

INDEX OF SHEETS ON SHEET NO. 2

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
FAU ROUTE 2753 (THATCHER AVENUE)
MADISON STREET TO HAWTHORNE AVENUE
RESURFACING
SECTION NO. 10-00092-00-RS
PROJECT NO. M-9003(792)
VILLAGE OF RIVER FOREST
COOK COUNTY
JOB NO. C-91-412-11**

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 2753	10-00092-00-RS	COOK	12	1
ILLINOIS PROJECT ---				
CONTRACT NO. 63577				

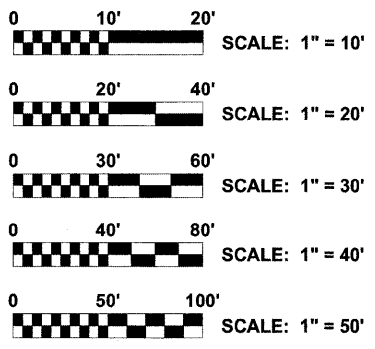
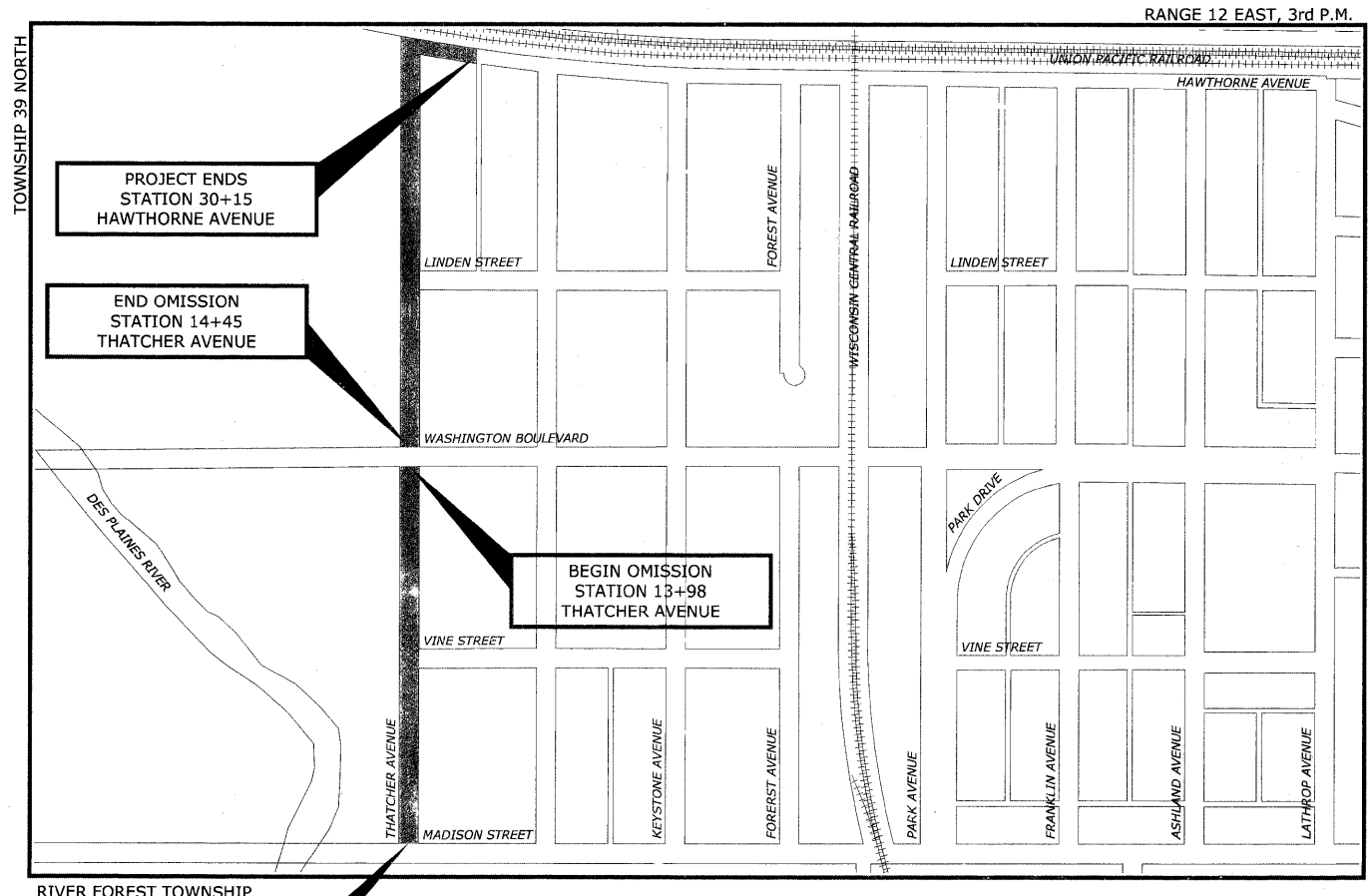


LOCATION OF SECTION INDICATED THUS:

TRAFFIC DATA
ADT (2010) = 6,800
POSTED SPEED LIMIT = 25 MPH
DESIGN SPEED LIMIT = 30 MPH

DESIGN DESIGNATION
COLLECTOR

PROJECT LOCATED IN THE VILLAGE OF RIVER FOREST



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

CONTRACT NO. 63577

- AREA OF IMPROVEMENT
NOT TO SCALE
GROSS LENGTH OF IMPROVEMENT = 2,915 FT (0.55 MI.)
NET LENGTH OF IMPROVEMENT = 2,868 FT (0.54 MI.)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 03/25 2011
John P. Riddle
VILLAGE OF RIVER FOREST, PRESIDENT

PASSED April 11 2011
Christopher J. ...
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASED FOR BID
BASED ON LIMITED
REVIEW APRIL 11, 2011
Dean M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS,
REGION 1 ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS



DATE SIGNED: 03/25/11
LICENSE EXPIRES: 11/30/11

EDWIN HANCOCK ENGINEERING COMPANY 9933
ROOSEVELT ROAD PHONE : (708) 865-0300
WESTCHESTER, ILLINOIS 60154

PROGRAM AND OFFICE ENGINEER: CHARLES P. RIDDLE, P.E., (847) 705-4406, SCHAUMBURG, IL

Drawing file: W:\Projects\74010215 - 2010 LAPP Project\Cover-ThatcherAve.dwg Mar 23, 2011 - 10:39am

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET, LOCATION MAP
2	INDEX OF SHEETS, I.D.O.T. STANDARD DRAWINGS, LEGEND OF SYMBOLS, AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	EXISTING AND PROPOSED TYPICAL CROSS SECTIONS
5-6	THATCHER AVENUE PAVING PLAN
7	DETAILS
8	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD 08)
9	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD 22)
10	BUTT JOINT AND HMA TAPER DETAILS (BD 32)
11	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC 10)
12	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC 13)

I.D.O.T. STANDARD DRAWINGS

STANDARD NO.	TITLE OR DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C&D PATCHES
604001-03	FRAMES AND LIDS, TYPE 1
701501-06	URBAN LANE CLOSURE, 2-LANE, 2-WAY, UNDIVIDED
701801-04	LANE CLOSURE, MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS

LEGEND OF SYMBOLS

EXISTING	PROPOSED	DESCRIPTION
		MANHOLE
		INLET
		CATCH BASIN
		FIRE HYDRANT
		WATER MAIN VALVE VAULT
		POWER POLE
		STREET LIGHT
		RIM ELEVATION INVERT ELEVATION
	A	STRUCTURE TO BE ADJUSTED
	A*	STRUCTURE TO BE ADJUSTED (SPECIAL)
	1C	TYPE 1 FRAME AND CLOSED LID
	1P	TYPE 1 FRAME AND OPEN LID
	RC	STRUCTURE TO BE RECONSTRUCTED

GENERAL NOTES

MAINTENANCE OF EXISTING DRAINAGE STRUCTURES
WHEN LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF GUTTERS AND DRAINAGE STRUCTURE OBSTRUCTING THE NATURAL FLOW OF WATER, THE LOOSE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE FACILITIES SHALL BE CLEAN AND FREE OF ALL OBSTRUCTIONS CAUSED BY CONSTRUCTION OPERATIONS.

MAINTENANCE OF SEWER FLOWS
THE CONTRACTOR SHALL CONDUCT HIS/HER OPERATIONS TO MAINTAIN FLOW AT ALL TIMES THROUGH EXISTING STORM AND SANITARY SEWER SYSTEMS. HE/SHE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER COLLECTED IN A SAFE MANNER WITHOUT DAMAGE OF ANY KIND TO ADJACENT PROPERTIES. THE ENDS OF EXISTING DRAINAGE LINES THAT ARE NOT TO BE INCORPORATED INTO THE PROJECT ARE TO BE SEALED AS SPECIFIED IN THE SPECIAL PROVISIONS. EXISTING STRUCTURES ARE TO BE INSPECTED BEFORE CONSTRUCTION STARTS - ACCUMULATION OF MATERIAL IN THE STRUCTURE DUE TO CONSTRUCTION OPERATIONS SHALL BE REMOVED BY THE CONTRACTOR AT HIS/HER EXPENSE.

FRAMES AND LIDS
THE LOCATION AND TYPE OF FRAMES AND LIDS REQUIRED FOR ALL MANHOLES AND VALVE VAULTS LISTED IN THE SUMMARY OF QUANTITIES MAY BE FOUND ON THE PLANS. WHERE LIDS ARE CALLED FOR ON THE PLANS, THEY SHALL BE IN ACCORDANCE WITH ARTICLE 604.04 OF THE STANDARD SPECIFICATIONS AND THE TERM LID IS USED IN LIEU OF GRATE. ALL LIDS ON SANITARY MANHOLES, COMBINED SEWER MANHOLES, AND VALVE VAULTS SHALL BE OF THE SELF SEALING TYPE.

THE FRAMES AND LIDS OF EXISTING CATCH BASINS, INLETS, MANHOLES, AND VALVE VAULTS THAT ARE TO BE ABANDONED ARE TO REMAIN THE PROPERTY OF THE VILLAGE OF RIVER FOREST. THE CONTRACTOR IS TO DELIVER FRAMES AND LIDS TO THE VILLAGE OF RIVER FOREST PUBLIC WORKS YARD LOCATED AT 45 FOREST AVENUE. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF EACH BID ITEM.

FRAMES AND LIDS, TYPE 1
ALL COMBINED SEWER CLOSED LIDS SHALL BE SELF-SEALING WITH RECESSED PICK HOLES. ALL FRAMES WITH CLOSED LIDS TO BE FURNISHED AS PART OF THIS CONTRACT FOR CONSTRUCTION, ADJUSTMENT, OR RECONSTRUCTION OF ANY MANHOLE OR VALVE VAULT SHALL HAVE CAST INTO THE LID ONE OF THE FOLLOWING WORDS:

STRUCTURE TYPE	NEENAH FOUNDRY* FRAME AND LID	SELF SEALING	WORD CAST INTO LID
SANITARY MANHOLE	R-1713-B	YES	SANITARY
COMBINATION MANHOLE	R-1713-B	YES	SANITARY
STORM MANHOLE	R-1713-B	NO	STORM
VALVE VAULT	R-1713-B	YES	WATER
CATCH BASIN	R-2504-D	NO	---
INLET	R-2504-D	NO	---

* NOTE: OR ENGINEER APPROVED EQUAL.
PRECAST CONCRETE SECTIONS FOR STRUCTURES
ALL NEW STRUCTURES AND STRUCTURES TO BE RECONSTRUCTED SHALL BE BUILT WITH PRECAST REINFORCED CONCRETE SECTIONS FABRICATED IN ACCORDANCE WITH A.S.T.M. DESIGNATION C-478.

CONNECTING EXISTING PIPES TO STRUCTURES
ANY PIPE, UP TO FOUR FEET (4') IN LENGTH PER EACH PIPE AND THE NECESSARY TRENCH BACKFILL, USED TO CONNECT EXISTING PIPES TO THE STRUCTURE TO BE RECONSTRUCTED OR TO THE NEW STRUCTURE SHALL BE INCLUDED IN THE COST OF THE STRUCTURE TO BE RECONSTRUCTED OR THE NEW STRUCTURE.

CONNECTION OF DISSIMILAR PIPE MATERIALS
NON-SHEAR CONNECTORS SHALL BE USED WHERE NEW SEWER PIPES ARE TO BE CONNECTED TO EXISTING SEWER PIPES. THE NON-SHEAR CONNECTORS SHALL CONSIST OF A SYNTHETIC RUBBER SLEEVE, WHICH WILL RESIST DETERIORATION CAUSED BY WASTES FOUND IN SEWERS AND TWO STAINLESS STEEL STRAPS. THE CONNECTORS SHALL BE "MISSION" COUPLINGS, "FERNCO" COUPLINGS, OR ENGINEER APPROVED EQUAL. ALL LABOR AND MATERIAL REQUIRED TO CONNECT NEW SEWER PIPE TO EXISTING SEWER PIPE SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

FORMS FOR CONCRETE SIDEWALKS, DRIVEWAYS AND GUTTER FLAGS
THIS WORK SHALL BE IN ACCORDANCE WITH ARTICLE 424.05, 423.05, AND 1103.05 EXCEPT THAT A 2 x 6 BOARD WILL BE USED AS A FORM FOR ALL SIDEWALKS TO BE INSTALLED FIVE INCHES (5") IN THICKNESS. A 2 x 8 BOARD WILL BE USED AS A FORM FOR ALL DRIVEWAYS TO BE INSTALLED SEVEN INCHES (7") IN THICKNESS. A 2 x 10 BOARD WILL BE USED AS A FORM FOR ALL DRIVEWAYS TO BE INSTALLED EIGHT INCHES (8") IN THICKNESS. A 2 x 12 BOARD WILL BE USED AS A FORM FOR ALL GUTTER FLAGS TO BE INSTALLED TEN INCHES (10") IN THICKNESS. ALL FORMS MUST BE OF A MINIMUM HEIGHT OF THE PROPOSED THICKNESS OF THE RESPECTIVE CONCRETE ITEMS TO BE INSTALLED.

AGGREGATE BEDDING FOR CONCRETE WORK
NEW SIDEWALK AND DRIVEWAY PAVEMENT SHALL BE PLACED ON A MINIMUM OF TWO INCHES (2") OF COMPACTED CA-6 STONE BEDDING. 8" DRIVEWAY PAVEMENT SHALL BE PLACED ON A MINIMUM OF FOUR INCHES (4") OF COMPACTED CA-6 STONE BEDDING. ADDITIONAL AGGREGATE REQUIRED TO ADJUST THE EXISTING ELEVATION OF THE SUBGRADE TO THE PROPOSED ELEVATION WILL BE INCLUDED AS PART OF THIS WORK.

THIS ITEM WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE UNIT PRICE FOR THE RESPECTIVE CONCRETE ITEMS IN THE CONTRACT.
BUTT JOINTS
BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER" DETAIL SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

PROTECTIVE COAT
ALL NEW CONCRETE SURFACES SHALL BE PROTECTIVELY COATED WITH A BOILED LINSEED OIL MIXTURE REGARDLESS OF THE TIME OF YEAR THE CONCRETE WORK IS CONSTRUCTED. THE PREPARATION AND APPLICATION OF THIS MIXTURE SHALL BE IN ACCORDANCE WITH ARTICLE 420.18 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

STANDARDS
REFERENCES TO STANDARDS IN THE PLANS AND SPECIAL PROVISIONS SHALL BE THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION AS SHOWN ON THE INDEX OF SHEETS. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED JANUARY 1, 2007, THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2011, THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JULY 2009 SIXTH EDITION, THE DETAILS IN THE PLANS, AND THE SPECIAL PROVISIONS.

UNDERGROUND UTILITIES
BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 811 FOR FIELD LOCATIONS OF BURIED ELECTRICAL, TELEPHONE AND GAS FACILITIES (48 HOURS NOTIFICATION IS REQUIRED).

THE LOCATIONS OF THE UNDERGROUND UTILITIES, IF SHOWN ON THE PLANS, HAVE BEEN OBTAINED BY FIELD SURVEYS AND SEARCHES OF AVAILABLE RECORDS. IT IS BELIEVED THAT DATA IS ESSENTIALLY CORRECT, BUT THE VILLAGE OF RIVER FOREST, THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND/OR OTHER OFFICES AND AGENCIES ASSOCIATED WITH THE DEVELOPMENT OF THESE PLANS DO NOT GUARANTEE THEIR ACCURACY OR COMPLETENESS. THE CONTRACTOR WILL BE REQUIRED TO VERIFY THE LOCATION OF EACH FACILITY WITH THE UTILITY COMPANY, AND SHALL TAKE DUE CARE IN ALL PHASES OF THE CONSTRUCTION TO PROTECT SUCH FACILITIES THAT MAY BE AFFECTED BY THE WORK. ALL DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE IN ACCORDANCE WITH ARTICLES 107.20 AND 107.31.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY OWNERS AND THE ENGINEER.

NOTIFICATION OF RESIDENTS
THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING WRITTEN NOTICE TO ALL RESIDENCES AND/OR PLACES OF BUSINESS AT LEAST ONE (1) WORKING DAY PRIOR TO PERFORMING ALL CONSTRUCTION ACTIVITY THAT WILL ELIMINATE ACCESS TO THEIR PROPERTY. THE WRITTEN NOTICE SHALL BE APPROVED BY THE ENGINEER AND A COPY PROVIDED TO THE VILLAGE PRIOR TO THE BEGINNING OF CONSTRUCTION.

MAINTENANCE OF TRAFFIC
THATCHER AVENUE WILL REMAIN OPEN TO THRU TRAFFIC DURING ALL OPERATIONS. FLAGGERS SHALL BE USED FOR THE INSTALLATION OF THE POLYMERIZED LEVELING BINDER, THE HOT-MIX ASPHALT SURFACE AND THERMOPLASTIC STRIPING.

FIELD OFFICE
THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR VILLAGE PROPERTY ROW WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.

BARRICADES
THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED, ONE (1) WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL.

OPEN EXCAVATIONS
THE CONTRACTOR SHALL NOT BE ALLOWED TO LEAVE EXCAVATION NECESSARY FOR PAVEMENT PATCHES OR STRUCTURE ADJUSTMENTS OPEN OVERNIGHT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETELY BACKFILLING OR INSTALLING A PLATE OVER ALL EXCAVATIONS AT THE END OF EACH DAY IN ACCORDANCE WITH ARTICLE 107.09.

CONCRETE BREAKERS
WHEN REMOVING PAVEMENT AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS SUCH AS DROP HAMMERS, WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES, WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.

SAW CUTTING
THE CONTRACTOR SHALL SAW CUT ASPHALT PAVEMENT AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING PAVEMENT TO BE REMOVED BY APPROVED MEANS OR AN APPROVED CONCRETE SAW TO A DEPTH AS DIRECTED BY THE ENGINEER. SUITABLE GUIDELINES OR DEVICES SHALL BE USED TO ASSURE CUTTING A NEAT, STRAIGHT LINE AS SHOWN ON THE PLANS. CARE SHALL BE TAKEN BY THE CONTRACTOR NOT TO DAMAGE THE REMAINING PAVEMENT DIRECTLY ADJACENT TO THE PAVEMENT TO BE REMOVED. DAMAGE TO THE EXISTING PAVEMENT RESULTING FROM PAVEMENT REMOVAL OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THE COST OF SAW CUTTING DESCRIBED ABOVE SHALL BE INCLUDED IN THE COST OF ITEM BEING REMOVED.

CURB AND GUTTER TRANSITIONS
TRANSITIONS FROM THE PROPOSED CURB AND GUTTERS TO THE DEPRESSED CURB AND GUTTERS AND PROPOSED CURB AND GUTTERS TO EXISTING CURB AND GUTTERS SHALL BE CONSTRUCTED AS DIRECTED BY THE ENGINEER.
THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE UNIT PRICE OF THE RESPECTIVE COMBINATION CONCRETE CURB AND GUTTER ITEMS.

PAVEMENT PATCHING
LOCATIONS OF PATCHES WILL BE DETERMINED IN FIELD BY ENGINEER AFTER MILLING OPERATIONS.

CURING AND PROTECTION
AFTER THE CONCRETE HAS BEEN FINISHED AND THE WATER SHEEN HAS DISAPPEARED FROM THE SURFACE OF THE CONCRETE, THE SURFACE SHALL BE SEALED WITH MEMBRANE CURING COMPOUND OF A TYPE APPROVED BY THE ENGINEER. THE SEAL SHALL BE MAINTAINED FOR THE SPECIFIED CURING PERIOD. THE EDGES OF THE CONCRETE SHALL ALSO BE SEALED IMMEDIATELY AFTER THE FORMS ARE REMOVED. IN ADDITION, ALL CONCRETE PLACED DURING PERIODS OF COLD WEATHER SHALL BE PROTECTED IN ACCORDANCE WITH ARTICLE 720.13 OF THE STANDARD SPECIFICATIONS. THIS WORK SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

THE WORK SHALL BE UNDER THE CHARGE AND CARE OF THE CONTRACTOR UNTIL FINAL ACCEPTANCE BY THE DEPARTMENT. THE CONTRACTOR SHALL ASSUME ALL RESPONSIBILITY FOR INJURY OR DAMAGE TO THE WORK FROM ANY CAUSE WHATSOEVER AND HE SHALL REBUILD, REPAIR OR RESTORE THE DAMAGED WORK AT HIS OWN EXPENSE.
THIS ITEM WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE UNIT PRICE FOR THE VARIOUS CONCRETE ITEMS IN THE CONTRACT.

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HANCOCK ENGINEERING
Civil Engineers
Municipal Consultants
Established 1911
9532 Roswell Road
Waukegan, Illinois 60154-2708
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Fax: 708/965-1212

DESIGNED	--	REVISED	--
DRAWN	--	REVISED	--
CHECKED	--	REVISED	--
DATE	01/28/11	REVISED	--

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, I.D.O.T. STANDARD DRAWINGS,
LEGEND OF SYMBOLS, AND GENERAL NOTES**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2753	10-00092-09-RS	COOK	12	2
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 63577	
FED. AID PROJECT			---	

SUMMARY OF QUANTITIES

CODE	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE RESURFACING 005 80% FEDERAL 20% LOCAL
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	170	170
25200100	SODDING	SQ YD	120	120
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	1830	1830
40600300	AGGREGATE (PRIME COAT)	TON	40	40
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	100	100
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1050	1050
42101300	PROTECTIVE COAT	SQ YD	160	160
42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	20	20
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	180	180
42400800	DETECTABLE WARNINGS	SQ FT	40	40
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	9120	9120
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	20	20
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	500	500
44000600	SIDEWALK REMOVAL	SQ FT	550	550
44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	100	100
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	200	200
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	500	500
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	700	700
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	9	9
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	12	12
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	10	10
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	10	10
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	500	500
67100100	MOBILIZATION	L SUM	1	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
70300100	SHORT TERM PAVEMENT MARKING	FOOT	650	650
★ 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	720	720
★ 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	260	260
★ 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	70	70
X4060826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	525	525
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	14	14

★ DENOTES SPECIALTY ITEM

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◆ Civil Engineers
◆ Municipal Consultants
◆ Established 1911

933 Roseville Road
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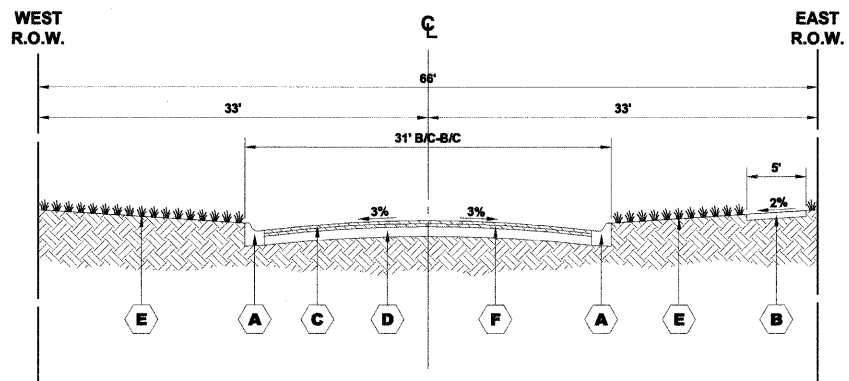
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DATE - 01/28/11	REVISED - --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

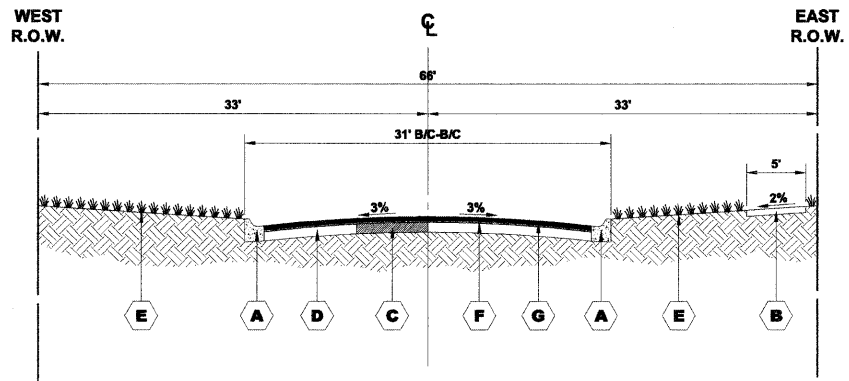
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2753	10-00092-00-RS	COOK	12	3
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 63577	
FED. AID PROJECT				



SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
A	EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12	D	EXISTING PORTLAND CEMENT CONCRETE BASE COURSE
B	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK	E	EXISTING LANDSCAPED PARKWAY
C	EXISTING HOT-MIX ASPHALT SURFACE COURSES, 2"	F	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"

**EXISTING TYPICAL SECTION
(MADISON STREET TO HAWTHORNE AVENUE)
STA. 1+00 TO STA. 30+15**



PROPOSED HOT-MIX ASPHALT SURFACE COURSE SHALL BE 1/4" ABOVE EDGE OF PAVEMENT.

SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
A	PROPOSED INTERMITTENT COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	E	EXISTING LANDSCAPED PARKWAY
B	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK	F	PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
C	PROPOSED CLASS D PATCH	G	PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
D	EXISTING PORTLAND CEMENT CONCRETE BASE COURSE		

**PROPOSED TYPICAL SECTION
(MADISON STREET TO HAWTHORNE AVENUE)
STA. 1+00 TO STA. 30+15**

HOT-MIX ASPHALT (HMA) MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2" (IL-9.5 mm)	4% @ 50 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL - 4.75, N50, 1"	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES (HOT-MIX ASPHALT BINDER COURSE, IL - 19.0), 8" (IN 3 LIFTS)	4% @ 70 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.
THE CONTRACTOR SHALL MILL BEFORE PATCHING.

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DATE	01/28/11	REVISED	---

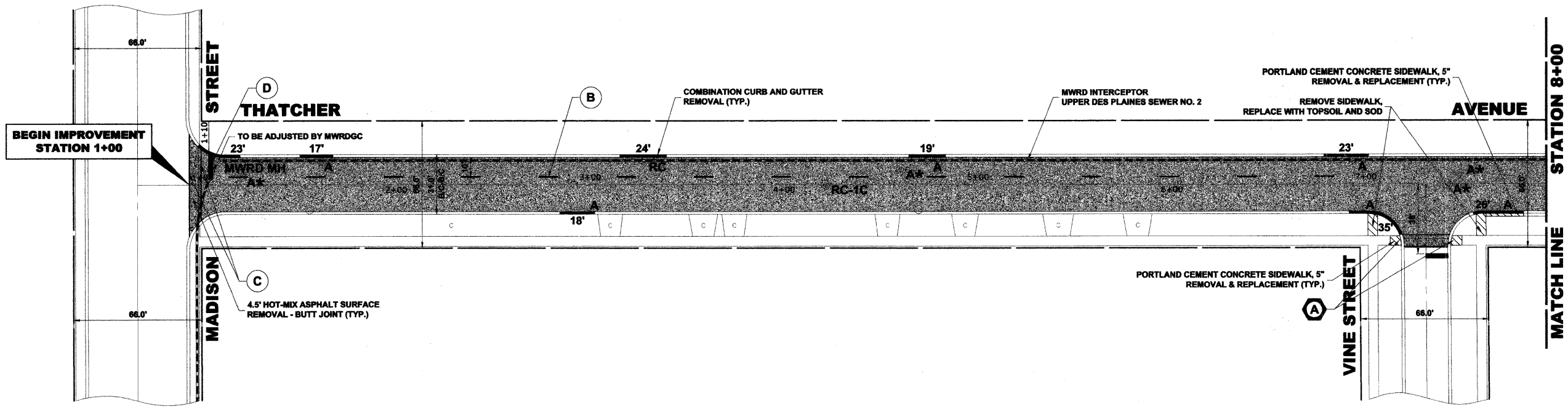
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED
TYPICAL CROSS SECTIONS**

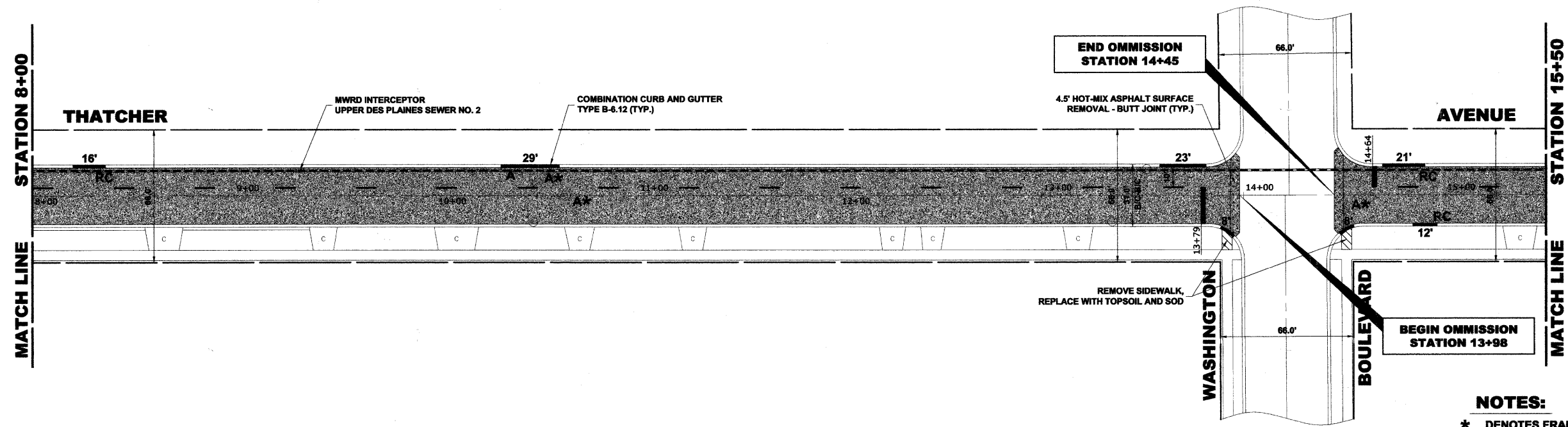
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2753	10-00092-00-RS	COOK	12	4
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 63577	
FED. AID PROJECT			---	

Drawing file: W:\Projects\74010215 - 2010 LAPP Project\Thatcher Ave\Thatcher Ave.dwg Mar 24, 2011 - 3:42pm



PAVEMENT MARKINGS LEGEND	
ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING LINE, 4", SKIP-DASH, YELLOW	(B)
THERMOPLASTIC PAVEMENT MARKING LINE, 6", CROSSWALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE, 24", STOP BAR, WHITE	(D)



- NOTES:**
- * DENOTES FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 - THE LOCATIONS FOR PATCHING "D" WORK TO BE DETERMINED IN FIELD BY THE ENGINEER
 - (A) DETECTABLE WARNING

HANCOCK ENGINEERING
 Civil Engineers
 Municipal Consultants
 Established 1911
 1531 Thompson Road
 Woodstock, Illinois 60154-3780
 Phone: 708/865-4369
 Fax: 708/865-3212

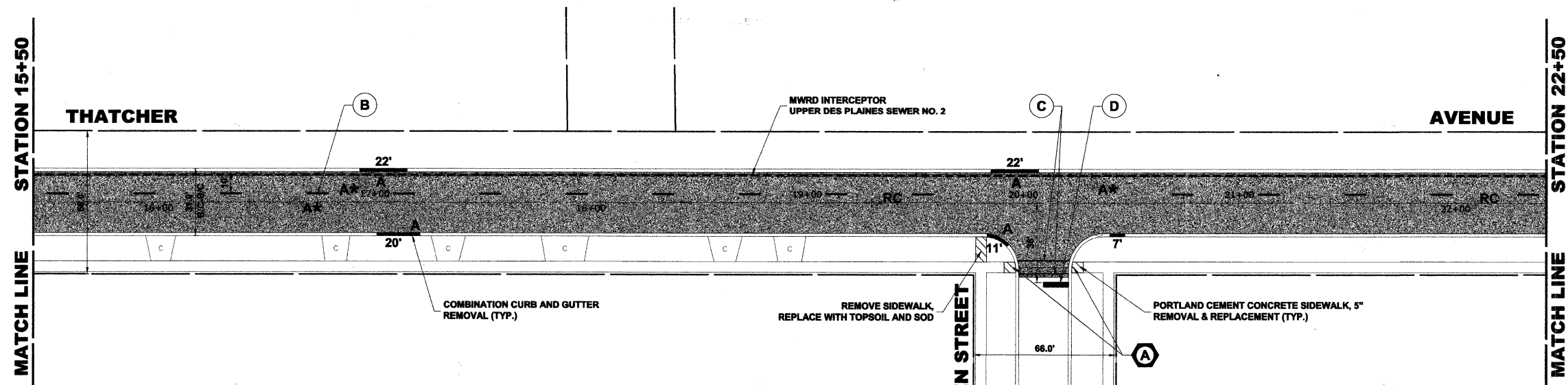
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DRAWN	--	REVISED	--
CHECKED	--	REVISED	--
DATE	01/28/11	REVISED	--

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

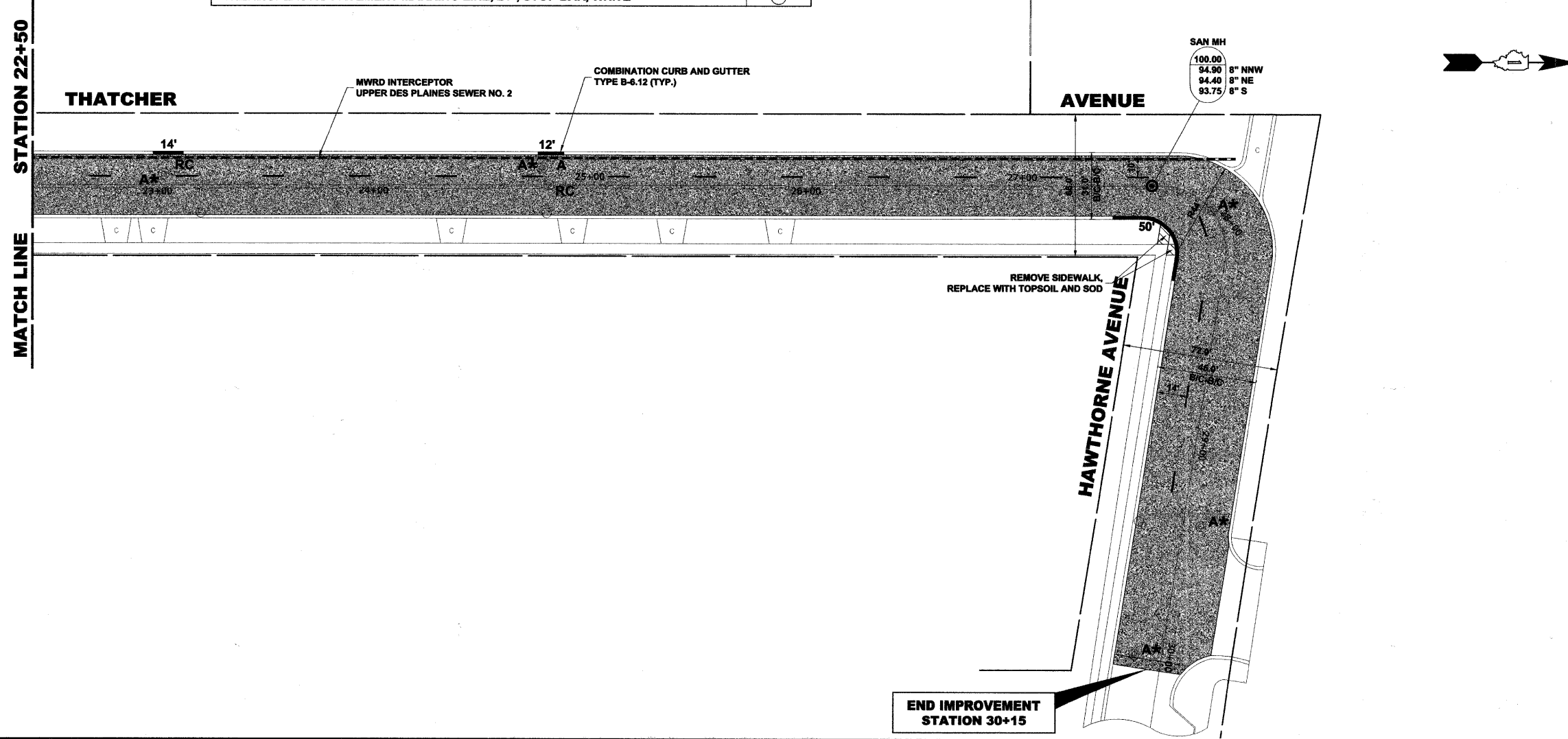
PAVING PLAN

SCALE: 1" = 30' SHEET NO. 1 OF 2 SHEETS STA. 1+00 TO STA. 15+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2753	10-00092-00-RS	COOK	12	5
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 63577	
FED. AID PROJECT				



PAVEMENT MARKINGS LEGEND	
ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING LINE, 4", SKIP-DASH, YELLOW	(B)
THERMOPLASTIC PAVEMENT MARKING LINE, 6", CROSSWALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE, 24", STOP BAR, WHITE	(D)



NOTES:

- * DENOTES FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
- THE LOCATIONS FOR PATCHING "D" WORK TO BE DETERMINED IN FIELD BY THE ENGINEER
- (A) DETECTABLE WARNING

END IMPROVEMENT
STATION 30+15

Drawing file: W:\Projects\74010215 - 2010 LAPP Project\Thatcher Ave\Thatcher Ave.dwg Mar 24, 2011 - 3:42pm

HANCOCK ENGINEERING
 Civil Engineers
 Municipal Consultants
 Established 1911
 9533 Roswell Road
 Westchester, Illinois 60154-2790
 Phone: 708/965-0300
 Fax: 708/965-1212

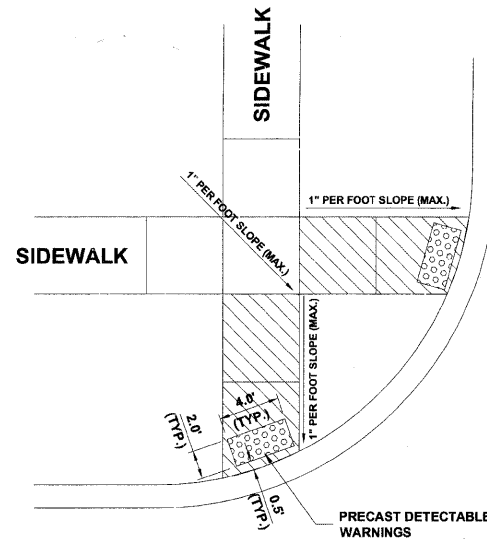
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DRAWN	--	REVISED	--
CHECKED	--	REVISED	--
DATE	01/28/11	REVISED	--

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

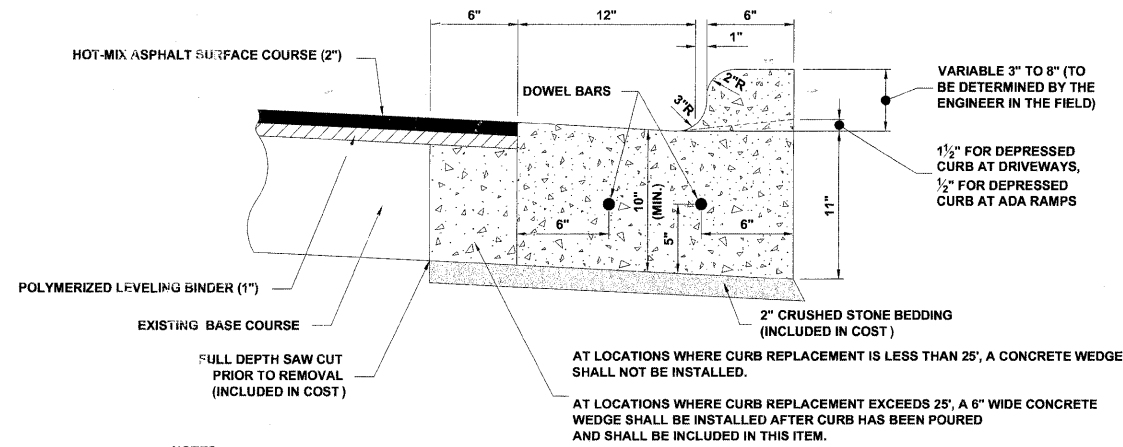
PAVING PLAN

SCALE: 1" = 30' SHEET NO. 2 OF 2 SHEETS STA. 15+50 TO STA. 30+15

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2753	10-00092-00-RS	COOK	12	6
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 63577	
FED. AID PROJECT				



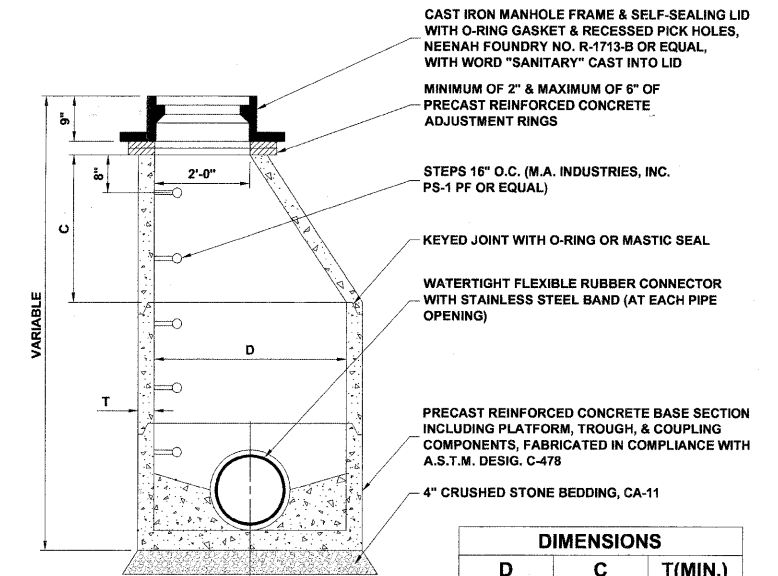
A.D.A. RAMP



NOTES:

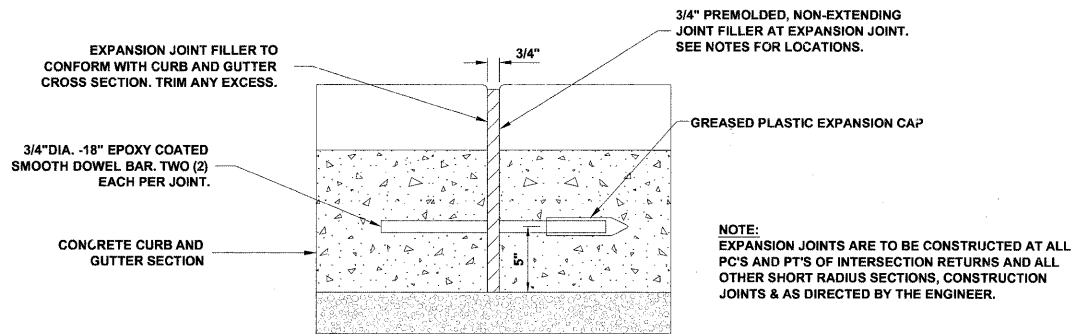
1. PROVIDE 2 - 3/4" DIAMETER, 18" LONG EPOXY COATED SMOOTH BARS WITH PLASTIC EXPANSION CAPS AT EACH EXPANSION JOINT.
2. CONTRACTION JOINT - 2" DEEP CONTRACTION JOINTS SHALL BE SAWED AT EQUAL SPACES (NOT EXCEEDING 15 FEET) BETWEEN NORMAL EXPANSION JOINTS, IN THE UPPER 1/3 OF CURB & GUTTERS WITHIN 24 HOURS OF PLACEMENT.
3. A LEAN CONCRETE WEDGE IS TO BE POURED IN THE SPACE BETWEEN THE NEW CURB & THE EXISTING PAVEMENT AFTER THE CURB HAS REACHED ITS INITIAL SET.

COMBINATION CONCRETE CURB & GUTTER

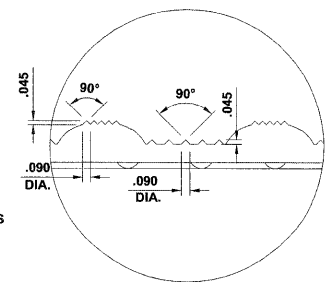
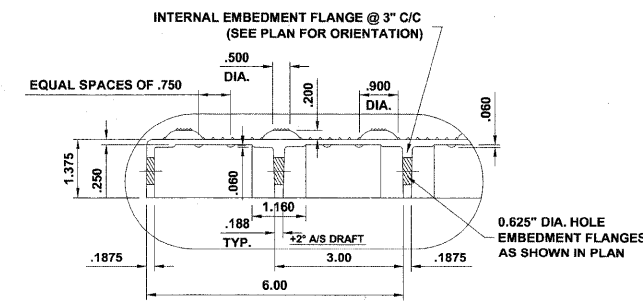
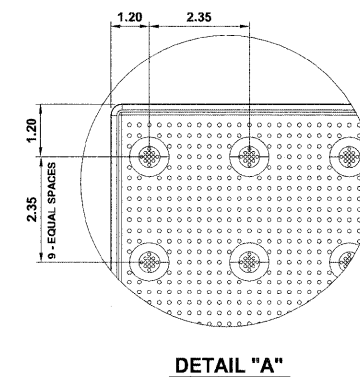
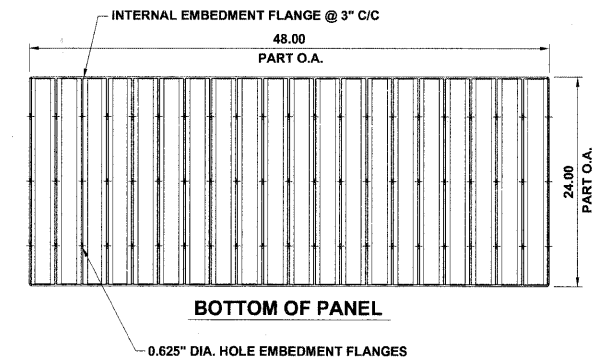
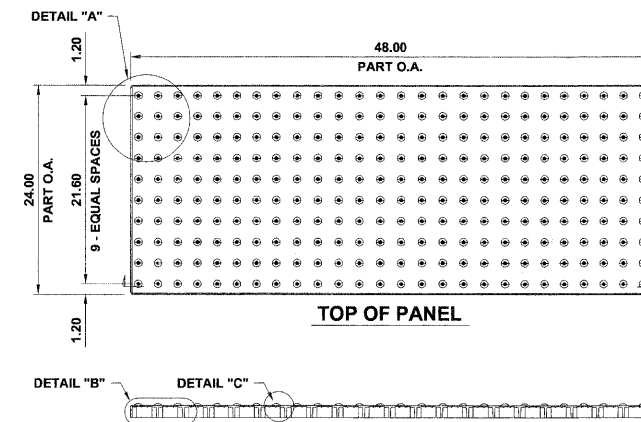


DIMENSIONS		
D	C	T(MIN.)
4'-0"	2'-6"	4"
5'-0"	3'-9"	5"

SANITARY MANHOLE

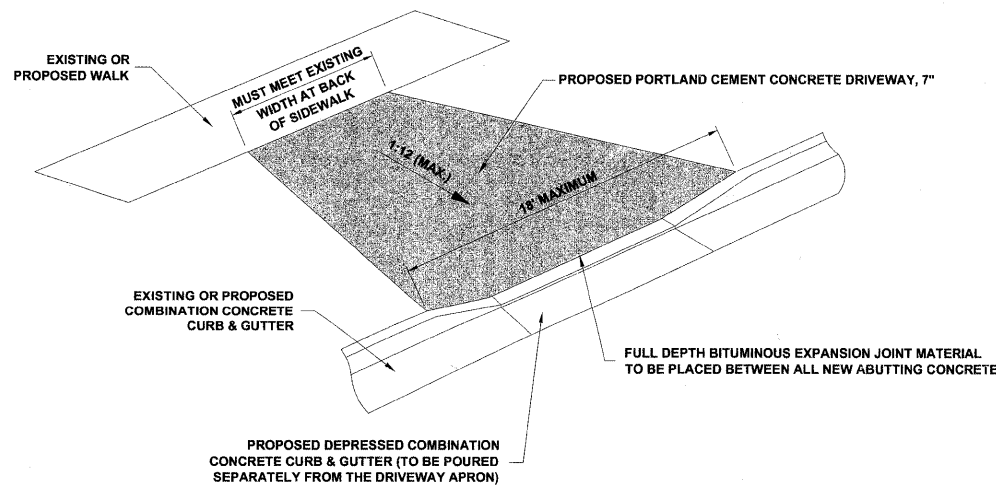


TYPICAL CURB AND GUTTER EXPANSION JOINT



NOTE: COMPOSITE PAVER TILE USED SHALL BE AS MANUFACTURED BY ADA SOLUTIONS, INC., OR AS APPROVED BY ENGINEER.

CAST-IN-PLACE COMPOSITE PAVER TILE FOR DETECTABLE WARNING



DRIVEWAY



◆ Civil Engineers
◆ Municipal Consultants
◆ Established 1911

9933 Roosevelt Road
Westchester, Illinois 60154-2789
Phone: 708.965.8300
Fax: 708.965.1312

DESIGNED - ---	REVISED - ---
DRAWN - ---	REVISED - ---
CHECKED - ---	REVISED - ---
DATE - 01/28/11	REVISED - ---

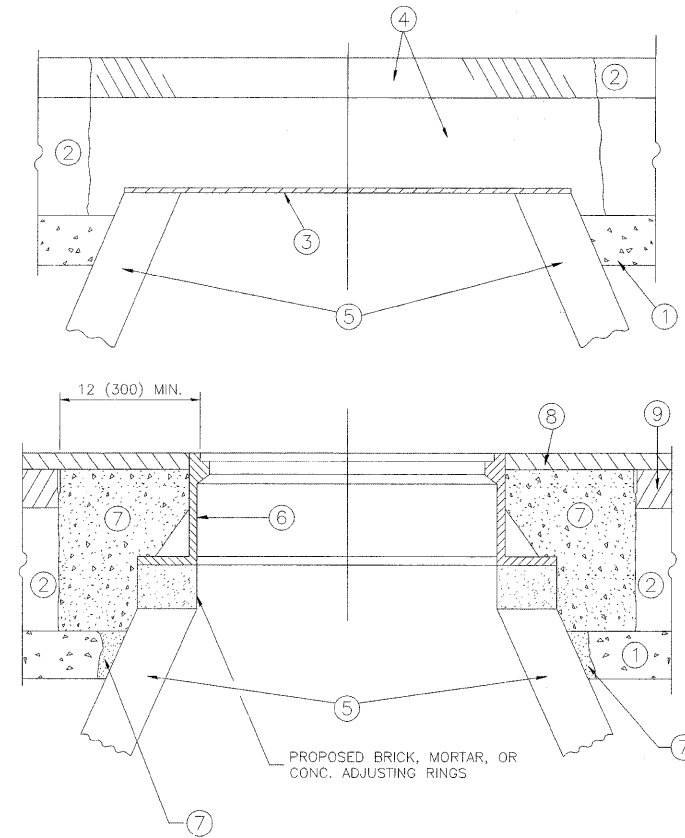
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2753	SECTION 10-00092-00-RS	COUNTY COOK	TOTAL SHEETS 12	SHEET NO. 7
FED. ROAD DIST. NO. 1 ILLINOIS		CONTRACT NO. 63577 FED. AID PROJECT ----		

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CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- *UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-1* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

USER NAME =	DESIGNED - R. SHAH	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - R. WIEDENIAN 05-14-04
PLOT SCALE =	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE =	DATE - 10-25-94	REVISED - R. BORO 03-09-11

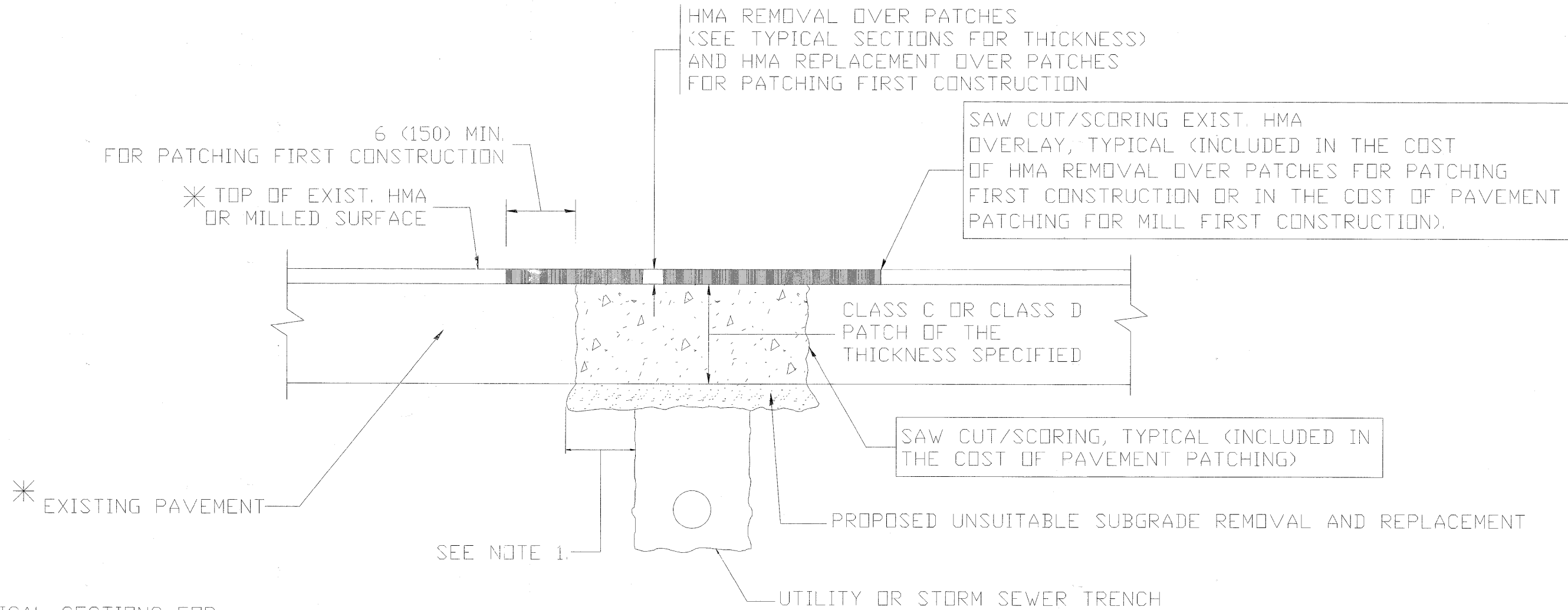
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2753	10-00092-00-RS	COOK	12	8
BD600-03 (BD-8)		CONTRACT NO. 63577		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 740-10-21501



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

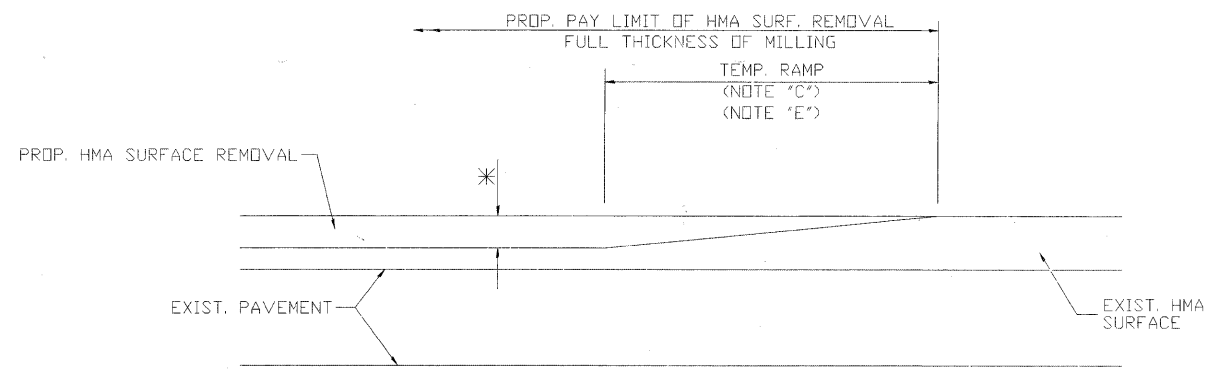
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

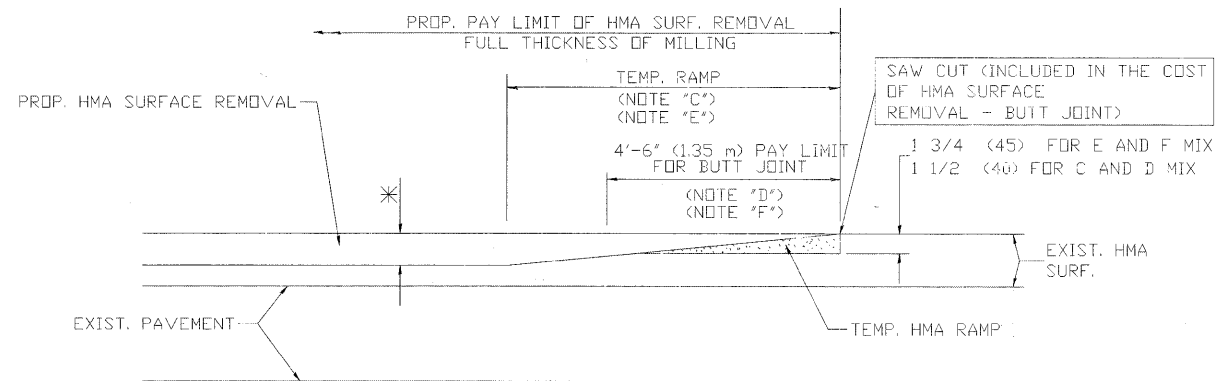
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USER NAME = DESIGNED - R. SHAH DRAWN - PLOT SCALE = PLOT DATE =			DESIGNED - R. SHAH DRAWN - CHECKED - DATE - 10-25-94			REVISED - A. ABBAS 04-27-98 REVISED - R. BORO 01-01-07 REVISED - R. BORO 03-04-07 REVISED - K. ENG 10-27-98			STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION			PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.U. RTE. 2753 SECTION 10-00092-00-RS COUNTY COOK TOTAL SHEETS 12 SHEET NO. 9	
SCALE: NONE						SHEET NO. 1 OF 1 SHEETS STA. TO STA.						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT CONTRACT NO. 63577 E.H.E. PROJECT NO. 740-10-21501				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

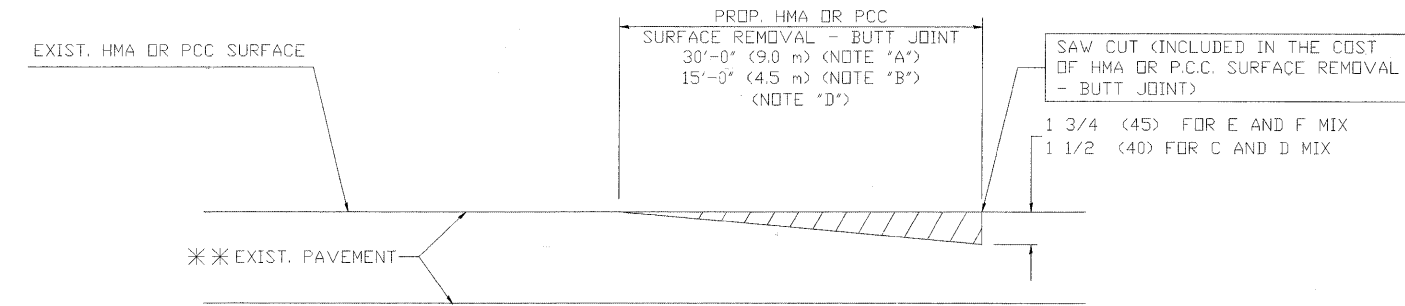
OPTION 1



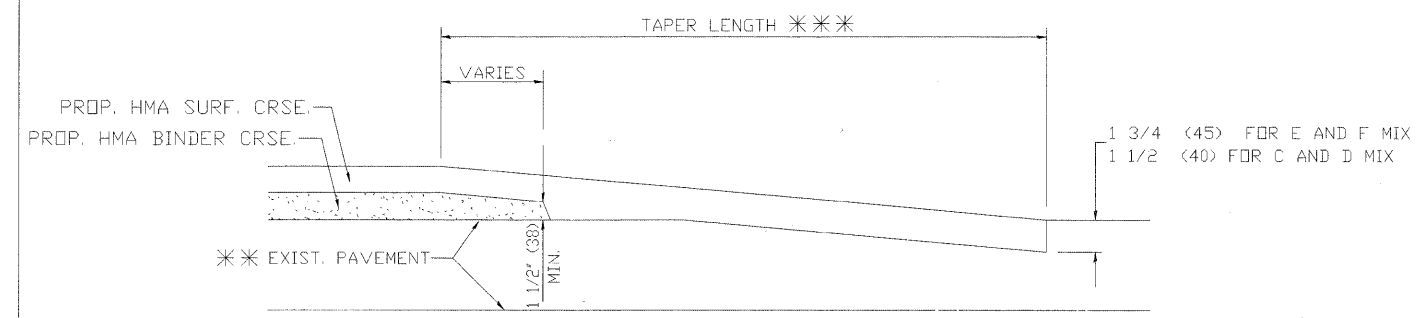
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

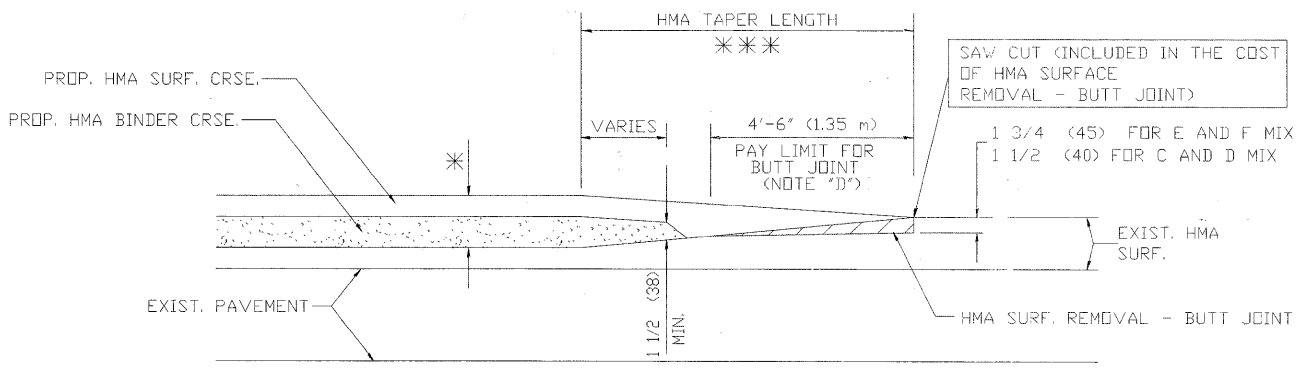
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR 'HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT'.
- *** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE 'A')
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE 'B')

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR 'HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT' OR FOR 'PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT'.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

Drawing file: W:\Projects\74010215 - 2010 L&EP Project\Workings-Hatcher\ave.dwg Mar 24, 2011 - 3:41 pm

USER NAME =	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
	DRAWN -	REVISED - A. ABBAS 05-21-97
PLOT SCALE =	CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE =	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

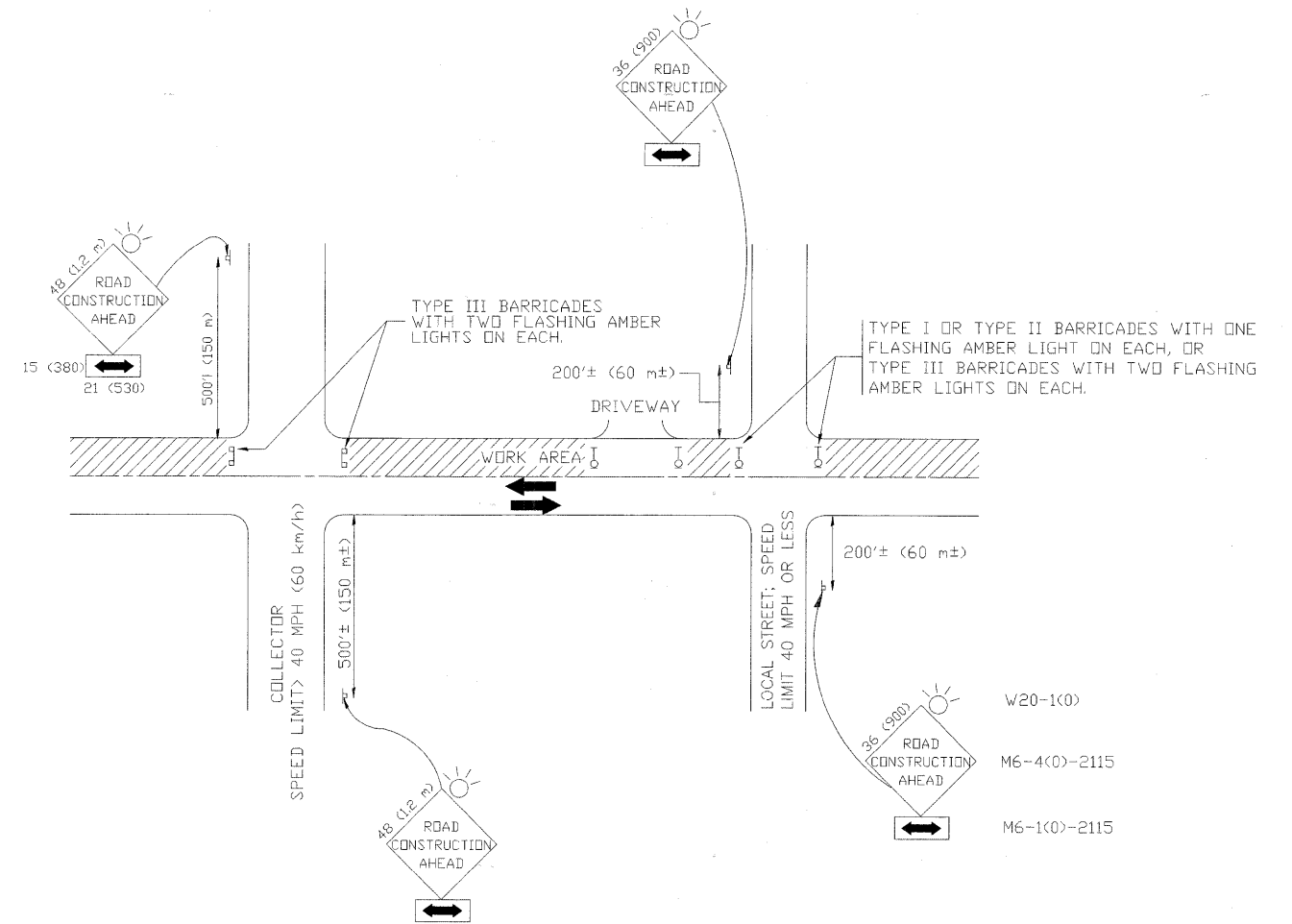
BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2753	10-00092-00-RS	COOK	12	10
BD400-05 (BD-32)		CONTRACT NO. 63577		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 740-10-21501

Drawing file: W:\Projects\74010215 - 2010 LAPR Project\Markings-HatcherAve.dwg Mar 24, 2011 - 3:41pm



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS:
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (12 m x 12 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

 - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
 - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

USER NAME =	DESIGNED - LHA	REVISED - J. OBERLE 10-16-95
PLOT SCALE =	DRAWN -	REVISED - A. HOUSEH 03-05-96
PLOT DATE =	CHECKED -	REVISED - A. HOUSEH 10-15-96
	DATE - 06-89	REVISED - T. RAJAMACHAR 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

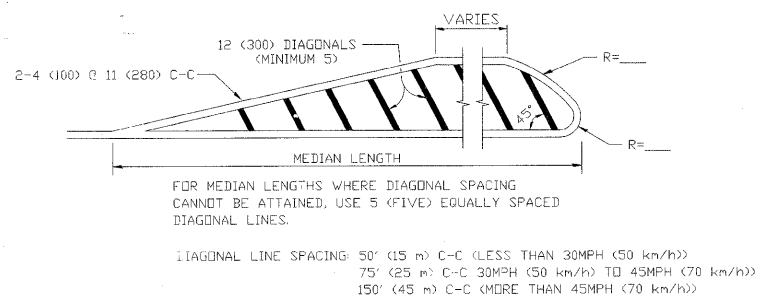
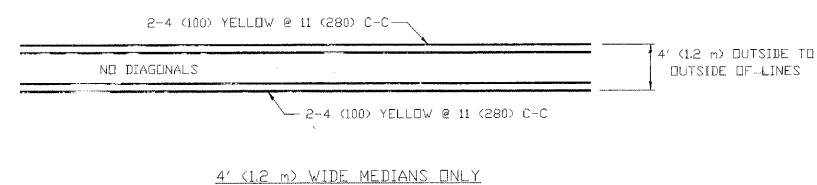
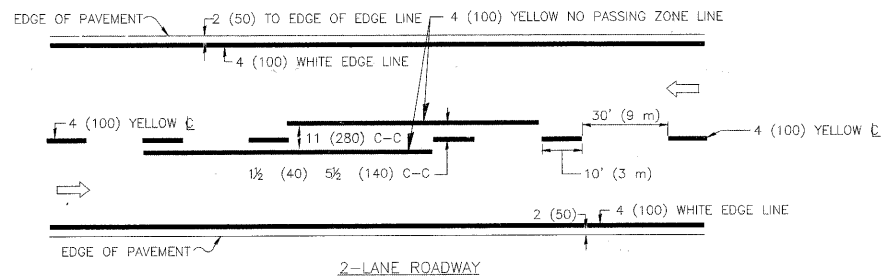
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

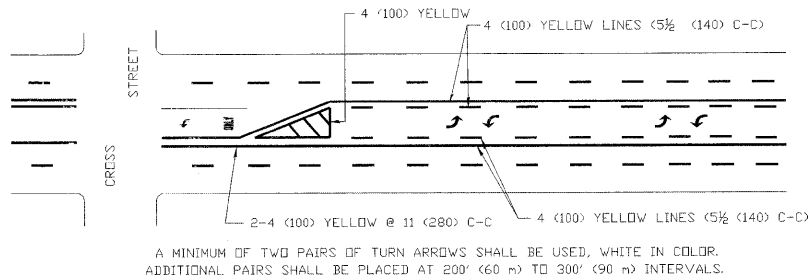
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2753	10-00092-00-RS	COOK	12	11
TC-10			CONTRACT NO. 63577	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 740-10-21501

Drawing file: W:\Projects\740\0215 - 2010 LAPP Project\Workings-ThatcherAve.dwg Mar 24, 2011 - 3:41 pm

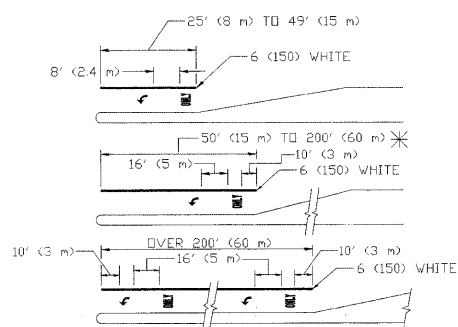


MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

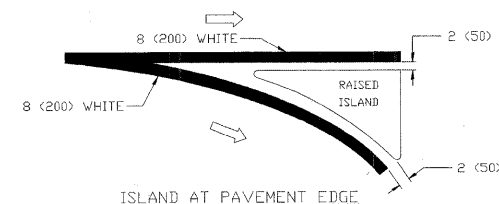
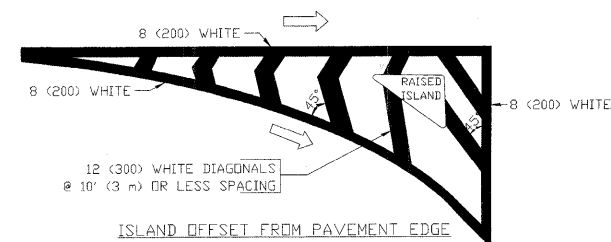


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

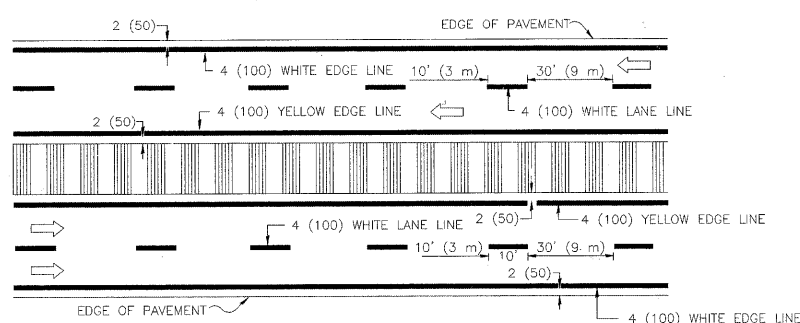
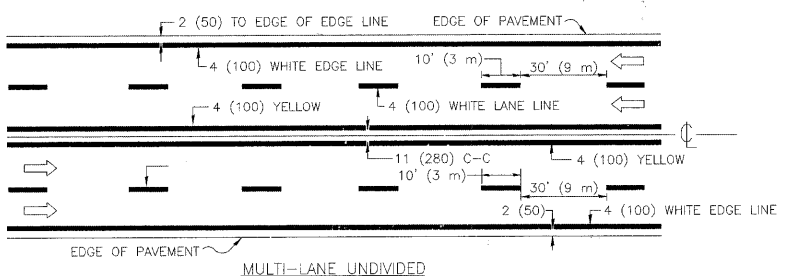


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW TWO WAY TRAFFIC WHITE ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

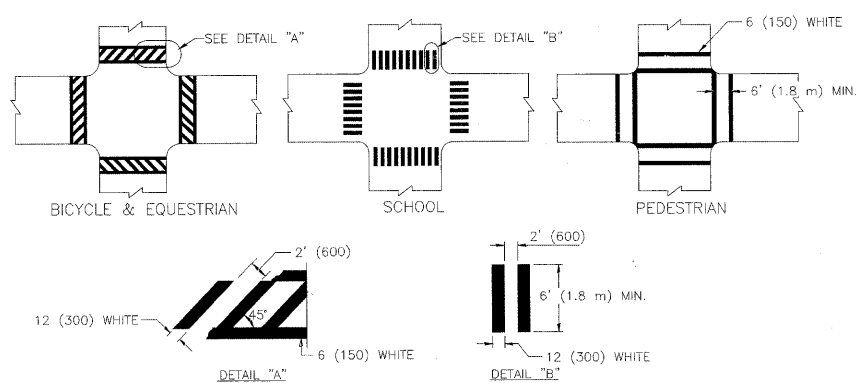
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

USER NAME = drvakosgn	DESIGNED = EVERS	REVISED = T.RAV/MACHER 10-27-94
PLOT SCALE = 50,000' / IN.	CHECKED =	REVISED = C. JUCIUS 09-09-09
PLOT DATE = 9/9/2009	DATE = 03-19-90	REVISED =

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2753	10-00092-00-RS	COOK	12	12
TC-13		CONTRACT NO. 63577		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
E.H.E. PROJECT NO. 740-10-21501				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.