

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 372 (IL 171 - 1ST AVE. FRONTAGE RD.)

JOLIET RD. TO 47TH ST.

SECTION 2010-155-RS

RESURFACING (3P)

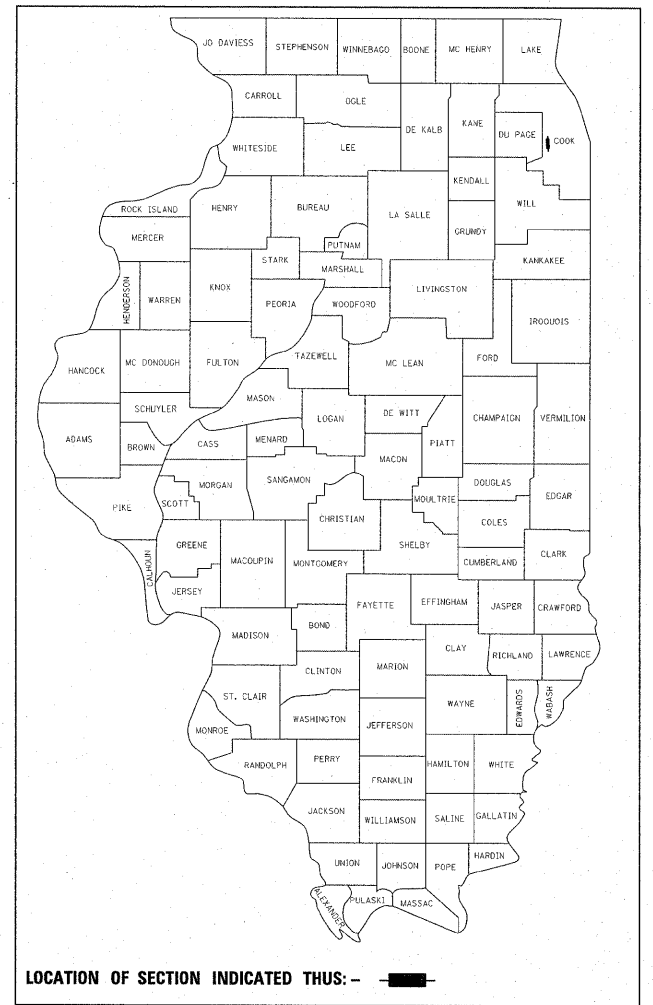
PROJECT: F-0372(010)

COOK COUNTY

C-91-329-11

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	2010-155-RS	COOK	27	1
		ILLINOIS	CONTRACT NO. 60N59	

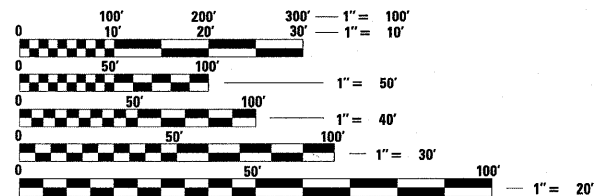
D-91-329-11



FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED
IN THE VILLAGES OF
McCOOK AND LYONS

TRAFFIC DATA:
2010 ADT = 3,450
SPEED LIMIT = 30 - 40 MPH

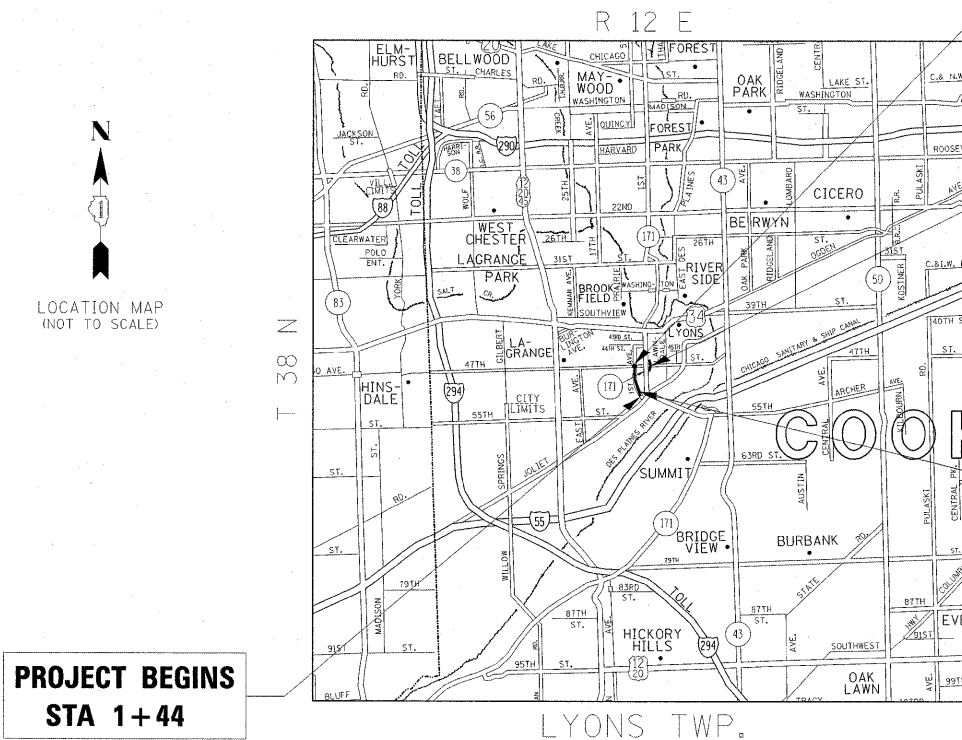


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: DAN WILGREEN (847) 705-4240
PROJECT MANAGER: KEN ENG (847) 705-4247

CONTRACT NO. 60N59



PROJECT ENDS
STA 31+02

IL 171 NB ENTR RAMP:
STA 10+00 TO STA 17+37

IL 171 NB EXIT RAMP:
STA 17+22 TO STA 24+50

OMISSION AT RR CROSSING
STA 3+13 TO STA 3+53

PROJECT BEGINS
STA 1+44

GROSS LENGTH = 4,423 FT. = 0.838 MILE
NET LENGTH = 4,383 FT. = 0.830 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED APRIL 6, 20 11

Diane M. O'Keefe DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 13 20 11

Scott E. Stitt, P.E. acting ENGINEER OF DESIGN AND ENVIRONMENT

May 13 20 11

Christine M. Reed DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

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16	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
17	HMA TAPER AT EDGE OF PCC PAVEMENT (BD-33)
18	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)
19	TYPICAL APPLICATIONS; RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)
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23	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES (TC-17)
24	ARTERIAL ROAD INFORMATION SIGN (TC-22)
25	TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS (TC-23)
26	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS (TS-05A)
27	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
604001-03	FRAME AND LIDS, TYPE 1
604086-02	FRAME AND GRATE, TYPE 23
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS-DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-07	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATION
886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS

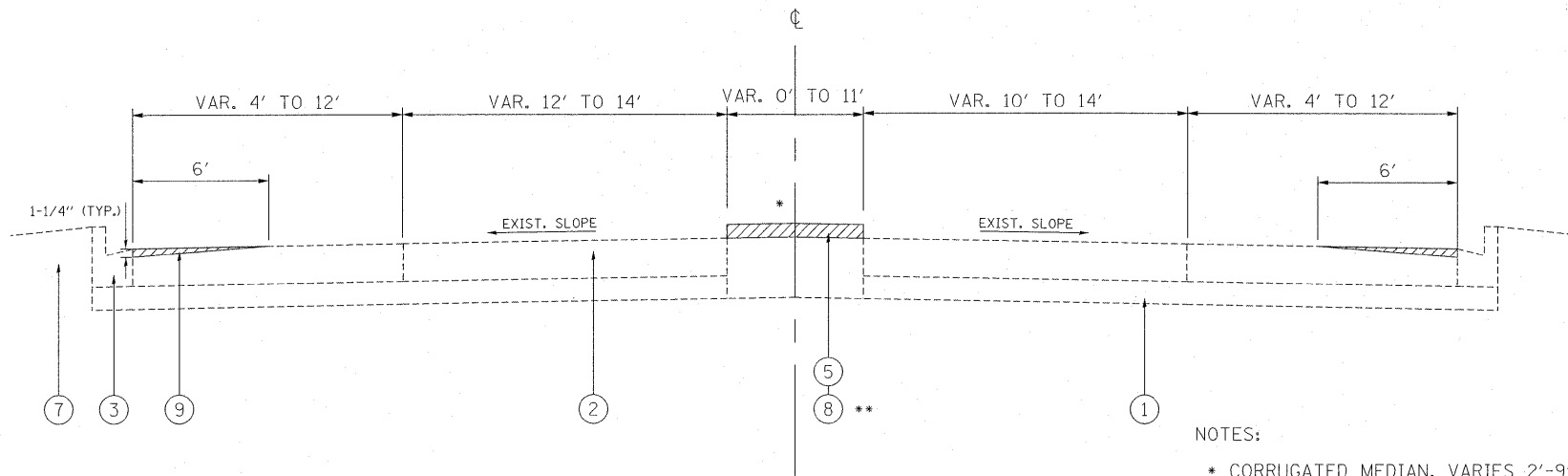
GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF MCCOOK AND LYONS.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- ALL PAVEMENT PATCHING AND CURB AND GUTTER REMOVAL AND REPLACEMENT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE RESIDENT ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD ENGINEER, AT (708) 597-9800 A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
- CLEANING OF LONGITUDINAL AND TRANSVERSE CRACKS (REMOVAL OF COLD PATCH MATERIALS AND BLOWING) SHALL BE INCIDENTAL TO THE PAY ITEM MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS.

FILE NAME =	USER NAME = tariqf	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 171 - 1st AVE. FRONTAGE ROAD (JOLIET RD. TO 47TH ST.) INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pv_work\px\dot\tariqf\0251775\0132-11-shr-plan\dgn	DRAWN -	REVISED -	372			2010-155-RS	COOK	27	2	
PLOT SCALE = 50.0000' / 1" IN.	CHECKED -	REVISED -	CONTRACT NO. 60N59							
PLOT DATE = 4/6/2011	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
					SCALE:	SHEET NO. OF SHEETS		STA. TO STA.		

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	334	334					70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	301	301				
25200110	SODDING, SALT TOLERANT	SO YD	334	334					* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	616	616				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	24	24					* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	10809	10809				
40600300	AGGREGATE (PRIME COAT)	TON	104	104					* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1252	1252				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	117	117					* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	422	422				
40600895	CONSTRUCTING TEST STRIP	EACH	2	2					* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	70	70				
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SO YD	1949	1949					* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	507	507				
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	206	206					* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	225	225				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	2312	2312					78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	180	180				
42001300	PROTECTIVE COAT	SO YD	446	446					* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1442	1442				
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SO FT	2176	2176					X4060826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1105	1105				
44022029	PARTIAL DEPTH REMOVAL 3"	SO YD	1202	1202					X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SO YD	5129	5129				
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SO YD	619	619					Δ X5539700	STORM SEWERS TO BE CLEANED	FOOT	300	300				
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SO YD	145	145					Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	2000	2000				
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SO YD	313	313					Δ Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	18	18				
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	3	3					Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4				
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	3	3					Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1				
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	15	15													
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	30	30													
60404940	FRAMES AND GRATES, TYPE 23	EACH	12	12													
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6													
67100100	MOBILIZATION	L SUM	1	1													
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1													
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1													
70300100	SHORT TERM PAVEMENT MARKING	FOOT	1800	1800													
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	616	616													
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	10809	10809													
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1252	1252													
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	422	422													
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	70	70													
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	507	507													

FILE NAME =	USER NAME = torlqfm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 171 - 1st AVE. FRONTAGE ROAD (JOLIET RD. TO 47TH ST.) SUMMARY OF QUANTITIES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
es:\pw_swork\p\tdat\torlqfm\025775\033290-att-plan.dgn	PLOT SCALE = 50,0000' / IN.	DRAWN -	REVISED -			372	2010-155-RS	COOK	27	3	
	PLOT DATE = 4/7/2011	CHECKED -	REVISED -			CONTRACT NO. 60N59					
		DATE -	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



IL 171 - 1st AVE. FRONTAGE RD.
EXISTING TYPICAL SECTION
 STA. 1+44 TO STA. 14+90
 STA. 19+50 TO STA. 20+82

NOTES:
 * CORRUGATED MEDIAN, VARIES 2'-9':
 STA 1+44 TO STA 2+60
 STA 19+50 TO STA 20+82
 * PAINTED MEDIAN, VARIES 0'-11':
 STA 9+13 TO STA 13+00
 * 10' LT TURN LANE:
 STA 5+60 TO STA 7+90

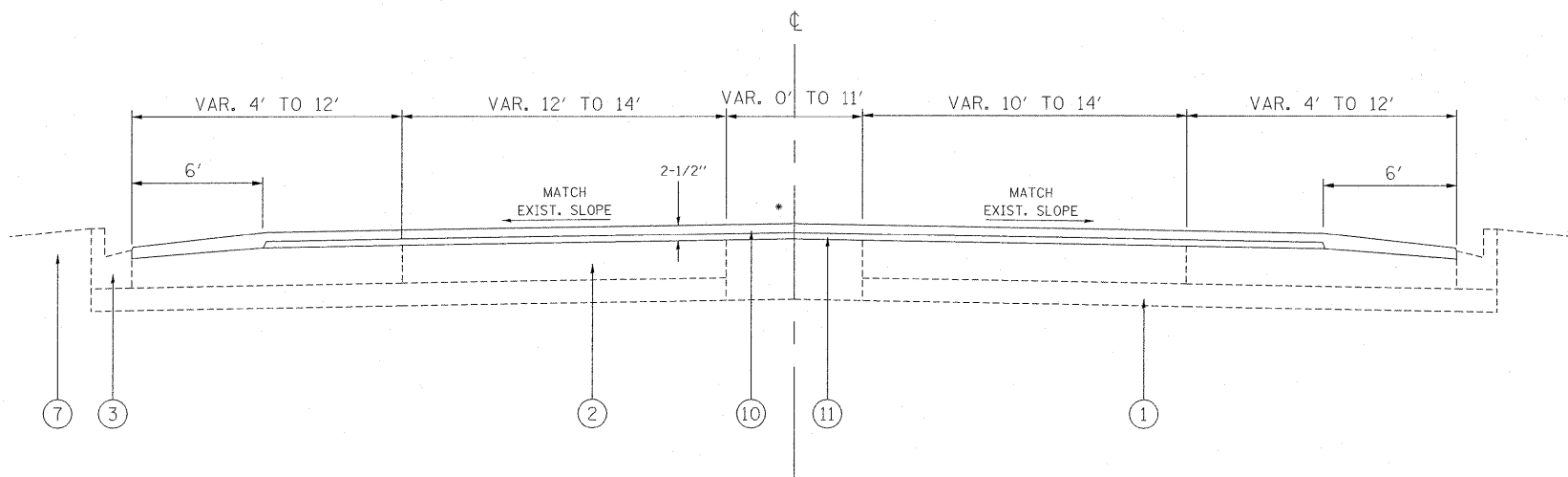
** CORRUGATED MEDIAN SHALL BE MILLED FLUSH TO MATCH EXISTING ADJACENT PCC PAVEMENT

LEGEND

- ① EXISTING SUBBASE GRANULAR MATERIAL, TYPE A, 6"
- ② EXISTING PCC PAVEMENT, 9"±
- ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER
- ④ EXISTING PCC MEDIAN
- ⑤ EXISTING CORRUGATED MEDIAN
- ⑥ EXISTING STABILIZED SUBBASE, 4"
- ⑦ EXISTING TOP SOIL AND SODDING
- ⑧ PROPOSED MEDIAN REMOVAL, PARTIAL DEPTH
- ⑨ PROPOSED PCC SURFACE REMOVAL, VARIABLE DEPTH
- ⑩ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1-1/2"
- ⑪ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑫ PROPOSED PARTIAL DEPTH REMOVAL, 3"
- ⑬ PROPOSED HMA BINDER COURSE, IL-19.0, N70, 3" (NOTE: SEE DETAIL ON SHEET #5)

OMISSION

STA. 3+13 TO STA. 3+53



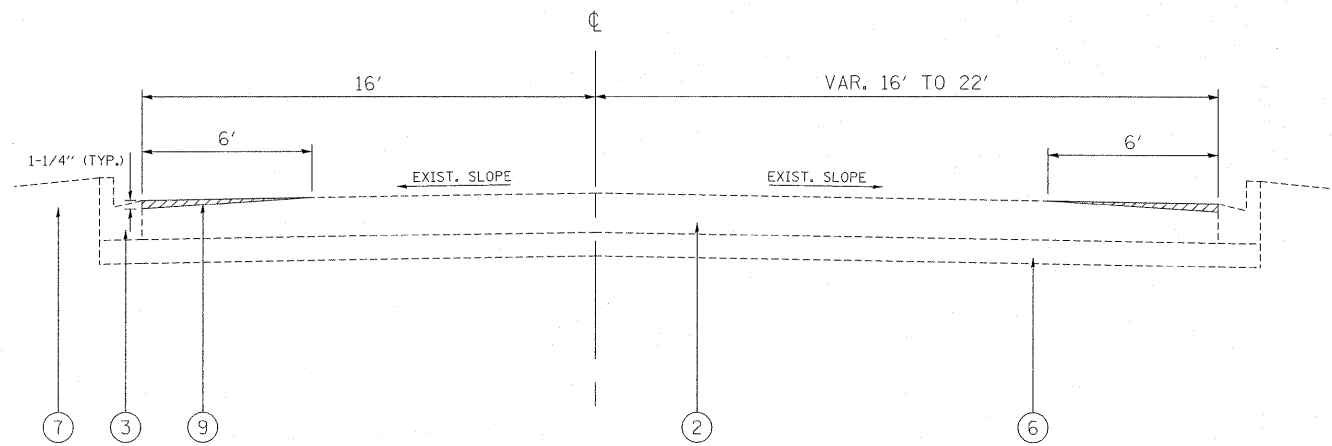
IL 171 - 1st AVE. FRONTAGE RD.
PROPOSED TYPICAL SECTION
 STA. 1+44 TO STA. 14+90
 STA. 19+50 TO STA. 20+82

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE USES	MIXTURE TYPE	AIR VOIDS @ Ndes
ROADWAY RESURFACING	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL-9.5 mm)	4% @ 70 GYR
	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	4% @ 50 GYR
PATCHES*	CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR
LONGITUDINAL JOINT REPAIR	HMA BINDER COURSE, IL-19.0, N70	4% @ 70 GYR

* LOCATIONS TO BE DETERMINED IN THE FIELD BY THE ENGINEER

NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURES IS 112 LBS/SOYD/IN.

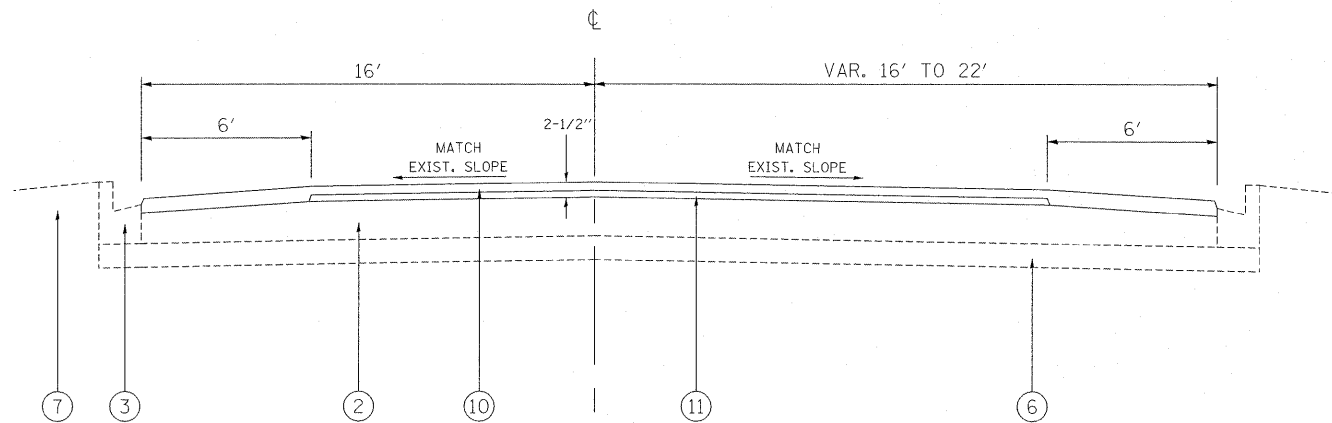
NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.



IL 171 - 1st AVE. FRONTAGE RD.
EXISTING TYPICAL SECTION

STA. 14+90 TO STA. 19+50

- LEGEND
- ① EXISTING SUBBASE GRANULAR MATERIAL, TYPE A, 6"
 - ② EXISTING PCC PAVEMENT, 9"±
 - ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER
 - ④ EXISTING PCC MEDIAN
 - ⑤ EXISTING CORRUGATED MEDIAN
 - ⑥ EXISTING STABILIZED SUBBASE, 4"
 - ⑦ EXISTING TOP SOIL AND SODDING
 - ⑧ PROPOSED MEDIAN REMOVAL, PARTIAL DEPTH
 - ⑨ PROPOSED PCC SURFACE REMOVAL, VARIABLE DEPTH
 - ⑩ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1-1/2"
 - ⑪ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
 - ⑫ PROPOSED PARTIAL DEPTH REMOVAL, 3"
 - ⑬ PROPOSED HMA BINDER COURSE, IL-19.0, N70, 3"
(NOTE: SEE DETAIL ON SHEET #5)

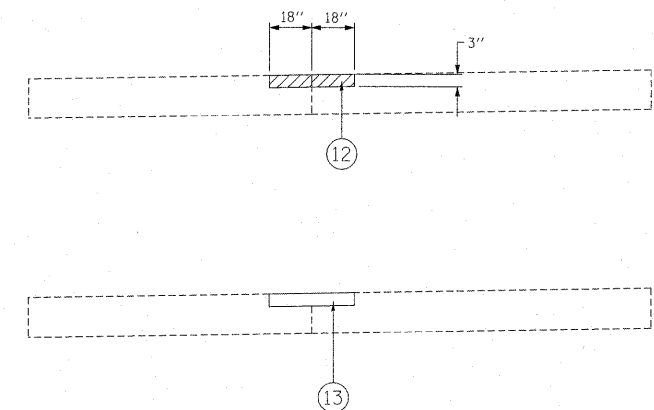


IL 171 - 1st AVE. FRONTAGE RD.
PROPOSED TYPICAL SECTION

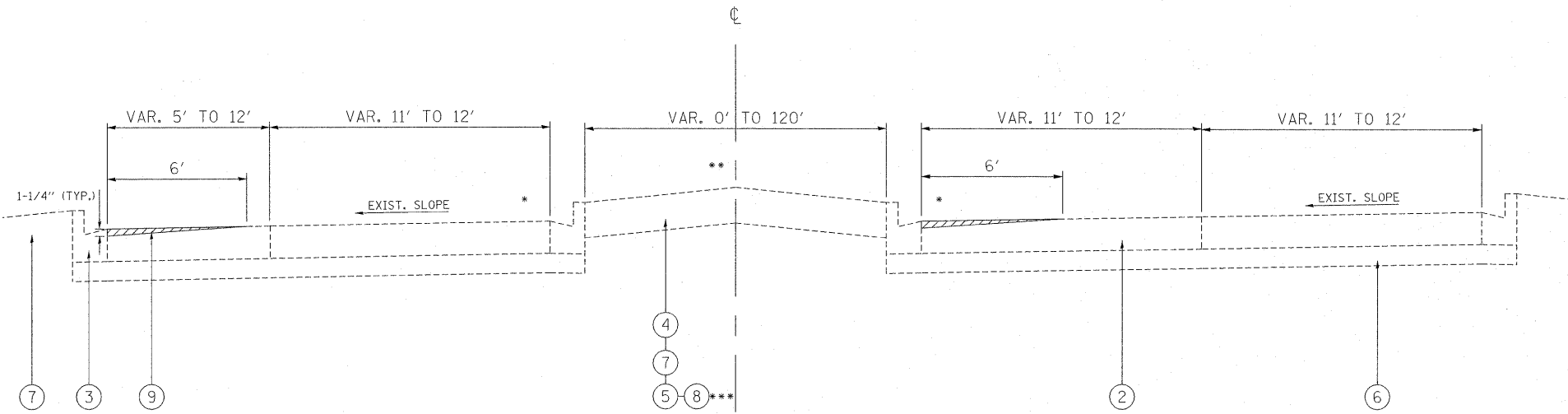
STA. 14+90 TO STA. 19+50

DETAIL A
LONGITUDINAL JOINT REPAIR (TYP.)

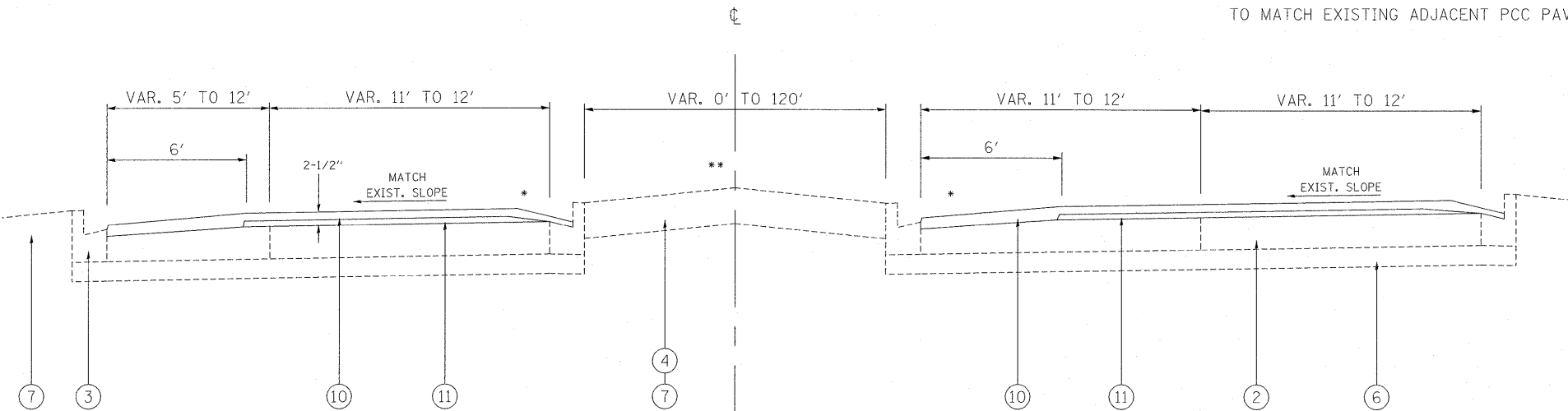
(LOCATIONS TO BE DETERMINED BY ENGINEER)



FILE NAME =	USER NAME = tariqfm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 171 - 1st AVE. FRONTAGE ROAD (JOLIET RD. TO 47TH ST.) EXISTING AND PROPOSED TYPICAL SECTIONS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct\pw_work\p\p\tdat\tariqfm\0251775\0132	l-sh-t-plen.dgn	DRAWN -	REVISED -			372	2010-155-RS	COOK	27	5	
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -			SCALE: SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 60N59			
PLOT DATE = 4/7/2011		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



IL 171 - 1st AVE. FRONTAGE RD.
(INLCUDES RAMP B1)
EXISTING TYPICAL SECTION
 STA. 20+82 TO STA. 31+02



IL 171 - 1st AVE. FRONTAGE RD.
(INLCUDES RAMP B1)
PROPOSED TYPICAL SECTION
 STA. 20+82 TO STA. 31+02

LEGEND

- ① EXISTING SUBBASE GRANULAR MATERIAL, TYPE A, 6"
- ② EXISTING PCC PAVEMENT, 9"±
- ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER
- ④ EXISTING PCC MEDIAN
- ⑤ EXISTING CORRUGATED MEDIAN
- ⑥ EXISTING STABILIZED SUBBASE, 4"
- ⑦ EXISTING TOP SOIL AND SODDING
- ⑧ PROPOSED MEDIAN REMOVAL, PARTIAL DEPTH
- ⑨ PROPOSED PCC SURFACE REMOVAL, VARIABLE DEPTH
- ⑩ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1-1/2"
- ⑪ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑫ PROPOSED PARTIAL DEPTH REMOVAL, 3"
- ⑬ PROPOSED HMA BINDER COURSE, IL-19.0, N70, 3"
 (NOTE: SEE DETAIL ON SHEET #5)

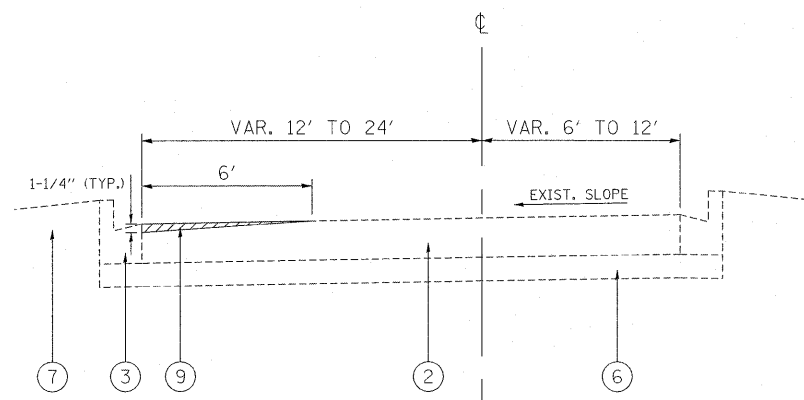
NOTES:

* 12' LEFT TURN LANE:
 STA 25+21 TO STA 28+15 RT
 STA 29+30 TO STA 31+02 LT

** MEDIANS: GRASS MEDIAN, VARIES 0'-120': PCC MEDIAN, 6': CORRUGATED MEDIAN, VARIES 2'-14':
 STA 20+82 TO STA 26+87 STA 26+87 TO STA 28+16 STA 23+98 TO STA 24+39
 STA 30+56 TO STA 31+02 STA 29+27 TO STA 30+56

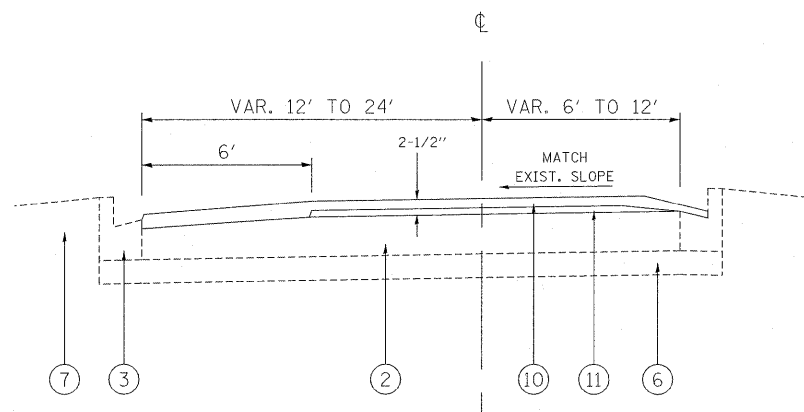
*** CORRUGATED MEDIAN SHALL BE MILLED FLUSH TO MATCH EXISTING ADJACENT PCC PAVEMENT

FILE NAME =	USER NAME = tariqfm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 171 - 1st AVE. FRONTAGE ROAD (JOLIET RD. TO 47TH ST.) EXISTING AND PROPOSED TYPICAL SECTIONS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ci\pw_work\p\dot\tariqfm\0251775\0132\11-ht-plen.dgn	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -			372	2010-155-RS	COOK	27	6	
PLOT DATE = 4/7/2011	DATE -	CHECKED -	REVISED -			CONTRACT NO. 60N59					
						SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT



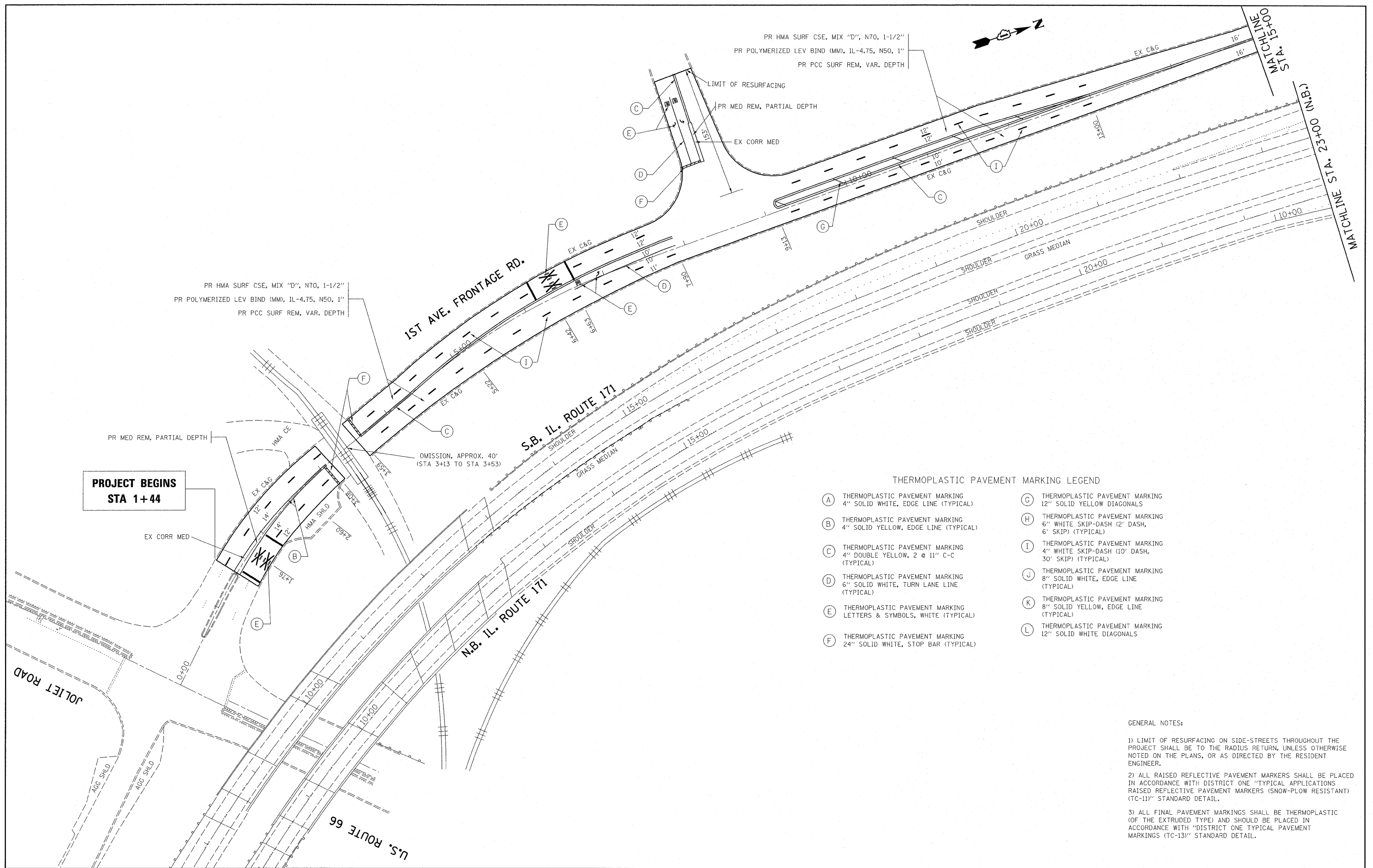
**RAMPS A, B, & C
EXISTING TYPICAL SECTION**

- LEGEND
- ① EXISTING SUBBASE GRANULAR MATERIAL, TYPE A, 6"
 - ② EXISTING PCC PAVEMENT, 9"±
 - ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER
 - ④ EXISTING PCC MEDIAN
 - ⑤ EXISTING CORRUGATED MEDIAN
 - ⑥ EXISTING STABILIZED SUBBASE, 4"
 - ⑦ EXISTING TOP SOIL AND SODDING
 - ⑧ PROPOSED MEDIAN REMOVAL, PARTIAL DEPTH
 - ⑨ PROPOSED PCC SURFACE REMOVAL, VARIABLE DEPTH
 - ⑩ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1-1/2"
 - ⑪ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
 - ⑫ PROPOSED PARTIAL DEPTH REMOVAL, 3"
 - ⑬ PROPOSED HMA BINDER COURSE, IL-19.0, N70, 3"
(NOTE: SEE DETAIL ON SHEET #5)



**RAMPS A, B, & C
PROPOSED TYPICAL SECTION**

FILE NAME =	USER NAME = tarisqfm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 171 - 1st AVE. FRONTAGE ROAD (JOLIET RD. TO 47TH ST.) EXISTING AND PROPOSED TYPICAL SECTIONS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct\pw_work\pwidot\tarisqfm\251775\0132	11-shr-plan.dgn	DRAWN -	REVISED -			372	2010-155-RS	COOK	27	7	
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -	REVISED -			CONTRACT NO. 60N59					
PLOT DATE = 4/7/2011	DATE -	REVISED -	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE:	SHEET NO. OF SHEETS		STA. TO STA.			



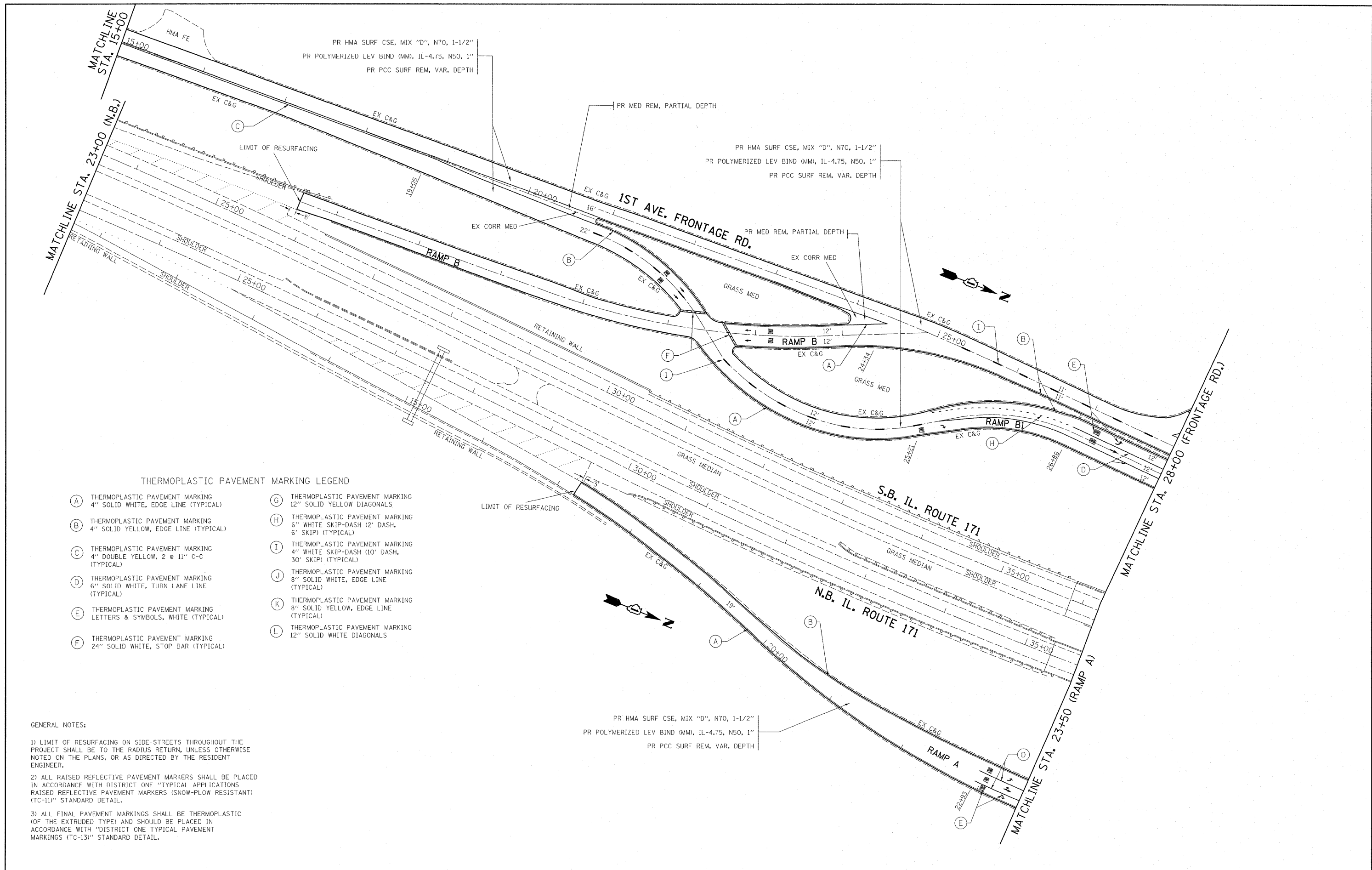
**PROJECT BEGINS
STA 1+44**

- THERMOPLASTIC PAVEMENT MARKING LEGEND**
- (A) THERMOPLASTIC PAVEMENT MARKING
4" SOLID WHITE, EDGE LINE (TYPICAL)
 - (B) THERMOPLASTIC PAVEMENT MARKING
4" SOLID YELLOW, EDGE LINE (TYPICAL)
 - (C) THERMOPLASTIC PAVEMENT MARKING
4" DOUBLE YELLOW, 2 @ 11" C-C (TYPICAL)
 - (D) THERMOPLASTIC PAVEMENT MARKING
6" SOLID WHITE, TURN LANE LINE (TYPICAL)
 - (E) THERMOPLASTIC PAVEMENT MARKING
LETTERS & SYMBOLS, WHITE (TYPICAL)
 - (F) THERMOPLASTIC PAVEMENT MARKING
24" SOLID WHITE, STOP BAR (TYPICAL)
 - (G) THERMOPLASTIC PAVEMENT MARKING
12" SOLID YELLOW DIAGONALS
 - (H) THERMOPLASTIC PAVEMENT MARKING
6" WHITE SKIP-DASH (2' DASH, 6' SKIP) (TYPICAL)
 - (I) THERMOPLASTIC PAVEMENT MARKING
4" WHITE SKIP-DASH (10' DASH, 30' SKIP) (TYPICAL)
 - (J) THERMOPLASTIC PAVEMENT MARKING
8" SOLID WHITE, EDGE LINE (TYPICAL)
 - (K) THERMOPLASTIC PAVEMENT MARKING
8" SOLID YELLOW, EDGE LINE (TYPICAL)
 - (L) THERMOPLASTIC PAVEMENT MARKING
12" SOLID WHITE DIAGONALS

GENERAL NOTES:

- 1) LIMIT OF RESURFACING ON SIDE-STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE NOTED ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER.
- 2) ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)" STANDARD DETAIL.
- 3) ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)" STANDARD DETAIL.

FILE NAME = d:\pw_work\pwsdos\tariqf\m\0251775\013211-sht-plandgn	USER NAME = tariqf	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 171 - 1st AVE. FRONTAGE ROAD (JOLIET RD. TO 47TH ST.) ROADWAY AND PAVEMENT MARKING PLANS	F.A.P. RTE. 372	SECTION 2010-155-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 8		
PLOT SCALE = 50,0000' / IN.	CHECKED -	REVISED -	SCALE:			SHEET NO. OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				
PLOT DATE = 4/7/2011	DATE -	REVISED -	CONTRACT NO. 60N59									



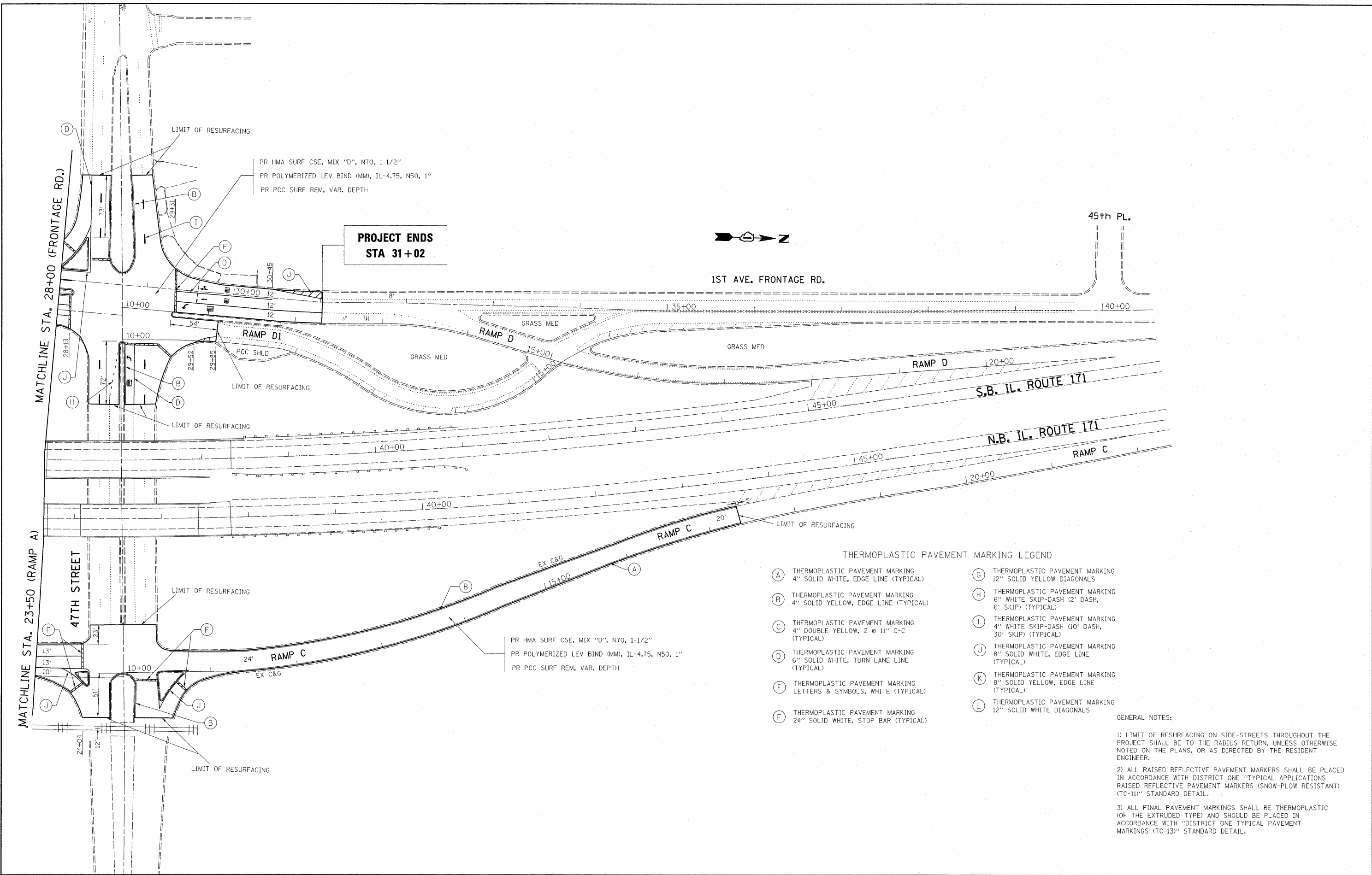
THERMOPLASTIC PAVEMENT MARKING LEGEND

- | | |
|---|--|
| (A) THERMOPLASTIC PAVEMENT MARKING
4" SOLID WHITE, EDGE LINE (TYPICAL) | (G) THERMOPLASTIC PAVEMENT MARKING
12" SOLID YELLOW DIAGONALS |
| (B) THERMOPLASTIC PAVEMENT MARKING
4" SOLID YELLOW, EDGE LINE (TYPICAL) | (H) THERMOPLASTIC PAVEMENT MARKING
6" WHITE SKIP-DASH (2' DASH,
6' SKIP) (TYPICAL) |
| (C) THERMOPLASTIC PAVEMENT MARKING
4" DOUBLE YELLOW, 2 @ 11" C-C
(TYPICAL) | (I) THERMOPLASTIC PAVEMENT MARKING
4" WHITE SKIP-DASH (10' DASH,
30' SKIP) (TYPICAL) |
| (D) THERMOPLASTIC PAVEMENT MARKING
6" SOLID WHITE, TURN LANE LINE
(TYPICAL) | (J) THERMOPLASTIC PAVEMENT MARKING
8" SOLID WHITE, EDGE LINE
(TYPICAL) |
| (E) THERMOPLASTIC PAVEMENT MARKING
LETTERS & SYMBOLS, WHITE (TYPICAL) | (K) THERMOPLASTIC PAVEMENT MARKING
8" SOLID YELLOW, EDGE LINE
(TYPICAL) |
| (F) THERMOPLASTIC PAVEMENT MARKING
24" SOLID WHITE, STOP BAR (TYPICAL) | (L) THERMOPLASTIC PAVEMENT MARKING
12" SOLID WHITE DIAGONALS |

GENERAL NOTES:

- 1) LIMIT OF RESURFACING ON SIDE-STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE NOTED ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER.
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FILE NAME =	USER NAME = tariqfm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 171 - 1st AVE. FRONTAGE ROAD (JOLIET RD. TO 47TH ST.) ROADWAY AND PAVEMENT MARKING PLANS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\pwsdot\tariqfm\d251775\0132	11-sh-t-plan.dgn	DRAWN -	REVISED -			372	2010-155-RS	COOK	27	9	
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -			SCALE: SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 60N59			
	PLOT DATE = 4/7/2011	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



**PROJECT ENDS
STA 31+02**



1ST AVE. FRONTAGE RD.

45th PL.

THERMOPLASTIC PAVEMENT MARKING LEGEND

- (A) THERMOPLASTIC PAVEMENT MARKING
4" SOLID WHITE, EDGE LINE (TYPICAL)
- (B) THERMOPLASTIC PAVEMENT MARKING
4" SOLID YELLOW, EDGE LINE (TYPICAL)
- (C) THERMOPLASTIC PAVEMENT MARKING
4" DOUBLE YELLOW, 2 @ 11" C-C (TYPICAL)
- (D) THERMOPLASTIC PAVEMENT MARKING
6" SOLID WHITE, TURN LANE LINE (TYPICAL)
- (E) THERMOPLASTIC PAVEMENT MARKING
LETTERS & SYMBOLS, WHITE (TYPICAL)
- (F) THERMOPLASTIC PAVEMENT MARKING
24" SOLID WHITE, STOP BAR (TYPICAL)
- (G) THERMOPLASTIC PAVEMENT MARKING
12" SOLID YELLOW DIAGONALS
- (H) THERMOPLASTIC PAVEMENT MARKING
6" WHITE SKIP-DASH (2' DASH, 6' SKIP) (TYPICAL)
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8" SOLID WHITE, EDGE LINE (TYPICAL)
- (K) THERMOPLASTIC PAVEMENT MARKING
8" SOLID YELLOW, EDGE LINE (TYPICAL)
- (L) THERMOPLASTIC PAVEMENT MARKING
12" SOLID WHITE DIAGONALS

GENERAL NOTES:

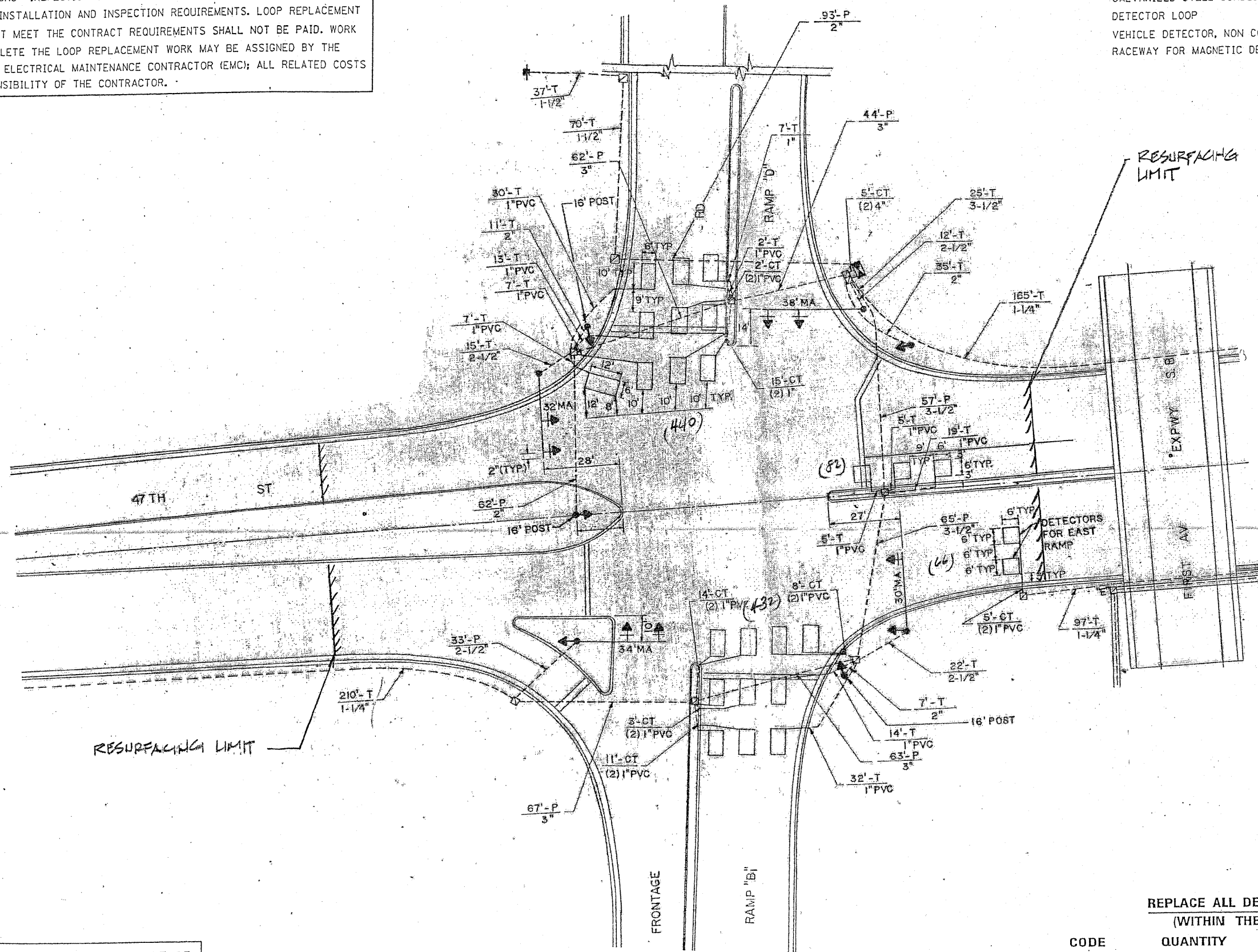
- 1) LIMIT OF RESURFACING ON SIDE-STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE NOTED ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER.
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FILE NAME = c:\pw_work\pwsdat\teriqf\m\0251775\0132\11-sht-plan.dgn	USER NAME = teriqf.m 11-sht-plan.dgn	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 171 - 1st AVE. FRONTAGE ROAD (JOLIET RD. TO 47TH ST.) ROADWAY AND PAVEMENT MARKING PLANS	F.A.P. RTE. 372	SECTION 2010-155-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 10
PLOT SCALE = 50,0000' / IN. PLOT DATE = 4/7/2011		CHECKED - DATE -	REVISED - REVISED -	SCALE: SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 60N59 ILLINOIS FED. AID PROJECT				

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE	
SIGNAL HEAD	
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	
DETECTOR LOOP	
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	



NORTH

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	1,020	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME *	USER NAME * nguyensm	DESIGNED -	REVISED -
c:\pwork\pvidot\nguyensm\120112618\11010101.dgn		DRAWN -	REVISED -
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = 12/1/2010		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

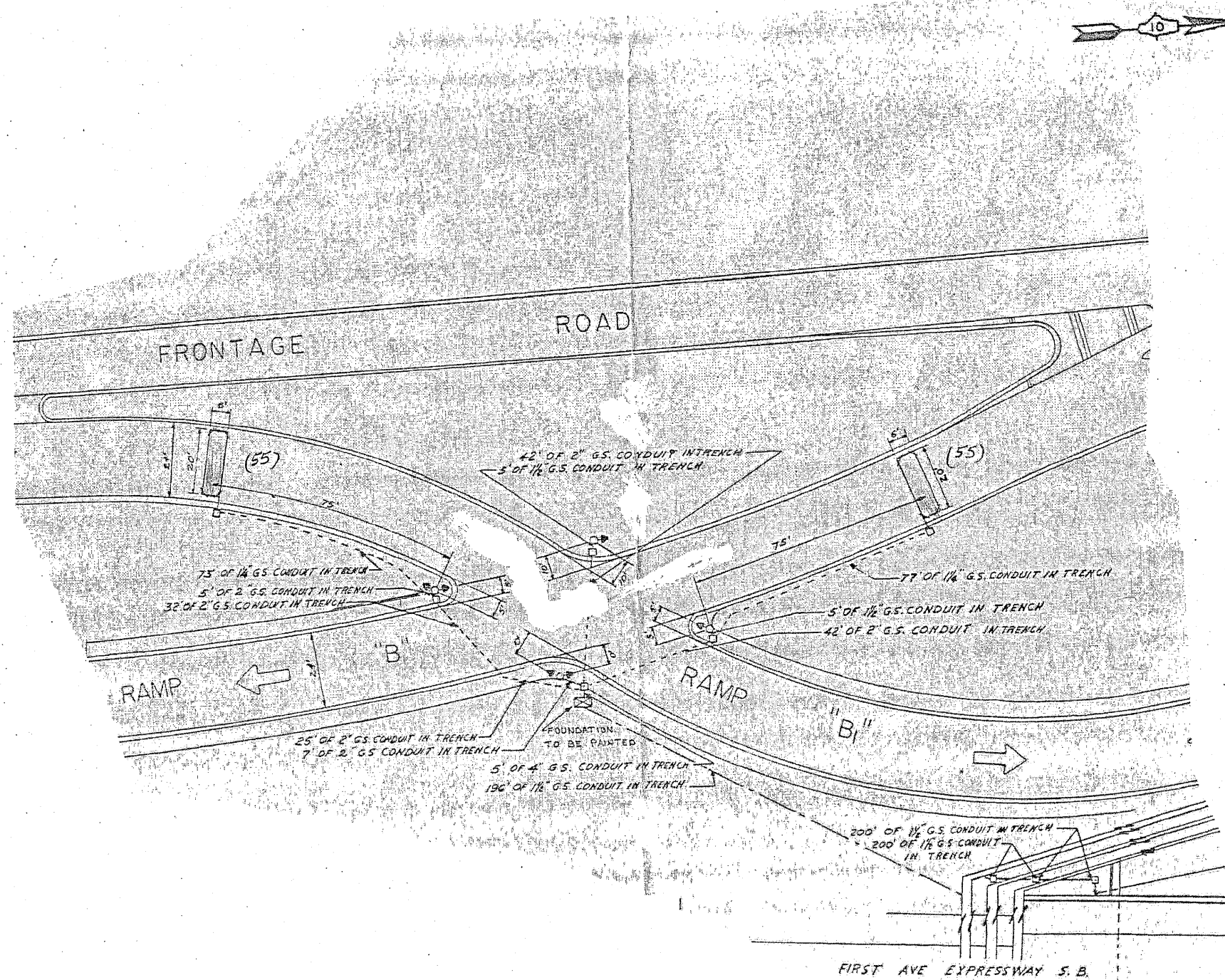
DISTRICT ONE - DETECTOR LOOP REPLACEMENT
47TH ST. @ ILL. RT. 171 - RAMP D

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	2010-155-RS	COOK	27	11
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		CONTRACT NO. 60NS9		

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

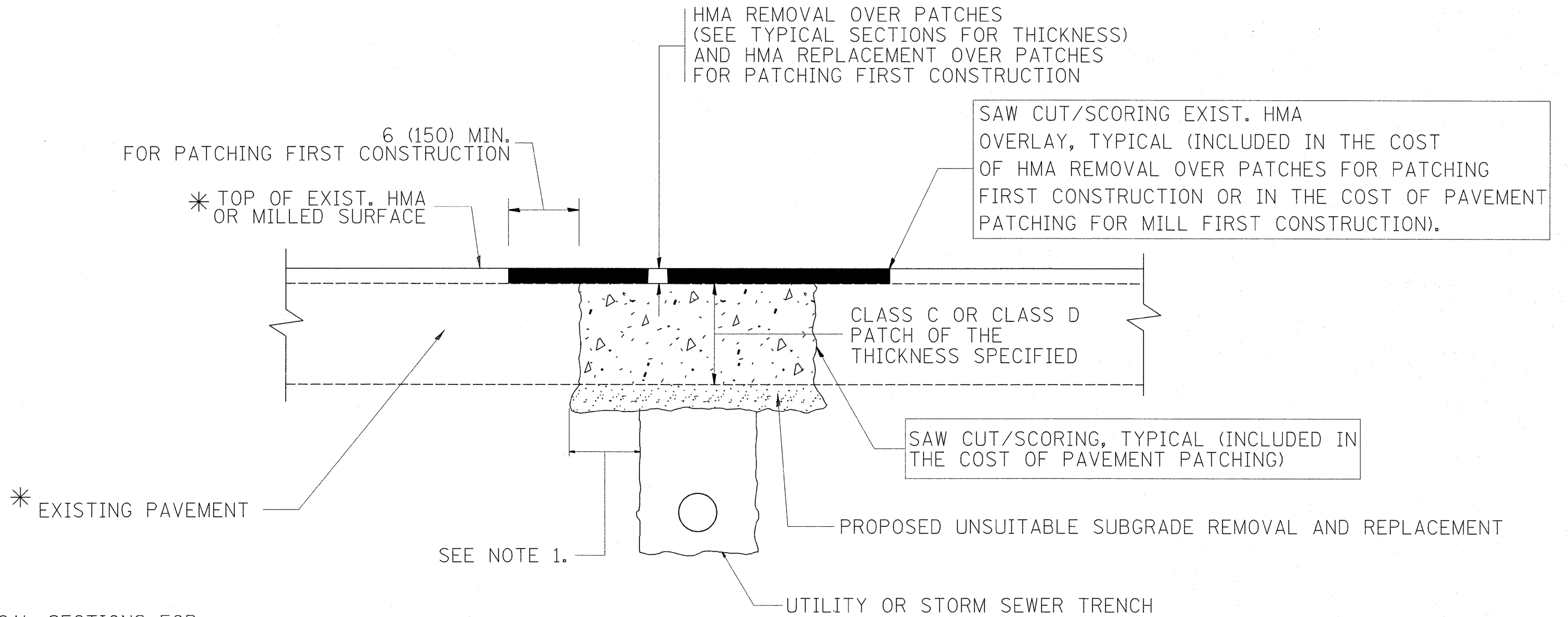
CODE	QUANTITY	UNIT	ITEM
88600600	110	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME *	USER NAME * nguyensm	DESIGNED -	REVISED -
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PLOT SCALE * 1/80.0000 "/td> <td></td> <td>CHECKED -</td> <td>REVISED -</td>		CHECKED -	REVISED -
PLOT DATE * 12/1/2010		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - DETECTOR LOOP REPLACEMENT
I-171 (FRONTAGE) RAMP B & B1

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	2010-155-R5	COOK	27	13
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 60N59	



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = tariqfm	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE. 372	SECTION 2010-155-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 14
es:\pw_work\pwsdot\tariqfm\d0251775\Dist5.dgn		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD400-04 (BD-22)		CONTRACT NO. 60N59	
		CHECKED -	REVISED - R. BORO 09-04-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE - 10-25-94	REVISED - K. ENG 10-27-08									

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

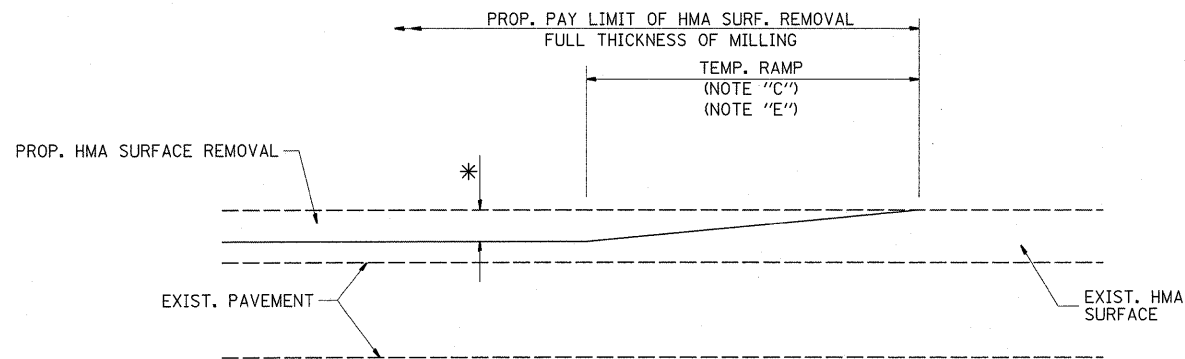
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

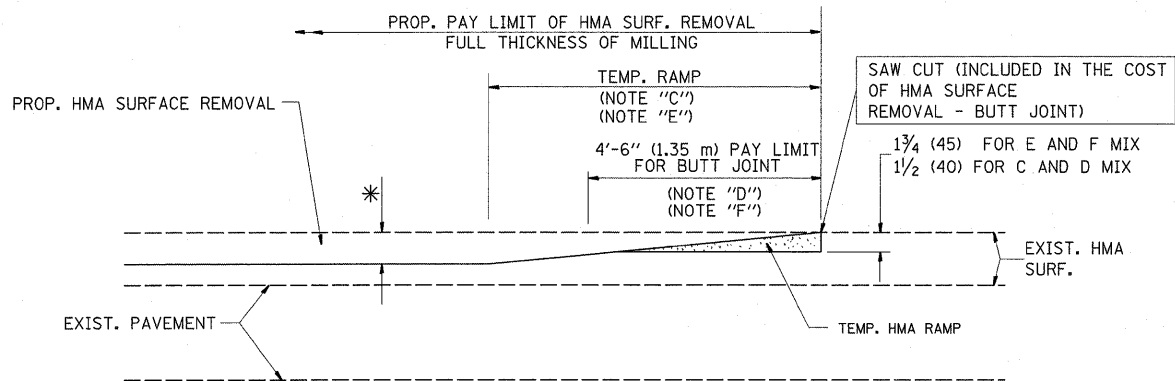
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = tariqfm	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.P. RTE. 372	SECTION 2010-155-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 15
ci:\pw_work\pwidot\tariqfm\d0251775\DistStd.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			BD600-06 (BD-24) CONTRACT NO. 60N59				
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - M. GOMEZ 01-22-01		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
PLOT DATE = 4/7/2011		DATE - 03-11-94	REVISED - R. BORO 12-15-09									



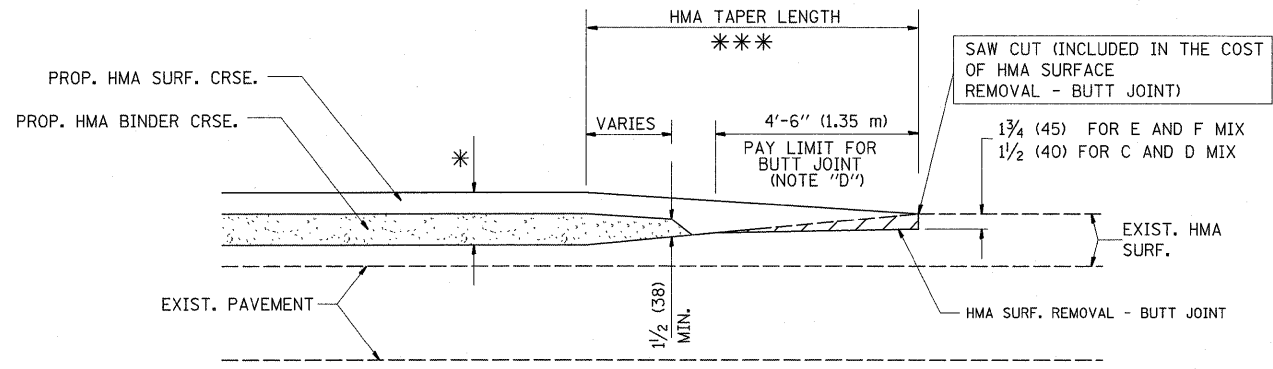
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

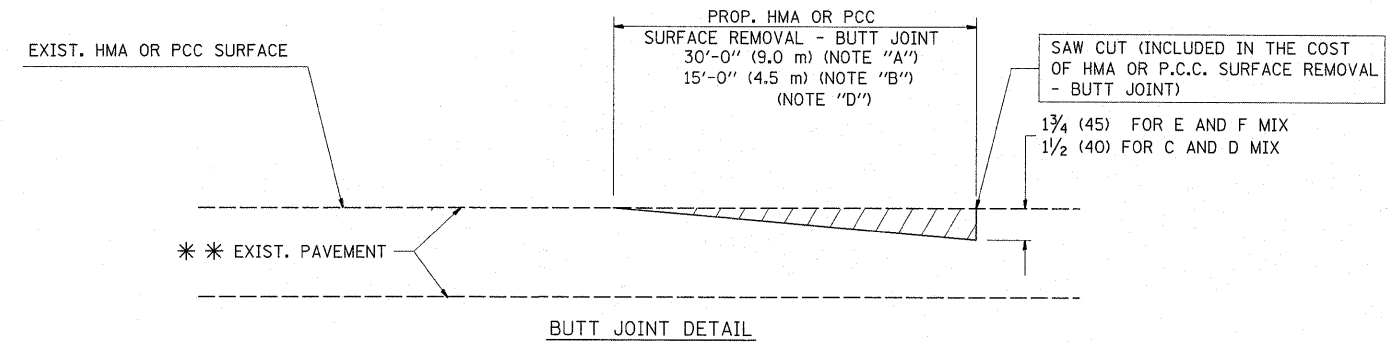


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

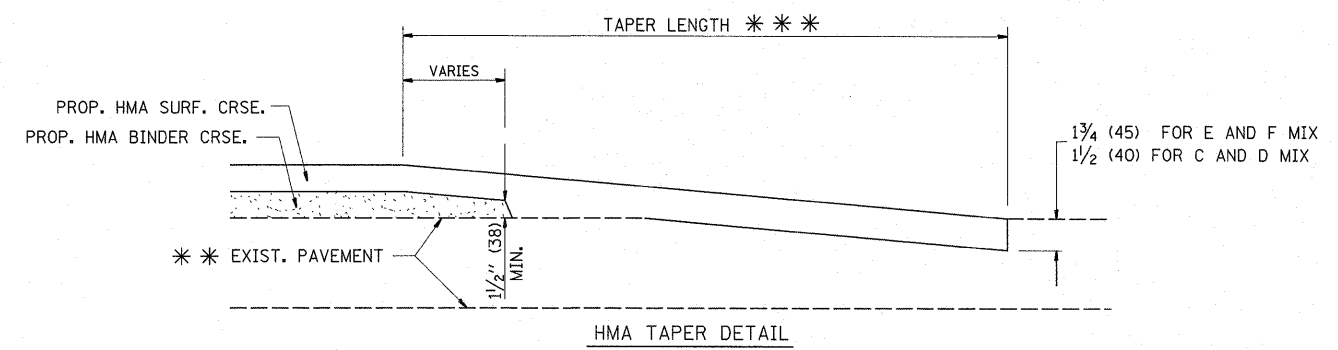
OPTION 2
TYPICAL TEMPORARY RAMP



**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

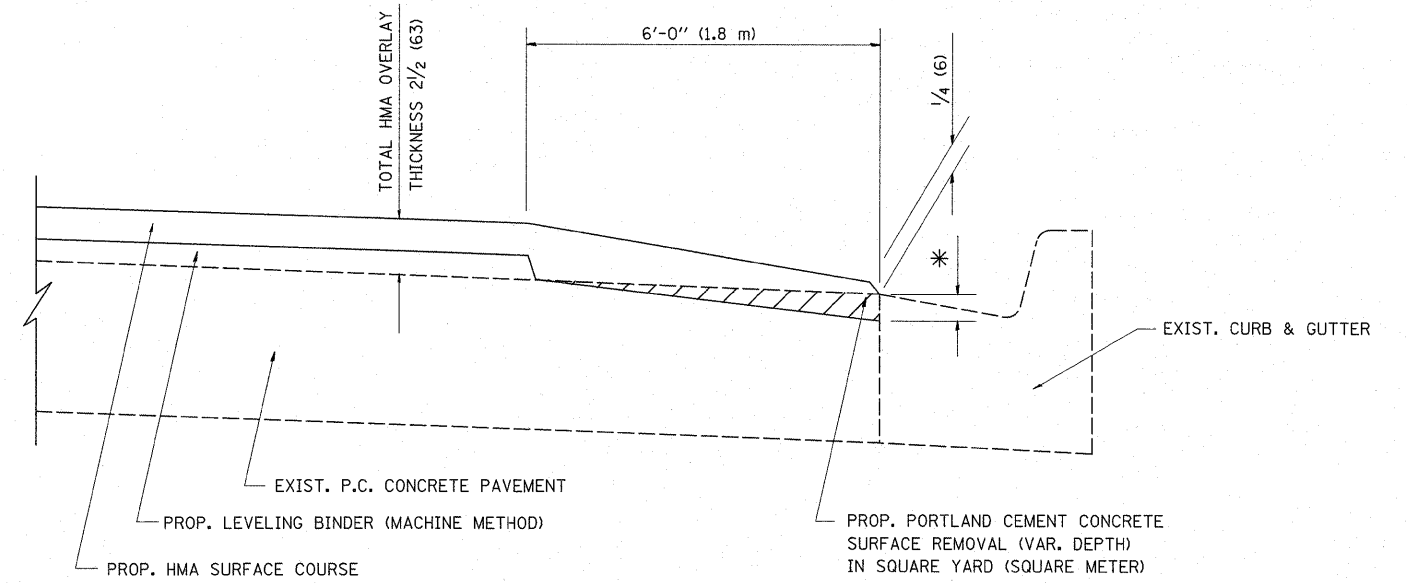
- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = tariqf	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS			F.A.P. RTE. 372	SECTION 2010-155-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 16
cs:\pw_work\pwidot\tariqf\m\0251775\0\st	adgn	DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD400-05 BD32		CONTRACT NO. 60N59	
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
	PLOT DATE = 4/7/2011	DATE - 06-13-90	REVISED - R. BORO 01-01-07									



HMA TAPER AT
EDGE OF P.C.C. PAVEMENT

HMA SURFACE	LEVELING BINDER	* MILLING AT GUTTER FLAG
MIX	THICKNESS	THICKNESS
C OR D	1 1/2 (38)	1 (25)
F	1 3/4 (44)	3/4 (19)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

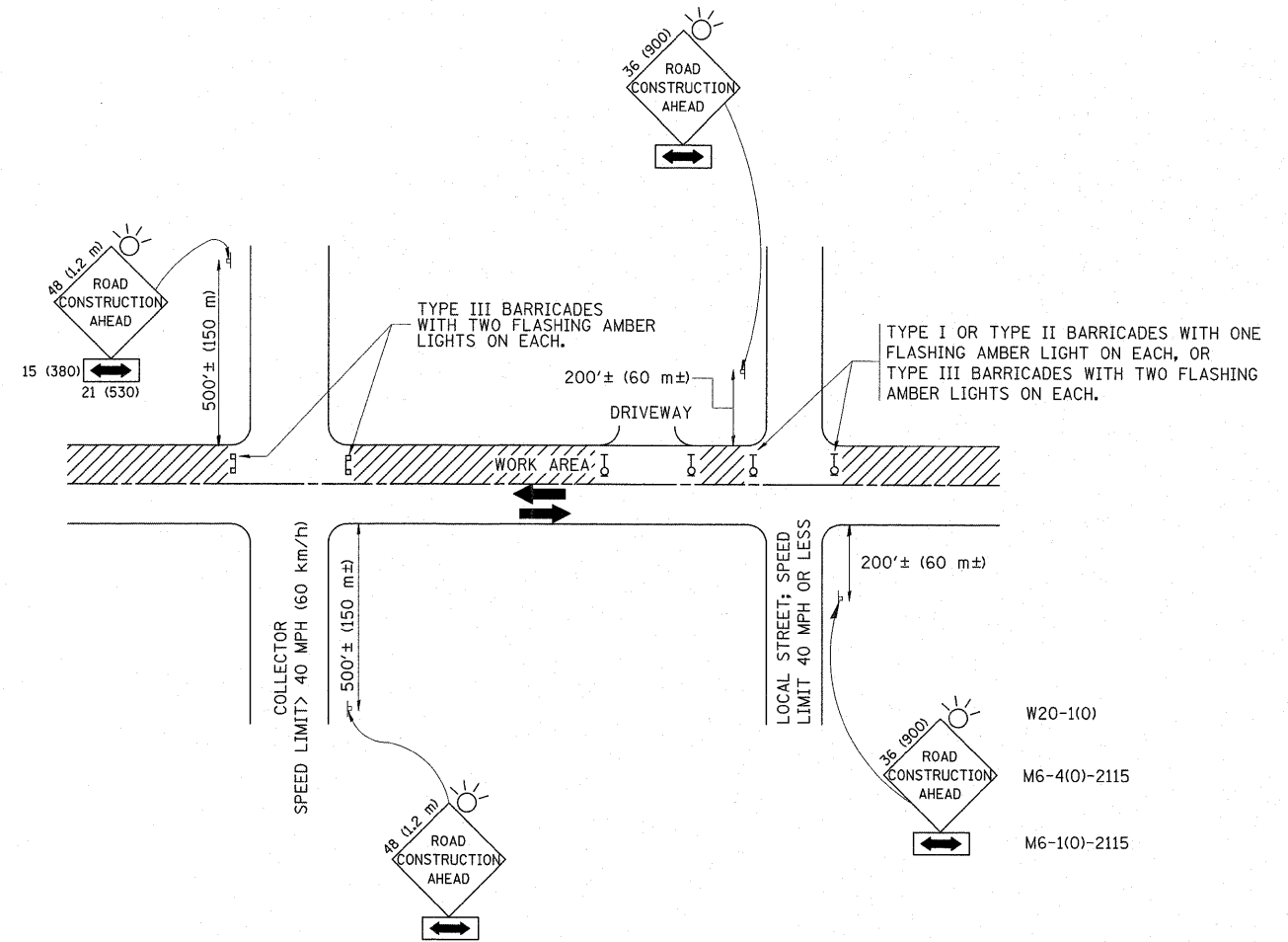
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PLOT SCALE = 50.0000' / IN.		CHECKED - A. ABBAS	REVISED - E. GOMEZ 12-21-00
PLOT DATE = 4/7/2011		DATE - 09-10-94	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HMA TAPER AT
EDGE OF P.C.C. PAVEMENT

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	2010-155-RS	COOK	27	17
BD400-06 (BD33)		CONTRACT NO. 60N59		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (Inches) unless otherwise shown.

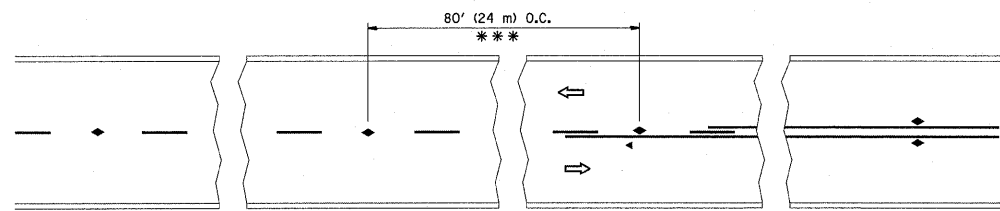
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 4/7/2011	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

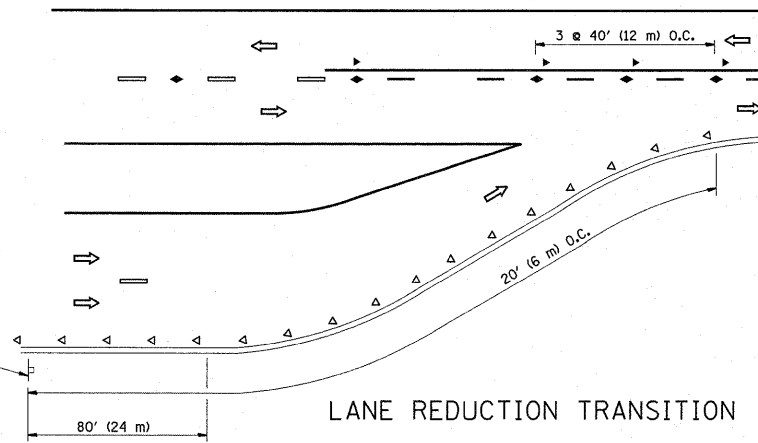
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

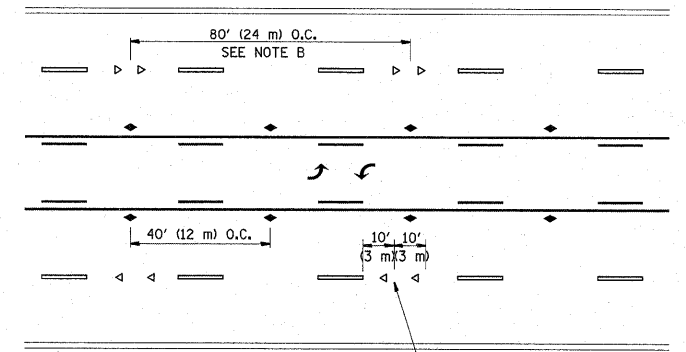
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TC-10			CONTRACT NO. 60N59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



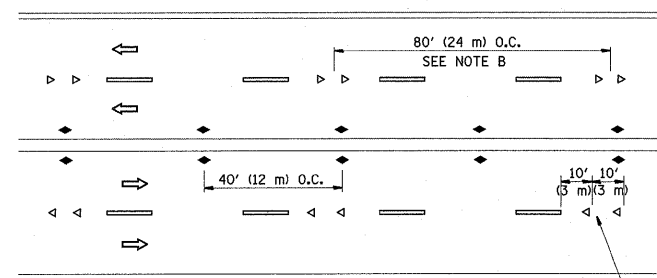
TWO-LANE/TWO-WAY



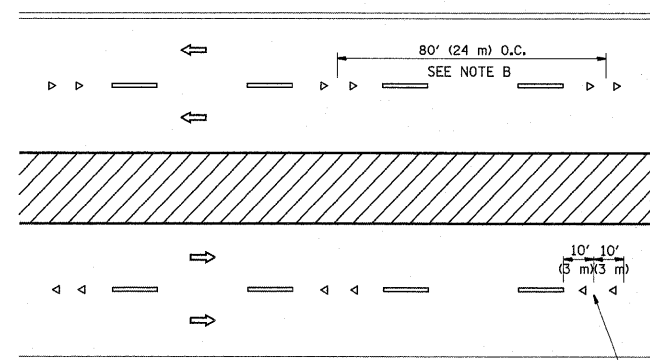
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

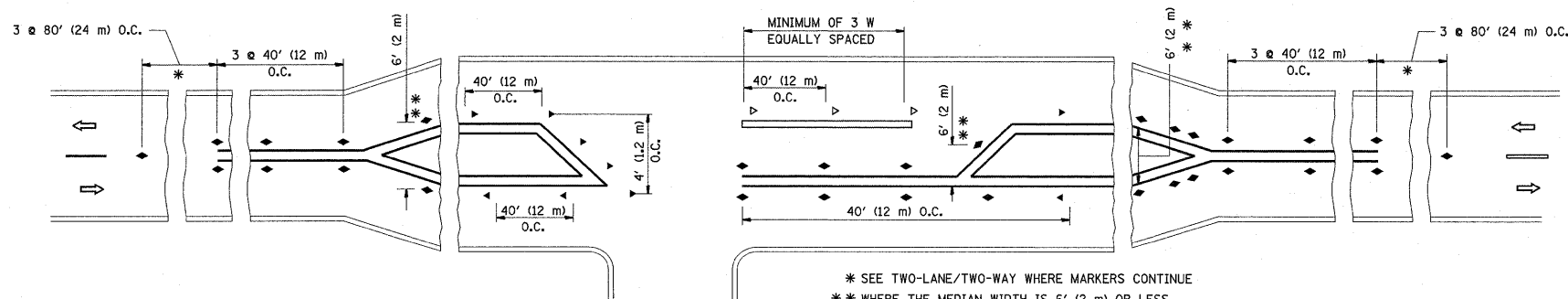
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

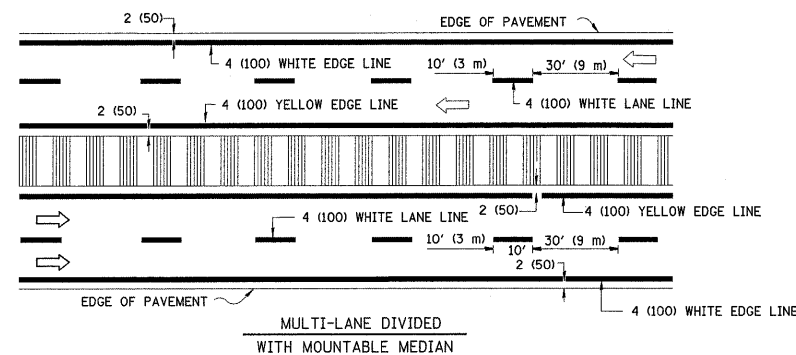
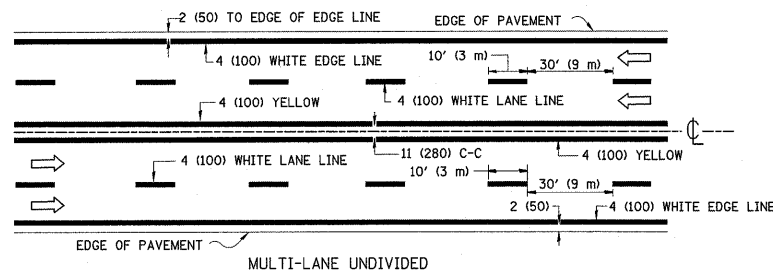
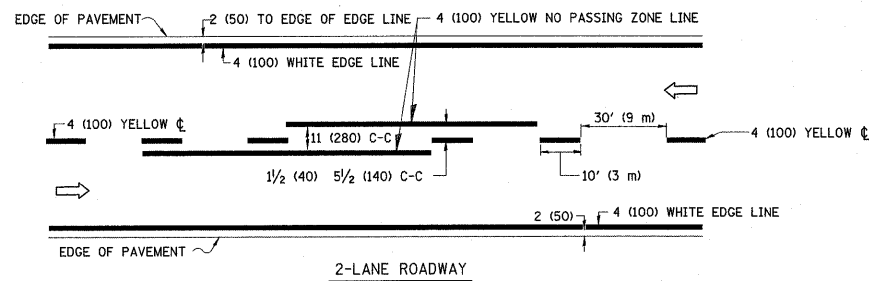
1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

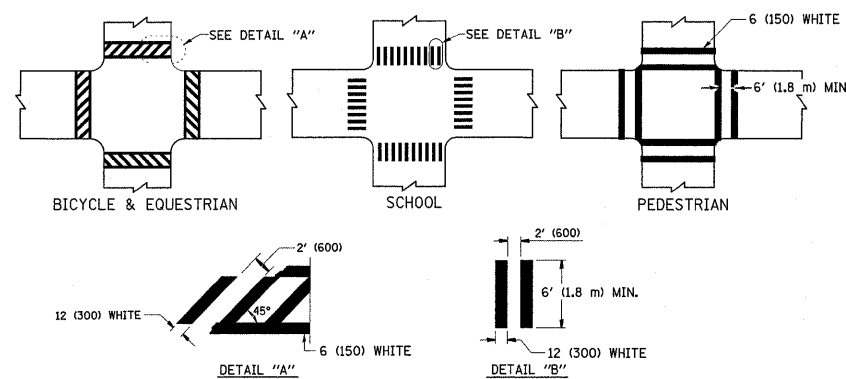
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = tariqfm	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pw_work\pwidot\tariqfm\d0251775\Dist5Std.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99		RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)		372	2010-155-RS	COOK	27	19
		CHECKED -	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-11		CONTRACT NO. 60N59
		DATE -	REVISED - C. JUCIUS 09-09-09						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

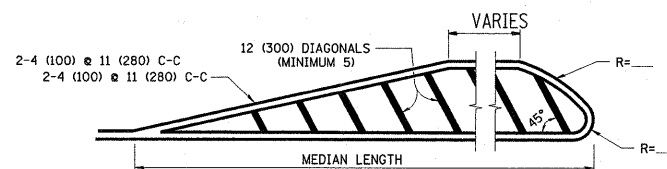
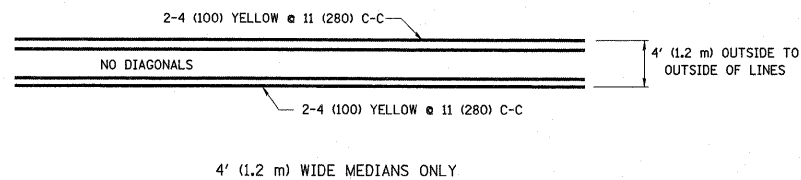


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

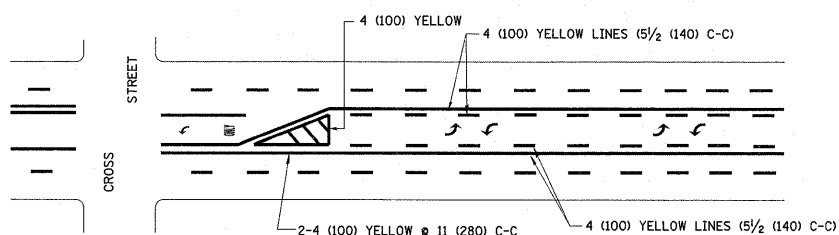


TYPICAL CROSSWALK MARKING

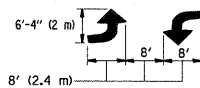


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

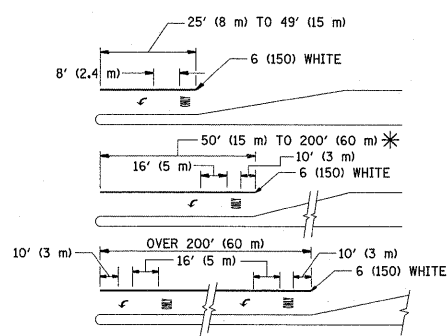


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

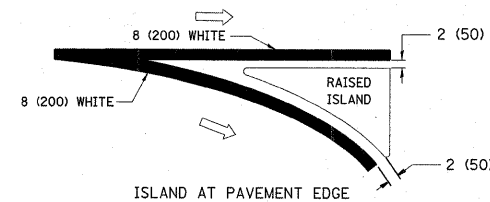
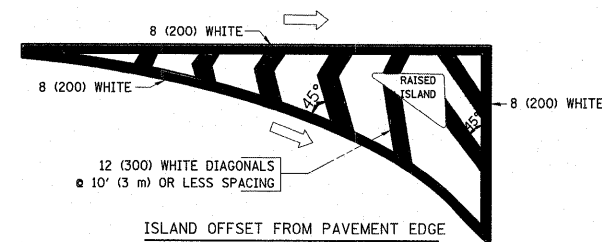


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))

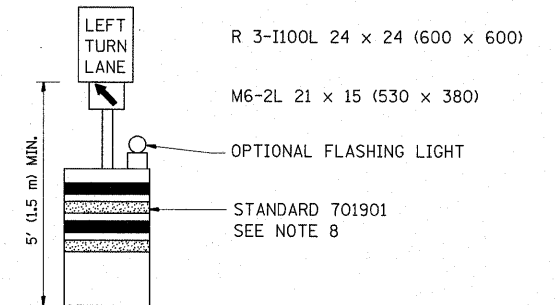
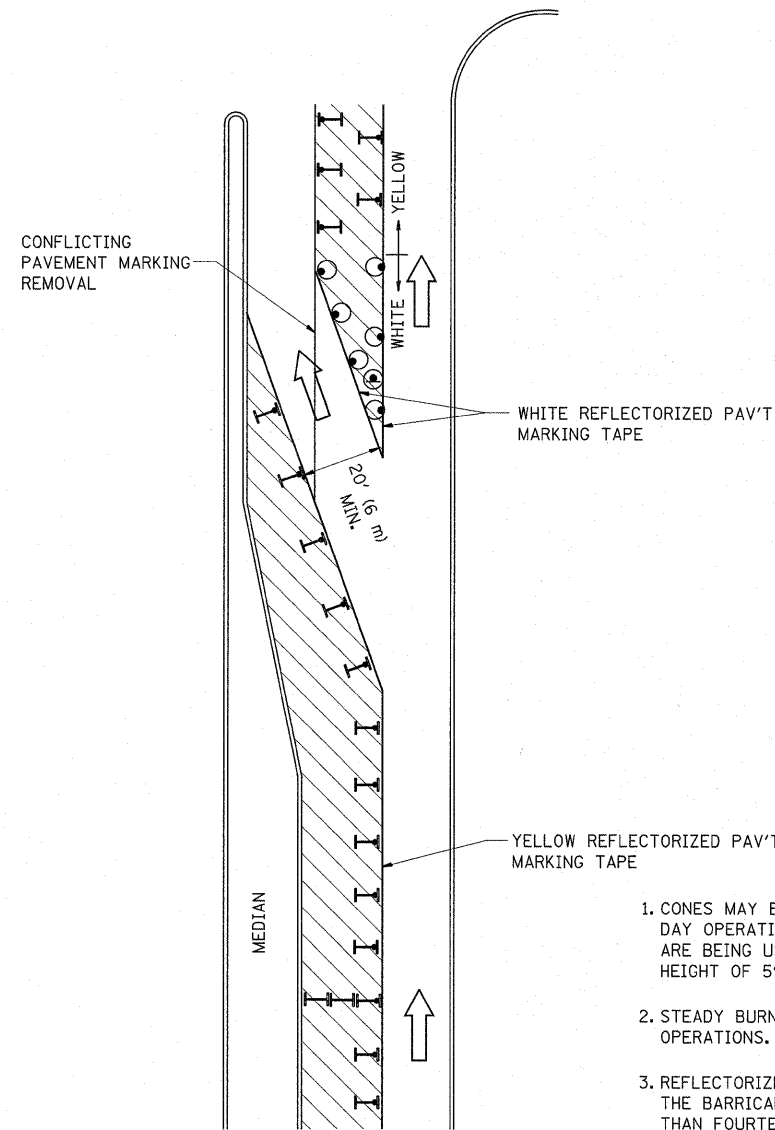
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = tariqfm	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
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	PLOT DATE = 4/7/2011	DATE - 03-19-90	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE		F.A.P. RTE. 372	SECTION 2010-155-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 20
TYPICAL PAVEMENT MARKINGS		TC-13		CONTRACT NO. 60N59		
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT			









GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

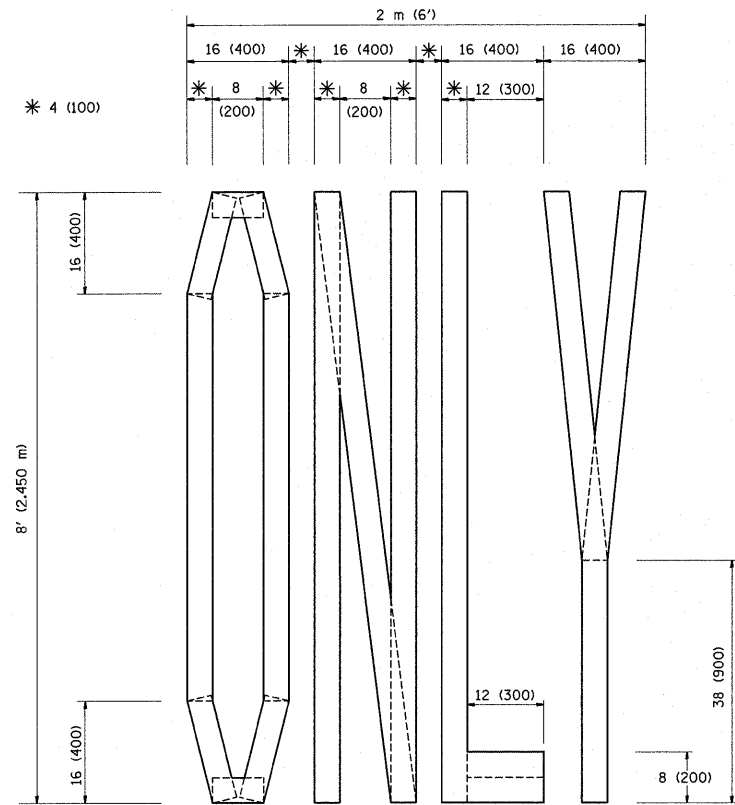
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PLOT SCALE = 50.0000' / IN.		REVISED - A. HOUSEH 10-12-96	REVISED -
PLOT DATE = 4/7/2011		REVISED -T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

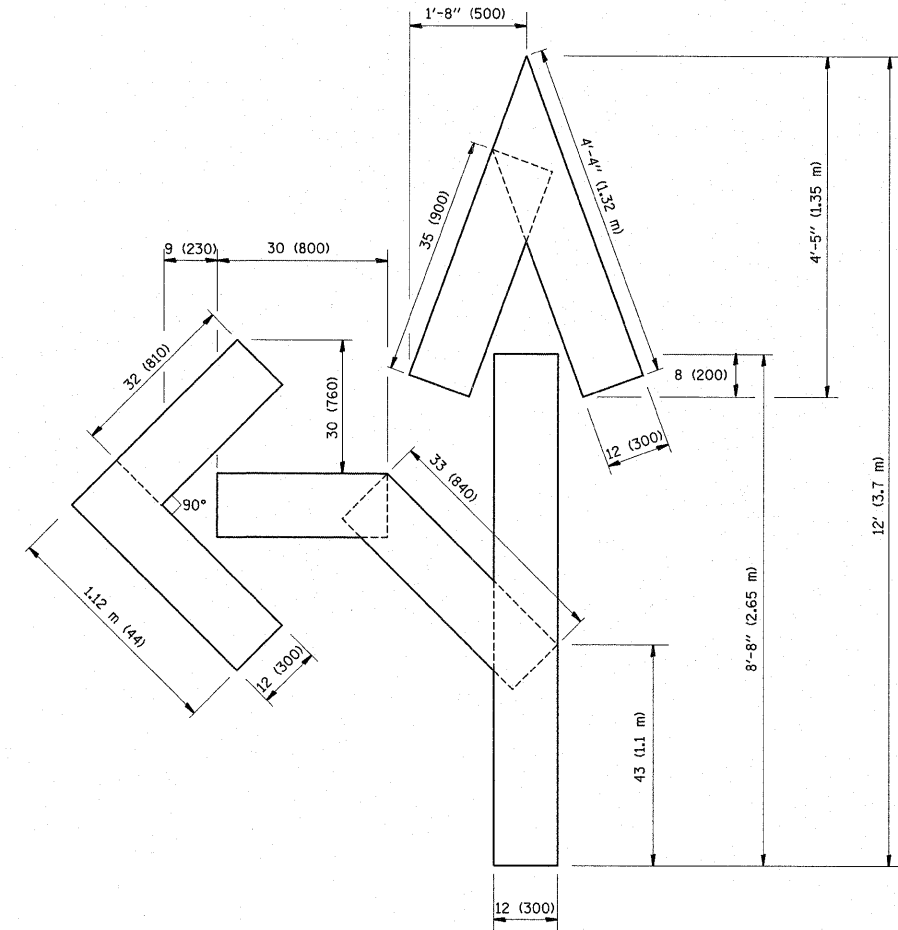
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

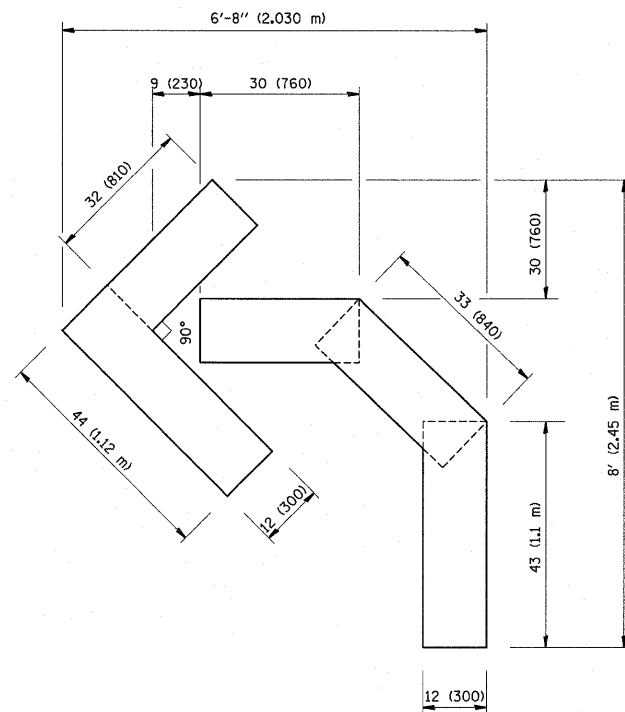
F.A.P. RTE. 372	SECTION 2010-155-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 21
TC-14			CONTRACT NO. 60N59	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = tariqm	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
c:\pwork\pwork\dot\tariqm\d0251775\DistS	drawn	DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000" / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 4/7/2011	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

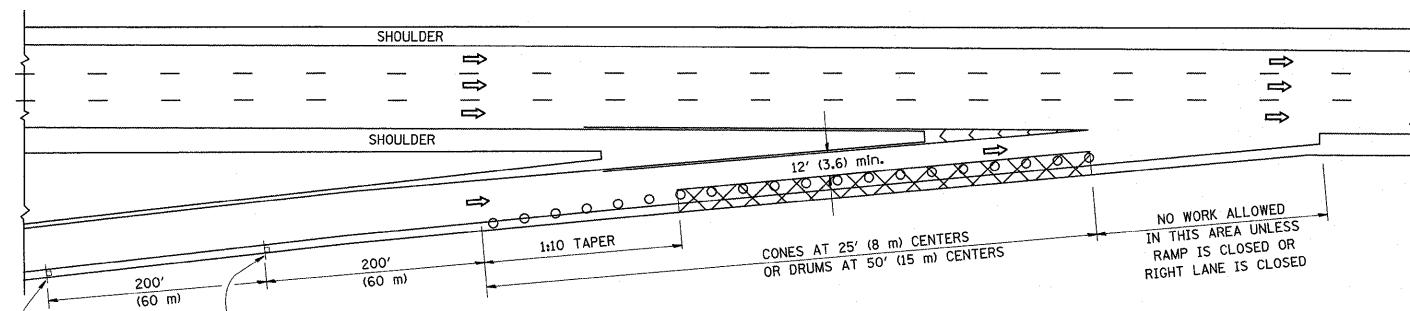
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			
SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA. TO STA.

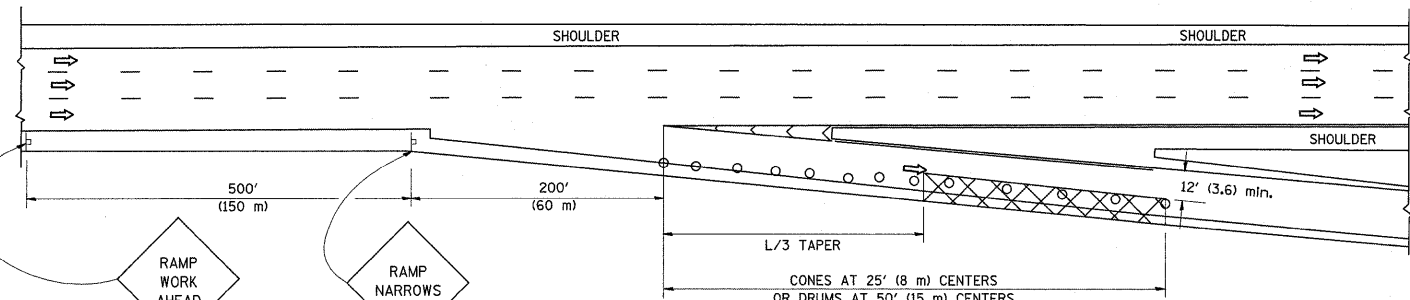
F.A.P. RTE. 372	SECTION 2010-155-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 22
TC-16			CONTRACT NO. 60N59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PARTIAL RAMP CLOSURE DETAILS

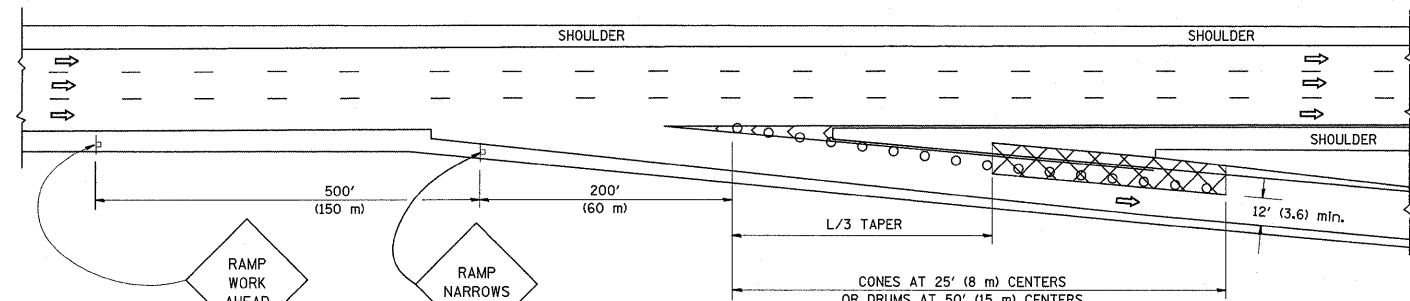
SHOULDER CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

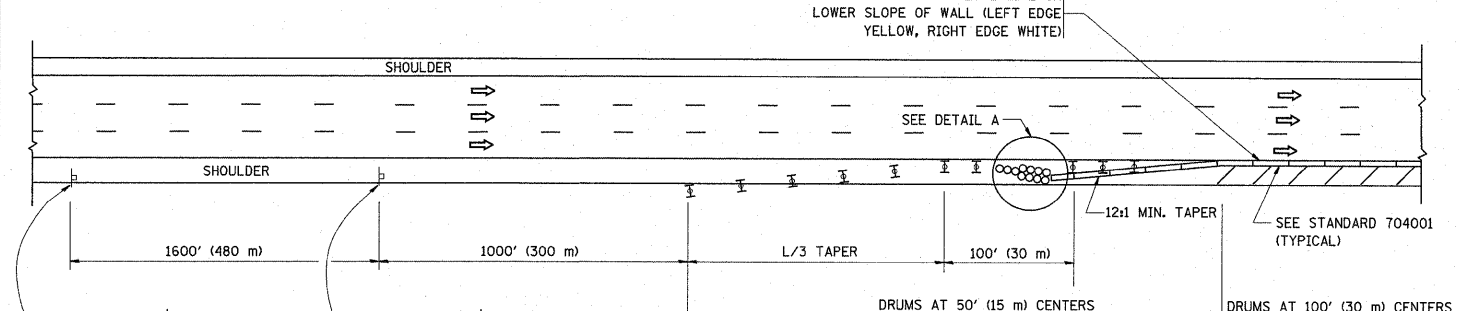
SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

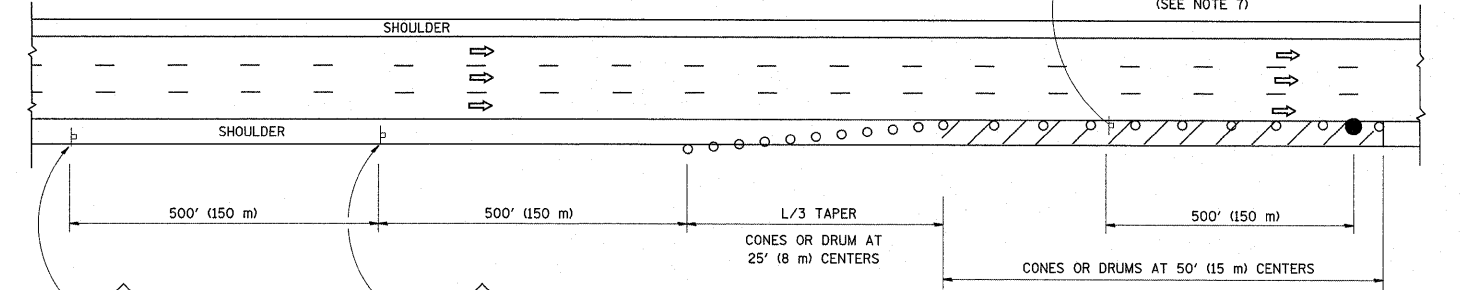
GENERAL NOTES

1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC: $L=0.65(W)(S)$ ENGLISH: $L=(W)(S)$
	W = WIDTH OF OFFSET IN FEET (METERS)
	S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

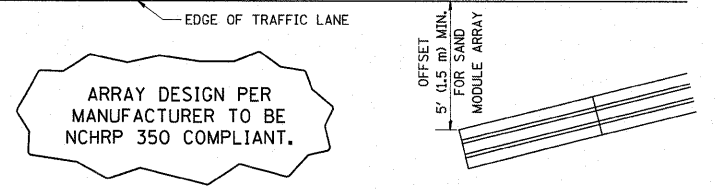


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
 1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCRANCH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.

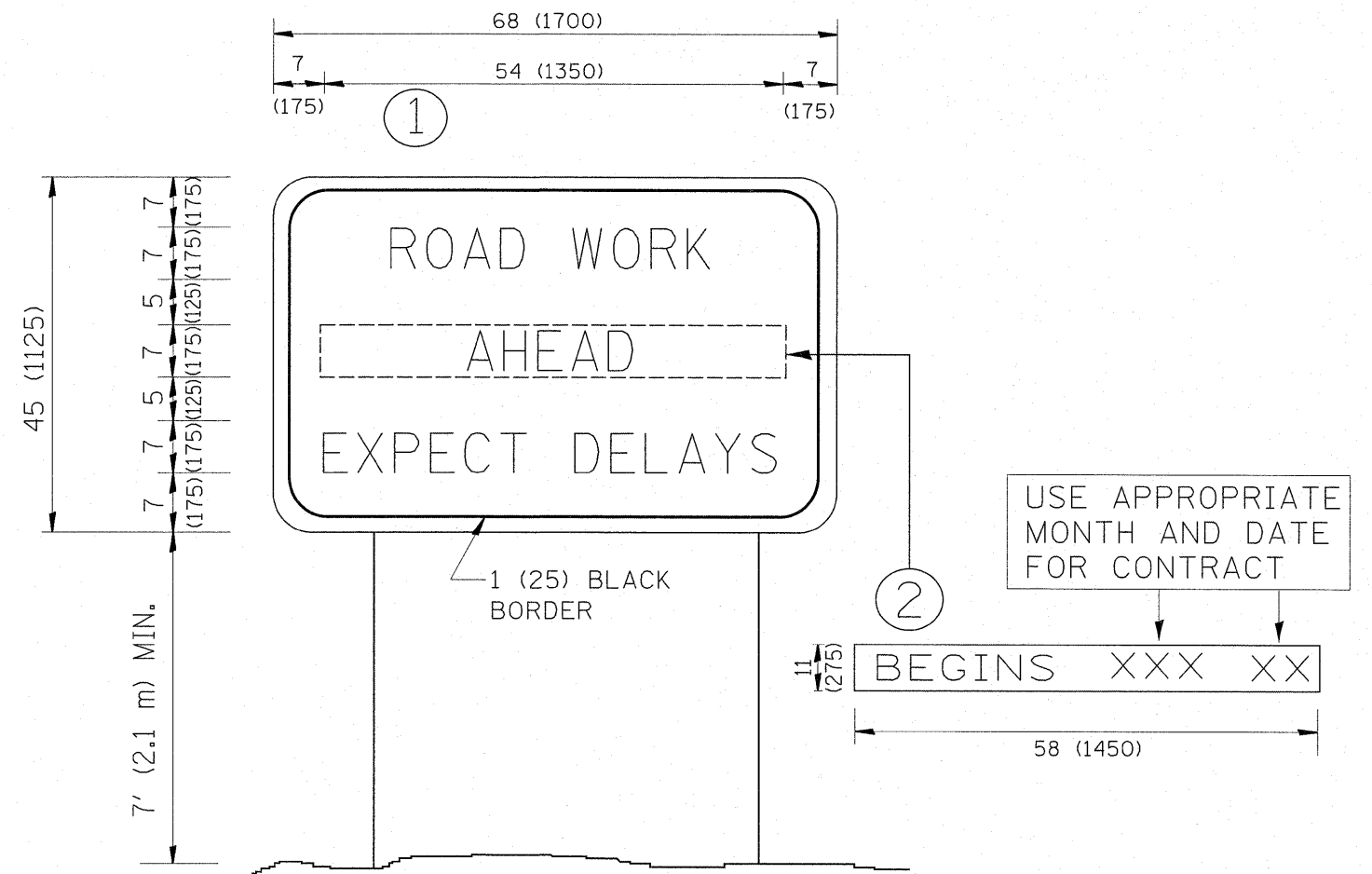


DETAIL "A"
 IMPACT ATTENUATOR, TEMPORARY
 (SEE NOTE 5)

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = tarigfm	DESIGNED -	REVISED - 04-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES		F.A.P. RTE. 372	SECTION 2010-155-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 23	
	PLT SCALE = 50,0000' / IN.	DRAWN - D.W.S.	REVISED - J.A.F. 12-06		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-17			
	PLT DATE = 4/7/2011	CHECKED -	REVISED - S.P.B. 01-07		FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT							
		DATE - 11-96	REVISED - S.P.B. 12-09		CONTRACT NO. 60N59							



NOTES:

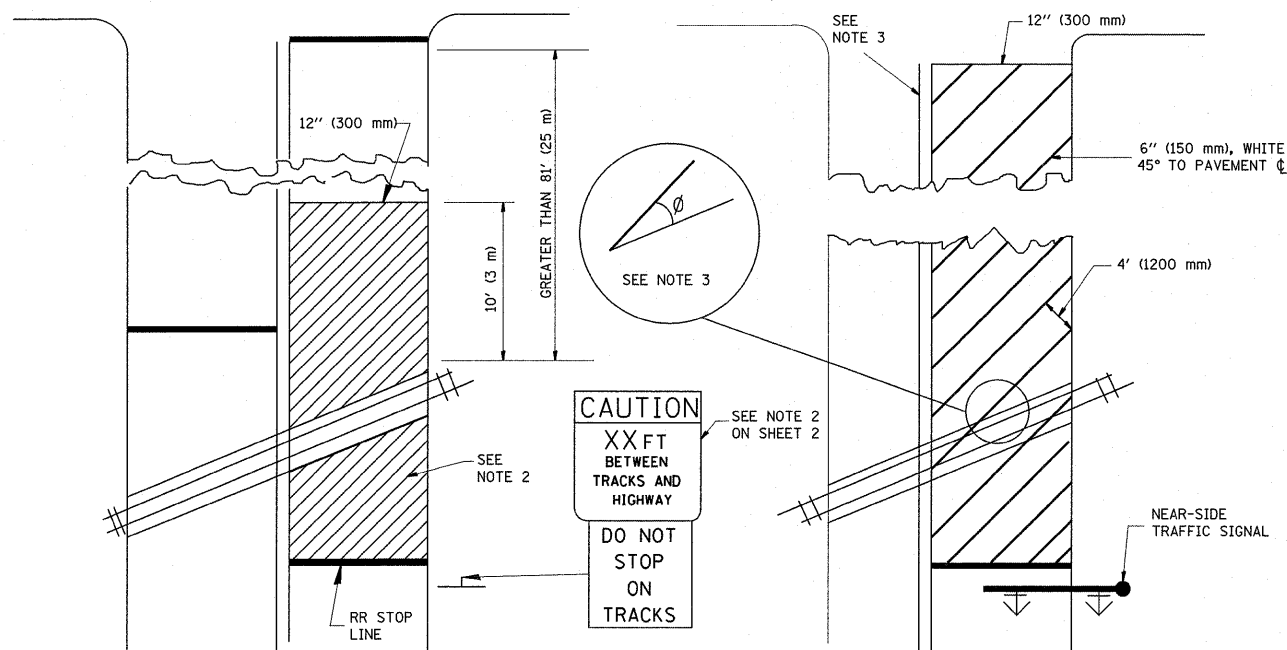
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = tariqf'm	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN		F.A.P. RTE. 372	SECTION 2010-155-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 24
ct:\pw_work\pwsdot\tariqf'm\0251775\DistStd.dgn		DRAWN -	REVISED - R. MIRS 12-11-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-22		CONTRACT NO. 60N59
		CHECKED -	REVISED - T. RAMMACHER 02-02-99		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						
		DATE -	REVISED - C. JUCIUS 01-31-07								

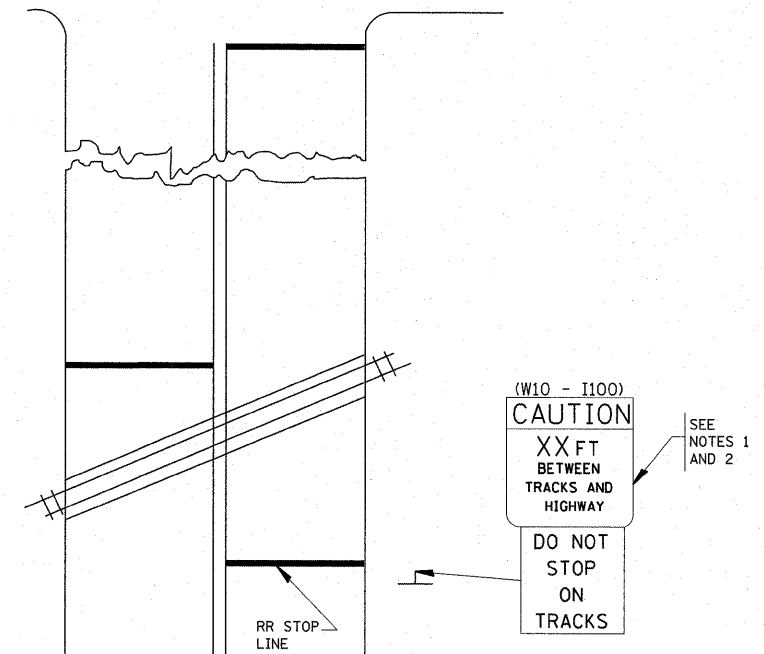
WITH INTERSECTION TRAFFIC SIGNALS

WITH NEAR-SIDE TRAFFIC SIGNALS



WITH NONSIGNALIZED INTERSECTION

81' (25 m) OR LESS TO CLOSEST RAIL



NOTES:

- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED, THE PAVEMENT MARKINGS EXTENDS TO THE INTERSECTION.
- WHERE THE ANGLE BETWEEN THE DIAGONAL STRIPES AND THE TRACK (θ) WOULD BE LESS THAN APPROXIMATELY 20°, THE STRIPES SHOULD BE SLOPED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.

NOTE :

- DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET (1.8 m) FROM THE RAIL CLOSEST TO THE INTERSECTION TO THE STOP LINE OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET (1.5 m). WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE THE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
- THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6-FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = tariqfm	DESIGNED -	REVISED - 01-01-07
es:\pw\work\p\dot\tariqfm\d0251775\01st5hd.dgn		DRAWN -	REVISED -
PLOT SCALE = 50,0000 ' / IN.		CHECKED -	REVISED -
PLOT DATE = 4/7/2011		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

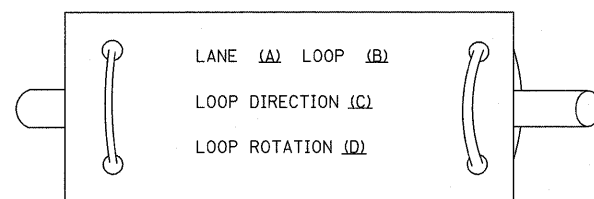
TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE. 372	SECTION 2010-155-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 25
TC-23			CONTRACT NO. 60N59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

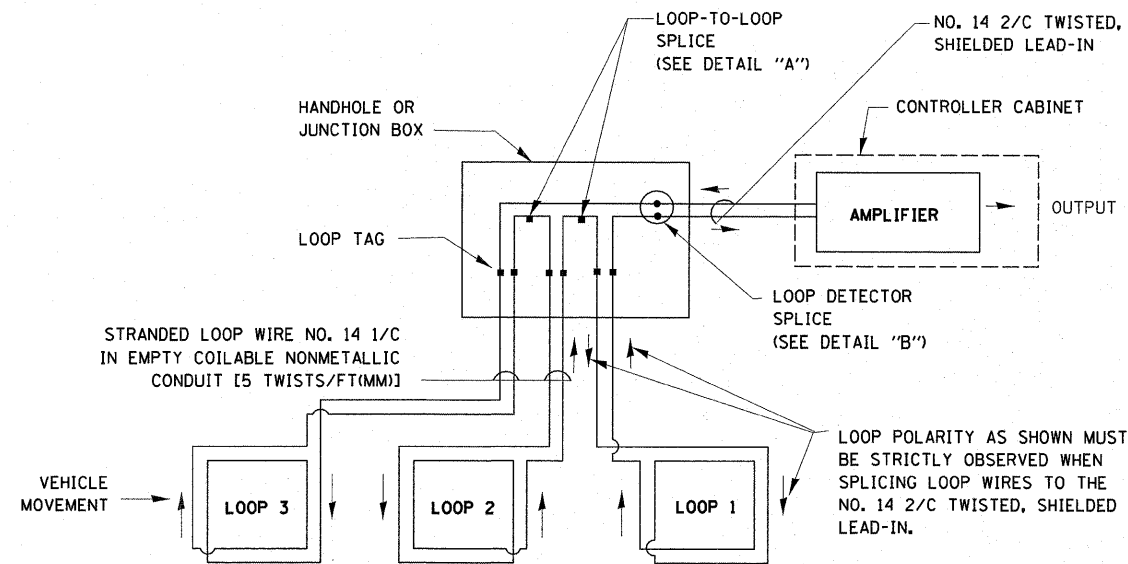
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

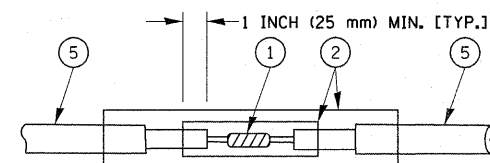


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

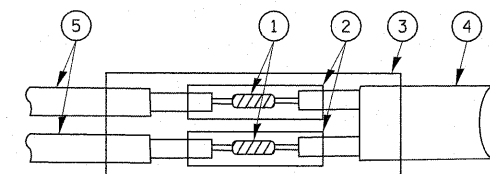


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

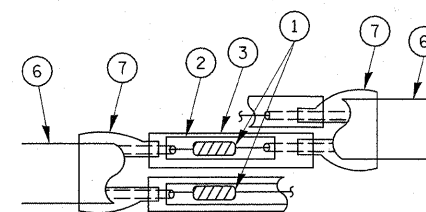


DETAIL "A" LOOP-TO-LOOP SPLICE

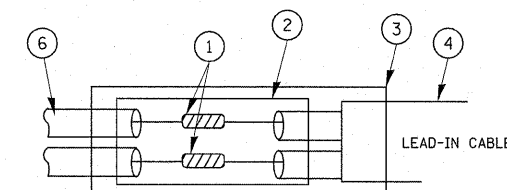


DETAIL "B" LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A" LOOP-TO-LOOP SPLICE



DETAIL "B" LOOP-TO-CONTROLLER SPLICE

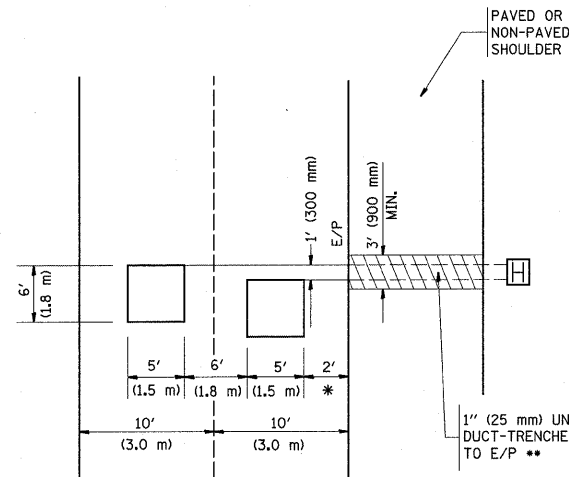
LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = tariqf	DESIGNED - DAD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A.P. RTE. 372	SECTION 2010-155-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 26
es:\pw_work\pwsdot\tariqf\20251775\01st15.dgn		DRAWN - BCK	REVISED -		SCALE: NONE	SHEET NO. 1 OF 6 SHEETS	STA. TO STA.	TS-05		CONTRACT NO. 60N59		
		CHECKED - DAD	REVISED -									
		DATE - 10-28-09	REVISED -									
		PLOT SCALE = 50:0000' / IN.										
		PLOT DATE = 4/7/2011										
								FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

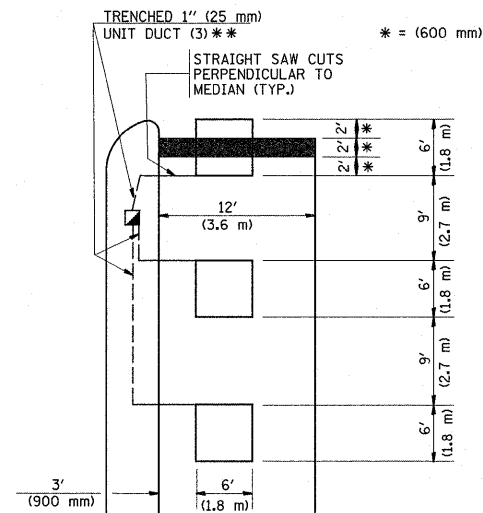


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

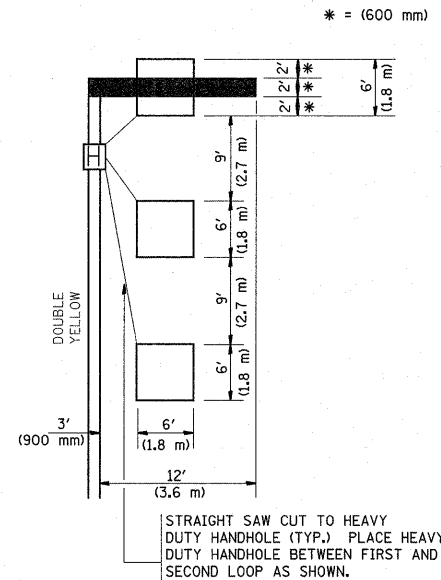
HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

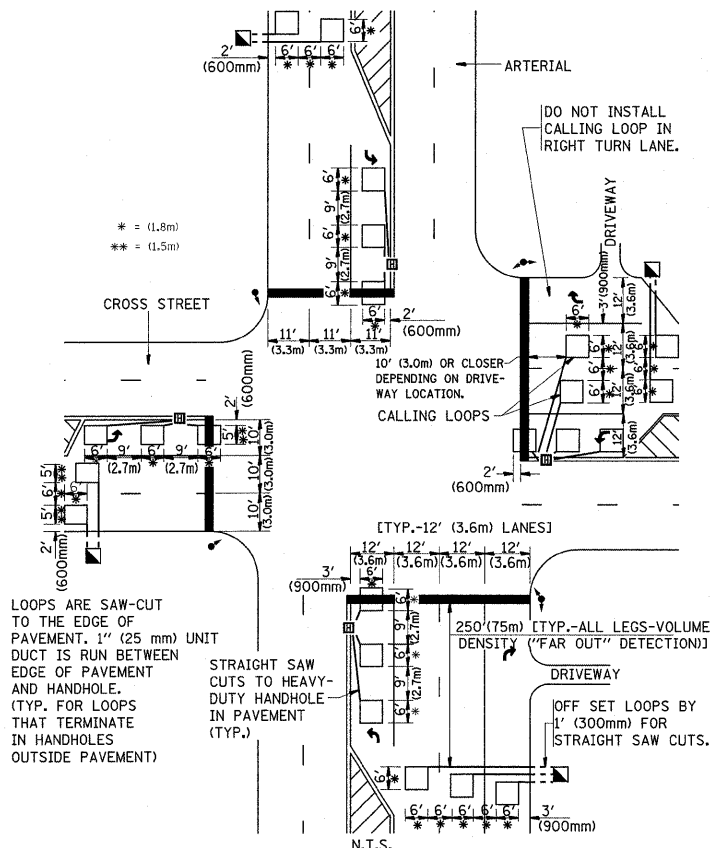
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



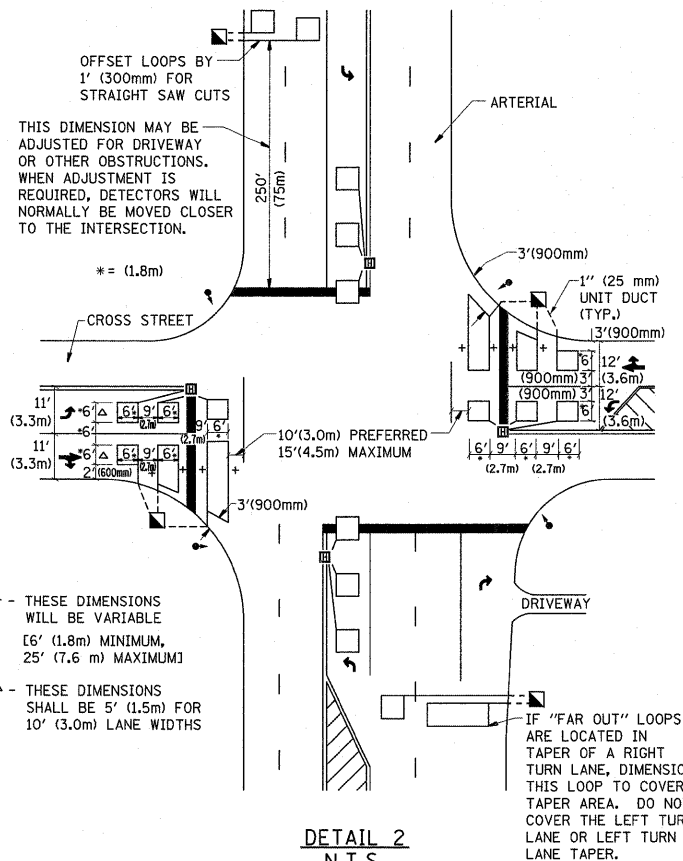
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = tariqf	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING			F.A.P. RTE. 372	SECTION 2010-155-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 27
ce:\pw_work\pw\dot\tariqf\m\0251775\Dist1\td.dgn		DRAWN -	REVISED -		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			TS-07		CONTRACT NO. 60N59		
		CHECKED - R.K.F.	REVISED -					FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT		
		DATE - 4/7/2011	REVISED -									