

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2659	11-00177-00-RS	DUPAGE	21	1
CONTRACT NO. 63585				

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FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

TRAFFIC DATA

ADT:
WEST AVENUE 5,800 VPD (2009) 7,000 VPD (2030)

POSTED SPEED DESIGN SPEED

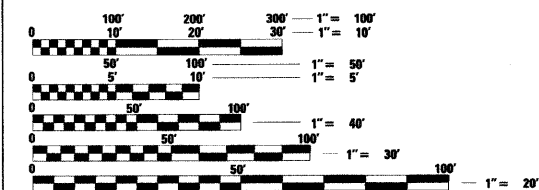
25 MPH (EXISTING) 35 MPH (EXISTING)
25 MPH (PROPOSED) 35 MPH (PROPOSED)

DESIGN DESIGNATION: 5,900 (2011) COLLECTOR 0.17 (RS)

PROJECT IS LOCATED IN THE CITY OF ELMHURST

DESCRIPTION OF IMPROVEMENT

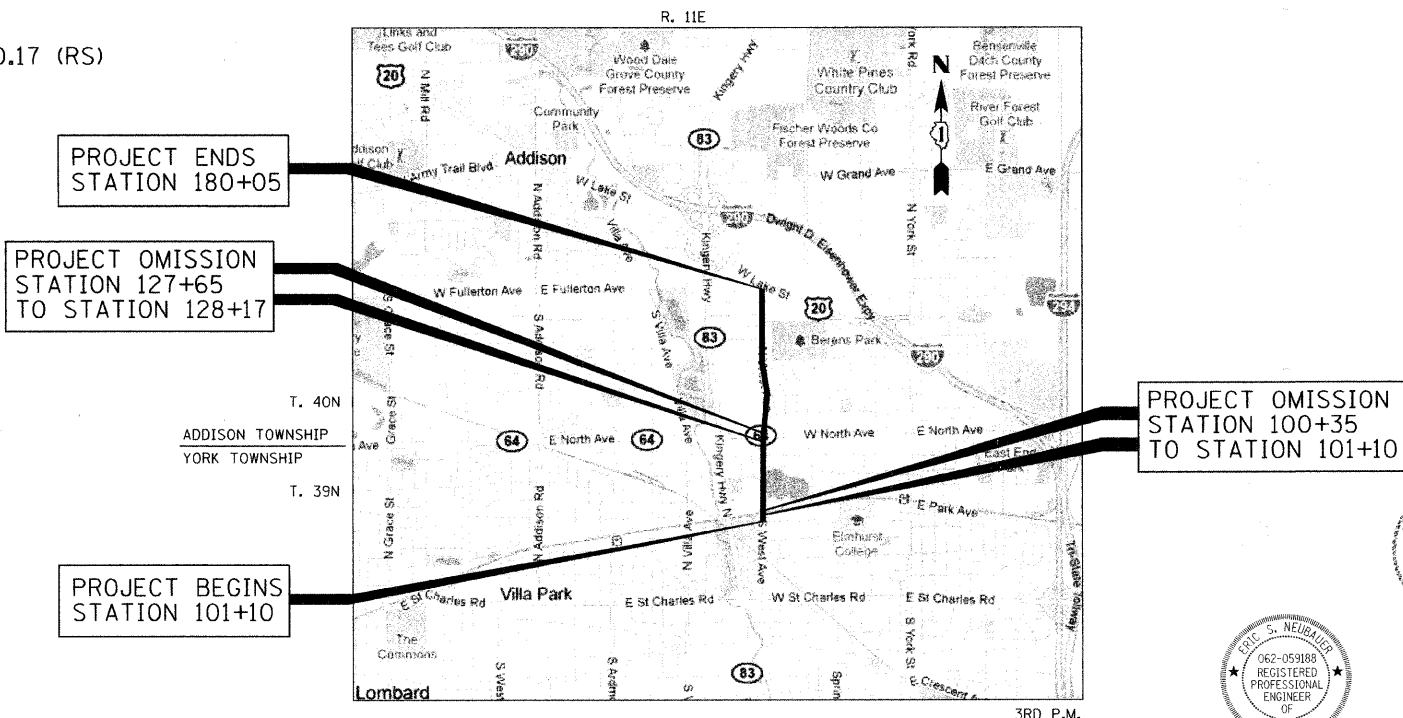
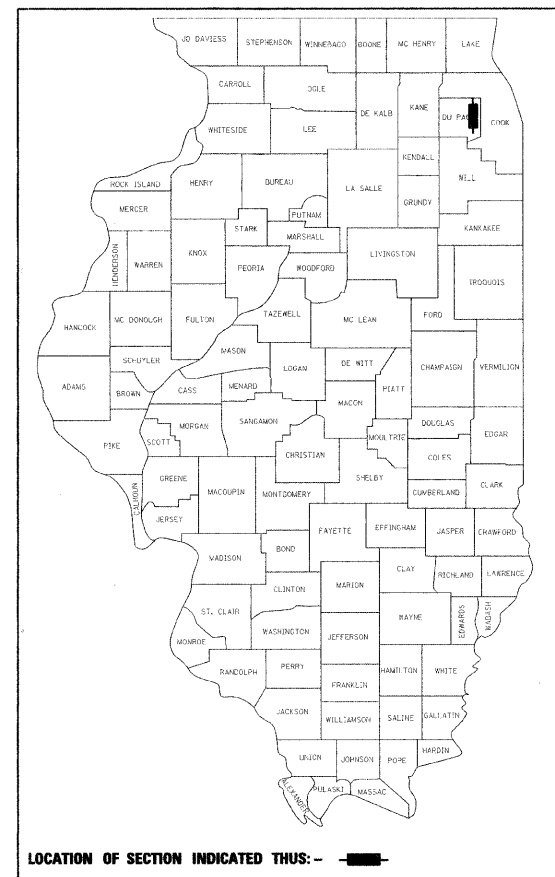
THIS IMPROVEMENT CONSISTS OF A VARIABLE DEPTH MILL AND OVERLAY, REMOVING AND REPLACING DETERIORATED PORTIONS OF CURB AND GUTTER AND SIDEWALKS, INSTALLATION OF NEW SIDEWALK, PAVEMENT PATCHING, STRUCTURE ADJUSTMENTS OR RECONSTRUCTION, STRIPING, AND ALL INCIDENTAL AND COLLATERAL WORK AS NECESSARY TO COMPLETE THE IMPROVEMENT SHOWN HEREIN AND AS DESCRIBED IN THE SPECIFICATIONS.



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY PROJECT**
FAU ROUTE 2659 (WEST AVENUE)
FAU ROUTE 1392 (FIRST STREET) TO FAU ROUTE 3537 (LAKE STREET)
RESURFACING
SECTION 11-00177-00-RS
PROJECT M-9003(794)
CITY OF ELMHURST
DUPAGE COUNTY
JOB NO. C-91-414-11



PROJECT ENDS STATION 180+05

PROJECT OMISSION STATION 127+65 TO STATION 128+17

PROJECT BEGINS STATION 101+10

PROJECT OMISSION STATION 100+35 TO STATION 101+10

LOCATION MAP

NOT TO SCALE
PROJECT LENGTH (GROSS / NET)
WEST AVENUE 7,895 FT / 7,843 FT (1.50 MILES / 1.49 MILES)



Eric S. Neubauber
ERIC S. NEUBAUER
REGISTERED PROFESSIONAL ENGINEER
NO. 062-059188
EXP. DATE 11/30/11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED ON _____, 2011
Michael J. Hughes
CITY OF ELMHURST, CITY ENGINEER - Director of P.A.

PASSED April 11, 2011
Eric S. Neubauber
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW APRIL 11, 2011
Debra M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

222 SOUTH RIVERSIDE PLAZA, SUITE 2320
CHICAGO, ILLINOIS 60605
(312) 669-9801
TRAN SYSTEMS
PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E., (847) 705-4406, SCHAMBERG, IL

CONTRACT NO. 63585

GENERAL NOTES

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2007.
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
- PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR MUST VERIFY THE ENGINEER'S LINE AND GRADE STAKES. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, HE MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANY WORK, OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1, KANDICE WITH UNION PACIFIC RAILROAD UTILITIES AT (312) 496-4738, THE FIBER OPTICS HOTLINE AT (800) 336-9193, AND THE CITY OF ELMHURST AT 630-530-3020 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOUR NOTIFICATION IS REQUIRED).
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR CITY PROPERTY OR RIGHT OF WAY WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
- SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM BEING REMOVED.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE ENGINEER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
- HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL VERIFY FINAL PATCH LOCATIONS IN THE FIELD, PRIOR TO REMOVAL.
- THE CONTRACTOR SHALL COORDINATE PAVING OPERATIONS FOR BOTH HMA LEVELING BINDER AND SURFACE COURSES SO THAT THE LONGITUDINAL JOINTS ARE CLOSED AND COMPACTED AT THE END OF EACH DAY. PAVING OPERATIONS SHALL BE SCHEDULED SO THAT ADJACENT LANES ARE PAVED IN THE SAME DIRECTION AS THE INITIAL LANE MINIMIZING THE TIME THE EDGE OF A PAVEMENT MAT IS ALLOWED TO COOL. PAVEMENT OPERATIONS SHALL ALSO INCLUDE A ONE-WAY DAILY DETOUR ROUTE AS NECESSARY TO MINIMIZE TRAFFIC IN THE CONSTRUCTION ZONE AND ACROSS THE PAVEMENT MAT EDGE. THE ONE-WAY DAILY DETOUR ROUTE SHALL BE COORDINATED WITH THE CITY OF ELMHURST AND APPROVED BY THE ENGINEER PRIOR TO INSTALLATION, AND THE COST TO INSTALL AND MAINTAIN THE DAILY ONE-WAY DETOUR ROUTE SHALL BE INCLUDED IN THE COST OF THE RESPECTIVE HMA ITEM.
- ALL PAVEMENT PATCHES WILL BE CLASS D UNLESS OTHERWISE NOTED.
- FOR ALL CLASS C PATCHES, SAWED TRANSVERSE CONTRACTION JOINTS, 3" DEEP, AT 15' SPACING WILL BE REQUIRED AND INCLUDED IN THE COST OF CLASS C PATCHES, 10'.

UTILITIES NOTES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGE DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER IN ACCORDANCE WITH ARTICLES 105.07 AND 107.20.
- ALL UTILITY OWNERS SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
- THE CONTRACTOR SHALL ENSURE THAT ALL WATER SYSTEM VALVES, VALVE VAULTS, FIRE HYDRANTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE TO THE CITY FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.
- ALL LOOSE MATERIAL DEPOSITED IN THE FLOWLINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
- THE EXISTING FRAMES AND LIDS SHALL REMAIN AS PROPERTY OF THE CITY OF ELMHURST. ALL OLD FRAMES AND LIDS NOT BEING REUSED SHALL BE REMOVED FROM PARKWAYS BY THE CONTRACTOR, DELIVERED TO AND STOCKPILED AT THE CITY MUNICIPAL SERVICE FACILITY WITHIN SEVEN (7) DAYS OF THEIR REMOVAL. THE UTILITY DEPARTMENT YARD IS LOCATED AT THE NORTH END OF THE WASTE WATER TREATMENT PLANT FACILITY, 625 SOUTH ROUTE 83.
- CATCH BASINS, MANHOLES AND INLETS CONSTRUCTED IN A LOCATION WHERE AN EXISTING STRUCTURE WAS REMOVED SHALL INCLUDE UP TO FIVE FEET OF PIPE TO CONNECT EACH EXISTING PIPE. THE NECESSARY PIPE BEYOND FIVE FEET WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "STORM SEWER" AND OF THE TYPE AND SIZE REQUIRED.
- THE CONTRACTOR SHALL CONFIRM ALL EXISTING STORM SEWER PIPE SIZES AND INVERTS PRIOR TO ORDERING STRUCTURES. ANY MODIFICATION OF STRUCTURES DUE TO THE FAILURE OF THE CONTRACTOR TO PERFORM THIS TASK SHALL BE AT THE CONTRACTOR'S EXPENSE AND MAY LEAD TO THE REJECTION OF THE STRUCTURE IN THE FIELD.

SIGNING AND STRIPING

- SEE IDOT STANDARD DETAIL 780001, DISTRICT ONE DETAIL TC-13 AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.
- THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL SUCH SIGNS THAT INTERFERE WITH CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING AND MUST BE RE-ERECTED AT A TEMPORARY LOCATION AND BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT IN ACCORDANCE WITH ARTICLE 107.25.

GENERAL NOTES (CONT'D)

TRAFFIC CONTROL

- SEE TRAFFIC CONTROL HIGHWAY STANDARDS CONCERNING TRAFFIC CONTROL AND PROTECTION.
- THE CONTRACTOR SHALL SCHEDULE CONSTRUCTION ACTIVITIES SO THAT THERE ARE ALWAYS TWO LANES OF TRAFFIC OPEN AT THE END OF EACH DAY. DETOURS WILL ONLY BE ALLOWED UPON SUBMITTAL OF A DETOUR PLAN AND APPROVED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE TRAFFIC CONTROL STANDARD PAY ITEM.

MISCELLANEOUS

- MATERIALS RESULTING FROM THE REMOVAL OF CONCRETE SURFACES, UTILITY STRUCTURE ADJUSTMENT, RESTORATION WORK, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IF THE CONTRACTOR DOES NOT REMOVE THESE MATERIALS AT THE REQUEST OF THE ENGINEER, THE CITY OF ELMHURST WILL HIRE A CONTRACTOR TO HAVE THE MATERIAL REMOVED AND THE CONTRACTOR SHALL BE BILLED (CHARGED) ACCORDINGLY.
- THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS, OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS/HER YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO USE OF THE WATER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. WITHIN 24 HOURS OF PLACING PRIME COAT AND THE LAYING OF HMA, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS AND ALL LOOSE MATERIAL.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE CITY WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO SIDEWALK REPLACEMENT AND/OR CURB AND GUTTER REPLACEMENT. AT LOCATIONS WHERE THE SIDEWALK OR CURB AND GUTTER IS SCHEDULED TO BE REMOVED, THE CONTRACTOR SHALL CONTACT THE BUSINESS/HOMEOWNER 24 HOURS PRIOR TO REMOVING THE CURB OR SIDEWALK. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES. THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE A DRIVEWAY FOR MORE THAN 48 HOURS UNDER ANY CIRCUMSTANCE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE BARRICADES TO PREVENT TRAFFIC FROM USING THE DRIVEWAYS DURING THIS PERIOD.
- IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO COORDINATE WITH THE UNION PACIFIC RAILROAD WHENEVER CONSTRUCTION ACTIVITY IS WITHIN 25 FEET OF THE RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY THE UNION PACIFIC RAILROAD TO MONITOR ON-COMING TRAIN TRAFFIC, AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD RIGHT OF WAY MAY PROCEED. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 105.05.
- WHEN REMOVING PAVEMENT, CURB AND GUTTER, SHOULDER, AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES AND BUILDING FOUNDATIONS WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.

NOTE: BOXED ITEMS ARE INCLUDED IN THE COST OF THE CONTRACT.

HIGHWAY STANDARDS

STD. NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
602301-03	INLET - TYPE A
602401-03	MANHOLE TYPE A
602701-02	MANHOLE STEPS
604001-03	FRAME AND LIDS TYPE 1
604056-03	FRAME AND GRATE TYPE 11V
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-07	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

DISTRICT ONE DETAILS

STD. NO.	DESCRIPTION
BD-01	DRIVEWAY DETAILS DISTANCE BETWEEN ROW AND FACE OF CURB AND EDGE OF SHOULDER >= 15'
BD-02	DRIVEWAY DETAILS DISTANCE BETWEEN ROW AND FACE OF CURB <= 15'
BD-08	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-16	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
TC-22	ARTERIAL ROAD INFORMATION SIGN
TC-26	DRIVEWAY ENTRANCE SIGNING
TS-07	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SUMMARY OF QUANTITIES				0005 ROADWAY 70% STP/30% LA	0005 ROADWAY 100% LA
* CODE NO	PAY ITEM	UNIT	QUANTITY		
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	14	14	
20101100	TREE TRUNK PROTECTION	EACH	9	9	
20101200	TREE ROOT PRUNING	EACH	9	9	
20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	7	7	
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	2	2	
20200100	EARTH EXCAVATION	CU YD	271	271	
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	220	220	
21101625	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	1,600	1600	
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	20	20	
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	20	20	
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	20	20	
25200110	SODDING, SALT TOLERANT	SQ YD	1,600	1600	
25200200	SUPPLEMENTAL WATERING	UNIT	6	6	
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	2,451	2,451	
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	5,277	5,277	
40600300	AGGREGATE (PRIME COAT)	TON	106	106	
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	5	5	
40600825	POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50	TON	1,497	1,497	
40600895	CONSTRUCTING TEST STRIP	EACH	1	1	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	256	256	
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	127	127	
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	2,995	2,995	
42001300	PROTECTIVE COAT	SQ YD	2,270	2,270	
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	117	117	
42300600	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 10 INCH	SQ YD	80	80	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	14,840	14,840	
42400800	DETECTABLE WARNINGS	SQ FT	637	637	
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	3,845	3,845	
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	197	197	
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	2,010	2,010	
44000600	SIDEWALK REMOVAL	SQ FT	5,980	5,980	
44201359	CLASS C PATCHES, TYPE IV, 10 INCH	SQ YD	69	69	
44201749	CLASS D PATCHES, TYPE I, 9 INCH	SQ YD	660	660	
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	660	660	
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	660	660	
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	660	660	
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1	
60236825	INLETS, TYPE A, TYPE IIV FRAME AND GRATE	EACH	4	4	
60266600	VALVE BOXES TO BE ADJUSTED	EACH	10	10	
60404805	FRAMES AND GRATES, TYPE IIV	EACH	3	3	
60406100	FRAMES AND LIDS, TYPE I, CLOSED LID	EACH	14	14	
60500040	REMOVING MANHOLES	EACH	1	1	
60500060	REMOVING INLETS	EACH	4	4	
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	2,010	2,010	
67100100	MOBILIZATION	L SUM	1	1	
* 70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	

SUMMARY OF QUANTITIES				0005 ROADWAY 70% STP/30% LA	0005 ROADWAY 100% LA
* CODE NO	PAY ITEM	UNIT	QUANTITY		
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	405	405	
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	218	218	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	6,020	6,020	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1,409	1,409	
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	434	434	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	350	350	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	191	191	
* 78001100	PAINT PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	73	73	
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	156	156	
* 78001130	PAINT PAVEMENT MARKING - LINE 6"	FOOT	209	209	
* 78001180	PAINT PAVEMENT MARKING - LINE 24"	FOOT	33	33	
* 78005100	EPOXY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	232	232	
* 78005110	EPOXY PAVEMENT MARKING - LINE 4"	FOOT	3,010	3,010	
* 78005130	EPOXY PAVEMENT MARKING - LINE 6"	FOOT	735	735	
* 78005150	EPOXY PAVEMENT MARKING - LINE 12"	FOOT	217	217	
* 78005180	EPOXY PAVEMENT MARKING - LINE 24"	FOOT	246	246	
* 78300100	PAVEMENT MARKING REMOVAL	SQ FT	296	296	
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	170	170	
K0026830	SHRUB REMOVAL	EACH	3	3	
X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SQ YD	17,856	17,856	
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	29	29	
* X7030025	WET REFLECTIVE TEMPORARY TAPE, TYPE III - LETTERS AND SYMBOLS	SQ FT	5	5	
* X7030030	WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH	FOOT	151	151	
* X7030040	WET REFLECTIVE TEMPORARY TAPE TYPE III, 6 INCH	FOOT	35	35	
* X7030050	WET REFLECTIVE TEMPORARY TAPE TYPE III, 12 INCH	FOOT	11	11	
* X7030055	WET REFLECTIVE TEMPORARY TAPE TYPE III, 24 INCH	FOOT	9	9	
* X7830068	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS	SQ FT	232		232
* X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	3,010		3,010
* X7830074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	735		735
* X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	217		217
* X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	246		246
XX000445	SAWCUT AND SEAL NEW JOINTS	FOOT	8,792		8,792
* XX006820	SIDEWALK APPROACH GUARDRAIL ADJUSTMENT	FOOT	1,015	1,015	
Z0004522	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6"	SQ YD	24	24	
Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	95	95	
Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	5	5	
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	251	251	
Z0042002	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	220	220	
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1	
Z0076600	TRAINEES	HOOR	500	500	

● CONSTRUCTION TYPE CODE=0042

* SPECIALTY ITEM

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 DRAWN - AJP
 CHECKED - ESN
 DATE - 3/28/2011

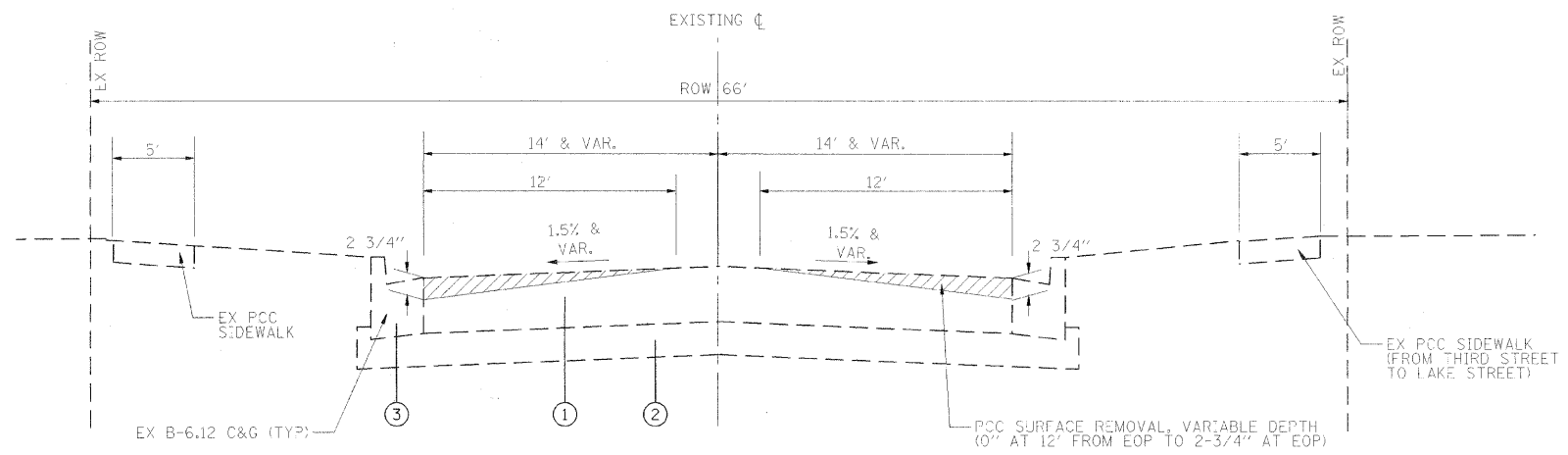
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

WEST AVENUE
 SUMMARY OF QUANTITIES

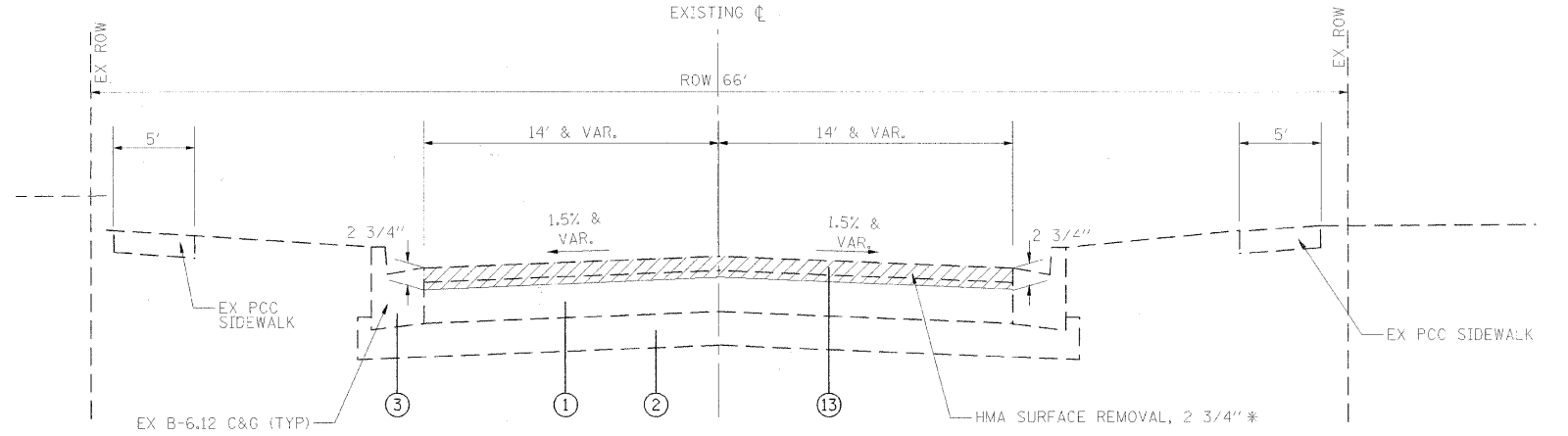
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F.A.U. R.T.C.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2659	11-00177-06-RS	DUPAGE	21	3
CONTRACT NO. 63585				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



EXISTING TYPICAL SECTION

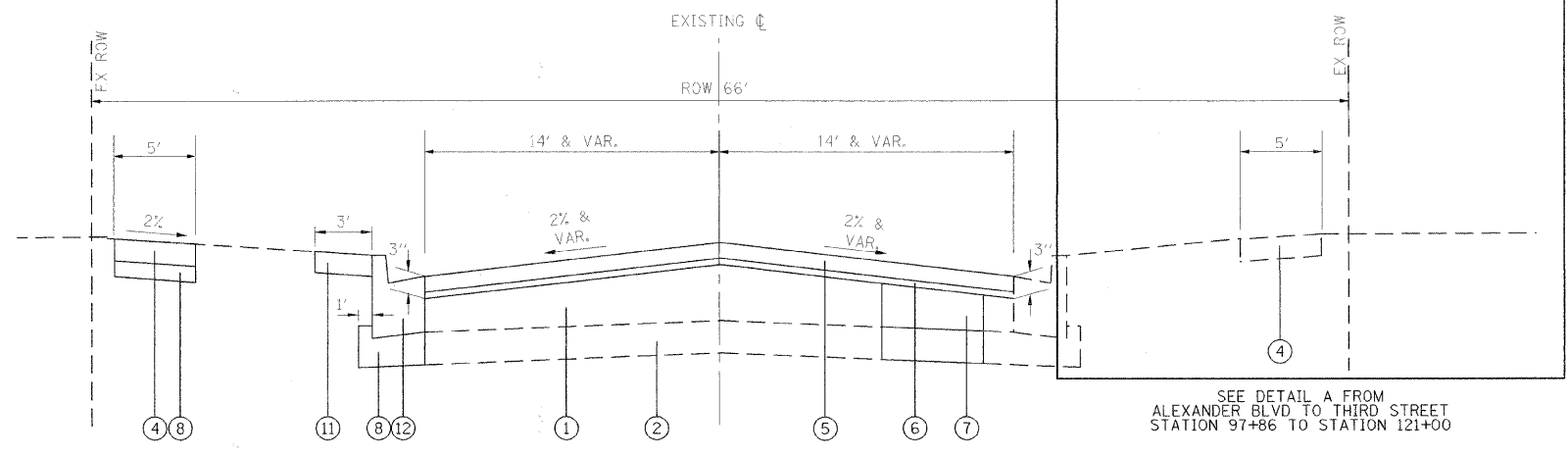
WEST AVENUE
FIRST AVENUE TO LAKE STREET
(STA. 101+10 TO STA. 121+47 AND STA. 132+48 TO STA. 180+05)



EXISTING TYPICAL SECTION

WEST AVENUE
THIRD STREET TO 431' NORTH OF NORTH AVENUE
(STA. 121+47 TO STA. 127+65 AND STA. 128+17 TO STA. 132+48)

* THIS HMA SURFACE REMOVAL SHALL INCLUDE 2 3/4" REMOVAL OF THE HMA SURFACE OR COMPLETE REMOVAL OF THE HMA AND REMOVAL OF THE PCC PAVEMENT SURFACE BELOW TO THE DEPTHS AS SHOWN IN THE TYPICAL SECTION ABOVE (TYPICAL SECTION FOR STA. 101+10 TO STA. 121+47 AND STA. 132+48 TO STA. 180+05) IF THE HMA OVERLAY IS LESS THAN 2 3/4".



PROPOSED TYPICAL SECTION

WEST AVENUE
FIRST STREET TO LAKE STREET
(STA. 101+10 TO STA. 127+65 AND STA. 128+17 TO STA. 180+05)

NOTE: HMA SURFACE COURSE SHALL BE 1/4" ABOVE THE EDGE OF PAVEMENT.

LEGEND

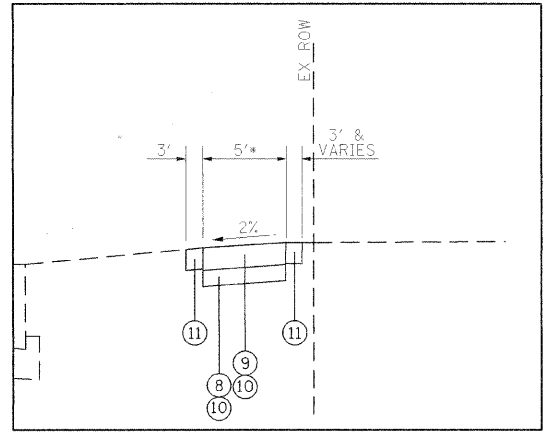
- ① EXISTING PCC PAVEMENT, 6"
- ② EXISTING SUBBASE GRANULAR MATERIAL, CA-6, 4"
- ③ COMBINATION CURB & GUTTER REMOVAL (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER) (SHALL INCLUDE REMOVAL AND DISPOSAL OF MATERIAL NECESSARY TO INSTALL PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B)
- ④ SIDEWALK REMOVAL AND PCC SIDEWALK, 5" (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- ⑤ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- ⑥ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑦ CLASS D PATCHES, 9" (AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER) CLASS C PATCHES, 10" (AS SHOWN ON THE PLANS)
- ⑧ SUB-BASE GRANULAR MATERIAL, TYPE B 4"
- ⑨ PROPOSED PCC SIDEWALK, 5" (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- ⑩ EARTH EXCAVATION (FOR PROPOSED SIDEWALK)
- ⑪ SODDING, SALT TOLERANT TOPSOIL FURNISH AND PLACE, 6"
- ⑫ COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.12 (AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER)
- ⑬ EXISTING HMA PAVEMENT, VARIABLE DEPTH

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

THE CONTRACTOR SHALL MILL BEFORE PATCHING.

MIXTURE TYPE	AIR VOIDS @ Ndes
HMA SURFACE COURSE, MIX "D", N70 (IL 9.5mm), 2"	4% @ 70 GYRATIONS
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"	4% @ 50 GYRATIONS
CLASS D PATCHES (HMA BINDER IL-19 mm), 9" (IN 3 LIFTS)	4% @ 70 GYRATIONS
HOT-MIX ASPHALT DRIVEWAYS, 6"	
HMA SURFACE COURSE, MIX "C", N50 (IL-9.5mm) 2"	4% @ 50 GYRATIONS
HMA BASE COURSE (HMA BINDER IL-19 mm), 4" (IN 2 LIFTS)	4% @ 50 GYRATIONS

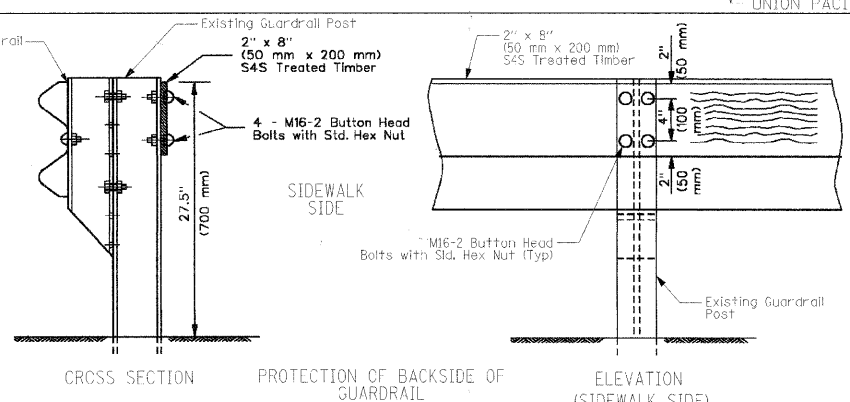
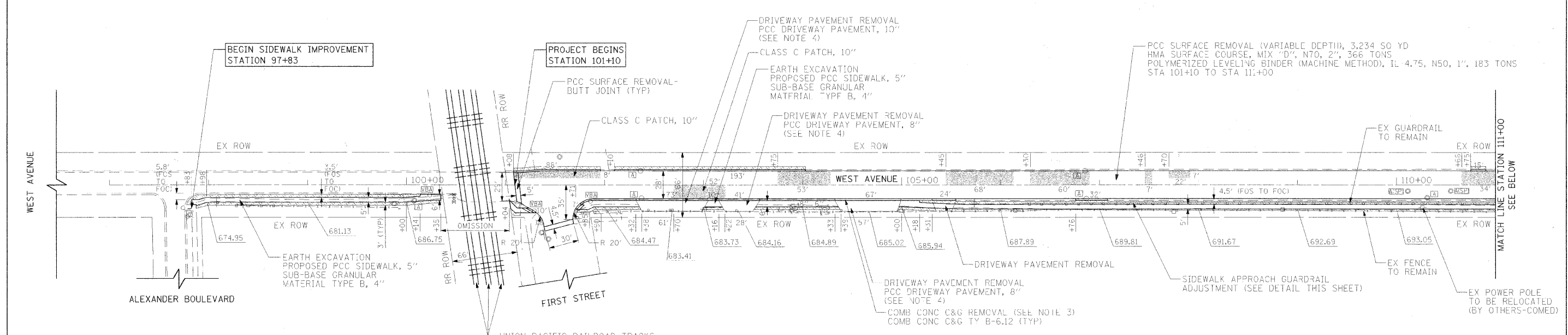
- NOTES:
- 1) THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
 - 2) THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.



DETAIL A

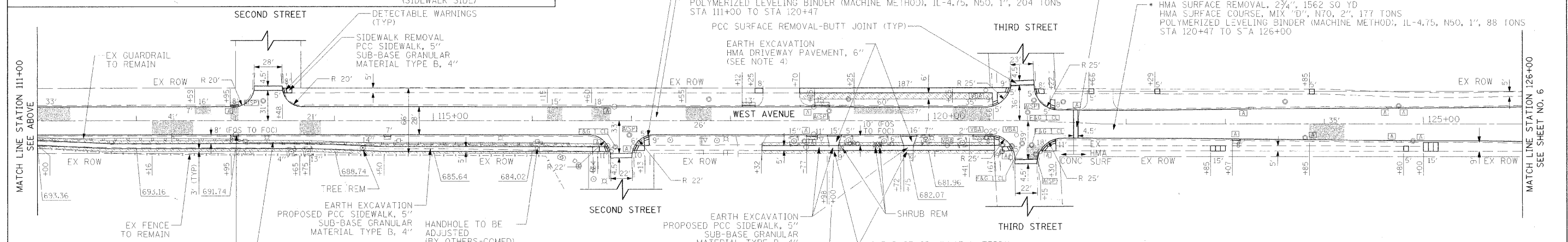
PROPOSED PCC SIDEWALK (EAST SIDE)
ALEXANDER BLVD TO THIRD STREET
STATION 97+86 TO STATION 121+00

* SIDEWALK WILL BE 6' WIDE AND LOCATED AT THE BACK OF CURB BETWEEN STA. 101+72 AND STA. 104+38



NOTES:
 1. THE TIMBER BOARDS SHALL BE INSTALLED ON THE BACKSIDE OF THE EXISTING GUARDRAIL ALONG THE EAST SIDE OF WEST AVENUE FROM STATION 106+22 TO STATION 116+37.
 2. THIS ITEM SHALL BE PAID FOR PER FOOT AS SIDEWALK APPROACH GUARDRAIL ADJUSTMENT.

* HMA SURFACE REMOVAL, 2 3/4" FROM STA 121+47 TO STA 127+65 AND STA 128+17 TO STA 132+48. THIS HMA SURFACE REMOVAL SHALL INCLUDE 2 3/4" REMOVAL OF THE HMA SURFACE OR COMPLETE REMOVAL OF THE HMA AND REMOVAL OF THE PCC PAVEMENT SURFACE BELOW TO THE DEPTHS AS SHOWN IN THE TYPICAL SECTION IF THE HMA OVERLAY IS LESS THAN 2 3/4".



NOTES:
 1. CLASS D PATCHES, 9" AS SHOWN ON THE PLANS AND AS DIRECTED BY ENGINEER.
 2. MORE QUANTITY IS INCLUDED FOR CLASS D PATCHES, 9" THAN IS SHOWN ON THE PLANS. THE REMAINING QUANTITY SHALL BE USED AT THE DIRECTION OF THE ENGINEER.
 3. REMOVAL AND DISPOSAL OF THE MATERIAL UNDER THE PROPOSED CURB AND GUTTER IN ORDER TO INSTALL THE PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B 4" SHALL BE CONSIDERED INCLUDED IN THE COST OF CURB AND GUTTER REMOVAL.
 4. IF UNSUITABLE MATERIAL IS ENCOUNTERED UNDER THE EXISTING DRIVEWAYS, A 4" DEPTH OF THE MATERIAL SHALL BE REMOVED AND DISPOSED OF AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL. SUB-BASE GRANULAR MATERIAL SHALL BE INSTALLED IN ITS PLACE AND SHALL BE PAID FOR AS SUB-BASE GRANULAR MATERIAL, TYPE B 4".
 5. THE ADJUSTMENT OF THE DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE ADJUSTED. THE ADJUSTMENT AND REPAIR OF THE DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE RECONSTRUCTED.

LEGEND

	DRAINAGE STRUCTURE TO BE ADJUSTED		PAVEMENT PATCHING (CLASS D PATCHES, 9" UNLESS OTHERWISE NOTED)
	DRAINAGE STRUCTURE TO BE RECONSTRUCTED		TREE PROTECTION
	FRAMES AND LIDS TO BE ADJUSTED, SPECIAL		TREE REMOVAL
	NEW FRAME & GRATE, TYPE III		SHRUB REMOVAL
	NEW FRAME & LID, TYPE I, CLOSED LID		SODDING, SALT TOLERANT TOPSOIL FURNISH AND PLACE, 6"
	FACE OF CURB		VALVE BOX TO BE ADJUSTED
	FACE OF SIDEWALK		
	VALVE BOX TO BE ADJUSTED		

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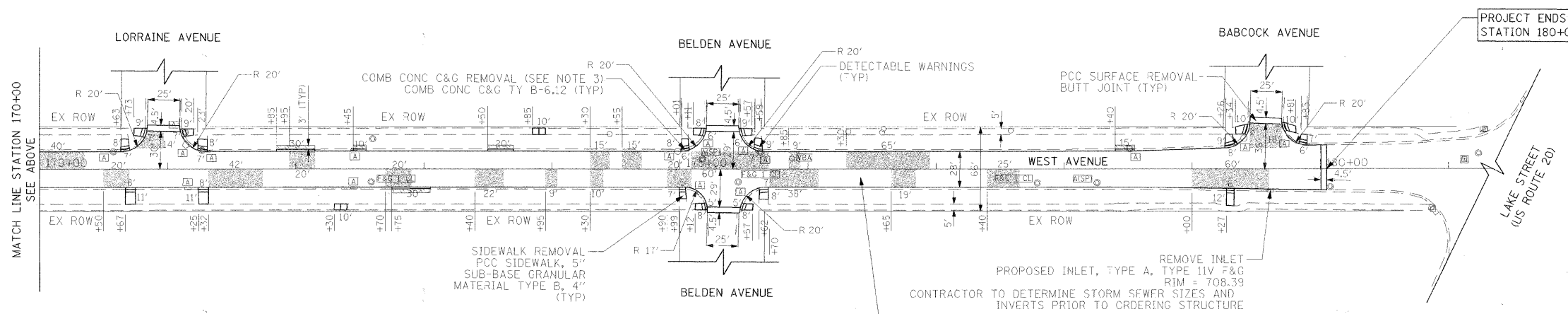
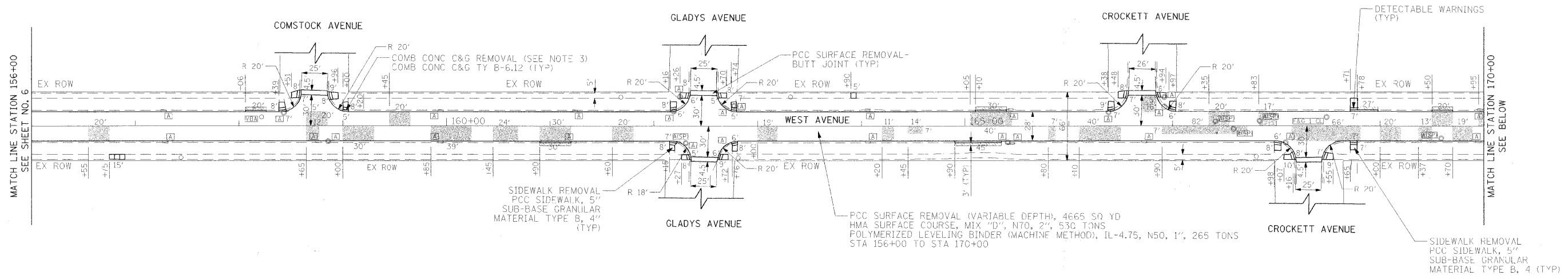
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DATE - 3/28/2011	REVISIONS -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**WEST AVENUE
IMPROVEMENT PLAN**

SCALE: 1"=50' SHEET NO. 1 OF 3 SHEETS STA. 97+86 TO STA. 126+00

F.A.U. R.T.E. = 2659	SECTION = 11-00177-00-R5	COUNTY = DUPAGE	TOTAL SHEETS = 21	SHEET NO. = 5
CONTRACT NO. 63585				
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



- NOTES:
1. CLASS D PATCHES, 9" AS SHOWN ON THE PLANS AND AS DIRECTED BY ENGINEER.
 2. MORE QUANTITY IS INCLUDED FOR CLASS D PATCHES, 9" THAN IS SHOWN ON THE PLANS, THE REMAINING QUANTITY SHALL BE USED AT THE DIRECTION OF THE ENGINEER.
 3. REMOVAL AND DISPOSAL OF THE MATERIAL UNDER THE PROPOSED CURB AND GUTTER IN ORDER TO INSTALL THE PROPOSED SUB-BASE GRANULAR MATERIAL, TY B 4" SHALL BE CONSIDERED INCLUDED IN THE COST OF COMB CURB AND GUTTER REMOVAL.
 4. IF UNSUITABLE MATERIAL IS ENCOUNTERED UNDER THE EXISTING DRIVEWAYS, A 4" DEPTH OF THE MATERIAL SHALL BE REMOVED AND DISPOSED OF AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL. SUB-BASE GRANULAR MATERIAL SHALL BE INSTALLED IN ITS PLACE AND SHALL BE PAID FOR AS SUB-BASE GRANULAR MATERIAL, TYPE B 4".
 5. THE ADJUSTMENT OF THE DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE ADJUSTED. THE ADJUSTMENT AND REPAIR OF THE DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE RECONSTRUCTED.

LEGEND

[Symbol]	DRAINAGE STRUCTURE TO BE ADJUSTED
[Symbol]	DRAINAGE STRUCTURE TO BE RECONSTRUCTED
[Symbol]	FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
[Symbol]	NEW FRAME & GRATE, TYPE IIV
[Symbol]	NEW FRAME & LID, TYPE I, CLOSED LID
[Symbol]	FACE OF CURB
[Symbol]	FACE OF SIDEWALK
[Symbol]	VALVE BOX TO BE ADJUSTED
[Symbol]	PAVEMENT PATCHING (CLASS D PATCHES, 9" UNLESS OTHERWISE NOTED)
[Symbol]	TREE PROTECTION
[Symbol]	TREE REMOVAL
[Symbol]	SHRUB REMOVAL
[Symbol]	SODDING, SALT TOLERANT TOPSOIL FURNISH AND PLACE, 6"

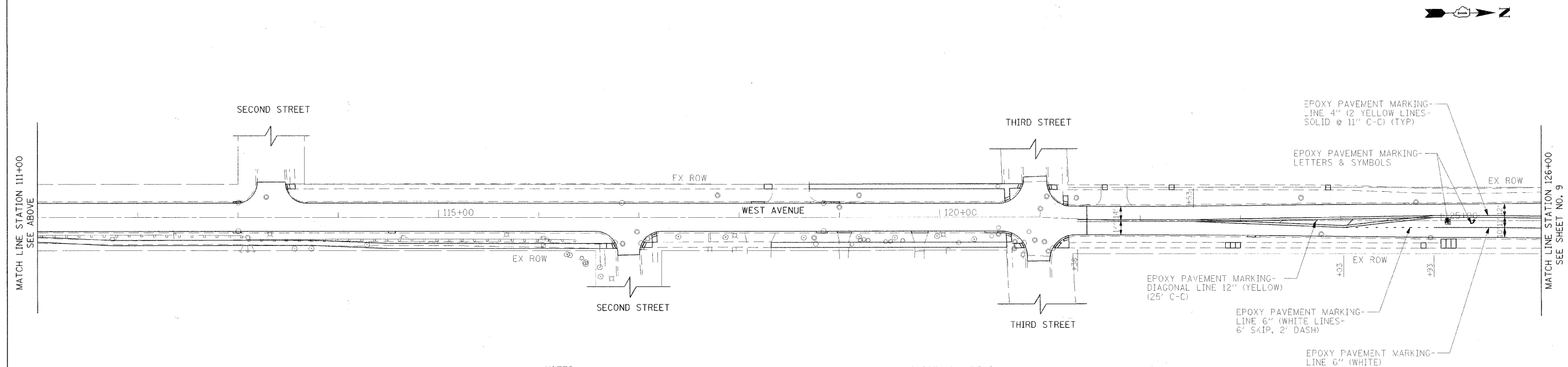
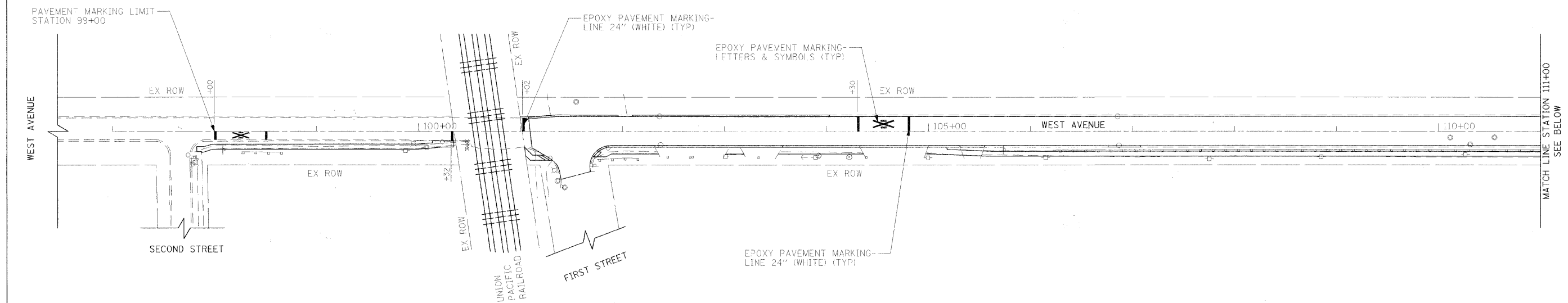
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REVISOR -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**WEST AVENUE
 IMPROVEMENT PLAN**

SCALE: 1"=50'	SHEET NO. 3 OF 3 SHEETS	STA. 155+00 TO STA. 181+55	F.A.U. RTE. 2659	SECTION 1-00:77-00-RS	COUNTY DUPAGE	TOTAL SHEETS 21	SHEET NO. 7
CONTRACT NO. 63585						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



PAVEMENT MARKING ITEM	CORRESPONDING RECESSED GROOVING ITEM
EPOXY PAVEMENT MARKING - LETTERS AND SYMBOLS	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS
EPOXY PAVEMENT MARKING - LINE 4"	GROOVING FOR RECESSED PAVEMENT MARKING 5"
EPOXY PAVEMENT MARKING - LINE 6"	GROOVING FOR RECESSED PAVEMENT MARKING 7"
EPOXY PAVEMENT MARKING - LINE 12"	GROOVING FOR RECESSED PAVEMENT MARKING 13"
EPOXY PAVEMENT MARKING - LINE 24"	GROOVING FOR RECESSED PAVEMENT MARKING 25"

NOTES: 1) EPOXY PAVEMENT MARKINGS SHALL BE INSTALLED IN GROOVED RECESSED CHANNELS CONSTRUCTED 0.040 INCHES BELOW THE SURFACE AND 1 INCH WIDER THAN THE PAVEMENT MARKING LINE. CONSTRUCTION OF THE RECESSED CHANNELS SHALL BE PAID FOR SEPARATELY PER FOOT AS GROOVING FOR RECESSED PAVEMENT MARKING OF THE WIDTH SPECIFIED AND PER SQUARE FOOT FOR GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS.

2) SEE TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKING FOR GUIDANCE

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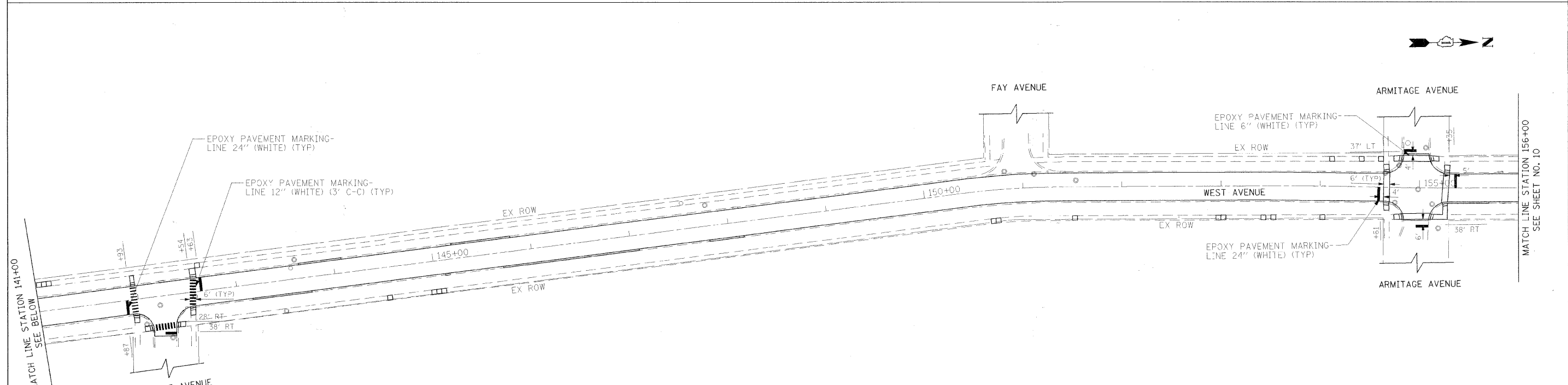
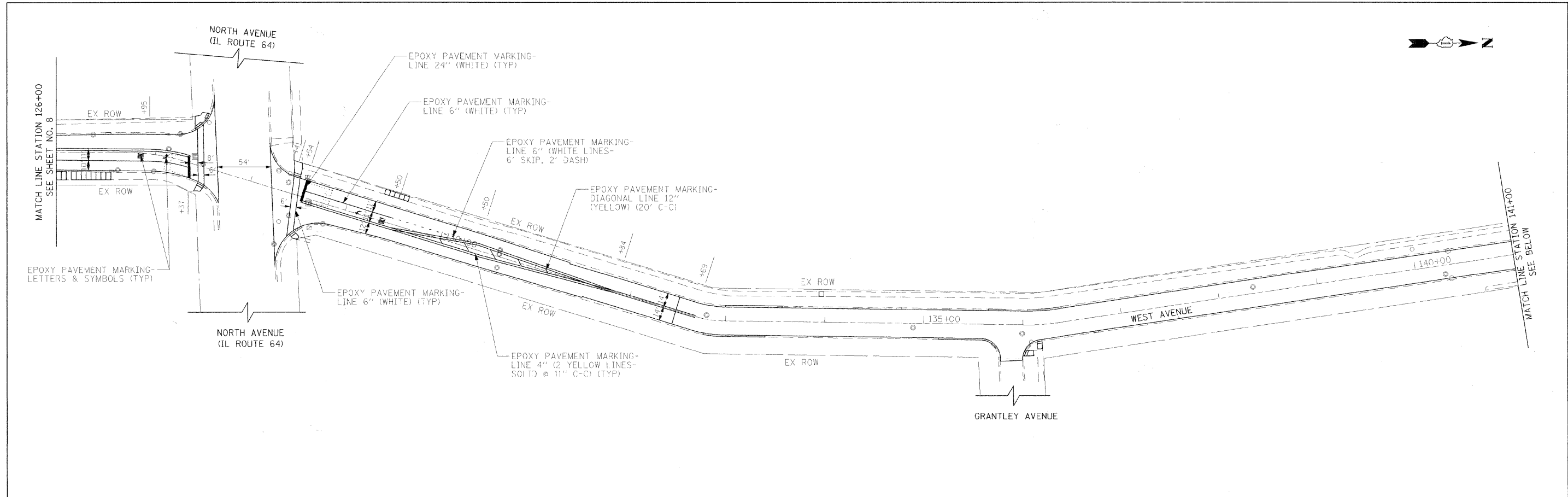
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**WEST AVENUE
PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET NO. 1 OF 3 SHEETS STA. 97+86 TO STA. 126+00

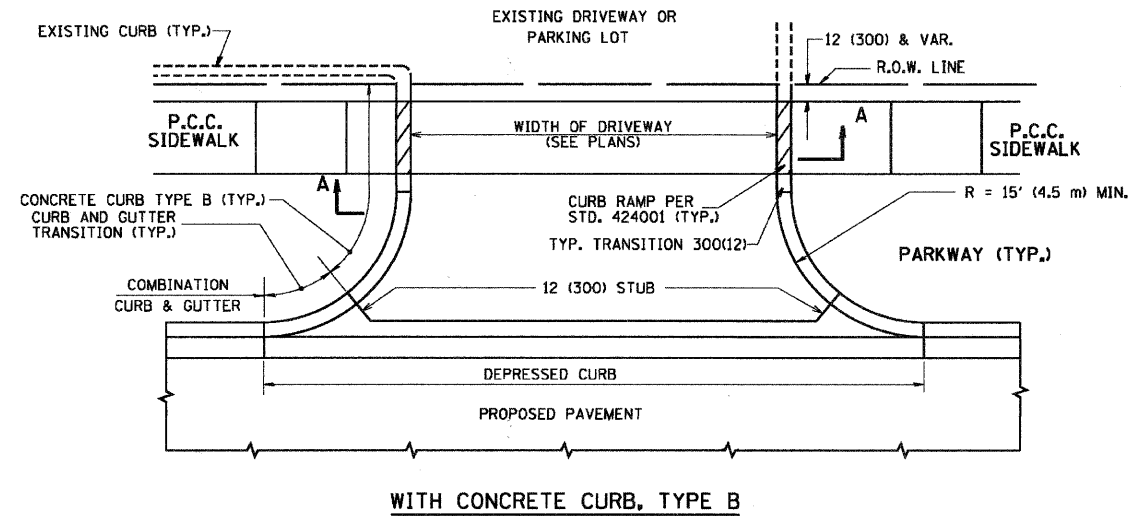
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CONTRACT NO. 63585				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



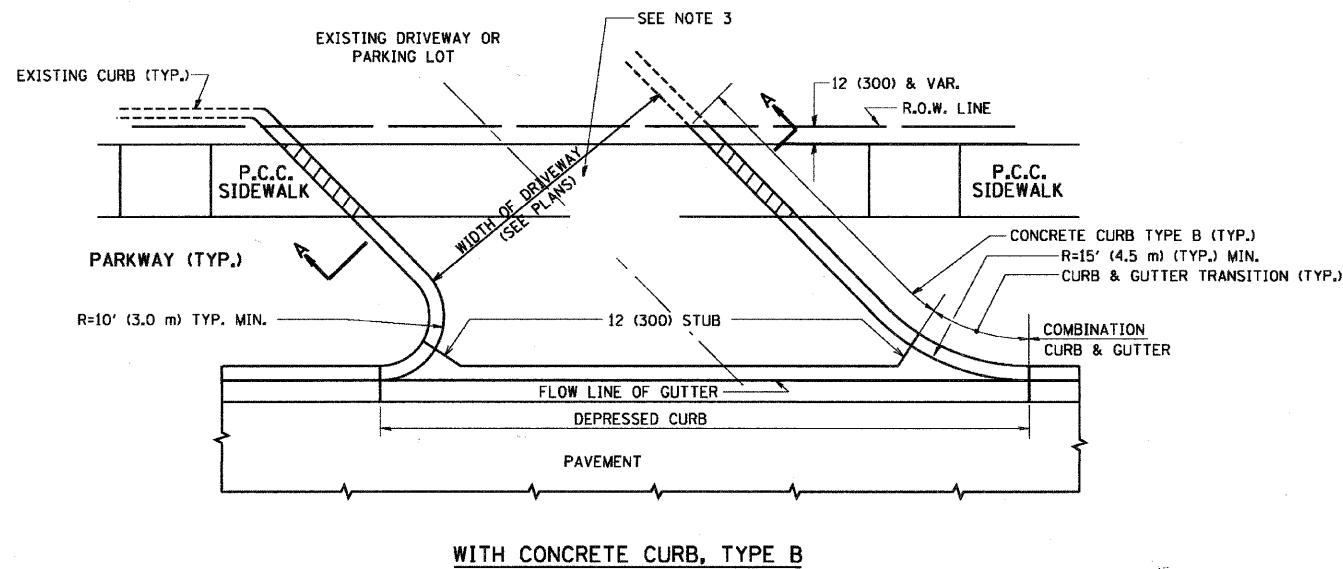
PAVEMENT MARKING ITEM	CORRESPONDING RECESSED GROOVING ITEM
EPOXY PAVEMENT MARKING - LETTERS AND SYMBOLS	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS
EPOXY PAVEMENT MARKING - LINE 4"	GROOVING FOR RECESSED PAVEMENT MARKING 5"
EPOXY PAVEMENT MARKING - LINE 6"	GROOVING FOR RECESSED PAVEMENT MARKING 7"
EPOXY PAVEMENT MARKING - LINE 12"	GROOVING FOR RECESSED PAVEMENT MARKING 13"
EPOXY PAVEMENT MARKING - LINE 24"	GROOVING FOR RECESSED PAVEMENT MARKING 25"

NOTES: 1) EPOXY PAVEMENT MARKINGS SHALL BE INSTALLED IN GROOVED RECESSED CHANNELS CONSTRUCTED 0.040 INCHES BELOW THE SURFACE AND 1 INCH WIDER THAN THE PAVEMENT MARKING LINE. CONSTRUCTION OF THE RECESSED CHANNELS SHALL BE PAID FOR SEPARATELY PER FOOT AS GROOVING FOR RECESSED PAVEMENT MARKING OF THE WIDTH SPECIFIED AND PER SQUARE FOOT FOR GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS.

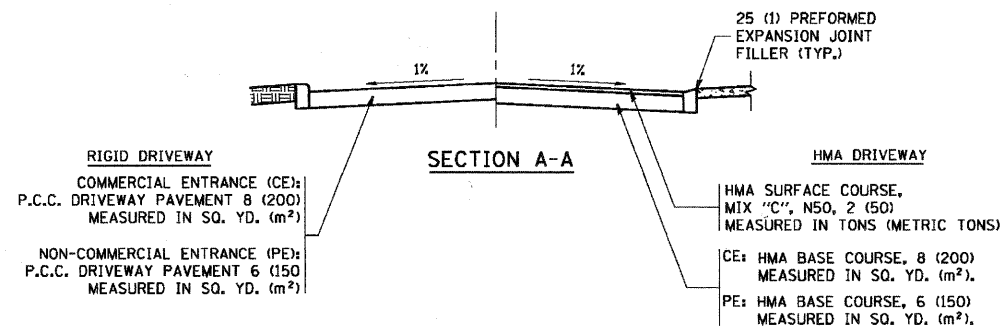
2) SEE TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKING FOR GUIDANCE



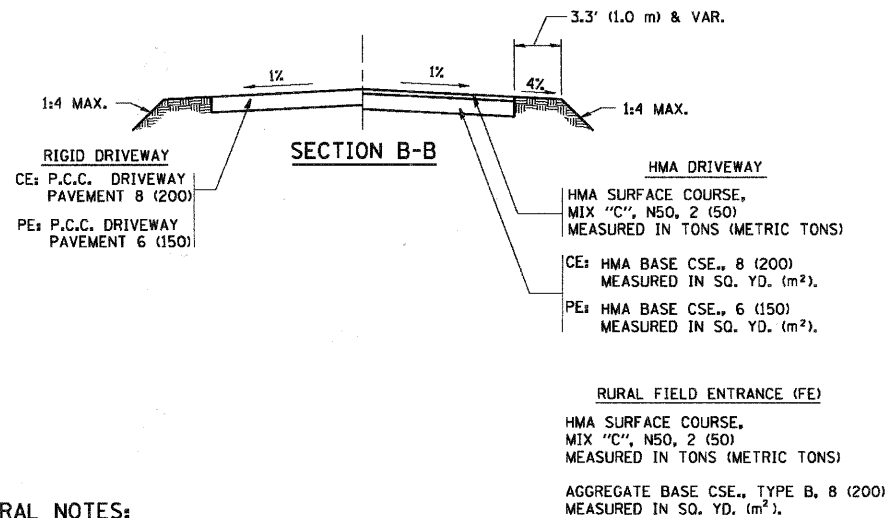
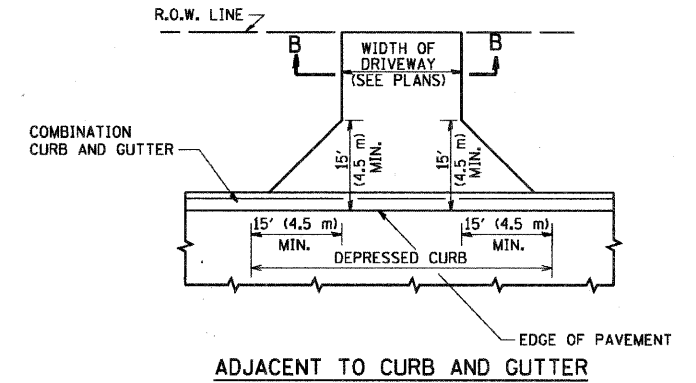
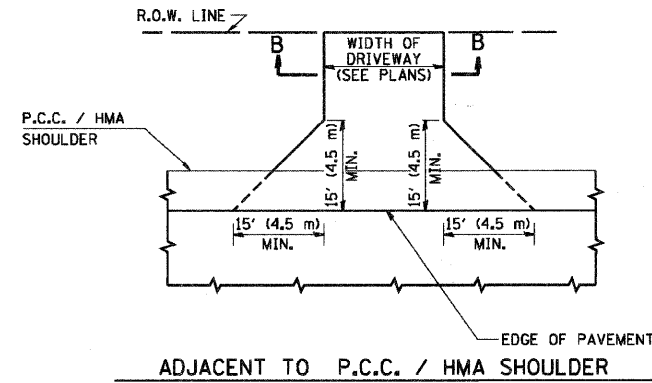
WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



SECTION A-A



GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

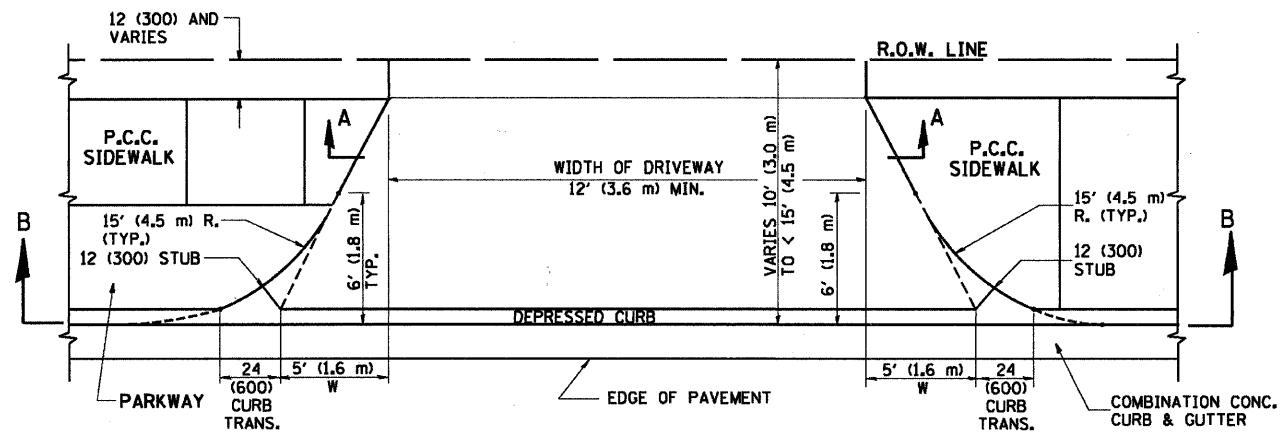
THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

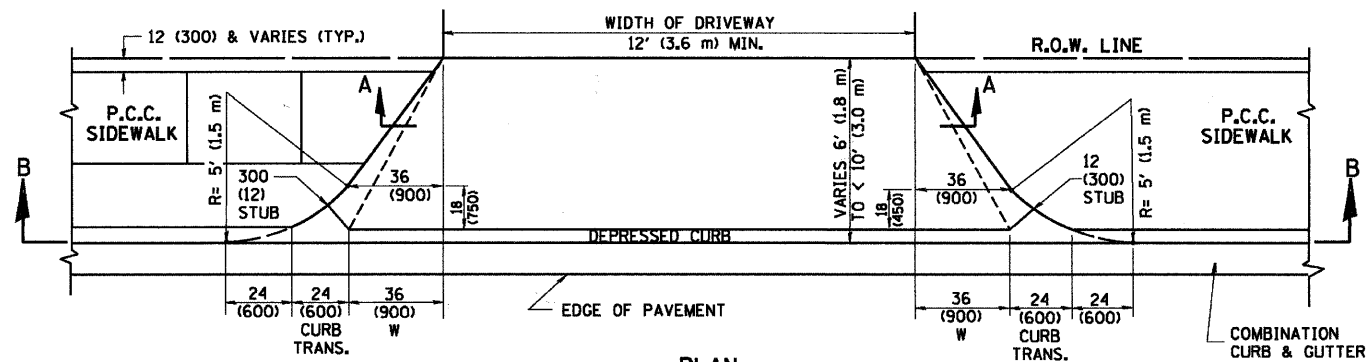
1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

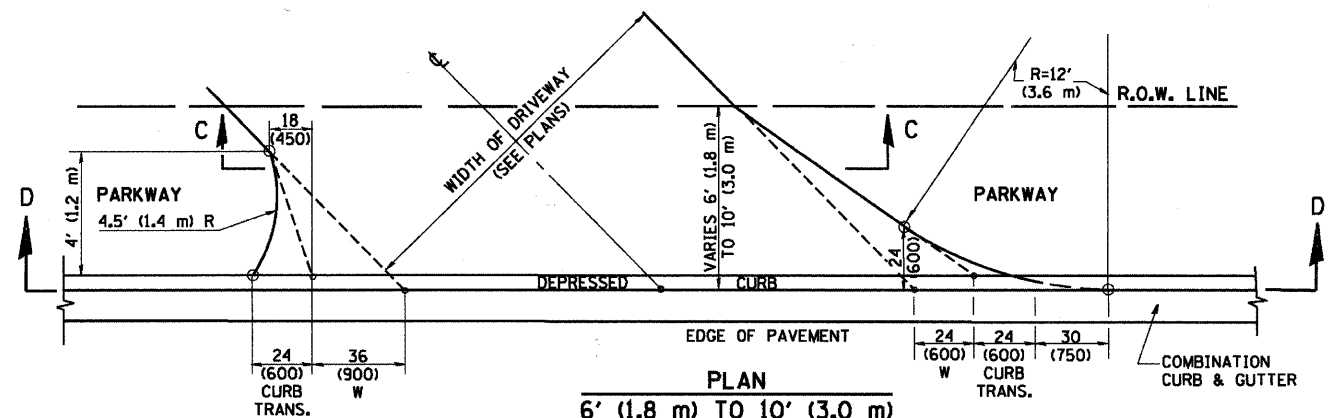
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PLOT SCALE = 49,9999' / IN.	CHECKED -	REVISOR - P. LGFLUER 04-15-03	2659			11-00:77-00-R5	DUPAGE	21	11	
PLOT DATE = 6/12/2008	DATE - 11-04-95	REVISOR - R. BORO 01-01-07	BD0156-07 (BD-01)			CONTRACT NO. 63585				
		REVISOR - R. BORO 06-11-08	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



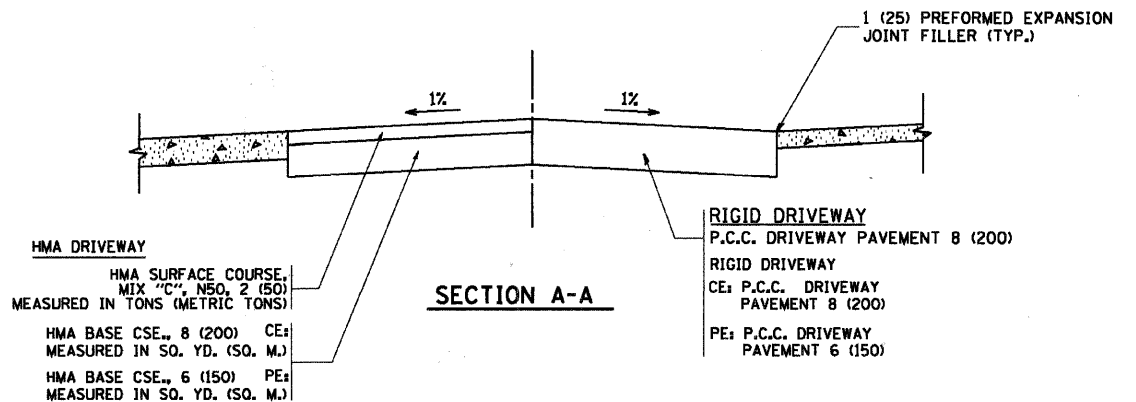
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10' (3.0 m) TO < 15' (4.5 m)



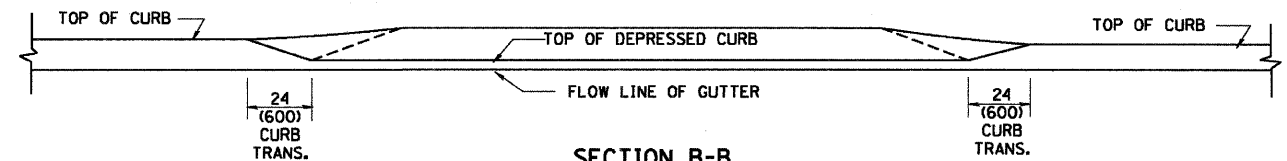
PLAN
6' (1.8 m) TO < 10' (3.0 m)



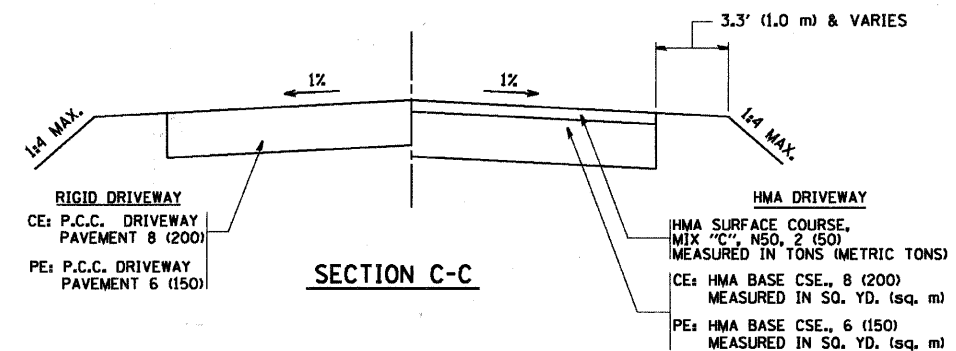
PLAN
6' (1.8 m) TO 10' (3.0 m)



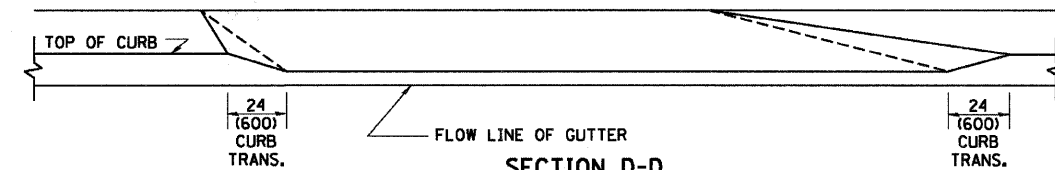
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

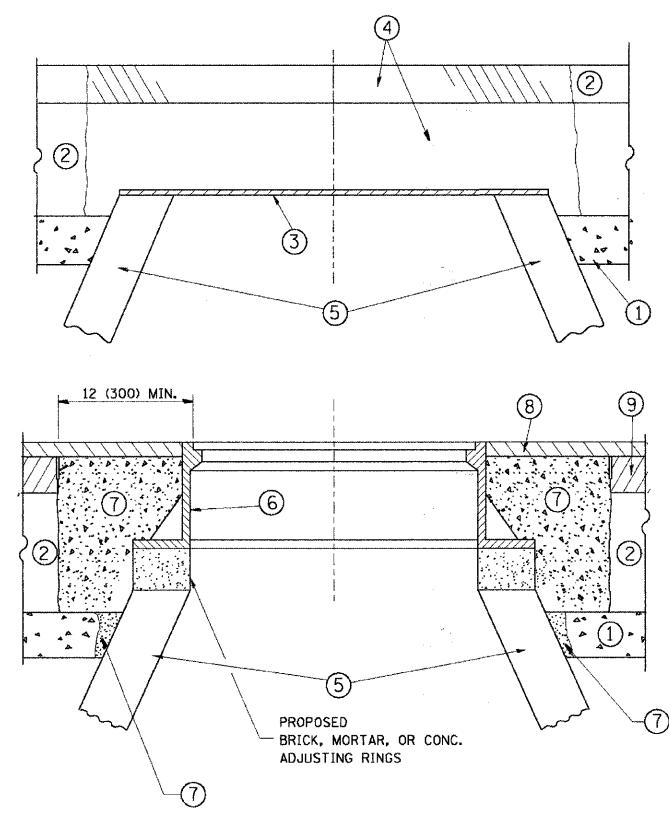
"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME = W:\dststd\22x34\bd82.dgn	USER NAME = gaglierobt	DESIGNED - R. SHAH	REVISED - T. HOLTZ 04-08-97
		DRAWN -	REVISED - M. GOMEZ 04-06-01
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - P. LAFLEUR 04-15-03
	PLOT DATE = 1/4/2008	DATE - 11-06-95	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)		2659	11-00177-00-RS	DUPAGE	21	12
SCALE: NONE		BD400-02 (BD-02)		CONTRACT NO. 63585		
SHEET NO. 1 OF 1 SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

~~CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.~~

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

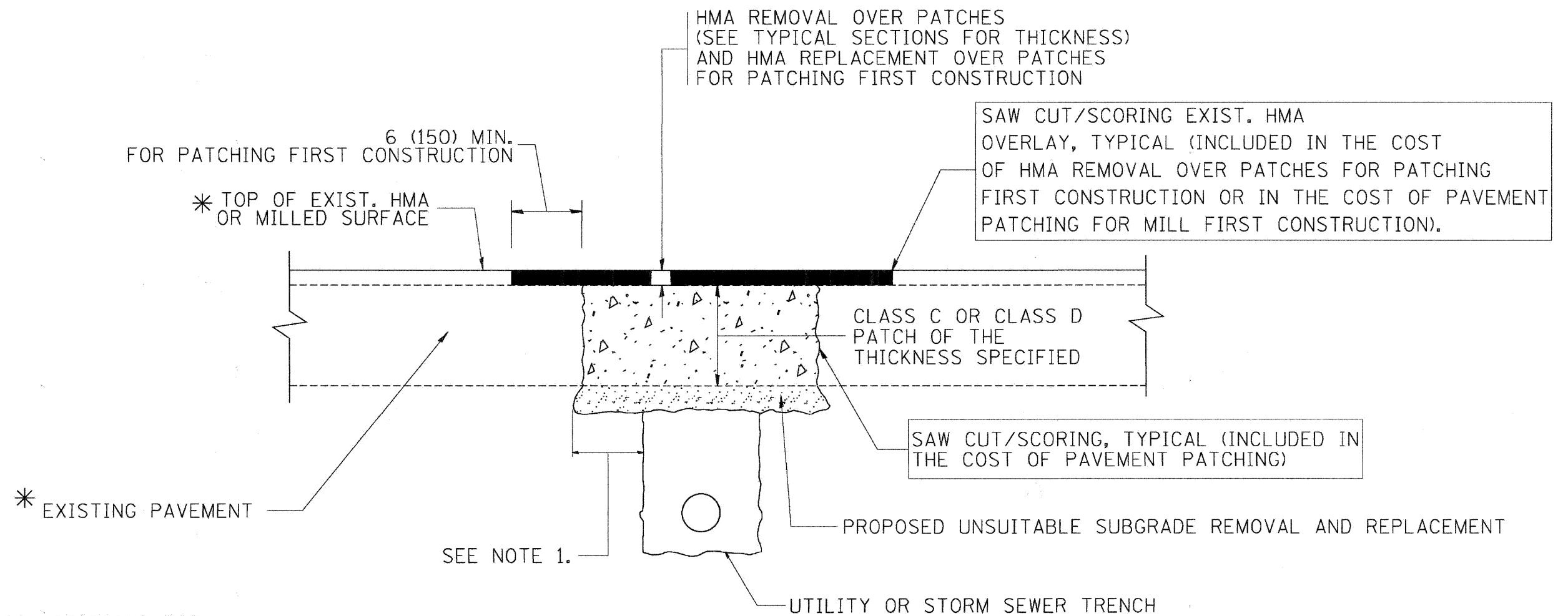
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	PLOT SCALE = 49.9999 1/4 IN.	CHECKED -	REVISED - R. BORO 01-01-07
	PLOT DATE = 3/18/2011	DATE - 10-25-94	REVISED - R. BORO 03-09-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2659	11-00177-00-RS	DUPAGE	21	13
BD600-03 (BD-8)			CONTRACT NO. 63585	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

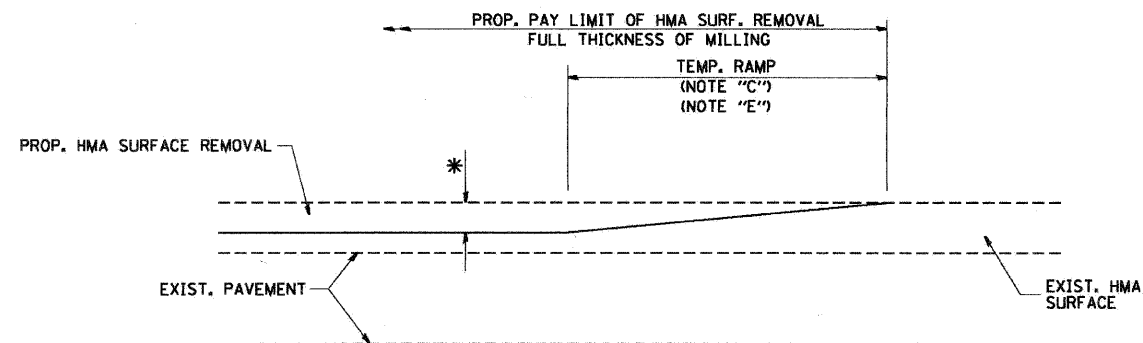
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		CHECKED -	REVISED - R. BORO 09-04-07
		DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

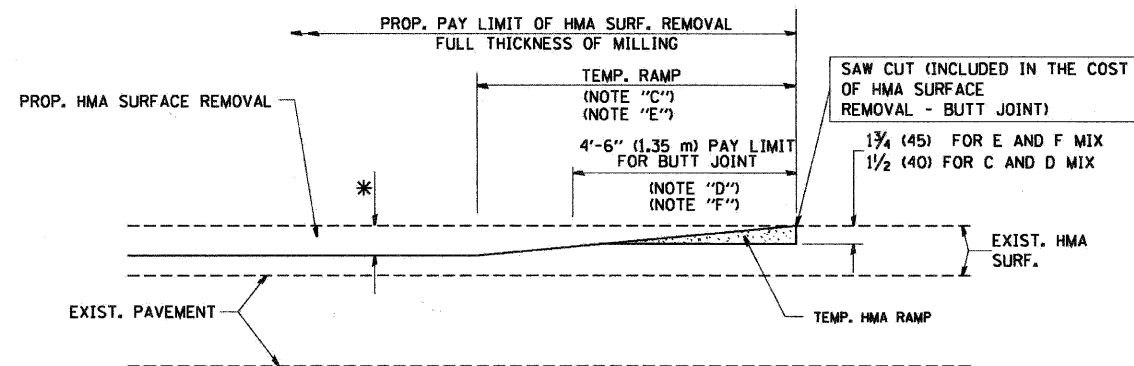
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F.A.U. RTE. 2659	SECTION 11-00177-00-RS	COUNTY DUPAGE	TOTAL SHEETS 21	SHEET NO. 14
BD400-04 (BD-22)			CONTRACT NO. 63585	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

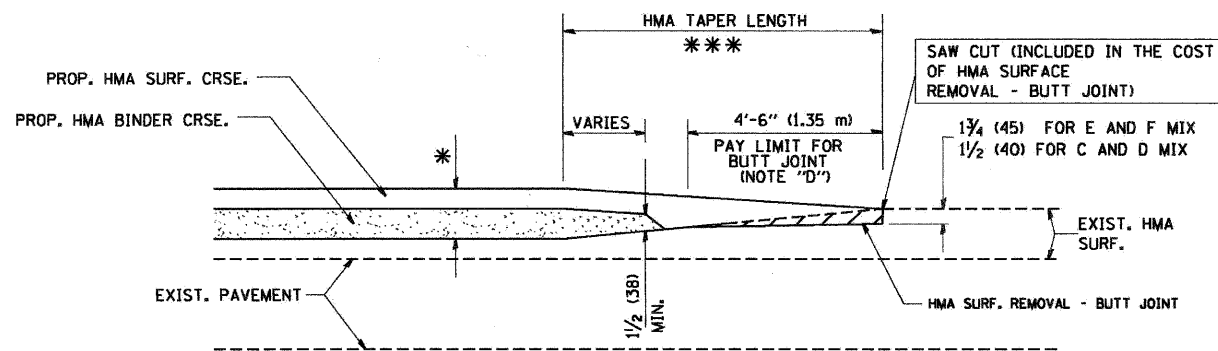
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

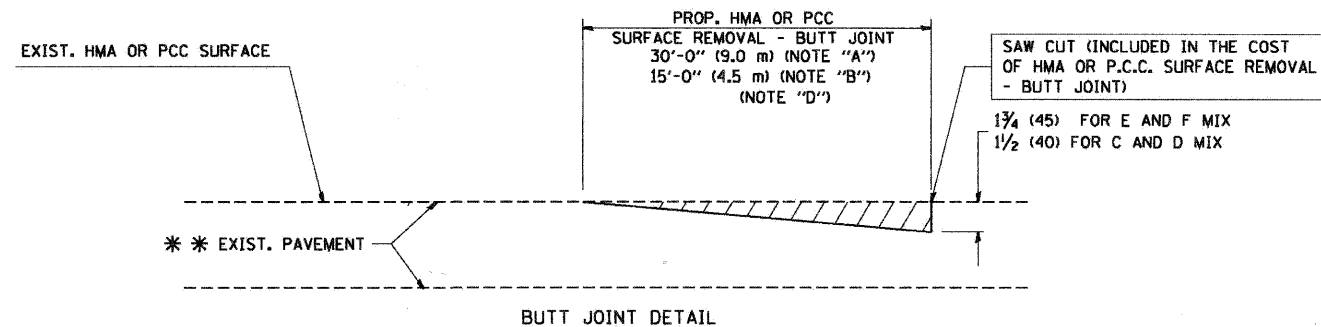
OPTION 2

TYPICAL TEMPORARY RAMP

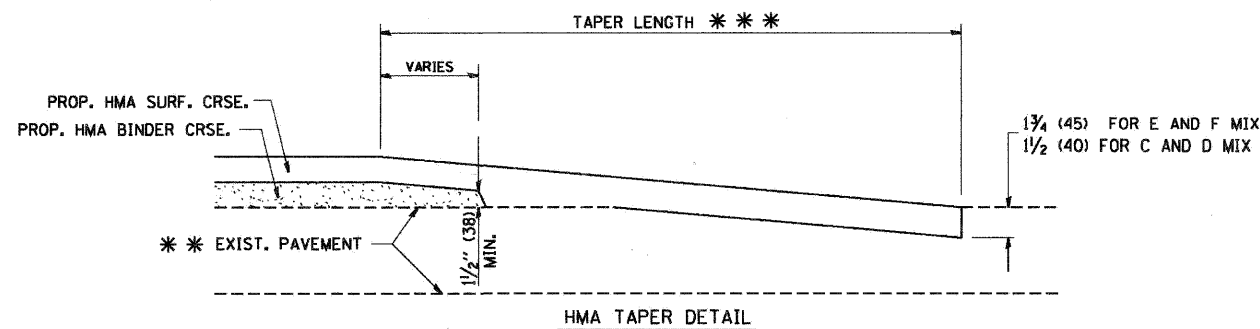


BUTT JOINT AND
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

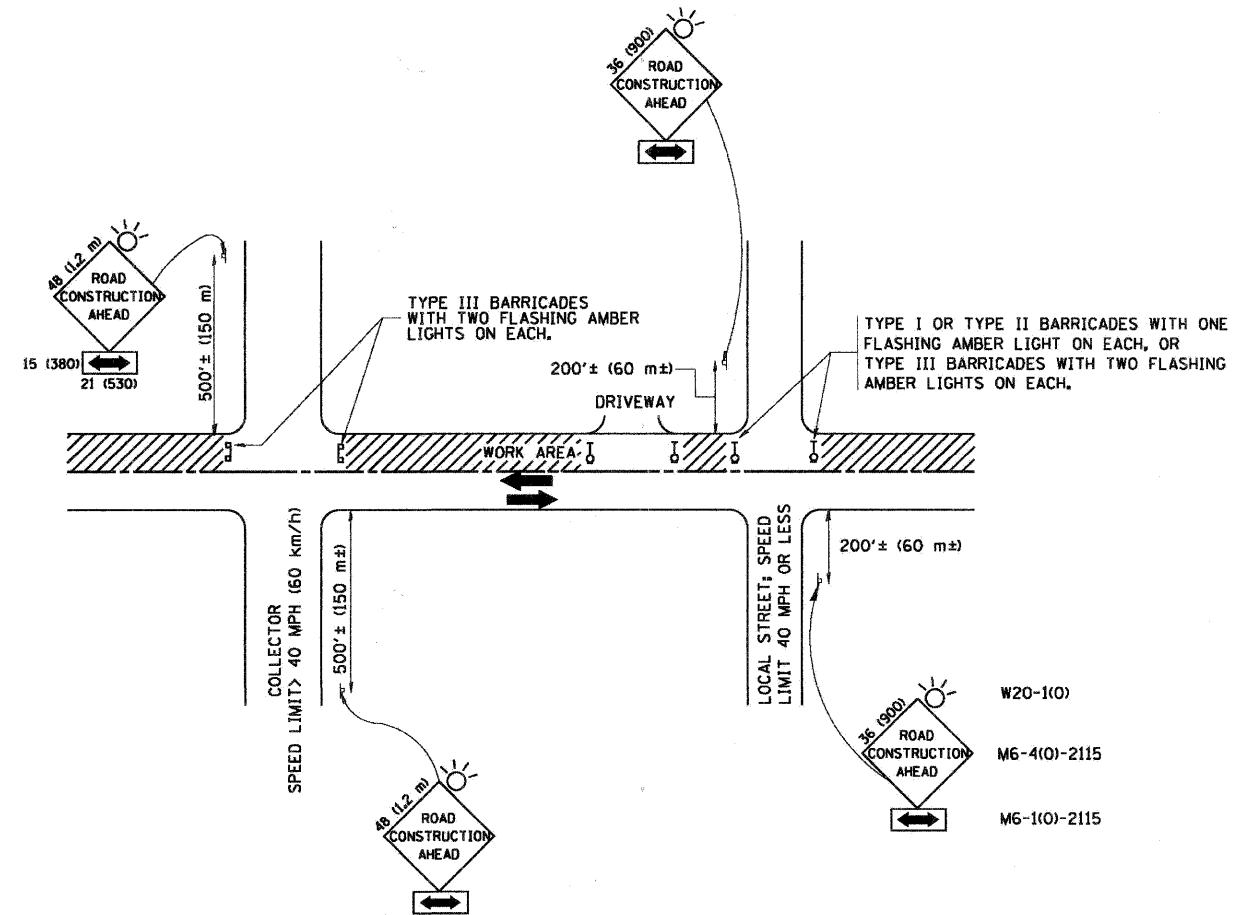
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	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2659	SECTION 11-00177-00-RS	COUNTY DUPAGE	TOTAL SHEETS 21	SHEET NO. 15
BD400-05 BD32			CONTRACT NO. 63585	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

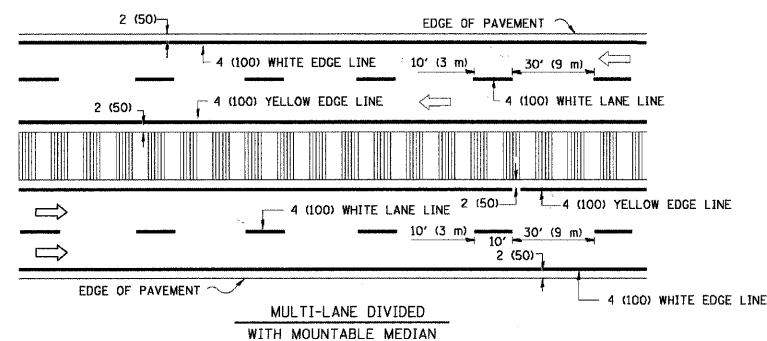
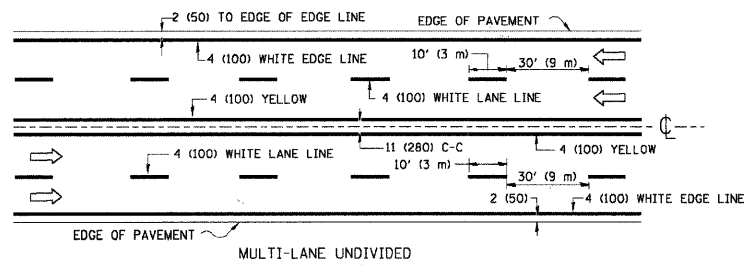
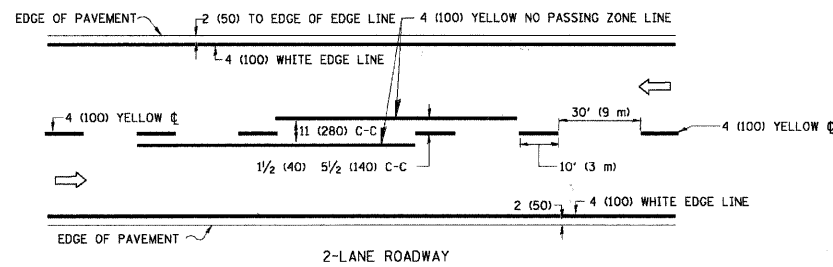
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	PLOT SCALE = 58,800' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2000	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

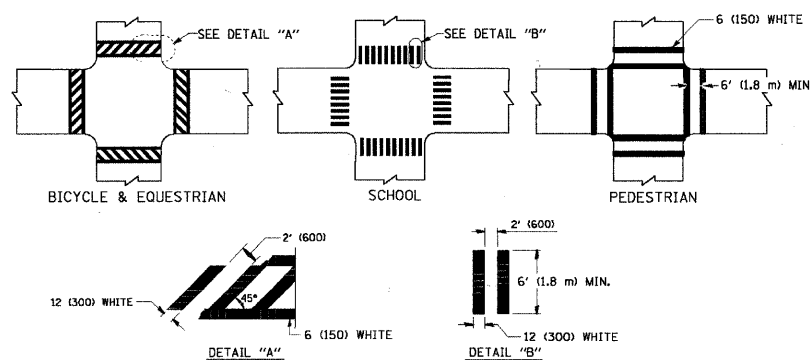
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F.A.J. RTE. 2659	SECTION 11-00177-00-R5	COUNTY DUPAGE	TOTAL SHEETS 21	SHEET NO. 16
TC-10		CONTRACT NO. 63585		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

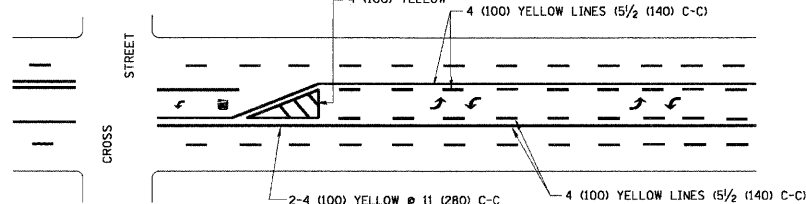
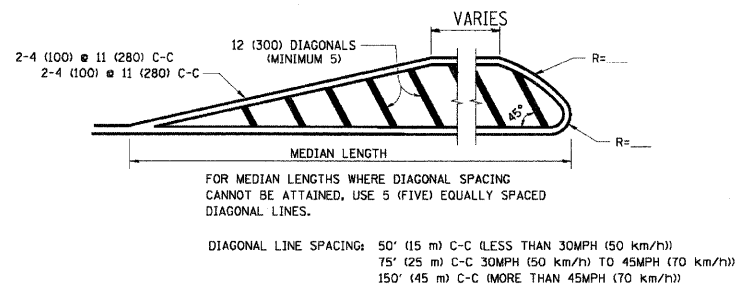
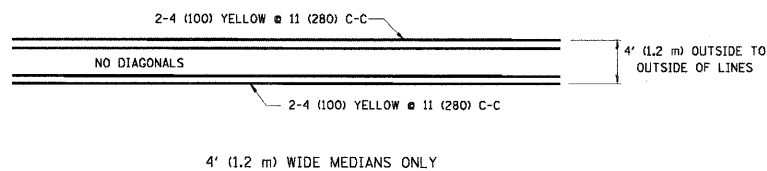


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

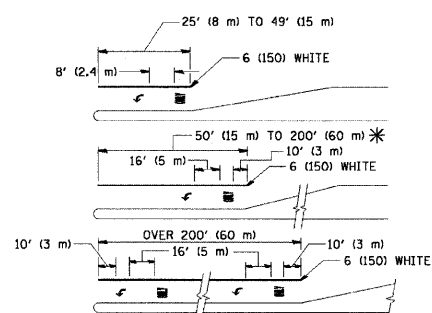
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



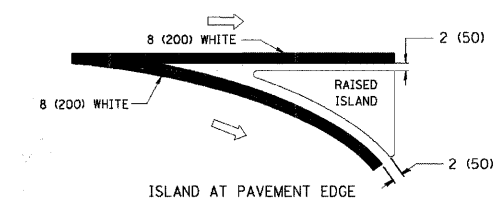
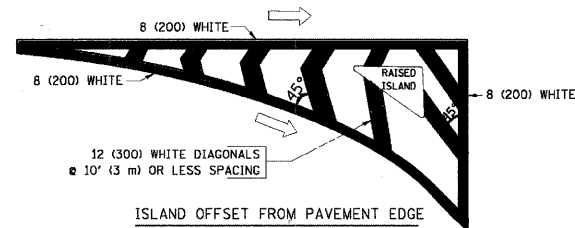
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN)	2 @ 6 (150)	SOLID	WHITE	NOT LESS THAN 6' (1.8 m) APART
A. DIAGONALS (BIKE & EQUESTRIAN)	12 (300) @ 45°	SOLID	WHITE	2' (600) APART
B. LONGITUDINAL BARS (SCHOOL)	12 (300) @ 90°	SOLID	WHITE	SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 78001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

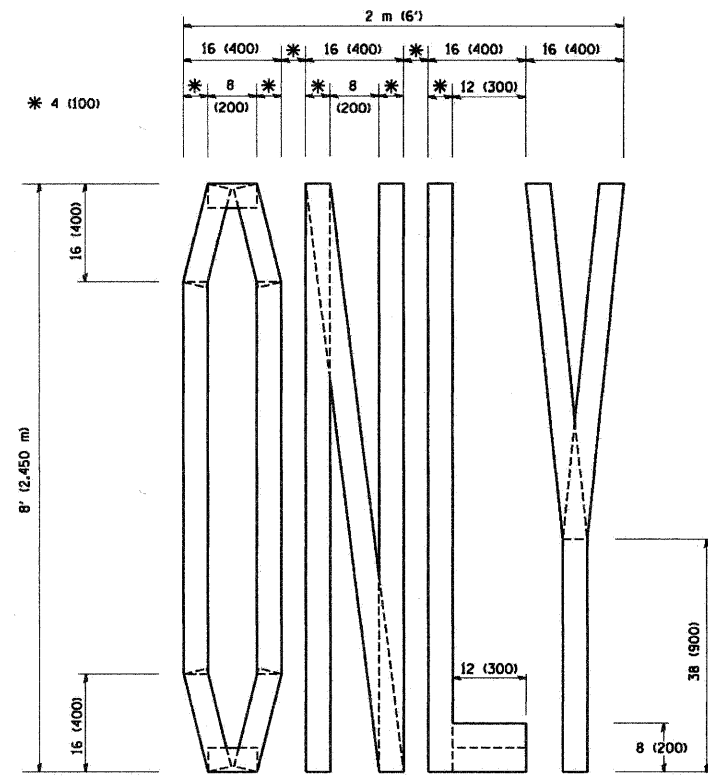
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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

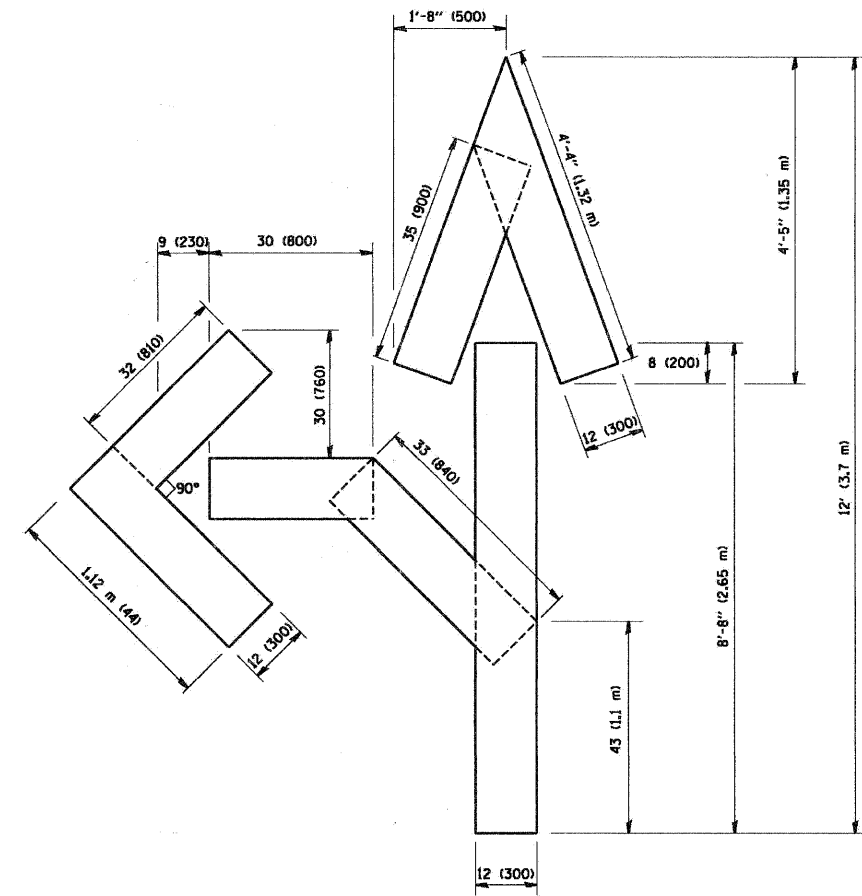
DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

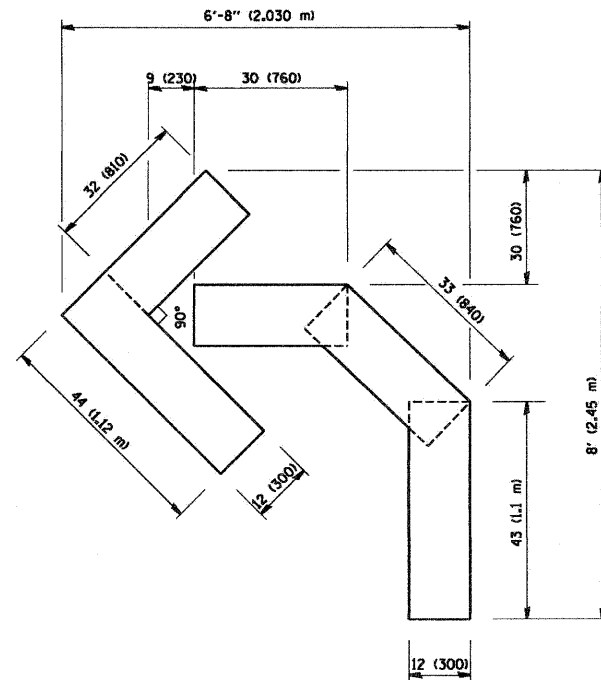
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2659	11-00177-CO-RS	DUPAGE	21	17
TC-13		CONTRACT NO. 63585		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

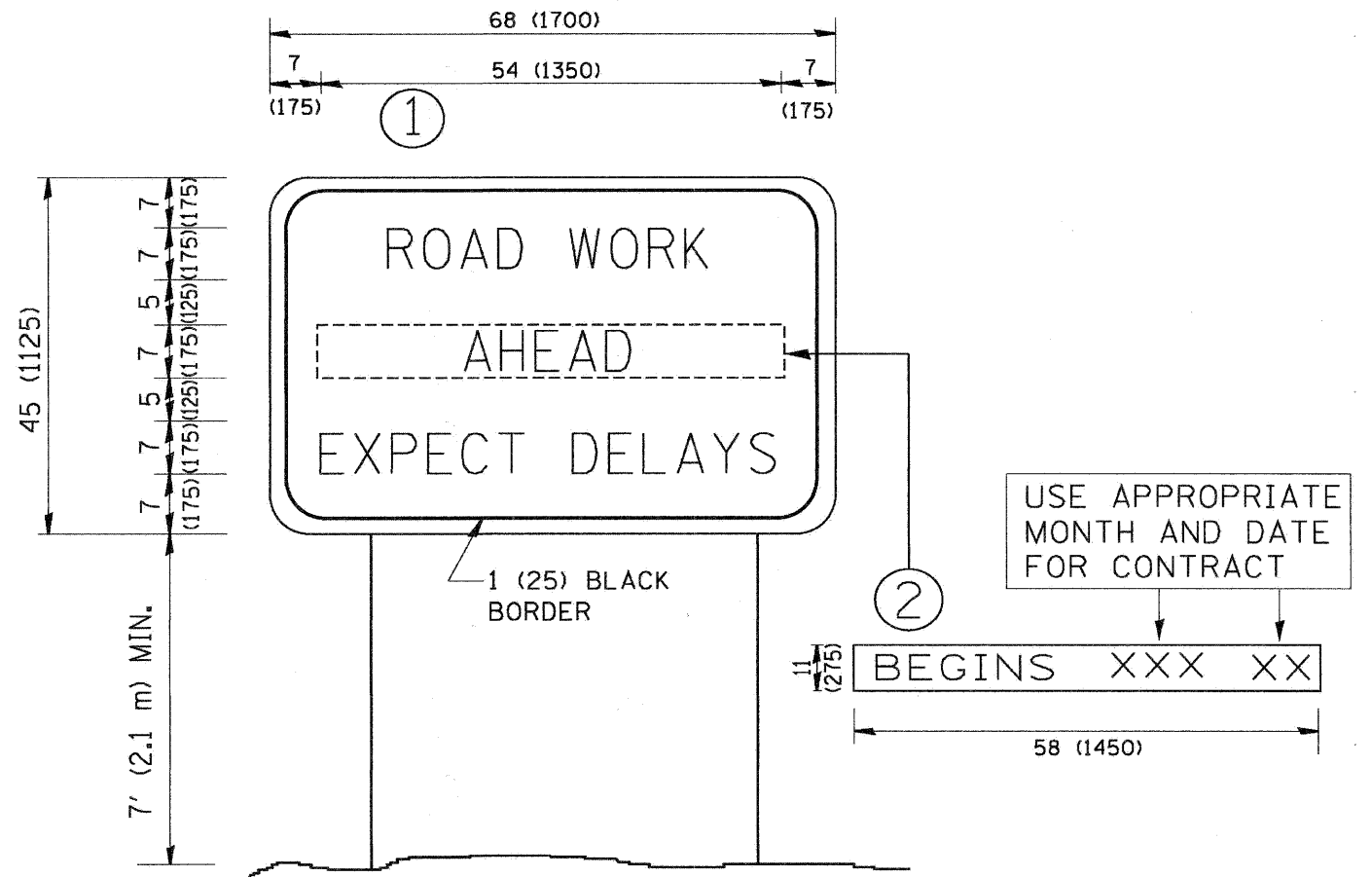
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	PLOT DATE = 1/4/2000	DATE = 09-18-94	REVISED - E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE. 2659	SECTION 11-00177-00-RS	COUNTY DUPAGE	TOTAL SHEETS 21	SHEET NO. 18
TC-16		CONTRACT NO. 63585		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

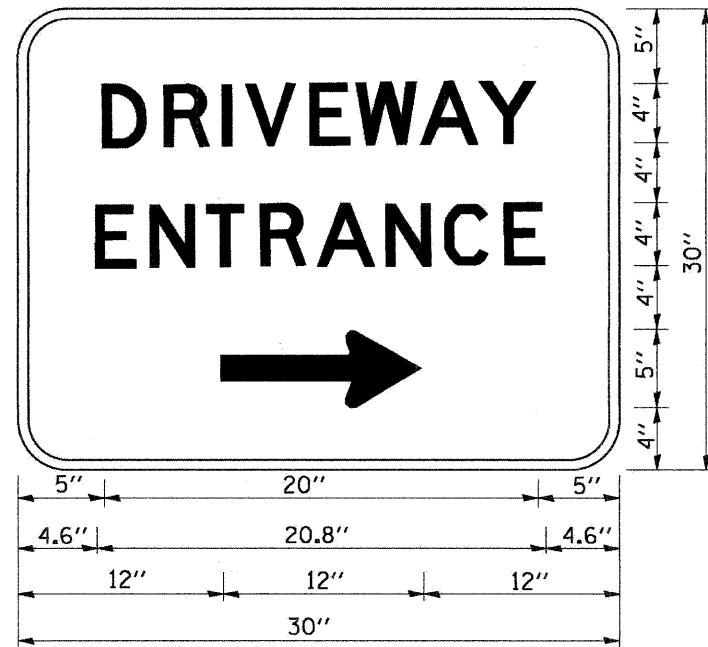
FILE NAME = W:\diststd\22x34\to22.dgn	USER NAME = geglernobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2659	SECTION 11-00177-00-RS	COUNTY DUPAGE	TOTAL SHEETS 21	SHEET NO. 19
TC-22		CONTRACT NO. 63585		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

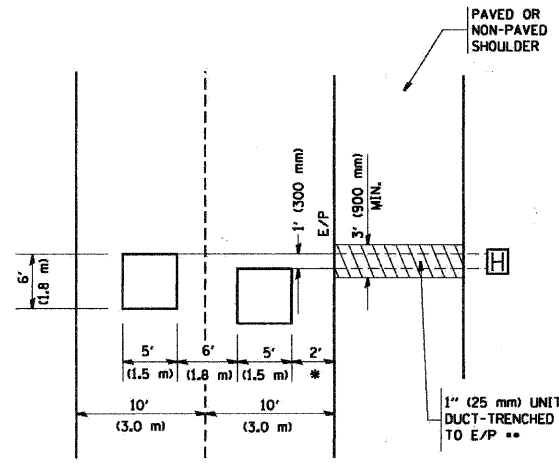
NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = M:\distd\22x34\to26.dgn	USER NAME = geglrobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	2659	11-00177-00-RS	DUPAGE	21	20
		PLOT SCALE = 50.000' / IN.	CHECKED -						TC-26		CONTRACT NO. 63585		
		PLOT DATE = 1/4/2008	DATE -						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



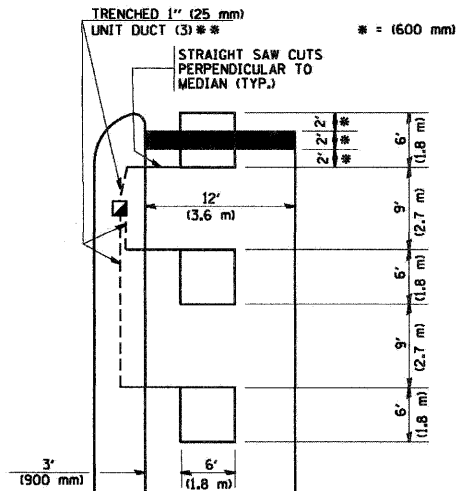
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

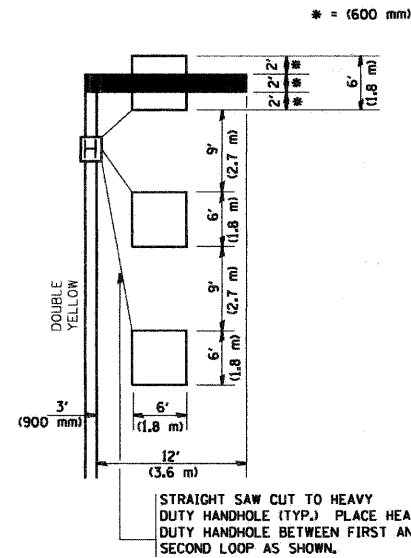


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

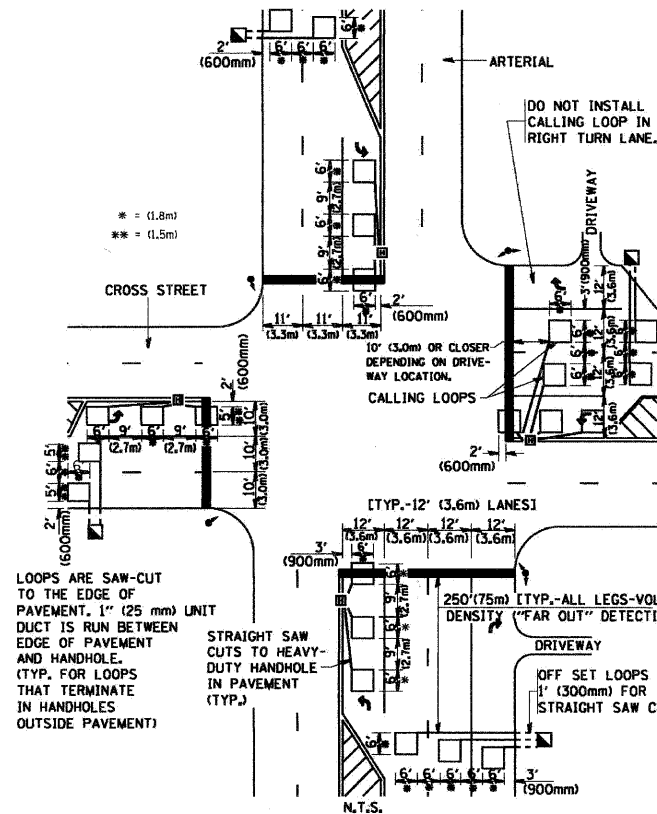
**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



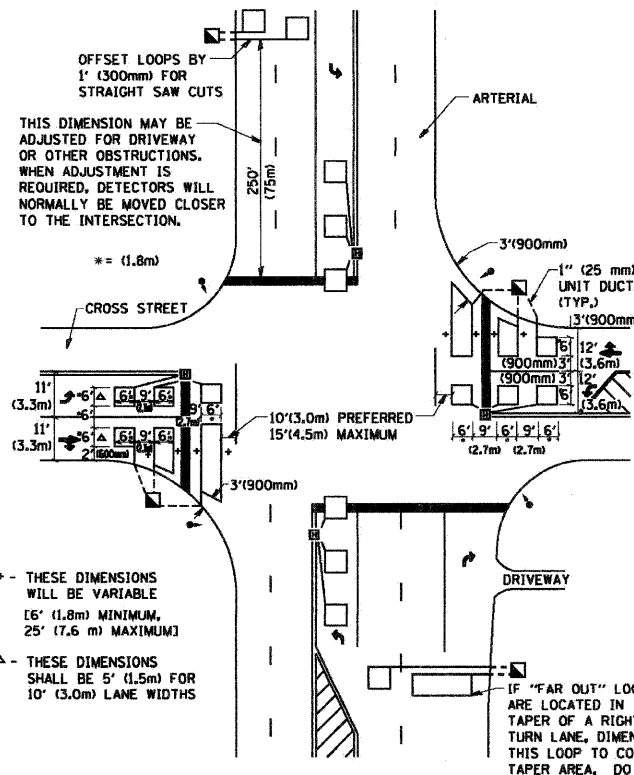
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\distatd\22x34\ts87.dgn	USER NAME = geglionab	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.U. RTE. 2659	SECTION 31-00177-00-R5	COUNTY DUPAGE	TOTAL SHEETS 21	SHEET NO. 21
PLOT SCALE = 5/8" @ 20' / IN.	CHECKED - DATE	R.K.F.	REVISED -	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	CONTRACT NO. 63585	