BE BORN BY THE CONTRACTOR AND NO COMPENSATION SHALL BE GIVEN.
5. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
6. EXISTING CONDITIONS WERE TAKEN FROM THE BEST AVAILABLE INFORMATION OR MAPPING. INFORMATION SHOWN CONCERNING FEATURES AND UTILITIES IS NOT GUARANTEED ALL INCLUSIVE OR CORRECT. THE CONTRACTOR IS TO VERIFY THE FEATURES PRIOR TO CONSTRUCTION. EXISTING UTILITIES ARE TO BE MAINTAINED IN SERVICE AT ALL TIMES. THE LOCATION OF MATERIALS AND DIMENSIONS OF EXISTING FACILITIES AND OBSTRUCTIONS ARE BASED UPON LOCATION RECORDS AND ARE SHOWN ON THE DRAWINGS STRICTLY AS AID TO THE CONTRACTOR, BUT MUST NOT BE CONSTRUED AS BEING ACCURATE, CORRECT OR COMPLETE. ALL STRUCTURES ABOVE OR BELOW GROUND THAT ARE ENCOUNTERED DURING CONSTRUCTION ARE TO BE PROPERLY SUPPORTED AND MAINTAINED.
7. THE LOCATION OF VARIOUS ITEMS SUCH AS PAVEMENT, BARRIER WALLS, AND DRAINAGE STRUCTURES BUILT IN CONTRACTS UNDER CONSTRUCTION DURING THE PREPARATION OF THESE PLANS IS BASED ON THE PUBLISHED CONTRACT PLAN DRAWINGS AVAILABLE DURING DESIGN. THE CONTRACTOR MUST FIELD VERIFY LIMITS, LOCATIONS AND ELEVATIONS OF THESE PREVIOUSLY CONSTRUCTED ITEMS.
8. EXISTING GRADES AND ELEVATIONS REPORTED ON THE DRAWINGS ARE BASED ON BEST AVAILABLE INFORMATION. IF THE CONTRACTOR DOES NOT CONCUR WITH THE ELEVATION PROVIDED ON THE DRAWINGS, THE CONTRACTOR MUST NOTIFY THE ENGINEER IN WRITING PRIOR TO CONSTRUCTION. NO CONSTRUCTION WILL BE ALLOWED TO BEGIN UNTIL THESE CONFLICTS ARE RESOLVED.
9. ALL DEWATERING OF THE SITE AS NEEDED FOR THE CONTRACTOR'S OPERATIONS WILL NOT BE PAID FOR SEPARATELY, BUT CONSIDERED AS INCLUDED IN THE OVERALL CONTRACT PRICE.
10. THE CONTRACTOR MUST BE RESPONSIBLE FOR THE CONSTRUCTION, REPAIR, SNOW REMOVAL, AND/OR MAINTENANCE OF ALL HAUL ROADS TO AND FROM THE DESIGNATED ENTRANCE TO THE VARIOUS WORK SITES. THIS WORK WILL BE INCIDENTAL TO THE PROJECT.
11. THE CONTRACTOR SHALL STAGE THE WORK AS TO MAINTAIN INGRESS AND EGRESS TO ALL ABUTTING PROPERTIES AT ALL TIMES
12. ALL CONSTRUCTION PERSONNEL WILL BE REQUIRED TO WEAR A FLUORESCENT SAFETY VEST AT ALL TIMES WHILE ON THE CONSTRUCTION SITE. COMPLIANCE WITH THIS REQUIREMENT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
13. THE UNIT PRICES FOR ITEMS USED TO CONSTRUCT TEMPORARY PAVEMENT OR ACCESS ROADS SHALL INCLUDE ALL EQUIPMENT, LABOR AND MATERIAL REQUIRED TO PLACE, REMOVE, AND DISPOSE OF THE TEMPORARY PAVEMENT OR ACCESS ROAD.
14. THE CONTRACTOR MUST COORDINATE OFF-SITE HAUL AND ACCESS ROUTES WITH THE PARTY HAVING JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE HAUL AND ACCESS ROUTES MUST BE MAINTAINED BY THE CONTRACTOR AND MUST BE RESTORED TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. FENCING, DRAINAGE, GRADING, RESURFACING, OR OTHER WORK NECESSARY TO CONSTRUCT AND MAINTAIN HAUL ROUTES IS THE CONTRACTOR'S RESPONSIBILITY AT NO ADDITIONAL COST AND MUST BE APPROVED BY THE ENGINEER PRIOR TO THE WORK.
15. WHERE ARTIFICIAL LIGHTING IS UTILIZED, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
16. FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF 12 SANDBAGS PER BARRICADE.

DENOTES ITEMS OR WORK NOT PAID FOR SEPARATELY.

17. CONSTRUCTION ACTIVITIES MAY OCCUR BETWEEN 7:00 A.M. AND 7:00 P.M. MONDAY THROUGH FRIDAY AND FROM 8:30 A.M. TO 5:00 P.M. ON SATURDAYS. CONSTRUCTION ACTIVITIES ON SUNDAYS ARE PROHIBITED. NO WORK WILL BE PERFORMED ON FEDERAL HOLIDAYS OBSERVED IN ILLINOIS. CONSTRUCTION ACTIVITIES ARE IDENTIFIED AS THE OPERATION OF HEAVY EQUIPMENT, TO INCLUDE BUT NOT LIMITED TO ALL CONSTRUCTION TRUCKS AND EQUIPMENT.

18. REMOVAL AND DISPOSAL OF UNSTABLE AND/OR UNSUITABLE MATERIAL WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL" WHICH PRICE SHALL INCLUDE OTHER ITEMS OF WORK INCLUDED UNDER THE GENERAL HEADING OF EARTHWORK IN THE STANDARD SPECIFICATIONS FOR WHICH NO PAYMENT WILL BE MADE, BUT WILL BE CONSIDERED AS INCLUDED IN THE OVERALL CONTRACT PRICE.

19. USE OF CCDD FILL OPERATION  
IF THE CONTRACTOR CHOOSES TO DISPOSE OF UNCONTAMINATED SOIL OR UNCONTAMINATED SOIL MIXED WITH CLEAN CONSTRUCTION AND DEMOLITION DEBRIS (CCDD) AT A CCDD FILL OPERATION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PERFORM ALL NECESSARY FIELD AND LABORATORY ANALYSIS AND TO OBTAIN THE LICENSED PROFESSIONAL ENGINEER'S CERTIFICATION REQUIRED TO USE THE SITE AS PER PUBLIC ACT 96-1416. NO ADDITIONAL COMPENSTATION WILL BE PROVIDED.

20. CHICAGO O'HARE INTERNATIONAL AIRPORT WILL BE IN OPERATION WHILE CONSTRUCTION UNDER THIS CONTRACT IS TAKING PLACE. TIMING AND COORDINATION OF THE WORK IS AN ESSENTIAL FEATURE OF THIS CONTRACT AND WILL REQUIRE THE COMPLETION OF ALL WORK HEREIN SPECIFIED SO AS TO OFFER THE LEAST OBSTRUCTION AND/OR IMPEDIMENT TO THE AIRPORT TRAFFIC AND THE GENERAL OPERATION OF THE AIRPORT. ALL EXISTING UTILITIES SERVING THE AIRPORT MUST REMAIN IN CONTINUOUS OPERATION DURING THE EXECUTION OF THE WORK, UNLESS REMOVED UNDER THIS OR A SEPARATE CONTRACT.

**RAILROAD**

1. A MINIMUM CONSTRUCTION CLEARANCE OF 21' VERTICALLY ABOVE THE PLANE OF TOP-OF-RAIL AND 12' HORIZONTALLY AT RIGHT ANGLE FROM CENTERLINE OF TRACK WILL BE REQUIRED DURING CONSTRUCTION.

2. ALL DEMOLITION WITHIN THE CANADIAN PACIFIC RAILWAY(CPR) RIGHT-OF-WAY AND/OR DEMOLITION THAT MAY IMPACT THE CPR TRACKS OR OPERATIONS MUST BE APPROVED BY THE CPR'S CHIEF ENGINEER.

3. NO WORK IS ALLOWED WITHIN 50 FEET OF TRACK CENTERLINE WHEN A TRAIN PASSES THE WORK SITE AND ALL PERSONNEL MUST CLEAR THE AREA WITHIN 25 FEET OF THE TRACK CENTERLINE AND SECURE ALL EQUIPMENT WHEN TRAINS ARE PRESENT. IF VARIATIONS FROM THIS REQUIREMENT ARE REQUIRED THE CONTRACTOR MUST COORDINATE WITH THE CPR, UPRR, AND METRA.

4. NO EXCAVATION IS ALLOWED WITHIN 12 FEET OF ANY TRACK. ALL EXCAVATIONS MUST BE SHORED IN ACCORDANCE WITH OSHA STANDARDS. SHORING FOR EXCAVATIONS ON RAILROAD RIGHT-OF-WAY MUST BE APPROVED BY THE ENGINEER AND/OR THE RAILROAD'S CHIEF ENGINEER THROUGH THE RESIDENT ENGINEER.

5. ALL CONTRACTORS WORKING WITHIN 50 FEET OF THE RAILROAD TRACKS MUST HAVE THE PROPER AUTHORIZATION DOCUMENT IN HAND (ENGINEERING RIGHT-OF-ENTRY AGREEMENT OR PLAIN LANGUAGE RELEASE FORM).

6. KEEP ALL DIRT AND OTHER FOREIGN MATERIALS OUT OF THE TRACK BALLAST SECTION. ALL BALLAST FOULED BY THE CONTRACTOR SHALL BE REMOVED AND REPLACED, THE COST OF WHICH IS CONSIDERED INCLUDED IN THE CONTRACT

7. ALL WORKERS ON CANADIAN PACIFIC PROPERTY MUST BE CANADIAN PACIFIC RAILROAD SAFETY TRAINED AND WEAR RAILROAD APPROVED PERSONAL PROTECTIVE EQUIPMENT.

8. WHEN WORK IS SCHEDULED WITHIN 25 FEET OF THE NEAREST OPERATIONAL TRACK CENTERLINE, A RAILROAD FLAGGER IS REQUIRED. THREE DIFFERENT RAILROADS ARE WITHIN THE PROJECT AREA: UNION PACIFIC, CANADIAN PACIFIC, AND METRA. FLAGGING COORDINATION IS REQUIRED WITH EACH RAILROAD.

9. SPECIAL ATTENTION IS CALLED TO ARTICLE 107.12 REGARDING RAILROAD FLAGGERS. THE NAME AND TELEPHONE NUMBER OF THE CANADIAN PACIFIC RAILWAY ENGINEER IS GLYN HUGHES (630) 860-4969. THE NAME AND TELEPHONE NUMBER OF THE METRA CONTACT IS JACK RABENS (312)-322-4145.

10. ALL RAILROAD EMBANKMENT LIFTS SHALL BE COMPACTED TO NOT LESS THAN 95 PERCENT OF THE STANDARD LABORATORY DENSITY IRRESPECTIVE OF EMBANKMENT HEIGHT, THE COST OF WHICH IS CONSIDERED INCLUDED IN FURNISHED EXCAVATION

11. ALL CONTRACTORS WORKING WITHIN METRA RIGHT-OF-WAY MUST OBTAIN A RIGHT-OF-ENTRY AGREEMENT. CONTACT METRA'S RIGHT-OF-WAY ADMINISTRATOR AT (312) 322-8006 FOR DETAILS. CONTRACTOR SHALL PAY ALL CHARGES AND FEES IN ACCORDANCE WITH ARTICLE 107.04 OF THE STANDARD SPECIFICATIONS.

12. ALL WORK WITHIN THE CANADIAN PACIFIC RAILWAY'S RIGHT-OF-WAY SHALL COMPLY WITH THE CPR MINIMUM SAFETY REQUIREMENTS FOR CONTRACTORS WORKING ON RAILWAY PROPERTY. ALL WORK WITHIN THE UPRR RIGHT-OF-WAY SHALL COMPLY WITH UPRR SAFETY REQUIREMENTS.

13. METRA SIGNAL AND COMMUNICATIONS DEPARTMENTS MUST LOCATE ANY BURIED CABLES AND/OR EQUIPMNET BEFORE DIGGING MAY BEGIN ON RAILROAD PROPERTY, AND MUST PROVIDE PROTECTION FOR AND SUCH FACILITIES DURING THE ACTUAL CONSTRUCTION. J.U.L.I.E AND D.J.G.G.E.R. DO NOT LOCATE METRA/RAILROAD UTILITIES/FACILITIES.

|  |                             |                   |           |   |   |             |      |        |                       |                       |                    |                       |                   |
|--|-----------------------------|-------------------|-----------|---|---|-------------|------|--------|-----------------------|-----------------------|--------------------|-----------------------|-------------------|
| FILE NAME =<br>...D168N06-SHT-GENNOTE-01.DGN | USER NAME = nrattem         | DESIGNED - MPK    | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>GENERAL NOTES<br/>CANADIAN PACIFIC RAILWAY</b> |             |      |        | F.A.U<br>RTE.<br>1321 | SECTION<br>2010-134-T | COUNTY<br>DUPAGE   | TOTAL<br>SHEETS<br>40 | SHEET<br>NO.<br>3 |
|  | PLOT SCALE = 50.0000' / IN. | DRAWN - NMR       | REVISED - |   | SCALE:  | SHEET NO. 1 | OF 1 | SHEETS | STA.                  | TO STA.               | CONTRACT NO. 60N06 |                       |                   |
|  | PLOT DATE = 3/25/2011       | CHECKED - LGP     | REVISED - |   | [ILLINOIS] FED. AID PROJECT                       |             |      |        |                       |                       |                    |                       |                   |
|  |                             | DATE - 03/17/2011 | REVISED - |   |   |             |      |        |                       |                       |                    |                       |                   |

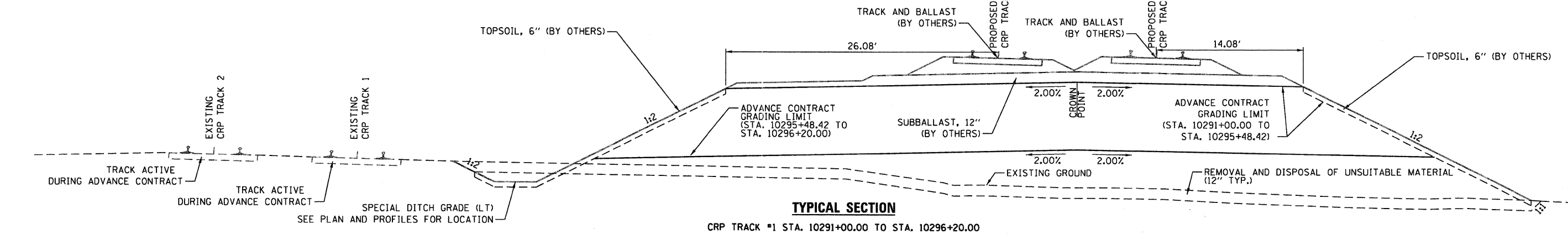
**SUMMARY OF QUANTITIES**

| ITEM NO. | SPECIALTY ITEM | SP | CODE NUMBER | ITEM  | UNIT   | TOTAL QUANTITY<br><i>URBAN</i><br><i>100% STATE</i> | CONSTRUCTION TYPE CODE   |  |
|----------|----------------|----|-------------|---|--------|---|--------------------------|--|
|          |                |    |             |   |        |   | BRIDGE, NEW CONSTRUCTION |  |
|          |                |    |             |   |        |   | 0008                     |  |
|          |                |    |             |   |        |   | 80% FED - 20% STATE      |  |
| 1        |                |    | 20100500    | TREE REMOVAL, ACRES                         | ACRE   | 0.25  | 0.25                     |  |
| 2        |                |    | 20200100    | EARTH EXCAVATION                            | CU YD  | 448   | 448                      |  |
| 3        |                |    | 20201200    | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | CU YD  | 4,843   | 4,843                    |  |
| 4        |                |    | 20400800    | FURNISHED EXCAVATION                        | CU YD  | 31,374  | 31,374                   |  |
| 5        |                |    | 21301052    | EXPLORATION TRENCH 52" DEPTH                | FOOT   | 1,000   | 1,000                    |  |
| 6        |                |    | 25000350    | SEEDING, CLASS 7                            | ACRE   | 3.25  | 3.25                     |  |
| 7        |                |    | 25000400    | NITROGEN FERTILIZER NUTRIENT                | POUND  | 293   | 293                      |  |
| 8        |                |    | 25000500    | PHOSPHORUS FERTILIZER NUTRIENT              | POUND  | 293   | 293                      |  |
| 9        |                |    | 25000600    | POTASSIUM FERTILIZER NUTRIENT               | POUND  | 293   | 293                      |  |
| 10       |                |    | 25100135    | MULCH, METHOD 4                             | ACRE   | 3.25  | 3.25                     |  |
| 11       |                |    | 28000200    | EARTH EXCAVATION FOR EROSION CONTROL        | CU YD  | 734   | 734                      |  |
| 12       |                |    | 28000250    | TEMPORARY EROSION CONTROL SEEDING           | POUND  | 9,295   | 9,295                    |  |
| 13       |                |    | 28000305    | TEMPORARY DITCH CHECKS                      | FOOT   | 364   | 364                      |  |
| 14       |                |    | 28000400    | PERIMETER EROSION BARRIER                   | FOOT   | 2,358   | 2,358                    |  |
| 15       |                |    | 28000500    | INLET AND PIPE PROTECTION                   | EACH   | 6   | 6                        |  |
| 16       |                |    | 28001000    | AGGREGATE (EROSION CONTROL)                 | TON    | 108   | 108                      |  |
| 17       |                |    | 28100105    | STONE RIPRAP, CLASS A3                      | SQ YD  | 47  | 47                       |  |
| 18       |                |    | 542A3367    | PIPE CULVERTS, CLASS A, TYPE 5 12"          | FOOT   | 372   | 372                      |  |
| 19       |                |    | 66400105    | CHAIN LINK FENCE, 4'                        | FOOT   | 675   | 675                      |  |
| 20       |                | *  | 66900200    | NON-SPECIAL WASTE DISPOSAL                  | CU YD  | 150   | 150                      |  |
| 21       |                | *  | 66900205    | SPECIAL WASTE DISPOSAL                      | CU YD  | 80  | 80                       |  |
| 22       |                |    | 67000400    | ENGINEER'S FIELD OFFICE, TYPE A             | CAL MO | 6   | 6                        |  |
| 23       |                |    | 67100100    | MOBILIZATION                                | L SUM  | 1   | 1                        |  |
| 24       |                |    | 70106800    | CHANGEABLE MESSAGE SIGN                     | CAL MO | 20  | 20                       |  |
| 25       |                |    | X7010216    | TRAFFIC CONTROL AND PROTECTION, (SPECIAL)   | L SUM  | 1   | 1                        |  |
| 26       |                |    | Z0013797    | STABILIZED CONSTRUCTION ENTRANCE            | SQ YD  | 630   | 630                      |  |
| 27       |                |    | Z0013798    | CONSTRUCTION LAYOUT                         | L SUM  | 1   | 1                        |  |
| 28       |                |    | Z0019600    | DUST CONTROL WATERING                       | UNIT   | 304   | 304                      |  |
| 29       |                |    | Z0022800    | FENCE REMOVAL                               | FOOT   | 2,457   | 2,457                    |  |
| 30       |                |    | Z0030850    | TEMPORARY INFORMATION SIGNING               | SQ FT  | 103   | 103                      |  |
| 31       |                |    | Z0048665    | RAILROAD PROTECTIVE LIABILITY INSURANCE     | L SUM  | 1   | 1                        |  |
| 32       |                |    | Z0055905    | TEMPORARY CONSTRUCTION FENCE                | FOOT   | 160   | 160                      |  |
| 33       |                |    | Z0073002    | TEMPORARY SOIL RETENTION SYSTEM             | SQ FT  | 3,477   | 3,477                    |  |

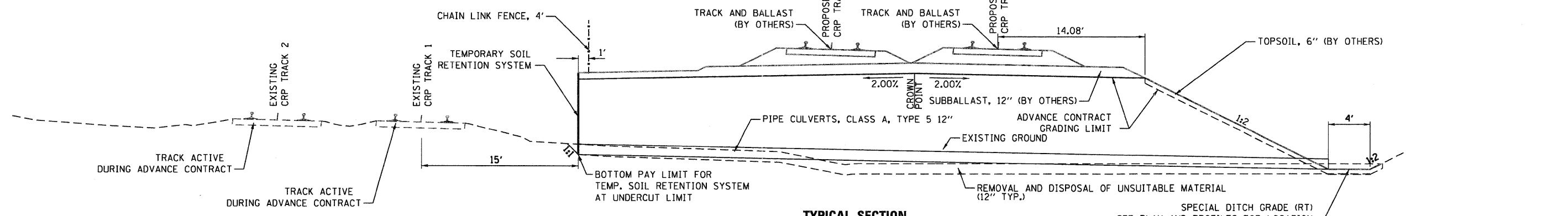
*\*Specialty Items*

|  |                     |                |           |   |   |                         |                     |                           |                    |                    |                |
|--|---------------------|----------------|-----------|---|---|-------------------------|---------------------|---------------------------|--------------------|--------------------|----------------|
| FILE NAME =<br>...D160N06-SHT-S00-01.DGN | USER NAME = rrtterm | DESIGNED - NMR | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>SUMMARY OF QUANTITIES<br/>CANADIAN PACIFIC RAILWAY</b> |                         | F.A.U. RTE.<br>1321 | SECTION<br>2010-134-T     | COUNTY             | TOTAL SHEETS<br>40 | SHEET NO.<br>4 |
| PLOT SCALE = 70,0000' / IN.              | CHECKED - LGP       | REVISED -      | REVISED - |   | SCALE:  | SHEET NO. 1 OF 1 SHEETS | STA.                | DUPAGE                    | CONTRACT NO. 60N06 |                    |                |
| PLOT DATE = 3/25/2011                    | DATE - 03/17/2011   | REVISED -      | REVISED - |   |   |                         | TO STA.             | ILLINOIS FED. AID PROJECT |                    |                    |                |
|  |                     |                |           |   |   |                         |                     |                           |                    |                    |                |

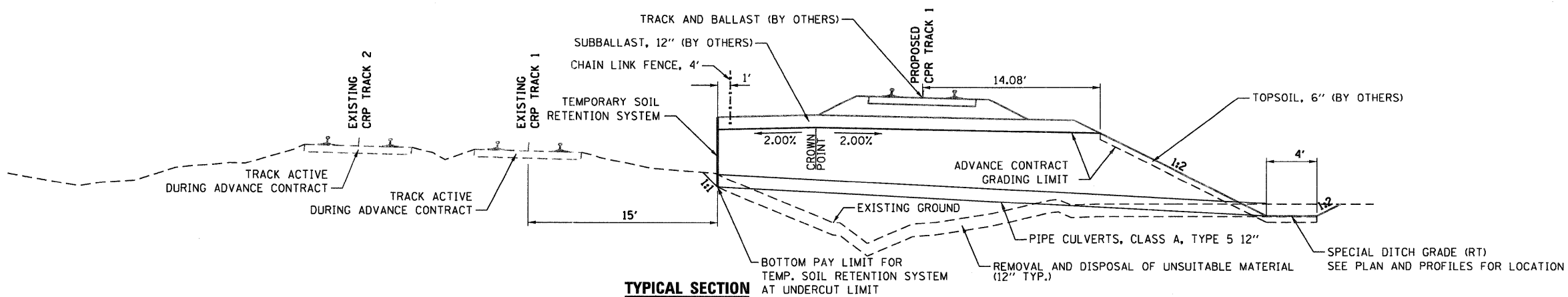




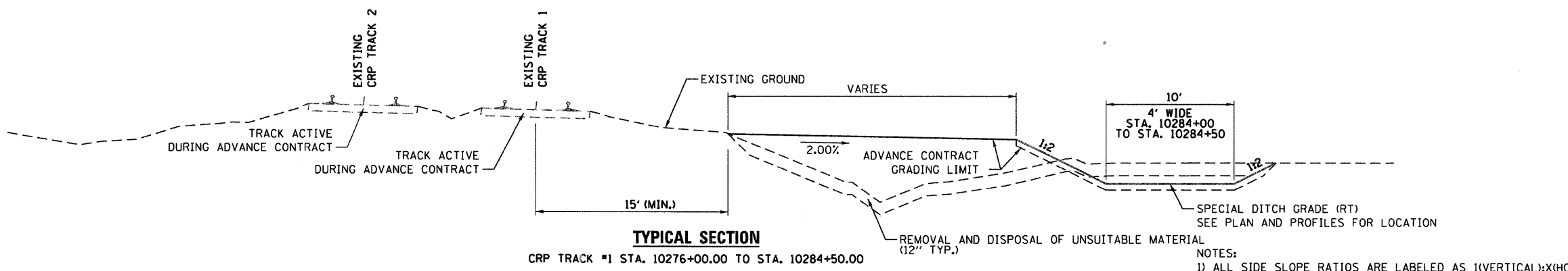
**TYPICAL SECTION**  
CRP TRACK #1 STA. 10291+00.00 TO STA. 10296+20.00



**TYPICAL SECTION**  
CRP TRACK #1 STA. 10288+00.00 TO STA. 10291+00.00



**TYPICAL SECTION**  
CRP TRACK #1 STA. 10284+50.00 TO STA. 10288+00.00



**TYPICAL SECTION**  
CRP TRACK #1 STA. 10276+00.00 TO STA. 10284+50.00

NOTES:  
1) ALL SIDE SLOPE RATIOS ARE LABELED AS 1(VERTICAL):X(HORIZONTAL)

|                                  |                     |                   |           |
|----------------------------------|---------------------|-------------------|-----------|
| FILE NAME =                      | USER NAME = nroterm | DESIGNED - NMR    | REVISED - |
| ...ND160N06-SHT-TYPICAL-RR01.DGN |                     | DRAWN - NMR       | REVISED - |
|                                  |                     | CHECKED - LGP     | REVISED - |
|                                  |                     | DATE - 03/17/2011 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS  
CANADIAN PACIFIC RAILWAY**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A.U. RTE.               | SECTION    | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------|--------|--------------|-----------|
| 1321                      | 2010-134-T | DUPAGE | 40           | 5         |
| CONTRACT NO. 60N06        |            |        |              |           |
| ILLINOIS FED. AID PROJECT |            |        |              |           |

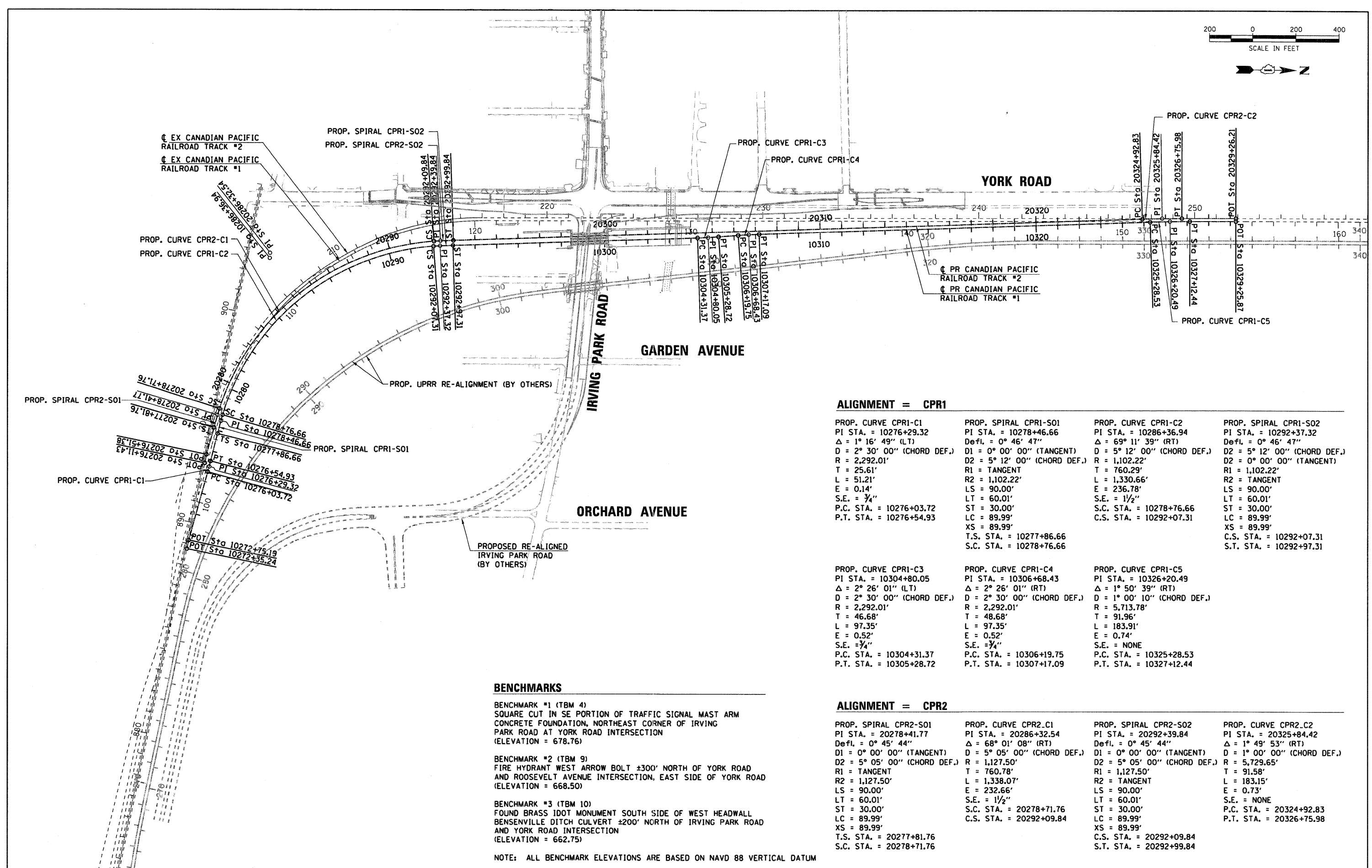
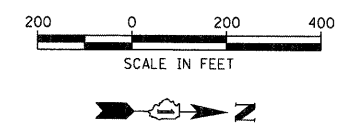
**EARTHWORK SCHEDULE**

| CANADIAN PACIFIC RAILWAY                  |    |             | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | EARTH EXCAVATION | EARTH EXCAVATION ADJUSTED FOR SHRINKAGE* | EMBANKMENT | EARTHWORK BALANCE SURPLUS (+) OR SHORTAGE (-) | FURNISHED EXCAVATION | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL ASSUMED LIMITS WITH ASSOCIATED SOIL BORINGS |
|---|----|-------------|---|------------------|--|------------|---|----------------------|---|
| STA.                                      | TO | STA.        | (CU YD)                                     | (CU YD)          | (CU YD)                                  | (CU YD)    | (CU YD)                                       | (CU YD)              |   |
| 10272+35.24                               |    | 10273+00.00 | 0.0   | 0.0              | 0.0                                      | 0.0        | 0.0   | 0.0                  |   |
| 10273+00.00                               |    | 10274+00.00 | 0.0   | 0.0              | 0.0                                      | 0.0        | 0.0   | 0.0                  |   |
| 10274+00.00                               |    | 10275+00.00 | 0.0   | 0.0              | 0.0                                      | 0.0        | 0.0   | 0.0                  |   |
| 10275+00.00                               |    | 10276+00.00 | 32.1  | 0.0              | 0.0                                      | 13.7       | -13.7   | 13.7                 |   |
| 10276+00.00                               |    | 10277+00.00 | 75.7  | 45.9             | 39.0                                     | 25.2       | 13.8  | -13.8                |   |
| 10277+00.00                               |    | 10278+00.00 | 90.2  | 57.6             | 48.9                                     | 34.5       | 14.4  | -14.4                |   |
| 10278+00.00                               |    | 10279+00.00 | 95.6  | 30.1             | 25.6                                     | 40.5       | -15.0   | 15.0                 |   |
| 10279+00.00                               |    | 10280+00.00 | 97.0  | 29.6             | 25.1                                     | 50.7       | -25.5   | 25.5                 |   |
| 10280+00.00                               |    | 10281+00.00 | 100.6                                       | 26.4             | 22.4                                     | 59.6       | -37.2   | 37.2                 |   |
| 10281+00.00                               |    | 10282+00.00 | 111.6                                       | 23.0             | 19.6                                     | 87.6       | -68.0   | 68.0                 |   |
| 10282+00.00                               |    | 10283+00.00 | 126.9                                       | 8.9              | 7.6                                      | 164.1      | -156.5  | 156.5                |   |
| 10283+00.00                               |    | 10284+00.00 | 149.0                                       | 1.0              | 0.9                                      | 220.8      | -220.0  | 220.0                |   |
| 10284+00.00                               |    | 10284+50.00 | 78.9  | 0.0              | 0.0                                      | 140.6      | -140.6  | 140.6                |   |
| 10284+50.00                               |    | 10285+00.00 | 80.7  | 0.0              | 0.0                                      | 294.4      | -294.4  | 294.4                |   |
| 10285+00.00                               |    | 10286+00.00 | 182.2                                       | 0.0              | 0.0                                      | 986.7      | -986.7  | 986.7                |   |
| 10286+00.00                               |    | 10287+00.00 | 207.1                                       | 0.0              | 0.0                                      | 1,240.0    | -1,240.0                                      | 1,240.0              |   |
| 10287+00.00                               |    | 10288+00.00 | 236.3                                       | 0.0              | 0.0                                      | 1,496.1    | -1,496.1                                      | 1,496.1              |   |
| 10288+00.00                               |    | 10289+00.00 | 270.5                                       | 0.0              | 0.0                                      | 1,990.5    | -1,990.5                                      | 1,990.5              |   |
| 10289+00.00                               |    | 10290+00.00 | 313.3                                       | 0.0              | 0.0                                      | 2,636.8    | -2,636.8                                      | 2,636.8              |   |
| 10290+00.00                               |    | 10291+00.00 | 359.3                                       | 0.0              | 0.0                                      | 3,046.4    | -3,046.4                                      | 3,046.4              |   |
| 10291+00.00                               |    | 10292+00.00 | 405.4                                       | 0.0              | 0.0                                      | 3,934.4    | -3,934.4                                      | 3,934.4              |   |
| 10292+00.00                               |    | 10293+00.00 | 427.2                                       | 0.3              | 0.3                                      | 4,470.9    | -4,470.6                                      | 4,470.6              |   |
| 10293+00.00                               |    | 10294+00.00 | 432.3                                       | 7.2              | 6.1                                      | 4,139.6    | -4,133.5                                      | 4,133.5              |   |
| 10294+00.00                               |    | 10294+84.86 | 379.8                                       | 11.6             | 9.9                                      | 3,505.0    | -3,495.2                                      | 3,495.2              |   |
| 10294+84.86                               |    | 10295+00.00 | 69.0  | 2.3              | 2.0                                      | 609.0      | -607.0  | 607.0                |   |
| 10295+00.00                               |    | 10295+48.42 | 223.4                                       | 7.0              | 6.0                                      | 1,562.5    | -1,556.5                                      | 1,556.5              |   |
| 10295+48.42                               |    | 10296+00.00 | 241.6                                       | 5.5              | 4.7                                      | 907.8      | -903.1  | 903.1                |   |
| 10296+00.00                               |    | 10296+20.00 | 56.9  | 1.0              | 0.8                                      | 97.5       | -96.7   | 96.7                 |   |
| SUBTOTAL                                  |    |             | 4,842.6                                     | 257.4            | 218.8                                    | 31,755.1   | -31,536.3                                     | 31,536.3             |   |
| TEMPORARY DITCH CPR TO UPRR               |    |             | 0.0   | 180.0            | 153.0                                    | 0.0        | 153.0   | -153.0               |   |
| ADDITIONAL EXC. AT CULVERT INLETS/OUTLETS |    |             | 0.0   | 11.0             | 9.4                                      | 0.0        | 9.4   | -9.4                 |   |
| TOTAL:                                    |    |             | 4,843                                       | 448              |  |            |   | 31,374               |   |

12" UNSUITABLE MATERIAL STA. 10284+00.00 TO 10296+20.00  
(SOIL BORINGS EB-01, EB-03, EB-04, EB-05, EB-06, EB-07)

\*EARTH EXCAVATION SHRINKAGE FACTOR=15%

|   |                     |                |                           |   |  |                         |                       |                  |                    |                |  |  |
|---|---------------------|----------------|---------------------------|---|--|-------------------------|-----------------------|------------------|--------------------|----------------|--|--|
| FILE NAME =<br>...ND160N06-SHT-SCH00-01.DGN | USER NAME = nratern | DESIGNED - NMR | REVISED -                 | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>EARTHWORK SCHEDULE<br/>CANADIAN PACIFIC RAILWAY</b> | F.A.U. RTE.<br>1321     | SECTION<br>2010-134-T | COUNTY<br>DUPAGE | TOTAL SHEETS<br>40 | SHEET NO.<br>6 |  |  |
| PLOT SCALE = 1,0000' / IN.                  | CHECKED - LGP       | REVISED -      | SCALE:                    |   |  | SHEET NO. 1 OF 1 SHEETS | STA.                  | TO STA.          | CONTRACT NO. 60N06 |                |  |  |
| PLOT DATE = 3/25/2011                       | DATE - 03/17/2011   | REVISED -      | ILLINOIS FED. AID PROJECT |   |  |                         |                       |                  |                    |                |  |  |
|   |                     |                |                           |   |  |                         |                       |                  |                    |                |  |  |



**ALIGNMENT = CPR1**

|  |  |   |  |
|--|--|---|--|
| <p>PROP. CURVE CPR1-C1<br/>PI STA. = 10276+29.32<br/>Δ = 1° 16' 49" (LT)<br/>D = 2° 30' 00" (CHORD DEF.)<br/>R = 2,292.01'<br/>T = 25.61'<br/>L = 51.21'<br/>E = 0.14'<br/>S.E. = 3/4"<br/>P.C. STA. = 10276+03.72<br/>P.T. STA. = 10276+54.93</p> | <p>PROP. SPIRAL CPR1-S01<br/>PI STA. = 10278+46.66<br/>Defl. = 0° 46' 47"<br/>D1 = 0° 00' 00" (TANGENT)<br/>D2 = 5° 12' 00" (CHORD DEF.)<br/>R = 1,102.22'<br/>T = 760.29'<br/>L = 1,330.66'<br/>E = 236.78'<br/>S.E. = 1 1/2"<br/>S.C. STA. = 10278+76.66<br/>C.S. STA. = 10292+07.31</p> | <p>PROP. CURVE CPR1-C2<br/>PI STA. = 10286+36.94<br/>Δ = 69° 11' 39" (RT)<br/>D = 5° 12' 00" (CHORD DEF.)<br/>R = 1,102.22'<br/>T = 760.29'<br/>L = 1,330.66'<br/>E = 236.78'<br/>S.E. = 1 1/2"<br/>S.C. STA. = 10278+76.66<br/>C.S. STA. = 10292+07.31</p> | <p>PROP. SPIRAL CPR1-S02<br/>PI STA. = 10292+37.32<br/>Defl. = 0° 46' 47"<br/>D2 = 5° 12' 00" (CHORD DEF.)<br/>D3 = 0° 00' 00" (TANGENT)<br/>R1 = 1,102.22'<br/>R2 = TANGENT<br/>LS = 90.00'<br/>LT = 60.01'<br/>ST = 30.00'<br/>LC = 89.99'<br/>XS = 89.99'<br/>C.S. STA. = 10292+07.31<br/>S.T. STA. = 10292+97.31</p> |
| <p>PROP. CURVE CPR1-C3<br/>PI STA. = 10304+80.05<br/>Δ = 2° 26' 01" (LT)<br/>D = 2° 30' 00" (CHORD DEF.)<br/>R = 2,292.01'<br/>T = 46.68'<br/>L = 97.35'<br/>E = 0.52'<br/>S.E. = 3/4"<br/>P.C. STA. = 10304+31.37<br/>P.T. STA. = 10305+28.72</p> | <p>PROP. CURVE CPR1-C4<br/>PI STA. = 10306+68.43<br/>Δ = 2° 26' 01" (RT)<br/>D = 2° 30' 00" (CHORD DEF.)<br/>R = 2,292.01'<br/>T = 48.68'<br/>L = 97.35'<br/>E = 0.52'<br/>S.E. = 3/4"<br/>P.C. STA. = 10306+19.75<br/>P.T. STA. = 10307+17.09</p>   | <p>PROP. CURVE CPR1-C5<br/>PI STA. = 10326+20.49<br/>Δ = 1° 50' 39" (RT)<br/>D = 1° 00' 10" (CHORD DEF.)<br/>R = 5,713.78'<br/>T = 91.96'<br/>L = 183.91'<br/>E = 0.74'<br/>S.E. = NONE<br/>P.C. STA. = 10325+28.53<br/>P.T. STA. = 10327+12.44</p>         |  |

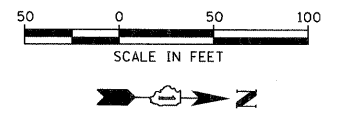
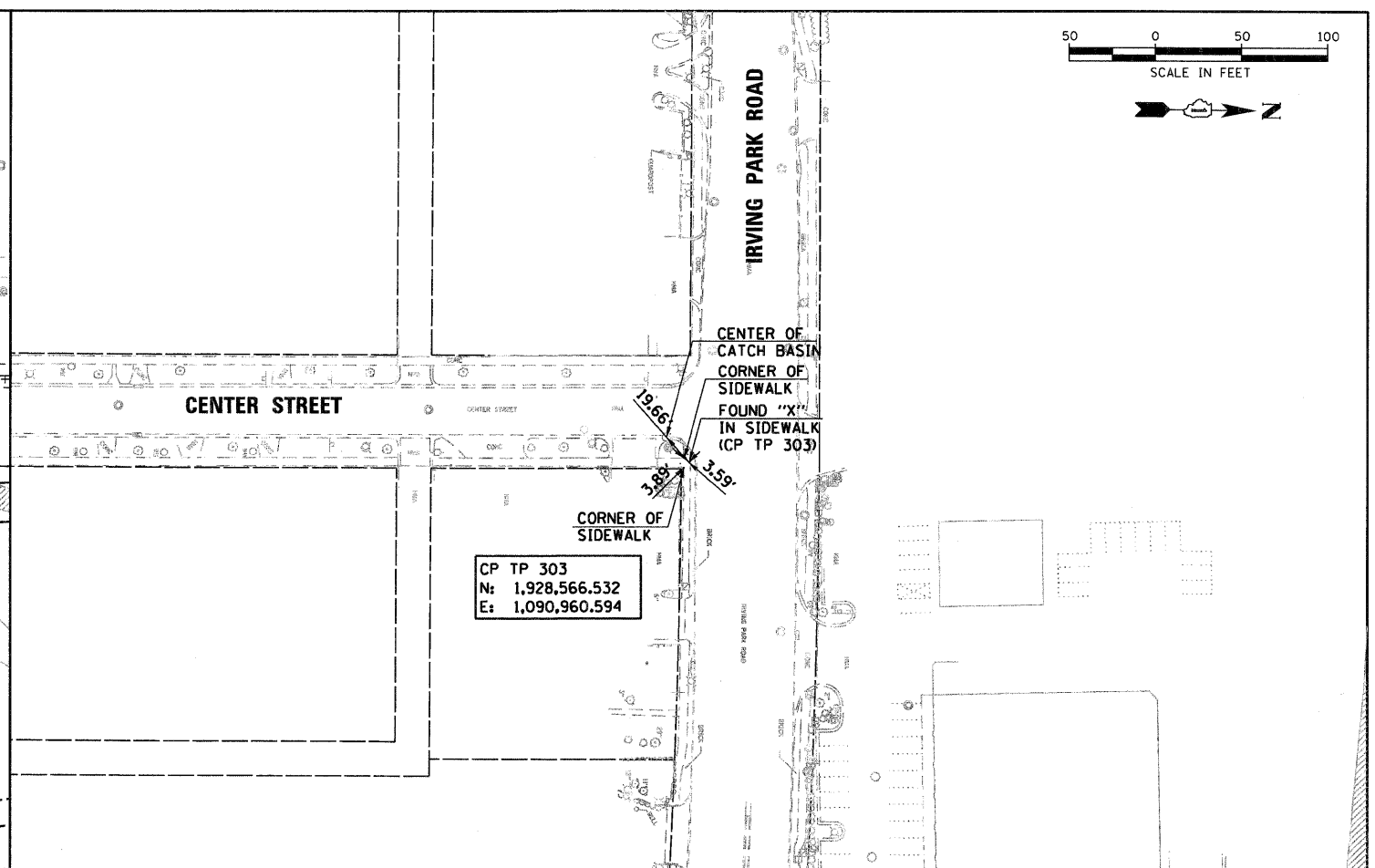
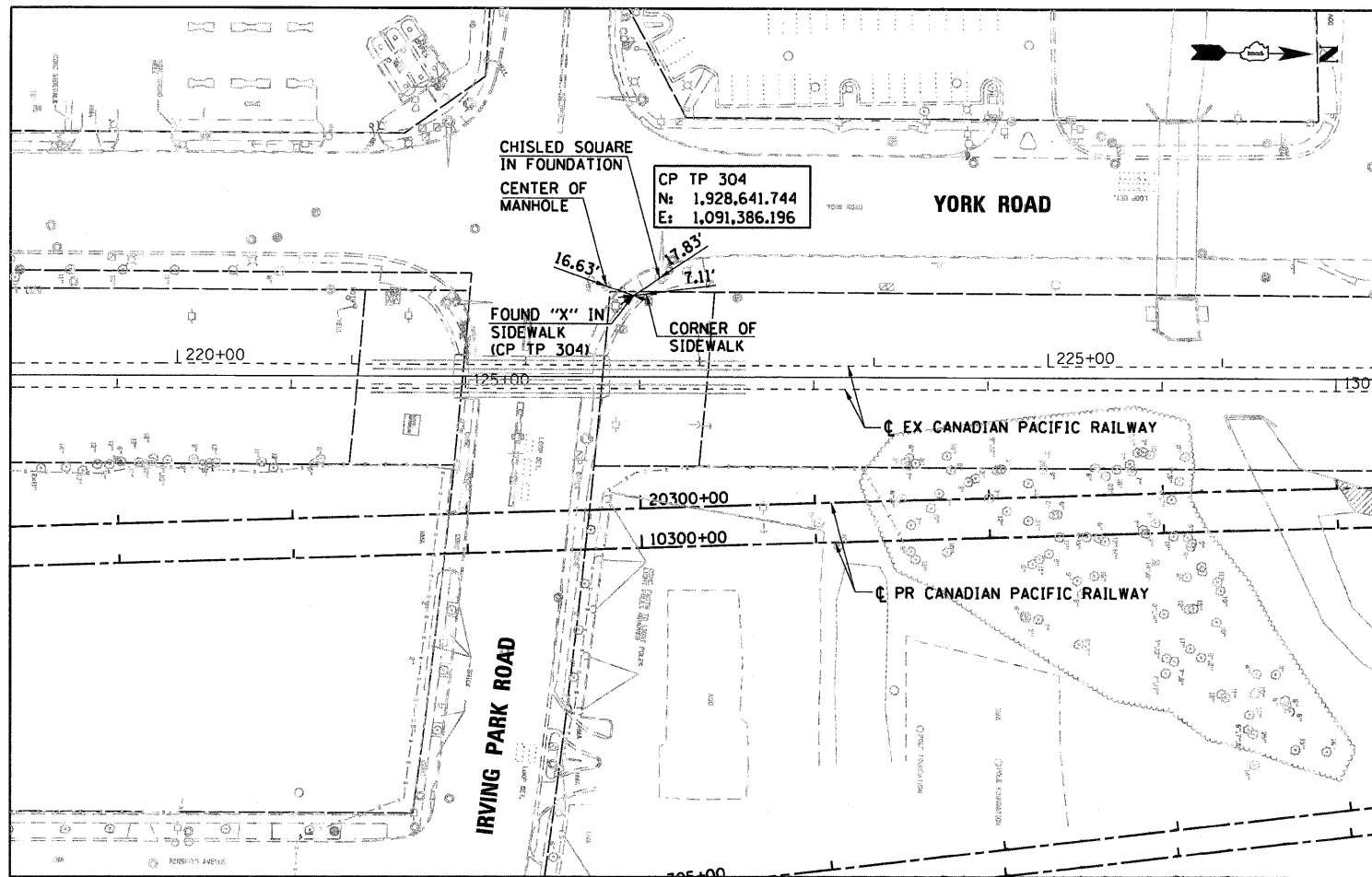
**BENCHMARKS**

- BENCHMARK #1 (TBM 4)  
SQUARE CUT IN SE PORTION OF TRAFFIC SIGNAL MAST ARM  
CONCRETE FOUNDATION, NORTHEAST CORNER OF IRVING  
PARK ROAD AT YORK ROAD INTERSECTION  
(ELEVATION = 678.76)
- BENCHMARK #2 (TBM 9)  
FIRE HYDRANT WEST ARROW BOLT ±300' NORTH OF YORK ROAD  
AND ROOSEVELT AVENUE INTERSECTION, EAST SIDE OF YORK ROAD  
(ELEVATION = 668.50)
- BENCHMARK #3 (TBM 10)  
FOUND BRASS IDOT MONUMENT SOUTH SIDE OF WEST HEADWALL  
BENSENVILLE DITCH CULVERT ±200' NORTH OF IRVING PARK ROAD  
AND YORK ROAD INTERSECTION  
(ELEVATION = 662.75)

NOTE: ALL BENCHMARK ELEVATIONS ARE BASED ON NAVD 88 VERTICAL DATUM

**ALIGNMENT = CPR2**

|  |   |  |   |
|--|---|--|---|
| <p>PROP. SPIRAL CPR2-S01<br/>PI STA. = 20278+41.77<br/>Defl. = 0° 45' 44"<br/>D1 = 0° 00' 00" (TANGENT)<br/>D2 = 5° 05' 00" (CHORD DEF.)<br/>R1 = TANGENT<br/>R2 = 1,127.50'<br/>LS = 90.00'<br/>LT = 60.01'<br/>ST = 30.00'<br/>LC = 89.99'<br/>XS = 89.99'<br/>T.S. STA. = 20277+81.76<br/>S.C. STA. = 20278+71.76</p> | <p>PROP. CURVE CPR2-C1<br/>PI STA. = 20286+32.54<br/>Δ = 68° 01' 08" (RT)<br/>D = 5° 05' 00" (CHORD DEF.)<br/>R = 1,127.50'<br/>T = 760.78'<br/>L = 1,338.07'<br/>E = 232.66'<br/>S.E. = 1 1/2"<br/>S.C. STA. = 20278+71.76<br/>C.S. STA. = 20292+09.84</p> | <p>PROP. SPIRAL CPR2-S02<br/>PI STA. = 20292+39.84<br/>Defl. = 0° 45' 44"<br/>D1 = 0° 00' 00" (TANGENT)<br/>D2 = 5° 05' 00" (CHORD DEF.)<br/>R1 = 1,127.50'<br/>R2 = TANGENT<br/>LS = 90.00'<br/>LT = 60.01'<br/>ST = 30.00'<br/>LC = 89.99'<br/>XS = 89.99'<br/>C.S. STA. = 20292+09.84<br/>S.T. STA. = 20292+99.84</p> | <p>PROP. CURVE CPR2-C2<br/>PI STA. = 20326+84.42<br/>Δ = 1° 49' 53" (RT)<br/>D = 1° 00' 00" (CHORD DEF.)<br/>R = 5,729.65'<br/>T = 91.58'<br/>L = 183.15'<br/>E = 0.73'<br/>S.E. = NONE<br/>P.C. STA. = 20324+92.83<br/>P.T. STA. = 20326+75.98</p> |
|--|---|--|---|

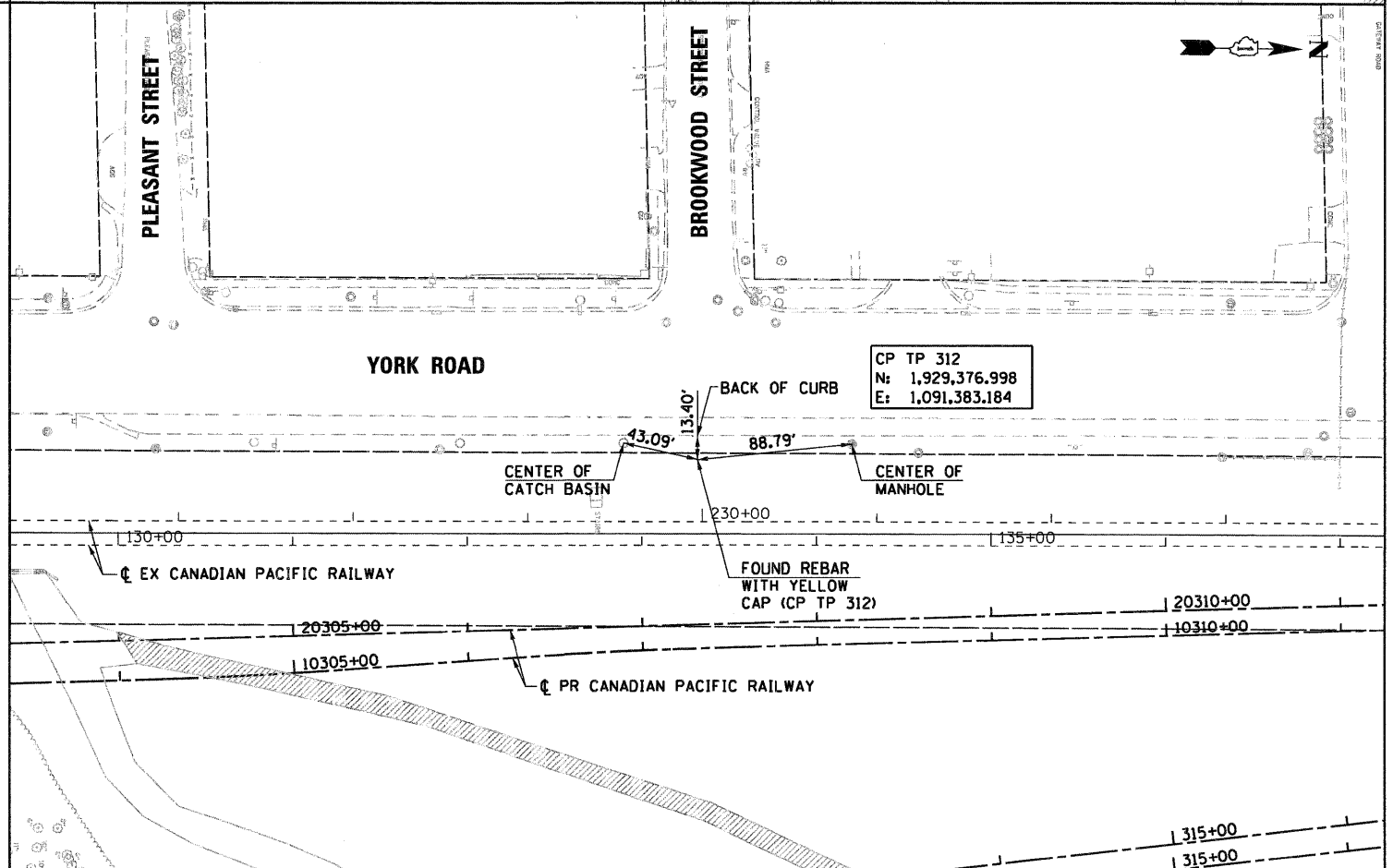


CANADIAN PACIFIC RAILWAY PROP. TRACK #1 COORDINATES

| ALIGNMENT CPR 1                      | NORTHING       | EASTING        | BEARING            | DISTANCE |
|--------------------------------------|----------------|----------------|--------------------|----------|
| POINT OF SWITCH PI STA. 10272+ 35.24 | 1,926,727.7085 | 1,092,978.8381 | N 78° 46' 00.08" W | 39.95    |
| PI TURNOUT PI STA. 10272+ 75.19      | 1,926,735.4910 | 1,092,939.6534 | N 74° 56' 54.08" W | 328.53   |
| CPR1-C1 PC STA. 10276+ 03.72         | 1,926,820.8067 | 1,092,622.3949 |                    |          |
| PI STA. 10276+ 29.32                 | 1,926,827.4571 | 1,092,597.6646 |                    |          |
| PT STA. 10276+ 54.93                 | 1,926,833.5532 | 1,092,572.7919 | N 76° 13' 43.12" W | 131.73   |
| CPR1-S01 TS STA. 10277+ 86.86        | 1,926,864.9108 | 1,092,444.8507 |                    |          |
| PI STA. 10278+ 46.66                 | 1,926,879.1949 | 1,092,386.5704 |                    |          |
| SC STA. 10278+ 76.66                 | 1,926,887.5210 | 1,092,357.7439 | N 73° 53' 22.01" W | N/A      |
| CPR1-C2 SC STA. 10278+ 76.66         | 1,926,887.5210 | 1,092,357.7439 |                    |          |
| PI STA. 10286+ 36.94                 | 1,927,098.4945 | 1,091,627.3139 |                    |          |
| CS STA. 10292+ 07.31                 | 1,927,856.2311 | 1,092,565.0795 | N 4° 41' 42.99" W  | N/A      |
| CPR1-S02 CS STA. 10292+ 07.31        | 1,927,856.2311 | 1,092,565.0795 |                    |          |
| PI STA. 10292+ 37.32                 | 1,927,886.1352 | 1,091,562.6235 |                    |          |
| ST STA. 10292+ 97.31                 | 1,927,946.0897 | 1,091,560.1567 | N 2° 21' 21.89" W  | 1134.06  |
| CPR1-C3 PC STA. 10304+ 31.37         | 1,929,079.1887 | 1,091,513.5358 |                    |          |
| PI STA. 10304+ 80.05                 | 1,929,127.8322 | 1,091,511.5344 |                    |          |
| PT STA. 10305+ 28.72                 | 1,929,176.3468 | 1,089,223.4604 | N 4° 47' 23.11" W  | 91.03    |
| CPR1-C4 PC STA. 10306+ 19.75         | 1,929,267.0597 | 1,091,499.8682 |                    |          |
| PI STA. 10306+ 68.43                 | 1,929,315.5743 | 1,091,495.8031 |                    |          |
| PT STA. 10307+ 17.09                 | 1,929,364.2178 | 1,091,493.8017 | N 2° 21' 21.89" W  | 1811.44  |
| CPR1-C5 PC STA. 10325+ 28.53         | 1,931,174.1237 | 1,091,419.3339 |                    |          |
| PI STA. 10326+ 20.49                 | 1,931,266.0086 | 1,091,415.5534 |                    |          |
| PT STA. 10327+ 12.44                 | 1,931,357.9677 | 1,091,414.7318 | N 0° 30' 42.83" W  | 213.43   |
| POT PI STA. 10329+ 25.87             | 1,931,571.3864 | 1,091,412.8250 |                    |          |

CANADIAN PACIFIC RAILWAY PROP. TRACK #2 COORDINATES

| ALIGNMENT CPR 2                      | NORTHING       | EASTING        | BEARING            | DISTANCE |
|--------------------------------------|----------------|----------------|--------------------|----------|
| POINT OF SWITCH PI STA. 20276+ 11.43 | 1,926,806.6365 | 1,092,581.4353 | N 78° 46' 00.08" W | 39.95    |
| PI TURNOUT PI STA. 20276+ 51.38      | 1,926,814.4190 | 1,092,542.2507 | N 74° 56' 54.08" W | 130.38   |
| CPR2-S01 TS STA. 20277+ 81.76        | 1,926,848.2772 | 1,092,416.3441 |                    |          |
| PI STA. 20278+ 41.77                 | 1,926,863.8599 | 1,092,358.3977 |                    |          |
| SC STA. 20278+ 71.76                 | 1,926,872.8017 | 1,092,329.7565 | N 72° 39' 41.78" W | N/A      |
| CPR2-C1 SC STA. 20278+ 71.76         | 1,926,872.8017 | 1,092,329.7565 |                    |          |
| PI STA. 20286+ 32.54                 | 1,927,099.5244 | 1,091,603.5482 |                    |          |
| CS STA. 20292+ 09.84                 | 1,927,857.8050 | 1,091,541.9678 | N 4° 38' 34.18" W  | N/A      |
| CPR2-S02 CS STA. 20292+ 09.84        | 1,927,857.8050 | 1,091,541.9678 |                    |          |
| PI STA. 20292+ 39.84                 | 1,927,887.7111 | 1,091,539.5392 |                    |          |
| ST STA. 20292+ 99.84                 | 1,927,947.6654 | 1,091,537.0724 | N 2° 21' 21.89" W  | 3192.99  |
| CPR2-C2 PC STA. 20324+ 92.83         | 1,931,137.9625 | 1,091,405.8091 |                    |          |
| PI STA. 20325+ 84.42                 | 1,931,229.4688 | 1,091,402.0441 |                    |          |
| PT STA. 20326+ 75.98                 | 1,931,321.0487 | 1,091,401.2056 | N 0° 31' 28.50" W  | 250.23   |
| POT PI STA. 20329+ 26.21             | 1,931,571.2621 | 1,091,398.9146 |                    |          |



FILE NAME = ...\\D16086-SHT-ATB-RR02.DGN

USER NAME = nmatern  
 PLOT SCALE = 50.00' / IN.  
 PLOT DATE = 3/25/2011

DESIGNED - NMR  
 DRAWN - NMR  
 CHECKED - LGP  
 DATE - 03/17/2011

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

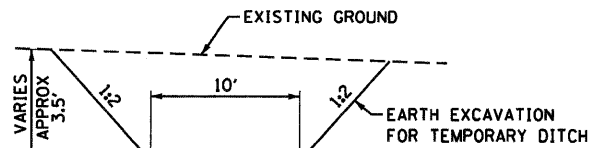
ALIGNMENT TIES & BENCHMARKS  
 CANADIAN PACIFIC RAILWAY

SCALE: 1"=50' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

| F.A.U. RTE.               | SECTION    | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------|--------|--------------|-----------|
| 1321                      | 2010-134-T | DUPAGE | 40           | 8         |
| CONTRACT NO. 60N06        |            |        |              |           |
| ILLINOIS FED. AID PROJECT |            |        |              |           |

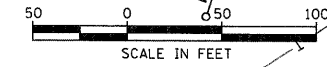
**LEGEND**

GRADING AREA

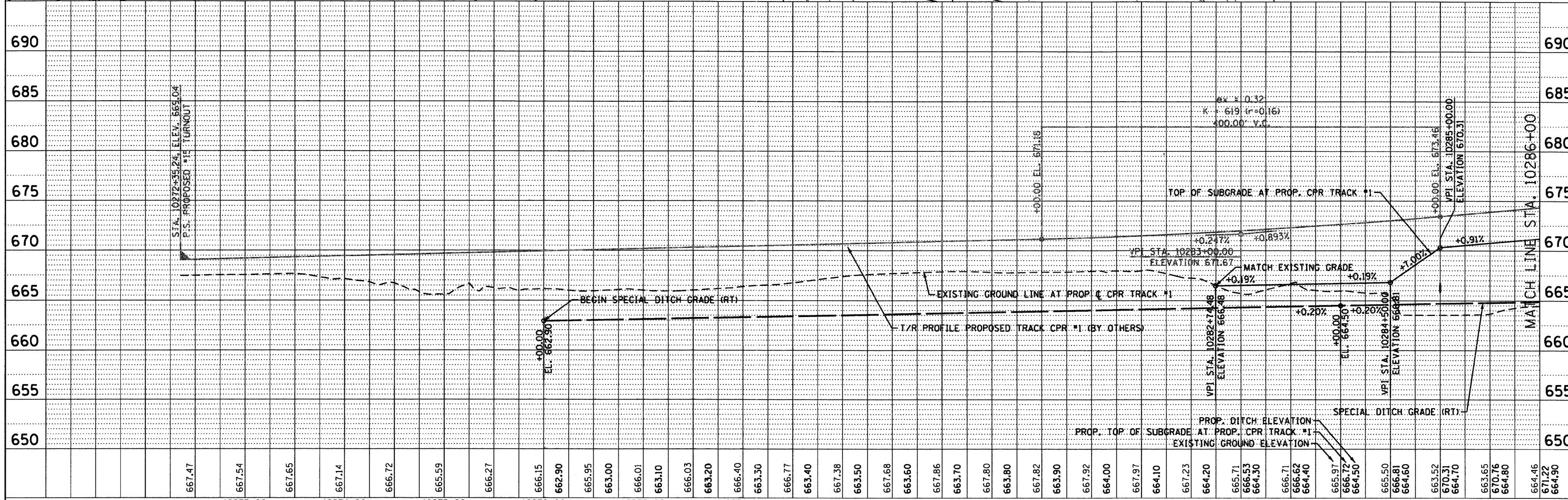
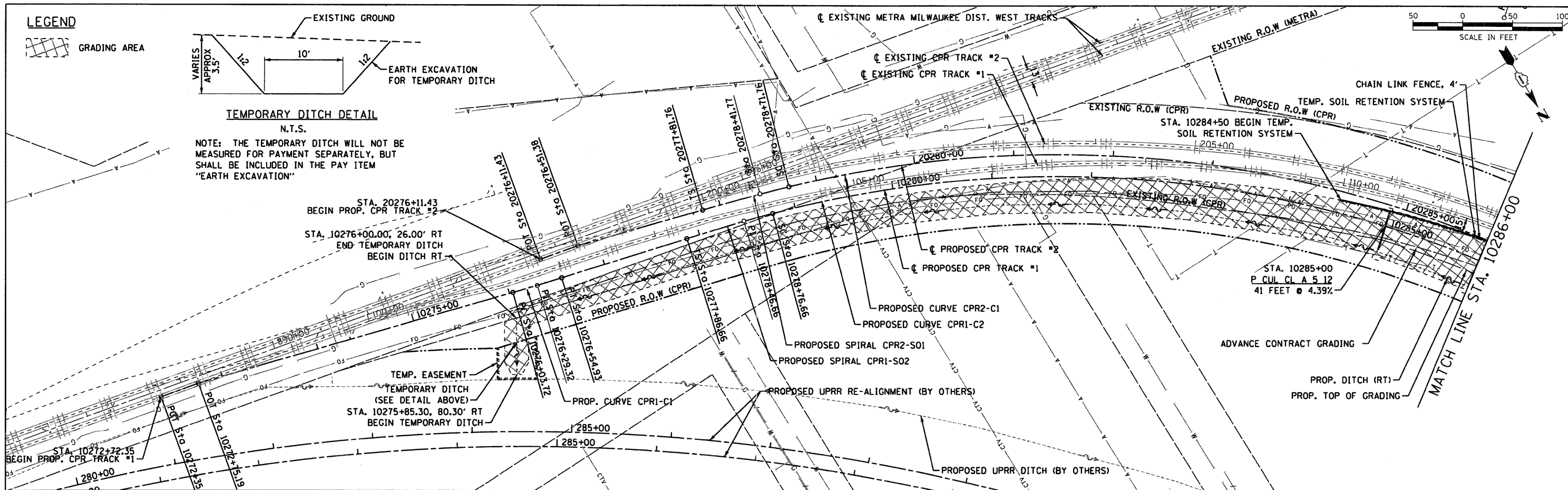


**TEMPORARY DITCH DETAIL**  
N.T.S.

NOTE: THE TEMPORARY DITCH WILL NOT BE MEASURED FOR PAYMENT SEPARATELY, BUT SHALL BE INCLUDED IN THE PAY ITEM "EARTH EXCAVATION"



SCALE IN FEET



|          |  |
|----------|--|
| DATE     |  |
| BY       |  |
| REVISION |  |
| NO.      |  |
| PLAN     |  |
| NO.      |  |
| NO.      |  |
| NO.      |  |
| NO.      |  |
| NO.      |  |
| NO.      |  |

|          |  |
|----------|--|
| DATE     |  |
| BY       |  |
| REVISION |  |
| NO.      |  |
| PROFILE  |  |
| NO.      |  |
| NO.      |  |
| NO.      |  |
| NO.      |  |
| NO.      |  |
| NO.      |  |



**LEGEND**

GRADING AREA

STA. 10291+00.00 END TEMP. SOIL RETENTION SYSTEM

TEMP SOIL RETENTION SYSTEM  
CHAIN LINK FENCE, 4'

EXISTING CPR TRACK #2  
EXISTING CPR TRACK #1

STA. 10289+00  
P.C.U.L. CL. A 5 12  
75 FEET @ 1.83%

STA. 10288+00  
P.C.U.L. CL. A 5 12  
64 FEET @ 1.68%

STA. 10287+00  
P.C.U.L. CL. A 5 12  
55 FEET @ 2.91%

PROP. CURVE CPR1-C2  
PROP. DITCH (RT)  
ADVANCE CONTRACT GRADING

STA. 10286+00  
P.C.U.L. CL. A 5 12  
48 FEET @ 3.53%

STA. 10290+00  
P.C.U.L. CL. A 5 12  
89 FEET @ 0.91%

STA. 10290+00.00  
END PROP. DITCH (RT)  
MATCH EXISTING

PROPOSED 90' SPIRAL (CPR2-S02)  
PROPOSED 90' SPIRAL (CPR1-S02)

PROP. TOP OF GRADING

PROPOSED RETAINING WALL RW-6 (BY OTHERS)

PROPOSED RETAINING WALL RW-4 (BY OTHERS)

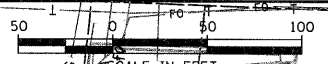
PROPOSED RETAINING WALL RW-3 (BY OTHERS)

PROPOSED R.O.W. (CPR)

PROPOSED CPR TRACK #2  
PROPOSED CPR TRACK #1

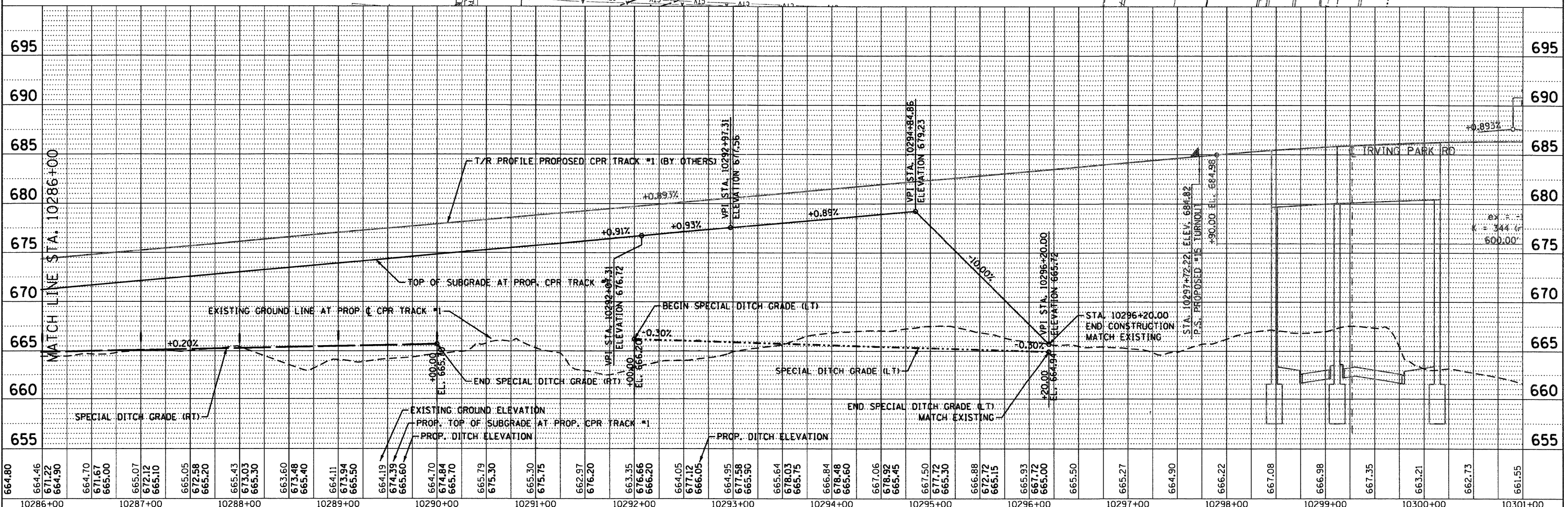
STA. 10296+20  
END CONSTRUCTION  
MATCH EXISTING

PROPOSED UPRR RE-ALIGNMENT (BY OTHERS)



|      |          |      |
|------|----------|------|
| PLAN | REVISION | DATE |
| NO.  | NO.      | NO.  |
| BY   | BY       | BY   |
| DATE | DATE     | DATE |
| NO.  | NO.      | NO.  |
| BY   | BY       | BY   |
| DATE | DATE     | DATE |
| NO.  | NO.      | NO.  |
| BY   | BY       | BY   |
| DATE | DATE     | DATE |
| NO.  | NO.      | NO.  |

|         |          |      |
|---------|----------|------|
| PROFILE | REVISION | DATE |
| NO.     | NO.      | NO.  |
| BY      | BY       | BY   |
| DATE    | DATE     | DATE |
| NO.     | NO.      | NO.  |
| BY      | BY       | BY   |
| DATE    | DATE     | DATE |
| NO.     | NO.      | NO.  |
| BY      | BY       | BY   |
| DATE    | DATE     | DATE |
| NO.     | NO.      | NO.  |



|          |          |          |          |          |          |          |          |          |          |          |          |          |          |          |          |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |
|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|
| 664.80   | 664.46   | 671.22   | 664.90   | 664.70   | 671.67   | 665.00   | 665.07   | 672.12   | 665.10   | 665.05   | 665.20   | 665.43   | 673.03   | 665.30   | 663.60   | 673.48 | 665.40 | 664.11 | 673.94 | 665.50 | 664.19 | 674.39 | 665.60 | 664.70 | 674.84 | 665.70 | 665.79 | 675.30 | 665.30 | 675.75 | 662.97 | 676.20 | 663.35 | 676.66 | 666.20 | 664.05 | 677.12 | 666.05 | 664.95 | 677.58 | 665.90 | 665.64 | 678.03 | 665.75 | 666.84 | 678.48 | 665.60 | 667.06 | 678.92 | 665.45 | 667.50 | 679.37 | 665.30 | 666.88 | 679.82 | 665.15 | 665.93 | 680.27 | 665.00 | 665.50 | 665.27 | 664.90 | 666.22 | 667.08 | 666.98 | 667.35 | 663.21 | 662.73 | 661.55 |
| 10286+00 | 10287+00 | 10288+00 | 10289+00 | 10290+00 | 10291+00 | 10292+00 | 10293+00 | 10294+00 | 10295+00 | 10296+00 | 10297+00 | 10298+00 | 10299+00 | 10300+00 | 10301+00 |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |

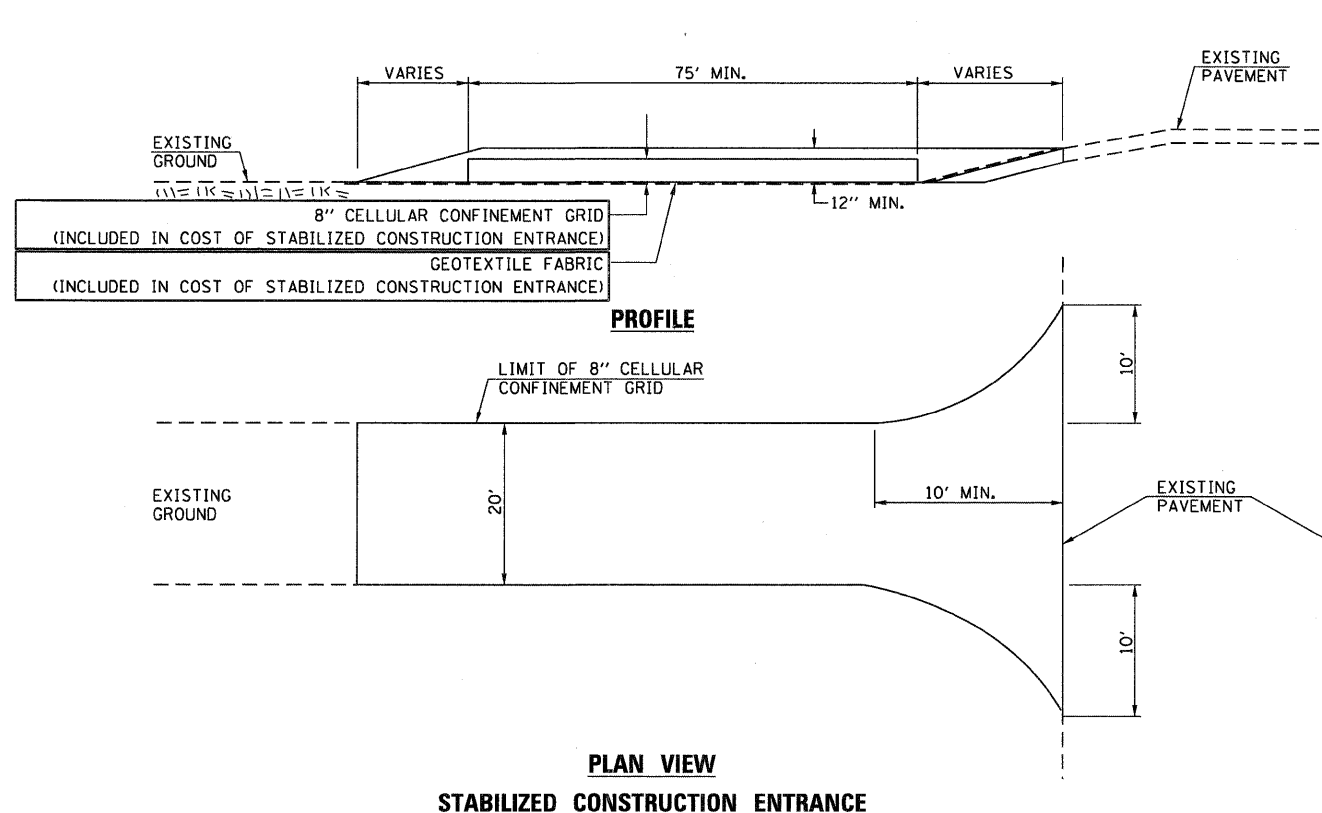




**ADVANCE CONTRACT STAGING NOTES**

1. THE RESIDENT ENGINEER SHALL NOTIFY THE IDOT TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY CONTROL DEVICES.
2. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TWO THROUGH TRAFFIC LANES IN EACH DIRECTION ON IL ROUTE 19, AND YORK ROAD UNLESS SHOWN OTHERWISE ON THE PLANS.
3. THE CONTRACTOR SHALL PROVIDE ALL SIGNS, VERTICAL PANELS, TYPE III BARRICADES, DRUMS, OR TYPE II BARRICADES, ALL TEMPORARY CONCRETE BARRIERS AND PROTECTION NECESSARY FOR THE MAINTENANCE OF TRAFFIC, OR AS DIRECTED BY THE ENGINEER.
4. THE CONTRACTOR SHALL INSTALL AND MAINTAIN PROPOSED AND TEMPORARY DRAINAGE SYSTEMS, AND EROSION CONTROL THROUGHOUT STAGE CONSTRUCTION DURING THE DURATION OF THE PROJECT.
5. THE CONTRACTOR SHALL COORDINATE THE WORK WITH ADJACENT CONTRACTORS.
6. ALL TYPE II BARRICADES, DRUMS, AND VERTICAL BARRICADES SHALL BE EQUIPPED WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS.
7. TYPE III BARRICADES SHALL BE PLACED ACROSS ALL CONSTRUCTION ENTRANCES AT ALL TIMES WORK IS NOT BEING PERFORMED. ADDITIONALLY, TEMPORARY FENCE SHALL BE PLACED ACROSS ALL CONSTRUCTION ENTRANCES AT ALL TIMES WORK IS NOT BEING PERFORMED. THE COST OF THE TYPE III BARRICADES AND TEMPORARY FENCE AND ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO PERFORM THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL).
8. ALL CONSTRUCTION WARNING SIGNS SHALL BE BLACK LEGEND ON ORANGE BACKGROUND.
9. DIMENSIONS FOR CONSTRUCTION WARNING SIGNS ON IL ROUTE 19 AND YORK ROAD SHALL BE 48" X 48". DIMENSIONS FOR CONSTRUCTION WARNING SIGNS ON SIDE ROADS SHALL BE IN ACCORDANCE WITH IDOT DISTRICT 1 DETAIL TC-10.
10. ALL "TRUCKS ENTERING HIGHWAY" AND "TRUCKS LEAVING HIGHWAY" WARNING SIGNS (W21-1104 AND W21-1105) SHALL BE EQUIPPED WITH HIGH INTENSITY FLASHING LIGHTS.
11. EXISTING TRAFFIC SIGNS IN CONFLICT WITH CONSTRUCTION OPERATIONS SHALL BE REMOVED, RELOCATED OR COVERED AS DIRECTED BY THE ENGINEER.
12. TYPE III BARRICADES SHALL HAVE TWO AMBER TYPE 'A' LOW INTENSITY FLASHING LIGHTS SPACED NEAR THE CENTERLINE OF THE SUPPORTS.
13. CONSTRUCTION EQUIPMENT SHALL NOT BE PARKED IMMEDIATELY BEHIND THE TYPE III BARRICADES AT ANY TIME.
14. PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE LOCATED ON EACH APPROACH LEG TO THE IL ROUTE 19 AT YORK ROAD INTERSECTION THROUGHOUT THE DURATION OF THE PROJECT. THE EXACT LOCATION AND MESSAGE SHALL BE APPROVED BY THE ENGINEER. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CALENDAR MONTH FOR CHANGEABLE MESSAGE SIGN.
15. SHOULD ROADWAY FLAGGERS BE REQUIRED FOR THE INGRESS AND EGRESS OF CONSTRUCTION VEHICLES AT CONSTRUCTION ENTRANCES, ALL LABOR, MATERIALS, TRANSPORTATION, EQUIPMENT AND INCIDENTAL WORK NECESSARY TO PROVIDE FLAGGERS AS WELL AS THE FLAGGING OPERATION ITSELF AT ALL CONSTRUCTION ENTRANCES SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL).
16. ARTERIAL ROAD INFORMATION SIGNS (1 EACH) SHALL BE PLACED ON EACH APPROACH TO THE IL ROUTE 19 AT YORK ROAD INTERSECTION AND SHALL BE IN ACCORDANCE WITH IDOT DISTRICT 1 DETAIL TC-22. THE EXACT LOCATION NEEDS TO BE COORDINATED WITH ADJACENT OMP ROADWAY CONSTRUCTION CONTRACT AND APPROVED BY THE ENGINEER. ALL COORDINATION, LABOR, MATERIALS, TRANSPORTATION, EQUIPMENT, AND INCIDENTAL WORK NECESSARY TO FURNISH, INSTALL, MAINTAIN, AND REMOVE ARTERIAL ROAD INFORMATION SIGNS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TEMPORARY INFORMATION SIGNING.

DENOTES ITEMS OR WORK NOT PAID FOR SEPARATELY.



**NOTES**

1. STONE
  - A. STONE SIZE - USE IDOT DESIGNATION CA-3
  - B. LENGTH - AS SHOWN IN THE MOT PLANS.
  - C. THICKNESS - NOT LESS THAN 4" ABOVE TOP OF CELLULAR CONFINEMENT GRID.
2. WIDTH - 20 FEET FULL WIDTH
3. CELLULAR CONFINEMENT GRID NOT LESS THAN EIGHT (8) INCHES IN DEPTH WILL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING OF STONE.
4. SURFACE WATER - ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION ENTRANCES SHALL BE PIPED UNDER THE ENTRANCE.
5. MAINTENANCE - THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH SHALL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY PER THIS DETAIL AND AS DIRECTED BY THE ENGINEER. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEAN OUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY SHALL BE REMOVED IMMEDIATELY.
6. PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED BY THE CONTRACTOR AFTER HEAVY USE AND EACH RAINFALL EVENT.
7. ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY (INCLUDING PIPE) TO CONSTRUCT, MAINTAIN, AND REMOVE STABILIZED CONSTRUCTION ENTRANCES SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR STABILIZED CONSTRUCTION ENTRANCE.

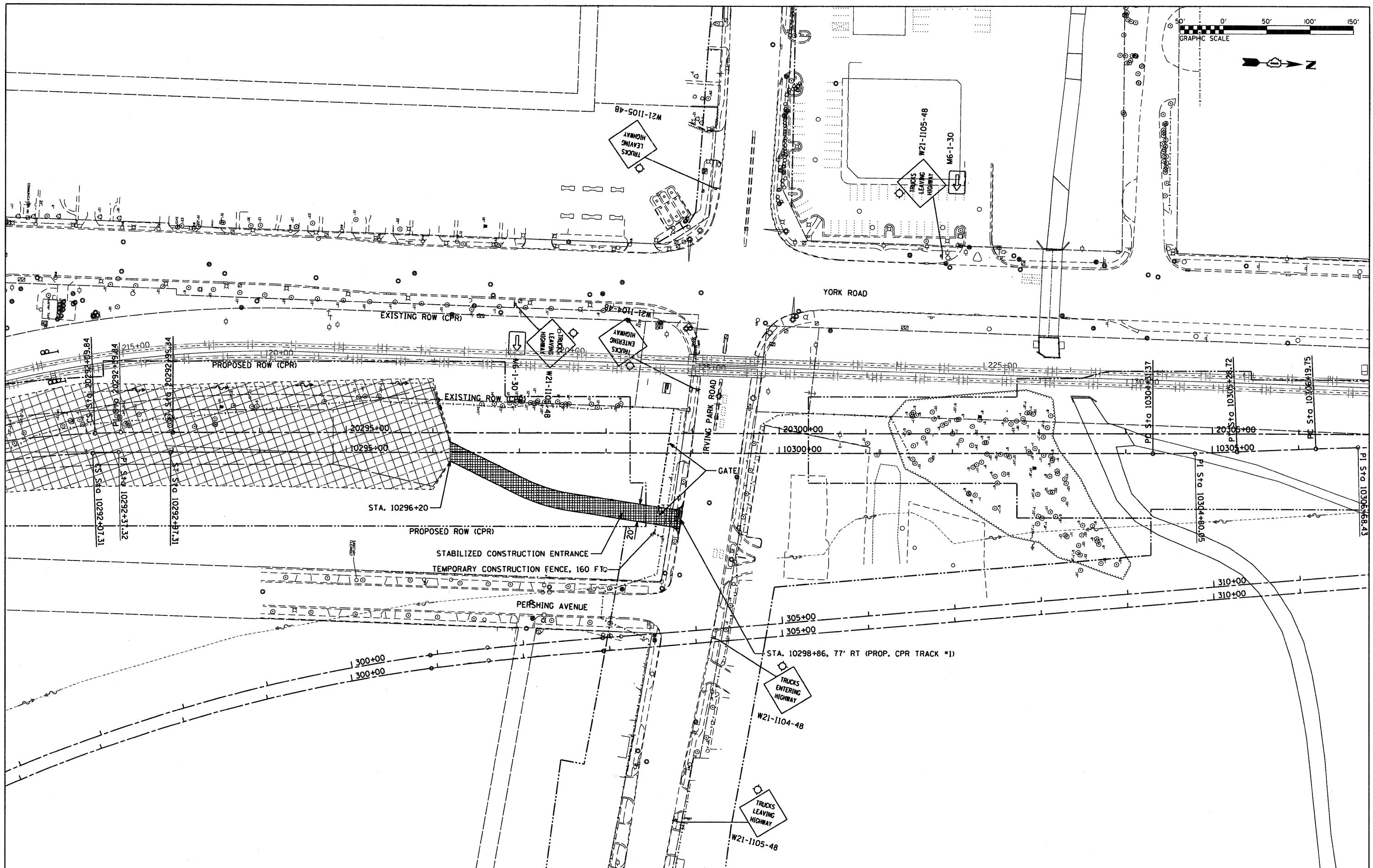
APPLICATION: TO BE USED TO REDUCE OR ELIMINATE TRACKING OF SEDIMENT ONTO PUBLIC STREETS. PLACE AT ALL POINTS OF CONSTRUCTION INGRESS AND EGRESS.

|  |                             |                   |           |
|--|-----------------------------|-------------------|-----------|
| FILE NAME =<br>...D160N06-SHT-MOT01-RR01.DGN | USER NAME = nrotarm         | DESIGNED - JJS    | REVISED - |
|  |                             | DRAWN - JJS       | REVISED - |
|  | PLOT SCALE = 50.0000" / IN. | CHECKED - LGP     | REVISED - |
|  | PLOT DATE = 3/25/2011       | DATE - 03/17/2011 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CANADIAN PACIFIC RAILWAY  
ADVANCE CONTRACT STAGING NOTES**

|                           |                       |                    |                         |                 |
|---------------------------|-----------------------|--------------------|-------------------------|-----------------|
| F.A.U. RTE.<br>1321       | SECTION<br>2010-134-T | COUNTY<br>DUPAGE   | TOTAL SHEETS<br>40      | SHEET NO.<br>12 |
| SCALE: N.T.S.             |                       |                    | SHEET NO. 1 OF 2 SHEETS |                 |
| STA. TO STA.              |                       | CONTRACT NO. 60N06 |                         |                 |
| ILLINOIS FED. AID PROJECT |                       |                    |                         |                 |



FILE NAME =  
 ... \DIS\806-SHT-MOT02-RR01.DGN

USER NAME = nratern  
 DESIGNED - JJS  
 DRAWN - JJS  
 PLOT SCALE = 50.0000' / IN.  
 CHECKED - LGP  
 PLOT DATE = 3/25/2011  
 DATE - 03/17/2011

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC  
 CANADIAN PACIFIC RAILWAY**

SCALE: 1"=50' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

| F.A.U. RTE.               | SECTION    | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------|--------|--------------|-----------|
| 1321                      | 2010-134-T | DUPAGE | 40           | 13        |
| CONTRACT NO. 60N06        |            |        |              |           |
| ILLINOIS FED. AID PROJECT |            |        |              |           |

**EROSION CONTROL GENERAL NOTES**

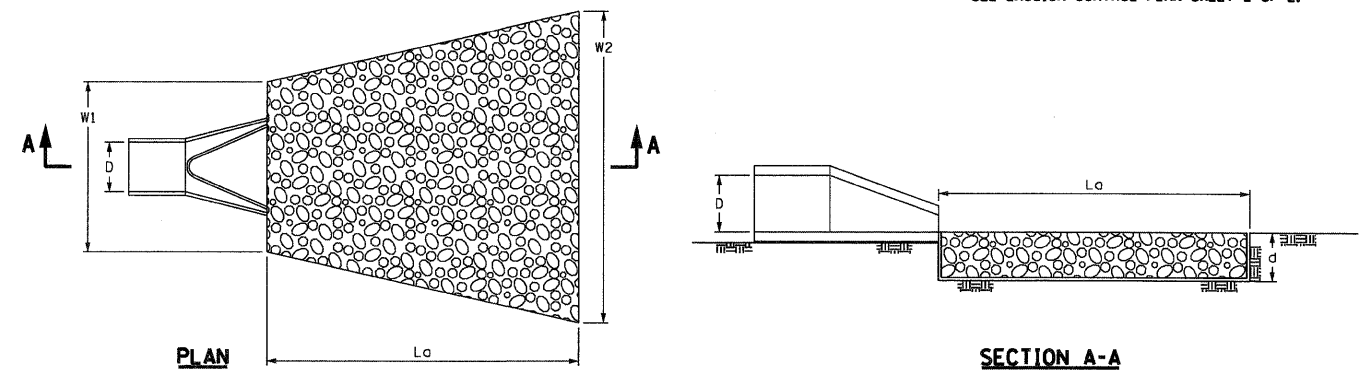
1. UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICE WILL BE INSTALLED, AT A MINIMUM, ACCORDING TO THE IDOT STANDARD SPECIFICATIONS, SPECIAL PROVISIONS, HIGHWAY STANDARDS AND DETAILS CONTAINED HEREIN, REVISED TO THE LATEST EDITION AS AMENDED. A COPY OF THE APPROVED EROSION AND SEDIMENTATION CONTROL PLAN IN CONJUNCTION WITH THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP) SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
2. THE CONTRACTOR SHALL CLEAN UP AND GRADE THE WORK AREAS AS THE PROJECT PROGRESSES TO ELIMINATE THE CONCENTRATION OF RUNOFF, OR SHALL INSTALL APPROPRIATE SEDIMENT AND EROSION CONTROL DEVICES TO HELP ELIMINATE EROSION AND TRAP SEDIMENT.
3. DURING DEWATERING OPERATIONS, WATER WILL BE PUMPED INTO A SILT BAG AND DISCHARGED INTO A SEDIMENTATION BASIN OR SILT TRAP. DEWATERING DIRECTLY INTO FIELD TILES, STORMWATER STRUCTURES OR WATERS OF THE US IS STRICTLY PROHIBITED.
4. THE CONTRACTOR SHALL MAINTAIN AND PRESERVE ANY EXISTING SUB-SURFACE DRAINAGE SYSTEMS, SUCH AS FIELD TILES ACCORDING TO EXISTING DESIGN AND CONSTRUCTION STANDARDS.
5. NO STREAMBANK WORK IS ALLOWED BEYOND THE PERMITTED AREA.
6. A WRITTEN STORMWATER POLLUTION PREVENTION PLAN (SWPPP), MEETING THE REQUIREMENTS OF THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY CURRENT NPDES STORM WATER PERMIT FOR CONSTRUCTION ACTIVITIES, HAS BEEN PROVIDED INCLUDING DETAILS OF SPECIFIC DEVICES INCLUDING, BUT NOT LIMITED TO, SILT FENCE AND DITCH CHECKS, TO BE UTILIZED FOR EROSION AND SEDIMENT CONTROL.
7. CONSTRUCTION ACTIVITIES SHALL BE SCHEDULED TO MINIMIZE THE TIME SOIL IS EXPOSED AND UNPROTECTED.
8. THE CONTRACTOR SHALL MAKE INSPECTIONS A MINIMUM OF ONCE EVERY SEVEN (7) DAYS OF THE FOLLOWING:
  - A. DISTURBED AREAS OF THE PROJECT SITE THAT HAVE NOT BEEN FULLY STABILIZED.
  - B. STRUCTURAL CONTROL MEASURES, INCLUDING BY NOT LIMITED TO, SILT FENCE AND DITCH CHECKS.
  - C. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE.
9. THE CONTRACTOR MUST COOPERATE WITH ANY AGENTS WHO MAKE THE SITE VISITS TO REVIEW COMPLIANCE WITH THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP) IN THE FIELD AND AUDIT THE LOGS AND RECORDS REQUIRED BY THE PERMIT.
10. STOCKPILES OF SOIL SHALL NOT BE LOCATED WITHIN FLOODPLAINS, RIPARIAN AREAS, WETLANDS AND WATERS OF THE US.
11. IF A STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN THREE (3) DAYS, THEN THE SEDIMENT AND EROSION CONTROL SHALL BE PROVIDED FOR SAID STOCKPILE OR A COVERING OVER THE STOCKPILE.
12. IF THE VOLUME, VELOCITY, SEDIMENT LOAD OR PEAK FLOW RATES OF STORMWATER RUNOFF ARE TEMPORARILY INCREASED DURING CONSTRUCTION THEN PROPERTIES AND SPECIAL MANAGEMENT AREAS DOWNSTREAM FROM SUCH DEVELOPMENT SITES SHALL BE PROTECTED FROM EROSION.
13. TEMPORARY DITCH CHECKS SHALL BE PLACED AFTER DITCH IS INSTALLED AND BEFORE STORM DRAIN BECOMES OPERATIONAL. THE TEMPORARY DITCH CHECK SHALL BE CONSTRUCTED SO THAT THE CENTER IS APPROXIMATELY SIX (6) INCHES LOWER THAN THE OUTER EDGES SO WATER WILL FLOW ACROSS THE CENTER AND NOT AROUND THE ENDS. SPACING BETWEEN TEMPORARY DITCH CHECKS SHALL BE AS SHOWN IN THE PLANS.
14. ANY AGGREGATE BASE EROSION CONTROL MEASURES SHALL BE REMOVED AND REPLACED WITH NEW AGGREGATE WHEN STRUCTURE BECOMES PLUGGED, SILT LADEN OR AS REQUIRED BY INSPECTION.
15. SEDIMENT CONTROL MEASURES SHALL BE PROVIDED FOR ALL EXISTING AND PROPOSED INLET STRUCTURES WITHIN THE CONSTRUCTION LIMITS.
16. INLET FILTERS ARE TO BE MAINTAINED WHERE INDICATED IN THE CURRENT STAGE AND IF INSTALLED IN A PREVIOUS STAGE.
17. ALL AREAS BROUGHT TO FINAL GRADE SHALL HAVE ALL TEMPORARY ESC MEASURES REMOVED, AND STABILIZED WITH TOPSOIL AND PERMANENT VEGETATION.
18. NATURAL RESOURCE CONSERVATION SERVICE (NRCS) AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) STANDARDS PRESENTED ON THE CONTRACT DRAWINGS ARE TO BE CONSIDERED A MINIMUM.
19. ANY EROSION AND/OR SEDIMENTATION CONTROL DEVICES DISRUPTED, THAT BELONG TO ANY CONCURRENT CONSTRUCTION CONTRACTS, SHALL BE REPLACED IN A KIND AT THE CONTRACTOR'S EXPENSE.
20. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING TEMPORARY DRAINAGE DURING THE PROJECT. DAMAGE TO THE SUBGRADE, EXISTING FACILITIES AND/OR CONSTRUCTED WORK WILL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE RESIDENT ENGINEER AT NO ADDITIONAL COST. ANY ALTERATIONS OR ADDITIONS TO THE EXISTING SOIL EROSION AND SEDIMENT CONTROL PLAN SHALL BE SUBMITTED TO THE KANE-DUPAGE COUNTY SOIL AND WATER CONSERVATION DISTRICT FOR APPROVAL AND EITHER ADDED TO THE EXISTING PLAN OR UTILIZED IN PLACE OF THE EXISTING PLAN.
21. TEMPORARY AND/OR PERMANENT SOIL STABILIZATION (VEGETATION OR HARDSCAPE) SHALL BE APPLIED TO EXPOSED AREAS AS SOON AS POSSIBLE. SOIL STABILIZATION SHALL BE PROVIDED WITHIN SEVEN (7) DAYS AFTER FINAL GRADE IS REACHED ON ANY PORTION OF THE SITE AND WITHIN SEVEN (7) DAYS TO EXPOSED AREAS WHICH MAY NOT BE AT FINAL GRADE BUT WILL REMAIN UNDISTURBED FOR LONGER THAN FOURTEEN (14) DAYS.
22. ALL EMBANKMENTS SHOULD BE TRACKED PER THE REQUIREMENTS OF THE ILLINOIS URBAN MANUAL SECTION 453 - SURFACE ROUGHENING. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF 'FURNISHED EXCAVATION' AND NO ADDITIONAL COMPENSTION WILL BE ALLOWED.

DENOTES ITEMS OR WORK NOT PAID FOR SEPARATELY.

**MAINTENANCE FOR SOIL EROSION CONTROL MEASURES**

1. INSPECTIONS SHALL BE PERFORMED AT LEAST ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A STORM THAT IS 0.5 INCHES OR GREATER EQUIVALENT SNOWFALL.
2. VEGETATIVE GROWTH OF TEMPORARY OR PERMANENT SEEDING, SODDING AND VEGETATED CHANNELS SHALL BE MAINTAINED ON A CONTINUAL BASIS WITH ADEQUATE WATER AND FERTILIZER.
3. THE VEGETATIVE COVER SHALL BE REMOVED AND RE-SEEDED/REPLACED AS NECESSARY UNTIL GERMINATION OR UNTIL BARE EARTH AREAS SUPPORT VEGETATION.
4. PERIMETER EROSION BARRIER SHALL BE INSPECTED PERIODICALLY AND REPAIRED AS NEEDED.
5. INLET PROTECTION, PERIMETER EROSION BARRIER AND DITCH CHECKS SHALL HAVE SEDIMENT REMOVED UPON REACHING HALF THE HEIGHT OR CAPACITY OF THE DEVICE.
6. FOR PROTECTION OF TREES, ANY PROTECTIVE MEASURES INSTALLED WHICH ARE DAMAGED OR REMOVED WILL BE REPAIRED IMMEDIATELY.
7. ALL ESC MEASURES WILL BE MAINTAINED IN ACCORDANCE WITH THE IDOT EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION (DATED JULY 1, 2010): (HTTP://WWW.DOT.IL.GOV/DESENV/ENVIRONMENTAL/1DOT%20FIELD%20GUIDE.PDF) AND IDOT'S BEST MANAGEMENT PRACTICES - MAINTENANCE GUIDES: HTTP://WWW.DOT.STATE.IL.US/DESENV/ENVIRONMENTAL/BESTPRACTICES.HTML

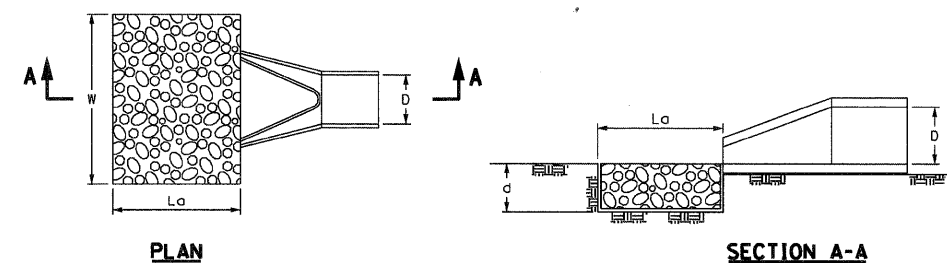
**RIPRAP PROTECTION AT CULVERT OUTLETS**



NOTE:  
PAID FOR AS STONE RIPRAP, CLASS A3 PER 50 YD.  
SEE EROSION CONTROL PLAN SHEET 2 OF 2.

| LOCATION           | RIPRAP GRADATION | Lo (FT) | W1 (FT) | W2 (FT) | D (IN) | d (IN) | AREA (SQ YD) |
|--------------------|------------------|---------|---------|---------|--------|--------|--------------|
| 10285+00, 24.7' RT | RR3              | 10      | 4       | 7.3     | 12     | 8      | 6.3          |
| 10286+00, 26.2' RT | RR3              | 10      | 4       | 7.3     | 12     | 8      | 6.3          |
| 10287+00, 27.6' RT | RR3              | 10      | 4       | 7.3     | 12     | 8      | 6.3          |
| 10288+00, 29.0' RT | RR3              | 10      | 4       | 7.3     | 12     | 8      | 6.3          |
| 10289+00, 30.4' RT | RR3              | 10      | 4       | 7.3     | 12     | 8      | 6.3          |
| 10290+00, 31.8' RT | RR3              | 10      | 4       | 7.3     | 12     | 8      | 6.3          |

**RIPRAP PROTECTION AT CULVERT INLETS**



NOTE:  
PAID FOR AS STONE RIPRAP, CLASS A3 PER 50 YD.  
SEE EROSION CONTROL PLAN SHEET 2 OF 2.

| LOCATION           | RIPRAP GRADATION | Lo (FT) | W (FT) | D (IN) | d (IN) | AREA (SQ YD) |
|--------------------|------------------|---------|--------|--------|--------|--------------|
| 10285+00, 16.1' LT | RR3              | 3       | 4      | 12     | 8      | 1.4          |
| 10286+00, 21.1' LT | RR3              | 3       | 4      | 12     | 8      | 1.4          |
| 10287+00, 26.8' LT | RR3              | 3       | 4      | 12     | 8      | 1.4          |
| 10288+00, 34.7' LT | RR3              | 3       | 4      | 12     | 8      | 1.4          |
| 10289+00, 44.4' LT | RR3              | 3       | 4      | 12     | 8      | 1.4          |
| 10290+00, 56.3' LT | RR3              | 3       | 4      | 12     | 8      | 1.4          |

FILE NAME = ...\\D168N06-SHT-EROS-RR01.DGN

USER NAME = nroterm  
DESIGNED - JJS  
DRAWN - JJS  
PLOT SCALE = 50,0000' / IN.  
PLOT DATE = 3/25/2011  
CHECKED - LGP  
DATE - 03/17/2011

REVISED -  
REVISED -  
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REVISED -

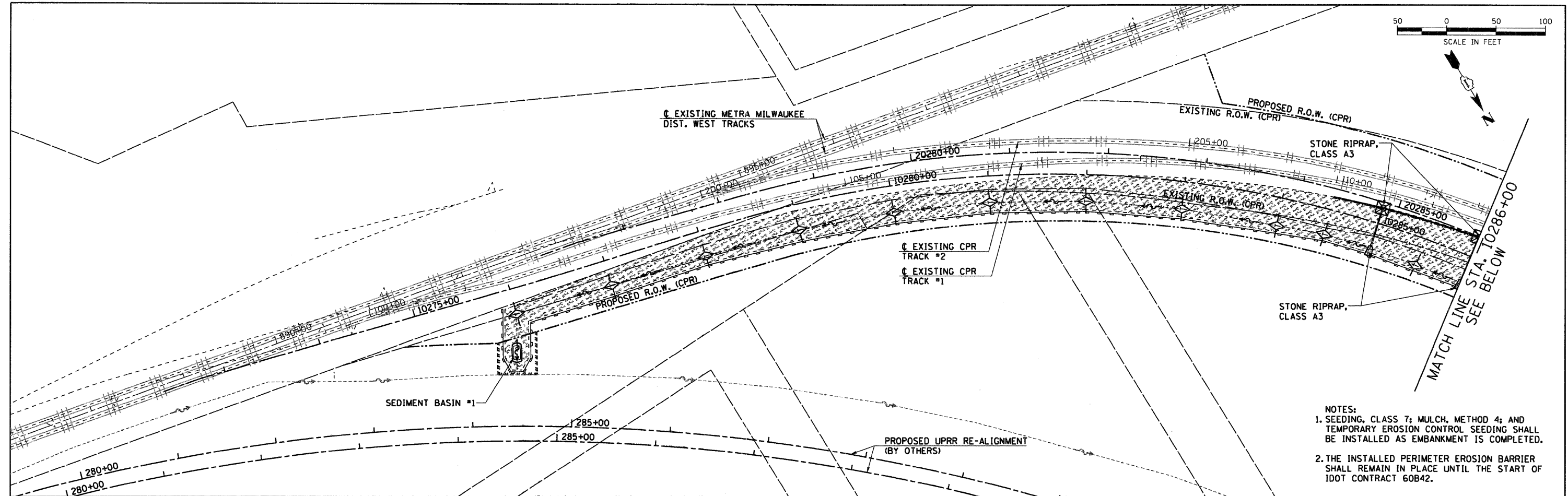
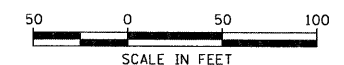
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EROSION CONTROL PLAN  
CANADIAN PACIFIC RAILWAY**

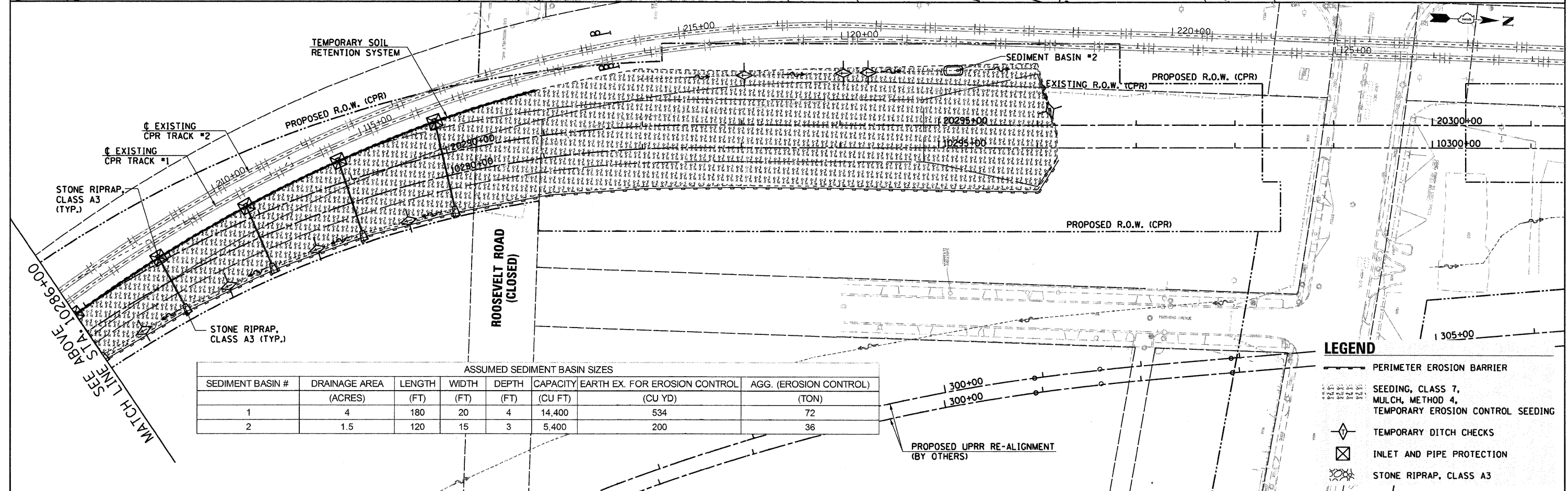
SCALE: NTS SHEET NO. 1 OF 2 SHEETS STA. TO STA.

|                    |            |        |                           |           |
|--------------------|------------|--------|---------------------------|-----------|
| F.A.U. RTE.        | SECTION    | COUNTY | TOTAL SHEETS              | SHEET NO. |
| 1321               | 2010-134-T | DUPAGE | 40                        | 14        |
| CONTRACT NO. 60N06 |            |        | ILLINOIS FED. AID PROJECT |           |





- NOTES:
1. SEEDING, CLASS 7; MULCH, METHOD 4; AND TEMPORARY EROSION CONTROL SEEDING SHALL BE INSTALLED AS EMBANKMENT IS COMPLETED.
  2. THE INSTALLED PERIMETER EROSION BARRIER SHALL REMAIN IN PLACE UNTIL THE START OF IDOT CONTRACT 60B42.



ASSUMED SEDIMENT BASIN SIZES

| SEDIMENT BASIN # | DRAINAGE AREA (ACRES) | LENGTH (FT) | WIDTH (FT) | DEPTH (FT) | CAPACITY (CU FT) | EARTH EX. FOR EROSION CONTROL (CU YD) | AGG. (EROSION CONTROL) (TON) |
|------------------|-----------------------|-------------|------------|------------|------------------|---------------------------------------|------------------------------|
| 1                | 4                     | 180         | 20         | 4          | 14,400           | 534                                   | 72                           |
| 2                | 1.5                   | 120         | 15         | 3          | 5,400            | 200                                   | 36                           |

- LEGEND**
- PERIMETER EROSION BARRIER
  - SEEDING, CLASS 7, MULCH, METHOD 4, TEMPORARY EROSION CONTROL SEEDING
  - TEMPORARY DITCH CHECKS
  - INLET AND PIPE PROTECTION
  - STONE RIPRAP, CLASS A3

FILE NAME = ...ND160N06-SHT-EROS-RR02.DGN

USER NAME = nroterm  
 DESIGNED - JJS  
 DRAWN - JJS  
 CHECKED - LGP  
 PLOT DATE = 3/25/2011  
 REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

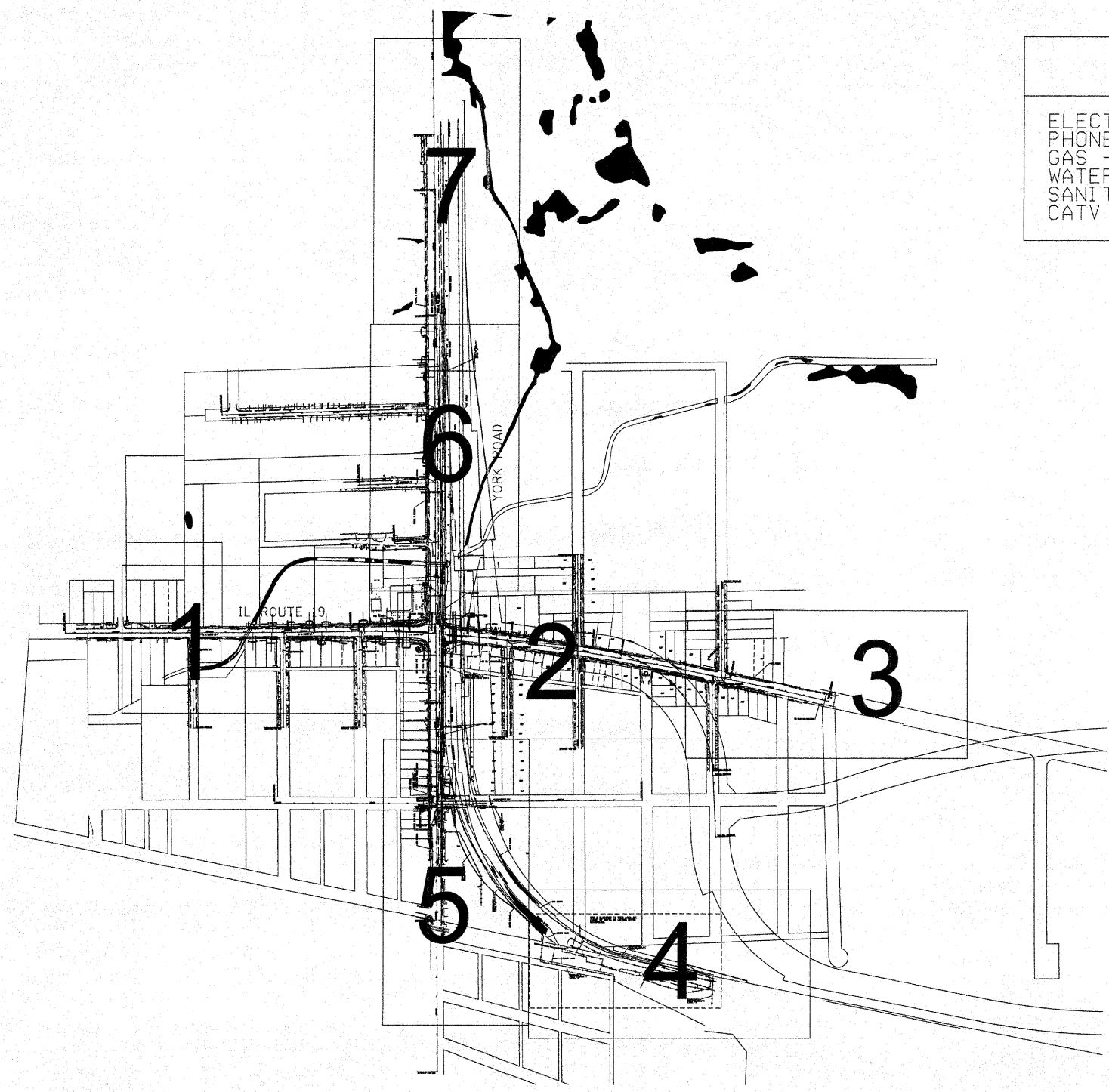
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**EROSION CONTROL PLAN**  
**CANADIAN PACIFIC RAILWAY**  
 SCALE: 1"=50'  
 SHEET NO. 2 OF 2 SHEETS  
 STA. 10272+26.24 TO STA. 10298+00

| F.A.U. RTE.        | SECTION    | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|------------|--------|---------------------------|-----------|
| 1321               | 2010-134-T | DUPAGE | 40                        | 15        |
| CONTRACT NO. 60N06 |            |        | ILLINOIS FED. AID PROJECT |           |



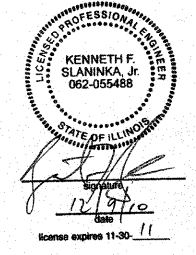
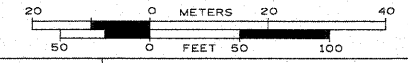
| UTILITIES OWNERS<br>UNLESS OTHERWISE NOTED: |                          |
|---|--------------------------|
| ELECTRIC                                    | - COMED                  |
| PHONE                                       | - AT&T                   |
| GAS   | - NICOR                  |
| WATER                                       | - VILLAGE OF BENSENVILLE |
| SANITARY                                    | - VILLAGE OF BENSENVILLE |
| CATV  | - COMCAST                |



|                     |                |
|---------------------|----------------|
| — A — A — A —       | AERIAL UTILITY |
| - - - - -           | UNKNOWN        |
| — CTV — CTV — CTV — | CABLE TV       |
| — T — T — T —       | TELEPHONE      |
| — G — G — G —       | GAS            |
| — E — E — E —       | ELECTRIC       |
| — W — W — W —       | WATER          |
| — FO — FO — FO —    | FIBER OPTIC    |
| — S — S — S —       | SEWER          |
| ⊕                   | TBE TEST HOLE  |

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ALL UTILITIES SHOWN QUALITY LEVEL "B"  
UNLESS NOTED OTHERWISE.



TBE Job No. IL09510417  
SUE Plan Page: Cover

|   |
|---|
| Utility Quality Level "A": Test Hole            |
| Utility Quality Level "B": Designating          |
| Utility Quality Level "C": Research with Survey |
| Utility Quality Level "D": Records Research     |

|          |          |         |
|----------|----------|---------|
| DESIGNED | EG       | REVISED |
| DRAWN    | KLC      | REVISED |
| CHECKED  | KFS      | REVISED |
| DATE     | 12/06/10 | REVISED |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

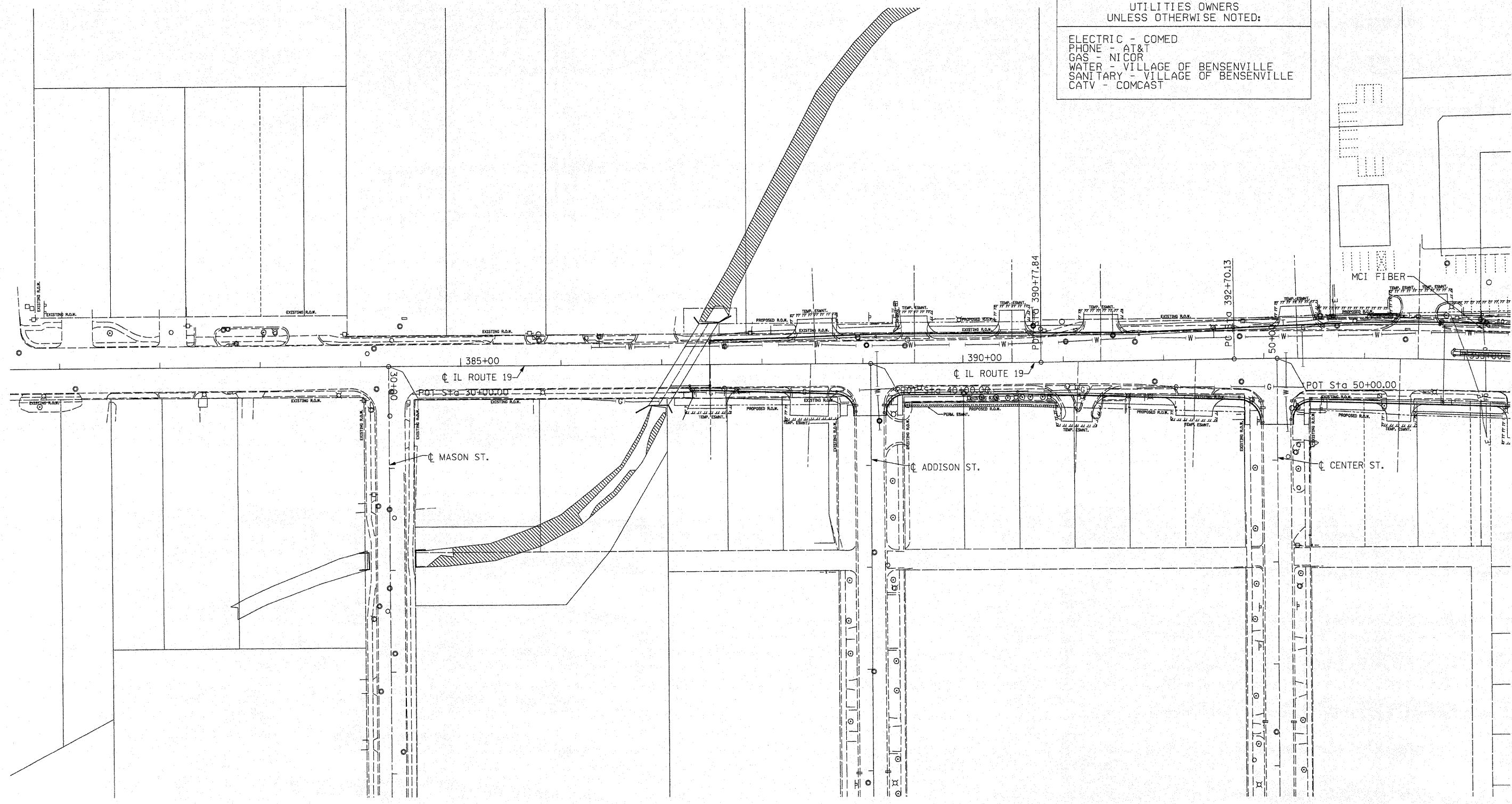
**IL RT.19 at York Road  
Bensenville, IL**

| F.A. RTE.                                     | SECTION    | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|------------|--------|--------------|-----------|
| 1321  | 2010-134-T | Cook   | 40           | 16        |
| Contract No. 60N06                            |            |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS IDOT Project No. |            |        |              |           |





UTILITIES OWNERS  
UNLESS OTHERWISE NOTED:  
ELECTRIC - COMED  
PHONE - AT&T  
GAS - NICOR  
WATER - VILLAGE OF BENSENVILLE  
SANITARY - VILLAGE OF BENSENVILLE  
CATV - COMCAST

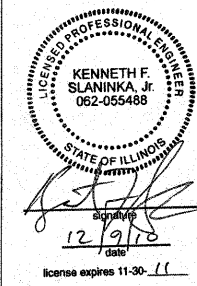


MATCH SHEET 2

- A — A — AERIAL UTILITY
- - - - - UNKNOWN
- CTV — CTV — CABLE TV
- T — T — TELEPHONE
- G — G — GAS
- E — E — ELECTRIC
- W — W — WATER
- FO — FO — FIBER OPTIC
- S — S — SEWER
- ⊕ TBE TEST HOLE

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ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



TBE Job No. IL09510417  
SUE Plan Page: 1 of 7

Utility Quality Level "A" : Test Hole  
Utility Quality Level "B" : Designating  
Utility Quality Level "C" : Research with Survey  
Utility Quality Level "D" : Records Research

|          |          |         |
|----------|----------|---------|
| DESIGNED | EG       | REVISED |
| DRAWN    | KLC      | REVISED |
| CHECKED  | KPS      | REVISED |
| DATE     | 12/06/10 | REVISED |

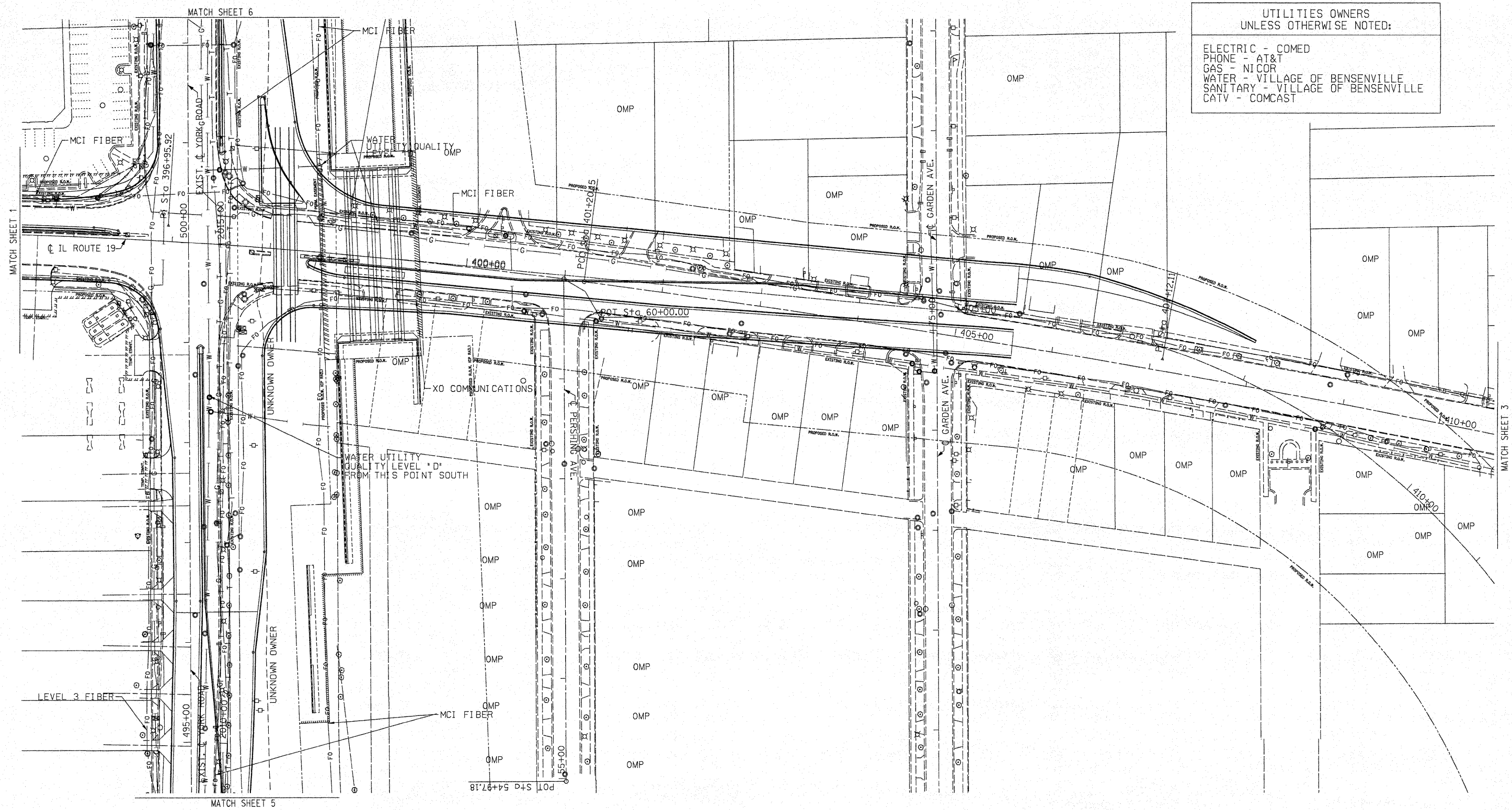
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL RT.19 at York Road  
Bensenville, IL

| F.A. RTE.                                       | SECTION    | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---|------------|--------|--------------------|-----------|
| 1321  | 2010-134-T | Cook   | 40                 | 17        |
| FED. ROAD DIST. NO. [ILLINOIS] IDOT Project No. |            |        | Contract No. 60N06 |           |

UTILITIES OWNERS  
UNLESS OTHERWISE NOTED:

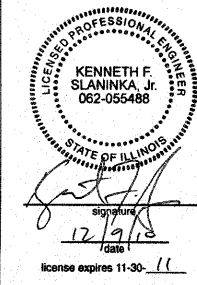
ELECTRIC - COMED  
PHONE - AT&T  
GAS - NICOR  
WATER - VILLAGE OF BENSENVILLE  
SANITARY - VILLAGE OF BENSENVILLE  
CATV - COMCAST



- A — A — AERIAL UTILITY
- - - - - UNKNOWN
- CTV — CTV — CABLE TV
- T — T — TELEPHONE
- G — G — GAS
- E — E — ELECTRIC
- W — W — WATER
- FO — FO — FIBER OPTIC
- S — S — SEWER
- ⊙ TBE TEST HOLE

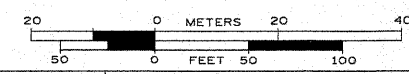
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CIVIL ENGINEERING \* TRANSPORTATION \* ENVIRONMENTAL \* PLANNING \* UTILITY ENGINEERING/LOCATING

TBE Job No. IL09510417  
SUE Plan Page: 2 of 7



Utility Quality Level "A" : Test Hole  
Utility Quality Level "B" : Designating  
Utility Quality Level "C" : Research with Survey  
Utility Quality Level "D" : Records Research

|                      |         |
|----------------------|---------|
| DESIGNED <i>EG</i>   | REVISED |
| DRAWN <i>KLC</i>     | REVISED |
| CHECKED <i>KPS</i>   | REVISED |
| DATE <i>12/06/10</i> | REVISED |

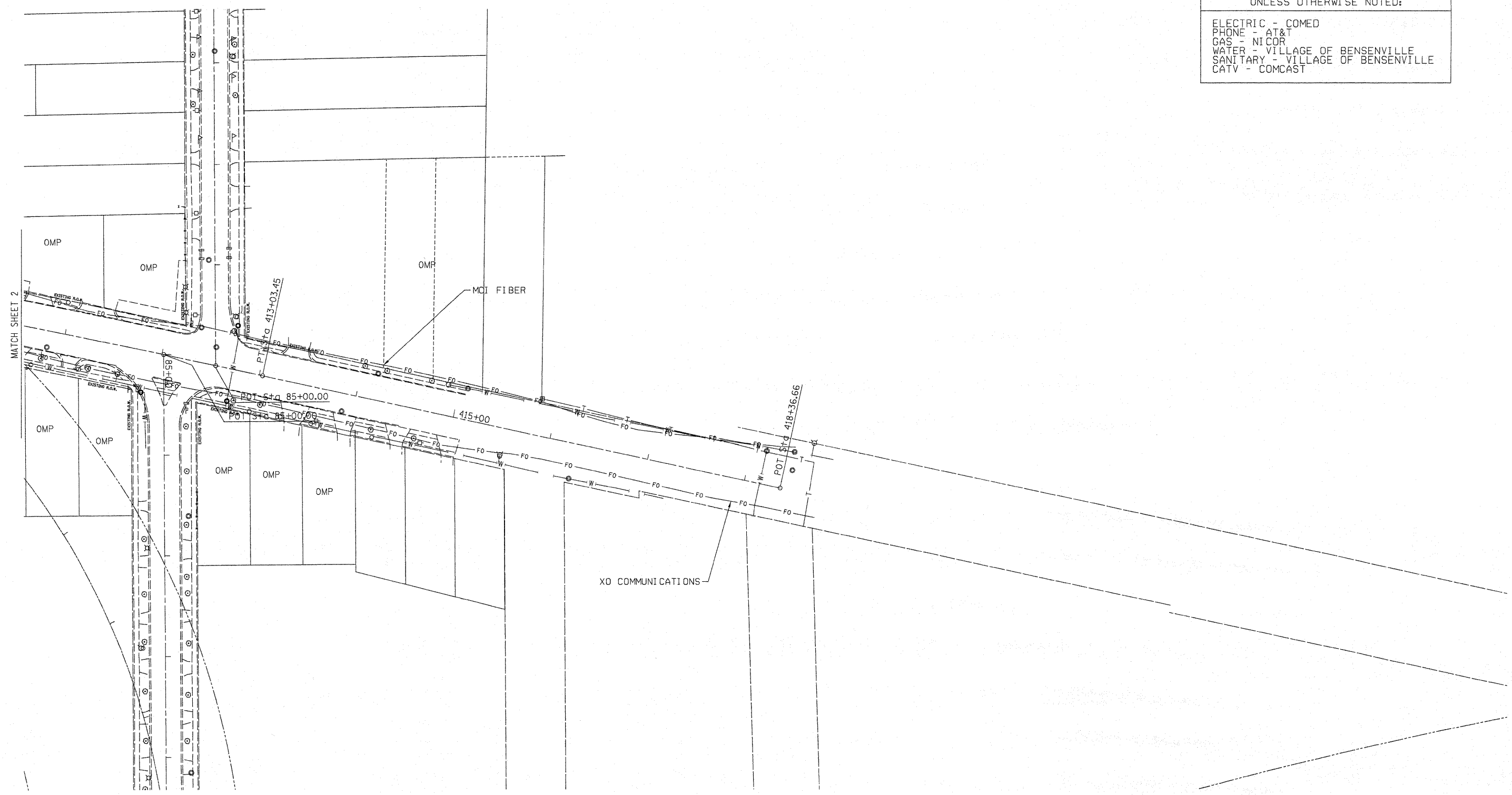
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL RT.19 at York Road  
Bensenville, IL

| F.A. RTE.           | SECTION    | COUNTY                    | TOTAL SHEETS | SHEET NO. |
|---------------------|------------|---------------------------|--------------|-----------|
| 1321                | 2010-134-T | Cook                      | 40           | 18        |
| FED. ROAD DIST. NO. |            | ILLINOIS IDOT Project No. |              |           |

UTILITIES OWNERS  
UNLESS OTHERWISE NOTED:

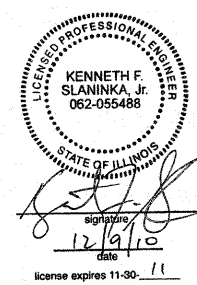
ELECTRIC - COMED  
PHONE - AT&T  
GAS - NICOR  
WATER - VILLAGE OF BENSENVILLE  
SANITARY - VILLAGE OF BENSENVILLE  
CATV - COMCAST



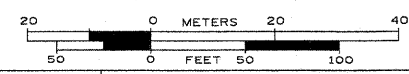
- A — A — AERIAL UTILITY
- - - - - UNKNOWN
- CTV — CTV — CABLE TV
- T — T — TELEPHONE
- G — G — GAS
- E — E — ELECTRIC
- W — W — WATER
- FO — FO — FIBER OPTIC
- S — S — SEWER
- ⊙ — ⊙ — TBE TEST HOLE

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ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



TBE Job No. IL09510417  
SUE Plan Page: 3 of 7



Utility Quality Level "A" : Test Hole  
Utility Quality Level "B" : Designating  
Utility Quality Level "C" : Research with Survey  
Utility Quality Level "D" : Records Research

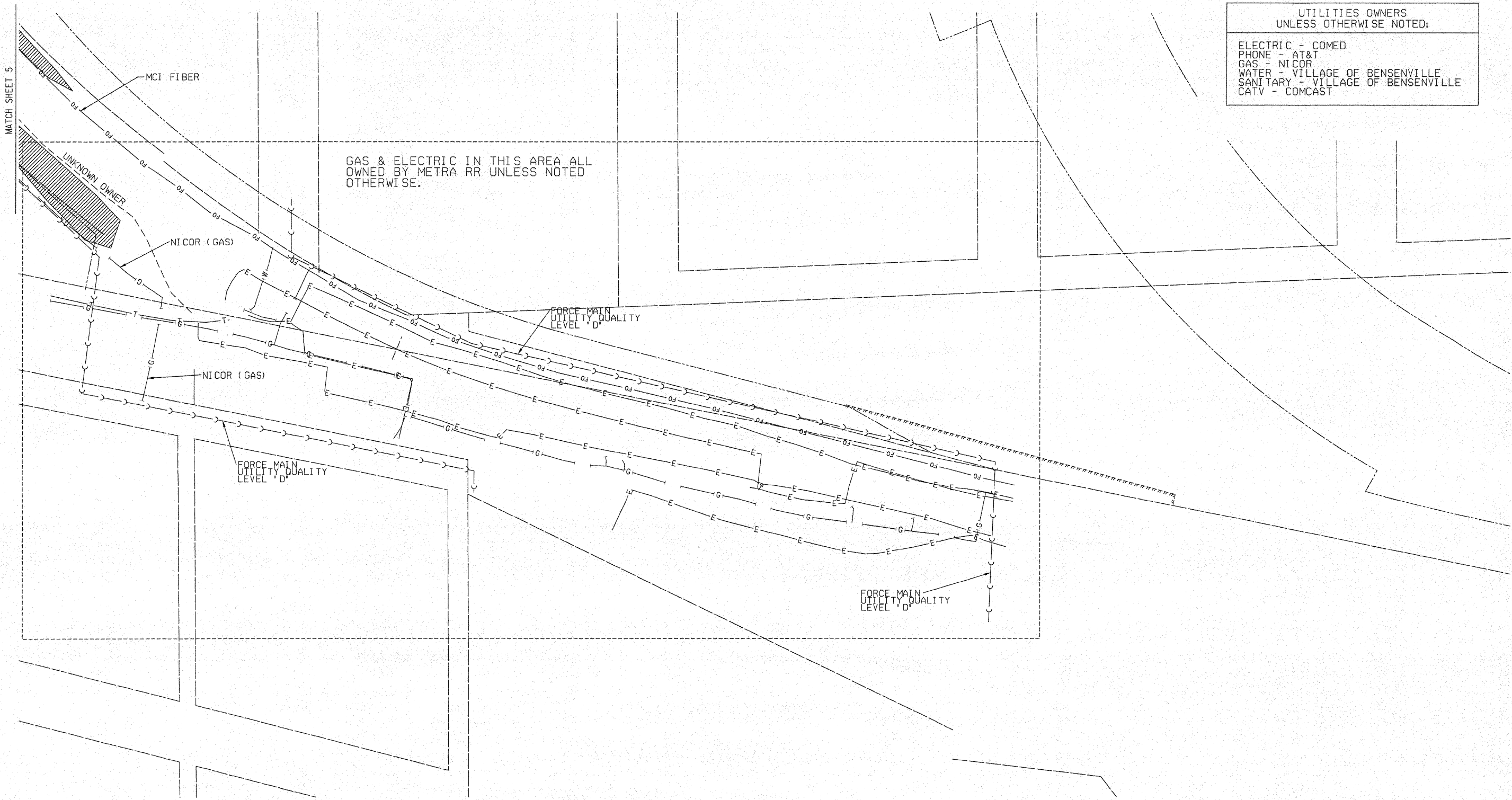
|          |          |         |
|----------|----------|---------|
| DESIGNED | EG       | REVISED |
| DRAWN    | KLC      | REVISED |
| CHECKED  | KP       | REVISED |
| DATE     | 12/06/10 | REVISED |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL RT.19 at York Road  
Bensenville, IL

| F.A. RTE.                                     | SECTION    | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---|------------|--------|--------------------|-----------|
| 1321  | 2010-134-T | Cook   | 40                 | 19        |
| FED. ROAD DIST. NO. ILLINOIS IDOT Project No. |            |        | Contract No. 60N06 |           |

UTILITIES OWNERS  
UNLESS OTHERWISE NOTED:  
ELECTRIC - COMED  
PHONE - AT&T  
GAS - NICOR  
WATER - VILLAGE OF BENSENVILLE  
SANITARY - VILLAGE OF BENSENVILLE  
CATV - COMCAST

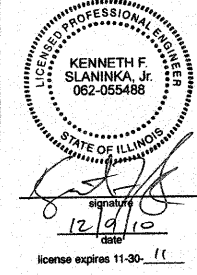
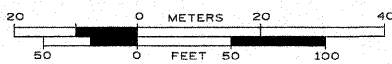


GAS & ELECTRIC IN THIS AREA ALL OWNED BY METRA RR UNLESS NOTED OTHERWISE.

- A — A — AERIAL UTILITY
- - - - - UNKNOWN
- CTV — CTV — CABLE TV
- T — T — TELEPHONE
- G — G — GAS
- E — E — ELECTRIC
- W — W — WATER
- FO — FO — FIBER OPTIC
- - - - - SEWER
- ⊙ TBE TEST HOLE

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ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



Utility Quality Level "A" : Test Hole  
Utility Quality Level "B" : Designating  
Utility Quality Level "C" : Research with Survey  
Utility Quality Level "D" : Records Research

|                      |         |
|----------------------|---------|
| DESIGNED <i>EG</i>   | REVISED |
| DRAWN <i>KLC</i>     | REVISED |
| CHECKED <i>KFS</i>   | REVISED |
| DATE <i>12/06/10</i> | REVISED |

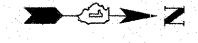
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL RT.19 at York Road  
Bensenville, IL

| F.A. RTE.                                     | SECTION    | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|------------|--------|--------------|-----------|
| 1321  | 2010-134-T | Cook   | 40           | 20        |
| Contract No. 60B42                            |            |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS IDOT Project No. |            |        |              |           |

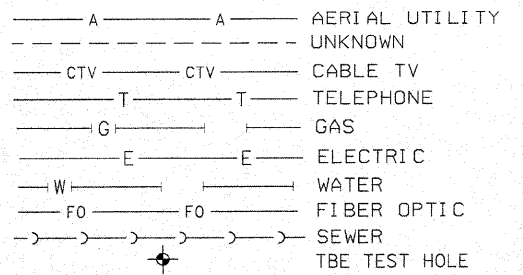
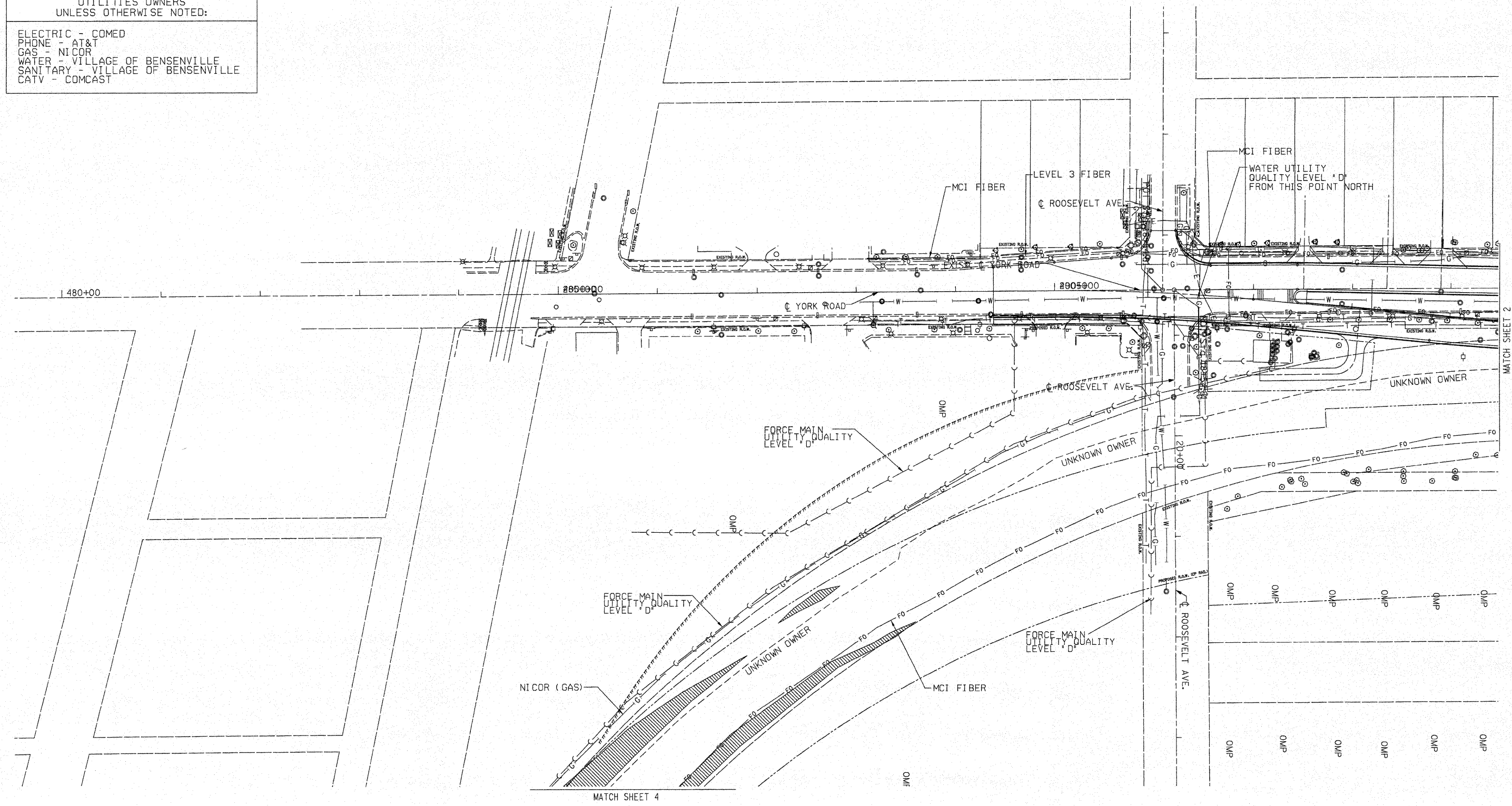
TBE Job No. IL09510417  
SUE Plan Page: 4 of 7





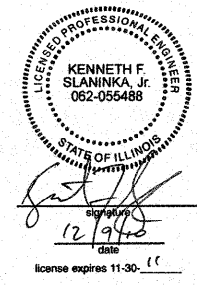
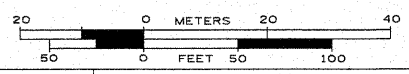
UTILITIES OWNERS  
UNLESS OTHERWISE NOTED:

ELECTRIC - COMED  
PHONE - AT&T  
GAS - NICOR  
WATER - VILLAGE OF BENSENVILLE  
SANITARY - VILLAGE OF BENSENVILLE  
CATV - COMCAST



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ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



TBE Job No. 1L09510417  
SUE Plan Page: 5 of 7

Utility Quality Level "A" : Test Hole  
Utility Quality Level "B" : Designating  
Utility Quality Level "C" : Research with Survey  
Utility Quality Level "D" : Records Research

|          |          |         |
|----------|----------|---------|
| DESIGNED | EG       | REVISED |
| DRAWN    | KLC      | REVISED |
| CHECKED  | KFS      | REVISED |
| DATE     | 12/06/10 | REVISED |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

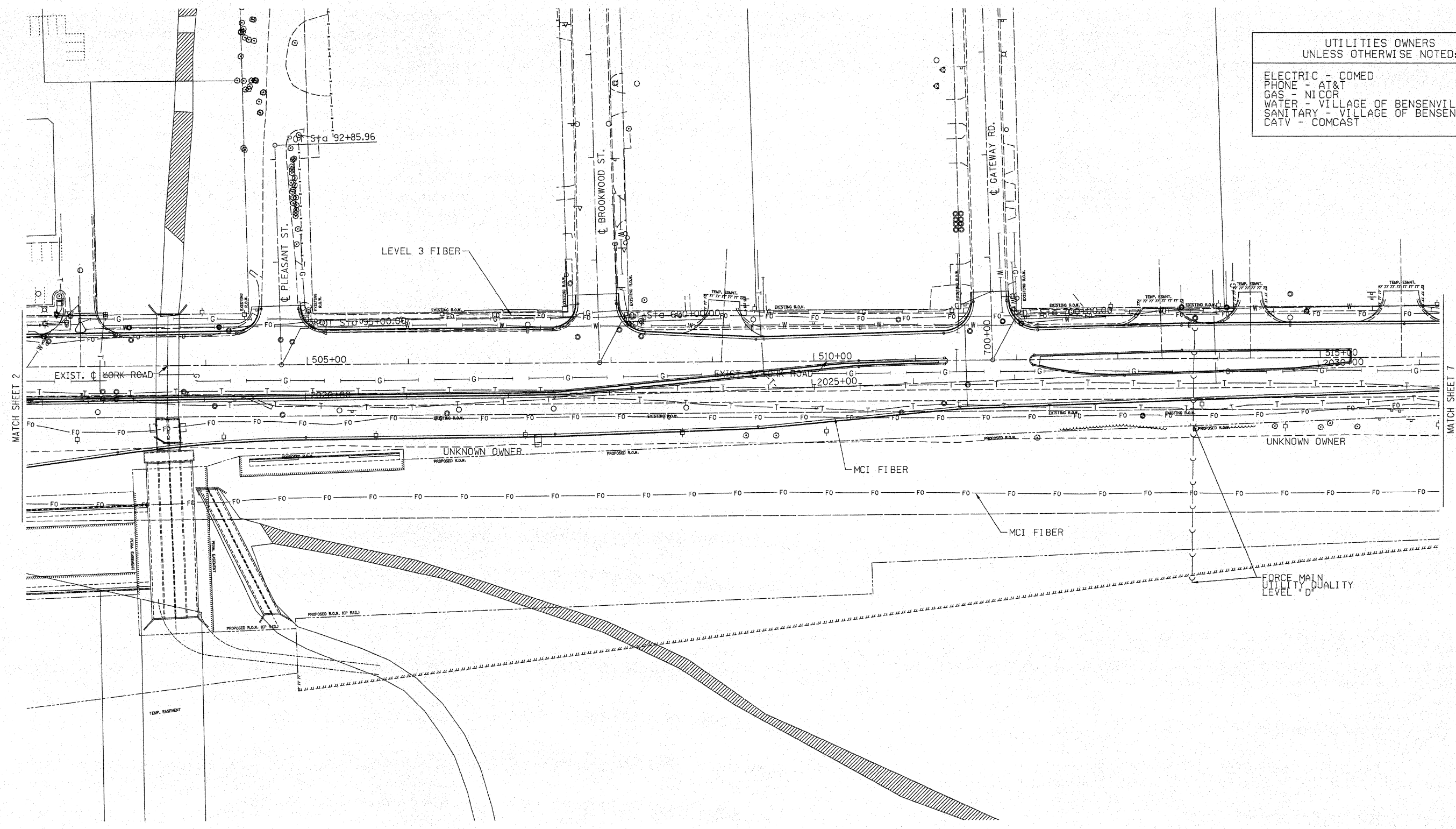
IL RT.19 at York Road  
Bensenville, IL

| F.A. RTE.           | SECTION    | COUNTY                    | TOTAL SHEETS       | SHEET NO. |
|---------------------|------------|---------------------------|--------------------|-----------|
| 1321                | 2010-134-T | Cook                      | 40                 | 21        |
| FED. ROAD DIST. NO. |            | ILLINOIS IDOT Project No. | Contract No. 60N06 |           |



UTILITIES OWNERS  
UNLESS OTHERWISE NOTED:

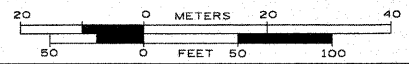
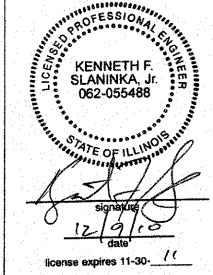
ELECTRIC - COMED  
PHONE - AT&T  
GAS - NICOR  
WATER - VILLAGE OF BENSENVILLE  
SANITARY - VILLAGE OF BENSENVILLE  
CATV - COMCAST



- A — A — AERIAL UTILITY
- - - - - UNKNOWN
- CTV — CTV — CABLE TV
- T — T — TELEPHONE
- G — G — GAS
- E — E — ELECTRIC
- W — W — WATER
- FO — FO — FIBER OPTIC
- S — S — SEWER
- (•) — (•) — TBE TEST HOLE

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ALL UTILITIES SHOWN QUALITY LEVEL "B"  
UNLESS NOTED OTHERWISE.



Utility Quality Level "A" : Test Hole  
Utility Quality Level "B" : Designating  
Utility Quality Level "C" : Research with Survey  
Utility Quality Level "D" : Records Research

|          |          |         |  |
|----------|----------|---------|--|
| DESIGNED | EG       | REVISED |  |
| DRAWN    | KLC      | REVISED |  |
| CHECKED  | KFS      | REVISED |  |
| DATE     | 12/06/10 | REVISED |  |

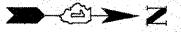
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL RT.19 at York Road  
Bensenville, IL

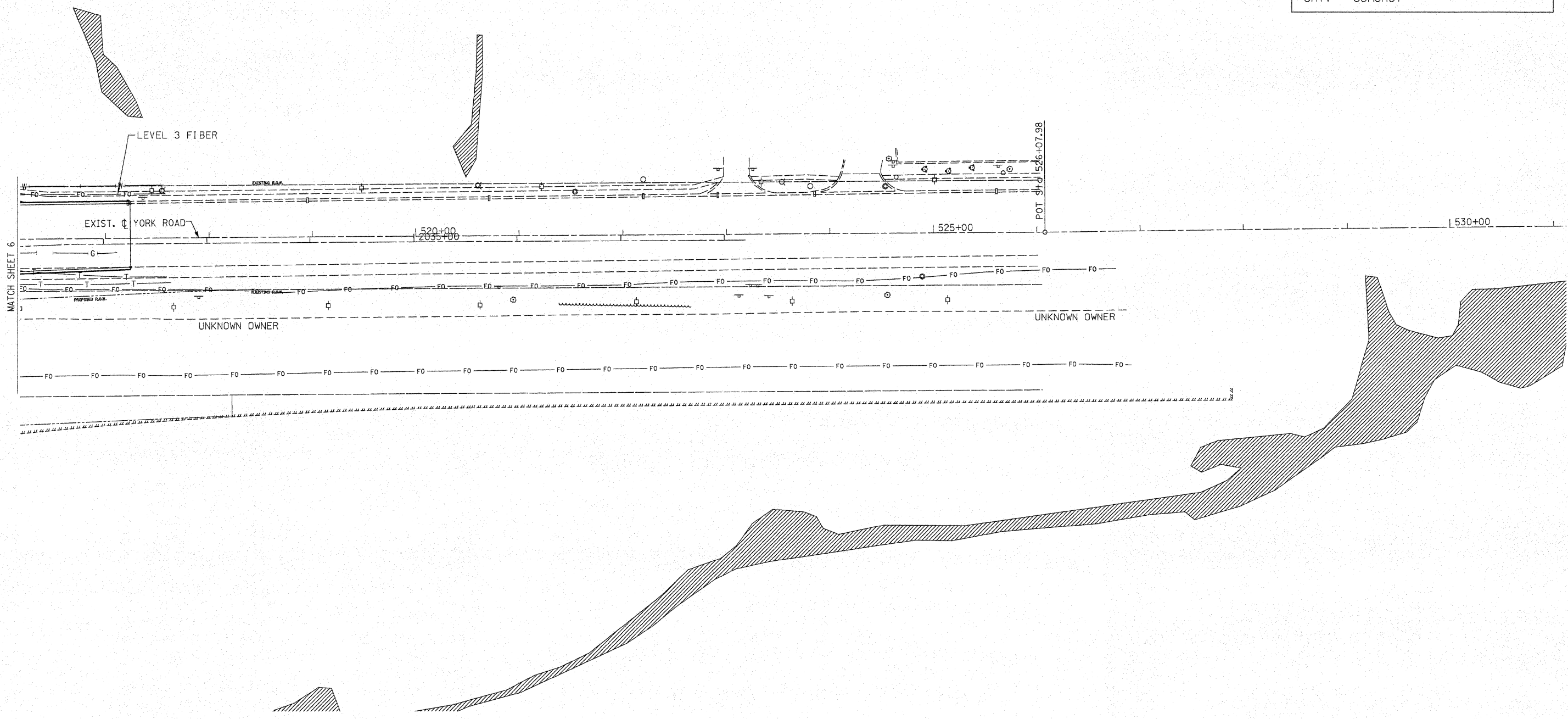
|   |            |        |                    |           |
|---|------------|--------|--------------------|-----------|
| F.A. RTE.                                       | SECTION    | COUNTY | TOTAL SHEETS       | SHEET NO. |
| 1321  | 2010-134-T | Cook   | 40                 | 22        |
| FED. ROAD DIST. NO. [ILLINOIS] 100T Project No. |            |        | Contract No. 60N06 |           |

TBE Job No. IL09510417  
SUE Plan Page: 6 of 7





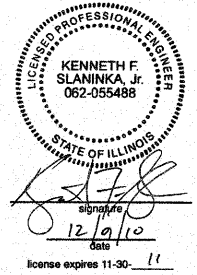
| UTILITIES OWNERS<br>UNLESS OTHERWISE NOTED: |                          |
|---|--------------------------|
| ELECTRIC                                    | - COMED                  |
| PHONE                                       | - AT&T                   |
| GAS   | - NICOR                  |
| WATER                                       | - VILLAGE OF BENSENVILLE |
| SANITARY                                    | - VILLAGE OF BENSENVILLE |
| CATV  | - COMCAST                |



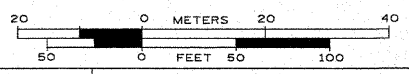
|               |                |
|---------------|----------------|
| — A — A —     | AERIAL UTILITY |
| - - - - -     | UNKNOWN        |
| — CTV — CTV — | CABLE TV       |
| — T — T —     | TELEPHONE      |
| — G — G —     | GAS            |
| — E — E —     | ELECTRIC       |
| — W — W —     | WATER          |
| — FO — FO —   | FIBER OPTIC    |
| — S — S —     | SEWER          |
| ⊕             | TBE TEST HOLE  |

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ALL UTILITIES SHOWN QUALITY LEVEL "B"  
UNLESS NOTED OTHERWISE.



TBE Job No. IL09510417  
SUE Plan Page: 7 of 7



|  |                      |         |
|--|----------------------|---------|
| Utility Quality Level "A" : Test Hole            | DESIGNED <i>FG</i>   | REVISED |
| Utility Quality Level "B" : Designating          | DRAWN <i>KLC</i>     | REVISED |
| Utility Quality Level "C" : Research with Survey | CHECKED <i>KPS</i>   | REVISED |
| Utility Quality Level "D" : Records Research     | DATE <i>12/06/10</i> | REVISED |

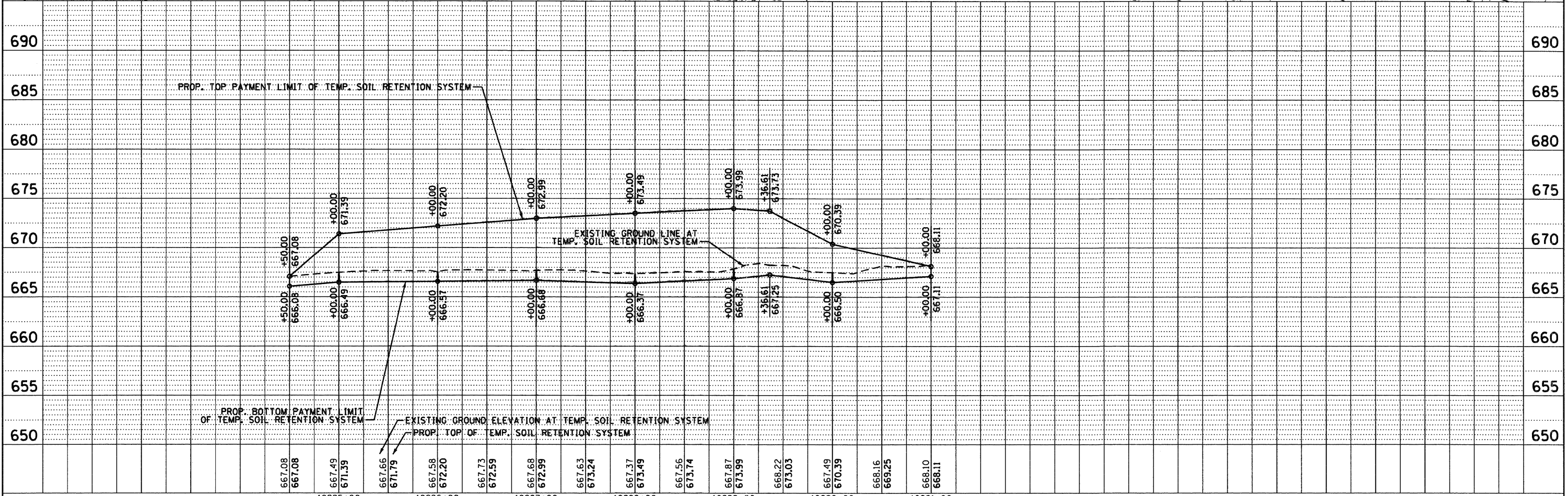
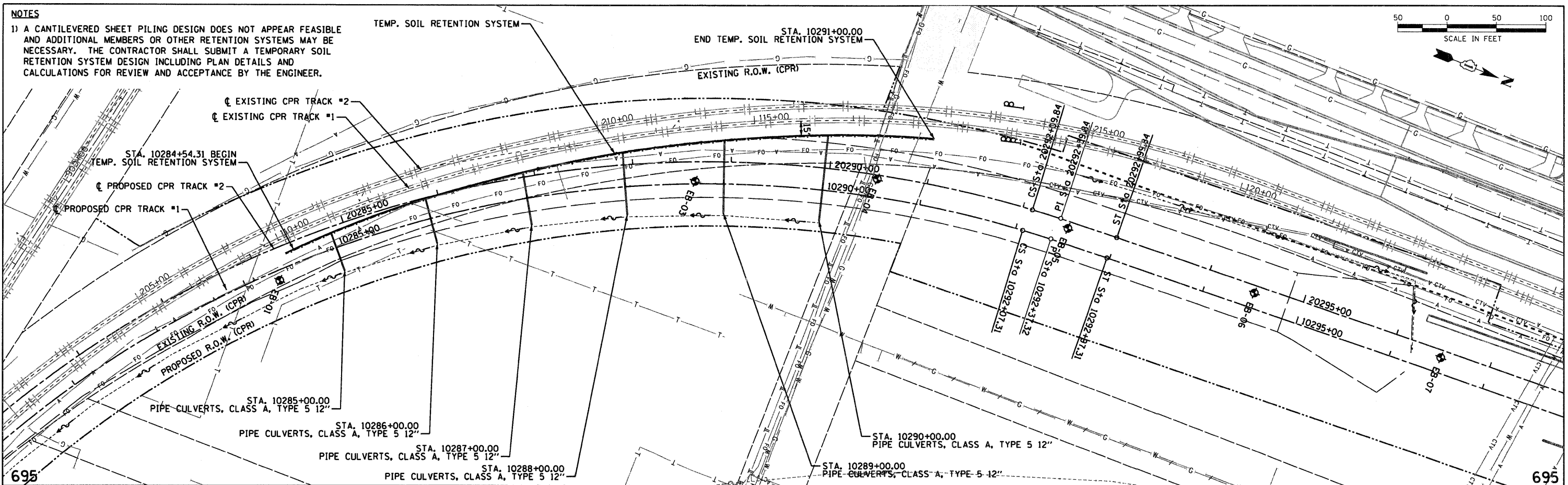
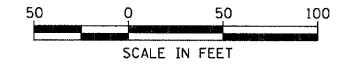
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL RT.19 at York Road  
Bensenville, IL

| F.A. RTE.                                       | SECTION    | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---|------------|--------|--------------------|-----------|
| 1321  | 2010-134-T | Cook   | 40                 | 23        |
| FED. ROAD DIST. NO. [ILLINOIS] IDOT Project No. |            |        | Contract No. 60N06 |           |

**NOTES**

1) A CANTILEVERED SHEET PILING DESIGN DOES NOT APPEAR FEASIBLE AND ADDITIONAL MEMBERS OR OTHER RETENTION SYSTEMS MAY BE NECESSARY. THE CONTRACTOR SHALL SUBMIT A TEMPORARY SOIL RETENTION SYSTEM DESIGN INCLUDING PLAN DETAILS AND CALCULATIONS FOR REVIEW AND ACCEPTANCE BY THE ENGINEER.



|      |    |
|------|----|
| DATE | BY |
|      |    |
| DATE | BY |
|      |    |
| DATE | BY |
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|      |    |
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| DATE | BY |
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| DATE | BY |
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| DATE | BY |
|      |    |

|                                   |                     |                   |           |  |  |                         |                                |    |                           |         |        |              |           |
|-----------------------------------|---------------------|-------------------|-----------|--|--|-------------------------|--------------------------------|----|---------------------------|---------|--------|--------------|-----------|
| FILE NAME =                       | USER NAME = nratarm | DESIGNED - NMR    | REVISED - | <p align="center"><b>STATE OF ILLINOIS</b><br/><b>DEPARTMENT OF TRANSPORTATION</b></p> | <p align="center"><b>TEMPORARY SOIL RETENTION SYSTEM</b><br/><b>CANADIAN PACIFIC RAILWAY</b></p> |                         |                                |    | F.A.U. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ...\\D160N06-SHT-DETAILS-RR01.DGN |                     | DRAWN - NMR       | REVISED - |  | 1321   | 2010-134-T              | DUPAGE                         | 40 | 24                        |         |        |              |           |
| PLOT SCALE = 50.0000' / IN.       |                     | CHECKED - LGP     | REVISED - |  | <p align="center">CONTRACT NO. 60N06</p>   |                         |                                |    | ILLINOIS FED. AID PROJECT |         |        |              |           |
| PLOT DATE = 3/25/2011             |                     | DATE - 03/17/2011 | REVISED - |  | SCALE: 1" = 50'  | SHEET NO. 1 OF 1 SHEETS | STA. 10282+00 TO STA. 10297+00 |    |                           |         |        |              |           |

Geo Services Inc. SOIL BORING LOG PAGE 1 of 1  
 805 Arbest-Curtis Dr. Naperville, IL 60565 (630) 353-2858 DATE 4/26/2010  
 LOGGED BY DR GSI JOB No. 09165

ROUTE IL RTE 19 DESCRIPTION Irving Park Rd, York Rd, CNRR Grade Separation Pri D-91-332-06  
 SECTION 32 WRS-5 LOCATION Addison Township T40 N, R11 E, NW 1/4 Section 13  
 COUNTY DuPage DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO. - Station -  
 BORING NO. **EB-01** Station: 20284+17  
 Offset: 31.5' Right  
 Ground Surface Elev. 665.1

| DEPTH (ft) | BULGE (in) | UCS (tsf) | MOIST (%) | Surface Water Elev. | Stream Bed Elev. | DEPTH (ft) | BULGE (in) | UCS (tsf) | MOIST (%) |
|------------|------------|-----------|-----------|---------------------|------------------|------------|------------|-----------|-----------|
| 0          |            |           |           | n/a                 | n/a              | 0          |            |           |           |
| 2          |            |           |           |                     |                  | 2          |            |           |           |
| 3          |            |           |           |                     |                  | 3          |            |           |           |
| 3          | NP         | 17        |           |                     |                  | 3          | NP         | 17        |           |
| 2          |            |           |           |                     |                  | 2          |            |           |           |
| 3          |            |           |           |                     |                  | 3          |            |           |           |
| 4          | NP         | 19        |           |                     |                  | 4          | NP         | 19        |           |
| 659.1      |            |           |           |                     |                  | 659.1      |            |           |           |
| 2          |            |           | 110       |                     |                  | 2          |            |           | 110       |
| 4          |            | 1.6B      | 20        |                     |                  | 4          |            | 1.6B      | 20        |
| 5          |            |           |           |                     |                  | 5          |            |           |           |
| 4          |            |           | 100       |                     |                  | 4          |            |           | 100       |
| 9          |            |           |           |                     |                  | 9          |            |           |           |
| 8          |            | 1.8B      | 20        |                     |                  | 8          |            | 1.8B      | 20        |
| 654.6      |            |           |           |                     |                  | 654.6      |            |           |           |
| 6          |            |           |           |                     |                  | 6          |            |           |           |
| 7          |            |           |           |                     |                  | 7          |            |           |           |
| 9          |            | 3.75P     | 19        |                     |                  | 9          |            | 3.75P     | 19        |
| 5          |            |           |           |                     |                  | 5          |            |           |           |
| 7          |            |           |           |                     |                  | 7          |            |           |           |
| 10         |            | 1.5P      | 16        |                     |                  | 10         |            | 1.5P      | 16        |
| 649.6      |            |           |           |                     |                  | 649.6      |            |           |           |
| 2          |            |           |           |                     |                  | 2          |            |           |           |
| 4          |            |           |           |                     |                  | 4          |            |           |           |
| 5          | NP         | 18        |           |                     |                  | 5          | NP         | 18        |           |
| 647.1      |            |           |           |                     |                  | 647.1      |            |           |           |
| 3          |            |           |           |                     |                  | 3          |            |           |           |
| 4          |            |           |           |                     |                  | 4          |            |           |           |
| 645.1-20   |            | 2.0P      | 22        |                     |                  | 645.1-20   |            | 2.0P      | 22        |

3.0" ASPHALT, 5.0" SAND & GRAVEL  
 SANDY LOAM to LOAM-brown-loose (A-2/A-4)  
 CLAY-gray-stiff (A-6)  
 SILTY CLAY-gray-stiff to very stiff (A-4/A-6)  
 SAND-gray-loose (A-3)  
 CLAY-gray-very stiff (A-6)  
 End Of Boring @ -20.0'  
 Hollow Stem Augers  
 CME Automatic Hammer

NOTE: EB-02 DOES NOT EXIST.

Geo Services Inc. SOIL BORING LOG PAGE 1 of 1  
 805 Arbest-Curtis Dr. Naperville, IL 60565 (630) 353-2858 DATE 4/26/2010  
 LOGGED BY DR GSI JOB No. 09165

ROUTE IL RTE 19 DESCRIPTION Irving Park Rd, York Rd, CNRR Grade Separation Pri D-91-332-06  
 SECTION 32 WRS-5 LOCATION Addison Township T40 N, R11 E, NW 1/4 Section 13  
 COUNTY DuPage DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO. - Station -  
 BORING NO. **EB-03** Station: 20288+62  
 Offset: 19.5' Right  
 Ground Surface Elev. 664.7

| DEPTH (ft) | BULGE (in) | UCS (tsf) | MOIST (%) | Surface Water Elev. | Stream Bed Elev. | DEPTH (ft) | BULGE (in) | UCS (tsf) | MOIST (%) |
|------------|------------|-----------|-----------|---------------------|------------------|------------|------------|-----------|-----------|
| 0          |            |           |           | n/a                 | n/a              | 0          |            |           |           |
| 3          |            |           |           |                     |                  | 3          |            |           |           |
| 4          | 4.5+P      | 17        |           |                     |                  | 4          | 4.5+P      | 17        |           |
| 5          |            |           | 111       |                     |                  | 5          |            |           | 111       |
| 6          |            |           |           |                     |                  | 6          |            |           |           |
| 7          |            | 4.75B     | 18        |                     |                  | 7          |            | 4.75B     | 18        |
| 654.2      |            |           |           |                     |                  | 654.2      |            |           |           |
| 5          |            |           |           |                     |                  | 5          |            |           |           |
| 8          |            |           |           |                     |                  | 8          |            |           |           |
| 10         |            | 3.75P     | 20        |                     |                  | 10         |            | 3.75P     | 20        |
| 5          |            |           |           |                     |                  | 5          |            |           |           |
| 7          |            |           |           |                     |                  | 7          |            |           |           |
| 8          |            | 3.0P      | 20        |                     |                  | 8          |            | 3.0P      | 20        |
| 654.2      |            |           |           |                     |                  | 654.2      |            |           |           |
| 4          |            |           | 113       |                     |                  | 4          |            |           | 113       |
| 6          |            |           |           |                     |                  | 6          |            |           |           |
| 8          |            | 2.5B      | 17        |                     |                  | 8          |            | 2.5B      | 17        |
| 2          |            |           |           |                     |                  | 2          |            |           |           |
| 2          |            |           |           |                     |                  | 2          |            |           |           |
| 3          | NP         | 24        |           |                     |                  | 3          | NP         | 24        |           |
| 649.2      |            |           |           |                     |                  | 649.2      |            |           |           |
| 2          |            |           |           |                     |                  | 2          |            |           |           |
| 5          |            |           |           |                     |                  | 5          |            |           |           |
| 6          |            |           | 14        |                     |                  | 6          |            |           | 14        |
| 646.7      |            |           |           |                     |                  | 646.7      |            |           |           |
| 2          |            |           |           |                     |                  | 2          |            |           |           |
| 4          |            |           | 112       |                     |                  | 4          |            |           | 112       |
| 644.7-20   |            | 2.1B      | 18        |                     |                  | 644.7-20   |            | 2.1B      | 18        |

3.0" ASPHALT, 7.0" SAND & GRAVEL  
 CLAY-brown & gray-very stiff to hard (A-6)  
 CLAY-gray-very stiff (A-6)  
 SAND-gray-loose (A-3)  
 SANDY CLAY LOAM-gray-medium dense (A-2)  
 CLAY LOAM-gray-very stiff (A-6)  
 End Of Boring @ -20.0'  
 Hollow Stem Augers  
 CME Automatic Hammer

|  |                      |               |           |   |                                    |                         |                    |               |                 |              |  |  |
|--|----------------------|---------------|-----------|---|------------------------------------|-------------------------|--------------------|---------------|-----------------|--------------|--|--|
| FILE NAME = ...\\D16086-SHT-BORINGS-RR01.dgn | USER NAME = cratterm | DESIGNED - DP | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>EMBANKMENT SOIL BORING LOGS</b> | F.A.U RTE. 1321         | SECTION 2010-134-T | COUNTY DUPAGE | TOTAL SHEETS 40 | SHEET NO. 25 |  |  |
| PLOT SCALE = 1,000' / IN.                    | DRAWN - JM           | CHECKED - NMR | REVISED - |   |                                    | CONTRACT NO. 60N06      |                    |               |                 |              |  |  |
| PLOT DATE = 3/25/2011                        | DATE - 03/17/2011    | REVISED -     | SCALE:    |   |                                    | SHEET NO. 1 OF 3 SHEETS |                    | STA.          | TO STA.         |              |  |  |
| ILLINOIS FED. AID PROJECT                    |                      |               |           |   |                                    |                         |                    |               |                 |              |  |  |



Geo Services Inc.  
Geotechnical, Environmental & Civil Engineering  
805 Anthony Court, Suite 204  
Naperville, Illinois 60565  
(630) 353-7308

**SOIL BORING LOG**

PAGE 1 of 1  
DATE 4/27/2010  
LOGGED BY DR  
GSI JOB No. 09165

ROUTE IL RTE 19 DESCRIPTION Irving Park Rd, York Rd, CNRR Grade Separation Pri. D-91-332-06  
SECTION 32 WRS-5 LOCATION Addison Township T40 N, R11 E, NW 1/4 Section 13  
COUNTY DuPage DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO. --  
Station --  
BORING NO. **EB-07**  
Station: 20296+50  
Offset: 7.5' Right  
Ground Surface Elev. 665.1

Surface Water Elev. n/a  
Stream Bed Elev. n/a  
Groundwater Elevation:  
First Encounter 656.6  
Upon Completion 652.1  
After Hrs.

| DEPTH<br>H<br>S                                | B<br>L<br>O<br>W<br>S | U<br>C<br>S<br>Qu | M<br>O<br>I<br>S<br>T | DEPTH<br>H<br>S | B<br>L<br>O<br>W<br>S | U<br>C<br>S<br>Qu | M<br>O<br>I<br>S<br>T |
|--|-----------------------|-------------------|-----------------------|-----------------|-----------------------|-------------------|-----------------------|
|  |                       |                   |                       |                 |                       |                   |                       |
| 12.0" TOPSOIL-black                            |                       |                   |                       |                 |                       |                   |                       |
|  |                       | AS                | 30                    |                 |                       |                   |                       |
|  | 2                     |                   | 94                    |                 |                       |                   |                       |
|  | 3                     |                   |                       |                 |                       |                   |                       |
|  | 3                     | 1.5B              | 16                    |                 |                       |                   |                       |
| SILTY CLAY-brown & gray<br>stiff to hard (A-6) |                       |                   |                       |                 |                       |                   |                       |
|  | 3                     |                   |                       |                 |                       |                   |                       |
|  | 4                     |                   |                       |                 |                       |                   |                       |
|  | -5                    | 4                 | 4.25P                 | 13              |                       |                   | -25                   |
|  | 3                     |                   | 111                   |                 |                       |                   |                       |
|  | 4                     |                   |                       |                 |                       |                   |                       |
|  | 6                     | 3.8B              | 15                    |                 |                       |                   |                       |
|  |                       |                   |                       |                 |                       |                   |                       |
|  | 5                     |                   | 115                   |                 |                       |                   |                       |
|  | 7                     |                   |                       |                 |                       |                   |                       |
|  | -10                   | 10                | 4.5B                  | 13              |                       |                   | -30                   |
|  | 4                     |                   | 114                   |                 |                       |                   |                       |
| CLAY-gray-stiff to hard (A-6)                  |                       |                   |                       |                 |                       |                   |                       |
|  | 5                     |                   |                       |                 |                       |                   |                       |
|  | 9                     | 2.4B              | 15                    |                 |                       |                   |                       |
|  | 3                     |                   |                       |                 |                       |                   |                       |
|  | 4                     |                   |                       |                 |                       |                   |                       |
|  | -15                   | 7                 | -                     | 21              |                       |                   | -35                   |
|  | 2                     |                   | 111                   |                 |                       |                   |                       |
|  | 3                     |                   |                       |                 |                       |                   |                       |
|  | 5                     | 1.25P             | 16                    |                 |                       |                   |                       |
|  |                       |                   |                       |                 |                       |                   |                       |
| SAND-gray-loose (A-3)                          |                       |                   |                       |                 |                       |                   |                       |
|  | 4                     |                   |                       |                 |                       |                   |                       |
|  | 4                     |                   |                       |                 |                       |                   |                       |
|  | 5                     | NP                | 16                    |                 |                       |                   | -40                   |

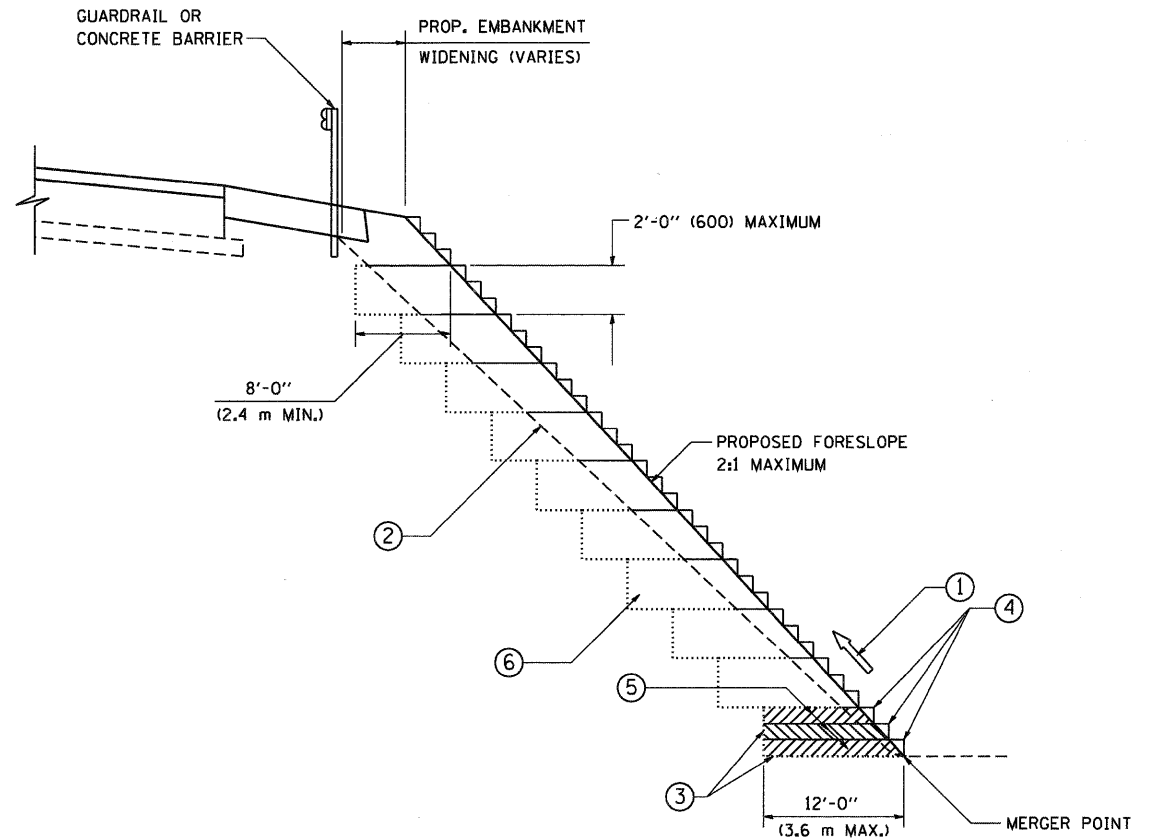
End Of Boring @ -20.0'  
Hollow Stem Augers  
CME Automatic Hammer

647.1

645.1 -20

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) ST-Sheby Tube Sample VS-Vane Shear Test  
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%)  
NR-No Recovery

|                                  |                     |                   |           |   |                             |                    |                         |        |              |           |  |
|----------------------------------|---------------------|-------------------|-----------|---|-----------------------------|--------------------|-------------------------|--------|--------------|-----------|--|
| FILE NAME =                      | USER NAME = rwaterm | DESIGNED - DP     | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION | EMBANKMENT SOIL BORING LOGS | F.A.U. RTE.        | SECTION                 | COUNTY | TOTAL SHEETS | SHEET NO. |  |
| ...ND160N06-SHT-BORINGS-RR03.dgn |                     | DRAWN - JM        | REVISED - |   |                             | 1321               | 2010-134-T              | DUPAGE | 40           | 27        |  |
| PLOT SCALE = 1.0000' / IN.       |                     | CHECKED - NMR     | REVISED - |   |                             | CONTRACT NO. 60N06 |                         |        |              |           |  |
| PLOT DATE = 3/25/2011            |                     | DATE - 03/17/2011 | REVISED - |   |                             | SCALE:             | SHEET NO. 3 OF 3 SHEETS | STA.   | TO STA.      |           |  |
| ILLINOIS FED. AID PROJECT        |                     |                   |           |   |                             |                    |                         |        |              |           |  |



**TYPICAL BENCHING DETAIL  
FOR EMBANKMENT**

**NOTES:**

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)  
UNLESS OTHERWISE SHOWN.

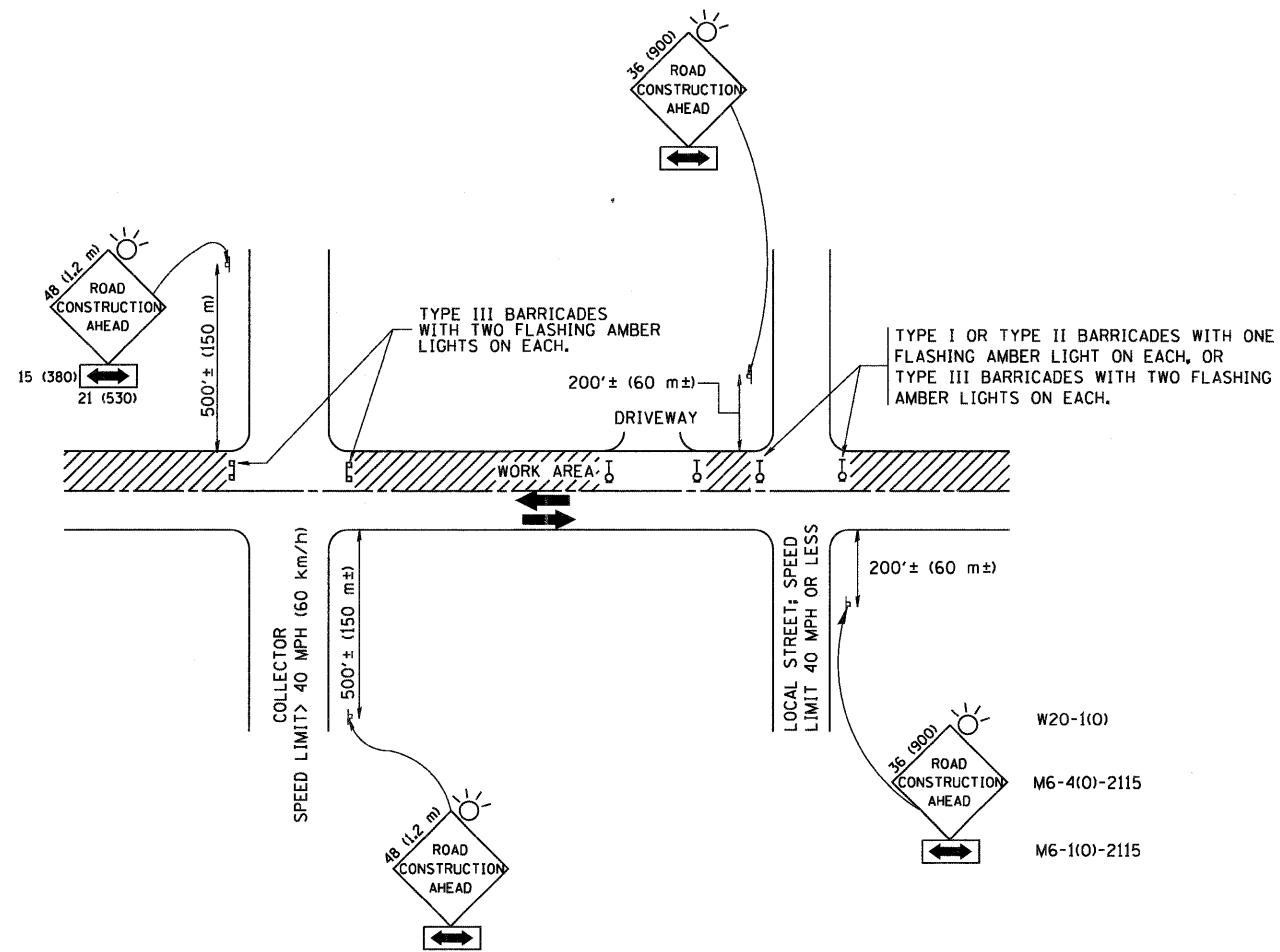
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|  |                           | DRAWN -<br>CADD     | REVISED - |
|  |                           | CHECKED -<br>S.E.B. | REVISED - |
|  |                           | DATE -<br>06-16-04  | REVISED - |
|  |                           |                     |           |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|  |                         |              |
|--|-------------------------|--------------|
| <b>BENCHING DETAIL<br/>FOR EMBANKMENT WIDENING</b> |                         |              |
| SCALE: NONE  | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. |

| F.A.U.<br>RTE.                                  | SECTION    | COUNTY             | TOTAL<br>SHEETS | SHEET<br>NO. |
|---|------------|--------------------|-----------------|--------------|
| 1321  | 2010-134-T | DUPAGE             | 40              | 28           |
| BD-51   |            | CONTRACT NO. 60N06 |                 |              |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |            |                    |                 |              |





TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

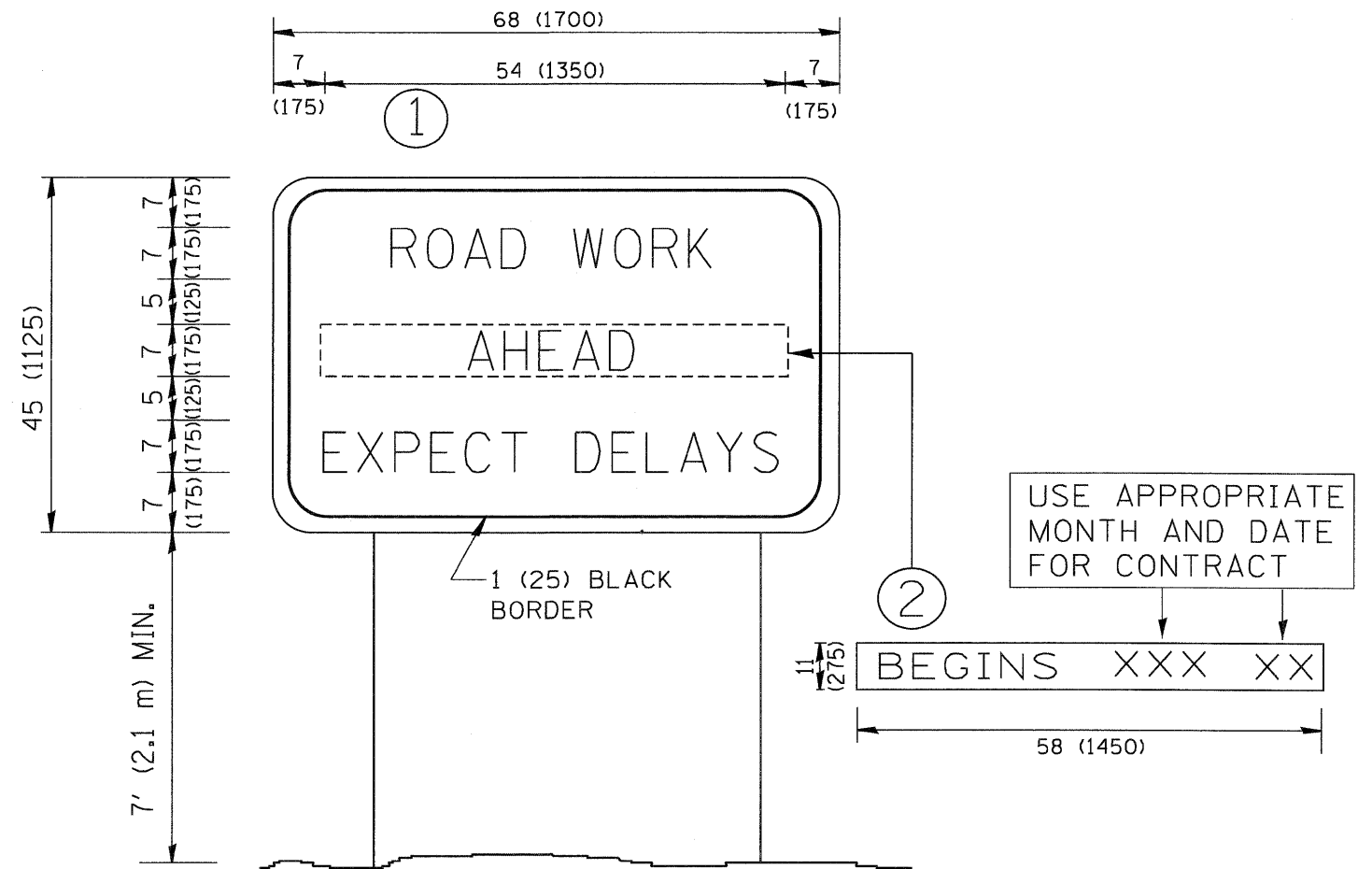
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|--|----------------------------|----------------|---------------------------------|
| FILE NAME =<br>W:\diststd\22x34\td18.dgn | USER NAME = goglienobt     | DESIGNED - LHA | REVISED - J. OBERLE 10-18-95    |
|  |                            | DRAWN -        | REVISED - A. HOUSEH 03-06-96    |
|  | PLOT SCALE = 50.000' / IN. | CHECKED -      | REVISED - A. HOUSEH 10-15-96    |
|  | PLOT DATE = 1/4/2008       | DATE - 06-89   | REVISED - T. RAMMACHER 01-06-00 |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |                       |                    |                    |                 |
|---|-----------------------|--------------------|--------------------|-----------------|
| F.A.D. RTE.<br>1321                             | SECTION<br>2010-134-T | COUNTY<br>DUPAGE   | TOTAL SHEETS<br>40 | SHEET NO.<br>29 |
| TC-10   |                       | CONTRACT NO. 60N06 |                    |                 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                       |                    |                    |                 |



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

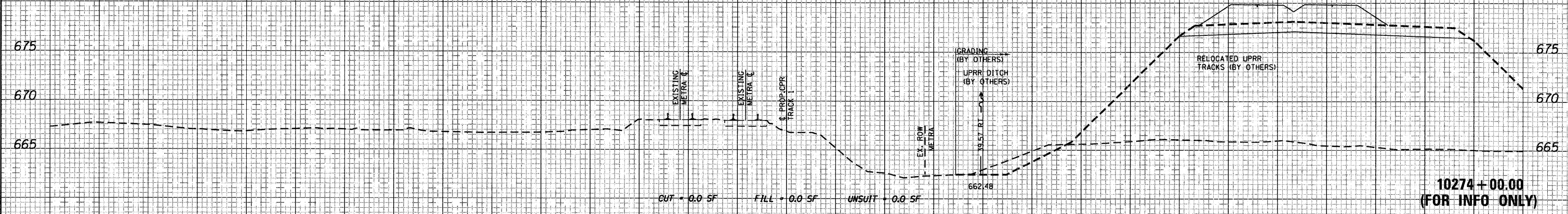
|  |                        |            |                                 |   |   |                         |      |   |            |                           |                 |              |
|--|------------------------|------------|---------------------------------|---|---|-------------------------|------|---|------------|---------------------------|-----------------|--------------|
| FILE NAME =<br>W:\diststd\22x34\to22.dgn | USER NAME = goglienobt | DESIGNED - | REVISED - R. MIRS 09-15-97      | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>ARTERIAL ROAD<br/>INFORMATION SIGN</b> |                         |      | F.A.D.<br>RTE.                                      | SECTION    | COUNTY                    | TOTAL<br>SHEETS | SHEET<br>NO. |
|  |                        | DRAWN -    | REVISED - R. MIRS 12-11-97      |   | SCALE: NONE                               | SHEET NO. 1 OF 1 SHEETS | STA. | 1321  | 2010-134-T | DUPAGE                    | 40              | 30           |
|  |                        | CHECKED -  | REVISED - T. RAMMACHER 02-02-99 |   |   |                         |      | <b>TC-22</b>  |            | <b>CONTRACT NO. 60N06</b> |                 |              |
|  |                        | DATE -     | REVISED - C. JUCIUS 01-31-07    |   |   |                         |      | FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT |            |                           |                 |              |

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

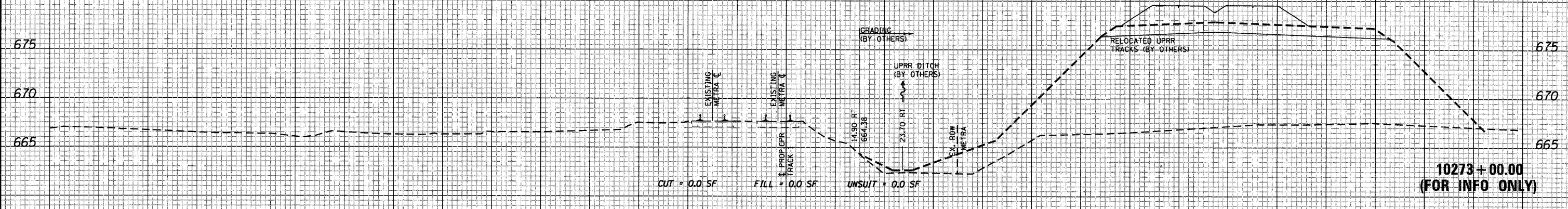
NOTES:  
 1) UTILITIES NOT SHOWN ON CROSS SECTIONS.  
 SEE UTILITY PLANS FOR LOCATION OF EXISTING UTILITIES.  
 2) FOR DESIGN CLARITY NOT ALL PROPOSED FUTURE TRACKS  
 (BY OTHERS) ARE SHOWN.  
 3) CULVERT INLET AND OUTLET PROTECTION NOT SHOWN.  
 SEE EROSION CONTROL PLAN FOR DESIGN DETAILS.

|              |  |
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| FINAL SURVEY |  |
| NOTE BOOK    |  |
| NO.          |  |

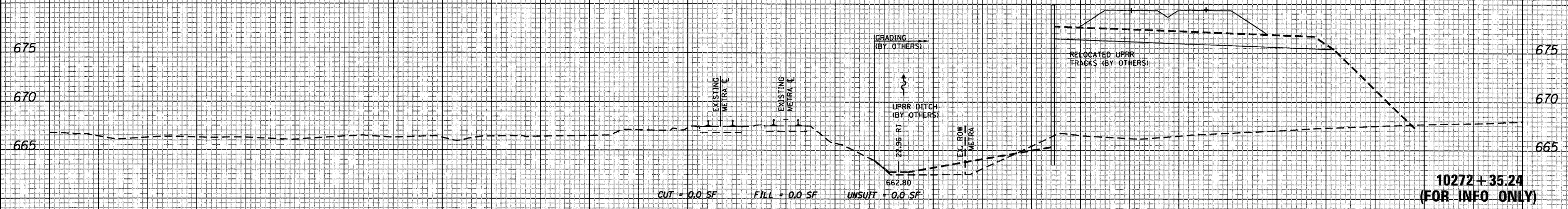
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| DATE            |  |
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| SAVED           |  |
| ORIGINAL SURVEY |  |
| NOTE BOOK       |  |
| NO.             |  |



10274 + 00.00  
(FOR INFO ONLY)



10273 + 00.00  
(FOR INFO ONLY)



10272 + 35.24  
(FOR INFO ONLY)

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

FILE NAME = ...\\D150N06-SHT-XSC-R01.dgn

USER NAME = nmatern  
 DESIGNED - NMR  
 DRAWN - NMR  
 CHECKED - LGP  
 DATE - 03/17/2011

REVISED -  
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

CANADIAN PACIFIC RAILWAY CROSS SECTIONS

SCALE: SHEET NO. 1 OF 10 SHEETS STA. 10272+35.24 TO STA. 10274+00.00

| F.A.U. RTE.        | SECTION    | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|------------|--------|---------------------------|-----------|
| 1321               | 2010-134-T | DUPAGE | 40                        | 31        |
| CONTRACT NO. 60N06 |            |        | ILLINOIS FED. AID PROJECT |           |

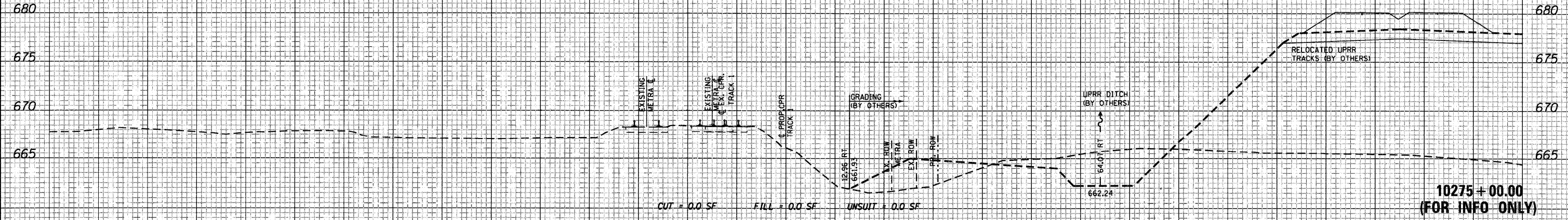
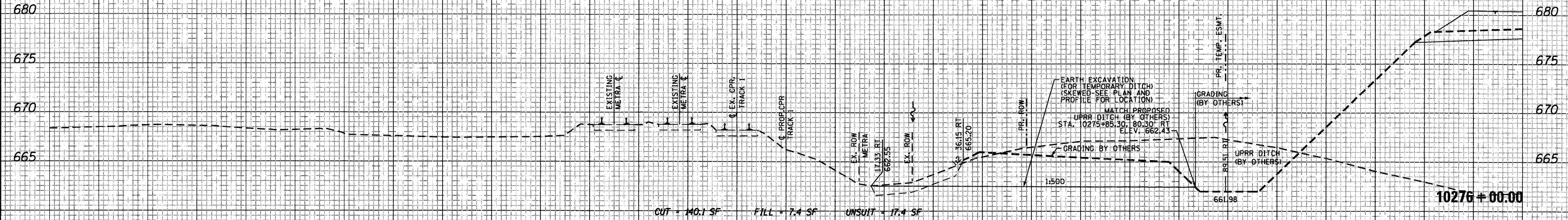
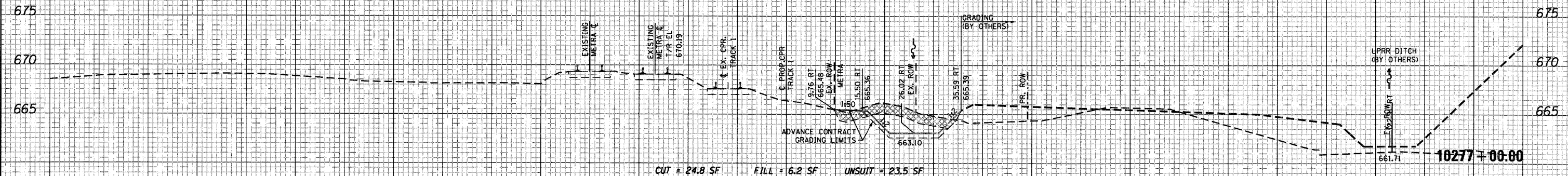


150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

NOTES:  
 1) UTILITIES NOT SHOWN ON CROSS SECTIONS.  
 SEE UTILITY PLANS FOR LOCATION OF EXISTING UTILITIES.  
 2) FOR DESIGN CLARITY NOT ALL PROPOSED FUTURE TRACKS  
 (BY OTHERS) ARE SHOWN.  
 3) CULVERT INLET AND OUTLET PROTECTION NOT SHOWN.  
 SEE EROSION CONTROL PLAN FOR DESIGN DETAILS.

DATE \_\_\_\_\_ BY \_\_\_\_\_  
 ORIGINAL SURVEY PLOTTED TEMPLATE AREAS CHECKED  
 FINAL SURVEY PLOTTED TEMPLATE AREAS CHECKED  
 NO. \_\_\_\_\_

DATE \_\_\_\_\_ BY \_\_\_\_\_  
 ORIGINAL SURVEY PLOTTED TEMPLATE AREAS CHECKED  
 FINAL SURVEY PLOTTED TEMPLATE AREAS CHECKED  
 NO. \_\_\_\_\_



150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

|  |                     |                   |           |   |  |                          |                                      |                     |                       |                  |                    |                 |
|--|---------------------|-------------------|-----------|---|--|--------------------------|--------------------------------------|---------------------|-----------------------|------------------|--------------------|-----------------|
| FILE NAME =<br>... \DISIGN\06-SHT-XSC-Rail.dgn | USER NAME = rmatern | DESIGNED - NMR    | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>CANADIAN PACIFIC RAILWAY CROSS SECTIONS</b> |                          |                                      | F.A.U. RTE.<br>1321 | SECTION<br>2010-134-T | COUNTY<br>DUPAGE | TOTAL SHEETS<br>40 | SHEET NO.<br>32 |
| PLOT SCALE = 10,000' / IN.                     |                     | DRAWN - NMR       | REVISED - |   | SCALE:   | SHEET NO. 2 OF 10 SHEETS | STA. 10275+00.00 TO STA. 10277+00.00 | CONTRACT NO. 60N06  |                       |                  |                    |                 |
| PLOT DATE = 3/25/2011                          |                     | CHECKED - LGP     | REVISED - |   | ILLINOIS FED. AID PROJECT                      |                          |                                      |                     |                       |                  |                    |                 |
|  |                     | DATE - 03/17/2011 | REVISED - |   |  |                          |                                      |                     |                       |                  |                    |                 |



- NOTES:
- 1) UTILITIES NOT SHOWN ON CROSS SECTIONS. SEE UTILITY PLANS FOR LOCATION OF EXISTING UTILITIES.
  - 2) FOR DESIGN CLARITY NOT ALL PROPOSED FUTURE TRACKS (BY OTHERS) ARE SHOWN.
  - 3) CULVERT INLET AND OUTLET PROTECTION NOT SHOWN. SEE EROSION CONTROL PLAN FOR DESIGN DETAILS.



DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
 SURVEYED: \_\_\_\_\_  
 TEMPLATE: \_\_\_\_\_  
 NOTE BOOK: \_\_\_\_\_  
 AREAS CHECKED: \_\_\_\_\_

DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
 ORIGINAL SURVEY: \_\_\_\_\_  
 TEMPLATE: \_\_\_\_\_  
 NOTE BOOK: \_\_\_\_\_  
 AREAS CHECKED: \_\_\_\_\_

|   |                      |                   |           |   |  |                          |                                      |                     |                       |                  |                    |                 |
|---|----------------------|-------------------|-----------|---|--|--------------------------|--------------------------------------|---------------------|-----------------------|------------------|--------------------|-----------------|
| FILE NAME =<br>... \D160N06-SHT-XSC-R01.dgn | USER NAME = nrotterm | DESIGNED - NMR    | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>CANADIAN PACIFIC RAILWAY CROSS SECTIONS</b> |                          |                                      | F.A.U. RTE.<br>1321 | SECTION<br>2010-134-T | COUNTY<br>DUPAGE | TOTAL SHEETS<br>40 | SHEET NO.<br>33 |
| PLOT SCALE = 10,0000' / IN.                 |                      | DRAWN - NMR       | REVISED - |   | SCALE:   | SHEET NO. 3 OF 10 SHEETS | STA. 10278+00.00 TO STA. 10281+00.00 | CONTRACT NO. 60N06  |                       |                  |                    |                 |
| PLOT DATE = 3/25/2011                       |                      | CHECKED - LGP     | REVISED - |   | ILLINOIS FED. AID PROJECT                      |                          |                                      |                     |                       |                  |                    |                 |
| DATE - 03/17/2011                           |                      | DATE - 03/17/2011 | REVISED - |   |  |                          |                                      |                     |                       |                  |                    |                 |



150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

NOTES:  
 1) UTILITIES NOT SHOWN ON CROSS SECTIONS.  
 SEE UTILITY PLANS FOR LOCATION OF EXISTING UTILITIES.  
 2) FOR DESIGN CLARITY NOT ALL PROPOSED FUTURE TRACKS  
 (BY OTHERS) ARE SHOWN.  
 3) CULVERT INLET AND OUTLET PROTECTION NOT SHOWN.  
 SEE EROSION CONTROL PLAN FOR DESIGN DETAILS.



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| NO. 3        |  |
| NO. 4        |  |
| NO. 5        |  |
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| NO. 7        |  |
| NO. 8        |  |
| NO. 9        |  |
| NO. 10       |  |

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| ORIGINAL SURVEY |  |
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|-------------------------------|---------------------|-------------------|-----------|---|--|--------------------|--------------------------|--------------------------------------|--------------|-----------|--|
| FILE NAME =                   | USER NAME = nmatern | DESIGNED - NMR    | REVISED - | <b>STATE OF ILLINOIS</b><br><b>DEPARTMENT OF TRANSPORTATION</b> | <b>CANADIAN PACIFIC RAILWAY CROSS SECTIONS</b> | F.A.U. RTE.        | SECTION                  | COUNTY                               | TOTAL SHEETS | SHEET NO. |  |
| ...\\D160N06-SHT-XSC-Rail.dgn |                     | DRAWN - NMR       | REVISED - |   |  | 1321               | 2010-134-T               | DUPAGE                               | 40           | 34        |  |
| PLOT SCALE = 10,0000' / IN.   |                     | CHECKED - LGP     | REVISED - |   |  | CONTRACT NO. 60N06 |                          | ILLINOIS FED. AID PROJECT            |              |           |  |
| PLOT DATE = 3/25/2011         |                     | DATE - 03/17/2011 | REVISED - |   |  | SCALE:             | SHEET NO. 4 OF 10 SHEETS | STA. 10282+00.00 TO STA. 10284+50.00 |              |           |  |

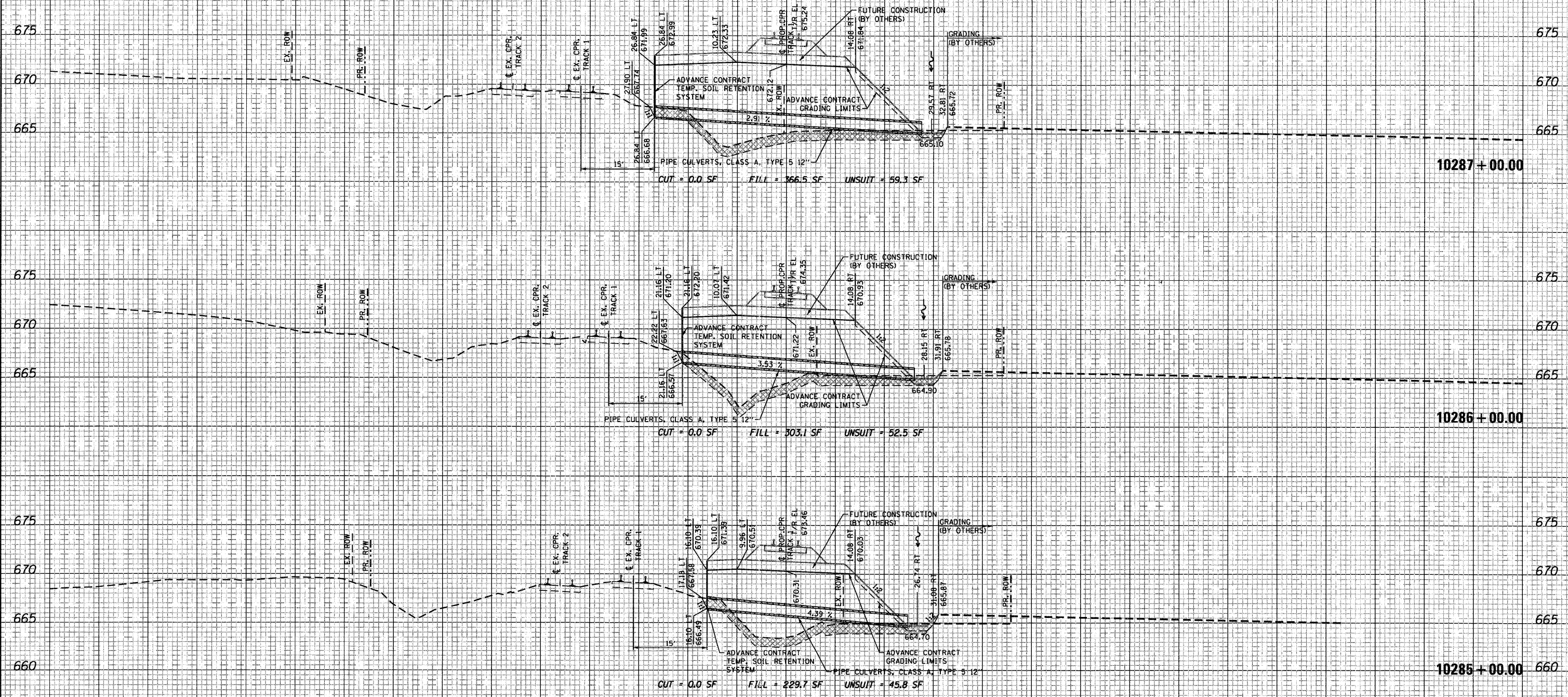


150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

NOTES:  
 1) UTILITIES NOT SHOWN ON CROSS SECTIONS.  
 SEE UTILITY PLANS FOR LOCATION OF EXISTING UTILITIES.  
 2) FOR DESIGN CLARITY NOT ALL PROPOSED FUTURE TRACKS  
 (BY OTHERS) ARE SHOWN.  
 3) CULVERT INLET AND OUTLET PROTECTION NOT SHOWN.  
 SEE EROSION CONTROL PLAN FOR DESIGN DETAILS.

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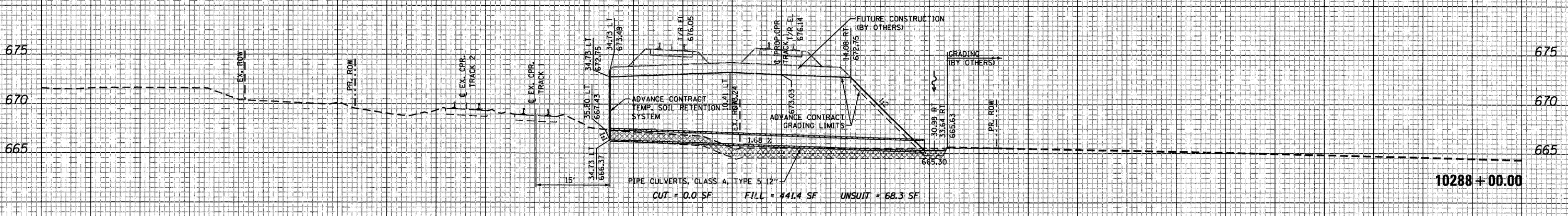
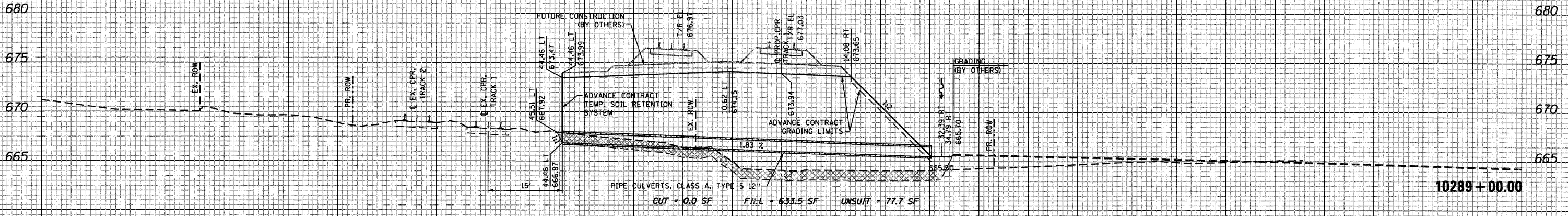
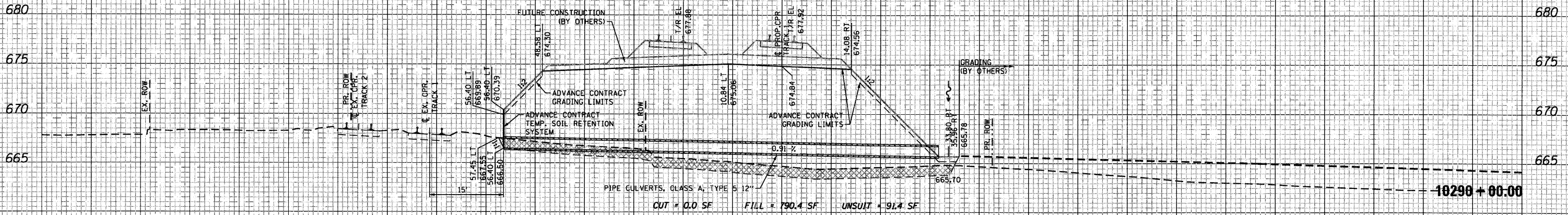
150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

|  |                     |                   |           |   |  |                          |                                      |                     |                       |                  |                    |                 |
|--|---------------------|-------------------|-----------|---|--|--------------------------|--------------------------------------|---------------------|-----------------------|------------------|--------------------|-----------------|
| FILE NAME =<br>... \D160N06-SHT-XSC-Rail.dgn | USER NAME = nmatern | DESIGNED - NMR    | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>CANADIAN PACIFIC RAILWAY CROSS SECTIONS</b> |                          |                                      | F.A.U. RTE.<br>1321 | SECTION<br>2010-134-T | COUNTY<br>DUPAGE | TOTAL SHEETS<br>40 | SHEET NO.<br>35 |
| PLOT SCALE = 10,0000' / IN.                  |                     | DRAWN - NMR       | REVISED - |   | SCALE:   | SHEET NO. 5 OF 10 SHEETS | STA. 10285+00.00 TO STA. 10287+00.00 | CONTRACT NO. 60N06  |                       |                  |                    |                 |
| PLOT DATE = 3/25/2011                        |                     | CHECKED - LGP     | REVISED - |   | ILLINOIS FED. AID PROJECT                      |                          |                                      |                     |                       |                  |                    |                 |
|  |                     | DATE - 03/17/2011 | REVISED - |   |  |                          |                                      |                     |                       |                  |                    |                 |



150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

NOTES:  
 1) UTILITIES NOT SHOWN ON CROSS SECTIONS.  
 SEE UTILITY PLANS FOR LOCATION OF EXISTING UTILITIES.  
 2) FOR DESIGN CLARITY NOT ALL PROPOSED FUTURE TRACKS  
 (BY OTHERS) ARE SHOWN.  
 3) CULVERT INLET AND OUTLET PROTECTION NOT SHOWN.  
 SEE EROSION CONTROL PLAN FOR DESIGN DETAILS.



150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

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| ORIGINAL SURVEY |  |
| NOTE BOOK       |  |
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FILE NAME = ...\\D160N06-SHT-XSC-Rail.dgn

USER NAME = nratern  
 PLOT SCALE = 10.0000' / IN.  
 PLOT DATE = 3/25/2011

DESIGNED - NMR  
 DRAWN - NMR  
 CHECKED - LGP  
 DATE - 03/17/2011

REVISED -  
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**CANADIAN PACIFIC RAILWAY CROSS SECTIONS**

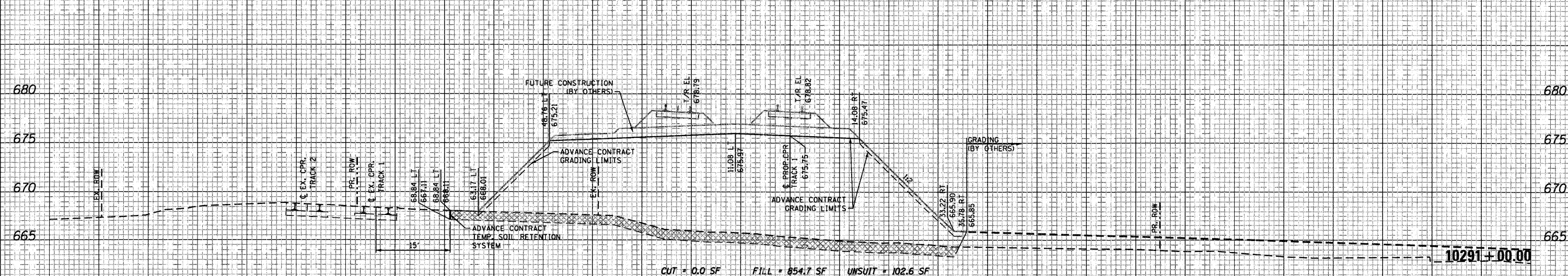
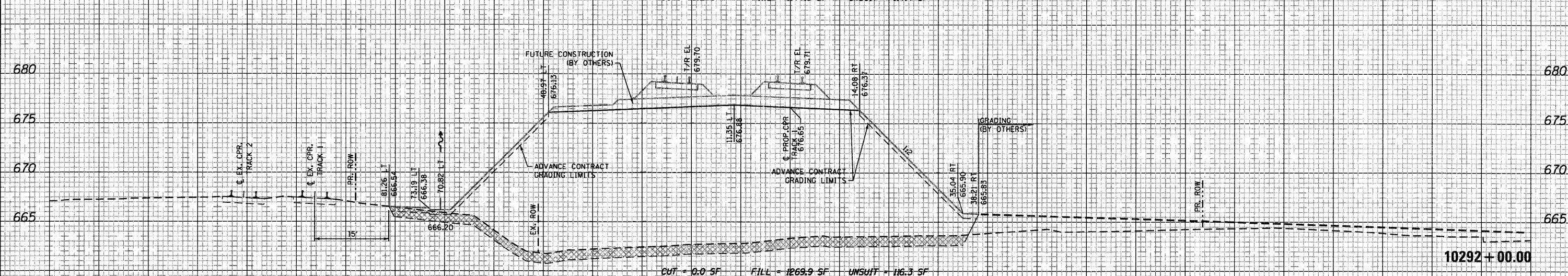
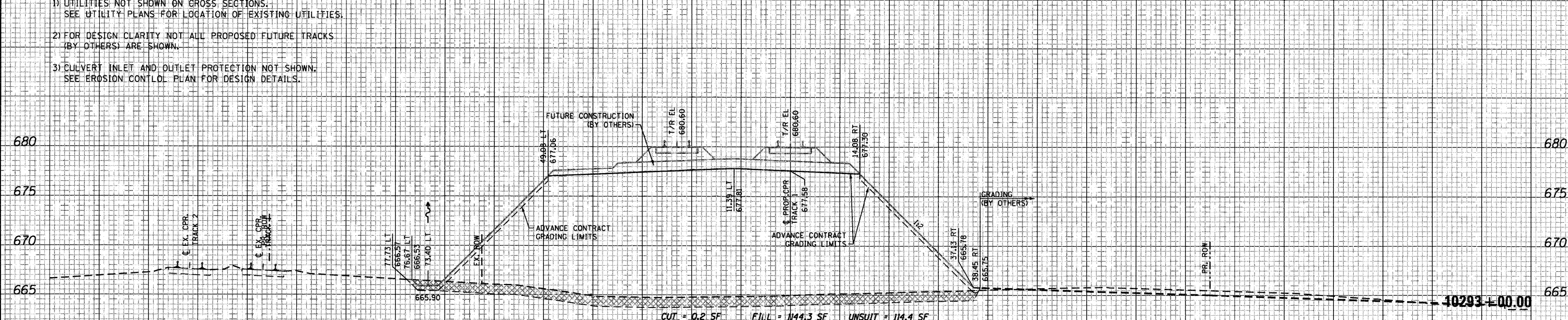
SCALE: SHEET NO. 6 OF 10 SHEETS STA. 10288+00.00 TO STA. 10290+00.00

| F.A.U. RTE.        | SECTION    | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|------------|--------|---------------------------|-----------|
| 1321               | 2010-134-T | DUPAGE | 40                        | 36        |
| CONTRACT NO. 60N06 |            |        | ILLINOIS FED. AID PROJECT |           |



150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

NOTES:  
 1) UTILITIES NOT SHOWN ON CROSS SECTIONS.  
 SEE UTILITY PLANS FOR LOCATION OF EXISTING UTILITIES.  
 2) FOR DESIGN CLARITY NOT ALL PROPOSED FUTURE TRACKS  
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 3) CULVERT INLET AND OUTLET PROTECTION NOT SHOWN.  
 SEE EROSION CONTROL PLAN FOR DESIGN DETAILS.



150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

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FILE NAME = ...\\DIB\006-SHT-XSC-Rail.dgn

USER NAME = nrratorm  
 PLOT SCALE = 10.0000' / IN.  
 PLOT DATE = 3/25/2011

DESIGNED - NMR  
 DRAWN - NMR  
 CHECKED - LGP  
 DATE - 03/17/2011

REVISED -  
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

CANADIAN PACIFIC RAILWAY CROSS SECTIONS

SCALE: SHEET NO. 7 OF 10 SHEETS STA. 10291+00.00 TO STA. 10293+00.00

| F.A.U. RTE.        | SECTION    | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|------------|--------|---------------------------|-----------|
| 1321               | 2010-134-T | DUPAGE | 40                        | 37        |
| CONTRACT NO. 60N06 |            |        | ILLINOIS FED. AID PROJECT |           |

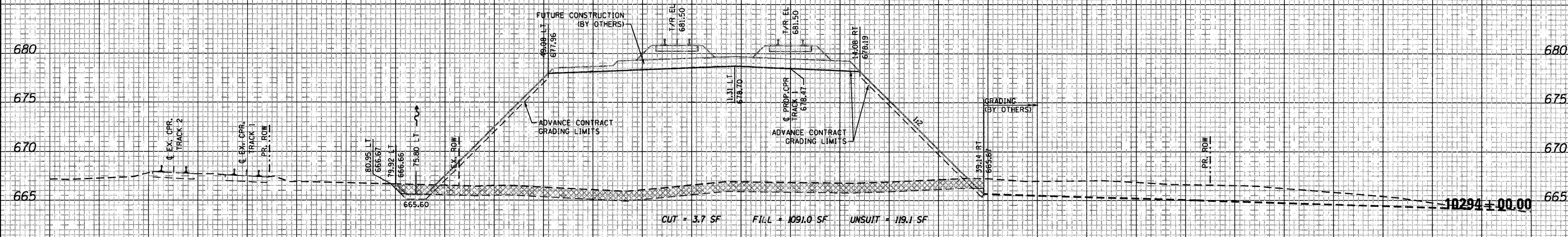
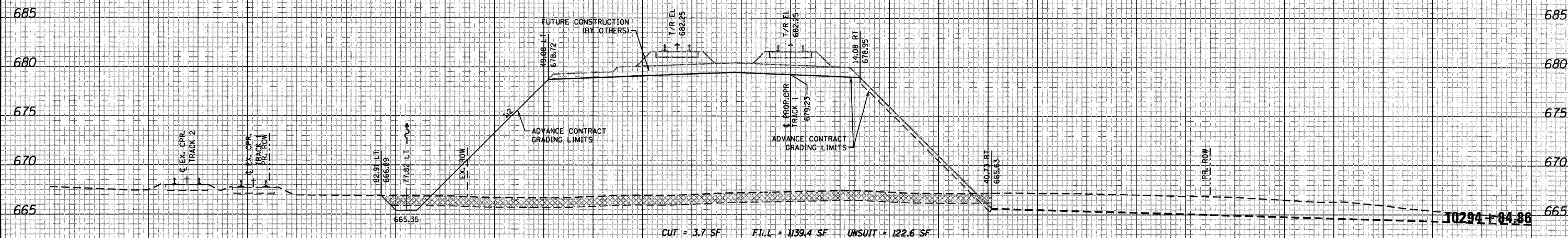


150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

NOTES:  
 1) UTILITIES NOT SHOWN ON CROSS SECTIONS.  
 SEE UTILITY PLANS FOR LOCATION OF EXISTING UTILITIES.  
 2) FOR DESIGN CLARITY NOT ALL PROPOSED FUTURE TRACKS  
 (BY OTHERS) ARE SHOWN.  
 3) CULVERT INLET AND OUTLET PROTECTION NOT SHOWN.  
 SEE EROSION CONTROL PLAN FOR DESIGN DETAILS.

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| AREAS CHECKED   |  |
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150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

FILE NAME = ...\\D150N06-SHT-XSC-Rail.dgn

USER NAME = nmatern  
 DESIGNED - NMR  
 DRAWN - NMR  
 CHECKED - LGP  
 PLOT DATE = 3/25/2011

REVISED -  
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

CANADIAN PACIFIC RAILWAY CROSS SECTIONS  
 SCALE: SHEET NO. 8 OF 10 SHEETS STA. 10294+00.00 TO STA. 10294+84.86

| F.A.U. RTE.        | SECTION    | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|------------|--------|---------------------------|-----------|
| 1321               | 2010-134-T | DUPAGE | 40                        | 38        |
| CONTRACT NO. 60N06 |            |        | ILLINOIS FED. AID PROJECT |           |

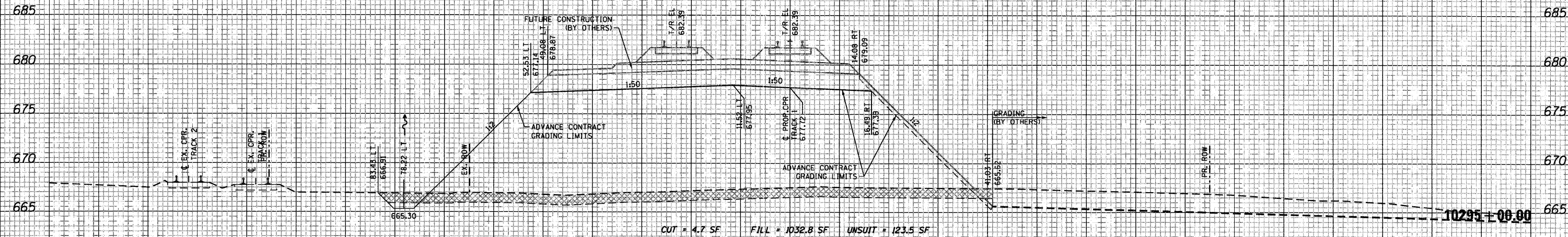
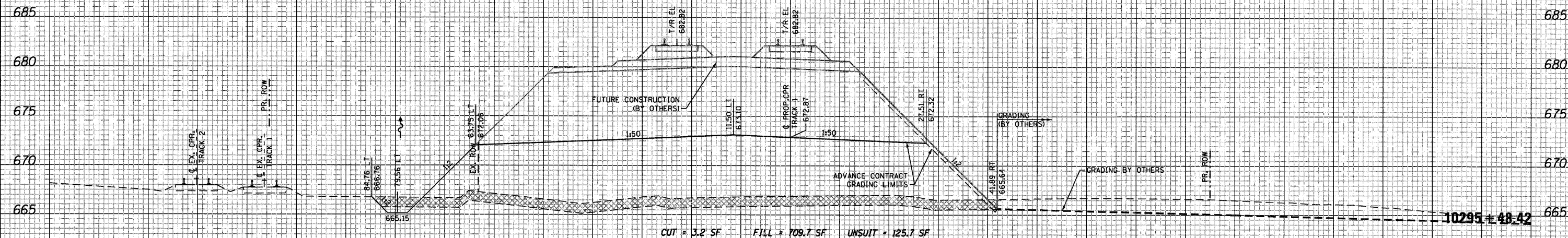


150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

NOTES:  
 1) UTILITIES NOT SHOWN ON CROSS SECTIONS.  
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| DATE          |  |
| BY            |  |
| NO.           |  |
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| NOTE BOOK     |  |
| AREAS CHECKED |  |

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| BY              |  |
| NO.             |  |
| ORIGINAL SURVEY |  |
| SKETCHED        |  |
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| NOTE BOOK       |  |
| AREAS CHECKED   |  |



150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

FILE NAME = ...\\D160N06-SHT-XSC-Res1.dgn

USER NAME = nratern  
 DESIGNED - NMR  
 DRAWN - NMR  
 CHECKED - LGP  
 PLOT SCALE = 10.0000' / IN.  
 PLOT DATE = 3/25/2011  
 DATE - 03/17/2011

REVISED -  
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 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

CANADIAN PACIFIC RAILWAY CROSS SECTIONS  
 SCALE: SHEET NO. 9 OF 10 SHEETS STA. 10295+00.00 TO STA. 10295+48.42

| F.A.U. RTE.        | SECTION    | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|------------|--------|---------------------------|-----------|
| 1321               | 2010-134-T | DUPAGE | 40                        | 39        |
| CONTRACT NO. 60N06 |            |        | ILLINOIS FED. AID PROJECT |           |

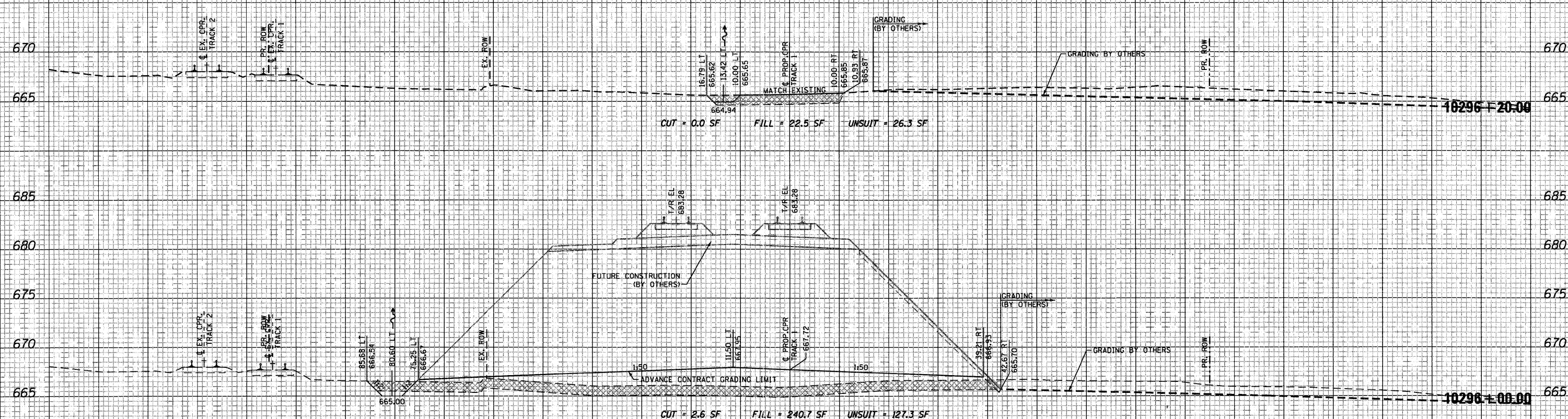


150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

NOTES:  
 1) UTILITIES NOT SHOWN ON CROSS SECTIONS.  
 SEE UTILITY PLANS FOR LOCATION OF EXISTING UTILITIES.  
 2) FOR DESIGN CLARITY NOT ALL PROPOSED FUTURE TRACKS  
 (BY OTHERS) ARE SHOWN.  
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| DATE         |  |
| BY           |  |
| SURVEYED     |  |
| PLOTTED      |  |
| TEMPLATE     |  |
| AREAS        |  |
| CHECKED      |  |
| FINAL SURVEY |  |
| NOTE BOOK    |  |
| NO.          |  |

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| DATE            |  |
| BY              |  |
| SURVEYED        |  |
| PLOTTED         |  |
| TEMPLATE        |  |
| AREAS           |  |
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| ORIGINAL SURVEY |  |
| NOTE BOOK       |  |
| NO.             |  |



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|------------------------------|---------------------|-------------------|-----------|---|---|--------------------------------------|--------|----|-------------|---------|--------|--------------|-----------|
| FILE NAME =                  | USER NAME = nmatern | DESIGNED - NMR    | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION | CANADIAN PACIFIC RAILWAY CROSS SECTIONS |                                      |        |    | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ... \DIGNB6-SHT-XSC-Rail.dgn |                     | DRAWN - NMR       | REVISED - |   | 1321                                    | 2010-134-T                           | DUPAGE | 40 | 40          |         |        |              |           |
| PLOT SCALE = 10.0000' / IN.  |                     | CHECKED - LGP     | REVISED - |   | CONTRACT NO. 60N06                      |                                      |        |    |             |         |        |              |           |
| PLOT DATE = 3/25/2011        |                     | DATE - 03/17/2011 | REVISED - |   | ILLINOIS FED. AID PROJECT               |                                      |        |    |             |         |        |              |           |
|                              |                     |                   |           | SCALE:  | SHEET NO. 10 OF 10 SHEETS               | STA. 10296+00.00 TO STA. 10296+20.00 |        |    |             |         |        |              |           |