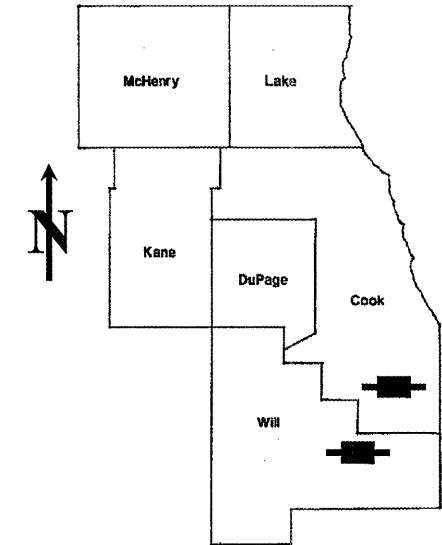


ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NUMBER
VARIOUS	2011-015-RS	COOK & WILL	34	1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
DISTRICT ONE
PROPOSED HIGHWAY PLANS

CONTRACT NO. 60P04

D-91-446-11



LOCATION OF IMPROVEMENT INDICATED THUS: 

FOR INDEX OF SHEETS SEE SHEET 2

VARIOUS ROUTES
 SECTION: 2011-015-RS
 VARIOUS LOCATIONS IN SOUTHERN COOK & WILL COUNTIES
 INTERMITTENT PAVEMENT RESURFACING
 COOK & WILL COUNTIES
 C-91-446-11

DISTRICT ONE - DESIGN - PLAN PREPARATION ENGINEER:
 KEN ENG / (847) 705-4247

CONTRACT NO. 60P04

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
SUBMITTED:	<u>APRIL 6,</u> 20 <u>11</u>
	<u>Diane M. O'Keefe</u> DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
	<u>May 13</u> 20 <u>11</u>
<u>acting</u>	<u>Scott E. Stett, P.E.</u> ENGINEER OF DESIGN AND ENVIRONMENT
	<u>May 13</u> 20 <u>11</u>
	<u>Christine M. Reed</u> DIRECTOR, DIVISION OF HIGHWAYS

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

J.U.L.I.E.: JOINT UTILITY LOCATION
INFORMATION FOR EXCAVATION
(312) 744-7000

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

SHEET NO.	DESCRIPTION	STANDARD NO.	DESCRIPTION
1	TITLE SHEET	000001-06	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	701011-02	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
3	SUMMARY OF QUANTITIES	701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
4-5	GENERAL LOCATION MAP	701306-03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS - DAY ONLY
6	SUMMARY OF PATCHING SCHEDULE	701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
7-19	PATCHING SCHEDULE	701336-06	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES
20	BUTT JOINT AND HMA TAPER DETAILS (BD-32)	701400-05	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
21	FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-08)	701401-06	LANE CLOSURE, FREEWAY/EXPRESSWAY
22	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE (TC-09)	701406-06	LANE CLOSURE, FREEWAY/EXPRESSWAY, DAY OPERATIONS ONLY
23	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)	701411-07	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP
24	TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)	701421-03	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS ≥ 45 MPH TO 55 MPH
25-26	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS (TC-12)	701426-04	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS
27	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)	701427	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
28	TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)	701502-04	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
29	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)	701601-07	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
30	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES (TC-17)	701602-05	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
31	ARTERIAL ROAD INFORMATION SIGN (TC-22)	701606-07	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
32	TRAFFIC CONTROL DETAILS FOR FREEWAY CENTER LANE CLOSURE SHOULDER LANE (TC-25)	701701-07	URBAN LANE CLOSURE, MULTILANE INTERSECTION
33	STANDARD TRAFFIC SIGNAL DESIGN DETAILS (TS-05, SHEET 1 OF 6)	701901-01	TRAFFIC CONTROL DEVICES
34	DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING (TS-07)		

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE (OR TOLLWAY) PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (OR ISTHA)

WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT MS. DEBBIE HANLON, AREA TRAFFIC FIELD ENGINEER AT (847) 438-2300 MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

THE EXISTING ROADWAY TYPICAL SECTION IS ASSUMED TO HAVE A 3 INCH HOT-MIX ASPHALT OVERLAY ON TOP OF A TEN INCH CONCRETE BASE.

ALL PAVEMENT PATCHES SHOWN IN THE PLANS ARE TWO (2) INCH MILL AND RESURFACE ONLY. THE MINIMUM WIDTH FOR MILLING AND PATCHING SHALL BE TWO (2) FEET.

NO PATCHING OR RESURFACING IS TO BE DONE WITHIN FIFTY (50) FEET OF ANY RAILROAD CROSSING WITHOUT OBTAINING THE PROPER RAILROAD PROTECTIVE LIABILITY INSURANCE.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING

THE COST OF TRAFFIC CONTROL AND PROTECTION FOR THE PROJECT (EXCLUDING ALL EXPRESSWAY LOCATIONS) SHALL BE INCLUDED IN THE COST OF THE ASSOCIATED ROAD WORK. TRAFFIC CONTROL AND PROTECTION FOR ALL EXPRESSWAY LOCATIONS SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).

THE COST OF ANY PARTIAL OR FULL DEPTH PATCHING REQUIRED AFTER THE REMOVAL OF THE EXISTING 2 INCH HOT-MIX ASPHALT SURFACE SHALL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

ANY DETECTOR LOOPS DAMAGED BY MILLING SHALL BE REPLACED IN KIND. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO QUANTIFY LOOP REPLACEMENTS NEEDED AND PROVIDE THE RESIDENT ENGINEER THIS INFORMATION PRIOR TO GRINDING OR REMOVAL.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).

OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS. ANY MILLED PAVEMENT IS TO BE RESURFACED BY THE END OF EACH DAY AND OPEN TO TRAFFIC.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

	MIXTURE TYPE	AIR VOIDS (%)
ARTERIAL ROUTES	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5MM), 2"	4% @ 70 GYR
EXPY. ROUTES	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5MM), 2"	4% @ 90 GYR

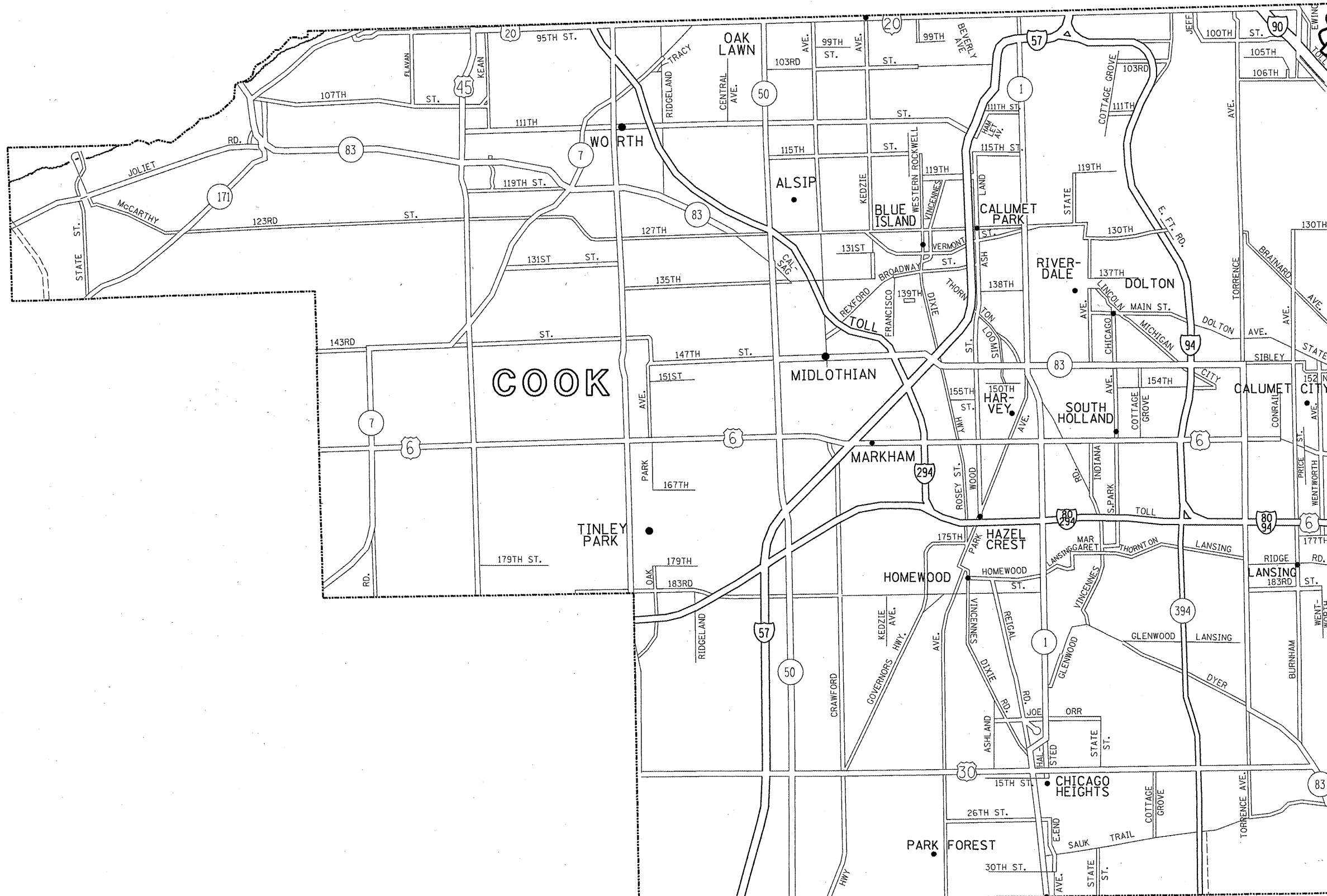
THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE			SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		COOK COUNTY 0005	WILL COUNTY 0005		CODE NO	ITEM	UNIT				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	9	4	5								
40600300	AGGREGATE (PRIME COAT)	TON	46	19	27								
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	69	28	41								
40600895	CONSTRUCTING TEST STRIP	EACH	1	1									
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	694	282	412								
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	2582	1044	1538								
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	9	9									
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SO YD	23129	9399	13730								
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	5	1								
67100100	MOBILIZATION	L SUM	1	1									
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	1	1									
70300100	SHORT TERM PAVEMENT MARKING	FOOT	3557	737	2820								
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	1186	246	940								
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	372	292	80								
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	39165	8150	31015								
* 78000300	THERMOPLASTIC PAVEMENT MARKING - LINE 5"	FOOT	8530	20	8510								
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	6740	4740	2000								
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	370	350	20								
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	670	650	20								
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	366	222	144								
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1283	981	302								
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1283	981	302								
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1530	1030	500								
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1									
Z0026346	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1									
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	719	565	154								
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1									
* X8850102	INDUCTION LOOP	FOOT	100	100									
* X8730312	ELECTRIC CABLE IN CONDUIT, LEAD- IN, NO 1B 4/C, TWISTED, SHIELDED * SPECIALTY ITEM	FOOT	300	300									

Rev

FILE NAME =	USER NAME = Veltchikov	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca\pw_work\p\dd\velchikov\0260198\Designdgn		DRAWN -	REVISED -			VAR.	2011-015-RS	COOK & WILL	34	3	
		CHECKED -	REVISED -			CONTRACT NO. 60P04					
		DATE -	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
PLOT SCALE = 100,000' / IN.		PLOT DATE = 4/14/2011		SCALE:	SHEET NO. OF SHEETS	STA.	TO STA.				



FILE NAME =
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USER NAME = VelichkovV
 PLOT SCALE = 100.0000' / IN.
 PLOT DATE = 4/14/2011

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 DATE -

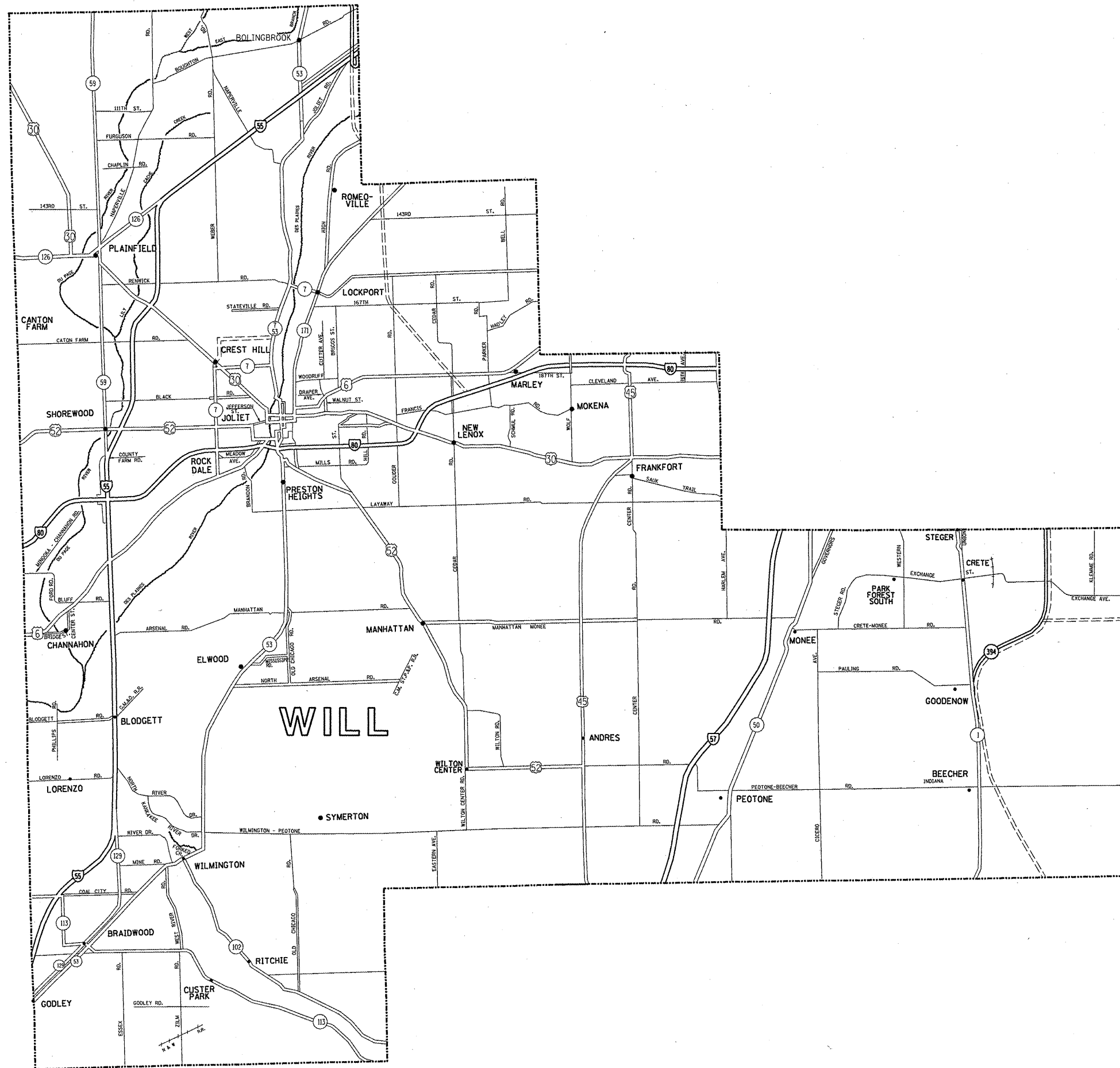
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

GENERAL LOCATION MAP - SOUTH COOK COUNTY

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-015-RS	COOK & WILL	34	4
CONTRACT NO. 60PO4				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



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		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL LOCATION MAP - WILL COUNTY				
SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-015-RS	COOK & WILL	34	5
CONTRACT NO. 60PO4				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

SUMMARY - SOUTH COOK & WILL COUNTIES ROUTES	HMA 2" MILL & RESURFACE (SY)
107TH STREET (IL 83 TO 104TH ST)	952
142ND STREET (INDIANA AVE TO LINCOLN AVE)	2428
CHICAGO ROAD (154TH ST TO LINCOLN AVE)	1490
ELEANOR STREET (WILLIAMS ST TO I-80)	849
HALSTED STREET (JACKSON ST TO 138TH ST)	363
HALSTED STREET (15TH ST. TO IL 1 (IN CHICAGO HEIGHTS))	1132
IL 83 (LAGRANGE RD TO ARCHER AVE)	520
INDIANA AVENUE (162ND ST TO TAFT ST)	645
THORNTON-LANSING ROAD (HICKORY ST TO TORRENCE AVE)	476
WILLIAM STREET (FRANCES ST TO 183RD ST)	356
WILLIAMS ST/MARGARET ST (INTERSECTION)	107
IL 394 SB (STEGER RD. TO ROUTE 1) - WILL COUNTY LOCATION	6121
IL 394 NB (ROUTE 1 TO STEGER RD.) - WILL COUNTY LOCATION	7582
* I-57 (NB 115TH ST LANE 3)	28
* I-57 (SB 127TH ST RAMP)	53
* I-57 (SB UNDER PEOTONE RD) - WILL COUNTY LOCATION	27
SOUTH COOK & WILL COUNTIES TOTAL =	23129 SY

* EXPRESSWAY LOCATION: USE POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5MM)

FILE NAME =	USER NAME = VelschkovVV	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF PATCHING SCHEDULE SOUTH COOK AND WILL COUNTIES	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pwwork\pmsdot\velschkovvv\d0260198\Design.dgn	DRAWN -	REVISED -	VAR.			2011-015-RS	COOK & WILL	34	6	
PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60P04							
PLOT DATE = 4/14/2011	DATE -	REVISED -	SCALE:			SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1

ROUTE: 107th St (IL 83 to 104th St)

CROSS STREET		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Butt Jt (West End)	0.1 M East of Butt Jt	EB	1	12	15	180	20
		EB	2	12	15	180	20
		EB	1	12	6	72	8
0.1 M	0.2 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
0.2 M	0.3 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
0.3 M	0.4 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
0.4 M	0.5 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
0.5 M	0.6 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
0.6 M	0.7 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
0.8 M	0.9 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
0.9 M	1.0 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
1.0 M	1.1 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
1.2 M	1.3 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
1.3 M	1.4 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
1.4 M	1.5 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
1.5 M	1.6 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
1.6 M	1.7 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	25	300	33
1.7 M	1.8 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	25	300	33
1.8 M	1.9 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	3	100	300	33
1.9 M	2.0 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	50	600	67
2.0 M	2.1 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
2.1 M	2.2 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
2.4 M	2.5 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
2.7 M	3.0 M East of Butt Jt	EB	1	12	6	72	8
		EB	1	12	6	72	8
Butt Jt (East End)	0.5 M West of Butt Jt	WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
0.5 M	1.0 M West of Butt Jt	WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
1.0 M	1.5 M West of Butt Jt	WB	1	12	6	72	8
		WB	1	12	6	72	8

ROUTE: 107th St (IL 83 to 104th St)

CROSS STREET		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)		
FROM	TO								
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	25	300	33		
		1.5 M	2.0 M West of Butt Jt	WB	1	12	6	72	8
				WB	1	12	6	72	8
				WB	1	12	6	72	8
				WB	1	12	6	72	8
WB	1			12	6	72	8		
WB	1			12	6	72	8		
WB	1			12	6	72	8		
WB	1			12	6	72	8		
WB	1			12	6	72	8		
WB	1			12	6	72	8		
WB	1			12	6	72	8		
WB	1			12	6	72	8		
WB	1			12	6	72	8		
WB	1			12	6	72	8		
WB	1			12	6	72	8		
WB	1			12	6	72	8		
2.0 M	2.5 M West of Butt Jt			WB	1	12	6	72	8
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		2.5 M	3.0 M West of Butt Jt	WB	1	12	6	72	8
WB	1			12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		
		WB	1	12	6	72	8		

TOTALS: 789 FT 952 SY

ROUTE: 142nd St (Indiana Ave to Lincoln Ave)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Lincoln	Chicago Rd	WB	1	10	6	60	7
Chicago Rd.		WB	1	10	8	80	9
		WB	1	10	6	60	7
		WB	1	10	6	60	7
		WB	1	10	6	60	7
		WB	1	10	6	60	7
	RR track	WB	1	10	20	200	22
RR Track	Indiana	WB	1	None			
Indiana	RR tracks	EB	1	None			
RR tracks		EB	1	10	6	60	7
		EB	1	10	8	80	9
		EB	1	10	20	200	22
	Chicago Rd	EB	1	10	8	80	9
Chicago Rd	Lincoln	EB	1	None			
Lincoln	Chicago Rd	WB	2	10	6	60	7
Chicago Rd		WB	2	10	15	150	17
		WB	2	10	6	60	7
		WB	2	10	8	80	9
		WB	2	10	18	180	20
	RR track	WB	2	5	400	2000	222
RR track		WB	2	5	50	250	28
		WB	2	10	200	2000	222
		WB	2	10	20	200	22
		WB	2	10	20	200	22
		WB	2	10	150	1500	167
		WB	2	10	300	3000	333
	Indiana	WB	2	10	50	500	56
Indiana		EB	2	10	50	500	56
		EB	2	10	100	1000	111
		EB	2	10	250	2500	278
	RR tracks	EB	2	10	600	6000	667
RR tracks		EB	2	10	20	200	22
		EB	2	5	30	150	17
		EB	2	10	6	60	7
		EB	2	10	10	100	11
	Chicago Rd.	EB	2	10	10	100	11
Chicago Rd	Lincoln	EB	2	10	6	60	7

TOTALS: 2425 FT 2428 SY

FILE NAME =	USER NAME = VelichkovV	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PATCHING SCHEDULE 142ND ST			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
et:\pw_vork\p1dot\velichkovv\d0260198\	Design.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	COOK & WILL	34	8
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -								CONTRACT NO. 60P04		
	PLOT DATE = 4/14/2011	DATE -	REVISED -								FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT

ROUTE: Eleanor St (Williams St. to I-80 (also known as South Park))

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
		SB	2	12	6	72	8
		SB	2	12	15	180	20
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	12	144	16
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
SB/WB Turn	Williams St	WB	2	12	12	144	16
		WB	2	12	12	144	16
		WB	2	6	15	90	10
		WB	2	12	12	144	16
Williams St	EB/NB Turn	EB	1	6	20	120	13
		EB	1	12	12	144	16
		EB	1	12	12	144	16
		EB	1	12	12	144	16
EB/NB Turn	I-80	NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	12	144	16
		NB	1	12	10	120	13
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	10	120	13
I-80	SB/WB Turn	SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	10	120	13
		SB	1	12	10	120	13
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	10	120	13
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
SB/WB Turn	Williams St	WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8

TOTALS: 719 FT 849 SY

ROUTE: Halsted St (138th St. to Jackson St.)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
138th	Bridge	NB	1	12	6	72	8
		NB	1	12	8	96	11
		NB	1	12	8	96	11
		NB	1	12	8	96	11
		NB	1	12	15	180	20
Bridge	Jackson	NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
Jackson	Bridge	SB	1	12	8	96	11
		SB	1	12	8	96	11
		SB	1	12	6	72	8
		SB	1	12	8	96	11
Bridge	138th	SB	1	12	6	72	8
		SB	1	12	6	72	8
138th	Bridge	NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
Bridge	Jackson	NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	15	180	20
Jackson	Bridge	SB	2	12	12	144	16
		SB	2	12	8	96	11
		SB	2	12	25	300	33
		SB	2	12	10	120	13
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	12	144	16
		SB	2	12	25	300	33
		SB	2	12	6	72	8
		SB	2	12	6	72	8

TOTALS: 272 FT 363 SY

ROUTE: Halsted St (ILL 1 to 15th St)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
15th Street	Rt 30	NB	1	12	12	144	16
		NB	1	12	50	600	67
		NB	1	12	12	144	16
Rt 30	15th Street	SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
15th Street	Rt 30	SB	1	12	12	144	16
		SB	1	12	25	300	33
		NB	2	12	12	144	16
		NB	2	12	100	1200	133
		NB	2	12	50	600	67
		NB	2	12	100	1200	133
Rt 30	13th Street	NB	2	6	12	72	8
		NB	2	6	100	600	67
		NB	2	12	12	144	16
13th Street	12th Street	NB	2	12	25	300	33
		NB	2	12	50	600	67
		NB	2	6	50	300	33
11th Street	12th Street	NB	2	12	25	300	33
		NB	2	12	30	360	40
		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	2	6	20	120	13
		SB	2	12	35	420	47
12th Street	13th Street	SB	2	6	50	300	33
		SB	2	12	30	360	40
		SB	2	12	20	240	27
Rt 30	15th Street	SB	2	12	12	144	16
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
Rt 30	15th Street	SB	2	12	12	144	16
		SB	2	12	25	300	33

TOTALS: 965 FT 1132 SY

ROUTE: IL 83 (LaGrange Rd to Archer Ave)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
1+90		EB	1	12	20	240	27
1+90		WB	1	12	20	240	27
21+00		EB	1	12	6	72	8
21+50		WB	1	12	6	72	8
22+50		EB	1	12	50	600	67
22+70		WB	1	12	50	600	67
27+40		WB	1	12	20	240	27
29+00		WB	1	12	6	72	8
37+00		WB	1	12	6	72	8
42+00		EB	1	12	100	1200	133
42+25		WB	1	12	75	900	100
100+50		WB	1	12	30	360	40

TOTALS: 389 FT 520 SY

ROUTE: Indiana Ave (162nd St to Taft St)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
162nd		NB	1	12	5	60	7
		NB	1	12	5	60	7
		NB	1	12	5	60	7
		NB	1	12	5	60	7
		NB	1	12	5	60	7
		NB	1	12	8	96	11
		NB	1	12	5	60	7
		NB	1	12	15	180	20
		NB	1	12	5	60	7
		NB	1	12	12	144	16
		NB	1	12	8	96	11
		NB	1	12	12	144	16
		NB	1	12	12	144	16
		NB	1	12	6	72	8
		NB	2	12	5	60	7
		NB	2	12	5	60	7
		NB	2	12	5	60	7
		NB	2	12	5	60	7
		NB	2	12	5	60	7
		NB	2	12	5	60	7
		NB	2	12	5	60	7
		NB	2	12	5	60	7
		NB	2	12	5	60	7
		NB	2	12	5	60	7
		NB	2	12	8	96	11
		NB	2	12	5	60	7
		NB	2	12	15	180	20
		NB	2	12	5	60	7
		NB	2	12	5	60	7
		NB	2	12	12	144	16
		NB	2	12	8	96	11
		NB	2	12	12	144	16
		NB	2	12	12	144	16
		NB	2	12	12	144	16
		NB	2	12	6	72	8
		NB	2	12	6	72	8
Taft		NB	2	12	6	72	8

CONTINUED ON NEXT SHEET

ROUTE: Indiana Ave (162nd St to Taft St)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Taft		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	12	144	16
		SB	1	12	5	60	7
		SB	1	12	5	60	7
		SB	1	12	5	60	7
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	15	180	20
		SB	1	12	10	120	13
		SB	1	12	5	60	7
		SB	1	12	5	60	7
		SB	1	12	5	60	7
		SB	1	12	8	96	11
		SB	1	12	12	144	16
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	12	144	16
		SB	2	12	5	60	7
		SB	2	12	5	60	7
		SB	2	12	5	60	7
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	15	180	20
		SB	2	12	10	120	13
		SB	2	12	5	60	7
		SB	2	12	5	60	7
		SB	2	12	5	60	7
		SB	2	12	8	96	11
	162nd	SB	2	12	6	72	8

TOTALS: 484 FT 645 SY

ROUTE: Thornton-Lansing Road (Hickory St. to Torrence Ave.)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Hickory Street	Torrence Ave.	WB	1&2	20	80	1600	178
		EB	1&2	30	10	300	33
		EB	1&2	10	8	80	9
		EB	LT	10	150	1500	167
		WB	1	12	50	600	67
		WB (edge)	2	4	50	200	22

TOTALS: 348 FT 476 SY

ROUTE: Williams Street (Frances St. to 183rd St.)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Frances Street	183rd	NB	1	10	20	200	22
		SB	1	10	20	200	22
		NB(edge)	1	4	50	200	22
		SB(edge)	1	4	150	600	67
		NB/SB	1	20	100	2000	222

TOTALS: 340 FT 356 SY

ROUTE: Williams St./Margaret St. (Intersection)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
	South leg	NB	1	10	40	400	44
	North leg (edge patch)	SB	2	4	70	280	31
	North leg (edge patch)	NB	2	4	70	280	31

TOTALS: 180 FT 107 SY

ROUTE: IL 394 SB (Steger Road to Route 1)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Steger	Richton	SB	2	12	15	180	20
		SB	2	12	6	72	8
		SB	2	12	8	96	11
		SB	2	12	8	96	11
		SB	2	12	8	96	11
		SB	2	12	10	120	13
		SB	2	4	50	200	22
		SB	2	12	10	120	13
		SB	2	12	200	2400	267
		SB	2	4	200	800	89
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	4	100	400	44
		SB	2	12	20	240	27
		SB	2	12	75	900	100
		SB	2	12	70	840	93
		SB	2	4	150	600	67
		SB	2	12	20	240	27
SB	2	12	6	72	8		
SB	2	12	6	72	8		
SB	2	12	6	72	8		
Richton	Exchange	SB	2	12	6	72	8
		SB	2	4	100	400	44
		SB	2	4	50	200	22
		SB	2	12	6	72	8
		SB	2	12	8	96	11
		SB	2	4	50	200	22
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	7	70	490	54
		SB	2	6	60	360	40
		SB	2	12	20	240	27
		SB	2	12	15	180	20
		SB	2	12	10	120	13
		SB	2	12	8	96	11
		SB	2	7	200	1400	156
		SB	2	7	70	490	54
Exchange	Burrville	SB	2	4	250	1000	111
		SB	2	12	6	72	8
		SB	2	4	100	400	44
		SB	2	12	8	96	11
		SB	2	12	10	120	13
		SB	2	12	6	72	8
Burrville	Cottage Groove	SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	4	250	1000	111
		SB	2	12	6	72	8
		SB	2	12	6	72	8

ROUTE: IL 394 SB (Steger Road to Route 1)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
		SB	2	4	50	200	22
		SB	2	12	8	96	11
		SB	2	12	10	120	13
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	4	25	100	11
		SB	2	12	10	120	13
		SB	2	12	6	72	8
		SB	2	4	25	100	11
		SB	2	12	6	72	8
		SB	2	12	15	180	20
		SB	2	4	100	400	44
		SB	2	12	6	72	8
		SB	2	4	200	800	89
		SB	2	4	75	300	33
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	4	100	400	44
		SB	2	4	200	800	89
		SB	2	4	300	1200	133
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
SB	2	12	6	72	8		
SB	2	12	6	72	8		
SB	2	12	6	72	8		
SB	2	12	6	72	8		
SB	2	12	6	72	8		
SB	2	4	400	1600	178		
Bemes	Elmscourt	SB	2	4	20	80	9
		SB	2	12	6	72	8
		SB	2	12	10	120	13
		SB	2	12	15	180	20

CONTINUED ON NEXT SHEET

FILE NAME =	USER NAME = VelichkovVV	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PATCHING SCHEDULE IL 394 SB	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pw\work\pmdot\velichkovvv\d0260198\design.dgn	DRAWN -	REVISED -	VAR.			2011-015-RS	COOK & WILL	34	13	
PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60PO4							
PLOT DATE = 4/14/2011	DATE -	REVISED -	SCALE:			SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT	

ROUTE: IL 394 SB (Steger Road to Route 1)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	4	50	200	22
		SB	1	4	75	300	33
		SB	1	4	100	400	44
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
Cottage Groove	Bemes	SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	4	125	500	56
		SB	1	4	25	100	11
		SB	1	12	6	72	8
		SB	1	12	20	240	27
		SB	1	4	150	600	67
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
Bemes	Elmscourt	SB	1	12	45	540	60
		SB	1	4	75	300	33
		SB	1	12	8	96	11
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	4	125	500	56
		SB	1	12	6	72	8
		SB	1	12	10	120	13
		SB	1	4	40	160	18
		SB	1	4	75	300	33
		SB	1	12	60	720	80
		SB	1	12	50	600	67
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
Elmscourt	Route 1	SB	1	4	50	200	22
		SB	1	4	50	200	22
		SB	1	12	6	72	8
		SB	1	4	50	200	22
		SB	1	4	50	200	22
		SB	1	4	50	200	22
		SB	1	4	25	100	11
		SB	1	12	6	72	8
		SB	1	12	6	72	8

ROUTE: IL 394 SB (Steger Road to Route 1)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
		SB	1	4	100	400	44

TOTALS: 8492 FT 6121 SY

ROUTE: IL 394 NB (Route 1 to Steger Road)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Route 1	Elmscourt	NB	1	12	100	1200	133
		NB	1	12	6	72	8
		NB	1	12	10	120	13
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
Elmscourt	Bemes	NB	1	12	40	480	53
		NB	1	12	20	240	27
		NB	1	12	10	120	13
		NB	1	12	10	120	13
		NB	1	12	10	120	13
		NB	1	12	8	96	11
		NB	1	12	10	120	13
		NB	1	12	20	240	27
		NB	1	4	30	120	13
		NB	1	12	20	240	27
		NB	1	12	40	480	53
		NB	1	12	15	180	20
		NB	1	12	60	720	80
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8

CONTINUED ON NEXT SHEET

FILE NAME = c:\pwork\pwork\velichkov\0260198\...	USER NAME = VelichkovV design.dgn	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PATCHING SCHEDULE IL 394 SB & IL 394 NB			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	COOK & WILL	34	15
PLOT DATE = 4/14/2011		DATE -	REVISED -					CONTRACT NO. 60PO4		ILLINOIS FED. AID PROJECT			

ROUTE: IL 394 NB (Route 1 to Steger Road)

CROSSSTREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
Bemes	Cottage Groove	NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	6	30	180	20
		NB	1	12	10	120	13
		NB	1	12	6	72	8
		NB	1	4	120	480	53
		NB	1	12	10	120	13
		NB	1	4	35	140	16
		NB	1	12	50	600	67
		NB	1	12	10	120	13
		NB	1	12	50	600	67
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
Cottage Groove	Burrville	NB	1	12	10	120	13
		NB	1	12	15	180	20
		NB	1	12	8	96	11
		NB	1	12	6	72	8
		NB	1	12	15	180	20
		NB	1	12	6	72	8
		NB	1	12	15	180	20
		NB	1	12	15	180	20
		NB	1	12	10	120	13
		NB	1	12	6	72	8
		NB	1	4	100	400	44
		NB	1	12	8	96	11
		NB	1	12	8	96	11
		NB	1	12	6	72	8
		NB	1	12	20	240	27
		NB	1	8	20	160	18
		NB	1	12	10	120	13
		NB	1	12	15	180	20
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8

ROUTE: IL 394 NB (Route 1 to Steger Road)

CROSSSTREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	40	480	53
Burrville	Exchange	NB	1	12	10	120	13
		NB	1	12	8	96	11
		NB	1	4	20	80	9
		NB	1	4	15	60	7
		NB	1	12	10	120	13
		NB	1	12	10	120	13
		NB	1	4	20	80	9
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	15	180	20
Exchange	Richton	NB	1	4	20	80	9
		NB	1	12	6	72	8
		NB	1	12	8	96	11
		NB	1	4	75	300	33
		NB	1	12	10	120	13
		NB	1	6	50	300	33
		NB	1	12	6	72	8
		NB	1	12	10	120	13
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	15	180	20
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	8	96	11

CONTINUED ON NEXT SHEET

ROUTE: IL 394 NB (Route 1 to Steger Road)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	4	700	2800	311
		NB	2	12	10	120	13
		NB	2	12	10	120	13
		NB	2	12	20	240	27
		NB	2	12	20	240	27
		NB	2	12	40	480	53
		NB	2	12	50	600	67
		NB	2	12	60	720	80
		NB	2	12	15	180	20
		NB	2	4	100	400	44
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
Burrville	Exchange	NB	2	12	15	180	20
		NB	2	12	8	96	11
		NB	2	12	6	72	8
		NB	2	4	25	100	11
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	10	120	13
		NB	2	12	15	180	20
		NB	2	12	150	1800	200
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	4	75	300	33
Exchange	Richton	NB	2	12	15	180	20
		NB	2	12	15	180	20
		NB	2	12	10	120	13
		NB	2	12	8	96	11
		NB	2	12	6	72	8
		NB	2	4	200	800	89
		NB	2	12	8	96	11
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
Richton	Steger	NB	2	4	100	400	44
		NB	2	12	15	180	20
		NB	2	6	100	600	67
		NB	2	4	25	100	11
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8

ROUTE: IL 394 NB (Route 1 to Steger Road)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
		NB	2	4	50	200	22
		NB	2	4	500	2000	222
		NB	2	12	8	96	11
		NB	2	12	6	72	8
		NB	2	4	200	800	89
		NB	2	4	500	2000	222
		NB	2	4	200	800	89
		NB	2	12	6	72	8

TOTALS: 9989 FT 7582 SY

ROUTE: I-57 (NB 115th St lane 3)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
115th St	NB bottom of ramp	NB		14	6	84	9
				14	6	84	9
				14	6	84	9

TOTALS: 18 FT 28 SY

ROUTE: I-57 (SB 127th St ramp)

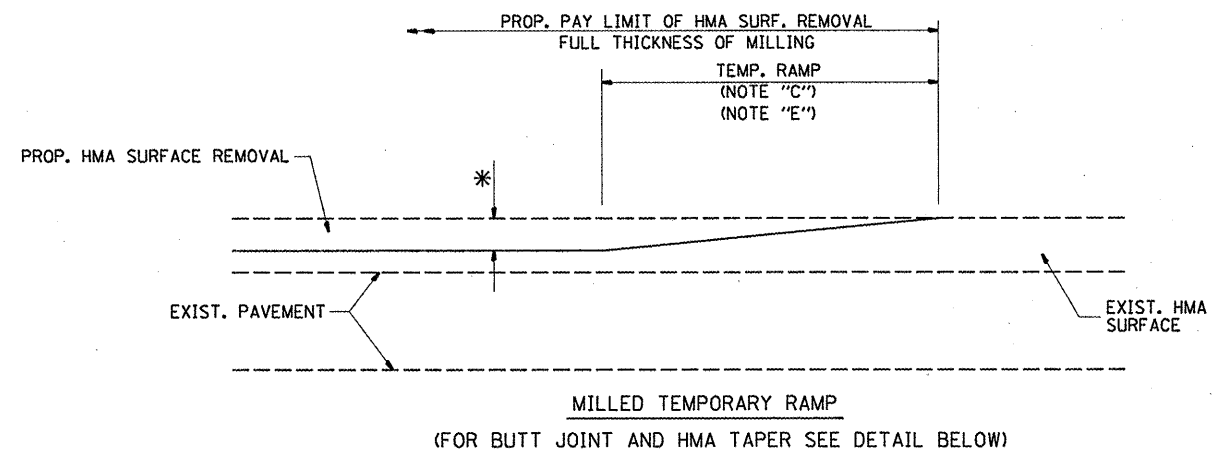
CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
127th St ramp SB		SB		20	6	120	13
		SB		20	6	120	13
		SB		20	6	120	13
		SB		20	6	120	13

TOTALS: 24 FT 53 SY

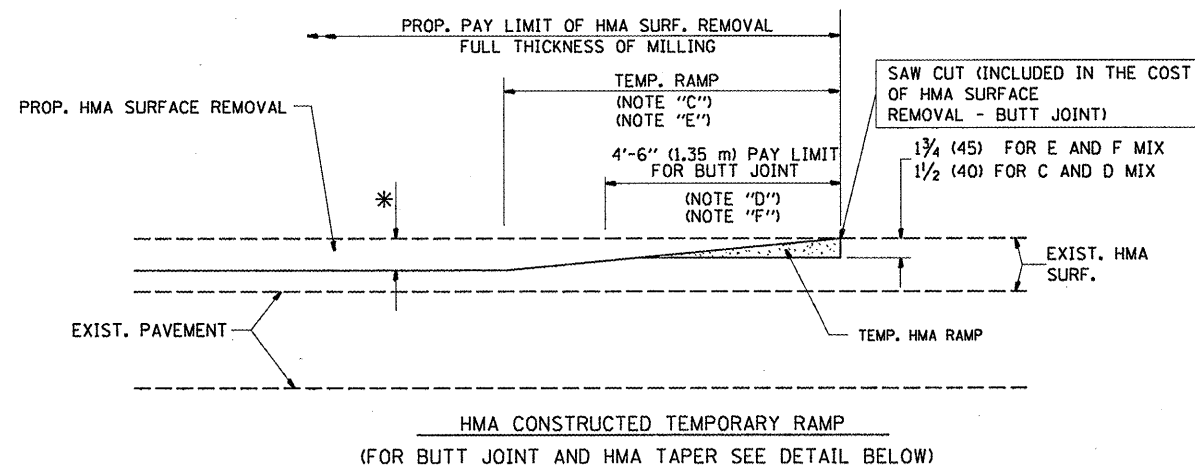
ROUTE: I-57 (SB under Peotone Rd)

CROSSSTREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
15' south of (not under) Peotone Rd.		SB	1	12	10	120	13
				12	10	120	13

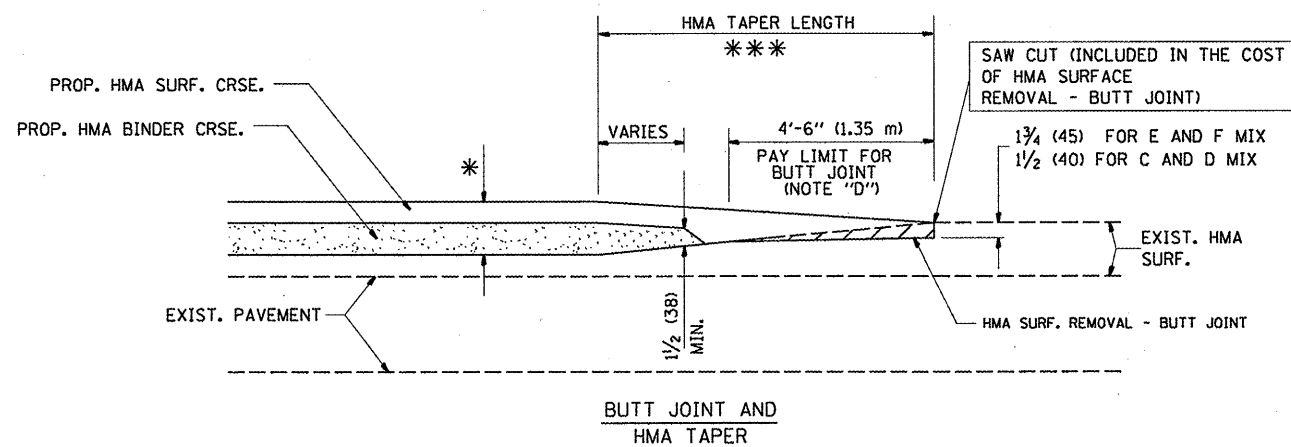
TOTALS: 20 FT 27 SY



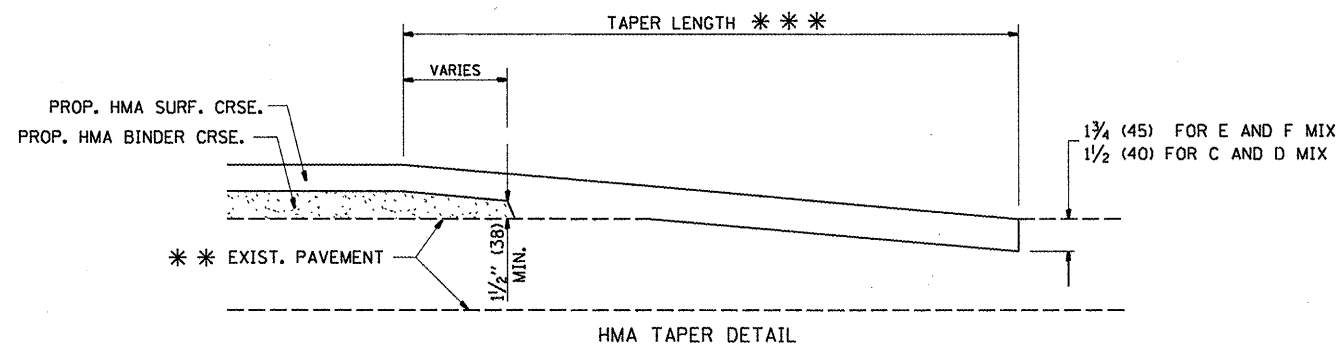
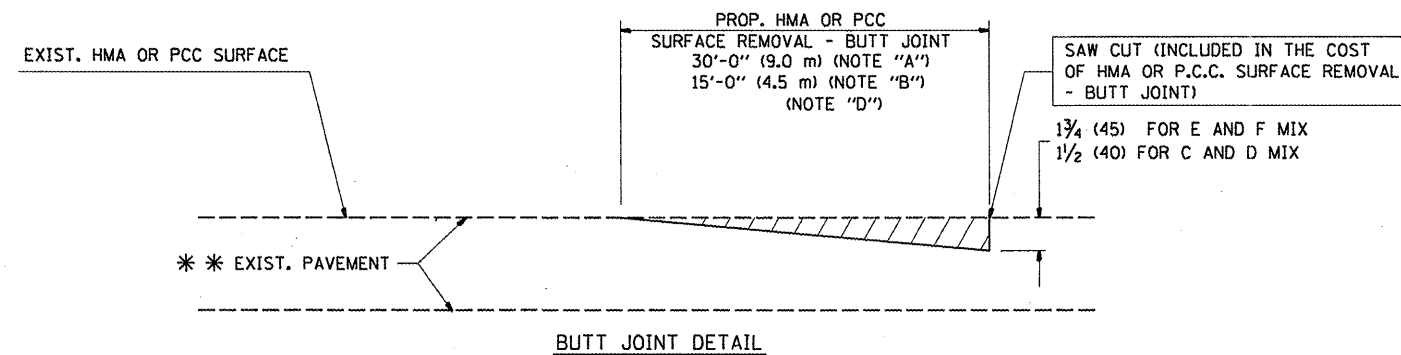
OPTION 1



**OPTION 2
TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

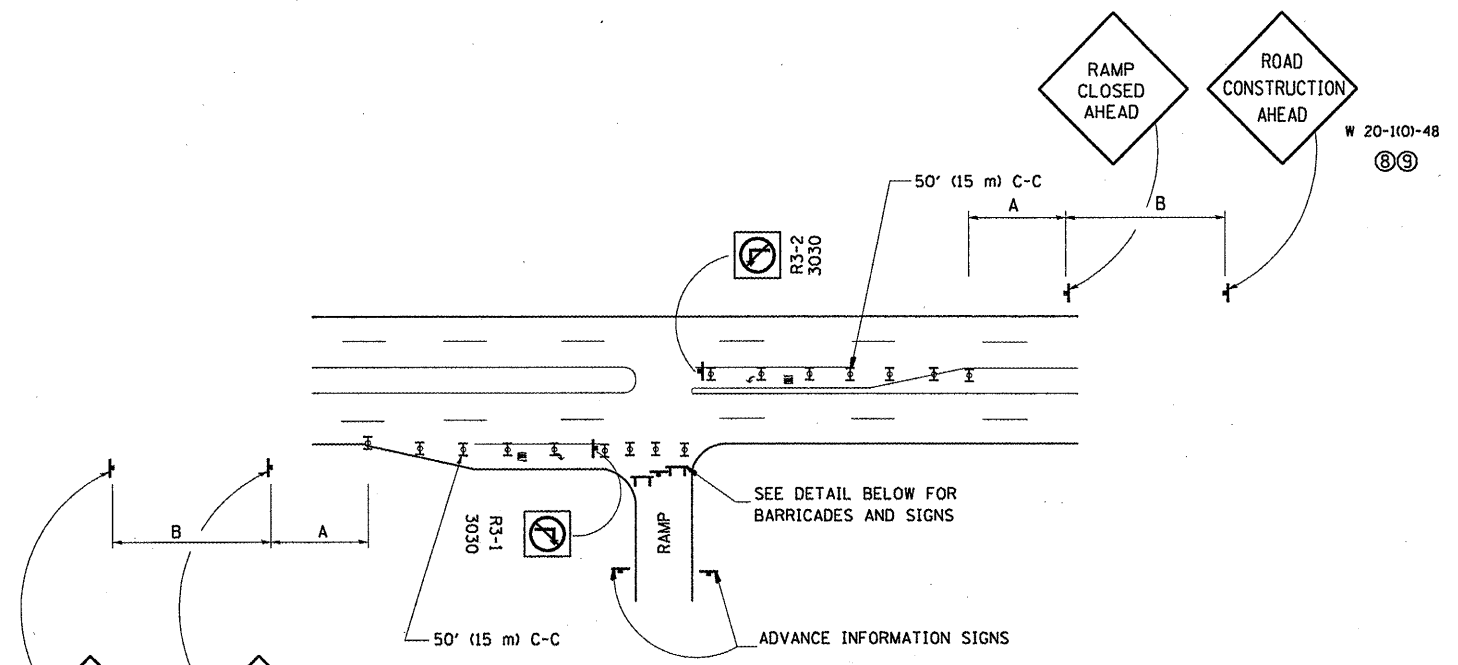
FILE NAME =	USER NAME = VelichkovVV	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
cr\pw_work\pmsdot\velichkovvv\d0260198\	stStd.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 100.0000 ' / IN.	CHECKED -	REVISIONS - M. GOMEZ 04-06-01	
PLOT DATE = 4/14/2011	DATE - 06-13-90	REVISIONS - R. BORO 01-01-07	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-015-RS	COOK	34	20
BD400-05 BD32		CONTRACT NO. 60P04		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

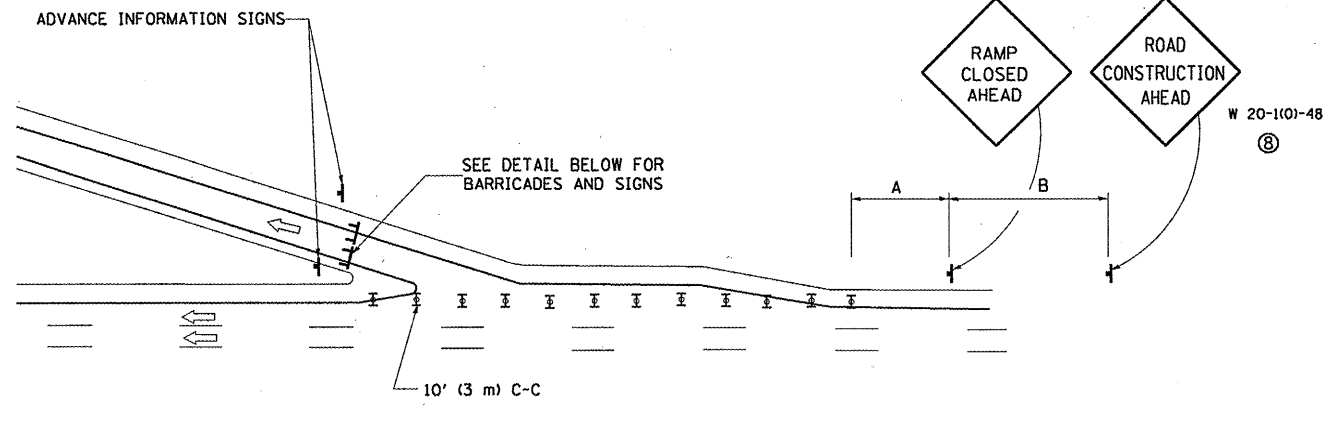
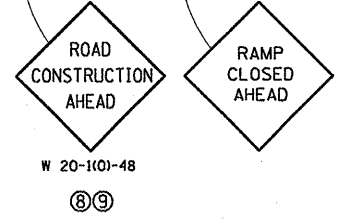


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

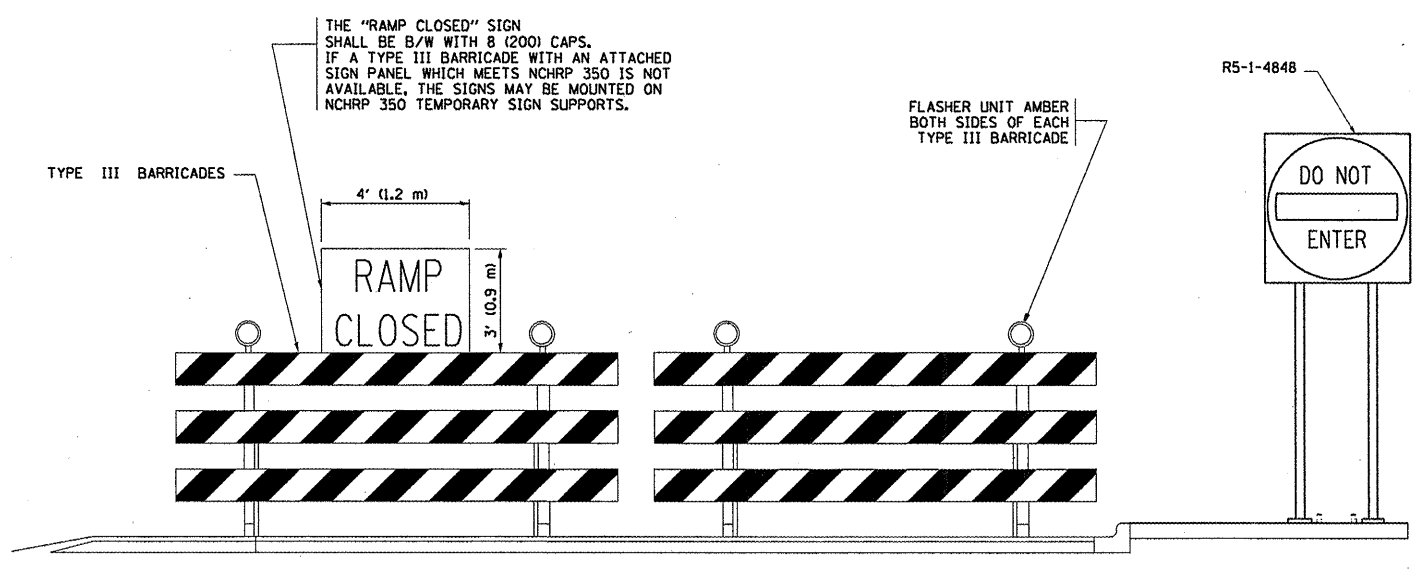
FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL ≥45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	150' (45 m)	150' (45 m)

DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



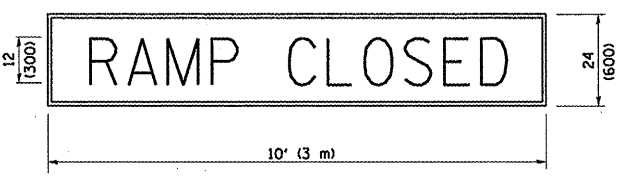
EXIT RAMP CLOSURE

- SYMBOLS**
- ▬ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
 - ⊞ TYPE III BARRICADE WITH FLASHING LIGHT



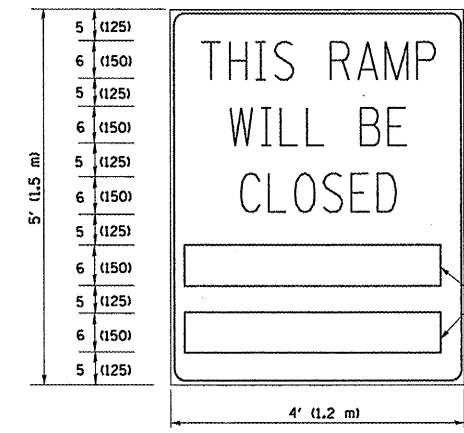
DETAIL FOR REQUIRED BARRICADES & SIGNS

RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE REFLECTORIZED BACKGROUND
1 (25) BORDER
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR THE CLOSED EXIT RAMPS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE REFLECTORIZED BACKGROUND
1/2 (12) BORDER

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

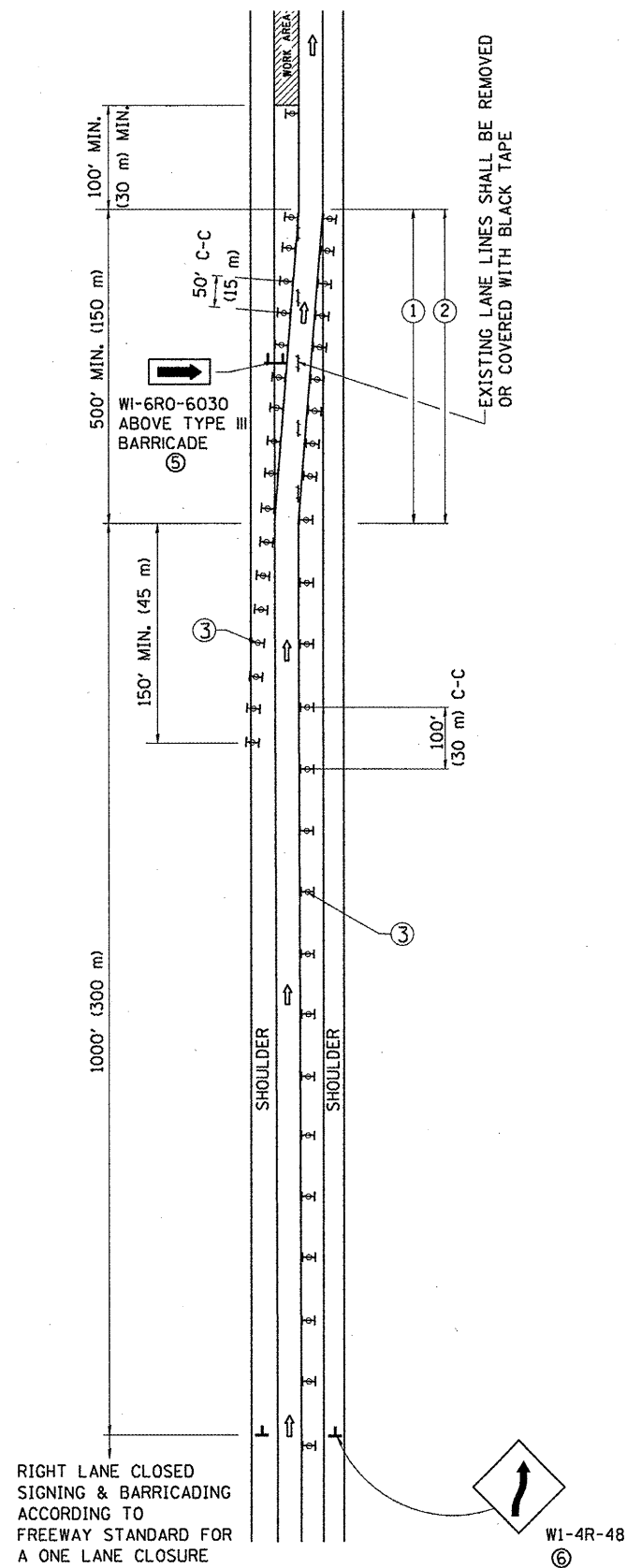
THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

GENERAL NOTES:

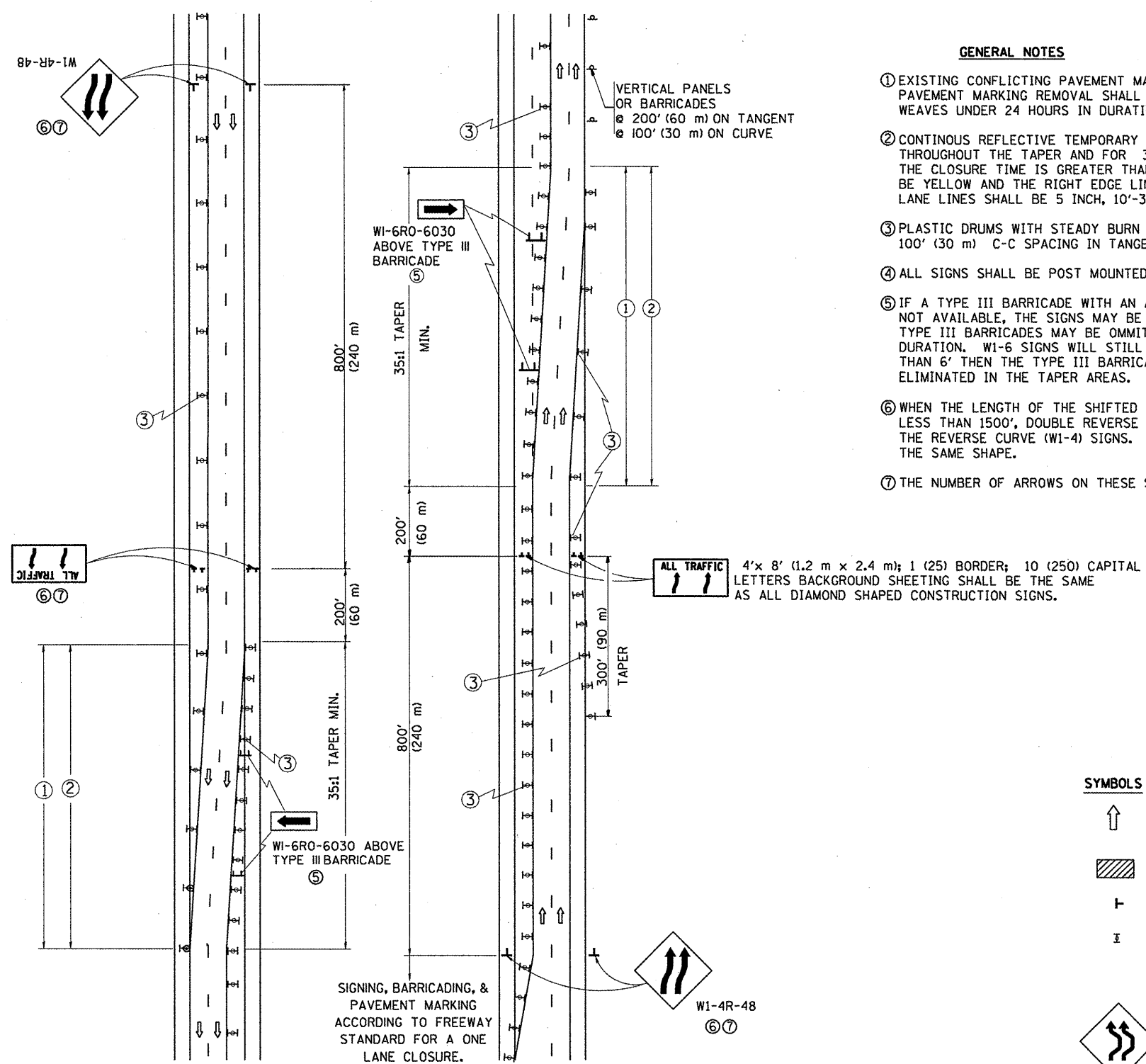
- 1 CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- 2 STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- 3 A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
- 4 ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
- 5 THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- 6 AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- 7 THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY-FOUR (24) HOURS IN LENGTH.
- 8 ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- 9 ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED ON CLOSURES LESS THAN 24 HOURS IN DURATION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

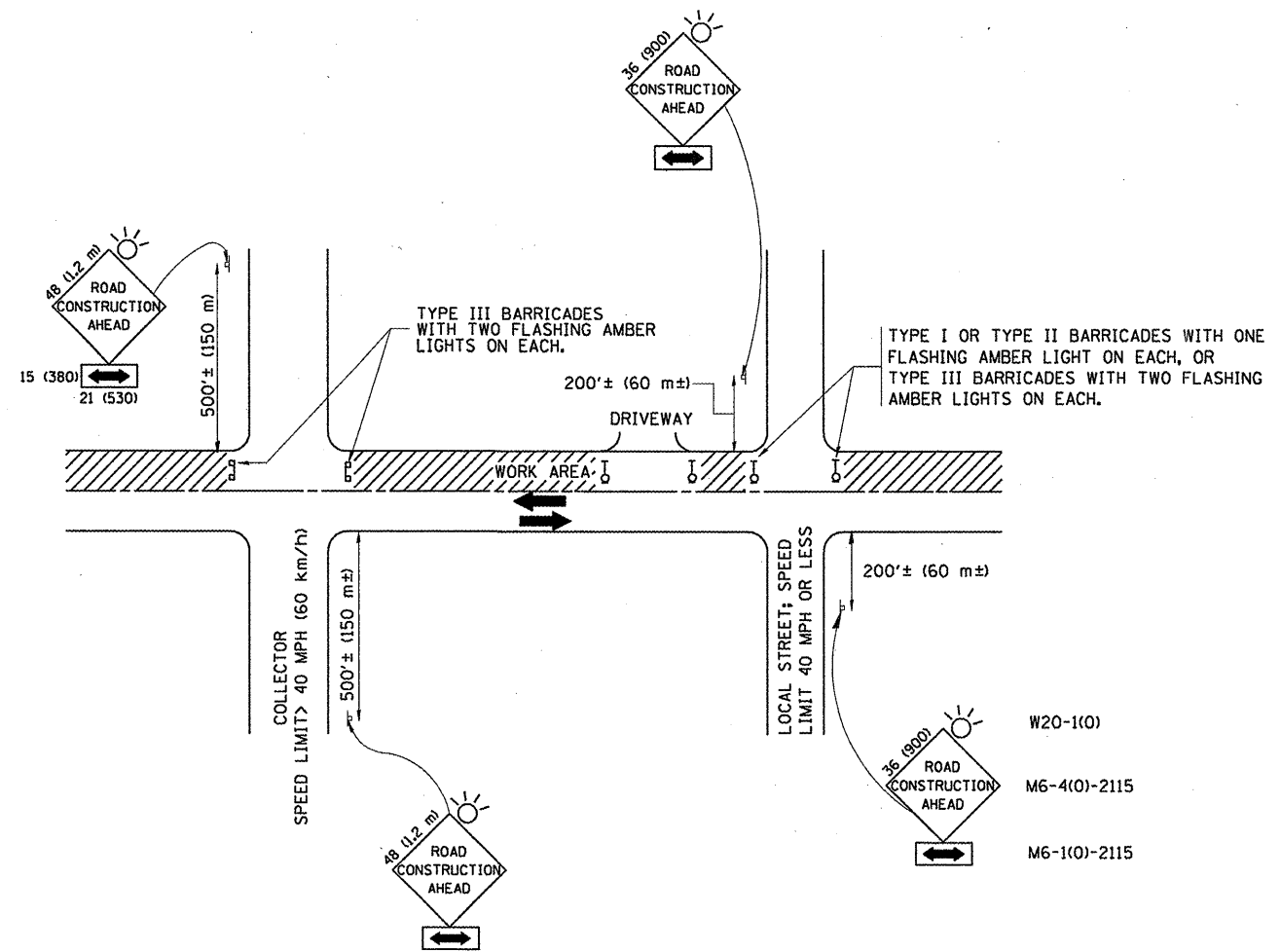
- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 24 HOURS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS. TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
 - WORK AREA
 - SIGN ON PORTABLE OR PERMANENT SUPPORT
 - TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = VelichkovVV	DESIGNED - DWS	REVISED - JAF 01-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw_work\pwidot\velichkovv\d0260198\d	atStd.dgn	DRAWN -	REVISED - JAF 02-06		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	2011-015-RS	COOK	34	22
		CHECKED -	REVISED - SPB 01-07					TC-09		CONTRACT NO. 60P04		
		DATE - 02-87	REVISED - SPB 12-09					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



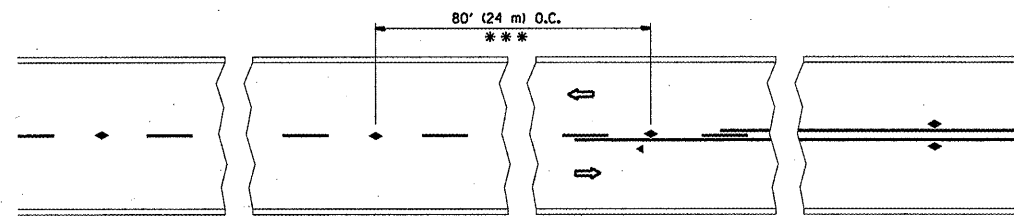
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).**
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

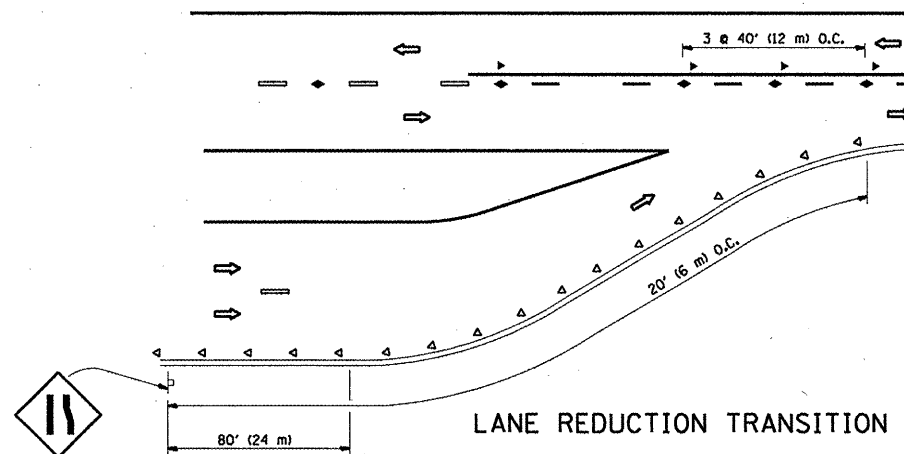
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = VelichkovVV	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pwwork\pwwdot\velichkovvv\d0260198\dstStd.dgn		DRAWN -	REVISED - A. HOUSEH 03-06-96			VAR.	2011-015-RS	COOK	34	23
PLOT SCALE = 100.0000 ' / IN.		CHECKED -	REVISED - A. HOUSEH 10-15-96			TC-10		CONTRACT NO. 60P04		
PLOT DATE = 4/14/2011		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00			SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

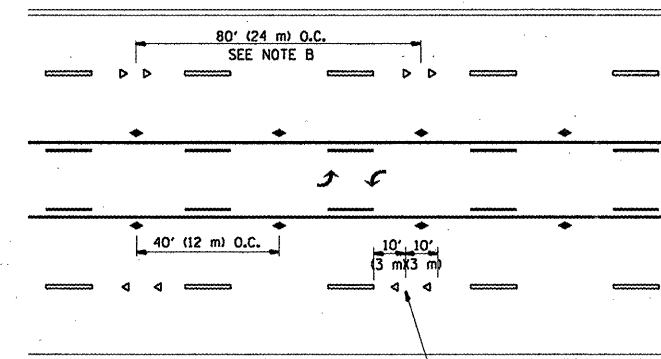


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

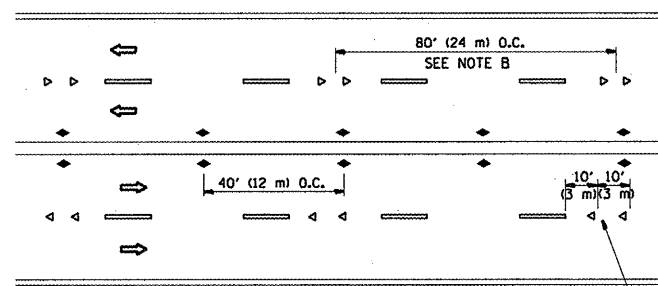
TWO-LANE/TWO-WAY



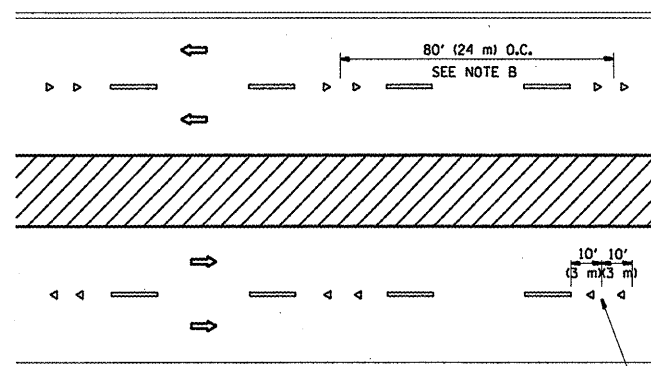
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

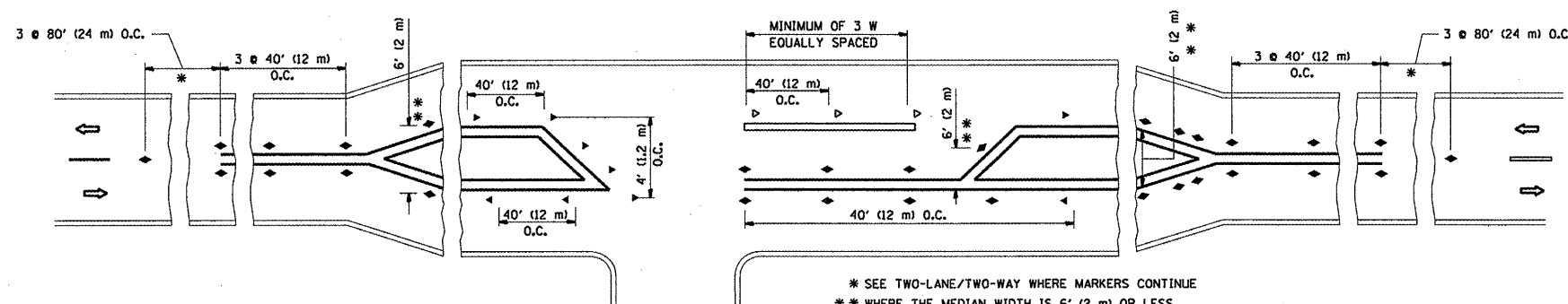
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◀ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

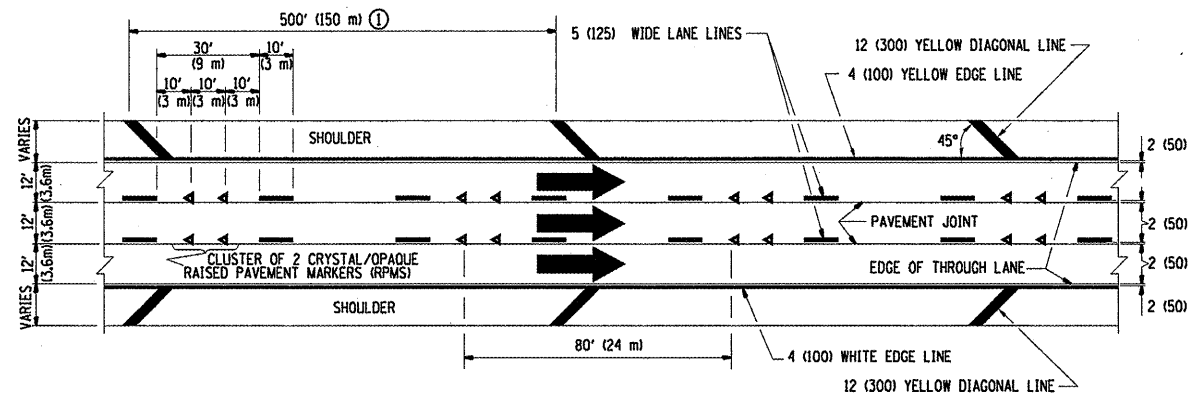


LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

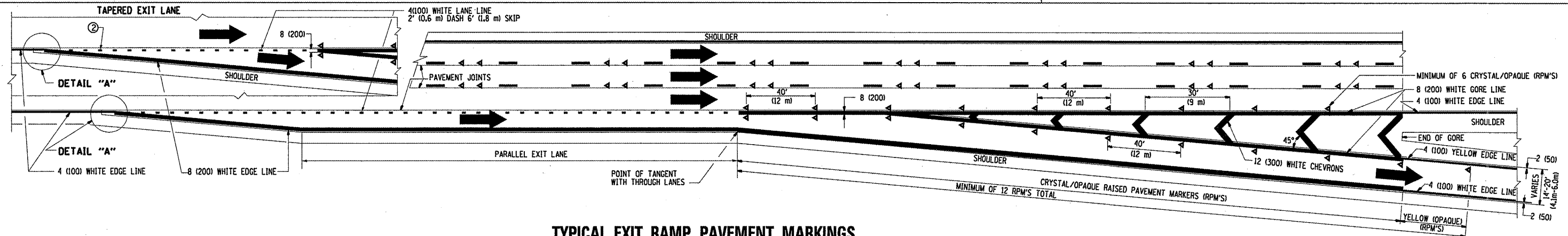
FILE NAME =	USER NAME = ValchikovVV	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS			F.A. -	SECTION	COUNTY	TOTAL	SHEET	
or\pwork\pwork\valchikovvv\d0260196V	atStd.dgn	DRAWN -	REVISED - T. RAMMACHER 03-12-99		RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			VAR.	2011-015-RS	COOK	34	24	
	PLOT SCALE = 100.0000 "/ IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	TC-11			CONTRACT NO. 60P04
	PLOT DATE = 4/14/2011	DATE -	REVISED - C. JUCIUS 09-09-09		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								



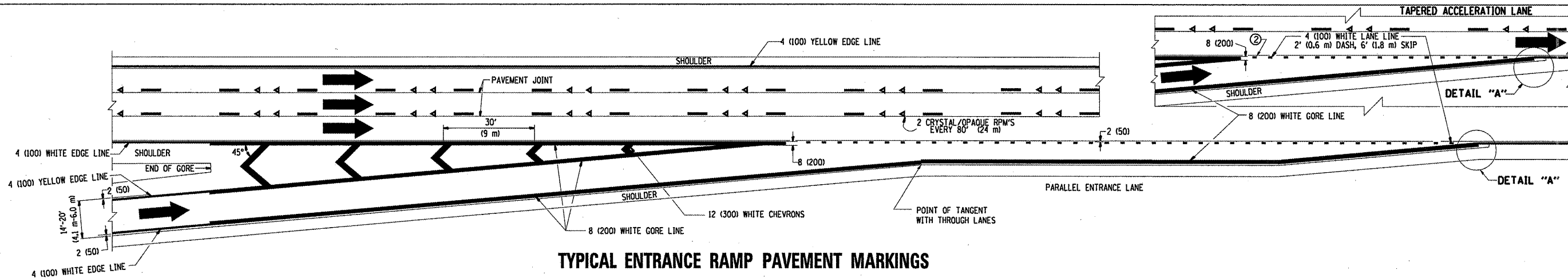
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

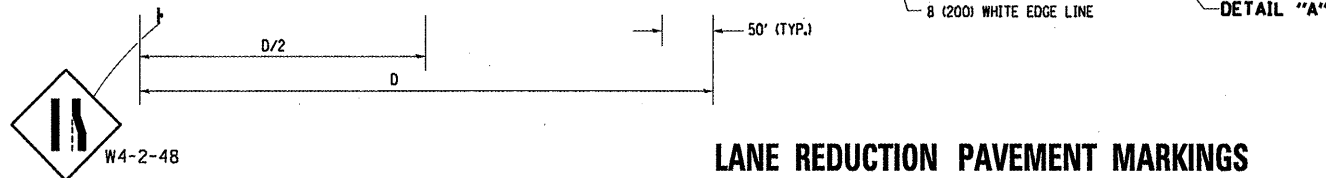
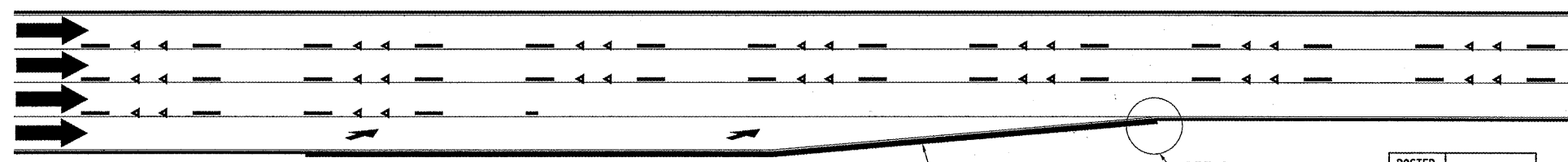
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE SHALL BE USED FOR ALL LANE LINES ON BITUMINOUS PAVEMENT.
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC.



TYPICAL EXIT RAMP PAVEMENT MARKINGS

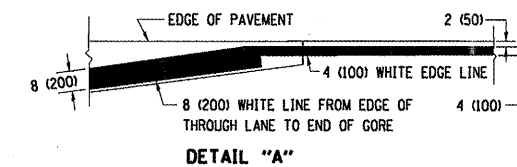


TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



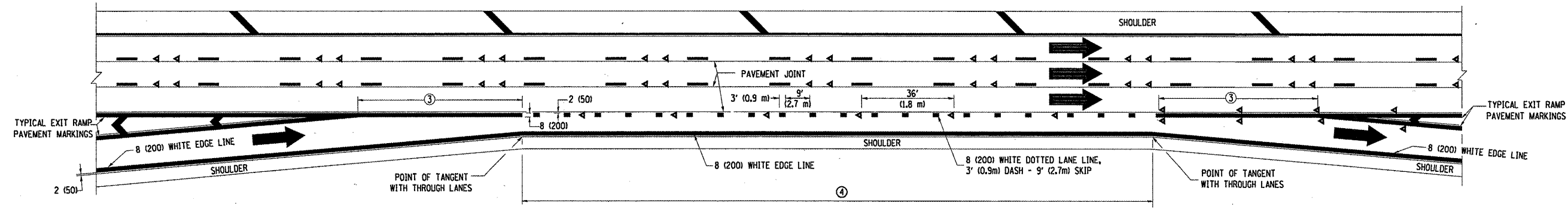
LANE REDUCTION PAVEMENT MARKINGS

POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)

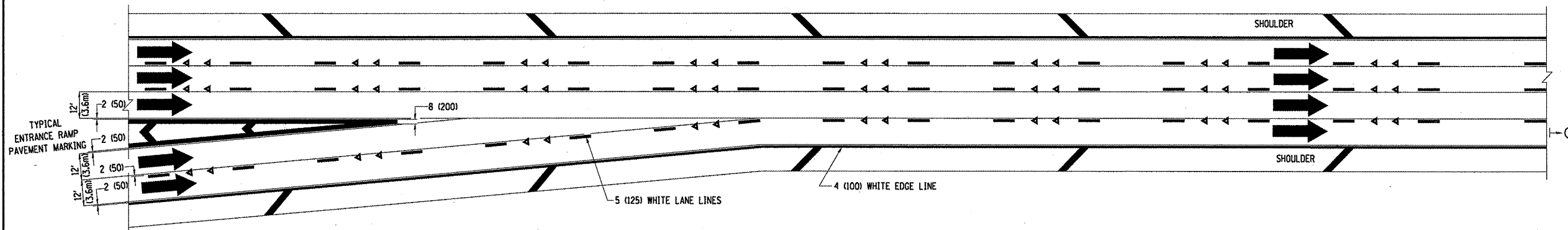


NOTES:

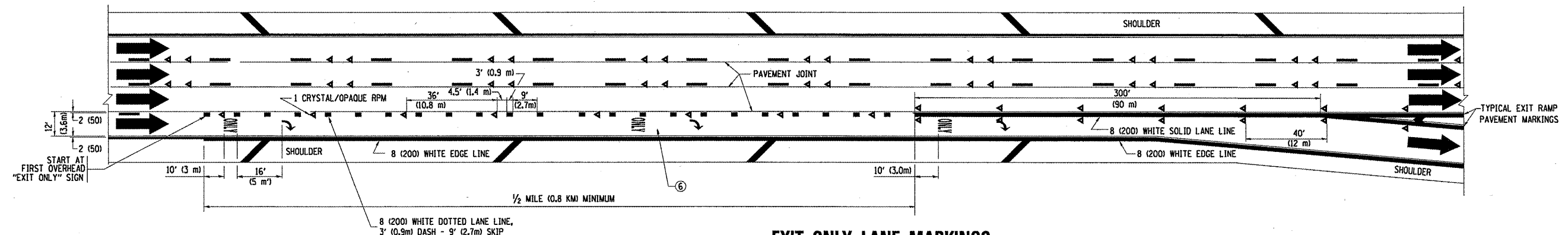
- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
- ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.



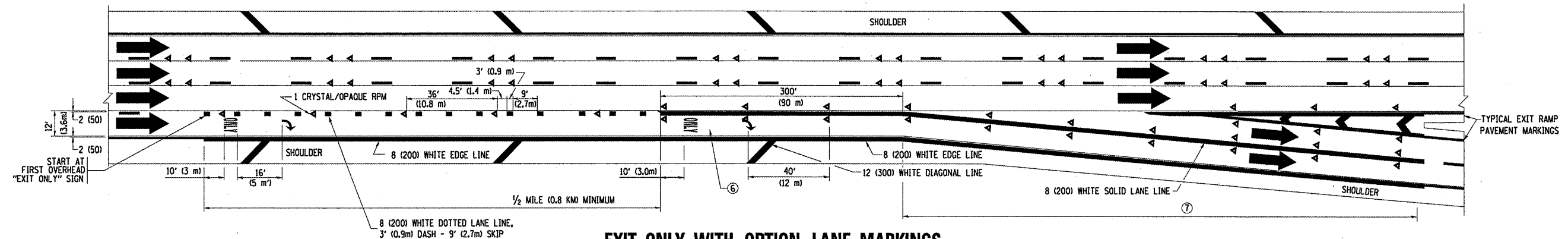
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



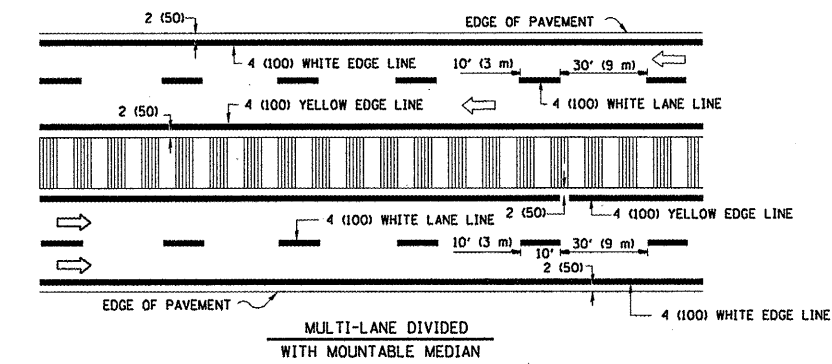
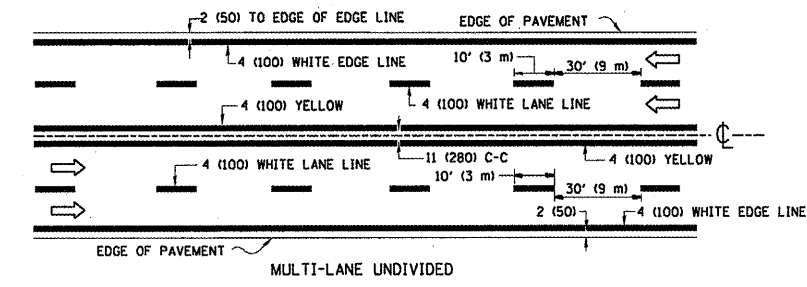
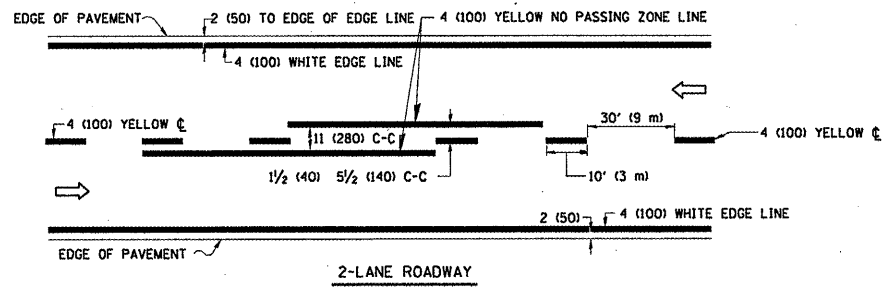
EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

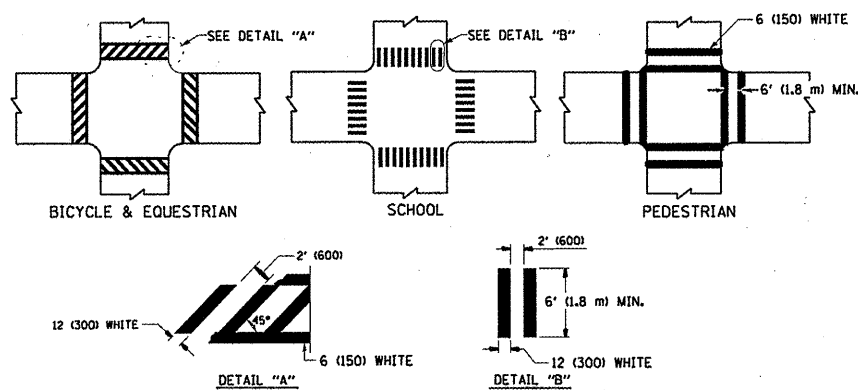
- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

FILE NAME =	USER NAME = velchikov	DESIGNED - D.W.S.	REVISED - D.W.S. 07-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\pav\dot\velchikov\vd0260198\d	atStd.dgn	DRAWN -	REVISED - J.A.F. 02-06		SCALE: NONE	SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.	2011-015-RS	COOK	34	26
	PLOT SCALE = 100.0000 "/td> <td>CHECKED -</td> <td>REVISED - S.P.B. 01-07</td> <td colspan="3"></td> <td colspan="2" style="text-align: center;">TC-12</td> <td colspan="3" style="text-align: center;">CONTRACT NO. 60PO4</td>	CHECKED -	REVISED - S.P.B. 01-07					TC-12		CONTRACT NO. 60PO4		
	PLOT DATE = 4/14/2011	DATE - 01-90	REVISED - S.P.B. 01-10					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

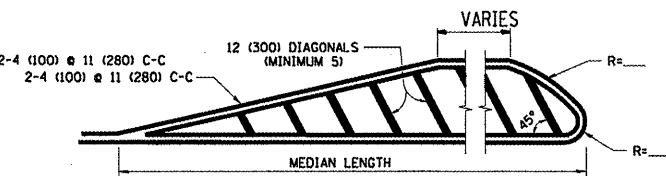
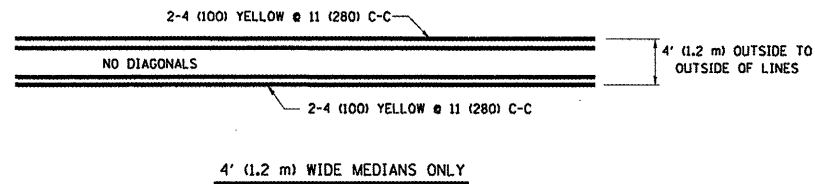


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

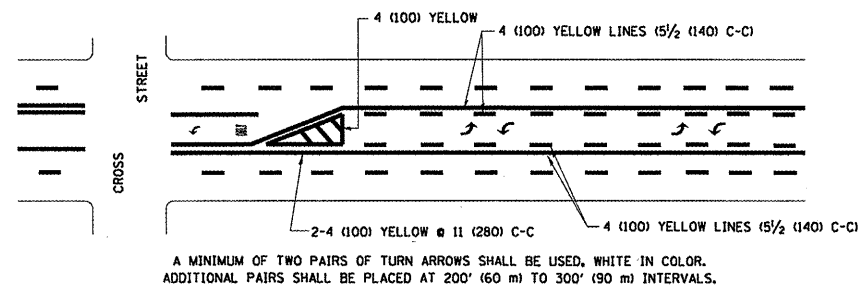


TYPICAL CROSSWALK MARKING

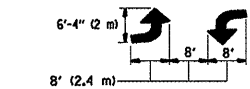


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

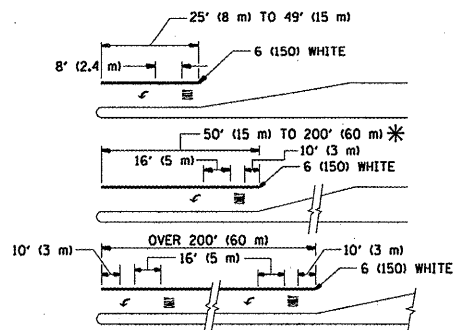


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

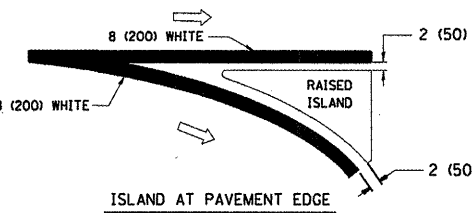
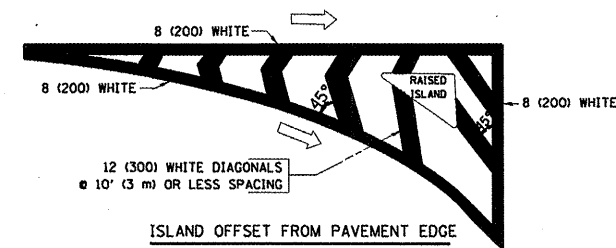
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

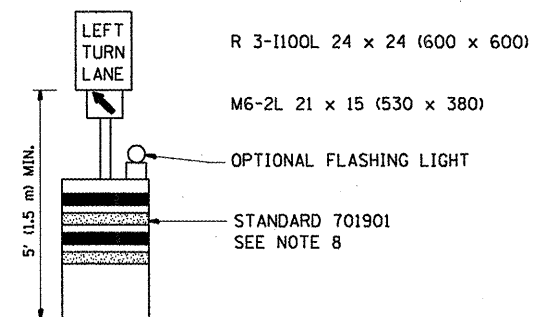
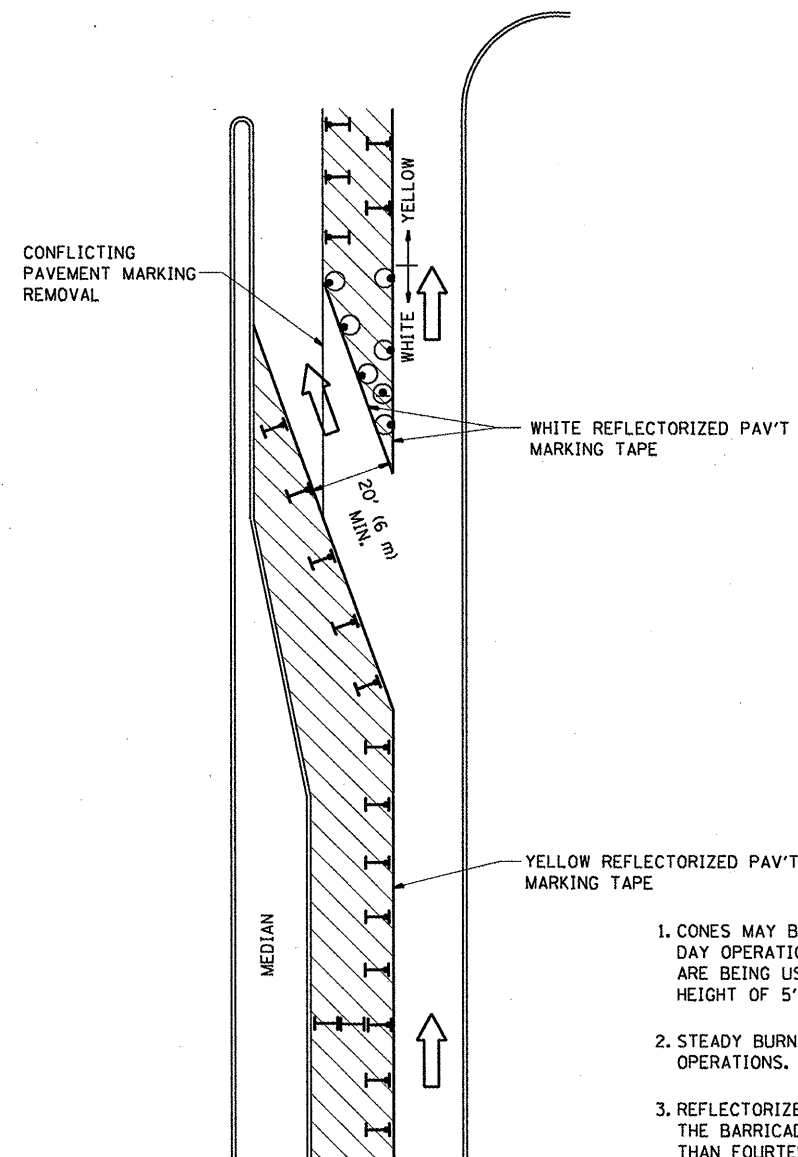


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

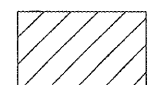







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

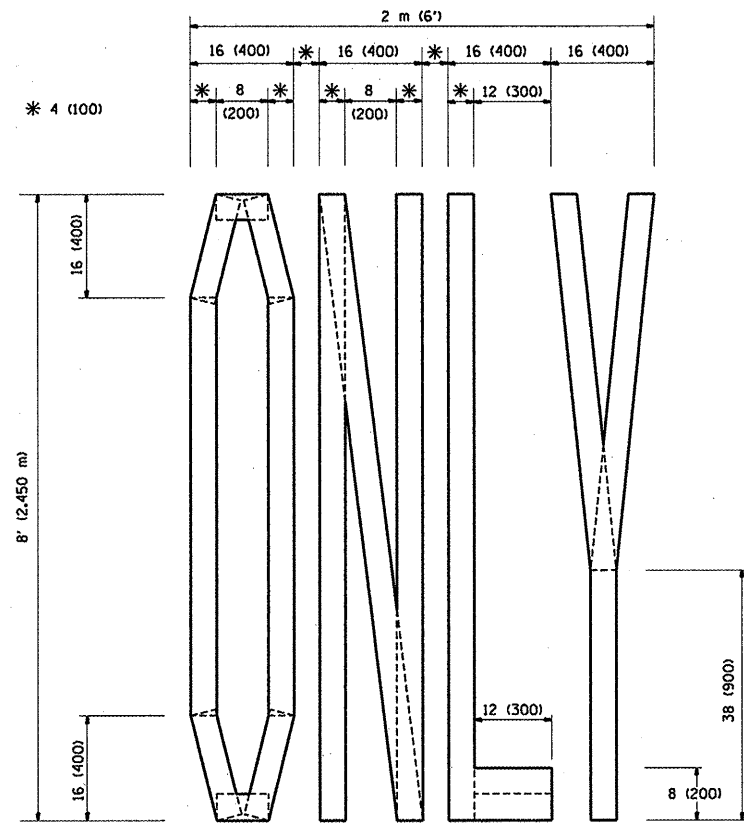
FILE NAME =	USER NAME = VelichkovVV	REVISED -T, RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
cr\pw\work\p1zdot\velichkovvv\d0260198\stStd.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -
PLOT SCALE = 100.0000' / IN.		REVISED - A. HOUSEH 10-12-96	REVISED -
PLOT DATE = 4/14/2011		REVISED -T, RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

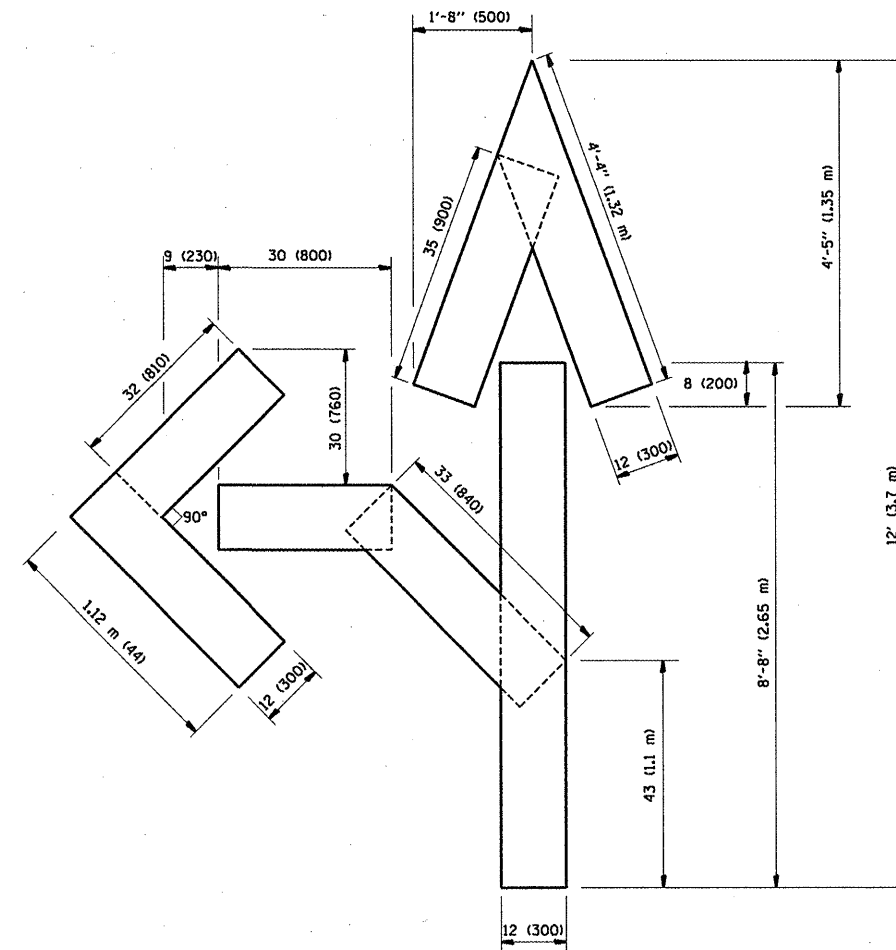
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

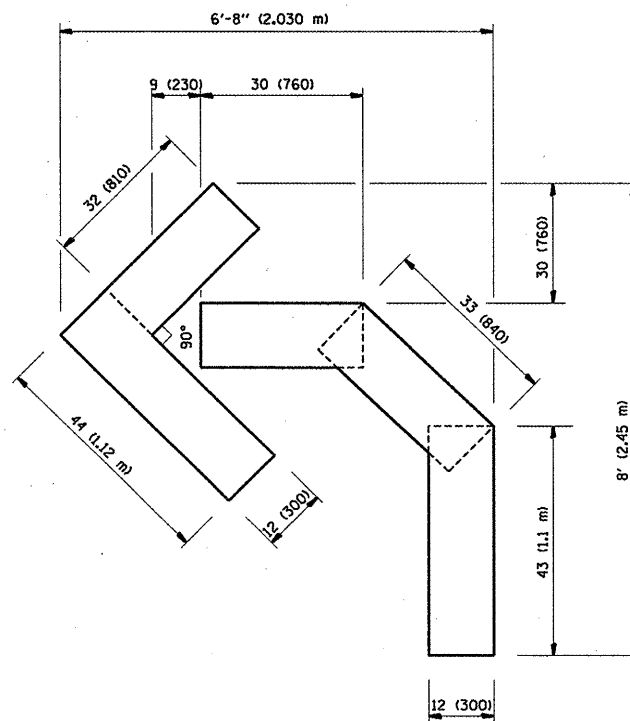
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-015-RS	COOK	34	28
TC-14		CONTRACT NO. 60P04		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = VelichkovVV	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
at\pw_work\pwsdot\velichkovvv\d0260198\d	stStd.dgn	DRAWN -	REVISED -T. RAMMACHER 11-04-97
PLOT SCALE = 100.0000 / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98	
PLOT DATE = 4/14/2011	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00	

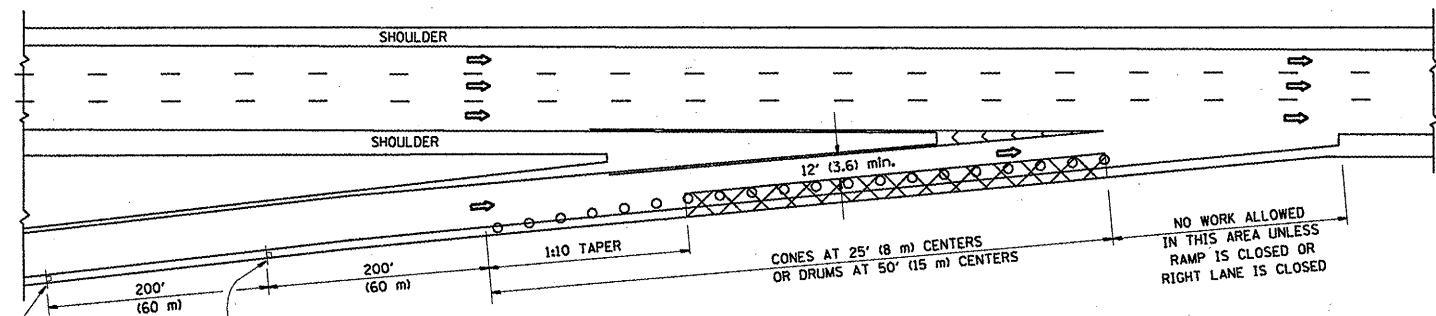
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

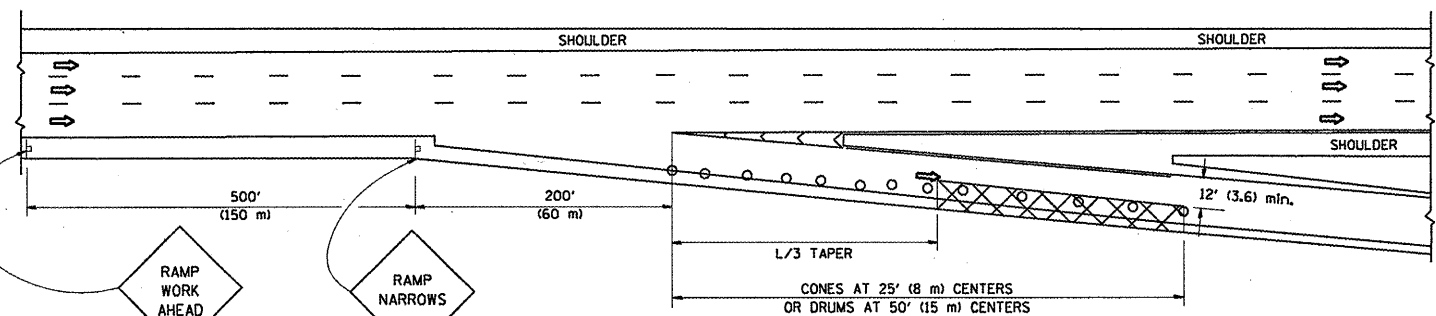
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-015-RS	COOK	34	29
TC-16		CONTRACT NO. 60P04		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

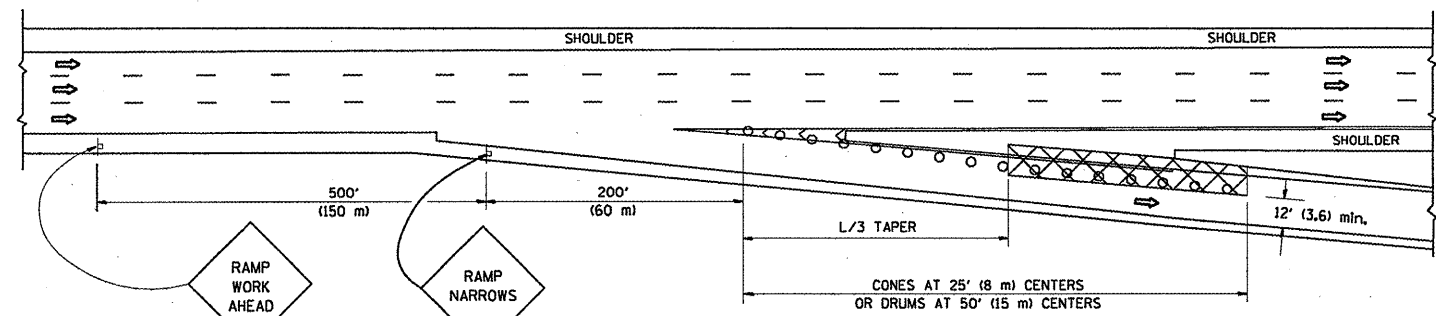
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

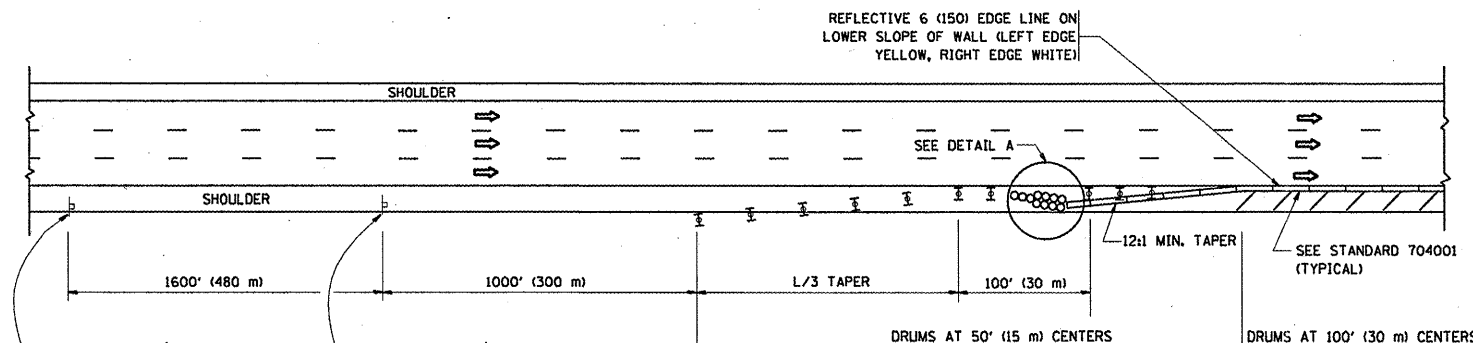
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

GENERAL NOTES

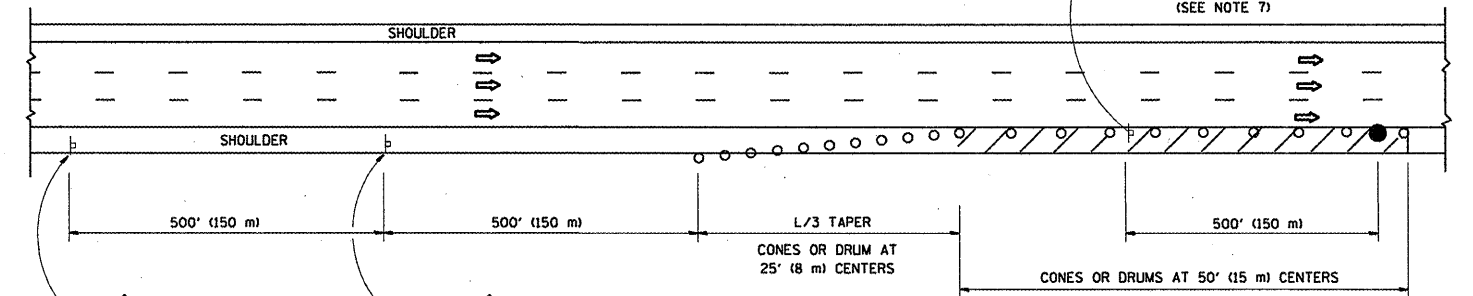
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH L=0.65(WKS) L=(WKS)
W = WIDTH OF OFFSET IN FEET (METERS)	
S = NORMAL POSTED SPEED MPH (KM/H)	
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

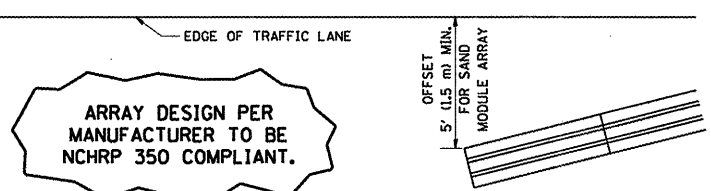


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
 1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCRUSH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.

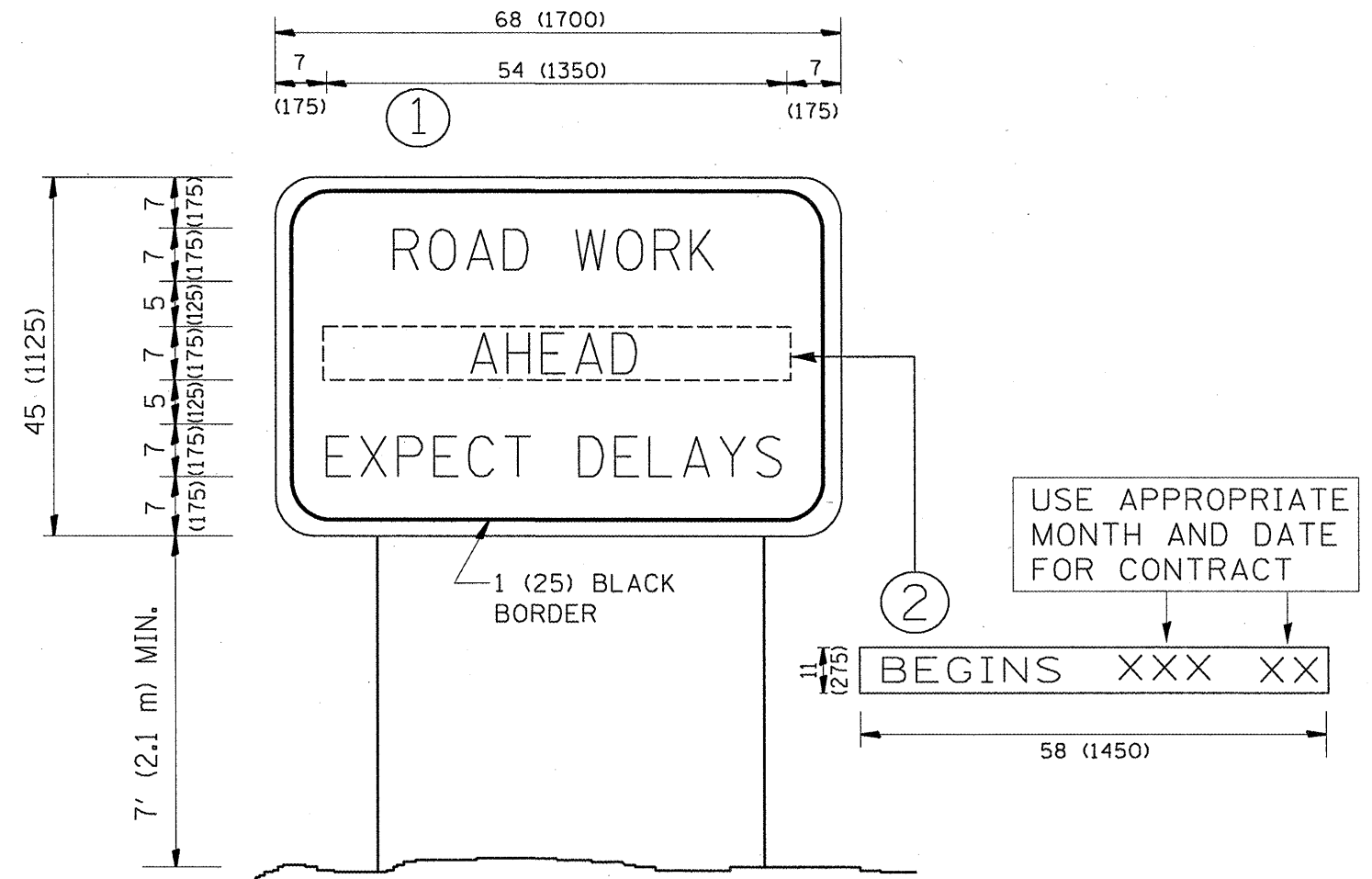


ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350 COMPLIANT.

**DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = ValichkovVV	DESIGNED -	REVISED - 04-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES	F.A. -	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
at\pwwork\pwwork\valichkovvv\02268198\d	st5td.dgn	DRAWN - D.W.S.	REVISED - J.A.F. 12-06			VAR.	2011-015-RS	COOK	34	30
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED - S.P.B. 01-07			TC-17		CONTRACT NO. 60PO4		
	PLOT DATE = 4/14/2011	DATE - 11-96	REVISED - S.P.B. 12-09			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



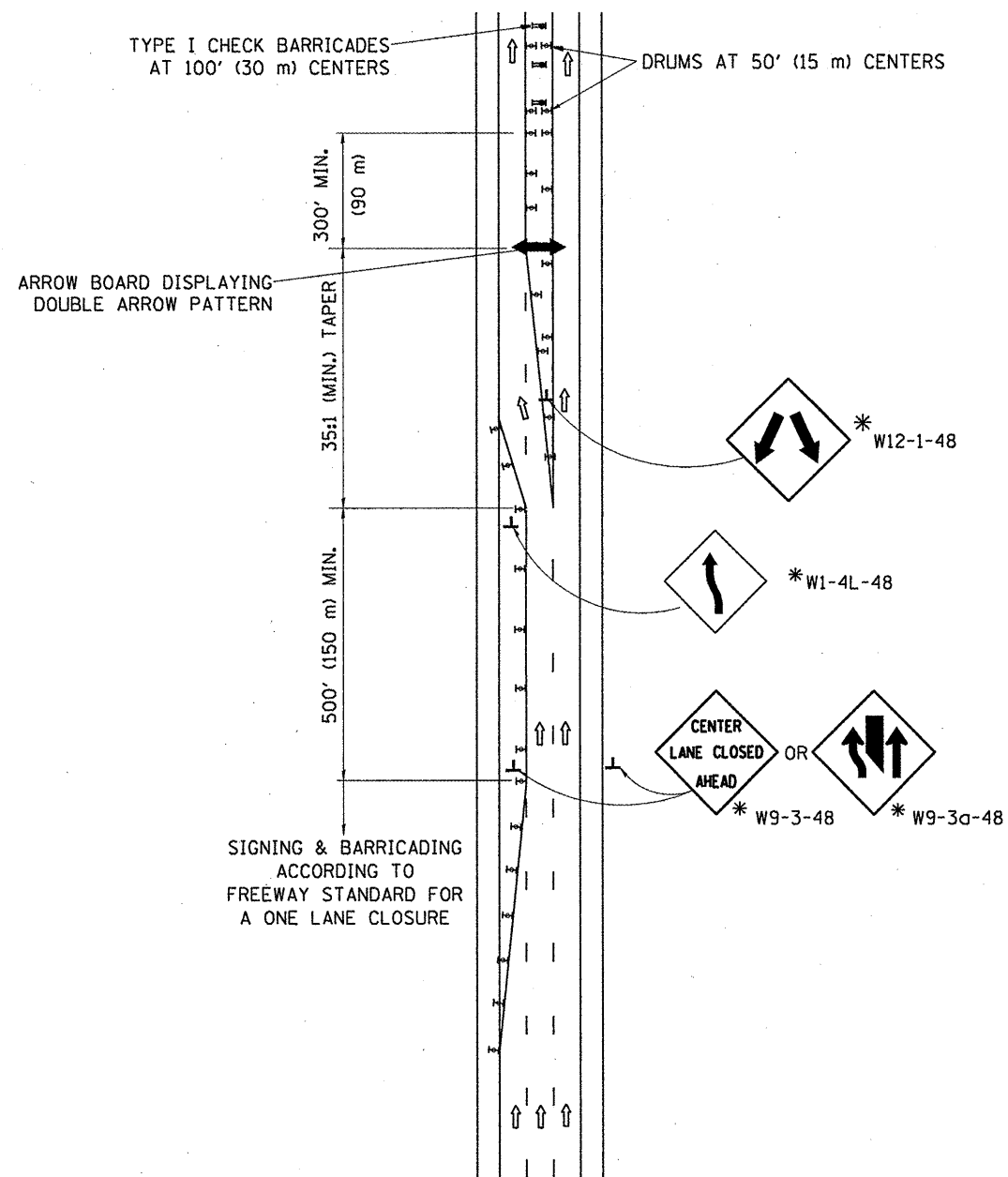
NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

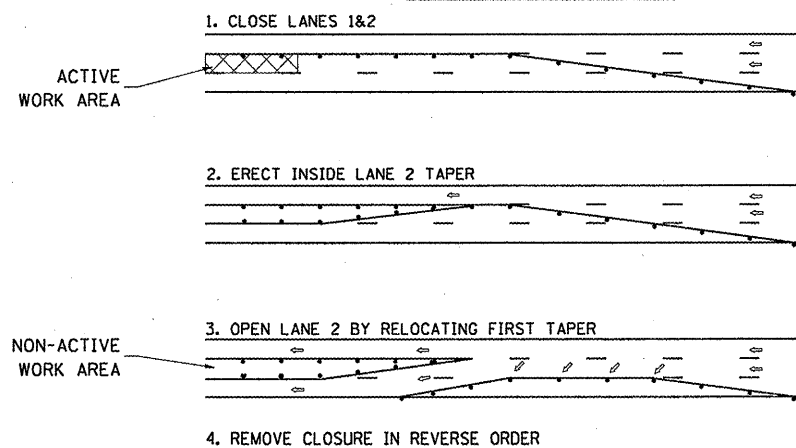
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = VelichkovVV	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
at:\pw\work\pwsdot\velichkovvv\d0268196\stStd.dgn	DRAWN -	REVISED - R. MIRS 12-11-97	VAR.			2011-015-RS	COOK	34	31	
PLOT SCALE = 100.0000 "/ IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	TC-22			CONTRACT NO. 60P04				
PLOT DATE = 4/14/2011	DATE -	REVISED - C. JUCIUS 01-31-07	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.	

CENTER LANE CLOSURE



INSTALLATION SEQUENCE

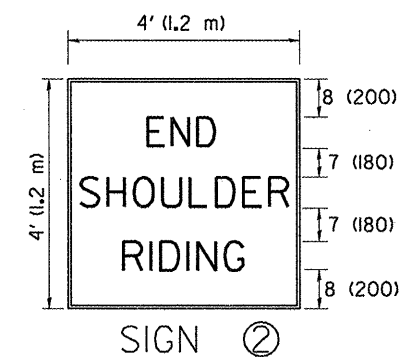
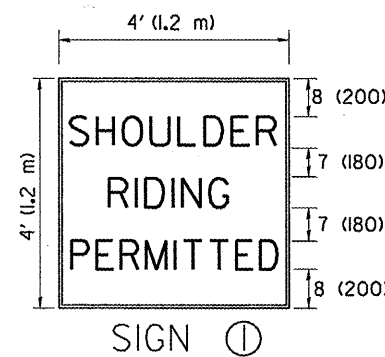
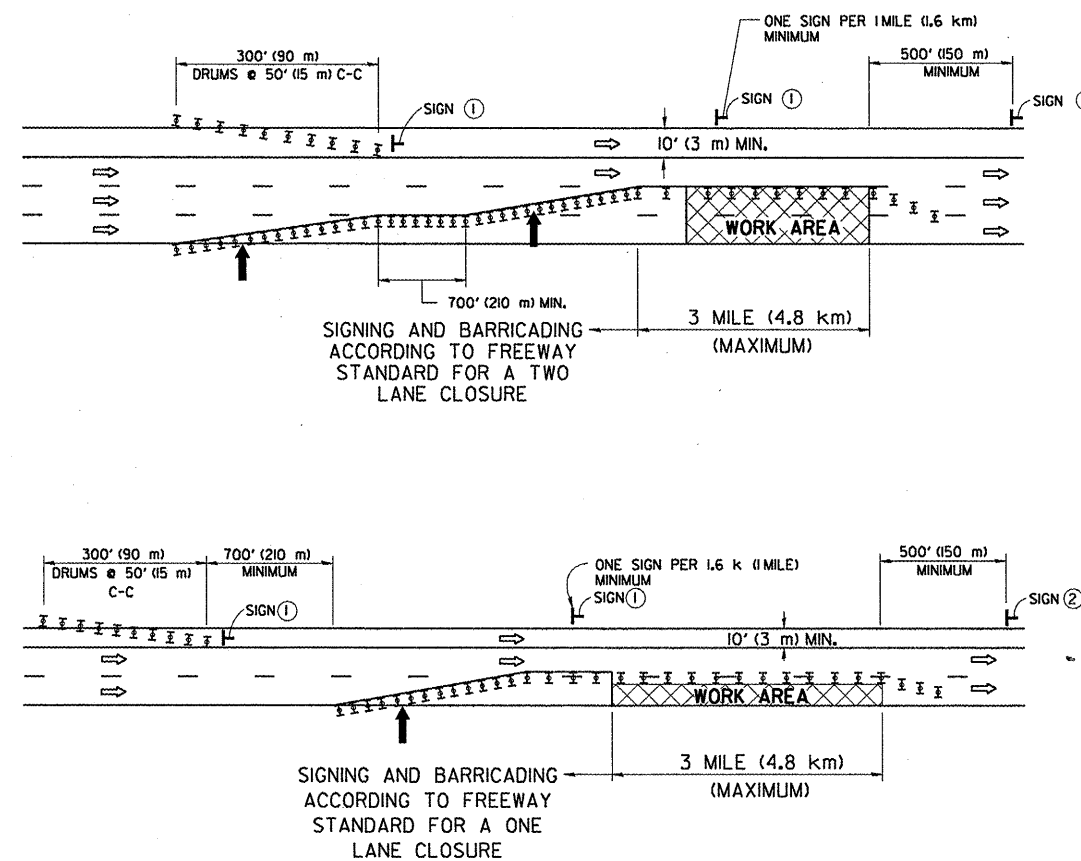


NOTES

1. DRUMS WITH STEADY BURN LIGHTS SHALL BE USED AT 50' (15 m) CENTERS ON ALL TAPERS AND TANGENTS IN ADVANCE OF WORK AREA.
2. CLOSURE SHALL BE USED ONLY FOR OPERATIONS LASTING 72 HOURS OR LESS.
3. CENTER LANE CLOSURE CONFIGURATION IS NOT TO BE USED WITH WORKERS PRESENT.

SHOULDER LANE

NOTE: CLOSURE SHALL BE USED ONLY FOR OPERATIONS LASTING 72 HOURS OR LESS.



6 (150) SERIES "C" LEGEND
BLACK LEGEND
WHITE REFLECT. BACKGROUND
1 (25) BORDER

SYMBOLS

- ↑ DIRECTION OF TRAFFIC
- ARROWBOARD
- ▣ ACTIVE WORK AREA
- ⊥ SIGN ON PORTABLE OR PERMANENT SUPPORT *
- ⊥ TYPE II BARRICADE, OR DRUM WITH MONO-DIRECTIONAL STEADY BURN LIGHT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

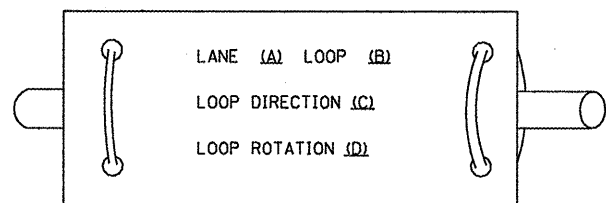
* ALL SIGNS SHALL BE MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).

FILE NAME =	USER NAME = ValichkovV	DESIGNED -	REVISED - J.A.F. 04-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY CENTER LANE CLOSURE SHOULDER LANE	F.A. -	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
at\pwork\pwork\valichkovv\d0260198\st5td.dgn	DRAWN -	REVISED - S.P.B. 01-07	VAR.			2011-015-RS	COOK	34	32	
PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED - S.P.B. 12-09	TC-25			CONTRACT NO. 60P04				
PLOT DATE = 4/14/2011	DATE -	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			

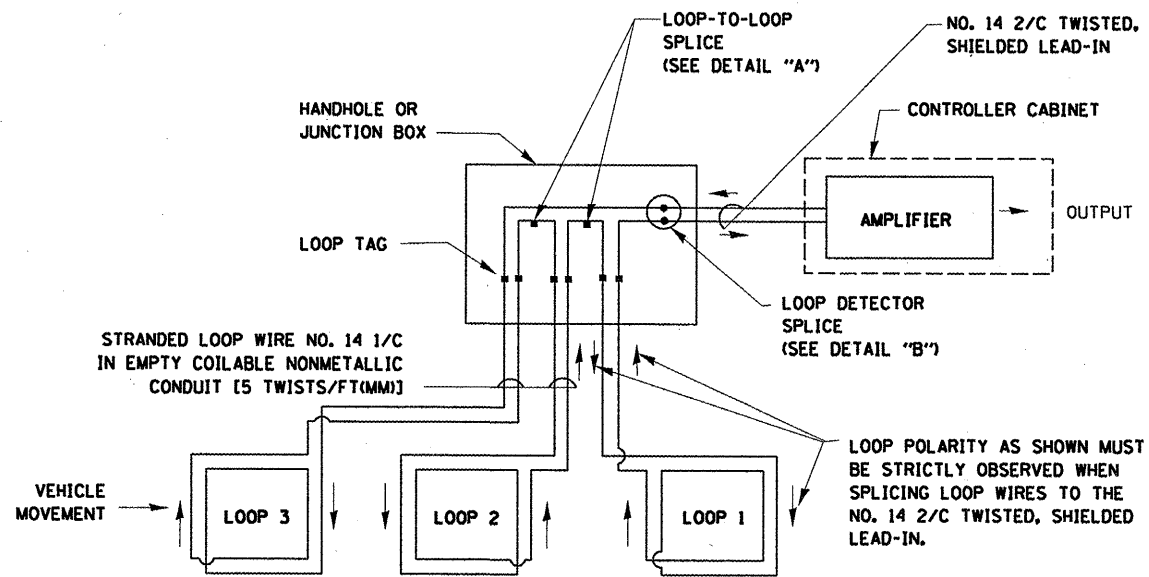
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

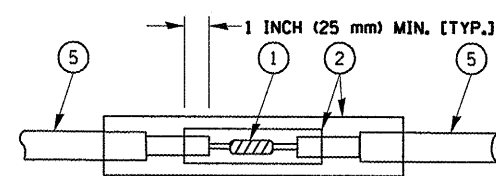


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

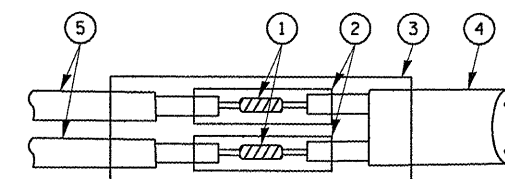


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

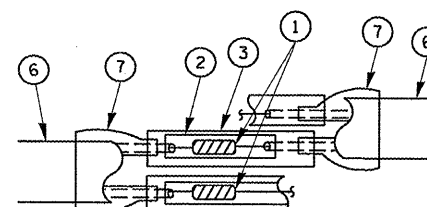


DETAIL "A" LOOP-TO-LOOP SPLICE

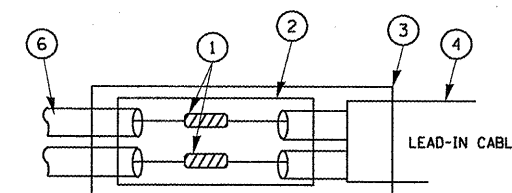


DETAIL "B" LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A" LOOP-TO-LOOP SPLICE



DETAIL "B" LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = VelichkovVV	DESIGNED - DAD	REVISED -
et\pwork\p\stdat\velichkovvv\d0260198\d	atStd.dgn	DRAWN - BCK	REVISED -
	PLOT SCALE = 100.0000 ' / IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 4/14/2011	DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

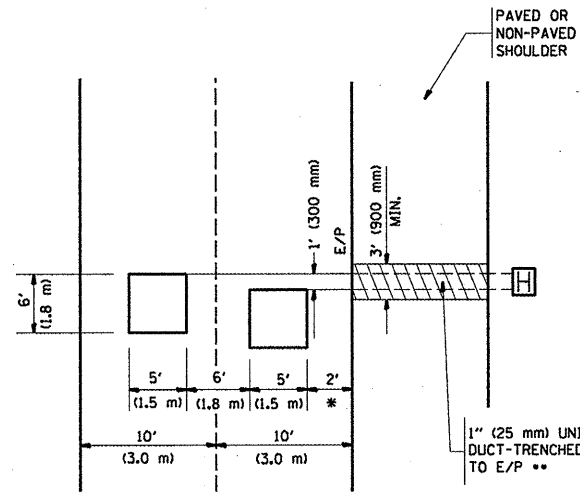
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 1 OF 6 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2011-015-RS	COOK	34	33
TS-05		CONTRACT NO. 60P04		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

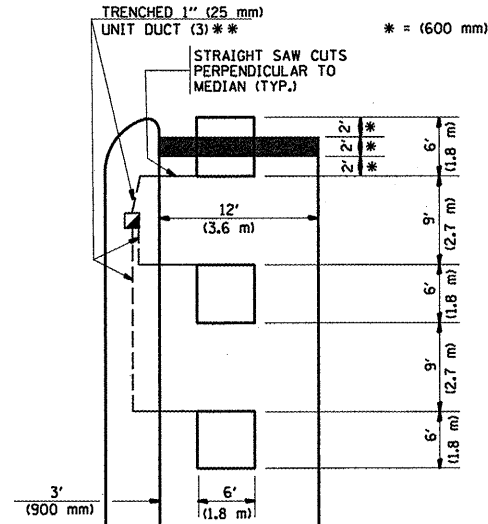


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

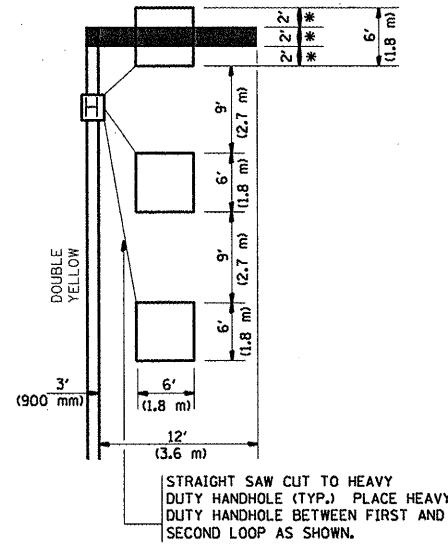


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

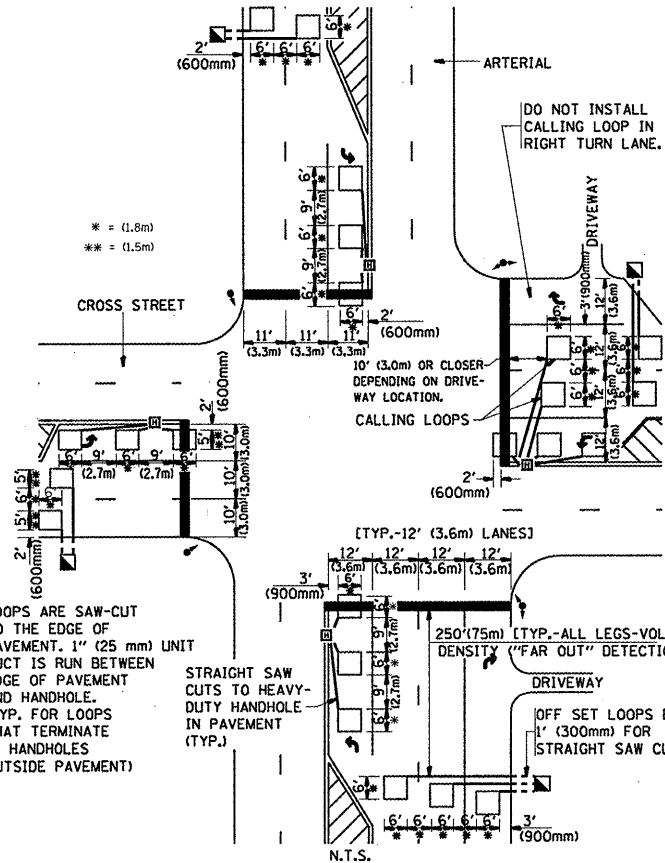
**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

* = (600 mm)



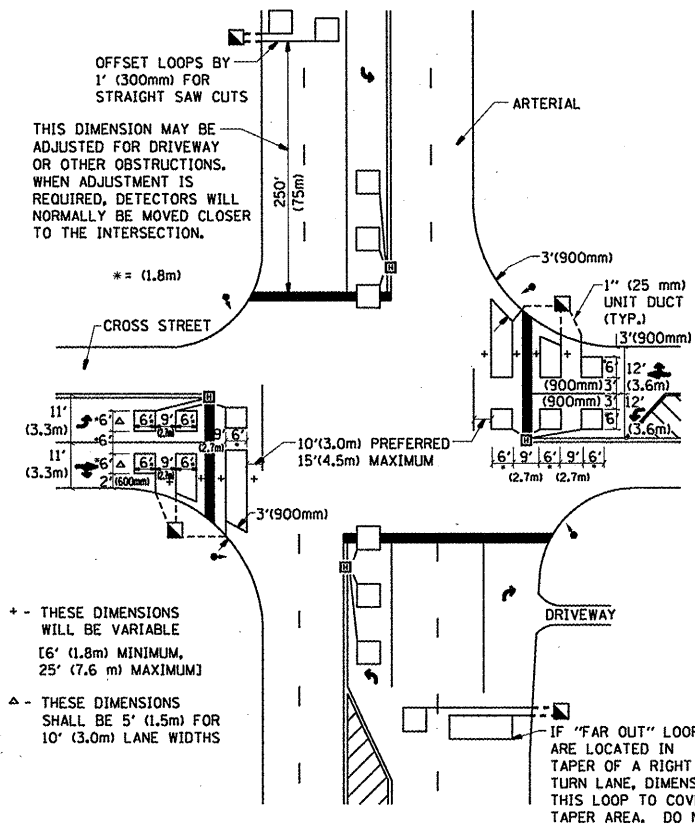
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = ValchikovV	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION		F.A. RTE.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\p\dot\valchikovv\d0260198\Std.dgn	std.dgn	DRAWN -	REVISED -		DETAILS FOR ROADWAY RESURFACING		VAR.	2011-015-RS	COOK	34	34
PLOT SCALE = 100.0000 1/4 IN.		CHECKED - R.K.F.	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TS-07		CONTRACT NO. 60P04
PLOT DATE = 4/14/2011		DATE -	REVISED -						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		