

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3824	09-00176-00-RS	DUPAGE	17	1

CONTRACT NO. 63588

INDEX OF SHEETS

- 1 COVER SHEET AND INDEX OF SHEETS
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY PROJECT**

**FAU ROUTE 2672 (SPRING ROAD)
HARRISON STREET TO FAU ROUTE 3824 (VALLETTE STREET)
RESURFACING
SECTION 11-00176-00-RS
PROJECT M-9003(793)
DUPAGE COUNTY
JOB NO. C-91-417-11**

TRAFFIC DATA

ADT:
4,200 VPD (2008) 5,000 VPD (2030)

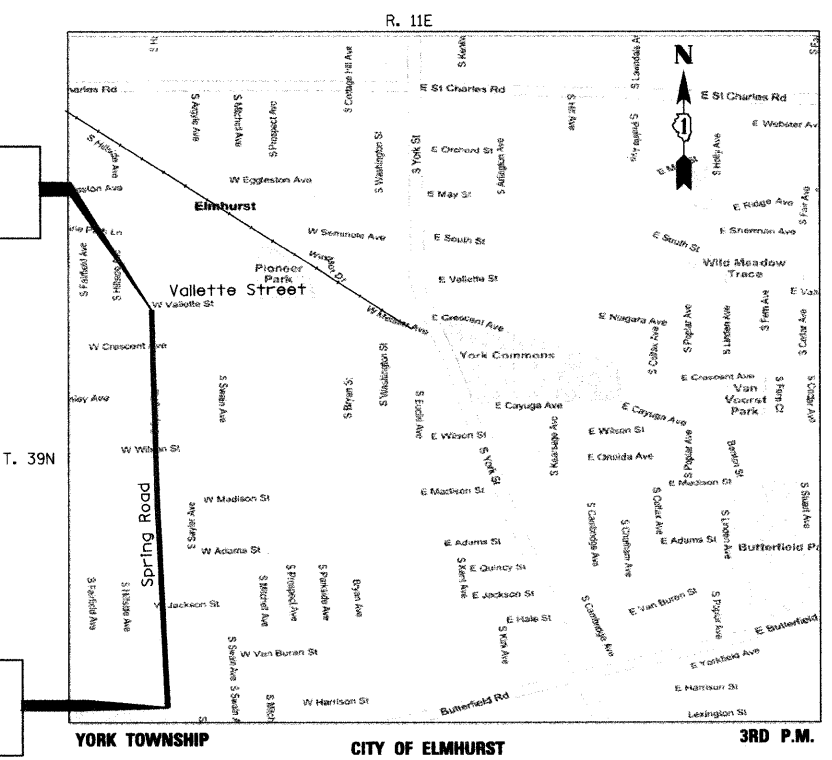
POSTED SPEED DESIGN SPEED
25 MPH (EXISTING) 30 MPH (EXISTING)
25 MPH (PROPOSED) 30 MPH (PROPOSED)

DESIGN DESIGNATION: 8,200 (2011) COLLECTOR 0.24 (RS)

PROJECT IS LOCATED IN THE CITY OF ELMHURST

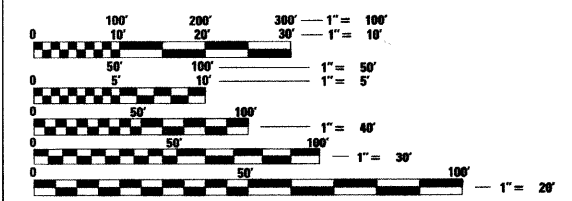
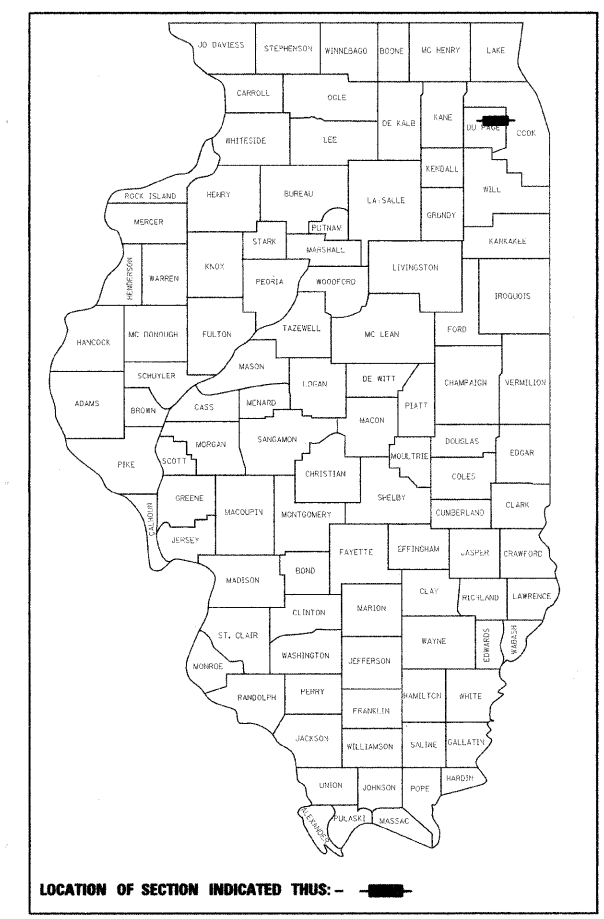
PROJECT ENDS STATION 152+41

PROJECT BEGINS STATION 100+34



LOCATION MAP

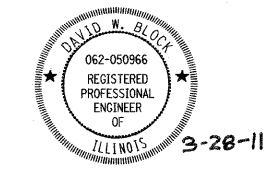
NOT TO SCALE
PROJECT LENGTH (GROSS AND NET)
5,207 FT (0.99 MILES)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CONTRACT NO. 63588



DAVID W. BLOCK, P.E.
NO. 062-050966
EXP. DATE 11/30/11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED ON: March 25, 2011
Michael J. Hayes
CITY OF ELMHURST, DIRECTOR OF PUBLIC WORKS

PASSED: April 11, 2011
C. H. Holt
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW: April 11, 2011
Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**



GENERAL NOTES

1. ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2007.
2. ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
4. BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1 AND THE CITY OF ELMHURST AT 630-530-3777 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
5. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR CITY PROPERTY OR ROW WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
6. SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.
7. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
8. OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
9. HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
10. QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL VERIFY FINAL PATCH LOCATIONS IN THE FIELD, PRIOR TO REMOVAL.
11. THE CONTRACTOR SHALL COORDINATE PAVING OPERATIONS FOR BOTH HMA LEVEL BINDER AND SURFACE COURSES SO THAT THE LONGITUDINAL JOINTS ARE CLOSED AND COMPACTED AT THE END OF EACH DAY. PAVING OPERATIONS SHALL BE SCHEDULED SO THAT ADJACENT LANES ARE PAVED IN THE SAME DIRECTION AS THE INITIAL LANE MINIMIZING THE TIME THE EDGE OF A PAVEMENT MAT IS ALLOWED TO COOL. IF THE CONTRACTOR ELECTS TO INSTALL A ONE-WAY DAILY DETOUR ROUTE FOR CONVENIENCE TO MINIMIZE TRAFFIC IN THE CONSTRUCTION ZONE AND ACROSS THE PAVEMENT MAT EDGE, THE ONE-WAY DAILY DETOUR ROUTE SHALL BE COORDINATED WITH AND APPROVED BY THE ENGINEER PRIOR TO INSTALLATION. THE COST TO INSTALL AND MAINTAIN THE DAILY ONE-WAY DETOUR ROUTE SHALL BE AT THE CONTRACTOR'S OWN EXPENSE.

STORM SEWERS, WATER MAINS, AND UTILITIES

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
2. THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN IF NOT SHOWN ON THE PLANS. ALL UTILITY PROPERTY DAMAGE DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.
4. ALL UTILITY OWNERS SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
5. THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
6. THE CONTRACTOR SHALL ENSURE ALL WATER SYSTEM VALVES, VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.
7. ALL LOOSE MATERIAL DEPOSITED IN THE FLOWLINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT IN ACCORDANCE WITH ARTICLE 107.15.
8. CATCH BASINS, MANHOLES AND INLETS CONSTRUCTED IN A LOCATION WHERE AN EXISTING STRUCTURE WAS REMOVED SHALL INCLUDE UP TO FIVE FEET OF PIPE TO CONNECT EACH EXISTING PIPE. THE NECESSARY PIPE BEYOND FIVE FEET WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "STORM" SEWER AND OF THE TYPE AND SIZE REQUIRED.

THE CONTRACTOR SHALL CONFIRM ALL EXISTING STORM SEWER PIPE SIZES AND INVERTS PRIOR TO ORDERING STRUCTURES. ANY MODIFICATION OF STRUCTURES DUE TO THE FAILURE OF THE CONTRACTOR TO PERFORM THIS TASK SHALL BE AT THE CONTRACTOR'S EXPENSE AND MAY LEAD TO THE REJECTION OF THE STRUCTURE IN THE FIELD.

SIGNING AND STRIPING

1. SEE IDOT STANDARD DETAIL 780001, DISTRICT ONE DETAIL TC-13 AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.
2. THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL SIGNS THAT INTERFERE WITH CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING AND MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT IN ACCORDANCE WITH ARTICLE 107.25.

TRAFFIC CONTROL

1. SEE TRAFFIC CONTROL HIGHWAY STANDARDS CONCERNING TRAFFIC CONTROL AND PROTECTION.
2. THE CONTRACTOR SHALL SCHEDULE CONSTRUCTION ACTIVITIES SO THAT THERE ARE ALWAYS TWO LANES OF TRAFFIC OPEN AT THE END OF EACH DAY. IF THE CONTRACTOR ELECTS TO INSTALL A DETOUR FOR CONVENIENCE, IT WILL ONLY BE ALLOWED UPON SUBMITTAL OF A DETOUR PLAN AND APPROVAL BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE AT THE CONTRACTOR'S OWN EXPENSE.

MISCELLANEOUS

1. MATERIALS RESULTING FROM THE REMOVAL OF CONCRETE SURFACES, UTILITY STRUCTURE ADJUSTMENT, RESTORATION WORK, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IF THE CONTRACTOR DOES NOT REMOVE THESE MATERIALS AT THE REQUEST OF THE ENGINEER, THE CITY OF ELMHURST WILL HIRE A CONTRACTOR TO HAVE THE MATERIAL REMOVED AND THE CONTRACTOR SHALL BE BILLED (CHARGED) ACCORDINGLY.
2. THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS, OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS/HER YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO USE OF THE WATER.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC WITHIN 24 HOURS OF PLACING PRIME COAT AND THE LAYING OF HMA. THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS AND ALL LOOSE MATERIAL.
4. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE ENGINEER WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO SIDEWALK REPLACEMENT AND/OR CURB AND GUTTER REPLACEMENT. AT LOCATIONS WHERE THE SIDEWALK OR CURB AND GUTTER IS SCHEDULED TO BE REMOVED, THE CONTRACTOR SHALL CONTACT THE BUSINESS/HOMEOWNER 24 HOURS PRIOR TO REMOVING THE CURB OR SIDEWALK. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES. THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE A DRIVEWAY FOR MORE THAN 48 HOURS UNDER ANY CIRCUMSTANCES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE BARRICADES TO PREVENT TRAFFIC FROM USING THE DRIVEWAYS DURING THIS PERIOD.
5. WHEN REMOVING PAVEMENT, CURB AND GUTTER, SHOULDER, AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES AND BUILDING FOUNDATIONS WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.

STD. NO.

- 000001-06
- 424001-05
- 442201-03
- 602301-03
- 604001-03
- 604056-03
- 606001-04
- 701301-04
- 701311-03
- 701501-06
- 701701-07
- 701801-04
- 701901-01
- 780001-02

HIGHWAY STANDARDS

DESCRIPTION

- STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- CURB RAMPS FOR SIDEWALKS
- CLASS C AND D PATCHES
- INLET - TYPE A
- FRAME AND LIDS TYPE 1
- FRAME AND GRATE TYPE 11V
- CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
- LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
- URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
- URBAN LANE CLOSURE, MULTILANE INTERSECTION
- LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
- TRAFFIC CONTROL DEVICES
- TYPICAL PAVEMENT MARKINGS

• SPECIALTY ITEM

SUMMARY OF QUANTITIES

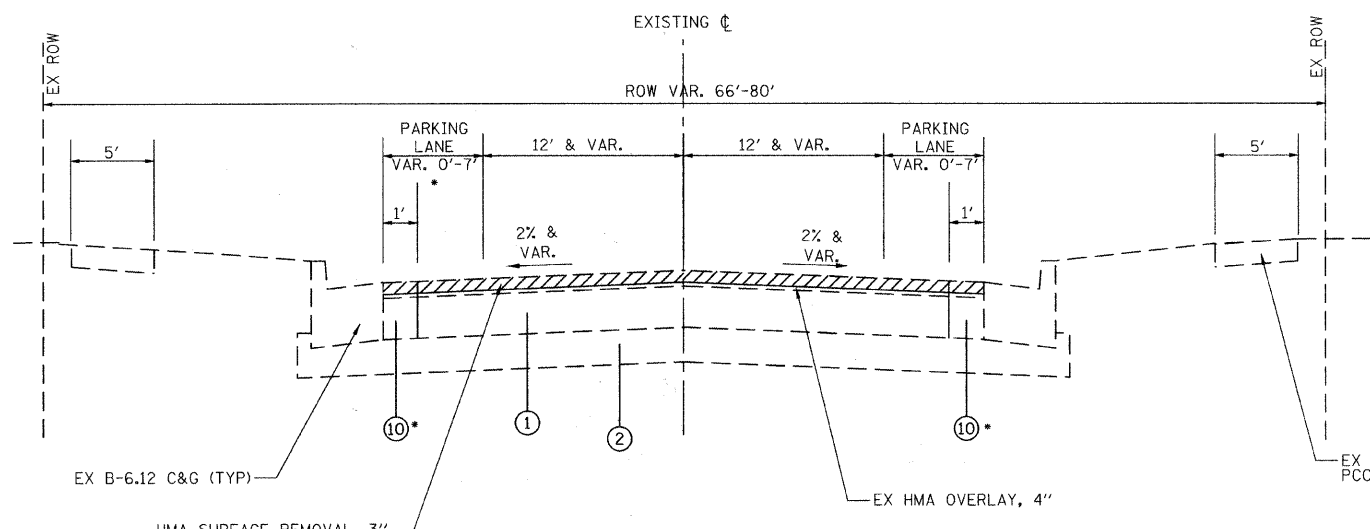
* CODE NO	PAY ITEM	UNIT	QUANTITY	0005 ROADWAY 70% STP 30% LA	0005 ROADWAY 100% LA
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	316	316	
21101625	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	885	885	
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	11	11	
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	11	11	
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	11	11	
25200110	SODDING, SALT TOLERANT	SQ YD	885	885	
25200200	SUPPLEMENTAL WATERING	UNIT	13	13	
31101200	SUB-BASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	3,089	3,089	
35300400	PORTLAND CEMENT CONCRETE BASE COURSE 9"	SQ YD	280	280	
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	3,973	3,973	
40600300	AGGREGATE (PRIME COAT)	TON	80	80	
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	5	5	
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	1,113	1,113	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	224	224	
40600990	TEMPORARY RAMP	SQ YD	300	300	
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	2,225	2,225	
42001300	PROTECTIVE COAT	SQ YD	2,320	2,320	
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	276	276	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	9,489	9,489	
42400800	DETECTABLE WARNINGS	SQ FT	635	635	
44000100	PAVEMENT REMOVAL	SQ YD	280	280	
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SQ YD	19,586	19,586	
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	558	558	
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	3,938	3,938	
44000600	SIDEWALK REMOVAL	SQ FT	10,124	10,124	
44201749	CLASS D PATCHES, TYPE I, 9 INCH	SQ YD	490	490	
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	490	490	
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	490	490	
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	490	490	
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	5,207	5,207	
60236825	INLETS, TYPE A, TYPE 11V FRAME AND GRATE	EACH	4	4	
60266600	VALVE BOXES TO BE ADJUSTED	EACH	9	9	
60404805	FRAMES AND GRATES, TYPE 11V	EACH	5	5	
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	5	5	
60500060	REMOVING INLETS	EACH	4	4	
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	3,938	3,938	
67100100	MOBILIZATION	L SUM	1	1	
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	91	91	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1,000	1,000	
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	636	636	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	334	334	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	91	91	
78005130	EPOXY PAVEMENT MARKING - LINE 6"	FOOT	500	500	
78005150	EPOXY PAVEMENT MARKING - LINE 12"	FOOT	318	318	
78005180	EPOXY PAVEMENT MARKING - LINE 24"	FOOT	167	167	
78300100	PAVEMENT MARKING REMOVAL	SQ FT	34	34	
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	47	47	
X7830074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	500	500	
X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	318	318	
X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	167	167	
Z0004522	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6"	SQ YD	282	282	
Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	31	31	
Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	5	5	
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	176	176	
Z0042002	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	160	160	

FILE NAME =	USER NAME = USER	DESIGNED - CEC	REVISED -
g:\CH09\0002\Road\Sheets\G-102-Gen\Notes	Spring.shd	DRAWN - JKM	REVISED -
	PLLOT SCALE = 69.9996 Ft / IN.	CHECKED - DWB	REVISED -
	PLLOT DATE = 4/6/2011	DATE - 04/06/2011	REVISED -

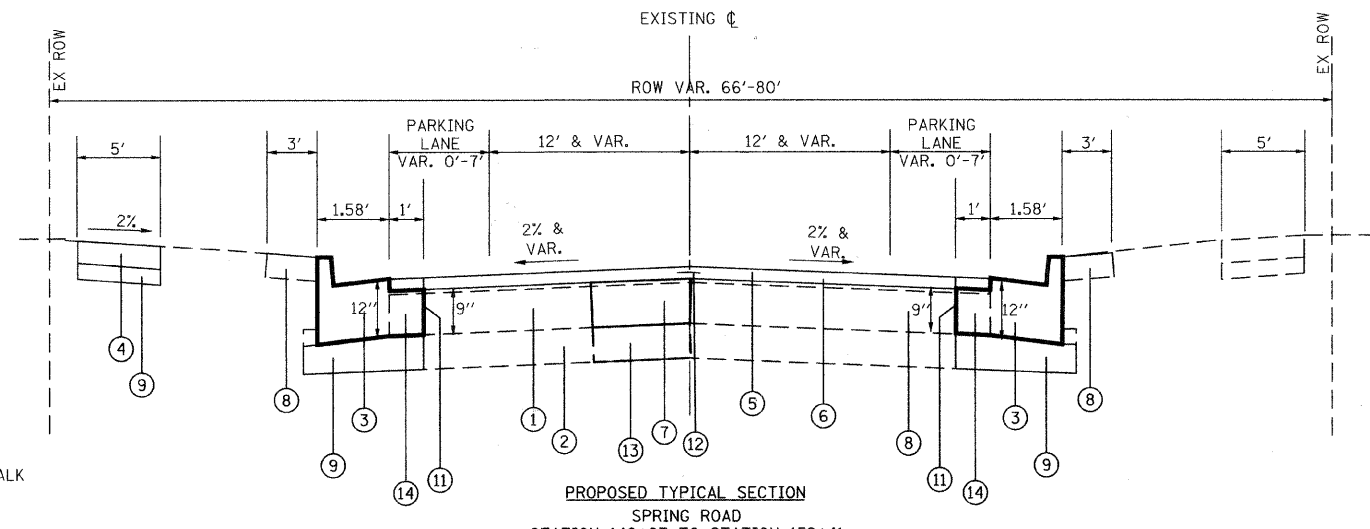
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SPRING ROAD
GENERAL NOTES, HIGHWAY STANDARDS,
AND SUMMARY OF QUANTITIES**

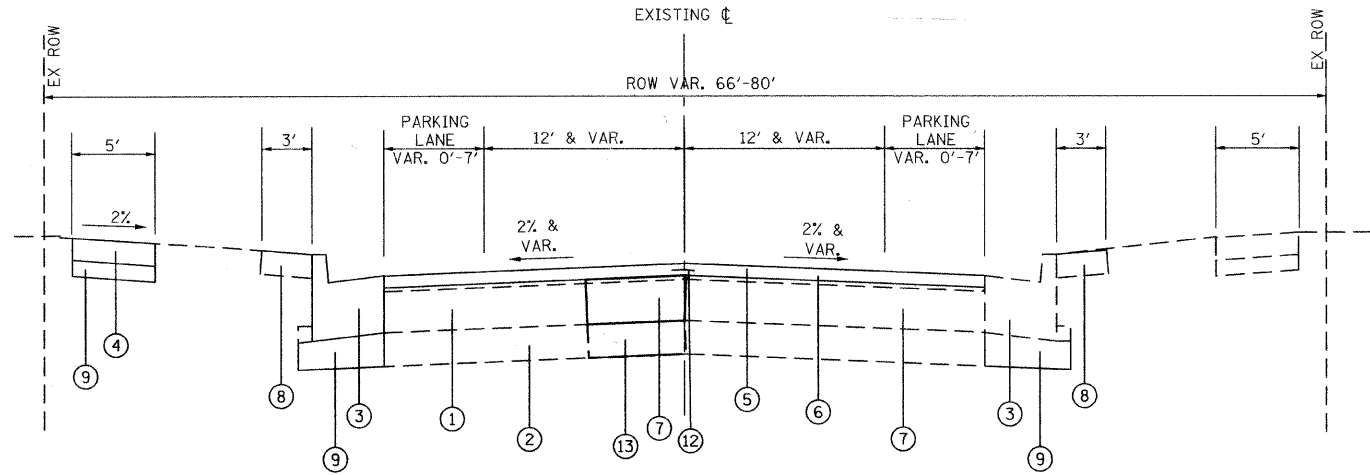
F.A.I.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2672	11-00176-00-RS	DUPAGE	17	2
CONTRACT NO. 63588				
NOT TO SCALE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



EXISTING TYPICAL SECTION
SPRING ROAD
STATION 100+34 TO 152+41



PROPOSED TYPICAL SECTION
SPRING ROAD
STATION 140+07 TO STATION 152+41



PROPOSED TYPICAL SECTION
SPRING ROAD
STATION 100+34 TO 140+07

- LEGEND**
- ① EXISTING HMA PAVEMENT, 12"
 - ② EXISTING SUBBASE GRAN. MAT'L., CA-6
 - ③ COMBINATION CURB & GUTTER REMOVAL (REMOVAL AND DISPOSAL OF THE MATERIAL UNDER THE PROPOSED CURB AND GUTTER LOCATION IN ORDER TO INSTALL THE PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B 4" UNDER THE PROPOSED CURB AND GUTTER SHALL BE INCLUDED IN THIS ITEM) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
 - ④ SIDEWALK REMOVAL AND PCC SIDEWALK 5", REMOVE AND REPLACE AS DIRECTED BY ENGINEER
 - ⑤ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
 - ⑥ LEVELING BINDER (MACHINE METHOD), N70, 1"
 - ⑦ CLASS D PATCHES, 9", AS DIRECTED BY THE ENGINEER
 - ⑧ SODDING, SALT TOLERANT TOPSOIL FURNISH AND PLACE, 6"
 - ⑨ SUB-BASE GRANULAR MATERIAL, TYPE B 4"
 - ⑩ PAVEMENT REMOVAL
 - ⑪ SAWCUT FULL DEPTH OF EXISTING PAVEMENT (INCLUDED IN THE COST OF PAVEMENT REMOVAL)
 - ⑫ STRIP REFLECTIVE CRACK CONTROL TREATMENT
 - ⑬ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SUBGRADE AS DIRECTED BY THE ENGINEER
 - ⑭ PORTLAND CEMENT CONCRETE BASE COURSE 9" (POURED MONOLITHICALLY WITH THE COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12)

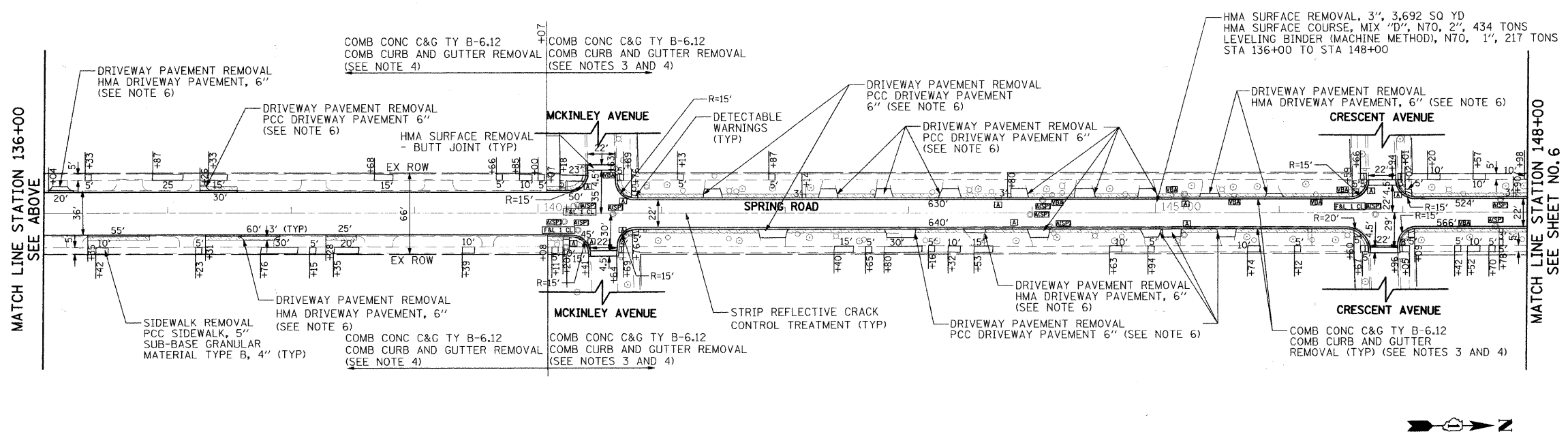
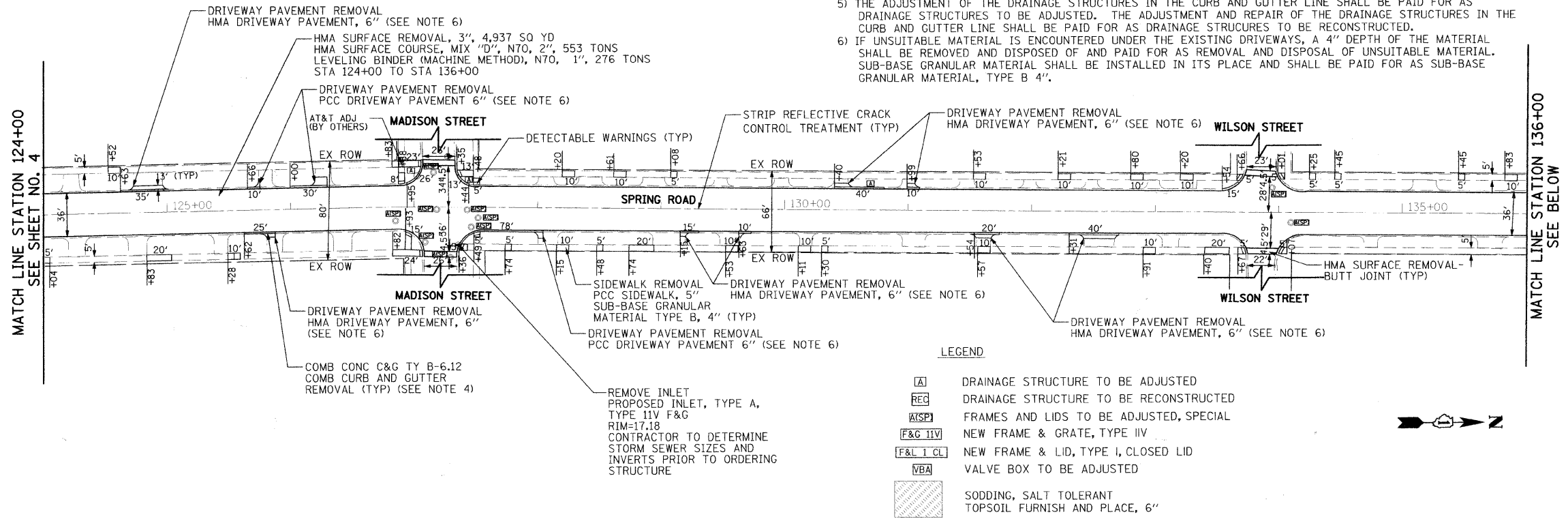
HOT-MIX ASPHALT MIXTURE REQUIREMENTS

THE CONTRACTOR SHALL MILL BEFORE PATCHING.

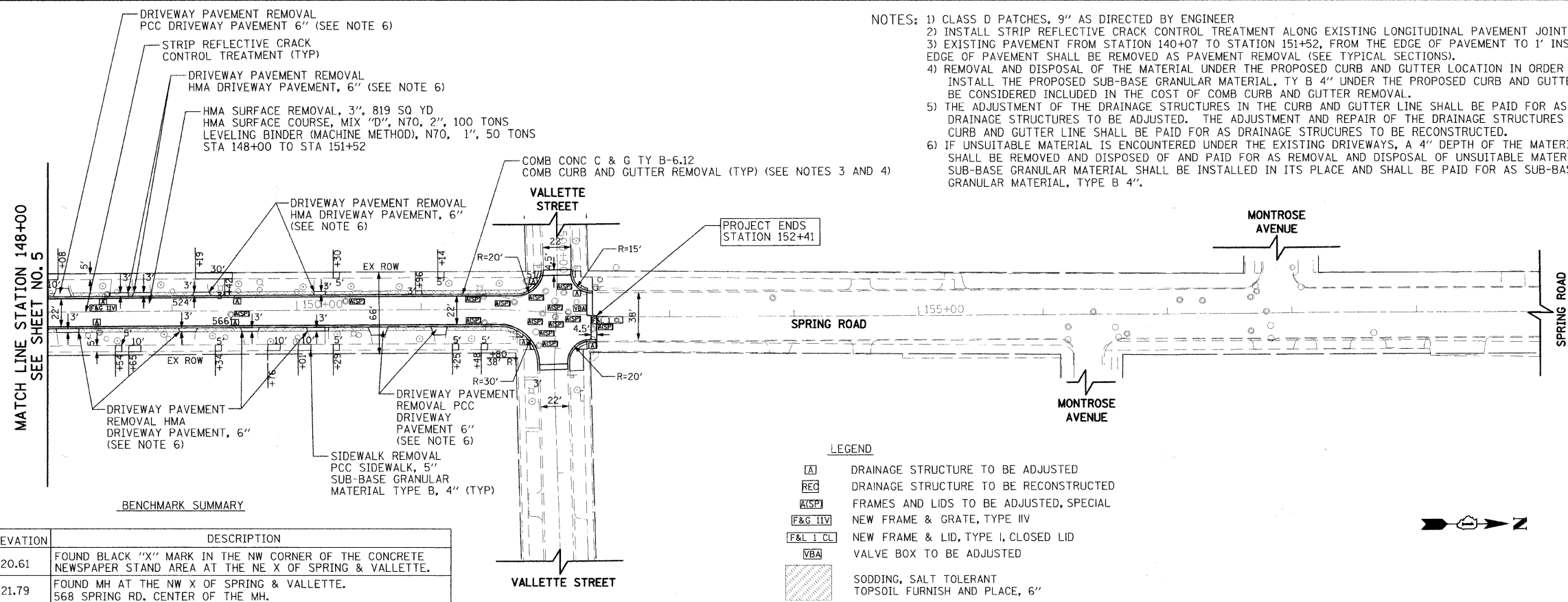
MIXTURE TYPE	AIR VOIDS @ Ndes
HMA SURFACE COURSE, MIX "D", N70 (IL 9.5mm), 2"	4% @ 70 GYRATIONS
LEVELING BINDER (MACHINE METHOD), N70, 1" (IL 9.5 mm)	4% @ 70 GYRATIONS
CLASS D PATCHES (HMA BINDER IL-19 MM), 9" (IN 3 LIFTS)	4% @ 70 GYRATIONS
HOT-MIX ASPHALT DRIVEWAYS 6"	
HMA SURFACE COURSE, MIX "C", N50 (IL 9.5mm) 2"	4% @ 50 GYRATIONS
HMA BASE COURSE (HMA BINDER IL - 19mm) 4" (IN 2 LIFTS)	4% @ 50 GYRATIONS

NOTES: 1) THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2) THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

- NOTES: 1) CLASS D PATCHES, 9" AS DIRECTED BY ENGINEER
 2) INSTALL STRIP REFLECTIVE CRACK CONTROL TREATMENT ALONG EXISTING LONGITUDINAL PAVEMENT JOINTS.
 3) EXISTING PAVEMENT FROM STATION 140+07 TO STATION 151+52, FROM THE EDGE OF PAVEMENT TO 1' INSIDE EDGE OF PAVEMENT SHALL BE REMOVED AS PAVEMENT REMOVAL (SEE TYPICAL SECTIONS).
 4) REMOVAL AND DISPOSAL OF THE MATERIAL UNDER THE PROPOSED CURB AND GUTTER LOCATION IN ORDER TO INSTALL THE PROPOSED SUB-BASE GRANULAR MATERIAL, TY B 4" UNDER THE PROPOSED CURB AND GUTTER SHALL BE CONSIDERED INCLUDED IN THE COST OF COMB CURB AND GUTTER REMOVAL.
 5) THE ADJUSTMENT OF THE DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE ADJUSTED. THE ADJUSTMENT AND REPAIR OF THE DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE RECONSTRUCTED.
 6) IF UNSUITABLE MATERIAL IS ENCOUNTERED UNDER THE EXISTING DRIVEWAYS, A 4" DEPTH OF THE MATERIAL SHALL BE REMOVED AND DISPOSED OF AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL. SUB-BASE GRANULAR MATERIAL SHALL BE INSTALLED IN ITS PLACE AND SHALL BE PAID FOR AS SUB-BASE GRANULAR MATERIAL, TYPE B 4".



FILE NAME = g:\ch89\0882\road\sheet\PP-202-Spring	USER NAME = JJSFR	DESIGNED - JKM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SPRING ROAD IMPROVEMENT PLAN			F.A.U. RTE. = 2672	SECTION = 11-00176-00-RS	COUNTY = DUPAGE	TOTAL SHEET SHEETS NO. = 17 / 5	
PLOT SCALE = 50.0000 1/4" IN.		DRAWN - JKM	REVISED -		SCALE:	SHEET NO. 2 OF 3 SHEETS	STA. 124+00.00 TO STA. 148+00.00	CONTRACT NO. 63588				
PLOT DATE = 4/6/2011		CHECKED - DWB	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE - 04/06/2011	REVISED -									



- NOTES:
- 1) CLASS D PATCHES, 9" AS DIRECTED BY ENGINEER
 - 2) INSTALL STRIP REFLECTIVE CRACK CONTROL TREATMENT ALONG EXISTING LONGITUDINAL PAVEMENT JOINTS.
 - 3) EXISTING PAVEMENT FROM STATION 140+07 TO STATION 151+52, FROM THE EDGE OF PAVEMENT TO 1' INSIDE EDGE OF PAVEMENT SHALL BE REMOVED AS PAVEMENT REMOVAL (SEE TYPICAL SECTIONS).
 - 4) REMOVAL AND DISPOSAL OF THE MATERIAL UNDER THE PROPOSED CURB AND GUTTER LOCATION IN ORDER TO INSTALL THE PROPOSED SUB-BASE GRANULAR MATERIAL, TY B 4" UNDER THE PROPOSED CURB AND GUTTER SHALL BE CONSIDERED INCLUDED IN THE COST OF COMB CURB AND GUTTER REMOVAL.
 - 5) THE ADJUSTMENT OF THE DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE ADJUSTED. THE ADJUSTMENT AND REPAIR OF THE DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE RECONSTRUCTED.
 - 6) IF UNSUITABLE MATERIAL IS ENCOUNTERED UNDER THE EXISTING DRIVEWAYS, A 4" DEPTH OF THE MATERIAL SHALL BE REMOVED AND DISPOSAL OF AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL. SUB-BASE GRANULAR MATERIAL SHALL BE INSTALLED IN ITS PLACE AND SHALL BE PAID FOR AS SUB-BASE GRANULAR MATERIAL, TYPE B 4".

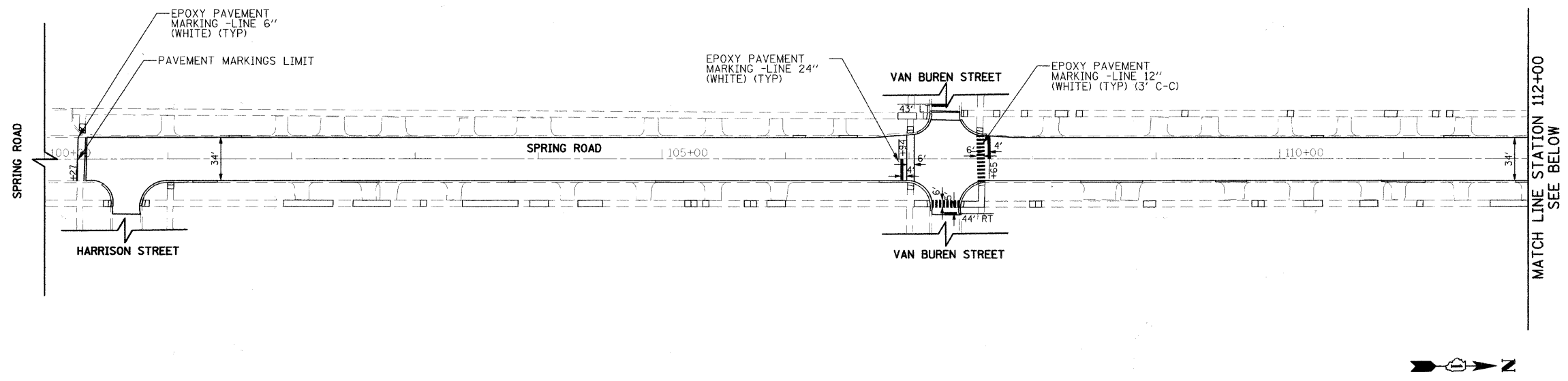
MATCH LINE STATION 148+00
SEE SHEET NO. 5

PROJECT ENDS
STATION 152+41

BENCHMARK SUMMARY

NAME	ELEVATION	DESCRIPTION
TBM # 1	20.61	FOUND BLACK "X" MARK IN THE NW CORNER OF THE CONCRETE NEWSPAPER STAND AREA AT THE NE X OF SPRING & VALLETTE.
TBM # 2	21.79	FOUND MH AT THE NW X OF SPRING & VALLETTE. 568 SPRING RD. CENTER OF THE MH.

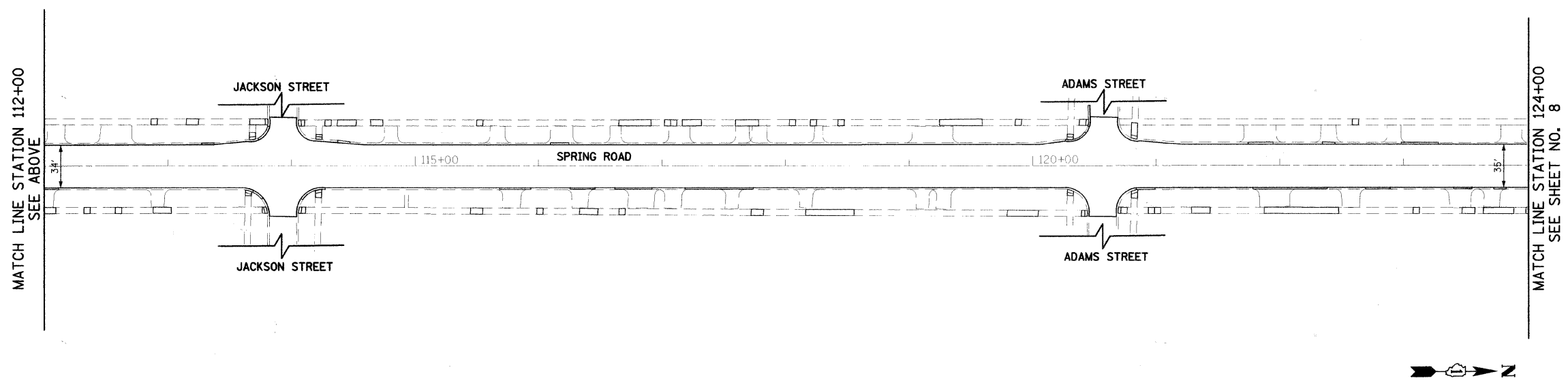
- LEGEND
- [A] DRAINAGE STRUCTURE TO BE ADJUSTED
 - [REC] DRAINAGE STRUCTURE TO BE RECONSTRUCTED
 - [AISP] FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
 - [F&G IIV] NEW FRAME & GRATE, TYPE IIV
 - [F&L I CL] NEW FRAME & LID, TYPE I, CLOSED LID
 - [VBA] VALVE BOX TO BE ADJUSTED
 - [Hatched Box] SODDING, SALT TOLERANT TOPSOIL FURNISH AND PLACE, 6"



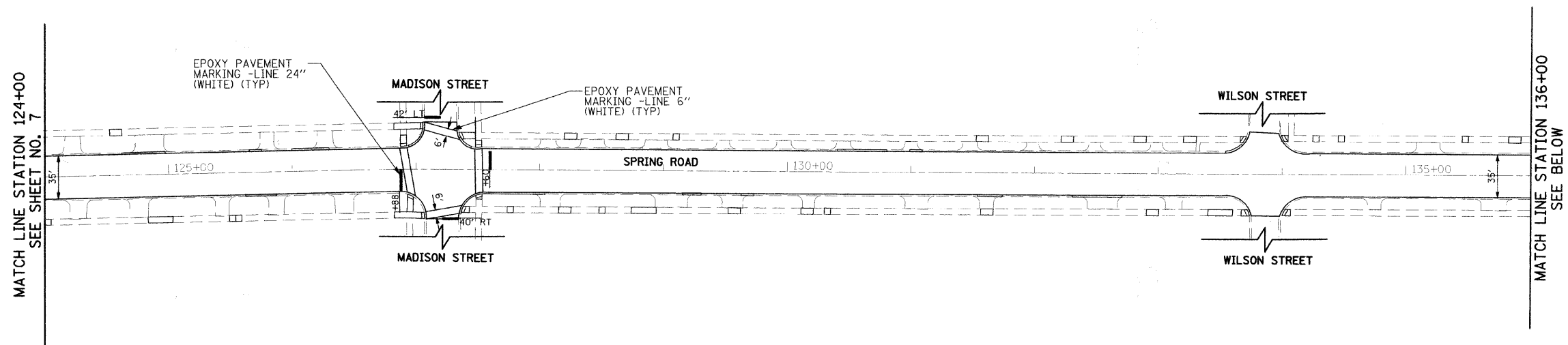
PAVEMENT MARKING ITEM	CORRESPONDING RECESSED GROOVING ITEM
EPOXY PAVEMENT MARKING - LINE 6"	GROOVING FOR RECESSED PAVEMENT MARKING 7"
EPOXY PAVEMENT MARKING - LINE 12"	GROOVING FOR RECESSED PAVEMENT MARKING 13"
EPOXY PAVEMENT MARKING - LINE 24"	GROOVING FOR RECESSED PAVEMENT MARKING 25"

NOTES: 1) EPOXY PAVEMENT MARKINGS SHALL BE INSTALLED IN GROOVED RECESSED CHANNELS CONSTRUCTED 0.040 INCHES BELOW THE SURFACE AND 1 INCH WIDER THAN THE PAVEMENT MARKING LINE. CONSTRUCTION OF THE RECESSED CHANNELS SHALL BE PAID FOR SEPARATELY PER FOOT AS GROOVING FOR RECESSED PAVEMENT MARKING OF THE WIDTH SPECIFIED AND PER SQUARE FOOT FOR GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS.

2) SEE TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKING FOR GUIDANCE



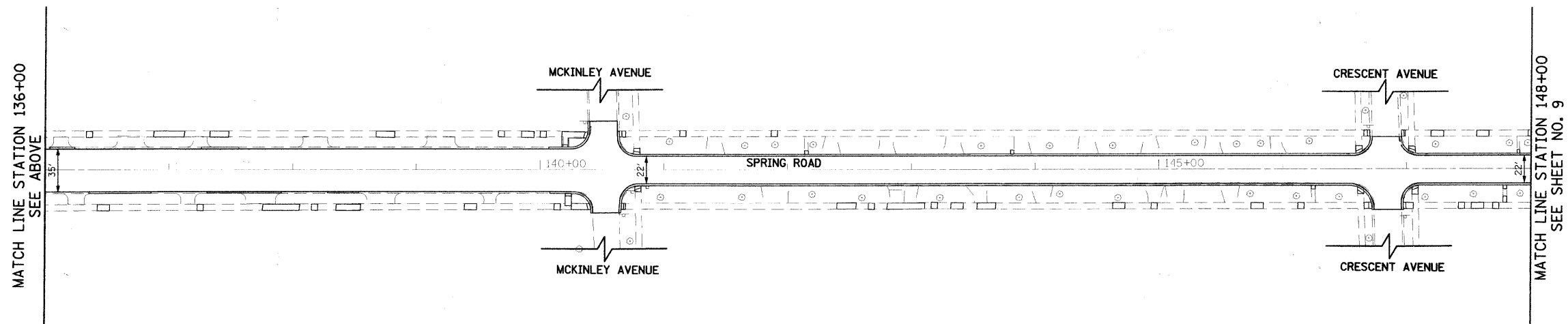
FILE NAME =	USER NAME = .USER.	DESIGNED - JKM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SPRING ROAD SPRING ROAD PAVEMENT MARKING PLAN	F.A.U. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
E:\ch09\0082\road\sheet\PM-501-Spring	4-01.SHT	DRAWN - JKM	REVISED -			2672	11-00176-00-RS	DUPAGE	17	7	
PLT SCALE = 50.000' / IN.		CHECKED - DWB	REVISED -			CONTRACT NO. 63588					
PLT DATE = 4/6/2011		DATE - 04/06/2011	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
					SCALE:	SHEET NO. 1 OF 3 SHEETS		STA. 100+34.00 TO STA. 124+00.00			



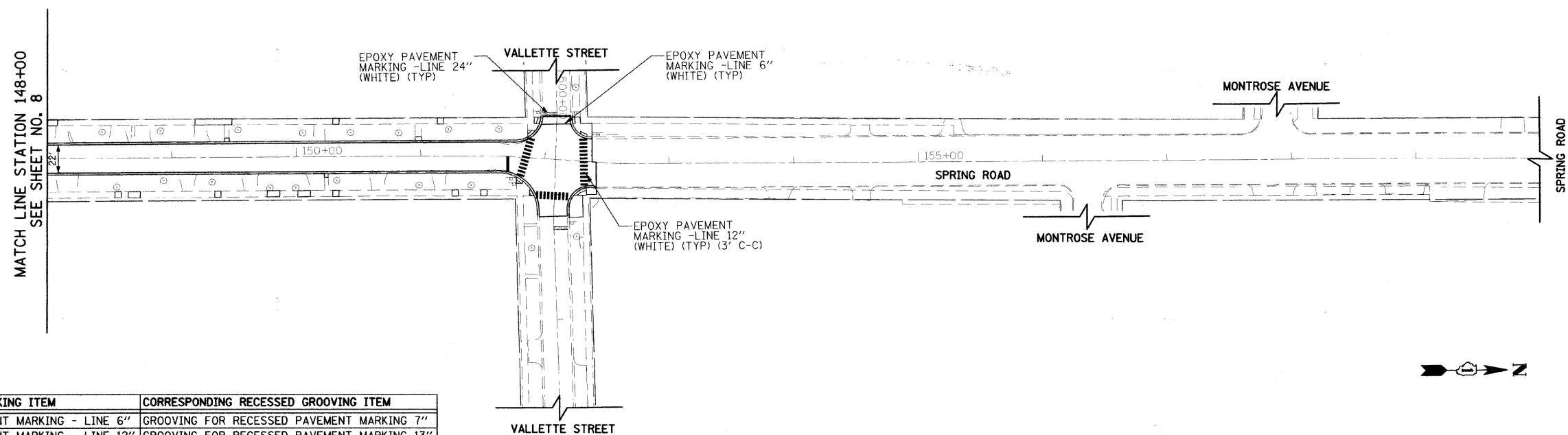
PAVEMENT MARKING ITEM	CORRESPONDING RECESSED GROOVING ITEM
EPOXY PAVEMENT MARKING - LINE 6"	GROOVING FOR RECESSED PAVEMENT MARKING 7"
EPOXY PAVEMENT MARKING - LINE 12"	GROOVING FOR RECESSED PAVEMENT MARKING 13"
EPOXY PAVEMENT MARKING - LINE 24"	GROOVING FOR RECESSED PAVEMENT MARKING 25"

NOTES: 1) EPOXY PAVEMENT MARKINGS SHALL BE INSTALLED IN GROOVED RECESSED CHANNELS CONSTRUCTED 0.040 INCHES BELOW THE SURFACE AND 1 INCH WIDER THAN THE PAVEMENT MARKING LINE. CONSTRUCTION OF THE RECESSED CHANNELS SHALL BE PAID FOR SEPARATELY PER FOOT AS GROOVING FOR RECESSED PAVEMENT MARKING OF THE WIDTH SPECIFIED AND PER SQUARE FOOT FOR GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS.

2) SEE TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKING FOR GUIDANCE



FILE NAME =	USER NAME = USER	DESIGNED - JKM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SPRING ROAD SPRING ROAD PAVEMENT MARKING PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
g:\ch\94\0082\road\sheet\PM-5R2-Spring	B2.SHT	DRAWN - JKM	REVISED -			2672	11-00176-00-RS	DUPAGE	17	8	
	PLOT SCALE = 50.000 1/2 IN.	CHECKED - DWB	REVISED -			CONTRACT NO. 63588					
	PLOT DATE = 4/6/2011	DATE - 04/06/2011	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
						SCALE:	SHEET NO. 2 OF 3 SHEETS		STA. 124+00.00 TO STA. 148+00.00		

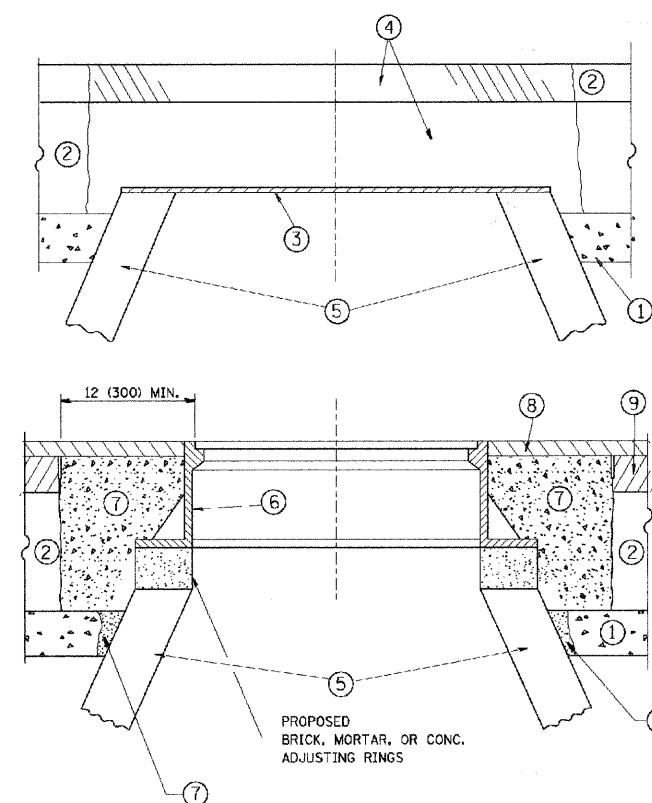


PAVEMENT MARKING ITEM	CORRESPONDING RECESSED GROOVING ITEM
EPOXY PAVEMENT MARKING - LINE 6"	GROOVING FOR RECESSED PAVEMENT MARKING 7"
EPOXY PAVEMENT MARKING - LINE 12"	GROOVING FOR RECESSED PAVEMENT MARKING 13"
EPOXY PAVEMENT MARKING - LINE 24"	GROOVING FOR RECESSED PAVEMENT MARKING 25"

NOTES: 1) EPOXY PAVEMENT MARKINGS SHALL BE INSTALLED IN GROOVED RECESSED CHANNELS CONSTRUCTED 0.040 INCHES BELOW THE SURFACE AND 1 INCH WIDER THAN THE PAVEMENT MARKING LINE. CONSTRUCTION OF THE RECESSED CHANNELS SHALL BE PAID FOR SEPARATELY PER FOOT AS GROOVING FOR RECESSED PAVEMENT MARKING OF THE WIDTH SPECIFIED AND PER SQUARE FOOT FOR GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS.

2) SEE TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKING FOR GUIDANCE

FILE NAME =	USER NAME = USER	DESIGNED - JKM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SPRING ROAD SPRING ROAD PAVEMENT MARKING PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT SCALE = 50.000' / IN.		CHECKED - DWB	REVISED -			CONTRACT NO. 63588					
PLOT DATE = 4/6/2011		DATE - 04/06/2011	REVISED -			SCALE: SHEET NO. 3 OF 3 SHEETS STA. 148+00.00 TO STA. 151+52.00		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

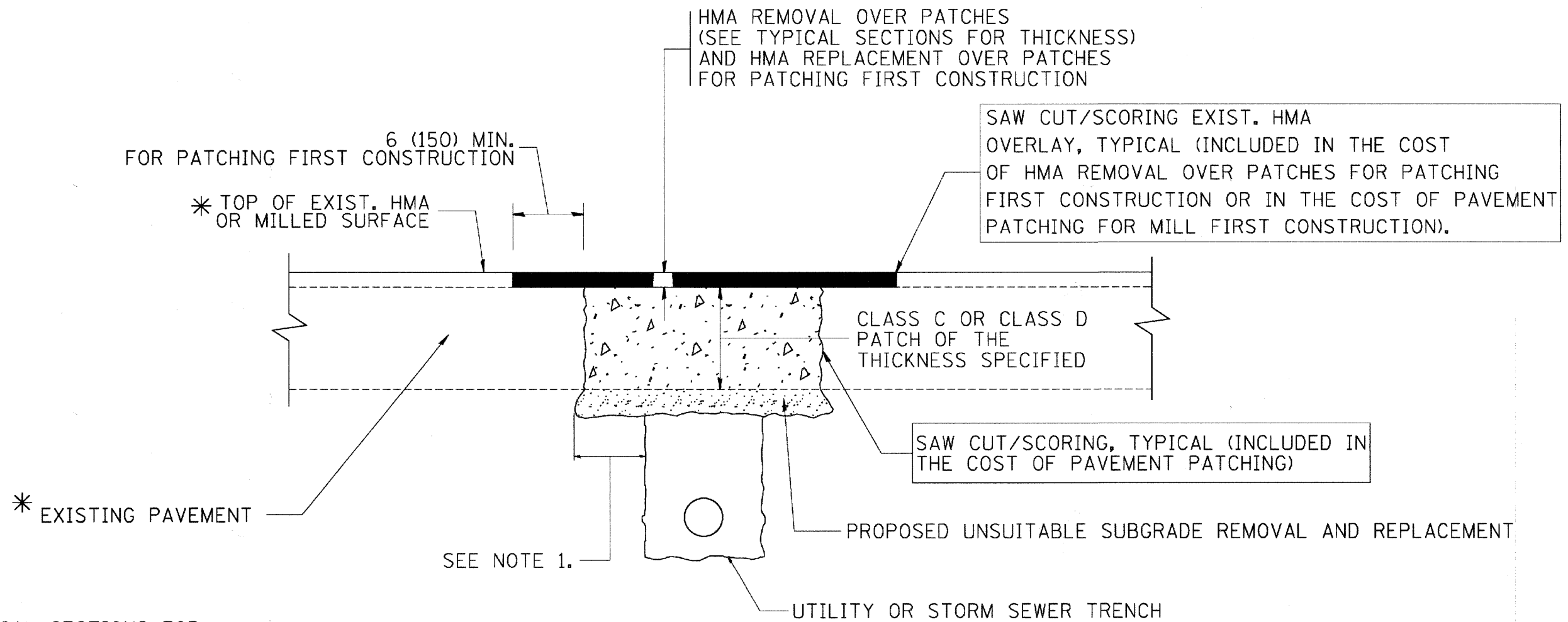
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = lsgso	DESIGNED - R. SHAH	REVISED - A. ARBAS 03-21-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	F.A. RTE. 2672	SECTION 11-00176-00-RS	COUNTY	TOTAL SHEETS 17	SHEET NO. 10	
PLOT SCALE - 40,0000 1/2 IN.		DRAWN	REVISED - R. WIEDEMAN 05-14-04			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	CONTRACT NO. 63588	
PLOT DATE - 3/19/2011		CHECKED -	REVISED - R. BORO 01-01-07					FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT	
		DATE - 10-25-94	REVISED - R. BORO 03-09-11								



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

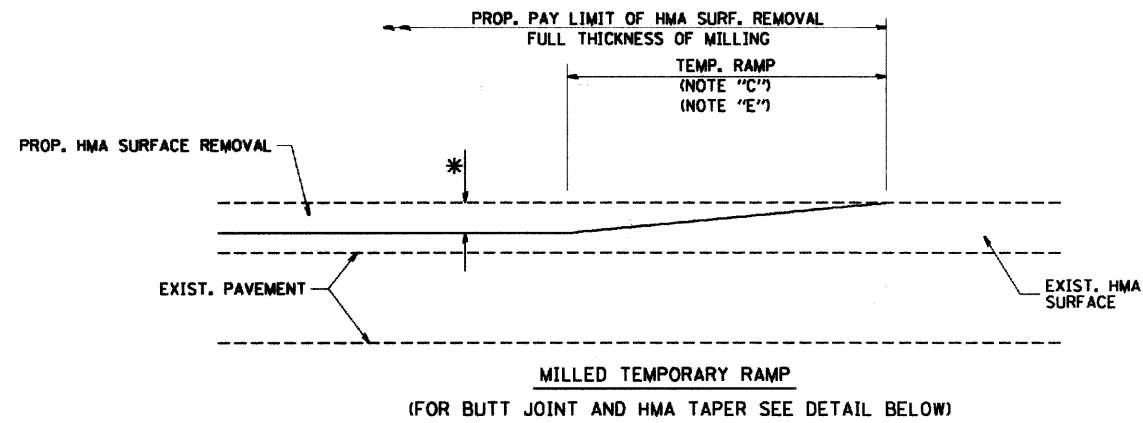
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

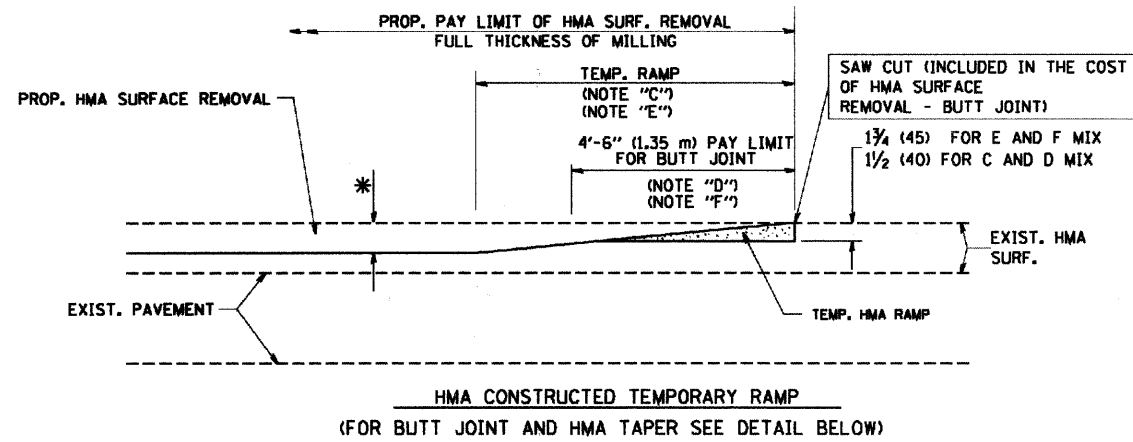
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

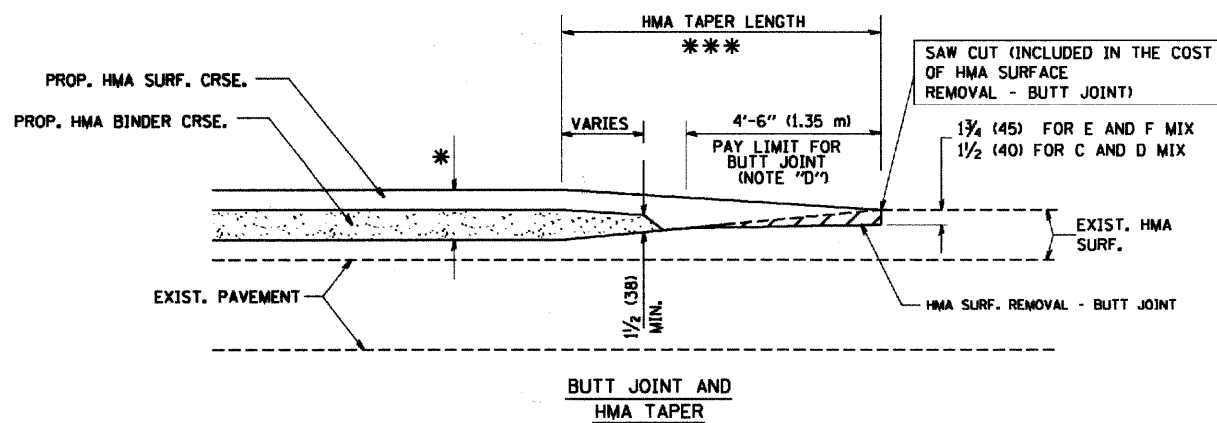
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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	BD400-04 (BD-22)		CONTRACT NO. 63588		
	PLOT DATE = 10/27/2008	DATE = 10-25-94	REVISED - R. BORO 09-04-07									
					REVISED - K. ENG 10-27-08	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						



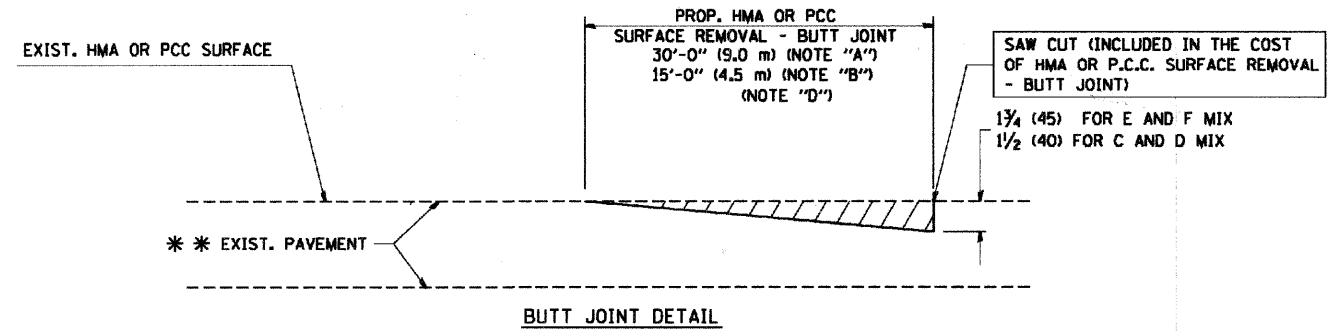
OPTION 1



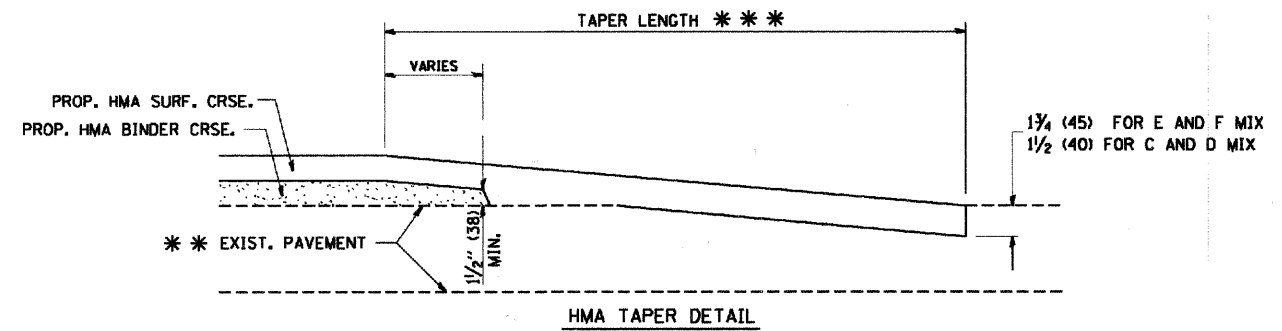
**OPTION 2
TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

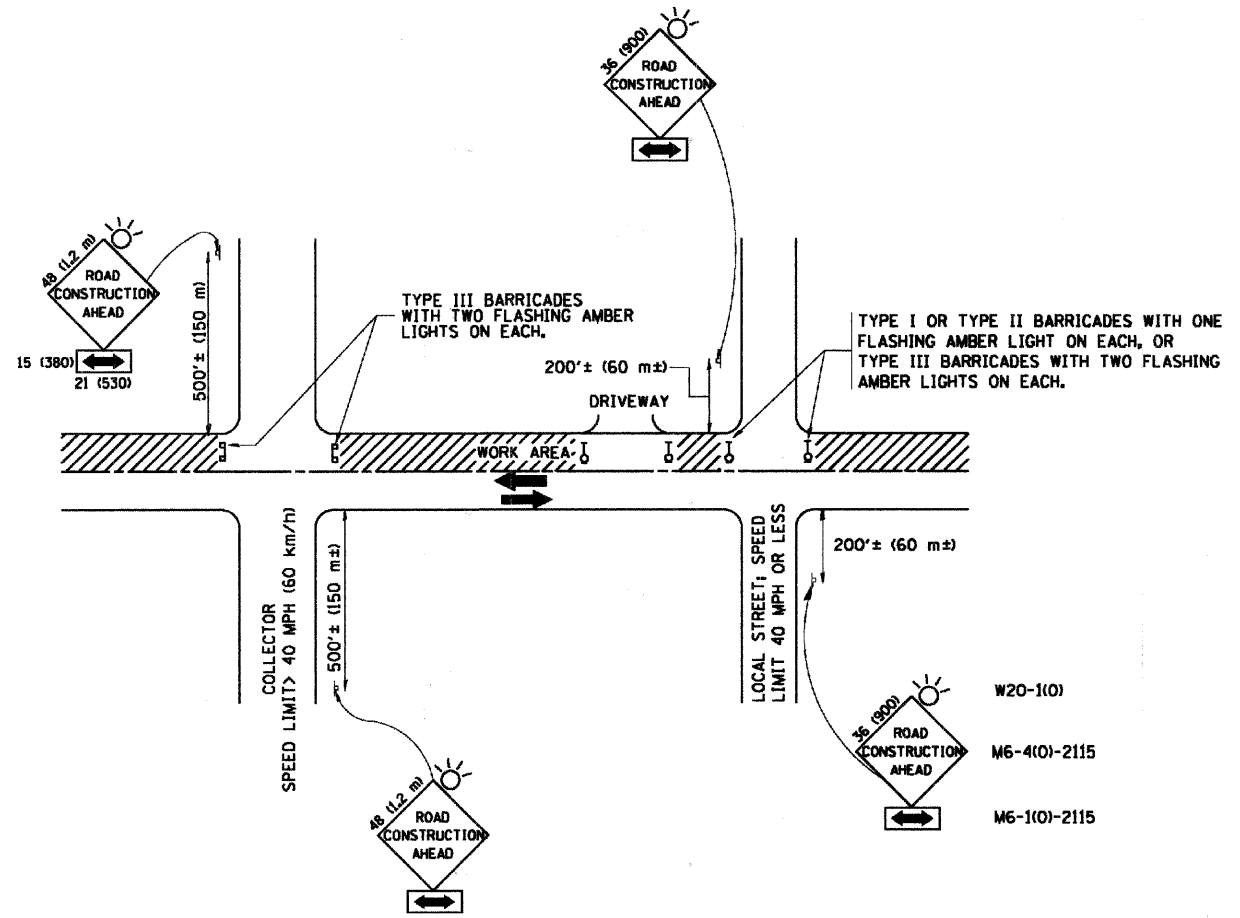
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PLOT SCALE = 58.0000' / IN	
PLOT DATE = 1/4/2008	

DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
DRAWN -	REVISED - A. ABBAS 03-21-97
CHECKED -	REVISED - M. GOMEZ 04-06-01
DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2672	11-00176-00-RS	DUPAGE	17	12
BD400-05 BD32			CONTRACT NO. 63588	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



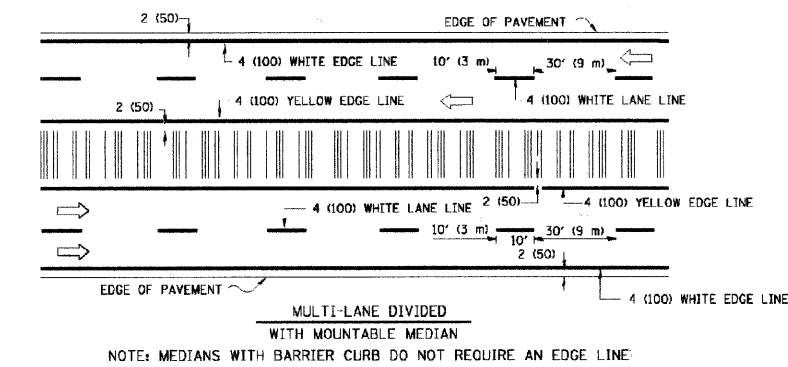
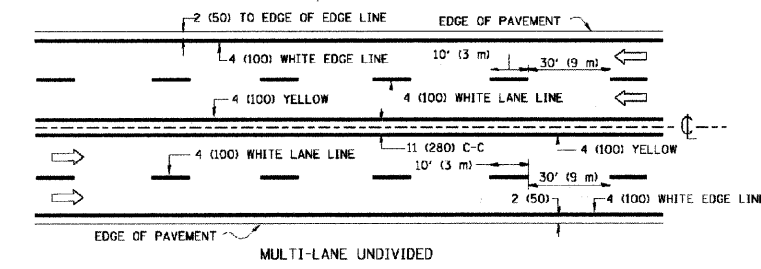
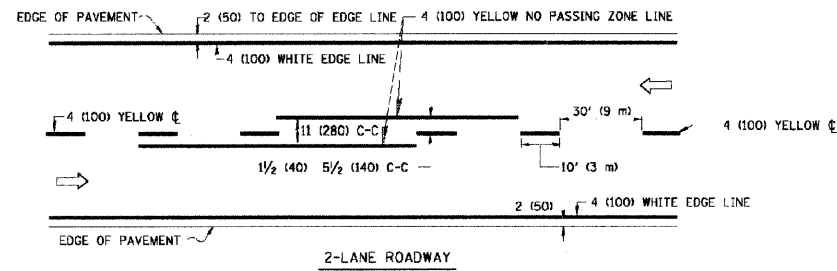
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

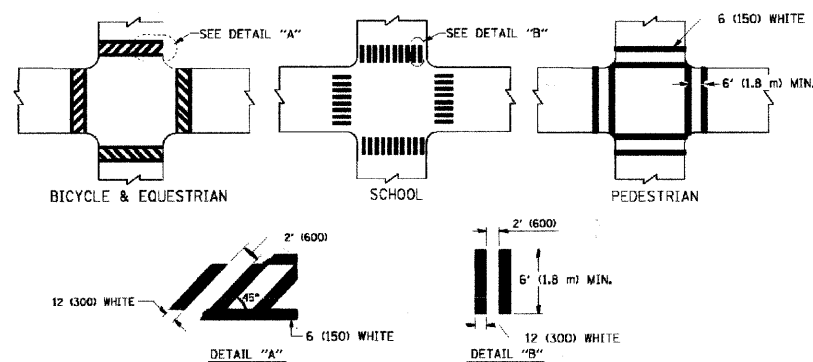
- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

All dimensions are in millimeters (inches) unless otherwise shown.

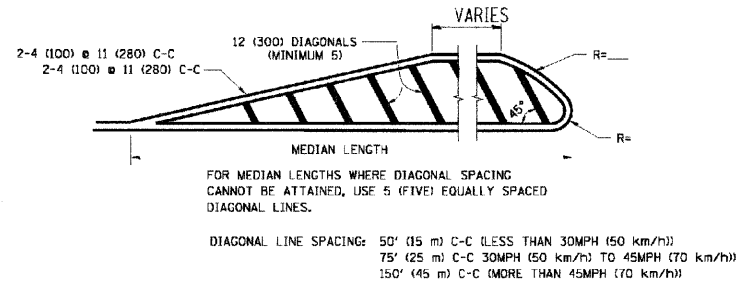
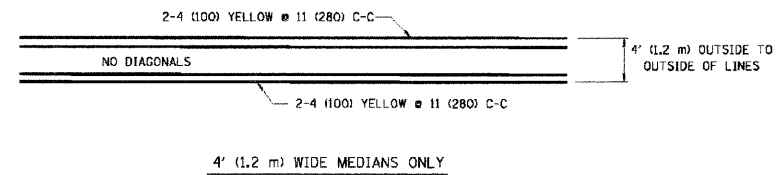
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	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - A. HOUSEH 03-06-96		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-10				
	PLOT DATE = 1/4/2000	DATE = 06-89	REVISED - A. HOUSEH 10-15-96					CONTRACT NO. 63588				
			REVISED - T. RAMMACHER 01-06-00					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



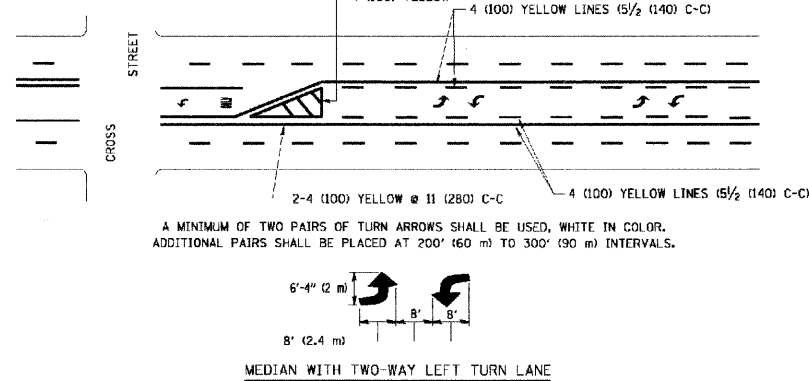
TYPICAL LANE AND EDGE LINE MARKING



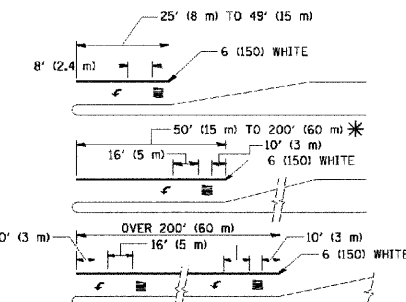
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING

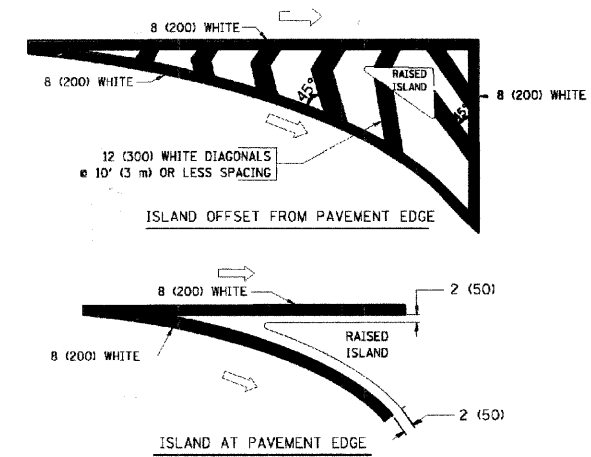


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.5 SQ. FT. (1.5 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

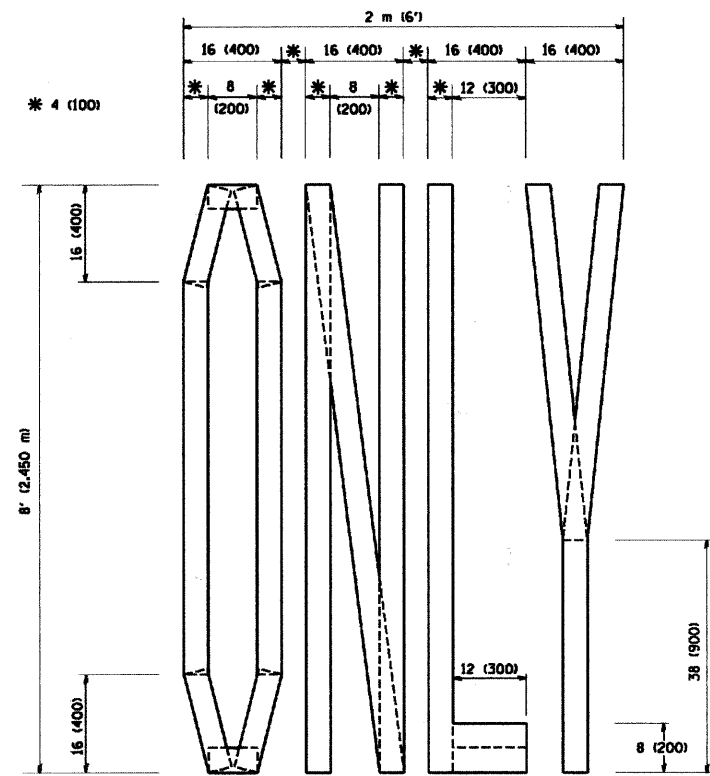


TYPICAL ISLAND MARKING

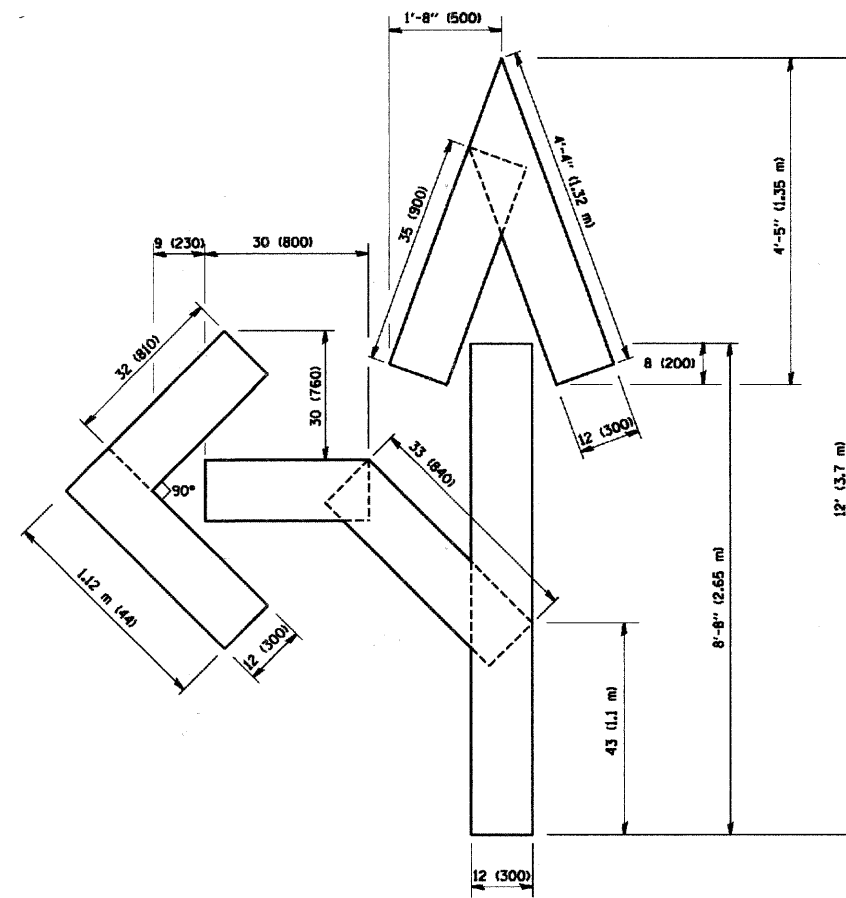
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" (3 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "X"-3.6 SQ. FT. (0.33 m ²) EACH "X"-54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

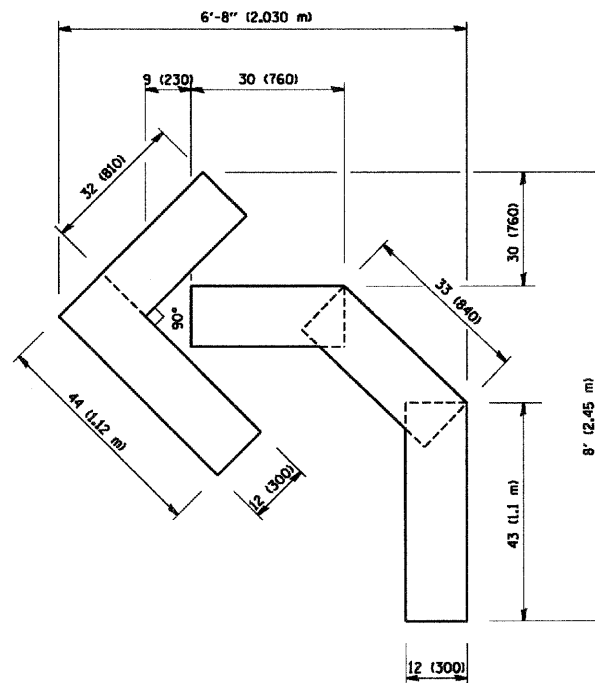
All dimensions are in inches (millimeters) unless otherwise shown.



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

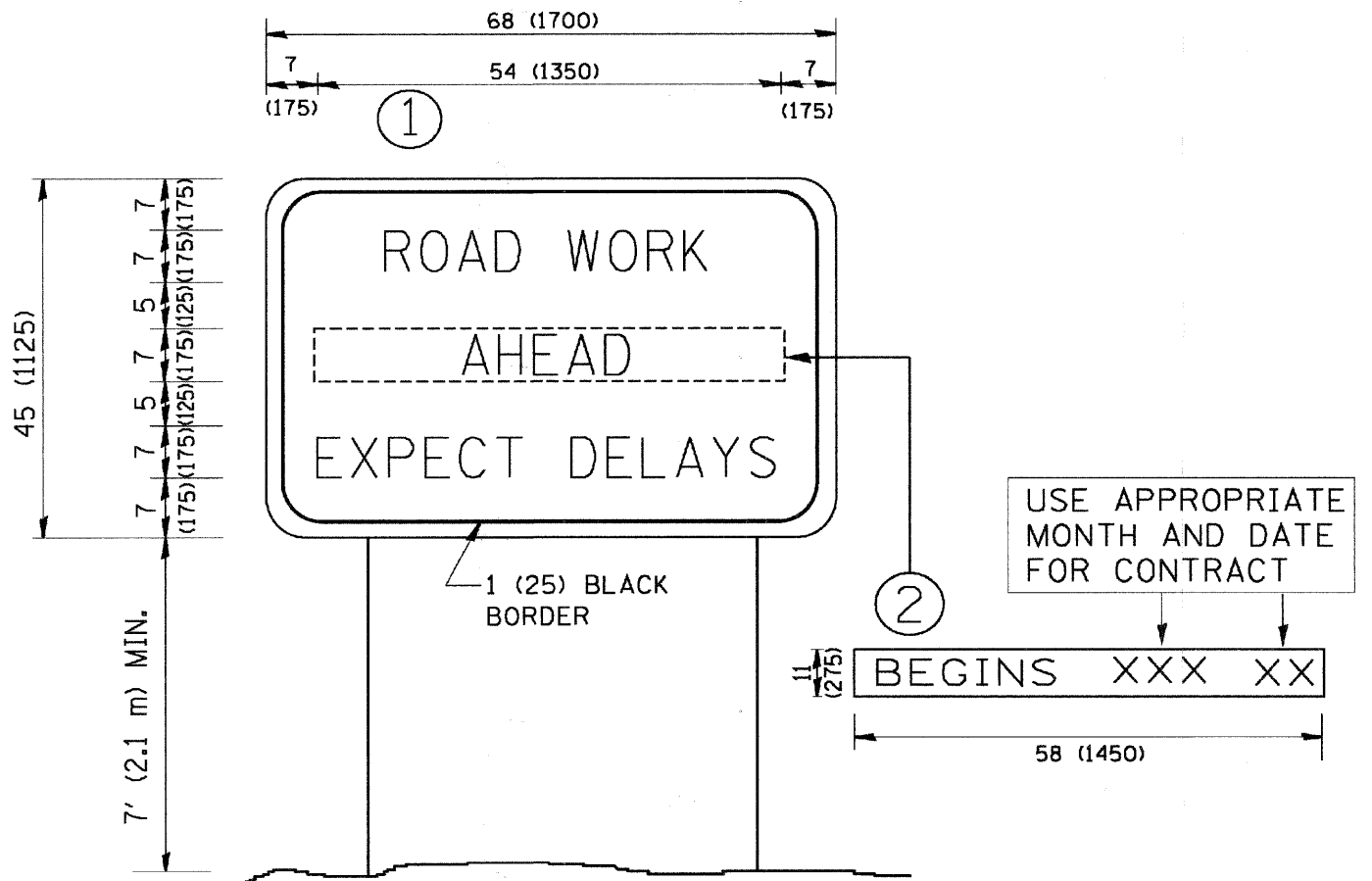
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		DRAWN -	REVISED - T. RAMMACHER 11-04-97
	PLOT SCALE = 58.0000' / IN.	CHECKED -	REVISED - T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2000	DATE = 09-18-94	REVISED - E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2672	11-00176-00-RS	DUPAGE	17	15
TC-16		CONTRACT NO. 63588		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

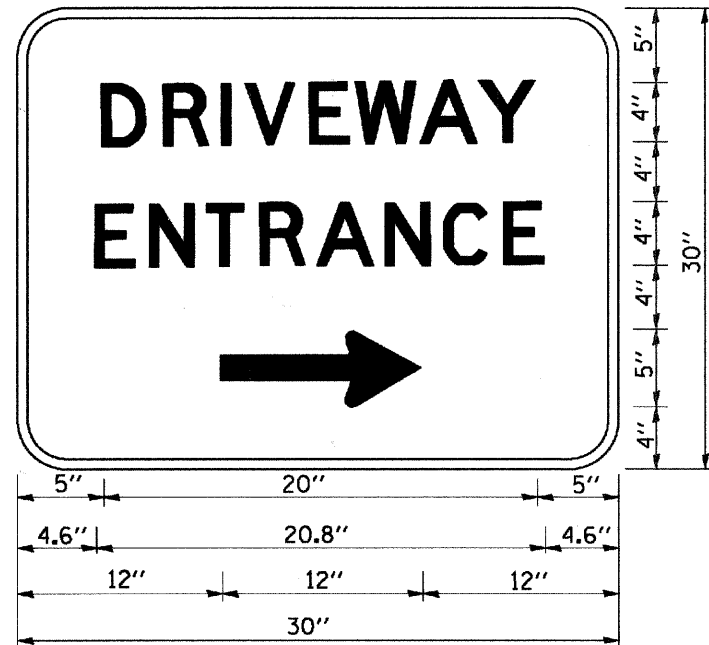
FILE NAME = M:\disto\22x34\to22.dgn	USER NAME = geglierobt	DESIGNED - DRAWN -	REVISED - REVISED -
		DESIGNED - CHECKED -	REVISED - REVISED -
		DESIGNED - DATE -	REVISED - REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 2672	SECTION 11-00176-00-RS	COUNTY DUPAGE	TOTAL SHEETS 17	SHEET NO. 16
TC-22			CONTRACT NO. 63588	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =
 W:\dists\td\22x34\to26.dgn

USER NAME = goglienobt

DESIGNED -
 DRAWN -

REVISED - C. JUCIUS 02-15-07

PLOT SCALE = 60.000' / IN.

CHECKED -
 DATE -

REVISED -
 REVISED -

PLOT DATE = 1/4/2008

DATE -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2672	11-00176-00-RS	DUPAGE	17	17
TC-26				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63588	