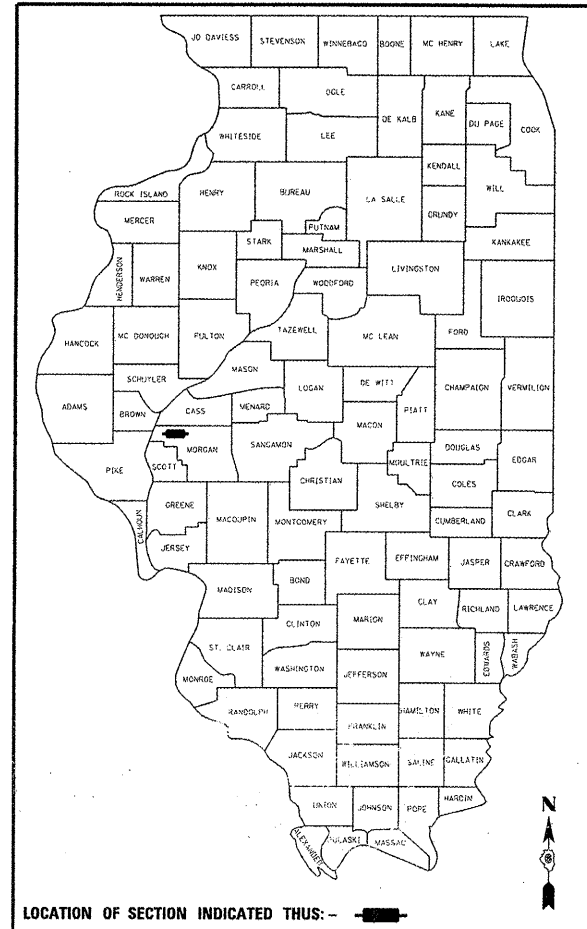


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	1

CONTRACT NO. 72667

✶793+5=798

D-96-538-02



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 310 (US 67 /IL 104)
SECTION 69-3(3HB)
PROJECT NHF-HPP-0310(145)
PAVING AND STRUCTURE PLANS
MORGAN COUNTY
C-96-037-08

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR LIST OF STANDARDS, SEE SHEET NO. 2

DESIGN DESIGNATION:
FAP 310
ARTERIAL
ADT=15,200 (2020)
% SU=3
% MU=13

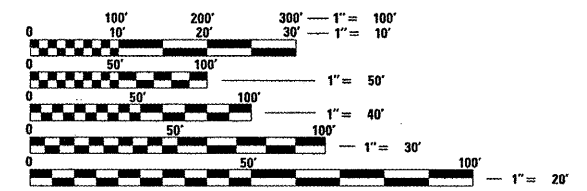
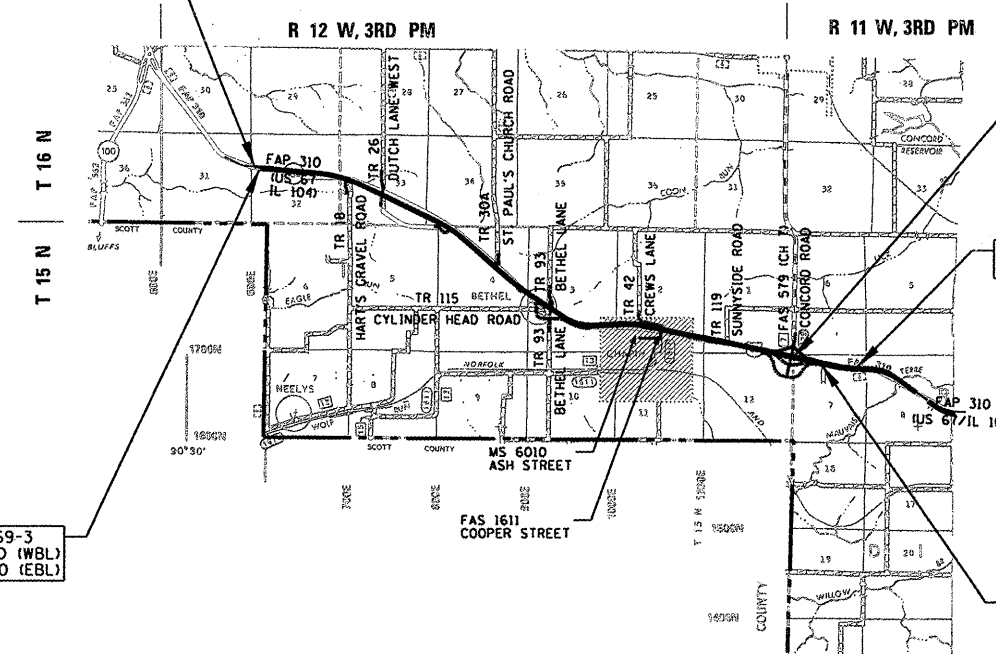
END IMPROVEMENT
STA 1145+73.37, 94.45' RT FAP 310

END SECTION 69-3
STA 1133+50.00 (WBL)
STA 1140+00.00 (EBL)

PROPOSED STRUCTURE CARRYING
CONCORD ROAD OVER FAP 310
STA 807+81.68
S.N. 069-0513
2 SPAN, P.P.C.I. BEAM (COMPOSITE)
INTEGRAL ABUTMENTS
202'-0" BK. TO BK. ABUTMENT

BEGIN IMPROVEMENT
STA 762+05.00 FAP 310

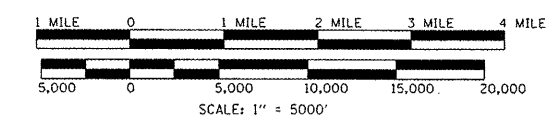
BEGIN SECTION 69-3
STA 800+00.00 (WBL)
STA 800+00.00 (EBL)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

CONTRACT NO. 72667



LOCATION MAP

LENGTH OF SECTION 69-3 = 34,000.00' = 6.439 MILES
LENGTH OF IMPROVEMENT = 38,368.37' = 7.267 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED *March 30 20 11*
Roger L. Orndall
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 13 20 11
Scott E. Stitt, P.E.
Acting ENGINEER OF DESIGN AND ENVIRONMENT

May 13 20 11
Charlotte M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

3/24/2011

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PROJECT ENGINEER: JOHN MCCANNICARO (217) 782-6990
SQUAD LEADER: VINCE MADONIA (217) 785-9046

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3/24/2011

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STANDARDS

Table with columns: STANDARD NO., DESCRIPTION. Lists various engineering standards such as STANDARD SYMBOLS, EARTH MEDIAN DITCH CHECK, TEMPORARY EROSION CONTROL SYSTEMS, etc.

INDEX OF SHEETS

Table with columns: SHEET NO., DESCRIPTION. Lists sheet numbers and their corresponding descriptions, such as COVER SHEET, INDEX AND LIST OF STANDARDS, GENERAL NOTES AND COMMITMENTS, etc.

Table with columns: F.A.P. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO. Values: 310, 69-3(3HB), MORGAN, 793, 2.

Vertical stamp area containing text: DISTRICT SIX, EXAMINED MARCH 21, OPERATIONS ENGINEER, EXAMINED MARCH 29, PROGRAM DEVELOPMENT ENGINEER, EXAMINED MARCH 11, PROJECT IMPLEMENTATION ENGINEER. Includes signatures.

Table with columns: REVISIONS, NAME, DATE. A grid for tracking revisions.

ILLINOIS DEPARTMENT OF TRANSPORTATION
HIGHWAY STANDARDS &
INDEX OF SHEETS
AND STANDARDS
FAP 310 (US 67/IL 104)

DATE 9/06
DRAWN BY EBB
CHECKED BY

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 Mo--25-2011 03:51:31PM

GENERAL NOTES

1. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER, AUTHORIZED AGENT, OR LAND SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. WHERE SECTION OR SUB-SECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR RE-ESTABLISH ANY SECTION OR SUB-SECTION MONUMENTS DESTROYED BY HIS OPERATIONS.
2. ALL UNSURFACED AREAS WITHIN THE R.O.W. AND EASEMENTS SHALL BE SEEDED, FERTILIZED, AND MULCHED AS SHOWN IN THE PLANS, SPECIAL PROVISIONS, AND AS DIRECTED BY THE ENGINEER. SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET OR IN AN UNTILLABLE CONDITION.
3. IN ACCORDANCE WITH STATE OF ILLINOIS P.A. 86-0674, THE CONTRACTOR IS TO NOTIFY ALL UTILITY COMPANIES NOT MORE THAN 14 DAYS NOR LESS THAN 48 HOURS (EXCLUSIVE OF SATURDAYS, SUNDAYS, AND HOLIDAYS) IN ADVANCE OF THE START OF EXCAVATION OR DEMOLITION.
 J.U.L.I.E. TELEPHONE NUMBER
 1-800-892-0123
 KNOWN UTILITIES LOCATED WITHIN THE LIMITS OF THIS IMPROVEMENT ARE:

FIBER OPTIC VERIZON NORTH, INC. 330 WEST BEECHER AVE JACKSONVILLE, IL 62650 217-243-02594290 MR. BRENT HINTHORNE	CABLE TV MEDIA COMM. 4290 BLUESTEM RD. P.O. BOX 288 CHARLESTON, IL 61920 217-348-5533 EXT. 3 MR. JOHN WILSON	GAS TRANSMISSION AMERENIP 370 S. MAIN ST., D-50 DECATUR, IL 62523 217-424-7004 MR. CAREY PHELPS
WATER/SEWER VILLAGE OF CHAPIN 510 EVERETT, BOX 213 CHAPIN, IL 62628 217-472-3111 MR. TODD BERRY	WATER CONCORD WATER SYSTEMS 970 WEST LAFFAYETTE AVE. JACKSONVILLE, IL 62650 217-245-4146 MR. ED DEGROOT	ELECTRIC AMEREN CIPS 700 JERSEY ST. P.O. BOX 1089 QUINCY, IL 217-221-0817 MR. SCOTT HEIKES
GAS AMEREN 700 JERSEY ST. P.O. BOX 1089 QUINCY, IL 62306 217-221-0869 MR. ANTHONY SANNEN	FIBER OPTIC AT&T 529 SOUTH 7TH ST., FLOOR 3B SPRINGFIELD, IL 62721 217-789-8367 MR. MARK MILLER	
4. THE LOCATION OF BURIED AND ABOVE GROUND UTILITIES SHOWN ARE APPROXIMATE, AND ARE SHOWN FOR CONTRACTOR INFORMATIONAL USE ONLY, AND ARE NOT TO BE REFERENCED FOR CONSTRUCTION PURPOSES. THE IMPLIED PRESENCE OR ABSENCE OF UTILITIES IS NOT TO BE CONSTRUED BY THE OWNER, ENGINEER, CONTRACTOR, OR SUBCONTRACTORS TO BE AN ACCURATE AND COMPLETE REPRESENTATION OF UTILITIES THAT MAY OR MAY NOT EXIST ON THE CONSTRUCTION SITE. BURIED AND ABOVE GROUND UTILITY LOCATIONS, IDENTIFICATION, AND MARKING ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. REROUTING, DISCONNECTION, PROTECTION, ETC. OF ANY UTILITIES MUST BE COORDINATED BETWEEN THE CONTRACTOR, UTILITY COMPANY, AND OWNER. SITE SAFETY, INCLUDING THE AVOIDANCE OF HAZARDS ASSOCIATED WITH BURIED AND ABOVE GROUND UTILITIES, REMAINS THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
5. ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUB-NUMBER IN THE INDEX OF SHEETS OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.
6. THE EXISTING ROAD SIGNS THAT INTERFERE WITH CONSTRUCTION WILL BE REMOVED AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO COMPENSATION WILL BE ALLOWED.
7. FERTILIZER NUTRIENTS SHALL BE APPLIED TO BOTH THE SEEDED AREAS AND THE AREAS COVERED WITH EROSION CONTROL BLANKET.
8. DO NOT INCLUDE MULCH OR EMULSIFIED ASPHALT ON EROSION CONTROL BLANKET AREAS.
9. BEFORE ORDERING PIPE CULVERTS OR PIPE DRAINS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR THE EXACT LENGTHS.
10. PROTECTIVE COAT SHALL BE APPLIED TO THE SURFACE OF NEW CONCRETE ACCORDING TO SECTION 503 OF THE STANDARD SPECIFICATIONS.
11. ALL TREES, BRUSH AND SHRUBS WITHIN THE CONSTRUCTION LIMITS WILL BE REMOVED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS. TREES ALONG THE EDGE OF RIGHT OF WAY, OUTSIDE THE CONSTRUCTION LIMITS SHALL BE SAVED IF, IN THE OPINION OF THE ENGINEER, THEY DO NOT INTERFERE WITH CONSTRUCTION OPERATIONS. THE CONTRACTOR WILL PROTECT ALL REMAINING TREES FROM DAMAGE DUE TO HIS OPERATIONS.
12. ALL EXISTING FENCE WITHIN THE PROPOSED RIGHT OF WAY SHALL BE REMOVED. THE COST OF THE FENCE REMOVAL WILL BE INCLUDED IN THE COST OF EARTH EXCAVATION.
13. EXISTING RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE REMOVED PRIOR TO RESURFACING.

14. ALL PIPE TEES WHICH ARE TO BE PLUGGED, SHALL RECEIVE 6" THICK PLUG OF SEAL-IN-PLACE CONCRETE AS DIRECTED BY THE ENGINEER TO SATISFACTORY SEAL THE END OF THE PIPE TEE. ALL COST INCLUDED IN BID PRICE OF THE PIPE TEE.
15. A GEOTECHNICAL REPORT HAS BEEN COMPLETED FOR THIS PROJECT AND IS AVAILABLE FOR REVIEW BY THE CONTRACTORS PRIOR TO BIDDING AT IDOT, DISTRICT SIX ANNEX BUILDING 2713 STEVENSON DRIVE, SPRINGFIELD, IL (MATERIALS SECTION).
16. UNLESS OTHERWISE NOTED, THE OFFSETS SHOWN IN THE PLANS FOR THE END OF PIPE CULVERTS ARE TO THE END OF THE LAST SECTION OF PIPE NOT TO THE END OF THE END SECTION.
17. THE REMOVAL OF OIL AND CHIP PAVEMENT SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.
18. THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT.

LOCATION(S):	CROSSOVER "A" & TRANSITIONS A & B	
MIXTURE USE(S):	HMA BASE COURSE	HMA SURFACE COURSE
AC/PG:	PG 64-22	PG 64-22
DESIGN AIR VOIDS:	4.0% @ N DESIGN=70	4.0% @ N DESIGN=70
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL 19.0	IL 9.5 or IL 12.5
FRICITION AGGREGATE	N/A	MIX "D"

LOCATION(S):	CROSSOVER "B" & TRANSITIONS C & D			
MIXTURE USE(S):	HMA BASE COURSE (LOWER LIFTS)	HMA BASE COURSE (TOP LIFT)	LEVELING BINDER	HMA SURFACE COURSE
AC/PG:	PG 64-22	SBS PG 64-28	SBS PG 64-28	SBS PG 64-28
DESIGN AIR VOIDS:	4.0% @ N DESIGN=70	4.0% @ N DESIGN=70	4.0% @ N DESIGN=70	4.0% @ N DESIGN=70
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL 19.0	IL 19.0	IL 9.5	IL 9.5 or IL 12.5
FRICITION AGGREGATE	N/A	NA	N/A	MIX "D"

LOCATION(S):	FAS 579 / CH 7 (CONCORD ROAD)			
MIXTURE USE(S):	HMA BASE COURSE	HMA BINDER COURSE	LEVELING BINDER	HMA SURFACE COURSE
AC/PG:	PG 64-22	SBS PG 64-28	SBS PG 64-28	SBS PG 64-28
DESIGN AIR VOIDS:	4.0% @ N DESIGN=50	4.0% @ N DESIGN=50	4.0% @ N DESIGN=50	4.0% @ N DESIGN=50
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL 19.0	IL 19.0	IL 9.5	IL 9.5 or IL 12.5
FRICITION AGGREGATE	N/A	NA	N/A	MIX "C"

LOCATION(S):	US 67 FRONTAGE ROAD (CONCORD) - STAGE 1		
MIXTURE USE(S):	HMA BASE COURSE	HMA BINDER COURSE	LEVELING BINDER
AC/PG:	PG 64-22	PG 64-22	PG 64-22
DESIGN AIR VOIDS:	4.0% @ N DESIGN=70	4.0% @ N DESIGN=70	4.0% @ N DESIGN=70
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL 19.0	IL 19.0	IL 9.5
FRICITION AGGREGATE	N/A	NA	N/A

LOCATION(S):	ALL LOCATIONS NOT SPECIFIED IN OTHER TABLES			
MIXTURE USE(S):	HMA BASE COURSE & WIDENING	HMA BINDER COURSE	LEVELING BINDER	HMA SURFACE COURSE
AC/PG:	PG 64-22	PG 64-22	PG 64-22	PG 64-22
DESIGN AIR VOIDS:	4.0% @ N DESIGN=50	4.0% @ N DESIGN=50	4.0% @ N DESIGN=50	4.0% @ N DESIGN=50
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL 19.0	IL 19.0	IL 9.5	IL 9.5 or IL 12.5
FRICITION AGGREGATE	N/A	NA	N/A	MIX "C"

LOCATION(S):	ALL		ALL	
MIXTURE USE(S):	HMA SHOULDER (LOWER LIFTS)	HMA SHOULDER (TOP LIFT)	PATCHING	INCIDENTAL SURFACING
AC/PG:	PG 58-22	PG 64-22	PG 64-22	PG 64-22
DESIGN AIR VOIDS:	2.0% @ N DESIGN=30	4.0% @ N DESIGN=50	NOTE 1	4.0% @ N DESIGN=50
MIXTURE COMPOSITION: (GRADATION MIXTURE)	BAM	IL 9.5 or IL 12.5	IL 19.0	IL 9.5 or IL 12.5
FRICITION AGGREGATE	N/A	MIX "C"	N/A	MIX "C"

NOTE 1: THE N DESIGN LEVEL SHALL MATCH THAT OF THE SURFACE SPECIFIED AT THE GIVEN LOCATION.

RATES OF APPLICATION TABLE

AGGREGATE (SURFACE, BASE, SUBBASE, OR BACKFILL)	2.05 TON / CU YD
SUBBASE GRANULAR MATERIAL, TYPE C	2.05 TON / CU YD
STONE DUMPED RIPRAP	1.50 TON / CU YD
ROCKFILL	1.89 TON / CU YD
HOT-MIX ASPHALT:	
BITUMINOUS MATERIALS (PRIME COAT)	0.00038 TON / SQ YD (on pavement)
BITUMINOUS MATERIALS (PRIME COAT)	0.001425 TON / SQ YD (on aggregate)
AGGREGATE PRIME COAT	0.002 TON / SQ YD
SURFACE / BINDER (112 lbs)	0.056 TON / SQ YD • IN
SEEDING AREAS:	
NITROGEN FERTILIZER NUTRIENT	90 LBS / ACRE
PHOSPHOROUS FERTILIZER NUTRIENT	90 LBS / ACRE
POTASSIUM FERTILIZER NUTRIENT	90 LBS / ACRE
AGRICULTURAL GROUND LIMESTONE	2 TON / ACRE
MULCH	2 TON / ACRE
LIME FOR MODIFIED SOILS, 12"	0.02 TON / SQ YD

CONTRACT NO. 72667				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	3
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

COMMITMENTS

1. INSTALL 22' WIDE AGG FE AT STA 43+00 ON US 67 FRONTAGE ROAD.
2. INSTALL 22' WIDE AGG FE AT STA 846+00 ON US 67 THAT WILL INCLUDE A SECURITY GATE AND KEY. THE PROPERTY OWNER IS RESPONSIBLE FOR SECURING GATE WHEN NOT IN USE.
3. INSTALL 22' WIDE AGG FE AT STA 1048+00 ON US 67.
4. INSTALL 22' WIDE HMA PE AT STA 1015+40 ON US 67.
5. INSTALL 22' WIDE AGG FE AT STA 1119+00 ON US 67.
6. INSTALL 24' WIDE AGG PE AT STA 77+48 ON BETHEL LANE. THIS IS TO PROVIDE A 30' TOP FOR THEIR ENTRANCE (24' + TWO 3' SHOULDERS).
7. CONSTRUCT SMALL BERM FROM STA 1064+00 TO STA 1068+00 RT.
8. INSTALL FE AT STA 99+75 RT ON RELOCATED CYLINDER HEAD ROAD.
9. INSTALL FE AT STA 94+50 LT ON RELOCATED CYLINDER HEAD ROAD.
10. CONCRETE DRIVEWAY AT STA 2+00 ON ASH STREET IS OUTSIDE OF CONSTRUCTION LIMITS - WILL NOT BE AFFECTED. ACCESS TO THIS RESIDENTIAL PARCEL TO BE ALLOWED AT ALL TIMES IN ACCORDANCE WITH ARTICLE 107.09 OF THE STANDARD SPECIFICATIONS.
11. DITCH TO BE GRADED ON MANKER PARCEL (6084164) IN BETHEL.
12. THE PROPERTY OWNERS REQUESTED A FE NOT BE CONSTRUCTED AT STA 1136+48 RT. A BREAK IN ACCESS CONTROL WILL BE PROVIDED IF THEY CHOOSE TO CONSTRUCT THIS FE IN THE FUTURE.
13. PARCEL 6084113 AND 6084138 (BUMGARDNER) REQUESTED THAT CONCRETE ROW MARKERS NOT BE USED ON THEIR PROPERTY. IT WAS AGREED THE FLUSH MOUNT METHOD WOULD BE USED FOR THESE PARCELS.
14. THE FIELD / RESIDENT ENGINEER SHALL CONTACT DISTRICT 6 STUDIES AND PLANS AT 217 / 782-6990 CONCERNING ANY MAJOR PLAN CHANGES TO MAKE SURE NO PREVIOUS COMMITMENTS (NOT LISTED) WERE MADE AFFECTING THE DESIGN, AND TO ALLOW IMPROVEMENTS IN THE DESIGN FOR FUTURE PROJECTS.
15. SEEDING SHALL BE COMPLETED AS DESIGNATED IN THE STORM WATER POLLUTION PREVENTION PLAN. ALL AREAS OF POTENTIAL FOR EROSION SHALL BE SEEDED BY OCTOBER 1, AND SHALL NOT BE REOPENED UNTIL AFTER THE WINTER SHUT DOWN PERIOD (SEE SWPPP).
16. ROADS SHALL BE OPENED AT ALL TIMES TO PROVIDE FARMERS ACCESS TO NECESSARY FARM FIELDS.
17. INDIVIDUAL 404 PERMIT.
18. DUST CONTROL WILL BE HANDLED PROPERLY BY IDOT'S CONTRACTORS. WATER TRUCKS SHALL BE PROVIDED TO KEEP DUST FROM THE PREGRADE FROM BLOWING NEAR HOMES.
19. STORM WATER POLLUTION PREVENTION PLAN REQUIRED FOR NPDES PERMIT BY IEPA.
20. SWPPP / NPDES PERMIT.

LEGEND

- PLAN VIEWS:**
- AC-- PROPOSED ACCESS CONTROL AND ROW
 - AC--- PROPOSED ACCESS CONTROL ONLY
 - J
 - TBF
 - TBR
 - BR*X
 - SR*X
 - W
 - W
 - EX
 - W
 - W
 - CH

- CROSS SECTION VIEWS:**
- STAGE CONSTRUCTION LINE
 - S.D. SPECIAL DITCH ELEVATION

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION	
NAME	DATE	<p align="center">GENERAL NOTES AND LEGEND</p> <p align="center">FAP 310 (US 67/IL 104)</p> <p align="right">DRAWN BY EBB CHECKED BY</p>	
DATE 6/06		MORGAN COUNTY	

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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	4
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

80% FEDERAL 20% STATE
CONSTRUCTION AND SAFETY TYPE CODE

CODE NO.	ITEM	SP. PROV.	UNIT	TOTAL QUANTITIES	CONSTRUCTION AND SAFETY TYPE CODE																
					0001 ROADWAY	0008 STRUCTURE															
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)		UNIT	299	299																
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)		UNIT	534	534																
20100500	TREE REMOVAL, ACRES		ACRE	18.9	18.9																
20200100	EARTH EXCAVATION	.	CU YD	550503	550503																
20200200	ROCK EXCAVATION		CU YD	100	100																
20200500	EARTH EXCAVATION (WIDENING)	.	CU YD	130	130																
20400800	FURNISHED EXCAVATION		CU YD	514850	514850																
20700220	POROUS GRANULAR EMBANKMENT		CU YD	40	40																
20800150	TRENCH BACKFILL		CU YD	4665	4665																
21101615	TOPSOIL FURNISH AND PLACE, 4"		SO YD	877140	877140																
21400100	GRADING AND SHAPING DITCHES	.	FOOT	500	500																
* 25000200	SEEDING, CLASS 2		ACRE	230	230																
* 25000350	SEEDING, CLASS 7		ACRE	40	40																
* 25000400	NITROGEN FERTILIZER NUTRIENT		POUND	20700	20700																
* 25000500	PHOSPHORUS FERTILIZER NUTRIENT		POUND	20700	20700																
* 25000600	POTASSIUM FERTILIZER NUTRIENT		POUND	20700	20700																
* 25000700	AGRICULTURAL GROUND LIMESTONE		TON	460	460																
* 25003200	INTERSEEDING, CLASS 2		ACRE	10	10																
* 25100115	MULCH, METHOD 2		ACRE	270	270																
* 25100635	HEAVY DUTY EROSION CONTROL BLANKET		SO YD	7197	7197																
28000200	EARTH EXCAVATION FOR EROSION CONTROL		CU YD	1000	1000																
28000250	TEMPORARY EROSION CONTROL SEEDING		POUND	75000	75000																
28000400	PERIMETER EROSION BARRIER		FOOT	7000	7000																
28000500	INLET AND PIPE PROTECTION		EACH	80	80																
28001000	AGGREGATE (EROSION CONTROL)		TON	1500	1500																
28100707	STONE DUMPED RIPRAP, CLASS A4	.	SO YD	19885	19885																
28100709	STONE DUMPED RIPRAP, CLASS A5	.	SO YD	426	426																

*SPECIALTY ITEM

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
FAP 310 (US 67/IL 104)
SHEET 1 OF 13
DATE 10/2010
DRAWN BY BGJ
CHECKED BY EBB

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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	5
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

SUMMARY OF QUANTITIES					80% FEDERAL 20% STATE CONSTRUCTION AND SAFETY TYPE CODE	
CODE NO.	ITEM	SP. PROV.	UNIT	TOTAL QUANTITIES	0001 ROADWAY	0008 STRUCTURE
28100711	STONE DUMPED RIPRAP, CLASS A6	•	SO YD	1187	1187	
28100825	STONE DUMPED RIPRAP, CLASS B3		TON	600	600	
28200200	FILTER FABRIC		SO YD	24854	24854	
31000600	PROCESSING LIME STABILIZED SOIL MIXTURE 12"		SO YD	398278	398278	
31001500	LIME		TON	7969	7969	
31100100	SUBBASE GRANULAR MATERIAL, TYPE A		TON	2200	2200	
31101900	SUBBASE GRANULAR MATERIAL, TYPE C		TON	24402	24402	
31200100	STABILIZED SUBBASE 4"		SO YD	261698	261698	
35100100	AGGREGATE BASE COURSE, TYPE A		TON	379	379	
35100700	AGGREGATE BASE COURSE, TYPE A 8"		SO YD	31771	31771	
35501316	HOT-MIX ASPHALT BASE COURSE, 8"		SO YD	12642	12642	
35501320	HOT-MIX ASPHALT BASE COURSE, 9"		SO YD	20270	20270	
35501332	HOT-MIX ASPHALT BASE COURSE, 12"		SO YD	6708	6708	
35600708	HOT-MIX ASPHALT BASE COURSE WIDENING, 8"		SO YD	65	65	
35600712	HOT-MIX ASPHALT BASE COURSE WIDENING, 9"		SO YD	460	460	
40200100	AGGREGATE SURFACE COURSE, TYPE A		TON	5956	5956	
40200700	AGGREGATE SURFACE COURSE, TYPE A 8"		SO YD	1870	1870	
40200800	AGGREGATE SURFACE COURSE, TYPE B		TON	2500	2500	
40201000	AGGREGATE FOR TEMPORARY ACCESS		TON	2000	2000	
40600200	BITUMINOUS MATERIALS (PRIME COAT)		TON	94.0	94.0	
40600300	AGGREGATE (PRIME COAT)		TON	153	153	
40600625	LEVELING BINDER (MACHINE METHOD), N50		TON	2251	2251	
40600635	LEVELING BINDER (MACHINE METHOD), N70		TON	344	344	
40600825	POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50		TON	55	55	
40600837	POLYMERIZED LEVELING BINDER (MACHINE METHOD), N70		TON	302	302	
40600895	CONSTRUCTING TEST STRIP		EACH	4	4	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT		SO YD	605	605	

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
FAP 310 (US 67/IL 104)
SHEET 2 OF 13

DATE 10/2010
DRAWN BY BGJ
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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	6
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

80% FEDERAL 20% STATE
CONSTRUCTION AND SAFETY TYPE CODE

CODE NO.	ITEM	SP. PROV.	UNIT	TOTAL QUANTITIES	CONSTRUCTION AND SAFETY TYPE CODE						
					0001 ROADWAY	0008 STRUCTURE					
40600990	TEMPORARY RAMP	.	SO YD	500	500						
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50		TON	5464	5464						
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70		TON	1433	1433						
40603230	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50		TON	196	196						
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50		TON	6993	6993						
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70		TON	619	619						
40603510	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50		TON	279	279						
40603540	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70		TON	844	844						
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING		TON	840	840						
42000501	PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)		SO YD	234743	234743						
42001200	PAVEMENT FABRIC		SO YD	10115	10115						
42001300	PROTECTIVE COAT		SO YD	351445	351445						
42100300	CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 10"		SO YD	1260	1260						
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH		SO YD	108	108						
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH		SO YD	381	381						
42400100	PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH		SO FT	538	538						
44000100	PAVEMENT REMOVAL	.	SO YD	102502	102502						
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"		SO YD	2385	2385						
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"		SO YD	651	651						
44000200	DRIVEWAY PAVEMENT REMOVAL		SO YD	825	825						
44000300	CURB REMOVAL		FOOT	1035	1035						
44000600	SIDEWALK REMOVAL		SO FT	737	737						
44004000	PAVED DITCH REMOVAL		FOOT	310	310						
44004250	PAVED SHOULDER REMOVAL		SO YD	1812	1812						
44200202	PAVEMENT PATCHING, TYPE II, 17 INCH		SO YD	43	43						
44200204	PAVEMENT PATCHING, TYPE III, 17 INCH		SO YD	21	21						
44200206	PAVEMENT PATCHING, TYPE IV, 17 INCH		SO YD	169	169						

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ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
FAP 310 (US 67/IL 104)
SHEET 3 OF 13

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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	7
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

80% FEDERAL 20% STATE
CONSTRUCTION AND SAFETY TYPE CODE

CODE NO.	ITEM	SP. PROV.	UNIT	TOTAL QUANTITIES	CONSTRUCTION AND SAFETY TYPE CODE																
					0001 ROADWAY	0008 STRUCTURE															
48101200	AGGREGATE SHOULDERS, TYPE B		TON	26325	26325																
48203029	HOT-MIX ASPHALT SHOULDERS, 8"		SQ YD	7730	7730																
48203100	HOT-MIX ASPHALT SHOULDERS		TON	294	294																
48300500	PORTLAND CEMENT CONCRETE SHOULDERS 10"		SQ YD	111158	111158																
50100300	REMOVAL OF EXISTING STRUCTURES NO. 1	.	EACH	1	1																
50100400	REMOVAL OF EXISTING STRUCTURES NO. 2	.	EACH	1	1																
50100500	REMOVAL OF EXISTING STRUCTURES NO. 3	.	EACH	1	1																
50100600	REMOVAL OF EXISTING STRUCTURES NO. 4	.	EACH	1	1																
50100700	REMOVAL OF EXISTING STRUCTURES NO. 5	.	EACH	1	1																
50100800	REMOVAL OF EXISTING STRUCTURES NO. 6	.	EACH	1	1																
50100900	REMOVAL OF EXISTING STRUCTURES NO. 7	.	EACH	1	1																
50101000	REMOVAL OF EXISTING STRUCTURES NO. 8	.	EACH	1	1																
50101100	REMOVAL OF EXISTING STRUCTURES NO. 9	.	EACH	1	1																
50101200	REMOVAL OF EXISTING STRUCTURES NO. 10	.	EACH	1	1																
50105220	PIPE CULVERT REMOVAL	.	FOOT	4930	4930																
50200100	STRUCTURE EXCAVATION		CU YD	214.1			214.1														
50300225	CONCRETE STRUCTURES		CU YD	240.6			240.6														
50300255	CONCRETE SUPERSTRUCTURE		CU YD	721.4			721.4														
50300260	BRIDGE DECK GROOVING		SO YD	1634			1634														
50300280	CONCRETE ENCASEMENT		CU YD	14.2			14.2														
50300300	PROTECTIVE COAT		SO YD	2141			2141														
50401105	FURNISHING AND ERECTING PRECAST PRESTRESSED CONCRETE I-BEAMS, 54 IN.		FOOT	2200			2200														
50800105	REINFORCEMENT BARS		POUND	90670			90670														
50800205	REINFORCEMENT BARS, EPOXY COATED		POUND	189300			189300														
50800515	BAR SPLICERS		EACH	138			138														
51100100	SLOPE WALL 4 INCH		SO YD	713			713														
51201610	FURNISHING STEEL PILES HP12X63		FOOT	1968			1968														

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ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
FAP 310 (US 67/IL 104)
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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	8
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

80% FEDERAL 20% STATE
CONSTRUCTION AND SAFETY TYPE CODE

CODE NO.	ITEM	SP. PROV.	UNIT	TOTAL QUANTITIES	CONSTRUCTION AND SAFETY TYPE CODE															
					0001 ROADWAY	0008 STRUCTURE														
51201800	FURNISHING STEEL PILES HP14X73		FOOT	2448		2448														
51202305	DRIVING PILES		FOOT	4416		4416														
51203800	TEST PILE STEEL HP14X73		EACH	2		2														
51500100	NAME PLATES		EACH	1		1														
54001001	BOX CULVERT END SECTIONS, CULVERT NO. 1		EACH	1	1															
54002020	EXPANSION BOLTS 3/4 INCH		EACH	109	109															
54003000	CONCRETE BOX CULVERTS		CU YD	418.3	418.3															
54010303	PRECAST CONCRETE BOX CULVERTS 3' x 3'		FOOT	4	4															
54010503	PRECAST CONCRETE BOX CULVERTS 5' x 3'		FOOT	144	144															
54010806	PRECAST CONCRETE BOX CULVERTS 8' x 6'		FOOT	116	116															
542A0217	PIPE CULVERTS, CLASS A, TYPE 1 12"		FOOT	52	52															
542A0229	PIPE CULVERTS, CLASS A, TYPE 1 24"		FOOT	256	256															
542A0235	PIPE CULVERTS, CLASS A, TYPE 1 30"		FOOT	124	124															
542A1063	PIPE CULVERTS, CLASS A, TYPE 2 18"		FOOT	72	72															
542A1069	PIPE CULVERTS, CLASS A, TYPE 2 24"		FOOT	2258	2258															
542A1075	PIPE CULVERTS, CLASS A, TYPE 2 30"		FOOT	1050	1050															
542A1081	PIPE CULVERTS, CLASS A, TYPE 2 36"		FOOT	766	766															
542A1087	PIPE CULVERTS, CLASS A, TYPE 2 42"		FOOT	170	170															
542A1093	PIPE CULVERTS, CLASS A, TYPE 2 48"		FOOT	634	634															
542A1099	PIPE CULVERTS, CLASS A, TYPE 2 54"		FOOT	54	54															
542A1105	PIPE CULVERTS, CLASS A, TYPE 2 60"		FOOT	680	680															
542A1111	PIPE CULVERTS, CLASS A, TYPE 2 66"		FOOT	182	182															
542A1117	PIPE CULVERTS, CLASS A, TYPE 2 72"		FOOT	328	328															
542A1909	PIPE CULVERTS, CLASS A, TYPE 3 24"		FOOT	348	348															
542A1915	PIPE CULVERTS, CLASS A, TYPE 3 30"		FOOT	94	94															
542A1927	PIPE CULVERTS, CLASS A, TYPE 3 42"		FOOT	156	156															
542A1933	PIPE CULVERTS, CLASS A, TYPE 3 48"		FOOT	74	74															

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ILLINOIS DEPARTMENT OF TRANSPORTATION
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CONTRACT NO. 72667			
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS
310	69-3(3HB)	MORGAN	793
SHEET NO.		9	
STA.		TO STA.	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	

SUMMARY OF QUANTITIES					80% FEDERAL 20% STATE CONSTRUCTION AND SAFETY TYPE CODE					
CODE NO.	ITEM	SP. PROV.	UNIT	TOTAL QUANTITIES	0001 ROADWAY	0008 STRUCTURE				
542A1939	PIPE CULVERTS, CLASS A, TYPE 3 54"		FOOT	60	60					
542A2749	PIPE CULVERTS, CLASS A, TYPE 4 24"		FOOT	252	252					
542A2755	PIPE CULVERTS, CLASS A, TYPE 4 30"		FOOT	68	68					
542A4027	PIPE CULVERTS, CLASS A, TYPE 6 42"		FOOT	230	230					
542A5473	PIPE CULVERTS, CLASS A, TYPE 1 EQUIVALENT ROUND-SIZE 18"		FOOT	124	124					
542A5482	PIPE CULVERTS, CLASS A, TYPE 1 EQUIVALENT ROUND-SIZE 27"		FOOT	62	62					
542A5485	PIPE CULVERTS, CLASS A, TYPE 1 EQUIVALENT ROUND-SIZE 30"		FOOT	56	56					
542A5491	PIPE CULVERTS, CLASS A, TYPE 1 EQUIVALENT ROUND-SIZE 36"		FOOT	52	52					
542A5497	PIPE CULVERTS, CLASS A, TYPE 1 EQUIVALENT ROUND-SIZE 42"		FOOT	148	148					
542A8227	PIPE CULVERTS, CLASS A, TYPE 2 EQUIVALENT ROUND-SIZE 42"		FOOT	164	164					
542D0217	PIPE CULVERTS, CLASS D, TYPE 1 12"		FOOT	80	80					
542D0220	PIPE CULVERTS, CLASS D, TYPE 1 15"		FOOT	1306	1306					
542D0223	PIPE CULVERTS, CLASS D, TYPE 1 18"		FOOT	566	566					
542D0229	PIPE CULVERTS, CLASS D, TYPE 1 24"		FOOT	340	340					
542D0235	PIPE CULVERTS, CLASS D, TYPE 1 30"		FOOT	128	128					
542D0241	PIPE CULVERTS, CLASS D, TYPE 1 36"		FOOT	168	168					
542D0247	PIPE CULVERTS, CLASS D, TYPE 1 42"		FOOT	128	128					
542D1063	PIPE CULVERTS, CLASS D, TYPE 2 18"		FOOT	86	86					
542D1069	PIPE CULVERTS, CLASS D, TYPE 2 24"		FOOT	424	424					
542D1075	PIPE CULVERTS, CLASS D, TYPE 2 30"		FOOT	154	154					
542D1081	PIPE CULVERTS, CLASS D, TYPE 2 36"		FOOT	312	312					
542D1129	PIPE CULVERTS, CLASS D, TYPE 2 84"		FOOT	62	62					
542D1915	PIPE CULVERTS, CLASS D, TYPE 3 30"		FOOT	110	110					
542D5470	PIPE CULVERTS, CLASS D, TYPE 1 EQUIVALENT ROUND-SIZE 15"		FOOT	60	60					
542D5473	PIPE CULVERTS, CLASS D, TYPE 1 EQUIVALENT ROUND-SIZE 18"		FOOT	198	198					
542D012	PIPE CULVERTS, CLASS D, TYPE 1 12" (TEMPORARY)		FOOT	772	772					
542D015	PIPE CULVERTS, CLASS D, TYPE 1 15" (TEMPORARY)		FOOT	122	122					

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ILLINOIS DEPARTMENT OF TRANSPORTATION
 SUMMARY OF QUANTITIES
 FAP 310 (US 67/IL 104)
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CONTRACT NO. 72667				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	10
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

SUMMARY OF QUANTITIES					80% FEDERAL 20% STATE CONSTRUCTION AND SAFETY TYPE CODE					
CODE NO.	ITEM	SP. PROV.	UNIT	TOTAL QUANTITIES	0001 ROADWAY	0008 STRUCTURE				
5421D018	PIPE CULVERTS, CLASS D, TYPE 1 18" (TEMPORARY)		FOOT	314	314					
5421D024	PIPE CULVERTS, CLASS D, TYPE 1 24" (TEMPORARY)		FOOT	84	84					
5421D042	PIPE CULVERTS, CLASS D, TYPE 1 42" (TEMPORARY)		FOOT	58	58					
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"		EACH	1	1					
54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"		EACH	2	2					
54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"		EACH	43	43					
54213675	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 30"		EACH	16	16					
54213681	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 36"		EACH	4	4					
54213687	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 42"		EACH	6	6					
54213693	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 48"		EACH	4	4					
54213705	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 60"		EACH	3	3					
54213711	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 66"		EACH	2	2					
54213717	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 72"		EACH	4	4					
54214503	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, EQUIVALENT ROUND-SIZE 18"		EACH	4	4					
54214512	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, EQUIVALENT ROUND-SIZE 27"		EACH	2	2					
54214515	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, EQUIVALENT ROUND-SIZE 30"		EACH	2	2					
54214521	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, EQUIVALENT ROUND-SIZE 36"		EACH	2	2					
54214527	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, EQUIVALENT ROUND-SIZE 42"		EACH	2	2					
54215454	CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 54"		EACH	2	2					
54215547	METAL END SECTIONS 12"		EACH	8	8					
54215550	METAL END SECTIONS 15"		EACH	43	43					
54215553	METAL END SECTIONS 18"		EACH	24	24					
54215559	METAL END SECTIONS 24"		EACH	32	32					
54215565	METAL END SECTIONS 30"		EACH	10	10					
54215571	METAL END SECTIONS 36"		EACH	14	14					
54215577	METAL END SECTIONS 42"		EACH	6	6					
54215619	METAL END SECTIONS 84"		EACH	2	2					

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ILLINOIS DEPARTMENT OF TRANSPORTATION
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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	11
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

80% FEDERAL 20% STATE
CONSTRUCTION AND SAFETY TYPE CODE

CODE NO.	ITEM	SP. PROV.	UNIT	TOTAL QUANTITIES	CONSTRUCTION AND SAFETY TYPE CODE						
					0001 ROADWAY	0008 STRUCTURE					
54215760	METAL END SECTIONS, EQUIVALENT ROUND-SIZE 15"		EACH	2	2						
54215763	METAL END SECTIONS, EQUIVALENT ROUND-SIZE 18"		EACH	10	10						
54218440	REINFORCED CONCRETE PIPE TEE, 36" PIPE WITH 36" RISER		EACH	1	1						
54218484	REINFORCED CONCRETE PIPE TEE, 60" PIPE WITH 36" RISER		EACH	3	3						
54218506	REINFORCED CONCRETE PIPE TEE, 72" PIPE WITH 36" RISER		EACH	1	1						
5422A024	PIPE CULVERTS, CLASS A, TYPE 2 24" (TEMPORARY)		FOOT	24	24						
5422D024	PIPE CULVERTS, CLASS D, TYPE 2 24" (TEMPORARY)		FOOT	212	212						
5422D030	PIPE CULVERTS, CLASS D, TYPE 2 30" (TEMPORARY)		FOOT	120	120						
5422D036	PIPE CULVERTS, CLASS D, TYPE 2 36" (TEMPORARY)		FOOT	60	60						
54247130	GRATING FOR CONCRETE FLARED END SECTION 24"		EACH	1	1						
54248510	CONCRETE COLLAR		CU YD	3.8	3.8						
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"		FOOT	79	79						
59300100	CONTROLLED LOW-STRENGTH MATERIAL		CU YD	58	58						
60100060	CONCRETE HEADWALLS FOR PIPE DRAINS		EACH	304	304						
60100925	PIPE DRAINS 8"		FOOT	2000	2000						
60100935	PIPE DRAINS 10"		FOOT	2000	2000						
60100945	PIPE DRAINS 12"		FOOT	4048	4048						
60100955	PIPE DRAINS 15"		FOOT	1000	1000						
60107600	PIPE UNDERDRAINS 4"		FOOT	117864	117864						
60108100	PIPE UNDERDRAINS 4" (SPECIAL)		FOOT	4105	4105						
60221000	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, OPEN LID		EACH	1	1						
60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID		EACH	7	7						
60222270	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 3V FRAME AND GRATE		EACH	1	1						
60222805	MANHOLES, TYPE A, 5'-DIAMETER, WITH MEDIAN INLET (604106)		EACH	21	21						
60224071	MANHOLES, TYPE A, 6'-DIAMETER, WITH MEDIAN INLET (604106)		EACH	1	1						
60224446	MANHOLES, TYPE A, 7'-DIAMETER, TYPE 1 FRAME, CLOSED LID		EACH	1	1						
60236200	INLETS, TYPE A, TYPE 8 GRATE		EACH	6	6						

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ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
FAP 310 (US 67/IL 104)
SHEET 8 OF 13

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SECTION 69-3(3HB)

MORGAN COUNTY

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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	12
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

80% FEDERAL 20% STATE
CONSTRUCTION AND SAFETY TYPE CODE

CODE NO.	ITEM	SP. PROV.	UNIT	TOTAL QUANTITIES	CONSTRUCTION AND SAFETY TYPE CODE															
					0001 ROADWAY	0008 STRUCTURE														
60237000	INLETS, TYPE A, TYPE 15 FRAME AND LID		EACH	2	2															
60240301	INLETS, TYPE B, TYPE 8 GRATE		EACH	1	1															
60246805	MEDIAN INLET (604106)		EACH	5	5															
60500040	REMOVING MANHOLES		EACH	1	1															
60500060	REMOVING INLETS		EACH	3	3															
60608600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06		FOOT	18	18															
60610400	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24		FOOT	44	44															
60618300	CONCRETE MEDIAN SURFACE, 4 INCH		SO FT	17351	17351															
60624600	CORRUGATED MEDIAN		SO FT	706	706															
61000225	TYPE F INLET BOX, STANDARD 610001		EACH	4	4															
21301060	EXPLORATION TRENCH 60" DEPTH		FOOT	50000	50000															
61100605	MISCELLANEOUS CONCRETE		CU YD	100	100															
61101009	STORM SEWERS PROTECTED, CLASS A, 8"		FOOT	500	500															
61101011	STORM SEWERS PROTECTED, CLASS A, 10"		FOOT	500	500															
61101013	STORM SEWERS PROTECTED, CLASS A, 12"		FOOT	8000	8000															
61101020	STORM SEWERS PROTECTED, CLASS A, 18"		FOOT	500	500															
* 63000001	STEEL PLATE BEAM GUARDRAIL TYPE A, 6 FOOT POSTS		FOOT	562.52	562.52															
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6		EACH	4	4															
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT		EACH	4	4															
63200310	GUARDRAIL REMOVAL		FOOT	880	880															
63500105	DELINEATORS		EACH	593	593															
64200105	SHOULDER RUMBLE STRIPS		FOOT	111654	111654															
66400105	CHAIN LINK FENCE, 4'		FOOT	4623	4623															
66500105	WOVEN WIRE FENCE, 4'		FOOT	31684	31684															
66501600	WOVEN WIRE GATES, 4' X 16' DOUBLE		EACH	2	2															
66600105	FURNISHING AND ERECTING RIGHT OF WAY MARKERS		EACH	271	271															
66700205	PERMANENT SURVEY MARKERS, TYPE I		EACH	33	33															

* SPECIALTY ITEM

REVISIONS	
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 SUMMARY OF QUANTITIES
 FAP 310 (US 67/IL 104)
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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	13
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

80% FEDERAL 20% STATE
CONSTRUCTION AND SAFETY TYPE CODE

CODE NO.	ITEM	SP. PROV.	UNIT	TOTAL QUANTITIES	CONSTRUCTION AND SAFETY TYPE CODE						
					0001 ROADWAY	0008 STRUCTURE					
66700305	PERMANENT SURVEY MARKERS, TYPE II		EACH	22	22						
67000400	ENGINEER'S FIELD OFFICE, TYPE A		CAL MO	30	30						
67000600	ENGINEER'S FIELD LABORATORY		CAL MO	30	30						
67100100	MOBILIZATION		L SUM	1	1						
70100205	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	•	EACH	4	4						
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	•	L SUM	1	1						
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	•	L SUM	1	1						
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	•	L SUM	1	1						
70101830	TRAFFIC CONTROL AND PROTECTION, STANDARD BLR 21	•	L SUM	1	1						
70101835	TRAFFIC CONTROL AND PROTECTION, STANDARD BLR 22	•	L SUM	1	1						
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501		L SUM	1	1						
70103815	TRAFFIC CONTROL SURVEILLANCE		CAL DA	36	36						
70300100	SHORT TERM PAVEMENT MARKING		FOOT	16,000	16,000						
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS		SO FT	3244	3244						
70300230	TEMPORARY PAVEMENT MARKING - LINE 5"		FOOT	541208	541208						
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"		FOOT	21,936	21,936						
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"		FOOT	7,956	7,956						
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"		FOOT	4,008	4,008						
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"		FOOT	904	904						
70301000	WORK ZONE PAVEMENT MARKING REMOVAL		SO FT	130,958	130,958						
* 72000100	SIGN PANEL - TYPE 1		SO FT	3465	3465						
* 72000200	SIGN PANEL - TYPE 2		SO FT	304	304						
* 72000300	SIGN PANEL - TYPE 3		SO FT	1398	1398						
* 72700100	STRUCTURAL STEEL SIGN SUPPORT - BREAKAWAY		POUND	11506	11506						
* 72800100	TELESCOPING STEEL SIGN SUPPORT		FOOT	8084	8084						
* 73000100	WOOD SIGN SUPPORT		FOOT	520	520						
* 73100100	BASE FOR TELESCOPING STEEL SIGN SUPPORT		EACH	8	8						

*SPECIALTY ITEM

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
FAP 310 (US 67/IL 104)
SHEET 10 OF 13

DATE 10/2010

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SECTION 69-3(3HB)

MORGAN COUNTY

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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	14
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

80% FEDERAL 20% STATE
CONSTRUCTION AND SAFETY TYPE CODE

CODE NO.	ITEM	SP. PROV.	UNIT	TOTAL QUANTITIES	CONSTRUCTION AND SAFETY TYPE CODE	
					0001 ROADWAY	0008 STRUCTURE
* 73400100	CONCRETE FOUNDATIONS		CU YD	25.9	25.9	
* 78001110	PAINT PAVEMENT MARKING - LINE 4"		FOOT	536	536	
* 78001120	PAINT PAVEMENT MARKING - LINE 5"		FOOT	88697	88697	
* 78001180	PAINT PAVEMENT MARKING - LINE 24"		FOOT	47	47	
* 78003100	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LETTERS AND SYMBOLS		SO FT	1829	1829	
* 78003180	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 24"		FOOT	452	452	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER		EACH	1481	1481	
* 78200300	PRISMATIC CURB REFLECTOR		EACH	250	250	
* 78200410	GUARDRAIL MARKERS, TYPE A		EACH	16	16	
* 78201000	TERMINAL MARKER - DIRECT APPLIED		EACH	4	4	
* 78300100	PAVEMENT MARKING REMOVAL		SO FT	23374	23374	
* 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL		EACH	100	100	
* 80400100	ELECTRIC SERVICE INSTALLATION		EACH	5	5	
* 81028550	UNDERGROUND CONDUIT, INTERMEDIATE METAL, 2" DIA.		FOOT	1111	1111	
* 81603000	UNIT DUCT, 600V, 2-1C NO.8, 1/C NO.8 GROUND, (XLP-TYPE USE), 3/4" DIA. POLYETHYLENE		FOOT	4210	4210	
* 81603025	UNIT DUCT, 600V, 2-1C NO.4, 1/C NO.4 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE		FOOT	2572	2572	
* 81603035	UNIT DUCT, 600V, 2-1C NO.6, 1/C NO.6 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE		FOOT	5200	5200	
* 81800200	AERIAL CABLE, 2-1/C NO. 4 WITH MESSENGER WIRE		FOOT	2150	2150	
* 82103900	LUMINAIRE, SODIUM VAPOR, MULTI-MOUNT, 250 WATT		EACH	46	46	
* 82500300	LIGHTING CONTROLLER, POLE MOUNTED, 240 VOLT, 30 AMP		EACH	3	3	
* 82500310	LIGHTING CONTROLLER, POLE MOUNTED, 240 VOLT, 60 AMP		EACH	1	1	
* 83062730	LIGHT POLE, WEATHERING STEEL, 45 FT. M.H., TENON MOUNT		EACH	28	28	
* 83062735	LIGHT POLE, WEATHERING STEEL, 45 FT. M.H., TENON MOUNT - TWIN		EACH	9	9	
* 83600355	LIGHT POLE FOUNDATION METAL, 15" BOLT CIRCLE, 8" X 6"		EACH	28	28	
* 83600357	LIGHT POLE FOUNDATION METAL, 15" BOLT CIRCLE, 8" X 8"		EACH	9	9	
* 83800650	BREAKAWAY DEVICE, COUPLING, WITH STAINLESS STEEL SCREEN		EACH	140	140	
* 84500120	REMOVAL OF ELECTRIC SERVICE INSTALLATION		EACH	1	1	
* X0325712	RELOCATE EXISTING ELECTRICAL SYSTEM		L SUM	1	1	
* X0327297	REMOVE BARRICADES		EACH	2	2	

* SPECIALTY ITEM

REVISIONS	
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ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
FAP 310 (US 67/IL 104)
SHEET 11 OF 13

DATE 10/2010

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CONTRACT NO. 72667

F.A.P. RYE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	15
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

80% FEDERAL 20% STATE
CONSTRUCTION AND SAFETY TYPE CODE

CODE NO.	ITEM	SP. PROV.	UNIT	TOTAL QUANTITIES	CONSTRUCTION AND SAFETY TYPE CODE																
					0001 ROADWAY	0008 STRUCTURE															
Z0002700	BARRICADES	.	EACH	4	4																
Z0007121	WELDED WIRE FABRIC	.	SQ FT	365	365																
Z0007601	BUILDING REMOVAL NO. 1	.	L SUM	1	1																
Z0007602	BUILDING REMOVAL NO. 2	.	L SUM	1	1																
Z0007603	BUILDING REMOVAL NO. 3	.	L SUM	1	1																
Z0007604	BUILDING REMOVAL NO. 4	.	L SUM	1	1																
Z0007605	BUILDING REMOVAL NO. 5	.	L SUM	1	1																
Z0007606	BUILDING REMOVAL NO. 6	.	L SUM	1	1																
Z0007607	BUILDING REMOVAL NO. 7	.	L SUM	1	1																
Z0007608	BUILDING REMOVAL NO. 8	.	L SUM	1	1																
Z0007609	BUILDING REMOVAL NO. 9	.	L SUM	1	1																
Z0010616	ROADWAY CLEANING (SPECIAL)	.	EACH	2	2																
Z0013798	CONSTRUCTION LAYOUT	.	L SUM	1	1																
Z0023602	GRANULAR CULVERT BACKFILL	.	CU YD	2000	2000																
Z0023900	FILLING EXISTING WELLS	.	EACH	5	5																
Z0030150	IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3	.	EACH	2	2																
Z0041500	PLUG EXISTING CULVERTS	.	EACH	4	4																
Z0046304	PIPE UNDERDRAINS FOR STRUCTURES 4"	.	FOOT	220			220														
Z0049909	REMOVAL AND DISPOSAL OF NON-FRIABLE ASBESTOS, BUILDING NO. 9	.	L SUM	1	1																
Z0054515	ROCK FILL - EMBANKMENT	.	TON	10700	10700																
Z0054517	ROCK FILL - FOUNDATION	.	TON	600	600																
Z0054530	ROCK FILL - SUBGRADE	.	TON	6100	6100																
Z0056100	SAND DRAINAGE BLANKET	.	CU YD	1200	1200																
Z0065796	RIPRAP SLURRY	.	SQ YD	1671	1671																
X0322118	REMOVE CONCRETE FLARED END SECTIONS	.	EACH	1	1																
X0323716	ACCESS GATE, DOUBLE, 30 FOOT	.	EACH	1	1																
X0325607	GROUND STABILIZATION GEOSYNTHETIC	.	SQ YD	7500	7500																
X0326911	TRANSVERSE DRAINS COMPLETE	.	EACH	31	31																

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SUMMARY OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 12 OF 13
 DATE 10/2010
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CONTRACT NO. 72667

F.A.P. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	16
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

80% FEDERAL 20% STATE
CONSTRUCTION AND SAFETY TYPE CODE

CODE NO.	ITEM	SP. PROV.	UNIT	TOTAL QUANTITIES	CONSTRUCTION AND SAFETY TYPE CODE														
					0001 ROADWAY	0008 STRUCTURE													
X0327009	REMOVE SIGN (SPECIAL)	.	EACH	8	8														
X0358300	REMOVE AND RELAY END SECTIONS	.	EACH	2	2														
X2020500	EARTH EXCAVATION (ROCKFILL)	.	CU YD	4300	4300														
X2070304	POROUS GRANULAR EMBANKMENT, SPECIAL	.	CU YD	322.3		322.3													
X2503000	MAINTENANCE MOWING	.	ACRE	160	160														
X2810208	STONE RIPRAP, CLASS A4 (SPECIAL)	.	TON	1799	1799														
X2810210	STONE RIPRAP, CLASS A5 (SPECIAL)	.	TON	1391	1391														
X5015225	PIPE CULVERT REMOVAL (SPECIAL)	.	FOOT	81	81														
X5428800	CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS	.	CU YD	75.2	75.2														
X6060424	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-4.24	.	FOOT	440	440														
X6063600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24	.	FOOT	51	51														
X6064201	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06	.	FOOT	2243	2243														
X6065701	CONCRETE MEDIAN, TYPE SM-4.06	.	SO FT	4654	4654														
X7210205	REMOVE SIGN COMPLETE	.	EACH	30	30														
* X7800620	URETHANE PAVEMENT MARKING - LINE 5"	.	FOOT	270604	270604														
* X7800630	URETHANE PAVEMENT MARKING - LINE 6"	.	FOOT	10968	10968														
* X7800640	URETHANE PAVEMENT MARKING - LINE 8"	.	FOOT	3978	3978														
* X7800650	URETHANE PAVEMENT MARKING - LINE 12"	.	FOOT	2004	2004														

*SPECIALTY ITEM

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
FAP 310 (US 67/IL 104)
SHEET 13 OF 13

DATE 10/2010
DRAWN BY BGJ
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	17
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

ESTIMATED QUANTITIES

ITEM	UNIT	TOTAL
THE FOLLOWING ITEMS INCLUDE ESTIMATED QUANTITIES FOR HANDLING FIELD TILES ACROSS MAINLINE (SEE SPECIAL PROVISIONS).		
EXPLORATION TRENCH, 60" DEPTH	FOOT	50,000
MISCELLANEOUS CONCRETE	CU YD	100
STORM SEWERS PROTECTED, CL A, SPL 8"	FOOT	500
STORM SEWERS PROTECTED, CL A, SPL 10"	FOOT	500
STORM SEWERS PROTECTED, CL A, SPL 12"	FOOT	8,000
STORM SEWERS PROTECTED, CL A, SPL 18"	FOOT	500
PIPE DRAINS 8"	FOOT	2000
PIPE DRAINS 10"	FOOT	2000
PIPE DRAINS 12"	FOOT	4000
PIPE DRAINS 15"	FOOT	1000
TRENCH BACKFILL	CU YD	2500
THE FOLLOWING ITEMS INCLUDE ESTIMATED QUANTITIES FOR HANDLING TEMPORARY EROSION CONTROL (SEE SPECIAL PROVISIONS).		
SEEDING, CLASS 7	ACRE	40
MULCH, METHOD 2	ACRE	40
AGGREGATE (EROSION CONTROL)	TON	1,500
TEMPORARY EROSION CONTROL SEEDING	POUND	75,000
INLET AND PIPE PROTECTION	EACH	80
PERIMETER EROSION BARRIER	FOOT	7,000
EARTH EXCAVATION FOR EROSION CONTROL	CU YD	1,000
HEAVY DUTY EROSION CONTROL BLANKET	SQ YD	2,000
THE FOLLOWING ITEMS INCLUDE ESTIMATED QUANTITIES FOR HANDLING PERMANENT SEEDING AFTER TEMPORARY EROSION CONTROL SEEDING HAS BEEN PLACED AND ESTABLISHED ON FINISHED HIGHLY ERODIBLE SLOPES (TO AVOID FURTHER GROUND DISTURBANCE).		
INTERSEEDING, CLASS 2	ACRE	10
MAINTENANCE MOWING	ACRE	160
THE FOLLOWING ITEMS INCLUDE ESTIMATED QUANTITIES TO PROVIDE TEMPORARY ACCESS (SEE SPECIAL PROVISIONS).		
AGGREGATE SURFACE COURSE, TYPE B	TON	2,500
THE FOLLOWING ITEMS INCLUDE ESTIMATED QUANTITIES FOR CORRECTING GEOTECHNICAL PROBLEMS. THESE QUANTITIES MAY BE REDUCED, INCREASED, OR DELETED BY THE ENGINEER BASED ON THE ACTUAL FIELD CONDITIONS. NO WORK INVOLVING THESE QUANTITIES SHALL BE PERFORMED WITHOUT DIRECTION AND APPROVAL OF THE ENGINEER.		
THE FOLLOWING ARE ESTIMATED QUANTITIES FOR CORRECTING UNSTABLE SUBGRADE CONDITIONS (SEE SPECIAL PROVISIONS).		
SUBBASE GRANULAR MATERIAL, TYPE A	TON	2,200
ROCK FILL - SUBGRADE	TON	6,100
EARTH EXCAVATION (ROCK FILL)*	CU YD	4,300
PROCESSING LIME STABILIZED SOIL 12"	SQ YD	6,500
EARTH EXCAVATION**	CU YD	4,300
LIME	TON	130
TRANSVERSE DRAINS, COMPLETE	EACH	5
* SEE ROCK FILL - SUBGRADE SPECIAL PROVISION.		
** REMOVING TOP 12" PRIOR TO MODIFYING LOWER LAYER AND REPLACING TO 12" ARE CONSIDERED SEPARATE OPERATIONS.		
THE FOLLOWING ARE ESTIMATED QUANTITIES FOR GROUND IMPROVEMENTS PRIOR TO PLACING CONCRETE BOX OR PIPE CULVERTS (SEE SPECIAL PROVISIONS).		
ESTIMATED LOCATIONS: STA 877+21 & VARIOUS		
ROCK FILL - FOUNDATION	TON	600
WHERE THE TOTAL THICKNESS OF AGGREGATE BELOW A CULVERT IS LESS THAN 12", ROCK FILL FOUNDATIONS SHALL BE GRADATION CA-07.		
THE FOLLOWING ARE ESTIMATED QUANTITIES FOR GROUND IMPROVEMENTS PRIOR TO CONSTRUCTING EMBANKMENTS (SEE SPECIAL PROVISIONS).		
ESTIMATED LOCATIONS: STA 826+00 TO STA 831+50 STA 946+50 TO STA 947+50 STA 994+50 TO STA 996+50		
ROCK FILL - EMBANKMENT	TON	10,700
GROUND STABILIZATION GEOSYNTHETIC	SQ YD	7,500
THE FOLLOWING ARE ESTIMATED QUANTITIES FOR CUT SLOPE SEEPAGE AREA PROTECTION WITH RIP RAP (SEE SPECIAL PROVISIONS).		
ESTIMATED LOCATIONS: STA 1085+00 TO STA 1089+00		
STONE DUMPED RIPRAP, CLASS B3	TON	600
FILTER FABRIC	SQ YD	900
RIPRAP SLURRY	SQ YD	100

TRANSVERSE DRAINS, COMPLETE

X0326911

STATION	EACH
FAP 310 (US 67)	
1079+00	2
1084+00	2
1089+00	2
1095+00	2
1100+00	2
RAMP A	
5+00	1
11+00	1
RAMP B	
12+00	1
16+00	1
RAMP C	
4+00	1
8+00	1
RAMP D	
12+00	1
17+00	1
CONCORD ROAD	
11+00	2
16+00	2
27+00	2
32+00	2
TOTAL	26

* - BOTH SIDES

SAND DRAINAGE BLANKET

Z0056100

STATION TO STATION	CU YD
FAP 310 (US 67)	
1091+00 1095+00	1,200
TOTAL	1,200

ROCK EXCAVATION

Z0200200

STATION	CU YD
FAP 310 (US 67)	
1092+06	100
TOTAL	100

POROUS GRANULAR EMBANKMENT

Z0700220

STATION	CU YD
FAP 310 (US 67)	
1092+06	40
TOTAL	40

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 1 OF 42

DATE 10/10
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	19
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

EARTH EXCAVATION SCHEDULE - CONTINUED

LOCATION STATION TO STATION	1	2	3	4	5	6	7	8	9	10	11	12	
	THEORETICAL		TOPSOIL ADJUSTMENT		4" TOPSOIL PLACEMENT	ADJUSTED EARTHWORK			TOPSOIL EXC. & PLACEMENT	EXCESS (BORROW)	CUMULATIVE EXCESS (BORROW)	FURNISHED EXCAVATION	
	CUT	FILL	4" CUT	4" FILL		CUT	FILL	FILL X 1.25					
CU YD					SO YD	CU YD							
STAGE III													
FAP 310 (US 67)													
762+00	770+00	485	42	305	24	2961	790	42	53	329	408	408	(327)
770+00	785+00	9339	340	1021	153	10566	10360	340	425	1174	8761	9169	(7009)
785+00	800+00	4842	362	698	118	7344	5540	362	453	816	4271	13440	(3417)
815+00	830+00	2803	1530	121	65	1674	2924	1530	1913	186	825	14265	(660)
875+00	890+00	1618	11	319	73	3528	1937	11	14	392	1531	15796	(1225)
890+00	905+00	377	0	132	3	1215	509	0	0	135	374	16170	(299)
935+00	950+00	805	4270	242	372	5526	1047	4270	5338	614	(4905)	11265	3924
950+00	965+00	1629	14565	423	924	12123	2052	14565	18206	1347	(17501)	(6236)	14001
965+00	980+00	7286	13208	711	588	11691	7997	13208	16510	1299	(9812)	(16048)	7850
980+00	995+00	5089	13700	652	625	11493	5741	13700	17125	1277	(12661)	(28709)	10129
995+00	1010+00	1761	25513	327	772	9891	2088	25513	31891	1099	(30902)	(59611)	24722
1010+00	1025+00	3779	20291	515	767	11538	4294	20291	25364	1282	(22352)	(81963)	17881
1025+00	1040+00	3208	14298	492	507	8991	3700	14298	17873	999	(15172)	(97135)	12137
1040+00	1055+00	4620	15174	574	389	8667	5194	15174	18968	963	(14737)	(111872)	11789
1055+00	1070+00	16894	4022	1098	193	11619	17992	4022	5028	1291	11673	(100199)	(9339)
1070+00	1085+00	5453	22477	370	747	10053	5823	22477	28096	1117	(23390)	(123589)	18712
1085+00	1100+00	70203	55183	1645	1108	24777	71848	55183	68979	2753	116	(123473)	(93)
1100+00	1115+00	6073	25464	620	879	13491	6693	25464	31830	1499	(26636)	(150109)	21309
1115+00	1130+00	4256	6531	1218	833	18459	5474	6531	8164	2051	(4741)	(154850)	3793
1130+00	1145+00	1925	9367	620	695	11835	2545	9367	11709	1315	(10479)	(165329)	8383
TEMPORARY ACCESS ROAD													
0+00	30+15	1628	2553	0	0	0	1628	2553	3191	0	(1563)	(1563)	1251
CONCORD ROAD													
27+00	37+65	920	1152	427	396	7407	1347	1152	1440	823	(916)	(916)	733
US 67 FRONTAGE ROAD (CONCORD)													
1+23	12+00	45	410	79	130	1881	124	410	513	209	(598)	(598)	478
50+00	54+20	7	168	3	17	180	10	168	210	20	(220)	(818)	176
RAMP A													
12+00	12+70	505	48	78	11	801	583	48	60	89	434	434	(347)
RAMP C													
2+00	9+42	1456	569	454	217	6039	1910	569	711	671	528	528	(422)
ASH STREET													
4+00	9+50	21	2	0	0	0	21	2	3	0	18	18	(15)
BETHEL LANE													
74+65	78+75	969	529	200	37	2133	1169	529	661	237	271	271	(217)
OLD CYLINDER HEAD ROAD													
3+00	9+50	699	79	149	26	1575	848	79	99	175	574	574	(459)
GILLILAND ROAD CONNECTOR													
11+30	12+85	570	0	64	0	576	634	0	0	64	570	570	(456)
GILLILAND ROAD EXTENSION													
20+75	27+50	714	152	137	24	1449	851	152	190	161	500	500	(400)
DUTCH LANE EAST													
138+04	148+80	3946	411	632	37	6021	4578	411	514	669	3395	3395	(2716)
HART'S GRAVEL ROAD													
289+67	298+78	4297	16436	295	761	9504	4592	16436	20545	1056	(17009)	(17009)	13607
SUBTOTAL STAGE III													
		168222	268857	14621	11491	235008	182843	268857	336076	26112	(179345)		143474

EARTHWORK NOTES:

A MASS DIAGRAM FOR THE EARTHWORK IS NOT INCLUDED IN THESE PLANS AND WILL NOT BE AVAILABLE TO THE CONTRACTOR UPON REQUEST. THE EARTHWORK SCHEDULE HAS BEEN INCLUDED IN THESE PLANS TO TAKE THE PLACE OF THE NEED FOR A MASS DIAGRAM. THE SCHEDULE GIVES QUANTITIES IN "COLUMN 10" FOR EXCESS EXCAVATION OR BORROW REQUIRED IN EACH SPECIFIED AREA ALONG THE LIMITS OF THE PROJECT. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE REQUIRED HAUL DISTANCES AND QUANTITIES FROM THE INFORMATION SHOWN.

THE QUANTITIES SHOWN IN THE SCHEDULE INCLUDE ALL MAINLINE, SIDEROAD, AND ENTRANCE QUANTITIES IN EACH SPECIFIED AREA.

ALL TOPSOIL NECESSARY FOR PLACEMENT ON THIS PROJECT SHALL BE TAKEN FROM AREAS WITHIN THE CONSTRUCTION LIMITS, AND ADDITIONAL QUANTITIES FOR FURNISHED EXCAVATION ARE INCLUDED TO REPLACE THIS MATERIAL LOSS. TOPSOIL EXCAVATION AND PLACEMENT WILL BE MEASURED FOR PAYMENT.

QUANTITIES FOR THE DEGRADING OF EXISTING CONDITIONS HAVE BEEN INCLUDED IN THE APPROXIMATE STATION RANGE WHERE SUCH DEGRADING TAKES PLACE.

A SHRINKAGE FACTOR OF 20% WAS USED TO DETERMINE THE EXCESS AND BORROW QUANTITIES. SHRINKAGE FACTOR = 1.00/(1.00-0.20) = 1.0/0.80 = 1.25

COLUMN 5 = (COLUMN 3 + COLUMN 4) x 9
 COLUMN 6 = COLUMN 1 + COLUMN 3
 COLUMN 8 = COLUMN 7 x 1.25 (SHRINKAGE FACTOR)
 COLUMN 9 = (COLUMN 3 + COLUMN 4) x 1.00 (SHRINKAGE FACTOR)
 COLUMN 10 = COLUMN 6 - COLUMN 8 - COLUMN 9
 COLUMN 12 = COLUMN 7 - 0.80 x (COLUMN 6 - COLUMN 9)

EARTHWORK QUANTITIES:

- 20200100 EARTH EXCAVATION = (COLUMN 6 - COLUMN 9) = 546,203 CU YD •
- 20400800 FURNISHED EXCAVATION = COLUMN 12 = 514,850 CU YD
- 21101615 TOPSOIL FURNISH AND PLACE, 4" = COLUMN 5 = 877,140 SO YD
- - SEE GEOTECHNICAL SCHEDULE FOR ADDITIONAL QUANTITIES

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 3 OF 42

DATE 10/10
 DRAWN BY EBB
 CHECKED BY

10:46:16 AM
4/26/2011

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EARTH EXCAVATION SCHEDULE - CONTINUED

CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	20
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

LOCATION STATION TO STATION	1		2		3		4		5		6		7		8		9		10		11		12	
	THEORETICAL		TOPSOIL ADJUSTMENT		4" TOPSOIL		ADJUSTED EARTHWORK		TOPSOIL EXC.		EXCESS		CUMULATIVE		FURNISHED									
	CUT	FILL	4" CUT	4" FILL	PLACEMENT	CUT	FILL	FILL X 1.25	& PLACEMENT	(BORROW)	EXCESS (BORROW)	EXCESS (BORROW)	EXCESS (BORROW)	EXCESS (BORROW)	EXCAVATION									
STAGE IV	CU YD										SO YD													
FAP 310 (US 67)																								
875+00 890+00	0	69	14	49	567	14	69	86	63	(135)	(135)	108												
890+00 905+00	38	0	35	0	315	73	0	0	35	38	(97)	(30)												
905+00 920+00	0	0	13	0	117	13	0	0	13	0	(97)	0												
935+00 950+00	12	9	7	7	126	19	9	11	14	(6)	(103)	5												
965+00 980+00	67	8446	6	497	4527	73	8446	10558	503	(10988)	(11091)	8790												
980+00 995+00	5560	14037	763	682	13005	6323	14037	17546	1445	(12668)	(23759)	10135												
995+00 1010+00	1899	15534	458	992	13050	2357	15534	19418	1450	(18511)	(42270)	14808												
1010+00 1025+00	6069	18522	748	966	15426	6817	18522	23153	1714	(18050)	(60320)	14440												
1025+00 1040+00	4709	19270	609	887	13464	5318	19270	24088	1496	(20266)	(80586)	16212												
1040+00 1055+00	2005	13183	359	1047	12654	2364	13183	16479	1406	(15521)	(96107)	12417												
1055+00 1070+00	4836	7932	591	973	14076	5427	7932	9915	1564	(6052)	(102159)	4842												
1070+00 1085+00	7835	5157	872	386	11322	8707	5157	6446	1258	1003	(101156)	(802)												
1085+00 1100+00	16534	2554	868	304	10548	17402	2554	3193	1172	13037	(88119)	(10430)												
1100+00 1115+00	17700	10	1046	34	9720	18746	10	13	1080	17653	(70466)	(14123)												
US 67 FRONTAGE ROAD (BETHEL)																								
520+00 524+68	39	94	25	11	324	64	94	118	36	(90)	(90)	72												
ST. PAUL'S CHURCH ROAD																								
121+27 127+83	3073	260	346	46	3528	3419	260	325	392	2702	2702	(2162)												
SERVICE DRIVE 92																								
600+50 603+76	426	113	61	31	828	487	113	141	92	254	254	(203)												
DUTCH LANE WEST																								
201+29 211+50	13064	4597	898	346	11196	13962	4597	5746	1244	6972	6972	(5577)												
PRIVATE DRIVE 81A																								
100+50 103+88	330	1	99	7	954	429	1	1	106	322	322	(257)												
SUBTOTAL STAGE IV	84196	109788	7818	7265	135747	92014	109788	137237	15083	(60306)		48245												
STAGE V																								
FAP 310 (US 67)																								
920+00 935+00	345	43	0	0	0	345	43	54	0	291	291	(233)												
935+00 950+00	391	69	0	0	0	391	69	86	0	305	596	(244)												
1070+00 1085+00	245	0	0	0	0	245	0	0	0	245	841	(196)												
SUBTOTAL STAGE V	981	112	0	0	0	981	112	140	0	841		(673)												
PROJECT TOTALS	586746	951810	56917	40543	877140	643663	951810	1189776	97460	(643573)		514850												

EARTHWORK NOTES:

A MASS DIAGRAM FOR THE EARTHWORK IS NOT INCLUDED IN THESE PLANS AND WILL NOT BE AVAILABLE TO THE CONTRACTOR UPON REQUEST. THE EARTHWORK SCHEDULE HAS BEEN INCLUDED IN THESE PLANS TO TAKE THE PLACE OF THE NEED FOR A MASS DIAGRAM. THE SCHEDULE GIVES QUANTITIES IN "COLUMN 10" FOR EXCESS EXCAVATION OR BORROW REQUIRED IN EACH SPECIFIED AREA ALONG THE LIMITS OF THE PROJECT. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE REQUIRED HAUL DISTANCES AND QUANTITIES FROM THE INFORMATION SHOWN.

THE QUANTITIES SHOWN IN THE SCHEDULE INCLUDE ALL MAINLINE, SIDEROAD, AND ENTRANCE QUANTITIES IN EACH SPECIFIED AREA.

ALL TOPSOIL NECESSARY FOR PLACEMENT ON THIS PROJECT SHALL BE TAKEN FROM AREAS WITHIN THE CONSTRUCTION LIMITS, AND ADDITIONAL QUANTITIES FOR FURNISHED EXCAVATION ARE INCLUDED TO REPLACE THIS MATERIAL LOSS. TOPSOIL EXCAVATION AND PLACEMENT WILL BE MEASURED FOR PAYMENT.

QUANTITIES FOR THE DEGRADING OF EXISTING CONDITIONS HAVE BEEN INCLUDED IN THE APPROXIMATE STATION RANGE WHERE SUCH DEGRADING TAKES PLACE.

A SHRINKAGE FACTOR OF 20% WAS USED TO DETERMINE THE EXCESS AND BORROW QUANTITIES. SHRINKAGE FACTOR = 1.00/(1.00-0.20) = 1.0/0.80 = 1.25

- COLUMN 5 = (COLUMN 3 + COLUMN 4) x 9
- COLUMN 6 = COLUMN 1 + COLUMN 3
- COLUMN 8 = COLUMN 7 x 1.25 (SHRINKAGE FACTOR)
- COLUMN 9 = (COLUMN 3 + COLUMN 4) x 1.00 (SHRINKAGE FACTOR)
- COLUMN 10 = COLUMN 6 - COLUMN 8 - COLUMN 9
- COLUMN 12 = COLUMN 7 - 0.80 x (COLUMN 6 - COLUMN 9)

EARTHWORK QUANTITIES:

- 20200100 EARTH EXCAVATION = (COLUMN 6 - COLUMN 9) = 546,203 CU YD
- 20400800 FURNISHED EXCAVATION = COLUMN 12 = 514,850 CU YD
- 21101615 TOPSOIL FURNISH AND PLACE, 4" = COLUMN 5 = 877,140 SO YD
- * - SEE GEOTECHNICAL SCHEDULE FOR ADDITIONAL QUANTITIES

EARTH EXCAVATION (WIDENING)

20200500

STATION TO STATION	SIDE	SO YD	DEPTH	CU YD
CONCORD ROAD				
33+39.0 34+84.0	LT	32.2	8.0	7.2
33+39.0 34+84.0	RT	32.2	8.0	7.2
COOPER STREET				
442+65.0 446+00.0	LT	79.2	9.0	19.8
444+71.2 447+38.1	RT	199.3	9.0	49.8
446+12.8 447+81.5	LT	21.3	9.0	5.3
ASH STREET				
8+73.1 9+87.6	LT	92.0	9.0	23.0
9+17.5 9+87.6	RT	67.3	9.0	16.8
TOTAL				129.1
USE				130

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
FAP 310 (US 67/IL 104)
SHEET 4 OF 42

DATE 10/10
DRAWN BY EBB
CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	21
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

SEEDING SCHEDULE

STATION TO STATION	SIDE	WIDTH	SEEDING CLASS 2 ACRE	FERTILIZER NUTRIENTS			MULCH METHOD 2 ACRE	AGRICULTURAL LIMESTONE TON	
				25000200	25000400	25000500			
				NITROGEN	PHOSPHORUS POUND	POTASSIUM			
FAP 310 (US 67)									
762+05.0	770+00.0	CL	28-38	0.65	58.5	58.5	58.5	0.65	1.3
770+00.0	785+00.0	LT	VAR	2.05	184.5	184.5	184.5	2.05	4.1
		CL	38	0.88	79.2	79.2	79.2	0.88	1.8
785+00.0	800+00.0	LT	VAR	3.23	290.7	290.7	290.7	3.23	6.5
		RT	VAR	4.00	360	360	360	4.00	8.0
800+00.0	815+00.0	LT	160	5.51	495.9	495.9	495.9	5.51	11.0
		CL	38	1.31	117.9	117.9	117.9	1.31	2.6
		RT	55	1.90	171	171	171	1.90	3.8
815+00.0	830+00.0	LT	VAR	3.06	275.4	275.4	275.4	3.06	6.1
		CL	38	1.31	117.9	117.9	117.9	1.31	2.6
		RT	VAR	2.16	194.4	194.4	194.4	2.16	4.3
830+00.0	845+00.0	LT	65	2.24	201.6	201.6	201.6	2.24	4.5
		CL	38	1.31	117.9	117.9	117.9	1.31	2.6
		RT	65	2.24	201.6	201.6	201.6	2.24	4.5
845+00.0	861+00.0	LT	VAR	3.56	320.4	320.4	320.4	3.56	7.1
		CL	VAR	1.18	106.2	106.2	106.2	1.18	2.4
		RT	65	2.39	215.1	215.1	215.1	2.39	4.8
861+00.0	875+00.0	LT	95	3.06	275.4	275.4	275.4	3.06	6.1
		CL	VAR	1.16	104.4	104.4	104.4	1.16	2.3
		RT	VAR	2.27	204.3	204.3	204.3	2.27	4.5
875+00.0	890+00.0	LT	VAR	2.87	258.3	258.3	258.3	2.87	5.7
		CL	VAR	1.04	93.6	93.6	93.6	1.04	2.1
		RT	VAR	3.41	306.9	306.9	306.9	3.41	6.8
890+00.0	905+00.0	LT	VAR	3.33	299.7	299.7	299.7	3.33	6.7
		CL	VAR	1.25	112.5	112.5	112.5	1.25	2.5
		RT	VAR	3.17	285.3	285.3	285.3	3.17	6.3
905+00.0	920+00.0	LT	100	3.45	310.5	310.5	310.5	3.45	6.9
		CL	38	1.31	117.9	117.9	117.9	1.31	2.6
		RT	70	2.42	217.8	217.8	217.8	2.42	4.8
920+00.0	935+00.0	LT	105	3.62	325.8	325.8	325.8	3.62	7.2
		CL	VAR	1.06	95.4	95.4	95.4	1.06	2.1
		RT	70	2.42	217.8	217.8	217.8	2.42	4.8
935+00.0	950+00.0	LT	VAR	2.75	247.5	247.5	247.5	2.75	5.5
		CL	VAR	1.30	117	117	117	1.30	2.6
		RT	60	2.07	186.3	186.3	186.3	2.07	4.1
950+00.0	965+00.0	LT	VAR	2.11	189.9	189.9	189.9	2.11	4.2
		CL	VAR	0.98	88.2	88.2	88.2	0.98	2.0
		RT	65	2.24	201.6	201.6	201.6	2.24	4.5
965+00.0	980+00.0	LT	65	2.24	201.6	201.6	201.6	2.24	4.5
		CL	VAR	1.06	95.4	95.4	95.4	1.06	2.1
		RT	75	2.59	233.1	233.1	233.1	2.59	5.2
980+00.0	995+00.0	LT	60	2.07	186.3	186.3	186.3	2.07	4.1
		CL	VAR	1.29	116.1	116.1	116.1	1.29	2.6
		RT	85	2.93	263.7	263.7	263.7	2.93	5.9
995+00.0	1010+00.0	LT	60	2.07	186.3	186.3	186.3	2.07	4.1
		CL	VAR	1.04	93.6	93.6	93.6	1.04	2.1
		RT	70	2.42	217.8	217.8	217.8	2.42	4.8
1010+00.0	1025+00.0	LT	60	2.07	186.3	186.3	186.3	2.07	4.1
		CL	38	1.31	117.9	117.9	117.9	1.31	2.6
		RT	VAR	2.96	266.4	266.4	266.4	2.96	5.9
1025+00.0	1040+00.0	LT	65	2.24	201.6	201.6	201.6	2.24	4.5
		CL	VAR	1.03	92.7	92.7	92.7	1.03	2.1
		RT	85	2.93	263.7	263.7	263.7	2.93	5.9
1040+00.0	1055+00.0	LT	65	2.24	201.6	201.6	201.6	2.24	4.5
		CL	VAR	1.19	107.1	107.1	107.1	1.19	2.4
		RT	60	2.07	186.3	186.3	186.3	2.07	4.1
1055+00.0	1070+00.0	LT	75	2.59	233.1	233.1	233.1	2.59	5.2
		CL	VAR	1.19	107.1	107.1	107.1	1.19	2.4
		RT	70	2.42	217.8	217.8	217.8	2.42	4.8
1070+00.0	1085+00.0	LT	VAR	2.76	248.4	248.4	248.4	2.76	5.5
		CL	VAR	0.97	87.3	87.3	87.3	0.97	1.9
		RT	VAR	3.12	280.8	280.8	280.8	3.12	6.2
1085+00.0	1098+00.0	LT	VAR	5.08	457.2	457.2	457.2	5.08	10.2
		CL	VAR	1.07	96.3	96.3	96.3	1.07	2.1
		RT	VAR	3.49	314.1	314.1	314.1	3.49	7.0
1098+00.0	1113+00.0	LT	VAR	3.17	285.3	285.3	285.3	3.17	6.3
		CL	VAR	1.09	98.1	98.1	98.1	1.09	2.2
		RT	VAR	3.21	288.9	288.9	288.9	3.21	6.4
1113+00.0	1128+00.0	LT	VAR	5.17	465.3	465.3	465.3	5.17	10.3
		CL	VAR	1.11	99.9	99.9	99.9	1.11	2.2
		RT	60	2.07	186.3	186.3	186.3	2.07	4.1
1128+00.0	1143+00.0	LT	VAR	4.80	432	432	432	4.80	9.6
		CL	VAR	0.90	81	81	81	0.90	1.8
		RT	VAR	1.98	178.2	178.2	178.2	1.98	4.0
1143+00.0	1158+00.0	LT	VAR	0.41	36.9	36.9	36.9	0.41	0.8
SUBTOTALS				168.62	15175.8	15175.8	15175.8	168.62	337.3

SEEDING SCHEDULE - CONTINUED

STATION TO STATION	SIDE	WIDTH	SEEDING CLASS 2 ACRE	FERTILIZER NUTRIENTS			MULCH METHOD 2 ACRE	AGRICULTURAL LIMESTONE TON	
				25000200	25000400	25000500			
				NITROGEN	PHOSPHORUS POUND	POTASSIUM			
PRIVATE DRIVE 81A									
100+00.0	103+88.0	LT	20	0.18	16.2	16.2	16.2	0.18	0.4
		RT	20	0.18	16.2	16.2	16.2	0.18	0.4
SWALE 851									
0+00.0	1+72.2	LT & RT	25	0.10	9	9	9	0.10	0.2
SWALE 922									
0+00.0	4+51.9	LT & RT	35	0.37	33.3	33.3	33.3	0.37	0.7
SERVICE DRIVE 93									
611+36.0	614+31.7	LT	35	0.24	21.6	21.6	21.6	0.24	0.5
		RT	10	0.07	6.3	6.3	6.3	0.07	0.1
TEMPORARY ACCESS ROAD									
0+00.0	15+00.0	LT & RT	VAR	2.50	225	225	225	2.50	5.0
15+00.0	27+50.0	LT & RT	VAR	2.65	238.5	238.5	238.5	2.65	5.3
CONCORD ROAD									
30+00.0	21+00.0	RT	VAR	1.84	165.6	165.6	165.6	1.84	3.7
		LT	VAR	1.96	176.4	176.4	176.4	1.96	3.9
21+00.0	35+00.0	RT	VAR	2.75	247.5	247.5	247.5	2.75	5.5
		LT	VAR	2.33	209.7	209.7	209.7	2.33	4.7
35+00.0	37+65.0	RT	15	0.10	9	9	9	0.10	0.2
		LT	20	0.13	11.7	11.7	11.7	0.13	0.3
US 67 FRONTAGE ROAD (CONCORD)									
1+23.3	8+00.0	LT	20	0.32	28.8	28.8	28.8	0.32	0.6
8+00.0	23+00.0	LT	VAR	1.43	128.7	128.7	128.7	1.43	2.9
		RT	VAR	1.32	118.8	118.8	118.8	1.32	2.6
23+00.0	38+00.0	LT	VAR	3.15	283.5	283.5	283.5	3.15	6.3
		RT	VAR	2.18	196.2	196.2	196.2	2.18	4.4
38+00.0	53+00.0	LT	VAR	1.60	144	144	144	1.60	3.2
		RT	VAR	1.92	172.8	172.8	172.8	1.92	3.8
53+00.0	54+20.0	LT	20	0.06	5.4	5.4	5.4	0.06	0.1
SWALE 25									
0+00.0	2+49.6	LT & RT	20	0.12	10.8	10.8	10.8	0.12	0.2
RAMP A									
0+00.0	10+00.0	LT	50	0.98	88.2	88.2	88.2	0.98	2.0
		RT	50	1.04	93.6	93.6	93.6	1.04	2.1
RAMP B									
11+00.0	19+49.2	LT	55	0.76	68.4	68.4	68.4	0.76	1.5
		RT	45	0.83	74.7	74.7	74.7	0.83	1.7
RAMP C									
0+00.0	12+00.0	LT	60	0.97	87.3	87.3	87.3	0.97	1.9
		RT	60	1.52	136.8	136.8	136.8	1.52	3.0
RAMP D									
10+00.0	20+15.8	LT	50	0.69	62.1	62.1	62.1	0.69	1.4
		RT	50	0.69	62.1	62.1	62.1	0.69	1.4
COOPER STREET									
442+65.0	450+00.0	LT	VAR	0.30	27	27	27	0.30	0.6
		RT	VAR	0.30	27	27	27	0.30	0.6
RELOCATED CREWS LANE									
38+57.3	49+00.0	RT	VAR	0.62	55.8	55.8	55.8	0.62	1.2
49+00.0	57+03.0	LT	VAR	0.91	81.9	81.9	81.9	0.91	1.8
		RT	VAR	0.55	49.5	49.5	49.5	0.55	1.1
NEW STREET									
300+00.0	313+35.3	LT	20	0.60	54	54	54	0.60	1.2
		RT	40	1.20	108	108	108	1.20	2.4
ASH STREET									
2+50.0	10+00.0	LT	10	0.17	15.3	15.3	15.3	0.17	0.3
		RT	10	0.14	12.6	12.6	12.6	0.14	0.3
BETHEL LANE									
72+70.4	80+00.0	LT	VAR	0.43	38.7	38.7	38.7	0.43	0.9
		RT	VAR	0.32	28.8	28.8	28.8	0.32	0.6
80+00.0	91+50.0	LT	VAR	1.47	132.3	132.3	132.3	1.47	2.9
		RT	VAR	0.81	72.9	72.9	72.9	0.81	1.6
US 67 FRONTAGE ROAD (BETHEL)									
510+13.4	524+68.0	LT	30	0.45	40.5				

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	22
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

SEEDING SCHEDULE - CONTINUED

STATION TO STATION	SIDE	WIDTH	SEEDING CLASS 2 ACRE	FERTILIZER NUTRIENTS			MULCH METHOD 2 ACRE	AGRICULTURAL LIMESTONE TON	
				NITROGEN	PHOSPHORUS	POTASSIUM			
				POUND					
RELOCATED CYLINDER HEAD ROAD									
93+83.0	102+00.0	LT	30	0.56	50.4	50.4	50.4	0.56	1.1
		RT	30	0.56	50.4	50.4	50.4	0.56	1.1
102+00.0	110+00.0	LT	25	0.44	39.6	39.6	39.6	0.44	0.9
		RT	30	0.52	46.8	46.8	46.8	0.52	1.0
SWALE 9655									
0+00.0	3+65.2	LT & RT	30	0.26	23.4	23.4	23.4	0.26	0.5
OLD CYLINDER HEAD ROAD									
0+00.0	7+50.0	LT	20	0.28	25.2	25.2	25.2	0.28	0.6
		RT	20	0.23	20.7	20.7	20.7	0.23	0.5
GILLILAND ROAD CONNECTOR									
10+00.0	13+17.9	LT	20	0.10	9	9	9	0.10	0.2
		RT	15	0.06	5.4	5.4	5.4	0.06	0.1
GILLILAND ROAD EXTENSION									
20+75.0	28+06.1	LT	10	0.14	12.6	12.6	12.6	0.14	0.3
		RT	10	0.12	10.8	10.8	10.8	0.12	0.2
ST. PAUL'S CHURCH ROAD									
120+00.0	127+83.0	LT	VAR	0.47	42.3	42.3	42.3	0.47	0.9
		RT	VAR	1.49	134.1	134.1	134.1	1.49	3.0
DUTCH LANE EAST									
138+04.0	150+00.0	LT	VAR	2.64	237.6	237.6	237.6	2.64	5.3
		RT	25	0.58	52.2	52.2	52.2	0.58	1.2
DUTCH LANE WEST									
194+41.0	200+00.0	LT	VAR	0.35	31.5	31.5	31.5	0.35	0.7
		RT	VAR	0.45	40.5	40.5	40.5	0.45	0.9
200+00.0	211+50.0	LT	VAR	1.59	143.1	143.1	143.1	1.59	3.2
		RT	VAR	1.07	96.3	96.3	96.3	1.07	2.1
HART'S GRAVEL ROAD									
289+67.0	300+00.0	LT	VAR	1.48	133.2	133.2	133.2	1.48	3.0
		RT	VAR	1.36	122.4	122.4	122.4	1.36	2.7
SUBTOTALS				14.75	1327.5	1327.5	1327.5	14.75	29.5
PROJECT TOTAL				227.34	20460.6	20460.6	20460.6	227.34	454.7
USE				230.00	20700	20700	20700	230.00	460

TREE REMOVAL (DIAMETER)

TREE REMOVAL (ACRES)

STATION TO STATION	ACRES
20100500	
FAP 310 (US 67)	
866+00 876+00	4.6
927+00 932+00	3.9
956+00 964+00	3.7
1083+00 1090+00	4.2
1098+00 1102+00	1.2
CONCORD ROAD	
31+00 34+00	0.7
COOPER STREET	
446+00 448+00	0.6
TOTAL	18.9

LOCATION	STATION TO STATION	20100110 20100210	
		6" TO 15"	OVER 15"
UNIT			
EX US 67/ IL 104	563+00 562+00	90	0
TR 93 Bethel Lane	77+00 78+00	0	82
TR 93 Bethel Lane	90+00 90+50	12	24
TR 115 Cylinder Head Rd.	95+50 98+00	0	32
Old Cylinder Head Road	5+00 8+00	16	52
Gilliland Road	10+50 12+50	14	41
EX US 67/ IL 104	528+00 524+50	0	17
Old Cylinder Head Road	506+50 507+25	6	0
Dutch Lane West	196+00 197+00	40	36
FAP 310 US 67	1094+50 1096+50	60	60
Hart's Gravel Road	290+50 291+50	0	21
FAP 310 US 67	1112+50 1113+00	0	45
FAP 310 US 67	1133+50 1134+00	0	29
EX US 67/ IL 104	306+50 305+00	14	95
FAP 310 US 67	1120+00 1121+00	15	0
FAP 310 US 67	1125+50 1127+00	32	0
Totals		299	534

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 6 OF 42

DATE 10/10
 DRAWN BY EBB
 CHECKED BY

3:57:09 PM

\$FILE\$

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	23
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

3:57:10 PM
Mar-25-2011 03:57:10PM

RIPRAP SCHEDULE

STATION	SIDE	LENGTH	WIDTH	28100707	28100709	28100711	28200200	20065796
				RIPRAP CLASS A4	RIPRAP CLASS A5	RIPRAP CLASS A6	FILTER FABRIC	RIPRAP SLURRY
SO YD								
FAP 310 (US 67)								
806+53.0	RT	126.0	13.0	182.0			182.0	
809+06.0	RT	135.0	13.0	195.0			195.0	
820+60.0	RT	30.0	12.0	40.0			40.0	
843+91.0	RT	23.5	12.0	31.3			31.3	
851+11.0	LT	50.0	20.0	111.1			111.1	
851+11.0	RT	VAR	VAR	32.2			32.2	
866+07.0	LT	43.8	20.0	97.3			97.3	
866+07.0	RT	VAR	VAR	63.3			63.3	
869+87.0	RT	722.0	26.0	2085.8			2085.8	
875+54.0	RT	251.0	26.0	725.1			725.1	
877+21.0	LT	34.5	26.0	99.7			99.7	
877+21.0	RT	50.0	26.0	144.4			144.4	
886+66.0	RT	40.0	19.0	84.4			84.4	
889+15.0	RT	186.0	12.0	248.0			248.0	
900+00.0	RT	15.0	13.0	21.7			21.7	
901+60.0	LT	38.0	15.0	63.3			63.3	
922+35.0	LT	20.4	35.0	79.3			79.3	
922+35.0	RT	VAR	VAR	113.2			113.2	
931+90.0	RT	VAR	VAR	30			30	
947+28.0	LT	69.3	20.0	154.0			154.0	
947+28.0	RT	VAR	VAR	18.0			18.0	
956+40.0	RT	45.0	13.0	65.0			65.0	
959+06.0	LT	11.3	10.0	12.6			12.6	
962+91.0	LT	15.3	10.0	17.0			17.0	
968+48.0	LT	555.0	16.0	986.7			986.7	
969+05.0	RT	VAR	VAR	33.0			33.0	
971+35.0	LT	42.8	20.0	95.1			95.1	
974+70.0	LT	30.6	20.0	68.0			68.0	
978+98.0	LT	33.0	30.0	110.0			110.0	
978+98.0	RT	VAR	VAR	72.8			72.8	
986+94.0	RT	VAR	VAR	55			55.0	
995+26.0	RT	114.3	10.0	127.0			127.0	
995+49.0	LT	32.1	20.0	71.3			71.3	
995+49.0	RT	VAR	VAR	166.8			166.8	
997+17.0	LT	171.5	13.0	247.7			247.7	
997+83.0	RT	69.0	13.0	99.7			99.7	
998+05.0	LT	20.5	20.0	45.6			45.6	
999+10.0	RT	82.7	13.0	119.5			119.5	
999+27.0	LT	64.0	13.0	92.4			92.4	
999+40.0	LT	40.0	16.0	71.1			71.1	
1001+27.0	RT	VAR	VAR	38.1			38.1	
1019+28.0	LT	84.8	31.0	292.1			292.1	
1019+28.0	RT	VAR	VAR	72			72.0	
1054+10.0	LT	56.0	13.0	80.9			80.9	
1068+01.0	LT	45.0	35.0	175.0			175.0	
1068+01.0	RT	VAR	VAR	35.0			35.0	
1075+55.0	LT	84.2	32.0	299.4			299.4	
1078+74.0	RT	212.0	13.0	306.2			306.2	
1079+75.0	LT	472.5	16.0	840.0			840.0	
1081+57.0	RT	350.0	13.0	505.6			505.6	505.6
1082+51.0	LT	78.3	13.0	113.1			113.1	
1082+59.8	RT	VAR	VAR	26.7			26.7	
1088+57.5	LT	60.1	15.0	100.2			100.2	
1090+31.0	RT	180.6	16.0	321.1			321.1	321.1
1091+06.0	LT	496.5	13.0			717.2	717.2	
1091+19.0	RT	46.4	22.5			116.0	116.0	
1092+74.0	RT	299.5	16.0	532.4			532.4	532.4
1093+97.0	LT	81.1	15.0	135.2			135.2	
1094+08.0	LT	130.2	12.0			173.6	173.6	
1094+41.0	LT	135.0	12.0			180.0	180.0	
1094+55.0	RT	VAR	VAR		182.4		182.4	
1096+10.0	RT	380.0	16.0	675.6			675.6	
1096+77.0	LT	543.1	13.0	784.5			784.5	
1102+50.0	LT	213.0	16.0	378.7			378.7	
1108+70.0	LT	89.0	16.0	158.2			158.2	
1120+72.0	LT	VAR	VAR	268.1			268.1	
1134+61.0	LT	VAR	VAR	150			150.0	
1137+28.0	LT	393.3	16.0	699.2			699.2	
1141+45.0	LT	VAR	VAR	40			40.0	
SUBTOTALS				14202.6	182.4	1186.8	15571.8	1359.1

RIPRAP SCHEDULE

STATION	SIDE	LENGTH	WIDTH	28100707	28100709	28100711	28200200	20065796
				RIPRAP CLASS A4	RIPRAP CLASS A5	RIPRAP CLASS A6	FILTER FABRIC	RIPRAP SLURRY
SO YD								
CONCORD ROAD								
8+69.0	RT	33.0	10.0	36.7			36.7	
17+69.0	LT	85.0	10.0	94.4			94.4	
17+69.0	RT	81.0	10.0	90.0			90.0	
22+36.0	LT	86.0	10.0	95.6			95.6	
22+36.0	RT	84.0	10.0	93.3			93.3	
30+25.0	LT	50.0	40.0	222.2			222.2	
US 67 FRONTAGE ROAD (CONCORD)								
13+50.0	LT	VAR	VAR	25.7			25.7	
25+30.0	LT	VAR	VAR	58.3			58.3	
31+90.0	LT	12.0	12.0	16.0			16.0	
RAMP B								
11+00.0	RT	30.0	20.0	66.7			66.7	
RAMP C								
9+00.0	LT	40.0	13.0	57.8			57.8	
COOPER STREET								
444+95.0	LT	190.0	30.0	633.3			633.3	
RELOCATED CREWS LANE								
45+50.0	LT	57.7	25.0	160.3			160.3	
NEW STREET								
302+09.0	LT	100.0	11.0	122.2			122.2	
302+09.0	RT	100.0	14.0	155.6			155.6	
312+60.0	LT	99.5	16.0	176.9			176.9	
312+60.0	RT	70.0	14.0	108.9			108.9	
ASH STREET								
2+50.0	LT	10.0	10.0	11.1			11.1	
RELOCATED CYLINDER HEAD ROAD								
96+65.0	LT	15.0	11.0	18.3			18.3	
96+65.0	RT	23.1	12.0	30.8			30.8	
GILLILAND ROAD CONNECTOR								
12+94.0	RT	27.2	8.0	24.2			24.2	
12+94.0	LT	91.0	8.0	80.9			80.9	
GILLILAND ROAD EXTENSION								
23+45.0	LT	27.6	8.0	24.5			24.5	
DUTCH LANE EAST								
140+00.0	RT	VAR	VAR	32.0			32.0	
DUTCH LANE WEST								
194+74.0	RT	20.0	10.0	22.2			22.2	
197+96.0	RT	99.0	14.0	154.0			154.0	
198+16.0	LT	142.0	14.0	220.9			220.9	
204+50.0	LT	115.0	14.0	178.9			178.9	
204+73.0	RT	189.5	14.0	294.8			294.8	
205+70.0	LT	136.0	14.0	211.6			211.6	211.6
206+05.0	LT	VAR	VAR	94.4			94.4	
206+05.0	RT	33.0	27.0	99.0			99.0	
206+23.0	RT	53.0	14.0	82.4			82.4	
207+63.0	LT	189.0	14.0	294.0			294.0	
HART'S GRAVEL ROAD								
291+09.0	RT	39.0	30.0		130.0		130.0	
292+61.0	RT	34.0	30.0		113.3		113.3	
293+28.0	LT	25.0	14.0	38.9			38.9	
296+61.0	LT	475.0	14.0	738.9			738.9	
295+21.0	RT	525.0	14.0	816.7			816.7	
TOTALS				19885.0	425.8	1186.8	21497.5	1570.6
IDOT - ESTIMATED QUANTITY							900.0	100.0
USE				19885	426	1187	22398	1671

ENERGY DISSIPATING BASIN SCHEDULE

STATION	SIDE	LENGTH	WIDTH	28200200	X2810208	X2810210
				FILTER FABRIC	RIPRAP CLASS A4	RIPRAP CLASS A5
SO YD				TON		
FAP 310 (US 67)						
851+11.0	RT	VAR	VAR	102.0	110.5	
866+07.0	RT	VAR	VAR	96.3	104.3	
922+35.0	RT	VAR	VAR	206.0	223.2	
931+90.0	RT	VAR	VAR	29.3	31.7	
947+28.0	RT	VAR	VAR	64.7	70.1	
969+05.0	RT	VAR	VAR	159.4	172.7	
978+98.0	RT	VAR	VAR	110.0	119.2	
986+94.0	RT	VAR	VAR	55.6	60.2	
995+49.0	RT	VAR	VAR	104.5	113.2	
1001+27.0	RT	VAR	VAR	37.6	40.7	
1019+28.0	RT	VAR	VAR	111.4	120.7	
1068+01.0	RT	VAR	VAR	62.7	67.9	
1082+60.0	RT	VAR	VAR	66.4	71.9	
1120+72.0	LT	VAR	VAR	29.3	31.7	
1134+61.0	LT	VAR	VAR	29.3	31.7	
1141+45.0	LT	VAR	VAR	29.3	31.7	
CONCORD ROAD						
29+93.0	RT	VAR	VAR	29.3	31.7	
US 67 FRONTAGE ROAD (CONCORD)						
13+50.0	LT	VAR	VAR	29.3	31.7	
25+30.0	LT	VAR	VAR	29.3	31.7	
COOPER STREET						
444+95.0	RT	VAR	VAR	98.5	106.7	
DUTCH LANE EAST						
140+00.0	RT	VAR	VAR	29.3	31.7	
DUTCH LANE WEST						
206+05.0	LT	VAR	VAR	151.1	163.7	
HART'S GRAVEL ROAD						
291+09.0	LT	VAR	VAR	378.3		662.0
292+61.0	LT	VAR	VAR	416.5		728.9
TOTALS				2455.4	1799.0	1390.9
USE				2456	1799	1391

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SCHEDULE OF QUANTITIES
FAP 310 (US 67/IL 104)
SHEET 7 OF 42

DATE 10/10
DRAWN BY EBB
CHECKED BY

FILE \$

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	24
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

PCC PAVEMENT SCHEDULE

STATION TO STATION	SIDE	DESCRIPTION	42000501	42100300	31200100	31000600	31001500	42001200	
			10" PCC PVMT (JOINTED)	10" CRC PVMT	STABILIZED SUBBASE 4"	PROCESS LIME SOIL 12"	LIME	PAVEMENT FABRIC	
			SO YD				TON	SO YD	
FAP 310 (US 67)									
791+07.75	795+40.24	LT	RAMP GORE	576.7		576.7	576.7	11.5	
792+57.68	796+17.68	RT	RAMP GORE	424.0		424.0	424.0	8.5	
817+49.93	821+82.49	RT	RAMP GORE	576.7		576.7	576.7	11.5	
820+36.49	823+96.51	LT	RAMP GORE	424.0		424.0	424.0	8.5	
800+00.00	1140+00.00	LT	EBL	90666.7		90666.7	90666.7	1813.3	
800+00.00	1133+50.00	RT	WBL	88933.3		88933.3	88933.3	1778.7	
852+96.88	860+68.65	RT	TURN LANE	975.1		975.1	975.1	19.5	190.7
859+31.53	865+31.80	LT	TURN LANE	791.3		791.3	791.3	15.8	175.0
859+31.53	866+63.63	LT	TURN LANE	767.9		767.9	767.9	15.4	25.8
880+22.37	888+75.00	RT	TURN LANE	891.9		891.9	891.9	17.8	67.9
880+22.37	888+00.00	RT	TURN LANE	1011.8		1011.8	1011.8	20.2	203.7
886+73.74	894+49.38	LT	TURN LANE	1011.9		1011.9	1011.9	20.2	205.4
885+83.74	894+49.38	LT	TURN LANE	907.7		918.5	918.5	18.4	135.8
922+28.54	928+24.41	RT	TURN LANE	771.5		771.5	771.5	15.4	176.1
926+81.41	932+80.42	LT	TURN LANE	785.6		785.6	785.6	15.7	181.7
948+72.09	948+49.22	RT	TURN LANE	813.0		813.0	813.0	16.3	45.5
948+06.19	955+81.72	RT	TURN LANE	1035.2		1035.2	1035.2	20.7	215.8
954+68.28	962+43.81	LT	TURN LANE	1035.2		1035.2	1035.2	20.7	215.8
954+00.78	961+77.90	LT	TURN LANE	813.0		813.0	813.0	16.3	45.5
970+25.52	976+21.59	RT	TURN LANE	772.7		772.7	772.7	15.5	177.6
974+78.49	980+75.53	LT	TURN LANE	778.1		778.1	778.1	15.6	179.8
991+56.00	999+81.00	RT	TURN LANE	864.1		864.1	864.1	17.3	56.8
992+76.53	999+21.00	RT	TURN LANE	839.8		839.8	839.8	16.8	175.1
997+79.00	1005+45.00	LT	TURN LANE	951.8		951.8	951.8	19.0	175.1
1024+96.62	1032+68.37	RT	TURN LANE	963.4		963.4	963.4	19.3	181.3
1031+07.46	1036+98.84	LT	TURN LANE	763.3		763.3	763.3	15.3	163.4
1030+92.46	1038+86.54	LT	TURN LANE	823.0		823.0	823.0	16.5	48.1
1049+75.00	1055+71.87	RT	TURN LANE	775.3		775.3	775.3	15.5	178.2
1054+28.87	1060+25.00	LT	TURN LANE	775.4		775.4	775.4	15.5	179.2
1070+37.10	1078+21.72	RT	TURN LANE	813.6		813.6	813.6	16.3	47.4
1069+71.19	1077+46.72	RT	TURN LANE	1034.6		1034.6	1034.6	20.7	213.8
1076+33.28	1084+08.81	LT	TURN LANE	1034.6		1034.6	1034.6	20.7	213.8
1075+58.28	1083+42.90	LT	TURN LANE	817.7		817.7	817.7	16.4	68.2
1093+18.92	1100+84.96	RT	TURN LANE	997.1		997.1	997.1	19.9	190.4
1099+69.33	1105+68.16	LT	TURN LANE	813.1		813.1	813.1	16.3	198.6
1099+13.33	1106+89.59	LT	TURN LANE	803.2		803.2	803.2	16.1	44.6
1120+89.03	1126+85.89	RT	TURN LANE	794.1		794.1	794.1	15.9	179.6
1125+55.89	1131+54.97	LT	TURN LANE	794.0		794.0	794.0	15.9	176.6
SUBTOTALS				209421.6	0.0	209432.4	209432.4	4188.6	4732.4

PCC PAVEMENT SCHEDULE - CONTINUED

STATION TO STATION	SIDE	DESCRIPTION	42000501	42100300	31200100	31000600	31001500	42001200	
			10" PCC PVMT (JOINTED)	10" CRC PVMT	STABILIZED SUBBASE 4"	PROCESS LIME SOIL 12"	LIME	PAVEMENT FABRIC	
			SO YD				TON	SO YD	
RAMP A									
0+23.81	1+82.11	LT & RT	RAMP	640.7		640.7	640.7	12.8	374.1
1+82.11	17+65.55	LT	RAMP	2815.0		2815.0	2815.0	56.3	
17+65.55	26+15.60	LT	RAMP	897.3		897.3	897.3	17.9	
RAMP B									
0+00.00	2+99.57	LT	RAMP	299.6		299.6	299.6	6.0	
2+99.57	17+94.45	LT	RAMP	2657.6		2657.6	2657.6	53.2	
17+94.45	19+22.62	LT & RT	RAMP	589.3		628.1	628.1	12.6	403.0
RAMP C									
0+26.56	2+23.51	LT & RT	RAMP	844.2		844.2	844.2	16.9	524.1
2+23.51	15+32.98	LT	RAMP	2327.9		2327.9	2327.9	46.6	
15+32.98	23+82.96	LT	RAMP	897.2		897.2	897.2	17.9	
RAMP D									
0+00.00	2+99.57	LT	RAMP	299.6		299.6	299.6	6.0	
2+99.57	19+03.51	LT	RAMP	2851.4		2851.4	2851.4	57.0	
19+03.51	19+91.96	LT & RT	RAMP	448.1		468.2	468.2	9.4	308.7
CONCORD ROAD									
8+03.45	17+69.00	LT		1502.0		1502.0	1502.0	30.0	
8+03.45	17+69.00	RT		1502.0		1502.0	1502.0	30.0	
14+60.45	17+69.00	LT & RT		445.5		445.5	445.5	8.9	
17+69.00	18+69.00	LT			381.4	381.4	381.4	7.6	
17+69.00	18+69.00	RT			248.1	248.1	248.1	5.0	
21+31.00	22+31.00	LT			248.1	248.1	248.1	5.0	
21+31.00	22+31.00	RT			381.4	381.4	381.4	7.6	
22+31.00	29+00.00	LT		1040.7		1040.7	1040.7	20.8	
22+31.00	29+00.00	RT		1040.7		1040.7	1040.7	20.8	
22+31.00	25+40.45	LT & RT		449.9		449.9	449.9	9.0	
SERVICE DRIVE 81									
580+61.94	581+10.20	LT & RT	INTERSECTION	248.9		248.9	248.9	5.0	248.9
COOPER STREET									
448+56.24	449+37.62	LT & RT	INTERSECTION	583.9		613.5	613.5	12.3	583.9
450+62.38	451+07.48	LT & RT	INTERSECTION	269.3		269.3	269.3	5.4	269.3
BETHEL LANE									
78+70.55	79+36.85	LT & RT	INTERSECTION	378.4		378.4	378.4	7.6	378.4
80+63.16	81+29.46	LT & RT	INTERSECTION	378.4		378.4	378.4	7.6	378.4
ST. PAUL'S CHURCH ROAD									
120+61.00	121+27.00	LT & RT	INTERSECTION	460.8		460.8	460.8	9.2	460.8
DUTCH LANE EAST									
148+79.83	149+36.82	LT & RT	INTERSECTION	356.3		356.3	356.3	7.1	356.3
DUTCH LANE WEST									
198+69.77	199+36.85	LT & RT	INTERSECTION	379.1		379.1	379.1	7.6	370.3
200+63.15	201+29.19	LT & RT	INTERSECTION	370.3		370.3	370.3	7.4	379.1
HART'S GRAVEL ROAD									
298+78.15	299+38.05	LT & RT	INTERSECTION	346.6		346.6	346.6	6.9	346.6
TOTALS				234742.3	1259.1	236100.7	236100.7	4722.1	10114.4
USE				234743	1260	236101	236101	4723	10115

PROTECTIVE COAT

PAY ITEM NO.	DESCRIPTION	42001300
42000501	PCC PAVEMENT 10" (JOINTED)	234743
42100300	CRC PAVEMENT 10"	1260
42300200	PCC DRIVEWAY PAVEMENT 6 INCH	108
42300400	PCC DRIVEWAY PAVEMENT 8 INCH	381
42400100	PCC SIDEWALK 4 INCH	538
48300500	PCC SHOULDER 10"	111380
60608600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06	3
60610400	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24	15
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	1928
60624600	CORRUGATED MEDIAN	79
X6060424	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-4.24	143
X6063600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24	16
X6064201	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06	333
X6065701	CONCRETE MEDIAN, TYPE SM-4.06	518
TOTAL		351445

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 8 OF 42

DATE 10/10
 DRAWN BY EBB
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	25
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

PCC SHOULDER SCHEDULE

STATION TO STATION	SIDE	WIDTH	48300500		31200100		31101900		31000600		31001500	
			10" PCC SHLD	STABILIZED SUBBASE 4"	SUB-BASE GRAN MTL TYPE C	PROCESS LIME SOIL 12"	LIME					
			SO YD	TON	TON	SO YD	TON					
FAP 310 (US 67)												
762+05.00	770+77.08	RT IS	4.0	387.6		103.1						
770+00.00	777+86.60	LT IS	4.0	349.6		93.6						
770+77.08	775+90.00	RT IS	4.0	228.0		194.7						
774+48.14	782+57.56	LT OS	10.0	899.4		218.6						
777+86.60	780+04.95	LT IS	4.0	97.0		82.9						
786+50.00	789+57.68	RT OS	10.0	341.9		83.1						
795+40.24	820+36.49	LT OS	10.0	2773.6	416.0	579.3	3189.7	63.8				
796+17.68	817+49.93	RT OS	10.0	2369.2	355.4	494.9	2724.5	54.5				
800+00.00	859+57.42	LT IS	4.0	2647.7	992.9	478.0	3640.6	72.8				
800+00.00	859+57.42	RT IS	4.0	2647.7	992.9	478.0	3640.6	72.8				
826+96.51	859+31.53	LT OS	10.0	3594.5	539.2	750.8	4133.6	82.7				
830+32.62	880+22.37	RT OS	10.0	5544.2	831.6	1158.0	6375.8	127.5				
860+43.18	886+87.64	LT IS	4.0	1175.3	440.7	212.2	1616.1	32.3				
860+43.18	886+87.64	RT IS	4.0	1175.3	440.7	212.2	1616.1	32.3				
860+44.63	865+16.22	LT OS	4.0	209.6	78.6	37.8	288.2	5.8				
865+16.22	866+63.63	LT OS	7.0	114.7	24.6	23.0	139.2	2.8				
866+63.63	885+83.74	LT OS	10.0	2133.5	320.0	445.6	2453.5	49.1				
880+22.37	881+69.87	RT OS	7.0	114.7	24.6	23.0	139.2	2.8				
881+69.87	886+98.28	RT OS	4.0	234.8	88.1	42.4	322.9	6.5				
885+83.74	886+32.86	LT OS	7.0	38.2	8.2	7.7	46.4	0.9				
887+84.11	927+12.06	LT IS	4.0	1745.8	654.7	315.1	2400.4	48.0				
887+84.11	927+12.06	RT IS	4.0	1745.8	654.7	315.1	2400.4	48.0				
887+88.00	893+62.92	LT OS	4.0	255.5	95.8	46.1	351.3	7.0				
888+15.60	888+75.00	RT OS	6.0	39.6	9.9	7.8	49.5	0.99				
888+75.00	948+72.09	RT OS	10.0	6663.4	999.5	1391.8	7662.9	153.3				
893+62.92	894+49.38	LT OS	7.0	67.2	14.4	13.5	81.7	1.6				
894+49.38	954+00.78	LT OS	10.0	6612.7	991.9	1381.2	7604.6	152.1				
927+94.13	954+73.21	LT IS	4.0	1190.7	446.5	214.9	1637.2	32.7				
927+94.13	954+73.21	RT IS	4.0	1190.7	446.5	214.9	1637.2	32.7				
948+72.09	950+19.52	RT OS	7.0	114.7	24.6	23.0	139.2	2.8				
950+19.52	954+95.20	RT OS	4.0	211.4	79.3	38.2	290.7	5.8				
954+00.78	954+45.61	LT OS	7.0	34.9	7.5	7.0	42.3	0.8				
955+54.73	960+30.48	LT OS	4.0	211.4	79.3	38.2	290.7	5.8				
955+76.79	975+09.04	LT IS	4.0	858.8	322.0	155.0	1180.8	23.6				
955+76.79	975+09.04	RT IS	4.0	858.8	322.0	155.0	1180.8	23.6				
956+04.39	956+49.22	RT OS	6.0	29.9	7.5	5.9	37.4	0.75				
956+49.22	991+56.00	RT OS	10.0	3896.4	584.5	813.9	4480.9	89.6				
960+30.48	961+77.90	LT OS	7.0	114.7	24.6	23.0	139.2	2.8				
961+77.90	1030+92.46	LT OS	10.0	7682.8	1152.4	1604.7	8835.3	176.7				
975+91.00	998+09.02	LT IS	4.0	985.8	369.7	177.9	1355.5	27.1				
975+91.00	998+09.02	RT IS	4.0	985.8	369.7	177.9	1355.5	27.1				
991+56.00	993+03.42	RT OS	7.0	114.7	24.6	23.0	139.2	2.8				
993+03.42	997+79.00	RT OS	4.0	211.4	79.3	38.2	290.6	5.8				
998+90.98	1031+41.69	LT IS	4.0	1444.8	541.8	260.8	1986.5	39.7				
998+90.98	1031+41.69	RT IS	4.0	1444.8	541.8	260.8	1986.5	39.7				
999+08.07	999+81.00	RT OS	7.0	56.7	12.2	11.4	68.9	1.4				
999+81.00	1070+37.10	RT OS	10.0	7840.1	1176.0	1637.6	9016.1	180.3				
1030+92.46	1031+57.85	LT OS	7.0	50.9	10.9	10.2	61.8	1.2				
1032+28.61	1054+59.02	LT IS	4.0	991.3	371.7	178.9	1363.0	27.3				
1032+28.61	1054+59.02	RT IS	4.0	991.3	371.7	178.9	1363.0	27.3				
1032+68.37	1037+39.04	LT OS	4.0	209.2	78.4	37.8	287.6	5.8				
1037+39.04	1038+86.54	LT OS	7.0	114.7	24.6	23.0	139.2	2.8				
1038+86.54	1075+58.28	LT OS	10.0	4079.7	612.0	852.1	4691.7	93.8				
1055+40.98	1076+38.21	LT IS	4.0	932.1	349.5	168.3	1281.6	25.6				
1055+40.98	1076+38.21	RT IS	4.0	932.1	349.5	168.3	1281.6	25.6				
1070+37.10	1071+84.52	RT OS	7.0	114.7	24.6	23.0	139.2	2.8				
1071+84.52	1076+58.13	RT OS	4.0	210.5	78.9	38.0	289.4	5.8				
1075+58.28	1076+12.04	LT OS	7.0	41.8	9.0	8.4	50.8	1.0				
1077+21.87	1081+95.49	LT OS	4.0	210.5	78.9	38.0	289.4	5.8				
1077+41.79	1099+83.46	LT IS	4.0	996.3	373.6	179.8	1369.9	27.4				
1077+41.79	1099+83.46	RT IS	4.0	996.3	373.6	179.8	1369.9	27.4				
1077+68.36	1078+21.72	RT OS	7.0	41.5	8.9	8.3	50.4	1.0				
1078+21.72	1133+50.00	RT OS	10.0	6142.5	921.4	1283.0	7063.9	141.3				
1081+95.49	1083+42.90	LT OS	7.0	114.7	24.6	23.0	139.2	2.8				
1083+42.90	1099+13.33	LT OS	10.0	1744.9	261.7	364.5	2006.7	40.1				
1099+13.33	1099+61.57	LT OS	7.0	37.5	8.0	7.5	45.6	0.9				
1100+69.96	1105+42.09	LT OS	4.0	209.8	78.7	37.9	288.5	5.8				
1100+75.17	1125+79.18	LT IS	4.0	1112.9	417.3	200.9	1530.2	30.6				
1100+75.17	1125+79.18	RT IS	4.0	1112.9	417.3	200.9	1530.2	30.6				
1105+42.09	1106+89.59	LT OS	7.0	114.7	24.6	23.0	139.2	2.8				
1106+89.59	1140+00.00	LT OS	10.0	3678.2	551.7	768.3	4230.0	84.6				
1126+64.82	1129+34.97	LT IS	4.0	120.1	45.0	21.7	165.1	3.3				
1126+64.82	1127+06.07	RT IS	4.0	18.3	6.9	3.3	25.2	0.5				
1129+34.97	1130+05.15	LT IS					42.9	0.9				
1131+62.23	1133+50.00	RT IS	4.0	83.5	31.3	15.1	114.7	2.3				
1138+12.30	1140+00.00	LT IS					114.7	2.3				
SUBTOTALS				99088.4	21490.5	20112.8	118433.1	2368.7				

PCC SHOULDER SCHEDULE - CONTINUED

STATION TO STATION	SIDE	WIDTH	48300500		31200100		31101900		31000600		31001500	
			10" PCC SHLD	STABILIZED SUBBASE 4"	SUB-BASE GRAN MTL TYPE C	PROCESS LIME SOIL 12"	LIME					
			SO YD	TON	TON	SO YD	TON					
RAMP A												
0+23.81	1+36.71	RT	VAR	146.0	25.4	29.7	142.4	2.8				
0+23.81	2+08.38	LT	VAR	81.8	30.3	14.7	109.5	2.2				
1+36.71	24+65.52	RT	6.0	1552.5	388.1	304.7	1940.7	38.8				
2+08.38	13+35.60	LT	4.0	501.0	187.9	90.4	688.9	13.8				
24+65.52	26+15.60	RT	7.5	125.1	25.0	25.3	150.1	3.0				
RAMP B												
0+00.00	0+56.40	RT	7.5	47.0	9.4	9.5	56.4	1.1				
0+56.40	18+48.72	RT	6.0	1194.9	298.7	234.5	1493.6	29.9				
6+60.09	17+94.45	LT	4.0	504.2	189.1	91.0	693.2	13.9				
17+94.45	19+22.62	LT	4.0	45.7	17.6	8.0	61.5	1.2				
18+48.72	19+22.62	RT	VAR	90.3	22.3	17.7	111.5	2.2				
RAMP C												
0+26.56	1+15.63	RT	VAR	106.9	17.9	21.9	123.1	2.5				
0+26.56	2+53.83	LT	VAR	93.2	34.2	16.8	125.7	2.5				
1+15.63	22+32.93	RT	6.0	1411.5	352.9	277.0	1764.4	35.3				
2+53.83	11+02.96	LT	4.0	377.4	141.5	68.1	518.9	10.4				
22+32.93	23+82.96	RT	7.5	125.0	25.0	25.3	150.0	3.0				
RAMP D												
0+00.00	0+56.43	RT	7.5	47.0	9.4	9.5	56.4	1.1				
0+56.43	19+03.51	RT	6.0	1231.4	307.8	241.7	1539.2	30.8				
6+60.11	19+03.51	LT	4.0	552.6	207.2	99.8	759.9	15.2				
19+03.51	19+91.96	RT	VAR	98.8	24.7	19.3	122.2	2.4				
19+03.51	19+91.96	LT	4.0	35.4	13.7	6.2	47.6	1.0				
CONCORD ROAD												
8+03.45	13+92.97	RT	10.0	655.0	98.3	136.8	753.3	15.1				
8+03.45	13+70.45	LT	10.0	630.0	94.5	131.6	724.5	14.5				
15+13.97	17+69.00	LT	10.0	283.4	42.5	59.2	325.9	6.5				
15+20.45	17+69.00	RT	10.0	276.2	41.4	57.7	317.6	6.4				
22+31.00	24+90.36	RT	10.0	288.2	43.2	60.2	331.4	6.6				
22+31.00	24+77.09	LT	10.0	273.4	41.0	57.1	314.4	6.3				

3:57:12 PM

Mar-25-2011 03:57:12PM

\$FILE\$

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	27

STA.	TO STA.

FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT
40200700	40600200	40300300

HMA/AGG PAVEMENT SCHEDULE - CONTINUED

STATION TO STATION	SIDE	WIDTH	AREA	40603310	40603340	40603510	40603540	40603080	40603085	40603230	40600625	40600635	40600825	40600837	35501316	35501320	35501332	35600708	35600712	31000600	31001500	35100700	40200700	40600200	40300300					
				HMA SURFACE COURSE				HMA BINDER COURSE				HMA LEVELING BINDER (MACHING METHOD)				HMA BASE COURSE				LIME	LIME	AGG BSE	AGG SURF	BITUMINOUS	AGGREGATE					
				MIX "C" N50	MIX "D" N70	MIX "C" N50	MIX "D" N70	N50	N70	POLYMER N50	POLYMER N70	8"	9"	12"	8"	9"	SOIL 12"	TON	TON	SOI YD	TON	SOI YD	TON							
ASH STREET																														
2+50.00	4+00.00	LT & RT	22	366.7	30.8			51.3																388.9	0.69	0.7				
4+00.00	8+73.10	LT & RT	VAR	1235.6	103.8						51.9															0.94	2.5			
8+73.10	9+87.60	LT & RT	VAR	580.8	48.8						24.4															0.44	1.2			
8+73.10	9+87.60	LT																								92.0				
9+17.51	9+87.60	RT																								67.3				
US 67 FRONTAGE ROAD (BETHEL)																														
510+25.39	510+85.39	LT & RT	VAR	331.7	37.1			46.4																			0.25	0.7		
510+85.39	520+14.60	LT & RT	24	2477.9	277.5			346.9																			1.88	5.0		
520+14.60	524+68.00	LT & RT	24	1209.1	135.4							101.6															0.92	2.4		
BETHEL LANE																														
72+70.40	73+20.40	LT & RT	20	111.1	9.3			15.6																			0.08	0.2		
73+20.40	78+25.62	LT & RT	24	1347.3	113.2			188.6																			1.02	2.7		
78+25.62	78+70.55	LT & RT	25.75	128.5	10.8			18.0																			0.10	0.3		
81+29.46	81+74.39	LT & RT	25.75	128.5	10.8			18.0																			131.9	0.24	0.3	
81+74.39	91+00.00	LT & RT	24	2468.3	207.3			345.6																			2536.9	4.55	4.9	
91+00.00	91+50.00	LT & RT	19.5	108.3	9.1			15.2																			112.0	0.20	0.2	
SERVICE DRIVE 91																														
591+29.03	591+71.17	LT & RT	VAR	186.8	15.7			26.2																			191.7	0.34		
591+71.17	593+85.00	LT & RT	22	522.7	43.9			73.2																			538.5	0.97		
RELOCATED CYLINDER HEAD ROAD																														
93+83.00	94+33.00	LT & RT	20	111.1	9.3			15.6																			114.8	0.21	0.2	
94+33.00	109+28.00	LT & RT	22	3654.4	307.0			511.6																			3765.2	6.75	7.3	
109+28.00	109+88.00	LT & RT	VAR	318.4	26.7			44.6																			325.3	0.24	0.6	
OLD CYLINDER HEAD ROAD																														
0+11.00	0+67.57	LT & RT	VAR	283.7	23.8			39.7																			290.2	0.22	0.6	
0+67.57	7+50.00	LT & RT	22	1668.2	140.1			233.5																			1718.7	3.08	3.3	
GILLILAND ROAD CONNECTOR																														
10+11.00	10+66.06	LT & RT	VAR	250.0	21.0			35.0																			256.3	0.46	0.5	
10+66.06	12+68.74	LT & RT	22	495.4	41.6			69.4																			510.5	0.92	1.0	
12+68.74	13+08.93	LT & RT	VAR	174.5	14.7			24.4																			179.2	0.32	0.3	
GILLILAND ROAD EXTENSION																														
20+75.00	21+25.00	LT & RT	14	77.8	6.5			10.9																			81.5	0.15	0.2	
21+25.00	27+16.17	LT & RT	18	1182.3	99.3			165.5																			1226.1	2.20	2.4	
27+16.17	27+95.11	LT & RT	VAR	269.0	22.6			37.7																			276.5	0.50	0.5	
SERVICE DRIVE 93																														
610+59.00	611+62.00	LT & RT																										249.7		
611+62.00	614+34.69	LT & RT																										484.8		
ST. PAUL'S CHURCH ROAD																														
121+27.00	122+18.40	LT & RT	27.25	276.7	23.2			38.7																			283.5	0.51	0.6	
122+18.40	127+33.00	LT & RT	22	1257.9	105.7			176.1																			1296.0	2.32	2.5	
127+33.00	127+83.00	LT & RT	21.25	118.1	9.9			16.5																			121.8	0.22	0.2	
SERVICE DRIVE 92																														
600+11.00	600+53.56	LT & RT	VAR	177.4	14.9			24.8																			182.3	0.33		
600+53.56	603+75.81	LT & RT	20	716.1	60.2			100.3																			740.0	1.33		
DUTCH LANE EAST																														
138+04.00	138+54.00	LT & RT	21	116.7	9.8			16.3																			120.4	0.22	0.2	
138+54.00	147+88.08	LT & RT	22	2283.3	191.8			319.7																			2352.5	4.22	4.6	
147+88.08	148+79.83	LT & RT	29.25	298.2	25.0			41.7																			305.0	0.55	0.6	
DUTCH LANE WEST																														
194+41.00	194+91.00	LT & RT	16	88.9	7.5			12.4																			92.6	0.17	0.2	
194+91.00	198+22.44	LT & RT	22	810.2	68.1			113.4																			834.7	1.50	1.6	
198+22.44	198+69.77	LT & RT	24	126.2	10.6			17.7																			129.7	0.23	0.3	
201+29.19	201+77.56	LT & RT	24	129.0	10.8			18.1																			132.6	0.24	0.3	
201+77.56	211+00.00	LT & RT	22	2254.9	189.4			315.7																			2323.2	4.17	4.5	
211+00.00	211+50.00	LT & RT	18	100.0	8.4			14.0																			103.7	0.19	0.2	
HART'S GRAVEL ROAD																														
289+67.00	290+17.00	LT & RT	19	105.6	8.9			14.8																			109.3	0.20	0.2	
290+17.00	298+17.83	LT & RT	22	1957.6	164.4			274.1																			2016.9	3.62	3.9	
298+17.83	298+78.15	LT & RT	25.25	169.2	14.2			23.7																			173.7	0.31	0.3	
PRIVATE DRIVE 81A																														
100+11.55	100+60.23	LT & RT	VAR																									328.6		
100+60.23	101+37.75	LT & RT	26																									223.9		
101+37.75	102+99.78	LT & RT	22																									396.1		
102+99.78	103+87.97	LT & RT	19																									186.2		
TOTALS					6992.3	618.7	278.2	843.6	5463.9	1432.6	195.3	2250.8	343.1	5																

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	28
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

HMA SHOULDER SCHEDULE

48203029 48203100 31101900 31000600 31001500

STATION TO STATION	SIDE	WIDTH	HMA SHLD 8"	HMA SHLD	SUB-BASE GRAN MTL TYPE C	PROCESS LIME SOIL 12"	LIME
			SO YD	TON		SO YD	TON
TEMPORARY ACCESS ROAD							
26+00.00	26+50.00	RT	1-10	30.6			
26+50.00	28+05.33	RT	10.0	172.6			
26+00.00	26+50.00	LT	1-8	25.0			
26+50.00	28+05.33	LT	8.0	138.1			
28+51.00	28+93.00	LT	4.0	18.7			
28+51.00	28+93.00	RT	4.0	18.7			
29+38.50	30+12.62	RT	4.0	65.3			
29+38.50	30+15.23	LT	4.0	41.8			
CONCORD ROAD							
7+72.22	8+03.50	LT	4-10	30.1	32.1	134.1	2.7
7+60.23	8+03.50	RT	4-10	43.4	22.5	89.1	1.8
29+00.00	34+34.00	LT	10.0	593.3	185.8	682.3	13.6
29+00.00	33+40.00	RT	10.0	488.9	153.1	562.3	11.2
34+34.00	34+84.00	LT	1-10	30.6			
33+40.00	33+90.00	RT	1-10	30.6			
EX US 67 / IL 104							
90+86.00	97+85.00	LT	3.0		19.6		
90+86.00	97+85.00	RT	3.0		19.6		
578+95.00	589+54.25	RT	3.0		44.5		
0+00.00	3+00.00	RT	3.0		12.6		
578+95.00	589+54.25	RT	3.0		44.5		
0+00.00	3+00.00	RT	3.0		12.6		
556+00.00	573+75.00	LT	3.0		74.6		
556+00.00	561+42.18	RT	3.0		22.8		
561+81.64	563+49.74	RT	3.0		7.1		
565+23.28	573+75.00	RT	3.0		35.8		
US 67 FRONTAGE ROAD (CONCORD)							
1+23.33	29+07.00	RT	3.0	927.9			
1+23.33	54+20.00	LT	3.0	1765.6			
29+07.00	29+67.00	RT	4.0	32.3			
30+33.00	31+91.00	RT	4.0	76.2			
31+91.00	54+20.00	RT	3.0	743.0			
SERVICE DRIVE 81							
581+10.20	581+81.81	LT	4.0	43.8			
581+10.20	581+81.81	RT	4.0	41.4			
COOPER STREET							
442+65.00	443+46.83	LT	4.0	36.4			
442+65.00	448+56.24	RT	4.0	263.9			
444+90.85	448+56.24	LT	4.0	162.4			
451+07.48	451+84.07	LT	4.0	41.3			
451+07.48	451+53.10	RT	4.0	28.0			
NEW STREET							
300+10.86	300+73.17	RT	4.0	41.1			
300+13.18	300+73.17	LT	4.0	40.9			
312+63.32	313+24.88	LT	4.0	42.5			
312+63.32	313+23.28	RT	4.0	42.0			
ASH STREET							
8+72.13	9+93.93	LT	4.0	69.5			
9+17.51	9+74.92	RT	4.0	48.0			
US 67 FRONTAGE ROAD (BETHEL)							
510+25.39	510+95.39	RT	4.0	44.4			
510+25.39	510+95.39	LT	4.0	44.4			
510+95.39	524+68.00	RT	3.0	457.5			
510+95.39	524+68.00	LT	3.0	457.5			
BETHEL LANE							
72+70.40	73+20.40	RT	4.0	22.2			
72+70.40	73+21.40	LT	4.0	22.7			
74+63.40	78+70.55	LT	4.0	181.0			
74+64.41	78+70.55	RT	4.0	180.5			
OLD CYLINDER HEAD ROAD							
0+07.85	1+84.43	LT	4.0	107.7			
0+07.85	0+67.57	RT	4.0	38.0			
TOTALS				7729.4	293.5	393.5	1467.8 29.4
USE				7730	294	394	1468 30

CONCRETE MEDIAN / CURB & GUTTER SCHEDULE

X6060424 X6063600 X6064201 60608600 60610400 X6065701 60624600 60618300 31200100 31101900 31000600 31001500

STATION TO STATION	SIDE	COMB CONCRETE CURB & GUTTER					MEDIAN TYPE SM-4.06	CORRUGATED MEDIAN	MEDIAN SURFACE 4 INCH	STABILIZED SUBBASE 4"	SUB-BASE GRAN MTL TYPE C	PROCESS LIME SOIL 12"	LIME TON
		TYPE B-4.24	TYPE M-4.24	TYPE M-4.06	TYPE M-6.06	TYPE M-6.24							
CONCORD ROAD													
7+70.55	7+80.41	LT & RT					85.4					9.5	0.2
7+80.41	8+03.45	LT			23.5							6.7	0.1
7+80.41	8+03.45	RT			23.5							6.7	0.1
7+80.41	8+03.45	LT & RT								318.6		27.2	
8+03.45	14+78.30	RT			674.9					156.2		193.7	3.9
8+03.45	14+78.30	LT			675.8					156.4		194.0	3.9
8+03.45	14+78.30	LT & RT								10617.1		940.5	
14+78.30	14+93.92	LT & RT					136.3			15.1		15.1	0.3
15+77.94	18+69.00	RT					1746.4			194.0		194.0	3.9
17+59.00	18+69.00	LT			110.0					39.8		45.3	0.9
17+59.00	18+69.00	RT			110.0					39.8		45.3	0.9
21+31.00	22+41.00	LT			110.0					39.8		45.3	0.9
21+31.00	22+41.00	RT			110.0					39.8		45.3	0.9
21+31.00	24+20.13	LT					1734.8			192.8		192.8	3.9
25+07.65	25+23.26	LT & RT					136.3			15.1		15.1	0.3
25+23.26	29+00.00	LT			376.7					87.2		108.1	2.2
25+23.26	29+00.00	RT			378.0					87.5		108.5	2.2
25+23.26	29+00.00	LT & RT							5770.2		511.1		
29+00.00	29+14.26	LT & RT							177.4		15.2		
29+00.00	29+14.26	LT			14.3							4.1	0.1
29+00.00	29+14.26	RT			14.3							4.1	0.1
29+14.26	29+98.00	LT & RT					814.3					90.5	1.8
29+75.17	31+29.00	LT & RT						705.3					
RAMP B													
18+78.77	19+07.83	LT			29.6	38.7				208.6		16.5	
RAMP D													
19+66.82	19+84.27	LT & RT			20.5	22.6				82.6		7.6	
COOPER STREET													
449+13.28	449+38.72	RT				17.6	43.5			176.0		11.1	
TOTALS			440.0	50.1	2242.3	17.6	43.5	4653.4	705.3	17350.5	1063.6	1529.2	1324.4 26.5
USE			440	51	2243	18	44	4654	706	17351	1064	1530	1325 27

* INCLUDED IN PCC PAVEMENT SCHEDULE.

PAVEMENT PATCHING SCHEDULE

44200202 44200204 44200206

STATION	SIDE	LENGTH	WIDTH	TYPE II 17 INCH	TYPE III 17 INCH	TYPE IV 17 INCH
				SO YD		
EX CONCORD ROAD						
13+20.0	LT	11.4	7.5	9.5		
13+20.0	RT	11.7	7.5	9.8		
EX US 67/IL 104						
570+05.0	LT & RT	30.2	7.7			25.8
563+50.8	LT & RT	30.2	6.0		20.1	
316+53.0	LT	17.8	6.5	12.9		35.8
316+53.0	RT	14.8	6.5	10.7		33.6
COOPER STREET						
444+96.0	LT & RT	32.3	16.5			73.4
TOTALS				42.8	20.1	168.7
USE				43	21	169

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 12 OF 42

DATE 10/10
 DRAWN BY EBB
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	29

ENTRANCE SCHEDULE

STATION	SIDE	TYPE	WIDTH	LENGTH	AREA (SQ YD)	35101400	40200100	42300200	42300400	40800050
						AGGREGATE BASE COURSE TYPE A	AGGREGATE SURF COURSE TYPE A	6"	8"	INCIDENTAL HMA SURFACING
						TON		SO YD		TON
FAP 310 (US 67)										
846+00.00	RT	FE	22	65.0	161.7		55.2			
860+04.00	RT	FE	22	127.4	566.5		193.6			
873+90.00	RT	FE	22	94.0	235.6		80.5			
912+90.00	RT	FE	22	80.0	198.3		67.8			
927+52.00	RT	FE	22	127.8	530.7		181.3			
941+50.00	RT	FE	22	59.0	147.0		50.2			
975+50.00	RT	FE	22	80.0	198.4		67.8			
998+50.00	LT	FE	22	93.7	231.6		79.1			
1007+45.00	LT	FE	22	60.0	149.5		51.1			
1015+10.00	RT	PE	22	108.0	267.5	91.4				52.4
1023+40.00	RT	FE	22	101.0	249.6		85.3			
1024+35.00	LT	FE	22	111.3	490.3		167.5			
1031+83.00	RT	PE/FE	22	VAR	378.2		129.2			
1048+00.00	RT	FE	22	60.0	149.5		51.1			
1054+95.00	LT	FE	22	131.0	538.7		184.0			
1054+95.00	RT	FE	22	140.0	656.5		224.3			
1087+00.00	RT	FE	22	115.0	283.9		97.0			
1100+40.00	RT	FE	22	107.6	266.0		90.9			
1105+10.00	LT	FE	22	112.3	278.3		95.1			
1111+25.00	LT	FE	22	70.0	174.1		59.5			
1119+00.00	LT	FE	22	150.0	369.6		126.3			
1119+70.00	RT	FE	22	110.0	470.0		160.6			
1126+15.00	LT	PE	22	216.8	532.0	181.8				104.3
1126+35.00	RT	PE/FE	22	110.0	512.3		175.1			
1133+55.00	LT	PE	22	122.7	302.6		103.4			
1136+50.00	RT	FE	22	135.7	563.4		192.5			
1139+50.00	LT	FE	22	120.0	296.1		101.2			
TRANSITION A										
5+23.40	RT	FE	22	79.0	217.7		74.4			
EX US 67 / IL 104										
88+04.00	RT	PE	22	24.0	74.7		25.5			4.5
43+10.00	RT	FE	22	24.1	75.5		25.8			
1+87.37	RT	FE	22	24.0	75.0		25.6			
0+70.00	RT	PE/MB	40	24.0	135.1	46.2				26.5
589+37.95	RT	FE	22	23.8	74.0		25.3			
580+00.00	RT	CE	35	18.2	86.8			86.8		
579+18.00	RT	CE	35	16.6	72.4			72.4		
573+32.06	RT	FE	22	29.0	86.5		29.5			
561+55.70	RT	OAK ST	28.6	10.0	40.0					2.8
534+40.00	RT	FE	22	25.2	78.1		26.7			
TEMPORARY ACCESS ROAD										
1+49.16	RT	PE	16	8.0	16.0		5.5			
3+43.00	RT	FE	16	24.8	69.1		23.6			
CONCORD ROAD										
31+60.00	RT	FE	22	38.0	95.5		32.6			
32+08.00	RT	PE	16	37.0	68.6		23.4			
33+50.00	RT	PE	16	37.0	68.6		23.4			
34+43.00	LT	FE	22	38.6	100.8		34.5			
36+17.00	LT	PE/MB	22	18.4	89.6		30.6			11.6
US 67 FRONTAGE ROAD										
11+20.50	LT	PE/MB	22	44.0	137.3		46.9			7.1
13+90.00	LT	PE	22	47.0	133.2		45.5			4.6
20+00.00	LT	FE	22	45.0	126.0		43.1			
20+00.00	RT	FE	22	44.0	123.6		42.2			
39+00.00	LT	FE	22	54.0	147.8		50.5			
39+00.00	RT	FE	22	54.0	148.2		50.7			
41+41.03	LT	PE	22	VAR	256.3		87.6			4.5
COOPER STREET										
444+19.00	RT	PE	12	14.0	19.7			19.7		
445+69.00	RT	CE	35	50.7	221.3				221.3	
NEW STREET										
310+50.00	LT	FE	22	22.0	78.8		26.9			
310+50.00	RT	FE	22	47.0	139.9		47.8			
ASH STREET										
2+92.76	LT	PE	16	18.0	43.0			43.0		
4+76.00	LT	PE	25	4.0	13.2					2.6
5+12.62	LT	PE	17	17.3	39.9					7.8
6+20.43	LT	PE	19.5	4.0	10.2					0.7
7+36.77	LT	PE	16	16.4	29.2					5.7
8+20.08	LT	PE	19.2	4.0	9.4					0.7
SERVICE DRIVE 91										
592+82.00	RT	PE	18	14.6	44.4			44.4		
593+80.00	RT	PE	16	131.5	287.7		98.3			8.7
SUBTOTALS						319.4	3815.3	107.1	380.5	244.5

ENTRANCE SCHEDULE - CONTINUED

STATION	SIDE	TYPE	WIDTH	LENGTH	AREA (SQ YD)	35101400	40200100	42300200	42300400	40800050
						AGGREGATE BASE COURSE TYPE A	AGGREGATE SURF COURSE TYPE A	6"	8"	INCIDENTAL HMA SURFACING
						TON		SO YD		TON
BETHEL ROAD										
72+75.00	RT	FE	22	8.0	31.3		10.7			
75+15.00	RT	CE	35	17.0	68.9		31.4			15.2
76+20.00	RT	CE	35	17.0	68.9		31.4			15.3
77+48.00	LT	PE/MB	24	76.0	213.7		73.0			5.3
87+00.00	LT	FE	22	25.4	87.1		29.7			
87+00.00	RT	FE	22	26.0	88.6		30.3			
88+74.70	RT	PE/MB	16	26.0	86.2	29.5				16.9
89+46.40	RT	PE/MB	16	26.0	86.2	29.5				16.9
91+12.00	RT	FE	35	27.4	130.3		44.5			
US 67 FRONTAGE ROAD (BETHEL)										
523+75.00	RT	FE	22	24.0	74.7		25.5			
524+38.00	RT	PE/MB	16	24.0	68.2		23.3			5.7
RELOCATED CYLINDER HEAD ROAD										
94+67.00	LT	FE	22	23.3	82.0		28.0			
95+14.00	LT	PE	12	28.0	54.0		18.4			4.5
95+33.00	LT	PE/MB	16	28.0	86.8		29.6			9.2
96+22.80	LT	PE/MB	24	38.0	150.4		51.4			12.7
97+20.00	LT	PE	16	46.6	107.7		36.8			6.6
98+50.00	LT	PE/MB	16	97.0	217.7		74.4			10.6
99+75.00	RT	FE	22	33.0	105.8		36.1			
106+00.00	LT	FE	22	28.0	93.6		32.0			
106+00.00	RT	FE	22	33.0	105.6		36.1			
OLD CYLINDER HEAD ROAD										
3+50.00	RT	CE	35	13.9	55.5		25.3			22.4
4+00.00	RT	CE	35	14.4	56.9		25.9			20.8
4+20.00	LT	PE	16	18.0	57.0		19.5			6.6
4+50.00	RT	PE/MB	16	16.9	35.4		16.1			20.7
5+00.00	LT	PE	16	18.0	57.0		19.5			6.6
5+63.00	RT	PE/MB	16	23.7	87.8		30.0			10.7
6+50.00	RT	PE/MB	16	32.7	85.3		29.1			7.1
6+95.00	RT	PE/MB	16	38.2	95.3		32.6			7.1
GILLILAND ROAD CONNECTOR										
11+00.00	RT	CE	30	17.0	59.4		27.1			24.9
12+25.00	RT	CE	30	24.0	82.8		37.7			24.9
12+53.00	LT	PE/MB	16	28.0	92.0		31.4			10.0
GILLILAND ROAD EXTENSION										
21+55.00	LT	PE/MB	16	32.2	102.7		35.1			10.6
23+82.00	RT	CE	20	12.8	31.0		14.1			20.1
SERVICE DRIVE 93										
611+36.00	LT	FE	22	69.2	181.6		62.1			
612+44.00	LT	FE	22	44.6	133.9		45.8			
ST. PAUL'S CHURCH ROAD										
124+00.00	LT	FE	22	38.0	118.0		40.3			
SERVICE DRIVE 92										
601+20.00	LT	FE	22	19.0	73.9		25.2			
603+03.00	LT	PE/MB	16	19.2	75.9		25.9			9.9
603+50.00	LT	FE	22	19.1	71.4		24.4			
DUTCH LANE EAST										
138+50.00	LT	FE	22	38.0	118.2		40.4			
139+70.00	RT	FE	22	61.5	165.0		56.4			
143+50.00	LT	FE	22	22.5	80.0		27.3			
143+50.00	RT	FE	22	46.0	137.6		47.0			
DUTCH LANE WEST										
96+00.00	LT	FE	22	23.5	82.5		28.2			
96+00.00	RT	FE	22	68.4	192.0		65.6			
203+50.00	LT	PE/MB	16	153.0	313.7		107.2			9.9
203+50.00	RT	FE	22	47.4	140.8		48.1			
207+00.00	RT	FE	22	66.6	187.8		64.2			
209+38.00	LT	FE	22	113.1	301.3		102.9			
209+38.00	RT	FE	22	143.8	376.5		128.6			
HART'S GRAVEL ROAD										
290+63.00	LT	FE	22	30.7	100.1		34.2			
292+28.00	RT	FE	22	63.6	191.0		65.3			
294+00.00	LT	FE	22	127.4	336.3		114.9			
TOTALS						378.2	5955.4	107.1	380.5	575.6
USE						379	5956	108	381	576

- SEE STAGING SCHEDULES FOR ADDITIONAL QUANTITIES

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SCHEDULE OF QUANTITIES
FAP 310 (US 67/IL 104)
SHEET 13 OF 42

DRAWN BY EBB
CHECKED BY
DATE 10/10

3:57:13 PM Mar-25-2011 03:57:13 PM

\$ FILE \$

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	30
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

AGGREGATE SHOULDERS, TYPE B

48101200

LOCATION	SIDE	TON
FAP 310 (US 67)	LT IS	4568.2
	LT OS	2504.7
	RT IS	4547.2
	RT OS	2417.9
RAMP A	RT	848.8
	LT	436.9
RAMP B	RT	684.6
	LT	394.2
RAMP C	RT	789.0
	LT	336.6
RAMP D	RT	701.7
	LT	429.2
CONCORD ROAD	RT	662.6
	LT	671.8
EX US 67 / IL 104	RT	111.1
	LT	114.8
US 67 FRONTAGE ROAD (CONCORD)	RT	944.3
	LT	987.5
SERVICE DRIVE 81	RT	6.7
	LT	6.2
COOPER STREET	RT	33.7
	LT	29.5
NEW STREET	RT	140.6
	LT	140.8
RELOCATED CREWS LANE	RT	207.1
	LT	193.3
ASH STREET	RT	18.8
	LT	23.5
US 67 FRONTAGE ROAD (BETHEL)	RT	215.1
	LT	214.8
BETHEL LANE	RT	139.0
	LT	130.9
SERVICE DRIVE 91	RT	30.6
	LT	30.9
RELOCATED CYLINDER HEAD ROAD	RT	176.0
	LT	131.6
OLD CYLINDER HEAD ROAD	RT	48.0
	LT	52.9
GILLILAND ROAD CONNECTOR	RT	24.5
	LT	27.4
GILLILAND ROAD EXTENSION	RT	95.0
	LT	82.0
ST. PAUL'S CHURCH ROAD	RT	69.4
	LT	82.6
SERVICE DRIVE 92	RT	43.7
	LT	30.6
DUTCH LANE EAST	RT	128.3
	LT	125.6
DUTCH LANE WEST	RT	177.1
	LT	172.5
HART'S GRAVEL ROAD	RT	107.0
	LT	106.8
TEMPORARY ACCESS ROAD	RT	283.7
	LT	217.1
TRANSITION A	RT	75.9
	LT	27.9
TRANSITION B	RT	47.8
	LT	13.4
TRANSITION C	RT	85.4
	LT	31.4
TRANSITION D	RT	33.7
	LT	63.9
TOTAL		26324.2
USE		26325

PCC SIDEWALK 4 INCH

42400100

STATION TO STATION	SIDE	WIDTH	SO FT
COOPER STREET			
444+03.0	444+13.0	RT	4
444+25.0	445+49.5	RT	4
TOTAL			538.0
USE			538

REMOVAL OF EXISTING STRUCTURES

PAY ITEM NO.	STATION	SIDE	OFFSET	DESCRIPTION	EACH
FAP 310 (US 67)					
50100300	868+60.0	RT	117.3	WOOD BRIDGE W/ CONC/STONE WALLS	1
50100400	894+88.0	LT	187.2	2' X 2' CONC BOX CULVERT	1
50100500	967+99.0	LT	51.1	2' X 2' CONC BOX CULVERT	1
50100600	974+64.2	RT	17.0	2' X 2' CONC BOX CULVERT	1
50100700	979+13.2	LT	22.4	4' X 4' CONC BOX CULVERT	1
50100800	994+89.9	RT	91.7	6' X 3' CONC BOX CULVERT	1
50100900	1019+24.3	RT	90.7	6' X 4' CONC BOX CULVERT	1
COOPER STREET					
50101000	444+90.3	RT	8.6	6' X 6' CONC BOX CULVERT W/ CMP EXTENSION	1
RELOCATED CYLINDER HEAD ROAD					
50101100	95+86.3	LT	45.1	ROOT CELLAR	1
DUCTH LANE EAST					
50101200	140+02.3	LT	6.7	2' X 2' CONC BOX CULVERT	1

* TRENCH BACKFILL REQUIRED - 21.1 CU YD

BUILDING REMOVAL SCHEDULE

PAY ITEM NO.	STATION	SIDE	OFFSET	DESCRIPTION	EACH
FAP 310 (US 67)					
Z0007601	886+43.4	LT	0.6	METAL GRAIN BIN	1
Z0007602	887+12.2	LT	46.3	METAL GRAIN BIN	1
Z0007603	887+16.7	LT	24.7	METAL GRAIN BIN	1
Z0007604	887+20.8	LT	3.9	METAL GRAIN BIN	1
Z0007605	887+25.9	RT	17.3	METAL GRAIN BIN	1
Z0007606	924+22.6	RT	152.8	BARN	1
Z0007607	929+72.8	RT	6.1	1 STORY HOUSE	1
Z0007608	930+30.1	RT	137.9	BARN	1
Z0007609	930+82.7	RT	60.0	GARAGE	1

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 14 OF 42

DATE 10/10
 DRAWN BY EBB
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	31
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

HMA SURFACE REMOVAL - BUTT JOINT

40600982

STATION TO STATION	SIDE	WIDTH	SO YD
EX US 67 / IL 104			
97+55.00 97+85.00	LT & RT	30	100.0
2+70.00 3+00.00	LT & RT	30	100.0
556+00.00 556+30.00	LT & RT	30	100.0
TRANSITION C			
61+35.40 61+75.40	LT & RT	28	124.4
US 67 FRONTAGE ROAD (CONCORD)			
53+80.00 54+20.00	LT & RT	24	106.7
COOPER STREET			
442+65.00 442+95.00	LT & RT	22	73.3
TOTAL			604.4
USE			605

HMA SURFACE REMOVAL, 2"

44000157

STATION TO STATION	SIDE	WIDTH	SO YD
EX US 67 / IL 104			
520+00.00 524+68.00	LT & RT	24	1248.0
CONCORD ROAD			
33+38.98 37+65.00	LT & RT	24	1136.1
TOTAL			2384.1
USE			2385

HMA SURFACE REMOVAL, 3"

44000161

STATION TO STATION	SIDE	WIDTH	SO YD
EX US 67 / IL 104			
86+09.30 90+15.60	LT & RT	VARIES	650.8
TOTAL			650.8
USE			651

PAVED SHOULDER REMOVAL

44004250

STATION TO STATION	SIDE	WIDTH	SO YD
EX US 67 / IL 104			
80+71.6 90+53.7	LT & RT	VARIES	388.0
85+52.1 90+86.0	RT	VARIES	168.3
48+49.4 53+18.2	RT	3.0	156.3
48+49.4 51+16.7	LT	3.0	89.1
17+96.3 19+03.5	RT	3.0	35.7
14+92.7 16+19.1	LT	3.5	49.2
564+77.9 565+13.2	RT	3.0	11.8
563+59.8 563+86.4	RT	3.0	8.9
519+91.8 524+68.0	RT	3.0	158.7
481+34.3 488+00.0	LT	3.0	221.9
349+00.0 353+65.4	LT	4.0	206.8
312+19.5 317+03.8	LT	4.5	242.2
COOPER STREET			
446+37.5 447+38.1	RT	3.0	33.5
446+39.8 447+63.9	LT	3.0	41.4
TOTAL			1811.7
USE			1812

SIDEWALK REMOVAL

44000600

STATION TO STATION	SIDE	WIDTH	SO FT
COOPER STREET			
444+03.0 445+87.2	RT	4	736.8
TOTAL			736.8
USE			737

CURB REMOVAL

44000300

STATION TO STATION	SIDE	FOOT
FAP 310 (US 67)		
762+05.0 770+24.0	RT	818
777+81.0 779+96.0	LT	217
TOTAL		1035

PAVEMENT REMOVAL

44000100

STATION TO STATION	SIDE	DESCRIPTION	SO YD
EX 4T02 LANE TRANSITION			
0+00.0 15+85.1	LT & RT	HMA PAVEMENT & SHOULDERS	3473.7
12+47.1 28+91.0	LT & RT	HMA PAVEMENT & SHOULDERS	3606.8
EX US 67/IL 104			
51+16.7 82+59.4	LT & RT	HMA PAVEMENT & SHOULDERS	11409.6
573+75.0 578+95.0	LT & RT	HMA PAVEMENT & SHOULDERS	2092.5
429+00.0 524+68.0	LT & RT	HMA PAVEMENT & SHOULDERS	29007.3
313+24.6 408+00.0	LT & RT	HMA PAVEMENT & SHOULDERS	32963.1
CONCORD ROAD			
10+12.3 25+28.6	LT & RT	HMA PAVEMENT & SHOULDERS	4112.1
COOPER STREET			
447+38.1 447+96.7	RT	HMA PAVEMENT & SHOULDERS	214.5
447+63.8 448+09.5	LT	HMA PAVEMENT & SHOULDERS	39.5
ASH STREET			
2+50.0 4+00.0	LT & RT	HMA PAVEMENT	370.3
ST. PAUL'S CHURCH ROAD			
10+19.7 18+46.1	LT & RT	HMA PAVEMENT	1941.8
HART'S GRAVEL ROAD			
19+46.9 19+81.7	LT & RT	HMA PAVEMENT	134.2
TEMPORARY ACCESS ROAD			
0+40.5 28+05.3	LT & RT	HMA PAVEMENT & SHOULDERS	6876.9
28+51.0 28+93.0	LT & RT	HMA PAVEMENT	140.0
29+38.5 30+15.4	LT & RT	HMA PAVEMENT	594.9
CROSSOVER "A"			
10+00.0 27+19.2	LT & RT	HMA PAVEMENT & SHOULDERS	2124.8
TRANSITION A			
0+00.0 9+99.8	LT & RT	HMA PAVEMENT & SHOULDERS	2140.0
TRANSITION B			
0+00.0 6+29.7	LT & RT	HMA PAVEMENT & SHOULDERS	1259.2
TOTAL			102501.3
USE			102502

DRIVEWAY PAVEMENT REMOVAL

44000200

STATION	SIDE	TYPE	SO YD
EX US 67 / IL 104			
0+70.0	RT	HMA	170.1
580+00.0	RT	CONC	101.7
579+18.0	RT	CONC	91.3
FAP 310 (US 67)			
1125+18.8	LT	HMA	100.4
COOPER STREET			
444+19.0	RT	CONC	18.0
445+69.0	RT	CONC	82.4
ASH STREET			
2+92.8	LT	CONC	25.3
SERVICE DRIVE 91			
592+82.0	RT	CONC	29.6
BETHEL LANE			
88+74.7	RT	HMA	54.6
89+46.4	RT	HMA	68.9
91+12.0	RT	HMA	82.5
TOTAL			824.9
USE			825

PAVED DITCH REMOVAL

44004000

STATION TO STATION	SIDE	FOOT
FAP 310 (US 67)		
1080+30.0 1083+28.6	RT	310
TOTAL		310

PIPE CULVERT REMOVAL

50105220

STATION	SIDE	OFFSET	DESCRIPTION	FOOT
FAP 310 (US 67)				
796+31.6	LT	134.6	15" CMP	36.5
796+79.7	LT	193.2	15" CMP	27.6
805+38.0	LT	128.2	24" CONC	118.9
805+41.4	LT	203.2	15" CMP	32.1
807+08.1	LT	131.5	15" CMP	38.1
818+78.9	LT	190.8	15" CMP	36.6
818+88.5	LT	132.3	15" CMP	42.3
846+40.6	LT	122.3	15" CMP	39.8
859+54.3	LT	222.5	8" CMP	49.1
866+97.0	LT	149.0	15" CMP	41.2
868+21.3	LT	148.5	15" CMP	36.0
869+81.7	LT	144.4	15" CMP	34.2
874+51.9	LT	3.9	48" STEEL	36.7
882+01.1	LT	129.9	15" CMP	48.6
887+05.5	LT	144.0	15" CMP	35.3
900+77.9	LT	81.6	12" CMP	26.4
900+83.4	LT	127.4	18" CONC	37.9
901+05.1	LT	84.4	12" CMP	26.8
901+59.1	LT	140.2	18" CONC	176.3
924+07.4	LT	141.4	15" CMP	38.3
947+29.3	LT	89.3	30" CONC	59.3
947+82.1	LT	55.5	15" CMP	33.2
955+71.2	LT	121.3	15" CMP	67.8
956+38.6	LT	59.7	15" CMP	53.4
957+15.5	LT	58.5	15" CMP	29.1
960+31.7	LT	115.5	15" CONC	50.0
961+58.4	LT	59.0	15" CMP	45.6
964+73.0	LT	53.4	15" CMP	41.2
978+86.6	RT	55.6	36" CONC	62.3
982+38.6	RT	47.8	15" CMP	38.7
986+94.3	RT	91.6	30" CONC	58.7
993+15.6	RT	93.5	30" CONC	56.1
995+20.5	RT	128.9	15" CMP	70.0
995+77.9	RT	159.1	15" CMP	27.6
1001+27.3	RT	90.9	30" CONC	46.7
1013+34.2	RT	60.0	15" CMP	41.1
1015+10.0	RT	122.7	15" CMP	39.3
1020+87.6	RT	50.9	15" CMP	37.5
1023+27.1	RT	129.0	15" CMP	38.9
1036+43.5	LT	37.2	15" CMP	41.5
1036+88.7	RT	40.5	24" CONC	82.8
1055+25.0	LT	6.1	15" CMP	40.7
1055+63.2	RT	78.4	15" CMP	39.3
1062+57.0	LT	7.0	15" CMP	39.4
1063+02.1	RT	80.8	18" CMP	39.8
1063+64.4	RT	37.6	24" CONC	95.1
1067+93.0	RT	39.9	24" CONC	93.9
1077+90.7	RT	91.5	18" CONC	37.5
1090+36.6	LT	9.2	15" CMP	15.0
1097+02.9	RT	129.6	24" CMP	63.8
1100+18.1	RT	68.4	24" CONC/CMP	164.7
1110+42.1	RT	34.4	4" CLAY	40.0
1113+61.0	LT	23.2	15" CMP	38.6
1119+54.5	LT	73.0	15" CMP	39.8
1120+70.9	LT	138.6	24" CONC	111.2
1125+23.9	LT	202.3	15" CMP	37.9
1126+11.1	LT	111.2	15" CMP	35.2
1126+42.1	LT	176.9	24" CONC	118.1
1133+53.5	LT	165.5	15" CMP	40.6
1134+65.0	LT	119.1	24" CONC	115.9
1137+30.6	LT	63.4	24" CONC	108.5
1140+64.3	RT	6.4	24" CONC	94.8
SUBTOTAL				3389.3

PIPE CULVERT REMOVAL (SPECIAL)

X5015225

STATION	SIDE	OFFSET	DESCRIPTION	FOOT
FAP 310 (US 67)				
779+70.0	LT	118.6	36" CONC	26
1103+07.6	RT	55.0	27" CONC	55
TOTAL				81

PIPE CULVERT REMOVAL

50105220

STATION	SIDE	OFFSET	DESCRIPTION	FOOT
EX US 67/IL 104				
1+94.6	RT	29.2	15" CMP	57.9
557+34.1	RT	30.7	15" CMP	36.3
CONCORD ROAD				
29+82.1	RT	24.2	30" CONC	56.2
31+77.4	RT	29.9	15" CMP	77.7
36+27.5	LT	22.2	12" CMP	41.0
US 67 FRONTAGE ROAD (CONCORD)				
4+07.1	LT	29.9	15" CMP	32.5
11+14.9	LT	7.9	15" CMP	39.7
13+56.8	RT	7.5	15" CMP	17.0
COOPER STREET				
444+82.7	LT	29.3	8" PVC	21.6
445+00.2	RT	26.2	12" CMP	15.8
445+17.1	LT	30.7	10" PVC	56.3
445+49.7	RT	20.2	12" CMP	87.7
ASH STREET				
2+65.7	LT & RT	0.0	15" CMP	33.9
2+92.4	LT	19.2	12" CMP	23.5
US 67 FRONTAGE ROAD (BETHEL)				
524+34.7	RT	30.0	15" CMP	35.6
BETHEL LANE				
77+61.4	LT	59.0	15" CMP	22.7
91+05.3	RT	15.8	12" CMP	33.9
SERVICE DRIVE 91				
592+81.0	LT	20.1	12" CMP	27.3
RELOCATED CYLINDER HEAD ROAD				
95+11.7	LT	24.2	15" CMP	16.7
95+83.5	LT	31.2	14" STEEL	128.1
96+53.3	LT	22.8	2-30" CMP	101.0
97+13.2	LT	54.5	15" CMP	25.5
98+29.8	LT	99.2	15" CMP	30.9
OLD CYLINDER HEAD ROAD				
3+38.3	RT	21.7	15" CMP	37.5
3+93.0	RT	21.4		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	32
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PIPE CULVERTS, CLASS A SCHEDULE

STRUCTURE NO.	STATION	SIDE	PIPE CULVERTS, CLASS A																										TRENCH BACKFILL	
			TYPE 1						TYPE 2						TYPE 3				TYPE 4	TYPE 6	CU YD									
			12"	24"	30"	18"ERS	27"ERS	30"ERS	36"ERS	42"ERS	18"	24"	30"	36"	42"	48"	54"	60"	66"	72"		42"ERS	24"	30"	42"	48"	54"	24"		30"
FAP 310 (US 67)																														
2	793+62.0	RT		36																										20.4
3	797+00.0	RT																												3.8
4	802+50.0	LT & RT																												40.3
6	809+50.0	LT & RT																												32.7
7	820+60.0	LT & RT																												52.5
8	843+91.0	RT																												19
9	851+11.0	LT & RT																												63.5
13	866+07.0	LT & RT																												69
17	877+21.0	LT & RT																												84.1
18	890+00.0	RT																												31.2
21	901+60.0	LT																												23.7
24	922+38.8	LT & RT																												117.9
25	931+90.0	RT																												10.2
27	947+28.0	LT & RT																												33.4
28	956+40.0	LT & RT																												42.3
29	969+05.0	LT & RT																												100.2
30	971+03.0	RT																												11.2
31	976+40.0	LT																												26.6
34	978+98.0	LT & RT																												93.1
35	986+94.0	LT & RT																												28.6
36	992+07.0	RT																												19.5
37	995+49.0	LT & RT																												122.3
39	999+40.0	RT																												17.1
39A	1001+27.0	LT & RT																												33
41	1019+28.0	LT & RT																												68
45	1036+87.0	LT																												11.7
46	1054+10.0	LT																												20
49	1068+01.0	LT & RT																												75
50	1075+55.0	LT																												10.2
51	1082+59.8	LT																												16.2
52	1082+85.0	LT																												11.7
53	1091+60.0	LT																												10.2
55	1102+00.0	LT																												14.5
56	1102+68.7	LT																												25.4
57	1102+68.7	RT																												
59	1120+72.0	LT & RT																												40.9
60	1125+25.0	LT																												10.2
62	1130+00.0	RT																												19
63	1133+60.0	RT																												11
65	1134+61.0	LT & RT																												36.8
66	1141+45.0	LT & RT																												26.9
SUBTOTALS				0	126		8	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1503.3

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 16 OF 42
 DRAWN BY EBB
 CHECKED BY
 DATE 10/10

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	33
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PIPE CULVERTS, CLASS A SCHEDULE - CONTINUED

STRUCTURE NO.	STATION	SIDE	PIPE CULVERTS, CLASS A																												TRENCH BACKFILL						
			TYPE 1								TYPE 2								TYPE 3								TYPE 4	TYPE 6									
			12"	24"	30"	18"ERS	27"ERS	30"ERS	36"ERS	42"ERS	18"	24"	30"	36"	42"	48"	54"	60"	66"	72"	42"ERS	24"	30"	42"	48"	54"	24"	30"	42"								
CONCORD ROAD																														CU YD							
78	8+69.0	RT	52																																5.2		
83	21+50.0	LT & RT																															230	28.9			
84	29+93.0	LT & RT									114																							22.3			
US 67 FRONTAGE ROAD (CONCORD)																																					
90	13+50.0	LT & RT									66																							8.2			
93	25+30.0	LT & RT																															116	21.3			
94	31+90.0	LT & RT																															136	21.9			
RAMP A																																					
98	7+00.0	LT & RT									96																							7.3			
RAMP B																																					
99	11+00.0	LT & RT																																74	22.2		
RAMP C																																					
100	9+00.0	LT & RT																																76	9.3		
RAMP D																																					
101	18+50.0	LT & RT																																138	7.3		
SERVICE DRIVE 81																																					
11	581+02.0	LT & RT																																116	22.6		
COOPER STREET																																					
107	448+98.0	LT & RT																																144	62.2		
108	451+02.0	LT & RT																																	120	50.6	
RELOCATED CREWS LANE																																					
109	45+50.0	LT & RT																																	108	15.4	
NEW STREET																																					
110	302+09.0	LT & RT																																	148	17.7	
112	312+60.0	LT & RT																																	52	18.2	
ASH STREET																																					
113	2+58.0	LT & RT																																	56	6.6	
SERVICE DRIVE 91																																					
117	591+55.0	LT & RT																																	72	6.8	
US 67 FRONTAGE ROAD (BETHEL)																																					
122	517+20.0	LT & RT																																	62	9.6	
RELOCATED CYLINDER HEAD ROAD																																					
128	96+65.0	LT & RT																																	116	15.9	
OLD CYLINDER HEAD ROAD																																					
132	0+41.5	LT & RT																																	72	27.7	
GILLILAND ROAD CONNECTOR																																					
141	12+94.0	LT & RT																																	82	6.5	
GILLILAND ROAD EXTENSION																																					
143	27+00.0	LT & RT																																	42	3.9	
DUTCH LANE EAST																																					
149	140+00.0	LT & RT																																	48	13.2	
DUTCH LANE WEST																																					
153	198+63.0	LT & RT																																		156	21.3
154	201+01.0	LT & RT																																		98	25.7
157	206+05.0	LT & RT																																		54	21.4
EX US 67 / IL 104																																					
22	563+50.8	LT & RT																																	58	9.9	
TOTALS			52	256			124	124	62	56	52	148	72	2258	1050	766	170	634	54	680	182	328	164	348	94	156	74	60	252	68	230		2012.4				

PIPE CULVERTS, CLASS A, TYPE 2 24" (TEMPORARY)

5422A024

STRUCTURE NO.	STATION	SIDE	FOOT
FAP 310 (US 67)			
45	1036+87	LT	8
46	1054+10	LT	8
50	1075+55	LT	8
TOTAL			24

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 17 OF 42

DATE 10/10
 DRAWN BY EBB
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	34
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PIPE CULVERTS, CLASS D SCHEDULE

STRUCTURE NO.	STATION	SIDE	PIPE CULVERTS, CLASS D																		TRENCH BACKFILL			
			TYPE 1						TYPE 2						TYPE 3		PIPE CULVERTS, CLASS D (TEMPORARY)				CU YD			
			12"	15"	18"	24"	30"	36"	42"	15"ERS	18"ERS	18"	24"	30"	36"	84"	30"	12"	15"	24"		42"	24"	30"
FAP 310 (US 67)																								
5	805+85.0	RT																			58		13.2	
9A	846+00.0	RT											68											
12	860+04.0	RT											82											
16	873+90.0	RT														62								
20	900+00.0	RT			80																			
23	912+90.0	RT											66											
26	941+50.0	RT											62											
144	959+08.0	LT																		20				
142	962+91.0	LT																		16				
32	975+50.0	LT			52																			
38	998+57.0	LT								68														
40	1015+10.0	RT																						
42	1023+40.0	RT												78										
43	1024+35.0	LT																						
44	1031+83.0	RT																						
46A	1048+00.0	RT								56														
47	1054+95.0	LT																						
48	1054+95.0	RT																						
58A	1119+00.0	LT																						
58	1119+70.0	RT														68								
61	1126+35.0	RT																						
64	1133+55.0	LT																						
CROSSOVER A																								
67	931+29.0	CL																			118			
68	933+44.0	CL																					17.3	
69	941+55.0	CL																					20.7	
TRANSITION A																								
70	971+13.4	LT & RT																				88	9.7	
71	974+66.0	RT																				32	3.3	
TEMPORARY ACCESS ROAD																								
72	3+43.0	RT																				42		
73	7+75.0	LT & RT																					6.2	
74	18+70.0	LT & RT																					6.7	
75	27+66.0	LT & RT																				100	10	
76	28+72.0	LT & RT																					2.6	
77	29+76.0	LT & RT																				84	5.2	
CONCORD ROAD																								
85	31+60.0	RT	42																					
86	32+08.0	RT	38																					
88	34+43.0	LT			44																			
US 67 FRONTAGE ROAD (CONCORD)																								
89	4+04.0	LT																				48		
91	13+90.0	LT																				48		
92	14+44.0	RT																				44		
95	39+00.0	RT																						
96	39+00.0	LT																						
97	41+41.0	LT																				66		
NEW STREET																								
111	310+50.0	RT																				56		
ASH STREET																								
114	3+15.0	LT																				54	1.8	
SUBTOTALS			80	150	220	112	68	168	128	0	36	0	424	154	312	62	0	772	56	84	58	212	120	107.6

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 18 OF 42
 DATE 10/10
 DRAWN BY EBB
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	35
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PIPE CULVERTS, CLASS D SCHEDULE - CONTINUED

STRUCTURE NO.	STATION	SIDE	PIPE CULVERTS, CLASS D																	PIPE CULVERTS, CLASS D (TEMPORARY)				TRENCH BACKFILL		
			TYPE 1							TYPE 2				TYPE 3	TYPE 1		TYPE 2									
			12"	15"	18"	24"	30"	36"	42"	15"ERS	18"ERS	18"	24"	30"	36"	84	30"	12"	15"	24"	42"	24"	30"			
			FOOT																						CU YD	
BETHEL LANE																										
115A	76+20.0	RT																								
115B	77+48.0	LT				56																				
118	88+75.0	RT			84																				3.7	
119	89+30.0	RT			44																				4.1	
120	91+12.0	RT			62																					
SERVICE DRIVE 91																										
116	593+43.0	RT			52																					
US 67 FRONTAGE ROAD (BETHEL)																										
	512+00.0	LT																								
121	517+00.0	LT			42																					
123	523+75.0	RT			46																					
124	524+38.0	RT			44																					
RELOCATED CYLINDER HEAD ROAD																										
125A	94+67.0	LT			42																					
125	95+29.0	LT			80																					
127	96+22.8	LT			74																					
129	97+20.0	LT																								
130	98+50.0	LT																								
130A	99+75.0	RT			50																					
131	106+00.0	RT			50																					
OLD CYLINDER HEAD ROAD																										
133	3+75.0	RT			96																					
134	4+20.0	LT			44																					
135	4+50.0	RT			40																					
136	5+00.0	LT			46																					
137	6+50.0	RT			34																					
138	6+95.0	RT			36																					
GILLILAND ROAD CONNECTOR																										
139	12+25.0	RT			62																					
140	12+53.0	LT			42																					
SERVICE DRIVE 93																										
33	613+90.0	LT & RT			62																				4.1	
ST. PAUL'S CHURCH ROAD																										
145	124+00.0	LT			54																					
SERVICE DRIVE 92																										
146	603+03.0	LT			62																					
147	603+50.0	LT			44																					
DUTCH LANE EAST																										
148	139+70.0	RT			46																					
150	143+50.0	LT			54																					
DUTCH LANE WEST																										
151	196+00.0	RT																								
152	196+00.0	LT																								
155	203+50.0	LT			48																					
156	203+50.0	RT			62																					
157A	207+00.0	RT																								
HART'S GRAVEL ROAD																										
160	294+00.0	LT																								
EX US 67 / IL 104																										
10	18+50.0	RT																								
15	1+87.0	RT																								
19	573+32.0	RT																								
TOTALS					80	1306	566	340	128	168	128	60	198	86	424	154	312	62	110	772	122	84	58	212	120	130.6

* - SEE STAGING SCHEDULES FOR ADDITIONAL QUANTITIES

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 19 OF 42
 DRAWN BY EBB
 CHECKED BY
 DATE 10/10

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	36
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

MANHOLE / INLET SCHEDULE

STRUCTURE NO.	STATION	SIDE	MANHOLES, TYPE A						INLETS, TYPE A		INLETS TYPE B TY 8 GRATE	MEDIAN INLET (604106)
			5' DIA. TY 1 FR OPEN	5' DIA. TY 1 FR CLOSED	5' DIA. TY 3V FR & GR	5' DIA. MED INLET (604106)	7' DIA. TY 1 FR CLOSED	6' DIA. MED INLET (604106)	TY 8 GRATE	TY 15 FR & LID		
			EACH									
FAP 310 (US 67)												
2	793+62.0	RT	1	1								
6	809+50.0	LT & RT				1						
8	843+91.0	RT				1						
9	851+11.0	LT & RT									1	
13	866+07.0	LT & RT									1	
17	877+21.0	LT & RT									1	
18	890+00.0	RT				1						
21	901+60.0	LT				1						
24	922+38.8	LT & RT									1	
25	931+90.0	RT				1						
27	947+28.0	LT & RT						1				
28	956+40.0	LT & RT				1						
30	971+03.0	RT				1						
31	976+40.0	LT				1						
36	992+07.0	RT				1						
39	999+40.0	RT				1						
41	1019+28.0	LT & RT									1	
45	1036+87.0	LT				1						
46	1054+10.0	LT				1						
49	1068+01.0	LT & RT				1						
50	1075+55.0	LT			1	1						
52	1082+85.0	LT			1	1						
53	1091+60.0	LT			1	1						
55	1102+00.0	LT				1						
56	1102+68.7	LT			1							
59	1120+72.0	LT & RT				1						
60	1125+25.0	LT				1						
62	1130+00.0	RT				1						
63	1133+60.0	RT				1						
66	1141+45.0	LT & RT			2							
CONCORD ROAD												
78	8+69.0	RT							2			
84	29+93.0	LT & RT			1							
COOPER STREET												
102	444+05.0	RT						1				
104	444+82.0	RT						1				
ASH STREET												
114	3+15.0	LT						1				
BETHEL LANE												
119	89+30.0	RT						1				
SERVICE DRIVE 91												
116	593+43.0	RT						1				
RELOCATED CYLINDER HEAD ROAD												
126	95+75.0	LT								1		
SERVICE DRIVE 93												
33	613+90.0	LT & RT						1				
DUTCH LANE WEST												
157	206+05.0	LT & RT					1					
TOTALS			1	7	1	21	1	1	6	2	1	5

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 20 OF 42

DATE 10/10
 DRAWN BY EBB
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	37
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PRECAST REINFORCED CONCRETE FLARED END SECTIONS SCHEDULE

54213657 54213663 54213669 54213675 54213681 54213687 54213693 54213705 54213711 54213717 54214503 54214512 54214515 54214521 54214527

STRUCTURE NO.	STATION	SIDE	PRECAST REINFORCED CONCRETE FLARED END SECTIONS														
			12"	18"	24"	30"	36"	42"	48"	60"	66"	72"	18" ERS	27" ERS	30" ERS	36" ERS	42" ERS
			EACH														
FAP 310 (US 67)																	
2	793+62.0	RT			1												
4	802+50.0	LT & RT			2												
6	809+50.0	LT & RT				2											
7	820+60.0	LT & RT					2										
8	843+91.0	RT			1												
13	866+07.0	LT & RT								2							
17	877+21.0	LT & RT									2						
18	890+00.0	RT			1												
21	901+60.0	LT			1												
25	931+90.0	RT			1												
27	947+28.0	LT & RT														2	
28	956+40.0	LT & RT						2									
29	969+05.0	LT & RT									2						
30	971+03.0	RT			1												
31	976+40.0	LT			1												
34	978+98.0	LT & RT								2							
35	986+94.0	LT & RT				2											
36	992+07.0	RT			1												
39	999+40.0	RT			1												
39A	1001+27.0	LT & RT				2											
41	1019+28.0	LT & RT								1							
45	1036+87.0	LT			1												
46	1054+10.0	LT			1												
49	1068+01.0	LT & RT						2									
50	1075+55.0	LT			1												
52	1082+85.0	LT			1												
53	1091+60.0	LT			1												
56	1102+68.7	LT				1											
57	1102+68.7	RT				1											
59	1120+72.0	LT & RT			2												
60	1125+25.0	LT			1												
62	1130+00.0	RT			1												
63	1133+60.0	RT			1												
65	1134+61.0	LT & RT			2												
66	1141+45.0	LT & RT			2												
CONCORD ROAD																	
78	8+69.0	RT	1														
83	21+50.0	LT & RT						2									
84	29+93.0	LT & RT				2											
US 67 FRONTAGE ROAD (CONCORD)																	
90	13+50.0	LT & RT			2												
93	25+30.0	LT & RT			2												
94	31+90.0	LT & RT			2												
RAMP A																	
98	7+00.0	LT & RT			2												
RAMP B																	
99	11+00.0	LT & RT							2								
RAMP C																	
100	9+00.0	LT & RT				2											
RAMP D																	
101	18+50.0	LT & RT			2												
SERVICE DRIVE 81																	
11	581+02.0	LT & RT			2												
COOPER STREET																	
107	448+98.0	LT & RT				2											
108	451+02.0	LT & RT						2									
NEW STREET																	
112	312+60.0	LT & RT													2		
ASH STREET																	
113	2+58.0	LT & RT													2		
SERVICE DRIVE 91																	
117	591+55.0	LT & RT			2												
US 67 FRONTAGE ROAD (BETHEL)																	
122	517+20.0	LT & RT												2			
OLD CYLINDER HEAD ROAD																	
132	0+41.5	LT & RT			2												
GILLILAND ROAD CONNECTOR																	
141	12+94.0	LT & RT											2				
GILLILAND ROAD EXTENSION																	
143	27+00.0	LT & RT											2				
DUTCH LANE EAST																	
149	140+00.0	LT & RT			2												
DUTCH LANE WEST																	
153	198+63.0	LT & RT						2									
154	201+01.0	LT & RT				2											
EX US 67 / IL 104																	
22	563+50.8	LT & RT			2												
TOTALS			1	2	43	16	4	6	4	3	2	4	4	2	2	2	

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 21 OF 42
 DATE 10/10
 DRAWN BY EBB
 CHECKED BY

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	38
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

METAL END SECTIONS SCHEDULE

STRUCTURE NO.	STATION	SIDE	METAL END SECTIONS									
			12"	15"	18"	24"	30"	36"	42"	84"	15" ERS	18" ERS
			EACH									
FAP 310 (US 67)												
5	805+85.0	RT								2		
9A	846+00.0	RT				2						
12	860+04.0	RT				2						
16	873+90.0	RT							2			
20	900+00.0	RT			2							
23	912+90.0	RT				2						
26	941+50.0	RT				2						
144	959+08.0	LT										2
142	962+91.0	LT										2
32	975+50.0	LT		2								
38	998+57.0	LT					2					
40	1015+10.0	RT				2						
42	1023+40.0	RT							2			
43	1024+35.0	LT							2			
44	1031+83.0	RT					2					
46A	1048+00.0	RT							2			
47	1054+95.0	LT							2			
48	1054+95.0	RT							2			
58A	1119+00.0	LT				2						
58	1119+70.0	RT				2						
61	1126+35.0	RT				2						
64	1133+55.0	LT						2				
TEMPORARY ACCESS ROAD												
73	7+75.0	LT & RT				2						
74	18+70.0	LT & RT				2						
75	27+66.0	LT & RT				2						
76	28+72.0	LT & RT		2								
77	29+76.0	LT & RT				2						
CONCORD ROAD												
85	31+60.0	RT	2									
86	32+08.0	RT	2									
88	34+43.0	LT		2								
US 67 FRONTAGE ROAD (CONCORD)												
89	4+04.0	LT				2						
91	13+90.0	LT				2						
92	14+44.0	RT				2						
95	39+00.0	RT							2			
96	39+00.0	LT							2			
97	41+41.0	LT							2			
NEW STREET												
111	310+50.0	RT							2			
ASH STREET												
114	3+15.0	LT		1								
BETHEL LANE												
115A	76+20.0	RT										2
115B	77+48.0	LT				2						
118	88+75.0	RT		1								
119	89+30.0	RT		1								
120	91+12.0	RT		2								
SUBTOTALS			4	11	8	26	6	14	6	2	2	4

METAL END SECTIONS SCHEDULE

STRUCTURE NO.	STATION	SIDE	METAL END SECTIONS									
			12"	15"	18"	24"	30"	36"	42"	84"	15" ERS	18" ERS
			EACH									
SERVICE DRIVE 91												
116	593+43.0	RT				1						
US 67 FRONTAGE ROAD (BETHEL)												
121	517+00.0	LT							2			
123	523+75.0	RT							2			
124	524+38.0	RT				2						
RELOCATED CYLINDER HEAD ROAD												
125A	94+67.0	LT					2					
125	95+29.0	LT					1					
127	96+22.8	LT					1					
129	97+20.0	LT								2		
130	98+50.0	LT								2		
130A	99+75.0	RT						2				
131	106+00.0	RT					2					
OLD CYLINDER HEAD ROAD												
133	3+75.0	RT						2				
134	4+20.0	LT						2				
135	4+50.0	RT						2				
136	5+00.0	LT						2				
137	6+50.0	RT						2				
138	6+95.0	RT						2				
GILLILAND ROAD CONNECTOR												
139	12+25.0	RT						2				
140	12+53.0	LT						2				
SERVICE DRIVE 93												
33	613+90.0	LT & RT							1			
ST. PAUL'S CHURCH ROAD												
145	124+00.0	LT							2			
SERVICE DRIVE 92												
146	603+03.0	LT							2			
147	603+50.0	LT							2			
DUTCH LANE EAST												
148	139+70.0	RT							2			
150	143+50.0	LT							2			
DUTCH LANE WEST												
151	196+00.0	RT								2		
152	196+00.0	LT							2			
155	203+50.0	LT							2			
156	203+50.0	RT							2			
157A	207+00.0	RT							2			
HART'S GRAVEL ROAD												
160	294+00.0	LT								2		
EX US 67 / IL 104												
10	18+50.0	RT										2
15	1+87.0	RT										2
19	573+32.0	RT										2
TOTALS			4	43	24	32	10	14	6	2	2	10

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 22 OF 42
 DRAWN BY EBB
 CHECKED BY
 DATE 10/10

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	39
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

CAST-IN-PLACE CONCRETE SCHEDULE

STRUCTURE NO.	STATION	SIDE	C.I.P. REINF	C.I.P. REINF	REINF BARS	WELDED WIRE FABRIC	CONCRETE BOX CULVERTS	CONCRETE COLLARS	EXPANSION BOLTS 3/4 INCH	GRANULAR CULVERT BACKFILL
			END SECT	END SECT	54"	CU YD	POUND	SQ FT	CU YD	EACH
FAP 310 (US 67)										
9	851+11.0	LT & RT	5.0		230					
14	866+07.0	LT			15		0.4	9		
24	922+38.8	LT & RT	15.0		780					
37	995+49.0	LT & RT	19.1		1250					
41	1019+28.0	LT & RT	5.1		380					
51	1082+59.8	LT	6		65	365	1.2			
54	1092+06.0	LT & RT			70070		291.7	40	896	
56	1102+68.7	LT			50		0.7			
57	1102+68.7	RT			50		0.7			
TRANSITION A										
70	971+13.4	LT & RT			35		0.4	4		
71	974+66.0	RT			35		0.4	4		
COOPER STREET										
106	444+96.0	LT & RT			1930		16.4			110
RELOCATED CREWS LANE										
109	45+50.0	LT & RT	3.8		190					
NEW STREET										
110	302+09.0	LT & RT	17.4		1110					
RELOCATED CYLINDER HEAD ROAD										
128	96+65.0	LT & RT	3.8		190					
DUTCH LANE WEST										
157	206+05.0	LT & RT		2						
HART'S GRAVEL ROAD										
158	291+09.2	LT & RT			11900		80.7	52	265	
159	292+61.0	LT & RT			2390		29.5		421	
TOTALS			75.2	2	90670	365	418.3	3.8	109	1692
USE			75.2	2	90670	365	418.3	3.8	109	2000

BOX CULVERT SCHEDULE

STRUCTURE NO.	STATION	SIDE	PRECAST CONCRETE BOX CULVERT	BOX CULVERT END SECTIONS NO. 1
			3'x3' 5'x3' 8'x6' FOOT	EACH
FAP 310 (US 67)				
14	866+07.0	LT	4	1
COOPER STREET				
106	444+96.0	LT & RT	144	
HART'S GRAVEL ROAD				
159	292+61.0	LT & RT		116
TOTALS			4	144

REINFORCED CONCRETE PIPE TEE SCHEDULE

STRUCTURE NO.	STATION	SIDE	REINFORCED CONCRETE PIPE TEE
			36" PIPE 36" RISER 60" PIPE 36" RISER 72" PIPE 36" RISER EACH
FAP 310 (US 67)			
9	851+11.0	LT & RT	1
13	866+07.0	LT & RT	1
17	877+21.0	LT & RT	1
24	922+38.8	LT & RT	1
41	1019+28.0	LT & RT	1
TOTALS			1

STORM SEWER SCHEDULE

STRUCTURE NO.	STATION	SIDE	S.S. CL A	TRENCH BACKFILL
			TY 1 12" FOOT	CU YD
COOPER STREET				
103	444+05.0	RT	75	13.4
105	444+82.0	RT	4	0.7
TOTALS			79	14.1

REMOVE AND RELAY END SECTION

STRUCTURE NO.	STATION	SIDE	EACH
FAP 310 (US 67)			
1	779+70.0	LT	1
3	797+00.0	RT	1
TOTAL			2

REMOVE CONCRETE FLARED END SECTIONS

STATION	SIDE	EACH
FAP 310 (US 67)		
770+90.0	RT	1
TOTAL		1

GRATING FOR CONCRETE FLARED END SECTION 24"

STRUCTURE NO.	STATION	SIDE	EACH
FAP 310 (US 67)			
25	931+90.0	RT	1
TOTAL			1

FILLING EXISTING CULVERT SCHEDULE

STATION	SIDE	SIZE	CONTROLLED LOW STRENGTH MATERIAL	PLUG EXISTING CULVERTS
			CU YD	EACH
FAP 310 (US 67)				
770+90.0	RT	24"	7.3	1
971+16.4	LT	2' BY 2'	9.0	1
1093+40.0	LT & RT	36"	41.1	1
COOPER STREET				
444+15.0	RT	12"		1
TOTAL			57.4	4
USE			58	4

SHOULDER DRAIN SCHEDULE

STRUCTURE NO.	STATION	SIDE	TYPE F INLET BOX	PIPE DRAINS 12" FOOT	METAL END SECTIONS 12" EACH
			EACH	FOOT	EACH
CONCORD ROAD					
78A	17+64.0	LT	1	12	1
78B	17+64.0	RT	1	12	1
83A	22+36.0	LT	1	12	1
83B	22+36.0	RT	1	12	1
TOTALS			4	48	4

REMOVING MANHOLES

STATION	SIDE	EACH
FAP 310 (US 67)		
770+90.0	RT	1
TOTAL		1

REMOVING INLETS

STATION	SIDE	EACH
FAP 310 (US 67)		
901+64.2	LT	1
1102+56.1	LT	1
COOPER STREET		
445+06.2	RT	1
TOTAL		3

ROADWAY CLEANING (SPECIAL)

STATION TO STATION	SIDE	WIDTH	EACH
FAP 310 (US 67)			
762+05.0	770+65.0	RT	0-37 1
777+86.6	779+95.4	LT	0-24 1
TOTAL			2

FILLING EXISTING WELLS

STATION	SIDE	OFFSET	EACH
FAP 310 (US 67)			
869+07.90	LT	14.5	1
873+39.70	LT	116.0	1
930+05.70	RT	42.5	1
1111+65.90	LT	99.4	1
1112+27.30	RT	87.5	1
TOTAL			5

IMPACT ATTENUATORS (NON-REDIRECTIVE) TEST LEVEL 3

STATION	SIDE	EACH
FAP 310 (US 67)		
807+38	RT	1
808+25	LT	1
TOTAL		2

REMOVE SIGN (SPECIAL)

STATION	SIDE	OFFSET	DESCRIPTION	EACH
FAP 310 (US 67)				
805+23.7	LT	34.3	TRIOPIA CUSD SIGN	1
886+22.0	LT	113.7	OLD BUSINESS SIGN	1
954+78.1	LT	154.9	CYLINDER HEAD SIGN	1
956+88.0	LT	253.5	CYLINDER HEAD SIGN	1
960+40.8	LT	40.3	CYLINDER HEAD SIGN	1
995+70.0	RT	135.6	ST. PAUL LUTHERAN CHURCH	1
OLD CYLINDER HEAD ROAD				
1+74.7	RT	10.3		1
ASH STREET				
9+43.1	RT	27.9	CHAPIN CHRISTIAN CHURCH & LIONS INTERNATIONAL	1
TOTAL				8

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 23 OF 42
 DATE 10/10
 DRAWN BY EBB
 CHECKED BY

3:57:17 PM Mar-25-2011 03:57:17 PM \$FILE\$

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	40
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PIPE UNDERDRAIN SCHEDULE
60107600 60108100 60100060 78001110

EASTBOUND OUTSIDE EDGE OF PAVEMENT							
PIPE UNDERDRAIN 4"			PIPE UNDERDRAIN 4" (SPECIAL)			CONCRETE HEADWALL	PAINT PAVEMENT MARKING 4"
BEGINNING STATION	ENDING STATION	LENGTH FOOT	OUTLET STATION	LENGTH FOOT	CONNECT TO STRUCT NO	EACH	FOOT
FAP 310 (US 67)							
781+86	782+58	71.6	781+86	20.6		1	2.0
792+08	793+74	166.0	793+74	24.0			
793+74	797+00	326.0	793+74				
797+00	799+50	250.0	797+00	18.1		1	2.0
799+50	802+00	250.0	799+50	18.1		1	2.0
802+00	804+50	250.0	802+00	18.1		1	2.0
804+50	807+00	250.0	804+50	18.1		1	2.0
807+00	809+50	250.0	807+00	18.1		1	2.0
809+50	812+00	250.0	809+50	18.1		1	2.0
812+00	814+50	250.0	812+00	18.1		1	2.0
814+50	817+00	250.0	814+50	18.1		1	2.0
817+00	819+50	250.0	817+00	18.1		1	2.0
819+50	820+60	110.0	820+60		7		
820+60	822+96	236.0	820+60		7		
822+96	828+00	104.5	1+50				
828+00	830+50	250.0	828+00	18.2		1	2.0
830+50	831+60	110.0	830+50	18.1		1	2.0
831+60	834+00	240.0	834+00	18.8		1	2.0
834+00	839+00	500.0	839+00	16.4		1	2.0
839+00	844+00	500.0	844+00	16.4		1	2.0
844+00	849+00	500.0	849+00	16.4		1	2.0
849+00	851+00	200.0	851+00	18.4		1	2.0
851+00	853+00	200.0	851+00			1	
853+00	858+00	500.0	853+00	17.4		1	2.0
858+00	859+30	130.0	859+30	17.3		1	2.0
860+45	865+00	455.0	865+00	11.2		1	2.0
865+00	868+00	300.0	868+00	17.4		1	2.0
868+00	873+00	500.0	868+00			1	
873+00	878+00	500.0	873+00	16.4		1	2.0
878+00	883+00	500.0	878+00	16.4		1	2.0
883+00	885+83	283.0	883+00	16.4		1	2.0
887+88	892+00	412.0	887+88	11.2		1	2.0
892+00	896+00	400.0	892+00	14.9		1	2.0
896+00	901+00	500.0	896+00	16.4		1	2.0
901+00	906+00	500.0	901+00	16.4		1	2.0
906+00	911+00	500.0	906+00	16.4		1	2.0
911+00	913+91	291.0	911+00	16.4		1	2.0
913+91	918+00	409.0	918+00	17.0		1	2.0
918+00	921+42	342.0	921+42	17.3		1	2.0
921+42	925+70	428.0	921+42			1	

PIPE UNDERDRAIN SCHEDULE
60107600 60108100 60100060 78001110

EASTBOUND OUTSIDE EDGE OF PAVEMENT							
PIPE UNDERDRAIN 4"			PIPE UNDERDRAIN 4" (SPECIAL)			CONCRETE HEADWALL	PAINT PAVEMENT MARKING 4"
BEGINNING STATION	ENDING STATION	LENGTH FOOT	OUTLET STATION	LENGTH FOOT	CONNECT TO STRUCT NO	EACH	FOOT
FAP 310 (US 67)							
948+10	954+00	590.0	948+10	17.6		1	2.0
955+55	959+08	353.0	955+55	12.2		1	2.0
959+08	964+00	492.0	964+00	16.4		1	2.0
964+00	969+55	555.0	969+55	16.4		1	2.0
989+17	992+68	351.0	992+68	16.4		1	2.0
992+68	997+50	482.0	992+68			1	
997+50	1003+00	550.0	997+50	16.4		1	2.0
1003+00	1007+91	491.0	1003+00	16.4		1	2.0
1007+91	1012+00	409.0	1012+00	16.4		1	2.0
1012+00	1016+00	400.0	1016+00	16.4		1	2.0
1016+00	1019+08	308.0	1019+08	18.3		1	2.0
1019+08	1023+00	392.0	1019+08			1	
1023+00	1027+00	400.0	1023+00	16.4		1	2.0
1027+00	1030+90	390.0	1027+00	16.4		1	2.0
1032+70	1037+00	430.0	1037+00	15.1		1	2.0
1037+00	1040+00	300.0	1037+00			1	
1040+00	1043+50	350.0	1040+00	16.4		1	2.0
1043+50	1046+00	250.0	1046+00	17.2		1	2.0
1046+00	1051+00	500.0	1051+00	16.4		1	2.0
1051+00	1056+00	500.0	1056+00	16.4		1	2.0
1056+00	1061+00	500.0	1061+00	16.4		1	2.0
1061+00	1066+00	500.0	1066+00	16.4		1	2.0
1066+00	1071+00	500.0	1071+00	16.4		1	2.0
1071+00	1075+58	458.0	1075+58	16.4		1	2.0
1077+22	1082+00	478.0	1082+00	11.2		1	2.0
1082+00	1087+00	500.0	1087+00	16.4		1	2.0
1087+00	1091+64	464.0	1091+64	16.4		1	2.0
1091+64	1095+00	336.0	1091+64			1	
1095+00	1099+13	413.0	1095+00	16.4		1	2.0
1100+70	1104+50	380.0	1100+70	11.2		1	2.0
1104+50	1109+00	450.0	1104+50	16.4		1	2.0
1109+00	1113+80	480.0	1109+00	16.4		1	2.0
1113+80	1116+00	220.0	1116+00	17.0		1	2.0
1116+00	1121+00	500.0	1121+00	18.7		1	2.0
1121+00	1125+00	400.0	1125+00	16.4		1	2.0
1125+00	1130+00	500.0	1130+00	16.4		1	2.0
1130+00	1135+00	500.0	1135+00	16.4		1	2.0
1135+00	1140+00	500.0	1140+00	16.4		1	2.0
SUBTOTALS		29286.1		1117.2		73	132.0

NOTE: MINIMUM SLOPE IS 0.30%

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SCHEDULE OF QUANTITIES
FAP 310 (US 67/IL 104)
SHEET 24 OF 42
DRAWN BY EBB
CHECKED BY
DATE 10/10

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	41
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

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PIPE UNDERDRAIN SCHEDULE
60107600 60108100 60100060 78001110

PIPE UNDERDRAIN 4"		PIPE UNDERDRAIN 4" (SPECIAL)		CONCRETE HEADWALL	PAINT PAVEMENT MARKING 4"		
BEGINNING STATION	ENDING STATION	LENGTH FOOT	OUTLET STATION	LENGTH FOOT	CONNECT TO STRUCT NO	EACH	FOOT
EASTBOUND INSIDE EDGE OF PAVEMENT							
FAP 310 (US 67)							
770+00	772+50	250.0	770+00	14.6		1	2.0
772+50	775+00	250.0	772+50	14.6		1	2.0
775+00	777+50	250.0	775+00	14.6		1	2.0
777+50	780+05	255.0	777+50	14.6		1	2.0
800+00	802+50	250.0	800+00	14.6		1	2.0
802+50	806+00	350.0	802+50	15.3		1	2.0
806+00	809+50	350.0	806+00	15.3		1	2.0
809+50	812+00	250.0	809+50	14.6		1	2.0
812+00	814+50	250.0	812+00	14.6		1	2.0
814+50	817+00	250.0	814+50	14.6		1	2.0
817+00	819+50	250.0	817+00	14.6		1	2.0
819+50	822+00	250.0	819+50	14.6		1	2.0
822+00	824+50	250.0	822+00	14.6		1	2.0
824+50	827+00	250.0	824+50	14.6		1	2.0
827+00	829+50	250.0	827+00	14.6		1	2.0
829+50	831+60	210.0	829+50	15.4		1	2.0
831+60	834+00	240.0	834+00	15.4		1	2.0
834+00	839+00	500.0	839+00	12.9		1	2.0
839+00	843+91	491.0	843+91	12.9		1	2.0
843+91	849+00	509.0	849+00	13.9		1	2.0
849+00	851+00	200.0	851+00	15.1		1	2.0
851+00	854+00	300.0	851+00			1	
854+00	858+00	400.0	854+00	14.2		1	2.0
858+00	859+00	100.0	859+00	13.2		1	2.0
860+81	865+00	419.0	865+00	11.2		1	2.0
865+00	868+00	300.0	868+00	13.5		1	2.0
868+00	873+00	500.0	868+00			1	
873+00	878+00	500.0	873+00	12.9		1	2.0
878+00	882+00	400.0	878+00	12.9		1	2.0
882+00	886+73	473.0	882+00	11.8		1	2.0
888+44	893+00	456.0	888+44	11.8		1	2.0
893+00	894+49	149.0	893+00	10.4		1	2.0
913+46	915+00	154.0	915+00	14.1		1	2.0
915+00	920+00	500.0	920+00	12.9		1	2.0
920+00	921+42	142.0	921+42	13.5		1	2.0
921+42	926+81	539.0	921+42			1	
928+25	931+90	365.0	931+90	19.0	25		
931+90	936+00	410.0	931+90		25		
936+00	941+00	500.0	936+00	12.8		1	2.0
941+00	946+00	500.0	941+00	12.8		1	2.0
946+00	950+00	400.0	946+00	12.8		1	2.0
950+00	954+68	468.0	950+00	12.9		1	2.0
956+40	959+08	268.0	956+40	16.0	30		
959+08	963+00	392.0	963+00	12.9		1	2.0
963+00	968+00	500.0	968+00	12.9		1	2.0
968+00	971+03	303.0	971+03	12.9		1	2.0
971+03	974+78	375.0	971+03			1	
976+40	981+00	460.0	976+40		31		
981+00	985+47	447.0	981+00	12.9		1	2.0
985+47	989+00	353.0	989+00	13.9		1	2.0
989+00	992+07	307.0	992+07	12.9		1	2.0
992+07	997+79	572.0	992+07			1	
999+40	1004+00	460.0	999+40	16.0	39		
1004+00	1007+91	391.0	1004+00	11.0		1	2.0
1007+91	1012+00	409.0	1012+00	12.9		1	2.0
1012+00	1016+00	400.0	1016+00	12.9		1	2.0
1016+00	1019+08	308.0	1019+08	13.1		1	2.0
1019+08	1021+64	256.0	1019+08			1	
1032+41	1036+87	446.0	1036+87	12.9		1	2.0
1040+56	1043+50	294.0	1040+56	14.5		1	2.0
1043+50	1046+00	250.0	1046+00	13.3		1	2.0
1046+00	1050+00	400.0	1050+00	12.9		1	2.0
1050+00	1054+10	410.0	1054+10	12.9		1	2.0
1055+72	1060+00	428.0	1060+00	11.7		1	2.0
1060+00	1065+00	500.0	1065+00	12.9		1	2.0
1065+00	1070+00	500.0	1070+00	12.9		1	2.0
1070+00	1075+55	555.0	1075+55	12.9		1	2.0
1077+85	1082+85	500.0	1082+85	11.3		1	2.0
1082+85	1086+00	315.0	1086+00	12.9		1	2.0
1086+00	1088+93	293.0	1088+93	12.9		1	2.0
1102+00	1105+68	368.0	1102+00	11.6		1	2.0
1119+93	1125+25	532.0	1125+25	12.9		1	2.0
1126+99	1130+00	301.0	1130+00	15.0	62		
SUBTOTALS		26373.0		882.5		67	122.0

NOTE: MINIMUM SLOPE IS 0.30%

PIPE UNDERDRAIN SCHEDULE
60107600 60108100 60100060 78001110

PIPE UNDERDRAIN 4"		PIPE UNDERDRAIN 4" (SPECIAL)		CONCRETE HEADWALL	PAINT PAVEMENT MARKING 4"		
BEGINNING STATION	ENDING STATION	LENGTH FOOT	OUTLET STATION	LENGTH FOOT	CONNECT TO STRUCT NO	EACH	FOOT
WESTBOUND INSIDE EDGE OF PAVEMENT							
FAP 310 (US 67)							
764+85	768+69	384.0	764+85	15.5		1	2.0
800+00	802+50	250.0	800+00	14.6		1	2.0
802+50	806+00	350.0	802+50	15.3		1	2.0
806+00	809+50	350.0	806+00	15.3		1	2.0
809+50	812+00	250.0	809+50	14.6		1	2.0
812+00	814+50	250.0	812+00	14.6		1	2.0
814+50	817+00	250.0	814+50	14.6		1	2.0
817+00	819+50	250.0	817+00	14.6		1	2.0
819+50	822+00	250.0	819+50	14.6		1	2.0
822+00	824+50	250.0	822+00	14.6		1	2.0
824+50	827+00	250.0	824+50	14.6		1	2.0
827+00	829+50	250.0	827+00	14.6		1	2.0
829+50	831+60	210.0	829+50	15.4		1	2.0
831+60	834+00	240.0	834+00	15.4		1	2.0
834+00	839+00	500.0	839+00	12.9		1	2.0
839+00	843+91	491.0	843+91	12.9		1	2.0
843+91	849+00	509.0	849+00	13.9		1	2.0
849+00	851+00	200.0	851+00	15.6		1	2.0
851+00	854+00	300.0	851+00			1	
854+00	858+00	400.0	854+00	13.6		1	2.0
858+00	859+00	100.0	859+00	11.1		1	2.0
860+69	865+00	431.0	865+00	12.8		1	2.0
865+00	868+00	300.0	868+00	13.5		1	2.0
868+00	873+00	500.0	868+00			1	
873+00	878+00	500.0	873+00	12.9		1	2.0
878+00	881+00	300.0	878+00	13.3		1	2.0
881+00	886+43	543.0	881+00	12.1		1	2.0
888+44	893+00	456.0	888+44	12.9		1	2.0
893+00	898+00	500.0	893+00	12.2		1	2.0
898+00	903+00	500.0	898+00	12.9		1	2.0
903+00	908+00	500.0	903+00	12.9		1	2.0
908+00	913+00	500.0	908+00	14.9		1	2.0
913+00	918+00	500.0	918+00	14.9		1	2.0
918+00	921+42	342.0	921+42	13.5		1	2.0
921+42	926+81	539.0	921+42			1	
948+10	950+00	190.0	948+10	13.9		1	2.0
950+00	954+27	427.0	950+00	12.0		1	2.0
956+40	959+08	268.0	956+40	14.0		1	2.0
959+08	964+00	492.0	964+00	12.9		1	2.0
964+00	969+00	500.0	969+00	12.9		1	2.0
969+00	971+03	203.0	971+03	11.4		1	2.0
971+03	974+78	375.0	971+03			1	
989+17	992+07	290.0	992+07	12.9		1	2.0
992+07	997+79	572.0	992+07			1	
999+40	1004+00	460.0	999+40	12.9		1	2.0
1004+00	1007+91	391.0	1004+00	12.4		1	2.0
1007+91	1012+00	409.0	1012+00	12.9		1	2.0
1012+00	1016+00	400.0	1016+00	12.9		1	2.0
1016+00	1019+08	308.0	1019+08	13.1		1	2.0
1019+08	1023+00	392.0	1019+08			1	
1023+00	1026+00	300.0	1023+00	12.9		1	2.0
1026+00	1031+22	522.0	1026+00	11.6		1	2.0
1032+69	1036+87	418.0	1036+87	16.1		1	2.0
1036+87	1040+00	313.0	1036+87			1	
1040+00	1043+50	350.0	1040+00	13.2		1	2.0
1043+50	1046+00	250.0	1046+00	13.3		1	2.0
1046+00	1050+00	400.0	1050+00	11.7		1	2.0
1050+00	1054+10	410.0	1054+10	11.8		1	2.0
1055+72	1060+00	428.0	1060+00	12.9		1	2.0
1060+00	1065+00	500.0	1065+00	12.9		1	2.0
1065+00	1070+00	500.0	1070+00	11.9		1	2.0
1070+00	1075+55	555.0	1075+55	11.8		1	2.0
1077+47	1082+85	538.0	1082+85	12.9		1	2.0
1082+85	1086+00	315.0	1086+00	12.9		1	2.0
1086+00	1091+60	560.0	1091+60	12.9		1	2.0
1091+60	1094+00	240.0	1091+60			1	
1094+00	1099+41	541.0	1094+00	11.2		1	2.0
1102+00	1107+00	500.0	1102+00	12.9		1	2.0
1107+00	1112+00	500.0	1107+00	12.9		1	2.0
1112+00	1113+80	180.0	1112+00	13.4		1	2.0
1113+80	1116+00	220.0	1116+00	13.6		1	2.0
1116+00	1120+72	472.0	1120+72	12.9		1	2.0
1120+72	1125+25	453.0	1125+25	11.8		1	2.0
1131+62	1133+60	198.0	1133+60		63		
SUBTOTALS		28035.0		868.8		73	130.0

NOTE: MINIMUM SLOPE IS 0.30%

REVISIONS	
NAME	DATE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	42
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

3:57:19 PM Mar-25-2011 03:57:19PM \$FILE\$

PIPE UNDERDRAIN SCHEDULE
60107600 60108100 60100060 78001110

WESTBOUND OUTSIDE EDGE OF PAVEMENT							
PIPE UNDERDRAIN 4"			PIPE UNDERDRAIN 4" (SPECIAL)			CONCRETE	PAINT
BEGINNING STATION	ENDING STATION	LENGTH	OUTLET STATION	LENGTH	CONNECT TO STRUCT NO	HEADWALL EACH	PAVEMENT MARKING 4" FOOT
		FOOT					FOOT
FAP 310 (US 67)							
786+50	789+00	250.0	786+50	18.1		1	2.0
789+00	789+58	57.7	789+00	19.3		1	2.0
793+62	799+50	588.0	793+62		2		
799+50	802+00	250.0	799+50	18.1		1	2.0
802+00	804+50	250.0	802+00	18.1		1	2.0
804+50	807+00	250.0	804+50	18.1		1	2.0
807+00	809+50	250.0	807+00	18.1		1	2.0
809+50	812+00	250.0	809+50	18.1		1	2.0
812+00	816+00	400.0	812+00	19.1		1	2.0
816+00	820+82	482.0	816+00	19.7		1	2.0
830+33	831+60	127.4	831+60	22+50			
831+60	834+00	240.0	834+00	18.8		1	2.0
834+00	839+00	500.0	839+00	16.4		1	2.0
839+00	844+00	500.0	844+00	16.4		1	2.0
844+00	849+00	500.0	849+00	16.4		1	2.0
849+00	851+00	200.0	851+00	18.4		1	2.0
851+00	853+00	200.0	853+00				
853+00	858+00	500.0	853+00	17.4		1	2.0
858+00	863+00	500.0	863+00	16.4		1	2.0
863+00	868+00	500.0	868+00	17.4		1	2.0
868+00	873+00	500.0	868+00				
873+00	878+00	500.0	873+00	16.4		1	2.0
878+00	883+00	500.0	878+00	17.9		1	2.0
883+00	886+98	398.0	883+00	11.2		1	2.0
888+75	892+69	394.0	888+75	16.4		1	2.0
913+46	917+50	404.0	917+50	16.4		1	2.0
917+50	921+42	392.0	921+42	17.3		1	2.0
921+42	925+00	358.0	921+42				
925+00	928+57	357.0	925+00	18.6		1	2.0
928+57	931+72	315.0	931+72	17.6		1	2.0
931+72	936+00	428.0	931+72				
936+00	941+00	500.0	936+00	15.4		1	2.0
941+00	946+00	500.0	941+00	15.4		1	2.0
946+00	951+00	500.0	946+00	15.8		1	2.0
951+00	954+95	395.0	951+00	11.5		1	2.0
956+50	959+08	258.0	956+50	17.4		1	2.0
959+08	964+00	492.0	964+00	16.4		1	2.0
964+00	969+00	500.0	969+00	16.4		1	2.0
969+00	971+27	227.0	971+27	16.4		1	2.0
971+27	976+00	473.0	976+00				
976+00	981+00	500.0	976+00	16.4		1	2.0
981+00	985+47	447.0	981+00	16.4		1	2.0
985+47	989+00	353.0	989+00	18.3		1	2.0
989+00	992+68	368.0	992+68	13.0		1	2.0
992+68	997+79	511.0	992+68				
999+81	1004+00	419.0	999+81	16.4		1	2.0
1004+00	1007+91	391.0	1004+00	16.4		1	2.0
1007+91	1012+00	409.0	1012+00	16.4		1	2.0
1012+00	1016+00	400.0	1016+00	16.4		1	2.0
1016+00	1019+08	308.0	1019+08	16.4		1	2.0
1019+08	1021+64	256.0	1019+08				
1040+56	1043+50	294.0	1040+56	19.1		1	2.0
1043+50	1046+00	250.0	1046+00	17.2		1	2.0
1046+00	1051+00	500.0	1051+00	16.4		1	2.0
1051+00	1056+00	500.0	1056+00	16.4		1	2.0
1056+00	1061+00	500.0	1061+00	16.4		1	2.0
1061+00	1066+00	500.0	1066+00	16.4		1	2.0
1066+00	1071+00	500.0	1071+00	14.2		1	2.0
1071+00	1076+58	558.0	1076+58	11.2		1	2.0
1078+22	1082+00	378.0	1082+00	16.4		1	2.0
1082+00	1086+00	400.0	1086+00	16.4		1	2.0
1086+00	1088+93	293.0	1088+93	16.4		1	2.0
1119+93	1125+00	507.0	1125+00	16.4		1	2.0
1125+00	1130+00	500.0	1130+00	16.4		1	2.0
1130+00	1133+50	350.0	1133+50	16.4		1	2.0
SUBTOTALS		25578.1		932.6		63	112.0

NOTE: MINIMUM SLOPE IS 0.30%

PIPE UNDERDRAIN SCHEDULE
60107600 60108100 60100060 78001110

RAMP EDGE OF PAVEMENT (SEE PLANS FOR SIDE)							
PIPE UNDERDRAIN 4"			PIPE UNDERDRAIN 4" (SPECIAL)			CONCRETE	PAINT
BEGINNING STATION	ENDING STATION	LENGTH	OUTLET STATION	LENGTH	CONNECT TO STRUCT NO	HEADWALL EACH	PAVEMENT MARKING 4" FOOT
		FOOT					FOOT
RAMP A							
1+20	6+14	494.0	6+14	12.9		1	2.0
6+14	10+00	386.0	10+00	14.6		1	2.0
10+00	13+26	326.0	13+26	15.4		1	2.0
13+26	15+00	174.0	13+26				
15+00	17+26	226.0	15+00	17.8		1	2.0
17+26	19+00	174.0	19+00	16.9		1	2.0
19+00	21+50	250.0	21+50	17.0		1	2.0
21+50	24+00	250.0	24+00	17.0		1	2.0
24+00	26+16	215.6	781+86				
SUBTOTALS		2495.6		111.6		8	14.0
RAMP B							
0+00	2+00	200.0	789+00				
2+00	4+50	250.0	2+00	16.9		1	2.0
4+50	7+03	253.0	4+50	16.6		1	2.0
7+03	9+69	266.0	9+69	12.4		1	2.0
9+69	14+00	431.0	9+69				
14+00	18+68	468.0	14+00	14.6		1	2.0
SUBTOTALS		1868.0		60.5		5	8.0
RAMP C							
0+59	5+00	441.0	5+00	14.6		1	2.0
5+00	10+18	518.0	10+18	12.4		1	2.0
10+18	14+10	392.0	10+18				
14+10	17+50	340.0	14+10		7		
17+50	20+00	250.0	17+50	15.7		1	2.0
20+00	22+50	250.0	20+00	15.7		1	2.0
22+50	23+83	133.0	22+50	16.9		1	2.0
SUBTOTALS		2324.0		75.3		6	10.0
RAMP D							
0+00	1+50	150.0	1+50	15.4		1	2.0
1+50	4+00	250.0	4+00	15.5		1	2.0
4+00	6+37	237.0	6+37		7		
6+37	10+44	407.0	10+44	12.4		1	2.0
10+44	13+18	274.0	10+44				
13+18	19+04	586.0	13+18	12.9		1	2.0
SUBTOTALS		1904.0		56.2		5	8.0

NOTE: MINIMUM SLOPE IS 0.30%

PROJECT TOTALS	117864		4105		300	536
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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SCHEDULE OF QUANTITIES
FAP 310 (US 67/IL 104)
SHEET 26 OF 42

DRAWN BY EBB
CHECKED BY
DATE 10/10

CHAIN LINK FENCE, 4'

STATION TO STATION		SIDE	OFFSET	FOOT
EX US 67 / IL 104				
578+70.00	581+00.00	LT	26.0	230.0
581+00.00	584+00.00	LT	20-26	300.1
584+00.00	589+54.25	LT	20.0	554.3
0+00.00	5+40.66	LT	20.0	540.7
552+87.07	577+25.00	LT	20.0	2437.9
COOPER STREET				
445+90.00	448+30.76	RT	42-82.5	244.2
445+40.00	447+40.00	LT	40.0	200.0
447+40.00	448+52.89	LT	40-61	114.9
TOTAL				4622.0
USE				4623

WOVEN WIRE FENCE, 4'

STATION TO STATION		SIDE	OFFSET	FOOT
FAP 310 (US 67)				
761+25.00	770+13.15	RT	120.0	888.1
770+13.15	772+50.00	RT	120-150	233.2
772+50.00	778+50.00	RT	140-150	600.1
778+50.00	790+09.90	RT	140.0	1159.9
824+00.00	859+79.00	RT	125.0	3579.0
860+29.00	871+03.73	RT	140.0	1074.7
871+03.73	873+00.00	RT	140-165	197.0
873+00.00	873+55.00	RT	165.0	55.0
874+05.00	886+50.00	RT	165.0	1245.0
898+28.51	901+62.13	RT	120-177	345.9
901+62.13	912+52.55	RT	120.0	1090.4
947+49.04	954+00.00	LT	140.0	651.0
956+00.00	960+00.00	LT	100-150	403.2
960+00.00	962+25.00	LT	100.0	225.0
962+25.00	966+50.00	LT	100-135	426.5
EX US 67 / IL 104				
90+86.00	97+85.00	LT	20.0	699.0
16+40.86	48+49.42	LT	20.0	3208.6
5+40.66	14+90.83	LT	20.0	950.2
524+68.00	552+87.07	LT	20.0	2819.1
RAMP A				
0+69.99	2+90.00	RT	90-163.8	232.1
2+90.00	8+50.00	RT	90.0	560.0
8+50.00	10+91.94	RT	90-140.1	247.1
RAMP B				
0+56.95	6+25.51	RT	87.1-136.8	570.8
6+25.51	18+75.70	RT	136.8-153	1163.7
RAMP C				
0+00.00	4+00.00	RT	70-238.9	444.3
4+00.00	12+32.96	RT	70.0	833.0
12+32.96	17+48.97	RT	62.4-70	516.1
RAMP D				
2+78.83	6+03.51	RT	58-71.5	325.0
6+03.51	9+00.00	RT	58-110	287.7
9+00.00	11+46.91	RT	90-110	214.4
11+46.91	16+48.63	RT	90.0	501.7
16+48.63	18+12.45	RT	90.0	185.7
18+12.45	19+39.46	RT	90-178.9	155.0
CONCORD ROAD				
8+03.45	9+00.00	RT	110-165	111.2
9+00.00	13+00.00	RT	110.0	400.0
8+13.45	9+00.00	LT	120-165	97.6
9+00.00	13+00.00	LT	120.0	400.0
26+84.77	30+19.23	RT	79.9-140.1	347.4
28+12.26	33+38.98	LT	60-110	511.5
33+38.98	34+21.00	LT	60.0	82.0
US 67 FRONTAGE ROAD (CONCORD)				
1+23.33	14+00.00	RT	20.0	1276.7
51+52.73	54+20.00	RT	20.0	267.3
RELOCATED CREWS LANE				
41+00.62	50+66.28	LT	20.0	965.7
US 67 FRONTAGE ROAD (BETHEL)				
517+00.00	524+68.00	LT	20.0	768.0
516+93.04	517+00.00	LT	20-64.5	45.1
BETHEL LANE				
76+40.00	77+18.54	RT	40.0	78.5
77+18.54	78+32.42	RT	40-84.5	122.3
77+75.00	78+74.53	LT	40-111.3	122.5
TOTAL				31683.2
USE				31684

GUARDRAIL SCHEDULE

STATION TO STATION	SIDE	SPBGR TYPE A 6' POSTS	TRAFFIC BARRIER TERM		GUARDRAIL MARKERS TYPE A	TERMINAL MARKERS DIRECT APPLIED
			TYPE 6	TYPE 1 SPECIAL (TANGENT)		
			FOOT		EACH	
CONCORD ROAD						
15+93.97	18+86.50	RT	196.88	1	1	4
17+06.47	18+86.50	LT	84.38	1	1	4
21+13.50	22+93.53	RT	84.38	1	1	4
21+13.50	24+06.03	LT	196.88	1	1	4
TOTALS			562.52	4	4	16

GUARDRAIL REMOVAL

STATION TO STATION	SIDE	FOOT
FAP 310 (US 67)		
1090+14	1094+54	RT
1090+14	1094+54	LT
TOTAL		880

WOVEN WIRE GATES, 4'x16' DOUBLE

STATION	SIDE	EACH
FAP 310 (US 67)		
850+50	RT	1
865+50	RT	1
TOTAL		2

ACCESS GATE, DOUBLE, 30 FOOT

STATION	SIDE	EACH
FAP 310 (US 67)		
846+00	RT	1
TOTAL		1

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	43

STA. TO STA.
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

PERMANENT SURVEY MARKERS

STATION	DESCRIPTION	TYPE	
		I	II
FAP 310 (US 67)			
800+00.00	POT		1
816+00.00	POT		1
832+00.00	POT		1
844+65.43	PC		1
852+43.22	PT		1
860+06.80	POT	1	
871+03.73	PC		1
882+48.95	PT		1
887+35.88	POT	1	
893+62.92	PC		1
912+52.55	PT		1
927+32.35	PC	1	
946+47.74	PT		1
955+25.00	POT	1	
960+00.00	POT		1
970+00.67	PC		1
985+00.00	POC		1
988+71.21	PT		1
998+50.00	POT	1	
1008+00.00	POT		1
1022+63.00	PC		1
1031+75.00	POC	1	
1039+56.79	PT		1
1044+00.00	POT		1
1060+00.00	POT		1
1076+90.00	POT	1	
1089+68.35	PC		1
1100+30.00	POC	1	
1114+00.00	POC		1
1119+17.31	PT		1
CONCORD ROAD			
7+13.45	POT	1	
20+00.00	POT	1	
28+12.26	PC	1	
33+38.97	PT	1	
37+65.00	POT	1	
RAMP A			
0+00.00	POT	1	
2+90.00	PC	1	
4+49.32	PT	1	
11+59.30	PC	1	
14+65.60	PT	1	
26+15.60	POT	1	
RAMP B			
0+00.00	POT	1	
8+00.08	PC	1	
11+57.24	PT	1	
19+49.18	POT	1	
RAMP C			
0+00.00	POT	1	
8+50.00	PC	1	
12+32.96	PT	1	
23+82.96	POT	1	
RAMP D			
0+00.00	POT	1	
8+00.09	PC	1	
11+46.91	PT	1	
16+48.63	PC	1	
18+12.45	PT	1	
20+15.82	POT	1	
TOTALS		33	22

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 27 OF 42
 DRAWN BY EBB
 CHECKED BY
 DATE 10/10

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	44
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

66600105

STATION	SIDE	OFFSET	EACH
FAP 310 (US 67)			
947+49.04	LT	140.00	1
954+00.00	LT	140.00	1
965+10.78	LT	121.08	1
966+50.00	LT	135.00	1
970+00.67	LT	135.00	1
973+00.00	LT	135.00	1
974+00.00	LT	120.00	1
988+71.21	LT	120.00	1
1022+63.00	LT	120.00	1
1023+75.00	LT	120.00	1
1023+75.00	LT	180.00	1
1025+00.00	LT	180.00	1
1025+00.00	LT	125.00	1
1031+00.00	LT	125.00	1
1037+99.01	LT	131.53	1
1039+56.79	LT	125.00	1
1054+35.00	LT	125.00	1
1054+35.00	LT	200.00	1
1055+55.00	LT	200.00	1
1055+55.00	LT	135.00	1
1074+00.00	LT	135.00	1
1075+00.00	LT	150.00	1
1078+00.00	LT	160.00	1
1085+00.00	LT	160.00	1
1092+00.00	LT	400.00	1
1104+50.00	LT	154.96	1
1105+80.08	LT	186.26	1
1105+93.73	LT	130.00	1
1111+64.39	LT	130.00	1
1134+88.20	LT	180.00	1
1152+50.00	LT	180.00	1
1154+00.00	LT	120.00	1
1155+00.00	LT	120.00	1
1155+00.00	LT	180.00	1
1159+36.60	LT	180.00	1
795+75.00	RT	220.00	1
824+00.00	RT	125.00	1
844+65.43	RT	125.00	1
852+43.22	RT	125.00	1
859+40.00	RT	125.00	1
859+40.00	RT	200.00	1
861+00.00	RT	200.00	1
861+00.00	RT	140.00	1
871+03.73	RT	140.00	1
873+00.00	RT	165.00	1
882+48.95	RT	165.00	1
886+30.00	RT	165.00	1
886+30.00	RT	215.00	1
901+62.13	RT	120.00	1
912+52.55	RT	120.00	1
912+52.55	RT	140.00	1
927+00.00	RT	140.00	1
927+00.00	RT	200.00	1
928+20.00	RT	200.00	1
928+20.00	RT	120.00	1
946+47.74	RT	120.00	1
954+00.00	RT	120.00	1
959+22.15	RT	130.00	1
959+78.53	RT	130.00	1
960+43.29	RT	229.56	1
961+92.86	RT	130.00	1
970+00.67	RT	130.00	1
975+75.00	RT	145.00	1
988+71.21	RT	145.00	1
994+97.10	RT	145.00	1
999+50.00	RT	135.00	1
1018+50.00	RT	135.00	1
1021+00.00	RT	165.00	1
1022+63.00	RT	161.18	1
1026+88.53	RT	160.00	1

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

66600105

STATION	SIDE	OFFSET	EACH
FAP 310 (US 67)			
1030+00.00	RT	160.00	1
1031+35.98	RT	147.51	1
1031+37.58	RT	196.62	1
1032+25.00	RT	141.09	1
1035+00.00	RT	130.00	1
1039+56.79	RT	120.00	1
1054+60.00	RT	120.00	1
1054+60.00	RT	200.00	1
1056+30.00	RT	200.00	1
1056+30.00	RT	130.00	1
1076+00.00	RT	130.00	1
1078+68.47	RT	175.00	1
1089+68.35	RT	175.00	1
1090+50.00	RT	200.00	1
1096+00.00	RT	200.00	1
1098+00.00	RT	160.00	1
1100+00.00	RT	165.00	1
1103+00.00	RT	190.00	1
1110+00.00	RT	120.00	1
1111+68.98	RT	120.00	1
1111+78.84	RT	181.46	1
1113+45.13	RT	120.00	1
1113+50.18	RT	156.24	1
1119+17.31	RT	120.00	1
1119+17.31	RT	170.00	1
1120+20.00	RT	170.00	1
1120+20.00	RT	120.00	1
1125+75.00	RT	120.00	1
1125+75.00	RT	170.00	1
1126+95.00	RT	170.00	1
1126+95.00	RT	120.00	1
1135+90.00	RT	120.00	1
1135+90.00	RT	200.00	1
1137+10.00	RT	200.00	1
1137+10.00	RT	120.00	1
1142+11.77	RT	120.00	1
1152+02.18	RT	175.35	1
1159+36.60	RT	175.00	1
EX US 67 / IL 104			
565+07.86	RT	40.00	1
564+70.88	RT	79.96	1
564+11.82	RT	102.98	1
563+75.00	RT	55.00	1
563+00.00	RT	40.00	1
RAMP A			
2+90.00	RT	90.00	1
4+49.32	RT	90.00	1
8+50.00	RT	90.00	1
RAMP C			
4+00.00	RT	70.00	1
8+50.00	RT	70.00	1
12+32.96	RT	70.00	1
RAMP D			
9+00.00	RT	110.00	1
11+46.91	RT	90.00	1
16+48.63	RT	90.00	1
18+12.45	RT	90.00	1
CONCORD ROAD			
9+00.00	LT	120.00	1
13+00.00	LT	120.00	1
28+12.26	LT	110.00	1
33+38.98	LT	60.00	1
35+00.00	LT	60.00	1
35+50.00	LT	30.00	1
9+00.00	RT	110.00	1
13+00.00	RT	110.00	1
26+84.77	RT	140.10	1
31+83.04	RT	64.70	1
33+80.31	RT	60.00	1
33+80.33	RT	30.00	1

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

66600105

STATION	SIDE	OFFSET	EACH
US 67 FRONTAGE ROAD (CONCORD)			
9+49.93	LT	40.00	1
10+50.00	LT	60.00	1
17+47.99	LT	60.00	1
20+37.26	LT	60.00	1
23+00.00	LT	110.00	1
28+35.00	LT	110.00	1
31+65.00	LT	110.00	1
34+00.00	LT	110.00	1
37+00.00	LT	90.00	1
38+00.00	LT	70.00	1
40+33.29	LT	70.00	1
40+33.29	LT	130.84	1
42+93.59	LT	71.51	1
46+00.00	LT	50.00	1
50+00.00	LT	50.00	1
51+52.73	LT	40.00	1
17+47.99	RT	60.00	1
20+37.26	RT	60.00	1
24+00.00	RT	90.00	1
28+35.00	RT	90.00	1
31+65.00	RT	100.00	1
34+00.00	RT	90.00	1
36+00.00	RT	70.00	1
40+33.29	RT	70.00	1
42+93.59	RT	70.00	1
45+00.00	RT	70.00	1
COOPER STREET			
444+70.00	RT	30.00	1
444+70.00	RT	52.00	1
445+10.00	RT	52.00	1
445+10.00	RT	42.00	1
445+90.00	RT	42.00	1
447+55.34	RT	69.52	1
442+65.00	LT	31.18	1
445+39.37	LT	40.00	1
447+40.00	LT	40.00	1
447+89.44	LT	49.30	1
NEW STREET			
300+60.00	LT	50.00	1
303+00.00	LT	50.00	1
303+00.00	LT	35.00	1
312+00.00	LT	35.00	1
301+75.00	RT	120.00	1
302+00.00	RT	60.00	1
312+00.00	RT	60.00	1
ASH STREET			
2+00.00	LT	29.21	1
2+46.57	LT	45.00	1
2+80.00	LT	45.00	1
3+00.00	LT	29.35	1
2+51.87	RT	30.56	1
3+94.38	RT	30.41	1
RELOCATED CREWS LANE			
42+00.00	RT	35.00	1
47+00.00	RT	50.00	1
51+51.94	RT	50.00	1
55+24.55	RT	50.00	1
55+24.55	LT	50.00	1
58+00.00	RT	21.02	1
58+00.00	LT	18.98	1
US 67 FRONTAGE ROAD (BETHEL)			
511+00.00	RT	45.00	1
515+24.86	RT	45.00	1
520+66.77	RT	45.00	1
524+15.05	RT	40.00	1
BETHEL LANE			
73+00.00	LT	25.27	1
74+75.00	LT	40.00	1
75+40.63	LT	40.00	1
76+65.56	LT	40.00	1
86+77.39	LT	40.00	1
87+87.40	LT	40.00	1
91+50.00	LT	40.00	1
92+00.00	LT	20.00	1
73+00.00	RT	15.16	1
74+43.68	RT	40.00	1
75+40.63	RT	40.00	1

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

66600105

STATION	SIDE	OFFSET	EACH
BETHEL LANE - CONTINUED			
77+18.54	RT	40.00	1
82+25.00	RT	50.00	1
84+73.35	RT	50.00	1
85+73.35	RT	40.00	1
87+87.40	RT	40.00	1
91+50.00	RT	40.00	1
92+00.00	RT	20.00	1
RELOCATED CYLINDER HEAD ROAD			
94+00.00	LT	27.43	1
95+06.86	LT	40.00	1
96+34.12	LT	40.00	1
100+56.85	LT	55.00	1
103+26.16	LT	40.00	1
108+48.45	LT	40.00	1
109+00.00	LT	40.00	1
94+00.00	RT	32.52	1
95+06.86	RT	45.00	1
100+71.82	RT	45.00	1
103+26.16	RT	45.00	1
108+48.45	RT	45.00	1
109+00.00	RT	45.00	1
OLD CYLINDER HEAD ROAD			
2+68.16	LT	30.00	1
5+19.02	LT	30.00	1
5+50.00	LT	30.00	1
1+05.62	RT	50.00	1
2+68.16	RT	50.00	1
3+30.00	RT	35.64	1
GILLILAND ROAD CONNECTOR			
10+64.31	RT	30.00	1
12+50.00	RT	30.00	1
12+65.00	LT	35.00	1
GILLILAND ROAD EXTENSION			
21+25.00	RT	35.00	1
22+50.00	RT	34.15	1
23+45.43	RT	33.49	1
24+32.48	RT	19.00	1
25+87.79	RT	19.00	1
26+25.00	RT	25.00	1
27+50.00	RT	40.00	1
ST. PAUL'S CHURCH ROAD			
122+85.90	LT	50.00	1
126+00.00	LT	50.00	1
126+94.65	LT	28.45	1
DUTCH LANE EAST			
139+11.25	RT	40.00	1
142+32.37	RT	40.00	1
144+33.90	RT	40.00	1
147+60.33	RT	40.00	1
DUTCH LANE WEST			
194+00.00	LT	13.65	1
195+86.46	LT	35.00	1
197+11.53	LT	50.00	1
205+44.30	LT	112.19	1
206+00.00	LT	130.00	1
207+60.22	LT	80.00	1
210+00.00	LT	70.00	1
212+00.00	LT	23.40	1
194+00.00	RT	34.35	1
197+11.53	RT	60.00	1
201+50.00	RT	65.00	1
202+89.56	RT	50.00	1
205+45.95	RT	60.00	1
208+50.00	RT	90.00	1
210+00.00	RT	90.00	1
212+00.00	RT	24.60	1
HARTS GRAVEL ROAD			
289+00.00	LT	20.00	

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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	45
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

LIGHTING SCHEDULE

PAY ITEM DESCRIPTION	CONCORD RD INTERCHANGE	RELOCATED CREW LANE	BETHEL LANE INTERSECTION	ST. PAUL'S CHURCH RD INTERSECTION	WEST CROSSOVER	TOTALS
ELECTRIC SERVICE INSTALLATION	1	1	1	1	1	5
UNDERGROUND CONDUIT, 2" DIA., INTERMEDIATE METAL	561	230	170	150		1111
UNIT DUCT, 600V, 2-1C NO.8, 1/C NO.8 GROUND, (XLP-TYPE USE), 3/4" DIA. POLYETHYLENE		1500	1300	1310		4210
UNIT DUCT, 600V, 2-1C NO.4, 1/C NO.4 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE	2592					2592
UNIT DUCT, 600V, 2-1C NO.6, 1/C NO.6 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE						5200
AERIAL CABLE, 2-1/C NO. 4 WITH MESSENGER WIRE					2150	2150
LUMINAIRE, SODIUM VAPOR, MULTI-MOUNT, 250 WATT	21	9	8	8		46
LIGHTING CONTROLLER PHOTOCELL RELAY		1	1	1		3
LIGHT POLE, WEATHERING STEEL, 45 FT. M.H., TENON MOUNT	15	5	4	4		28
LIGHT POLE, WEATHERING STEEL, 45 FT. M.H., TENON MOUNT - TWIN	3	2	2	2		9
LIGHT POLE FOUNDATION METAL, 15" BOLT CIRCLE, 8" X 6"	15	5	4	4		28
LIGHT POLE FOUNDATION METAL, 15" BOLT CIRCLE, 8" X 8"	3	2	2	2		9
BREAKAWAY DEVICE, COUPLING, WITH STAINLESS STEEL SCREEN	64	28	24	24		140
RELOCATE EXISTING ELEC. SYSTEM (FROM EXISTING CROSS-OVER)					1	1
LIGHTING CONTROLLER, POLE MOUNTED, 240VOLT, 60AMP						

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 29 OF 42
 DATE 10/10
 DRAWN BY EBB
 CHECKED BY

SIGNING SCHEDULE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	46
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

STATION	LANE	SIDE	FACING	SIGN NUMBER (MUTCD)	SIZE (IN)	MESSAGE	NUMBER OF POSTS	SIGN PANEL			TELESCOPING STEEL SIGN SUPPORT	WOOD SIGN SUPPORT	BASE FOR TELESCOPING STEEL SIGN	STRUCTURAL STEEL SIGN SUPPORT BREAKAWAY		CONCRETE FOUNDATION	
								TYPE 1	TYPE 2	TYPE 3				SIZE	POUND		CU YD
								SQ FT						FOOT	EACH		
FAP 310 (US 67)																	
734+00	WB	OUTSIDE	EAST	SIGN #01	126x84	CONCORD ARENZVILLE 1 MILE	2			73.50				W10x22	1012.00	2.36	
744+25	WB	MEDIAN	EAST	R2-1	48x60	SPEED LIMIT 65	2		20.00			40.00					
744+25	WB	OUTSIDE	EAST	R2-1	48x60	SPEED LIMIT 65	2		20.00			40.00					
748+96	WB	OUTSIDE	EAST	R1-1104	48x96	SPEED LIMIT OVER 4 TONS	2			32.00		40.00					
752+00	WB	MEDIAN	EAST	R1-1104	48x96	SPEED LIMIT OVER 4 TONS	2			32.00		40.00					
759+00	EB	OUTSIDE	WEST	SIGN #02	120x54	JACKSONVILLE 6 TAYLORVILLE 61 ALTON 72	2			45.00				W6x15	540.00	1.40	
760+44		MEDIAN	SOUTH	R1-2	36x36	YIELD	2	9.00			36.00						
			SOUTH	R6-1R	36x12	ONE WAY		3.00									
			NORTH	R6-1L	36x12	ONE WAY		3.00									
761+25	WB	OUTSIDE	NORTH	R1-1	36x36	STOP	2	9.00			38.00						
			NORTH	R6-3A	24x18	DIVIDED HIGHWAY		3.00									
			NORTH	R6-1R	36x12	ONE WAY		3.00									
			SOUTH	R6-1L	36x12	ONE WAY		3.00									
796+96		MEDIAN	NORTH	R1-2	36x36	YIELD	2	9.00			36.00						
			NORTH	R1-1R	36x12	ONE WAY		3.00									
			SOUTH	R1-1L	36x12	ONE WAY		3.00									
763+00	WB	OUTSIDE	EAST	SIGN #03	126x84	CONCORD ARENZVILLE 1/2 MILE	2			73.50			W8x18	756.00	1.40		
766+20	WB	MEDIAN	EAST	R3-1100L	24x24	LEFT TURN LANE	1	4.00			17.00						
768+40	EB	OUTSIDE	WEST	M6-2L	21x15	ARROW		2.19									
				R3-1100R	24x24	RIGHT TURN LANE	1	4.00		17.00							
768+40				M6-2R	21x15	ARROW		2.19									
776+00	EB	OUTSIDE	WEST	R1-1104	48x96	SPEED LIMIT OVER 4 TONS	2			32.00		40.00					
779+00	EB	OUTSIDE	WEST	R2-1	48x60	SPEED LIMIT 65	2		20.00			38.00					
782+50	EB	OUTSIDE	WEST	M3-3	24x12	SOUTH	1	2.00				18.00					
				M1-1100	24x24	US 67		4.00									
782+50	EB	OUTSIDE	WEST	M3-2	24x12	EAST	1	2.00				18.00					
				M1-1100	24x24	IL 104		4.00									
789+57.68	WB	OUTSIDE	EAST	SIGN #04	126x84	CONCORD ARENZVILLE	2			73.50			W10x22	968.00	2.36		
7+50	RAMP A	OUTSIDE	WEST	W1-4L	36x36	CURVE RIGHT AHEAD	1	9.00			17.00						
				W13-1	24x24	40 MPH		4.00									
5+00	RAMP B	OUTSIDE	EAST	W13-3	48x60	RAMP 45 MPH	2		20.00			38.00					
795+50	EB	GORE	WEST	W4-1R	36x36	MERGE ARROW RT	1	9.00				17.00					
796+72.68	WB	GORE	EAST	SIGN #05	54x60	EXIT WITH ARROW	2			27.00		34.00					
11+94	RAMP B	OUTSIDE	EAST	W3-1A	36x36	STOP AHEAD SYMBOL	1	9.00				17.00					
15+50	RAMP B	OUTSIDE	EAST	SIGN #06	96x42	CONCORD ARENZVILLE	2			28.00		40.00					
16+14.18	RAMP B	INSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00				16.00					
16+14.18	RAMP B	OUTSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00				16.00					
18+14	RAMP B	INSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00				17.00					
18+14	RAMP B	OUTSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00				17.00					
19+90.00	RAMP B	ISLAND	EAST	R1-1	36x36	STOP	1	9.00			16.00			1			
				R1-1100	30x18	CROSS TRAFFIC DOES NOT STOP		3.75									
19+80.00	RAMP B	OUTSIDE	EAST	R1-1	36x36	STOP	1	9.00			18.00						
				R1-1100	30x18	CROSS TRAFFIC DOES NOT STOP		3.75									
7+50.00	RAMP C	OUTSIDE	EAST	W1-2R	36x36	CURVE RIGHT AHEAD	1	9.00			17.00						
				W13-1	24x24	40 MPH		4.00									
19+74.00	RAMP D	ISLAND	WEST	R1-1	36x36	STOP	1	9.00			16.00			1			
				R1-1100	30x18	CROSS TRAFFIC DOES NOT STOP		3.75									
19+60.00	RAMP D	OUTSIDE	WEST	R1-1	36x36	STOP	1	9.00			18.00						
				R1-1100	30x18	CROSS TRAFFIC DOES NOT STOP		3.75									
18+80.14	RAMP D	INSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00			17.00						
18+80.14	RAMP D	OUTSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00			17.00						
16+01.93	RAMP D	INSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00			16.00						
16+01.93	RAMP D	OUTSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00			16.00						
15+12.42	RAMP D	OUTSIDE	WEST	SIGN #07	96x42	CONCORD ARENZVILLE	2			28.00		40.00					
12+78	RAMP D	OUTSIDE	EAST	W3-1A	36x36	STOP AHEAD SYMBOL	1	9.00			17.00						
817+30	WB	GORE	EAST	W4-1R	36x36	MERGE ARROW RT	1	9.00			17.00						
819+81.51	EB	GORE	WEST	SIGN #05	54x60	EXIT WITH ARROW	2			27.00		34.00					
5+00	RAMP D	OUTSIDE	WEST	W13-3	48x60	RAMP 45 MPH	2		20.00			38.00					
826+96.51	EB	OUTSIDE	WEST	SIGN #04	126x84	CONCORD ARENZVILLE	2			73.50			W8x18	738.00	1.40		
831+00	WB	OUTSIDE	EAST	M3-1	24x12	NORTH	1	2.00			17.00						
				M1-1100	24x24	US 67		4.00									
831+00	WB	OUTSIDE	EAST	M3-4	24x12	WEST	1	2.00			17.00						
				M1-1100	24x24	IL 104		4.00									
833+00	WB	OUTSIDE	EAST	R2-1	48x60	SPEED LIMIT 65	2		20.00			38.00					
836+00	WB	OUTSIDE	EAST	R1-1104	48x96	SPEED LIMIT OVER 4 TONS	2			32.00		40.00					
840+00	WB	OUTSIDE	EAST	SIGN #12	120x54	MEREDOSIA 13 BEARDSTOWN 23 QUINCY 64	2			45.00			W6x15	555.00	1.40		
849+97	WB	OUTSIDE	EAST	W2-2L	36x36	SIDE ROAD SYMBOL 90 LT	1	9.00			17.00						
				SIGN #13		TO FRONTAGE ROAD											
				SIGN #14		CHAPIN'S BALL PARK											

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 30 OF 42
 DATE 10/10
 DRAWN BY EBB
 CHECKED BY

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SIGNING SCHEDULE - CONTINUED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	47
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

STATION	LANE	SIDE	FACING	SIGN NUMBER (MUTCD)	SIZE (IN)	MESSAGE	NUMBER OF POSTS	SIGN PANEL			TELESCOPING STEEL SIGN SUPPORT	WOOD SIGN SUPPORT	BASE FOR TELESCOPING STEEL SIGN	STRUCTURAL STEEL SIGN SUPPORT BREAKAWAY		CONCRETE FOUNDATION	
								TYPE 1	TYPE 2	TYPE 3				SIZE	POUND		CU YD
								SQ FT						FOOT	EACH		
FAP 310 (US 67)																	
849+97	WB	INSIDE	EAST	W2-2L SIGN #13 SIGN #14	36x36	SIDE ROAD SYMBOL 90 LT TO FRONTAGE ROAD CHAPIN'S BALL PARK	1	9.00									
852+97	WB	INSIDE	EAST	R3-1100L M6-2L	24x24 21x15	LEFT TURN LANE ARROW	1	4.00 2.19									
854+00	EB	OUTSIDE	WEST	SIGN #03	126x84	CONCORD ARENZVILLE 1/2 MILE	2		73.50				W10x22	968.00	2.36		
857+00	WB	INSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00		16.00							
857+00	WB	OUTSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00		16.00							
859+00	WB	INSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00		17.00							
859+00	WB	OUTSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00		17.00							
859+00		MEDIAN	NORTH	R1-2	36x36	YIELD	2	9.00		36.00							
			NORTH	R6-1R	36x12	ONE WAY		3.00									
			SOUTH	R6-1L	36x12	ONE WAY		3.00									
859+58	EB	OUTSIDE	SOUTH	R1-1	36x36	STOP	2	9.00		38.00							
			SOUTH	R6-1R	36x12	ONE WAY		3.00									
			SOUTH	R6-3A	24x18	DIVIDED HIGHWAY		3.00									
			NORTH	R6-1L	36x12	ONE WAY		3.00									
860+30	EB	OUTSIDE	NORTH	R1-1	36x36	STOP	2	9.00		38.00							
			NORTH	R6-1R	36x12	ONE WAY		3.00									
			NORTH	R6-3A	24x18	DIVIDED HIGHWAY		3.00									
			SOUTH	R6-1L	36x12	ONE WAY		3.00									
860+70		MEDIAN	SOUTH	R1-2	36x36	YIELD	2	9.00		36.00							
			SOUTH	R6-1R	36x12	ONE WAY		3.00									
			NORTH	R6-1L	36x12	ONE WAY		3.00									
863+00	EB	OUTSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00		16.00							
863+00	EB	INSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00		16.00							
865+32	EB	INSIDE	WEST	R3-1100L M6-2L	24x24 21x15	LEFT TURN LANE ARROW	1	4.00 2.19		17.00							
866+64	EB	OUTSIDE	WEST	R3-1100R M6-2L	24x24 21x15	RIGHT TURN LANE ARROW	1	4.00 2.19		17.00							
869+64	EB	OUTSIDE	WEST	W2-2L SIGN #13	36x36	SIDE ROAD SYMBOL 90 LT TO FRONTAGE ROAD	1	9.00		17.00							
				SIGN #15		CHAPIN'S BALL PARK											
869+64	EB	INSIDE	WEST	W2-2L SIGN #13 SIGN #15	36x36	SIDE ROAD SYMBOL 90 LT TO FRONTAGE ROAD CHAPIN'S BALL PARK	1	9.00		17.00							
873+90	EB	MEDIAN	NORTH	R6-1R	36x12	ONE WAY	1	3.00		15.00							
877+22	WB	INSIDE	EAST	W2-1 SIGN #16	48x48 48x15	CROSS ROAD SYMBOL COOPER ST. CREWS LANE	2	5.00	16.00	36.00							
877+22	WB	OUTSIDE	EAST	W2-1 SIGN #16	48x48 48x15	CROSS ROAD SYMBOL COOPER ST. CREWS LANE	2	5.00	16.00	36.00							
880+00	EB	OUTSIDE	WEST	SIGN #01	126x84	CONCORD ARENZVILLE 1 MILE	2		73.50				W10x22	968.00	2.36		
880+22	WB	INSIDE	EAST	R3-1100L M6-2L	24x24 21x15	LEFT TURN LANE ARROW	1	4.00 2.19		17.00							
880+22	WB	OUTSIDE	EAST	R3-1100R M6-2L	24x24 21x15	RIGHT TURN LANE ARROW	1	4.00 2.19		17.00							
883+40	WB	INSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00		16.00							
883+40	WB	OUTSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00		16.00							
886+25	WB	INSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00		17.00							
886+25	WB	OUTSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00		17.00							
886+60		MEDIAN	SOUTH	R1-2	36x36	YIELD	2	9.00		36.00							
			SOUTH	R6-1R	36x12	ONE WAY		3.00									
			NORTH	R6-1L	36x12	ONE WAY		3.00									
887+00	EB	OUTSIDE	NORTH	R1-1	36x36	STOP	2	9.00		38.00							
			NORTH	R6-1R	36x12	ONE WAY		3.00									
			NORTH	R6-3A	24x18	DIVIDED HIGHWAY		3.00									
			SOUTH	R6-1L	36x12	ONE WAY		3.00									
887+85	EB	OUTSIDE	SOUTH	R1-1	36x36	STOP	2	9.00		38.00							
			SOUTH	R6-1R	36x12	ONE WAY		3.00									
			SOUTH	R6-3A	24x18	DIVIDED HIGHWAY		3.00									
			NORTH	R6-1L	36x12	ONE WAY		3.00									
888+00		MEDIAN	NORTH	R1-2	36x36	YIELD	2	9.00		36.00							
			NORTH	R6-1R	36x12	ONE WAY		3.00									
			SOUTH	R6-1L	36x12	ONE WAY		3.00									
888+50	EB	OUTSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00		17.00							
888+50	EB	INSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00		17.00							
890+50	EB	OUTSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00		16.00							
890+50	EB	INSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00		16.00							

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 31 OF 42

DATE 10/10
 DRAWN BY EBB
 CHECKED BY

SIGNING SCHEDULE - CONTINUED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	48
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

STATION	LANE	SIDE	FACING	SIGN NUMBER (MUTCD)	SIZE (IN)	MESSAGE	NUMBER OF POSTS	SIGN PANEL			TELESCOPING STEEL SIGN SUPPORT	WOOD SIGN SUPPORT	BASE FOR TELESCOPING STEEL SIGN EACH	STRUCTURAL STEEL SIGN SUPPORT BREAKAWAY		CONCRETE FOUNDATION	
								TYPE 1	TYPE 2	TYPE 3				SIZE	POUND		CU YD
								SQ FT						FOOT			
FAP 310 (US 67)																	
894+50	EB	OUTSIDE	WEST	R3-1100R	24x24	RIGHT TURN LANE	1	4.00									
				M6-2L	21x15	ARROW		2.19									
894+50	EB	INSIDE	WEST	R3-1100L	24x24	LEFT TURN LANE	1	4.00									
				M6-2L	21x15	ARROW		2.19									
897+50	EB	OUTSIDE	WEST	W2-1	48x48	CROSS ROAD SYMBOL	2		16.00								
				SIGN # 17	48x15	CREWS LANE COOPER ST.		5.00									
897+50	EB	OUTSIDE	WEST	W2-1	48x48	CROSS ROAD SYMBOL	2		16.00								
				SIGN # 17	48x15	CREWS LANE COOPER ST.		5.00									
912+90	WB	INSIDE	NORTH	R6-1R	36x12	ONE WAY	1	3.00									
922+28	WB	INSIDE	EAST	R3-1100L	24x24	LEFT TURN LANE	1	4.00									
				M6-2L	21x15	ARROW		2.19									
924+50	WB	INSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00									
924+50	WB	OUTSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00									
926+50	WB	INSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00									
926+50	WB	OUTSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00									
926+80		MEDIAN	SOUTH	R1-2	36x36	YIELD	2	9.00									
			SOUTH	R6-1R	36x12	ONE WAY		3.00									
			NORTH	R6-1L	36x12	ONE WAY		3.00									
927+78	WB	OUTSIDE	NORTH	R1-1	36x36	STOP	2	9.00									
			NORTH	R6-1R	36x12	ONE WAY		3.00									
			NORTH	R6-3A	24x18	DIVIDED HIGHWAY		3.00									
			SOUTH	R6-1L	36x12	ONE WAY		3.00									
928+10		MEDIAN	NORTH	R1-2	36x36	YIELD	2	9.00									
			NORTH	R6-1R	36x12	ONE WAY		3.00									
			SOUTH	R6-1L	36x12	ONE WAY		3.00									
928+50	EB	OUTSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00									
928+50	EB	INSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00									
930+50	EB	OUTSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00									
930+50	EB	INSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00									
932+80	EB	INSIDE	WEST	R3-1100L	24x24	LEFT TURN LANE	1	4.00									
				M6-2L	21x15	ARROW		2.19									
941+50	WB	INSIDE	NORTH	R6-1R	36x12	ONE WAY	1	3.00									
945+06	WB	INSIDE	EAST	W2-1	36x36	CROSS ROAD SYMBOL	1	9.00									
				SIGN #18	36x09	BETHAL LANE		2.25									
945+06	WB	OUTSIDE	EAST	W2-1	36x36	CROSS ROAD SYMBOL	1	9.00									
				SIGN #18	36x09	BETHAL LANE		2.25									
948+00	EB	OUTSIDE	WEST	R1-1104	48x96	SPEED LIMIT OVER 4 TONS	2			32.00							
948+06	WB	INSIDE	EAST	R3-1100L	24x24	LEFT TURN LANE	1	4.00									
				M6-2L	21x15	ARROW		2.19									
948+62	WB	OUTSIDE	EAST	R3-1100R	24x24	RIGHT TURN LANE	1	4.00									
				M6-2L	21x15	ARROW		2.19									
951+00	EB	OUTSIDE	WEST	R2-1	48x60	SPEED LIMIT 65	2		20.00								
952+00	WB	INSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00									
952+00	WB	OUTSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00									
954+00	WB	INSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00									
954+00	WB	OUTSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00									
954+55		MEDIAN	SOUTH	R1-2	36x36	YIELD	2	9.00									
			SOUTH	R6-1R	36x12	ONE WAY		3.00									
			NORTH	R6-1L	36x12	ONE WAY		3.00									
954+60	EB	OUTSIDE	SOUTH	R1-1	36x36	STOP	2	9.00									
			SOUTH	R6-1R	36x12	ONE WAY		3.00									
			SOUTH	R6-3A	24x18	DIVIDED HIGHWAY		3.00									
			NORTH	R6-1L	36x12	ONE WAY		3.00									
955+82	WB	OUTSIDE	NORTH	R1-1	36x36	STOP	2	9.00									
			NORTH	R6-1R	36x12	ONE WAY		3.00									
			NORTH	R6-3A	24x18	DIVIDED HIGHWAY		3.00									
			SOUTH	R6-1L	36x12	ONE WAY		3.00									
956+18		MEDIAN	NORTH	R1-2	36x36	YIELD	2	9.00									
			NORTH	R6-1R	36x12	ONE WAY		3.00									
			SOUTH	R6-1L	36x12	ONE WAY		3.00									
956+50	EB	OUTSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00									
956+50	EB	INSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00									
958+50	EB	OUTSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00									
958+50	EB	INSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00									
960+00	WB	OUTSIDE	EAST	R2-1	48x60	SPEED LIMIT 65	2		20.00								
961+78	EB	OUTSIDE	WEST	R3-1100R	24x24	RIGHT TURN LANE	1	4.00									
				M6-2L	21x15	ARROW		2.19									
963+00	WB	OUTSIDE	EAST	R1-1104	48x96	SPEED LIMIT OVER 4 TONS	2			32.00							

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 32 OF 42
 DATE 10/10
 DRAWN BY EBB
 CHECKED BY

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 Mar-25-2011 03:57:21 PM
 \$FILE\$

SIGNING SCHEDULE - CONTINUED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	49
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

STATION	LANE	SIDE	FACING	SIGN NUMBER (MUTCD)	SIZE (IN)	MESSAGE	NUMBER OF POSTS	SIGN PANEL			TELESCOPING STEEL SIGN SUPPORT	WOOD SIGN SUPPORT	BASE FOR TELESCOPING STEEL SIGN	STRUCTURAL STEEL SIGN SUPPORT BREAKAWAY		CONCRETE FOUNDATION
								72000100	72000200	72000300				SIZE	POUND	
								TYPE 1	TYPE 2	TYPE 3						
FAP 310 (US 67)																
962+44	EB	INSIDE	WEST	R3-1100L	24x24	LEFT TURN LANE	1	4.00				17.00				
				M6-2L	21x15	ARROW		2.19								
965+44	EB	OUTSIDE	WEST	W2-1	36x36	CROSS ROAD SYMBOL	1	9.00				17.00				
				SIGN #18	36x09	BETHAL LANE		2.25								
965+44	EB	INSIDE	WEST	W2-1	36x36	CROSS ROAD SYMBOL	1	9.00				17.00				
				SIGN #18	36x09	BETHAL LANE		2.25								
970+23	WB	INSIDE	EAST	R3-1100L	24x24	LEFT TURN LANE	1	4.00				17.00				
				M6-2L	21x15	ARROW		2.19								
972+50	WB	INSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00				16.00				
972+50	WB	OUTSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00				16.00				
974+50	WB	INSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00				17.00				
974+50	WB	OUTSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00				17.00				
974+92		MEDIAN		R1-2	36x36	YIELD	2	9.00				36.00				
				R6-1R	36x12	ONE WAY		3.00								
				R6-1L	36x12	ONE WAY		3.00								
975+25	EB	OUTSIDE		R1-1	36x36	STOP	2	9.00				38.00				
				R6-1R	36x12	ONE WAY		3.00								
				R6-3A	24x18	DIVIDED HIGHWAY		3.00								
				R6-1L	36x12	ONE WAY		3.00								
975+65	WB	OUTSIDE		R1-1	36x36	STOP	2	9.00				38.00				
				R6-1R	36x12	ONE WAY		3.00								
				R6-3A	24x18	DIVIDED HIGHWAY		3.00								
				R6-1L	36x12	ONE WAY		3.00								
976+15		MEDIAN		R1-2	36x36	YIELD	2	9.00				36.00				
				R6-1R	36x12	ONE WAY		3.00								
				R6-1L	36x12	ONE WAY		3.00								
976+50	EB	OUTSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00				17.00				
976+50	EB	INSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00				17.00				
978+50	EB	OUTSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00				16.00				
978+50	EB	INSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00				16.00				
980+75	EB	INSIDE	WEST	R3-1100L	24x24	LEFT TURN LANE	1	4.00				17.00				
				M6-2L	21x15	ARROW		2.19								
988+56	WB	INSIDE	EAST	W2-2R	36x36	SIDE ROAD SYMBOL 90 RT	1	9.00				17.00				
				SIGN #19	36x15	ST. PAULS CHURCH RD		3.75								
988+56	WB	OUTSIDE	EAST	W2-2R	36x36	SIDE ROAD SYMBOL 90 RT	1	9.00				17.00				
				SIGN #19	36x15	ST. PAULS CHURCH RD		3.75								
991+56	WB	OUTSIDE	EAST	R3-1100R	24x24	RIGHT TURN LANE	1	4.00				17.00				
				M6-2L	21x15	ARROW		2.19								
992+76	WB	INSIDE	EAST	R3-1100L	24x24	LEFT TURN LANE	1	4.00				17.00				
				M6-2L	21x15	ARROW		2.19								
995+00	WB	INSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00				16.00				
995+00	WB	OUTSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00				16.00				
997+50	WB	INSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00				17.00				
997+50	WB	OUTSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00				17.00				
997+75		MEDIAN		R1-2	36x36	YIELD	2	9.00				36.00				
				R6-1R	36x12	ONE WAY		3.00								
				R6-1L	36x12	ONE WAY		3.00								
998+20	EB	OUTSIDE		R1-1	36x36	STOP	2	9.00				38.00				
				R6-1R	36x12	ONE WAY		3.00								
				R6-3A	24x18	DIVIDED HIGHWAY		3.00								
				R6-1L	36x12	ONE WAY		3.00								
998+70	WB	OUTSIDE		R1-1	36x36	STOP	2	9.00				38.00				
				R6-1R	36x12	ONE WAY		3.00								
				R6-3A	24x18	DIVIDED HIGHWAY		3.00								
				R6-1L	36x12	ONE WAY		3.00								
999+15		MEDIAN		R1-2	36x36	YIELD	2	9.00				36.00				
				R6-1R	36x12	ONE WAY		3.00								
				R6-1L	36x12	ONE WAY		3.00								
999+50	EB	OUTSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00				17.00				
999+50	EB	INSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00				17.00				
1001+50	EB	OUTSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00				16.00				
1001+50	EB	INSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00				16.00				
1005+45	EB	INSIDE	WEST	R3-1100L	24x24	LEFT TURN LANE	1	4.00				17.00				
				M6-2L	21x15	ARROW		2.19								
1007+45	EB	INSIDE	SOUTH	R6-1R	36x12	ONE WAY	1	3.00				15.00				
1008+45	EB	OUTSIDE	WEST	W2-2L	36x36	SIDE ROAD SYMBOL 90 LT	1	9.00				17.00				
				SIGN #19	36x15	ST. PAULS CHURCH RD		3.75								

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 33 OF 42
 DRAWN BY EBB
 CHECKED BY
 DATE 10/10

SIGNING SCHEDULE - CONTINUED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	50
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

STATION	LANE	SIDE	FACING	SIGN NUMBER (MUTCD)	SIZE (IN)	MESSAGE	NUMBER OF POSTS	SIGN PANEL			TELESCOPING STEEL SIGN SUPPORT	WOOD SIGN SUPPORT	BASE FOR TELESCOPING STEEL SIGN	STRUCTURAL STEEL SIGN SUPPORT BREAKAWAY		CONCRETE FOUNDATION	
								TYPE 1	TYPE 2	TYPE 3				SIZE	POUND		CU YD
								SQ FT						FOOT			
FAP 310 (US 67)																	
1008+45	EB	INSIDE	WEST	W2-2L SIGN #19	36x36 36x15	SIDE ROAD SYMBOL 90 LT ST. PAULS CHURCH RD	1	9.00									
1015+40	WB	INSIDE	NORTH	R6-1R	36x12	ONE WAY	1	3.00									
1021+96	WB	INSIDE	EAST	W2-2L SIGN #20	36x36 54x09	SIDE ROAD SYMBOL 90 LT DUTCH LANE EAST	2	9.00 3.38									
1021+96	WB	OUTSIDE	EAST	W2-2L SIGN #20	36x36 54x09	SIDE ROAD SYMBOL 90 LT DUTCH LANE EAST	2	9.00 3.38									
1023+40	WB	INSIDE	NORTH	R6-1R	36x12	ONE WAY	1	3.00									
1024+35	EB	INSIDE	SOUTH	R6-1R	36x12	ONE WAY	1	3.00									
1024+96	WB	INSIDE	EAST	R3-1100L M6-2L	24x24 21x15	LEFT TURN LANE ARROW	1	4.00 2.19									
1029+00	WB	INSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00									
1029+00	WB	OUTSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00									
1031+00	WB	INSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00									
1031+00	WB	OUTSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00									
1031+10		MEDIAN	SOUTH	R1-2	36x36	YIELD	2	9.00									
	SOUTH		R6-1R	36x12	ONE WAY		3.00										
	NORTH		R6-1L	36x12	ONE WAY		3.00										
1031+60	EB	OUTSIDE	SOUTH	R1-1	36x36	STOP	2	9.00									
	SOUTH		R6-1R	36x12	ONE WAY		3.00										
	SOUTH		R6-3A	24x18	DIVIDED HIGHWAY		3.00										
	NORTH		R6-1L	36x12	ONE WAY		3.00										
1032+25	WB	OUTSIDE	NORTH	R1-1	36x36	STOP	2	9.00									
	NORTH		R6-1R	36x12	ONE WAY		3.00										
	NORTH		R6-3A	24x18	DIVIDED HIGHWAY		3.00										
	SOUTH		R6-1L	36x12	ONE WAY		3.00										
1032+55		MEDIAN	NORTH	R1-2	36x36	YIELD	2	9.00									
	NORTH		R6-1R	36x12	ONE WAY		3.00										
	SOUTH		R6-1L	36x12	ONE WAY		3.00										
1033+00	EB	OUTSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00									
1033+00	EB	INSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00									
1035+00	EB	OUTSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00									
1035+00	EB	INSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00									
1036+99	EB	INSIDE	WEST	R3-1100L M6-2L	24x24 21x15	LEFT TURN LANE ARROW	1	4.00 2.19									
1038+87	EB		OUTSIDE	WEST	R3-1100R M6-2L	24x24 21x15	RIGHT TURN LANE ARROW	1	4.00 2.19								
1041+87	EB		OUTSIDE	WEST	W2-2R SIGN #20	36x36 54x09	SIDE ROAD SYMBOL 90 RT DUTCH LANE EAST	2	9.00 3.38								
1041+87	EB	INSIDE	WEST	W2-2R SIGN #20	36x36 54x09	SIDE ROAD SYMBOL 90 RT DUTCH LANE EAST	2	9.00 3.38									
1049+75	WB	INSIDE	EAST	R3-1100L M6-2L	24x24 21x15	LEFT TURN LANE ARROW	1	4.00 2.19									
1052+00	WB		INSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00								
1052+00	WB	OUTSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00									
1054+00	WB	INSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00									
1054+00	WB	OUTSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00									
1054+38		MEDIAN	SOUTH	R1-2	36x36	YIELD	2	9.00									
	SOUTH		R6-1R	36x12	ONE WAY		3.00										
	NORTH		R6-1L	36x12	ONE WAY		3.00										
1054+68	EB	OUTSIDE	SOUTH	R1-1	36x36	STOP	2	9.00									
	SOUTH		R6-1R	36x12	ONE WAY		3.00										
	SOUTH		R6-3A	24x18	DIVIDED HIGHWAY		3.00										
	NORTH		R6-1L	36x12	ONE WAY		3.00										
1055+20	WB	OUTSIDE	NORTH	R1-1	36x36	STOP	2	9.00									
	NORTH		R6-1R	36x12	ONE WAY		3.00										
	NORTH		R6-3A	24x18	DIVIDED HIGHWAY		3.00										
	SOUTH		R6-1L	36x12	ONE WAY		3.00										
1055+60		MEDIAN	NORTH	R1-2	36x36	YIELD	2	9.00									
	NORTH		R6-1R	36x12	ONE WAY		3.00										
	SOUTH		R6-1L	36x12	ONE WAY		3.00										
1056+00	EB	OUTSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00									
1056+00	EB	INSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00									
1058+00	EB	OUTSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00									
1058+00	EB	INSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00									
1060+25	EB	INSIDE	WEST	R3-1100L M6-2L	24x24 21x15	LEFT TURN LANE ARROW	1	4.00 2.19									

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 34 OF 42
 DATE 10/10
 DRAWN BY EBB
 CHECKED BY

3:57:22 PM

Mar-25-2011 03:57:22PM

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SIGNING SCHEDULE - CONTINUED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	51
STA. TO STA.		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		

STATION	LANE	SIDE	FACING	SIGN NUMBER (MUTCD)	SIZE (IN)	MESSAGE	NUMBER OF POSTS	SIGN PANEL			TELESCOPING STEEL SIGN SUPPORT	WOOD SIGN SUPPORT	BASE FOR TELESCOPING STEEL SIGN	STRUCTURAL STEEL SIGN SUPPORT BREAKAWAY		CONCRETE FOUNDATION	
								TYPE 1	TYPE 2	TYPE 3				SIZE	POUND		CU YD
FAP 310 (US 67)																	
1066+71	WB	INSIDE	EAST	W2-1	36x36	CROSS ROAD SYMBOL	2	9.00									
				SIGN #21	54x09	DUTCH LANE WEST		3.38									
1066+71	WB	OUTSIDE	EAST	W2-1	36x36	CROSS ROAD SYMBOL	2	9.00									
				SIGN #21	54x09	DUTCH LANE WEST		3.38									
1069+71	WB	INSIDE	EAST	R3-1100L	24x24	LEFT TURN LANE	1	4.00									
				M6-2L	21x15	ARROW		2.19									
1070+37	WB	OUTSIDE	EAST	R3-1100R	24x24	RIGHT TURN LANE	1	4.00									
				M6-2L	21x15	ARROW		2.19									
1073+75	WB	INSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00									
1073+75	WB	OUTSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00									
1075+75	WB	INSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00									
1075+75	WB	OUTSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00									
1076+30		MEDIAN	SOUTH	R1-2	36x36	YIELD	2	9.00									
			SOUTH	R6-1R	36x12	ONE WAY		3.00									
			NORTH	R6-1L	36x12	ONE WAY		3.00									
1076+25	EB	OUTSIDE	SOUTH	R1-1	36x36	STOP	2	9.00									
			SOUTH	R6-1R	36x12	ONE WAY		3.00									
			SOUTH	R6-3A	24x18	DIVIDED HIGHWAY		3.00									
			NORTH	R6-1L	36x12	ONE WAY		3.00									
1077+45	WB	OUTSIDE	NORTH	R1-1	36x36	STOP	2	9.00									
			NORTH	R6-1R	36x12	ONE WAY		3.00									
			NORTH	R6-3A	24x18	DIVIDED HIGHWAY		3.00									
			SOUTH	R6-1L	36x12	ONE WAY		3.00									
1077+50		MEDIAN	NORTH	R1-2	36x36	YIELD	2	9.00									
			NORTH	R6-1R	36x12	ONE WAY		3.00									
			SOUTH	R6-1L	36x12	ONE WAY		3.00									
1078+00	EB	OUTSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00									
1078+00	EB	INSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00									
1080+00	EB	OUTSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00									
1080+00	EB	INSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00									
1080+00	WB	INSIDE	EAST	R2-5C	48x60	SPEED ZONE AHEAD WITH FLAG	2		20.00								
				R2-1101	30x30	55 MPH		6.25									
1080+00	WB	OUTSIDE	EAST	R2-5C	48x60	SPEED ZONE AHEAD WITH FLAG	2		20.00								
				R2-1101	30x30	55 MPH		6.25									
1084+00	WB	INSIDE	EAST	R2-1101	30x30	55 MPH WITH FLAG	1	6.25									
1084+00	WB	OUTSIDE	EAST	R2-1101	30x30	55 MPH WITH FLAG	1	6.25									
1083+43	EB	OUTSIDE	WEST	R3-1100R	24x24	RIGHT TURN LANE	1	4.00									
				M6-2L	21x15	ARROW		2.19									
1084+09	EB	INSIDE	WEST	R3-1100L	24x24	LEFT TURN LANE	1	4.00									
				M6-2L	21x15	ARROW		2.19									
1087+09	EB	OUTSIDE	WEST	W2-1	36x36	CROSS ROAD SYMBOL	2	9.00									
				SIGN #21	54x09	DUTCH LANE WEST		3.38									
1087+09	EB	INSIDE	WEST	W2-1	36x36	CROSS ROAD SYMBOL	2	9.00									
				SIGN #21	54x09	DUTCH LANE WEST		3.38									
1087+00	WB	INSIDE	NORTH	R6-1R	36x12	ONE WAY	1	3.00									
1090+18	WB	INSIDE	EAST	W2-2L	36x36	SIDE ROAD SYMBOL 90 LT	2	9.00									
				SIGN #22	57x09	HART'S GRAVEL RD		3.56									
1090+18	WB	OUTSIDE	EAST	W2-2L	36x36	SIDE ROAD SYMBOL 90 LT	2	9.00									
				SIGN #22	57x09	HART'S GRAVEL RD		3.56									
1093+18	WB	INSIDE	EAST	R3-1100L	24x24	LEFT TURN LANE	1	4.00									
				M6-2L	21x15	ARROW		2.19									
1094+00	EB	OUTSIDE	WEST	R1-1104	48x96	SPEED LIMIT OVER 4 TONS	2										
1097+00	EB	OUTSIDE	WEST	R2-1	48x60	SPEED LIMIT 65	2		20.00								
1097+25	WB	INSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00									
1097+25	WB	OUTSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00									
1099+25	WB	INSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00									
1099+25	WB	OUTSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00									
1099+70		MEDIAN	SOUTH	R1-2	36x36	YIELD	2	9.00									
			SOUTH	R6-1R	36x12	ONE WAY		3.00									
			NORTH	R6-1L	36x12	ONE WAY		3.00									
1099+85	EB	OUTSIDE	SOUTH	R1-1	36x36	STOP	2	9.00									
			SOUTH	R6-1R	36x12	ONE WAY		3.00									
			SOUTH	R6-3A	24x18	DIVIDED HIGHWAY		3.00									
			NORTH	R6-1L	36x12	ONE WAY		3.00									
1100+65	WB	OUTSIDE	NORTH	R1-1	36x36	STOP	2	9.00									
			NORTH	R6-1R	36x12	ONE WAY		3.00									
			NORTH	R6-3A	24x18	DIVIDED HIGHWAY		3.00									
			SOUTH	R6-1L	36x12	ONE WAY		3.00									

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 35 OF 42
 DATE 10/10
 DRAWN BY EBB
 CHECKED BY

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 Mar-25-2011 03:57:23 PM
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SIGNING SCHEDULE - CONTINUED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	52

STA.	TO STA.
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

STATION	LANE	SIDE	FACING	SIGN NUMBER (MUTCD)	SIZE (IN)	MESSAGE	NUMBER OF POSTS	SIGN PANEL			TELESCOPING STEEL SIGN SUPPORT	WOOD SIGN SUPPORT	BASE FOR TELESCOPING STEEL SIGN	STRUCTURAL STEEL SIGN SUPPORT BREAKAWAY		CONCRETE FOUNDATION
								TYPE 1	TYPE 2	TYPE 3				SIZE	POUND	
FAP 310 (US 67)																
1100+90		MEDIAN	NORTH	R1-2	36x36	YIELD	2	9.00								
			NORTH	R6-1R	36x12	ONE WAY		3.00								
			SOUTH	R6-1L	36x12	ONE WAY		3.00								
1101+25	EB	OUTSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00								
1101+25	EB	INSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00								
1103+25	EB	OUTSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00								
1103+25	EB	INSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00								
1105+10	EB	INSIDE	SOUTH	R6-1R	36x12	ONE WAY	1	3.00								
1106+00	WB	INSIDE	EAST	W9-1L	36x36	LEFT LANE ENDS AHEAD WITH FLAG	1	9.00								
1106+00	WB	OUTSIDE	EAST	W9-1L	36x36	LEFT LANE ENDS AHEAD WITH FLAG	1	9.00								
1105+69	EB	INSIDE	WEST	R3-1100L	24x24	LEFT TURN LANE	1	4.00								
				M6-2L	21x15	ARROW		2.19								
1106+90	EB	OUTSIDE	WEST	R3-1100R	24x24	RIGHT TURN LANE	1	4.00								
				M6-2L	21x15	ARROW		2.19								
1109+00	WB	INSIDE	EAST	W4-2L	36x36	MERGE RIGHT AHEAD WITH FLAG	1	9.00								
1109+00	WB	OUTSIDE	EAST	W4-2L	36x36	MERGE RIGHT AHEAD WITH FLAG	1	9.00								
1109+90	EB	OUTSIDE	WEST	W2-2R	36x36	SIDE ROAD SYMBOL 90 RT	2	9.00								
				SIGN #22	57x09	HART'S GRAVEL RD		3.56								
1109+90	EB	INSIDE	WEST	W2-2R	36x36	SIDE ROAD SYMBOL 90 RT	2	9.00								
				SIGN #22	57x09	HART'S GRAVEL RD		3.56								
1111+25	EB	INSIDE	SOUTH	R6-1R	36x12	ONE WAY	1	3.00								
1115+00	WB	INSIDE	EAST	W1-8	18x24	CHEVRON	1	3.00								
1116+00	WB	INSIDE	EAST	W1-8	18x24	CHEVRON	1	3.00								
1117+00	WB	INSIDE	EAST	W1-8	18x24	CHEVRON	1	3.00								
1118+00	WB	INSIDE	EAST	W1-8	18x24	CHEVRON	1	3.00								
1119+00	WB	INSIDE	EAST	W1-8	18x24	CHEVRON	1	3.00								
1119+00	EB	INSIDE	SOUTH	R6-1R	36x12	ONE WAY	1	3.00								
1119+70	WB	INSIDE	NORTH	R6-1R	36x12	ONE WAY	1	3.00								
1120+00	WB	INSIDE	EAST	W1-8	18x24	CHEVRON	1	3.00								
1121+00	WB	INSIDE	EAST	W1-8	18x24	CHEVRON	1	3.00								
1122+00	WB	INSIDE	EAST	W1-8	18x24	CHEVRON	1	3.00								
1123+00	WB	INSIDE	EAST	W1-8	18x24	CHEVRON	1	3.00								
1123+25	WB	INSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00								
1123+25	WB	OUTSIDE	WEST	R5-1A	36x24	WRONG WAY	1	6.00								
1124+00	WB	INSIDE	EAST	W1-8	18x24	CHEVRON	1	3.00								
1125+00	WB	INSIDE	EAST	W1-8	18x24	CHEVRON	1	3.00								
1125+25	WB	INSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00								
1125+25	WB	OUTSIDE	WEST	R5-1	36x36	DO NOT ENTER	1	9.00								
1125+60		MEDIAN	SOUTH	R1-2	36x36	YIELD	2	9.00								
			SOUTH	R6-1R	36x12	ONE WAY		3.00								
			NORTH	R6-1L	36x12	ONE WAY		3.00								
1125+90	EB	OUTSIDE	SOUTH	R1-1	36x36	STOP	2	9.00								
			SOUTH	R6-1R	36x12	ONE WAY		3.00								
			SOUTH	R6-3A	24x18	DIVIDED HIGHWAY		3.00								
			NORTH	R6-1L	36x12	ONE WAY		3.00								
1126+50	WB	OUTSIDE	NORTH	R1-1	36x36	STOP	2	9.00								
			NORTH	R6-1R	36x12	ONE WAY		3.00								
			NORTH	R6-3A	24x18	DIVIDED HIGHWAY		3.00								
			SOUTH	R6-1L	36x12	ONE WAY		3.00								
1126+80		MEDIAN	NORTH	R1-2	36x36	YIELD	2	9.00								
			NORTH	R6-1R	36x12	ONE WAY		3.00								
			SOUTH	R6-1L	36x12	ONE WAY		3.00								
1127+25	EB	OUTSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00								
1127+25	EB	INSIDE	EAST	R5-1	36x36	DO NOT ENTER	1	9.00								
1129+25	EB	OUTSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00								
1129+25	EB	INSIDE	EAST	R5-1A	36x24	WRONG WAY	1	6.00								
1130+00	WB	OUTSIDE	EAST	W1-4R	36x36	REVERSE CURVE RIGHT AHEAD	1	9.00								
1133+55	EB	INSIDE	SOUTH	R6-1R	36x12	ONE WAY	1	3.00								
1139+60		MEDIAN	EAST	W6-3	36x36	TWO-WAY TRAFFIC AHEAD	2	9.00								
			WEST	R4-7	48x60	KEEP RIGHT				20.00						
1048+00		OUTSIDE	WEST	W6-1	36x36	DIVIDED HIGHWAY AHEAD	1	9.00								
1151+00		OUTSIDE	WEST	W1-4R	36x36	REVERSE CURVE RIGHT AHEAD	1	9.00								
SUB TOTALS - US 67								2263.36	304.00	865.00	5719.00	440.00	2	6505.00	15.04	

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 36 OF 42

DATE 10/10
 DRAWN BY EBB
 CHECKED BY

SIGNING SCHEDULE - CONTINUED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	53
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

STATION	LANE	SIDE	FACING	SIGN NUMBER (MUTCD)	SIZE (IN)	MESSAGE	NUMBER OF POSTS	SIGN PANEL			TELESCOPING STEEL SIGN SUPPORT	WOOD SIGN SUPPORT	BASE FOR TELESCOPING STEEL SIGN	STRUCTURAL STEEL SIGN SUPPORT BREAKAWAY		CONCRETE FOUNDATION		
								TYPE 1	TYPE 2	TYPE 3				SIZE	POUND		CU YD	
																		SO FT
114+85	OLD 67	OUTSIDE	SOUTH	R1-1	36x36	STOP	1	9.00										
109+00	OLD 67	OUTSIDE	WEST	M4-5	24x12	TO	1	2.00										
				M1-1100	24x24	US 67		4.00										
				M6-1	21x15	ARROW		2.19										
109+00	OLD 67	OUTSIDE	WEST	M4-5	24x12	TO	1	2.00										
				M1-1100	24x24	IL 104		4.00										
				M6-1	21x15	ARROW		2.19										
106+25	OLD 67	OUTSIDE	WEST	W3-1a	36x36	STOP AHEAD	1	9.00										
86+00	OLD 67	OUTSIDE	EAST	W1-4L	36x36	REVERSE CURVE LT AHEAD	1	9.00										
				W13-1	24x24	40 MPH		4.00										
22+00	US 67 FRONTAGE RD	OUTSIDE	EAST	W3-1a	36x36	STOP AHEAD	1	9.00										
25+50	US 67 FRONTAGE RD	OUTSIDE	EAST	SIGN #06	96x42	CONCORD ARENZVILLE	2			28.00		40.00						
28+70	US 67 FRONTAGE RD	OUTSIDE	WEST	W1-4L	36x36	REVERSE CURVE LEFT AHEAD	1	9.00										
				W13-1	24x24	40 MPH		4.00										
29+40	US 67 FRONTAGE RD	OUTSIDE	EAST	R1-1	36x36	STOP	1	9.00										
				R1-4	18x06	ALL WAYS		0.75										
30+16	US 67 FRONTAGE RD	OUTSIDE	NORTH	W1-7	48x24	DOUBLE HEADED LARGE ARROW	2	8.00										
30+81	US 67 FRONTAGE RD	OUTSIDE	WEST	R1-1	36x36	STOP	1	9.00										
				R1-4	18x06	ALL WAYS		0.75										
31+50	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-4R	36x36	REVERSE CURVE RIGHT AHEAD	1	9.00										
				W13-1	24x24	40 MPH		4.00										
34+00	US 67 FRONTAGE RD	OUTSIDE	WEST	SIGN #07	96x42	CONCORD ARENZVILLE	2			28.00		40.00						
38+00	US 67 FRONTAGE RD	OUTSIDE	WEST	W3-1a	36x36	STOP AHEAD	1	9.00										
47+00	US 67 FRONTAGE RD	OUTSIDE	WEST	W1-4R	36x36	REVERSE CURVE RIGHT AHEAD	1	9.00										
				W13-1	24x24	40 MPH		4.00										
11+00	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
12+60	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
14+20	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
15+80	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
17+40	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
21+60	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
23+20	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
24+80	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
26+40	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
28+00	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
32+00	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
33+60	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
35+20	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
36+80	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
38+20	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
39+80	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
43+00	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
44+60	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
46+20	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										
47+80	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00										
				W1-8R	18x24	CHEVRON		3.00										

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 37 OF 42
 DATE 10/10
 DRAWN BY EBB
 CHECKED BY

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 Mar-25-2011 03:57:24PM
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SIGNING SCHEDULE - CONTINUED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	54
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

STATION	LANE	SIDE	FACING	SIGN NUMBER (MUTCD)	SIZE (IN)	MESSAGE	NUMBER OF POSTS	SIGN PANEL			TELESCOPING STEEL SIGN SUPPORT	WOOD SIGN SUPPORT	BASE FOR TELESCOPING STEEL SIGN	STRUCTURAL STEEL SIGN SUPPORT BREAKAWAY		CONCRETE FOUNDATION	
								TYPE 1	TYPE 2	TYPE 3				SIZE	POUND		CU YD
								SQ FT						FOOT	EACH		
49+40	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00				14.00					
			WEST	W1-8R	18x24	CHEVRON		3.00									
51+00	US 67 FRONTAGE RD	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00				14.00					
			WEST	W1-8R	18x24	CHEVRON		3.00									
23+00	EX US 67/ IL 104	OUTSIDE	EAST	W3-1a	36x36	STOP AHEAD SYMBOL	1	9.00				17.00					
21+00	EX US 67/ IL 104	OUTSIDE	EAST	W2-2R	36x36	SIDE ROAD SYMBOL 90 RT	1	9.00				17.00					
19+00	EX US 67/ IL 104	OUTSIDE	EAST	M4-5	24x12	TO	1	2.00				17.00					
				M1-1100	24x24	US 67		4.00									
				M6-1	21x15	ARROW		2.19									
19+00	EX US 67/ IL 104	OUTSIDE	EAST	M4-5	24x12	TO	1	2.00				17.00					
				M1-1100	24x24	IL 104		4.00									
				M6-1	21x15	ARROW		2.19									
15+60	EX US 67/ IL 104	OUTSIDE	NORTH	W1-7	48x24	DOUBLE HEAD ARROW	2	8.00				17.00					
				SIGN # 15		CHAPIN'S BALL PARK											
15+22.38	EX US 67/ IL 104	OUTSIDE	EAST	R1-1	36x36	STOP	1	9.00				18.00					
16+21.77	EX US 67/ IL 104	OUTSIDE	WEST	R1-1	36x36	STOP	1	9.00				18.00					
14+50	EX US 67/ IL 104	OUTSIDE	EAST	W14-1	30x30	DEAD END	1	6.25				17.00					
12+00	EX US 67/ IL 104	OUTSIDE	WEST	M4-5	24x12	TO	1	2.00				17.00					
				M1-1100	24x24	US 67		4.00									
				M6-1	21x15	ARROW		2.19									
12+00	EX US 67/ IL 104	OUTSIDE	WEST	M4-5	24x12	TO	1	2.00				17.00					
				M1-1100	24x24	IL 104		4.00									
				M6-1	21x15	ARROW		2.19									
10+00	EX US 67/ IL 104	OUTSIDE	WEST	W2-2L	36x36	SIDE ROAD SYMBOL 90 LT	1	9.00				17.00					
8+00	EX US 67/ IL 104	OUTSIDE	EAST	W3-1a	36x36	STOP AHEAD SYMBOL	1	9.00				17.00					
584+00	EX US 67/ IL 104	OUTSIDE	EAST	W8-1108	36x36	ROAD ENDS 500 FT	1	9.00				17.00					
578+75	EX US 67/ IL 104	OUTSIDE	EAST	R11-1108	36x36	ROAD ENDS	1	9.00				17.00					
				OM4-1	18x18	OBJECT MARKER	1	2.25				16.00					
				OM4-1	18x18	OBJECT MARKER	1	2.25				16.00					
7+82	CONCORD RD	OUTSIDE	NORTH	R1-1	36x36	STOP	1	9.00				17.00					
				R1-4	18x06	ALL WAYS	1	0.75									
7+85	CONCORD RD	MEDIAN	SOUTH	R4-7	24x30	KEEP RIGHT SYMBOL	1	5.00				16.00					
10+00	CONCORD RD	OUTSIDE	NORTH	SIGN #15		CHAPIN'S BALL PARK	2					17.00					
				SIGN # 23		CHAPIN CEMETARY											
11+00	CONCORD RD	OUTSIDE	SOUTH	SIGN #08	198x114	BEARSTOWN/JACKSONVILLE	1							W14x38	1900.00	4.18	
13+00	CONCORD RD	OUTSIDE	NORTH	W3-1a	36x36	STOP AHEAD	1	9.00				17.00					
15+90	CONCORD RD	MEDIAN	SOUTH	R4-7	24x30	KEEP RIGHT SYMBOL	1	5.00				17.00					
18+00	CONCORD RD	OUTSIDE	NORTH	SIGN #09	196x60	JACKSONVILLE	2							W6x15	555.00	1.40	
18+72.61	CONCORD RD	MEDIAN	NORTH	R3-1100L	24x24	LEFT TURN LANE	1	4.00				17.00					
				M6-2L	21x15	ARROW		2.19									
21+30.28	CONCORD RD	MEDIAN	SOUTH	R3-1100L	24x24	LEFT TURN LANE	1	4.00				17.00					
				M6-2L	21x15	ARROW		2.19									
22+00	CONCORD RD	OUTSIDE	SOUTH	SIGN #10	196x60	BEARSTOWN	2							W6x15	570.00	1.40	
24+15.41	CONCORD RD	MEDIAN	NORTH	R4-7	24x30	KEEP RIGHT SYMBOL	1	5.00				17.00					
26+70.00	CONCORD RD	OUTSIDE	SOUTH	W1-2R	36x36	CURVE RIGHT AHEAD	1	9.00				17.00					
31+00	CONCORD RD	OUTSIDE	NORTH	SIGN #11	198x114	JACKSONVILLE/BEARSTOWN	1							W14x38	1976.00	3.80	
35+50	CONCORD RD	OUTSIDE	NORTH	W6-1	36x36	DIVIDED HIGHWAY AHEAD	1	9.00				17.00					
37+50	CONCORD RD	OUTSIDE	NORTH	W1-2R	36x36	CURVE RIGHT AHEAD	1	9.00				17.00					
300+42	NEW STREET	OUTSIDE	WEST	R1-1	36x36	STOP	1	9.00				17.00					
				R1-1100	30x18	CROSS TRAFFIC DOES NOT STOP		3.75									
312+95	NEW STREET	OUTSIDE	EAST	R1-1	36x36	STOP	1	9.00				17.00					
				R1-1100	30x18	CROSS TRAFFIC DOES NOT STOP		3.75									
499+00	COOPER STREET	OUTSIDE	SOUTH	R1-1	36x36	STOP	1	9.00				18.00					
				R1-1100	30x18	CROSS TRAFFIC DOES NOT STOP		3.75									

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 38 OF 42
 DATE 10/10
 DRAWN BY EBB
 CHECKED BY

SIGNING SCHEDULE - CONTINUED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	55
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

STATION	LANE	SIDE	FACING	SIGN NUMBER (MUTCD)	SIZE (IN)	MESSAGE	NUMBER OF POSTS	SIGN PANEL			TELESCOPING STEEL SIGN SUPPORT	WOOD SIGN SUPPORT	BASE FOR TELESCOPING STEEL SIGN	STRUCTURAL STEEL SIGN SUPPORT BREAKAWAY		CONCRETE FOUNDATION	
								TYPE 1	TYPE 2	TYPE 3				SIZE	POUND		CU YD
38+75	RELOCATED CREWS LN	OUTSIDE	WEST	R11-I100	36x36	ROAD ENDS	1	9.00				17.00					
				SIGN #		PRIVATE DRIVE											
39+58.10	RELOCATED CREWS LN	OUTSIDE	EAST	R1-1	36x36	STOP	1	9.00				18.00					
40+53.04	RELOCATED CREWS LN	OUTSIDE	WEST	R1-1	36x36	STOP	1	9.00				18.00					
				R1-I100	30x18	CROSS TRAFFIC DOES NOT STOP		3.75									
46+00.00	RELOCATED CREWS LN	OUTSIDE	EAST	W1-1R	36x36	TURN RIGHT SIGN	1	9.00				17.00					
				W13-1	24x24	25 MPH		4.00									
46+00	RELOCATED CREWS LN	OUTSIDE	WEST	W3-1a	36x36	STOP AHEAD	1	9.00				17.00					
60+00.00	RELOCATED CREWS LN	OUTSIDE	NORTH	W1-1L	36x36	TURN LEFT SIGN	1	9.00				17.00					
				W13-1	24x24	25 MPH		4.00									
52+00	RELOCATED CREWS LN	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00				14.00					
			WEST	W1-8R	18x24	CHEVRON		3.00									
52+80	RELOCATED CREWS LN	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00				14.00					
			WEST	W1-8R	18x24	CHEVRON		3.00									
53+60	RELOCATED CREWS LN	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00				14.00					
			WEST	W1-8R	18x24	CHEVRON		3.00									
54+40	RELOCATED CREWS LN	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00				14.00					
			WEST	W1-8R	18x24	CHEVRON		3.00									
55+20	RELOCATED CREWS LN	OUTSIDE	EAST	W1-8L	18x24	CHEVRON	1	3.00				14.00					
			WEST	W1-8R	18x24	CHEVRON		3.00									
573+75	EX US 67/ IL 104		WEST	R11-I100	36x36	ROAD ENDS	1	9.00				17.00					
				OM4-1	18x18	OBJECT MARKER	1	2.25				16.00					
				OM4-1	18x18	OBJECT MARKER	1	2.25				16.00					
568+75	EX US 67/ IL 104	OUTSIDE	WEST	W8-I108	36x36	ROAD ENDS 500 FT	1	9.00				17.00					
565+25	EX US 67/ IL 104	OUTSIDE	WEST	W14-1	30x30	DEAD END	1	6.25				17.00					
565+25	EX US 67/ IL 104	OUTSIDE	EAST	R1-1	30x30	STOP	1	6.25				17.00					
564+70	EX US 67/ IL 104	OUTSIDE	SOUTH	R1-1	30x30	STOP	1	6.25				17.00					
563+50	EX US 67/ IL 104	OUTSIDE	WEST	R1-1	30x30	STOP	1	6.25				17.00					
558+50	EX US 67/ IL 104	OUTSIDE	WEST	W3-1a	36x36	STOP AHEAD SYMBOL	1	9.00				17.00					
540+00	EX US 67/ IL 104	OUTSIDE	EAST	W1-4R	36x36	REVERSE CURVE RIGHT AHEAD	1	9.00				17.00					
69+00	BETHEL LANE	OUTSIDE	SOUTH	W2-1	36x36	CROSS ROAD AHEAD	1	9.00				17.00					
71+00	BETHEL LANE	OUTSIDE	SOUTH	W1-2R	36x36	CURVE RIGHT AHEAD	1	9.00				17.00					
				W13-1	24x24	35 MPH		4.00									
74+50	BETHEL LANE	OUTSIDE	SOUTH	W3-1a	36x36	STOP AHEAD	1	9.00				17.00					
76+50	BETHEL LANE	OUTSIDE	SOUTH	SIGN #		JACKSONVILLE/BEARDSTOWN	2										
77+00	BETHEL LANE	OUTSIDE	NORTH	W2-1	36x36	CROSS ROAD AHEAD	1	9.00				17.00					
78+00	BETHEL LANE	OUTSIDE	NORTH	W1-2L	36x36	CURVE LEFT AHEAD	1	9.00				17.00					
				W13-1	24x24	35 MPH		4.00									
81+50	BETHEL LANE	OUTSIDE	SOUTH	W1-4L	36x36	REVERSE CURVE LEFT AHEAD	1	9.00				17.00					
				W13-1	24x24	25 MPH		4.00									
87+50	BETHEL LANE	OUTSIDE	NORTH	W3-1a	36x36	STOP AHEAD	1	9.00				17.00					
90+00	BETHEL LANE	OUTSIDE	NORTH	W2-2R	36x36	SIDE ROAD SYMBOL 90 RT	1	9.00				17.00					
95+00	BETHEL LANE	OUTSIDE	NORTH	W1-4L	36x36	REVERSE CURVE LEFT AHEAD	1	9.00				17.00					
				W13-1	24x24	25 MPH		4.00									
591+42	SERVICE RD 91	OUTSIDE	SOUTH	R1-1	36x36	STOP	1	9.00				17.00					
591+52	SERVICE RD 91	OUTSIDE	NORTH	W14-1	36x36	DEAD END	1	9.00				17.00					
593+90	SERVICE RD 91		NORTH	R11-I100	36x36	ROAD ENDS	1	9.00				17.00					
				OM4-1	18x18	OBJECT MARKER	1	2.25				16.00					
				OM4-1	18x18	OBJECT MARKER	1	2.25				16.00					

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Mar-25-2011 03:57:24PM

FILE \$

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 39 OF 42

DATE 10/10
 DRAWN BY EBB
 CHECKED BY

SIGNING SCHEDULE - CONTINUED

STATION	LANE	SIDE	FACING	SIGN NUMBER (MUTCD)	SIZE (IN)	MESSAGE	NUMBER OF POSTS	SIGN PANEL			TELESCOPING STEEL SIGN SUPPORT	WOOD SIGN SUPPORT	BASE FOR TELESCOPING STEEL SIGN	STRUCTURAL STEEL SIGN SUPPORT BREAKAWAY		CONCRETE FOUNDATION	
								TYPE 1	TYPE 2	TYPE 3				SIZE	POUND		CU YD
								SO FT	FOOT	EACH				SIZE	POUND		CU YD
510+85.00	US 67 FRONTAGE RD	OUTSIDE	EAST	R1-1	36x36	STOP	1	9.00				18.00					
				R1-1100	30x18	CROSS TRAFFIC DOES NOT STOP		3.75									
512+00.00	US 67 FRONTAGE RD	OUTSIDE	WEST	W1-4R	36x36	REVERSE CURVE RIGHT AHEAD	1	9.00				17.00					
514+00.00	US 67 FRONTAGE RD	OUTSIDE	EAST	M4-5	2412	TO	1	2.00				17.00					
				M1-1100	24x24	US 67		4.00									
				M6-1	21x15	ARROW		2.19									
514+00.00	US 67 FRONTAGE RD	OUTSIDE	EAST	M4-5	2412	TO	1	2.00				17.00					
				M1-1100	24x24	IL 104		4.00									
				M6-1	21x15	ARROW		2.19									
518+00.00	US 67 FRONTAGE RD	OUTSIDE	EAST	W3-1A	36x36	STOP AHEAD	1	9.00				17.00					
109+30.00	RELOCATED CYLINDER HEAD RD	OUTSIDE	EAST	R1-1	36x36	STOP	1	9.00				18.00					
				R1-1100	30x18	CROSS TRAFFIC DOES NOT STOP		3.75									
108+50	RELOCATED CYLINDER HEAD RD	OUTSIDE	WEST	W1-4L	36x36	REVERSE CURVE LEFT AHEAD	1	9.00				17.00					
				W13-1	24x24	40 MPH		4.00									
107+00.00	RELOCATED CYLINDER HEAD RD	OUTSIDE	WEST	W2-2R	36x36	SIDE ROAD SYMBOL 90 RT	1	9.00				17.00					
106+50.00	RELOCATED CYLINDER HEAD RD	OUTSIDE	EAST	M4-5	2412	TO	1	2.00				17.00					
				M1-1100	24x24	US 67		4.00									
				M6-1	21x15	ARROW		2.19									
106+50.00	RELOCATED CYLINDER HEAD RD	OUTSIDE	EAST	M4-5	2412	TO	1	2.00				17.00					
				M1-1100	24x24	IL 104		4.00									
				M6-1	21x15	ARROW		2.19									
103+00.00	RELOCATED CYLINDER HEAD RD	OUTSIDE	EAST	W3-1a	36x36	STOP AHEAD	1	9.00				17.00					
94+00.00	RELOCATED CYLINDER HEAD RD	OUTSIDE	EAST	W2-2L	36x36	SIDE ROAD SYMBOL 90 LT	1	9.00				17.00					
91+00	RELOCATED CYLINDER HEAD RD	OUTSIDE	EAST	W1-4R	36x36	REVERSE CURVE RIGHT AHEAD	1	9.00				17.00					
				W13-1	24x24	40 MPH		4.00									
0+42.00	OLD CYLINDER HEAD RD	OUTSIDE	WEST	R1-1	36x36	STOP	1	9.00				18.00					
				R1-1100	30x18	CROSS TRAFFIC DOES NOT STOP		3.75									
1+25.00	OLD CYLINDER HEAD RD	OUTSIDE	EAST	W14-1	36x36	DEAD END	1	9.00				17.00					
6+50.00	OLD CYLINDER HEAD RD	OUTSIDE	WEST	R1-1	36x36	STOP	1	9.00				17.00					
7+55.00	OLD CYLINDER HEAD RD		EAST	R11-1100	36x36	ROAD ENDS	1	9.00				17.00					
				OM4-1	18x18	OBJECT MARKER	1	2.25				16.00					
				OM4-1	18x18	OBJECT MARKER	1	2.25				16.00					
10+50.00	GILLILAND RD CONNECTOR	OUTSIDE	NORTH	R1-1	36x36	STOP	1	9.00				17.00					
27+65	GILLILAND RD EXTENSION	OUTSIDE	NORTH	R1-1	36x36	STOP	1	9.00				17.00					
21+25.00	OLD CYLINDER HEAD RD	OUTSIDE	EAST	R1-1	36x36	STOP	1	9.00				17.00					
21+00.00	OLD CYLINDER HEAD RD	OUTSIDE	WEST	W14-1	36x36	DEAD END	1	9.00				17.00					
121+50	ST PAULS CHURCH RD	OUTSIDE	SOUTH	W1-10L	36x36	CURVE LEFT & SIDE ROAD RIGHT AHEAD	1	9.00				17.00					
				W13-1	24x24	35 MPH		4.00									
127+00	ST PAULS CHURCH RD	OUTSIDE	NORTH	W3-1a	36x36	STOP AHEAD	1	9.00				17.00					
130+00	ST PAULS CHURCH RD	OUTSIDE	NORTH	W1-10R	36x36	CURVE RIGHT & SIDE ROAD LEFT AHEAD	1	9.00				17.00					
				W13-1	24x24	35 MPH		4.00									
600+50	SERVICE RD 92	OUTSIDE	WEST	R1-1	36x36	STOP	1	9.00				17.00					
600+70	SERVICE RD 92	OUTSIDE	EAST	W14-1	36x36	DEAD END	1	9.00				17.00					
603+80	SERVICE RD 92		EAST	R11-1100	36x36	ROAD ENDS	1	9.00				17.00					
				OM4-1	18x18	OBJECT MARKER	1	2.25				16.00					
				OM4-1	18x18	OBJECT MARKER	1	2.25				16.00					
148+00	DUTCH LANE EAST	OUTSIDE	NORTH	W1-4R	36x36	REVERSE CURVE RIGHT AHEAD	1	9.00				17.00					
				W13-1	24x24	20 MPH		4.00									
143+00	DUTCH LANE EAST	OUTSIDE	EAST	W3-1a	36x36	STOP AHEAD	1	9.00				17.00					
134+00	DUTCH LANE EAST	OUTSIDE	EAST	W1-4R	36x36	REVERSE CURVE RIGHT AHEAD	1	9.00				17.00					
				W13-1	24x24	20 MPH		4.00									
198+00	DUTCH LANE WEST	OUTSIDE	NORTH	W1-2L	36x36	CURVE LEFT AHEAD	1	9.00				17.00					
				W13-1	24x24	40 MPH		4.00									
193+00	DUTCH LANE WEST	OUTSIDE	SOUTH	W3-1a	36x36	STOP AHEAD	1	9.00				17.00					
191+00	DUTCH LANE WEST	OUTSIDE	SOUTH	W1-2R	36x36	CURVE RIGHT AHEAD	1	9.00				17.00					
				W13-1	24x24	40 MPH		4.00									
202+00	DUTCH LANE WEST	OUTSIDE	SOUTH	W1-4L	36x36	REVERSE CURVE LEFT AHEAD	1	9.00				17.00					
				W13-1	24x24	35 MPH		4.00									
207+00	DUTCH LANE EAST	OUTSIDE	NORTH	W3-1a	36x36	STOP AHEAD	1	9.00				17.00					
212+75	DUTCH LANE EAST	OUTSIDE	NORTH	W1-4L	36x36	REVERSE CURVE LEFT AHEAD	1	9.00				17.00					
				W13-1	24x24	35 MPH		4.00									
298+00	HART'S GRAVEL RD	OUTSIDE	NORTH	W1-5L	36x36	WINDING ROAD LEFT AHEAD	1	9.00				17.00					
				W13-1	24x24	35 MPH		4.00									
293+00	HART'S GRAVEL RD	OUTSIDE	SOUTH	W3-1a	36x36	STOP AHEAD	1	9.00				17.00					
288+75	HART'S GRAVEL RD	OUTSIDE	SOUTH	W1-5R	36x36	WINDING ROAD RIGHT AHEAD	1	9.00				17.00					
				W13-1	24x24	35 MPH		4.00									
SUB TOTALS - SIDE ROADS								1200.78	0.00	532.84	2365.00	80.00	6	5001.00	10.78		
TOTALS									3465	304	1398	8084	520	8	11506	25.9	

F.A.P. RTE. 310	SECTION 69-3(3HB)	COUNTY MORGAN	TOTAL SHEETS 793	SHEET NO. 56
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULE OF QUANTITIES
 FAP 310 (US 67/IL 104)
 SHEET 40 OF 42
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	57
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

REMOVE SIGN COMPLETE

X7240205

STATION	LANE	SIDE	FACING	SIGN NUMBER (MUTCD)	SIZE (IN)	MESSAGE	NUMBER OF POSTS	REMOVE SIGN COMPLETE EACH
765+00 TO 770+00				W1-B	18x24	CHEVRON		1
776+80 TO 781+80				W1-B	18x24	CHEVRON		1
783+00 TO 791+00				W1-B	18x24	CHEVRON		1
744+25	WB	OUTSIDE	EAST	R2-5c	48x60	SPEED ZONE AHEAD WITH FLAG	1	1
				R1-1101	30x30	55 MPH		1
744+25	WB	MEDIAN	EAST	R2-5c	48x60	SPEED ZONE AHEAD WITH FLAG	1	1
				R1-1101	30x30	55 MPH		1
748+97	WB	MEDIAN	EAST	R2-1	48x60	SPEED LIMIT 55 WITH FLAG	2	1
748+97	WB	OUTSIDE	EAST	R2-1	48x60	SPEED LIMIT 55 WITH FLAG	2	1
754+72	WB	MEDIAN	EAST	W9-1L	36x36	LEFT LANE ENDS WITH FLAG	1	1
754+72	WB	OUTSIDE	EAST	W9-1L	36x36	LEFT LANE ENDS WITH FLAG	1	1
756+81	WB	MEDIAN	WEST	W8-1103	36x36	EXPRESSWAY ENDS 1000 FT		1
756+81	WB	OUTSIDE	WEST	W8-1103	36x36	EXPRESSWAY ENDS 1000 FT		1
758+81	WB	MEDIAN	WEST	W9-2R	36x36	LANE ENDS MERGE RIGHT	1	1
758+81	WB	OUTSIDE	WEST	W9-2R	36x36	LANE ENDS MERGE RIGHT	1	1
113+75	OLD 67	OUTSIDE	EAST	W14-1	30x30	DEAD END	1	1
761+81	WB	MEDIAN	SOUTH	W1-42	36x36	REVERSE CURVE LT WITH FLAG	1	1
761+81	WB	OUTSIDE	SOUTH	W1-42	36x36	REVERSE CURVE LT WITH FLAG	1	1
774+25	WB	OUTSIDE	EAST	W6-3	36x36	TWO-WAY TRAFFIC	1	1
774+25	WB	MEDIAN	EAST	W6-3	36x36	TWO-WAY TRAFFIC	1	1
774+25	WB	MEDIAN	WEST	R4-7	36x48	KEEP RIGHT		1
779+90	WB	MEDIAN	EAST	W1-42	36x36	REVERSE CURVE LT	1	1
782+00	EB	OUTSIDE	WEST	W6-1	36x36	DIVIDED HIGHWAY SYMBOL	1	1
89+30	OLD 67	OUTSIDE	EAST	W8-1108	36x36	ROAD ENDS 500 FT	1	1
84+25	OLD 67	CL	WEST	R11-1100	36x36	ROAD ENDS WITH OBJECT MARKERS	3	1
34+30	CONNECTOR	OUTSIDE	EAST	W2-2R	48x48	SIDE ROAD SYMBOL 90 RT	1	1
21+00 TO 30+00	CONNECTOR			W16-8	36x15	ARENZVILLE RD		
35+50 TO 41+50	CONNECTOR			W1-B	18x24	CHEVRON		1
37+00	CONNECTOR	OUTSIDE	EAST	W1-B	18x24	CHEVRON		1
				R3-1100R	24x24	RIGHT TURN LANE	1	1
64+50	CONNECTOR	OUTSIDE	WEST	M6-2R	21x15	ARROW		
805+80	EB	OUTSIDE	SOUTH	W1-4L	36x36	REVERSE CURVE LT SYMBOL	1	1
				R1-1	36x36	STOP	1	1
TOTAL								30

RAISED REFLECTIVE PAVEMENT MARKER REMOVAL
78300200

STATION TO STATION	SIDE	SPACING	EACH
EX US 67 / IL 104			
81+30.0 97+85.0	CL	80	21
48+50.0 52+50.0	CL	80	5
0+00.0 3+00.0	CL	80	4
578+95.0 589+54.3	CL	80	14
556+00.0 575+75.0	CL	80	25
521+80.0 524+68.0	CL	80	4
481+35.0 484+80.0	CL	80	5
351+20.0 353+65.0	CL	80	4
312+20.0 315+60.0	CL	80	5
EX CONCORD ROAD			
25+30.0 29+55.0	CL	80	6
TOTAL			93
USE			100

NOTE: THIS IS AN ESTIMATED QUANTITY.

REVISIONS	
NAME	DATE

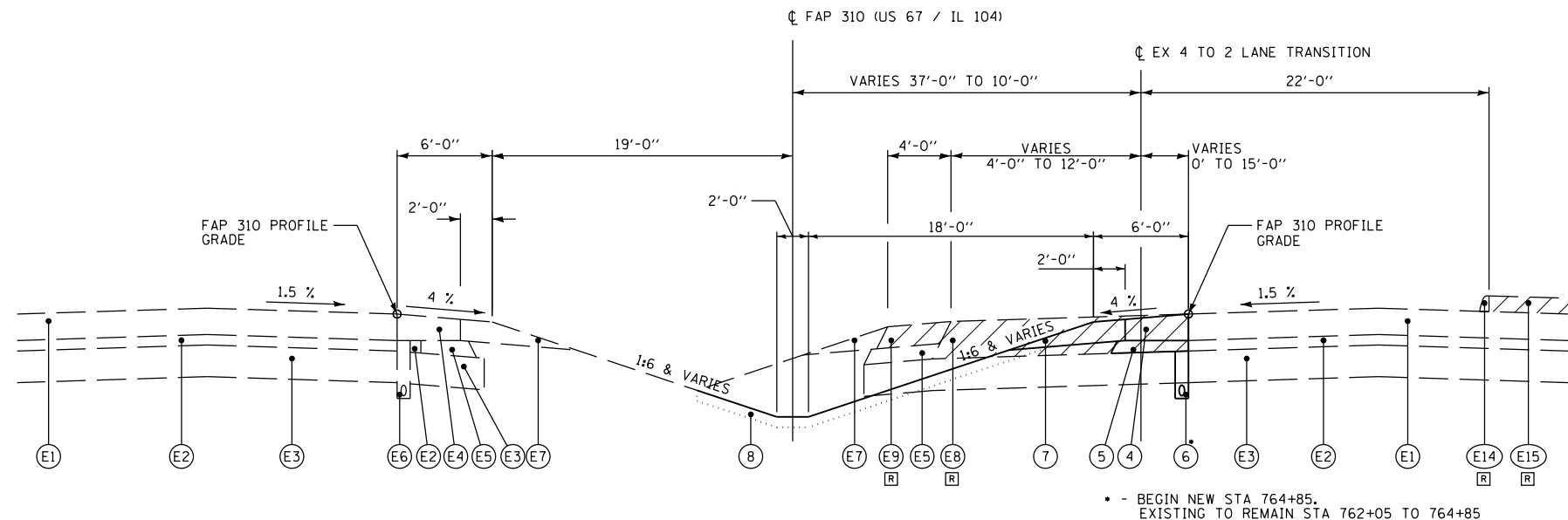
ILLINOIS DEPARTMENT OF TRANSPORTATION
SCHEDULE OF QUANTITIES
FAP 310 (US 67/IL 104)
SHEET 41 OF 42

DATE 10/10
DRAWN BY EBB
CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	59
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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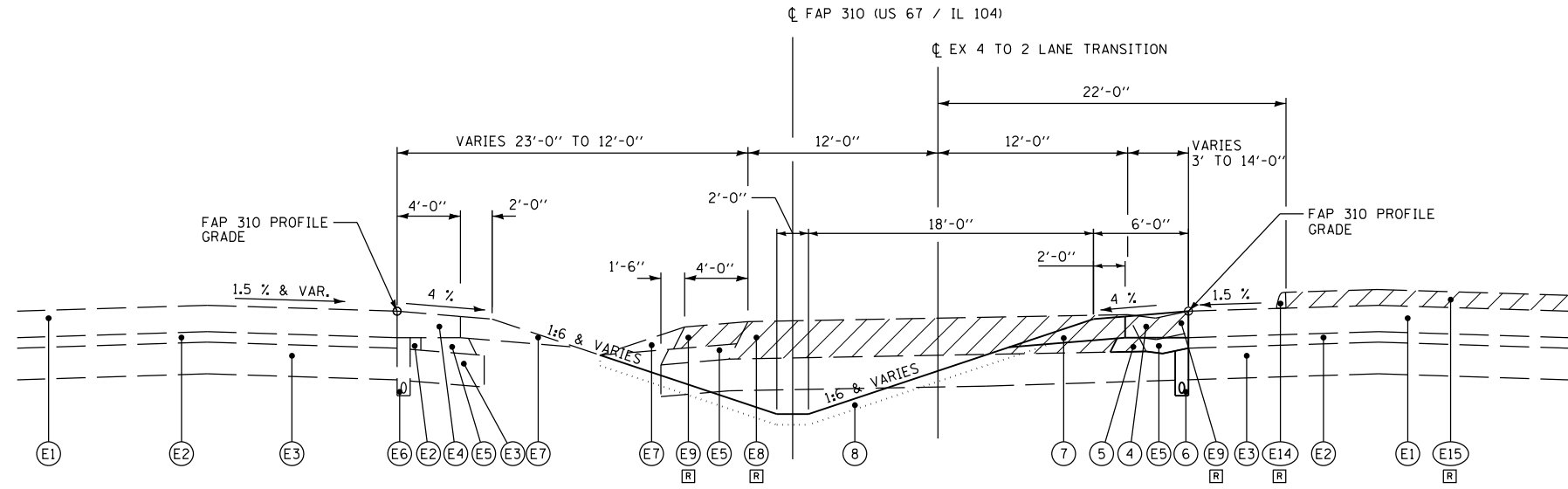
TYPICAL SECTION (FAP 310) WITH EXISTING 4 TO 2 LANE TRANSITION
 STA 762+05.00 TO STA 769+39.68 (FAP 310)

EXISTING LEGEND

- (E1) EXISTING PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- (E2) EXISTING STABILIZED SUB-BASE, 4"
- (E3) EXISTING LIME MODIFIED SOIL, 12"
- (E4) EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 10"
- (E5) EXISTING SUB-BASE GRANULAR MATERIAL, TYPE C
- (E6) EXISTING PIPE UNDERDRAINS, 4"
- (E7) EXISTING AGGREGATE SHOULDER
- (E8) EXISTING HOT-MIX-ASPHALT PAVEMENT (FULL DEPTH), 14"
- (E9) EXISTING HOT-MIX ASPHALT SHOULDER, 8"
- (E14) EXISTING HOT-MIX ASPHALT CURB
- (E15) EXISTING TOPSOIL 6"
- (R) REMOVAL PAY ITEM

PROPOSED LEGEND

- (4) PORTLAND CEMENT CONCRETE SHOULDERS 10" - STD 483001 WITH RUMBLE STRIPS - STD 642001
- (5) SUB-BASE GRANULAR MATERIAL, TYPE C
- (6) PIPE UNDERDRAINS 6" - STD 601001
- (7) AGGREGATE SHOULDERS, TYPE B
- (8) TOPSOIL FURNISH AND PLACE, 4"



TYPICAL SECTION (FAP 310) WITH EXISTING 4 TO 2 LANE TRANSITION
 STA 769+39.68 TO STA 770+77.08 (FAP 310)

NOTE: NOT TO SCALE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 1 OF 24

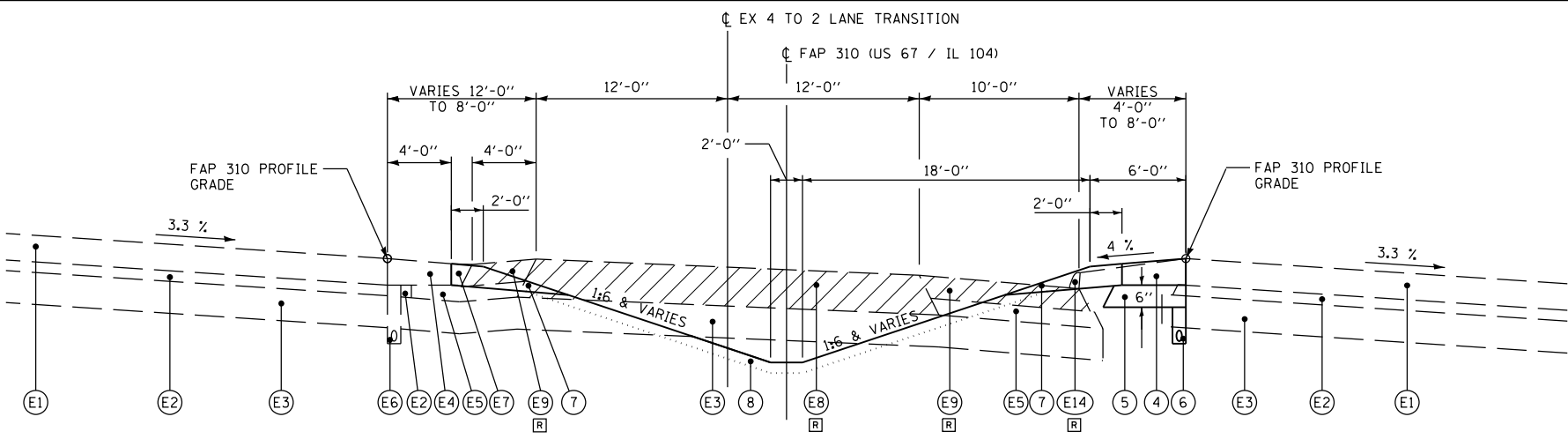
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 DATE 4/2009

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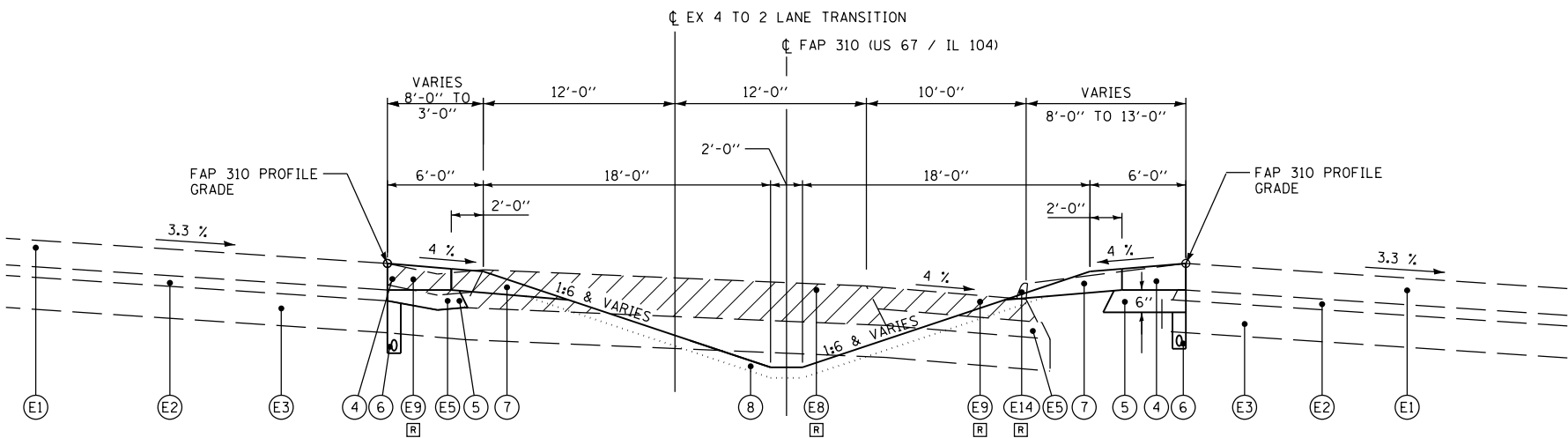
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	60
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

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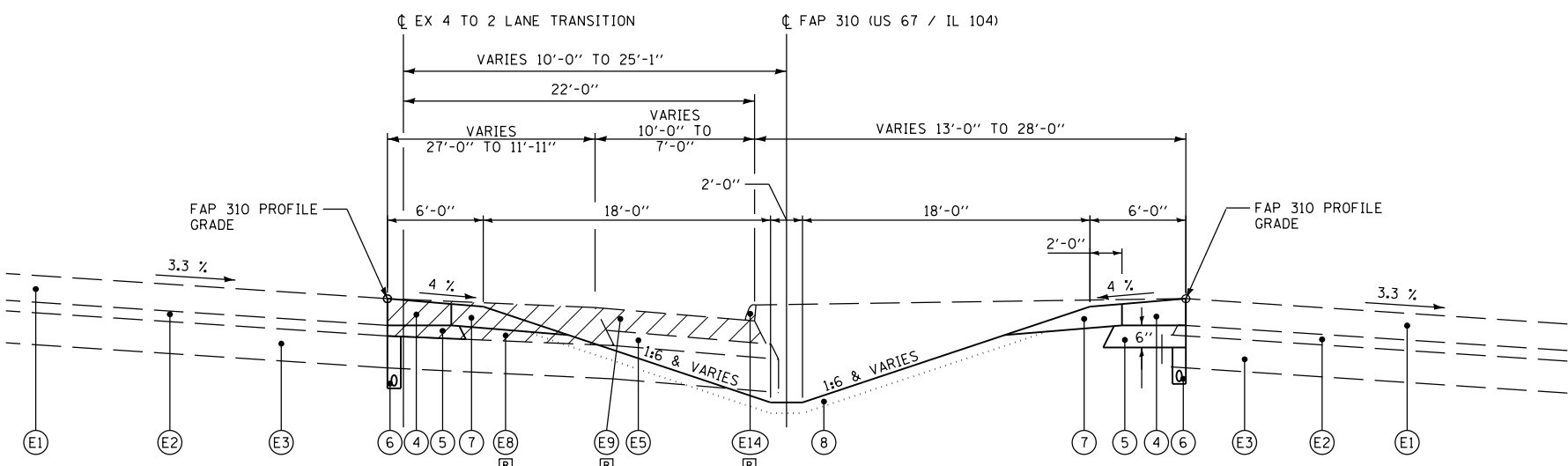
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TYPICAL SECTION (FAP 310) WITH EXISTING 4 TO 2 LANE TRANSITION
STA 770+77.08 TO STA 771+25.73 (FAP 310)



TYPICAL SECTION (FAP 310) WITH EXISTING 4 TO 2 LANE TRANSITION
STA 771+25.73 TO STA 771+93.13 (FAP 310)



TYPICAL SECTION (FAP 310) WITH EXISTING 4 TO 2 LANE TRANSITION
STA 771+93.13 TO STA 774+48.14 (FAP 310)

EXISTING LEGEND

- (E1) EXISTING PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- (E2) EXISTING STABILIZED SUB-BASE, 4"
- (E3) EXISTING LIME MODIFIED SOIL, 12"
- (E4) EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 10"
- (E5) EXISTING SUB-BASE GRANULAR MATERIAL, TYPE C
- (E6) EXISTING PIPE UNDERDRAINS, 4"
- (E7) EXISTING AGGREGATE SHOULDER
- (E8) EXISTING HOT-MIX-ASPHALT PAVEMENT (FULL DEPTH), 14"
- (E9) EXISTING HOT-MIX ASPHALT SHOULDER, 8"
- (E14) EXISTING HOT-MIX ASPHALT CURB
- (R) REMOVAL PAY ITEM

PROPOSED LEGEND

- (4) PORTLAND CEMENT CONCRETE SHOULDERS 10" - STD 483001 WITH RUMBLE STRIPS - STD 642001
- (5) SUB-BASE GRANULAR MATERIAL, TYPE C
- (6) PIPE UNDERDRAINS 6" - STD 601001
- (7) AGGREGATE SHOULDERS, TYPE B
- (8) TOPSOIL FURNISH AND PLACE, 4"

NOTE: NOT TO SCALE

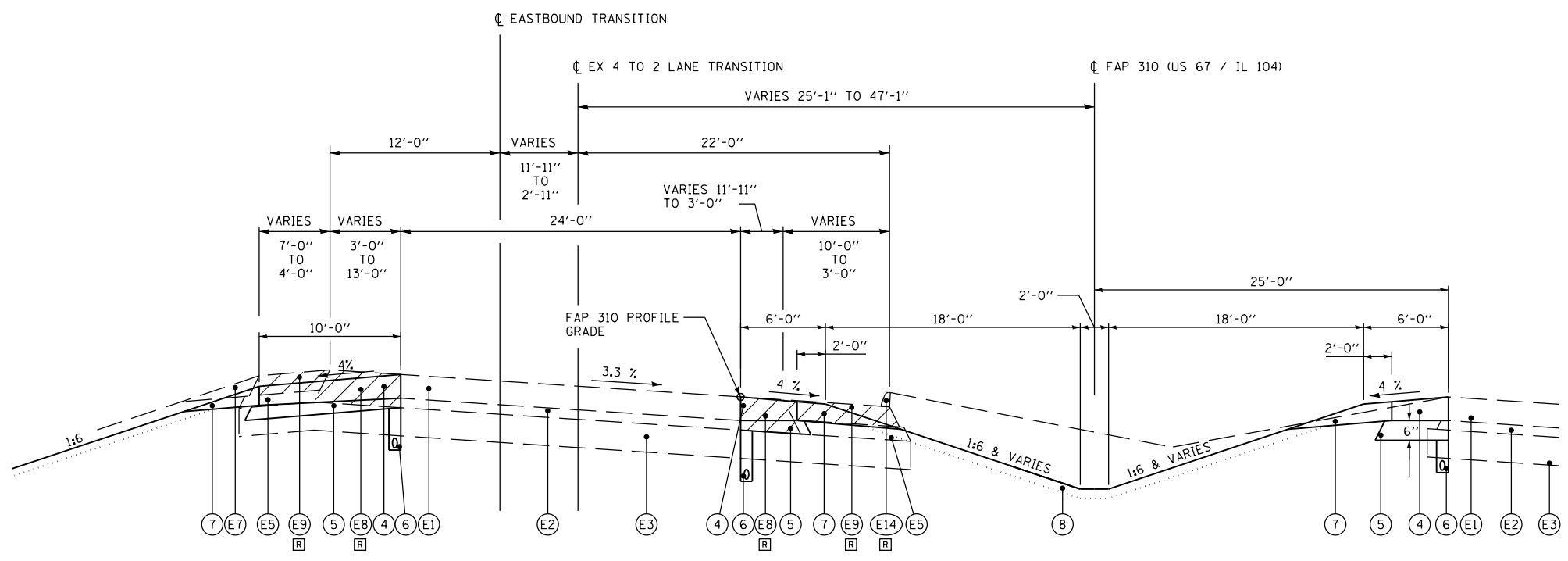
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
FAP 310 (US 67/IL 104)
SHEET 2 OF 24
DRAWN BY SEB
CHECKED BY
DATE 4/2009

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	61
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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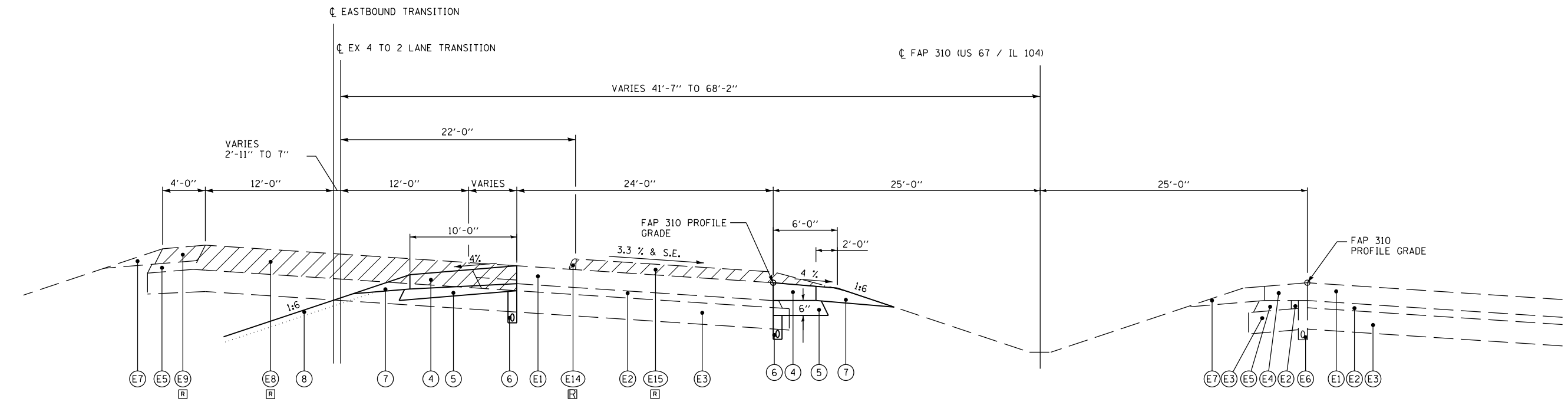
TYPICAL SECTION (FAP 310) WITH EXISTING 4 TO 2 LANE TRANSITION
 STA 774+48.14 TO STA 777+86.60 (FAP 310) WESTBOUND MEDIAN PCC SHOULDER RESUMES STA 775+90.00

EXISTING LEGEND

- (E1) EXISTING PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- (E2) EXISTING STABILIZED SUB-BASE, 4"
- (E3) EXISTING LIME MODIFIED SOIL, 12"
- (E4) EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 10"
- (E5) EXISTING SUB-BASE GRANULAR MATERIAL, TYPE C
- (E6) EXISTING PIPE UNDERDRAINS, 4"
- (E7) EXISTING AGGREGATE SHOULDER
- (E8) EXISTING HOT-MIX-ASPHALT PAVEMENT (FULL DEPTH), 14"
- (E9) EXISTING HOT-MIX ASPHALT SHOULDER, 8"
- (E14) EXISTING HOT-MIX ASPHALT CURB
- (E15) EXISTING TOPSOIL 6"
- (R) REMOVAL PAY ITEM

PROPOSED LEGEND

- (4) PORTLAND CEMENT CONCRETE SHOULDERS 10" - STD 483001 WITH RUMBLE STRIPS - STD 642001
- (5) SUB-BASE GRANULAR MATERIAL, TYPE C
- (6) PIPE UNDERDRAINS 6" - STD 601001
- (7) AGGREGATE SHOULDERS, TYPE B
- (8) TOPSOIL FURNISH AND PLACE, 4"



TYPICAL SECTION (FAP 310) WITH EXISTING 4 TO 2 LANE TRANSITION
 STA 777+86.60 TO STA 779+75.77 (FAP 310) EASTBOUND MEDIAN PCC SHOULDER RESUMES STA 780+04.95

NOTE: NOT TO SCALE

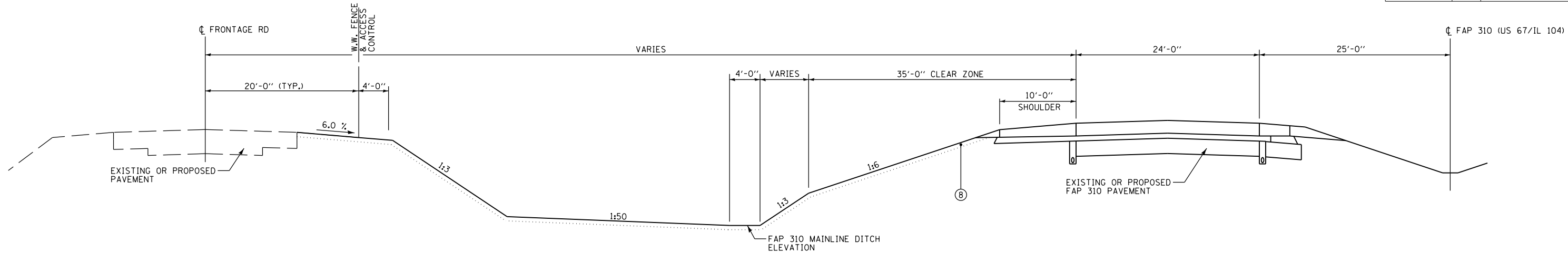
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 3 OF 24
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 DATE 4/2009

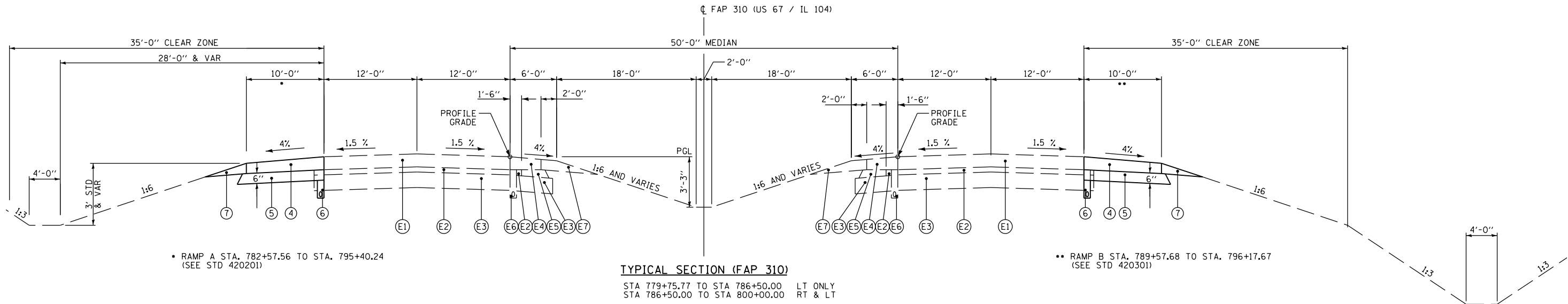
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	62
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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TYPICAL GRADING SECTION (FAP 310) ADJACENT TO FRONTAGE ROAD



TYPICAL SECTION (FAP 310)

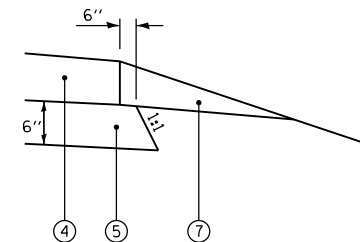
STA 779+75.77 TO STA 786+50.00 LT ONLY
STA 786+50.00 TO STA 800+00.00 RT & LT

EXISTING LEGEND

- (E1) EXISTING PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- (E2) EXISTING STABILIZED SUB-BASE, 4"
- (E3) EXISTING LIME MODIFIED SOIL, 12"
- (E4) EXISTING PORTLAND CEMENT CONCRETE SHOULDERS 10"
- (E5) EXISTING SUB-BASE GRANULAR MATERIAL, TYPE C
- (E6) EXISTING PIPE UNDERDRAINS, 4"
- (E7) EXISTING AGGREGATE SHOULDER
- (R) REMOVAL PAY ITEM

PROPOSED LEGEND

- (4) PORTLAND CEMENT CONCRETE SHOULDERS 10" - STD 483001 WITH RUMBLE STRIPS - STD 642001
- (5) SUB-BASE GRANULAR MATERIAL, TYPE C
- (6) PIPE UNDERDRAINS 6" - STD 601001
- (7) AGGREGATE SHOULDERS, TYPE B
- (8) TOPSOIL FURNISH AND PLACE, 4"



LIMITS OF SUB-BASE GRANULAR STABILIZATION

PCC SHOULDERS ADJACENT TO EXISTING PAVEMENT WITHOUT LIME STABILIZATION UNDER SHOULDERS

NOTE: NOT TO SCALE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
FAP 310 (US 67/IL 104)
SHEET 4 OF 24

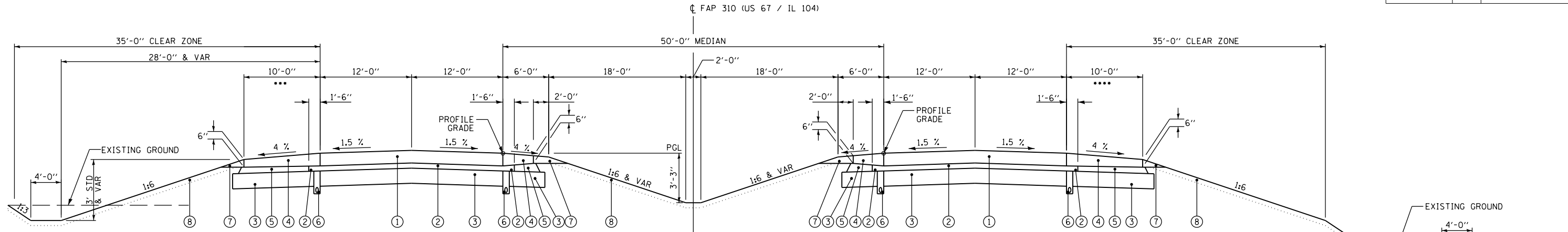
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	63
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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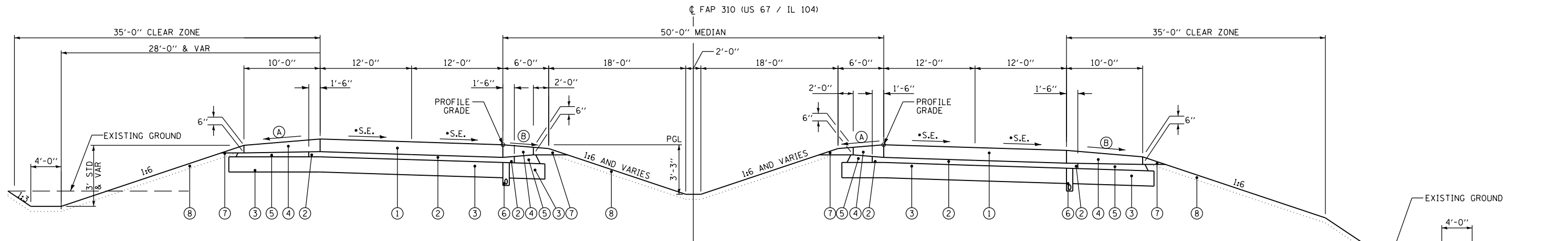
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TYPICAL SECTION (FAP 310)

STA 800+00.00 TO STA 892+24.00
 STA 913+91.00 TO STA 925+31.00
 STA 948+49.00 TO STA 968+87.00
 STA 989+85.00 TO STA 1021+20.00
 STA 1041+00.00 TO STA 1088+44.00
 STA 1120+42.00 TO STA 1133+50.00
 STA 1133+50.00 TO STA 1140+00.00

*** RAMP D STA. 820+36.49 TO STA. 826+96.51
 (SEE STD 420301)
 *** RAMP C STA. 817+49.93 TO STA. 830+32.62
 (SEE STD 420201)



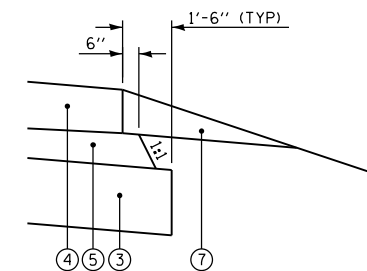
TYPICAL SUPERELEVATED SECTION (FAP 310)

STA 892+24.00 TO STA 894+09.00 S.E. TRANSITION
 STA 894+09.00 TO STA 912+06.00 S.E. = 3.1% (LT)
 STA 912+06.00 TO STA 913+91.00 S.E. TRANSITION
 STA 925+31.00 TO STA 928+13.00 S.E. TRANSITION
 STA 928+13.00 TO STA 945+67.00 S.E. = 5.4% (RT)
 STA 945+67.00 TO STA 948+49.00 S.E. TRANSITION
 STA 968+87.00 TO STA 970+23.00 S.E. TRANSITION
 STA 970+23.00 TO STA 988+49.00 S.E. = 1.5% (RT)
 STA 988+49.00 TO STA 989+85.00 S.E. TRANSITION
 STA 1021+20.00 TO STA 1023+13.00 S.E. TRANSITION
 STA 1023+13.00 TO STA 1039+07.00 S.E. = 3.3% (LT)
 STA 1039+07.00 TO STA 1041+00.00 S.E. TRANSITION
 STA 1088+44.00 TO STA 1090+06.00 S.E. TRANSITION
 STA 1090+06.00 TO STA 1118+80.00 S.E. = 2.5% (LT)
 STA 1118+80.00 TO STA 1120+42.00 S.E. TRANSITION

PROPOSED LEGEND

- ① PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED) - STD 420101
- ② STABILIZED SUB-BASE 4"
- ③ LIME MODIFIED SOIL 12"
- ④ PORTLAND CEMENT CONCRETE SHOULDERS 10" - STD 483001 WITH RUMBLE STRIPS - STD 642001
- ⑤ SUB-BASE GRANULAR MATERIAL, TYPE C
- ⑥ PIPE UNDERDRAINS 6" - STD 601001
- ⑦ AGGREGATE SHOULDERS, TYPE B
- ⑧ TOPSOIL FURNISH AND PLACE, 4"

- (A) SHOULDER SLOPE - HIGH SIDE OF S.E.: WHEN THE S.E. RATE OF THE PAVEMENT IS BETWEEN 0 AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN S.E. RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND THE SHOULDER SHALL BE 8%.
- (B) SHOULDER SLOPE - LOW SIDE OF S.E.: SLOPE SHALL BE THE SAME AS THE S.E. BUT NOT LESS THAN 4%.



LIMITS OF LIME STABILIZATION
 PCC SHOULDERS ADJACENT TO NEW PAVEMENT

REVISIONS	
NAME	DATE

NOTE: NOT TO SCALE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 5 OF 24

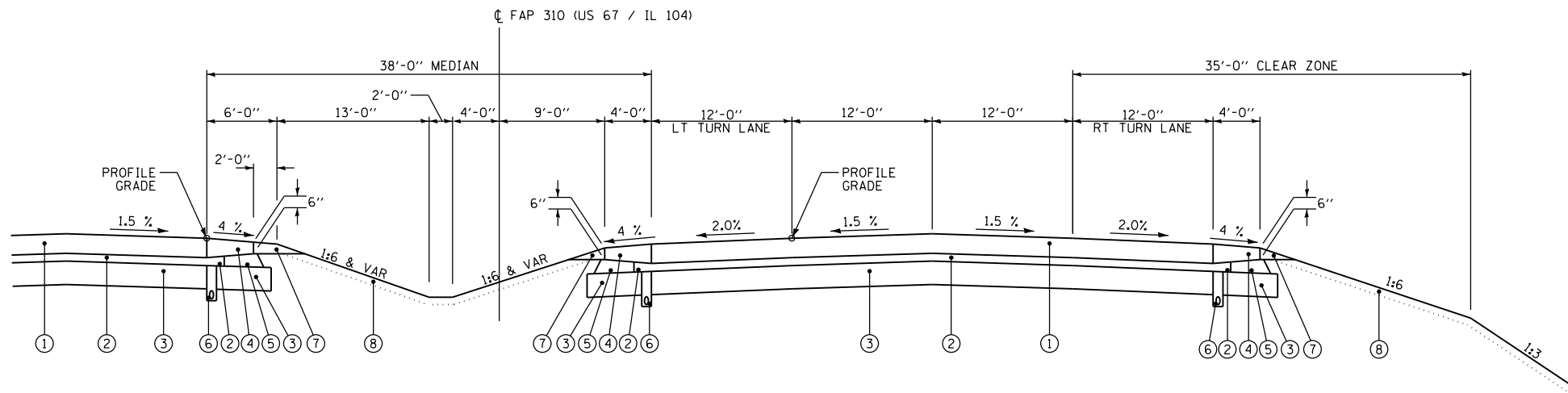
DATE 4/2009
 DRAWN BY SEB
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	64
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

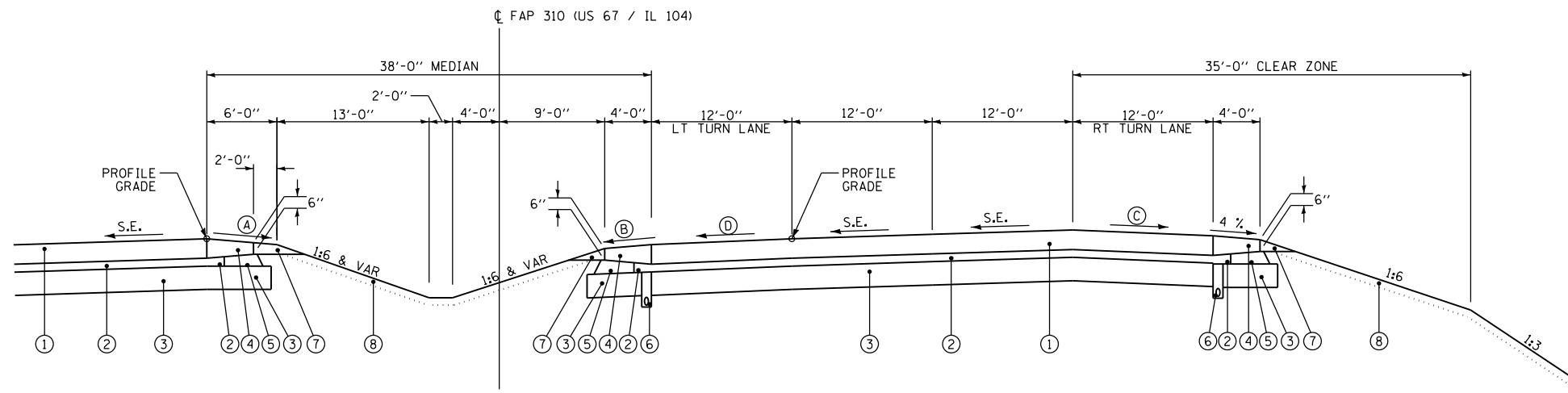
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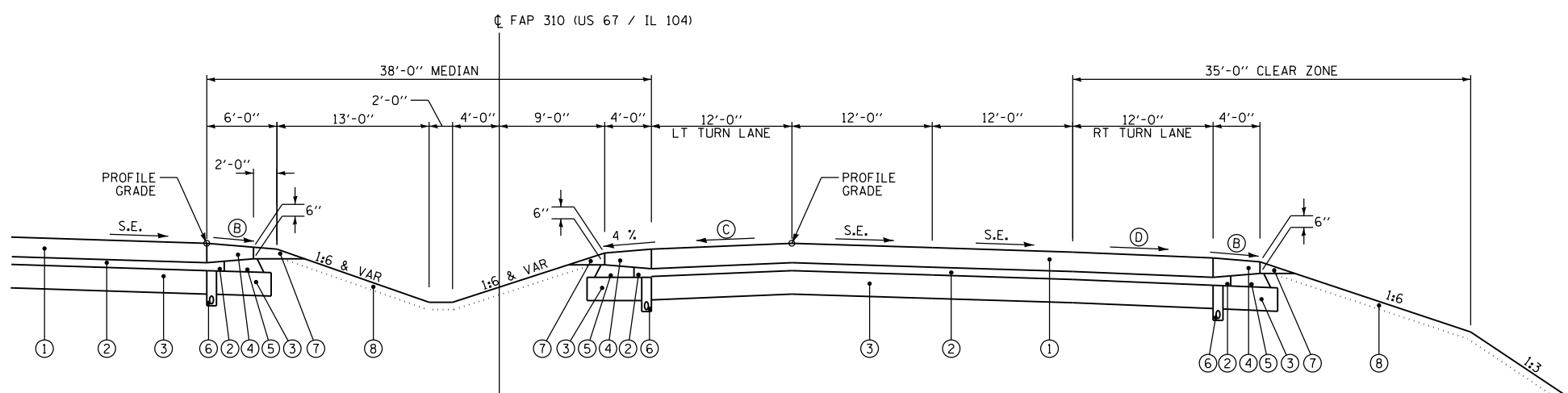


TYPICAL TURN LANE SECTION (FAP 310)



TYPICAL SUPERELEVATED TURN LANE SECTION (FAP 310)

LT TURN LANE ON LOW SIDE
RT TURN LANE ON HIGH SIDE



TYPICAL SUPERELEVATED TURN LANE SECTION (FAP 310)

LT TURN LANE ON HIGH SIDE
RT TURN LANE ON LOW SIDE

PROPOSED LEGEND

- ① PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED) - STD 420101
 - ② STABILIZED SUB-BASE 4"
 - ③ LIME MODIFIED SOIL 12"
 - ④ PORTLAND CEMENT CONCRETE SHOULDERS 10" - STD 483001 WITH RUMBLE STRIPS - STD 642001
 - ⑤ SUB-BASE GRANULAR MATERIAL, TYPE C
 - ⑥ PIPE UNDERDRAINS 6" - STD 601001
 - ⑦ AGGREGATE SHOULDERS, TYPE B
 - ⑧ TOPSOIL FURNISH AND PLACE, 4"
- (A) SHOULDER SLOPE - HIGH SIDE OF S.E.: WHEN THE S.E. RATE OF THE PAVEMENT IS BETWEEN 0 AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN S.E. RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND THE SHOULDER SHALL BE 8%.
 - (B) SHOULDER SLOPE - LOW SIDE OF S.E.: SLOPE SHALL BE THE SAME AS THE S.E. BUT NOT LESS THAN 4%.
 - (C) TURN LANE SLOPE - HIGH SIDE OF S.E.: WHEN THE S.E. RATE OF THE PAVEMENT IS BETWEEN 0 AND 2% THE TURN LANE SHALL BE SLOPED AT 2%. WHEN S.E. RATE OF THE PAVEMENT EXCEEDS 2% THE TURN LANE SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND THE TURN LANE SHALL BE 4%.
 - (D) TURN LANE SLOPE - LOW SIDE OF S.E.: SLOPE SHALL BE THE SAME AS THE S.E. BUT NOT LESS THAN 2.0%.

STRUCTURAL DESIGN INFORMATION (FAP 310)

ROAD CLASSIFICATION: CLASS I
 STRUCTURAL DESIGN TRAFFIC:
 PU = 12,768 SU = 456 MU = 1976
 PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:
 PU = 32 SU = 45 MU = 45
 MINIMUM SUBGRADE SUPPORT RATING: POOR
 RIGID PAVEMENT DESIGN: MINIMUM TF = 10.05
 PCC THICKNESS 10"

NOTE: NOT TO SCALE

REVISIONS	
NAME	DATE

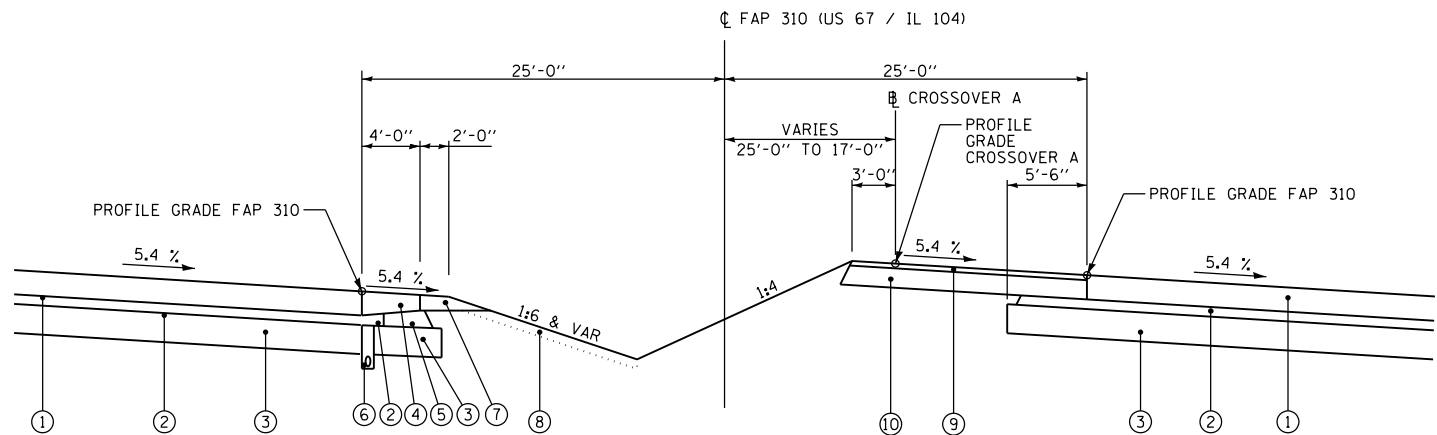
ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 6 OF 24

DATE 4/2009
 DRAWN BY SEB
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	65
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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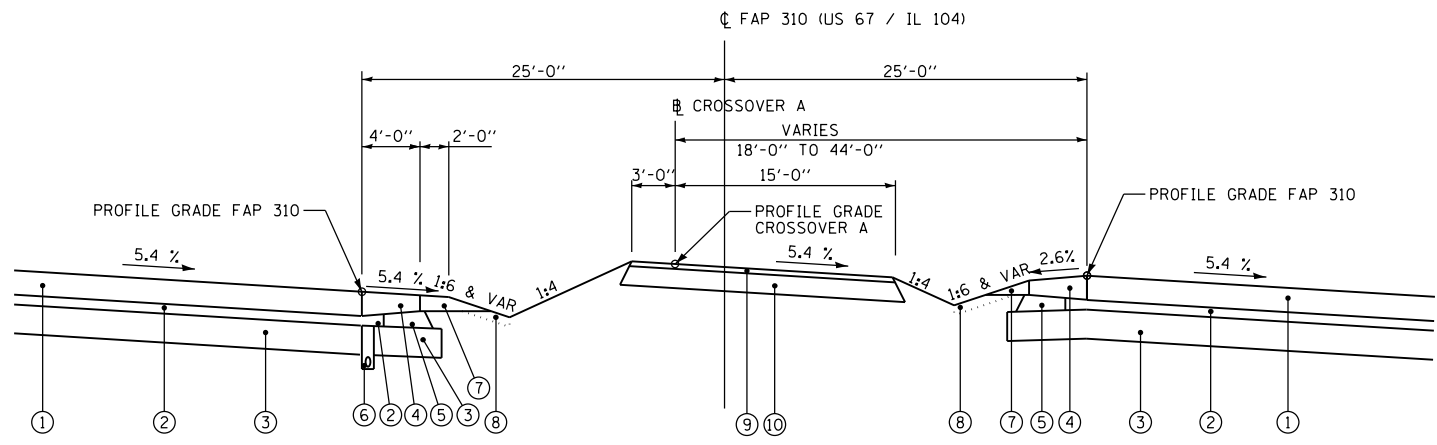
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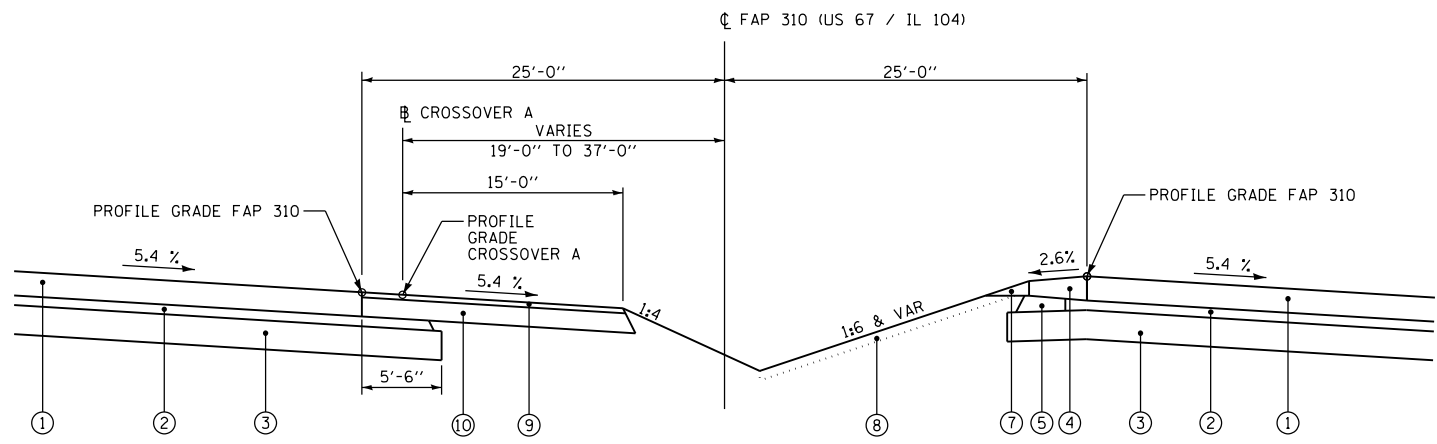
TYPICAL SECTION FAP 310 (US 67/IL 104) WITH CROSSOVER A
 STA 928+57.27 TO STA 931+84.52 FAP 310 STA 10+00.00 TO STA 13+24.49 CROSSOVER A
 STA 942+39.47 TO STA 945+67.00 STA 23+94.40 TO STA 27+19.16

PROPOSED LEGEND

- ① PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED) - STD 420101
- ② STABILIZED SUB-BASE 4"
- ③ LIME MODIFIED SOIL 12"
- ④ PORTLAND CEMENT CONCRETE SHOULDERS 10" - STD 483001 WITH RUMBLE STRIPS - STD 642001
- ⑤ SUB-BASE GRANULAR MATERIAL, TYPE C
- ⑥ PIPE UNDERDRAINS 6" - STD 601001
- ⑦ AGGREGATE SHOULDERS, TYPE B
- ⑧ TOPSOIL FURNISH AND PLACE, 4"
- ⑨ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- ⑩ HOT-MIX ASPHALT BASE COURSE, 8" (ANY WIDTH)



TYPICAL SECTION FAP 310 (US 67/IL 104) WITH CROSSOVER A
 STA 931+84.52 TO STA 933+86.84 FAP 310 STA 13+24.49 TO STA 15+30.22 CROSSOVER A
 STA 940+37.42 TO STA 942+39.47 STA 21+88.94 TO STA 23+94.40



TYPICAL SECTION FAP 310 (US 67/IL 104) WITH CROSSOVER A
 STA 933+86.84 TO STA 940+37.42 FAP 310 STA 15+30.22 TO STA 21+88.94 CROSSOVER A

NOTE: NOT TO SCALE

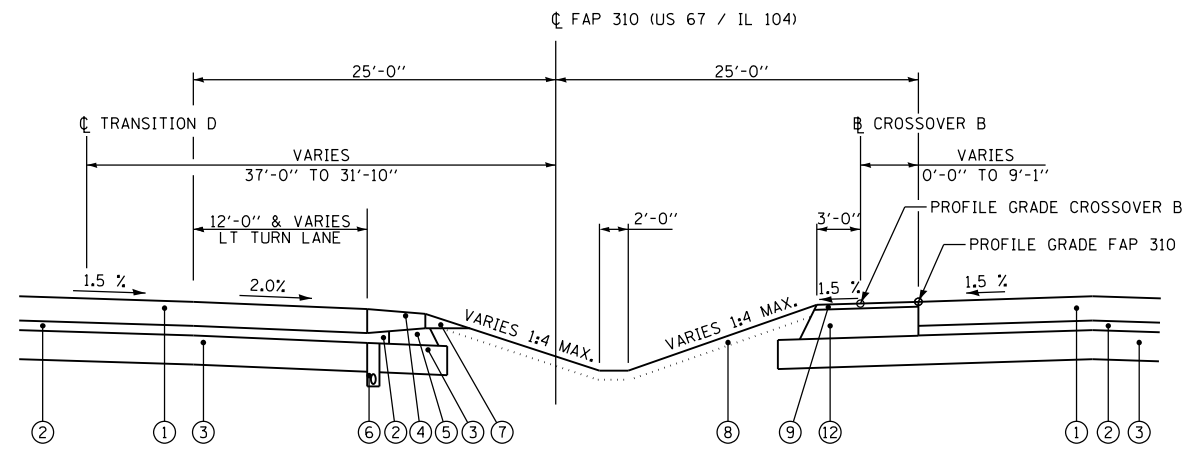
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 7 OF 24
 DRAWN BY SEB
 CHECKED BY
 DATE 4/2009

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	66
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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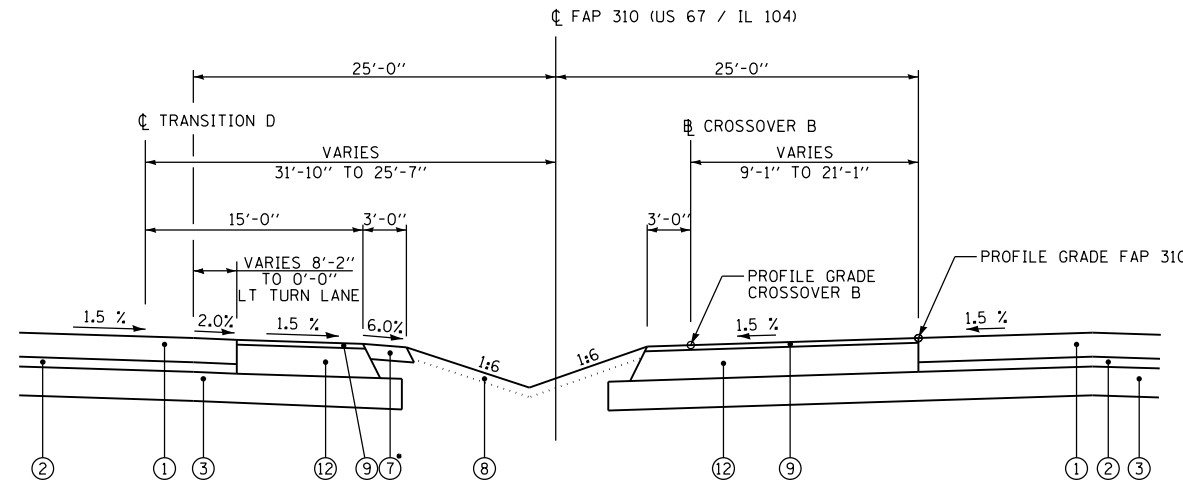


TYPICAL SECTION FAP 310 (US 67/IL 104) WITH CROSSOVER B & TRANSITION D

STA 1126+83.80 TO STA 1130+05.25 FAP 310 STA 15+30.22 TO STA 22+99.37 CROSSOVER B STA 0+00.00 TO STA 3+21.51 TRANSITION D

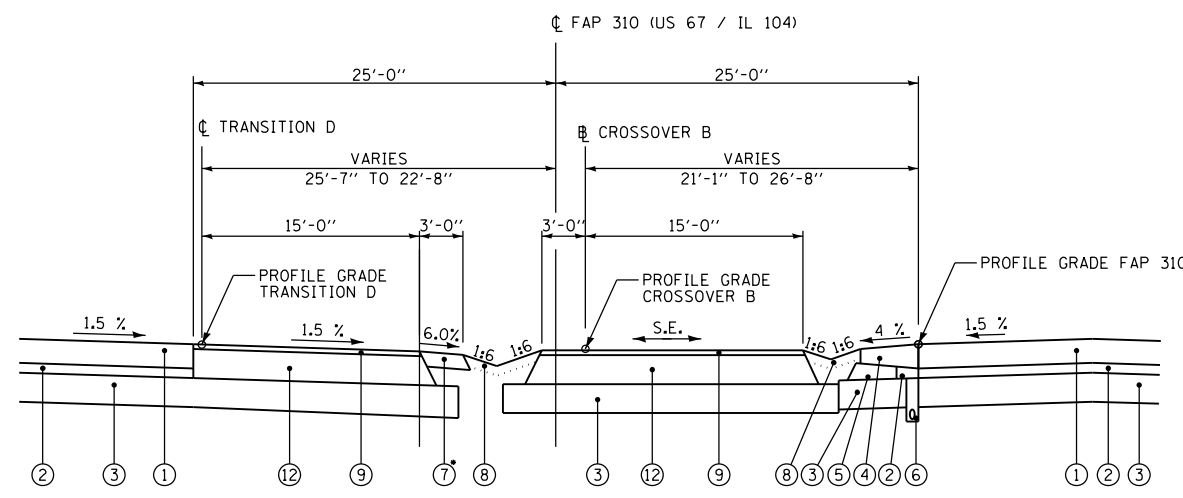
PROPOSED LEGEND

- ① PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED) - STD 420101
- ② STABILIZED SUB-BASE 4"
- ③ LIME MODIFIED SOIL 12"
- ④ PORTLAND CEMENT CONCRETE SHOULDERS 10" - STD 483001 WITH RUMBLE STRIPS - STD 642001
- ⑤ SUB-BASE GRANULAR MATERIAL, TYPE C
- ⑥ PIPE UNDERDRAINS 6" - STD 601001
- ⑦ AGGREGATE SHOULDERS, TYPE B
- ⑧ TOPSOIL FURNISH AND PLACE, 4"
- ⑨ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- ⑫ HOT-MIX ASPHALT BASE COURSE, 12" (ANY WIDTH)



TYPICAL SECTION FAP 310 (US 67/IL 104) WITH CROSSOVER B & TRANSITION D

STA 1130+05.25 TO STA 131+62.23 FAP 310 STA 22+99.37 TO STA 24+56.81 CROSSOVER B STA 3+21.51 TO STA 4+78.61 TRANSITION D



TYPICAL SECTION FAP 310 (US 67/IL 104) WITH CROSSOVER B & TRANSITION D

STA 131+62.23 TO STA 1132+18.86 FAP 310 STA 24+56.81 TO STA 25+13.72 CROSSOVER B STA 4+78.61 TO STA 5+35.31 TRANSITION D

NOTE: NOT TO SCALE

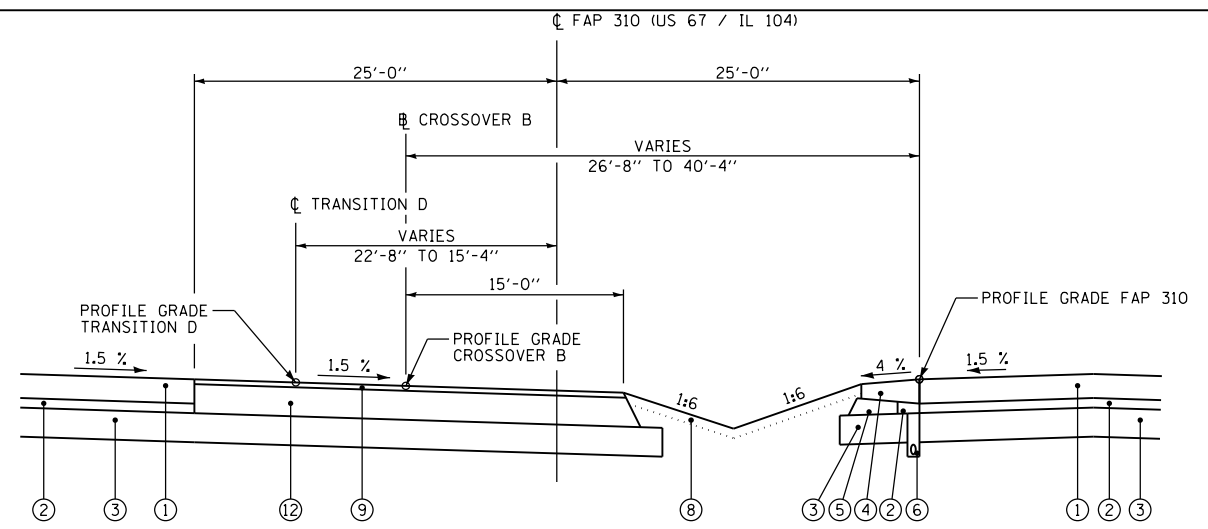
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 8 OF 24
 DATE 4/2009
 DRAWN BY SEB
 CHECKED BY

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	67
STA. 310		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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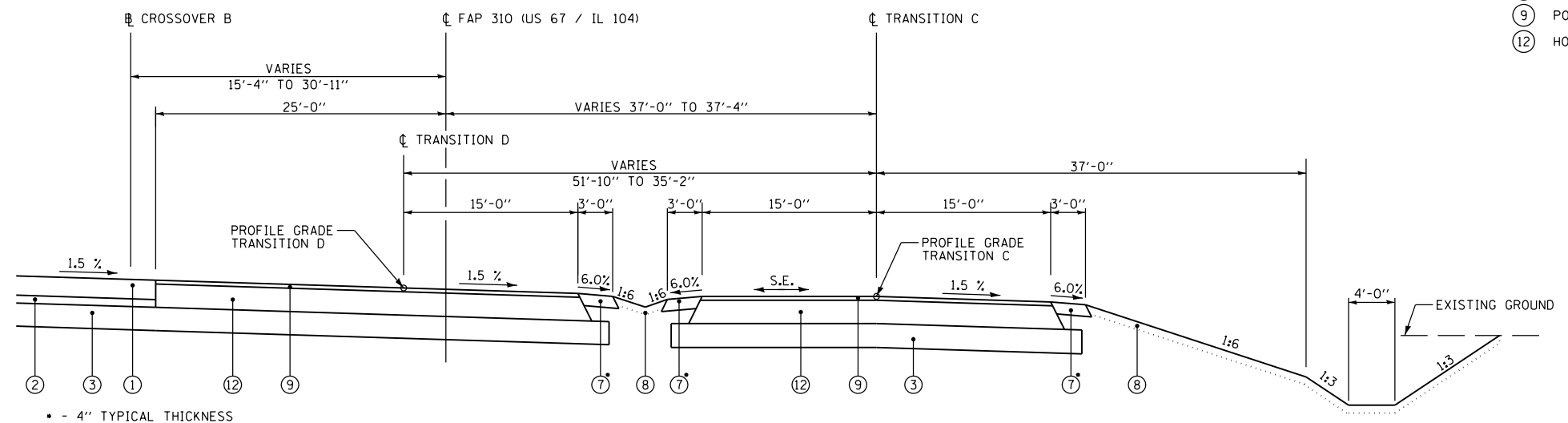


TYPICAL SECTION FAP 310 (US 67/IL 104) WITH CROSSOVER B & TRANSITION D

STA 1132+18.86 TO STA 133+50.00 FAP 310 STA 25+13.72 TO STA 26+45.57 CROSSOVER B STA 5+35.31 TO STA 6+66.69 TRANSITION D

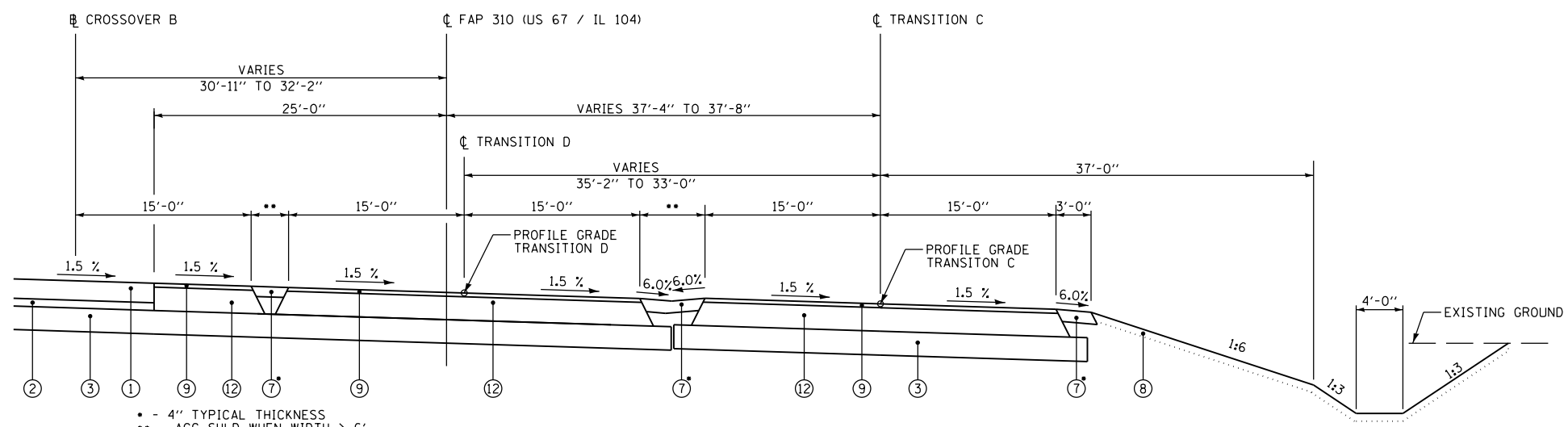
PROPOSED LEGEND

- ① PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED) - STD 420101
- ② STABILIZED SUB-BASE 4"
- ③ LIME MODIFIED SOIL 12"
- ④ PORTLAND CEMENT CONCRETE SHOULDERS 10" - STD 483001 WITH RUMBLE STRIPS - STD 642001
- ⑤ SUB-BASE GRANULAR MATERIAL, TYPE C
- ⑥ PIPE UNDERDRAINS 6" - STD 601001
- ⑦ AGGREGATE SHOULDERS, TYPE B
- ⑧ TOPSOIL FURNISH AND PLACE, 4"
- ⑨ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- ⑫ HOT-MIX ASPHALT BASE COURSE, 12" (ANY WIDTH)



TYPICAL SECTION FAP 310 (US 67/IL 104) WITH CROSSOVER B & TRANSITION C & TRANSITION D

STA 133+50.00 TO STA 1135+67.67 FAP 310 STA 26+45.57 TO STA 28+63.82 CROSSOVER B STA 6+66.69 TO STA 8+85.03 TRANSITION D STA 49+50.00 TO STA 51+67.67 TRANSITION C



TYPICAL SECTION FAP 310 (US 67/IL 104) WITH CROSSOVER B & TRANSITION C & TRANSITION D

STA 1135+67.67 TO STA 1135+93.63 FAP 310 STA 28+63.82 TO STA 28+89.81 CROSSOVER B STA 8+85.03 TO STA 9+11.09 TRANSITION D STA 51+67.67 TO STA 51+93.63 TRANSITION C

REVISIONS	
NAME	DATE

NOTE: NOT TO SCALE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
FAP 310 (US 67/IL 104)
SHEET 9 OF 24

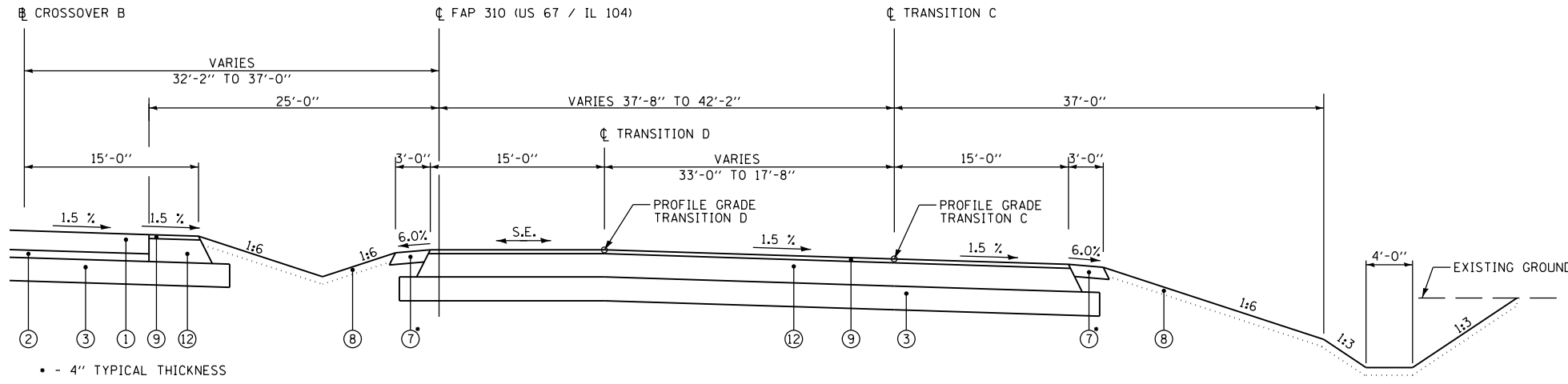
DATE 4/2009 DRAWN BY SEB
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	68
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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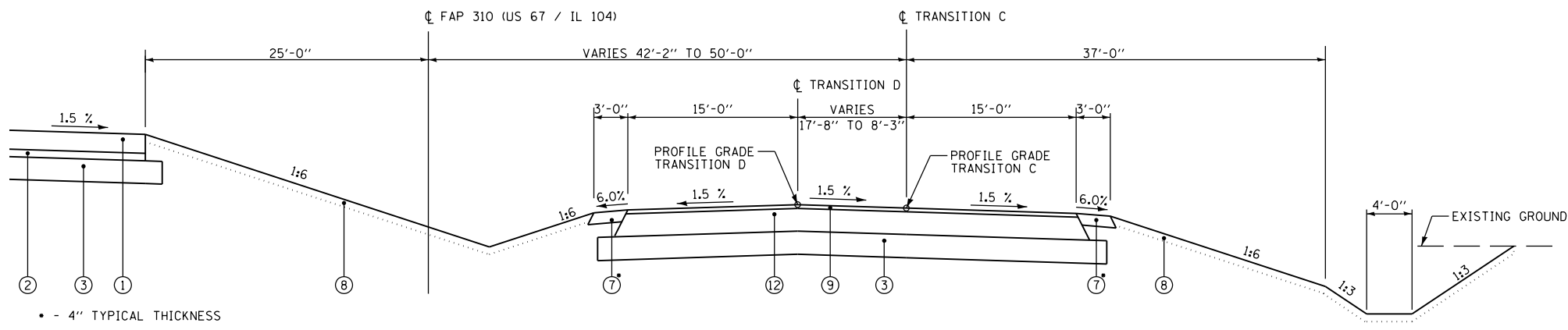
• - 4" TYPICAL THICKNESS

TYPICAL SECTION FAP 310 (US 67/IL 104) WITH CROSSOVER B & TRANSITION C & TRANSITION D

STA 1135+93.63 TO STA 1138+12.30 FAP 310 STA 28+89.81 TO STA 31+08.55 CROSSOVER B STA 9+11.09 TO STA 11+30.68 TRANSITION D STA 51+93.63 TO STA 54+12.36 TRANSITION C

PROPOSED LEGEND

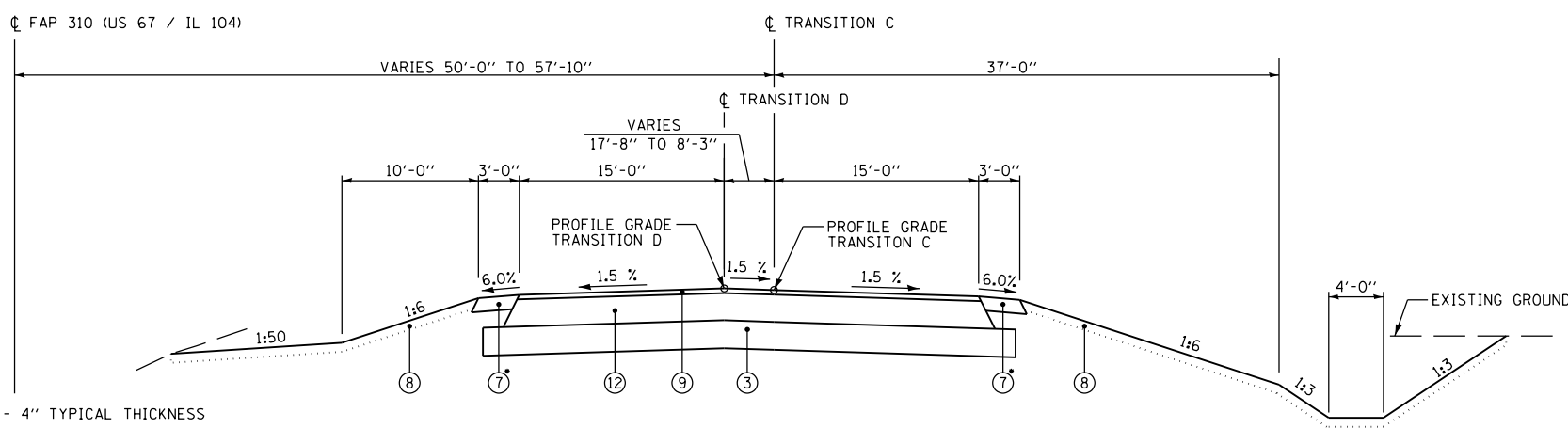
- ① PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED) - STD 420101
- ② STABILIZED SUB-BASE 4"
- ③ LIME MODIFIED SOIL 12"
- ④ PORTLAND CEMENT CONCRETE SHOULDERS 10" - STD 483001 WITH RUMBLE STRIPS - STD 642001
- ⑤ SUB-BASE GRANULAR MATERIAL, TYPE C
- ⑥ PIPE UNDERDRAINS 6" - STD 601001
- ⑦ AGGREGATE SHOULDERS, TYPE B
- ⑧ TOPSOIL FURNISH AND PLACE, 4"
- ⑨ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- ⑩ HOT-MIX ASPHALT BASE COURSE, 12" (ANY WIDTH)



• - 4" TYPICAL THICKNESS

TYPICAL SECTION FAP 310 (US 67/IL 104) WITH TRANSITION C & TRANSITION D

STA 1138+12.30 TO STA 1140+00.00 FAP 310 STA 11+30.68 TO STA 13+19.17 TRANSITION D STA 54+12.36 TO STA 56+00.23 TRANSITION C



• - 4" TYPICAL THICKNESS

TYPICAL SECTION TRANSITION C & TRANSITION D

STA 1140+00.00 TO STA 1141+35.20 FAP 310 STA 13+19.17 TO STA 14+54.93 TRANSITION D STA 56+00.23 TO STA 57+35.65 TRANSITION C

NOTE: NOT TO SCALE

REVISIONS	
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ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 10 OF 24

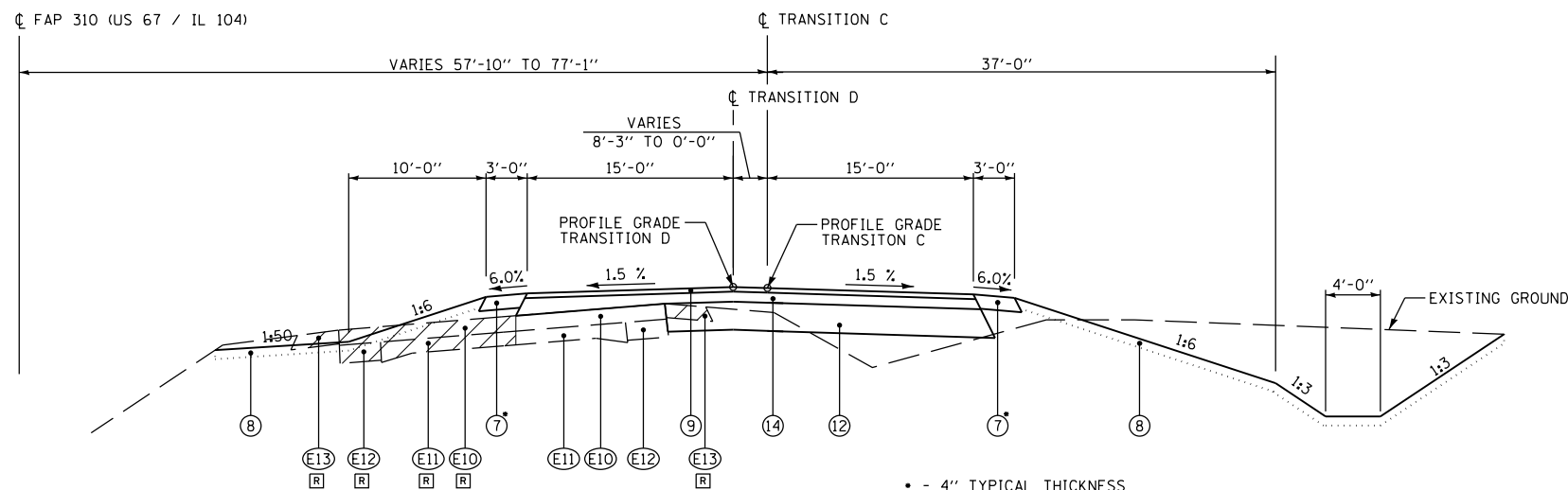
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 DATE 4/2009

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	69
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



TYPICAL SECTION TRANSITION C & TRANSITION D

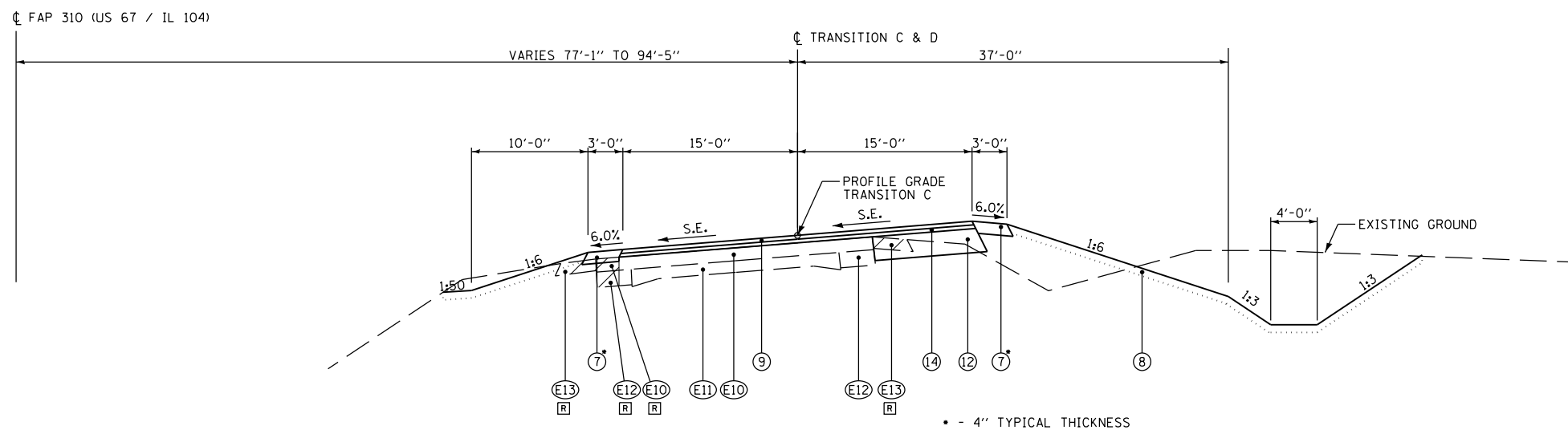
STA 1141+35.20 TO STA 1143+83.80 FAP 310 STA 14+54.93 TO STA 17+04.58 TRANSITION D STA 57+35.65 TO STA 59+85.00 TRANSITION C

EXISTING LEGEND

- (E10) EXISTING HOT-MIX ASPHALT SURFACE & BINDER, 7" & VARIES
- (E11) EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, 9"-6"-9"
- (E12) EXISTING PORTLAND CEMENT CONCRETE BASE COURSE WIDENING, 9"
- (E13) EXISTING HOT-MIX ASPHALT SHOULDER, 6"
- (R) REMOVAL PAY ITEM

PROPOSED LEGEND

- (7) AGGREGATE SHOULDERS, TYPE B
- (8) TOPSOIL FURNISH AND PLACE, 4"
- (9) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (12) HOT-MIX ASPHALT BASE COURSE, 12" (ANY WIDTH)
- (14) POLYMERIZED LEVELING BINDER (MACHINE METHOD), N70, VARIABLE DEPTH



TYPICAL SECTION TRANSITION C & TRANSITION D

STA 1143+83.80 TO STA 1145+73.37 FAP 310 STA 17+04.58 TO STA 17+24.96 TRANSITION D STA 59+85.00 TO STA 61+55.00 S.E. TRANS TRANSITION C
STA 61+55.00 TO STA 61+75.36 S.E. = 5.1%

NOTE: NOT TO SCALE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 11 OF 24

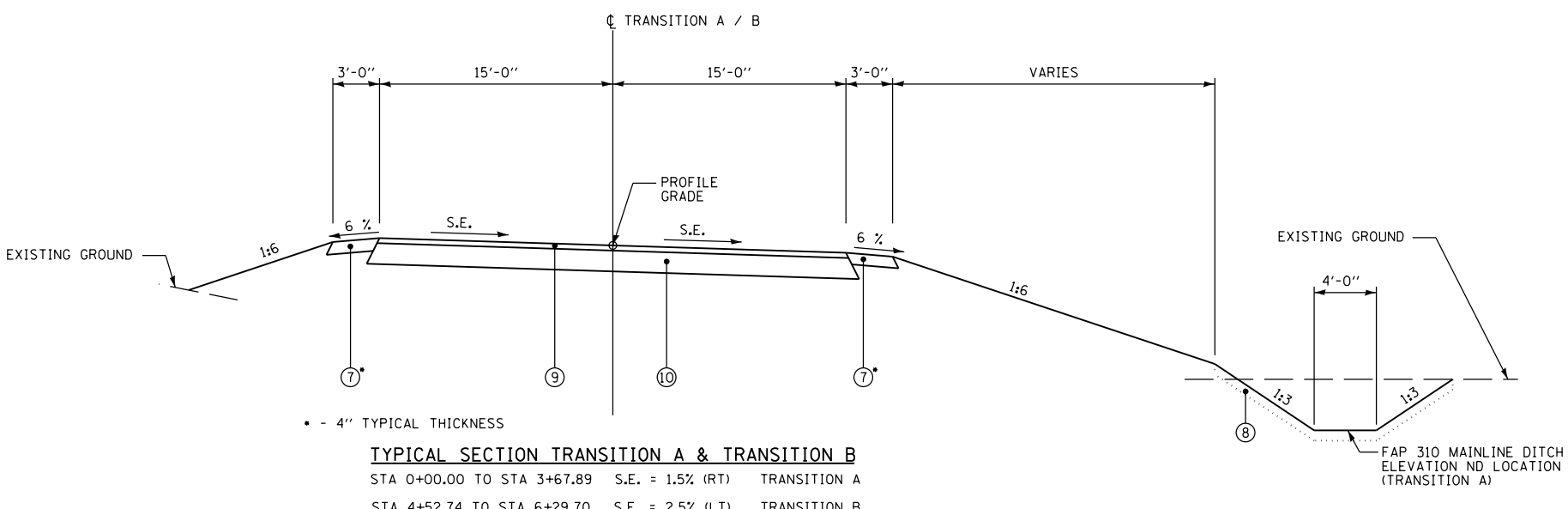
DATE 4/2009
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	70
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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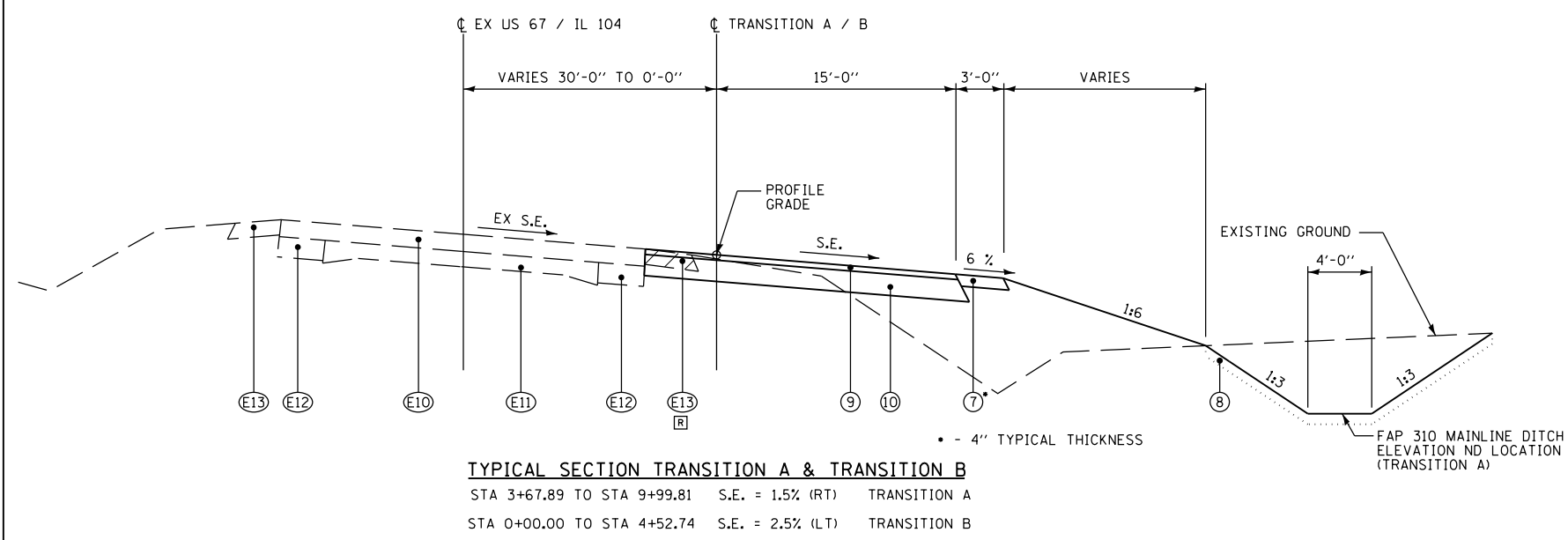


EXISTING LEGEND

- (E10) EXISTING HOT-MIX ASPHALT SURFACE & BINDER, 7" & VARIES
- (E11) EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, 9"-6"-9"
- (E12) EXISTING PORTLAND CEMENT CONCRETE BASE COURSE WIDENING, 9"
- (E13) EXISTING HOT-MIX ASPHALT SHOULDER, 6"
- (R) REMOVAL PAY ITEM

PROPOSED LEGEND

- (7) AGGREGATE SHOULDERS, TYPE B
- (8) TOPSOIL FURNISH AND PLACE, 4"
- (9) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (10) HOT-MIX ASPHALT BASE COURSE, 8" (ANY WIDTH)



NOTE: NOT TO SCALE

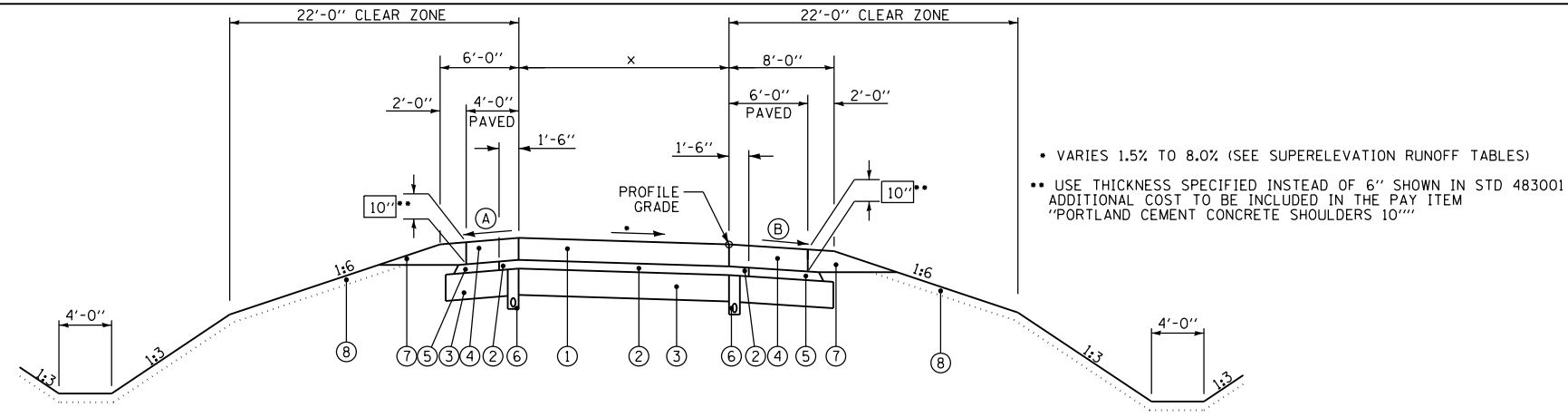
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 12 OF 24
 DRAWN BY SEB
 CHECKED BY
 DATE 4/2009

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	71
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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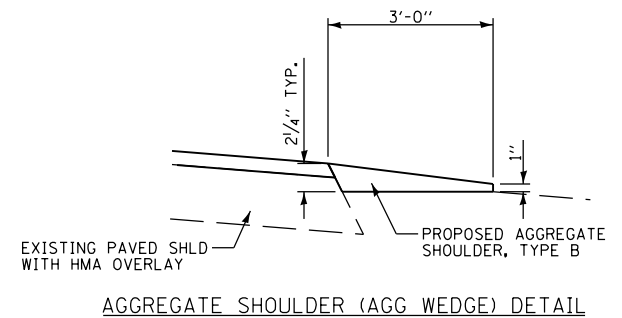
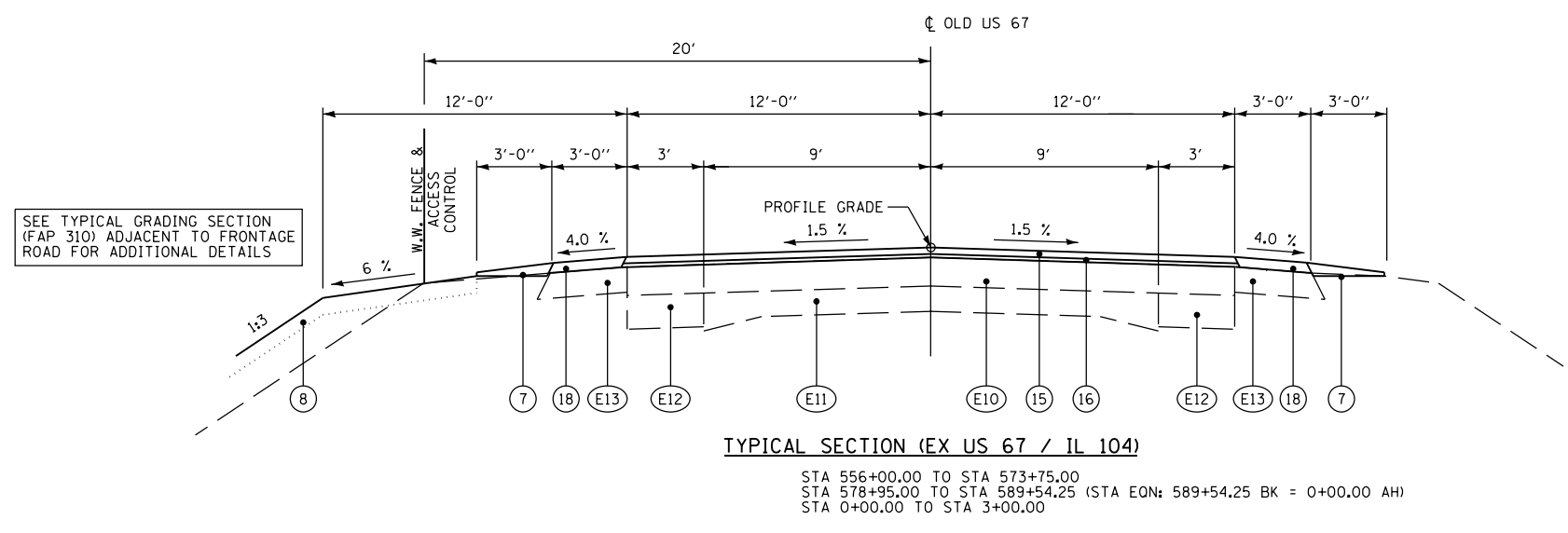
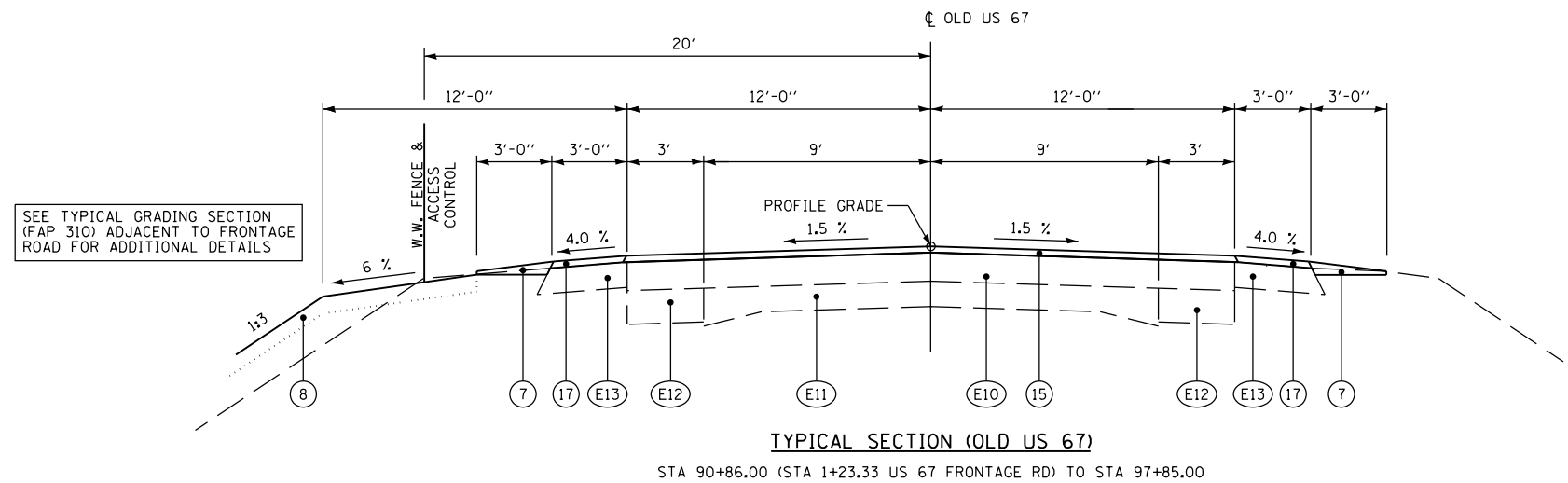
- | | | | |
|--------------------------------|---------------------------------------|--------------------------------|---------------------------------------|
| TYPICAL SECTION RAMP A | | TYPICAL SECTION RAMP C | |
| STA. 0+23.81 TO STA. 1+20.74 | x = VARIES (SEE INTERSECTION DETAIL) | STA. 0+26.56 TO STA. 1+66.19 | x = VARIES (SEE INTERSECTION DETAIL) |
| STA. 1+20.74 TO STA. 2+08.38 | x = 21'-2" - 16'-0" | STA. 1+66.19 TO STA. 2+53.83 | x = 21'-2" - 16'-0" |
| STA. 2+08.38 TO STA. 13+35.60 | x = 16'-0" | STA. 2+53.83 TO STA. 11+02.96 | x = 16'-0" |
| STA. 13+35.60 TO STA. 26+15.60 | x = VARIES (SEE RAMP TERMINAL DETAIL) | STA. 11+02.96 TO STA. 23+82.96 | x = VARIES (SEE RAMP TERMINAL DETAIL) |
-
- | | | | |
|--------------------------------|---------------------------------------|--------------------------------|---------------------------------------|
| TYPICAL SECTION RAMP B | | TYPICAL SECTION RAMP D | |
| STA. 0+00.00 TO STA. 6+60.08 | x = VARIES (SEE RAMP TERMINAL DETAIL) | STA. 0+00.00 TO STA. 6+60.11 | x = VARIES (SEE RAMP TERMINAL DETAIL) |
| STA. 6+60.08 TO STA. 17+94.45 | x = 16'-0" | STA. 6+60.11 TO STA. 19+03.51 | x = 16'-0" |
| STA. 17+94.45 TO STA. 19+22.62 | x = VARIES (SEE INTERSECTION DETAIL) | STA. 19+03.51 TO STA. 19+91.96 | x = VARIES (SEE INTERSECTION DETAIL) |

EXISTING LEGEND

- (E10) EXISTING HOT-MIX ASPHALT SURFACE & BINDER, 7" & VARIES
- (E11) EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, 9"-6"-9"
- (E12) EXISTING PORTLAND CEMENT CONCRETE BASE COURSE WIDENING, 9"
- (E13) EXISTING HOT-MIX ASPHALT SHOULDER, 6"
- [R] REMOVAL PAY ITEM

PROPOSED LEGEND

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED) - STD 420101
 - (2) STABILIZED SUB-BASE 4"
 - (3) LIME MODIFIED SOIL 12"
 - (4) PORTLAND CEMENT CONCRETE SHOULDERS 10" - STD 483001
 - (5) SUB-BASE GRANULAR MATERIAL, TYPE C
 - (6) PIPE UNDERDRAINS 6" - STD 601001
 - (7) AGGREGATE SHOULDERS, TYPE B
 - (8) TOPSOIL FURNISH AND PLACE, 4"
 - (15) HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 1 1/2"
 - (16) LEVELING BINDER (MACHINE METHOD), N50, 3/4"
 - (17) HOT-MIX ASPHALT SHOULDERS, 1 1/2"
 - (18) HOT-MIX ASPHALT SHOULDERS, 2 1/4"
- (A) SHOULDER SLOPE - HIGH SIDE OF S.E.: WHEN THE S.E. RATE OF THE PAVEMENT IS BETWEEN 0 AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN S.E. RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND THE SHOULDER SHALL BE 8%.
- (B) SHOULDER SLOPE - LOW SIDE OF S.E.: SLOPE SHALL BE THE SAME AS THE S.E. BUT NOT LESS THAN 4%.



REVISIONS	
NAME	DATE

NOTE: NOT TO SCALE

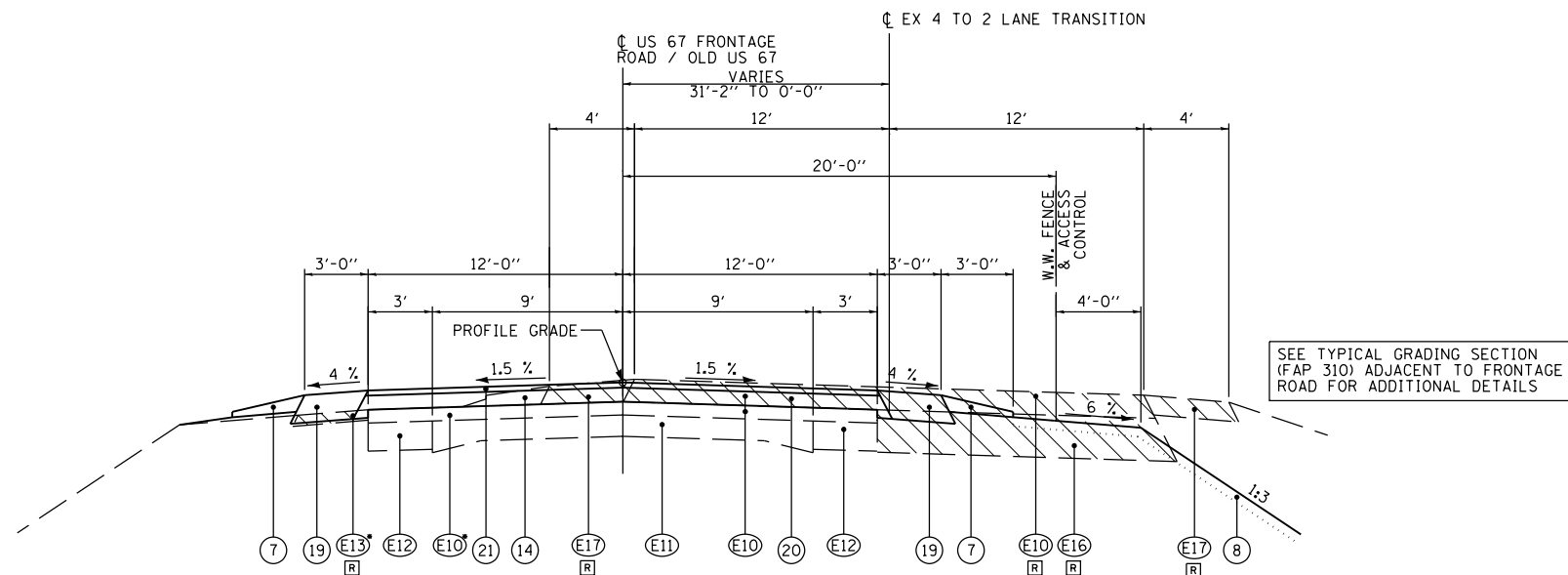
ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 13 OF 24

DATE 4/2009
 DRAWN BY SEB
 CHECKED BY

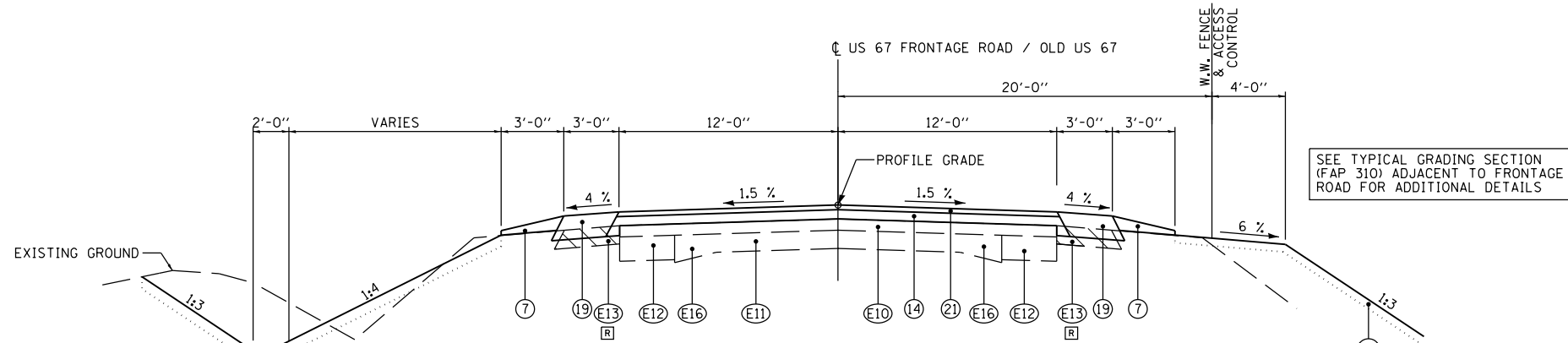
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	72
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

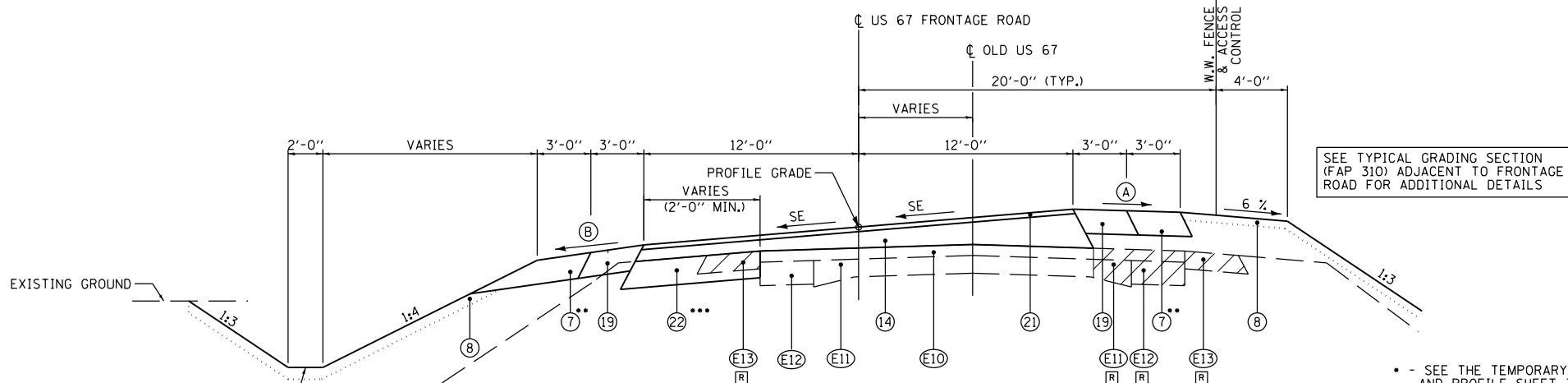
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TYPICAL SECTION (US 67 FRONTAGE ROAD)
STA 1+23.33 (STA 90+86.00 OLD US 67) TO STA 6+80.47



TYPICAL SECTION (US 67 FRONTAGE ROAD)
STA 6+80.47 TO STA 7+97.00
STA 53+05.00 TO STA 54+20.00
STA 522+08.00 TO STA 522+70.00



TYPICAL SUPERELEVATED SECTION (US 67 FRONTAGE ROAD)
STA 7+97.00 TO STA 10+09.00 S.E. TRANSITION
STA 10+09.00 TO STA 11+35.99 S.E. = 8.0%
STA 49+52.51 TO STA 50+93.00 S.E. = 8.0%
STA 50+93.00 TO STA 53+05.00 S.E. TRANSITION
STA 520+14.60 TO STA 520+15.00 S.E. TRANSITION
STA 520+15.00 TO STA 522+08.00 S.E. = 6.0%
STA 522+70.00 TO STA 524+68.00 S.E. TRANSITION

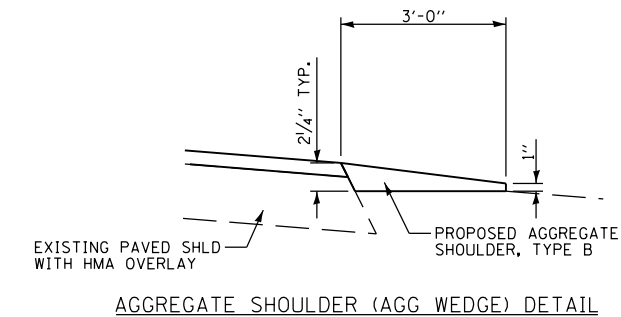
EXISTING LEGEND

- (E10) EXISTING HOT-MIX ASPHALT SURFACE & BINDER, 7" & VARIES
- (E11) EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, 9"-6"-9"
- (E12) EXISTING PORTLAND CEMENT CONCRETE BASE COURSE WIDENING, 9"
- (E13) EXISTING HOT-MIX ASPHALT SHOULDER, 6"
- (E16) EXISTING HOT-MIX ASPHALT BASE COURSE, 12"
- (E17) EXISTING HOT-MIX ASPHALT SHOULDER, 8" & VARIES
- (R) REMOVAL PAY ITEM

PROPOSED LEGEND

- (7) AGGREGATE SHOULDERS, TYPE B
- (8) TOPSOIL FURNISH AND PLACE, 4"
- (14) LEVELING BINDER (MACHINE METHOD), N50, VARIABLE DEPTH
- (19) HOT-MIX ASPHALT SHOULDERS, 8"
- (20) HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- (21) HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 2"
- (22) HOT-MIX ASPHALT BASE COURSE, 9" (ANY WIDTH)

- (A) SHOULDER SLOPE - HIGH SIDE OF S.E.: WHEN THE S.E. RATE OF THE PAVEMENT IS BETWEEN 0 AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN S.E. RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND THE SHOULDER SHALL BE 8%.
- (B) SHOULDER SLOPE - LOW SIDE OF S.E.: SLOPE SHALL BE THE SAME AS THE S.E. BUT NOT LESS THAN 4%.



AGGREGATE SHOULDER (AGG WEDGE) DETAIL

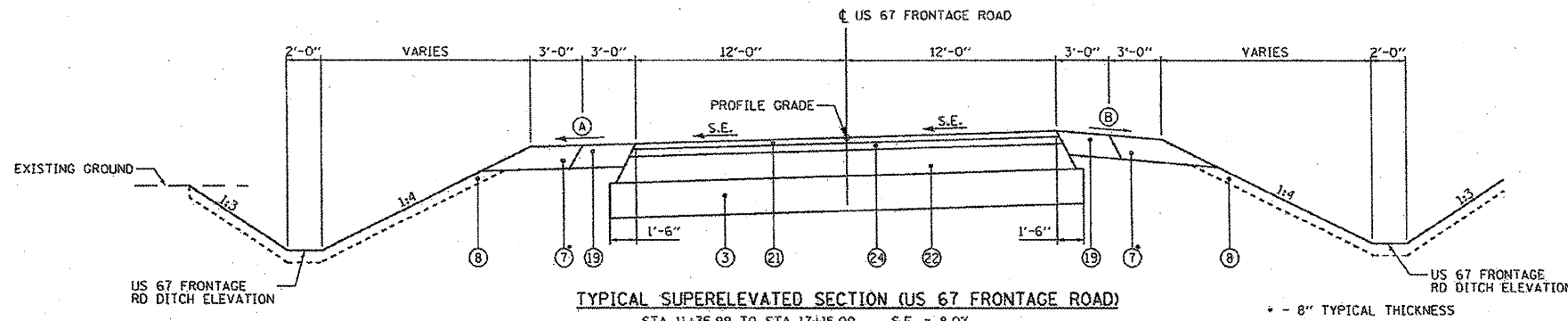
- SEE THE TEMPORARY PROFILE ON THE PLAN AND PROFILE SHEET TO PROVIDE TRANSITION FOR HOT-MIX ASPHALT BASE COURSE.
 .. - 8" TYPICAL THICKNESS
 ... - BEGIN STA 9+49.93, END STA 51+52.74

REVISIONS	
NAME	DATE

NOTE: NOT TO SCALE
 ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 14 OF 24
 DATE 4/2009
 DRAWN BY SEB
 CHECKED BY

10:55:02 AM
4/26/2011

CONTRACT NO. 12667			
F.A.P. SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310 69-3(3HB)	MORGAN	793	73
STA.		TO STA.	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	



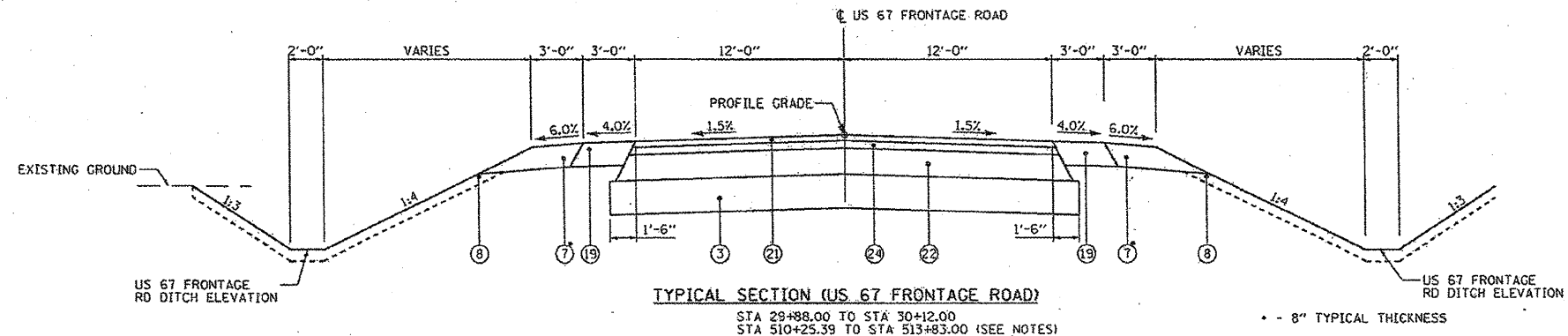
TYPICAL SUPERELEVATED SECTION (US 67 FRONTAGE ROAD)

STA 11+35.99 TO STA 17+15.00	S.E. = 8.0%	
STA 17+15.00 TO STA 18+93.00	S.E. TRANSITION	
STA 18+93.00 TO STA 20+11.00	S.E. TRANSITION	
STA 20+71.00 TO STA 27+76.00	S.E. = 8.0%	
STA 27+76.00 TO STA 29+88.00	S.E. TRANSITION	
STA 30+12.00 TO STA 32+24.00	S.E. TRANSITION	
STA 32+24.00 TO STA 39+85.00	S.E. = 8.0%	
STA 39+85.00 TO STA 41+63.00	S.E. TRANSITION	
STA 41+63.00 TO STA 43+41.00	S.E. TRANSITION	
STA 43+41.00 TO STA 49+52.51	S.E. = 8.0%	
STA 513+83.00 TO STA 515+76.00	S.E. TRANSITION	(SEE NOTES)
STA 515+76.00 TO STA 520+14.60	S.E. = 6.0%	(SEE NOTES)

PROPOSED LEGEND

- ③ LIME MODIFIED SOIL 12"
 - ⑦ AGGREGATE SHOULDERS, TYPE B
 - ⑧ TOPSOIL FURNISH AND PLACE, 4"
 - ⑱ HOT-MIX ASPHALT SHOULDERS, 8"
 - ⑳ HOT-MIX ASPHALT SURFACE COURSE, MIX "C", NSO, 2"
 - ㉑ HOT-MIX ASPHALT BASE COURSE, 9"
 - ㉒ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2½"
- Ⓐ SHOULDER SLOPE - HIGH SIDE OF S.E.: WHEN THE S.E. RATE OF THE PAVEMENT IS BETWEEN 0 AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN S.E. RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND THE SHOULDER SHALL BE 8%.
- Ⓑ SHOULDER SLOPE - LOW SIDE OF S.E.: SLOPE SHALL BE THE SAME AS THE S.E. BUT NOT LESS THAN 4%.

NOTES:
1) WITHOUT LIME MODIFIED SOIL.
2) WITH HOT-MIX ASPHALT BINDER COURSE, IL-19.0, NSO, 2½"



TYPICAL SECTION (US 67 FRONTAGE ROAD)

STA 29+88.00 TO STA 30+12.00
STA 510+25.39 TO STA 513+83.00 (SEE NOTES)

NOTE: NOT TO SCALE

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		TYPICAL SECTIONS
DATE 4/2009		FAP 310 (US 67/IL 104) SHEET 15 OF 24
DRAWN BY		DATE 4/2009
CHECKED BY		DATE 4/2009

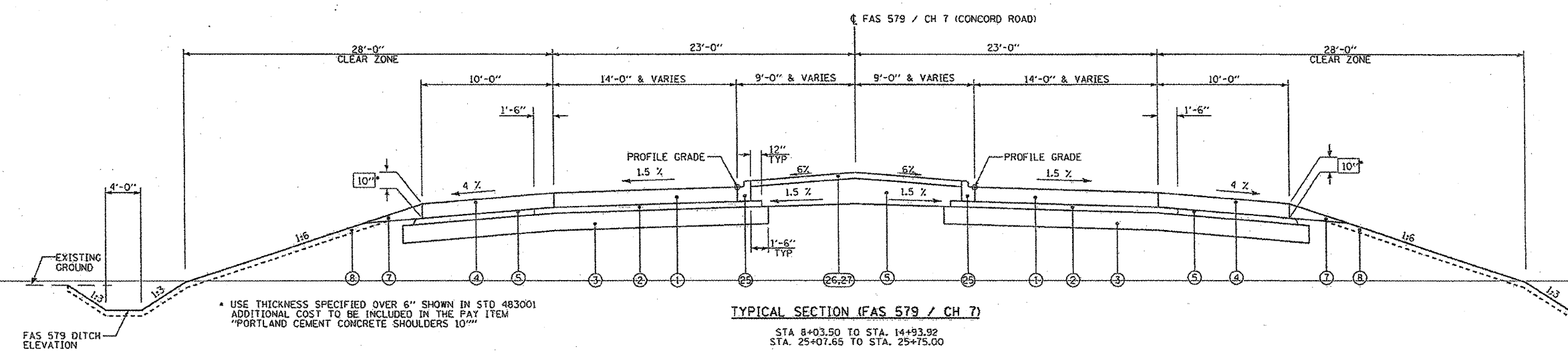
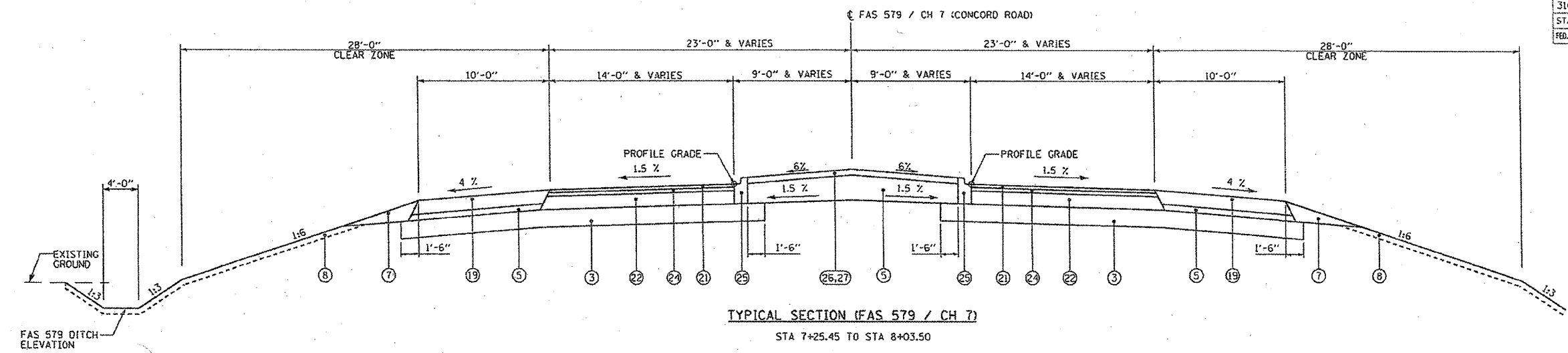
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4/26/2011

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CONTRACT NO. 12667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
310	69-3(3HB)	MORGAN	193
STA. TO STA.		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	



* USE THICKNESS SPECIFIED OVER 6" SHOWN IN STD 483001
ADDITIONAL COST TO BE INCLUDED IN THE PAY ITEM
"PORTLAND CEMENT CONCRETE SHOULDERS 10"

PROPOSED LEGEND

- ① PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED) - STD 420101
- ② STABILIZED SUB-BASE 4"
- ③ LIME MODIFIED SOIL 12"
- ④ PORTLAND CEMENT CONCRETE SHOULDERS 10" - STD 483001
- ⑤ SUB-BASE GRANULAR MATERIAL, TYPE C, 4"
- ⑦ AGGREGATE SHOULDERS, TYPE B
- ⑧ TOPSOIL FURNISH AND PLACE, 4"
- ⑨ HOT-MIX ASPHALT SHOULDERS, 8"
- ⑪ HOT-MIX ASPHALT SURFACE COURSE, MIX "C", NSO, 2"
- ⑫ HOT-MIX ASPHALT BASE COURSE, 9"
- ⑬ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2 1/2"
- ⑮ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- ⑯ CONCRETE MEDIAN, TYPE M-4.06 (WHEN WIDTH < 12')
- ⑰ CONCRETE MEDIAN SURFACE, 4 INCH

NOTE: NOT TO SCALE

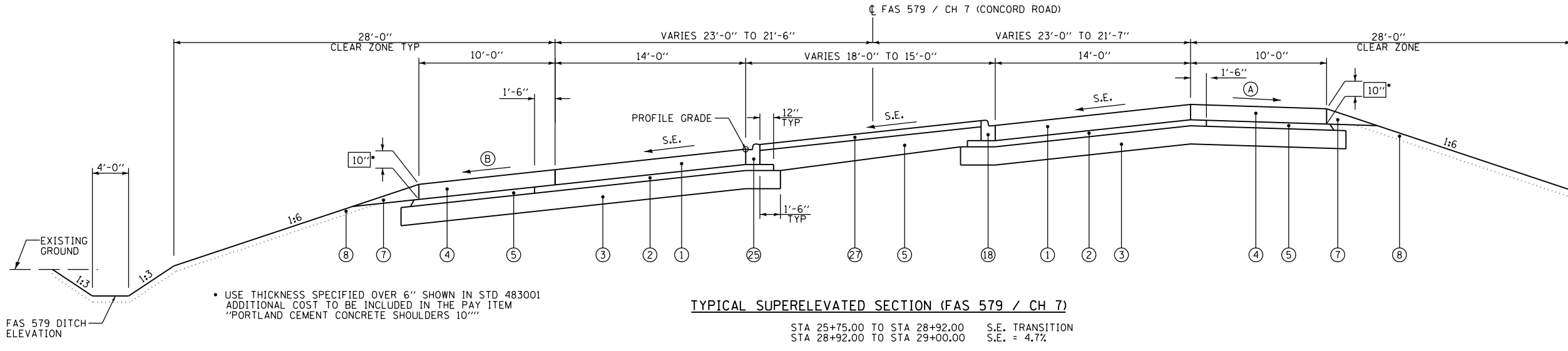
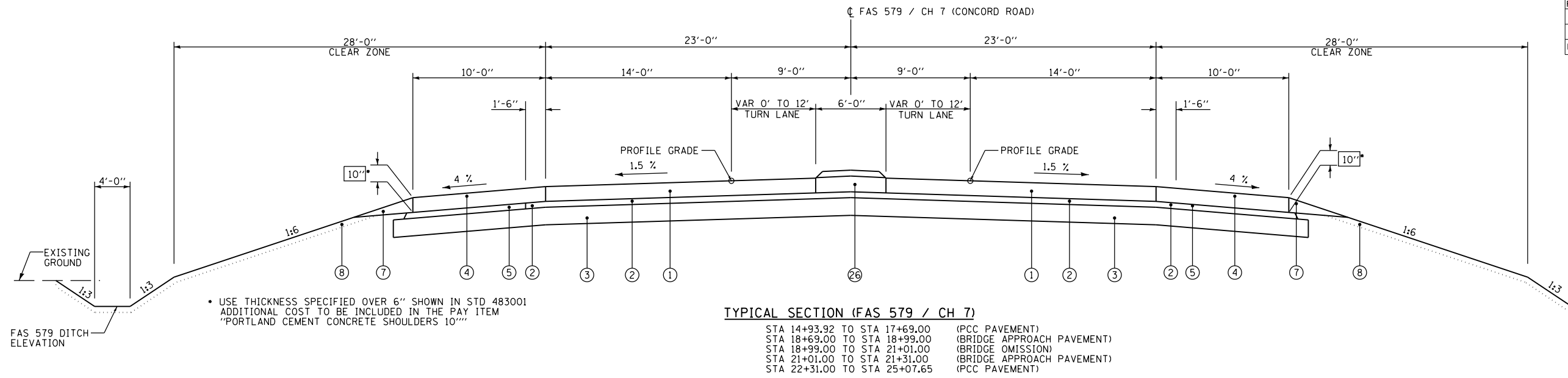
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
FAP 310 (US 67/IL 104)
SHEET 16 OF 24
DRAWN BY SEB
CHECKED BY
DATE 4/2009

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	75
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

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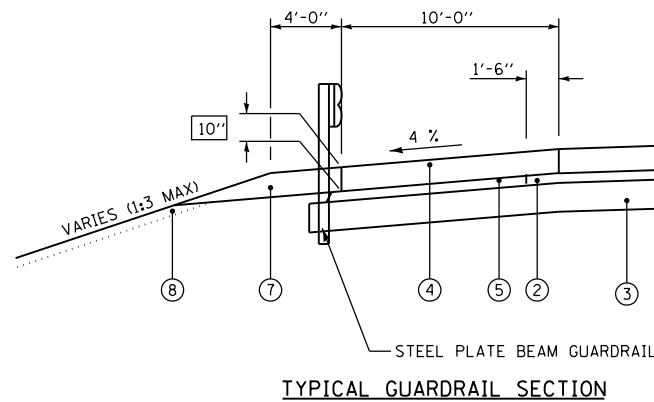
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PROPOSED LEGEND

- ① PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED) - STD 420101
- ② STABILIZED SUB-BASE 4"
- ③ LIME MODIFIED SOIL 12"
- ④ PORTLAND CEMENT CONCRETE SHOULDERS 10" - STD 483001
- ⑤ SUB-BASE GRANULAR MATERIAL, TYPE C, 4"
- ⑦ AGGREGATE SHOULDERS, TYPE B
- ⑧ TOPSOIL FURNISH AND PLACE, 4"
- ②⑤ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- ②⑥ CONCRETE MEDIAN, TYPE M-4.06 (WHEN WIDTH < 12')
- ②⑦ CONCRETE MEDIAN SURFACE, 4 INCH

- (A) SHOULDER SLOPE - HIGH SIDE OF S.E.: WHEN THE S.E. RATE OF THE PAVEMENT IS BETWEEN 0 AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN S.E. RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND THE SHOULDER SHALL BE 8%.
- (B) SHOULDER SLOPE - LOW SIDE OF S.E.: SLOPE SHALL BE THE SAME AS THE S.E. BUT NOT LESS THAN 4%.



NOTE: NOT TO SCALE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 17 OF 24

DATE 4/2009

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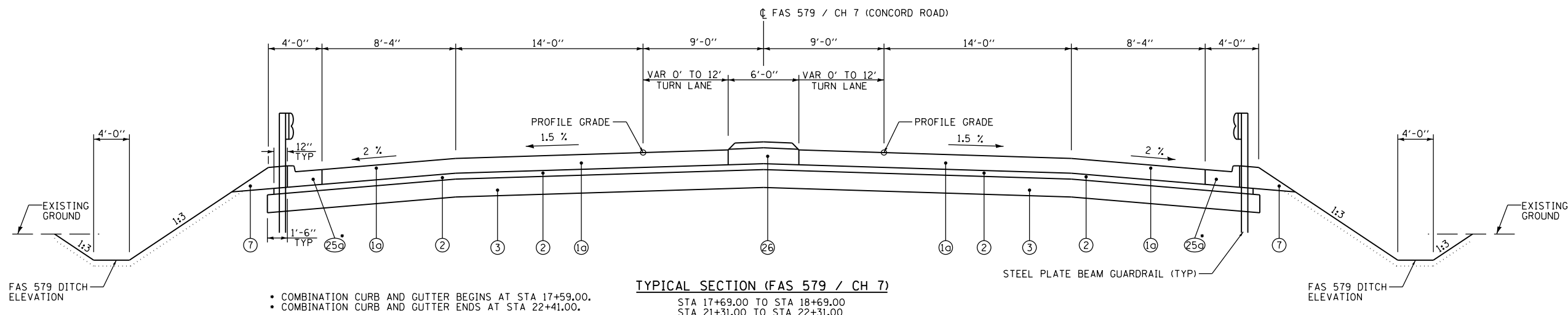
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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	76
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



PROPOSED LEGEND

- ① PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED) - STD 420101
- ①a CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 10" - STD 421001
- ② STABILIZED SUB-BASE 4"
- ③ LIME MODIFIED SOIL 12"
- ④ PORTLAND CEMENT CONCRETE SHOULDERS 10" - STD 483001
- ⑤ SUB-BASE GRANULAR MATERIAL, TYPE C, 4"
- ⑦ AGGREGATE SHOULDERS, TYPE B
- ⑧ TOPSOIL FURNISH AND PLACE, 4"
- ②5a COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- ②5b COMBINATION CONCRETE CURB AND GUTTER, TYPE B-4.24
- ②6 CONCRETE MEDIAN, TYPE M-4.06 (WHEN WIDTH < 12')
- ②7 CONCRETE MEDIAN SURFACE, 4 INCH

- Ⓐ SHOULDER SLOPE - HIGH SIDE OF S.E.: WHEN THE S.E. RATE OF THE PAVEMENT IS BETWEEN 0 AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN S.E. RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND THE SHOULDER SHALL BE 8%.
- Ⓑ SHOULDER SLOPE - LOW SIDE OF S.E.: SLOPE SHALL BE THE SAME AS THE S.E. BUT NOT LESS THAN 4%.

NOTE: NOT TO SCALE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 18 OF 24

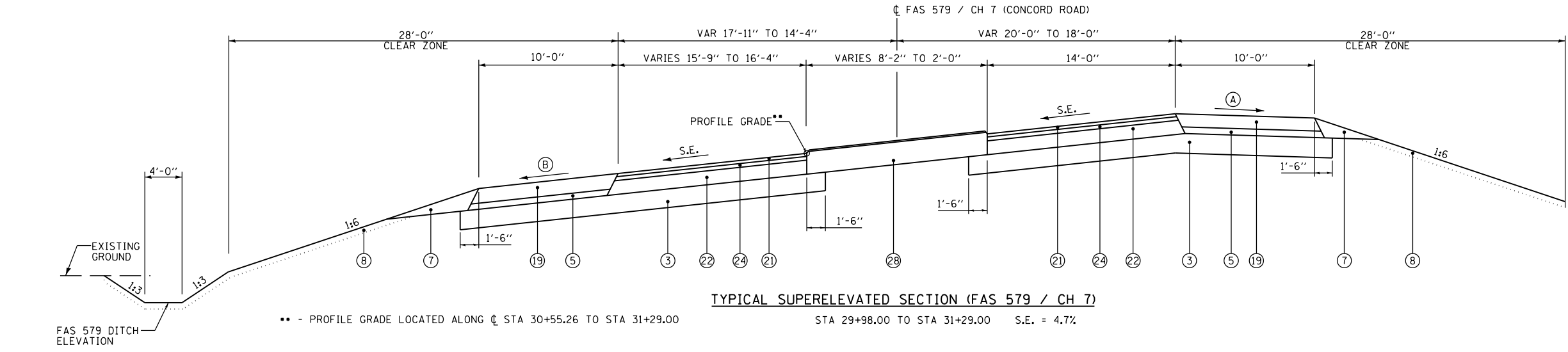
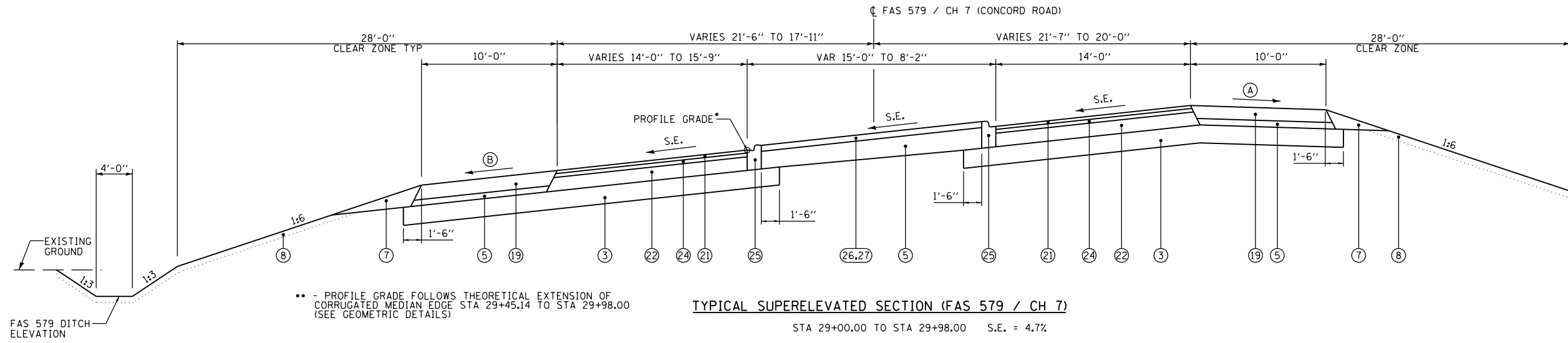
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	77
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

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PROPOSED LEGEND

- (5) SUB-BASE GRANULAR MATERIAL, TYPE C, 4"
- (7) AGGREGATE SHOULDERS, TYPE B
- (8) TOPSOIL FURNISH AND PLACE, 4"
- (19) HOT-MIX ASPHALT SHOULDERS, 8"
- (21) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 2"
- (22) HOT-MIX ASPHALT BASE COURSE, 9"
- (24) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- (25) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06
- (26) CONCRETE MEDIAN, TYPE M-4.06 (WHEN WIDTH < 12')
- (27) CONCRETE MEDIAN SURFACE, 4 INCH
- (28) CORRUGATED MEDIAN - STD 606306

- (A) SHOULDER SLOPE - HIGH SIDE OF S.E.: WHEN THE S.E. RATE OF THE PAVEMENT IS BETWEEN 0 AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN S.E. RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND THE SHOULDER SHALL BE 8%.
- (B) SHOULDER SLOPE - LOW SIDE OF S.E.: SLOPE SHALL BE THE SAME AS THE S.E. BUT NOT LESS THAN 4%.

NOTE: NOT TO SCALE

REVISIONS	
NAME	DATE

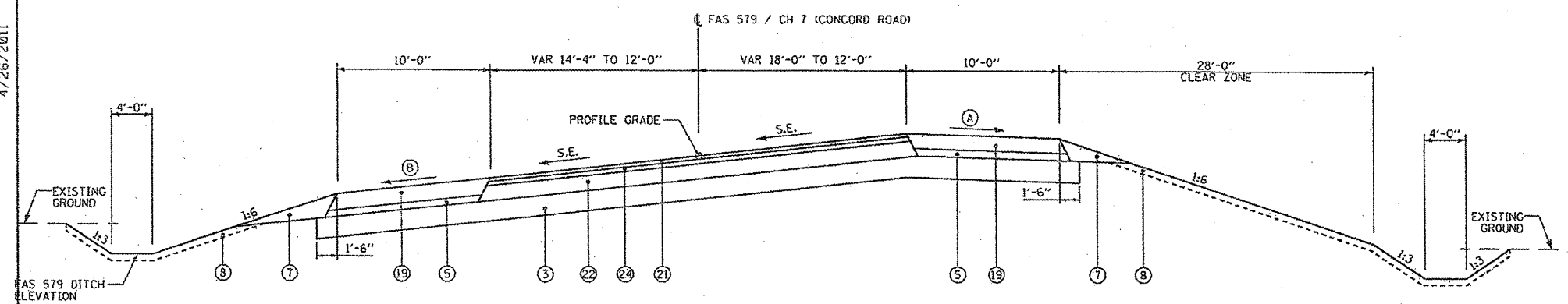
ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
FAP 310 (US 67/IL 104)
SHEET 19 OF 24

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CHECKED BY
DATE 4/2009

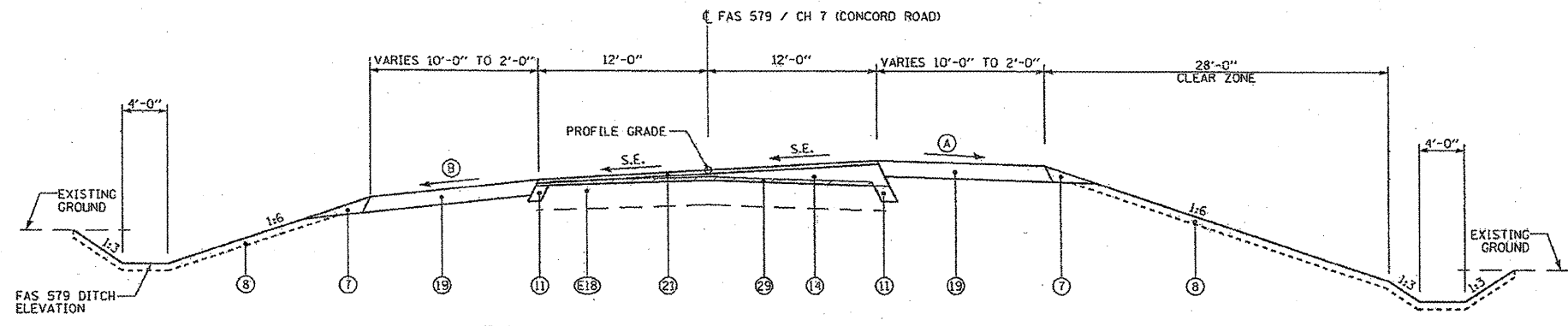
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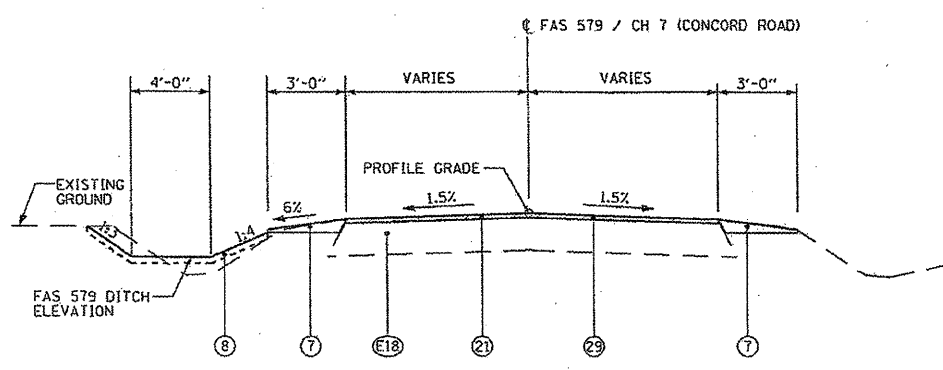
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F.A.P. NO.	SECTION	COUNTY	TOTAL SHEET NO.
310	69-3(3HB)	MORGAN	193 78
STA.	TO STA.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			



TYPICAL SUPERELEVATED SECTION (FAS 579 / CH 7)
 STA 31+29.00 TO STA 32+91.00 S.E. = 4.7%
 STA 32+91.00 TO STA 33+38.98 S.E. TRANSITION



* - LT HMA SHLD VARIES FROM 10'-0" TO 2'-0" FROM STATION 34+34.00 TO STATION 34+84.00
 STA 33+38.98 TO STA 34+84.00 S.E. TRANSITION
 * - RT HMA SHLD VARIES FROM 10'-0" TO 2'-0" FROM STATION 33+40.00 TO STATION 33+90.00



TYPICAL SECTION (FAS 579 / CH 7)
 STA 34+84.00 TO STA 37+65.00 LT
 STA 33+90.00 TO STA 37+65.00 RT

EXISTING LEGEND

- (E18) EXISTING HOT-MIX-ASPHALT PAVEMENT, VARIABLE DEPTH

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT BASE COURSE WIDENING, 8" (2' WIDTH)
 - (14) POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50, VARIABLE DEPTH
 - (19) HOT-MIX ASPHALT SHOULDERS, 8"
 - (21) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 2"
 - (22) HOT-MIX ASPHALT BASE COURSE, 9"
 - (24) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
 - (29) HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- (A) SHOULDER SLOPE - HIGH SIDE OF S.E.: WHEN THE S.E. RATE OF THE PAVEMENT IS BETWEEN 0 AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN S.E. RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND THE SHOULDER SHALL BE 8%.
 (B) SHOULDER SLOPE - LOW SIDE OF S.E.: SLOPE SHALL BE THE SAME AS THE S.E. BUT NOT LESS THAN 4%.

NOTE: NOT TO SCALE

REVISIONS	
NAME	DATE

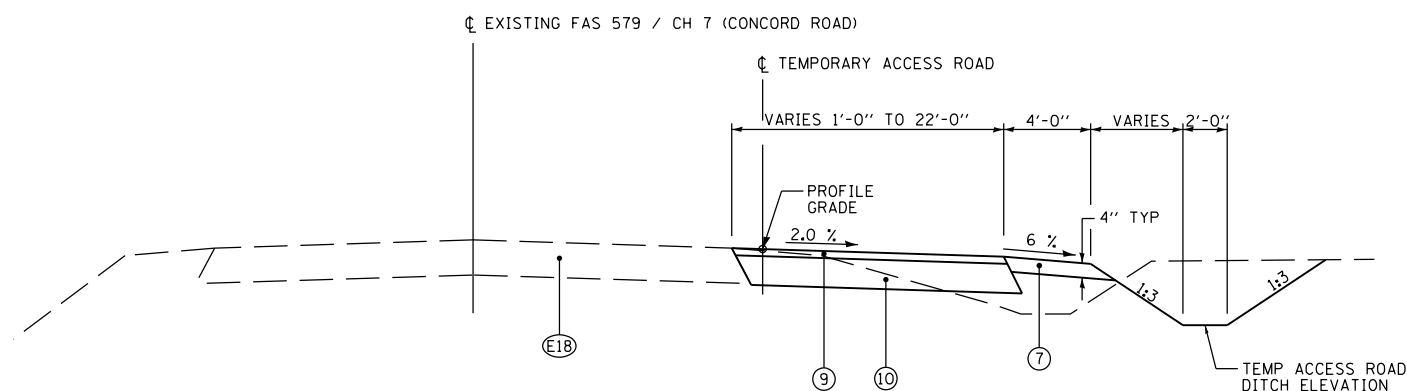
ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 20 OF 24
 DRAWN BY SEB
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 DATE 4/2009

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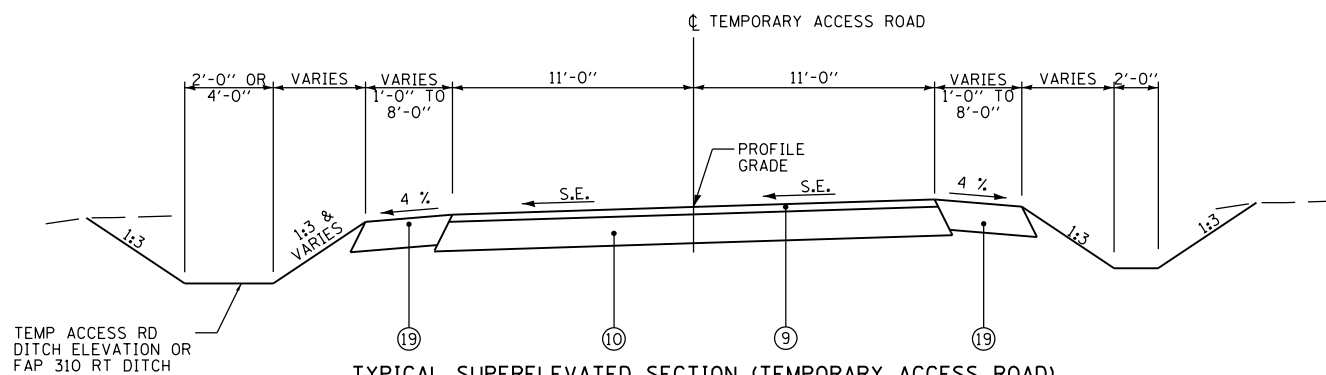
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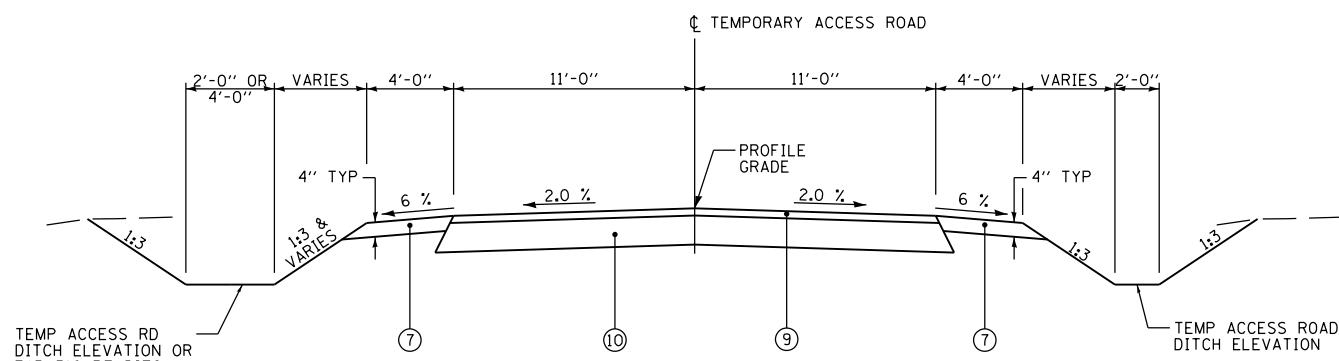
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	79
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



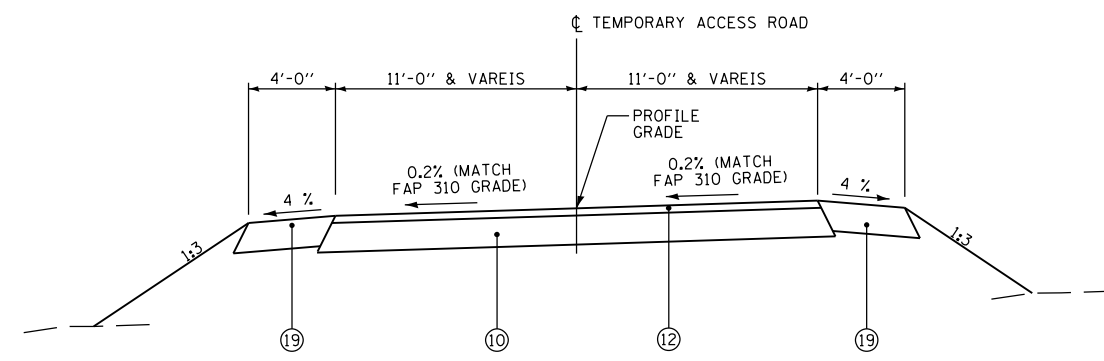
TYPICAL SECTION (TEMPORARY ACCESS ROAD)
STA 0+40.48 TO STA 2+16.74



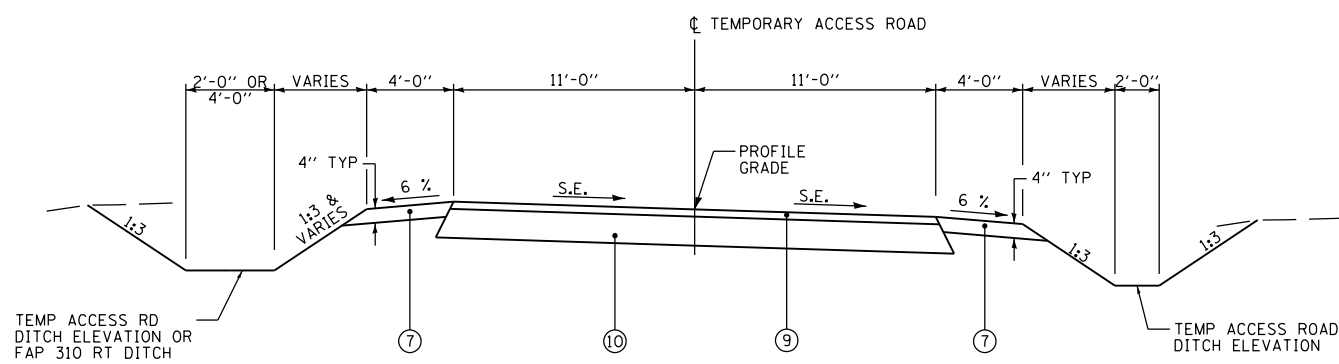
TYPICAL SUPERELEVATED SECTION (TEMPORARY ACCESS ROAD)
STA 26+07.00 TO STA 26+67.00 S.E. TRANSITION
STA 26+67.00 TO STA 27+77.01 S.E. = 2.0%
STA 26+77.01 TO STA 28+05.33 S.E. TRANSITION



TYPICAL SECTION (TEMPORARY ACCESS ROAD)
STA 2+16.74 TO STA 6+41.00
STA 15+69.00 TO STA 16+81.00
STA 22+94.00 TO STA 26+00.00



TYPICAL SECTION (TEMPORARY ACCESS ROAD)
STA 28+51.01 TO STA 28+93.01
STA 29+38.51 TO STA 30+15.36



TYPICAL SUPERELEVATED SECTION (TEMPORARY ACCESS ROAD)
STA 6+41.00 TO STA 8+45.00 S.E. TRANSITION
STA 8+45.00 TO STA 13+65.00 S.E. = 8.0%
STA 13+65.00 TO STA 15+69.00 S.E. = 8.0%
STA 16+81.00 TO STA 18+85.00 S.E. TRANSITION
STA 18+85.00 TO STA 20+90.00 S.E. = 8.0%
STA 20+90.00 TO STA 22+94.00 S.E. TRANSITION

EXISTING LEGEND

- (E18) EXISTING HOT-MIX-ASPHALT PAVEMENT, VARIABLE DEPTH

PROPOSED LEGEND

- (7) AGGREGATE SHOULDERS, TYPE B
- (9) HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 2"
- (10) HOT-MIX ASPHALT BASE COURSE, 8" (ANY WIDTH)
- (19) HOT-MIX ASPHALT SHOULDERS, 8"

NOTE: NOT TO SCALE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
FAP 310 (US 67/IL 104)
SHEET 21 OF 24

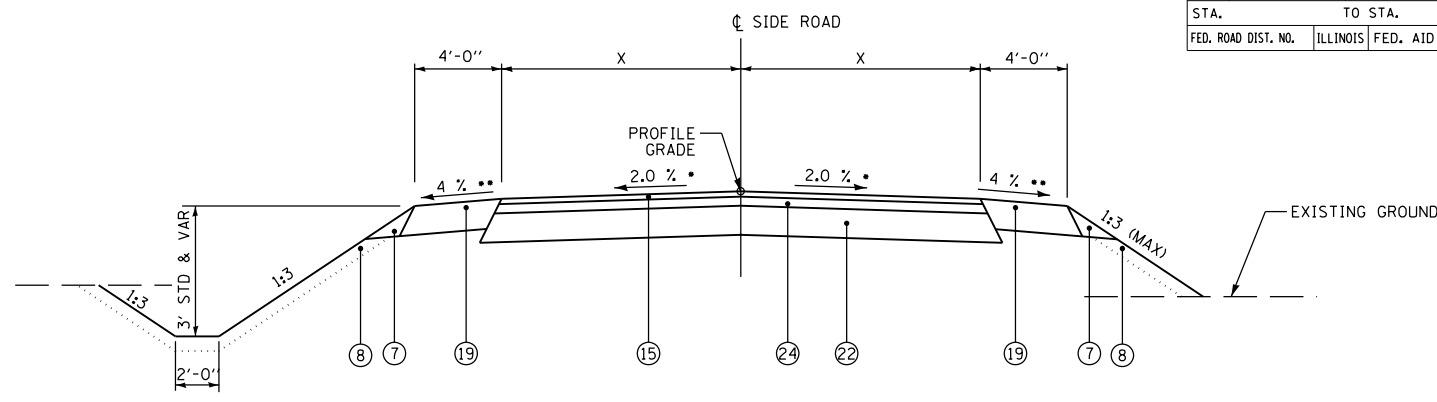
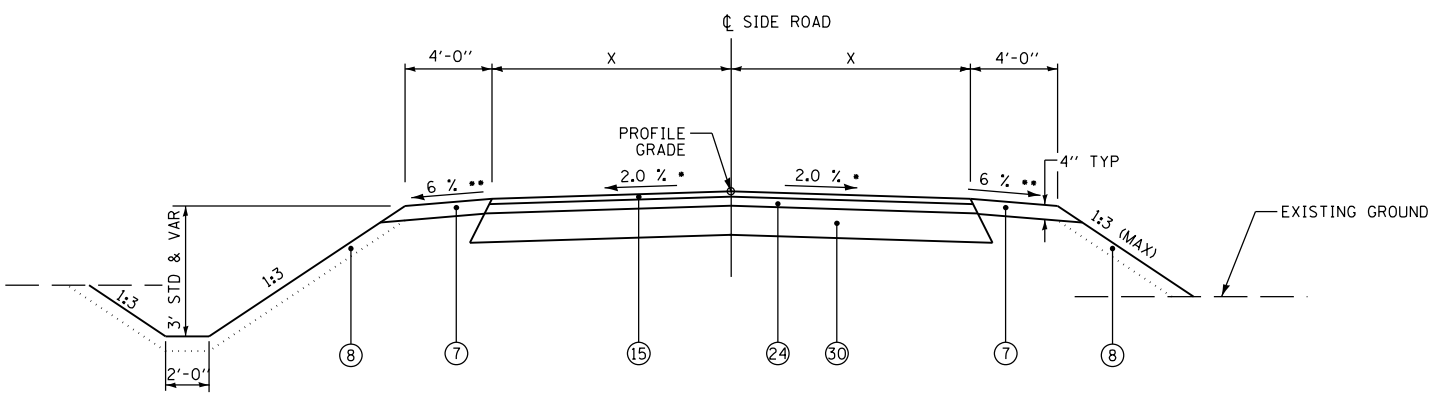
DATE 4/2009
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	80
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

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• S.E. RATES PER PLAN SHEETS •• FOR S.E. SECTIONS SEE NOTES (C) AND (D) FOR CORRESPONDING TREATMENT OF SHOULDER SLOPES

• S.E. RATES PER PLAN SHEETS •• FOR S.E. SECTIONS SEE NOTES (A) AND (B) FOR CORRESPONDING TREATMENT OF SHOULDER SLOPES

TYPICAL SECTION (HOT-MIX ASPHALT W/ AGG SHLD SIDE ROAD)

TYPICAL SECTION (HOT-MIX ASPHALT W/ HMA SHLD SIDE ROAD)

STA 38+67.25 TO STA 50+24.00	NORMAL CROWN	TR 42 (RELOCATED CREWS LANE)	X = 11'-0"
STA 50+24.00 TO STA 51+95.00	S.E. TRANSITION		
STA 51+95.00 TO STA 54+82.00	S.E. = 6.0%		
STA 54+82.00 TO STA 56+53.00	S.E. TRANSITION		
STA 56+53.00 TO STA 57+03.00	NORMAL CROWN		
STA 300+73.17 TO STA 312+63.32	NORMAL CROWN	MS ____ (NEW STREET)	X = 12'-0"
STA 2+50.00 TO STA 4+00.00	NORMAL CROWN	MS 6010 (ASH STREET)	X = 11'-0"
		(3' AGG SHLD)	
STA 81+29.46 TO STA 81+30.00	NORMAL CROWN	TR 93 (BETHEL LANE)	X = 12'-0"
STA 81+30.00 TO STA 82+40.00	S.E. TRANSITION		
STA 82+40.00 TO STA 84+32.00	S.E. = 4.0%		
STA 84+32.00 TO STA 84+32.00	S.E. TRANSITION		
STA 84+32.00 TO STA 85+05.00	S.E. TRANSITION		
STA 85+05.00 TO STA 86+39.00	S.E. = 6.0%		
STA 86+39.00 TO STA 89+08.00	S.E. TRANSITION		
STA 89+08.00 TO STA 90+87.00	S.E. TRANSITION		
STA 90+87.00 TO STA 91+37.00	NORMAL CROWN		
STA 591+29.03 TO STA 593+85.00	NORMAL CROWN	SERVICE DRIVE 91	X = 11'-0"
STA 93+83.00 TO STA 95+47.00	S.E. TRANSITION	TR 115 (RELOCATED CYLINDER HEAD ROAD)	X = 11'-0"
STA 95+47.00 TO STA 100+30.00	S.E. = 6.0%		
STA 100+30.00 TO STA 101+94.00	S.E. TRANSITION		
STA 101+94.00 TO STA 102+03.00	NORMAL CROWN		
STA 102+03.00 TO STA 103+67.00	S.E. TRANSITION		
STA 103+67.00 TO STA 107+85.00	S.E. = 6.0%		
STA 107+85.00 TO STA 109+28.00	S.E. TRANSITION		
STA 0+67.57 TO STA 1+90.00	NORMAL CROWN	OLD CYLINDER HEAD ROAD	X = 11'-0"
STA 1+90.00 TO STA 7+50.00	NORMAL CROWN		
STA 10+11.00 TO STA 13+08.93	NORMAL CROWN	GILLILAND ROAD CONNECTOR	X = 11'-0"
STA 20+75.00 TO STA 27+95.11	NORMAL CROWN	GILLILAND ROAD EXTENSION	X = 9'-0"
STA 121+27.00 TO STA 121+97.00	NORMAL CROWN	TR 30A (ST. PAUL'S CHURCH ROAD)	X = 11'-0"
STA 121+97.00 TO STA 123+11.00	S.E. TRANSITION		
STA 123+11.00 TO STA 126+69.00	S.E. = 4.0%		
STA 126+69.00 TO STA 127+83.00	S.E. TRANSITION		
STA 600+11.00 TO STA 603+75.81	NORMAL CROWN	SERVICE DRIVE 92	X = 10'-0"
STA 138+04.00 TO STA 139+47.00	S.E. TRANSITION	SBI 104, ALT 6.98 (DUTCH LANE EAST)	X = 11'-0"
STA 139+47.00 TO STA 141+97.00	S.E. = 6.0%		
STA 141+97.00 TO STA 143+30.00	S.E. TRANSITION		
STA 143+30.00 TO STA 143+60.00	NORMAL CROWN		
STA 143+60.00 TO STA 144+55.00	S.E. TRANSITION		
STA 144+55.00 TO STA 147+39.00	S.E. = 4.0%		
STA 147+39.00 TO STA 148+34.00	S.E. TRANSITION		
STA 148+34.00 TO STA 148+79.83	NORMAL CROWN		
STA 194+41.00 TO STA 194+91.00	NORMAL CROWN	TR 26 (DUTCH LANE WEST)	X = 11'-0"
STA 194+91.00 TO STA 196+14.00	S.E. TRANSITION		
STA 196+14.00 TO STA 196+84.00	S.E. = 4.0%		
STA 196+84.00 TO STA 198+07.00	S.E. TRANSITION		
STA 198+07.00 TO STA 198+69.77	NORMAL CROWN		
STA 201+29.19 TO STA 202+01.00	NORMAL CROWN		
STA 202+01.00 TO STA 203+15.00	S.E. TRANSITION		
STA 203+15.00 TO STA 205+21.00	S.E. = 4.0%		
STA 205+21.00 TO STA 206+35.00	S.E. TRANSITION		
STA 206+35.00 TO STA 206+37.00	NORMAL CROWN		
STA 206+37.00 TO STA 208+01.00	S.E. TRANSITION		
STA 208+01.00 TO STA 209+16.00	S.E. = 6.0%		
STA 209+16.00 TO STA 210+80.00	S.E. TRANSITION		
STA 210+80.00 TO STA 211+50.00	NORMAL CROWN		
STA 289+67.00 TO STA 290+90.00	NORMAL CROWN	TR 18 (HART'S GRAVEL ROAD)	X = 11'-0"
STA 290+90.00 TO STA 292+13.00	S.E. TRANSITION		
STA 292+13.00 TO STA 292+78.00	S.E. = 6.0%		
STA 292+78.00 TO STA 294+42.00	S.E. TRANSITION		
STA 294+42.00 TO STA 294+43.00	NORMAL CROWN		
STA 294+43.00 TO STA 295+57.00	S.E. TRANSITION		
STA 295+57.00 TO STA 296+97.00	S.E. = 4.0%		
STA 296+97.00 TO STA 298+11.00	S.E. TRANSITION		
STA 298+11.00 TO STA 298+78.15	NORMAL CROWN		

STA 581+10.20 TO STA 581+81.76	NORMAL CROWN	SERVICE DRIVE 81	X = 16'-0"
STA 451+07.48 TO STA 451+72.82	NORMAL CROWN	FAS 1611 (COOPER STREET)	X = 16'-0"
STA 300+12.00 TO STA 300+73.17	NORMAL CROWN	NEW STREET	X = 12'-0"
STA 312+63.32 TO STA 313+24.25			
STA 72+70.40 TO STA 74+43.00	NORMAL CROWN	TR 93 (BETHEL LANE)	X = 12'-0"
STA 74+43.00 TO STA 75+68.00	S.E. TRANSITION		
STA 75+68.00 TO STA 76+91.00	S.E. = 4.0%		
STA 76+91.00 TO STA 78+16.00	S.E. TRANSITION		
STA 78+16.00 TO STA 78+70.55	NORMAL CROWN		
STA 109+28.00 TO STA 109+88.00	NORMAL CROWN	TR 115 (RELOCATED CYLINDER HEAD ROAD)	X = 11'-0"
STA 0+11.00 TO STA 0+67.57	NORMAL CROWN	OLD CYLINDER HEAD ROAD	X = 11'-0"
STA 0+67.57 TO STA 1+90.00	LT ONLY		

PROPOSED LEGEND

- (7) AGGREGATE SHOULDERS, TYPE B
- (8) TOPSOIL FURNISH AND PLACE, 4"
- (15) HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 1 1/2"
- (19) HOT-MIX ASPHALT SHOULDERS, 8"
- (22) HOT-MIX ASPHALT BASE COURSE, 9"
- (24) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- (30) AGGREGATE BASE COURSE, TYPE A 8"
- (A) SHOULDER SLOPE - HIGH SIDE OF S.E.: WHEN THE S.E. RATE OF THE PAVEMENT IS BETWEEN 0 AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN S.E. RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND THE SHOULDER SHALL BE 8%.
- (B) SHOULDER SLOPE - LOW SIDE OF S.E.: SLOPE SHALL BE THE SAME AS THE S.E. BUT NOT LESS THAN 4%.
- (C) SHOULDER SLOPE - HIGH SIDE OF S.E.: WHEN THE S.E. RATE OF THE PAVEMENT IS BETWEEN 0 AND 2% THE SHOULDER SHALL BE SLOPED AT 6%. WHEN S.E. RATE OF THE PAVEMENT EXCEEDS 6% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND THE SHOULDER SHALL BE 8%.
- (D) SHOULDER SLOPE - LOW SIDE OF S.E.: SLOPE SHALL BE THE SAME AS THE S.E. BUT NOT LESS THAN 6%.

NOTE: NOT TO SCALE

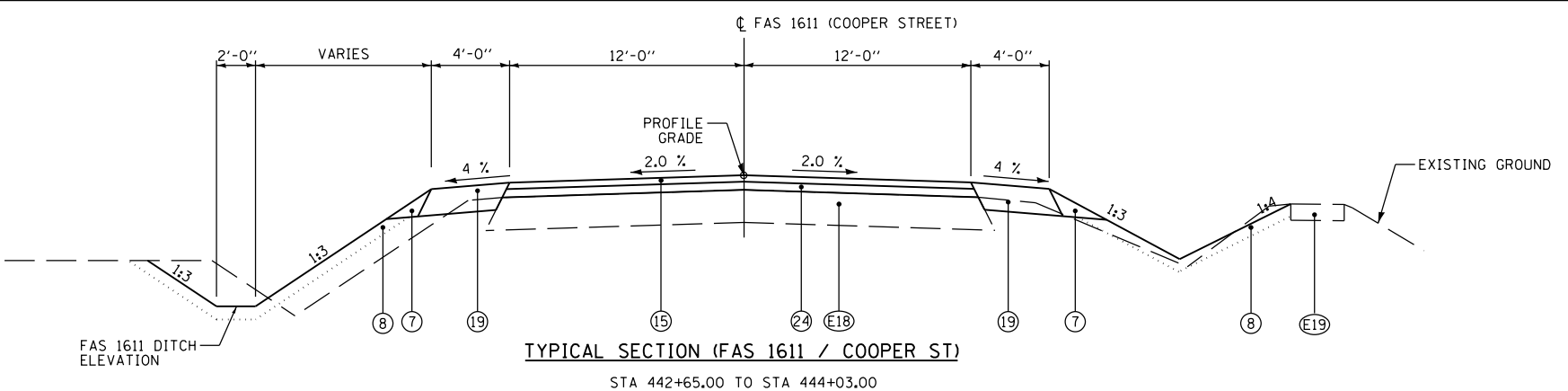
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 22 OF 24
 DRAWN BY SEB
 CHECKED BY
 DATE 4/2009

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	81
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

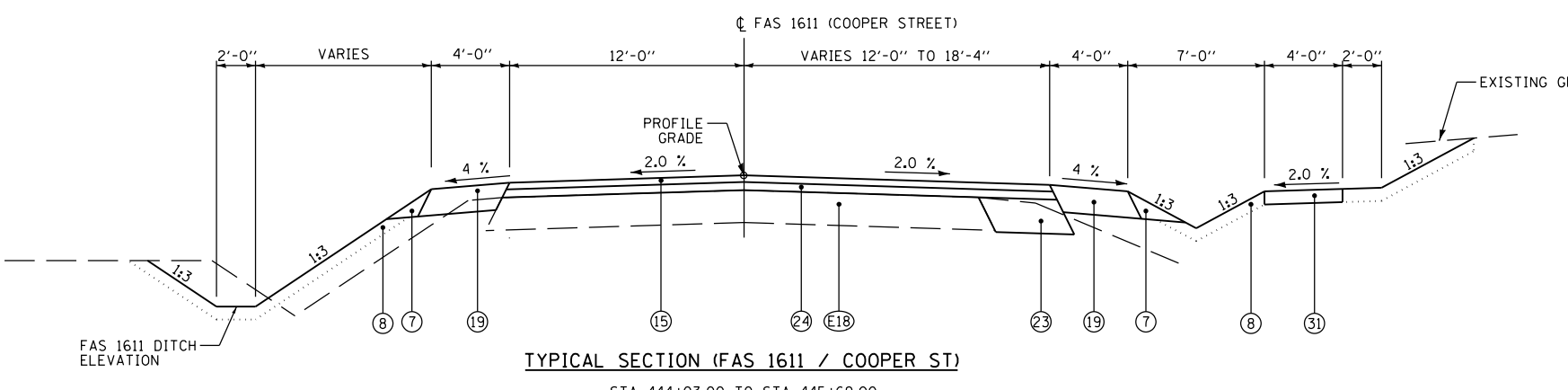
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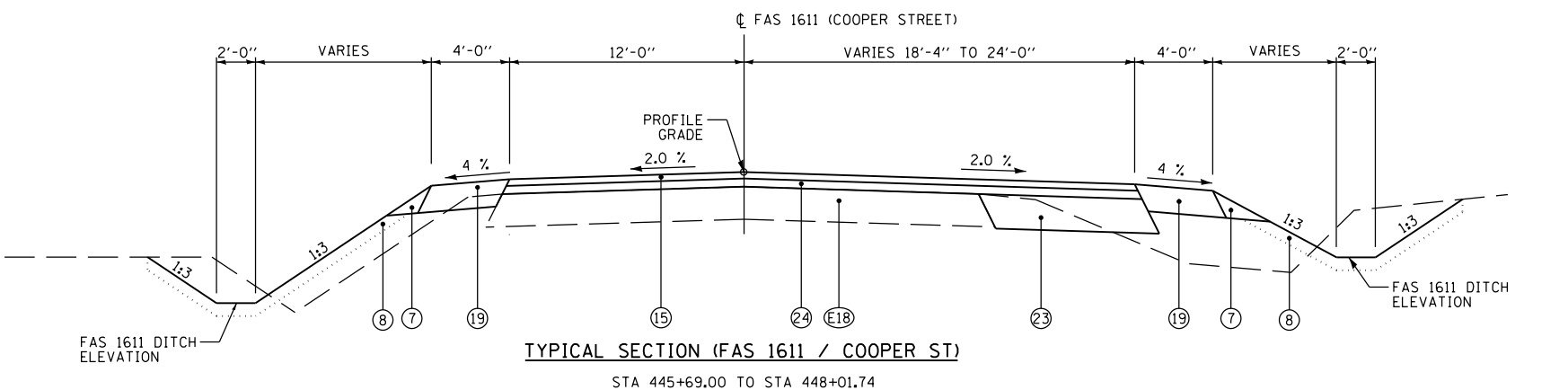
TYPICAL SECTION (FAS 1611 / COOPER ST)

STA 442+65.00 TO STA 444+03.00



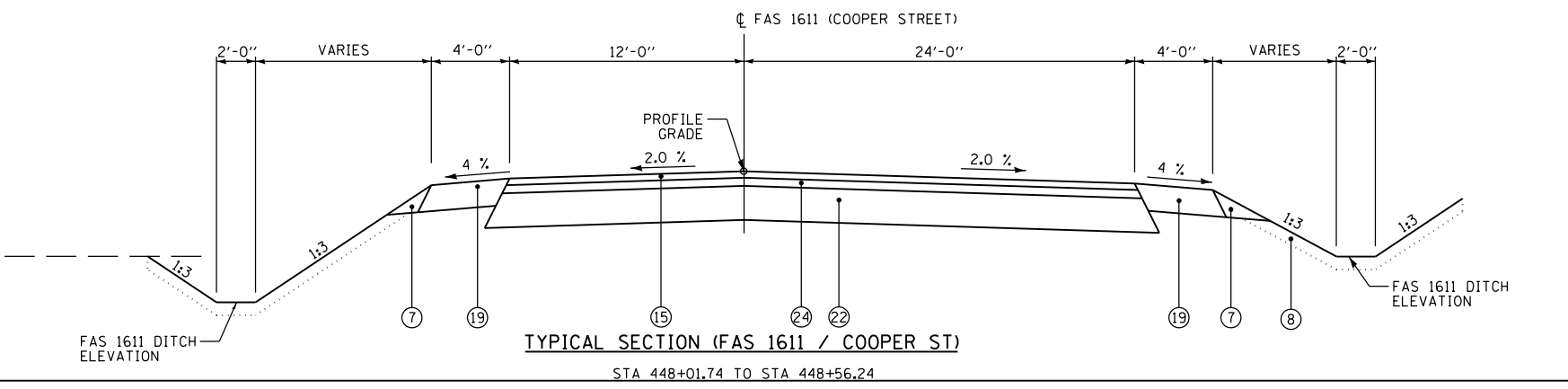
TYPICAL SECTION (FAS 1611 / COOPER ST)

STA 444+03.00 TO STA 445+69.00



TYPICAL SECTION (FAS 1611 / COOPER ST)

STA 445+69.00 TO STA 448+01.74



TYPICAL SECTION (FAS 1611 / COOPER ST)

STA 448+01.74 TO STA 448+56.24

EXISTING LEGEND

- (E18) EXISTING HOT-MIX-ASPHALT PAVEMENT, VARIABLE DEPTH
- (E19) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK

PROPOSED LEGEND

- (7) AGGREGATE SHOULDERS, TYPE B
- (8) TOPSOIL FURNISH AND PLACE, 4"
- (15) HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 1 1/2"
- (19) HOT-MIX ASPHALT SHOULDERS, 8"
- (22) HOT-MIX ASPHALT BASE COURSE, 9"
- (23) HOT-MIX ASPHALT BASE COURSE WIDENING, 9" (WHEN WIDTH < 6')
- (24) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- (31) PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH

NOTE: NOT TO SCALE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
 FAP 310 (US 67/IL 104)
 SHEET 23 OF 24
 DRAWN BY SEB
 CHECKED BY
 DATE 4/2009

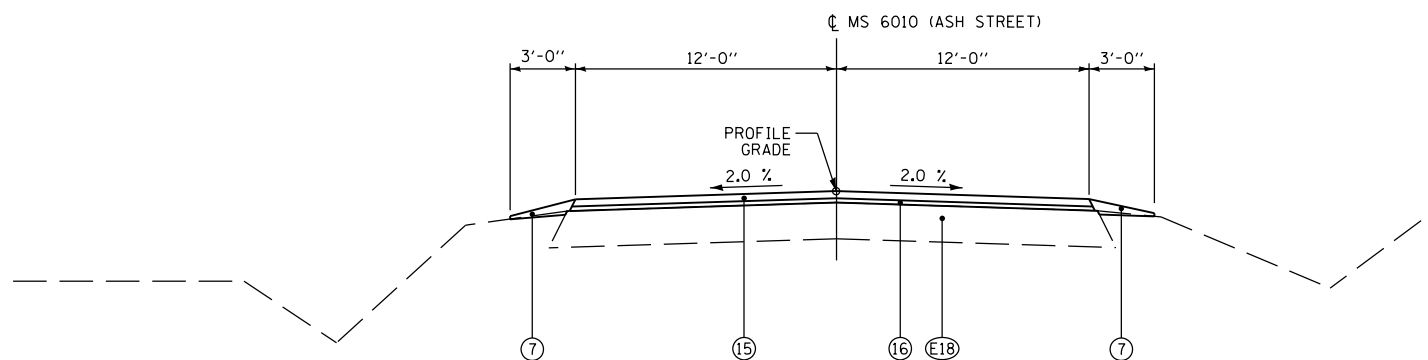
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	82
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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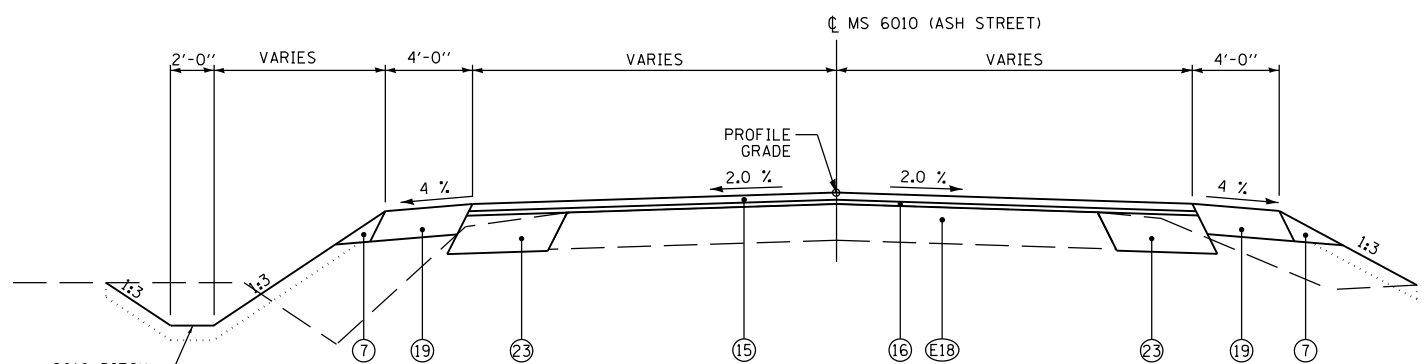
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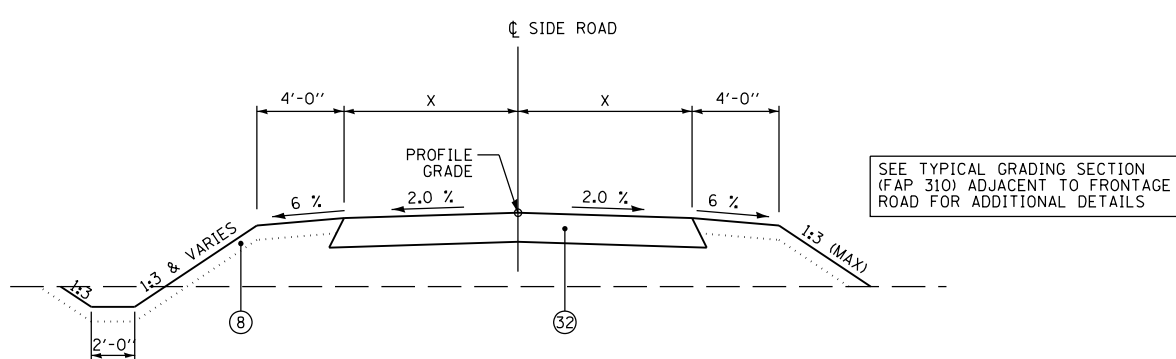
TYPICAL SECTION (MS 6010 / ASH ST)

STA 4+00.00 TO STA 8+73.10
STA 8+73.10 TO STA 9+17.51 RT



TYPICAL SECTION (MS 6010 / ASH ST)

STA 8+73.10 TO STA 9+17.51 LT
STA 9+17.51 TO STA 9+87.60



TYPICAL SECTION (AGGREGATE SIDE ROADS)

STA 100+11.55 TO STA 103+87.97 NORMAL CROWN PRIVATE DRIVE 81A X = VARIES 8'-0" TO 15'-0"
STA 610+59.0 TO STA 614+34.69 NORMAL CROWN SERVICE DRIVE 93 X = 8'-0"

EXISTING LEGEND

(E18) EXISTING HOT-MIX-ASPHALT PAVEMENT, VARIABLE DEPTH

PROPOSED LEGEND

- (7) AGGREGATE SHOULDERS, TYPE B
- (8) TOPSOIL FURNISH AND PLACE, 4"
- (15) HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 1 1/2"
- (16) LEVELING BINDER (MACHINE METHOD), N50, 3/4"
- (19) HOT-MIX ASPHALT SHOULDERS, 8"
- (22) HOT-MIX ASPHALT BASE COURSE, 9"
- (23) HOT-MIX ASPHALT BASE COURSE WIDENING, 9" (WHEN WIDTH < 6')
- (32) AGGREGATE SURFACE COURSE, TYPE A 8"

NOTE: NOT TO SCALE

REVISIONS	
NAME	DATE

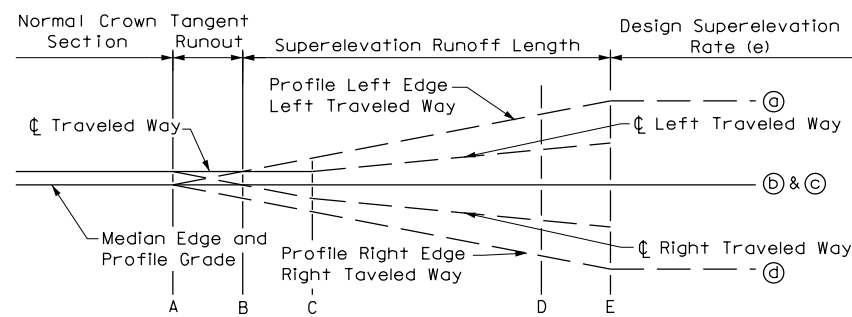
ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
FAP 310 (US 67/IL 104)
SHEET 24 OF 24

DRAWN BY SEB
CHECKED BY
DATE 4/2009

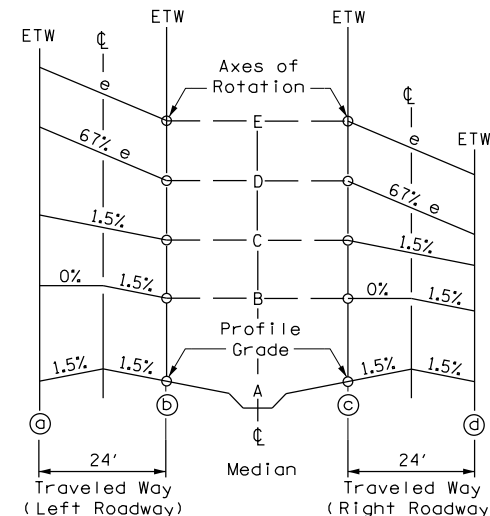
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	83
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

EXISTING CURVE 13 (FOR INFORMATION ONLY)											
SECTION	STATION	OUTSIDE EDGE	SLOPE %	CL	SLOPE %	MEDIAN EDGE	MEDIAN EDGE	SLOPE %	CL	SLOPE %	OUTSIDE EDGE
A	768+19.00	611.19	-1.50	611.37	1.50	611.19	611.19	1.50	611.37	-1.50	611.19
	768+25.00	611.22	-1.32	611.38	1.50	611.20	611.20	1.32	611.36	-1.50	611.18
	768+50.00	611.36	-0.57	611.43	1.50	611.25	611.25	0.57	611.32	-1.50	611.14
B	768+69.00	611.47	0.00	611.47	1.50	611.29	611.29	0.00	611.29	-1.50	611.11
	768+75.00	611.49	0.09	611.48	1.50	611.30	611.30	-0.09	611.29	-1.50	611.11
	769+00.00	611.59	0.47	611.53	1.50	611.35	611.35	-0.47	611.29	-1.50	611.11
	769+25.00	611.68	0.86	611.58	1.50	611.40	611.40	-0.86	611.30	-1.50	611.12
	769+50.00	611.78	1.24	611.63	1.50	611.45	611.45	-1.24	611.30	-1.50	611.12
C	769+67.00	611.84	1.50	611.66	1.50	611.48	611.48	-1.50	611.30	-1.50	611.12
	769+75.00	611.89	1.62	611.69	1.62	611.50	611.50	-1.62	611.31	-1.62	611.11
	770+00.00	612.03	2.00	611.79	2.00	611.55	611.55	-2.00	611.31	-2.00	611.07
D	770+13.00	612.11	2.21	611.84	2.21	611.58	611.58	-2.21	611.31	-2.21	611.05
	770+25.00	612.17	2.39	611.89	2.39	611.60	611.60	-2.39	611.31	-2.39	611.03
	770+50.00	612.32	2.78	611.98	2.78	611.65	611.65	-2.78	611.32	-2.78	610.98
	770+75.00	612.46	3.16	612.08	3.16	611.70	611.70	-3.16	611.32	-3.16	610.94
E	770+84.00	612.51	3.30	612.11	3.30	611.72	611.72	-3.30	611.32	-3.30	610.93
MAXIMUM SUPERELEVATION											
E	779+71.00	614.28	3.30	613.89	3.30	613.49	613.49	-3.30	613.10	-3.30	612.70
	779+75.00	614.28	3.24	613.89	3.24	613.50	613.50	-3.24	613.11	-3.24	612.72
	780+00.00	614.23	2.85	613.89	2.85	613.55	613.55	-2.85	613.21	-2.85	612.87
	780+25.00	614.19	2.47	613.90	2.47	613.60	613.60	-2.47	613.30	-2.47	613.01
D	780+42.00	614.16	2.21	613.90	2.21	613.63	613.63	-2.21	613.37	-2.21	613.10
	780+50.00	614.15	2.08	613.90	2.08	613.65	613.65	-2.08	613.40	-2.08	613.15
	780+75.00	614.11	1.70	613.90	1.70	613.70	613.70	-1.70	613.50	-1.70	613.29
	780+88.00	614.09	1.50	613.91	1.50	613.73	613.73	-1.50	613.55	-1.50	613.37
	781+00.00	614.09	1.32	613.93	1.50	613.75	613.75	-1.32	613.59	-1.50	613.41
C	781+25.00	614.09	0.93	613.98	1.50	613.80	613.80	-0.93	613.69	-1.50	613.51
	781+50.00	614.10	0.55	614.03	1.50	613.85	613.85	-0.55	613.78	-1.50	613.60
	781+75.00	614.10	0.17	614.08	1.50	613.90	613.90	-0.17	613.88	-1.50	613.70
B	781+86.00	614.10	0.00	614.10	1.50	613.92	613.92	0.00	613.92	-1.50	613.74
	782+00.00	614.08	-0.42	614.13	1.50	613.95	613.95	0.42	614.00	-1.50	613.82
	782+25.00	614.04	-1.17	614.18	1.50	614.00	614.00	1.17	614.14	-1.50	613.96
A	782+36.00	614.02	-1.50	614.20	1.50	614.02	614.02	1.50	614.20	-1.50	614.02

CURVE MLE15BR2											
SECTION	STATION	OUTSIDE EDGE	SLOPE %	CL	SLOPE %	MEDIAN EDGE	MEDIAN EDGE	SLOPE %	CL	SLOPE %	OUTSIDE EDGE
A	892+24.00	619.82	-1.50	620.00	1.50	619.82	619.82	1.50	620.00	-1.50	619.82
	892+25.00	619.82	-1.50	620.00	1.47	619.83	619.83	1.50	620.01	-1.47	619.83
	892+50.00	619.80	-1.50	619.98	0.63	619.90	619.90	1.50	620.08	-0.63	620.00
B	892+69.00	619.78	-1.50	619.96	0.00	619.96	619.96	1.50	620.14	0.00	620.14
	892+75.00	619.77	-1.50	619.95	-0.20	619.98	619.98	1.50	620.16	0.20	620.18
	893+00.00	619.75	-1.50	619.93	-1.04	620.05	620.05	1.50	620.23	1.04	620.35
C	893+13.68	619.73	-1.50	619.91	-1.50	620.09	620.09	1.50	620.27	1.50	620.45
	893+25.00	619.72	-1.69	619.92	-1.69	620.13	620.13	1.69	620.33	1.69	620.53
D	893+47.44	619.70	-2.07	619.94	-2.07	620.19	620.19	2.07	620.44	2.07	620.69
	893+50.00	619.69	-2.11	619.95	-2.11	620.20	620.20	2.11	620.45	2.11	620.71
	893+75.00	619.67	-2.53	619.97	-2.53	620.28	620.28	2.53	620.58	2.53	620.88
	894+00.00	619.64	-2.95	620.00	-2.95	620.35	620.35	2.95	620.70	2.95	621.06
E	894+09.00	619.63	-3.10	620.01	-3.10	620.38	620.38	3.10	620.75	3.10	621.12
MAXIMUM SUPERELEVATION											
E	912+06.00	624.94	-3.10	625.31	-3.10	625.68	625.68	3.10	626.06	3.10	626.43
	912+25.00	625.04	-2.78	625.37	-2.78	625.71	625.71	2.78	626.04	2.78	626.38
	912+50.00	625.16	-2.36	625.45	-2.36	625.73	625.73	2.36	626.01	2.36	626.30
D	912+67.56	625.25	-2.07	625.49	-2.07	625.74	625.74	2.07	625.99	2.07	626.24
	912+75.00	625.28	-1.94	625.51	-1.94	625.75	625.75	1.94	625.98	1.94	626.21
	913+00.00	625.38	-1.52	625.57	-1.52	625.75	625.75	1.52	625.93	1.52	626.12
C	913+01.32	625.39	-1.50	625.57	-1.50	625.75	625.75	1.50	625.93	1.50	626.11
	913+25.00	625.48	-1.50	625.66	-0.71	625.75	625.75	1.50	625.93	0.71	626.01
B	913+46.00	625.55	-1.50	625.73	0.00	625.73	625.73	1.50	625.91	0.00	625.91
	913+50.00	625.57	-1.50	625.75	0.13	625.73	625.73	1.50	625.91	-0.13	625.90
	913+75.00	625.64	-1.50	625.82	0.97	625.71	625.71	1.50	625.89	-0.97	625.77
A	913+91.00	625.69	-1.50	625.87	1.50	625.69	625.69	1.50	625.87	-1.50	625.69



AXIS OF ROTATION ABOUT MEDIAN EDGES OF TRAVELED WAY



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SUPERELEVATION RUNOFF TABLES
 SHEET 1 OF 6
 MAINLINE
 FAP 310 (US 67/IL 104)

DATE 4/06 DRAWN BY EBB CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	84
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

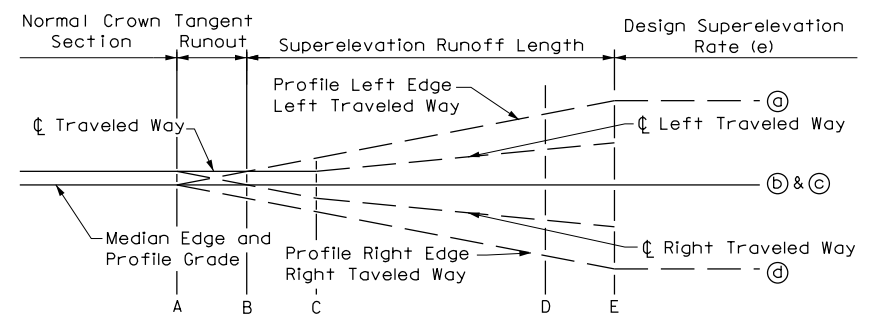
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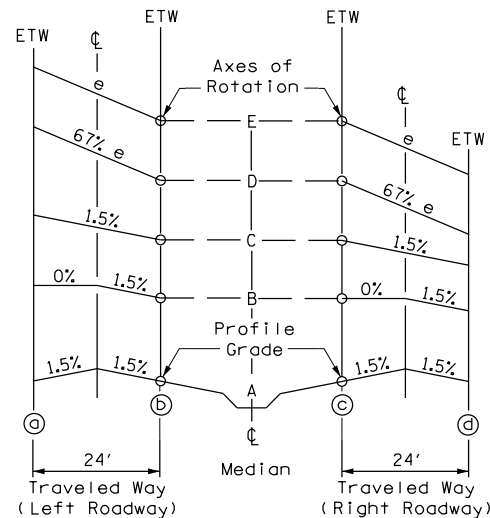
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CURVE MLE16R2											
SECTION	STATION	OUTSIDE EDGE	SLOPE %	CL	SLOPE %	MEDIAN EDGE	MEDIAN EDGE	SLOPE %	CL	SLOPE %	OUTSIDE EDGE
A	925+31.00	626.01	-1.50	626.19	1.50	626.01	626.01	1.50	626.19	-1.50	626.01
	925+50.00	626.29	-0.77	626.38	1.50	626.20	626.20	0.77	626.29	-1.50	626.11
B	925+70.00	626.57	0.00	626.57	1.50	626.39	626.39	0.00	626.39	-1.50	626.21
	925+75.00	626.64	0.19	626.62	1.50	626.44	626.44	-0.19	626.42	-1.50	626.24
	926+00.00	626.98	1.15	626.84	1.50	626.66	626.66	-1.15	626.52	-1.50	626.34
C	926+09.19	627.09	1.50	626.92	1.50	626.74	626.74	-1.50	626.56	-1.50	626.38
	926+25.00	627.29	1.80	627.08	1.80	626.86	626.86	-1.80	626.64	-1.80	626.43
	926+50.00	627.59	2.28	627.31	2.28	627.04	627.04	-2.28	626.76	-2.28	626.49
	926+75.00	627.86	2.76	627.53	2.76	627.20	627.20	-2.76	626.86	-2.76	626.53
	927+00.00	628.11	3.24	627.72	3.24	627.33	627.33	-3.24	626.95	-3.24	626.56
D	927+18.94	628.29	3.60	627.86	3.60	627.43	627.43	-3.60	626.99	-3.60	626.56
	927+25.00	628.34	3.72	627.90	3.72	627.45	627.45	-3.72	627.01	-3.72	626.56
	927+50.00	628.56	4.19	628.05	4.19	627.55	627.55	-4.19	627.05	-4.19	626.54
	927+75.00	628.75	4.67	628.19	4.67	627.63	627.63	-4.67	627.07	-4.67	626.51
	928+00.00	628.92	5.15	628.30	5.15	627.68	627.68	-5.15	627.07	-5.15	626.45
E	928+13.00	629.00	5.40	628.35	5.40	627.71	627.71	-5.40	627.06	-5.40	626.41
MAXIMUM SUPERELEVATION											
E	945+67.00	635.94	5.40	635.30	5.40	634.65	634.65	-5.40	634.00	-5.40	633.35
	945+75.00	635.93	5.25	635.30	5.25	634.67	634.67	-5.25	634.04	-5.25	633.41
	946+00.00	635.89	4.77	635.32	4.77	634.75	634.75	-4.77	634.18	-4.77	633.61
	946+25.00	635.85	4.29	635.34	4.29	634.83	634.83	-4.29	634.31	-4.29	633.80
	946+50.00	635.81	3.81	635.36	3.81	634.90	634.90	-3.81	634.44	-3.81	633.99
D	946+61.06	635.80	3.60	635.37	3.60	634.93	634.93	-3.60	634.50	-3.60	634.07
	946+75.00	635.78	3.33	635.38	3.33	634.98	634.98	-3.33	634.58	-3.33	634.18
	947+00.00	635.74	2.85	635.39	2.85	635.05	635.05	-2.85	634.71	-2.85	634.36
	947+25.00	635.70	2.38	635.41	2.38	635.13	635.13	-2.38	634.84	-2.38	634.55
	947+50.00	635.66	1.90	635.43	1.90	635.20	635.20	-1.90	634.97	-1.90	634.74
C	947+70.81	635.62	1.50	635.44	1.50	635.26	635.26	-1.50	635.08	-1.50	634.90
	947+75.00	635.62	1.34	635.46	1.50	635.28	635.28	-1.34	635.11	-1.50	634.93
	948+00.00	635.58	0.38	635.53	1.50	635.35	635.35	-0.38	635.30	-1.50	635.12
B	948+10.00	635.56	0.00	635.56	1.50	635.38	635.38	0.00	635.38	-1.50	635.20
	948+25.00	635.54	-0.58	635.61	1.50	635.43	635.43	0.58	635.49	-1.50	635.31
A	948+49.00	635.50	-1.50	635.68	1.50	635.50	635.50	1.50	635.68	-1.50	635.50

CURVE MLE17											
SECTION	STATION	OUTSIDE EDGE	SLOPE %	CL	SLOPE %	MEDIAN EDGE	MEDIAN EDGE	SLOPE %	CL	SLOPE %	OUTSIDE EDGE
A	968+87.00	629.05	-1.50	629.23	1.50	629.05	629.05	1.50	629.23	-1.50	629.05
	969+00.00	628.99	-1.21	629.14	1.50	628.96	628.96	1.21	629.10	-1.50	628.92
	969+25.00	628.88	-0.66	628.96	1.50	628.78	628.78	0.66	628.86	-1.50	628.68
	969+50.00	628.80	-0.11	628.81	1.50	628.63	628.63	0.11	628.65	-1.50	628.47
B	969+55.00	628.78	0.00	628.78	1.50	628.60	628.60	0.00	628.60	-1.50	628.42
	969+75.00	628.73	0.44	628.68	1.50	628.50	628.50	-0.44	628.45	-1.50	628.27
	970+00.00	628.69	0.99	628.57	1.50	628.39	628.39	-0.99	628.27	-1.50	628.09
C	970+23.00	628.66	1.50	628.48	1.50	628.30	628.30	-1.50	628.12	-1.50	627.94
MAXIMUM SUPERELEVATION											
C	988+49.00	634.16	1.50	633.98	1.50	633.80	633.80	-1.50	633.62	-1.50	633.44
	988+50.00	634.16	1.48	633.98	1.50	633.80	633.80	-1.48	633.62	-1.50	633.44
	988+75.00	633.99	0.93	633.88	1.50	633.70	633.70	-0.93	633.59	-1.50	633.41
	989+00.00	633.83	0.37	633.78	1.50	633.60	633.60	-0.37	633.56	-1.50	633.38
B	989+17.00	633.71	0.00	633.71	1.50	633.53	633.53	0.00	633.53	-1.50	633.35
	989+25.00	633.66	-0.18	633.68	1.50	633.50	633.50	0.18	633.52	-1.50	633.34
	989+50.00	633.49	-0.73	633.58	1.50	633.40	633.40	0.73	633.49	-1.50	633.31
	989+75.00	633.33	-1.28	633.48	1.50	633.30	633.30	1.28	633.45	-1.50	633.27
A	989+85.00	633.26	-1.50	633.44	1.50	633.26	633.26	1.50	633.44	-1.50	633.26



AXIS OF ROTATION ABOUT MEDIAN EDGES OF TRAVELED WAY



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SUPERELEVATION RUNOFF TABLES
 SHEET 2 OF 6
 MAINLINE
 FAP 310 (US 67/IL 104)

DATE 4/06
 DRAWN BY EBB
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	85
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

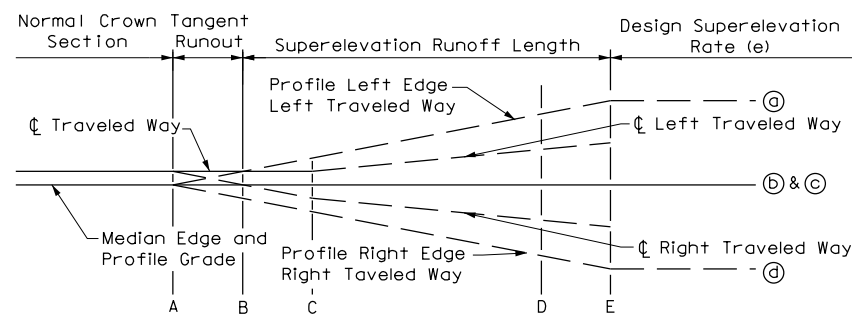
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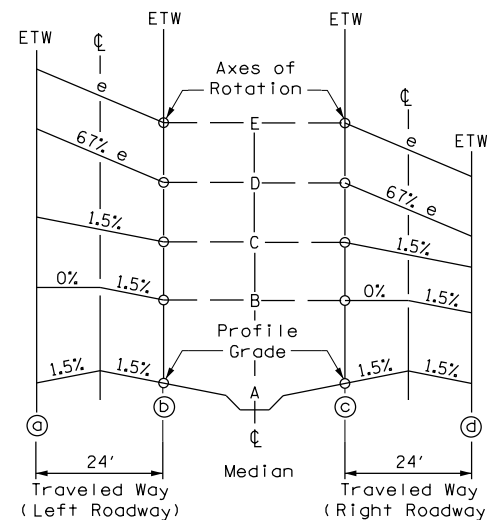
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CURVE MLE18											
SECTION	STATION	OUTSIDE EDGE	SLOPE %	CL	SLOPE %	MEDIAN EDGE	MEDIAN EDGE	SLOPE %	CL	SLOPE %	OUTSIDE EDGE
A	1021+20.00	634.86	-1.50	635.04	1.50	634.86	634.86	1.50	635.04	-1.50	634.86
	1021+25.00	634.87	-1.50	635.05	1.33	634.89	634.89	1.50	635.07	-1.33	634.91
	1021+50.00	634.90	-1.50	635.08	0.48	635.03	635.03	1.50	635.21	-0.48	635.15
B	1021+64.00	634.92	-1.50	635.10	0.00	635.10	635.10	1.50	635.28	0.00	635.28
	1021+75.00	634.94	-1.50	635.12	-0.38	635.16	635.16	1.50	635.34	0.38	635.39
	1022+00.00	634.97	-1.50	635.15	-1.23	635.30	635.30	1.50	635.48	1.23	635.63
C	1022+07.82	634.98	-1.50	635.16	-1.50	635.34	635.34	1.50	635.52	1.50	635.70
	1022+25.00	635.01	-1.79	635.22	-1.79	635.44	635.44	1.79	635.65	1.79	635.87
D	1022+48.73	635.04	-2.20	635.30	-2.20	635.57	635.57	2.20	635.83	2.20	636.10
	1022+50.00	635.04	-2.22	635.31	-2.22	635.58	635.58	2.22	635.84	2.22	636.11
	1022+75.00	635.08	-2.65	635.39	-2.65	635.71	635.71	2.65	636.03	2.65	636.35
	1023+00.00	635.11	-3.08	635.48	-3.08	635.85	635.85	3.08	636.22	3.08	636.59
E	1023+13.00	635.13	-3.30	635.53	-3.30	635.92	635.92	3.30	636.32	3.30	636.71
MAXIMUM SUPERELEVATION											
E	1039+07.00	639.13	-3.30	639.53	-3.30	639.92	639.92	3.30	640.32	3.30	640.71
	1039+25.00	639.26	-2.99	639.62	-2.99	639.98	639.98	2.99	640.33	2.99	640.69
	1039+50.00	639.43	-2.56	639.74	-2.56	640.05	640.05	2.56	640.36	2.56	640.67
D	1039+71.27	639.59	-2.20	639.85	-2.20	640.11	640.11	2.20	640.38	2.20	640.64
	1039+75.00	639.61	-2.14	639.87	-2.14	640.13	640.13	2.14	640.38	2.14	640.64
	1040+00.00	639.79	-1.71	639.99	-1.71	640.20	640.20	1.71	640.41	1.71	640.61
C	1040+12.18	639.88	-1.50	640.06	-1.50	640.24	640.24	1.50	640.42	1.50	640.60
	1040+25.00	639.97	-1.50	640.15	-1.06	640.28	640.28	1.50	640.46	1.06	640.58
	1040+50.00	640.15	-1.50	640.33	-0.21	640.35	640.35	1.50	640.53	0.21	640.55
B	1040+56.00	640.19	-1.50	640.37	0.00	640.37	640.37	1.50	640.55	0.00	640.55
	1040+75.00	640.32	-1.50	640.50	0.65	640.43	640.43	1.50	640.61	-0.65	640.53
A	1041+00.00	640.50	-1.50	640.68	1.50	640.50	640.50	1.50	640.68	-1.50	640.50

CURVE MLE19											
SECTION	STATION	OUTSIDE EDGE	SLOPE %	CL	SLOPE %	MEDIAN EDGE	MEDIAN EDGE	SLOPE %	CL	SLOPE %	OUTSIDE EDGE
A	1088+44.00	589.47	-1.50	589.65	1.50	589.47	589.47	1.50	589.65	-1.50	589.47
	1088+50.00	589.38	-1.50	589.56	1.32	589.40	589.40	1.50	589.58	-1.32	589.42
	1088+75.00	588.99	-1.50	589.17	0.55	589.10	589.10	1.50	589.28	-0.55	589.22
B	1088+93.00	588.73	-1.50	588.91	0.00	588.91	588.91	1.50	589.09	0.00	589.09
	1089+00.00	588.63	-1.50	588.81	-0.22	588.83	588.83	1.50	589.01	0.22	589.04
	1089+25.00	588.29	-1.50	588.47	-0.99	588.59	588.59	1.50	588.77	0.99	588.89
C	1089+41.43	588.08	-1.50	588.26	-1.50	588.44	588.44	1.50	588.62	1.50	588.80
	1089+50.00	587.97	-1.63	588.17	-1.63	588.37	588.37	1.63	588.56	1.63	588.76
D	1089+52.19	587.95	-1.67	588.15	-1.67	588.35	588.35	1.67	588.55	1.67	588.75
	1089+75.00	587.68	-2.02	587.93	-2.02	588.17	588.17	2.02	588.41	2.02	588.65
	1090+00.00	587.42	-2.41	587.71	-2.41	588.00	588.00	2.41	588.28	2.41	588.57
E	1090+06.00	587.36	-2.50	587.66	-2.50	587.96	587.96	2.50	588.26	2.50	588.56
MAXIMUM SUPERELEVATION											
E	1118+80.00	613.22	-2.50	613.52	-2.50	613.82	613.82	2.50	614.12	2.50	614.42
	1119+00.00	613.22	-2.19	613.49	-2.19	613.75	613.75	2.19	614.01	2.19	614.28
	1119+25.00	613.23	-1.80	613.45	-1.80	613.66	613.66	1.80	613.88	1.80	614.10
D	1119+33.81	613.23	-1.67	613.43	-1.67	613.63	613.63	1.67	613.83	1.67	614.03
C	1119+44.57	613.23	-1.50	613.41	-1.50	613.59	613.59	1.50	613.77	1.50	613.95
	1119+50.00	613.24	-1.50	613.42	-1.33	613.58	613.58	1.50	613.76	1.33	613.91
	1119+75.00	613.24	-1.50	613.42	-0.56	613.49	613.49	1.50	613.67	0.56	613.73
B	1119+93.00	613.25	-1.50	613.43	0.00	613.43	613.43	1.50	613.61	0.00	613.61
	1120+00.00	613.25	-1.50	613.43	0.21	613.40	613.40	1.50	613.58	-0.21	613.55
	1120+25.00	613.25	-1.50	613.43	0.98	613.31	613.31	1.50	613.49	-0.98	613.38
A	1120+42.00	613.25	-1.50	613.43	1.50	613.25	613.25	1.50	613.43	-1.50	613.25



AXIS OF ROTATION ABOUT MEDIAN EDGES OF TRAVELED WAY



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUPERELEVATION RUNOFF TABLES
 SHEET 3 OF 6
 MAINLINE
 FAP 310 (US 67/IL 104)
 DRAWN BY EBB
 CHECKED BY
 DATE 4/06

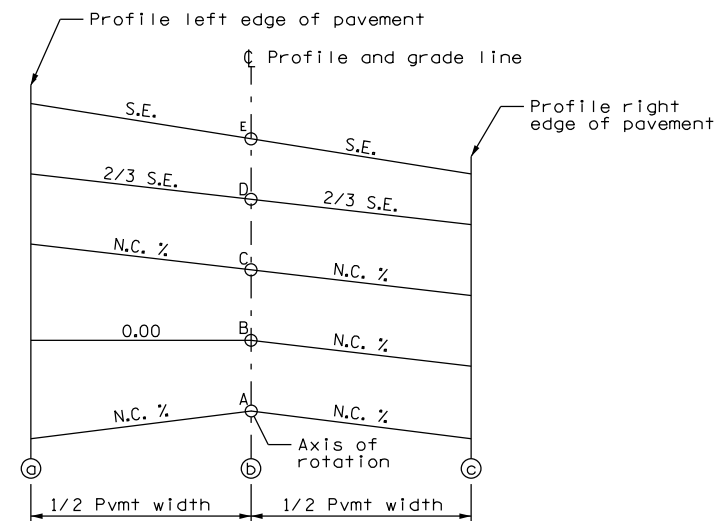
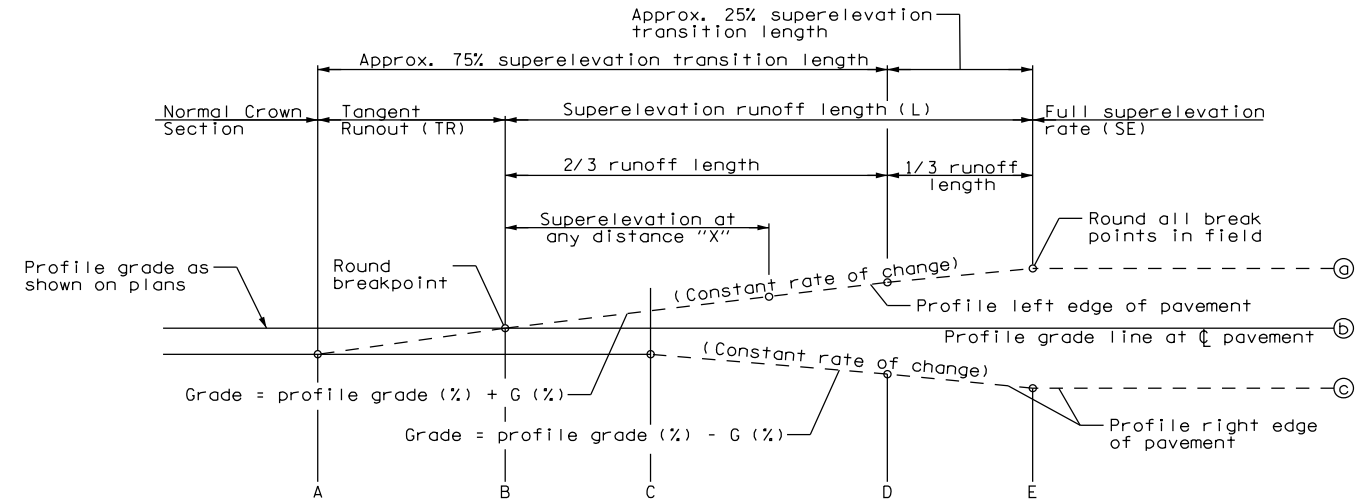
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	86
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

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SUPERELEVATION TRANSITION CURVE DATA

LOCATION	CURVE NAME	N.C.	S.E.	PROFILE BREAK POINTS				
				A	B	C	D	E
US 67 FRONTAGE ROAD (CONCORD)	CON-FR-1	-1.50%	±8.00%	7+97.00	8+31.00	8+64.38	9+49.67	10+09.00
US 67 FRONTAGE ROAD (CONCORD)	CON-FR-1	0.00%	±8.00%	N/A	N/A	18+93.00	17+74.33	17+15.00
US 67 FRONTAGE ROAD (CONCORD)	CON-FR-2	0.00%	±8.00%	N/A	N/A	18+93.00	20+11.67	20+71.00
US 67 FRONTAGE ROAD (CONCORD)	CON-FR-2	-1.50%	±8.00%	29+88.00	29+54.00	29+20.63	28+35.33	27+76.00
US 67 FRONTAGE ROAD (CONCORD)	CON-FR-3	-1.50%	±8.00%	30+12.00	30+46.00	30+79.38	31+64.67	32+24.00
US 67 FRONTAGE ROAD (CONCORD)	CON-FR-3	0.00%	±8.00%	N/A	N/A	41+63.00	40+44.33	39+85.00
US 67 FRONTAGE ROAD (CONCORD)	CON-FR-4	0.00%	±8.00%	N/A	N/A	41+63.00	42+81.67	43+41.00
US 67 FRONTAGE ROAD (CONCORD)	CON-FR-4	-1.50%	±8.00%	53+05.00	52+71.00	52+37.63	51+52.33	50+93.00
TEMPORARY ACCESS ROAD	RUNARD1-3	-2.00%	±8.00%	6+41.00	6+82.00	7+22.75	7+90.67	8+45.00
TEMPORARY ACCESS ROAD	RUNARD1-3	-2.00%	±8.00%	15+69.00	15+28.00	14+87.25	14+19.33	13+65.00
TEMPORARY ACCESS ROAD	RUNARD1-4	-2.00%	±8.00%	16+81.00	17+22.00	17+62.75	18+30.67	18+85.00
TEMPORARY ACCESS ROAD	RUNARD1-4	-2.00%	±8.00%	22+94.00	22+53.00	22+12.25	21+44.33	20+90.00
TEMPORARY ACCESS ROAD	RUNARD1-5	-2.00%	±2.00%	26+07.00	26+37.00	26+67.00	N/A	26+67.00
TEMPORARY ACCESS ROAD	RUNARD1-5	±0.20%	±2.00%	N/A	N/A	28+04.01	N/A	27+77.01
RELOCATED CREWS LANE	C141	-2.00%	±6.00%	50+24.00	50+67.00	51+09.67	51+52.33	51+95.00
RELOCATED CREWS LANE	C141	-2.00%	±6.00%	56+53.00	56+10.00	55+67.33	55+24.67	54+82.00
US 67 FRONTAGE ROAD (BETHEL)	C142	-1.50%	±6.00%	513+83.00	514+22.00	514+60.50	515+24.67	515+76.00
US 67 FRONTAGE ROAD (BETHEL)	C142	-1.50%	±6.00%	522+08.00	521+69.00	521+30.50	520+66.33	520+15.00
US 67 FRONTAGE ROAD (BETHEL)	709	-1.50%	±6.20%	522+70.00	523+09.00	523+47.47	524+15.00	524+68.00
BETHEL LANE	C61	-2.00%	±4.00%	74+43.00	74+85.00	75+26.50	75+40.33	75+68.00
BETHEL LANE	C61	-2.00%	±4.00%	78+16.00	77+74.00	77+32.50	77+18.67	76+91.00
BETHEL LANE	C200	-2.00%	±4.00%	81+30.00	81+67.00	82+03.50	82+15.67	82+40.00
BETHEL LANE	C200	0.00%	±4.00%	N/A	N/A	85+24.00	84+75.33	84+51.00
BETHEL LANE	C201	0.00%	±4.00%	N/A	N/A	85+24.00	85+72.67	85+97.00
BETHEL LANE	C201	-2.00%	±4.00%	88+73.00	88+36.00	87+99.50	87+87.33	87+63.00
RELOCATED CYLINDER HEAD ROAD	CYLHD1	-2.00%	±6.00%	93+83.00	94+24.00	94+65.00	95+06.00	95+47.00
RELOCATED CYLINDER HEAD ROAD	CYLHD1	-2.00%	±6.00%	101+94.00	101+53.00	101+12.00	100+71.00	100+30.00
RELOCATED CYLINDER HEAD ROAD	CYLHD2	-2.00%	±6.00%	102+03.00	102+44.00	102+85.00	103+26.00	103+67.00
RELOCATED CYLINDER HEAD ROAD	CYLHD2	-2.00%	±6.00%	109+28.00	108+92.25	108+56.50	108+20.75	107+85.00
ST. PAUL'S CHURCH ROAD	C56	-2.00%	±4.00%	121+97.00	122+35.00	122+73.00	122+85.67	123+11.00
ST. PAUL'S CHURCH ROAD	C56	-2.00%	±4.00%	127+83.00	127+45.00	127+07.00	126+94.33	126+69.00
DUTCH LANE EAST	C40	-2.00%	±6.00%	138+04.00	138+40.00	138+75.67	139+11.33	139+47.00
DUTCH LANE EAST	C40	-2.00%	±6.00%	143+40.00	143+04.00	142+68.33	142+32.67	141+97.00
DUTCH LANE EAST	C41	-2.00%	±4.00%	143+60.00	143+92.00	144+23.50	144+34.00	144+55.00
DUTCH LANE EAST	C41	-2.00%	±4.00%	148+34.00	148+02.00	147+70.50	147+60.00	147+39.00
DUTCH LANE WEST	C13	-2.00%	±4.00%	194+91.00	195+32.00	195+73.00	195+86.67	196+14.00
DUTCH LANE WEST	C13	-2.00%	±4.00%	198+07.00	197+66.00	197+25.00	197+11.33	196+84.00
DUTCH LANE WEST	C11	-2.00%	±4.00%	202+01.00	202+39.00	202+77.00	202+89.67	203+15.00
DUTCH LANE WEST	C11	-2.00%	±4.00%	206+35.00	205+97.00	205+59.00	205+46.33	205+21.00
DUTCH LANE WEST	C12	-2.00%	±6.00%	206+37.00	206+78.00	207+19.00	207+60.00	208+01.00
DUTCH LANE WEST	C12	-2.00%	±6.00%	210+80.00	210+39.00	209+98.00	209+57.00	209+16.00
HARTS GRAVEL ROAD	HRT720	0.00%	±6.00%	N/A	N/A	290+90.00	290+08.00	289+67.00
HARTS GRAVEL ROAD	C22	0.00%	±6.00%	N/A	N/A	290+90.00	291+72.00	292+13.00
HARTS GRAVEL ROAD	C22	-2.00%	±6.00%	294+42.00	294+01.00	293+60.00	293+19.00	292+78.00
HARTS GRAVEL ROAD	C25	-2.00%	±4.00%	294+43.00	294+81.00	295+19.00	295+31.67	295+57.00
HARTS GRAVEL ROAD	C25	-2.00%	±4.00%	298+11.00	297+73.00	297+35.00	297+22.33	296+97.00



AXIS OF ROTATION ABOUT CENTERLINE OF TRAVELED WAY - TWO LANE ROADS

REVISIONS	
NAME	DATE

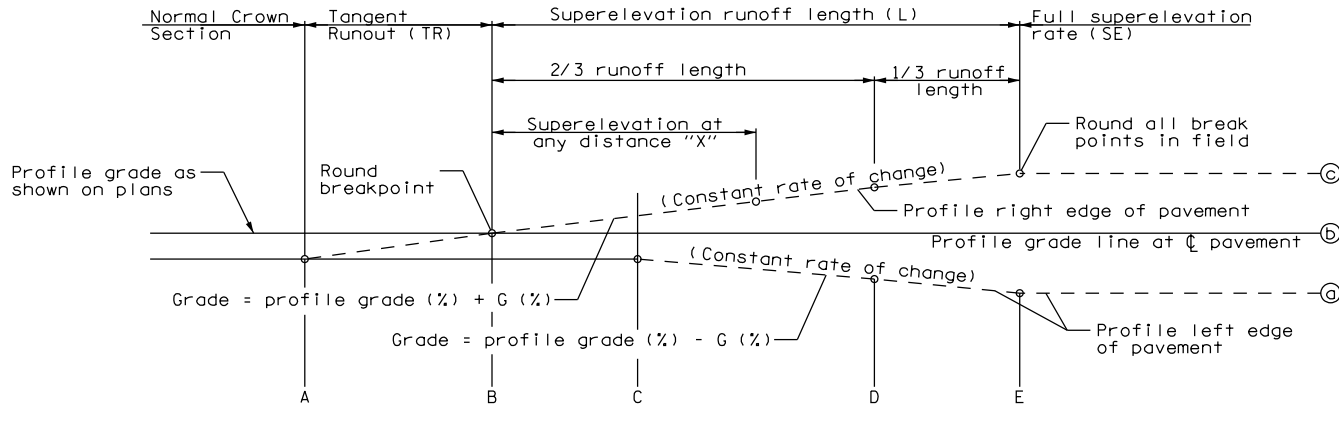
ILLINOIS DEPARTMENT OF TRANSPORTATION
 SUPERELEVATION RUNOFF TABLES
 SHEET 4 OF 6
 SIDEROADS & TRANSITIONS
 FAP 310 (US 67/IL 104)

DRAWN BY EBB
 CHECKED BY
 DATE 4/06

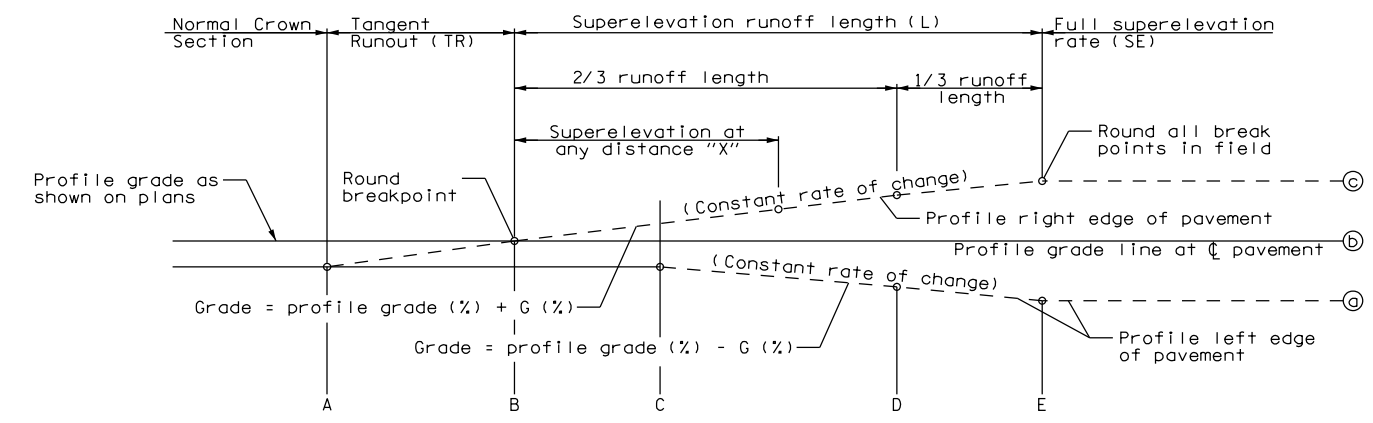
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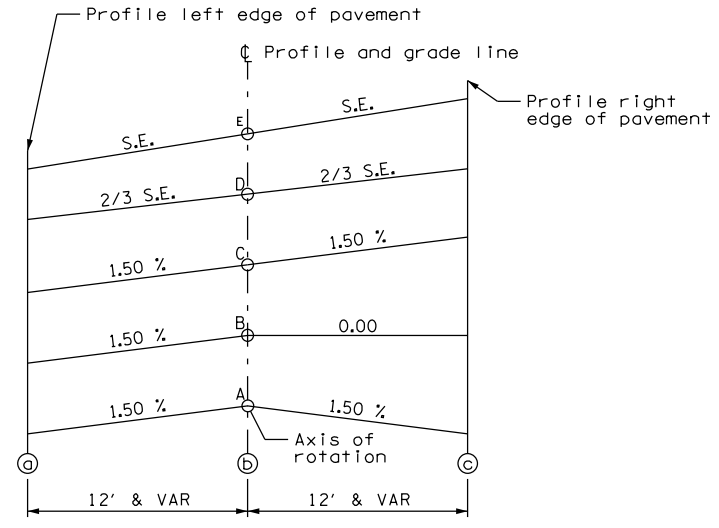
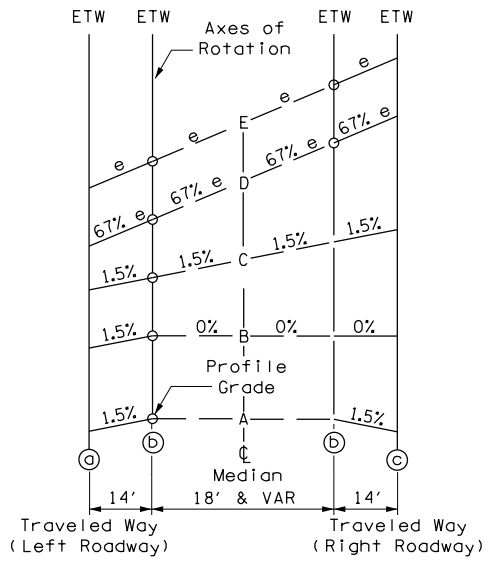
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	87
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



AXIS OF ROTATION ABOUT MEDIAN EDGES OF TRAVELED WAY



AXIS OF ROTATION ABOUT MEDIAN EDGES OF TRAVELED WAY



AXIS OF ROTATION ABOUT CENTERLINE OF TRAVELED WAY

SUPERELEVATION TRANSITION CURVE DATA								
LOCATION	CURVE NAME	N.C.	S.E.	PROFILE BREAK POINTS				
				A	B	C	D	E
CONCORD ROAD	CONCORD-1	-1.50%	±4.70%	25+75.00	26+52.00	27+28.60	28+12.00	28+92.00

SUPERELEVATION TRANSITION CURVE DATA								
LOCATION	CURVE NAME	N.C.	S.E.	PROFILE BREAK POINTS				
				A	B	C	D	E
CONCORD ROAD	CONCORD-1	-1.50%	±4.70%	34+84.00	34+37.00	33+90.40	33+39.67	32+91.00

REVISIONS	
NAME	DATE

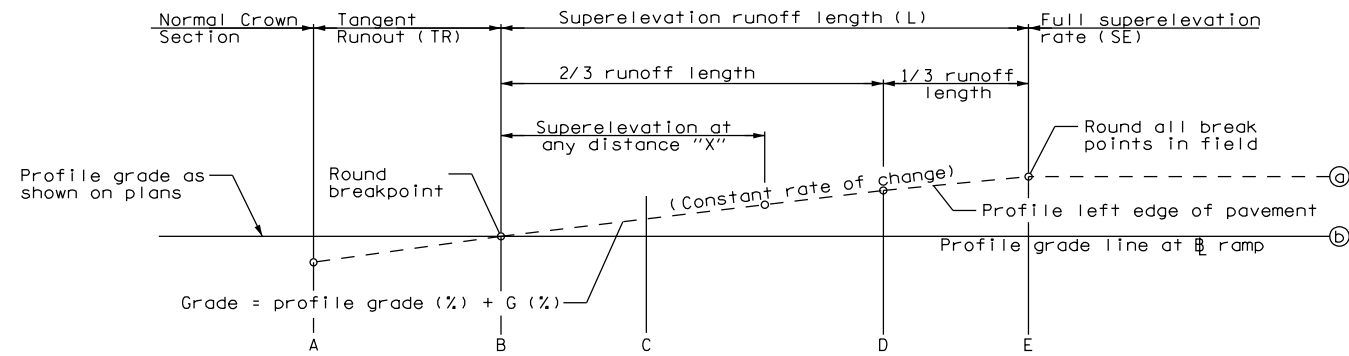
ILLINOIS DEPARTMENT OF TRANSPORTATION
SUPERELEVATION RUNOFF TABLES
 SHEET 5 OF 6
 CONCORD ROAD
 FAP 310 (US 67/IL 104)

DATE 4/06

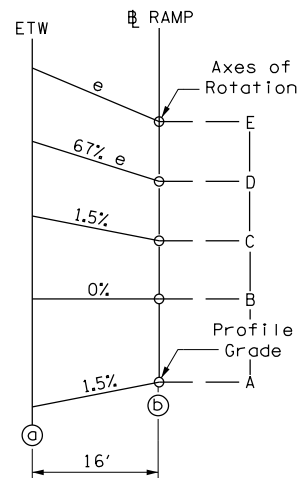
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	88
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



AXIS OF ROTATION ABOUT RAMP BASELINE



SUPERELEVATION TRANSITION CURVE DATA

LOCATION	CURVE NAME	N.C.	S.E.	PROFILE BREAK POINTS				
				A	B	C	D	E
RAMP A	CONA-1	-1.50%	-7.60%	N/A	N/A	1+59.00	2+73.60	3+55.00
RAMP A	CONA-1	1.50%	-7.60%	6+62.00	6+14.00	5+65.84	4+51.33	3+70.00
RAMP A	CONA-2	1.50%	8.00%	N/A	N/A	10+21.00	11+43.67	12+29.00
RAMP B	CONB-1	1.50%	8.00%	N/A	N/A	6+60.08	7+83.93	8+70.08
RAMP B	CONB-1	1.50%	8.00%	N/A	N/A	12+96.00	11+73.33	10+88.00
RAMP C	CONC1	1.50%	8.00%	N/A	N/A	7+11.00	8+33.67	9+19.00
RAMP D	COND-1	1.50%	8.00%	N/A	N/A	6+60.11	7+83.96	8+70.11
RAMP D	COND-1	-1.50%	8.00%	13+66.00	13+18.00	12+70.00	11+47.33	10+62.00
RAMP D	COND-2	-1.50%	-5.80%	N/A	N/A	15+69.00	16+34.50	16+88.00
RAMP D	COND-2	-1.50%	-5.80%	N/A	N/A	18+92.00	18+26.50	17+73.00

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SUPERELEVATION RUNOFF TABLES
 SHEET 6 OF 6
 RAMPS
 FAP 310 (US 67/IL 104)

DATE 5/06
 DRAWN BY EBB
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	89
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

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Mar-25-2011 03:58:05PM

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EXIST. EASTBOUND TRANSITION CURVE
 PI STA = 53+40.45
 $\Delta = 1^\circ 57' 02''$ LT
 $D = 0^\circ 17' 11''$
 $R = 20,000.00'$
 $T = 340.45'$
 $L = 680.84'$
 $E = 2.90'$
 $S.E. = \text{None}$
 P.C. STA = 50+00.00
 P.T. STA = 56+80.84

EXIST. CURVE TRANS001
 PI STA. = 4+14.04
 $\Delta = 4^\circ 44' 31''$ (LT)
 $D = 0^\circ 34' 23''$
 $R = 10,000.00'$
 $T = 414.04'$
 $L = 827.61'$
 $E = 8.57'$
 P.C. STA. = 0+00.00
 PRC STA. = 8+27.61

EXIST. CURVE TRANS002
 PI STA. = 10+61.61
 $\Delta = 7^\circ 08' 29''$ (RT)
 $D = 1^\circ 31' 40''$
 $R = 3,750.00'$
 $T = 234.00'$
 $L = 467.39'$
 $E = 7.29'$
 PRC STA. = 8+27.61
 P.T. STA. = 12+95.00

EXIST. CURVE TRANS003
 PI STA. = 24+75.77
 $\Delta = 7^\circ 53' 58''$ (RT)
 $D = 1^\circ 08' 45''$
 $R = 5,000.00'$
 $T = 345.23'$
 $L = 689.36'$
 $E = 11.90'$
 P.C. STA. = 21+30.54
 P.T. STA. = 28+19.90

EXIST. CURVE 307
 PI STA. = 100+03.06
 $\Delta = 10^\circ 08' 04''$ (LT)
 $D = 1^\circ 00' 18''$
 $R = 5,700.43'$
 $T = 505.47'$
 $L = 1,008.30'$
 $E = 22.37'$
 P.C. STA. = 94+97.59
 P.T. STA. = 105+05.89

EXIST. CURVE 13
 PI STA. = 775+28.87
 $\Delta = 10^\circ 17' 12''$ (RT)
 $D = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 515.72'$
 $L = 1,028.67'$
 $E = 23.16'$
 P.C. STA. = 770+13.15
 P.T. STA. = 780+41.82

PROP. CURVE CONB-1
 PI STA. = 9+82.02
 $\Delta = 26^\circ 55' 32''$ (RT)
 $D = 7^\circ 32' 20''$
 $R = 760.00'$
 $T = 181.94'$
 $L = 357.16'$
 $E = 21.47'$
 $e = 0.080$
 P.C. STA. = 8+00.08
 P.T. STA. = 11+57.24
 S.E. TRANSITION
 STA 10+88.00 TO STA 12+96.00
 STA 6+60.08 TO STA 8+70.08

PROP. CURVE CONCORD-1
 PI STA. = 30+76.49
 $\Delta = 11^\circ 23' 17''$ (LT)
 $D = 2^\circ 09' 44''$
 $R = 2,650.00'$
 $T = 264.23'$
 $L = 526.71'$
 $E = 13.14'$
 $e = 0.047$
 P.C. STA. = 28+12.26
 P.T. STA. = 33+38.97
 S.E. TRANSITION
 STA 26+67.00 TO STA 28+60.00
 STA 32+91.00 TO STA 34+84.00

PROP. CURVE CONA-1
 PI STA. = 3+69.83
 $\Delta = 9^\circ 13' 14''$ (LT)
 $D = 5^\circ 47' 15''$
 $R = 990.00'$
 $T = 79.83'$
 $L = 159.32'$
 $E = 3.21'$
 $e = 0.080$
 P.C. STA. = 2+90.00
 P.T. STA. = 4+49.32
 $e = 0.076$
 S.E. TRANSITION
 STA 1+59.00 TO STA 3+55.00
 STA 3+70.00 TO STA 6+62.00

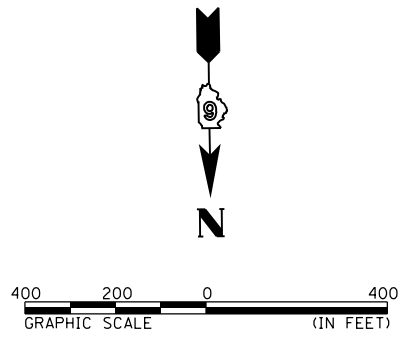
PROP. CURVE CONA-2
 PI STA. = 13+14.56
 $\Delta = 23^\circ 05' 31''$ (RT)
 $D = 7^\circ 32' 20''$
 $R = 760.00'$
 $T = 155.26'$
 $L = 306.30'$
 $E = 15.70'$
 $e = 0.080$
 P.C. STA. = 11+59.30
 P.T. STA. = 14+65.60
 S.E. TRANSITION
 STA 10+21.00 TO STA 12+29.00

PROP. CURVE CONFR-1
 PI STA. = 14+05.92
 $\Delta = 69^\circ 16' 52''$ (LT)
 $D = 8^\circ 40' 52''$
 $R = 660.00'$
 $T = 455.99'$
 $L = 798.06'$
 $E = 142.20'$
 $e = 0.080$
 P.C. STA. = 9+49.93
 P.T. STA. = 17+47.99
 S.E. TRANSITION
 STA 9+97.00 TO STA 10+09.00
 STA 17+15.00 TO STA 18+93.00

PROP. CURVE CONFR-2
 PI STA. = 24+93.02
 $\Delta = 69^\circ 15' 10''$ (RT)
 $D = 8^\circ 40' 52''$
 $R = 660.00'$
 $T = 455.75'$
 $L = 797.73'$
 $E = 142.07'$
 $e = 0.080$
 P.C. STA. = 20+37.27
 P.T. STA. = 28+35.00
 S.E. TRANSITION
 STA 18+93.00 TO STA 20+71.00
 STA 27+76.00 TO STA 29+88.00

DESCRIPTION	COORDINATES	
	NORTH	EAST
FAP 310		
PT 760+26.10	1,128,545.606	2,243,630.921
PC 770+13.15	1,128,581.287	2,242,644.512
PI 775+28.87	1,128,599.930	2,242,129.130
PT 780+41.82	1,128,710.307	2,241,625.361
POT 805+64.84	1,129,250.298	2,239,160.797
PR FRONTAGE ROAD		
POT 7+98.93	1,128,783.450	2,240,528.982
PC 9+49.93	1,128,815.795	2,240,381.487
PI 14+05.92	1,128,913.471	2,239,936.079
PT 17+47.99	1,128,531.426	2,239,687.141
PC 20+37.27	1,128,289.061	2,239,529.218
PI 24+93.02	1,127,907.217	2,239,280.411
PT 28+35.00	1,128,004.622	2,238,835.190
POT 30+00.00	1,128,039.887	2,238,674.002
PR RAMP A		
POT 0+00.00	1,128,749.748	2,238,829.314
PC 2+90.00	1,128,763.201	2,239,119.002
PI 3+69.83	1,128,766.904	2,239,198.749
PT 4+49.32	1,128,783.338	2,239,276.871
PC 11+59.30	1,128,929.487	2,239,971.642
PI 13+14.56	1,128,961.447	2,240,123.576
PT 14+65.60	1,128,931.257	2,240,275.871
POT 26+15.60	1,128,707.639	2,241,403.920
PR RAMP B		
POT 0+00.00	1,128,955.167	2,240,741.420
PC 8+00.08	1,129,167.845	2,239,970.123
PI 9+82.02	1,129,216.208	2,239,794.730
PT 11+57.24	1,129,338.752	2,239,660.251
POT 19+49.18	1,129,872.163	2,239,074.890

DESCRIPTION	COORDINATES	
	NORTH	EAST
EX US 67		
PC 115+83.38	1,128,380.490	2,243,666.417
PT 105+05.89	1,128,422.530	2,242,589.743
PI 100+03.06	1,128,442.252	2,242,084.660
PC 94+97.59	1,128,550.541	2,241,590.927
POT 67+57.21	1,129,137.625	2,238,914.179
EX TRANS FROM 4 TO 2 LANES		
PC 0+00.00	1,128,589.049	2,243,453.471
PI 4+14.04	1,128,604.016	2,243,039.703
PRC 8+27.61	1,128,584.728	2,242,626.113
PT 12+95.00	1,128,592.068	2,242,159.079
PC 21+30.54	1,128,657.204	2,241,326.081
PI 24+75.77	1,128,684.117	2,240,981.903
PT 28+19.90	1,128,758.077	2,240,644.689
POT 28+91.00	1,128,773.309	2,240,575.240



REVISIONS	
NAME	DATE

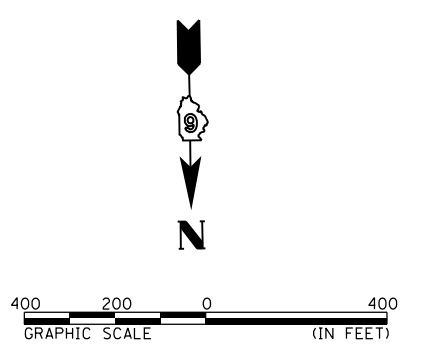
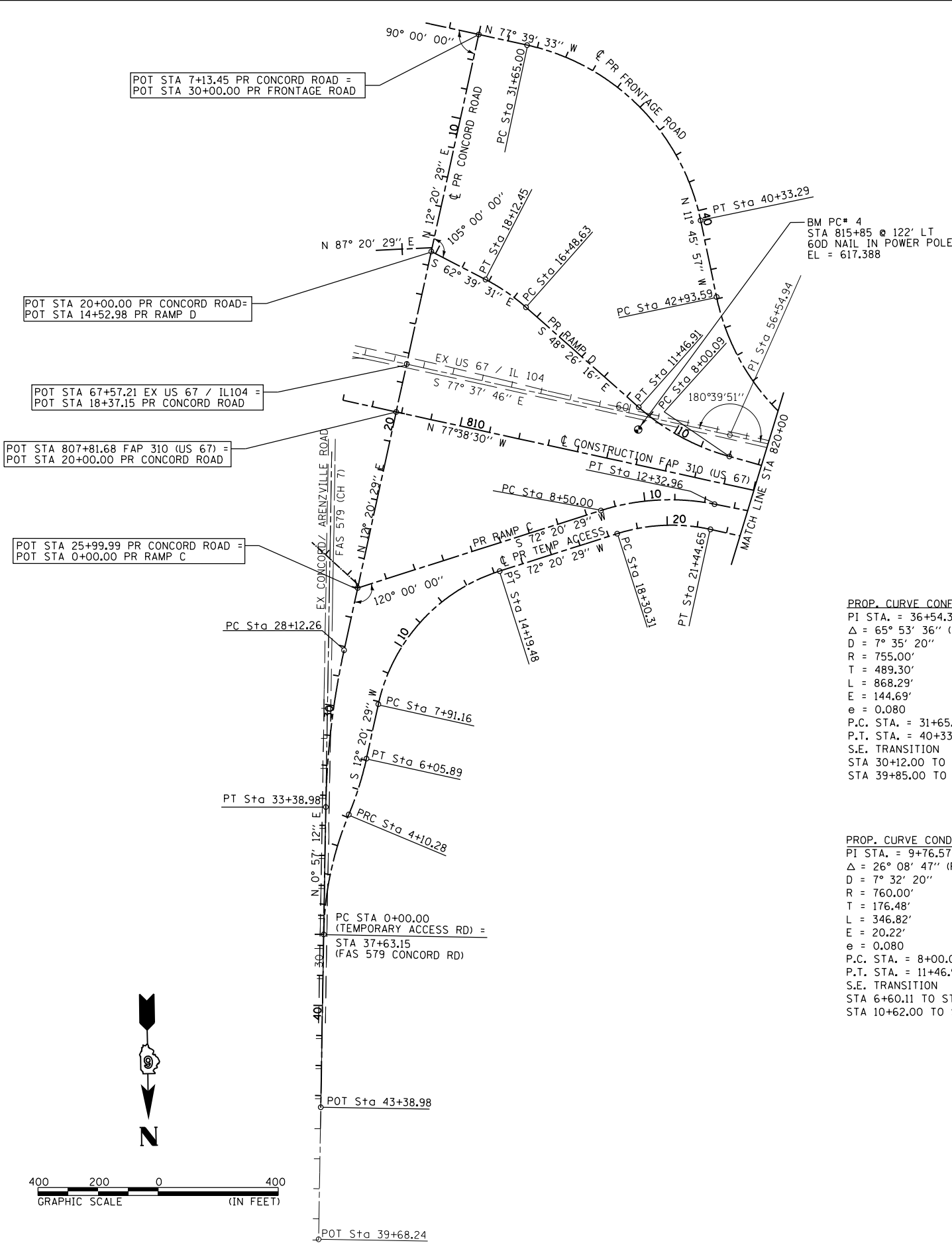
ILLINOIS DEPARTMENT OF TRANSPORTATION
 ALIGNMENT & BENCHMARK DATA
 SHEET 1 OF 9
 FAP 310 (US 67/IL 104)
 DRAWN BY Lin
 CHECKED BY
 DATE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	90
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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DESCRIPTION	COORDINATES	
	NORTH	EAST
PR RAMP D		
PC 8+00.09	1,129,445.009	2,237,839.125
PI 9+76.57	1,129,398.096	2,238,009.260
PT 11+46.91	1,129,281.011	2,238,141.311
PC 16+48.63	1,128,948.148	2,238,516.721
PI 17+30.96	1,128,893.529	2,238,578.323
PT 18+12.45	1,128,855.716	2,238,651.454
POT 20+15.77	1,128,762.329	2,238,832.067
PR RAMP C		
POT 0+00.00	1,129,882.835	2,239,077.226
PC 8+50.00	1,129,624.993	2,238,267.280
PI 10+45.63	1,129,565.648	2,238,080.861
PT 12+32.96	1,129,603.690	2,237,888.957
PR TEMP ACCESS		
PC 0+00.00	1,131,036.626	2,239,190.522
PI 2+07.64	1,130,829.012	2,239,187.068
PRC 4+10.28	1,130,637.481	2,239,106.873
PI 5+08.35	1,130,547.014	2,239,068.994
PT 6+05.89	1,130,451.204	2,239,048.032
PC 7+91.16	1,130,270.221	2,239,008.434
PI 11+37.57	1,129,931.816	2,238,934.394
PT 14+19.48	1,129,826.734	2,238,604.306
PC 18+30.31	1,129,702.110	2,238,212.829
PI 19+91.18	1,129,653.313	2,238,059.547
PT 21+44.65	1,129,687.741	2,237,902.411
PR FRONTAGE ROAD		
POT 30+00.00	1,128,039.887	2,238,674.002
PC 31+65.00	1,128,075.152	2,238,512.815
PI 36+54.30	1,128,179.728	2,238,034.816
PT 40+33.29	1,128,658.751	2,237,935.040
PC 42+93.59	1,128,913.577	2,237,881.963
PR CONCORD ROAD		
POT 7+13.45	1,128,039.887	2,238,674.002
POT 14+52.98	1,128,762.329	2,238,832.067
POT 18+37.15	1,129,137.625	2,238,914.179
POT 20+00.00	1,129,296.711	2,238,948.986
POT 25+99.99	1,129,882.835	2,239,077.225
PC 28+12.26	1,130,090.195	2,239,122.594
PI 30+76.49	1,130,348.318	2,239,179.069
PT 33+38.98	1,130,612.511	2,239,183.465



PROP. CURVE RUNARD1-1
 PI STA. = 2+07.64
 $\Delta = 21^\circ 45' 57''$ (RT)
 D = 5° 18' 19"
 R = 1,080.00'
 T = 207.64'
 L = 410.28'
 E = 19.78'
 e = NC
 P.C. STA. = 0+00.00
 P.R.C. STA. = 4+10.28

PROP. CURVE RUNARD1-2
 PI STA. = 5+08.35
 $\Delta = 10^\circ 22' 40''$ (LT)
 D = 5° 18' 19"
 R = 1,080.00'
 T = 98.08'
 L = 195.62'
 E = 4.44'
 e = NC
 P.R.C. STA. = 4+10.28
 P.T. STA. = 6+05.89

PROP. CURVE RUNARD1-3
 PI STA. = 11+37.57
 $\Delta = 60^\circ 00' 00''$ (RT)
 D = 9° 32' 57"
 R = 600.00'
 T = 346.41'
 L = 628.32'
 E = 92.82'
 e = 0.080
 P.C. STA. = 7+91.16
 P.T. STA. = 14+19.48
 S.E. TRANSITION
 STA 6+41.00 TO STA 8+45.00
 STA 13+65.00 TO STA 15+69.00

PROP. CURVE RUNARD1-4
 PI STA. = 19+91.18
 $\Delta = 30^\circ 01' 00''$ (RT)
 D = 9° 32' 57"
 R = 600.00'
 T = 160.86'
 L = 314.33'
 E = 21.19'
 e = 0.080
 P.C. STA. = 18+30.31
 P.T. STA. = 21+44.65
 S.E. TRANSITION
 STA 16+81.00 TO STA 18+85.00
 STA 20+90.00 TO STA 22+94.00

PROP. CURVE CONFR-3
 PI STA. = 36+54.30
 $\Delta = 65^\circ 53' 36''$ (RT)
 D = 7° 35' 20"
 R = 755.00'
 T = 489.30'
 L = 868.29'
 E = 144.69'
 e = 0.080
 P.C. STA. = 31+65.00
 P.T. STA. = 40+33.29
 S.E. TRANSITION
 STA 30+12.00 TO STA 32+24.00
 STA 39+85.00 TO STA 41+63.00

PROP. CURVE CONFR-4
 PI STA. = 47+76.42
 $\Delta = 65^\circ 11' 58''$ (LT)
 D = 7° 35' 20"
 R = 755.00'
 T = 482.84'
 L = 859.15'
 E = 141.19'
 e = 0.080
 P.C. STA. = 42+93.59
 P.T. STA. = 51+52.73
 S.E. TRANSITION
 STA 41+63.00 TO STA 43+41.00
 STA 50+93.00 TO STA 53+05.00

PROP. CURVE COND-1
 PI STA. = 9+76.57
 $\Delta = 26^\circ 08' 47''$ (RT)
 D = 7° 32' 20"
 R = 760.00'
 T = 176.48'
 L = 346.82'
 E = 20.22'
 e = 0.080
 P.C. STA. = 8+00.09
 P.T. STA. = 11+46.91
 S.E. TRANSITION
 STA 6+60.11 TO STA 8+70.11
 STA 10+62.00 TO STA 13+66.00

PROP. CURVE COND-2
 PI STA. = 17+30.96
 $\Delta = 14^\circ 13' 15''$ (LT)
 D = 8° 40' 52"
 R = 660.00'
 T = 82.33'
 L = 163.81'
 E = 5.12'
 e = 0.058
 P.C. STA. = 16+48.63
 P.T. STA. = 18+12.45
 S.E. TRANSITION
 STA 15+69.00 TO STA 16+88.00
 STA 17+73.00 TO STA 18+92.00

PROP. CURVE CONCI
 PI STA. = 10+45.63
 $\Delta = 28^\circ 52' 16''$ (RT)
 D = 7° 32' 20"
 R = 760.00'
 T = 195.64'
 L = 382.96'
 E = 24.78'
 e = 0.080
 P.C. STA. = 8+50.00
 P.T. STA. = 12+32.96
 S.E. TRANSITION
 STA 7+11.00 TO STA 9+19.00

PROP. CURVE CONCORD-1
 PI STA. = 30+76.49
 $\Delta = 11^\circ 23' 17''$ (LT)
 D = 2° 09' 44"
 R = 2,650.00'
 T = 264.23'
 L = 526.71'
 E = 13.14'
 e = 0.047
 P.C. STA. = 28+12.26
 P.T. STA. = 33+38.97
 S.E. TRANSITION
 STA 26+67.00 TO STA 28+60.00
 STA 32+91.00 TO STA 34+84.00

REVISIONS	
NAME	DATE

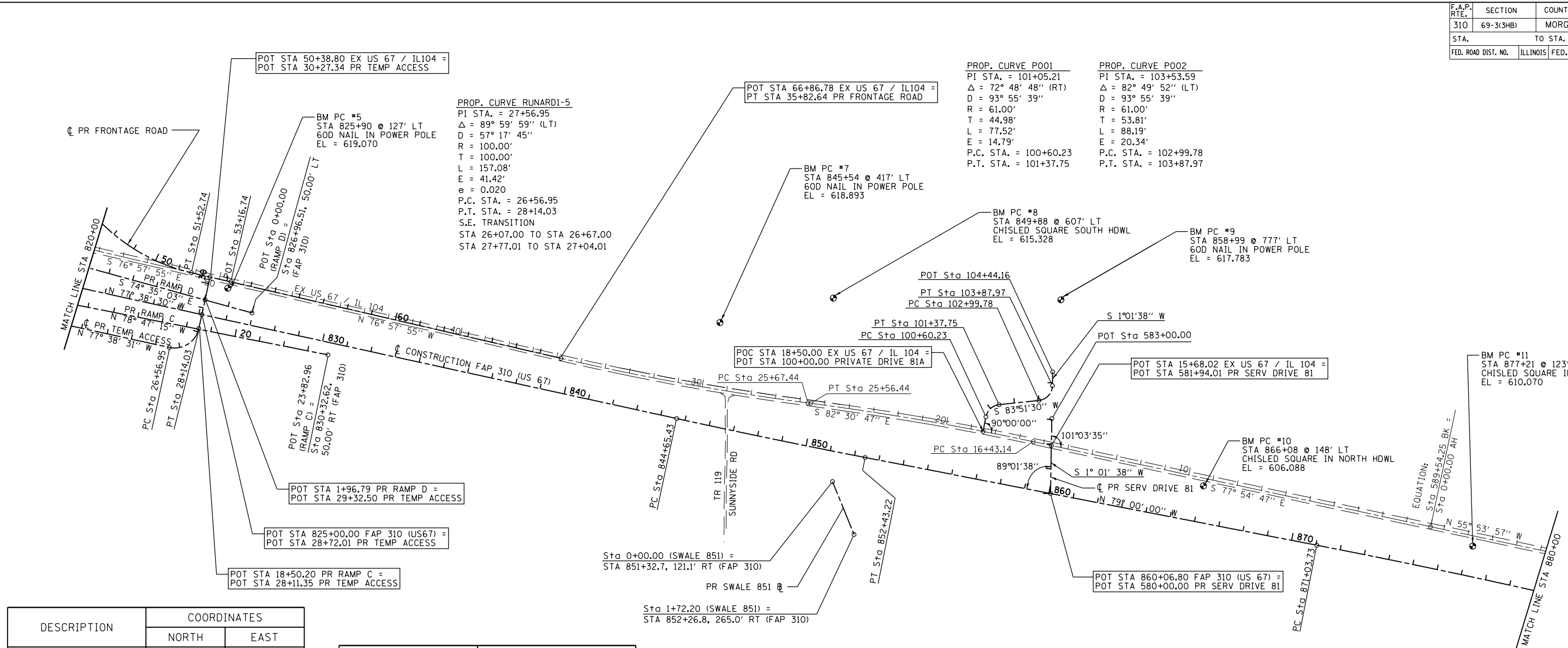
ILLINOIS DEPARTMENT OF TRANSPORTATION
ALIGNMENT & BENCHMARK DATA
 SHEET 2 OF 9
 FAP 310 (US 67/IL 104)

DATE _____ DRAWN BY Lin
 CHECKED BY _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	91
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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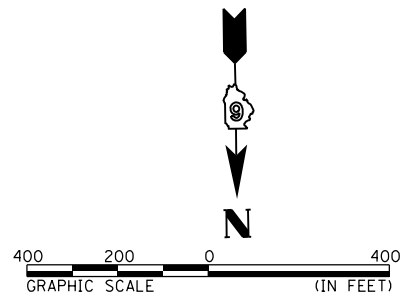


DESCRIPTION	COORDINATES	
	NORTH	EAST
PR FRONTAGE ROAD		
PI 47+76.43	1,129,386.270	2,237,783.505
PT 51+52.74	1,129,495.169	2,237,313.108
POT 53+16.74	1,129,532.157	2,237,153.334
PR TEMP ACCESS		
PC 26+56.95	1,129,797.384	2,237,401.975
PI 27+56.95	1,129,818.786	2,237,304.292
POT 28+11.35	1,129,723.712	2,237,283.498
PT 28+14.03	1,129,721.103	2,237,282.890
POT 28+72.01	1,129,664.470	2,237,270.482
POT 29+32.50	1,129,605.377	2,237,257.534
POT 30+27.34	1,129,512.733	2,237,237.236
FAP 310		
PC 844+65.43	1,130,085.120	2,235,350.598
POT 825+00.00	1,129,664.470	2,237,270.482
PI 848+54.34	1,130,168.358	2,234,970.693
PT 852+43.22	1,130,242.567	2,234,588.921
POT 860+06.80	1,130,388.265	2,233,839.374
PC 871+03.73	1,130,597.569	2,232,762.597
PR RAMP C		
POT 18+50.20	1,129,723.712	2,237,283.498
POT 23+82.96	1,129,827.307	2,236,760.904
PR RAMP D		
POT 0+00.00	1,129,657.688	2,237,067.824
POT 1+96.79	1,129,605.377	2,237,257.534

DESCRIPTION	COORDINATES	
	NORTH	EAST
EX US 67/104		
POT 15+69.02	1,130,194.288	2,233,835.896
PC 16+43.14	1,130,178.560	2,233,909.343
PI 21+00.03	1,130,082.888	2,234,356.110
PT 25+56.44	1,130,023.355	2,234,809.110
PC 25+67.44	1,130,021.921	2,234,820.017
PI 30+75.44	1,129,955.729	2,235,323.683
PT 35+82.64	1,129,841.156	2,235,818.591
POT 50+38.80	1,129,512.733	2,237,237.236
POT 66+86.78	1,129,841.156	2,235,818.591

DESCRIPTION	COORDINATES	
	NORTH	EAST
PRIVATE DRIVE 81A		
POT 100+00.00	1,130,137.085	2,234,112.003
PC 100+60.23	1,130,077.975	2,234,100.464
PI 101+05.21	1,130,033.825	2,234,091.846
PT 101+37.75	1,130,029.012	2,234,047.121
PC 102+99.78	1,130,011.676	2,233,886.018
PI 103+53.59	1,130,005.920	2,233,832.519
PT 103+87.97	1,129,952.120	2,233,831.554
POT 104+44.16	1,129,895.933	2,233,830.546

EXIST. CURVE 304 PI STA. = 21+00.03 Δ = 4° 36' 00" (LT) D = 0° 30' 13" R = 11,375.65' T = 456.90' L = 913.30' E = 9.17' P.C. STA. = 16+43.14 P.T. STA. = 25+56.44	EXIST. CURVE 305 PI STA. = 30+75.44 Δ = 5° 32' 52" (RT) D = 0° 32' 47" R = 10,484.88' T = 508.00' L = 1,015.20' E = 12.30' P.C. STA. = 25+67.44 P.T. STA. = 35+82.64	PROP. CURVE MLE14R2 PI STA. = 848+54.34 Δ = 1° 21' 30" (LT) D = 0° 10' 29" R = 32,808.33' T = 388.92' L = 777.80' E = 2.31' P.C. STA. = 844+65.43 P.T. STA. = 852+43.22	PROP. CURVE CONFR-4 PI STA. = 47+76.42 Δ = 65° 11' 58" (LT) D = 7° 35' 20" R = 755.00' T = 482.84' L = 859.15' E = 141.19' e = 0.080 P.C. STA. = 42+93.59 P.T. STA. = 51+52.74 S.E. TRANSITION STA 41+63.00 TO STA 43+41.00 STA 50+93.00 TO STA 53+05.00	PROP. CURVE MLE15AR2 PI STA. = 876+76.40 Δ = 2° 00' 00" (RT) D = 0° 10' 29" R = 32,808.32' T = 572.67' L = 1,145.22' E = 5.00' e = NC P.C. STA. = 871+03.73 P.T. STA. = 882+48.95
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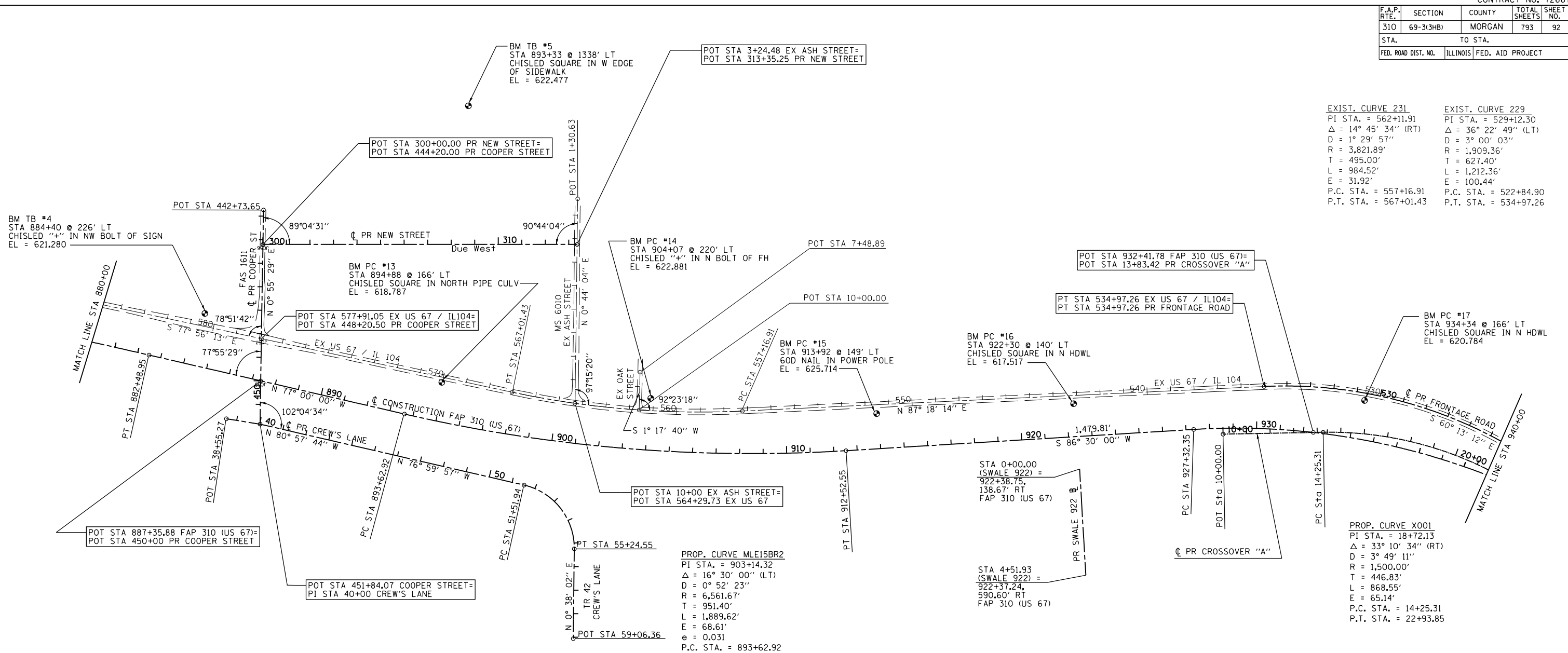
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 ALIGNMENT & BENCHMARK DATA
 SHEET 3 OF 9
 FAP 310 (US 67/IL 104)
 DATE _____
 DRAWN BY Lin
 CHECKED BY _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	92
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

EXIST. CURVE 231 PI STA. = 562+11.91 Δ = 14° 45' 34" (RT) D = 1° 29' 57" R = 3,821.89' T = 495.00' L = 984.52' E = 31.92' P.C. STA. = 557+16.91 P.T. STA. = 567+01.43	EXIST. CURVE 229 PI STA. = 529+12.30 Δ = 36° 22' 49" (LT) D = 3° 00' 03" R = 1,909.36' T = 627.40' L = 1,212.36' E = 100.44' P.C. STA. = 522+84.90 P.T. STA. = 534+97.26
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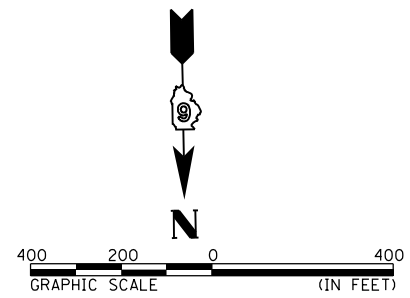
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DESCRIPTION	COORDINATES	
	NORTH	EAST
FAP 310		
PT 882+48.95	1,130,835.662	2,231,642.458
POT 887+35.88	1,130,945.196	2,231,168.014
PC 893+62.92	1,131,086.250	2,230,557.039
PI 903+14.32	1,131,300.268	2,229,630.027
PT 912+52.55	1,131,242.187	2,228,680.406
PC 927+32.35	1,131,151.847	2,227,203.360
POT 932+41.78	1,131,164.655	2,226,694.730
PI 937+25.11	1,131,091.240	2,226,212.458
PR CREW'S LANE		
POT 38+55.27	1,131,106.507	2,231,313.917
POT 40+00.00	1,131,129.242	2,231,170.985
PC 51+51.94	1,131,388.386	2,230,048.573
PI 53+73.18	1,131,438.156	2,229,833.007
PT 55+24.55	1,131,659.379	2,229,835.454
POT 59+06.36	1,132,041.159	2,229,839.678
PR CROSSOVER A		
POT 10+00.00	1,131,171.856	2,227,078.086
PC 14+25.31	1,131,163.868	2,226,652.855
PI 18+72.13	1,131,155.476	2,226,206.106
PT 22+93.85	1,131,392.919	2,225,827.588

DESCRIPTION	COORDINATES	
	NORTH	EAST
PR COOPER ST		
POT 442+73.65	1,130,218.942	2,231,156.292
POT 444+20.00	1,130,365.271	2,231,158.654
POT 448+20.50	1,130,765.724	2,231,165.118
POT 450+00.00	1,130,945.196	2,231,168.014
POT 451+84.07	1,131,129.242	2,231,170.985
PR NEW STREET		
POT 300+00.00	1,130,365.271	2,231,158.654
POT 313+35.25	1,130,365.271	2,229,823.401
PR FRONTAGE ROAD		
POT 524+14.95	1,131,219.257	2,225,865.662
PT 534+97.26	1,130,969.195	2,226,903.844
PI 529+71.12	1,130,943.036	2,226,348.387

DESCRIPTION	COORDINATES	
	NORTH	EAST
EX US 67/IL 104		
PI 529+12.30	1,130,939.681	2,226,277.138
PT 534+97.26	1,130,969.195	2,226,903.844
PC 557+16.91	1,131,073.608	2,229,121.037
POT 577+91.05	1,130,765.724	2,231,165.118
PI 562+11.91	1,131,096.894	2,229,615.490
POT 564+29.73	1,131,040.739	2,229,832.060
POT 561+55.70	1,131,069.071	2,229,559.567
PT 567+01.43	1,130,993.443	2,230,099.559
EX OAK STREET		
POT 8+38.97	1,130,908.077	2,229,555.929
POT 10+00.00	1,131,069.071	2,229,559.567
EX ASH STREET		
POT 1+30.63	1,130,171.442	2,229,820.916
POT 3+24.48	1,130,365.271	2,229,823.401
POT 10+00.00	1,131,040.739	2,229,832.060



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
ALIGNMENT & BENCHMARK DATA
SHEET 4 OF 9
FAP 310 (US 67/IL 104)

DRAWN BY Lin
CHECKED BY

DATE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	93
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

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PROP. CURVE X001
 PI STA. = 18+72.13
 $\Delta = 33^\circ 10' 34''$ (RT)
 $D = 3^\circ 49' 11''$
 $R = 1,500.00'$
 $T = 446.83'$
 $L = 868.55'$
 $E = 65.14'$
 $P.C. STA. = 14+25.31$
 $P.T. STA. = 22+93.85$

EXIST. CURVE 229
 PI STA. = 529+12.30
 $\Delta = 36^\circ 22' 49''$ (LT)
 $D = 3^\circ 00' 03''$
 $R = 1,909.36'$
 $T = 627.40'$
 $L = 1,212.36'$
 $E = 100.44'$
 $P.C. STA. = 522+84.90$
 $P.T. STA. = 534+97.26$

BM FH #1
 STA 948+62 @ 1199' LT
 60D NAIL IN POWER POLE
 EL = 633.726

BM PC #18
 STA 947+29 @ 60' LT
 CHISLED SQUARE IN N PIPE CULV
 EL = 629.769

BM #104
 STA 960+32 @ 115' LT
 CHISLED SQUARE IN E HDWL
 EL = 633.826

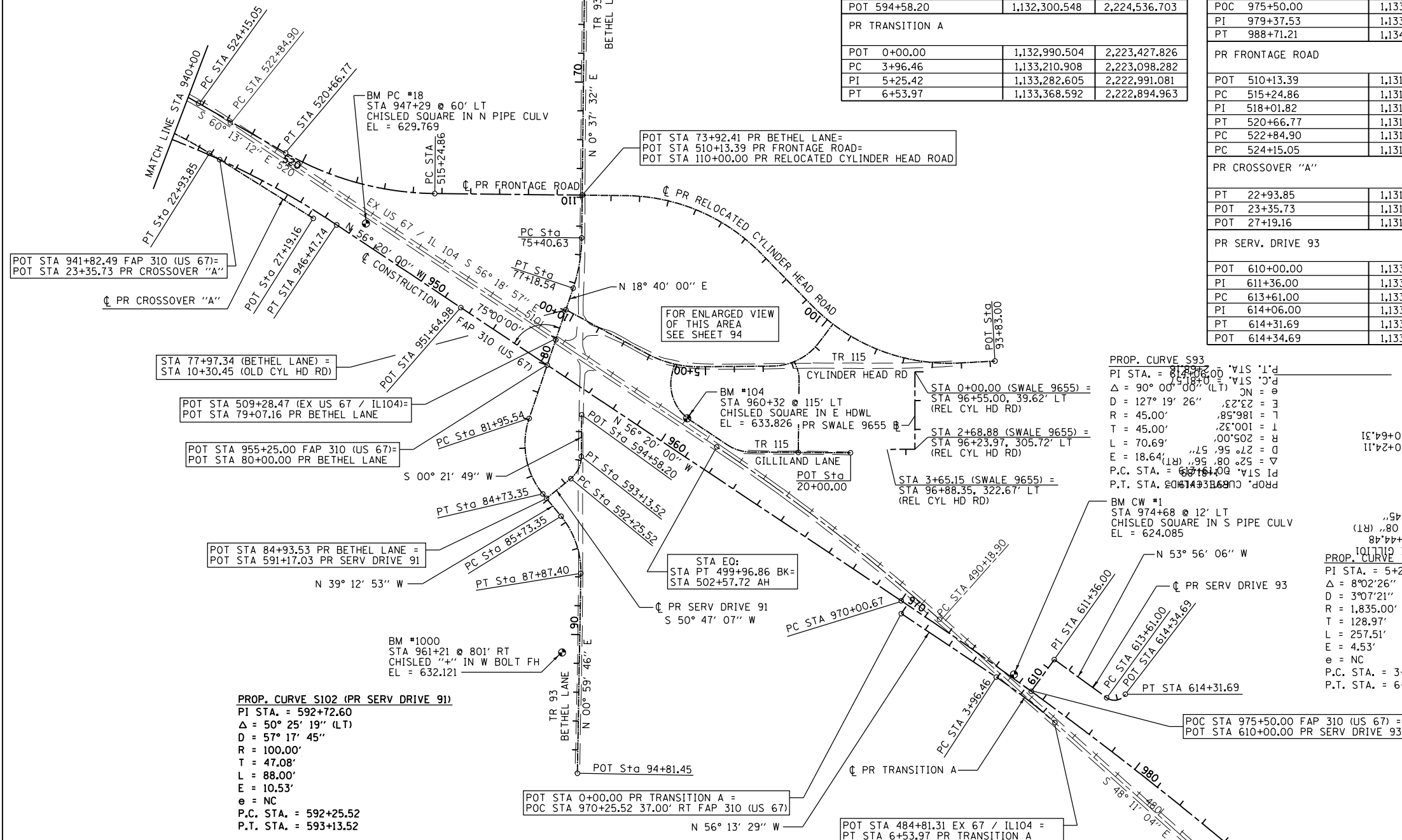
BM CW #1
 STA 974+68 @ 12' LT
 CHISLED SQUARE IN S PIPE CULV
 EL = 624.085

BM FH #2
 STA 986+94 @ 120' RT
 CHISLED SQUARE IN
 CTR FLARED END SEC
 EL = 628.526

DESCRIPTION	COORDINATES	
	NORTH	EAST
PR SERV. DRIVE 91		
POT 591+17.03	1,132,590.663	2,224,658.449
PC 592+25.52	1,132,522.076	2,224,574.396
PI 592+72.60	1,132,492.311	2,224,537.920
PT 593+13.52	1,132,445.232	2,224,537.621
POT 594+58.20	1,132,300.548	2,224,536.703
PR TRANSITION A		
POT 0+00.00	1,132,990.504	2,223,427.826
PC 3+96.46	1,133,210.908	2,223,098.282
PI 5+25.42	1,133,282.605	2,222,991.081
PT 6+53.97	1,133,368.592	2,222,894.963

DESCRIPTION	COORDINATES	
	NORTH	EAST
FAP 310		
PT 946+47.74	1,131,641.584	2,225,386.211
POT 951+64.98	1,131,928.322	2,224,955.724
POT 955+25.00	1,132,127.900	2,224,656.091
PC 970+00.67	1,132,945.954	2,223,427.924
POC 975+50.00	1,133,259.958	2,222,977.237
PI 979+37.53	1,133,465.309	2,222,648.202
PT 988+71.21	1,134,090.160	2,221,950.162
PR FRONTAGE ROAD		
POT 510+13.39	1,131,538.360	2,224,534.699
PC 515+24.86	1,131,532.772	2,225,046.139
PI 518+01.82	1,131,529.746	2,225,323.079
PT 520+66.77	1,131,392.190	2,225,563.459
PC 522+84.90	1,131,287.645	2,225,755.073
PC 524+15.05	1,131,219.210	2,225,865.744
PR CROSSOVER "A"		
PT 22+93.85	1,131,392.919	2,225,827.588
POT 23+35.73	1,131,415.175	2,225,792.109
POT 27+19.16	1,131,618.926	2,225,467.301
PR SERV. DRIVE 93		
POT 610+00.00	1,133,259.958	2,222,977.237
PI 611+36.00	1,133,150.023	2,222,897.174
PC 613+61.00	1,133,282.481	2,222,715.295
PT 614+06.00	1,133,308.972	2,222,678.919
PT 614+31.69	1,133,272.597	2,222,652.428
POT 614+34.69	1,133,270.172	2,222,650.661

DESCRIPTION	COORDINATES	
	NORTH	EAST
PR BETHEL LANE		
POT 65+37.68	1,130,683.681	2,224,525.368
POT 73+92.41	1,131,538.360	2,224,534.699
PC 75+40.63	1,131,686.573	2,224,536.318
PI 76+30.33	1,131,776.263	2,224,537.297
PT 77+18.54	1,131,861.240	2,224,566.005
POT 77+97.34	1,131,538.360	2,224,534.699
POT 79+07.16	1,132,039.939	2,224,626.375
POT 80+00.00	1,132,127.900	2,224,656.091
PC 81+95.54	1,132,313.149	2,224,718.675
PI 83+47.60	1,132,457.213	2,224,767.344
PT 84+73.35	1,132,575.029	2,224,671.206
POT 84+93.53	1,132,590.663	2,224,658.449
PC 85+73.35	1,132,652.507	2,224,607.983
PI 86+85.00	1,132,739.009	2,224,537.397
PT 87+87.40	1,132,850.639	2,224,539.338
POT 94+81.45	1,133,544.579	2,224,551.402



PROP. CURVE C61 (BETHEL LN)
 PI STA. = 76+30.33
 $\Delta = 18^\circ 02' 28''$ (RT)
 $D = 10^\circ 08' 27''$
 $R = 565.00'$
 $T = 89.70'$
 $L = 177.91'$
 $E = 7.08'$
 $e = 0.040$
 $P.C. STA. = 75+40.63$
 $P.T. STA. = 77+18.54$
 S.E. TRANSITION
 STA 74+43.00 TO STA 75+68.00
 STA 76+91.00 TO STA 78+16.00

PROP. CURVE C200 (BETHEL LN)
 PI STA. = 83+47.60
 $\Delta = 57^\circ 52' 53''$ (LT)
 $D = 20^\circ 50' 05''$
 $R = 275.00'$
 $T = 152.06'$
 $L = 277.81'$
 $E = 39.24'$
 $e = 0.040$
 $P.C. STA. = 81+95.54$
 $P.T. STA. = 84+73.35$
 S.E. TRANSITION
 STA 81+30.00 TO STA 82+40.00
 STA 84+51.00 TO STA 85+24.00

PROP. CURVE C201 (BETHEL LN)
 PI STA. = 86+85.00
 $\Delta = 40^\circ 12' 39''$ (RT)
 $D = 18^\circ 47' 08''$
 $R = 305.00'$
 $T = 111.65'$
 $L = 214.05'$
 $E = 19.79'$
 $e = 0.040$
 $P.C. STA. = 85+73.35$
 $P.T. STA. = 87+87.40$
 S.E. TRANSITION
 STA 85+24.00 TO STA 85+97.00
 STA 87+63.00 TO STA 88+73.00

PROP. CURVE MLE17
 PI STA. = 979+37.53
 $\Delta = 8^\circ 10' 00''$ (RT)
 $D = 0^\circ 26' 12''$
 $R = 13,123.33'$
 $T = 936.85'$
 $L = 1,870.54'$
 $E = 33.40'$
 $e = 0.015$
 $P.C. STA. = 970+00.67$
 $P.T. STA. = 988+71.21$
 S.E. TRANSITION
 STA 968+87.00 TO STA 970+23.00
 STA 988+49.00 TO STA 989+85.00

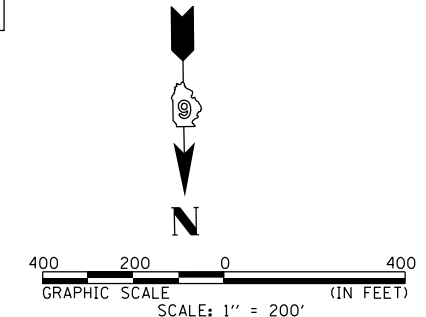
EXIST. CURVE 225
 PI STA. = 495+08.70
 $\Delta = 8^\circ 07' 54''$ (LT)
 $D = 0^\circ 49' 53''$
 $R = 6,890.74'$
 $T = 489.80'$
 $L = 977.96'$
 $E = 17.39'$
 $P.C. STA. = 490+18.90$
 $P.T. STA. = 499+96.86$

PROP. CURVE S93
 PI STA. = 81+20.00
 $\Delta = 90^\circ 00' 00''$ (LT)
 $D = 127^\circ 19' 26''$
 $R = 45.00'$
 $T = 45.00'$
 $L = 70.69'$
 $E = 18.64'$
 $P.C. STA. = 63+14.00$
 $P.T. STA. = 63+14.00$

PROP. CURVE MLE16R2
 PI STA. = 937+25.11
 $\Delta = 1^\circ 56' 26''$
 $D = 37^\circ 10' 00''$ (RT)
 $R = 2,952.75'$
 $T = 992.75'$
 $L = 1,082.21'$
 $E = 79.33'$
 $e = 0.062$ (MATCH EX)
 $P.C. STA. = 927+32.35$
 $P.T. STA. = 946+47.74$
 S.E. TRANSITION
 STA 925+31.00 TO STA 928+13.00
 STA 945+67.00 TO STA 948+49.00

PROP. CURVE 709
 PI STA. = 529+71.12
 $\Delta = 32^\circ 28' 29''$ (LT)
 $D = 3^\circ 00' 03''$
 $R = 1,909.36'$
 $T = 556.07'$
 $L = 1,082.21'$
 $E = 79.33'$
 $e = 0.062$ (MATCH EX)
 $P.C. STA. = 524+15.05$
 $P.T. STA. = 534+97.26$
 S.E. TRANSITION
 STA 522+70.00 TO STA 524+68.00

PROP. CURVE P103A
 PI STA. = 5+25.42
 $\Delta = 8^\circ 02' 26''$ (RT)
 $D = 3^\circ 07' 21''$
 $R = 1,835.00'$
 $T = 128.97'$
 $L = 257.51'$
 $E = 4.53'$
 $e = NC$
 $P.C. STA. = 3+96.46$
 $P.T. STA. = 6+53.97$



REVISIONS	
NAME	DATE

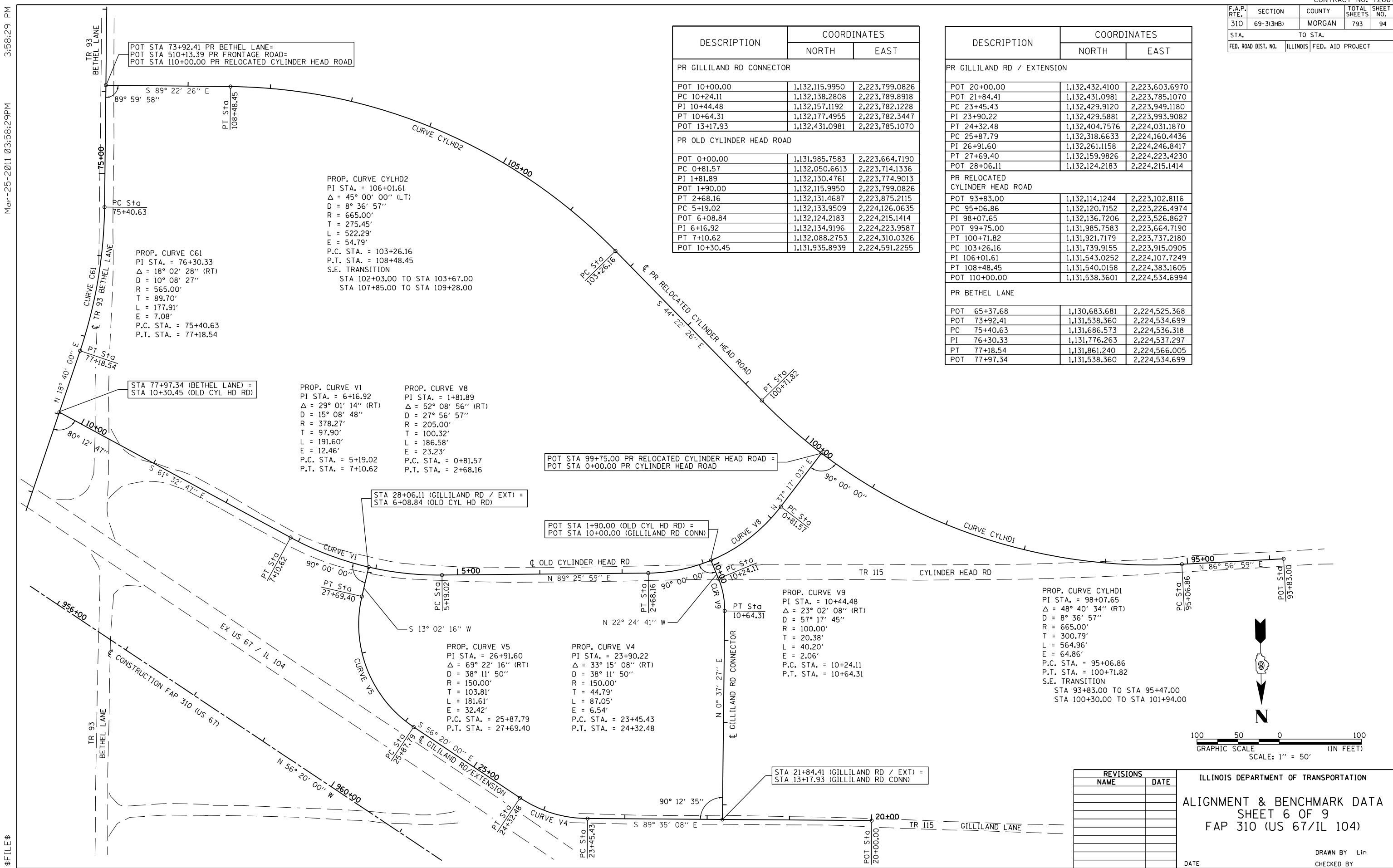
ILLINOIS DEPARTMENT OF TRANSPORTATION
ALIGNMENT & BENCHMARK DATA
 SHEET 5 OF 9
 FAP 310 (US 67/IL 104)

DATE _____
 DRAWN BY Lin
 CHECKED BY _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	94
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

DESCRIPTION	COORDINATES	
	NORTH	EAST
PR GILLILAND RD CONNECTOR		
POT 10+00.00	1,132,115.9950	2,223,799.0826
PC 10+24.11	1,132,138.2808	2,223,789.8918
PI 10+44.48	1,132,157.1192	2,223,782.1228
PT 10+64.31	1,132,177.4955	2,223,782.3447
POT 13+17.93	1,132,431.0981	2,223,785.1070
PR OLD CYLINDER HEAD ROAD		
POT 0+00.00	1,131,985.7583	2,223,664.7190
PC 0+81.57	1,132,050.6613	2,223,714.1336
PI 1+81.89	1,132,130.4761	2,223,774.9013
POT 1+90.00	1,132,115.9950	2,223,799.0826
PT 2+68.16	1,132,131.4687	2,223,875.2115
PC 5+19.02	1,132,133.9509	2,224,126.0635
POT 6+08.84	1,132,124.2183	2,224,215.1414
PI 6+16.92	1,132,134.9196	2,224,223.9587
PT 7+10.62	1,132,088.2753	2,224,310.0326
POT 10+30.45	1,131,935.8939	2,224,591.2255

DESCRIPTION	COORDINATES	
	NORTH	EAST
PR GILLILAND RD / EXTENSION		
POT 20+00.00	1,132,432.4100	2,223,603.6970
POT 21+84.41	1,132,431.0981	2,223,785.1070
PC 23+45.43	1,132,429.9120	2,223,949.1180
PI 23+90.22	1,132,429.5881	2,223,993.9082
PT 24+32.48	1,132,404.7576	2,224,031.1870
PC 25+87.79	1,132,318.6633	2,224,160.4436
PI 26+91.60	1,132,261.1158	2,224,246.8417
PT 27+69.40	1,132,159.9826	2,224,223.4230
POT 28+06.11	1,132,124.2183	2,224,215.1414
PR RELOCATED CYLINDER HEAD ROAD		
POT 93+83.00	1,132,114.1244	2,223,102.8116
PC 95+06.86	1,132,120.7152	2,223,226.4974
PI 98+07.65	1,132,136.7206	2,223,526.8627
POT 99+75.00	1,131,985.7583	2,223,664.7190
PT 100+71.82	1,131,921.7179	2,223,737.2180
PC 103+26.16	1,131,739.9155	2,223,915.0905
PI 106+01.61	1,131,543.0252	2,224,107.7249
PT 108+48.45	1,131,540.0158	2,224,383.1605
POT 110+00.00	1,131,538.3601	2,224,534.6994
PR BETHEL LANE		
POT 65+37.68	1,130,683.681	2,224,525.368
POT 73+92.41	1,131,538.360	2,224,534.699
PC 75+40.63	1,131,686.573	2,224,536.318
PI 76+30.33	1,131,776.263	2,224,537.297
PT 77+18.54	1,131,861.240	2,224,566.005
POT 77+97.34	1,131,538.360	2,224,534.699



PROP. CURVE CYLHD2
 PI STA. = 106+01.61
 $\Delta = 45^\circ 00' 00''$ (LT)
 D = $8^\circ 36' 57''$
 R = 665.00'
 T = 275.45'
 L = 522.29'
 E = 54.79'
 P.C. STA. = 103+26.16
 P.T. STA. = 108+48.45
 S.E. TRANSITION
 STA 102+03.00 TO STA 103+67.00
 STA 107+85.00 TO STA 109+28.00

PROP. CURVE C61
 PI STA. = 76+30.33
 $\Delta = 18^\circ 02' 28''$ (RT)
 D = $10^\circ 08' 27''$
 R = 565.00'
 T = 89.70'
 L = 177.91'
 E = 7.08'
 P.C. STA. = 75+40.63
 P.T. STA. = 77+18.54

PROP. CURVE V1
 PI STA. = 6+16.92
 $\Delta = 29^\circ 01' 14''$ (RT)
 D = $15^\circ 08' 48''$
 R = 378.27'
 T = 97.90'
 L = 191.60'
 E = 12.46'
 P.C. STA. = 5+19.02
 P.T. STA. = 7+10.62

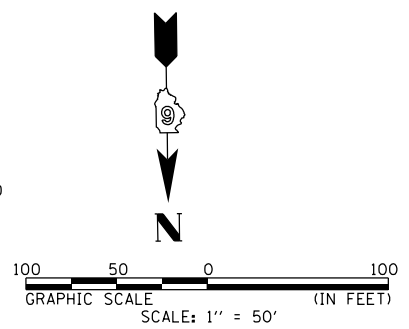
PROP. CURVE V8
 PI STA. = 1+81.89
 $\Delta = 52^\circ 08' 56''$ (RT)
 D = $27^\circ 56' 57''$
 R = 205.00'
 T = 100.32'
 L = 186.58'
 E = 23.23'
 P.C. STA. = 0+81.57
 P.T. STA. = 2+68.16

PROP. CURVE V9
 PI STA. = 10+44.48
 $\Delta = 23^\circ 02' 08''$ (RT)
 D = $57^\circ 17' 45''$
 R = 100.00'
 T = 20.38'
 L = 40.20'
 E = 2.06'
 P.C. STA. = 10+24.11
 P.T. STA. = 10+64.31

PROP. CURVE CYLHD1
 PI STA. = 98+07.65
 $\Delta = 48^\circ 40' 34''$ (RT)
 D = $8^\circ 36' 57''$
 R = 665.00'
 T = 300.79'
 L = 564.96'
 E = 64.86'
 P.C. STA. = 95+06.86
 P.T. STA. = 100+71.82
 S.E. TRANSITION
 STA 93+83.00 TO STA 95+47.00
 STA 100+30.00 TO STA 101+94.00

PROP. CURVE V5
 PI STA. = 26+91.60
 $\Delta = 69^\circ 22' 16''$ (RT)
 D = $38^\circ 11' 50''$
 R = 150.00'
 T = 103.81'
 L = 181.61'
 E = 32.42'
 P.C. STA. = 25+87.79
 P.T. STA. = 27+69.40

PROP. CURVE V4
 PI STA. = 23+90.22
 $\Delta = 33^\circ 15' 08''$ (RT)
 D = $38^\circ 11' 50''$
 R = 150.00'
 T = 44.79'
 L = 87.05'
 E = 6.54'
 P.C. STA. = 23+45.43
 P.T. STA. = 24+32.48



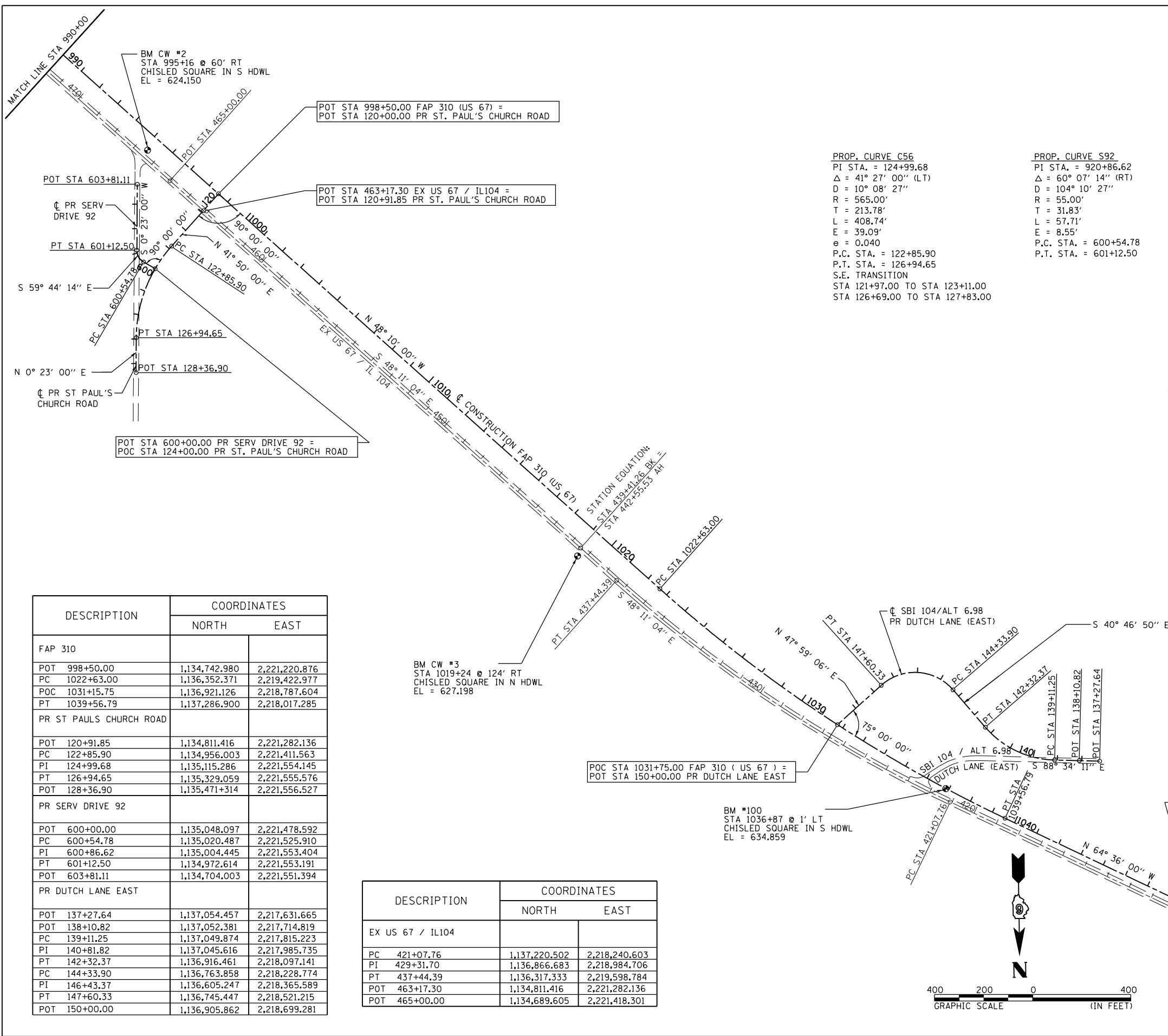
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 ALIGNMENT & BENCHMARK DATA
 SHEET 6 OF 9
 FAP 310 (US 67/IL 104)
 DATE _____
 DRAWN BY Lin
 CHECKED BY _____

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	95
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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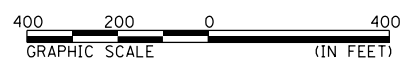


<p>PROP. CURVE C56 PI STA. = 124+99.68 $\Delta = 41^\circ 27' 00''$ (LT) D = 10° 08' 27" R = 565.00' T = 213.78' L = 408.74' E = 39.09' e = 0.040 P.C. STA. = 122+85.90 P.T. STA. = 126+94.65 S.E. TRANSITION STA 121+97.00 TO STA 123+11.00 STA 126+69.00 TO STA 127+83.00</p>	<p>PROP. CURVE S92 PI STA. = 920+86.62 $\Delta = 60^\circ 07' 14''$ (RT) D = 104° 10' 27" R = 55.00' T = 31.83' L = 57.71' E = 8.55' P.C. STA. = 600+54.78 P.T. STA. = 601+12.50</p>	<p>EXIST. CURVE 222 PI STA. = 429+31.70 $\Delta = 16^\circ 23' 05''$ (RT) D = 1° 00' 04" R = 5,723.16' T = 823.94' L = 1,636.63' E = 59.01' P.C. STA. = 421+07.76 P.T. STA. = 437+44.39</p>	<p>PROP. CURVE MLE18 PI STA. = 1031+15.75 $\Delta = 16^\circ 26' 00''$ (LT) D = 0° 58' 13" R = 5,905.50' T = 852.75' L = 1,693.79' E = 61.25' e = 0.033 P.C. STA. = 1022+63.00 P.T. STA. = 1039+56.79 S.E. TRANSITION STA 1021+20.00 TO STA 1023+13.00 STA 1039+07.00 TO STA 1041+00.00</p>
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<p>PROP. CURVE C40 PI STA. = 140+81.82 $\Delta = 47^\circ 47' 21''$ (RT) D = 14° 52' 55" R = 385.00' T = 170.57' L = 321.12' E = 36.09' e = 0.060 P.C. STA. = 139+11.25 P.T. STA. = 142+32.37 S.E. TRANSITION STA 138+04.00 TO STA 139+47.00 STA 141+97.00 TO STA 143+00.00</p>	<p>PROP. CURVE C41 PI STA. = 146+43.37 $\Delta = 91^\circ 14' 04''$ (LT) D = 27° 56' 57" R = 205.00' T = 209.47' L = 326.43' E = 88.09' e = 0.040 P.C. STA. = 144+33.90 P.T. STA. = 147+60.33 S.E. TRANSITION STA 143+60.00 TO STA 144+55.00 STA 147+39.00 TO STA 148+34.00</p>
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DESCRIPTION	COORDINATES	
	NORTH	EAST
FAP 310		
POT 998+50.00	1,134,742.980	2,221,220.876
PC 1022+63.00	1,136,352.371	2,219,422.977
POC 1031+15.75	1,136,921.126	2,218,787.604
PT 1039+56.79	1,137,286.900	2,218,017.285
PR ST PAULS CHURCH ROAD		
POT 120+91.85	1,134,811.416	2,221,282.136
PC 122+85.90	1,134,956.003	2,221,411.563
PI 124+99.68	1,135,115.286	2,221,554.145
PT 126+94.65	1,135,329.059	2,221,555.576
POT 128+36.90	1,135,471+314	2,221,556.527
PR SERV DRIVE 92		
POT 600+00.00	1,135,048.097	2,221,478.592
PC 600+54.78	1,135,020.487	2,221,525.910
PI 600+86.62	1,135,004.445	2,221,553.404
PT 601+12.50	1,134,972.614	2,221,553.191
POT 603+81.11	1,134,704.003	2,221,551.394
PR DUTCH LANE EAST		
POT 137+27.64	1,137,054.457	2,217,631.665
POT 138+10.82	1,137,052.381	2,217,714.819
PC 139+11.25	1,137,049.874	2,217,815.223
PI 140+81.82	1,137,045.616	2,217,985.735
PT 142+32.37	1,136,916.461	2,218,097.141
PC 144+33.90	1,136,763.858	2,218,228.774
PI 146+43.37	1,136,605.247	2,218,365.589
PT 147+60.33	1,136,745.447	2,218,521.215
POT 150+00.00	1,136,905.862	2,218,699.281

DESCRIPTION	COORDINATES	
	NORTH	EAST
EX US 67 / IL104		
PC 421+07.76	1,137,220.502	2,218,240.603
PI 429+31.70	1,136,866.683	2,218,984.706
PT 437+44.39	1,136,317.333	2,219,598.784
POT 463+17.30	1,134,811.416	2,221,282.136
POT 465+00.00	1,134,689.605	2,221,418.301



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 ALIGNMENT & BENCHMARK DATA
 SHEET 7 OF 9
 FAP 310 (US 67/IL 104)
 DATE _____
 DRAWN BY Lin
 CHECKED BY _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	96
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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EXIST. CURVE 84
 PI STA. = 355+35.94
 $\Delta = 24^\circ 16' 39''$ (RT)
 $D = 1^\circ 39' 59''$
 $R = 3,438.56'$
 $T = 739.60'$
 $L = 1,457.00'$
 $E = 78.64'$
 P.C. STA. = 347+96.34
 P.T. STA. = 362+53.34

PROP. CURVE MLE19
 PI STA. = 1104+58.92
 $\Delta = 8^\circ 02' 26''$ (RT)
 $D = 0^\circ 41' 55''$
 $R = 8,202.08'$
 $T = 1,490.57'$
 $L = 2,948.96'$
 $E = 134.34'$
 $e = 0.025$
 P.C. STA. = 1089+68.35
 P.T. STA. = 1119+17.31
 S.E. TRANSITION
 STA 1088+44.00 TO STA 1090+06.00
 STA 1118+80.00 TO STA 1120+42.00

PROP. CURVE C11
 PI STA. = 204+20.00
 $\Delta = 26^\circ 00' 00''$ (LT)
 $D = 10^\circ 08' 27''$
 $R = 565.00'$
 $T = 130.44'$
 $L = 256.39'$
 $E = 14.86'$
 $e = 0.04$
 P.C. STA. = 202+89.56
 P.T. STA. = 205+45.95
 S.E. TRANSITION
 STA 202+01.00 TO STA 203+15.00
 STA 205+21.00 TO STA 206+35.00

PROP. CURVE C12
 PI STA. = 208+59.28
 $\Delta = 16^\circ 56' 39''$ (RT)
 $D = 8^\circ 36' 57''$
 $R = 665.00'$
 $T = 99.05'$
 $L = 196.66'$
 $E = 7.34'$
 $e = 0.06$
 P.C. STA. = 207+60.22
 P.T. STA. = 209+56.88
 S.E. TRANSITION
 STA 206+37.00 TO STA 208+01.00
 STA 209+16.00 TO STA 210+80.00

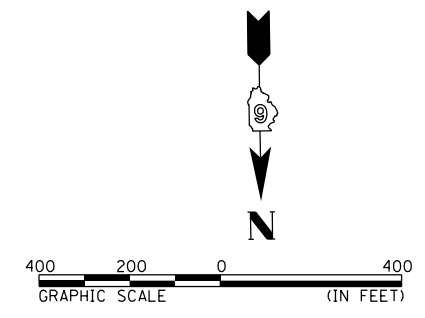
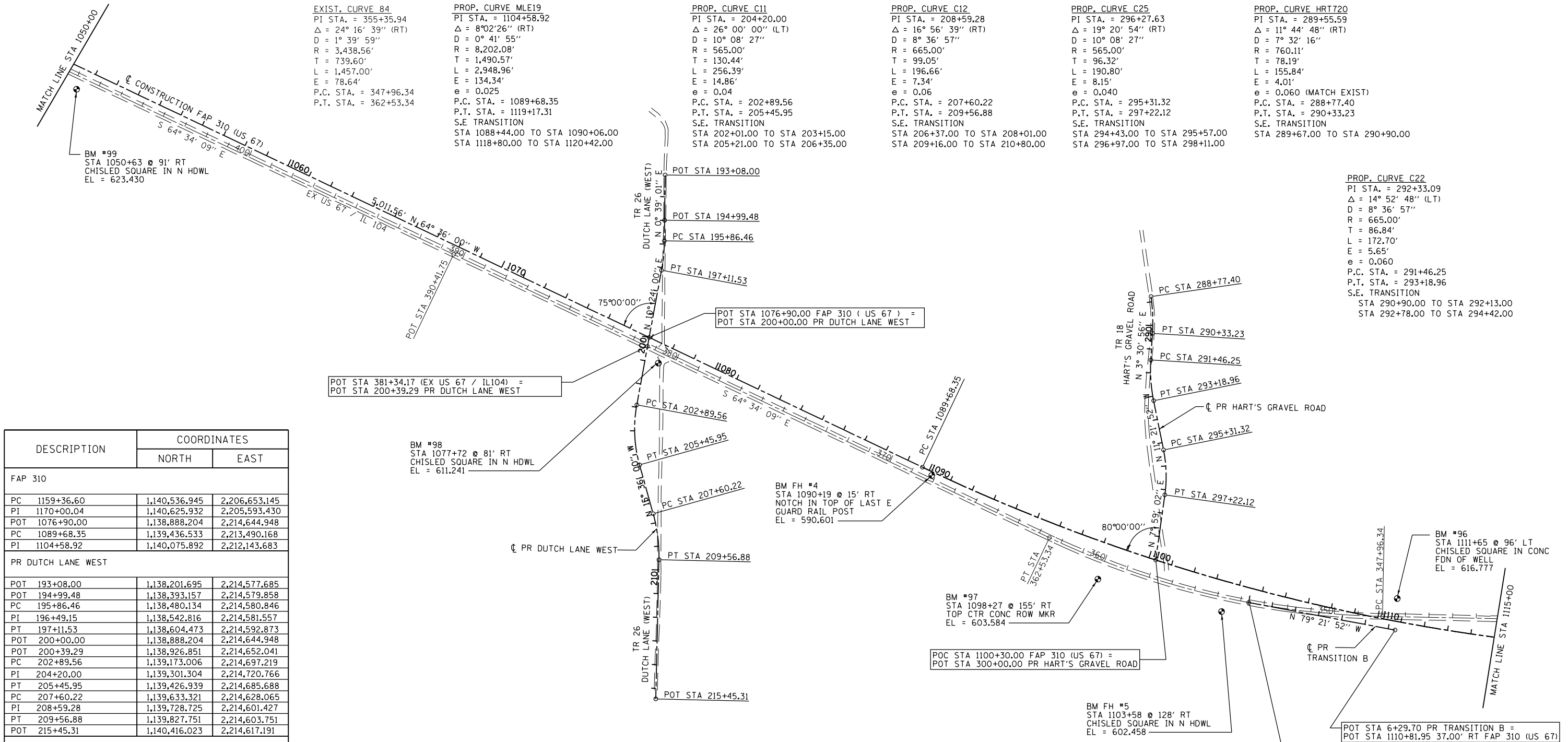
PROP. CURVE C25
 PI STA. = 296+27.63
 $\Delta = 19^\circ 20' 54''$ (RT)
 $D = 10^\circ 08' 27''$
 $R = 565.00'$
 $T = 96.32'$
 $L = 190.80'$
 $E = 8.15'$
 $e = 0.040$
 P.C. STA. = 295+31.32
 P.T. STA. = 297+22.12
 S.E. TRANSITION
 STA 294+43.00 TO STA 295+57.00
 STA 296+97.00 TO STA 298+11.00

PROP. CURVE HRT720
 PI STA. = 289+55.59
 $\Delta = 11^\circ 44' 48''$ (RT)
 $D = 7^\circ 32' 16''$
 $R = 760.11'$
 $T = 78.19'$
 $L = 155.84'$
 $E = 4.01'$
 $e = 0.060$ (MATCH EXIST)
 P.C. STA. = 288+77.40
 P.T. STA. = 290+33.23
 S.E. TRANSITION
 STA 289+67.00 TO STA 290+90.00

PROP. CURVE C22
 PI STA. = 292+33.09
 $\Delta = 14^\circ 52' 48''$ (LT)
 $D = 8^\circ 36' 57''$
 $R = 665.00'$
 $T = 86.84'$
 $L = 172.70'$
 $E = 5.65'$
 $e = 0.060$
 P.C. STA. = 291+46.25
 P.T. STA. = 293+18.96
 S.E. TRANSITION
 STA 290+90.00 TO STA 292+13.00
 STA 292+78.00 TO STA 294+42.00

DESCRIPTION	COORDINATES	
	NORTH	EAST
FAP 310		
PC 1159+36.60	1,140,536.945	2,206,653.145
PI 1170+00.04	1,140,625.932	2,205,593.430
POT 1076+90.00	1,138,888.204	2,214,644.948
PC 1089+68.35	1,139,436.533	2,213,490.168
PI 1104+58.92	1,140,075.892	2,212,143.683
PR DUTCH LANE WEST		
POT 193+08.00	1,138,201.695	2,214,577.685
POT 194+99.48	1,138,393.157	2,214,579.858
PC 195+86.46	1,138,480.134	2,214,580.846
PI 196+49.15	1,138,542.816	2,214,581.557
PT 197+11.53	1,138,604.473	2,214,592.873
POT 200+00.00	1,138,888.204	2,214,644.948
POT 200+39.29	1,138,926.851	2,214,652.041
PC 202+89.56	1,139,173.006	2,214,697.219
PI 204+20.00	1,139,301.304	2,214,720.766
PT 205+45.95	1,139,426.939	2,214,685.688
PC 207+60.22	1,139,633.321	2,214,628.065
PI 208+59.28	1,139,728.725	2,214,601.427
PT 209+56.88	1,139,827.751	2,214,603.751
POT 215+45.31	1,140,416.023	2,214,617.191
PR HART'S GRAVEL ROAD		
PC 288+77.40	1,138,715.410	2,212,524.474
PI 289+55.59	1,138,792.798	2,212,513.290
PT 290+33.23	1,138,870.842	2,212,518.096
PC 291+46.25	1,138,983.646	2,212,525.027
PI 292+33.09	1,139,070.324	2,212,530.352
PT 293+18.96	1,139,155.462	2,212,513.240
PC 295+31.32	1,139,363.662	2,212,471.393
PI 296+27.63	1,139,458.089	2,212,452.414
PT 297+22.12	1,139,553.470	2,212,465.792
POT 300+00.00	1,139,828.662	2,212,504.388
PR TRANSITION B		
POT 0+00.00	1,140,008.633	2,212,111.484
POT 6+29.70	1,140,124.850	2,211,492.605

DESCRIPTION	COORDINATES	
	NORTH	EAST
EX US 67 / IL104		
PC 347+96.34	1,140,067.006	2,211,546.076
POC 353+65.40	1,140,008.633	2,212,111.184
PI 355+35.94	1,140,052.120	2,212,285.526
PT 362+53.34	1,139,734.519	2,212,953.461
POT 381+34.17	1,138,926.851	2,214,652.041
POT 390+41.75	1,138,537.115	2,215,471.680



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
ALIGNMENT & BENCHMARK DATA
 SHEET 8 OF 9
 FAP 310 (US 67/IL 104)

DATE _____ DRAWN BY Lin
 CHECKED BY _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	97
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

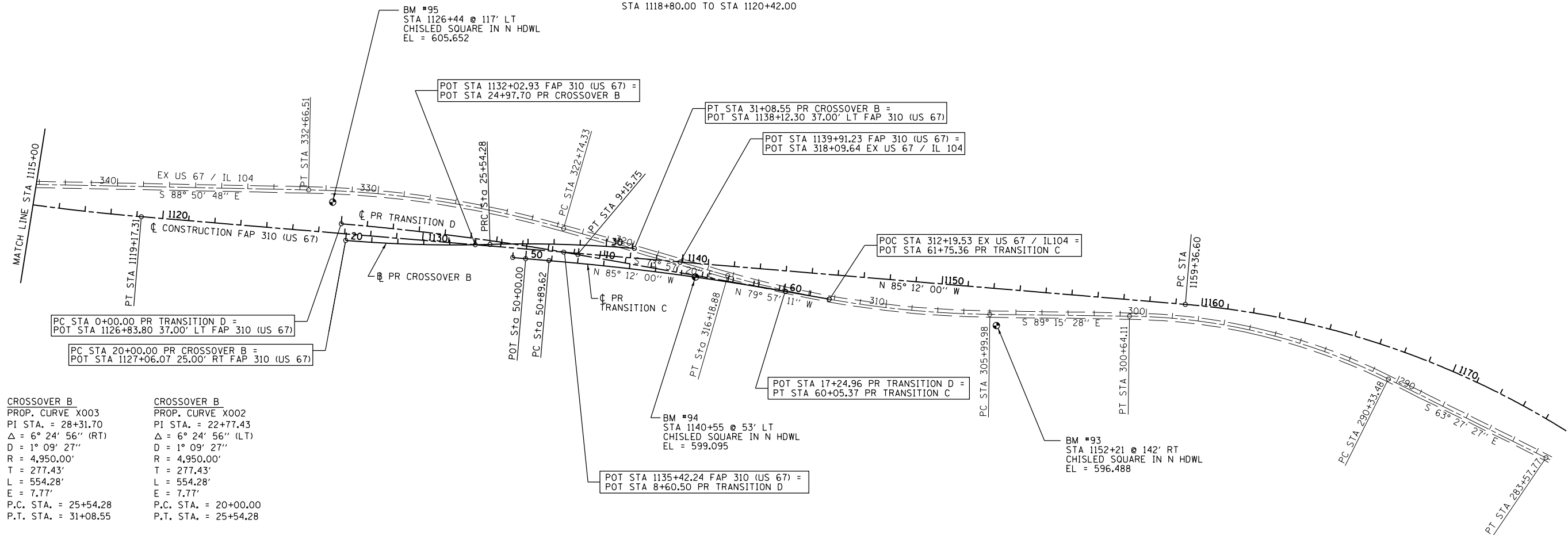
PROP. CURVE MLE19
 PI STA. = 1104+58.92
 $\Delta = 8^{\circ}02'26''$ (RT)
 $D = 0^{\circ}41'55''$
 $R = 8,202.08'$
 $T = 1,490.57'$
 $L = 2,948.96'$
 $E = 134.34'$
 $e = 0.025$
 P.C. STA. = 1089+68.35
 P.T. STA. = 1119+17.31
 S.E. TRANSITION
 STA 1088+44.00 TO STA 1090+06.00
 STA 1118+80.00 TO STA 1120+42.00

EXIST. CURVE 214
 PI STA. = 327+73.23
 $\Delta = 14^{\circ}53'28''$ (LT)
 $D = 1^{\circ}30'03''$
 $R = 3,817.53'$
 $T = 498.90'$
 $L = 992.18'$
 $E = 32.46'$
 P.C. STA. = 322+74.33
 P.T. STA. = 332+66.51

PROP. CURVE TRANSC
 PI STA. = 55+47.82
 $\Delta = 5^{\circ}14'49''$ (RT)
 $D = 0^{\circ}34'23''$
 $R = 10,000.00'$
 $T = 458.20'$
 $L = 915.75'$
 $E = 10.49'$
 $e = NC$
 P.C. STA. = 50+89.62
 P.T. STA. = 60+05.37

PROP. CURVE TRANSD
 PI STA. = 4+58.20
 $\Delta = 5^{\circ}14'49''$ (RT)
 $D = 0^{\circ}34'23''$
 $R = 10,000.00'$
 $T = 458.20'$
 $L = 915.75'$
 $E = 10.49'$
 $e = NC$
 P.C. STA. = 0+00.00
 P.T. STA. = 9+15.75

PROP. CURVE MLE20
 PI STA. = 1170+00.04
 $\Delta = 35^{\circ}55'08''$ (RT)
 $D = 1^{\circ}44'47''$
 $R = 3,280.83'$
 $T = 1,063.44'$
 $L = 2,056.77'$
 $E = 168.05'$
 $e = 0.051$
 $T.R. = 39$
 S.E. RUN = 231
 P.C. STA. = 1159+36.60
 P.C.C. STA. = 1179+93.37



CROSSOVER B
 PROP. CURVE X003
 PI STA. = 28+31.70
 $\Delta = 6^{\circ}24'56''$ (RT)
 $D = 1^{\circ}09'27''$
 $R = 4,950.00'$
 $T = 277.43'$
 $L = 554.28'$
 $E = 7.77'$
 P.C. STA. = 25+54.28
 P.T. STA. = 31+08.55

CROSSOVER B
 PROP. CURVE X002
 PI STA. = 22+77.43
 $\Delta = 6^{\circ}24'56''$ (LT)
 $D = 1^{\circ}09'27''$
 $R = 4,950.00'$
 $T = 277.43'$
 $L = 554.28'$
 $E = 7.77'$
 P.C. STA. = 20+00.00
 P.T. STA. = 25+54.28

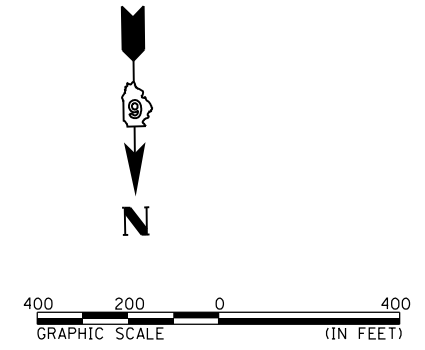
DESCRIPTION	COORDINATES	
	NORTH	EAST
FAP 310		
PT 1119+17.31	1,140,200.619	2,210,658.339
POT 1135+43.24	1,140,336.674	2,209,038.111
POT 1139+91.23	1,140,374.161	2,208,591.694
PC 1159+36.60	1,140,536.945	2,206,653.145
PR TRANSITION C		
POT 49+50.00	1,140,357.3742	2,209,233.7698
POT 50+00.00	1,140,361.5581	2,209,183.9452
PC 50+89.62	1,140,369.0577	2,209,094.6352
PI 55+47.82	1,140,407.3985	2,208,638.0466
PT 60+05.37	1,140,487.3324	2,208,186.8773
POT 61+75.36	1,140,516.9871	2,208,019.4980
PR TRANSITION D		
PC 0+00.00	1,140,227.8879	2,209,891.4370
PI 4+58.20	1,140,266.2287	2,209,434.8484
POT 8+60.50	1,140,336.6739	2,209,038.1108
PT 9+15.75	1,140,346.1626	2,208,983.6790
POT 17+24.96	1,140,487.3324	2,208,186.8773

DESCRIPTION	COORDINATES	
	NORTH	EAST
EX US 67 / IL104		
PT 283+57.77	1,141,125.099	2,205,269.947
PC 290+33.48	1,140,823.147	2,205,874.442
PI 295+57.68	1,140,588.901	2,206,343.392
PT 300+64.11	1,140,582.111	2,206,867.548
PC 305+99.98	1,140,575.169	2,207,403.370
PI 311+12.48	1,140,568.530	2,207,915.827
POT 312+19.53	1,140,516.987	2,208,019.498
POT 318+09.64	1,140,516.987	2,208,019.498
PC 322+74.33	1,140,245.728	2,209,038.284
PI 327+73.23	1,140,107.840	2,209,517.751
PT 332+66.51	1,140,097.798	2,210,016.550
CROSSOVER B		
PC 20+00.00	1,140,291.5332	2,209,874.4409
PI 22+77.43	1,140,314.7478	2,209,597.9863
POT 24+97.70	1,140,308.1975	2,209,377.2269
PRC 25+54.28	1,140,306.9257	2,209,320.6691
PI 28+31.70	1,140,299.1035	2,209,043.3519
PT 31+08.55	1,140,322.3181	2,208,766.8974

EXIST. CURVE 96
 PI STA. = 295+57.68
 $\Delta = 25^{\circ}48'02''$ (LT)
 $D = 2^{\circ}30'12''$
 $R = 2,288.74'$
 $T = 524.20'$
 $L = 1,030.62'$
 $E = 59.26'$
 P.C. STA. = 290+33.48
 P.T. STA. = 300+64.11

EXIST. CURVE 208
 PI STA. = 311+12.48
 $\Delta = 15^{\circ}18'08''$ (RT)
 $D = 1^{\circ}30'07''$
 $R = 3,815.04'$
 $T = 512.50'$
 $L = 1,018.90'$
 $E = 34.27'$
 P.C. STA. = 305+99.98
 P.T. STA. = 316+18.88

EXIST. CURVE 207
 PI STA. = 278+76.27
 $\Delta = 24^{\circ}26'03''$ (LT)
 $D = 2^{\circ}29'53''$
 $R = 2,293.55'$
 $T = 496.60'$
 $L = 978.10'$
 $E = 53.15'$
 P.C. STA. = 273+79.67
 P.T. STA. = 283+57.77



REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		ALIGNMENT & BENCHMARK DATA SHEET 9 OF 9 FAP 310 (US 67/IL 104)

DATE _____ DRAWN BY Lin
 CHECKED BY _____

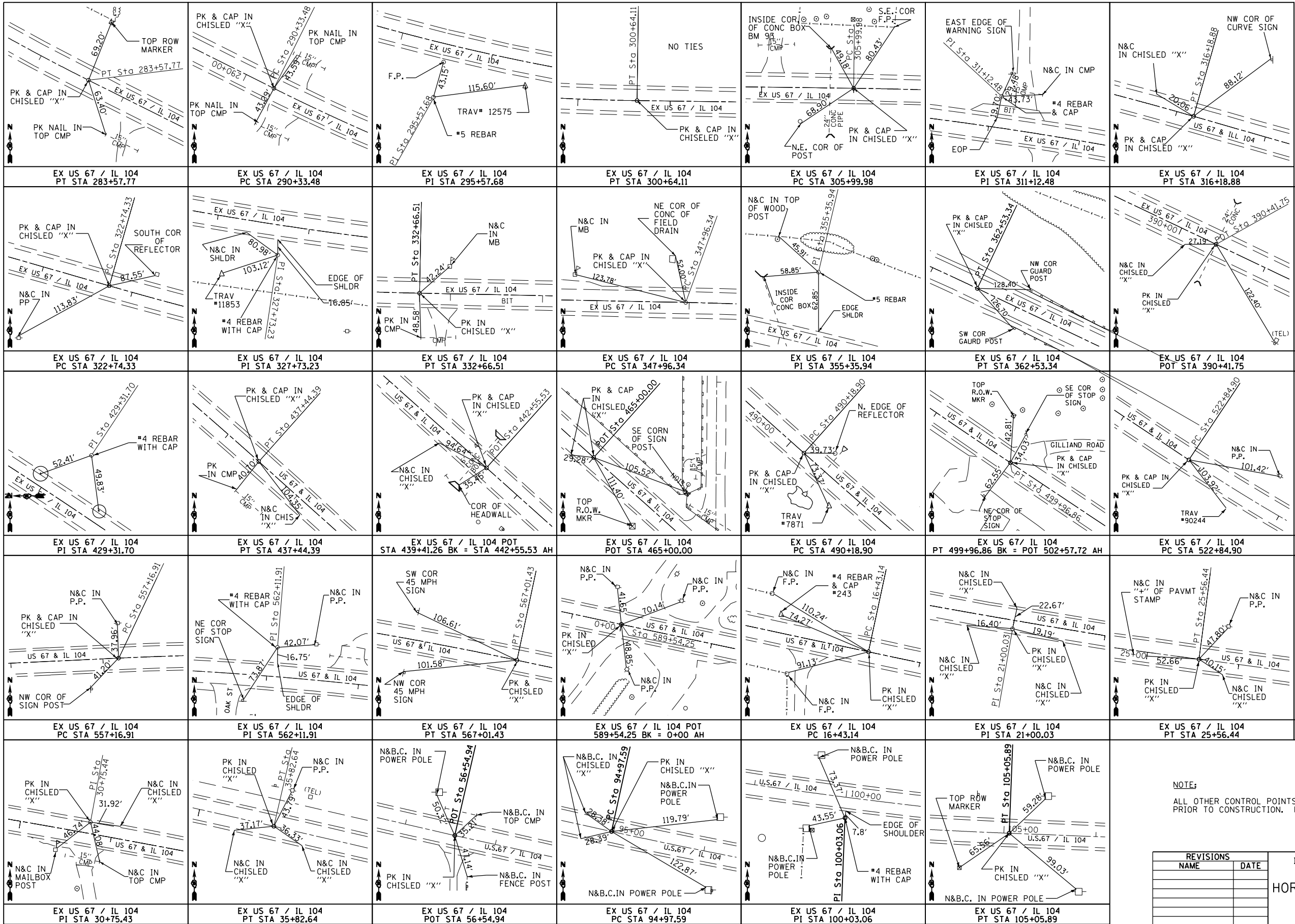
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	98
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

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NOTE:
ALL OTHER CONTROL POINTS WILL BE STAKED BY IDOT
PRIOR TO CONSTRUCTION. NO CROSS TIES AVAILABLE.

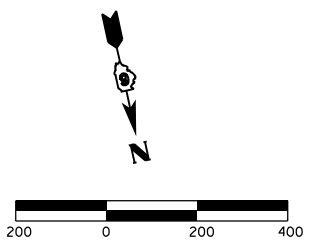
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
HORIZONTAL CONTROL AND TIES
SHEET 1 OF 1
FAP 310 (US 67/IL 104)

DATE 9/06
DRAWN BY B.G.J.
CHECKED BY

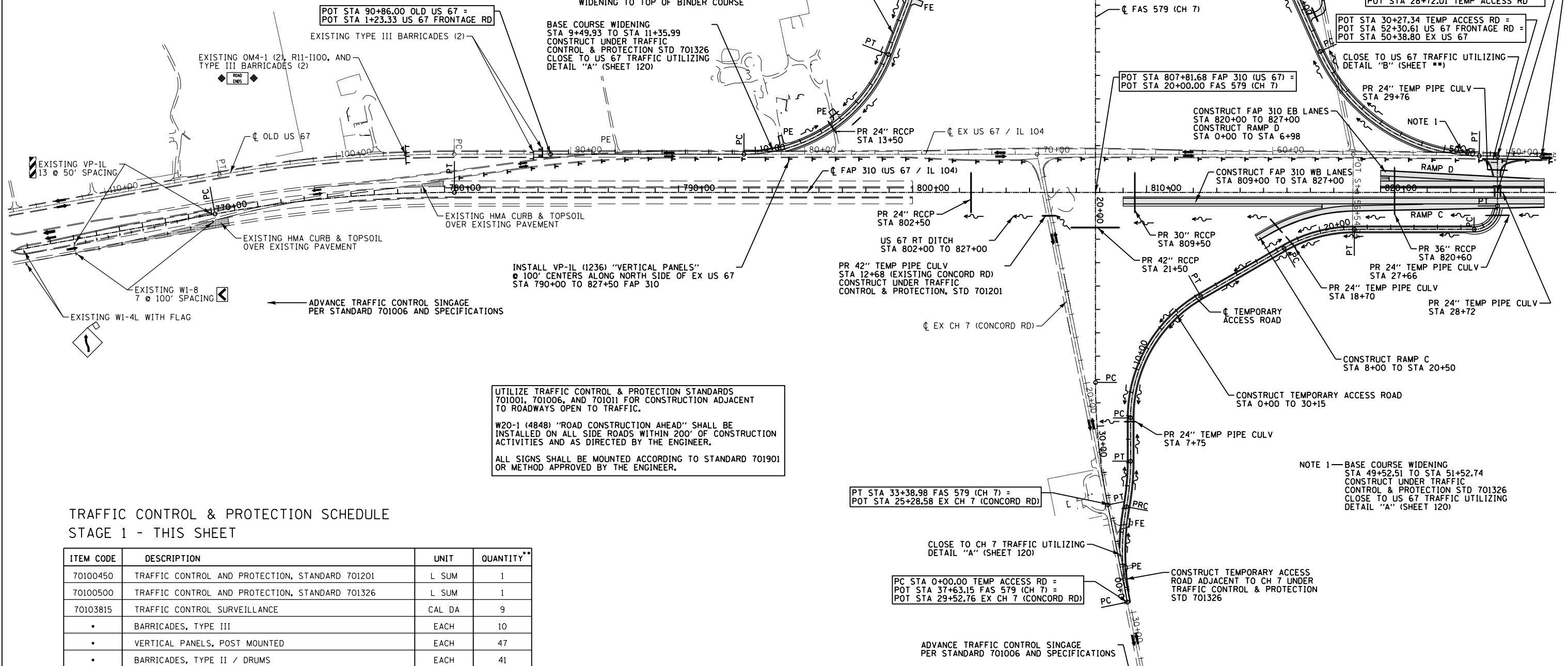
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	99
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

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LEGEND

- STAGE I CONSTRUCTION
- TYPE III BARRICADES WITH R11-2 (4830) "ROAD CLOSED"
- TYPE III BARRICADES WITH R11-4 (4830) "ROAD CLOSED TO THRU TRAFFIC"
- VP-1L (1236) "VERTICAL PANELS"
- DIRECTION OF TRAFFIC



UTILIZE TRAFFIC CONTROL & PROTECTION STANDARDS 701001, 701006, AND 701011 FOR CONSTRUCTION ADJACENT TO ROADWAYS OPEN TO TRAFFIC.

W20-1 (4848) "ROAD CONSTRUCTION AHEAD" SHALL BE INSTALLED ON ALL SIDE ROADS WITHIN 200' OF CONSTRUCTION ACTIVITIES AND AS DIRECTED BY THE ENGINEER.

ALL SIGNS SHALL BE MOUNTED ACCORDING TO STANDARD 701901 OR METHOD APPROVED BY THE ENGINEER.

**TRAFFIC CONTROL & PROTECTION SCHEDULE
STAGE 1 - THIS SHEET**

ITEM CODE	DESCRIPTION	UNIT	QUANTITY**
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	9
	• BARRICADES, TYPE III	EACH	10
	• VERTICAL PANELS, POST MOUNTED	EACH	47
	• BARRICADES, TYPE II / DRUMS	EACH	41

- ITEM WILL NOT BE MEASURED AND PAID FOR SEPARATELY. THE COST WILL BE INCLUDED IN THE CONTRACT UNIT BID PRICES OF THE TRAFFIC CONTROL PAY ITEMS IN THE CONTRACT.
- ** - QUANTITY MAY BE ADJUSTED ACCORDING TO SITE CONDITIONS AND AS DIRECTED BY THE ENGINEER.

SEE SHEET 125 FOR STAGE CONSTRUCTION PAVEMENT MARKING SCHEDULE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**TRAFFIC CONTROL & STAGING
STAGE I - 761+00 TO 827+50
FAP 310 (US 67/IL 104)
SHEET 1 OF 27**

DATE 7/09

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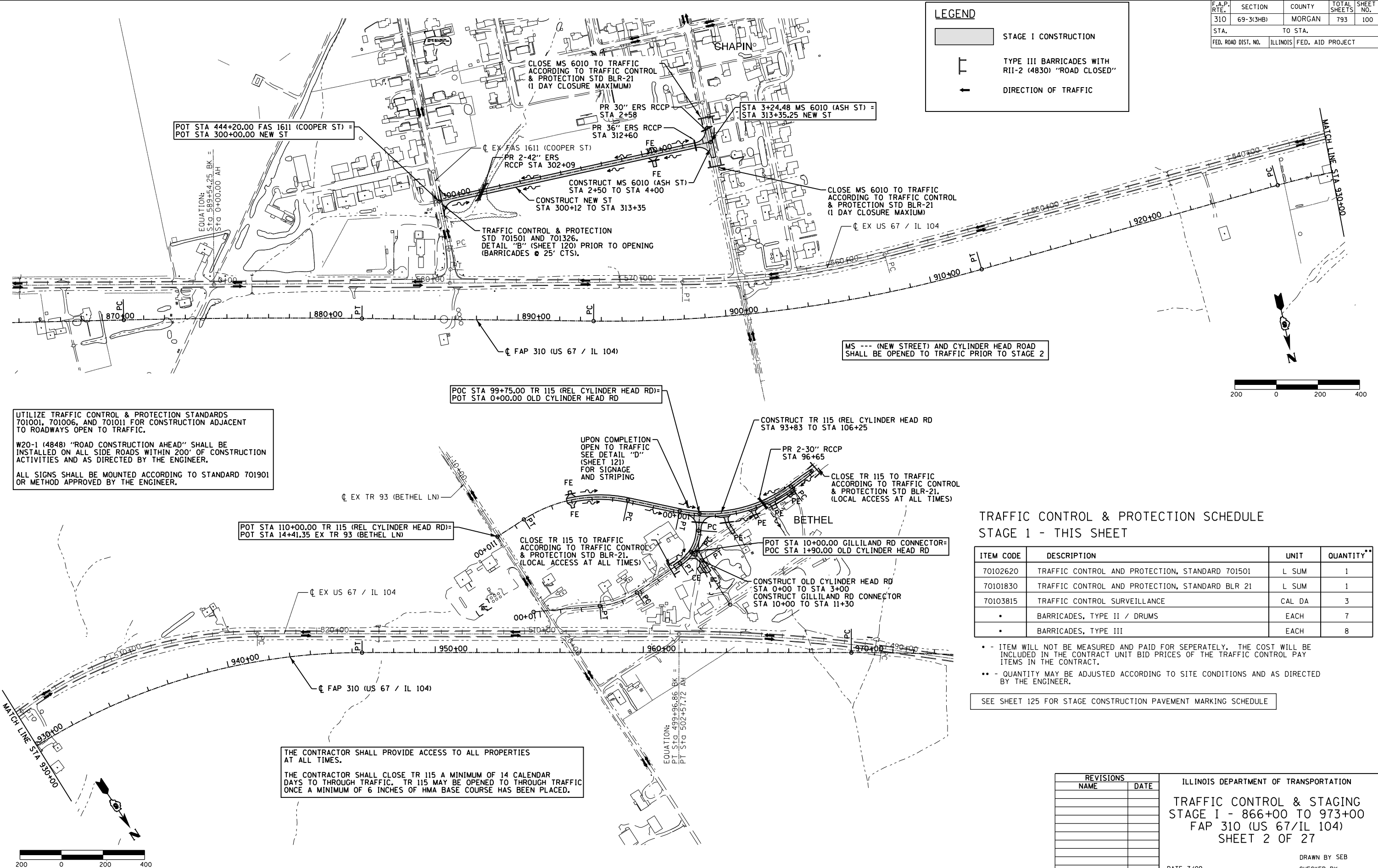
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	100
STA. TO STA.		ILLINOIS FED. AID PROJECT		
FED. ROAD DIST. NO.				

LEGEND

- STAGE I CONSTRUCTION
- TYPE III BARRICADES WITH RII-2 (4830) "ROAD CLOSED"
- DIRECTION OF TRAFFIC

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UTILIZE TRAFFIC CONTROL & PROTECTION STANDARDS 701001, 701006, AND 701011 FOR CONSTRUCTION ADJACENT TO ROADWAYS OPEN TO TRAFFIC.

W20-1 (4848) "ROAD CONSTRUCTION AHEAD" SHALL BE INSTALLED ON ALL SIDE ROADS WITHIN 200' OF CONSTRUCTION ACTIVITIES AND AS DIRECTED BY THE ENGINEER.

ALL SIGNS SHALL BE MOUNTED ACCORDING TO STANDARD 701901 OR METHOD APPROVED BY THE ENGINEER.

THE CONTRACTOR SHALL PROVIDE ACCESS TO ALL PROPERTIES AT ALL TIMES.

THE CONTRACTOR SHALL CLOSE TR 115 A MINIMUM OF 14 CALENDAR DAYS TO THROUGH TRAFFIC. TR 115 MAY BE OPENED TO THROUGH TRAFFIC ONCE A MINIMUM OF 6 INCHES OF HMA BASE COURSE HAS BEEN PLACED.

**TRAFFIC CONTROL & PROTECTION SCHEDULE
STAGE 1 - THIS SHEET**

ITEM CODE	DESCRIPTION	UNIT	QUANTITY**
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
70101830	TRAFFIC CONTROL AND PROTECTION, STANDARD BLR 21	L SUM	1
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	3
*	BARRICADES, TYPE II / DRUMS	EACH	7
*	BARRICADES, TYPE III	EACH	8

* - ITEM WILL NOT BE MEASURED AND PAID FOR SEPARATELY. THE COST WILL BE INCLUDED IN THE CONTRACT UNIT BID PRICES OF THE TRAFFIC CONTROL PAY ITEMS IN THE CONTRACT.

** - QUANTITY MAY BE ADJUSTED ACCORDING TO SITE CONDITIONS AND AS DIRECTED BY THE ENGINEER.

SEE SHEET 125 FOR STAGE CONSTRUCTION PAVEMENT MARKING SCHEDULE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**TRAFFIC CONTROL & STAGING
STAGE I - 866+00 TO 973+00
FAP 310 (US 67/IL 104)
SHEET 2 OF 27**

DATE 7/09

DRAWN BY SEB
CHECKED BY