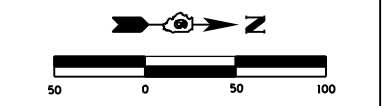
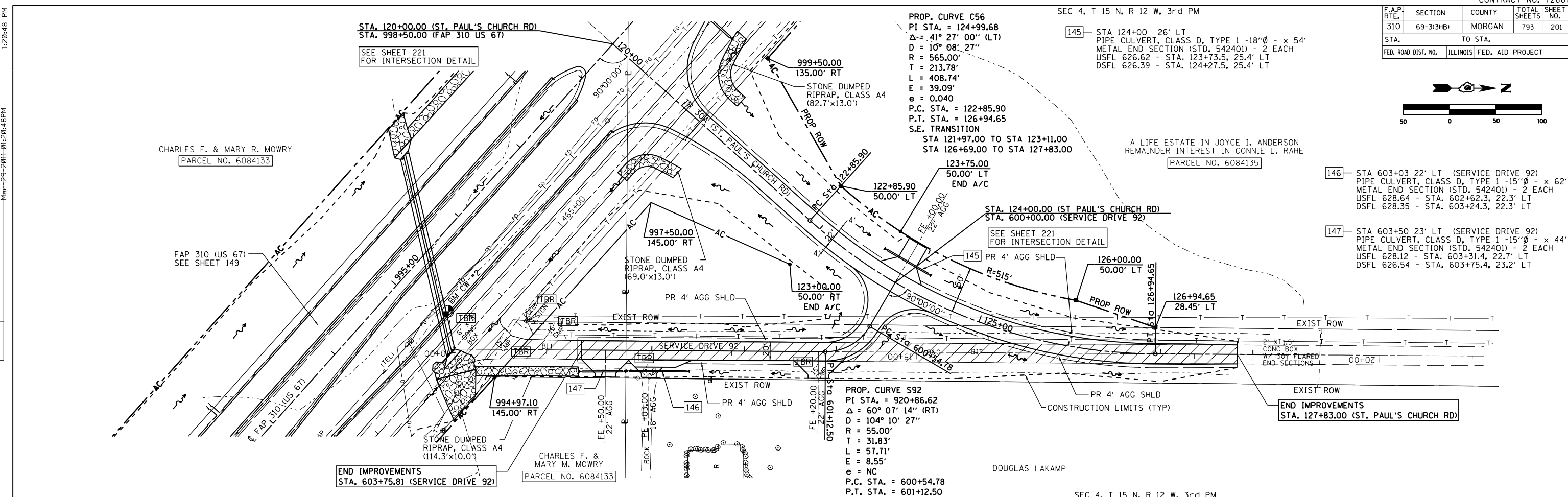


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	201

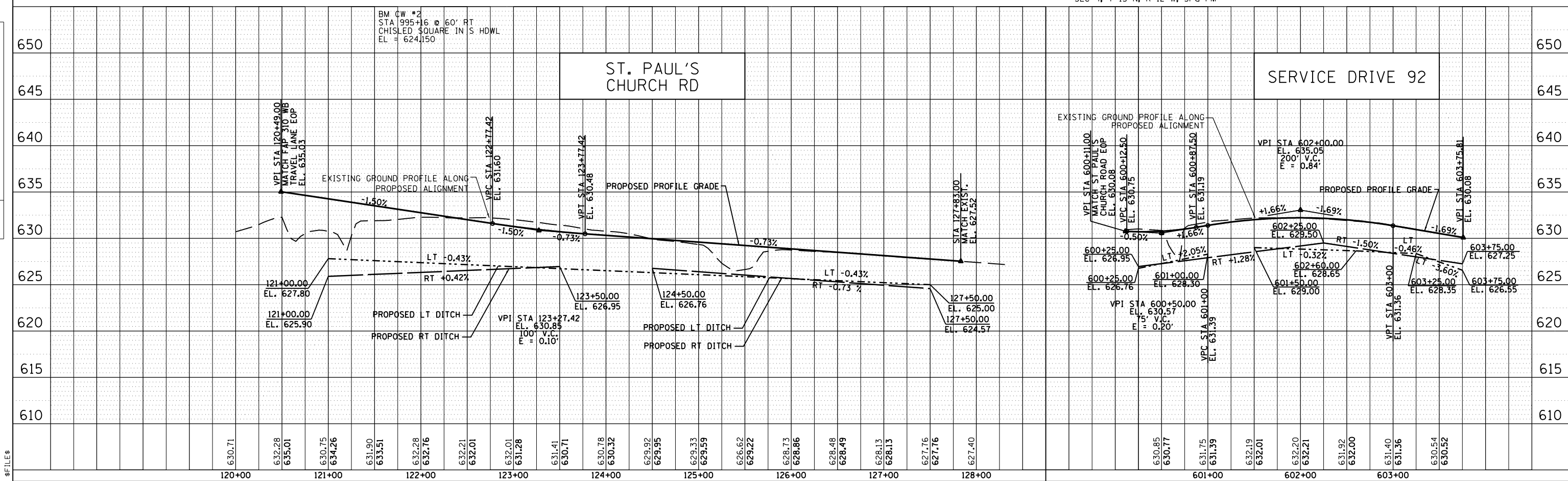


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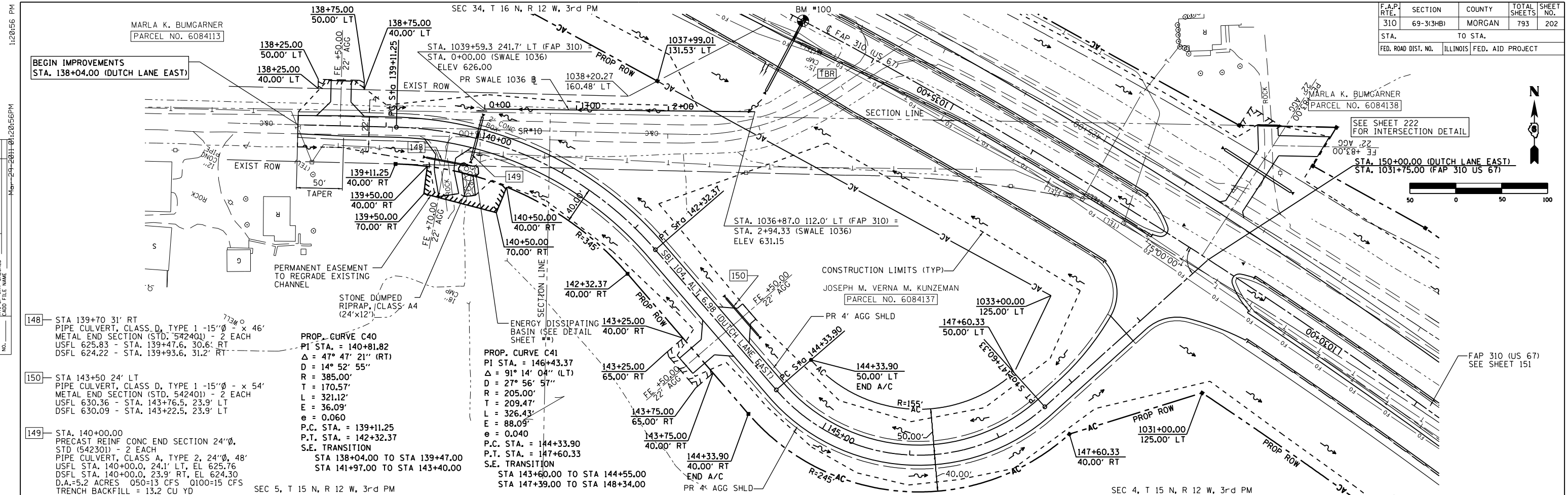
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PROFILE	DATE
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STRUCTURE NOTATIONS CHKD	



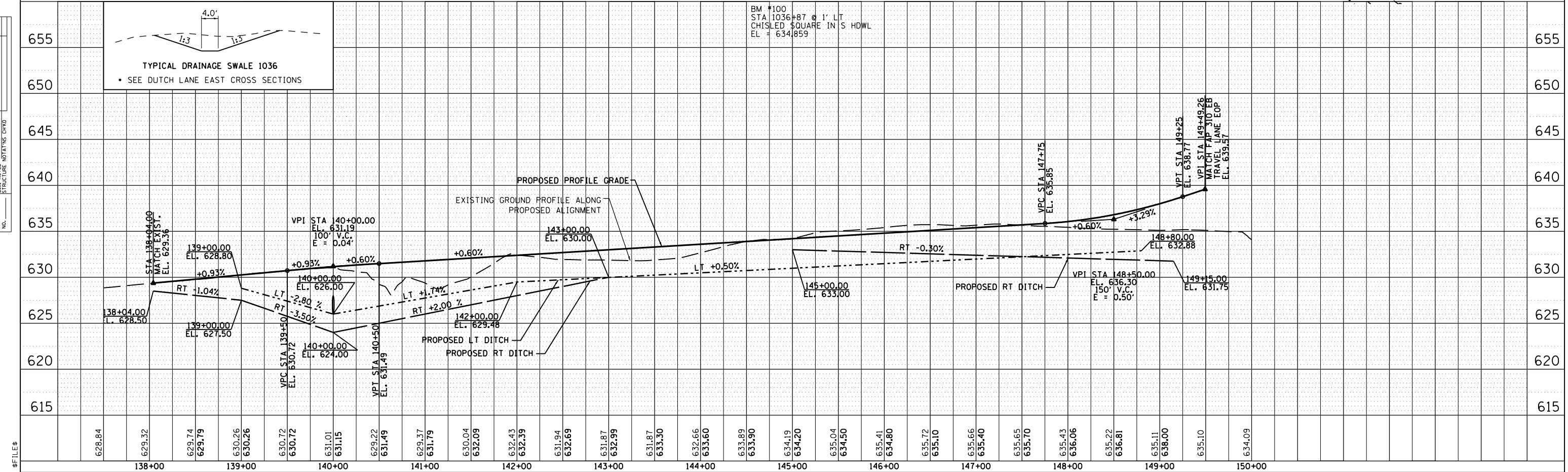
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	202
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



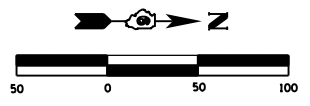
- 148 STA 139+70 31' RT  
PIPE CULVERT, CLASS D, TYPE 1 - 15"Ø - x 46'  
METAL END SECTION (STD. 542401) - 2 EACH  
USFL 625.83 - STA. 139+47.6, 30.6' RT  
DSFL 624.22 - STA. 139+93.6, 31.2' RT
- 150 STA 143+50 24' LT  
PIPE CULVERT, CLASS D, TYPE 1 - 15"Ø - x 54'  
METAL END SECTION (STD. 542401) - 2 EACH  
USFL 630.36 - STA. 143+76.5, 23.9' LT  
DSFL 630.09 - STA. 143+22.5, 23.9' LT
- 149 STA. 140+00.00  
PRECAST REINF CONC END SECTION 24"Ø,  
STD (542301) - 2 EACH  
PIPE CULVERT, CLASS A, TYPE 2, 24"Ø, 48'  
USFL STA. 140+00.0, 24.1' LT, EL 625.76  
DSFL STA. 140+00.0, 23.9' RT, EL 624.30  
D.A.=5.2 ACRES 050-13 CFS 0100-15 CFS  
TRENCH BACKFILL = 13.2 CU YD

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DATE	



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	203
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



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NOTATIONS CHKD	

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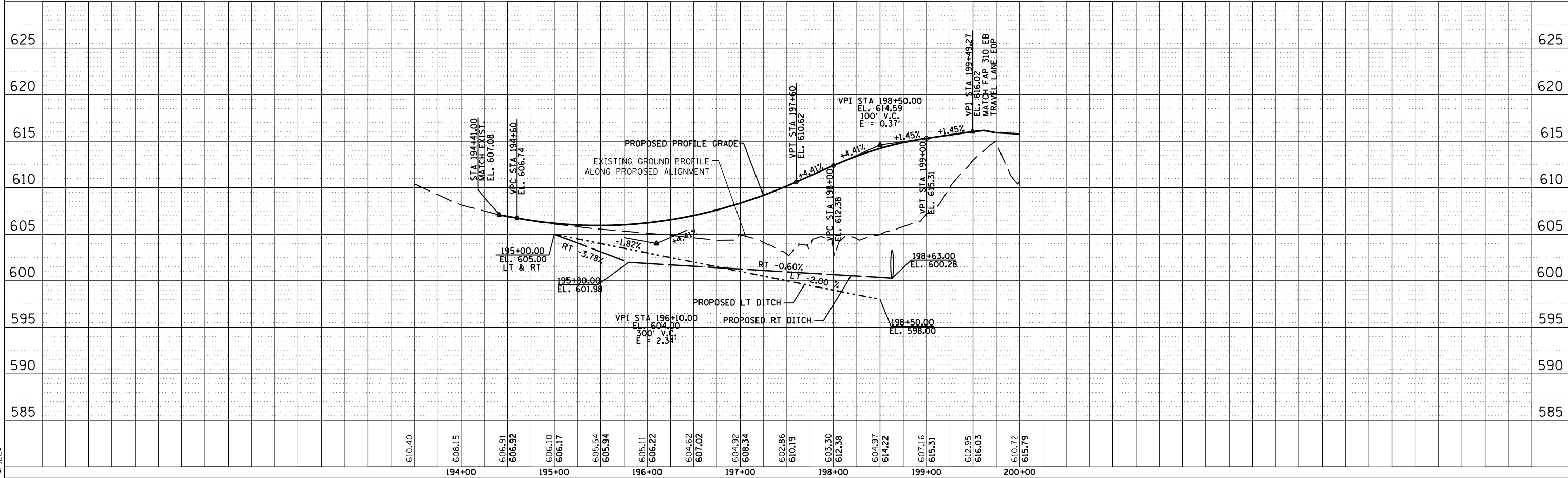
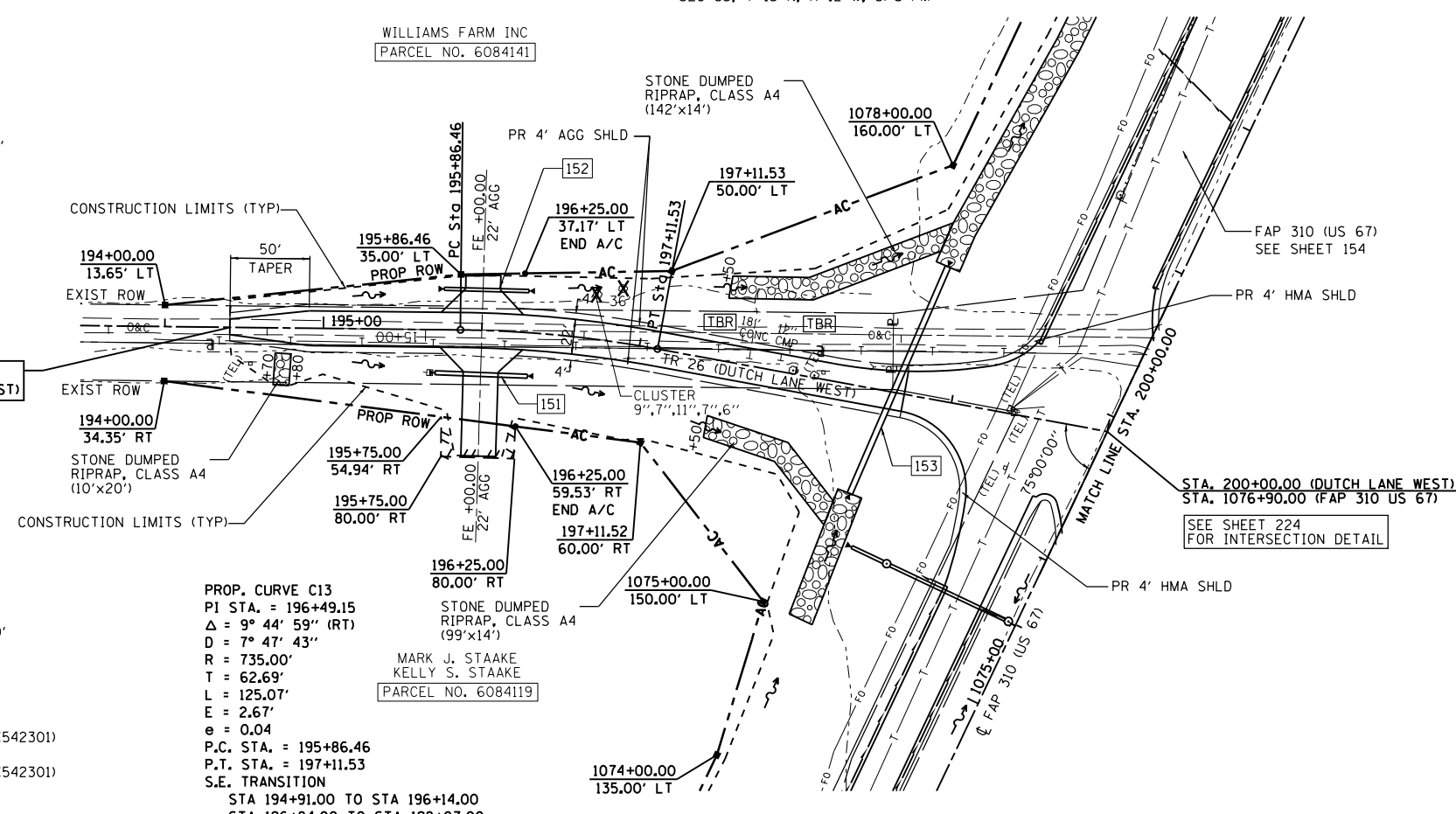
SEC 33, T 15 N, R 12 W, 3rd PM

152 - STA 196+00.0 26.0' LT  
PIPE CULVERT, CLASS D, TYPE 1 - 24"Ø - x 52'  
METAL END SECTION (STD. 542401) - 2 EACH  
USFL 603.48 - STA. 195+76.0, 25.8' LT  
DSFL 602.44 - STA. 196+28.0, 26.2' LT

151 - STA 196+00.0 27' RT  
PIPE CULVERT, CLASS D, TYPE 1 - 30"Ø - x 60'  
METAL END SECTION (STD. 542401) - 2 EACH  
USFL 602.04 - STA. 195+70.7, 27.2' RT  
DSFL 601.68 - STA. 196+30.7, 27.4' RT

153 - STA. 198+63.00 SKEW= 13° LT AH  
PRECAST REINF CONC END SECTION 42"Ø, STD (542301)  
PIPE CULVERT, CLASS A, TYPE 3, 42"Ø, 156'  
PRECAST REINF CONC END SECTION 42"Ø, STD (542301)  
USFL STA. 198+47.3, 67.8' RT, EL 600.33  
DSFL STA. 198+82.4, 84.2' LT, EL 597.37  
TRENCH BACKFILL = 21.3 CU YD

PROP. CURVE C13  
PI STA. = 196+49.15  
Δ = 9° 44' 59" (RT)  
D = 7° 47' 43"  
R = 735.00'  
T = 62.69'  
L = 125.07'  
E = 2.67'  
e = 0.04  
P.C. STA. = 195+86.46  
P.T. STA. = 197+11.53  
S.E. TRANSITION  
STA 194+91.00 TO STA 196+14.00  
STA 196+84.00 TO STA 198+07.00

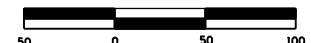


SECTION 69-3(3HB)

MORGAN COUNTY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	204

SEC 33, T 16 N, R 12 W, 3rd PM



154 STA. 201+01.00 SKEW= 13° LT AH  
 PRECAST REINF CONC END SECTION 30"Ø, STD (542301)  
 PIPE CULVERT, CLASS A, TYPE 2, 30"Ø, 98'  
 PRECAST REINF CONC END SECTION 30"Ø, STD (542301)  
 USFL STA. 200+90.6, 45.0' RT, EL 609.45  
 DSFL STA. 201+12.7, 50.5' LT, EL 607.86  
 TRENCH BACKFILL = 25.7 CU YD

155 STA 203+50 27' LT  
 PIPE CULVERT, CLASS D, TYPE 1, 18"Ø, 48'  
 METAL END SECTION (STD. 542401) - 2 EACH  
 USFL 603.75 - STA. 203+30.9, 27.0' LT  
 DSFL 601.26 - STA. 203+78.9, 27.8' LT

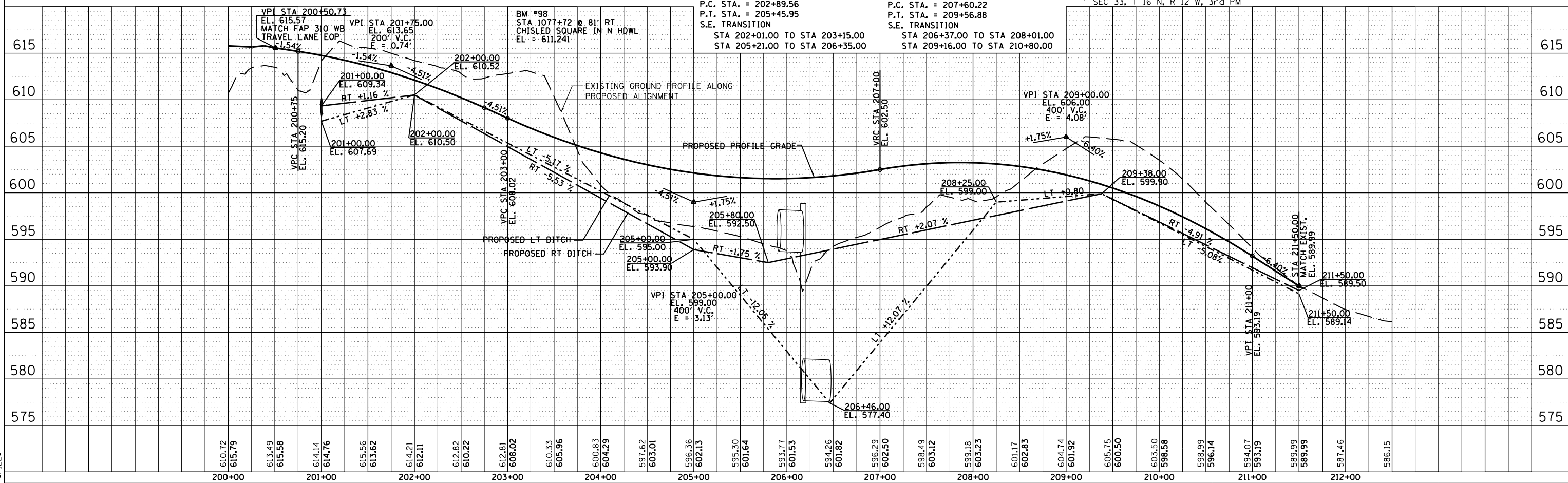
SCOTT & STEPHANIE L. COGSWELL  
 [PARCEL NO. 6084142] PR PERMANENT EASEMENT

END IMPROVEMENTS  
 STA. 211+50.00 (DUTCH LANE WEST)

157 STA. 206+05.00 SKEW= 30° LT AH  
 CAST-IN-PLACE REINF CONC END SECTION 54"Ø, STD (542206) - 2 EACH  
 PIPE CULVERT, CLASS A, TYPE 3, 54"Ø, 60'  
 USFL STA. 206+18.4, 22.9' LT, EL 578.66  
 DSFL STA. 206+48.3, 74.6' LT, EL 577.46  
 MANHOLE TYPE A, 7'Ø W TYPE 1 FRAME CLOSED LID  
 STA. 206+16.8, 20.1' LT, INV EL 578.66, TL EL 599.22  
 PIPE CULVERT, CLASS A, TYPE 2, 54"Ø, 54'  
 USFL STA. 205+88.5, 29.1' RT, EL 592.50  
 DSFL STA. 206+14.9, 16.8' LT, EL 588.26  
 D.A.=68.4 ACRES Q50=128 CFS Q100=151 CFS  
 TRENCH BACKFILL = 21.4 CU YD

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PROP. CURVE C11  
 PI STA. = 204+20.00  
 $\Delta = 26^\circ 00' 00''$  (LT)  
 $\Delta = 16^\circ 56' 39''$  (RT)  
 $D = 10^\circ 08' 27''$   
 $R = 565.00'$   
 $T = 130.44'$   
 $L = 256.39'$   
 $E = 14.86'$   
 $e = 0.04$   
 $P.C. STA. = 202+89.56$   
 $P.T. STA. = 205+45.95$

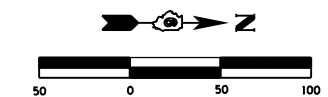
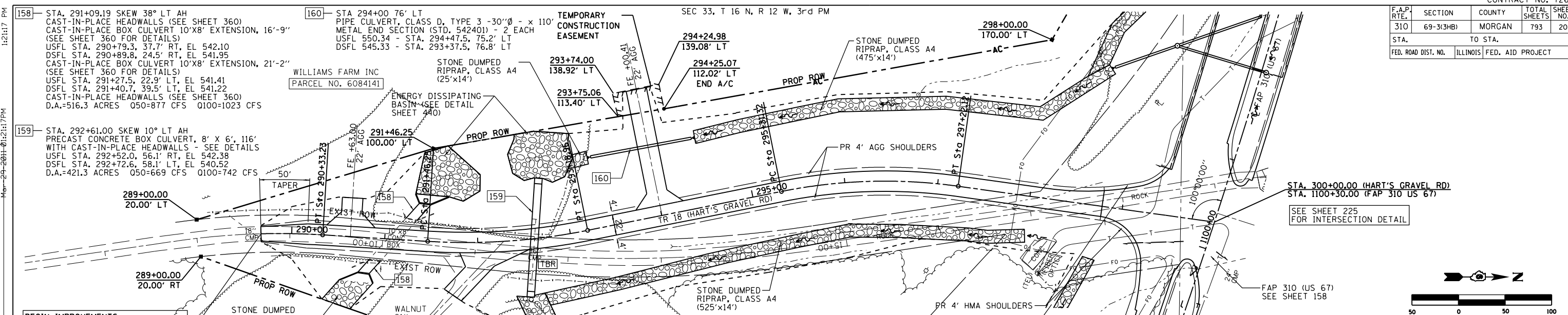
PROP. CURVE C12  
 PI STA. = 208+59.28  
 $\Delta = 16^\circ 56' 39''$  (RT)  
 $D = 8^\circ 36' 57''$   
 $R = 665.00'$   
 $T = 99.05'$   
 $L = 196.66'$   
 $E = 7.34'$   
 $e = 0.06$   
 $P.C. STA. = 207+60.22$   
 $P.T. STA. = 209+56.88$

SEC 33, T 16 N, R 12 W, 3rd PM

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	205

STA.	TO STA.
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT



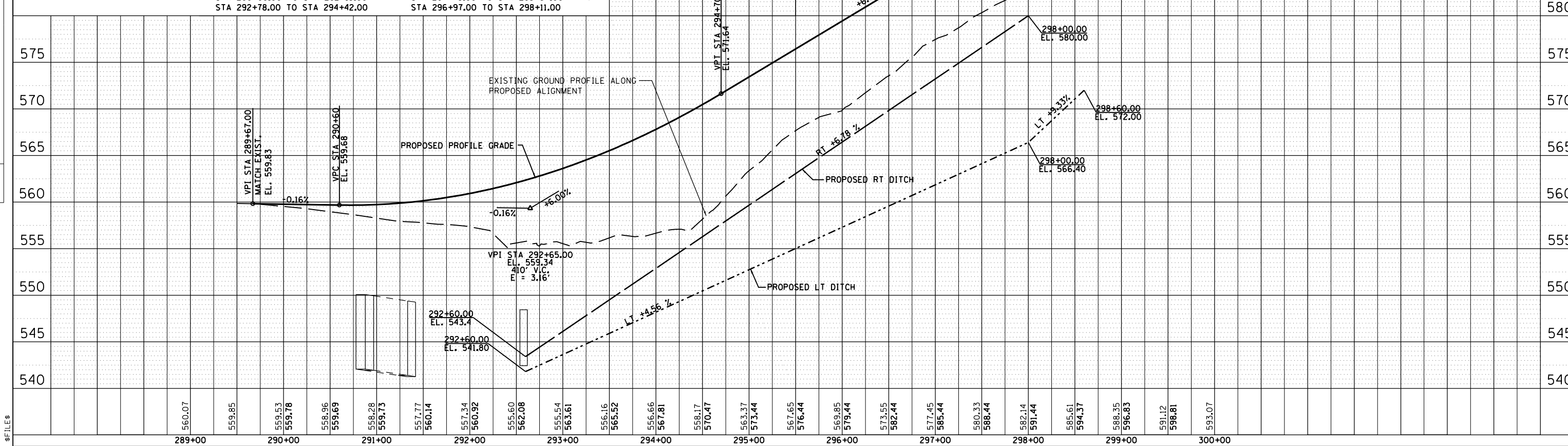
DATE: 12/17/17 PM

PLAN	DATE
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**PROP. CURVE HRT720**  
 PI STA. = 289+55.59  
 $\Delta = 11^\circ 44' 48''$  (RT)  
 $D = 7^\circ 32' 16''$   
 $R = 760.11'$   
 $T = 78.19'$   
 $L = 155.84'$   
 $E = 4.01'$   
 $e = 0.060$  (MATCH EXIST)  
 P.C. STA. = 288+77.40  
 P.T. STA. = 290+33.23  
 S.E. TRANSITION  
 STA 289+67.00 TO STA 290+90.00

**PROP. CURVE C22**  
 PI STA. = 292+33.09  
 $\Delta = 14^\circ 52' 48''$  (LT)  
 $D = 8^\circ 36' 57''$   
 $R = 665.00'$   
 $T = 86.84'$   
 $L = 172.70'$   
 $E = 5.65'$   
 $e = 0.060$   
 P.C. STA. = 291+46.25  
 P.T. STA. = 293+18.96  
 S.E. TRANSITION  
 STA 292+78.00 TO STA 294+42.00

**PROP. CURVE C25**  
 PI STA. = 296+27.63  
 $\Delta = 19^\circ 20' 54''$  (RT)  
 $D = 10^\circ 08' 27''$   
 $R = 565.00'$   
 $T = 96.32'$   
 $L = 190.80'$   
 $E = 8.15'$   
 $e = 0.040$   
 P.C. STA. = 295+31.32  
 P.T. STA. = 297+22.12  
 S.E. TRANSITION  
 STA 294+43.00 TO STA 295+57.00  
 STA 296+97.00 TO STA 298+11.00



DATE: 12/17/17 PM

PROFILE	DATE
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	206
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

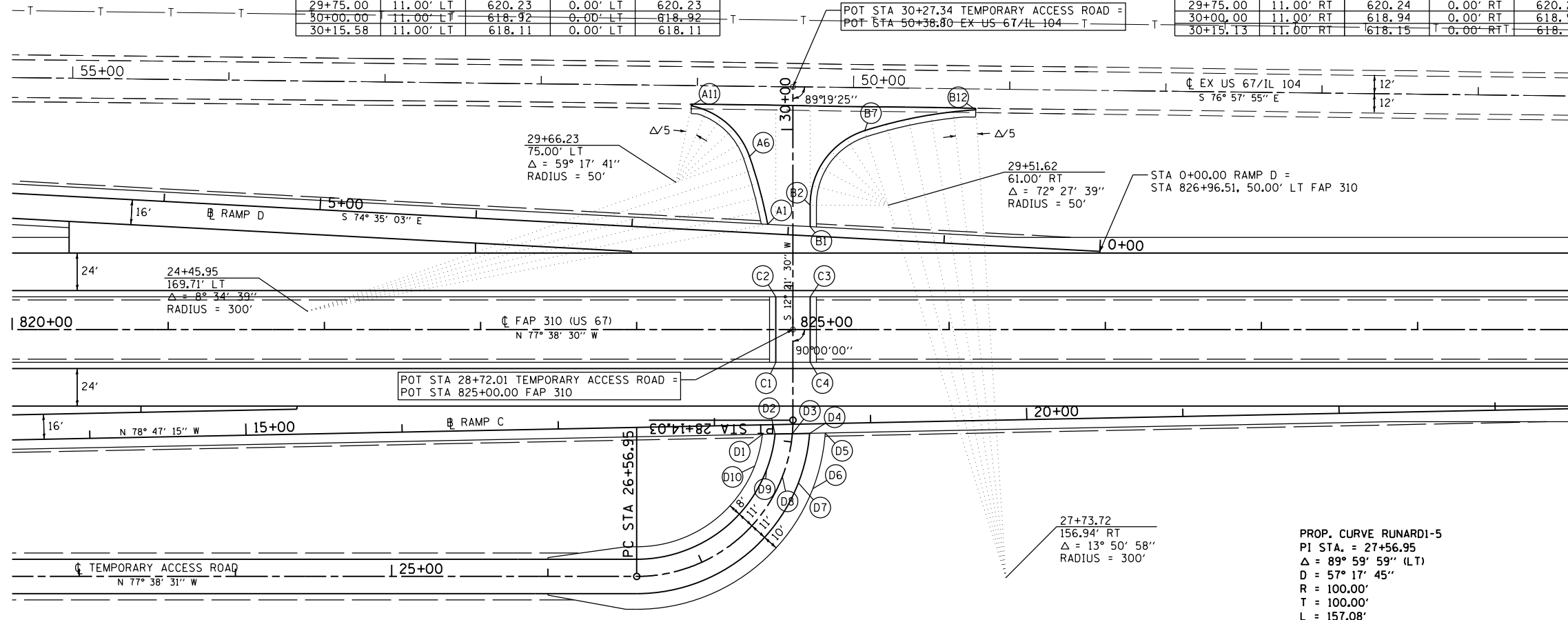
	TEMPORARY ACCESS ROAD	EX US 67/IL 104	FAP 310	ELEVATION			
A1	29+39.38	16.20' LT	50+53.97	88.15' LT	824+83.80	67.37' LT	622.09
A2	29+48.18	18.00' LT	50+55.87	79.37' LT	824+82.00	76.17' LT	621.56
A3	29+56.92	20.06' LT	50+58.03	70.66' LT	824+79.94	84.91' LT	621.02
A4	29+65.60	22.38' LT	50+60.46	62.01' LT	824+77.62	93.59' LT	620.44
A5	29+74.20	24.96' LT	50+63.14	53.43' LT	824+75.04	102.19' LT	619.83
A6	29+82.72	27.80' LT	50+66.07	44.95' LT	824+72.20	110.71' LT	619.17
A7	29+92.07	32.20' LT	50+70.58	35.65' LT	824+67.80	120.06' LT	618.43
A8	30+00.31	38.42' LT	50+76.90	27.48' LT	824+61.58	128.31' LT	617.97
A9	30+07.10	46.21' LT	50+84.77	20.78' LT	824+53.79	135.10' LT	617.86
A10	30+12.15	55.22' LT	50+93.84	15.85' LT	824+44.78	140.14' LT	617.95
A11	30+15.23	65.08' LT	51+03.74	12.88' LT	824+34.92	143.22' LT	618.04

	TEMPORARY ACCESS ROAD	EX US 67/IL 104	FAP 310	ELEVATION			
B1	29+37.92	11.00' RT	50+26.75	89.28' LT	825+11.00	65.92' LT	622.17
B2	29+51.62	11.00' RT	50+26.91	75.59' LT	825+11.00	79.61' LT	621.46
B3	29+64.13	12.59' RT	50+25.47	63.06' LT	825+12.59	92.12' LT	620.78
B4	29+75.85	17.26' RT	50+20.93	51.29' LT	825+17.26	103.84' LT	619.97
B5	29+86.02	24.72' RT	50+13.60	41.03' LT	825+24.72	114.01' LT	618.81
B6	29+94.01	34.48' RT	50+03.93	32.93' LT	825+34.48	122.00' LT	618.01
B7	29+99.29	45.93' RT	49+92.54	27.51' LT	825+45.93	127.29' LT	617.96
B8	30+03.33	59.86' RT	49+78.66	23.31' LT	825+59.86	131.32' LT	618.02
B9	30+06.69	73.97' RT	49+64.60	19.78' LT	825+73.97	134.68' LT	618.09
B10	30+09.36	88.22' RT	49+50.38	16.94' LT	825+88.22	137.35' LT	618.16
B11	30+11.34	102.59' RT	49+36.03	14.80' LT	826+02.59	139.33' LT	618.22
B12	30+12.62	117.03' RT	49+21.61	13.34' LT	826+17.03	140.61' LT	618.29

NOTE: ELEVATIONS SHOWN ARE FOR MATCHING INTO EXISTING US 67/IL 104 EDGE OF PAVEMENT.  
ADDITIONAL HOT-MIX ASPHALT WILL BE REQUIRED TO TIE INTO THE PROPOSED US 67 FRONTAGE ROAD.

STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
29+39.10	11.00' LT	622.11	0.00' LT	622.11
29+50.00	11.00' LT	621.54	0.00' LT	621.54
29+75.00	11.00' LT	620.23	0.00' LT	620.23
30+00.00	11.00' LT	618.92	0.00' LT	618.92
30+15.58	11.00' LT	618.11	0.00' LT	618.11

STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
29+37.92	11.00' RT	622.17	0.00' RT	622.16
29+50.00	11.00' RT	621.54	0.00' RT	621.54
29+75.00	11.00' RT	620.24	0.00' RT	620.23
30+00.00	11.00' RT	618.94	0.00' RT	618.92
30+15.13	11.00' RT	618.15	0.00' RT	618.14



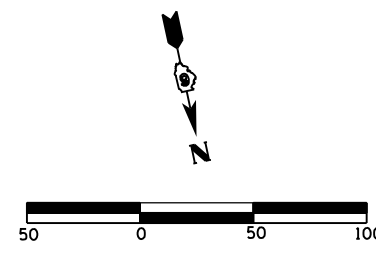
	TEMPORARY ACCESS ROAD	FAP 310	ELEVATION		
C1	28+51.01	11.00' LT	824+89.00	21.00' RT	622.37
C2	28+93.01	11.00' LT	824+89.00	21.00' LT	622.37
C3	28+93.01	11.00' RT	825+11.00	21.00' LT	622.41
C4	28+51.01	11.00' RT	825+11.00	21.00' RT	622.41

	TEMPORARY ACCESS ROAD	ELEVATION	
D1	28+02.81	19.00' LT	622.03
D2	28+04.01	11.00' LT	622.05
D3	28+05.33	0.00' RT	622.07
D4	28+06.40	11.00' RT	622.10
D5	28+07.20	21.00' RT	622.12
D6	27+77.01	21.00' RT	621.13
D7	27+77.01	11.00' RT	621.73
D8	27+77.01	0.00' RT	621.51
D9	27+77.01	11.00' LT	621.29
D10	27+77.01	19.00' LT	620.81

PROP. CURVE RUNARD1-5  
 PI STA. = 27+56.95  
 $\Delta = 89^\circ 59' 59''$  (LT)  
 D = 57' 17' 45"  
 R = 100.00'  
 T = 100.00'  
 L = 157.08'  
 E = 41.42'  
 $e = 0.02$   
 P.C. STA. = 26+56.95  
 P.T. STA. = 28+14.03  
 S.E. TRANSITION  
 STA 26+07.00 TO STA 26+67.00  
 STA 27+77.01 TO STA 28+04.01

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GEOMETRIC DETAILS  
 TEMPORARY ACCESS ROAD  
 FAP 310 (US 67/IL 104)  
 SHEET 1 OF 23  
 DATE 5/06  
 DRAWN BY EBB  
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	207
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

CONCORD ROAD & EOP ELEVATIONS

STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
7+25.45	9.00' RT	624.90	23.00' RT	624.89
7+50.00	9.00' RT	624.66	23.00' RT	624.57
7+75.00	9.00' RT	624.46	23.00' RT	624.28
7+85.45	9.00' RT	624.39	23.00' RT	624.18

CONCORD ROAD & EOP ELEVATIONS

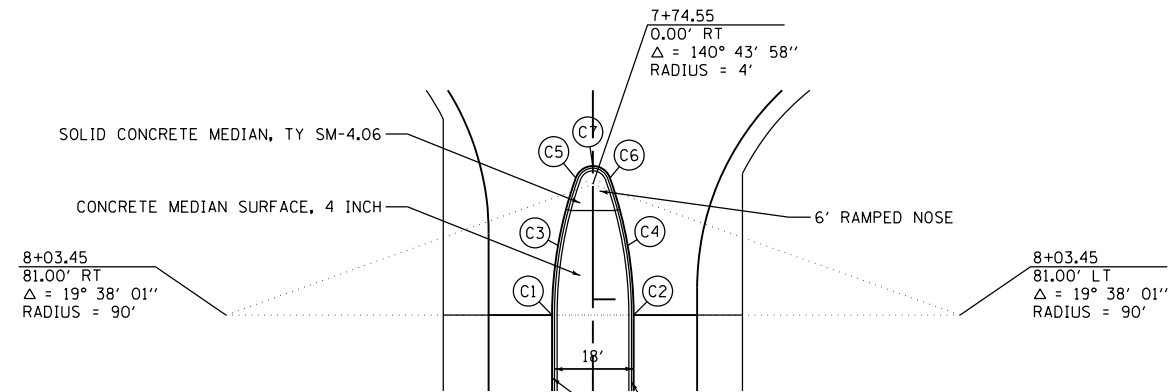
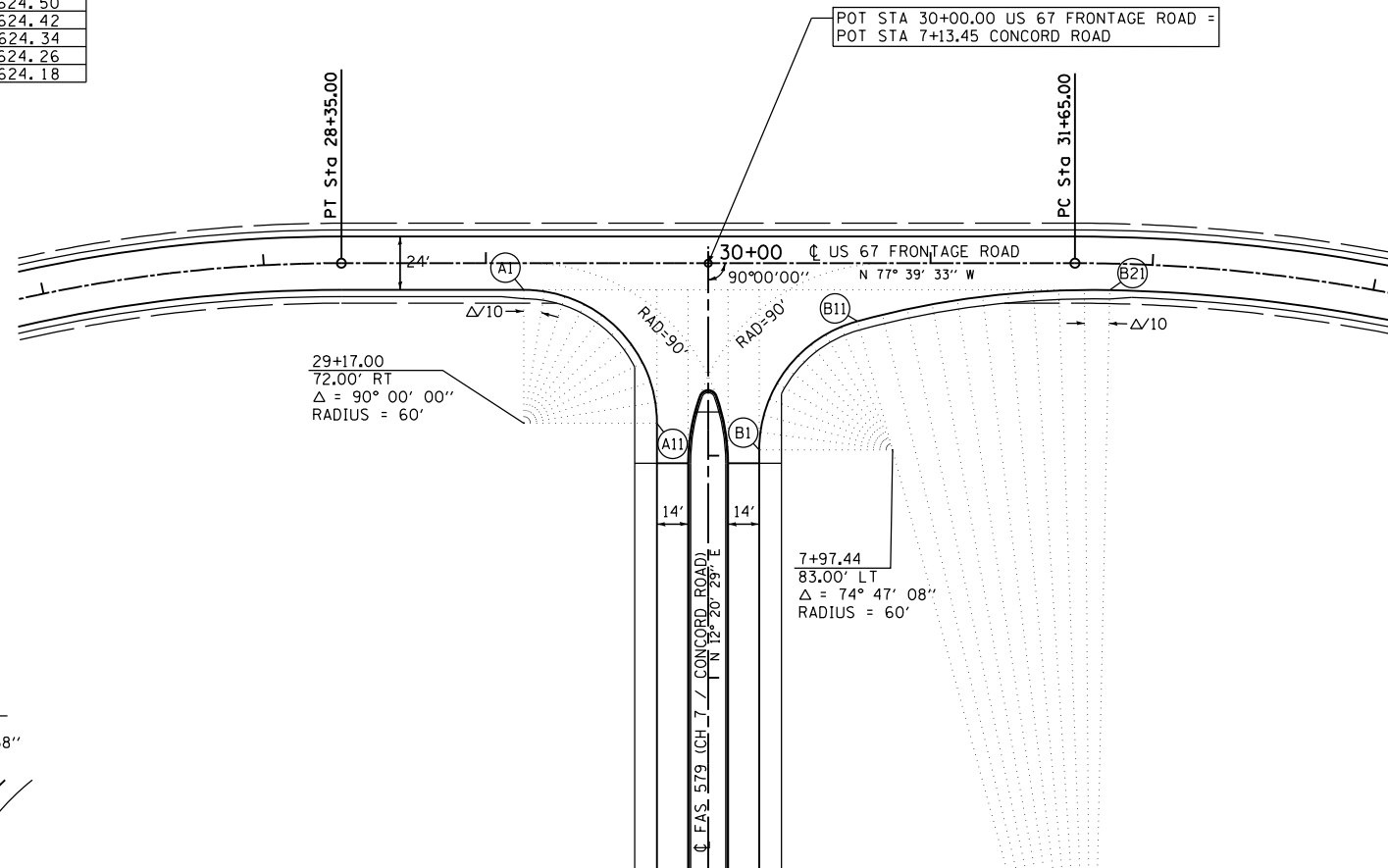
STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
7+25.45	9.00' LT	624.90	23.00' LT	624.89
7+50.00	9.00' LT	624.66	23.00' LT	624.58
7+75.00	9.00' LT	624.46	23.00' LT	624.31
7+97.44	9.00' LT	624.32	23.00' LT	624.11

	US 67 FRONTAGE ROAD		CONCORD ROAD		ELEVATION
B1	30+23.00	83.99' RT	7+97.44	23.00' LT	624.11
B2	30+23.51	76.18' RT	7+89.63	23.51' LT	624.16
B3	30+25.03	68.51' RT	7+81.96	25.03' LT	624.21
B4	30+27.54	61.09' RT	7+74.54	27.54' LT	624.26
B5	30+30.99	54.07' RT	7+67.52	30.99' LT	624.31
B6	30+35.33	47.56' RT	7+61.00	35.33' LT	624.36
B7	30+40.48	41.66' RT	7+55.11	40.48' LT	624.41
B8	30+46.35	36.49' RT	7+49.94	46.35' LT	624.46
B9	30+52.84	32.12' RT	7+45.57	52.84' LT	624.50
B10	30+59.85	28.64' RT	7+42.09	59.85' LT	624.55
B11	30+67.26	26.10' RT	7+39.54	67.26' LT	624.59
B12	30+78.36	23.25' RT	7+36.69	78.36' LT	624.59
B13	30+89.54	20.72' RT	7+34.16	89.54' LT	624.52
B14	31+00.79	18.51' RT	7+31.96	100.79' LT	624.43
B15	31+12.09	16.62' RT	7+30.07	112.09' LT	624.33
B16	31+23.45	15.06' RT	7+28.51	123.45' LT	624.23
B17	31+34.85	13.83' RT	7+27.27	134.84' LT	624.13
B18	31+46.27	12.92' RT	7+26.37	146.27' LT	624.03
B19	31+57.72	12.34' RT	7+25.79	157.72' LT	623.90
B20	31+69.25	12.08' RT	7+25.54	169.18' LT	623.77
B21	31+80.90	12.00' RT	7+25.61	180.65' LT	623.63

	US 67 FRONTAGE ROAD		CONCORD ROAD		ELEVATION
A1	29+17.00	12.00' RT	7+25.45	83.00' LT	624.73
A2	29+26.39	12.74' RT	7+26.19	73.61' LT	624.77
A3	29+35.54	14.94' RT	7+28.39	64.46' LT	624.78
A4	29+44.24	18.54' RT	7+31.99	55.76' LT	624.74
A5	29+52.27	23.46' RT	7+36.91	47.73' LT	624.66
A6	29+59.43	29.57' RT	7+43.02	40.57' LT	624.58
A7	29+65.54	36.73' RT	7+50.18	34.46' LT	624.50
A8	29+70.46	44.76' RT	7+58.21	29.54' LT	624.42
A9	29+74.06	53.46' RT	7+66.91	25.94' LT	624.34
A10	29+76.26	62.61' RT	7+76.06	23.74' LT	624.26
A11	29+77.00	72.00' RT	7+85.45	23.00' LT	624.18

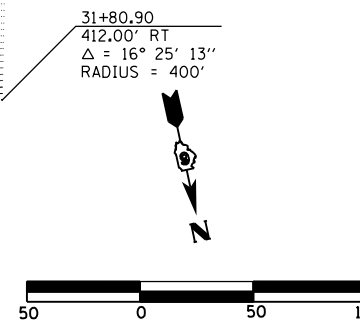
PROP. CURVE CON-FR-2  
 PI STA. = 24+93.02  
 $\Delta = 69^\circ 15' 10''$  (RT)  
 $D = 8^\circ 40' 52''$   
 $R = 660.00'$   
 $T = 455.75'$   
 $L = 797.73'$   
 $E = 142.07'$   
 $e = 0.080$   
 P.C. STA. = 20+37.26  
 P.T. STA. = 28+35.00  
 S.E. TRANSITION  
 STA 18+93.00 TO STA 20+71.00  
 STA 27+76.00 TO STA 29+88.00

PROP. CURVE CON-FR-3  
 PI STA. = 36+54.30  
 $\Delta = 65^\circ 53' 36''$  (RT)  
 $D = 7^\circ 35' 20''$   
 $R = 755.00'$   
 $T = 489.30'$   
 $L = 868.29'$   
 $E = 144.69'$   
 $e = 0.080$   
 P.C. STA. = 31+65.00  
 P.T. STA. = 40+33.29  
 S.E. TRANSITION  
 STA 30+12.00 TO STA 32+24.00  
 STA 39+85.00 TO STA 41+63.00



	US 67 FRONTAGE ROAD		ELEVATION
C1	8+03.45	9.00' RT	624.29
C2	8+03.45	9.00' LT	624.29
C3	7+88.10	7.68' RT	624.37
C4	7+88.10	7.68' LT	624.37
C5	7+73.21	3.77' RT	624.47
C6	7+73.21	3.77' LT	624.47
C7	7+70.55	0.00' RT	624.49

MEDIAN NOSE DETAIL



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GEOMETRIC DETAILS  
 INTERSECTION OF CONCORD ROAD  
 AND US 67 FRONTAGE ROAD  
 FAP 310 (US 67/IL 104)  
 SHEET 2 OF 23

DATE 5/06  
 DRAWN BY EBB  
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	208
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

	CONCORD ROAD	RAMP D	ELEVATION
A1	13+40.74	23.00' LT	631.61
A2	13+51.51	23.29' LT	631.86
A3	13+62.25	24.16' LT	632.10
A4	13+72.93	25.61' LT	632.35
A5	13+83.51	27.63' LT	632.59
A6	13+93.97	30.21' LT	632.84
A7	14+04.28	33.36' LT	633.08
A8	14+14.40	37.06' LT	633.31
A9	14+24.30	41.29' LT	633.48
A10	14+33.97	46.06' LT	633.60
A11	14+43.36	51.33' LT	633.66
A12	14+53.41	58.44' LT	633.65
A13	14+62.25	67.01' LT	633.58
A14	14+69.67	76.84' LT	633.43
A15	14+75.50	87.68' LT	633.23
A16	14+79.59	99.29' LT	633.03

PROP. CURVE COND-2  
 PI STA. = 17+30.96  
 $\Delta = 14^\circ 13' 15''$  (LT)  
 $D = 8^\circ 40' 52''$   
 $R = 660.00'$   
 $T = 82.33'$   
 $L = 163.81'$   
 $E = 5.12'$   
 $e = 0.058$   
 P.C. STA. = 16+48.63  
 P.T. STA. = 18+12.45  
**S.E. TRANSITION**  
 STA 15+69.00 TO STA 16+88.00  
 STA 17+73.00 TO STA 18+92.00

	CONCORD ROAD	RAMP D	ELEVATION
B1	14+97.49	104.29' LT	632.61
B2	14+95.66	95.84' LT	632.94
B3	14+94.64	87.24' LT	633.28
B4	14+94.46	78.59' LT	633.62
B5	14+95.11	69.96' LT	633.96
B6	14+96.58	61.43' LT	634.29
B7	14+98.87	53.08' LT	634.63
B8	15+01.94	44.99' LT	634.97
B9	15+05.78	37.23' LT	635.31
B10	15+10.35	29.88' LT	635.64
B11	15+15.61	23.00' LT	635.98

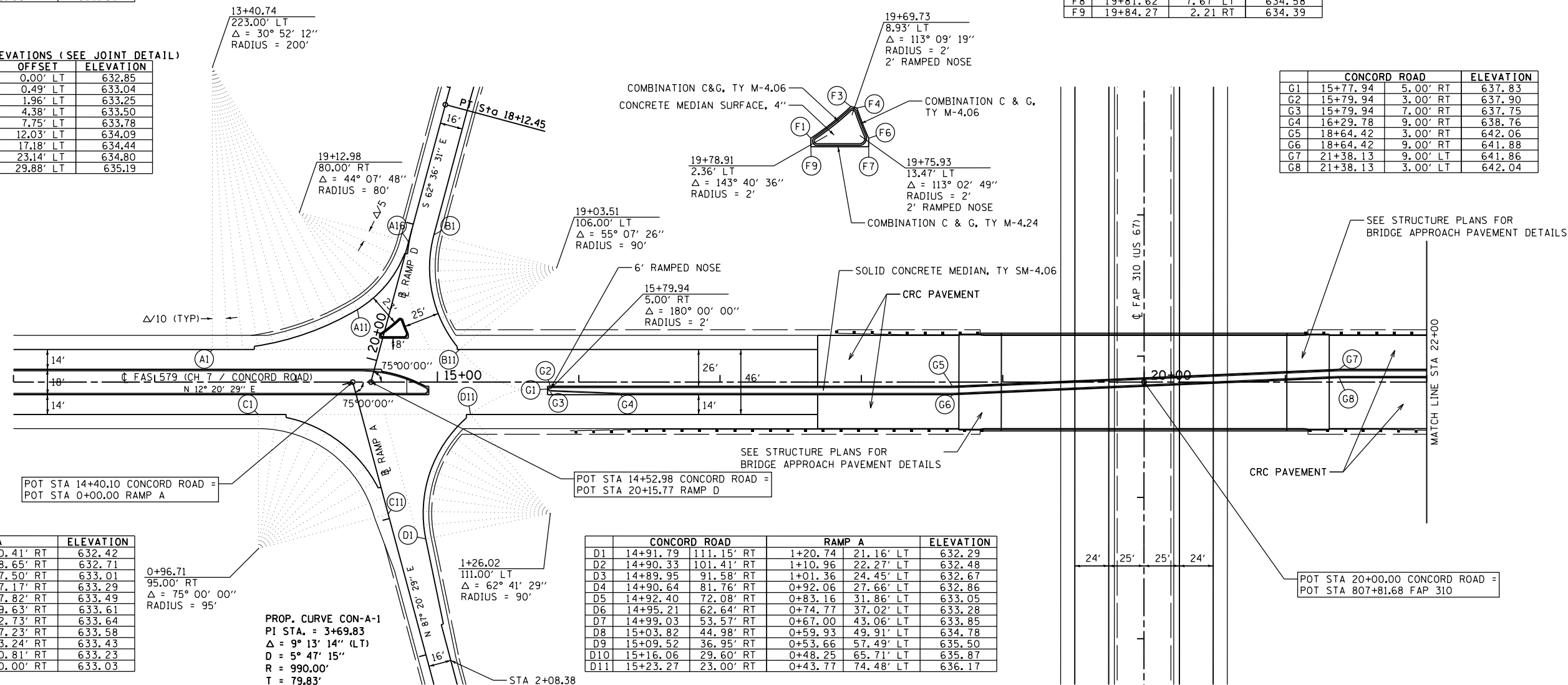
	RAMP D	ELEVATION	
F1	19+81.37	2.99' RT	634.30
F2	19+74.33	2.73' LT	634.30
F3	19+66.82	7.81' LT	634.29
F4	19+66.82	9.99' LT	634.39
F5	19+71.53	13.27' LT	634.54
F6	19+76.08	16.78' LT	634.70
F7	19+79.82	14.39' LT	634.75
F8	19+81.62	7.67' LT	634.58
F9	19+84.27	2.21' RT	634.39

	CONCORD ROAD	ELEVATION	
G1	15+77.94	5.00' RT	637.83
G2	15+79.94	3.00' RT	637.90
G3	15+79.94	7.00' RT	637.75
G4	16+29.78	9.00' RT	638.76
G5	18+64.42	3.00' RT	642.06
G6	18+64.42	9.00' RT	641.88
G7	21+38.13	9.00' LT	641.86
G8	21+38.13	3.00' LT	642.04

RAMP D CNTRL ELEVATIONS (SEE JOINT DETAIL)

STATION	OFFSET	ELEVATION
19+03.51	0.00' LT	632.85
19+13.70	0.49' LT	633.04
19+23.78	1.96' LT	633.25
19+33.68	4.38' LT	633.50
19+43.31	7.75' LT	633.78
19+52.56	12.03' LT	634.09
19+61.36	17.18' LT	634.44
19+69.63	23.14' LT	634.80
19+77.28	29.88' LT	635.19

ISLAND DETAIL



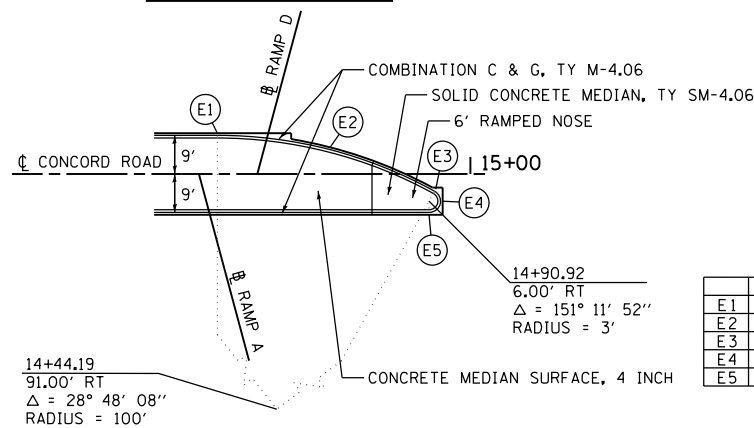
	CONCORD ROAD	RAMP A	ELEVATION		
C1	13+73.37	23.00' RT	0+04.94	70.41' RT	632.42
C2	13+85.77	23.81' RT	0+08.94	58.65' RT	632.71
C3	13+97.96	26.24' RT	0+14.43	47.50' RT	633.01
C4	14+09.73	30.23' RT	0+21.34	37.17' RT	633.29
C5	14+20.87	35.73' RT	0+29.53	27.82' RT	633.49
C6	14+31.20	42.63' RT	0+38.87	19.63' RT	633.61
C7	14+40.55	50.83' RT	0+49.21	12.73' RT	633.64
C8	14+48.74	60.17' RT	0+60.35	7.23' RT	633.58
C9	14+55.64	70.50' RT	0+72.12	3.24' RT	633.43
C10	14+61.14	81.65' RT	0+84.31	0.81' RT	633.23
C11	14+65.13	93.41' RT	0+96.71	0.00' RT	633.03

PROP. CURVE CON-A-1  
 PI STA. = 3+69.83  
 $\Delta = 9^\circ 13' 14''$  (LT)  
 $D = 5^\circ 47' 15''$   
 $R = 990.00'$   
 $T = 79.83'$   
 $L = 159.32'$   
 $E = 3.21'$   
 $e = 0.076$   
 P.C. STA. = 2+90.00  
 P.T. STA. = 4+49.32  
**S.E. TRANSITION**  
 STA 1+59.00 TO STA 3+55.00  
 STA 3+70.00 TO STA 6+62.00

	CONCORD ROAD	RAMP A	ELEVATION		
D1	14+91.79	111.15' RT	1+20.74	21.16' LT	632.29
D2	14+90.33	101.41' RT	1+10.96	22.27' LT	632.48
D3	14+89.95	91.58' RT	1+01.36	24.45' LT	632.67
D4	14+90.64	81.76' RT	0+92.06	27.66' LT	632.86
D5	14+92.40	72.08' RT	0+83.16	31.86' LT	633.05
D6	14+95.21	62.64' RT	0+74.77	37.02' LT	633.28
D7	14+99.03	53.57' RT	0+67.00	43.06' LT	633.85
D8	15+03.82	44.98' RT	0+59.93	49.91' LT	634.78
D9	15+09.52	36.95' RT	0+53.66	57.49' LT	635.50
D10	15+16.06	29.60' RT	0+48.25	65.71' LT	635.87
D11	15+23.27	23.00' RT	0+43.77	74.48' LT	636.17

RAMP A B & EOP ELEVATIONS				
STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
0+28.10	0.00' LT	634.18	16.00' LT	634.66
0+50.00	0.00' LT	633.81	16.00' LT	634.13
0+75.00	0.00' LT	633.39	16.00' LT	633.51
1+00.00	0.00' LT	632.98	16.00' LT	632.90
1+20.74	0.00' LT	632.63	16.00' LT	632.39

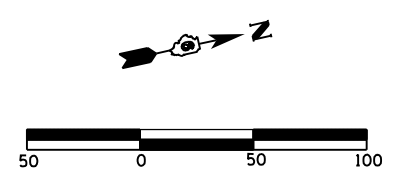
MEDIAN DETAIL



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GEOMETRIC DETAILS  
 INTERSECTION OF CONCORD ROAD  
 RAMP A AND RAMP D  
 FAP 310 (US 67/IL 104)  
 SHEET 3 OF 23

DATE 5/06  
 DRAWN BY EBB  
 CHECKED BY





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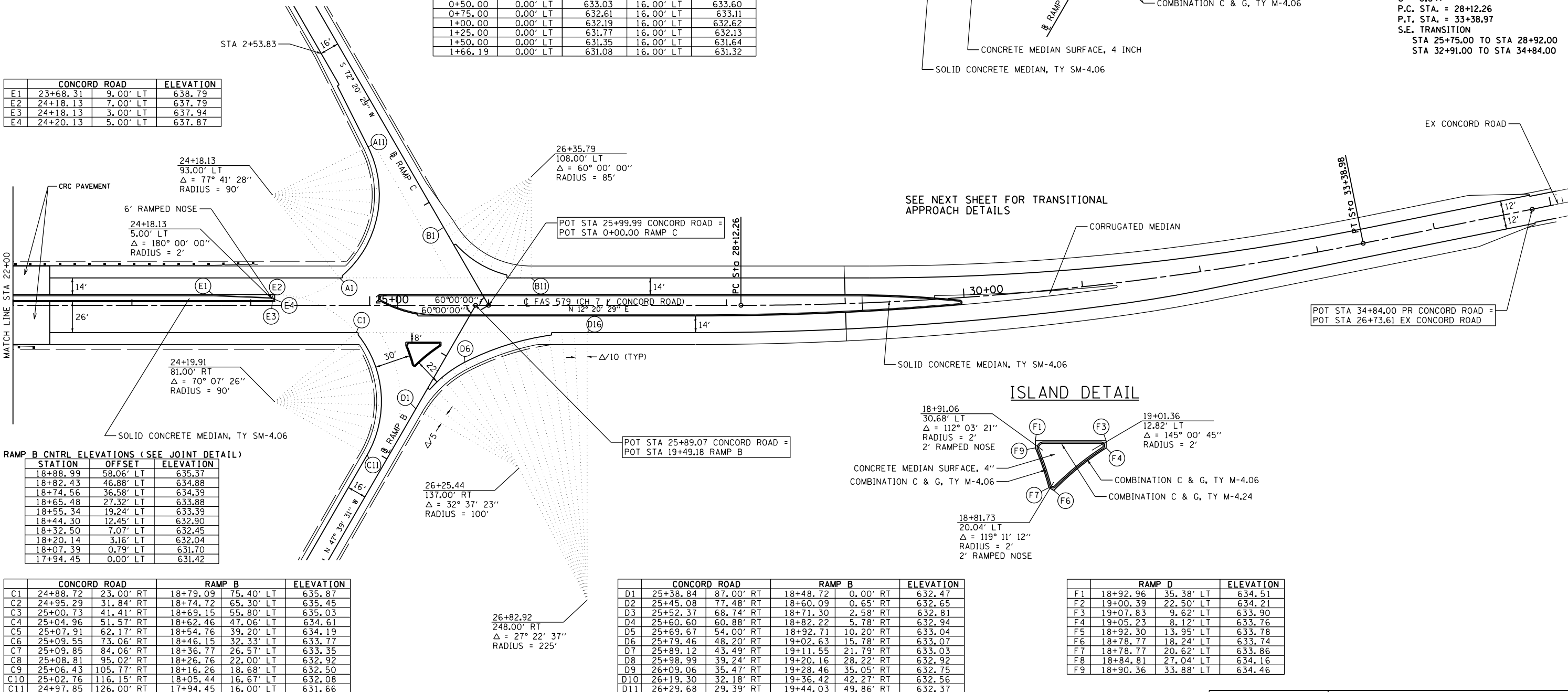
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	CONCORD ROAD		RAMP C		ELEVATION
A1	24+74.70	23.00' LT	0+82.57	97.01' LT	636.22
A2	24+83.64	31.29' LT	0+85.27	85.12' LT	635.81
A3	24+91.38	40.71' LT	0+89.56	73.71' LT	634.99
A4	24+97.78	51.09' LT	0+95.36	62.98' LT	633.85
A5	25+02.71	62.25' LT	1+02.55	53.13' LT	632.97
A6	25+06.09	73.96' LT	1+11.00	44.34' LT	632.45
A7	25+07.86	86.03' LT	1+20.57	36.78' LT	632.22
A8	25+07.98	98.22' LT	1+31.07	30.58' LT	632.02
A9	25+06.45	110.32' LT	1+42.31	25.86' LT	631.81
A10	25+03.29	122.10' LT	1+54.09	22.69' LT	631.61
A11	24+98.58	133.35' LT	1+66.19	21.16' LT	631.40

	CONCORD ROAD		RAMP C		ELEVATION
B1	25+62.18	65.50' LT	0+75.63	0.00' RT	632.60
B2	25+67.02	58.04' LT	0+66.75	0.47' RT	632.74
B3	25+72.62	51.12' LT	0+57.96	1.86' RT	632.88
B4	25+78.91	44.83' LT	0+49.37	4.16' RT	633.01
B5	25+85.83	39.23' LT	0+41.06	7.35' RT	633.09
B6	25+93.29	34.39' LT	0+33.13	11.39' RT	633.08
B7	26+01.22	30.35' LT	0+25.67	16.23' RT	632.99
B8	26+09.52	27.16' LT	0+18.76	21.83' RT	632.83
B9	26+18.12	24.86' LT	0+12.47	28.12' RT	632.62
B10	26+26.90	23.47' LT	0+06.87	35.04' RT	632.41
B11	26+35.79	23.00' LT	0+02.02	42.50' RT	632.20

RAMP C @ EOP ELEVATIONS				
STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
0+35.80	0.00' LT	633.27	16.00' LT	633.88
0+50.00	0.00' LT	633.03	16.00' LT	633.60
0+75.00	0.00' LT	632.61	16.00' LT	633.11
1+00.00	0.00' LT	632.19	16.00' LT	632.62
1+25.00	0.00' LT	631.77	16.00' LT	632.13
1+50.00	0.00' LT	631.35	16.00' LT	631.64
1+66.19	0.00' LT	631.08	16.00' LT	631.32

CONCORD ROAD		ELEVATION
E1	23+68.31	9.00' LT 638.79
E2	24+18.13	7.00' LT 637.79
E3	24+18.13	3.00' LT 637.94
E4	24+20.13	5.00' LT 637.87

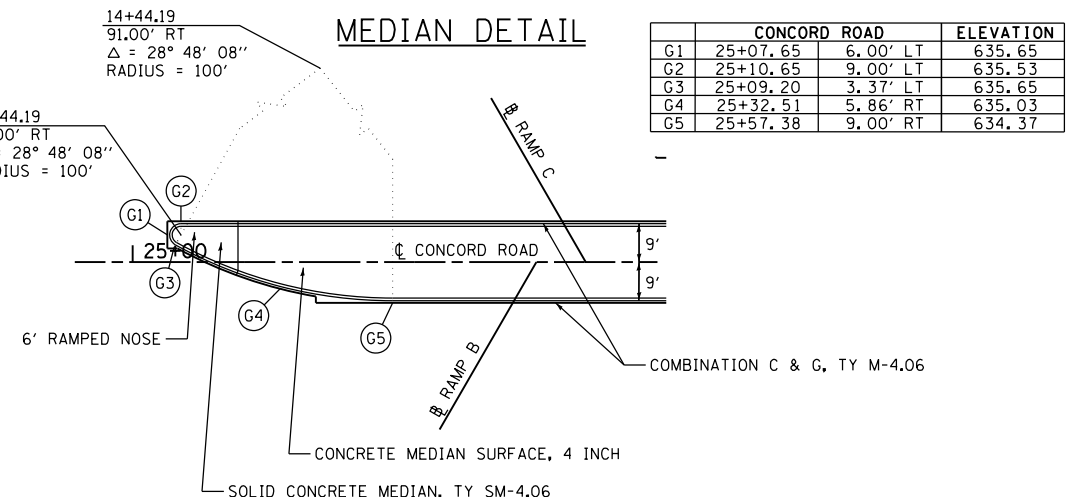


RAMP B CNTRL ELEVATIONS (SEE JOINT DETAIL)		
STATION	OFFSET	ELEVATION
18+88.99	58.06' LT	635.37
18+82.43	46.88' LT	634.88
18+74.56	36.58' LT	634.39
18+65.48	27.32' LT	633.88
18+55.34	19.24' LT	633.39
18+44.30	12.45' LT	632.90
18+32.50	7.07' LT	632.45
18+20.14	3.16' LT	632.04
18+07.39	0.79' LT	631.70
17+94.45	0.00' LT	631.42

	CONCORD ROAD		RAMP B		ELEVATION
C1	24+88.72	23.00' RT	18+79.09	75.40' LT	635.87
C2	24+95.29	31.84' RT	18+74.72	65.30' LT	635.45
C3	25+00.73	41.41' RT	18+69.15	55.80' LT	635.03
C4	25+04.96	51.57' RT	18+62.46	47.06' LT	634.61
C5	25+07.91	62.17' RT	18+54.76	39.20' LT	634.19
C6	25+09.55	73.06' RT	18+46.15	32.33' LT	633.77
C7	25+09.85	84.06' RT	18+36.77	26.57' LT	633.35
C8	25+08.81	95.02' RT	18+26.76	22.00' LT	632.92
C9	25+06.43	105.77' RT	18+16.26	18.68' LT	632.50
C10	25+02.76	116.15' RT	18+05.44	16.67' LT	632.08
C11	24+97.85	126.00' RT	17+94.45	16.00' LT	631.66

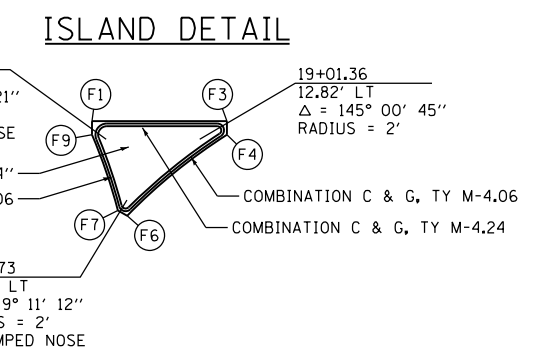
	CONCORD ROAD		RAMP B		ELEVATION
D1	25+38.84	87.00' RT	18+48.72	0.00' RT	632.47
D2	25+45.08	77.48' RT	18+60.09	0.65' RT	632.65
D3	25+52.37	68.74' RT	18+71.30	2.58' RT	632.81
D4	25+60.60	60.88' RT	18+82.22	5.78' RT	632.94
D5	25+69.67	54.00' RT	18+92.71	10.20' RT	633.04
D6	25+79.46	48.20' RT	19+02.63	15.78' RT	633.07
D7	25+89.12	43.49' RT	19+11.55	21.79' RT	633.03
D8	25+98.99	39.24' RT	19+20.16	28.22' RT	632.92
D9	26+09.06	35.47' RT	19+28.46	35.05' RT	632.75
D10	26+19.30	32.18' RT	19+36.42	42.27' RT	632.56
D11	26+29.68	29.39' RT	19+44.03	49.86' RT	632.37
D12	26+40.18	27.10' RT			632.18
D13	26+50.78	25.31' RT			631.99
D14	26+61.45	24.03' RT			631.79
D15	26+72.18	23.26' RT			631.59
D16	26+82.92	23.00' RT			631.37

	RAMP D		ELEVATION
F1	18+92.96	35.38' LT	634.51
F2	19+00.39	22.50' LT	634.21
F3	19+07.83	9.62' LT	633.90
F4	19+05.23	8.12' LT	633.76
F5	18+92.30	13.95' LT	633.78
F6	18+78.77	18.24' LT	633.74
F7	18+78.77	20.62' LT	633.86
F8	18+84.81	27.04' LT	634.16
F9	18+90.36	33.88' LT	634.46



**PROP. CURVE CONCORD-1**  
PI STA. = 30+76.49  
Δ = 11° 23' 17" (LT)  
D = 2° 09' 44"  
R = 2,650.00'  
T = 264.23'  
L = 526.71'  
E = 13.14'  
e = 0.047  
P.C. STA. = 28+12.26  
P.T. STA. = 33+38.97  
**S.E. TRANSITION**  
STA 25+75.00 TO STA 28+92.00  
STA 32+91.00 TO STA 34+84.00

SEE NEXT SHEET FOR TRANSITIONAL APPROACH DETAILS



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	209

STA. TO STA.  
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

ILLINOIS DEPARTMENT OF TRANSPORTATION  
GEOMETRIC DETAILS  
INTERSECTION OF CONCORD ROAD  
RAMP B AND RAMP C  
FAP 310 (US 67/IL 104)  
SHEET 4 OF 23

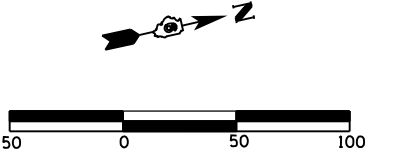
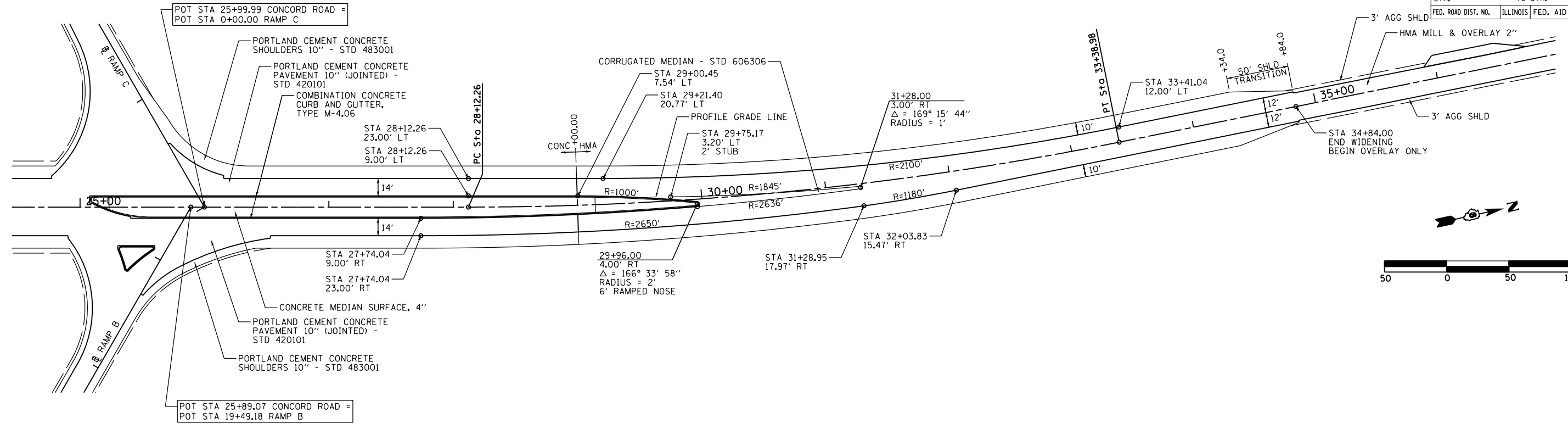
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DATE 5/06

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	210
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



CONCORD ROAD STATION	OFFSET TO LEFT EDGE & ELEVATION						OFFSET TO RIGHT EDGE & ELEVATION					
	OUTSIDE PAVEMENT		C&G TY M-4.06		CORRUGATED MEDIAN		C&G TY M-4.06		CORRUGATED MEDIAN		OUTSIDE PAVEMENT	
	OFFSET	ELEVATION	OFFSET	ELEVATION	OFFSET	ELEVATION	OFFSET	ELEVATION	OFFSET	ELEVATION	OFFSET	ELEVATION
27+70.00	23.00' LT	628.33	9.00' LT	628.65	-	-	9.00' RT	629.07	-	-	23.00' RT	629.39
27+75.00	23.00' LT	628.16	9.00' LT	628.50	-	-	9.00' RT	628.93	-	-	23.00' RT	629.27
27+80.00	23.00' LT	627.99	9.00' LT	628.34	-	-	8.99' RT	628.79	-	-	22.99' RT	629.14
27+85.00	23.00' LT	627.82	9.00' LT	628.19	-	-	8.98' RT	628.65	-	-	22.98' RT	629.02
27+90.00	23.00' LT	627.65	9.00' LT	628.03	-	-	8.95' RT	628.51	-	-	22.95' RT	628.89
27+95.00	23.00' LT	627.48	9.00' LT	627.87	-	-	8.92' RT	628.37	-	-	22.92' RT	628.76
28+00.00	23.00' LT	627.30	9.00' LT	627.71	-	-	8.87' RT	628.23	-	-	22.87' RT	628.63
28+05.00	23.00' LT	627.13	9.00' LT	627.55	-	-	8.82' RT	628.08	-	-	22.82' RT	628.50
28+10.00	23.00' LT	626.95	9.00' LT	627.38	-	-	8.75' RT	627.93	-	-	22.76' RT	628.37
28+15.00	23.00' LT	626.77	9.00' LT	627.22	-	-	8.68' RT	627.79	-	-	22.68' RT	628.23
28+20.00	22.99' LT	626.60	8.99' LT	627.06	-	-	8.61' RT	627.63	-	-	22.61' RT	628.10
28+25.00	22.97' LT	626.42	8.97' LT	626.89	-	-	8.54' RT	627.48	-	-	22.54' RT	627.96
28+30.00	22.94' LT	626.24	8.94' LT	626.72	-	-	8.46' RT	627.33	-	-	22.47' RT	627.82
28+35.00	22.90' LT	626.05	8.90' LT	626.56	-	-	8.39' RT	627.18	-	-	22.39' RT	627.68
28+40.00	22.86' LT	625.87	8.86' LT	626.39	-	-	8.32' RT	627.02	-	-	22.32' RT	627.53
28+45.00	22.80' LT	625.69	8.80' LT	626.22	-	-	8.24' RT	626.86	-	-	22.25' RT	627.39
28+50.00	22.73' LT	625.50	8.73' LT	626.05	-	-	8.17' RT	626.70	-	-	22.17' RT	627.24
28+55.00	22.66' LT	625.32	8.66' LT	625.87	-	-	8.10' RT	626.54	-	-	22.10' RT	627.10
28+60.00	22.57' LT	625.13	8.57' LT	625.70	-	-	8.02' RT	626.38	-	-	22.03' RT	626.95
28+65.00	22.48' LT	624.94	8.48' LT	625.53	-	-	7.95' RT	626.21	-	-	21.95' RT	626.79
28+70.00	22.38' LT	624.75	8.37' LT	625.35	-	-	7.88' RT	626.04	-	-	21.88' RT	626.64
28+75.00	22.26' LT	624.57	8.26' LT	625.18	-	-	7.80' RT	625.88	-	-	21.81' RT	626.49
28+80.00	22.14' LT	624.38	8.14' LT	625.01	-	-	7.73' RT	625.72	-	-	21.73' RT	626.34
28+85.00	22.01' LT	624.20	8.00' LT	624.84	-	-	7.66' RT	625.55	-	-	21.66' RT	626.19
28+90.00	21.87' LT	624.02	7.86' LT	624.67	-	-	7.58' RT	625.39	-	-	21.58' RT	626.04
28+95.00	21.72' LT	623.84	7.71' LT	624.50	-	-	7.51' RT	625.22	-	-	21.51' RT	625.88
29+00.00	21.56' LT	623.68	7.55' LT	624.34	-	-	7.43' RT	625.04	-	-	21.44' RT	625.70
29+05.00	21.39' LT	623.52	7.37' LT	624.18	-	-	7.36' RT	624.87	-	-	21.36' RT	625.53
29+10.00	21.21' LT	623.35	7.16' LT	624.01	-	-	7.29' RT	624.69	-	-	21.29' RT	625.35
29+15.00	21.02' LT	623.19	6.91' LT	623.85	-	-	7.21' RT	624.52	-	-	21.21' RT	625.17
29+20.00	20.83' LT	623.03	6.63' LT	623.69	-	-	7.14' RT	624.34	-	-	21.14' RT	625.00
29+25.00	20.62' LT	622.86	6.31' LT	623.54	-	-	7.06' RT	624.17	-	-	21.06' RT	624.82
29+30.00	20.42' LT	622.70	5.96' LT	623.38	-	-	6.99' RT	623.99	-	-	20.99' RT	624.65
29+35.00	20.22' LT	622.54	5.57' LT	623.23	-	-	6.91' RT	623.82	-	-	20.92' RT	624.47
29+40.00	20.03' LT	622.38	5.15' LT	623.08	-	-	6.84' RT	623.64	-	-	20.84' RT	624.30
29+45.00	19.83' LT	622.21	4.69' LT	622.93	-	-	6.76' RT	623.46	-	-	20.77' RT	624.12
29+50.00	19.64' LT	622.06	4.20' LT	622.79	4.43' LT X	622.78	6.69' RT	623.30	-	-	20.69' RT	623.96
29+55.00	19.45' LT	621.91	3.67' LT	622.65	4.18' LT X	622.63	6.61' RT	623.14	-	-	20.62' RT	623.79
29+60.00	19.26' LT	621.76	3.11' LT	622.52	3.93' LT X	622.48	6.54' RT	622.97	-	-	20.54' RT	623.63
29+65.00	19.08' LT	621.61	2.52' LT	622.39	3.69' LT X	622.34	6.46' RT	622.82	-	-	20.47' RT	623.47
29+70.00	18.89' LT	621.47	1.88' LT	622.27	3.45' LT X	622.20	6.39' RT	622.66	-	-	20.39' RT	623.32
29+75.00	18.71' LT	621.33	1.22' LT	622.15	3.21' LT X	622.05	6.31' RT	622.50	-	-	20.32' RT	623.16
29+80.00	18.54' LT	621.18	0.51' LT	622.03	2.98' LT X	621.91	6.24' RT	622.35	-	-	20.24' RT	623.01
29+85.00	18.36' LT	621.04	0.22' RT	621.92	2.75' LT X	621.78	6.16' RT	622.19	-	-	20.16' RT	622.85
29+90.00	18.19' LT	620.90	1.00' RT	621.80	2.53' LT X	621.64	6.09' RT	622.04	-	-	20.09' RT	622.70
29+95.00	18.02' LT	620.77	1.81' RT	621.70	2.31' LT X	621.50	6.01' RT	621.90	-	-	20.01' RT	622.55
30+00.00	17.85' LT	620.63	-	-	2.10' LT X	621.37	-	-	5.94' RT	621.75	19.94' RT	622.41

TABLE CONTINUED ON NEXT SHEET

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GEOMETRIC DETAILS  
 CONCORD ROAD  
 TRANSITIONAL APPROACH  
 FAP 310 (US 67/IL 104)  
 SHEET 5 OF 23

DATE 9/09  
 DRAWN BY SEB  
 CHECKED BY



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	212
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

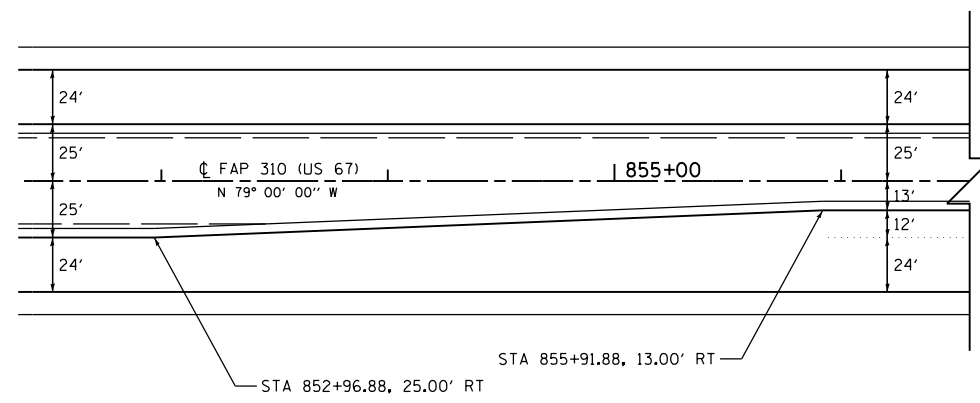
Mar-30-2011 08:45:39AM

	EX US 67/IL 104		SERVICE DRIVE 81		ELEVATION
A1	16+23.99	143.16' LT	580+64.24	82.39' LT	620.00
A2	16+13.59	142.06' LT	580+63.33	71.97' LT	619.95
A3	16+03.55	139.16' LT	580+64.25	61.56' LT	619.80
A4	15+94.16	134.56' LT	580+66.96	51.46' LT	619.54
A5	15+85.71	128.40' LT	580+71.38	41.98' LT	619.24
A6	15+78.45	120.87' LT	580+77.38	33.42' LT	618.95
A7	15+72.62	112.20' LT	580+84.77	26.03' LT	618.66
A8	15+68.37	102.64' LT	580+93.34	20.03' LT	618.36
A9	15+65.86	92.49' LT	581+02.82	15.62' LT	618.07
A10	15+65.14	82.06' LT	581+12.92	12.91' LT	617.77
A11	15+66.24	71.66' LT	581+23.33	12.00' LT	617.48
A12	15+70.00	52.44' LT	581+42.93	12.00' LT	616.93
A13	15+74.46	39.45' LT	581+56.53	13.89' LT	616.66
A14	15+82.29	28.16' LT	581+69.11	19.40' LT	616.55
A15	15+92.89	19.43' LT	581+79.71	28.13' LT	616.59
A16	16+05.47	13.91' LT	581+87.54	39.42' LT	616.76
A17	16+19.07	12.03' LT	581+92.00	52.41' LT	616.86

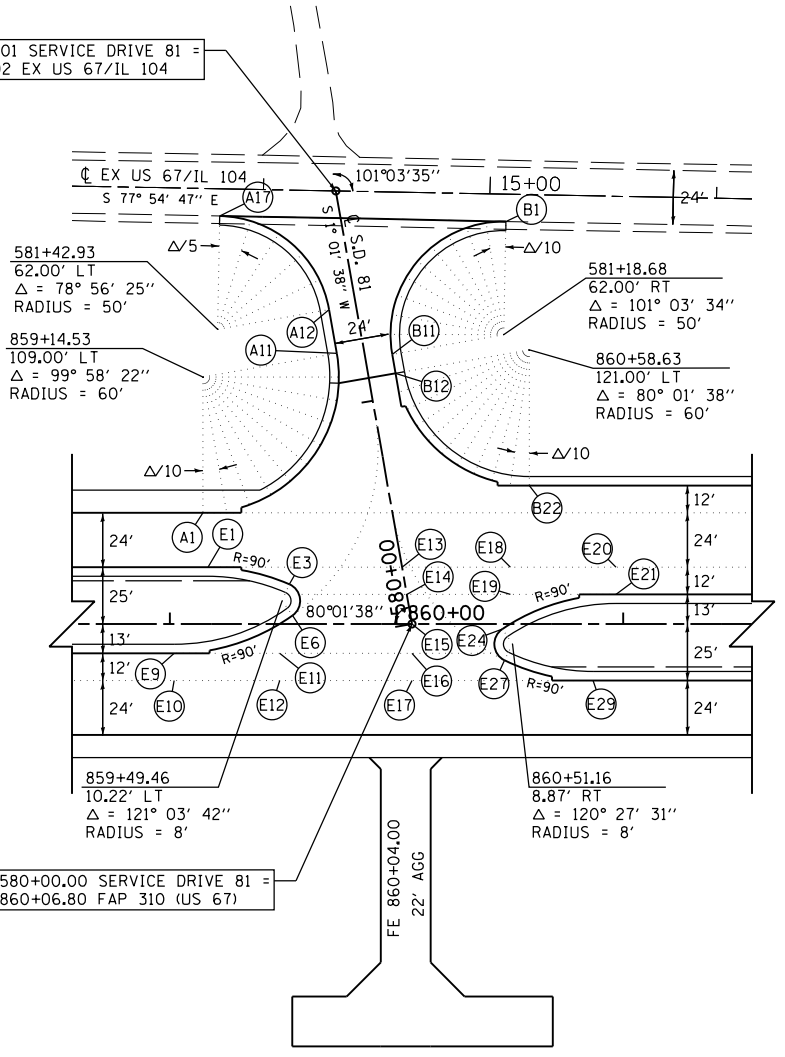
SERVICE DRIVE 81 C & EOP ELEVATIONS				
STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
580+51.86	12.00' LT	619.68	0.00' LT	619.55
580+75.00	12.00' LT	618.97	0.00' LT	618.84
581+00.00	12.00' LT	618.20	0.00' LT	618.08
581+25.00	12.00' LT	617.43	0.00' LT	617.32
581+50.00	12.00' LT	616.80	0.00' LT	616.70
581+75.00	12.00' LT	616.60	0.00' LT	616.51
581+84.12	12.00' LT	616.62	0.00' LT	N/A

SERVICE DRIVE 81 C & EOP ELEVATIONS				
STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
580+47.64	12.00' RT	619.54	0.00' RT	619.64
580+50.00	12.00' RT	619.53	0.00' RT	619.60
580+75.00	12.00' RT	618.77	0.00' RT	618.84
581+00.00	12.00' RT	618.00	0.00' RT	618.08
581+25.00	12.00' RT	617.24	0.00' RT	617.32
581+50.00	12.00' RT	616.62	0.00' RT	616.70
581+75.00	12.00' RT	616.43	0.00' RT	616.51
581+79.50	12.00' RT	616.44	0.00' RT	616.52

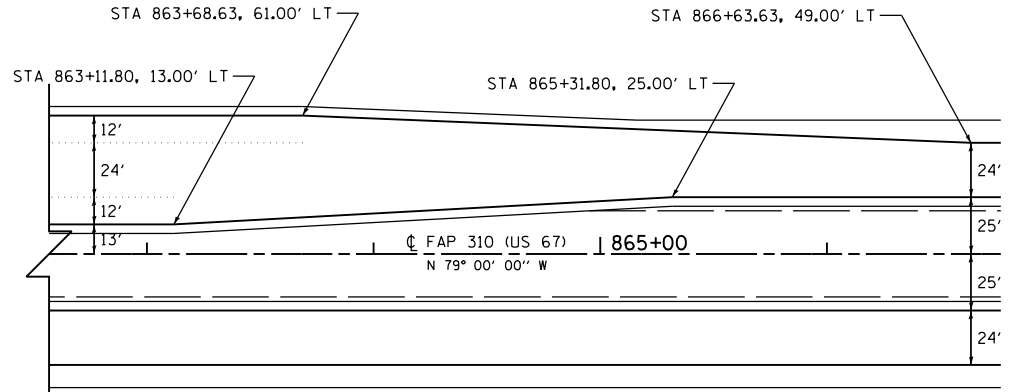
	EX US 67/IL 104		SERVICE DRIVE 81		ELEVATION
B1	14+92.72	12.04' LT	581+67.75	71.59' RT	615.81
B2	15+01.50	12.81' LT	581+68.67	62.83' RT	615.89
B3	15+10.00	15.12' LT	581+68.04	54.05' RT	615.95
B4	15+17.96	18.88' LT	581+65.88	45.51' RT	615.96
B5	15+25.15	23.97' LT	581+62.25	37.48' RT	615.97
B6	15+31.32	30.25' LT	581+57.27	30.22' RT	616.10
B7	15+36.30	37.52' LT	581+51.10	23.94' RT	616.35
B8	15+39.93	45.55' LT	581+43.92	18.84' RT	616.62
B9	15+42.09	54.08' LT	581+35.95	15.08' RT	616.89
B10	15+42.72	62.87' LT	581+27.45	12.78' RT	617.16
B11	15+41.80	71.63' LT	581+18.68	12.00' RT	617.43
B12	15+40.17	79.95' LT	581+10.20	12.00' RT	617.69
B13	15+37.99	88.04' LT	581+01.84	12.59' RT	617.95
B14	15+34.71	95.74' LT	580+93.65	14.33' RT	618.19
B15	15+30.39	102.92' LT	580+85.78	17.19' RT	618.41
B16	15+25.11	109.42' LT	580+78.39	21.12' RT	618.59
B17	15+18.98	115.12' LT	580+71.62	26.05' RT	618.74
B18	15+12.12	119.91' LT	580+65.60	31.87' RT	618.85
B19	15+04.65	123.71' LT	580+60.44	38.47' RT	618.93
B20	14+96.73	126.42' LT	580+56.26	45.72' RT	618.97
B21	14+88.51	128.01' LT	580+53.12	53.48' RT	618.98
B22	14+80.14	128.43' LT	580+51.10	61.61' RT	618.96



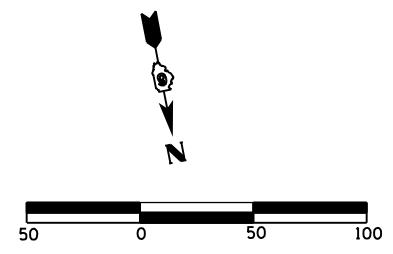
POT STA 581+94.01 SERVICE DRIVE 81 =  
POT STA 15+68.02 EX US 67/IL 104



POT STA 580+00.00 SERVICE DRIVE 81 =  
POT STA 860+06.80 FAP 310 (US 67)



	FAP 310 (US 67)		ELEVATION
E1	859+16.80	25.00 LT	620.00
E2	859+35.11	23.12 LT	619.90
E3	859+52.65	17.55 LT	619.77
E4	859+56.96	13.00 LT	619.72
E5	859+57.46	10.22 LT	619.69
E6	859+54.10	3.70 LT	619.63
E7	859+48.48	0.00 RT	619.59
E8	859+29.29	8.72 RT	619.71
E9	859+01.88	13.00 RT	619.80
E10	859+01.88	25.00 RT	620.04
E11	859+48.48	13.00 RT	619.74
E12	859+48.48	25.00 RT	619.87
E13	860+02.40	25.00 LT	619.59
E14	860+04.51	13.00 LT	619.57
E15	860+06.80	0.00 RT	619.56
E16	860+06.80	13.00 RT	619.56
E17	860+06.80	25.00 RT	619.56
E18	860+50.21	25.00 LT	619.26
E19	860+50.21	13.00 LT	619.15
E20	860+96.80	25.00 LT	618.88
E21	860+96.80	13.00 LT	618.64
E22	860+70.62	9.11 LT	618.88
E23	860+50.21	0.00 RT	619.02
E24	860+46.71	2.23 RT	619.09
E25	860+43.16	8.87 RT	619.18
E26	860+44.31	13.00 RT	619.21
E27	860+47.69	16.08 RT	619.20
E28	860+66.72	22.74 RT	619.10
E29	860+86.75	25.00 RT	618.97



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GEOMETRIC DETAILS  
 INTERSECTION OF FAP 310  
 AND SERVICE DRIVE 81  
 FAP 310 (US 67/IL 104)  
 SHEET 7 OF 23  
 DATE 6/06  
 DRAWN BY EBB  
 CHECKED BY

\$ FILE \$



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	214
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

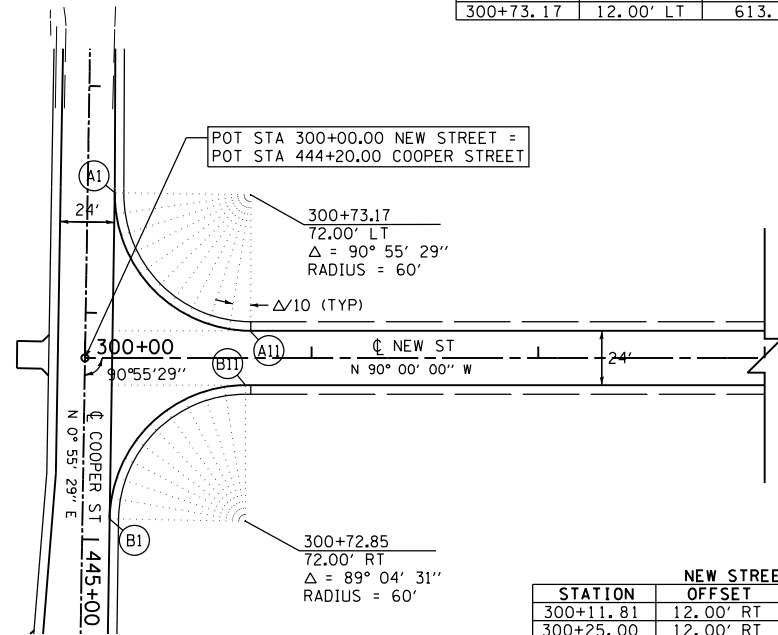
	COOPER STREET		NEW STREET		ELEVATION
A1	443+46.83	12.00' LT	300+13.18	72.97' LT	614.99
A2	443+56.31	12.75' LT	300+13.78	63.48' LT	614.87
A3	443+65.55	15.00' LT	300+15.87	54.20' LT	614.72
A4	443+74.33	18.67' LT	300+19.41	45.37' LT	614.50
A5	443+82.41	23.69' LT	300+24.29	37.20' LT	614.24
A6	443+89.60	29.92' LT	300+30.40	29.92' LT	614.02
A7	443+95.71	37.20' LT	300+37.59	23.69' LT	613.87
A8	444+00.59	45.37' LT	300+45.67	18.67' LT	613.79
A9	444+04.13	54.20' LT	300+54.45	15.00' LT	613.71
A10	444+06.22	63.48' LT	300+63.69	12.75' LT	613.63
A11	444+06.82	72.97' LT	300+73.17	12.00' LT	613.55

	NEW STREET		ASH STREET		ELEVATION
C1	312+65.17	12.00' LT	3+13.37	70.23' RT	619.17
C2	312+74.48	12.73' LT	3+12.53	60.93' RT	619.24
C3	312+83.56	14.89' LT	3+10.25	51.88' RT	619.31
C4	312+92.20	18.44' LT	3+06.59	43.28' RT	619.37
C5	313+00.19	23.28' LT	3+01.65	35.36' RT	619.43
C6	313+07.32	29.30' LT	2+95.53	28.30' RT	619.49
C7	313+13.44	36.36' LT	2+88.40	22.28' RT	619.54
C8	313+18.38	44.28' LT	2+80.41	17.44' RT	619.59
C9	313+22.04	52.88' LT	2+71.77	13.89' RT	619.63
C10	313+24.32	61.93' LT	2+62.69	11.73' RT	619.67
C11	313+25.16	71.23' LT	2+53.38	11.00' RT	619.68

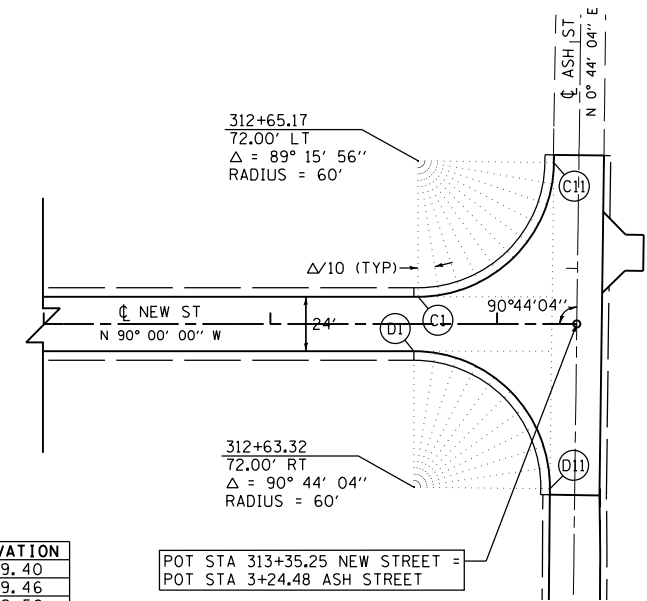
STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
300+12.20	12.00' LT	614.44	0.00' LT	614.36
300+25.00	12.00' LT	614.20	0.00' LT	614.19
300+50.00	12.00' LT	613.82	0.00' LT	613.94
300+73.17	12.00' LT	613.55	0.00' LT	613.79

STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
312+65.17	12.00' LT	619.17	0.00' LT	619.41
312+75.00	12.00' LT	619.26	0.00' LT	619.46
313+00.00	12.00' LT	619.49	0.00' LT	619.58
313+24.41	12.00' LT	619.72	0.00' LT	N/A

STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
2+50.00	11.00' RT	619.68	0.00' RT	620.02
2+75.00	11.00' RT	619.68	0.00' RT	619.99
3+00.00	11.00' RT	619.69	0.00' RT	619.95
3+12.62	11.00' RT	619.72	0.00' RT	619.94



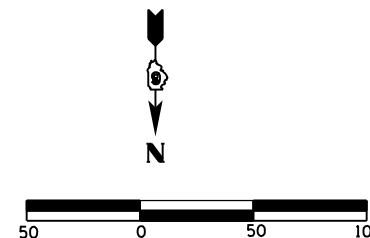
STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
300+11.81	12.00' RT	614.30	0.00' RT	N/A
300+25.00	12.00' RT	614.09	0.00' RT	614.19
300+50.00	12.00' RT	613.76	0.00' RT	613.94
300+70.85	12.00' RT	613.56	0.00' RT	613.80



STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
312+63.32	12.00' RT	619.16	0.00' RT	619.40
312+75.00	12.00' RT	619.26	0.00' RT	619.46
313+00.00	12.00' RT	619.47	0.00' RT	619.58
313+24.10	12.00' RT	619.68	0.00' RT	619.70

	COOPER STREET		NEW STREET		ELEVATION
B1	444+90.85	12.00' LT	300+10.85	71.03' RT	614.27
B2	444+81.56	12.72' LT	300+11.73	61.75' RT	614.24
B3	444+72.49	14.88' LT	300+14.03	52.72' RT	614.21
B4	444+63.87	18.41' LT	300+17.70	44.16' RT	614.12
B5	444+55.89	23.23' LT	300+22.65	36.27' RT	614.02
B6	444+48.76	29.23' LT	300+28.76	29.23' RT	613.92
B7	444+42.65	36.26' LT	300+35.89	23.23' RT	613.84
B8	444+37.70	44.16' LT	300+43.87	18.41' RT	613.77
B9	444+34.03	52.72' LT	300+52.49	14.88' RT	613.70
B10	444+31.73	61.75' LT	300+61.56	12.72' RT	613.63
B11	444+30.85	71.03' LT	300+70.85	12.00' RT	613.56

	NEW STREET		ASH STREET		ELEVATION
D1	312+63.32	12.00' RT	3+37.40	71.77' RT	619.16
D2	312+72.79	12.75' RT	3+38.03	62.30' RT	619.22
D3	312+82.01	14.99' RT	3+40.14	53.05' RT	619.28
D4	312+90.77	18.65' RT	3+43.69	44.24' RT	619.33
D5	312+98.84	23.64' RT	3+48.58	36.11' RT	619.38
D6	313+06.02	29.85' RT	3+54.69	28.85' RT	619.43
D7	313+12.13	37.11' RT	3+61.88	22.64' RT	619.47
D8	313+17.03	45.24' RT	3+69.95	17.65' RT	619.52
D9	313+20.57	54.05' RT	3+78.70	13.98' RT	619.56
D10	313+22.69	63.30' RT	3+87.93	11.75' RT	619.60
D11	313+23.32	72.77' RT	3+97.39	11.00' RT	619.60



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GEOMETRIC DETAILS  
 INTERSECTION OF NEW ST  
 COOPER ST & ASH ST  
 FAP 310 (US 67/IL 104)  
 SHEET 9 OF 23

DATE 6/06  
 DRAWN BY EBB  
 CHECKED BY

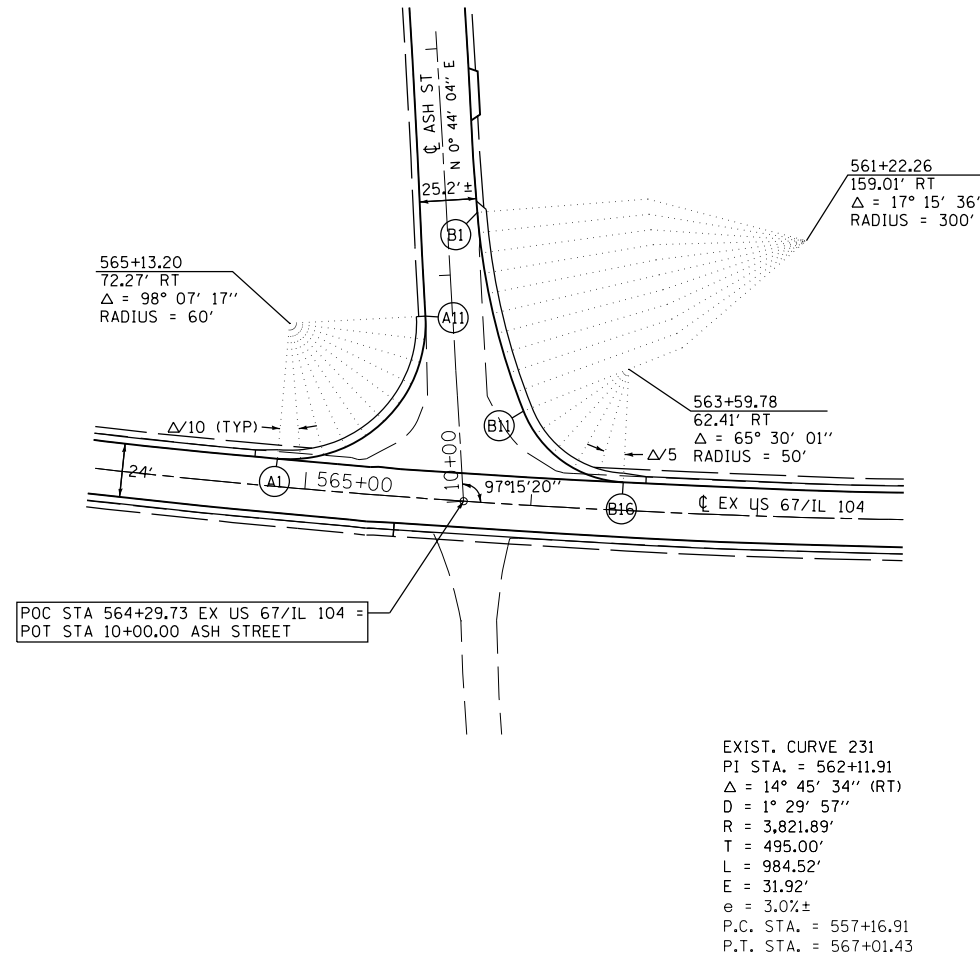
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	215
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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	EX US 67/IL 104		ASH STREET		ELEVATION
A1	565+13.24	12.27' RT	9+76.41	80.91' RT	620.69
A2	565+02.98	13.13' RT	9+77.06	70.67' RT	620.71
A3	564+93.00	15.69' RT	9+75.95	60.47' RT	620.70
A4	564+83.59	19.89' RT	9+73.13	50.60' RT	620.66
A5	564+75.02	25.60' RT	9+68.66	41.36' RT	620.56
A6	564+67.51	32.66' RT	9+62.68	33.02' RT	620.43
A7	564+61.31	40.88' RT	9+55.37	25.81' RT	620.25
A8	564+56.58	50.02' RT	9+46.94	19.96' RT	620.06
A9	564+53.48	59.81' RT	9+37.64	15.63' RT	619.87
A10	564+52.11	69.99' RT	9+27.73	12.95' RT	619.68
A11	564+52.52	80.24' RT	9+17.51	12.00' RT	619.49

	EX US 67/IL 104		ASH STREET		ELEVATION
B1	564+32.32	127.61' RT	8+73.10	13.63' LT	619.00
B2	564+30.83	118.68' RT	8+82.13	13.93' LT	619.03
B3	564+29.07	109.81' RT	8+91.15	14.50' LT	619.10
B4	564+27.05	100.99' RT	9+00.15	15.34' LT	619.18
B5	564+24.76	92.23' RT	9+09.11	16.45' LT	619.27
B6	564+22.22	83.54' RT	9+18.04	17.84' LT	619.35
B7	564+19.42	74.93' RT	9+26.93	19.49' LT	619.44
B8	564+16.37	66.40' RT	9+35.76	21.40' LT	619.54
B9	564+13.08	57.97' RT	9+44.53	23.59' LT	619.68
B10	564+09.54	49.63' RT	9+53.23	26.03' LT	619.85
B11	564+05.77	41.40' RT	9+61.85	28.74' LT	620.06
B12	563+99.72	31.69' RT	9+72.20	33.53' LT	620.37
B13	563+91.64	23.58' RT	9+81.20	40.55' LT	620.68
B14	563+81.95	17.48' RT	9+88.37	49.42' LT	620.95
B15	563+71.15	13.69' RT	9+93.35	59.68' LT	621.08
B16	563+59.78	12.41' RT	9+95.86	70.81' LT	621.15



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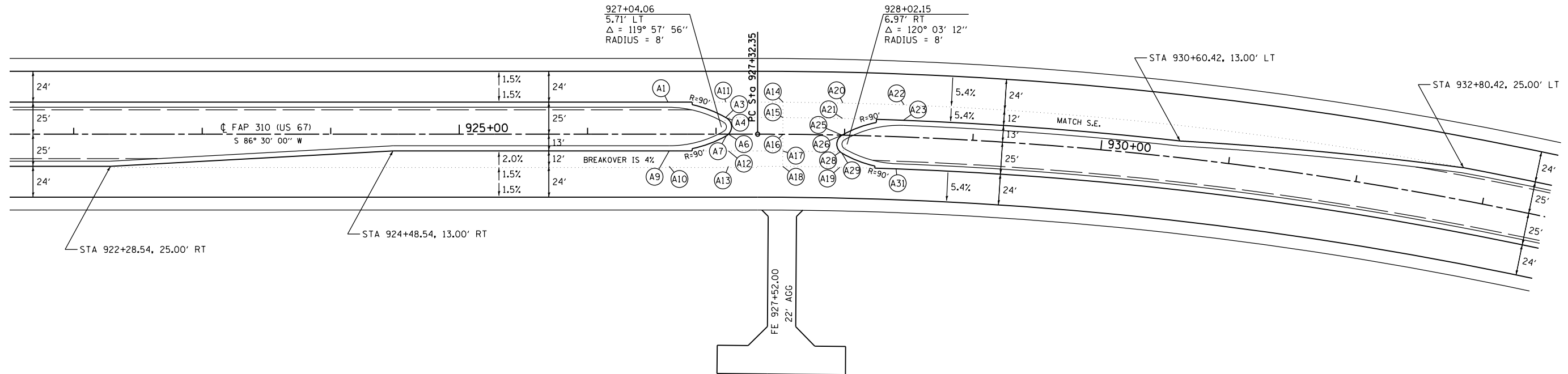
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GEOMETRIC DETAILS  
 INTERSECTION OF EX US 67  
 ASH ST  
 FAP 310 (US 67/IL 104)  
 SHEET 10 OF 23  
 DRAWN BY EBB  
 CHECKED BY  
 DATE 6/06

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	216
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

	FAP 310 (US 67)		ELEVATION
A1	926+62.54	25.00' LT	627.12
A2	926+86.15	21.85' LT	627.17
A3	927+07.44	13.00' LT	626.97
A4	927+08.11	12.61' LT	626.97
A5	927+12.06	5.71' LT	627.08
A6	927+09.66	0.00' RT	627.14
A7	927+08.01	1.24' RT	627.15
A8	926+86.54	10.01' RT	627.11
A9	926+63.54	13.00' RT	626.95
A10	926+63.70	25.00' RT	627.13
A11	927+07.44	25.00' LT	627.37
A12	927+09.66	13.00' RT	627.31
A13	927+09.66	25.00' RT	627.38
A14	927+52.00	25.00' LT	627.56
A15	927+52.00	13.00' LT	627.05
A16	927+52.00	0.19' RT	627.32
A17	927+52.00	13.00' RT	627.59
A18	927+52.00	25.00' RT	627.56
A19	927+96.88	25.00' RT	627.68
A20	927+98.17	25.00' LT	627.68
A21	927+98.17	13.00' LT	627.07
A22	928+45.42	25.00' LT	627.74
A23	928+45.42	13.00' LT	627.09
A24	928+20.87	9.67' LT	627.17
A25	927+98.17	0.00' RT	627.44
A26	927+98.04	0.10' RT	627.44
A27	927+94.13	6.97' RT	627.63
A28	927+96.88	13.00' RT	627.81
A29	927+98.25	13.96' RT	627.80
A30	928+18.93	22.19' RT	627.75
A31	928+41.07	25.00' RT	627.73

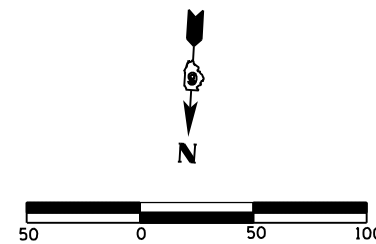
PROP. CURVE MLE16R2  
 PI STA. = 937+25.11  
 $\Delta = 37^\circ 10' 00''$  (RT)  
 $D = 1^\circ 56' 26''$   
 $R = 2,952.75'$   
 $T = 992.75'$   
 $L = 1,915.39'$   
 $E = 162.42'$   
 $e = 0.054$   
 P.C. STA. = 927+32.35  
 P.T. STA. = 946+47.74  
 S.E. TRANSITION  
 STA 925+31.00 TO STA 928+13.00  
 STA 945+67.00 TO STA 948+49.00



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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GEOMETRIC DETAILS  
 CROSSOVER STA 927+52  
 FAP 310 (US 67/IL 104)  
 SHEET 11 OF 23

DATE 6/06  
 DRAWN BY EBB  
 CHECKED BY





F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	218
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

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POT STA 21+81.41 GILLILAND ROAD EXTENSION =  
POT STA 13+17.93 GILLILAND ROAD CONNECTOR

PROP. CURVE V4  
PI STA. = 23+90.22  
 $\Delta = 33^\circ 15' 08''$  (RT)  
D = 38' 11" 50"  
R = 150.00'  
T = 44.79'  
L = 87.05'  
E = 6.54'  
e = N.C.  
P.C. STA. = 23+45.43  
P.T. STA. = 24+32.48

PROP. CURVE V9  
PI STA. = 10+44.48  
 $\Delta = 23^\circ 02' 08''$  (RT)  
D = 57' 17" 45"  
R = 100.00'  
T = 20.38'  
L = 40.20'  
E = 2.06'  
e = N.C.  
P.C. STA. = 10+24.11  
P.T. STA. = 10+64.31

PROP. CURVE V5  
PI STA. = 26+91.60  
 $\Delta = 69^\circ 22' 16''$  (RT)  
D = 38' 11" 50"  
R = 150.00'  
T = 103.81'  
L = 181.61'  
E = 32.42'  
e = N.C.  
P.C. STA. = 25+87.79  
P.T. STA. = 27+69.40

PROP. CURVE C61  
PI STA. = 76+30.32  
 $\Delta = 18^\circ 02' 28''$  (RT)  
D = 10' 08" 27"  
R = 565.00'  
T = 89.70'  
L = 177.91'  
E = 7.08'  
e = 0.040  
P.C. STA. = 75+40.63  
P.T. STA. = 77+18.53  
S.E. TRANSITION  
STA 74+43.00 TO STA 75+68.00  
STA 76+91.00 TO STA 78+16.00

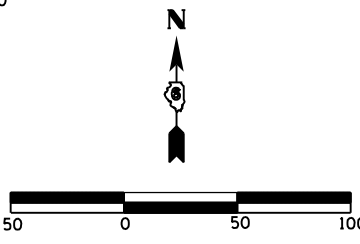
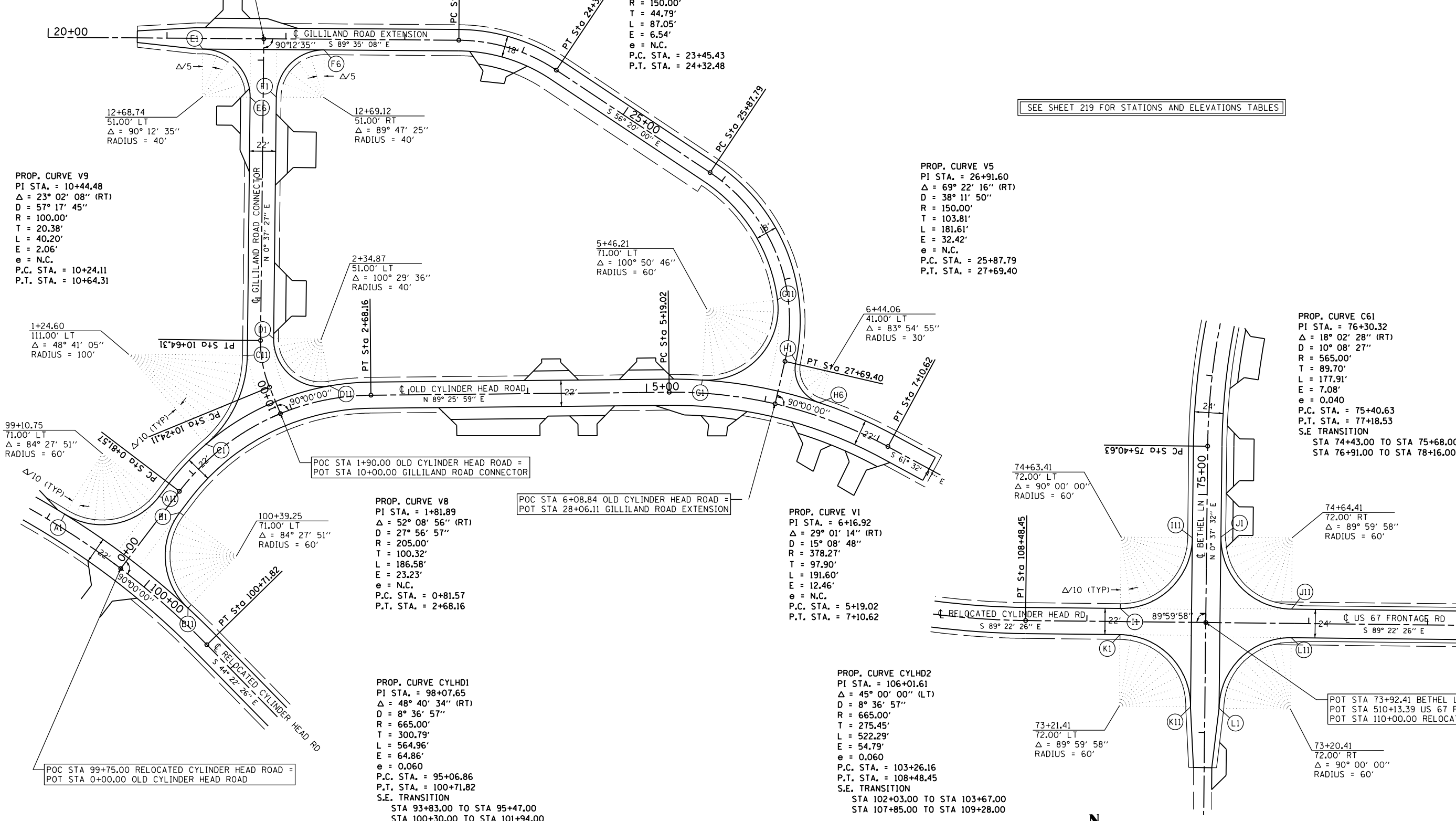
PROP. CURVE V8  
PI STA. = 1+81.89  
 $\Delta = 52^\circ 08' 56''$  (RT)  
D = 27' 56" 57"  
R = 205.00'  
T = 100.32'  
L = 186.58'  
E = 23.23'  
e = N.C.  
P.C. STA. = 0+81.57  
P.T. STA. = 2+68.16

PROP. CURVE V1  
PI STA. = 6+16.92  
 $\Delta = 29^\circ 01' 14''$  (RT)  
D = 15' 08" 48"  
R = 378.27'  
T = 97.90'  
L = 191.60'  
E = 12.46'  
e = N.C.  
P.C. STA. = 5+19.02  
P.T. STA. = 7+10.62

PROP. CURVE CYLHD1  
PI STA. = 98+07.65  
 $\Delta = 48^\circ 40' 34''$  (RT)  
D = 8' 36" 57"  
R = 665.00'  
T = 300.79'  
L = 564.96'  
E = 64.86'  
e = 0.060  
P.C. STA. = 95+06.86  
P.T. STA. = 100+71.82  
S.E. TRANSITION  
STA 93+83.00 TO STA 95+47.00  
STA 100+30.00 TO STA 101+94.00

PROP. CURVE CYLHD2  
PI STA. = 106+01.61  
 $\Delta = 45^\circ 00' 00''$  (LT)  
D = 8' 36" 57"  
R = 665.00'  
T = 275.45'  
L = 522.29'  
E = 54.79'  
e = 0.060  
P.C. STA. = 103+26.16  
P.T. STA. = 108+48.45  
S.E. TRANSITION  
STA 102+03.00 TO STA 103+67.00  
STA 107+85.00 TO STA 109+28.00

SEE SHEET 219 FOR STATIONS AND ELEVATIONS TABLES



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**GEOMETRIC DETAILS**  
 INTERSECTIONS IN BETHEL  
 FAP 310 (US 67/IL 104)  
 SHEET 13 OF 23  
 DATE 6/06  
 DRAWN BY EBB  
 CHECKED BY

Table with columns: F.A.P. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO. (Values: 310, 69-3(3HB), MORGAN, 793, 219)

STA. TO STA. FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

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Mar-30-2011 08:46:17 AM

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Table with columns: OLD CYLINDER HD RD, REL. CYLINDER HD RD, ELEVATION. Rows A1 to A11.

Table: OLD CYLINDER HEAD ROAD & EOP ELEVATIONS. Columns: STATION, OFFSET, ELEVATION, OFFSET, ELEVATION.

Table: OLD CYLINDER HEAD ROAD & EOP ELEVATIONS. Columns: STATION, OFFSET, ELEVATION, OFFSET, ELEVATION.

Table with columns: OLD CYLINDER HD RD, REL. CYLINDER HD RD, ELEVATION. Rows B1 to B11.

Table with columns: OLD CYLINDER HD RD, GILLILAND RD CONNECTOR, ELEVATION. Rows C1 to C11.

Table: GILLILAND ROAD CONNECTOR & EOP ELEVATIONS. Columns: STATION, OFFSET, ELEVATION, OFFSET, ELEVATION.

Table: GILLILAND ROAD CONNECTOR & EOP ELEVATIONS. Columns: STATION, OFFSET, ELEVATION, OFFSET, ELEVATION.

Table with columns: OLD CYLINDER HD RD, GILLILAND RD CONNECTOR, ELEVATION. Rows D1 to D11.

Table with columns: GILLILAND RD EXTENSION, GILLILAND RD CONNECTOR, ELEVATION. Rows E1 to E6.

Table: GILLILAND ROAD CONNECTOR & EOP ELEVATIONS. Columns: STATION, OFFSET, ELEVATION, OFFSET, ELEVATION.

Table: GILLILAND ROAD CONNECTOR & EOP ELEVATIONS. Columns: STATION, OFFSET, ELEVATION, OFFSET, ELEVATION.

Table with columns: GILLILAND RD EXTENSION, GILLILAND RD CONNECTOR, ELEVATION. Rows F1 to F6.

Table with columns: GILLILAND RD EXTENSION, OLD CYLINDER HD RD, ELEVATION. Rows G1 to G11.

Table: GILLILAND ROAD EXTENSION & EOP ELEVATIONS. Columns: STATION, OFFSET, ELEVATION, OFFSET, ELEVATION.

Table: GILLILAND ROAD EXTENSION & EOP ELEVATIONS. Columns: STATION, OFFSET, ELEVATION, OFFSET, ELEVATION.

Table with columns: GILLILAND RD EXTENSION, OLD CYLINDER HD RD, ELEVATION. Rows H1 to H6.

Table with columns: BETHEL LANE, REL. CYLINDER HD RD, ELEVATION. Rows K1 to K11.

Table: RELOCATED CYLINDER HEAD ROAD & EOP ELEVATIONS. Columns: STATION, OFFSET, ELEVATION, OFFSET, ELEVATION.

Table: RELOCATED CYLINDER HEAD ROAD & EOP ELEVATIONS. Columns: STATION, OFFSET, ELEVATION, OFFSET, ELEVATION.

Table with columns: BETHEL LANE, REL. CYLINDER HD RD, ELEVATION. Rows I1 to I11.

Table with columns: BETHEL LANE, US 67 FRONTAGE ROAD, ELEVATION. Rows L1 to L11.

Table: US 67 FRONTAGE ROAD & EOP ELEVATIONS. Columns: STATION, OFFSET, ELEVATION, OFFSET, ELEVATION.

Table: US 67 FRONTAGE ROAD & EOP ELEVATIONS. Columns: STATION, OFFSET, ELEVATION, OFFSET, ELEVATION.

Table with columns: BETHEL LANE, US 67 FRONTAGE ROAD, ELEVATION. Rows J1 to J11.

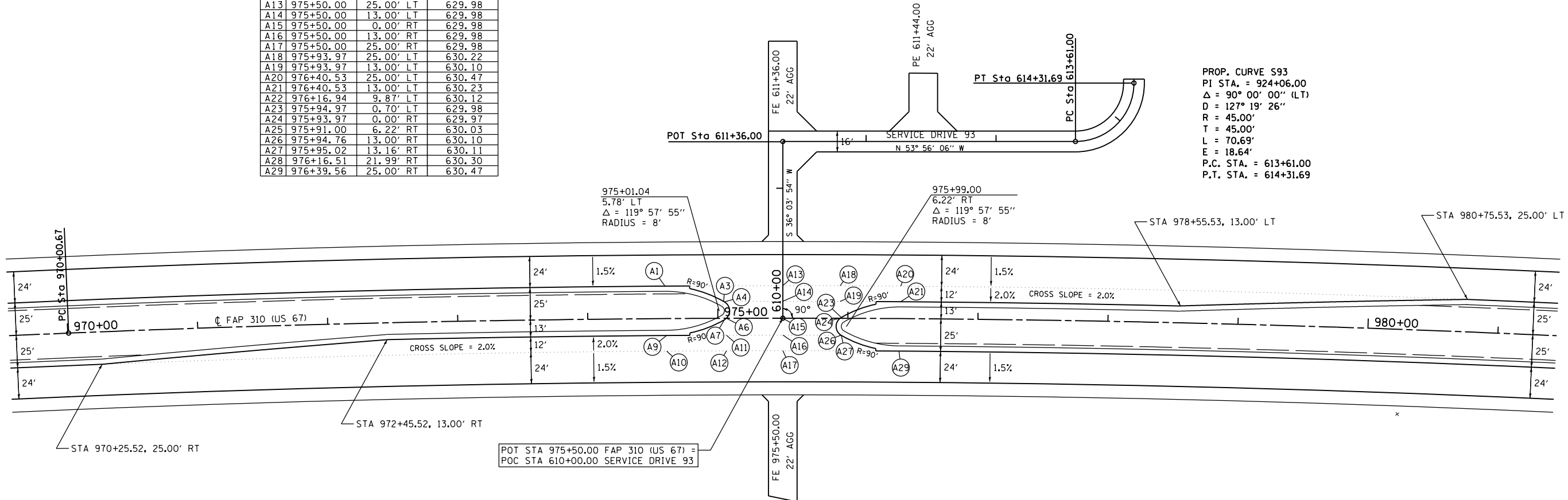
Table: REVISIONS. Columns: NAME, DATE.

ILLINOIS DEPARTMENT OF TRANSPORTATION GEOMETRIC DETAILS INTERSECTIONS IN BETHEL FAP 310 (US 67/IL 104) SHEET 14 OF 23

DRAWN BY EBB CHECKED BY DATE 6/06

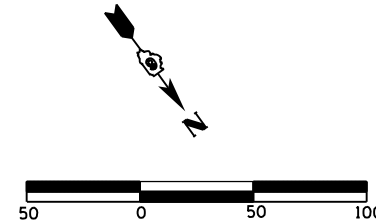
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	220
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

	FAP 310 (US 67)	ELEVATION
A1	974+59.55 25.00' LT	629.48
A2	974+83.12 21.87' LT	629.56
A3	975+04.54 13.00' LT	629.60
A4	975+05.07 12.70' LT	629.60
A5	975+09.04 5.78' LT	629.57
A6	975+06.57 0.00' RT	629.49
A7	975+05.02 1.16' RT	629.49
A8	974+83.55 9.99' RT	629.39
A9	974+60.52 13.00' RT	629.24
A10	974+60.52 25.00' RT	629.48
A11	975+06.57 13.00' RT	629.62
A12	975+06.57 25.00' RT	629.74
A13	975+50.00 25.00' LT	629.98
A14	975+50.00 13.00' LT	629.98
A15	975+50.00 0.00' RT	629.98
A16	975+50.00 13.00' RT	629.98
A17	975+50.00 25.00' RT	629.98
A18	975+93.97 25.00' LT	630.22
A19	975+93.97 13.00' LT	630.10
A20	976+40.53 25.00' LT	630.47
A21	976+40.53 13.00' LT	630.23
A22	976+16.94 9.87' LT	630.12
A23	975+94.97 0.70' LT	629.98
A24	975+93.97 0.00' RT	629.97
A25	975+91.00 6.22' RT	630.03
A26	975+94.76 13.00' RT	630.10
A27	975+95.02 13.16' RT	630.11
A28	976+16.51 21.99' RT	630.30
A29	976+39.56 25.00' RT	630.47



PROP. CURVE S93  
 PI STA. = 924+06.00  
 Δ = 90° 00' 00" (LT)  
 D = 127° 19' 26"  
 R = 45.00'  
 T = 45.00'  
 L = 70.69'  
 E = 18.64'  
 P.C. STA. = 613+61.00  
 P.T. STA. = 614+31.69

PROP. CURVE MLE17  
 PI STA. = 979+37.53  
 Δ = 8° 10' 00" (RT)  
 D = 0° 26' 12"  
 R = 13,123.33'  
 T = 936.85'  
 L = 1,870.54'  
 E = 33.40'  
 e = 0.015  
 P.C. STA. = 970+00.67  
 P.T. STA. = 988+71.21  
 S.E. TRANSITION  
 STA 968+87.00 TO STA 970+23.00  
 STA 988+49.00 TO STA 989+85.00



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GEOMETRIC DETAILS  
 CROSSOVER STA 975+50  
 FAP 310 (US 67/IL 104)  
 SHEET 15 OF 23  
 DATE 6/06  
 DRAWN BY EBB  
 CHECKED BY

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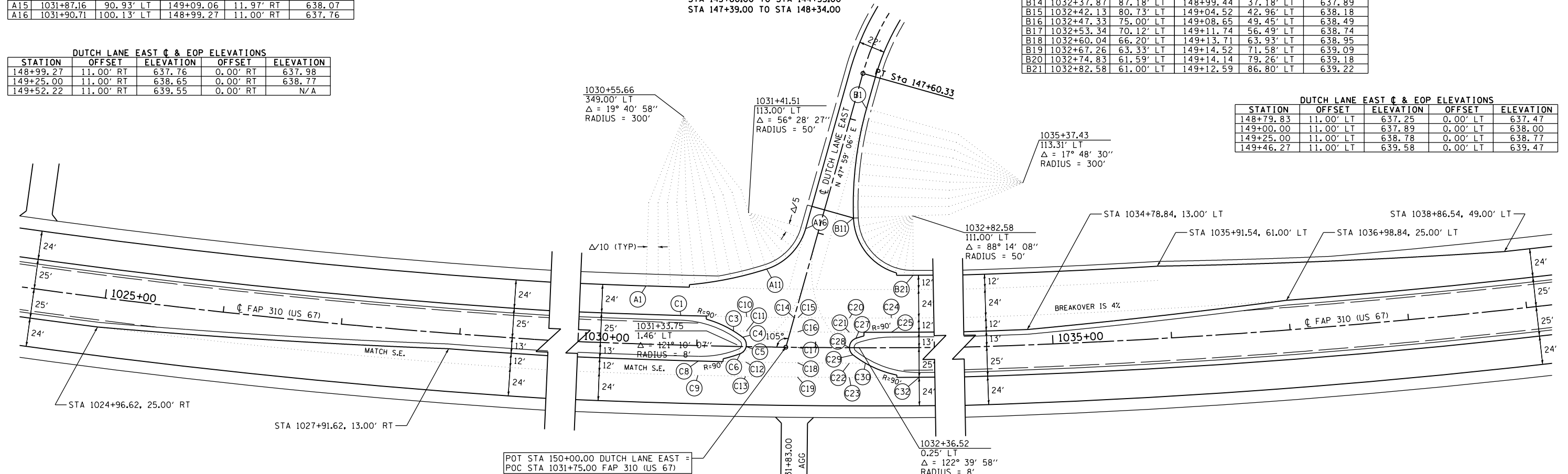
	FAP 310 (US 67)		DUTCH LANE EAST		ELEVATION
A1	1030+55.66	49.00' LT	149+82.14	127.30' RT	639.18
A2	1030+66.05	49.17' LT	149+79.51	117.34' RT	639.22
A3	1030+76.43	49.67' LT	149+76.53	107.47' RT	639.25
A4	1030+86.79	50.51' LT	149+73.21	97.72' RT	639.27
A5	1030+97.12	51.68' LT	149+69.57	88.08' RT	639.28
A6	1031+07.40	53.19' LT	149+65.59	78.57' RT	639.29
A7	1031+17.64	55.03' LT	149+61.29	69.21' RT	639.29
A8	1031+27.81	57.20' LT	149+56.67	59.99' RT	639.27
A9	1031+37.91	59.69' LT	149+51.73	50.95' RT	639.21
A10	1031+47.92	62.51' LT	149+46.49	42.07' RT	639.11
A11	1031+57.84	65.65' LT	149+40.95	33.39' RT	638.97
A12	1031+66.92	69.70' LT	149+34.73	25.76' RT	638.80
A13	1031+75.03	75.42' LT	149+27.14	19.49' RT	638.59
A14	1031+81.86	82.58' LT	149+18.47	14.84' RT	638.35
A15	1031+87.16	90.93' LT	149+09.06	11.97' RT	638.07
A16	1031+90.71	100.13' LT	148+99.27	11.00' RT	637.76

DUTCH LANE EAST C & EOP ELEVATIONS				
STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
148+99.27	11.00' RT	637.76	0.00' RT	637.98
149+25.00	11.00' RT	638.65	0.00' RT	638.77
149+52.22	11.00' RT	639.55	0.00' RT	N/A

PROP. CURVE C41  
 PI STA. = 146+43.37  
 $\Delta = 91^\circ 14' 04''$  (LT)  
 $D = 27^\circ 56' 57''$   
 $R = 205.00'$   
 $T = 209.47'$   
 $L = 326.43'$   
 $E = 88.09'$   
 $e = 0.040$   
 P.C. STA. = 144+33.90  
 P.T. STA. = 147+60.33  
 S.E. TRANSITION  
 STA 143+60.00 TO STA 144+55.00  
 STA 147+39.00 TO STA 148+34.00

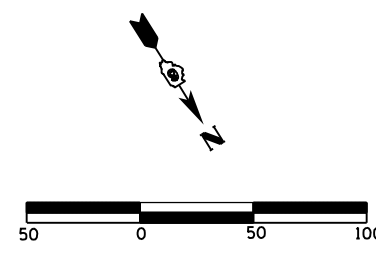
	FAP 310 (US 67)		DUTCH LANE EAST		ELEVATION
B1	1032+42.79	201.48' LT	147+88.08	11.00' LT	635.73
B2	1032+40.33	192.46' LT	147+97.40	11.15' LT	635.81
B3	1032+38.17	183.38' LT	148+06.71	11.58' LT	635.89
B4	1032+36.31	174.23' LT	148+16.01	12.30' LT	635.97
B5	1032+34.75	165.03' LT	148+25.28	13.32' LT	636.09
B6	1032+33.49	155.79' LT	148+34.51	14.62' LT	636.22
B7	1032+32.53	146.51' LT	148+43.70	16.20' LT	636.35
B8	1032+31.87	137.21' LT	148+52.83	18.07' LT	636.50
B9	1032+31.50	127.89' LT	148+61.91	20.23' LT	636.68
B10	1032+31.44	118.57' LT	148+70.91	22.66' LT	636.90
B11	1032+31.66	109.25' LT	148+79.83	25.37' LT	637.14
B12	1032+32.57	101.60' LT	148+86.95	28.28' LT	637.37
B13	1032+34.66	94.19' LT	148+93.54	32.25' LT	637.62
B14	1032+37.87	87.18' LT	148+99.44	37.18' LT	637.89
B15	1032+42.13	80.73' LT	149+04.52	42.96' LT	638.18
B16	1032+47.33	75.00' LT	149+08.65	49.45' LT	638.49
B17	1032+53.34	70.12' LT	149+11.74	56.49' LT	638.74
B18	1032+60.04	66.20' LT	149+13.71	63.93' LT	638.95
B19	1032+67.26	63.33' LT	149+14.52	71.58' LT	639.09
B20	1032+74.83	61.59' LT	149+14.14	79.26' LT	639.18
B21	1032+82.58	61.00' LT	149+12.59	86.80' LT	639.22

DUTCH LANE EAST C & EOP ELEVATIONS				
STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
148+79.83	11.00' LT	637.25	0.00' LT	637.47
149+00.00	11.00' LT	637.89	0.00' LT	638.00
149+25.00	11.00' LT	638.78	0.00' LT	638.77
149+46.27	11.00' LT	639.58	0.00' LT	639.47



PROP. CURVE MLE18  
 PI STA. = 1031+15.75  
 $\Delta = 16^\circ 26' 00''$  (LT)  
 $D = 0^\circ 58' 13''$   
 $R = 5,905.50'$   
 $T = 852.75'$   
 $L = 1,693.79'$   
 $E = 61.25'$   
 $e = 0.033$   
 P.C. STA. = 1022+63.00  
 P.T. STA. = 1039+56.79  
 S.E. TRANSITION  
 STA 1021+20.00 TO STA 1023+13.00  
 STA 1039+07.00 TO STA 1041+00.00

	FAP 310 (US 67)		ELEVATION
C1	1030+85.98	25.00' LT	640.09
C2	1031+13.60	20.62' LT	640.06
C3	1031+30.68	13.00' LT	639.98
C4	1031+38.46	7.93' LT	639.94
C5	1031+41.61	0.00' RT	639.84
C6	1031+36.84	5.91' RT	639.89
C7	1031+19.57	11.21' RT	639.85
C8	1031+01.62	13.00' RT	639.75
C9	1031+01.62	25.00' RT	640.15
C10	1031+41.61	25.00' LT	640.27
C11	1031+41.61	13.00' LT	640.06
C12	1031+41.61	13.00' RT	640.06
C13	1031+41.61	25.00' RT	640.27
C14	1031+81.73	25.00' LT	640.35
C15	1031+85.00	25.00' LT	640.35
C16	1031+85.00	13.00' LT	640.35
C17	1031+85.00	0.00' RT	640.35
C18	1031+85.00	13.00' RT	640.35
C19	1031+85.00	25.00' RT	640.35
C20	1032+28.52	25.00' LT	640.40
C21	1032+28.52	13.00' LT	640.32
C22	1032+28.52	13.00' RT	640.32
C23	1032+28.52	25.00' RT	640.40
C24	1032+63.84	25.00' LT	640.41
C25	1032+63.84	13.00' LT	640.33
C26	1032+48.60	11.68' LT	640.32
C27	1032+33.82	7.78' LT	640.28
C28	1032+28.52	0.00' RT	640.23
C29	1032+31.64	6.09' RT	640.27
C30	1032+42.01	13.00' RT	640.33
C31	1032+57.84	20.15' RT	640.38
C32	1032+87.11	25.00' RT	640.40



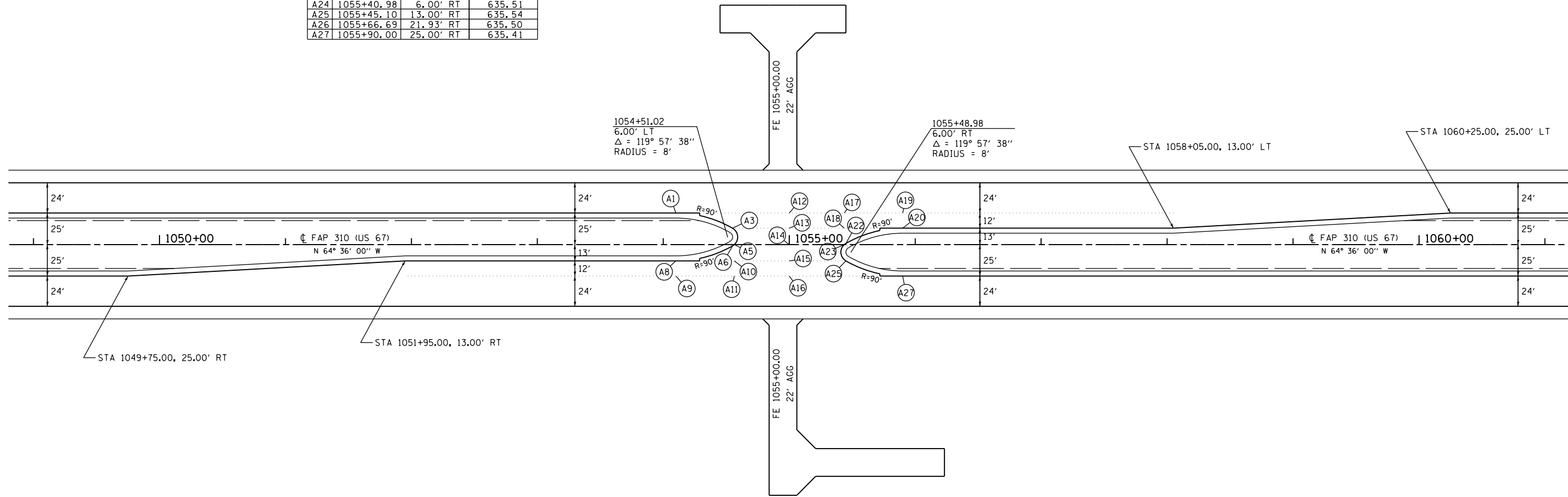
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GEOMETRIC DETAILS  
 INTERSECTION OF FAP 310  
 AND DUTCH LANE EAST  
 FAP 310 (US 67/IL 104)  
 SHEET 17 OF 23

DATE 6/06  
 DRAWN BY EBB  
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	223
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

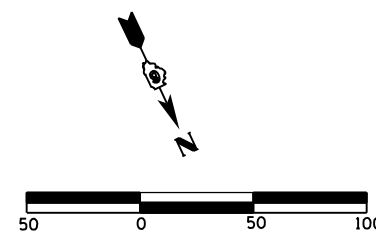
	FAP 310 (US 67)			ELEVATION
A1	1054+10.00	25.00' LT		636.35
A2	1054+33.31	21.93' LT		636.19
A3	1054+54.90	13.00' LT		636.01
A4	1054+59.02	6.00' LT		635.93
A5	1054+56.32	0.00' RT		635.88
A6	1054+55.03	0.93' RT		635.88
A7	1054+33.31	9.93' RT		636.01
A8	1054+10.00	13.00' RT		636.11
A9	1054+10.00	25.00' RT		636.35
A10	1054+56.32	13.00' RT		636.00
A11	1054+56.32	25.00' RT		636.12
A12	1055+00.00	25.00' LT		635.90
A13	1055+00.00	13.00' LT		635.90
A14	1055+00.00	0.00' RT		635.90
A15	1055+00.00	13.00' RT		635.90
A16	1055+00.00	25.00' RT		635.90
A17	1055+43.68	25.00' LT		635.67
A18	1055+43.68	13.00' LT		635.56
A19	1055+90.00	25.00' LT		635.41
A20	1055+90.00	13.00' LT		635.17
A21	1055+66.69	9.93' LT		635.32
A22	1055+44.97	0.93' LT		635.42
A23	1055+43.68	0.00' RT		635.43
A24	1055+40.98	6.00' RT		635.51
A25	1055+45.10	13.00' RT		635.54
A26	1055+66.69	21.93' RT		635.50
A27	1055+90.00	25.00' RT		635.41



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Mar-30-2011 08:46:41 AM

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GEOMETRIC DETAILS  
 CROSSOVER STA 1055+00  
 FAP 310 (US 67/IL 104)  
 SHEET 18 OF 23  
 DRAWN BY EBB  
 CHECKED BY  
 DATE 6/06



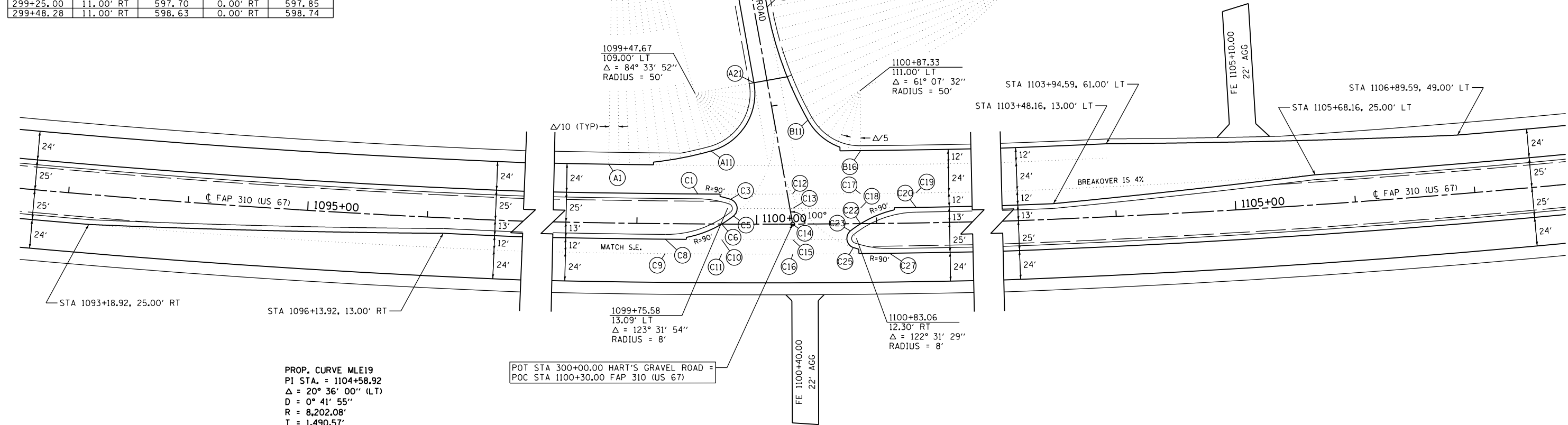


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	FAP 310 (US 67)	HART'S GRAVEL ROAD	ELEVATION
A1	1098+75.66 49.00' LT	299+23.68 142.32' RT	596.14
A2	1098+84.36 49.12' LT	299+25.22 133.81' RT	596.31
A3	1098+93.05 49.48' LT	299+26.52 125.26' RT	596.47
A4	1099+01.72 50.08' LT	299+27.56 116.68' RT	596.63
A5	1099+10.38 50.92' LT	299+28.36 108.07' RT	596.78
A6	1099+19.01 52.00' LT	299+28.91 99.45' RT	596.92
A7	1099+27.61 53.31' LT	299+29.21 90.81' RT	597.05
A8	1099+36.18 54.86' LT	299+29.26 82.16' RT	597.18
A9	1099+44.69 56.65' LT	299+29.07 73.52' RT	597.30
A10	1099+53.16 58.67' LT	299+28.62 64.88' RT	597.38
A11	1099+61.56 60.93' LT	299+27.93 56.27' RT	597.41
A12	1099+68.54 63.46' LT	299+26.69 49.00' RT	597.40
A13	1099+75.07 66.98' LT	299+24.40 41.99' RT	597.34
A14	1099+81.01 71.41' LT	299+21.10 35.40' RT	597.26
A15	1099+86.24 76.66' LT	299+16.87 29.36' RT	597.13
A16	1099+90.64 82.61' LT	299+11.79 24.01' RT	596.97
A17	1099+94.12 89.13' LT	299+05.99 19.46' RT	596.77
A18	1099+96.59 96.09' LT	298+99.57 15.82' RT	596.53
A19	1099+98.00 103.33' LT	298+92.70 13.16' RT	596.26
A20	1099+98.32 110.69' LT	298+85.50 11.54' RT	595.94
A21	1099+97.54 118.02' LT	298+78.15 11.00' RT	595.60

HART'S GRAVEL ROAD C & EOP ELEVATIONS				
STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
298+78.15	11.00' RT	595.60	0.00' RT	595.82
299+00.00	11.00' RT	596.64	0.00' RT	596.83
299+25.00	11.00' RT	597.70	0.00' RT	597.85
299+48.28	11.00' RT	598.63	0.00' RT	598.74



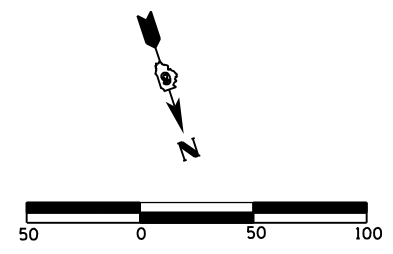
PROP. CURVE C25  
 PI STA. = 296+27.63  
 $\Delta = 19^\circ 20' 54''$  (RT)  
 $D = 10^\circ 08' 27''$   
 $R = 565.00'$   
 $T = 96.32'$   
 $L = 190.80'$   
 $E = 8.15'$   
 $e = 0.040$   
 P.C. STA. = 295+31.32  
 P.T. STA. = 297+22.12  
 S.E. TRANSITION  
 STA 294+43.00 TO STA 295+57.00  
 STA 296+97.00 TO STA 298+11.00

	FAP 310 (US 67)	HART'S GRAVEL ROAD	ELEVATION
B1	1100+08.73 181.29' LT	298+17.83 11.00' LT	592.29
B2	1100+10.71 171.39' LT	298+27.92 11.17' LT	592.89
B3	1100+13.03 161.56' LT	298+37.99 11.68' LT	593.46
B4	1100+15.68 151.81' LT	298+48.04 12.53' LT	594.01
B5	1100+18.66 142.16' LT	298+58.06 13.71' LT	594.54
B6	1100+21.95 132.61' LT	298+68.03 15.23' LT	595.05
B7	1100+25.56 123.17' LT	298+77.94 17.09' LT	595.55
B8	1100+29.49 113.85' LT	298+87.79 19.27' LT	596.05
B9	1100+33.72 104.67' LT	298+97.56 21.79' LT	596.55
B10	1100+38.25 95.63' LT	299+07.23 24.63' LT	597.05
B11	1100+43.07 86.75' LT	299+16.81 27.80' LT	597.55
B12	1100+49.28 78.05' LT	299+26.43 32.36' LT	598.08
B13	1100+57.19 70.85' LT	299+34.87 38.86' LT	598.58
B14	1100+66.46 65.46' LT	299+41.73 47.00' LT	599.01
B15	1100+76.65 62.13' LT	299+46.72 56.40' LT	599.35
B16	1100+87.32 61.00' LT	299+49.61 66.65' LT	599.61

HART'S GRAVEL ROAD C & EOP ELEVATIONS				
STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
298+78.15	11.00' LT	595.60	0.00' LT	595.82
299+00.00	11.00' LT	596.73	0.00' LT	596.83
299+25.00	11.00' LT	597.85	0.00' LT	597.85
299+52.18	11.00' LT	599.01	0.00' LT	N/A

PROP. CURVE MLE19  
 PI STA. = 1104+58.92  
 $\Delta = 20^\circ 36' 00''$  (LT)  
 $D = 0^\circ 41' 55''$   
 $R = 8,202.08'$   
 $T = 1,490.57'$   
 $L = 2,948.96'$   
 $E = 134.34'$   
 $e = 0.025$   
 P.C. STA. = 1089+68.35  
 P.T. STA. = 1119+17.31  
 S.E. TRANSITION  
 STA 1088+44.00 TO STA 1090+06.00  
 STA 1118+80.00 TO STA 1120+42.00

	FAP 310 (US 67)	ELEVATION
C1	1099+50.63 25.00' LT	598.17
C2	1099+64.50 23.92' LT	598.40
C3	1099+78.03 20.70' LT	598.61
C4	1099+83.59 13.00' LT	598.63
C5	1099+80.57 6.84' LT	598.50
C6	1099+70.73 0.00' RT	598.18
C7	1099+53.91 7.90' RT	597.92
C8	1099+23.92 13.00' RT	597.37
C9	1099+23.92 25.00' RT	597.67
C10	1099+70.73 13.00' RT	598.36
C11	1099+70.73 25.00' RT	598.53
C12	1100+30.00 25.00' LT	599.57
C13	1100+30.00 13.00' LT	599.57
C14	1100+30.00 0.00' RT	599.57
C15	1100+30.00 13.00' RT	599.57
C16	1100+30.00 25.00' RT	599.57
C17	1100+86.79 25.00' LT	600.50
C18	1100+86.79 13.00' LT	600.40
C19	1101+33.16 25.00' LT	601.23
C20	1101+33.16 13.00' LT	601.05
C21	1101+04.03 8.11' LT	600.59
C22	1100+86.79 0.00' RT	600.30
C23	1100+78.12 6.01' RT	600.23
C24	1100+75.10 13.00' RT	600.24
C25	1100+80.41 19.85' RT	600.36
C26	1100+95.24 23.70' RT	600.63
C27	1101+10.50 25.00' RT	600.88



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GEOMETRIC DETAILS  
 INTERSECTION OF FAP 310  
 AND HART'S GRAVEL ROAD  
 FAP 310 (US 67/IL 104)  
 SHEET 20 OF 23  
 DRAWN BY EBB  
 CHECKED BY  
 DATE 6/06

\$FILE\$

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	226
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

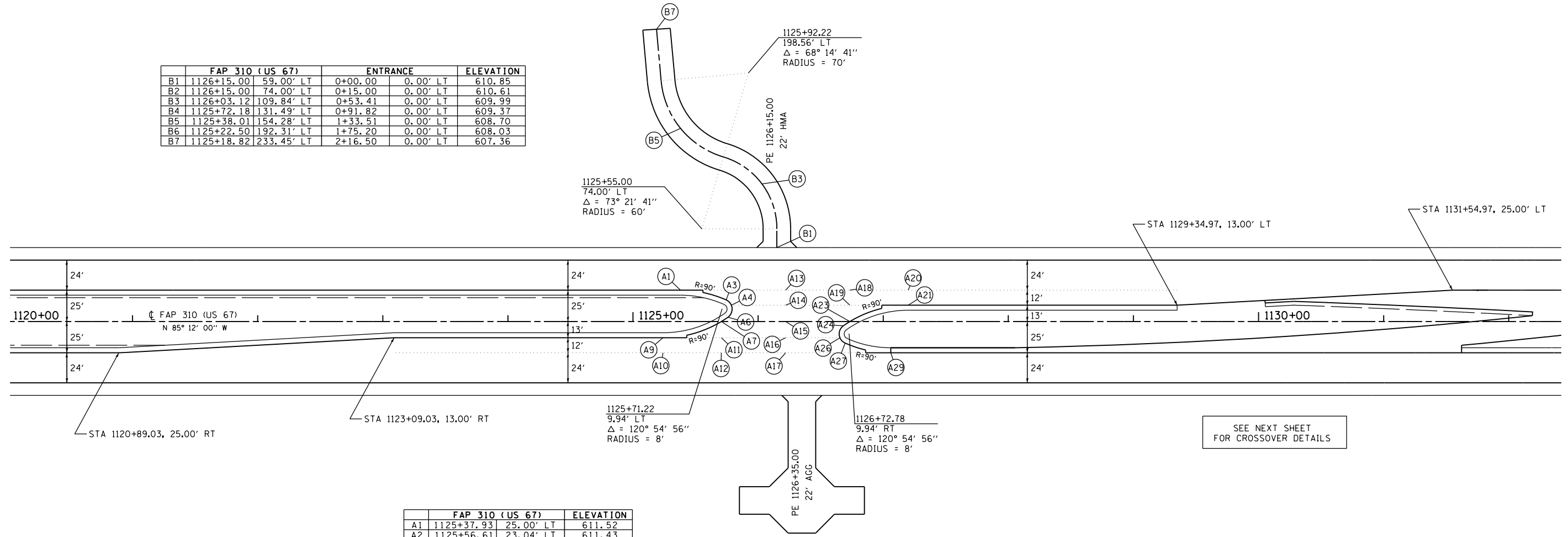
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PROP. CURVE ENT-1  
 PI STA. = 0+59.69  
 $\Delta = 73^\circ 21' 41''$  (LT)  
 D = 95° 29' 35"  
 R = 60.00'  
 T = 44.69'  
 L = 76.82'  
 E = 14.82'  
 P.C. STA. = 0+15.00  
 P.T. STA. = 0+91.82

PROP. CURVE ENT-2  
 PI STA. = 1+39.26  
 $\Delta = 68^\circ 14' 41''$  (RT)  
 D = 81° 51' 04"  
 R = 70.00'  
 T = 47.43'  
 L = 83.38'  
 E = 14.56'  
 P.C. STA. = 0+91.82  
 P.T. STA. = 1+75.20

	FAP 310 (US 67)		ENTRANCE		ELEVATION
B1	1126+15.00	59.00' LT	0+00.00	0.00' LT	610.85
B2	1126+15.00	74.00' LT	0+15.00	0.00' LT	610.61
B3	1126+03.12	109.84' LT	0+53.41	0.00' LT	609.99
B4	1125+72.18	131.49' LT	0+91.82	0.00' LT	609.37
B5	1125+38.01	154.28' LT	1+33.51	0.00' LT	608.70
B6	1125+22.50	192.31' LT	1+75.20	0.00' LT </td <td>608.03</td>	608.03
B7	1125+18.82	233.45' LT	2+16.50	0.00' LT	607.36



SEE NEXT SHEET FOR CROSSOVER DETAILS

	FAP 310 (US 67)		ELEVATION
A1	1125+37.93	25.00' LT	611.52
A2	1125+56.61	23.04' LT	611.43
A3	1125+74.47	17.25' LT	611.31
A4	1125+78.61	13.00' LT	611.27
A5	1125+79.22	9.94' LT	611.24
A6	1125+75.82	3.40' LT	611.18
A7	1125+70.62	0.00' RT	611.14
A8	1125+51.19	8.80' RT	611.24
A9	1125+24.03	13.00' RT	611.33
A10	1125+24.03	25.00' RT	611.57
A11	1125+70.62	13.00' RT	611.28
A12	1125+70.62	25.00' RT	611.40
A13	1126+22.00	25.00' LT	611.22
A14	1126+22.00	13.00' LT	611.22
A15	1126+22.00	0.00' RT	611.22
A16	1126+22.00	13.00' RT	611.22
A17	1126+22.00	25.00' RT	611.22
A18	1126+73.37	25.00' LT	611.04
A19	1126+73.37	13.00' LT	610.92
A20	1127+19.97	25.00' LT	610.88
A21	1127+19.97	13.00' LT	610.64
A22	1126+92.81	8.80' LT	610.74
A23	1126+73.37	0.00' RT	610.78
A24	1126+68.17	3.40' RT	610.86
A25	1126+64.78	9.94' RT	610.94
A26	1126+65.39	13.00' RT	610.97
A27	1126+69.53	17.25' RT	610.98
A28	1126+87.39	23.04' RT	610.97
A29	1127+06.07	25.00' RT	610.93

REVISIONS	
NAME	DATE

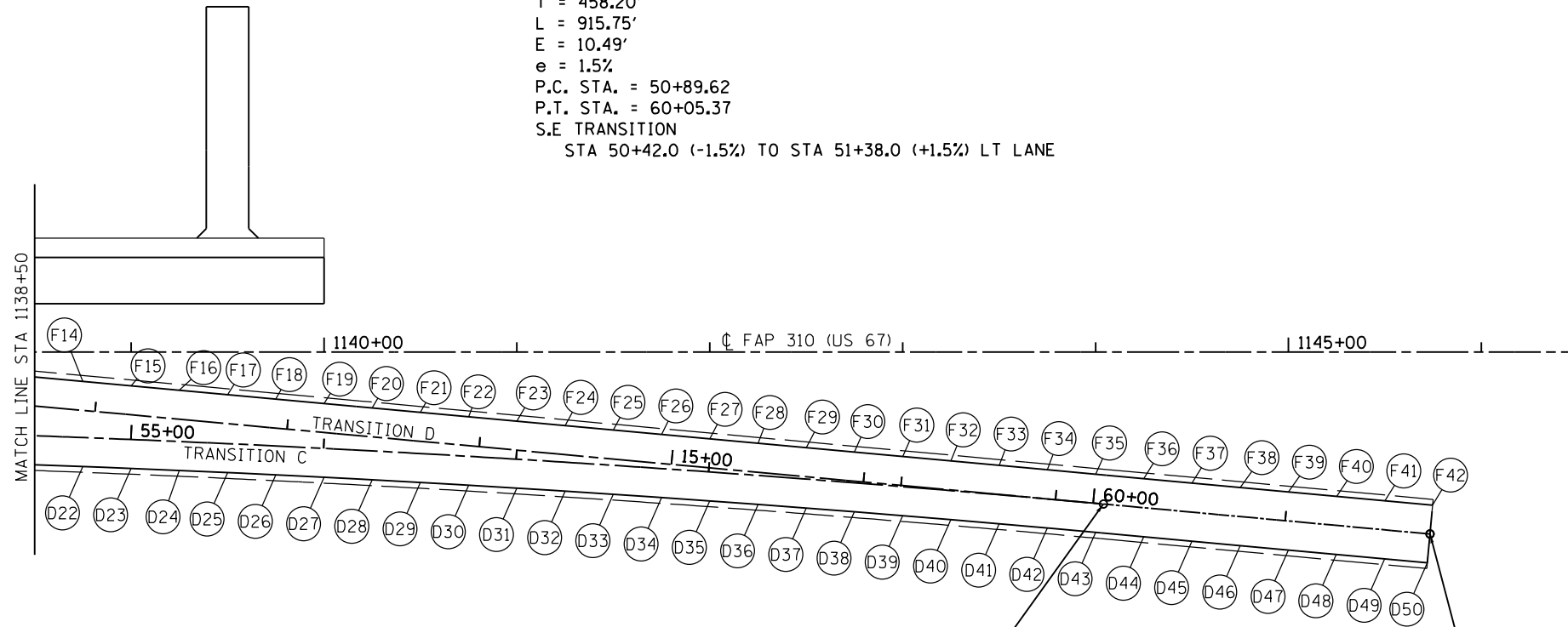
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GEOMETRIC DETAILS  
 CROSSOVER STA 1126+22  
 FAP 310 (US 67/IL 104)  
 SHEET 21 OF 23  
 DATE 6/06  
 DRAWN BY EBB  
 CHECKED BY

\$FILE\$



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	228
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PROP. CURVE TRASC (TRANSITION C)  
 PI STA. = 55+47.82  
 $\Delta = 5^\circ 14' 49''$  (RT)  
 $D = 0^\circ 34' 23''$   
 $R = 10,000.00'$   
 $T = 458.20'$   
 $L = 915.75'$   
 $E = 10.49'$   
 $e = 1.5\%$   
 P.C. STA. = 50+89.62  
 P.T. STA. = 60+05.37  
 S.E TRANSITION  
 STA 50+42.0 (-1.5%) TO STA 51+38.0 (+1.5%) LT LANE



PT STA 60+05.37 TRANSITION C =  
 POT STA 17+24.96 TRANSITION D

END IMPROVEMENTS  
 POT STA 61+75.36 TRANSITION C =  
 POC STA 312+19.53 EX US 67  
 STA 1145+73.37 94.45' RT FAP 310

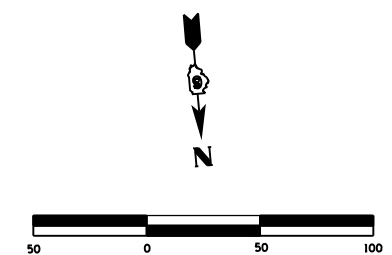
MRK	FAP 310 (US 67)		TRANSITION D		ELEVATION
	STA	OFFSET	STA	OFFSET	
F14	1138+75.00	15.25' RT	11+92.26	15.00' LT	602.96
F15	1139+00.00	17.55' RT	12+17.37	15.00' LT	602.53
F16	1139+25.00	19.84' RT	12+42.47	15.00' LT	602.13
F17	1139+50.00	22.14' RT	12+67.58	15.00' LT	601.78
F18	1139+75.00	24.43' RT	12+92.68	15.00' LT	601.47
F19	1140+00.00	26.73' RT	13+17.79	15.00' LT	601.21
F20	1140+25.00	29.02' RT	13+42.89	15.00' LT	600.99
F21	1140+50.00	31.32' RT	13+68.00	15.00' LT	600.82
F22	1140+75.00	33.62' RT	13+93.10	15.00' LT	600.69
F23	1141+00.00	35.91' RT	14+18.21	15.00' LT	600.61
F24	1141+25.00	38.21' RT	14+43.31	15.00' LT	600.56
F25	1141+50.00	40.50' RT	14+68.42	15.00' LT	600.56
F26	1141+75.00	42.80' RT	14+93.52	15.00' LT	600.57
F27	1142+00.00	45.10' RT	15+18.63	15.00' LT	600.58
F28	1142+25.00	47.39' RT	15+43.73	15.00' LT	600.60
F29	1142+50.00	49.69' RT	15+68.84	15.00' LT	600.61
F30	1142+75.00	51.98' RT	15+93.95	15.00' LT	600.62
F31	1143+00.00	54.28' RT	16+19.05	15.00' LT	600.61
F32	1143+25.00	56.57' RT	16+44.16	15.00' LT	600.58
F33	1143+50.00	58.87' RT	16+69.26	15.00' LT	600.52
F34	1143+75.00	61.17' RT	16+94.37	15.00' LT	600.44
F35	1144+00.00	63.46' RT	17+19.47	15.00' LT	600.33
MRK	FAP 310 (US 67)		TRANSITION C		ELEVATION
	STA	OFFSET	STA	OFFSET	
F35	1144+00.00	63.46' RT	59+99.89	15.00' LT	600.33
F36	1144+25.00	65.76' RT	60+24.99	15.00' LT	600.19
F37	1144+50.00	68.05' RT	60+50.10	15.00' LT	600.04
F38	1144+75.00	70.35' RT	60+75.20	15.00' LT	599.85
F39	1145+00.00	72.64' RT	61+00.31	15.00' LT	599.67
F40	1145+25.00	74.94' RT	61+25.41	15.00' LT	599.48
F41	1145+50.00	77.24' RT	61+50.52	15.00' LT	599.29
F42	1145+74.74	79.51' RT	61+75.36	15.00' LT	599.11

MRK	FAP 310 (US 67)		TRANSITION C		ELEVATION
	STA	OFFSET	STA	OFFSET	
D22	1138+75.00	59.44' RT	54+75.67	15.00' RT	602.72
D23	1139+00.00	60.44' RT	55+00.73	15.00' RT	602.30
D24	1139+25.00	61.50' RT	55+25.79	15.00' RT	601.93
D25	1139+50.00	62.62' RT	55+50.86	15.00' RT	601.60
D26	1139+75.00	63.80' RT	55+75.92	15.00' RT	601.31
D27	1140+00.00	65.05' RT	56+00.99	15.00' RT	601.07
D28	1140+25.00	66.36' RT	56+26.06	15.00' RT	600.87
D29	1140+50.00	67.74' RT	56+51.14	15.00' RT	600.71
D30	1140+75.00	69.17' RT	56+76.22	15.00' RT	600.60
D31	1141+00.00	70.67' RT	57+01.30	15.00' RT	600.53
D32	1141+25.00	72.24' RT	57+26.38	15.00' RT	600.50
D33	1141+50.00	73.86' RT	57+51.48	15.00' RT	600.52
D34	1141+75.00	75.55' RT	57+76.57	15.00' RT	600.53
D35	1142+00.00	77.30' RT	58+01.67	15.00' RT	600.55
D36	1142+25.00	79.12' RT	58+26.77	15.00' RT	600.57
D37	1142+50.00	80.99' RT	58+51.88	15.00' RT	600.59
D38	1142+75.00	82.94' RT	58+76.99	15.00' RT	600.61
D39	1143+00.00	84.94' RT	59+02.11	15.00' RT	600.60
D40	1143+25.00	87.01' RT	59+27.23	15.00' RT	600.57
D41	1143+50.00	89.14' RT	59+52.36	15.00' RT	600.51
D42	1143+75.00	91.33' RT	59+77.50	15.00' RT	600.42
D43	1144+00.00	93.59' RT	60+02.64	15.00' RT	600.31
D44	1144+25.00	95.88' RT	60+27.74	15.00' RT	600.18
D45	1144+50.00	98.18' RT	60+52.85	15.00' RT	600.02
D46	1144+75.00	100.48' RT	60+77.96	15.00' RT	599.83
D47	1145+00.00	102.77' RT	61+03.06	15.00' RT	599.64
D48	1145+24.82	105.05' RT	61+27.98	15.00' RT	599.46
D49	1145+49.82	107.35' RT	61+53.09	15.00' RT	599.27
D50	1145+72.00	109.38' RT	61+75.36	15.00' RT	599.11

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GEOMETRIC DETAILS  
 TRANSITION D  
 FAP 310 (US 67/IL 104)  
 SHEET 23 OF 23

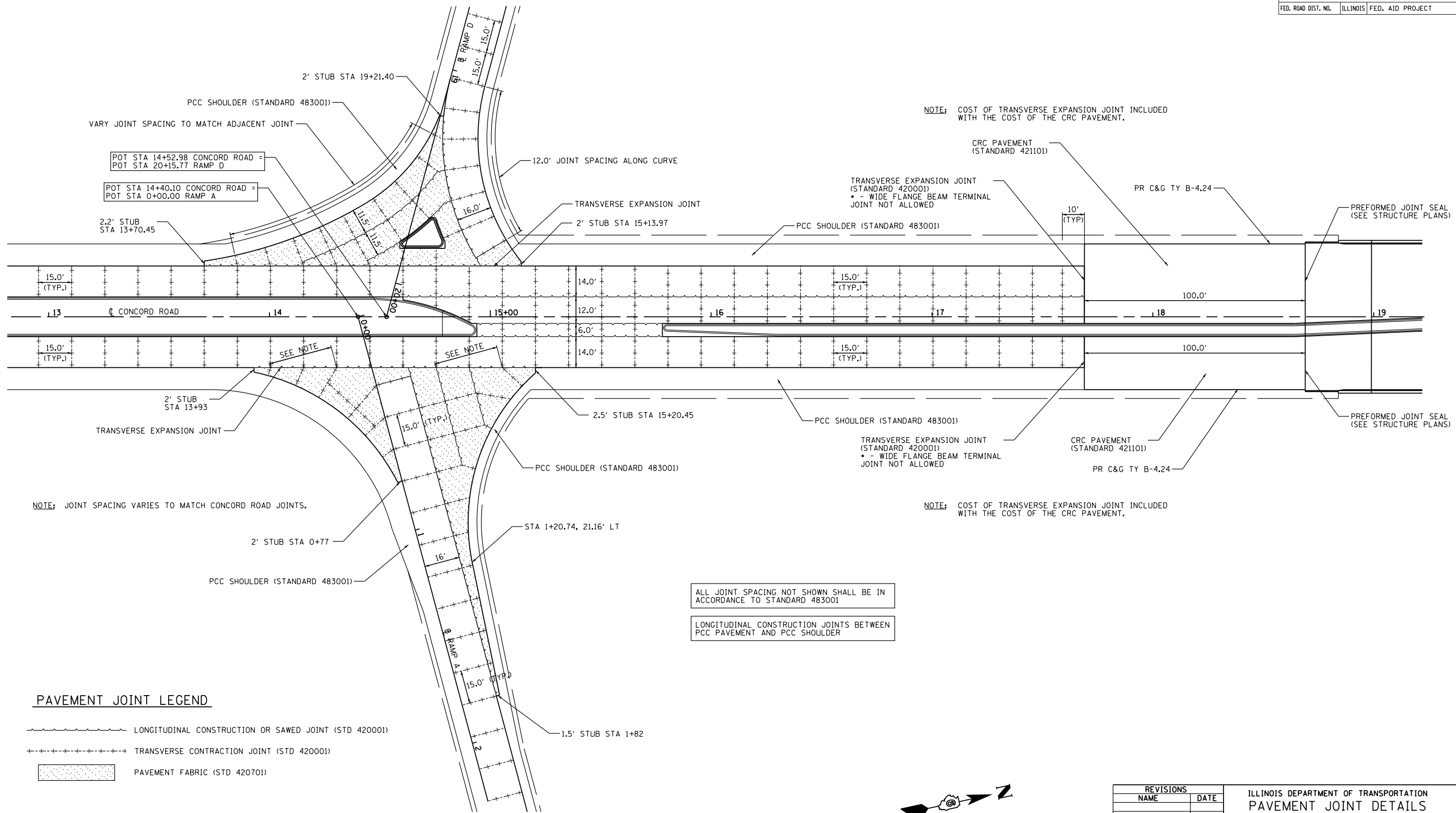
DATE 9/09  
 DRAWN BY B.G.J.  
 CHECKED BY



8:47:14 AM  
 Mar-30-2011 08:47:14AM  
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	229
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

8:47:22 AM  
Mar-30-2011 08:47:22AM  
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NOTE: COST OF TRANSVERSE EXPANSION JOINT INCLUDED WITH THE COST OF THE CRC PAVEMENT.

TRANSVERSE EXPANSION JOINT (STANDARD 420001)  
• - WIDE FLANGE BEAM TERMINAL JOINT NOT ALLOWED

TRANSVERSE EXPANSION JOINT (STANDARD 420001)  
• - WIDE FLANGE BEAM TERMINAL JOINT NOT ALLOWED

NOTE: COST OF TRANSVERSE EXPANSION JOINT INCLUDED WITH THE COST OF THE CRC PAVEMENT.

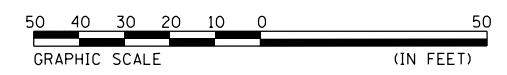
NOTE: JOINT SPACING VARIES TO MATCH CONCORD ROAD JOINTS.

ALL JOINT SPACING NOT SHOWN SHALL BE IN ACCORDANCE TO STANDARD 483001

LONGITUDINAL CONSTRUCTION JOINTS BETWEEN PCC PAVEMENT AND PCC SHOULDER

**PAVEMENT JOINT LEGEND**

- — — — — LONGITUDINAL CONSTRUCTION OR SAWED JOINT (STD 420001)
- +++++ TRANSVERSE CONTRACTION JOINT (STD 420001)
- [Pattern] PAVEMENT FABRIC (STD 420701)



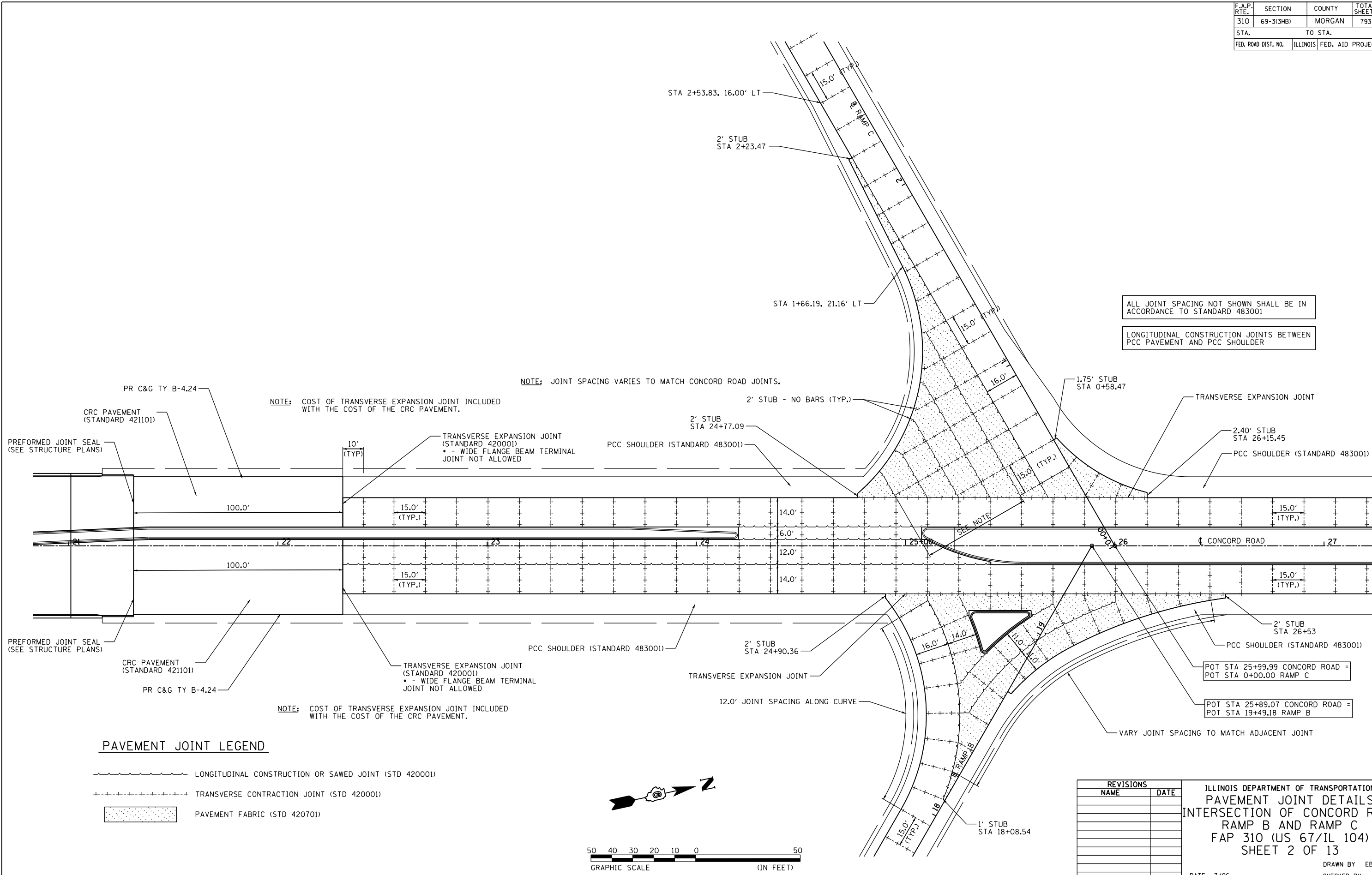
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT JOINT DETAILS**  
 INTERSECTION OF CONCORD ROAD  
 RAMP A AND RAMP D  
 FAP 310 (US 67/IL 104)  
 SHEET 1 OF 13  
 DATE 7/06  
 DRAWN BY EBB  
 CHECKED BY

8:47:29 AM  
 Mar-30-2011 08:47:29AM

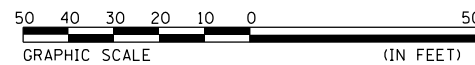
CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	230
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



**PAVEMENT JOINT LEGEND**

- LONGITUDINAL CONSTRUCTION OR SAWED JOINT (STD 420001)
- +++++ TRANSVERSE CONTRACTION JOINT (STD 420001)
- [Pattern] PAVEMENT FABRIC (STD 420701)



REVISIONS	
NAME	DATE

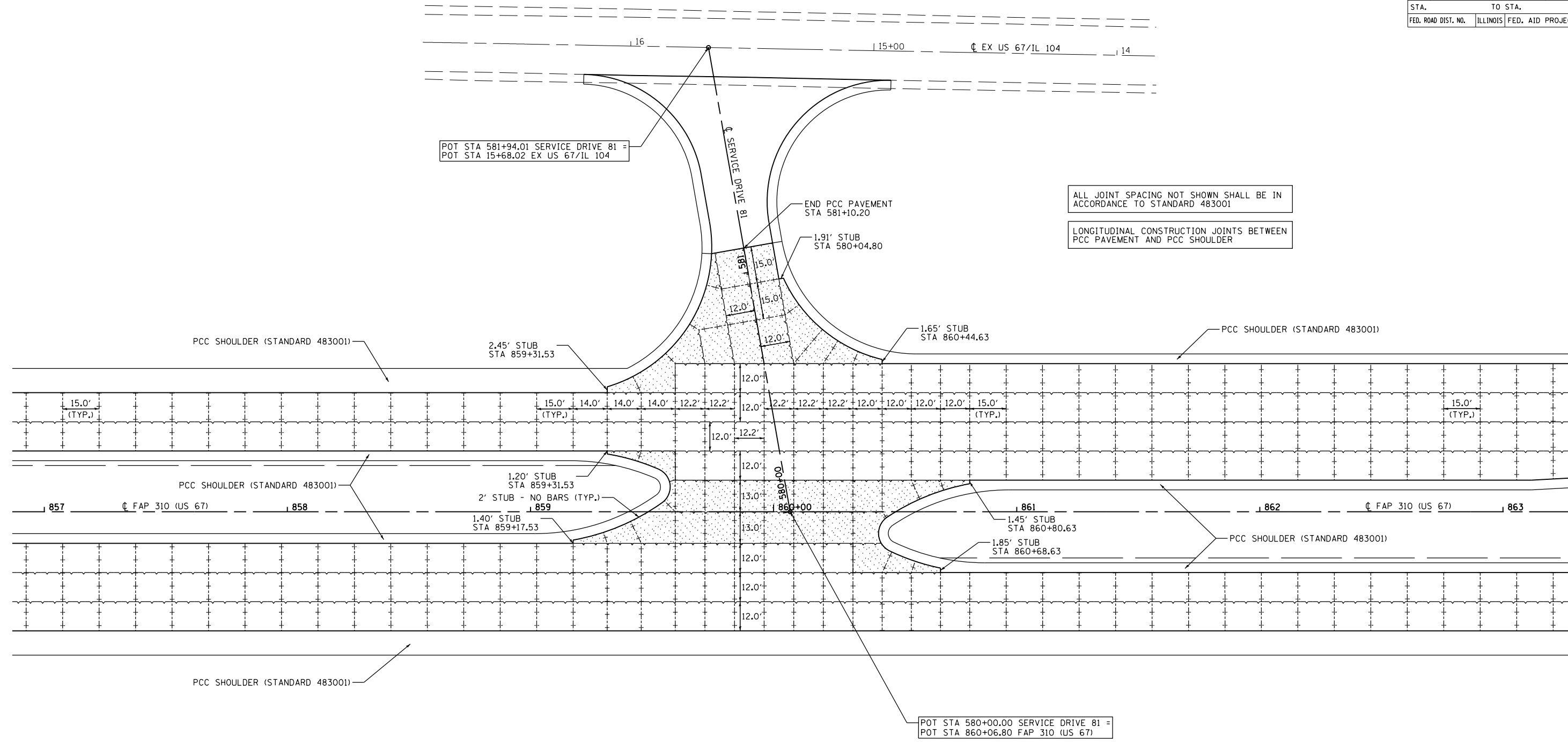
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 PAVEMENT JOINT DETAILS  
 INTERSECTION OF CONCORD ROAD  
 RAMP B AND RAMP C  
 FAP 310 (US 67/IL 104)  
 SHEET 2 OF 13

DATE 7/06  
 DRAWN BY EBB  
 CHECKED BY

\$FILE\$

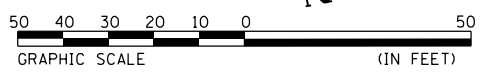
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	231
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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Mar-30-2011 08:47:36AM  
\$FILE\$



**PAVEMENT JOINT LEGEND**

- LONGITUDINAL CONSTRUCTION OR SAWED JOINT (STD 420001)
- +++++ TRANSVERSE CONTRACTION JOINT (STD 420001)
- PAVEMENT FABRIC (STD 420701)



REVISIONS	
NAME	DATE

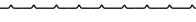
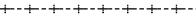

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 PAVEMENT JOINT DETAILS  
 INTERSECTION OF FAP 310  
 AND SERVICE DRIVE 81  
 FAP 310 (US 67/IL 104)  
 SHEET 3 OF 13  
 DATE 7/06  
 DRAWN BY EBB  
 CHECKED BY

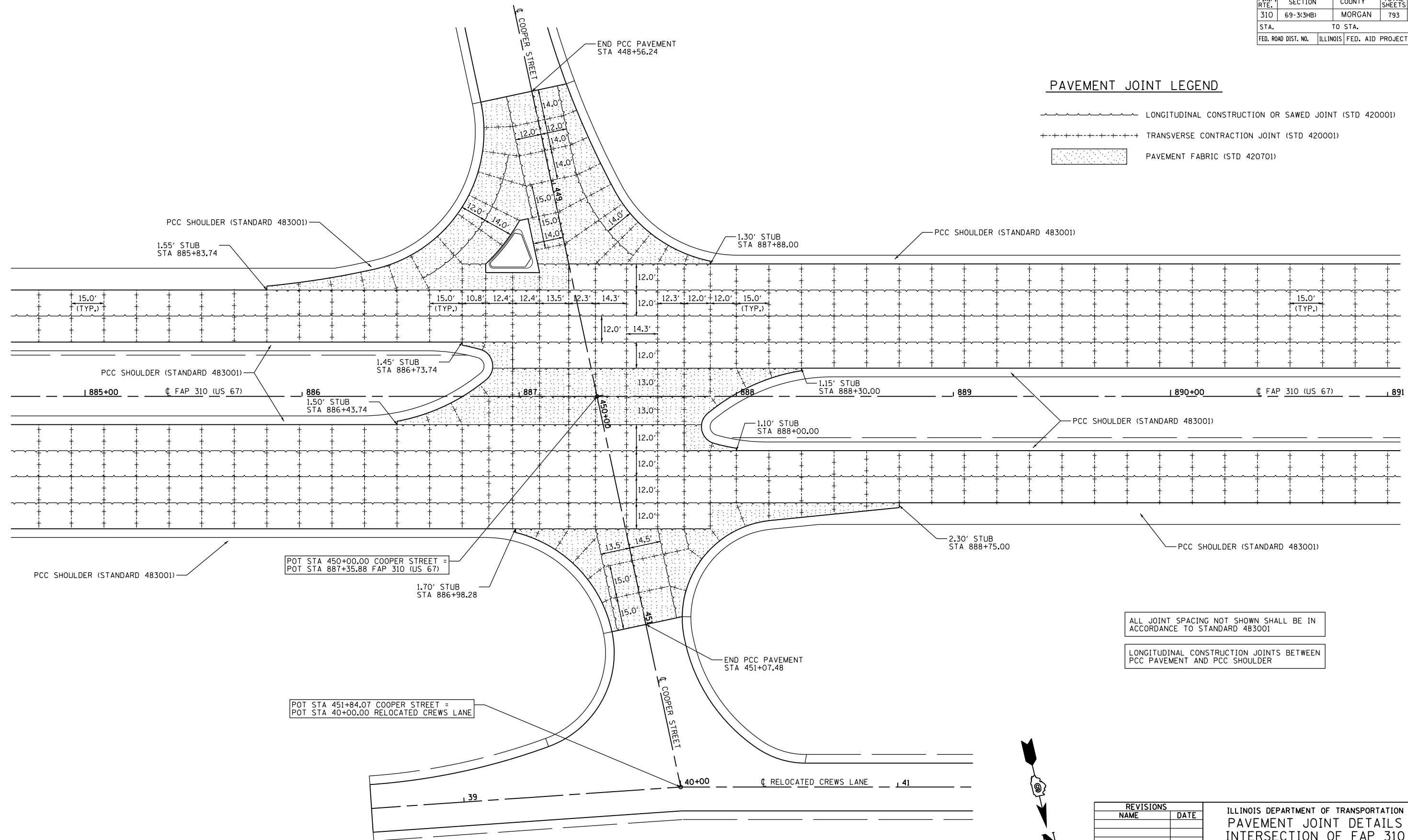
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	232
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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Mar-30-2011 08:47:45AM

**PAVEMENT JOINT LEGEND**

-  LONGITUDINAL CONSTRUCTION OR SAWED JOINT (STD 420001)
-  TRANSVERSE CONTRACTION JOINT (STD 420001)
-  PAVEMENT FABRIC (STD 420701)



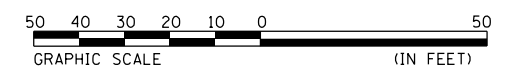
ALL JOINT SPACING NOT SHOWN SHALL BE IN ACCORDANCE TO STANDARD 483001

LONGITUDINAL CONSTRUCTION JOINTS BETWEEN PCC PAVEMENT AND PCC SHOULDER

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 PAVEMENT JOINT DETAILS  
 INTERSECTION OF FAP 310  
 COOPER ST & CREWS LANE  
 FAP 310 (US 67/IL 104)  
 SHEET 4 OF 13

DATE 7/06  
 DRAWN BY EBB  
 CHECKED BY



\$FILE\$

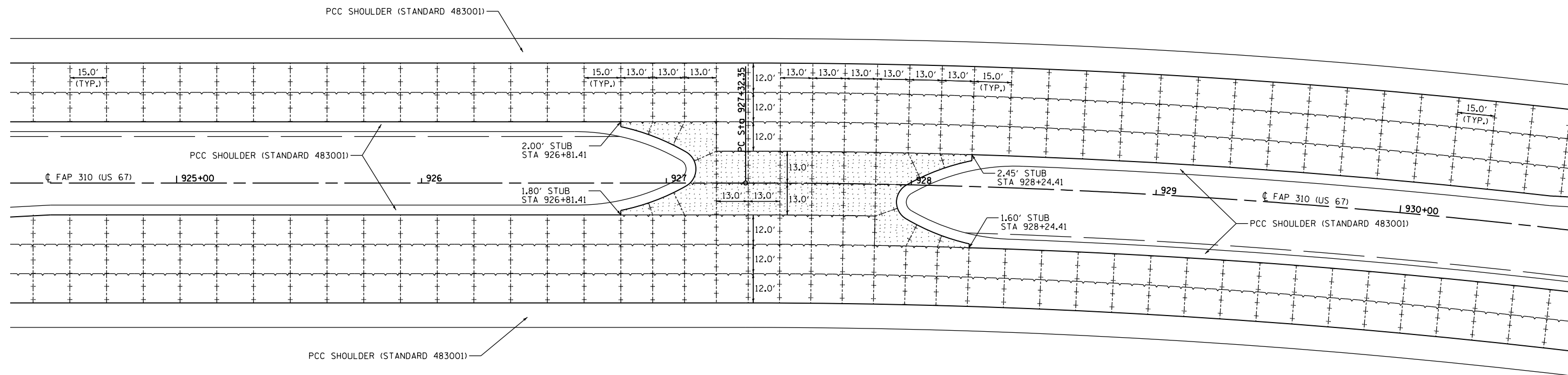


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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	233
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PROP. CURVE MLE16R2  
 PI STA. = 937+25.11  
 $\Delta = 37^\circ 10' 00''$  (RT)  
 $D = 1^\circ 56' 26''$   
 $R = 2,952.75'$   
 $T = 992.75'$   
 $L = 1,915.39'$   
 $E = 162.42'$   
 $e = 0.054$   
 P.C. STA. = 927+32.35  
 P.T. STA. = 946+47.74  
 S.E. TRANSITION  
 STA 925+31.00 TO STA 928+13.00  
 STA 945+67.00 TO STA 948+49.00

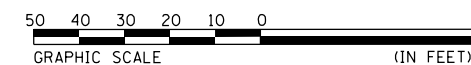


ALL JOINT SPACING NOT SHOWN SHALL BE IN ACCORDANCE TO STANDARD 483001

LONGITUDINAL CONSTRUCTION JOINTS BETWEEN PCC PAVEMENT AND PCC SHOULDER

**PAVEMENT JOINT LEGEND**

- LONGITUDINAL CONSTRUCTION OR SAWED JOINT (STD 420001)
- TRANSVERSE CONTRACTION JOINT (STD 420001)
- PAVEMENT FABRIC (STD 420701)



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 PAVEMENT JOINT DETAILS  
 CROSSOVER STA 927+53  
 FAP 310 (US 67/IL 104)  
 SHEET 5 OF 13

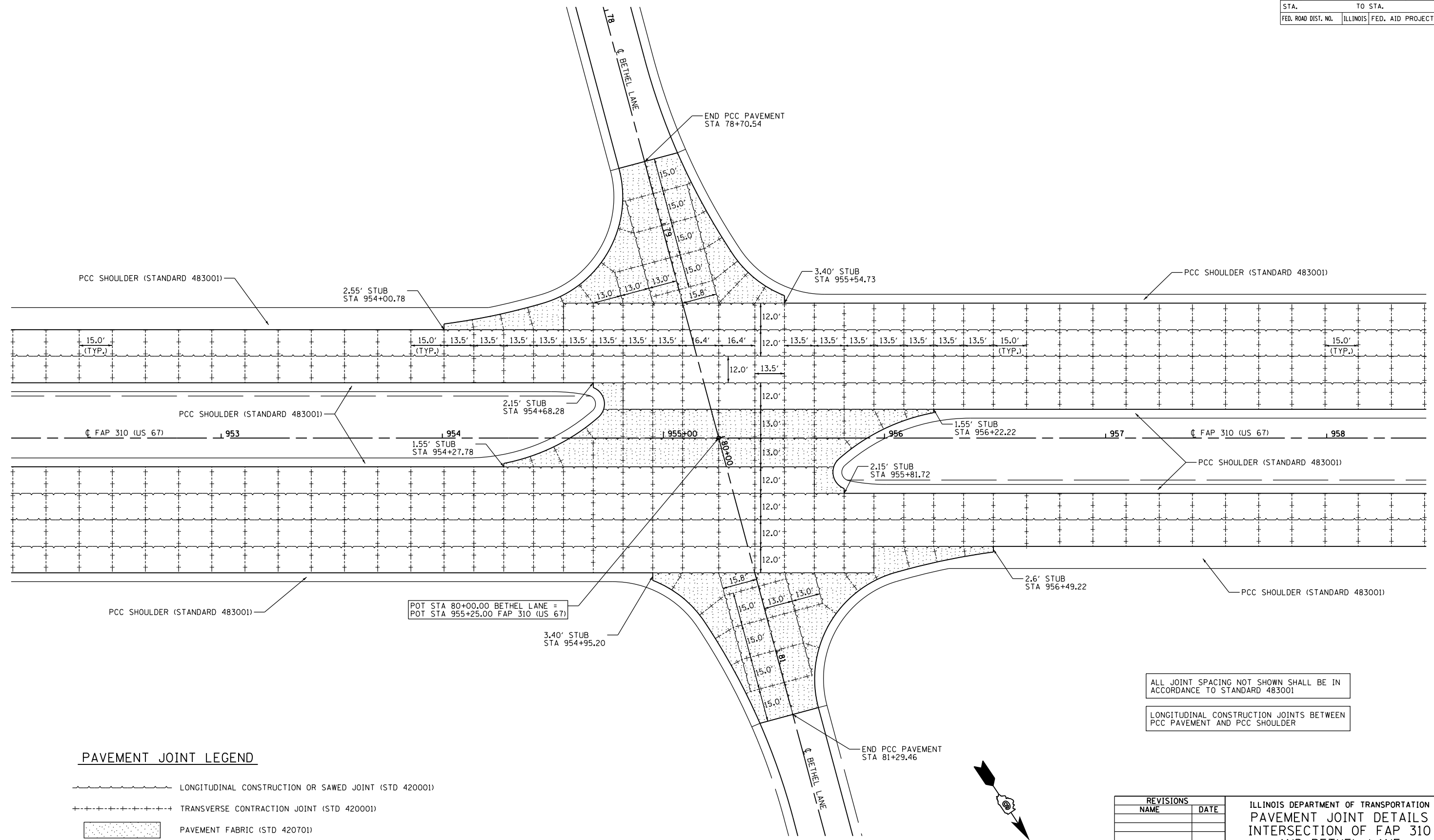
DATE 7/06  
 DRAWN BY EBB  
 CHECKED BY

\$FILE\$



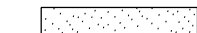
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	234
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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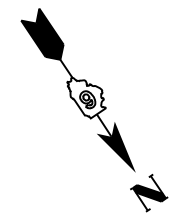
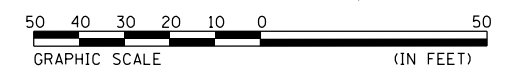


**PAVEMENT JOINT LEGEND**

-  LONGITUDINAL CONSTRUCTION OR SAWED JOINT (STD 420001)
-  TRANSVERSE CONTRACTION JOINT (STD 420001)
-  PAVEMENT FABRIC (STD 420701)

ALL JOINT SPACING NOT SHOWN SHALL BE IN ACCORDANCE TO STANDARD 483001

LONGITUDINAL CONSTRUCTION JOINTS BETWEEN PCC PAVEMENT AND PCC SHOULDER



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 PAVEMENT JOINT DETAILS  
 INTERSECTION OF FAP 310  
 AND BETHEL LANE  
 FAP 310 (US 67/IL 104)  
 SHEET 6 OF 13

DATE 7/06  
 DRAWN BY EBB  
 CHECKED BY

\$FILE\$

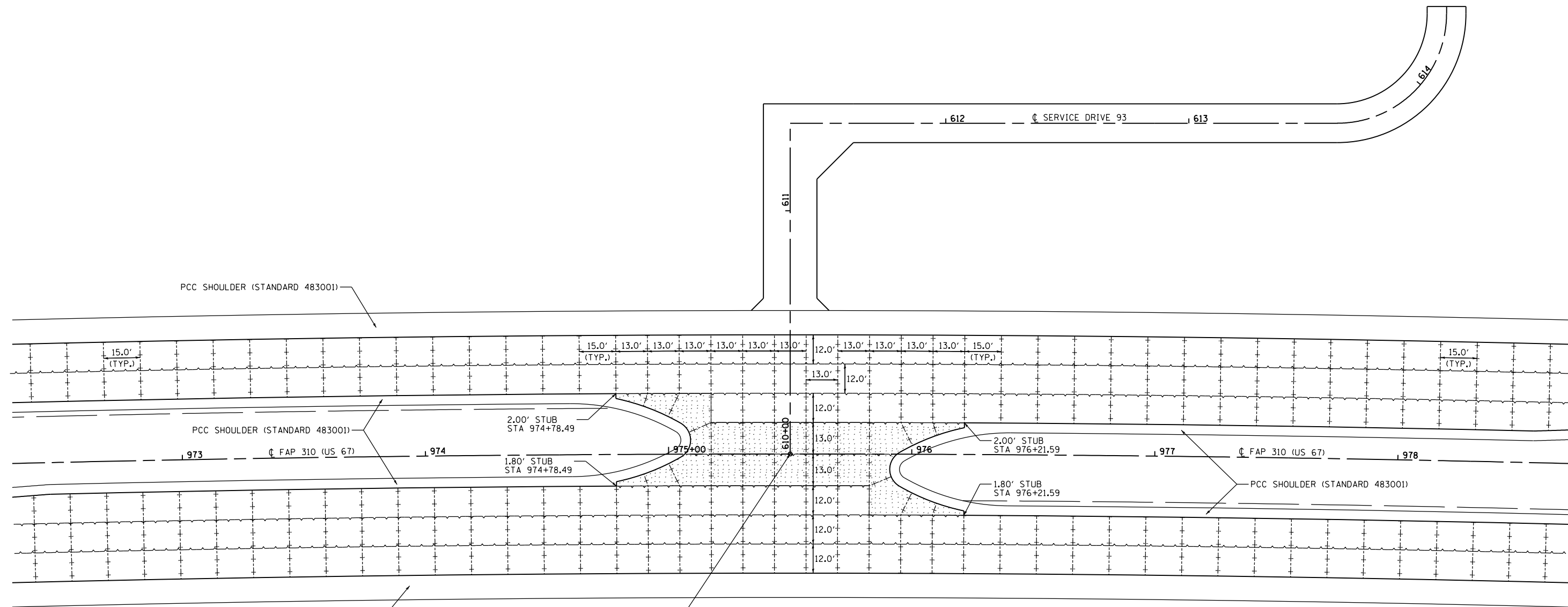
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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	235
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



POT STA 610+00.00 SERVICE DRIVE 93 =  
 POT STA 975+50.00 FAP 310 (US 67)

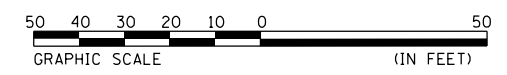
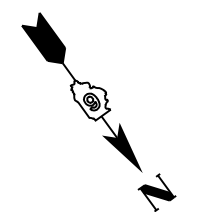
ALL JOINT SPACING NOT SHOWN SHALL BE IN ACCORDANCE TO STANDARD 483001

LONGITUDINAL CONSTRUCTION JOINTS BETWEEN PCC PAVEMENT AND PCC SHOULDER

PROP. CURVE MLE17  
 PI STA. = 979+37.53  
 $\Delta$  = 8° 10' 00" (RT)  
 D = 0° 26' 12"  
 R = 13,123.33'  
 T = 936.85'  
 L = 1,870.54'  
 E = 33.40'  
 e = 0.015  
 P.C. STA. = 970+00.67  
 P.T. STA. = 988+71.21  
 S.E. TRANSITION  
 STA 968+87.00 TO STA 970+23.00  
 STA 988+49.00 TO STA 989+85.00

**PAVEMENT JOINT LEGEND**

- LONGITUDINAL CONSTRUCTION OR SAWED JOINT (STD 420001)
- TRANSVERSE CONTRACTION JOINT (STD 420001)
- PAVEMENT FABRIC (STD 420701)



REVISIONS	
NAME	DATE

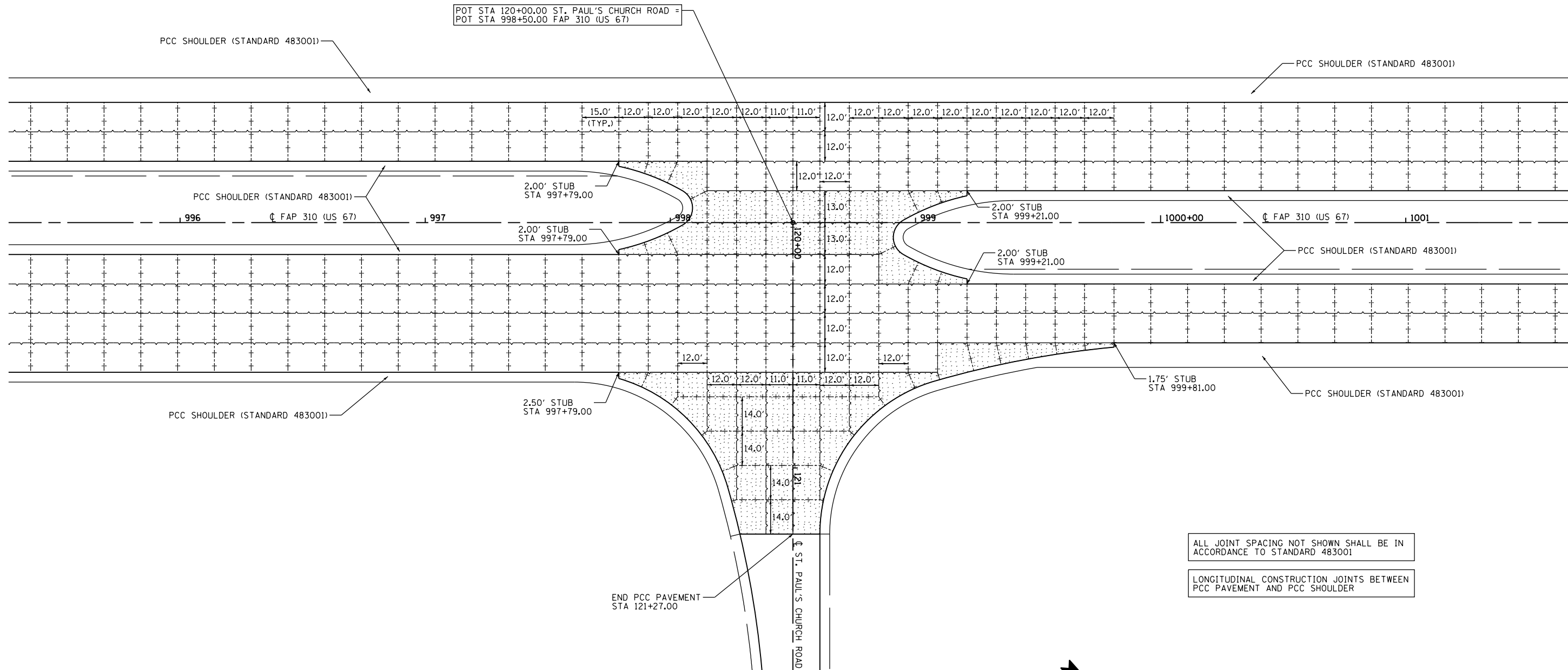
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 PAVEMENT JOINT DETAILS  
 CROSSOVER STA 975+50  
 FAP 310 (US 67/IL 104)  
 SHEET 7 OF 13  
 DATE 7/06  
 DRAWN BY EBB  
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	236
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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Mar-30-2011 08:48:12AM

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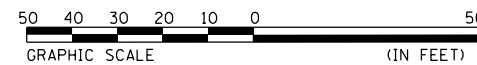
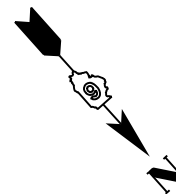


ALL JOINT SPACING NOT SHOWN SHALL BE IN ACCORDANCE TO STANDARD 483001

LONGITUDINAL CONSTRUCTION JOINTS BETWEEN PCC PAVEMENT AND PCC SHOULDER

**PAVEMENT JOINT LEGEND**

- LONGITUDINAL CONSTRUCTION OR SAWED JOINT (STD 420001)
- TRANSVERSE CONTRACTION JOINT (STD 420001)
- PAVEMENT FABRIC (STD 420701)



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 PAVEMENT JOINT DETAILS  
 INTERSECTION OF FAP 310  
 AND ST. PAUL'S CHURCH ROAD  
 FAP 310 (US 67/IL 104)  
 SHEET 8 OF 13

DATE 7/06  
 DRAWN BY EBB  
 CHECKED BY

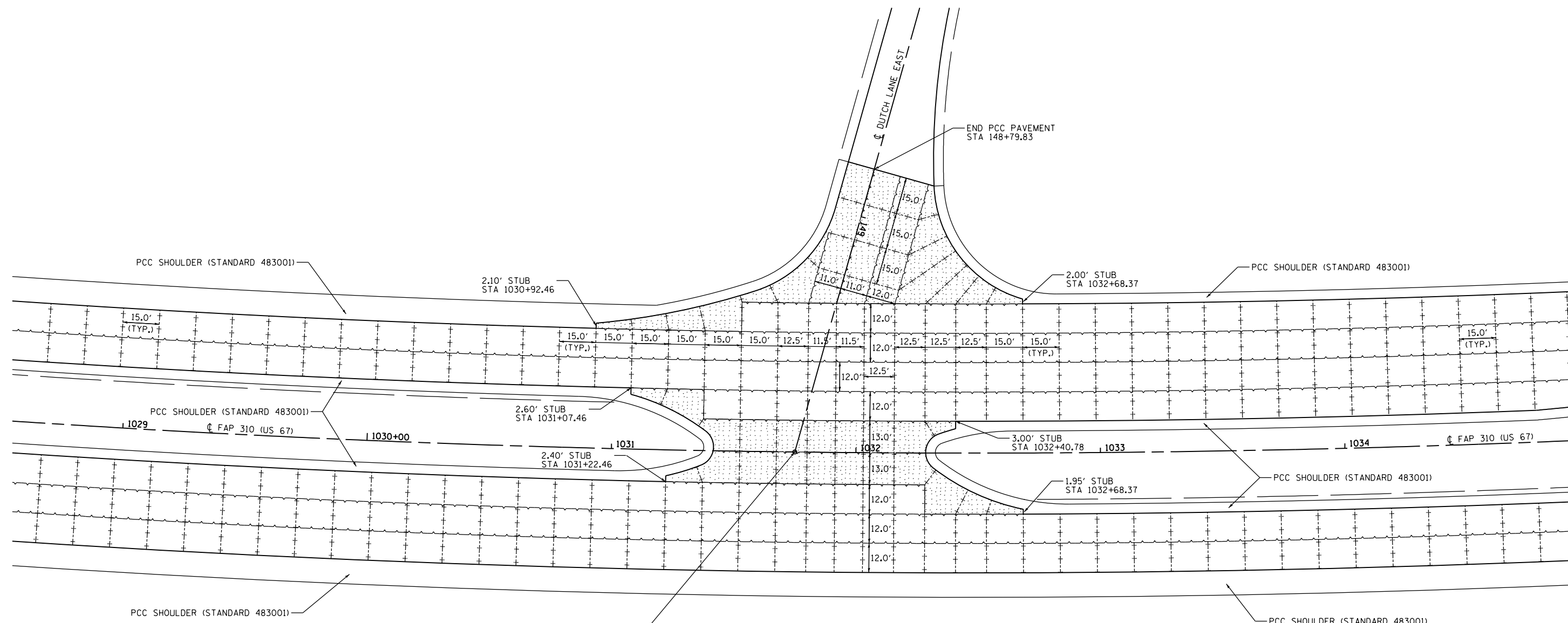
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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	237
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



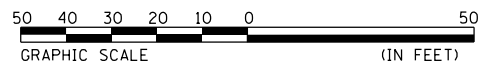
ALL JOINT SPACING NOT SHOWN SHALL BE IN ACCORDANCE TO STANDARD 483001

LONGITUDINAL CONSTRUCTION JOINTS BETWEEN PCC PAVEMENT AND PCC SHOULDER

PROP. CURVE MLE18  
 PI STA. = 1031+15.75  
 $\Delta = 16^\circ 26' 00''$  (LT)  
 $D = 0^\circ 58' 13''$   
 $R = 5,905.50'$   
 $T = 852.75'$   
 $L = 1,693.79'$   
 $E = 61.25'$   
 $e = 0.033$   
 P.C. STA. = 1022+63.00  
 P.T. STA. = 1039+56.79  
 S.E. TRANSITION  
 STA 1021+20.00 TO STA 1023+13.00  
 STA 1039+07.00 TO STA 1041+00.00

**PAVEMENT JOINT LEGEND**

- LONGITUDINAL CONSTRUCTION OR SAWED JOINT (STD 420001)
- TRANSVERSE CONTRACTION JOINT (STD 420001)
- PAVEMENT FABRIC (STD 420701)



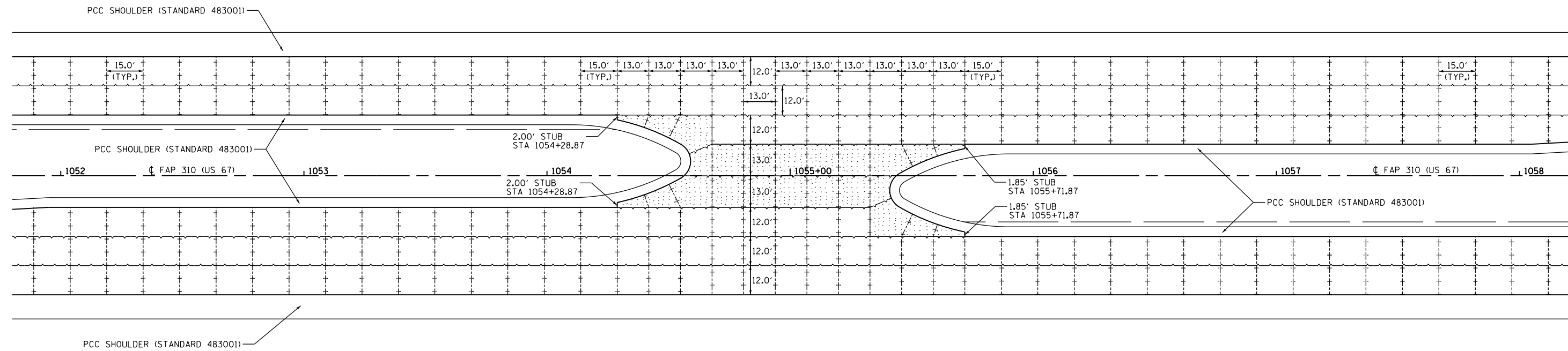
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 PAVEMENT JOINT DETAILS  
 INTERSECTION OF FAP 310  
 AND DUTCH LANE EAST  
 FAP 310 (US 67/IL 104)  
 SHEET 9 OF 13

DATE 7/06  
 DRAWN BY EBB  
 CHECKED BY

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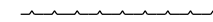
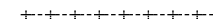

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	238
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

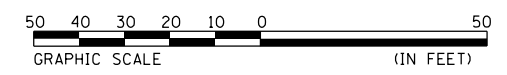
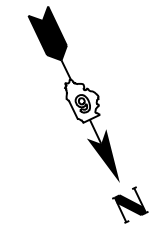


ALL JOINT SPACING NOT SHOWN SHALL BE IN ACCORDANCE TO STANDARD 483001

LONGITUDINAL CONSTRUCTION JOINTS BETWEEN PCC PAVEMENT AND PCC SHOULDER

**PAVEMENT JOINT LEGEND**

-  LONGITUDINAL CONSTRUCTION OR SAWED JOINT (STD 420001)
-  TRANSVERSE CONTRACTION JOINT (STD 420001)
-  PAVEMENT FABRIC (STD 420701)



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT JOINT DETAILS**  
 CROSSOVER STA 1055+00  
 FAP 310 (US 67/IL 104)  
 SHEET 10 OF 13

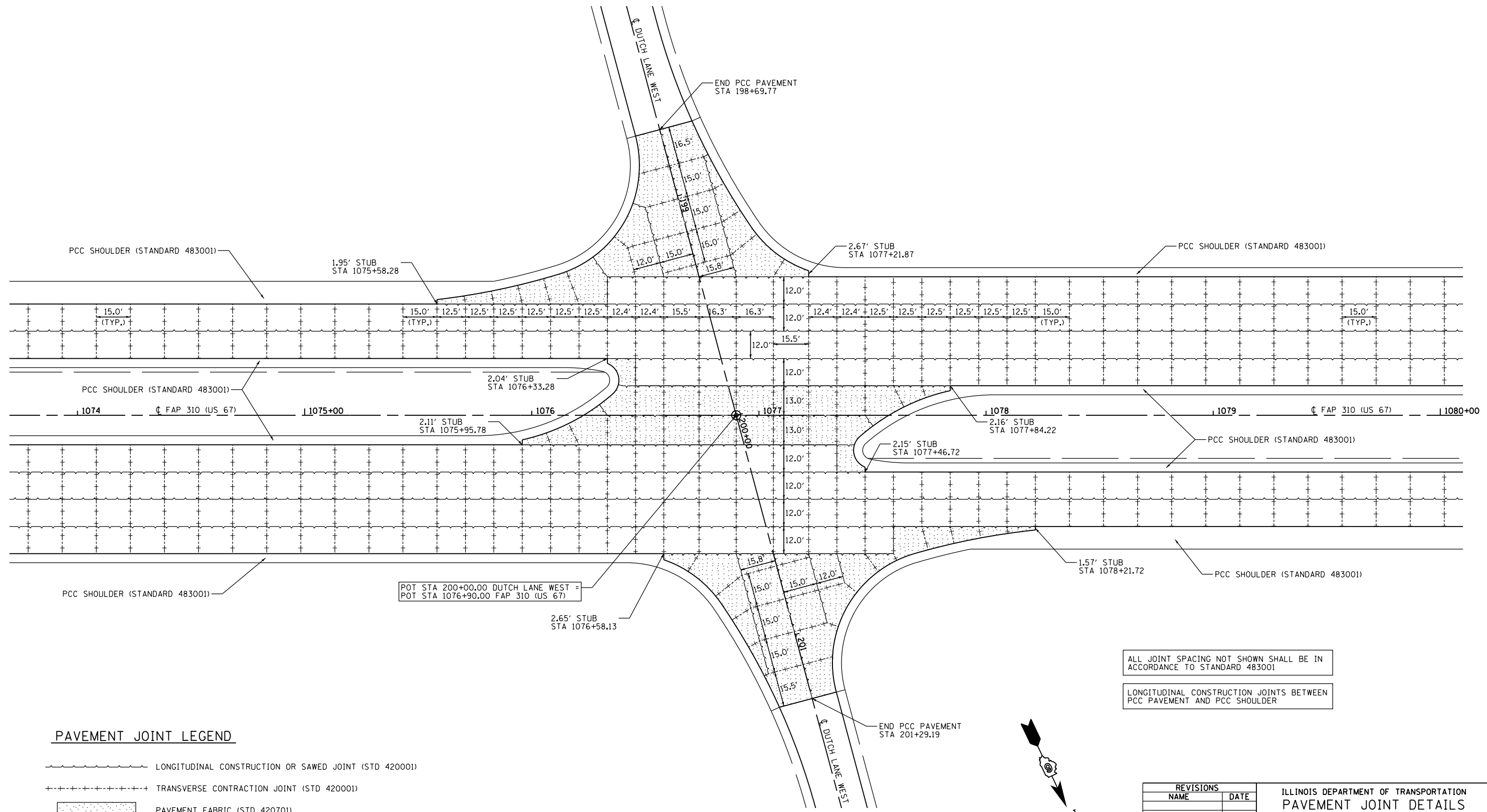
DATE 7/06  
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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	239
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



**PAVEMENT JOINT LEGEND**

- LONGITUDINAL CONSTRUCTION OR SAWED JOINT (STD 420001)
- TRANSVERSE CONTRACTION JOINT (STD 420001)
- PAVEMENT FABRIC (STD 420701)

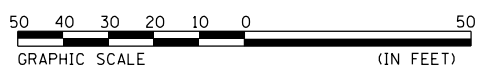
ALL JOINT SPACING NOT SHOWN SHALL BE IN ACCORDANCE TO STANDARD 483001

LONGITUDINAL CONSTRUCTION JOINTS BETWEEN PCC PAVEMENT AND PCC SHOULDER

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 PAVEMENT JOINT DETAILS  
 INTERSECTION OF FAP 310  
 AND DUTCH LANE WEST  
 FAP 310 (US 67/IL 104)  
 SHEET 11 OF 13

DATE 7/06  
 DRAWN BY EBB  
 CHECKED BY



SECTION 69-3(3HB)

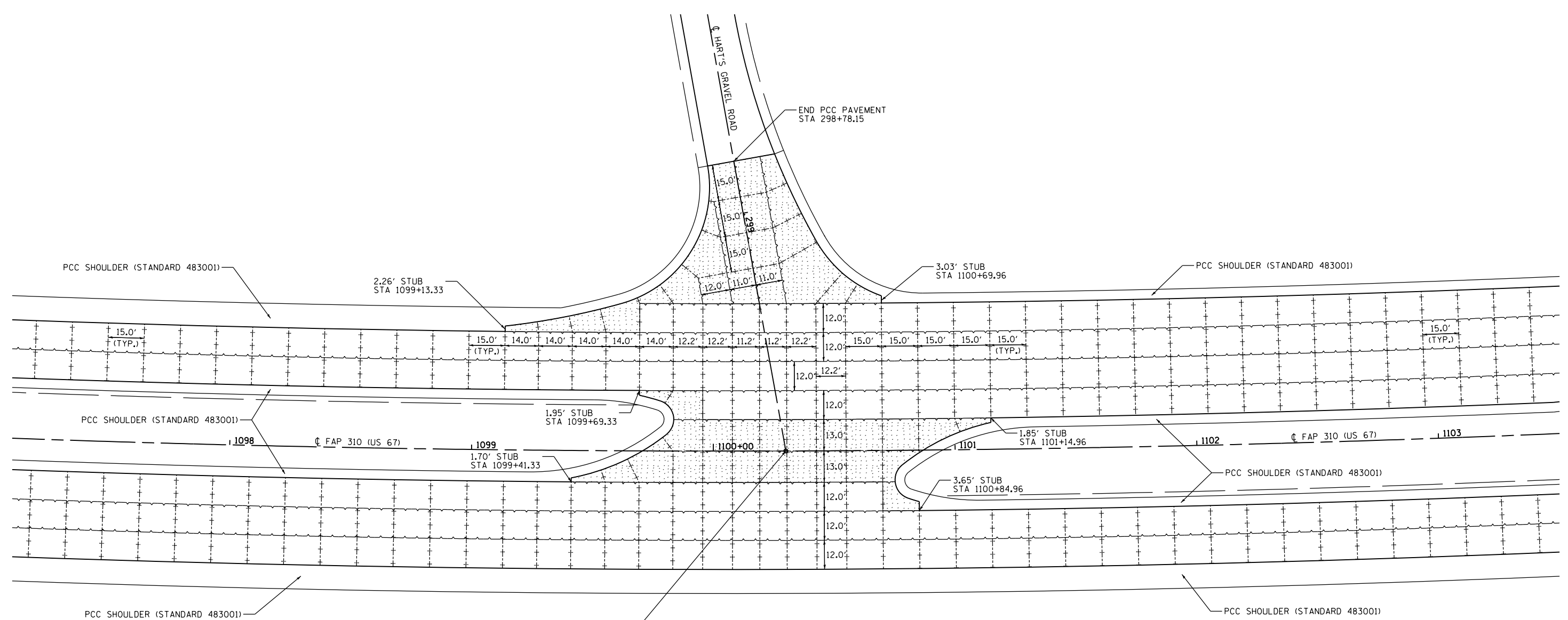
MORGAN COUNTY

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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	240
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



POT STA 300+00.00 HART'S GRAVEL ROAD =  
 POT STA 1100+30.00 FAP 310 (US 67)

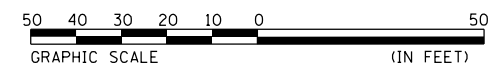
ALL JOINT SPACING NOT SHOWN SHALL BE IN ACCORDANCE TO STANDARD 483001

LONGITUDINAL CONSTRUCTION JOINTS BETWEEN PCC PAVEMENT AND PCC SHOULDER

PROP. CURVE MLE19  
 PI STA. = 1104+58.92  
 $\Delta = 20^\circ 36' 00''$  (LT)  
 $D = 0^\circ 41' 55''$   
 $R = 8,202.08'$   
 $T = 1,490.57'$   
 $L = 2,948.96'$   
 $E = 134.34'$   
 $e = 0.025$   
 P.C. STA. = 1089+68.35  
 P.T. STA. = 1119+17.31  
 S.E. TRANSITION  
 STA 1088+44.00 TO STA 1090+06.00  
 STA 1118+80.00 TO STA 1120+42.00

**PAVEMENT JOINT LEGEND**

- LONGITUDINAL CONSTRUCTION OR SAWED JOINT (STD 420001)
- TRANSVERSE CONTRACTION JOINT (STD 420001)
- PAVEMENT FABRIC (STD 420701)



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 PAVEMENT JOINT DETAILS  
 INTERSECTION OF FAP 310  
 AND HART'S GRAVEL ROAD  
 FAP 310 (US 67/IL 104)  
 SHEET 12 OF 13

DATE 7/06  
 DRAWN BY EBB  
 CHECKED BY

FILE\$



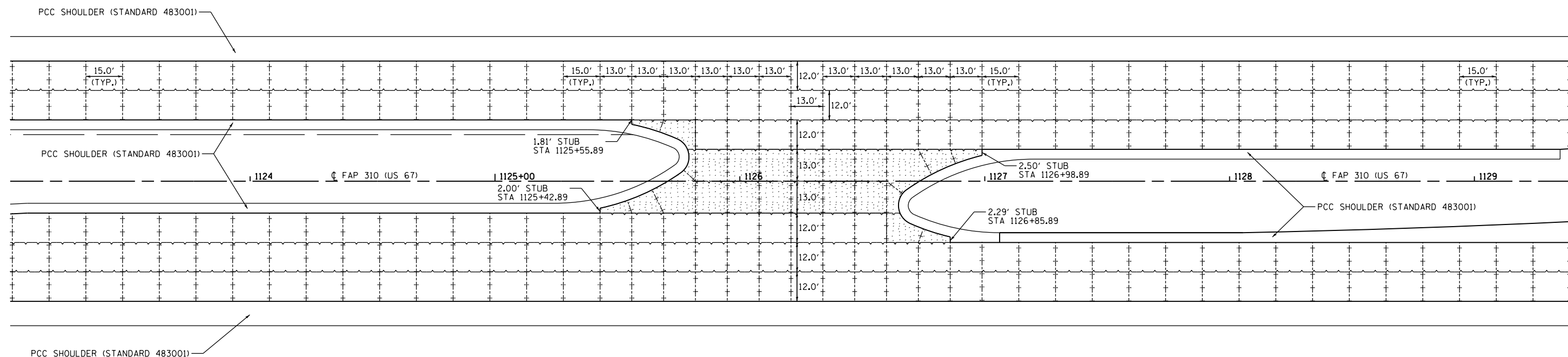
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CONTRACT NO. 72667

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	241
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

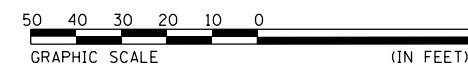


ALL JOINT SPACING NOT SHOWN SHALL BE IN ACCORDANCE TO STANDARD 483001

LONGITUDINAL CONSTRUCTION JOINTS BETWEEN PCC PAVEMENT AND PCC SHOULDER

**PAVEMENT JOINT LEGEND**

- LONGITUDINAL CONSTRUCTION OR SAWED JOINT (STD 420001)
- +++++ TRANSVERSE CONTRACTION JOINT (STD 420001)
- PAVEMENT FABRIC (STD 420701)

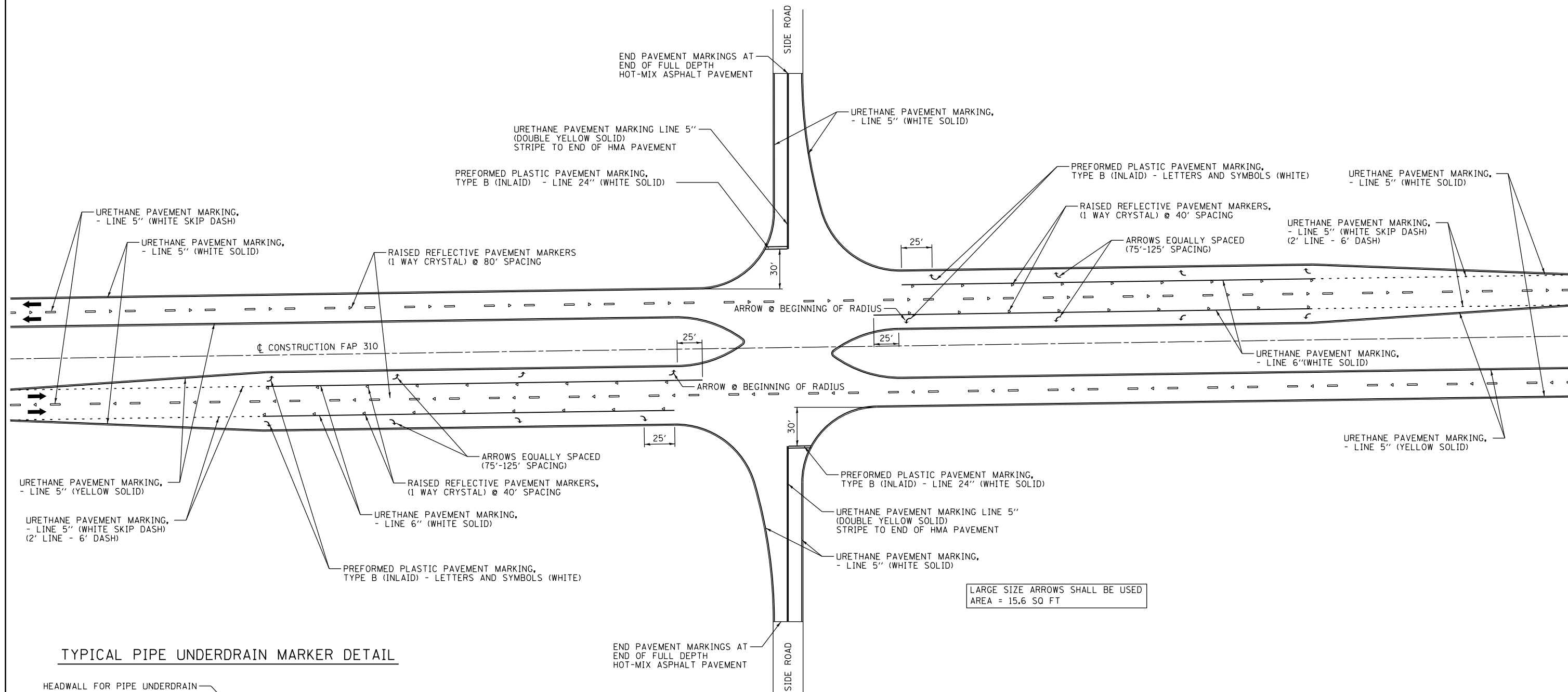


REVISIONS	
NAME	DATE

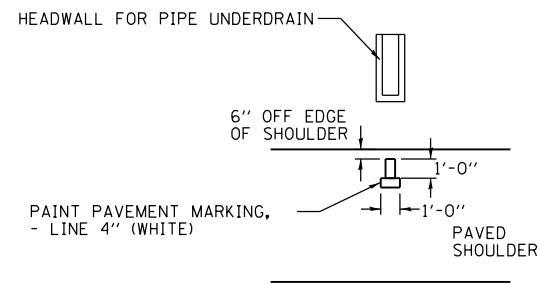
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 PAVEMENT JOINT DETAILS  
 CROSSOVER STA 1126+22  
 FAP 310 (US 67/IL 104)  
 SHEET 13 OF 13

DATE 7/06  
 DRAWN BY EBB  
 CHECKED BY

TYPICAL FINAL INTERSECTION STRIPING DETAIL



TYPICAL PIPE UNDERDRAIN MARKER DETAIL



OUTLET MARKERS SHALL BE PLACED ON FINAL SHOULDER

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
c:\pwork\pwork\pwork\sparksgw\0264881\Sign	TYP.dgn	DRAWN -	REVISED -
	PLOT SCALE = 100.0002' / in.	CHECKED -	REVISED -
	PLOT DATE = Mar-30-2011 08:48:55AM	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

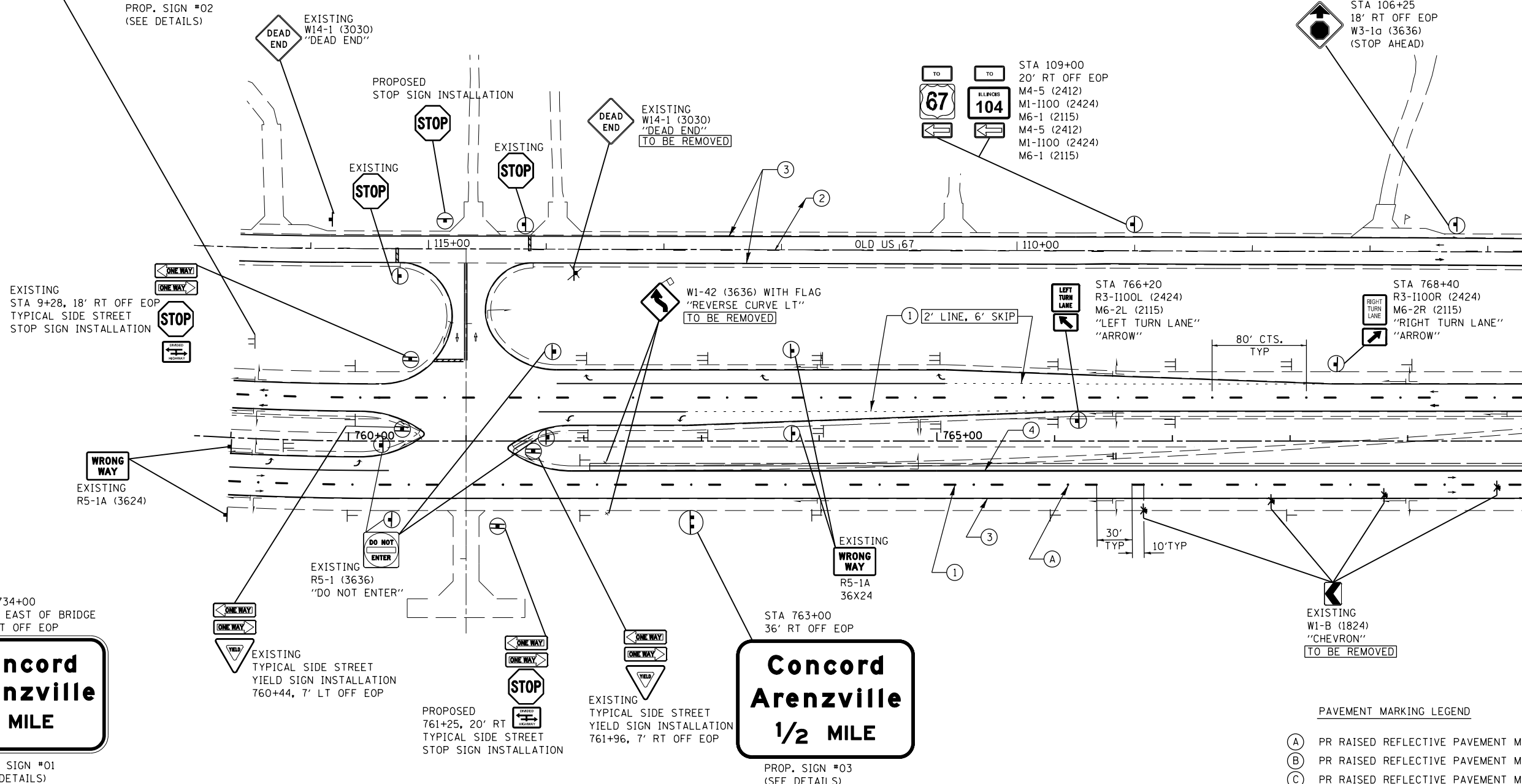
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	242
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

STA 759+00, 36' LT OFF EOP

**Jacksonville 6**  
**Taylorville 61**  
**Alton 72**

DELINEATOR SYMBOLS

├ - SINGLE  
 ═ - DOUBLE



STA 734+00  
 (JUST EAST OF BRIDGE  
 36' RT OFF EOP)

**Concord  
 Arenzville  
 1 MILE**

STA 763+00  
 36' RT OFF EOP

**Concord  
 Arenzville  
 1/2 MILE**

- PAVEMENT MARKING LEGEND
- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
  - (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
  - (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
  - (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
  - (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
  - (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
  - (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
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  - (6) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 8" (SOLID WHITE)
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  - (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
  - (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

TYPICAL SIDE STREET STOP SIGN INSTALLATION

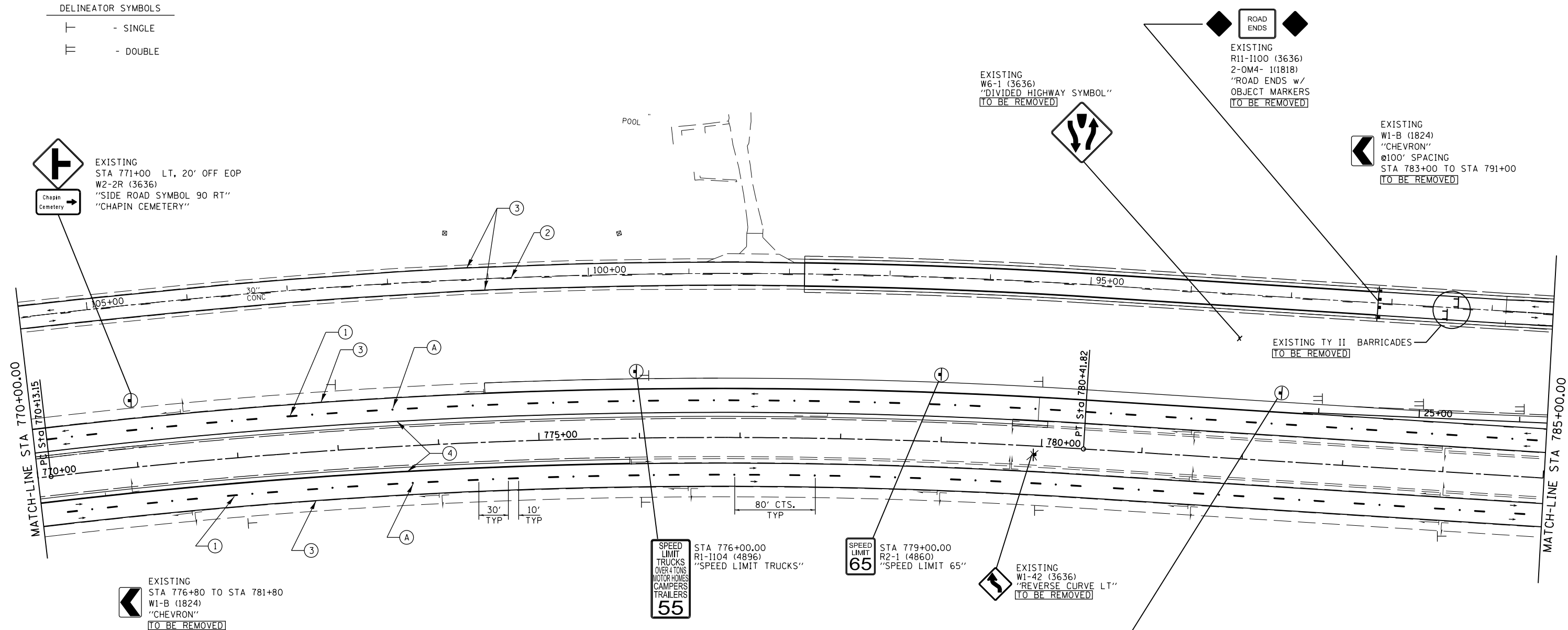
R6-1L (3612) "ONE WAY LT" BACK TO BACK  
 R61R (3612) "ONE WAY RT"  
 R1-1 (3636) "STOP"  
 R6-3A (2418) "DIVIDED HIGHWAY"  
 INSTALL ON 2 TELESCOPING STEEL POST

TYPICAL SIDE STREET YIELD SIGN INSTALLATION

R6-1L (3612) "ONE WAY LT" BACK TO BACK  
 R61R (3612) "ONE WAY RT"  
 R1-2 (3636) "YIELD"  
 INSTALL ON 2 TELESCOPING STEEL POST

DELINEATOR SYMBOLS

- ├ - SINGLE
- ├├ - DOUBLE



EXISTING  
STA 776+80 TO STA 781+80  
W1-B (1824)  
"CHEVRON"  
TO BE REMOVED

SPEED LIMIT  
TRUCKS  
OVER 4 TONS  
MOTOR HOMES  
CAMPERS  
TRAILERS  
55  
STA 776+00.00  
R1-1104 (4896)  
"SPEED LIMIT TRUCKS"

SPEED LIMIT  
65  
STA 779+00.00  
R2-1 (4860)  
"SPEED LIMIT 65"

EXISTING  
W1-42 (3636)  
"REVERSE CURVE LT"  
TO BE REMOVED

PROPOSED  
STA 782+50  
20' LT OFF EOP  
M3-3 (2412)  
M1-1100 (2424)  
M3-2 (2412)  
M1-1100 (2424)

PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
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- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)



FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
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	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
	PLOT DATE = Mar-30-2011 08:49:02AM	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	244
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DELINEATOR SYMBOLS

- T - SINGLE
- ≡ - DOUBLE



EXISTING  
 STA 783+00 TO STA 791+00  
 @100' SPACING  
 W1-B (1824)  
 "CHEVRON"  
 [TO BE REMOVED]

PT. STA. 28+19.90 EX. EB. TRANSITION =  
 STA 85+28.86 (EX US 67 / IL 104) =  
 STA 6+80.47 (US 67 FRONTAGE RD.)



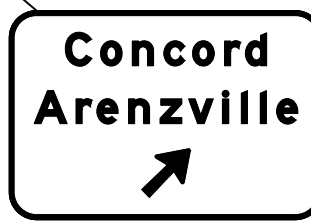
STA 86+00.00  
 LT. 20' OFF EOP  
 W1-4L (3636)  
 W13-1(2424)  
 "REVERSE CURVE  
 LEFT AHEAD"  
 SIGN w/ "40 MPH"  
 ADV. PLATE



W2-2R (3636)  
 W16-8 (3615)  
 "SIDE ROAD SYMBOL 90 RT  
 & ARENZVILLE RD."  
 [TO BE REMOVED]



STA 795+50  
 LT. CENTER OF GORE  
 W4-1R (3636)  
 "MERGE ARROW RT"



STA 789+57.68  
 RT. 36' OFF EOP  
 PR SIGN #04  
 (SEE DETAILS)



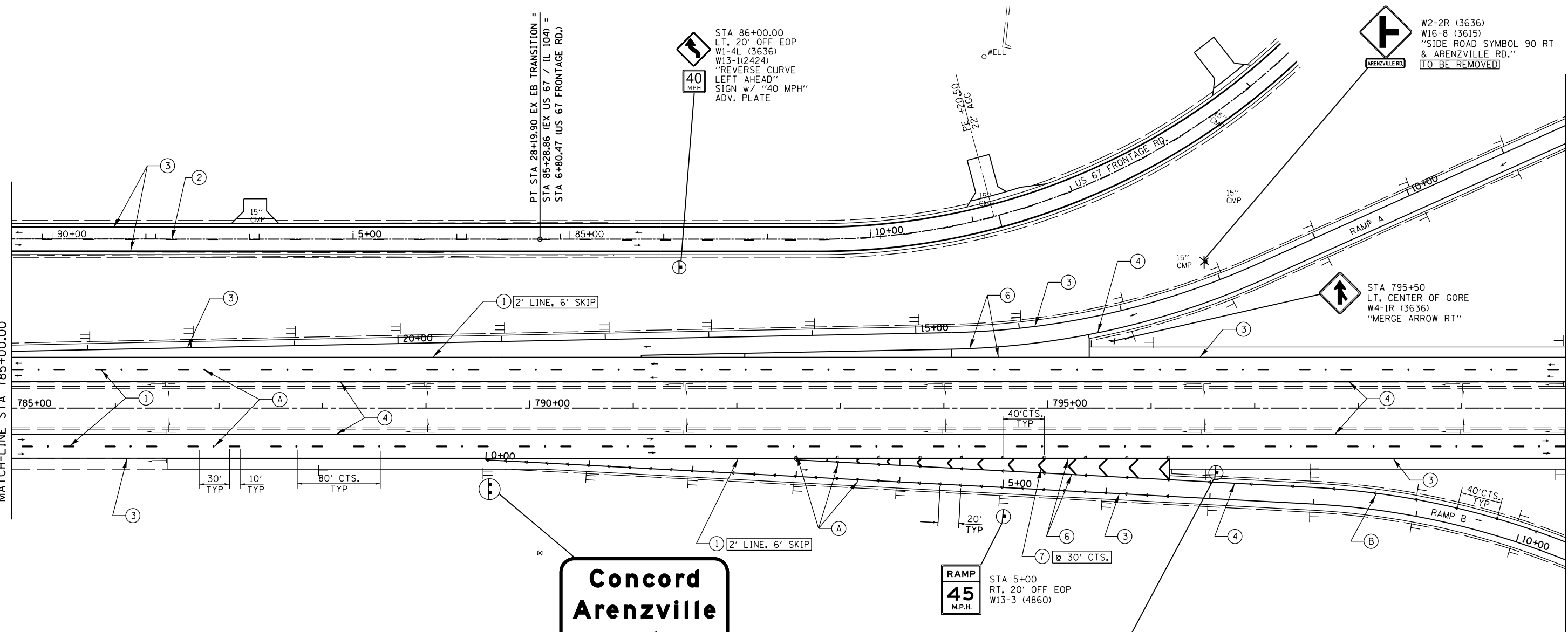
STA 5+00  
 RT. 20' OFF EOP  
 W13-3 (4860)



STA 796+72.68  
 RT. CENTER OF GORE  
 PR SIGN #05  
 (SEE DETAILS)

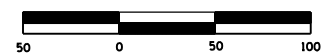
MATCH-LINE STA 785+00.00

MATCH-LINE STA 800+00.00



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
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FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
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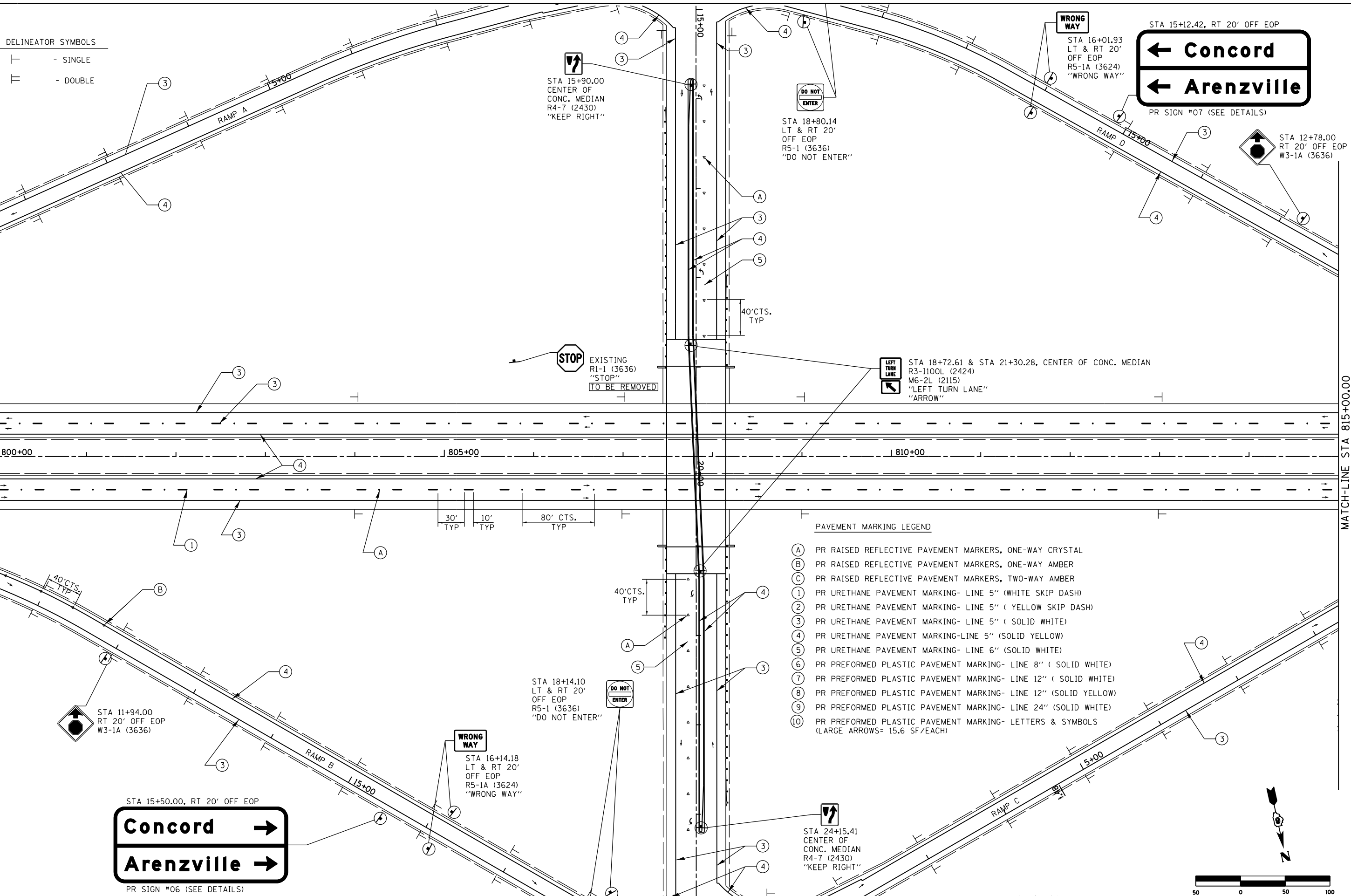
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

STRIPING & SIGNING PLAN SHEETS  
 FAP 310 (US 67)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	245
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DELINEATOR SYMBOLS  
 T - SINGLE  
 TT - DOUBLE



STA 15+90.00  
 CENTER OF  
 CONC. MEDIAN  
 R4-7 (2430)  
 "KEEP RIGHT"

STA 18+80.14  
 LT & RT 20'  
 OFF EOP  
 R5-1 (3636)  
 "DO NOT ENTER"

WRONG WAY  
 STA 16+01.93  
 LT & RT 20'  
 OFF EOP  
 R5-1A (3624)  
 "WRONG WAY"

← Concord  
 ← Arenzville  
 PR SIGN #07 (SEE DETAILS)

STA 12+78.00  
 RT 20' OFF EOP  
 W3-1A (3636)

STOP EXISTING  
 R1-1 (3636)  
 "STOP"  
 TO BE REMOVED

LEFT TURN LANE  
 STA 18+72.61 & STA 21+30.28, CENTER OF CONC. MEDIAN  
 R3-1100L (2424)  
 M6-2L (2115)  
 "LEFT TURN LANE"  
 "ARROW"

STA 18+14.10  
 LT & RT 20'  
 OFF EOP  
 R5-1 (3636)  
 "DO NOT ENTER"

WRONG WAY  
 STA 16+14.18  
 LT & RT 20'  
 OFF EOP  
 R5-1A (3624)  
 "WRONG WAY"

STA 24+15.41  
 CENTER OF  
 CONC. MEDIAN  
 R4-7 (2430)  
 "KEEP RIGHT"

Concord →  
 Arenzville →  
 PR SIGN #06 (SEE DETAILS)

- PAVEMENT MARKING LEGEND
- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
  - (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
  - (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
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FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
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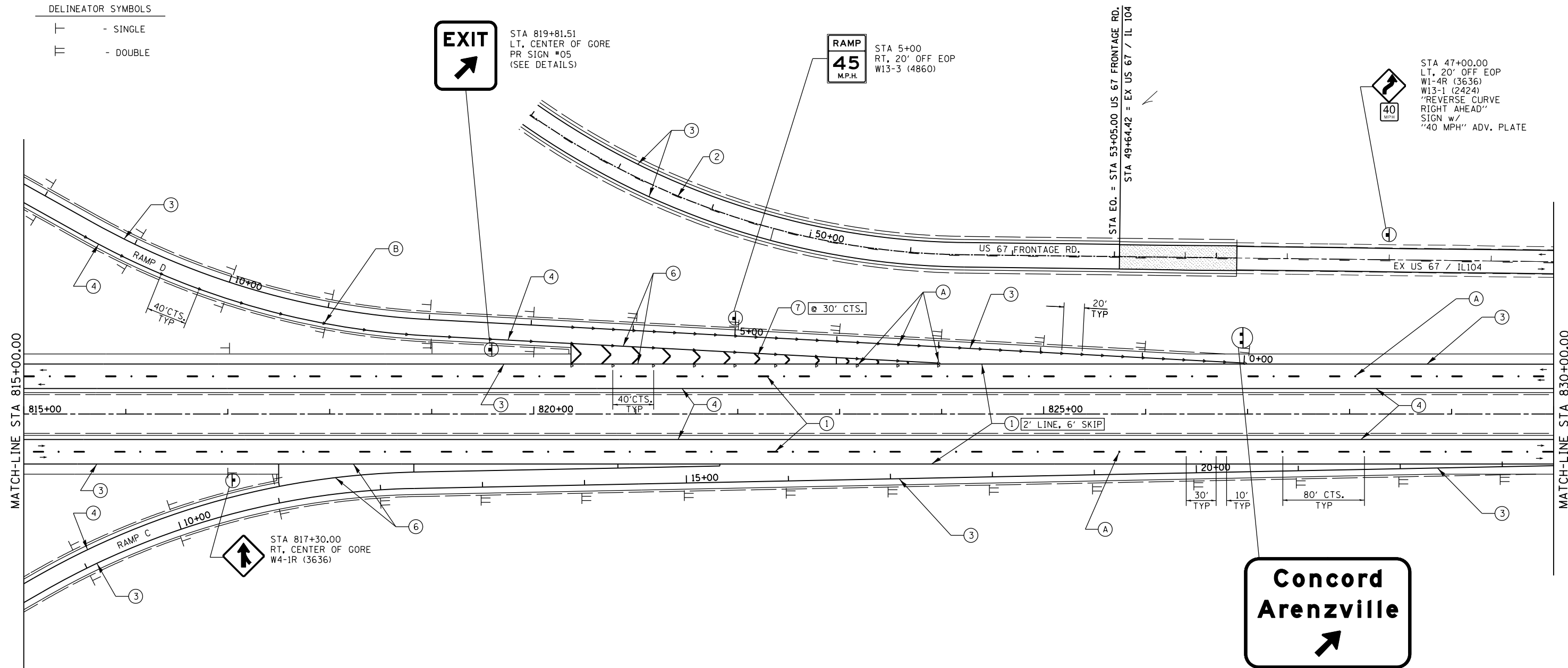
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

STRIPING & SIGNING PLAN SHEETS  
 FAP 310 (US 67)

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	246
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DELINEATOR SYMBOLS

- ┆ - SINGLE
- ≡ - DOUBLE



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
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FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

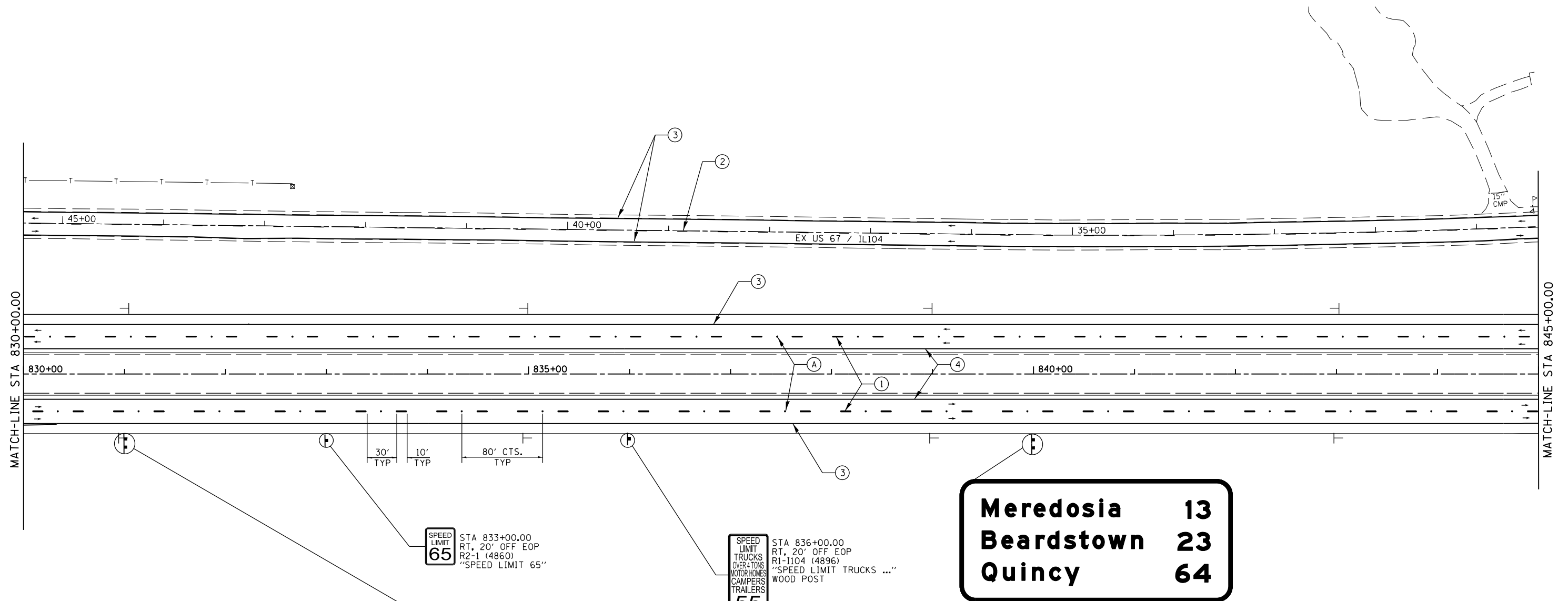
STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	247
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DELINEATOR SYMBOLS

- T - SINGLE
- TT - DOUBLE



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
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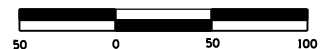
SPEED LIMIT 65  
 STA 833+00.00  
 RT, 20' OFF EOP  
 R2-1 (4860)  
 "SPEED LIMIT 65"

SPEED LIMIT 55  
 STA 836+00.00  
 RT, 20' OFF EOP  
 R1-1104 (4896)  
 "SPEED LIMIT TRUCKS ..."  
 WOOD POST

NORTH WEST  
 67 104  
 STA 831+00.00  
 RT 20' OFF EOP  
 M3-1 (2412)  
 M1-1100 (2424)  
 M3-4 (2412)  
 M1-1100 (2424)

**Meredosia 13**  
**Beardstown 23**  
**Quincy 64**

STA 840+00.00, RT 36' OFF EOP  
 PR SIGN #12 (SEE DETAILS)



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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION



STRIPING & SIGNING PLAN SHEETS  
 FAP 310 (US 67)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	248
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



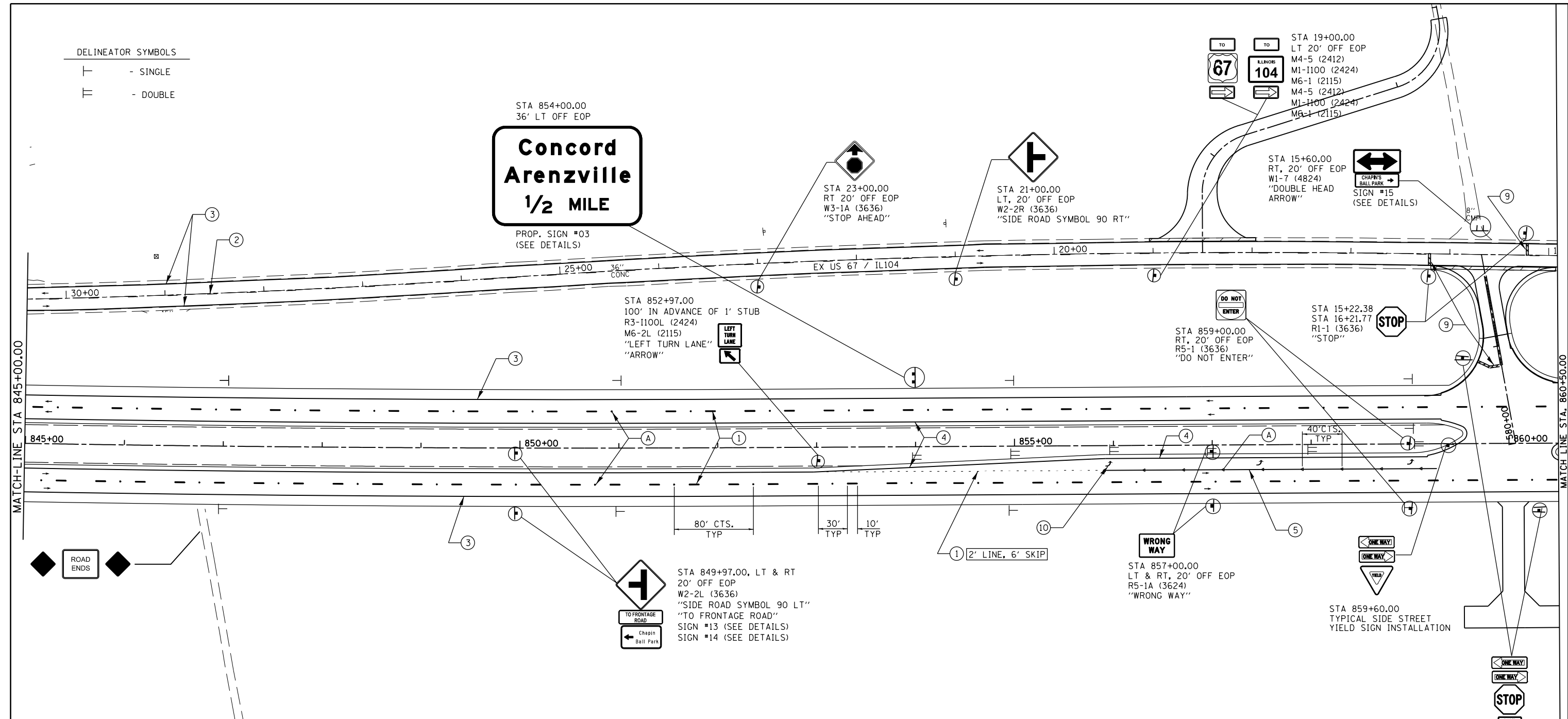
DELINEATOR SYMBOLS

-  - SINGLE
-  - DOUBLE

STA 854+00.00  
36' LT OFF EOP

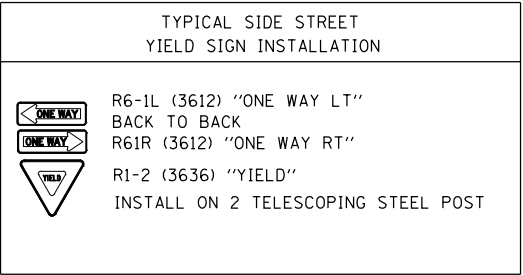
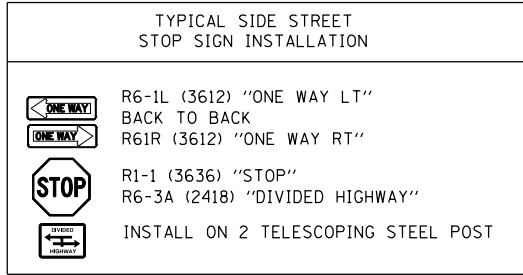
**Concord  
Arenzville  
1/2 MILE**

PROP. SIGN #03  
(SEE DETAILS)



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

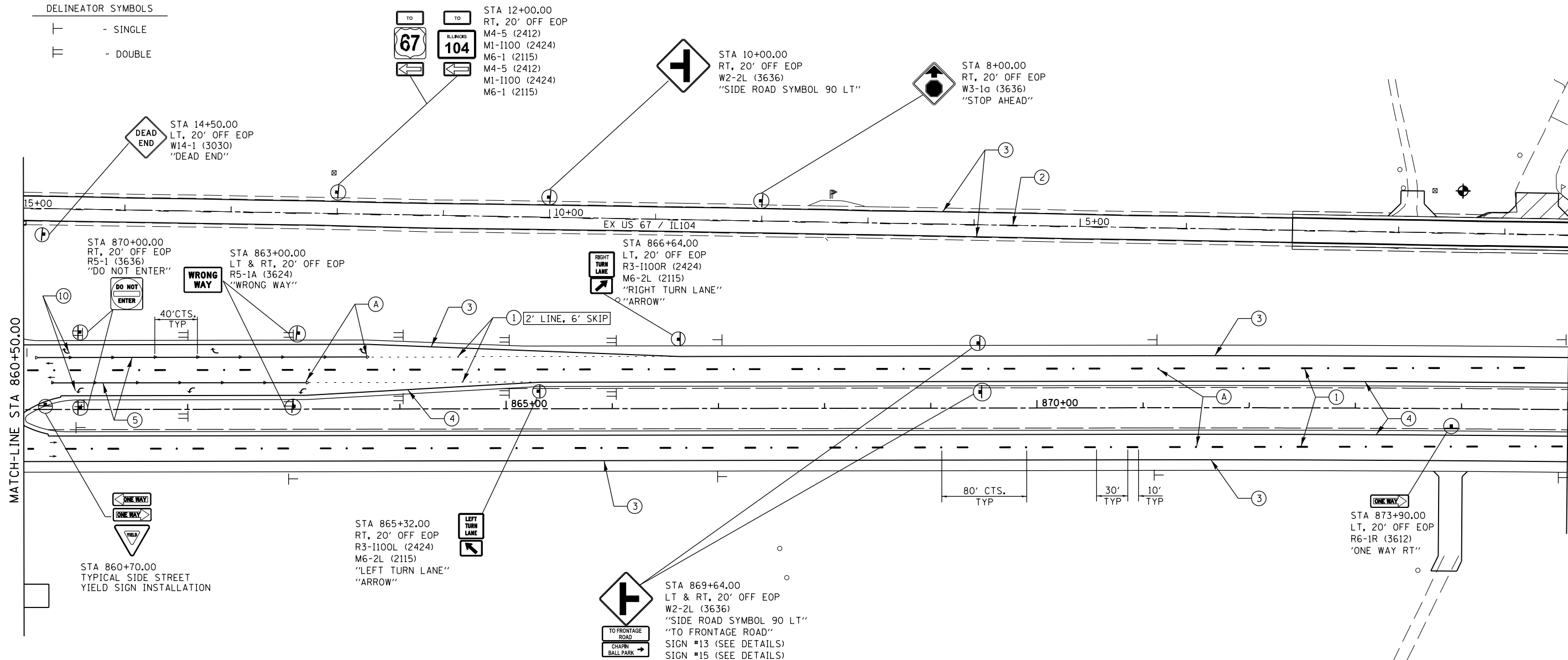
STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	249
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

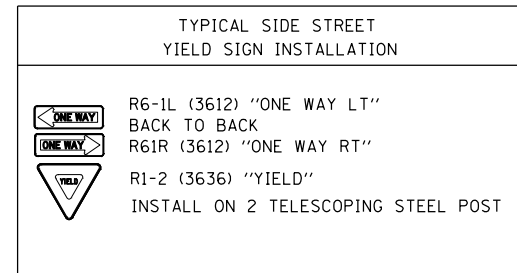
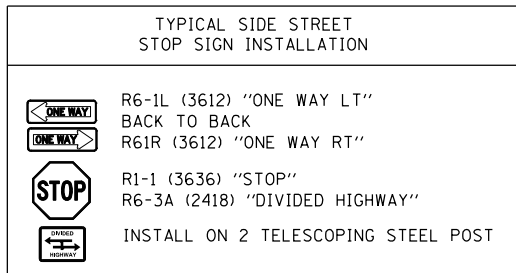
DELINEATOR SYMBOLS

- ├ - SINGLE
- ═ - DOUBLE



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
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- (8) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)



FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Mar-30-2011 08:49:07AM	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	250
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DELINEATOR SYMBOLS

- T - SINGLE
- ≡ - DOUBLE

**Concord  
Arenzville  
1 MILE**

STA 880+00.00  
LT, 36' OFF EOP  
PR SIGN #01  
(SEE DETAILS)

STA 578+75.00  
CENTERLINE  
R11-1108 (3636)  
.2-OM4-1 (1818)  
"ROAD ENDS"  
w/ 2- OBJECT MARKERS

STA 449+00.00  
RT, 12' OFF EOP  
R1-1 (3636)  
R1-1100 (3018)  
"STOP"  
"CROSS TRAFFIC  
DOES NOT STOP"

STA 584+00.00  
LT, 20' OFF EOP  
W8-1108 (3636)  
"ROAD ENDS 500 FT"

STA 888+50.00  
LT & RT, 20' OFF EOP  
R5-1 (3636)  
"DO NOT ENTER"

STA 883+40.00  
LT & RT, 20' OFF EOP  
R5-1A (3624)  
"WRONG WAY"

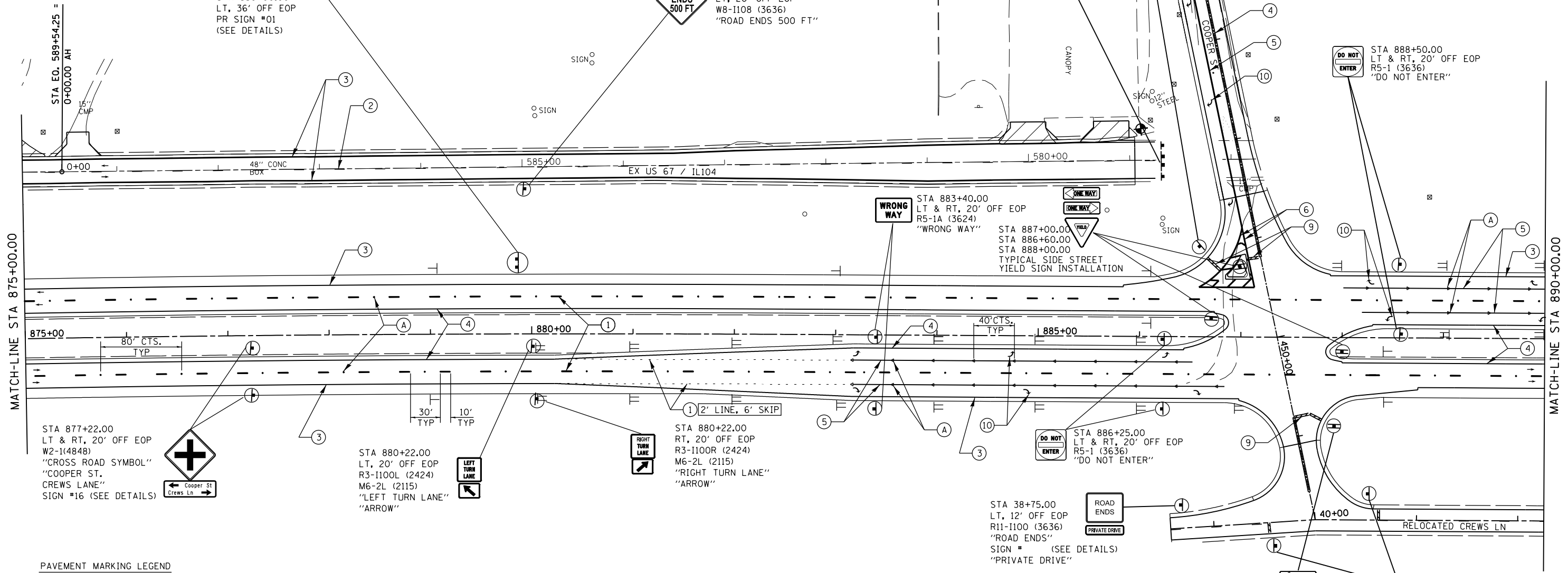
STA 887+00.00  
STA 886+60.00  
STA 888+00.00  
TYPICAL SIDE STREET  
YIELD SIGN INSTALLATION

STA 886+25.00  
LT & RT, 20' OFF EOP  
R5-1 (3636)  
"DO NOT ENTER"

STA 38+75.00  
LT, 12' OFF EOP  
R11-1100 (3636)  
"ROAD ENDS"  
SIGN # (SEE DETAILS)  
"PRIVATE DRIVE"

STA 39+58.10  
STA 40+53.04  
R1-1 (3636)  
"STOP"

STA 887+85.00  
TYPICAL SIDE STREET  
STOP SIGN INSTALLATION



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

TYPICAL SIDE STREET  
STOP SIGN INSTALLATION

	R6-1L (3612) "ONE WAY LT"
	R6-1R (3612) "ONE WAY RT"
	R1-1 (3636) "STOP"
	R6-3A (2418) "DIVIDED HIGHWAY"
INSTALL ON 2 TELESCOPING STEEL POST	

TYPICAL SIDE STREET  
YIELD SIGN INSTALLATION

	R6-1L (3612) "ONE WAY LT"
	R6-1R (3612) "ONE WAY RT"
	R1-2 (3636) "YIELD"
INSTALL ON 2 TELESCOPING STEEL POST	

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
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PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -	REVISED -
PLOT DATE = Mar-30-2011 08:49:07AM	DATE -	REVISED -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

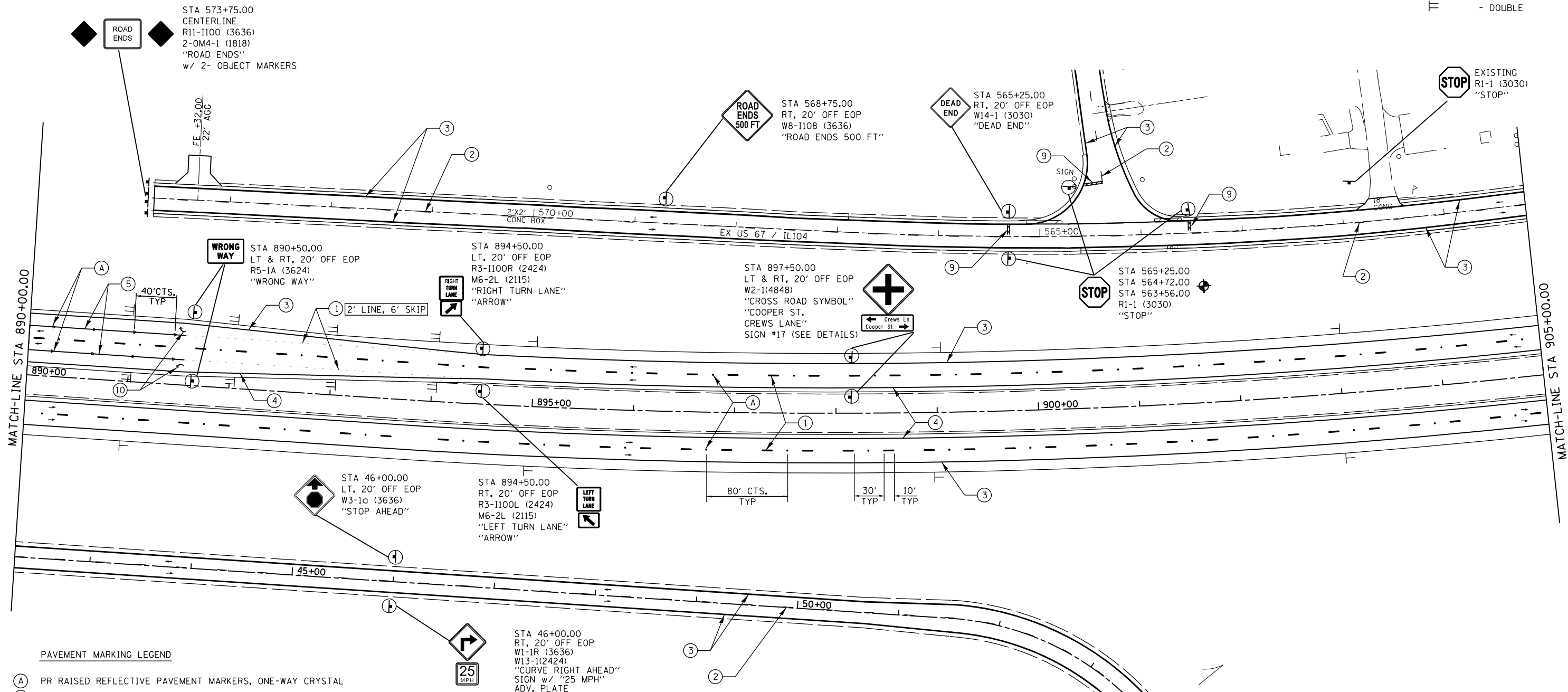
**STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	251
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DELINEATOR SYMBOLS

- ┆ - SINGLE
- ≡ - DOUBLE



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 8" (SOLID WHITE)
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- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

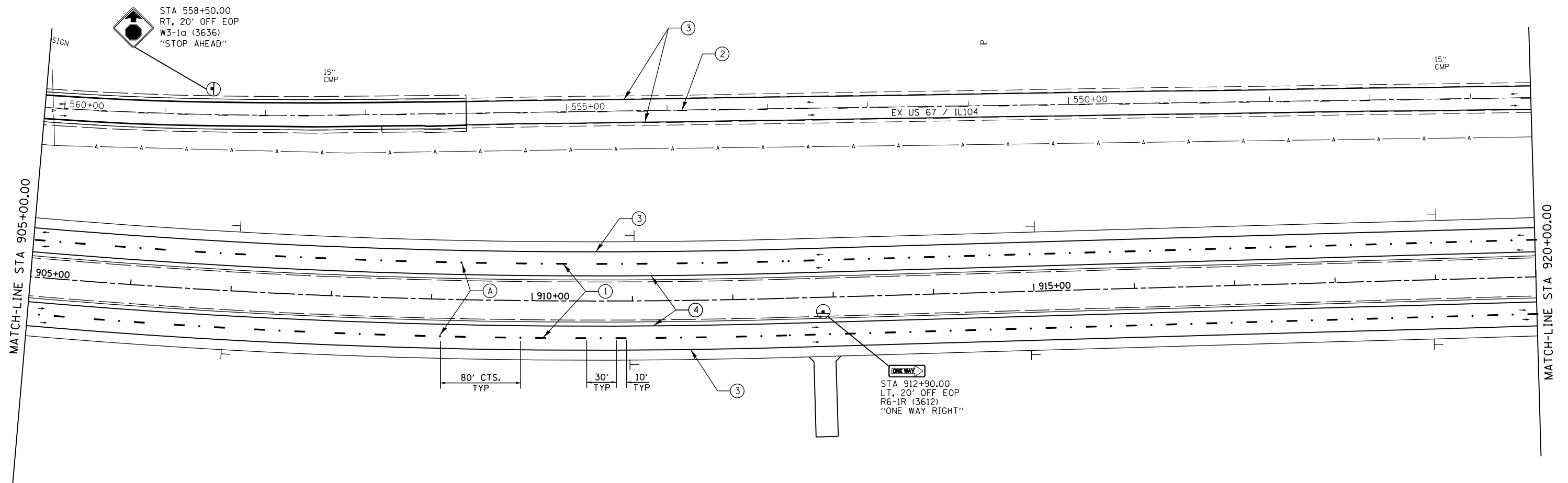
STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	252
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DELINEATOR SYMBOLS

- T - SINGLE
- ≡ - DOUBLE



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
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	PLOT DATE = Mar-30-2011 08:49:09AM	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

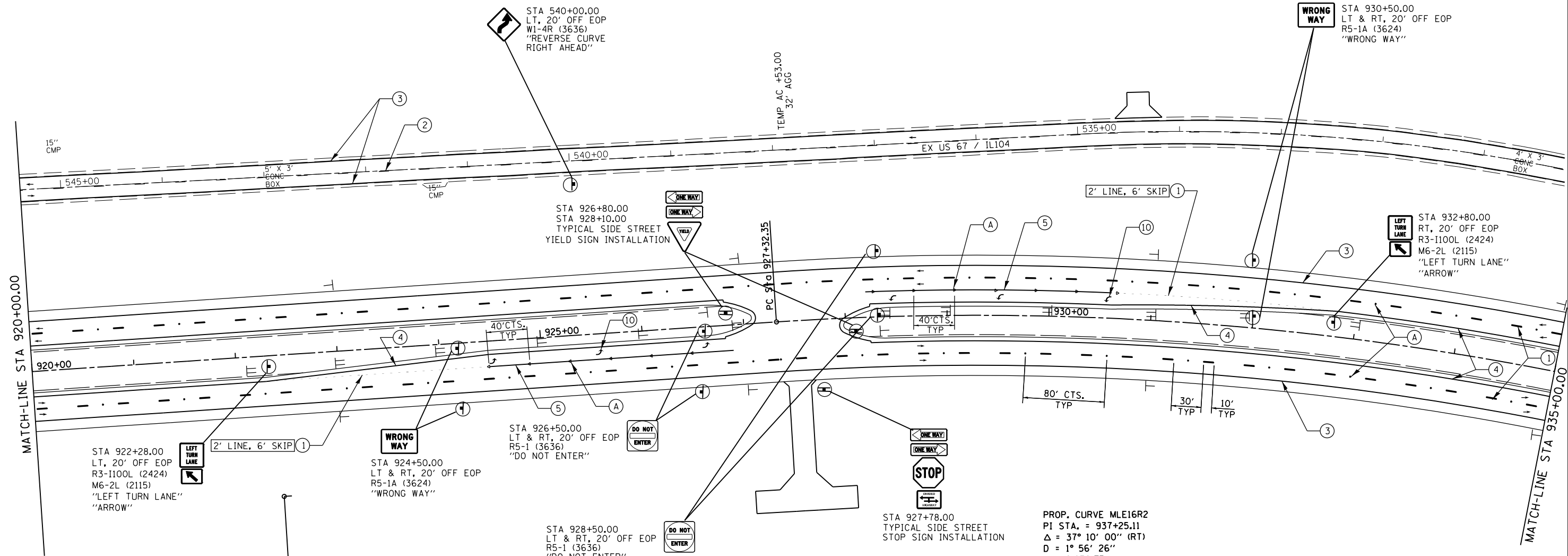
STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	253
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

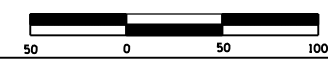
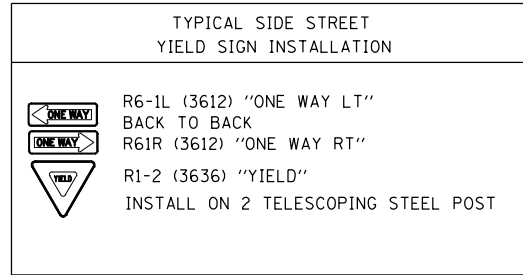
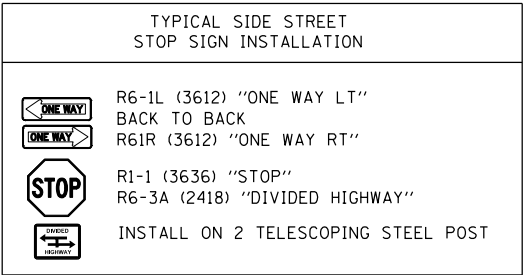
DELINEATOR SYMBOLS

- T - SINGLE
- TT - DOUBLE



PAVEMENT MARKING LEGEND

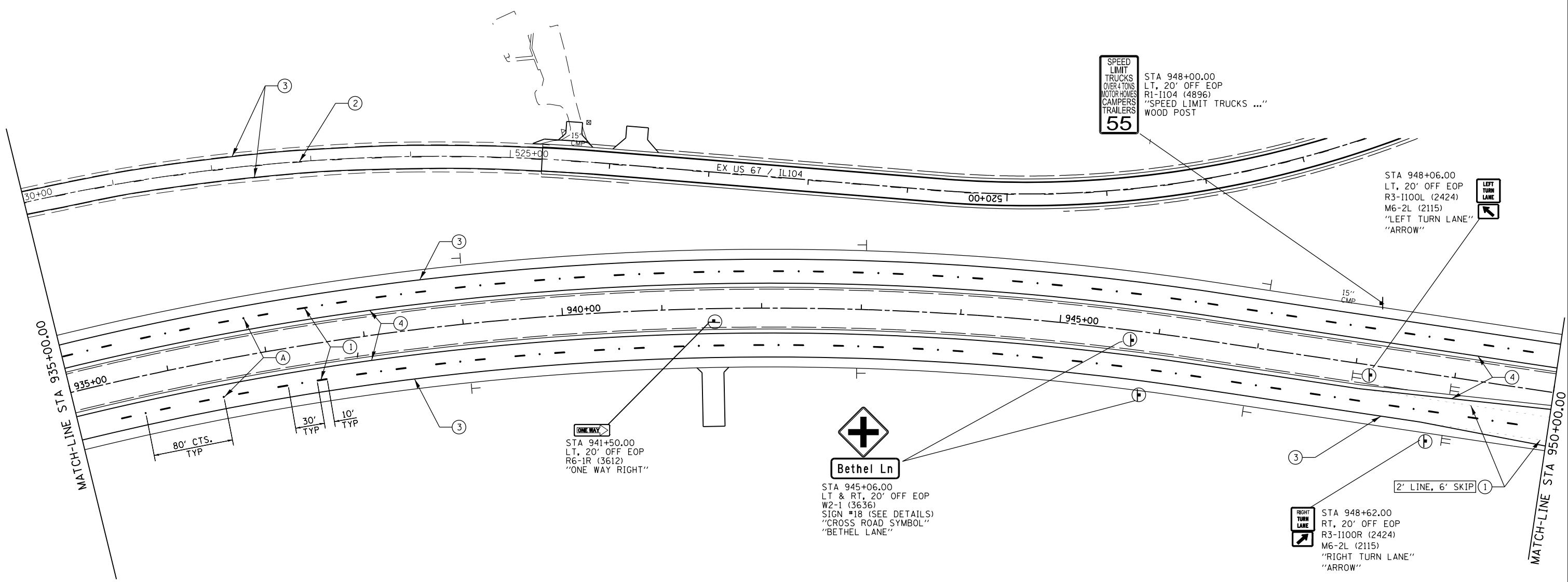
- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)



FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS FAP 310 (US 67)</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = Mar-30-2011 08:49:09AM		DATE -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							

DELINEATOR SYMBOLS

- ┆ - SINGLE
- ≡ - DOUBLE

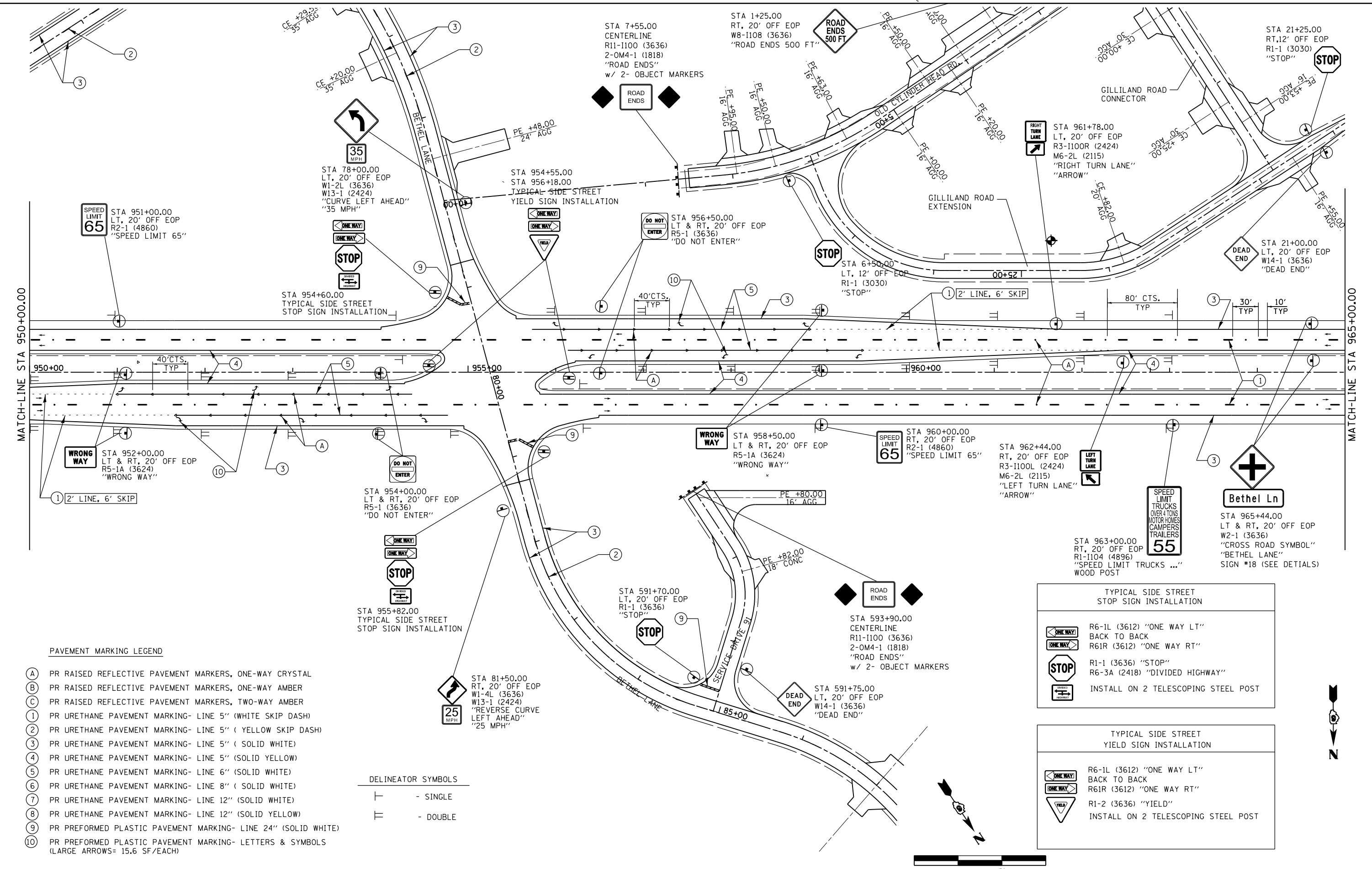


PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
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- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)



FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS FAP 310 (US 67)</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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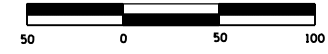
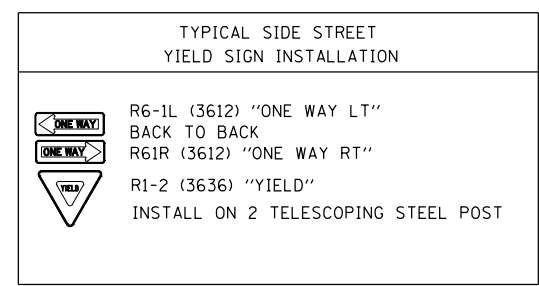
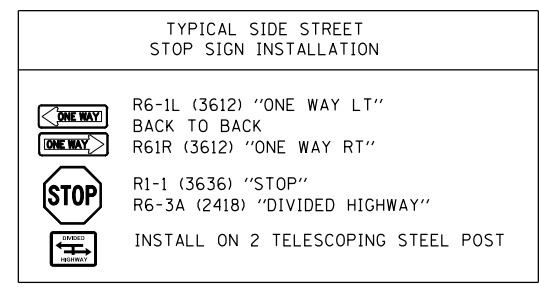


MATCH-LINE STA 950+00.00

MATCH-LINE STA 965+00.00

- PAVEMENT MARKING LEGEND**
- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
  - (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
  - (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
  - (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
  - (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
  - (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
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  - (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
  - (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
  - (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
  - (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

- DELINEATOR SYMBOLS**
- ┆ - SINGLE
  - ═ - DOUBLE



FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	256
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



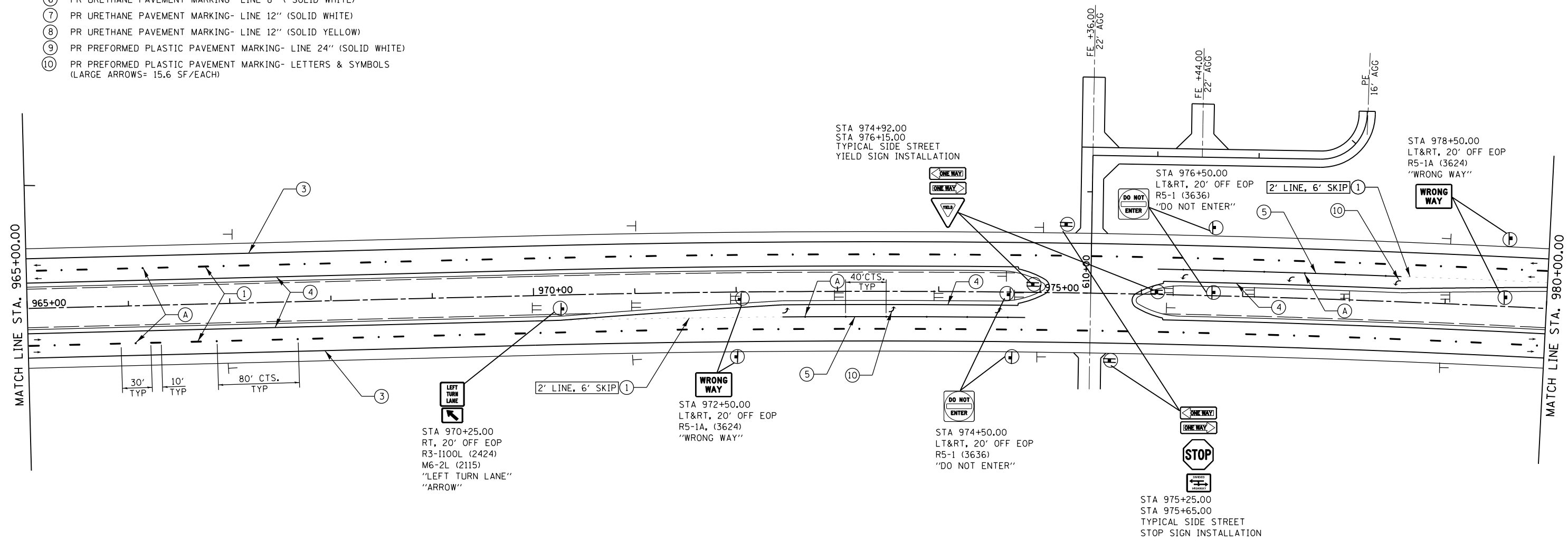


**PAVEMENT MARKING LEGEND**

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
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- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

**DELINEATOR SYMBOLS**

- ┆ - SINGLE
- ≡ - DOUBLE



**TYPICAL SIDE STREET STOP SIGN INSTALLATION**

- R6-1L (3612) "ONE WAY LT" BACK TO BACK
- R61R (3612) "ONE WAY RT"
- R1-1 (3636) "STOP"
- R6-3A (2418) "DIVIDED HIGHWAY"
- INSTALL ON 2 TELESCOPING STEEL POST

**TYPICAL SIDE STREET YIELD SIGN INSTALLATION**

- R6-1L (3612) "ONE WAY LT" BACK TO BACK
- R61R (3612) "ONE WAY RT"
- R1-2 (3636) "YIELD"
- INSTALL ON 2 TELESCOPING STEEL POST

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
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		CHECKED -	REVISED -
		DATE -	REVISED -



**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

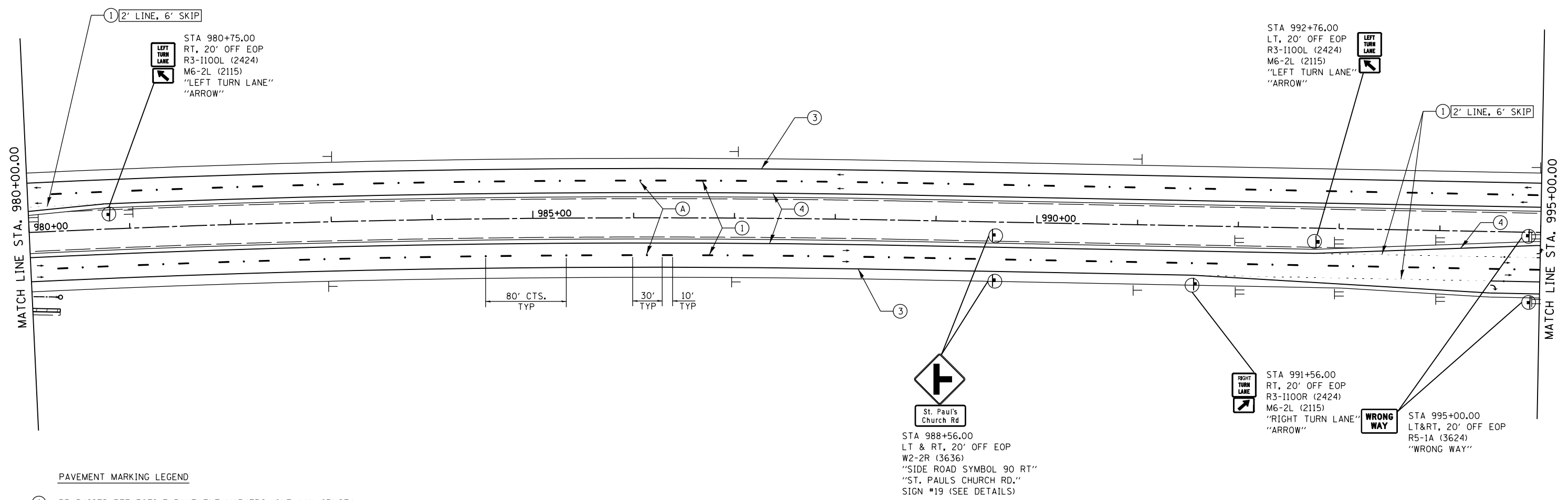
**STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	257
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

SCALE: SHEET NO. OF SHEETS STA. TO STA.

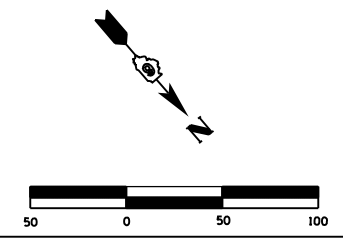
DELINEATOR SYMBOLS

-  - SINGLE
-  - DOUBLE



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
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- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)



FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
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PLOT DATE = Mar-30-2011 08:49:19AM		DATE -	REVISED -

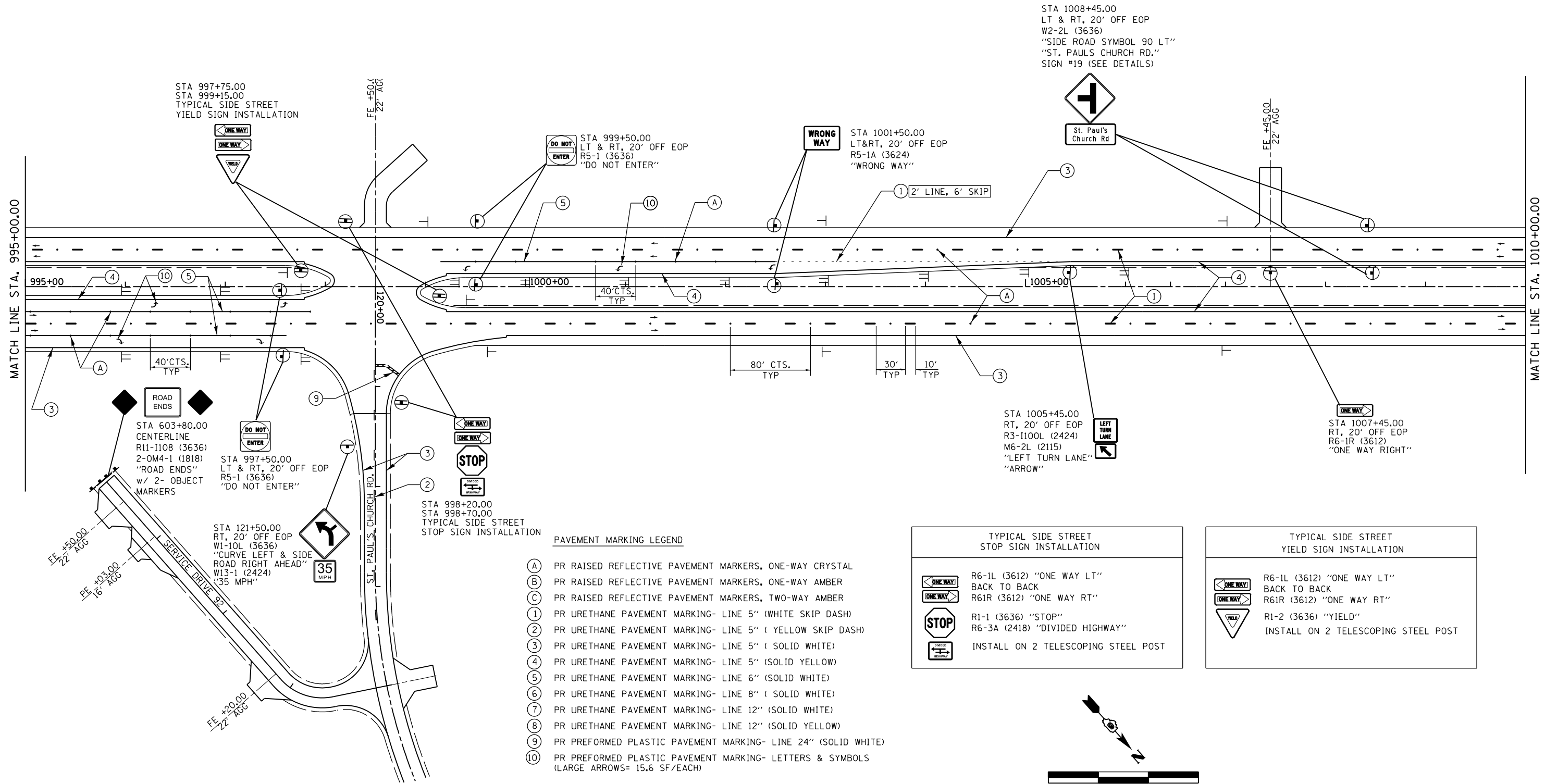
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

STRIPING & SIGNING PLAN SHEETS			
FAP 310 (US 67)			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	258
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DELINEATOR SYMBOLS

- ┆ - SINGLE
- ═ - DOUBLE



STA 997+75.00  
STA 999+15.00  
TYPICAL SIDE STREET  
YIELD SIGN INSTALLATION

STA 999+50.00  
LT & RT, 20' OFF EOP  
R5-1 (3636)  
"DO NOT ENTER"

STA 1001+50.00  
LT&RT, 20' OFF EOP  
R5-1A (3624)  
"WRONG WAY"

STA 1008+45.00  
LT & RT, 20' OFF EOP  
W2-2L (3636)  
"SIDE ROAD SYMBOL 90 LT"  
"ST. PAULS CHURCH RD."  
SIGN #19 (SEE DETAILS)

STA 1005+45.00  
RT, 20' OFF EOP  
R3-1100L (2424)  
M6-2L (2115)  
"LEFT TURN LANE"  
"ARROW"

STA 1007+45.00  
RT, 20' OFF EOP  
R6-1R (3612)  
"ONE WAY RIGHT"

STA 603+80.00  
CENTERLINE  
R11-1108 (3636)  
2-0M4-1 (1818)  
"ROAD ENDS"  
w/ 2- OBJECT  
MARKERS

STA 997+50.00  
LT & RT, 20' OFF EOP  
R5-1 (3636)  
"DO NOT ENTER"

STA 121+50.00  
RT, 20' OFF EOP  
W1-10L (3636)  
"CURVE LEFT & SIDE  
ROAD RIGHT AHEAD"  
W13-1 (2424)  
"35 MPH"

STA 998+20.00  
STA 998+70.00  
TYPICAL SIDE STREET  
STOP SIGN INSTALLATION

PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" ( YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" ( SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" ( SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

TYPICAL SIDE STREET STOP SIGN INSTALLATION

- R6-1L (3612) "ONE WAY LT" BACK TO BACK
- R61R (3612) "ONE WAY RT"
- R1-1 (3636) "STOP"
- R6-3A (2418) "DIVIDED HIGHWAY"
- INSTALL ON 2 TELESCOPING STEEL POST

TYPICAL SIDE STREET YIELD SIGN INSTALLATION

- R6-1L (3612) "ONE WAY LT" BACK TO BACK
- R61R (3612) "ONE WAY RT"
- R1-2 (3636) "YIELD"
- INSTALL ON 2 TELESCOPING STEEL POST

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
c:\pwork\pwork\sparksgw\10264881\Signs-mainline_2b_v8.dgn		DRAWN -	REVISED -
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

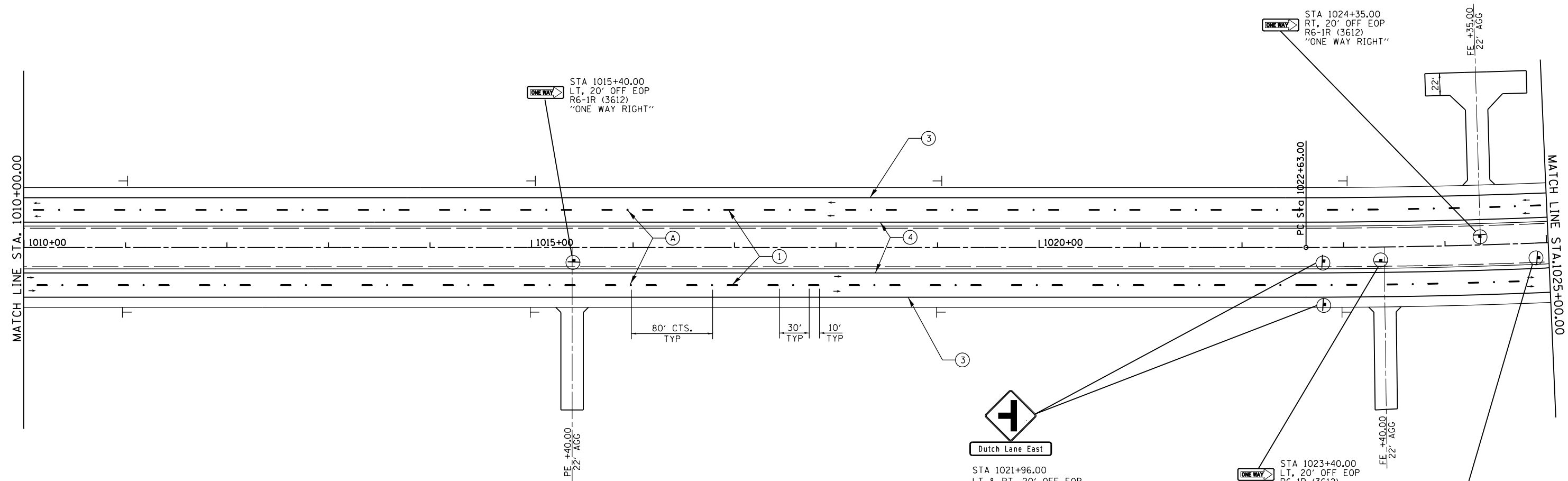
STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)

SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	259
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DELINEATOR SYMBOLS

- T - SINGLE
- TT - DOUBLE



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
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- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

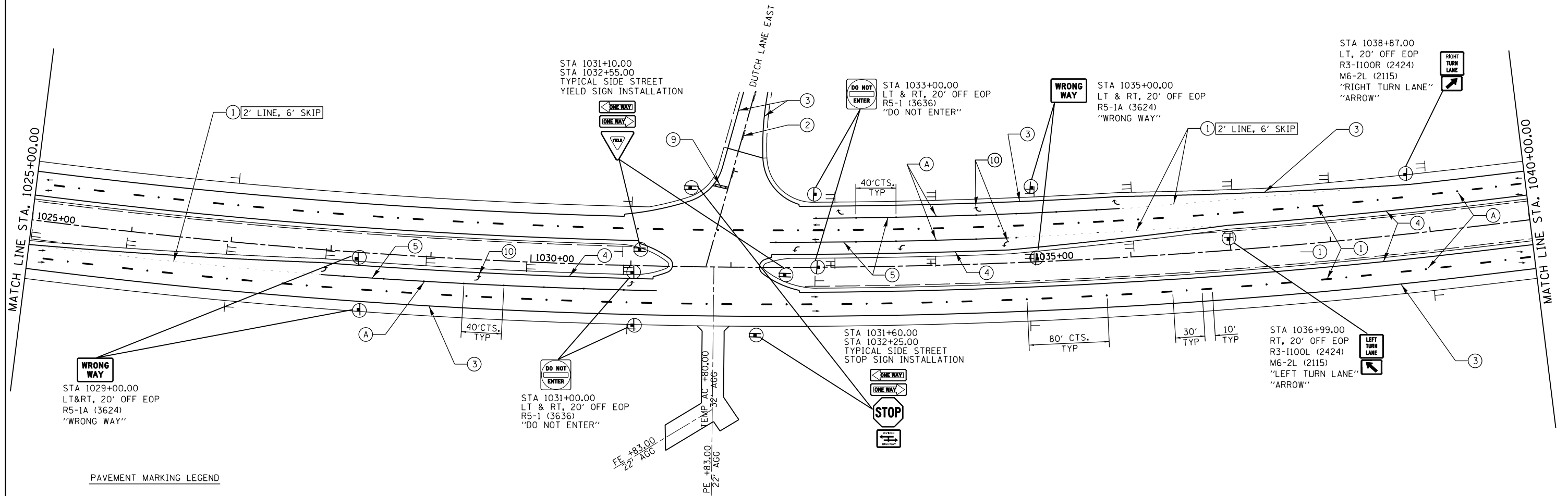
STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	260
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

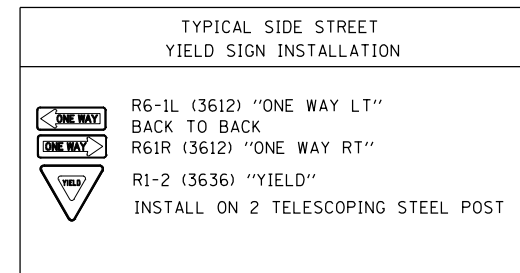
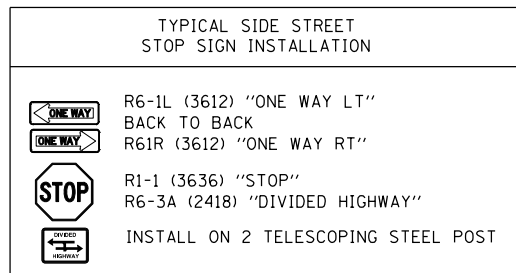
DELINEATOR SYMBOLS

- ├── SINGLE
- ════ DOUBLE



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
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- (2) PR URETHANE PAVEMENT MARKING- LINE 5" ( YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" ( SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" ( SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)



FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

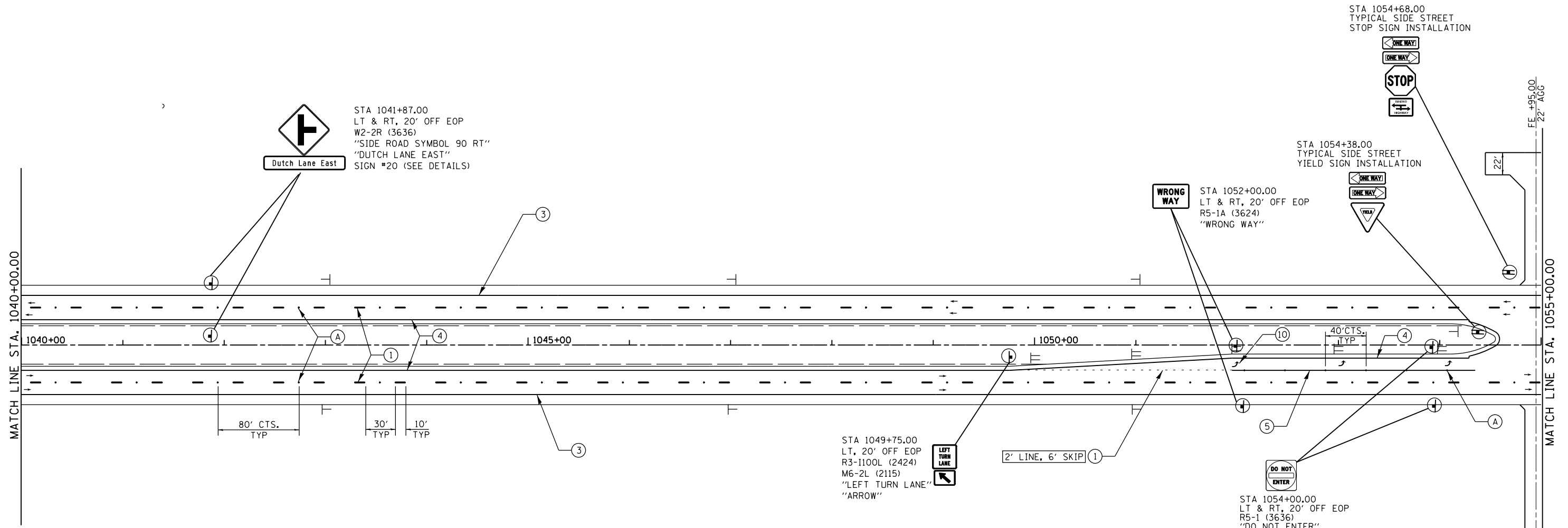
STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	261
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

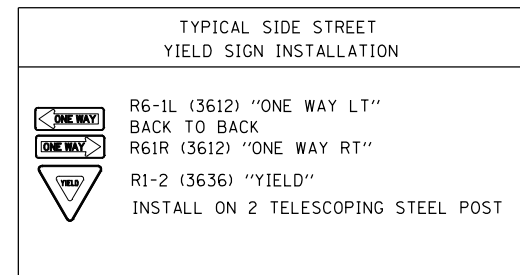
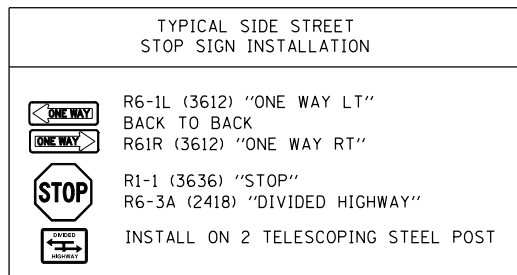
DELINEATOR SYMBOLS

- T - SINGLE
- TT - DOUBLE



PAVEMENT MARKING LEGEND

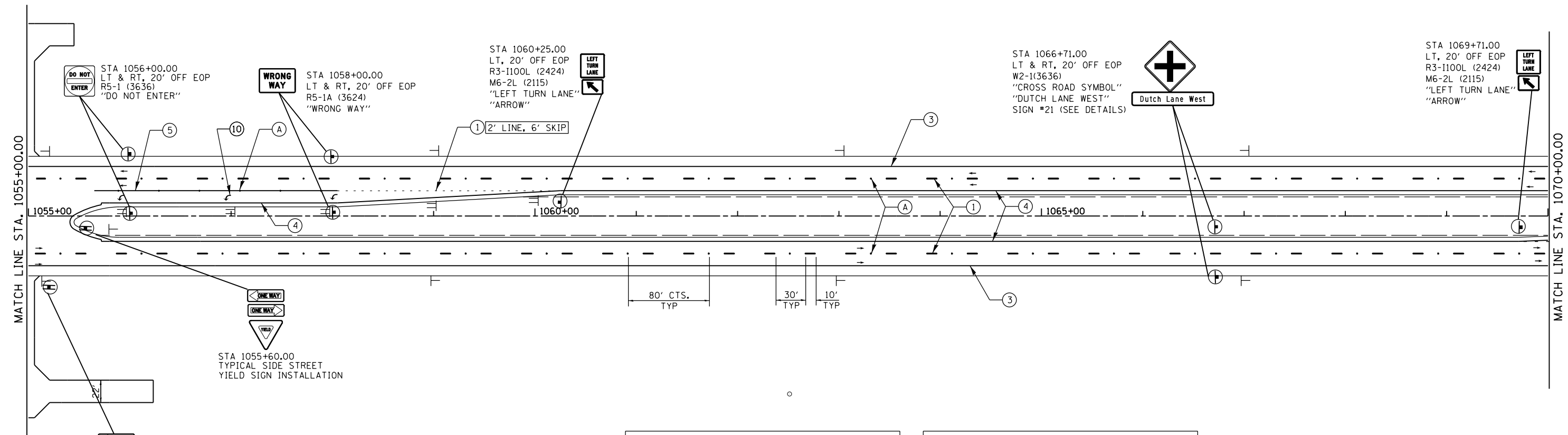
- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
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- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)



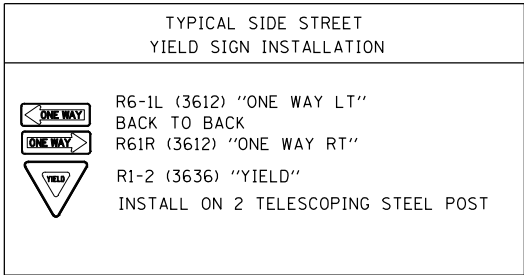
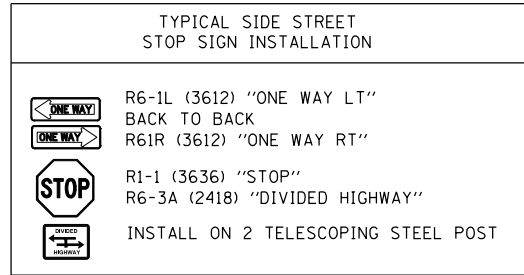
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FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT													

DELINEATOR SYMBOLS

- ┆ - SINGLE
- ≡ - DOUBLE



- PAVEMENT MARKING LEGEND**
- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
  - (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
  - (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
  - (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
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  - (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)



FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

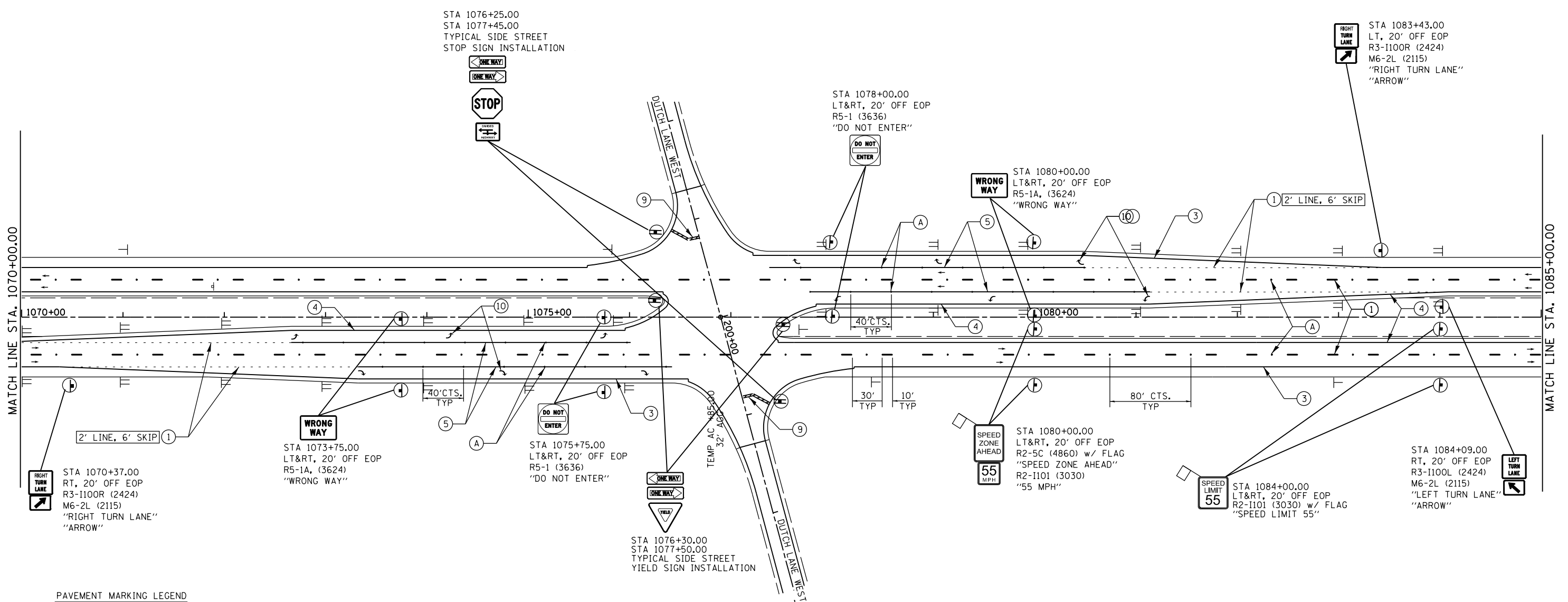
**STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	263
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

SCALE: SHEET NO. OF SHEETS STA. TO STA.

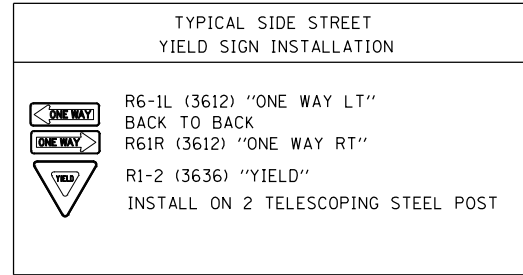
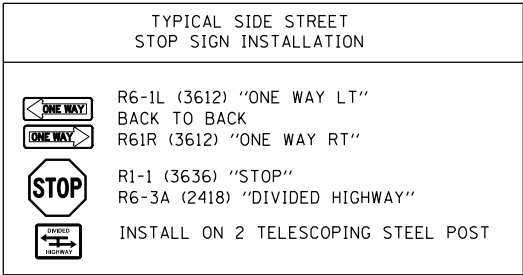
DELINEATOR SYMBOLS

- ┆ - SINGLE
- ≡ - DOUBLE



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
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- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
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- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)



FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS FAP 310 (US 67)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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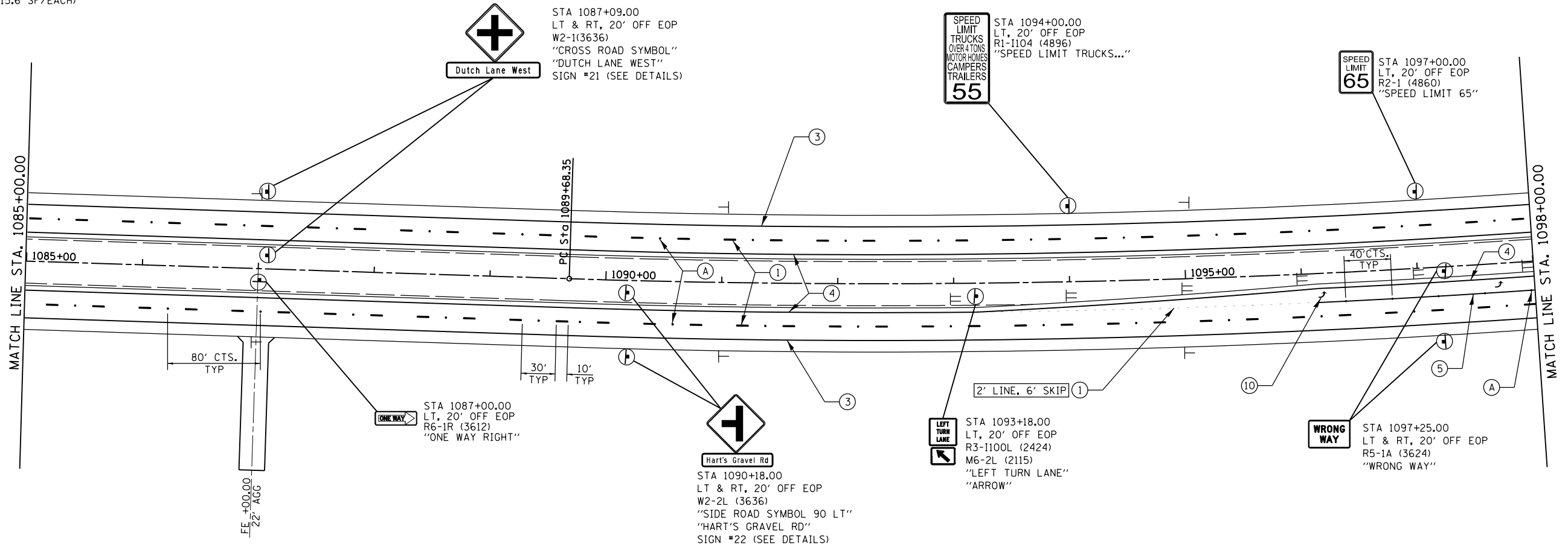


PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
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- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

DELINEATOR SYMBOLS

- ┌ - SINGLE
- ≡ - DOUBLE



FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
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

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

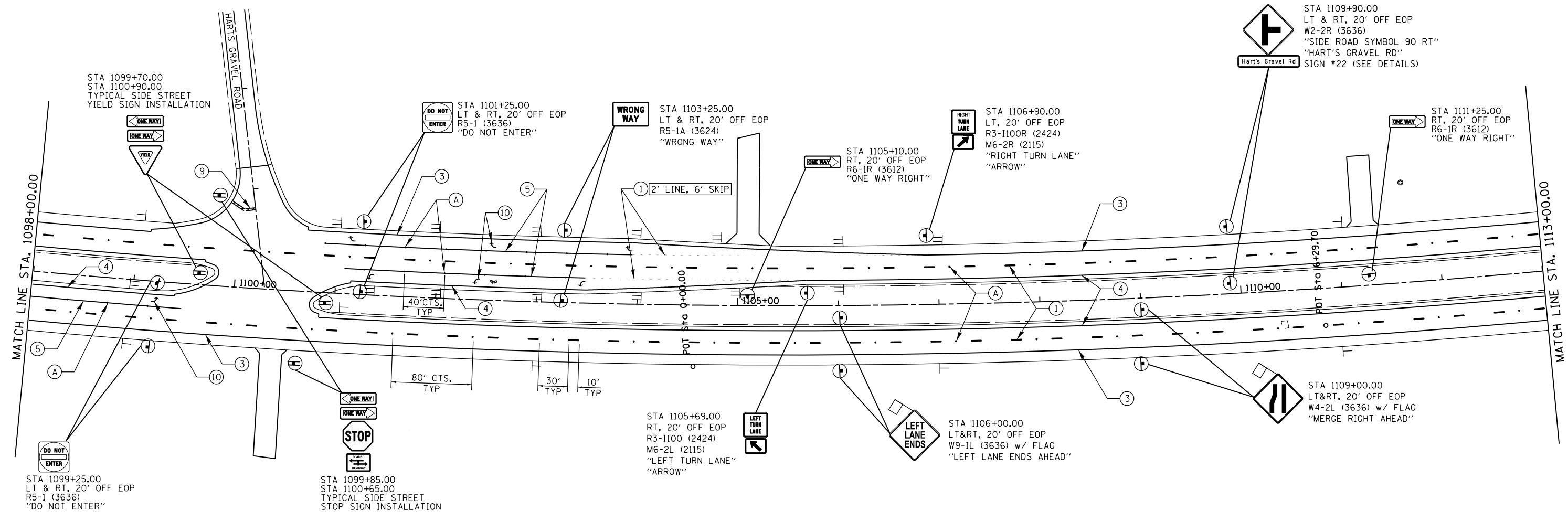
STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

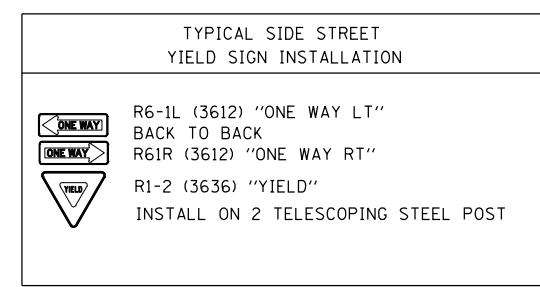
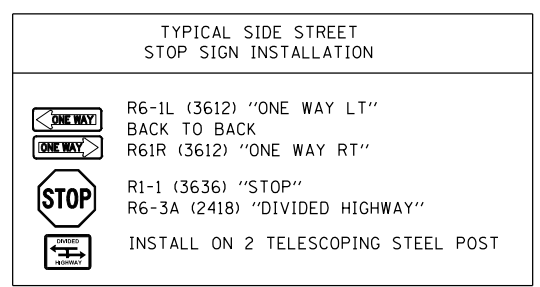
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310	69-3	MORGAN	793	265
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DELINEATOR SYMBOLS

-  - SINGLE
-  - DOUBLE

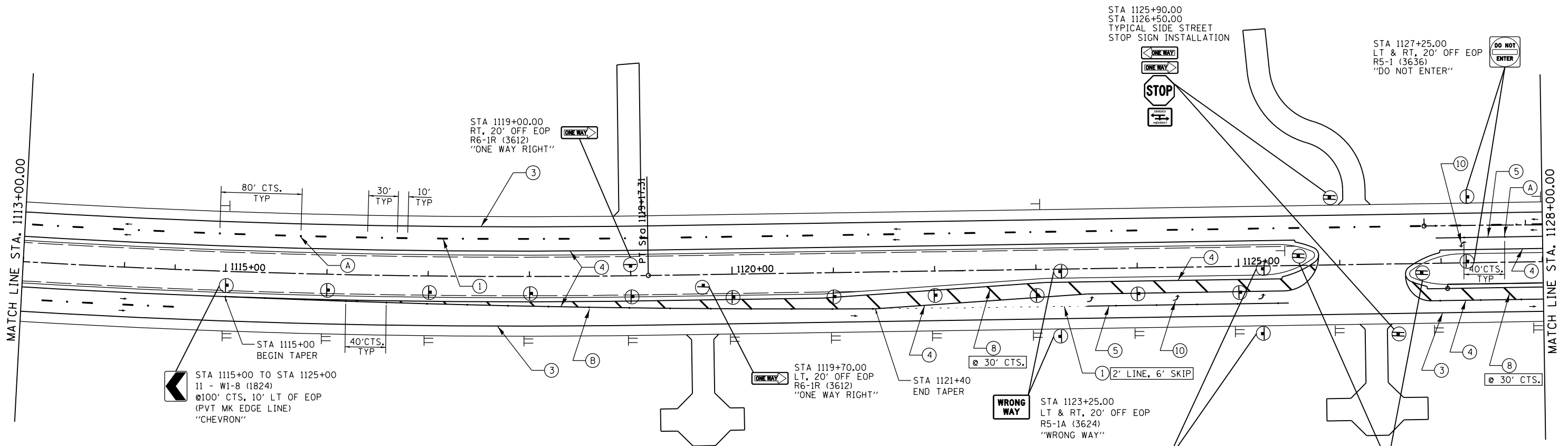


- PAVEMENT MARKING LEGEND**
- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
  - (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
  - (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
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  - (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
  - (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)



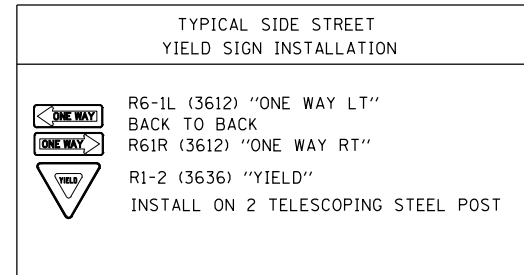
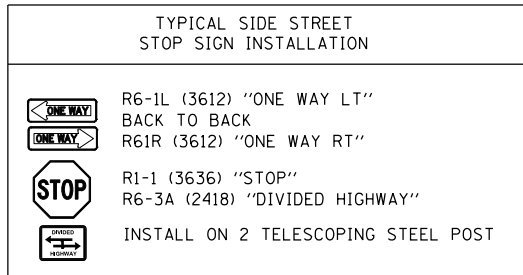
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PLOT DATE = Mar-30-2011 08:49:24AM		DATE -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.				

DELINEATOR SYMBOLS	
	- SINGLE
	- DOUBLE



**PAVEMENT MARKING LEGEND**

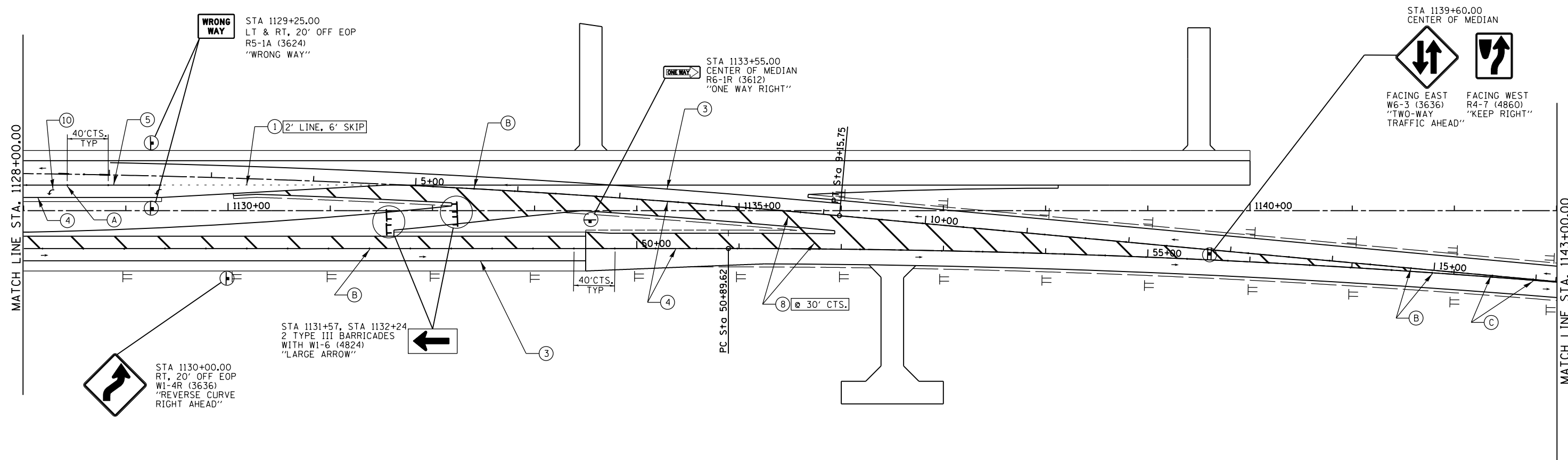
- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
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- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)



FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS FAP 310 (US 67)</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\sparksgw\10264881\Signs-main\line_2b.v8.dgn		DRAWN -	REVISED -		310	69-3	MORGAN	793	267			
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED -		CONTRACT NO. 72667							
PLOT DATE = Mar-30-2011 08:49:25AM		DATE -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							

DELINEATOR SYMBOLS

- ┆ - SINGLE
- ▬ - DOUBLE



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
ct:\pw\work\p\dot\sparksgw\10264881\Signs_main\line_2b_v8.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1".	CHECKED -	REVISED -
	PLOT DATE = Mar-30-2011 08:49:25AM	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

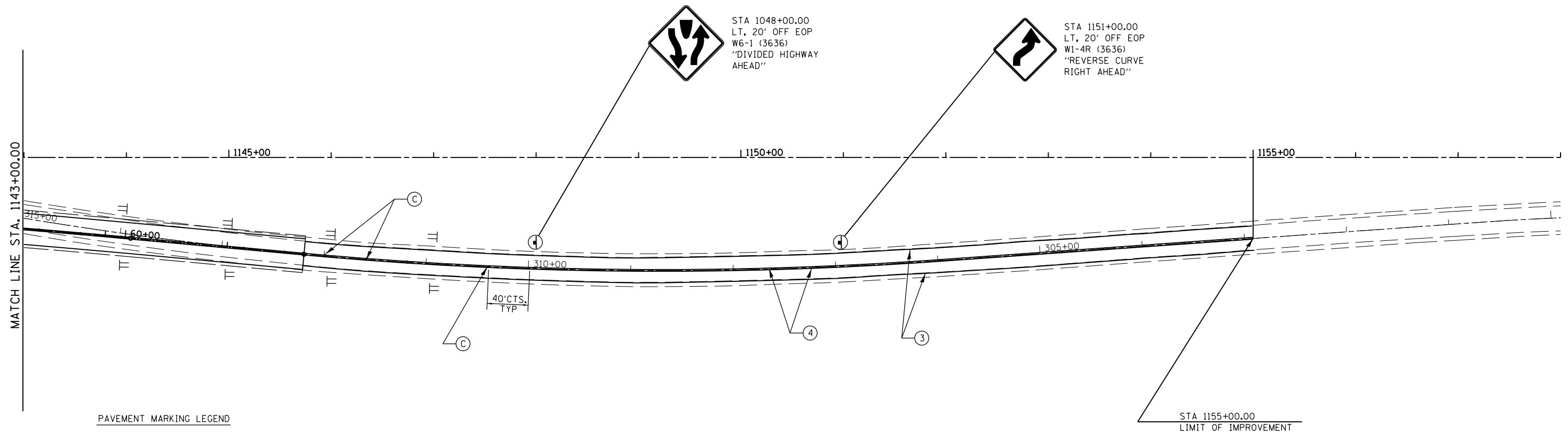
STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	268
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DELINEATOR SYMBOLS

- ┆ - SINGLE
- ≡ - DOUBLE



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
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- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

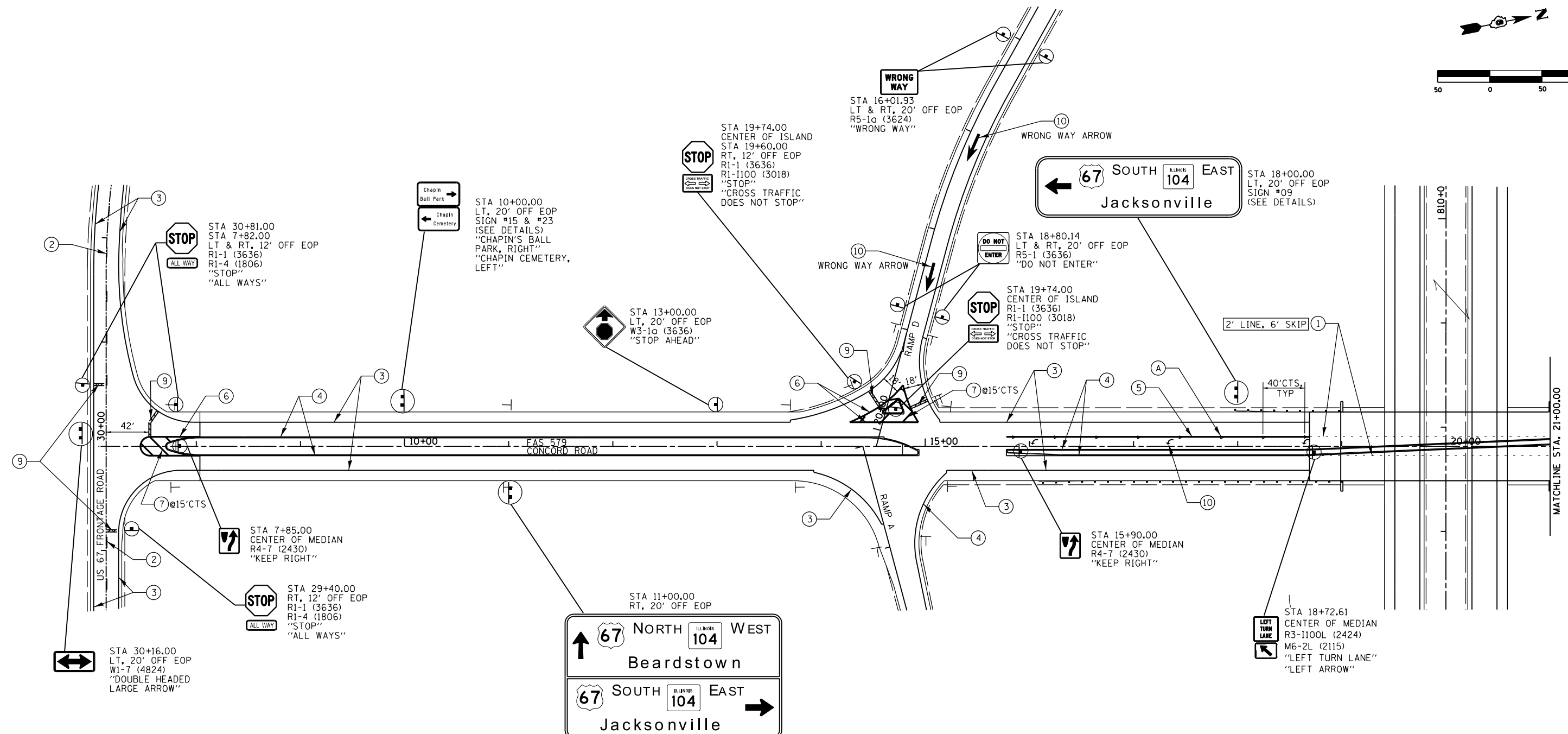
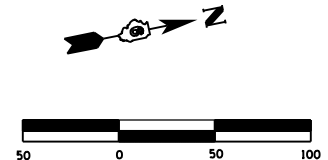
FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
c:\pwwork\pwwork\sparksgw\10264881\Signs-main\line_2b.v8.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Mar-30-2011 08:49:26AM	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

STRIPING & SIGNING PLAN SHEETS  
FAP 310 (US 67)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	MORGAN	793	269
CONTRACT NO. 72667				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



DELINEATOR SYMBOLS  
 ┤ - SINGLE  
 ═ - DOUBLE

TYPICAL SIDE STREET STOP SIGN INSTALLATION

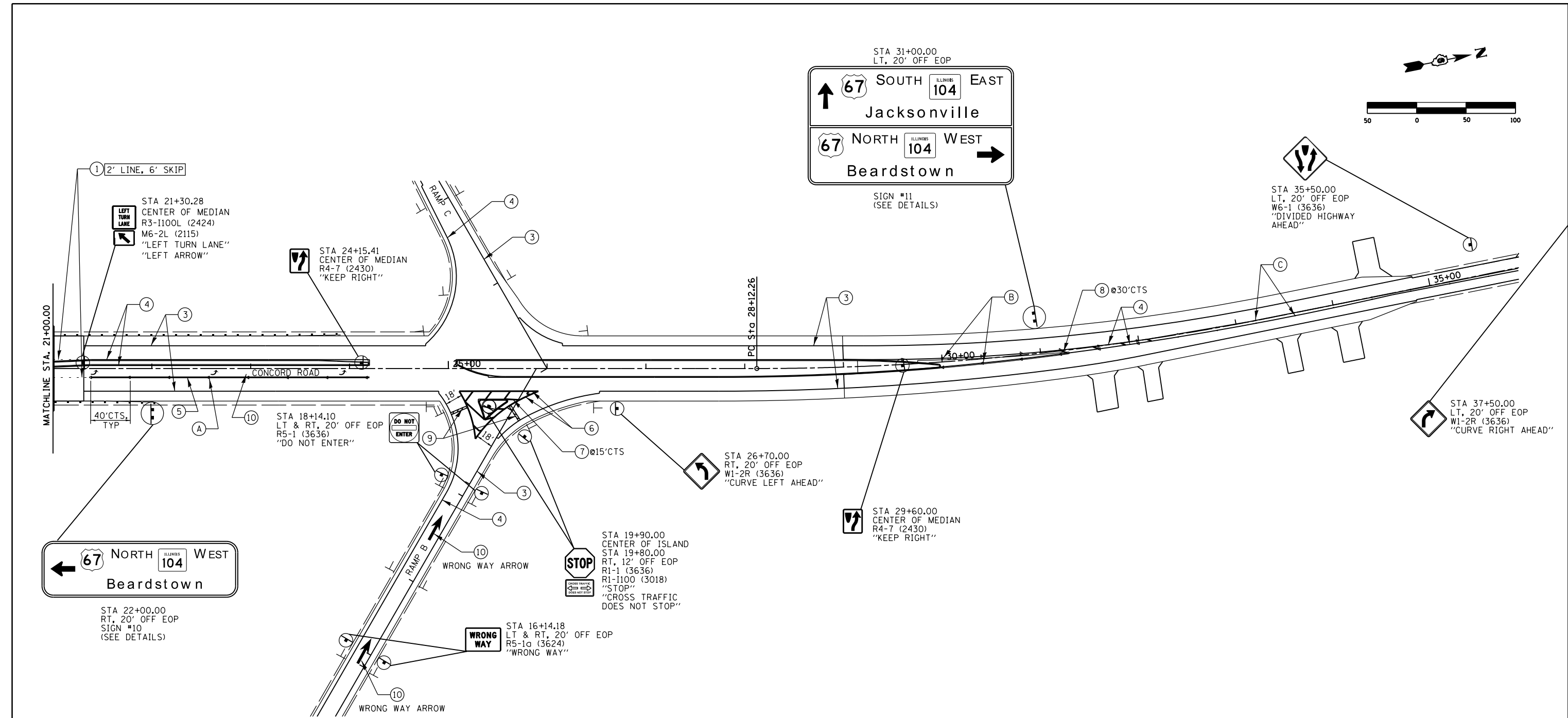
R6-1L (3612) "ONE WAY LT" BACK TO BACK  
 R61R (3612) "ONE WAY RT"  
 R1-1 (3636) "STOP"  
 R6-3A (2418) "DIVIDED HIGHWAY"  
 INSTALL ON 2 TELESCOPING STEEL POST

TYPICAL SIDE STREET YIELD SIGN INSTALLATION

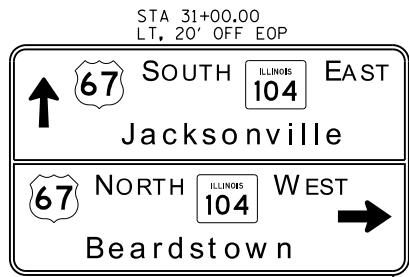
R6-1L (3612) "ONE WAY LT" BACK TO BACK  
 R61R (3612) "ONE WAY RT"  
 R1-2 (3636) "YIELD"  
 INSTALL ON 2 TELESCOPING STEEL POST

- PAVEMENT MARKING LEGEND
- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
  - (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
  - (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
  - (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
  - (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
  - (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
  - (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
  - (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
  - (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
  - (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
  - (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
  - (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
  - (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS CONCORD ROAD</b>				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\sparksgw\10264881\P&P	Sideroads15-27.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.			793	270
	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED -		CONTRACT NO. 72667								
	PLOT DATE = Mar-30-2011 08:49:32AM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								



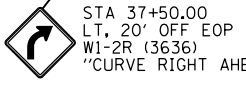
STA 22+00.00  
RT, 20' OFF EOP  
SIGN #10  
(SEE DETAILS)



STA 31+00.00  
LT, 20' OFF EOP  
SIGN #11  
(SEE DETAILS)



STA 35+50.00  
LT, 20' OFF EOP  
W6-1 (3636)  
"DIVIDED HIGHWAY  
AHEAD"



STA 37+50.00  
LT, 20' OFF EOP  
W1-2R (3636)  
"CURVE RIGHT AHEAD"

**DELINEATOR SYMBOLS**

- - SINGLE
- ≡ - DOUBLE

**TYPICAL SIDE STREET  
STOP SIGN INSTALLATION**

	R6-1L (3612) "ONE WAY LT" BACK TO BACK
	R61R (3612) "ONE WAY RT"
	R1-1 (3636) "STOP" R6-3A (2418) "DIVIDED HIGHWAY"
	INSTALL ON 2 TELESCOPING STEEL POST

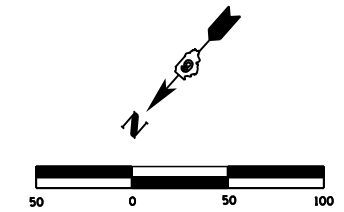
**TYPICAL SIDE STREET  
YIELD SIGN INSTALLATION**

	R6-1L (3612) "ONE WAY LT" BACK TO BACK
	R61R (3612) "ONE WAY RT"
	R1-2 (3636) "YIELD"
	INSTALL ON 2 TELESCOPING STEEL POST

**PAVEMENT MARKING LEGEND**

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

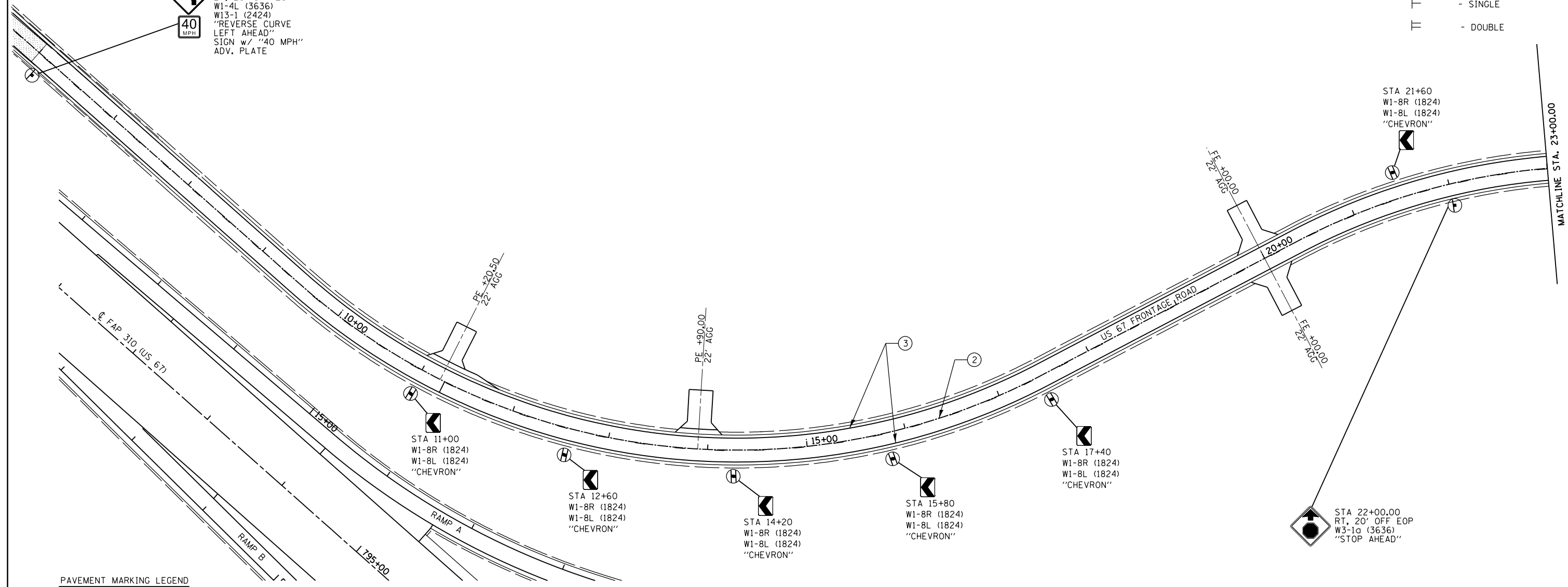
FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS CONCORD ROAD</b>				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pw\work\p\idot\sparksgw\10264881\p&p	Sideroads15-27.dgn	DRAWN -	REVISED -		SCALE:      SHEET NO.      OF SHEETS      STA.      TO STA.							793	271
	PLOT SCALE = 100.0000' / 1".	CHECKED -	REVISED -						CONTRACT NO. 72667				
	PLOT DATE = Mar-30-2011 08:49:33AM	DATE -	REVISED -						ILLINOIS FED. AID PROJECT				



DELINEATOR SYMBOLS

	- SINGLE
	- DOUBLE

STA 86+00.00  
 LT, 20' OFF EOP  
 W1-4L (3636)  
 W13-1 (2424)  
 "REVERSE CURVE  
 LEFT AHEAD"  
 SIGN w/ "40 MPH"  
 ADV. PLATE



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
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- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
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- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

TYPICAL SIDE STREET STOP SIGN INSTALLATION

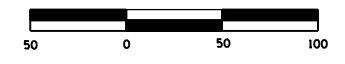
	R6-1L (3612) "ONE WAY LT"
	R61R (3612) "ONE WAY RT"
	R1-1 (3636) "STOP"
	R6-3A (2418) "DIVIDED HIGHWAY"
	INSTALL ON 2 TELESCOPING STEEL POST

TYPICAL SIDE STREET YIELD SIGN INSTALLATION

	R6-1L (3612) "ONE WAY LT"
	R61R (3612) "ONE WAY RT"
	R1-2 (3636) "YIELD"
	INSTALL ON 2 TELESCOPING STEEL POST

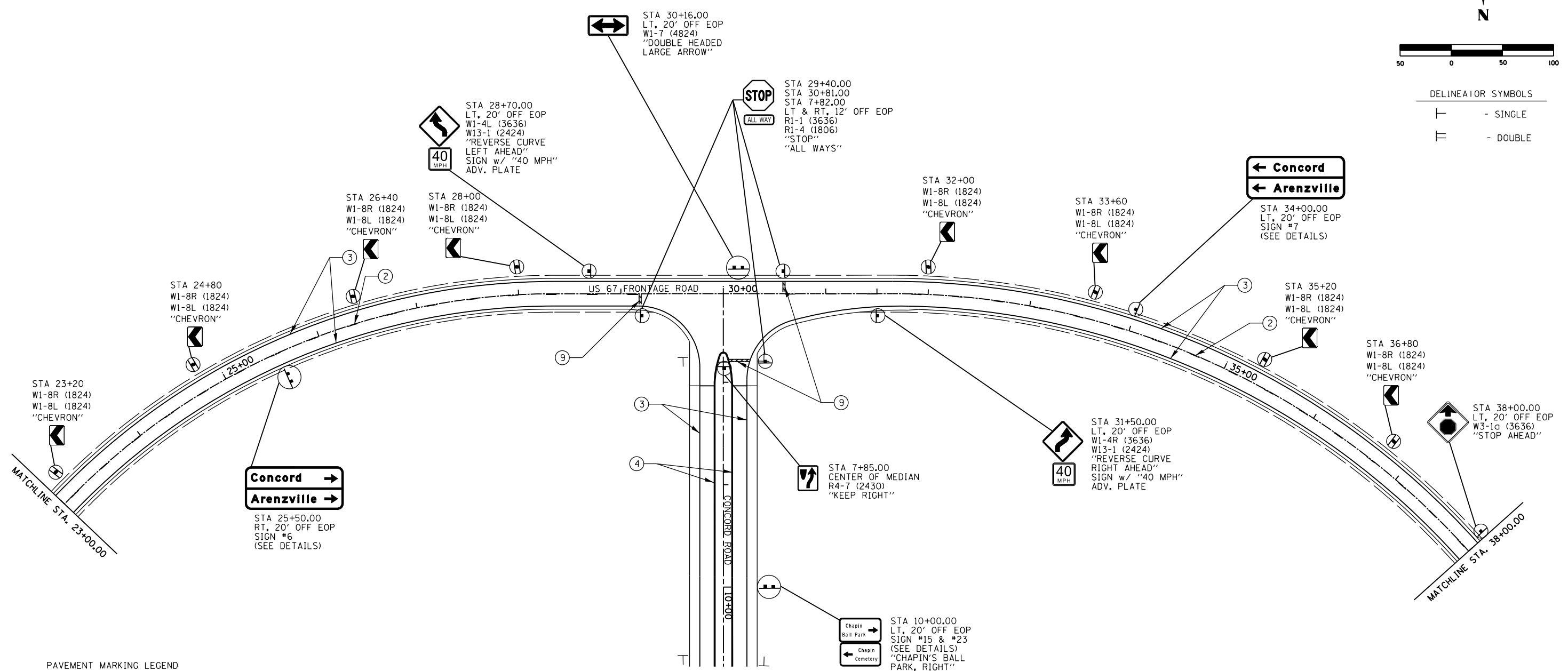
FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS US 67 FRONTAGE ROAD</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ci:\pw\work\p\idot\sparksgw\0264881\p&p	Sideroads15-27.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.			793	272
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 72667								
	PLOT DATE = Mar-30-2011 08:49:34AM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								





DELINEATOR SYMBOLS

	- SINGLE
	- DOUBLE



- PAVEMENT MARKING LEGEND**
- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
  - (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
  - (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
  - (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
  - (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
  - (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
  - (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
  - (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
  - (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
  - (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
  - (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
  - (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
  - (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

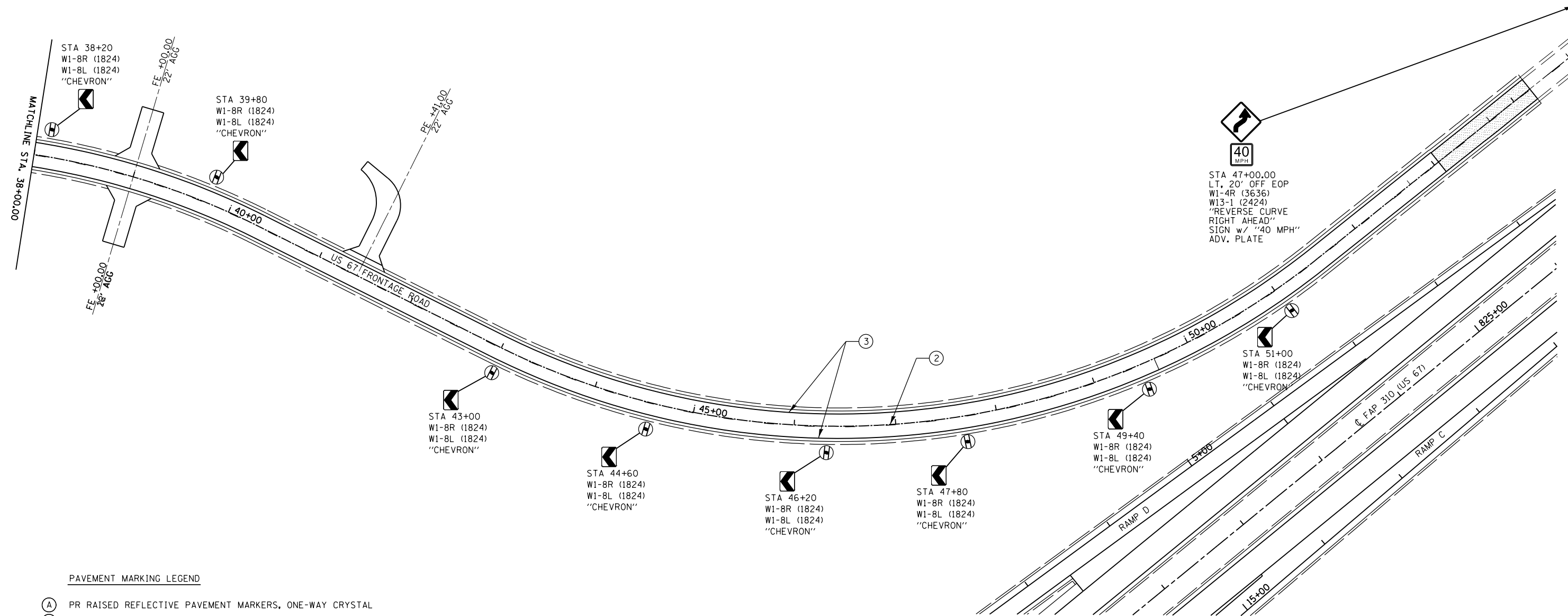
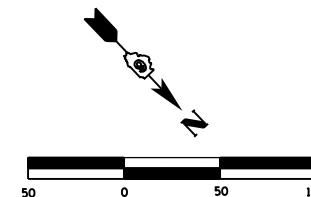
**TYPICAL SIDE STREET STOP SIGN INSTALLATION**

	R6-1L (3612) "ONE WAY LT" BACK TO BACK
	R61R (3612) "ONE WAY RT"
	R1-1 (3636) "STOP" R6-3A (2418) "DIVIDED HIGHWAY"
	INSTALL ON 2 TELESCOPING STEEL POST

**TYPICAL SIDE STREET YIELD SIGN INSTALLATION**

	R6-1L (3612) "ONE WAY LT" BACK TO BACK
	R61R (3612) "ONE WAY RT"
	R1-2 (3636) "YIELD"
	INSTALL ON 2 TELESCOPING STEEL POST

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS US 67 FRONTAGE ROAD</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ei:\pw\work\p\idot\sparksgw\10264881\P&P	Sideroads15-27.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.			793	273
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -		CONTRACT NO. 72667								
	PLOT DATE = Mar-30-2011 08:49:34AM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								



**PAVEMENT MARKING LEGEND**

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" ( YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" ( SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
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- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

TYPICAL SIDE STREET  
STOP SIGN INSTALLATION

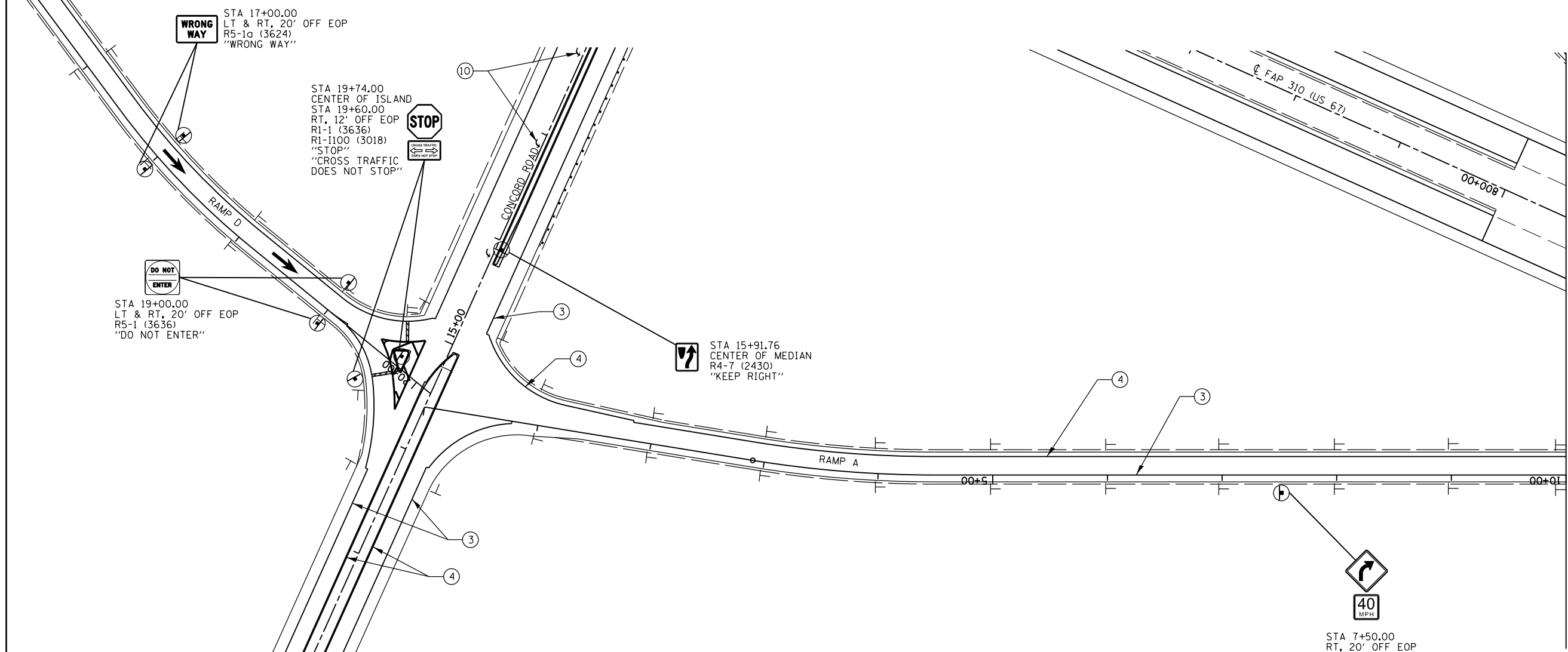
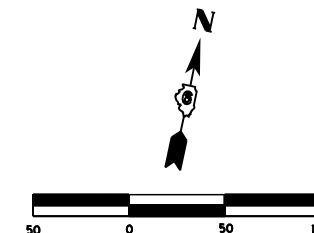
	R6-1L (3612) "ONE WAY LT" BACK TO BACK
	R61R (3612) "ONE WAY RT"
	R1-1 (3636) "STOP" R6-3A (2418) "DIVIDED HIGHWAY"
	INSTALL ON 2 TELESCOPING STEEL POST

TYPICAL SIDE STREET  
YIELD SIGN INSTALLATION

	R6-1L (3612) "ONE WAY LT" BACK TO BACK
	R61R (3612) "ONE WAY RT"
	R1-2 (3636) "YIELD" INSTALL ON 2 TELESCOPING STEEL POST

DELINEATOR SYMBOLS

	- SINGLE
	- DOUBLE



**PAVEMENT MARKING LEGEND**

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
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- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

TYPICAL SIDE STREET  
STOP SIGN INSTALLATION

	R6-1L (3612) "ONE WAY LT"
	R6-1R (3612) "ONE WAY RT"
	R1-1 (3636) "STOP"
	R6-3A (2418) "DIVIDED HIGHWAY"
	INSTALL ON 2 TELESCOPING STEEL POST

TYPICAL SIDE STREET  
YIELD SIGN INSTALLATION

	R6-1L (3612) "ONE WAY LT"
	R6-1R (3612) "ONE WAY RT"
	R1-2 (3636) "YIELD"
	INSTALL ON 2 TELESCOPING STEEL POST

**DELINEATOR SYMBOLS**

- SINGLE
- DOUBLE

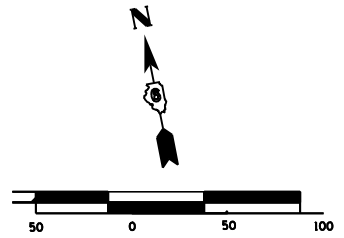
FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
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	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED -
	PLOT DATE = Mar-30-2011 08:49:35AM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STRIPING & SIGNING PLAN SHEETS  
RAMP A**

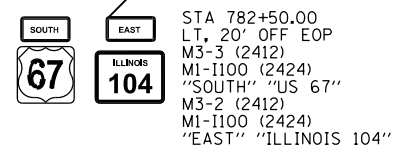
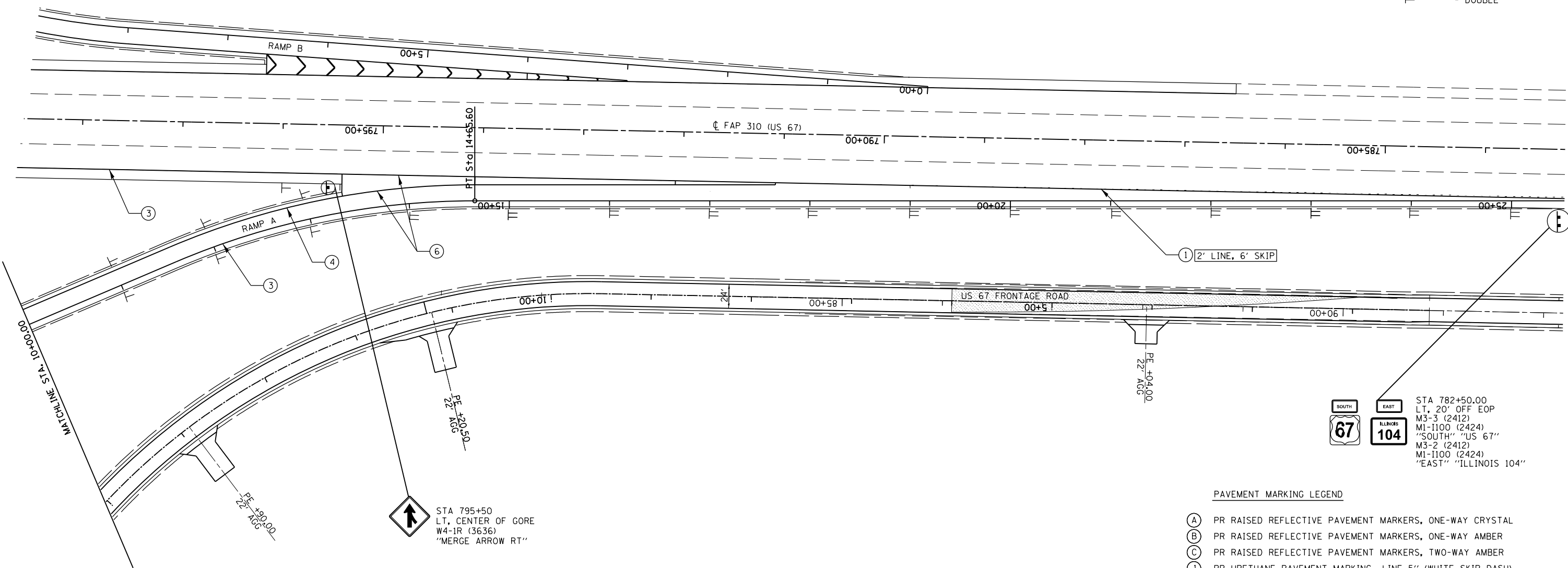
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			793	275
			CONTRACT NO. 72667	
ILLINOIS FED. AID PROJECT				



DELINEATOR SYMBOLS

- SINGLE
- DOUBLE



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
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- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

DELINEATOR SYMBOLS

- SINGLE
- DOUBLE

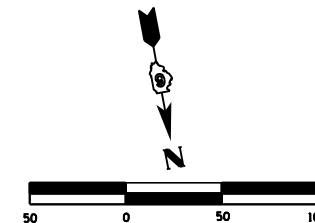
TYPICAL SIDE STREET STOP SIGN INSTALLATION

- R6-1L (3612) "ONE WAY LT" BACK TO BACK
- R61R (3612) "ONE WAY RT"
- R1-1 (3636) "STOP"
- R6-3A (2418) "DIVIDED HIGHWAY"
- INSTALL ON 2 TELESCOPING STEEL POST

TYPICAL SIDE STREET YIELD SIGN INSTALLATION

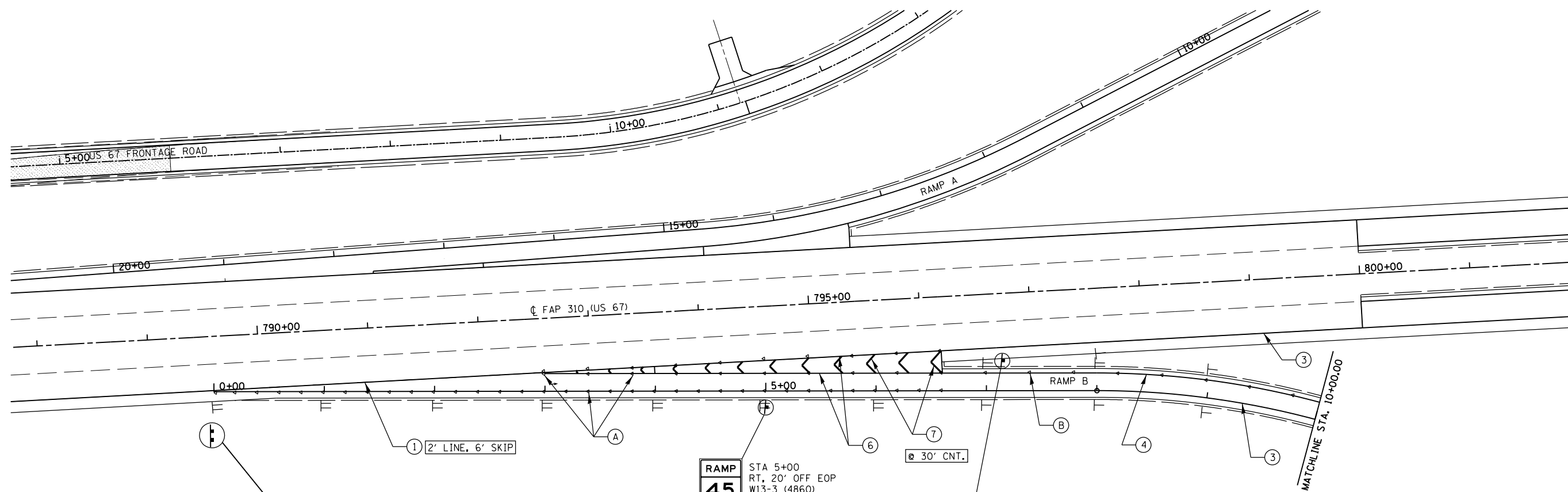
- R6-1L (3612) "ONE WAY LT" BACK TO BACK
- R61R (3612) "ONE WAY RT"
- R1-2 (3636) "YIELD"
- INSTALL ON 2 TELESCOPING STEEL POST

FILE NAME = c:\pwork\pwork\pwork\sparksgw\10264881\P&P	USER NAME = sparksgw Sideroads15-27.dgn	DESIGNED - DRAWN -	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
					<b>RAMP A</b>						793	276
					SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 72667		
					ILLINOIS FED. AID PROJECT							
PLOT SCALE = 100.0000' / 1" / 1" / 1"		CHECKED -	REVISED -									
PLOT DATE = Mar-30-2011 08:49:36AM		DATE -	REVISED -									



DELINEATOR SYMBOLS

- ├── - SINGLE
- ├──├── - DOUBLE



STA 789+57.68  
RT, 36' OFF EOP  
**Concord  
Arenzville**  
PR SIGN #04  
(SEE DETAILS)

**RAMP  
45  
M.P.H.**  
STA 5+00  
RT, 20' OFF EOP  
W13-3 (4860)  
"RAMP 45 MPH"

**EXIT**  
STA 796+72.68  
RT, CENTER OF GORE  
PR SIGN #05  
(SEE DETAILS)

TYPICAL SIDE STREET  
STOP SIGN INSTALLATION

	R6-1L (3612) "ONE WAY LT"
	R61R (3612) "ONE WAY RT"
	R1-1 (3636) "STOP"
	R6-3A (2418) "DIVIDED HIGHWAY"
	INSTALL ON 2 TELESCOPING STEEL POST

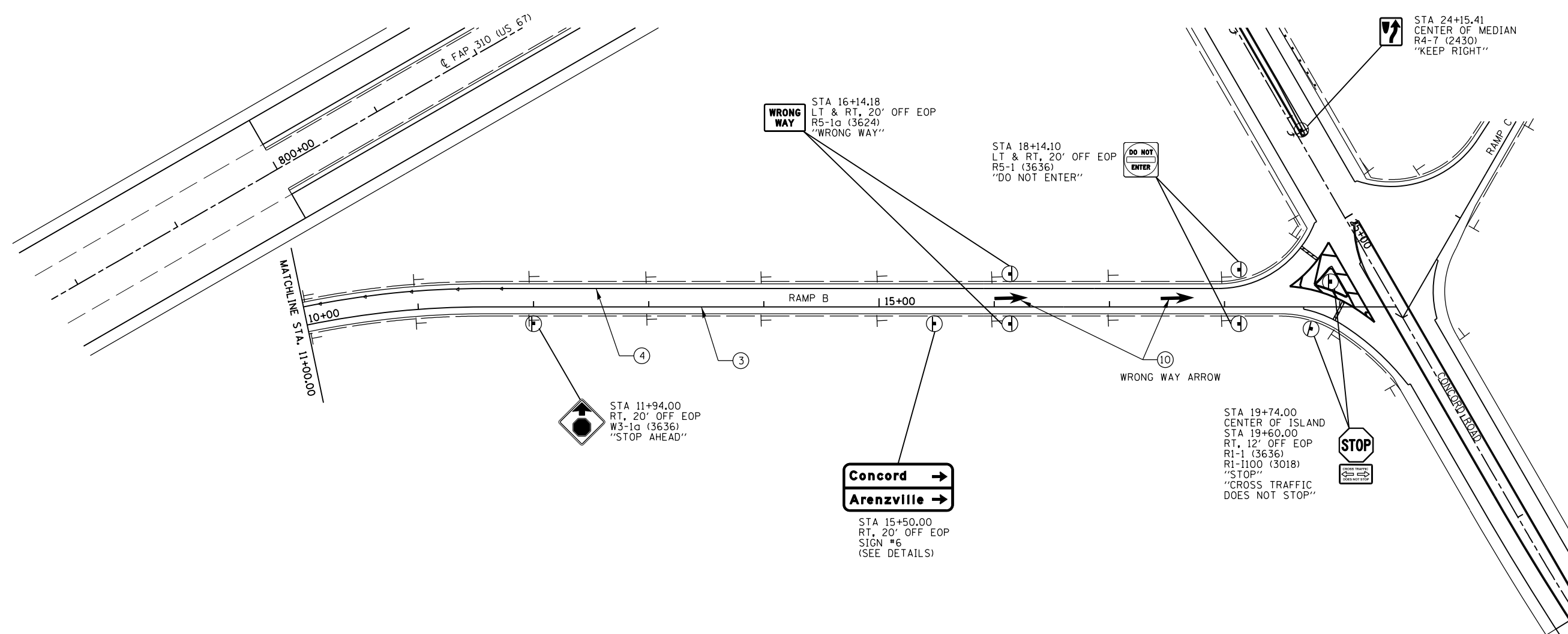
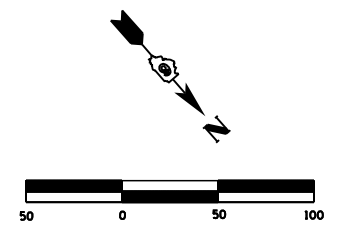
TYPICAL SIDE STREET  
YIELD SIGN INSTALLATION

	R6-1L (3612) "ONE WAY LT"
	R61R (3612) "ONE WAY RT"
	R1-2 (3636) "YIELD"
	INSTALL ON 2 TELESCOPING STEEL POST

PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS</b>				F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\sparksgw\10264881\P&P	Sideroads15-27.dgn	DRAWN -	REVISED -		<b>RAMP B</b>							793	277
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	<b>CONTRACT NO. 72667</b>			
	PLOT DATE = Mar-30-2011 08:49:37AM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								



**PAVEMENT MARKING LEGEND**

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
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- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

**TYPICAL SIDE STREET STOP SIGN INSTALLATION**

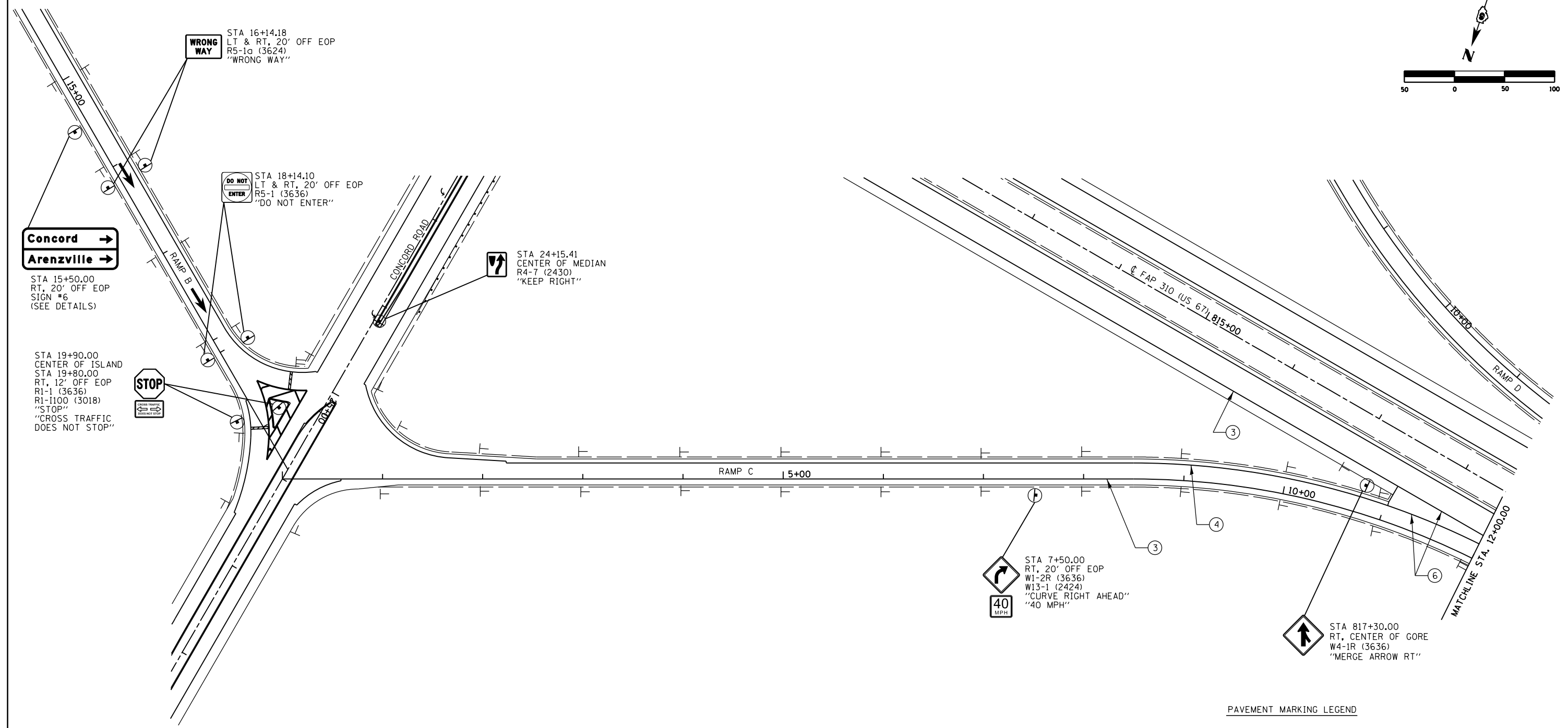
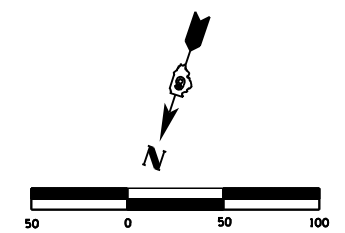
	R6-1L (3612) "ONE WAY LT" BACK TO BACK
	R61R (3612) "ONE WAY RT"
	R1-1 (3636) "STOP"
	R6-3A (2418) "DIVIDED HIGHWAY"
	INSTALL ON 2 TELESCOPING STEEL POST

**TYPICAL SIDE STREET YIELD SIGN INSTALLATION**

	R6-1L (3612) "ONE WAY LT" BACK TO BACK
	R61R (3612) "ONE WAY RT"
	R1-2 (3636) "YIELD"
	INSTALL ON 2 TELESCOPING STEEL POST

**DELINEATOR SYMBOLS**

	- SINGLE
	- DOUBLE



**Concord** →  
**Arenzville** →

STA 15+50.00  
RT, 20' OFF EOP  
SIGN #6  
(SEE DETAILS)

STA 19+90.00  
CENTER OF ISLAND  
STA 19+80.00  
RT, 12' OFF EOP  
R1-1 (3636)  
R1-1100 (3018)  
"STOP"  
"CROSS TRAFFIC  
DOES NOT STOP"

**WRONG WAY**  
STA 16+14.18  
LT & RT, 20' OFF EOP  
R5-1a (3624)  
"WRONG WAY"

**DO NOT ENTER**  
STA 18+14.10  
LT & RT, 20' OFF EOP  
R5-1 (3636)  
"DO NOT ENTER"

↑  
STA 24+15.41  
CENTER OF MEDIAN  
R4-7 (2430)  
"KEEP RIGHT"

↪  
40  
MPH  
STA 7+50.00  
RT, 20' OFF EOP  
W1-2R (3636)  
W13-1 (2424)  
"CURVE RIGHT AHEAD"  
"40 MPH"

↑  
STA 817+30.00  
RT, CENTER OF GORE  
W4-1R (3636)  
"MERGE ARROW RT"

**DELINEATOR SYMBOLS**

┆ - SINGLE  
┆┆ - DOUBLE

**TYPICAL SIDE STREET STOP SIGN INSTALLATION**

R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
 R61R (3612) "ONE WAY RT"  
 R1-1 (3636) "STOP"  
 R6-3A (2418) "DIVIDED HIGHWAY"  
 INSTALL ON 2 TELESCOPING STEEL POST

**TYPICAL SIDE STREET YIELD SIGN INSTALLATION**

R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
 R61R (3612) "ONE WAY RT"  
 R1-2 (3636) "YIELD"  
 INSTALL ON 2 TELESCOPING STEEL POST

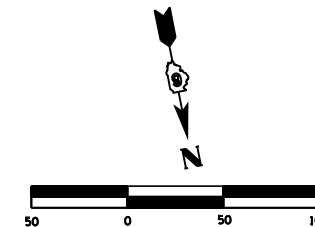
**PAVEMENT MARKING LEGEND**

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
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- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
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- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
ei:\pw\work\p\dot\sparksgw\0264881\p&p	Sideroads15-27.dgn	DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
	PLOT DATE = Mar-30-2011 08:49:38AM	DATE -	REVISED -

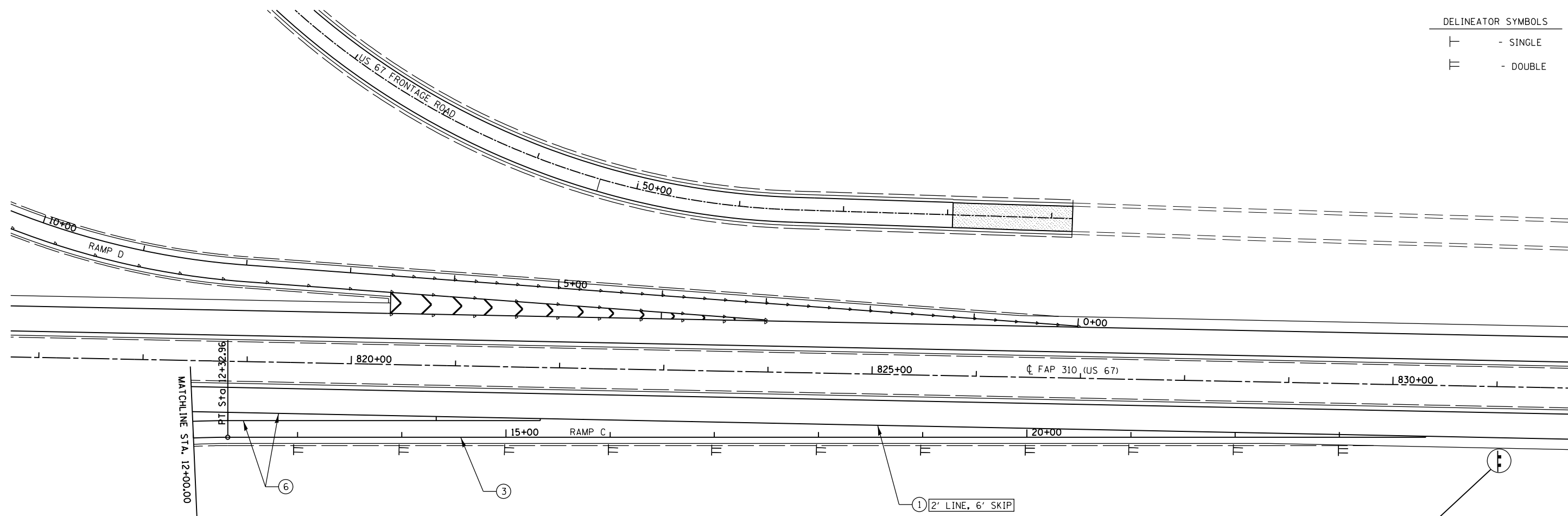
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>STRIPING &amp; SIGNING PLAN SHEETS</b>				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
<b>RAMP C</b>							793	279
SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 72667			
					ILLINOIS FED. AID PROJECT			



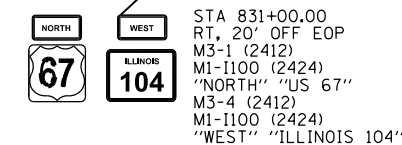
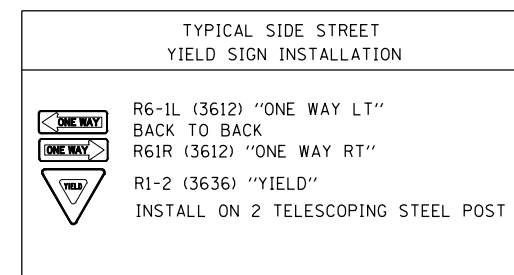
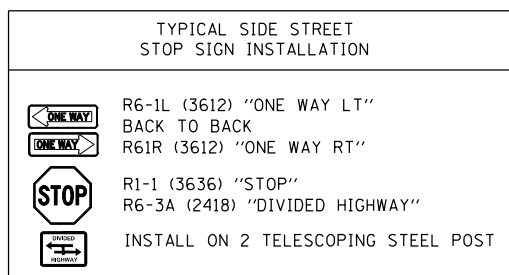
DELINEATOR SYMBOLS

- ┆ - SINGLE
- ≡ - DOUBLE



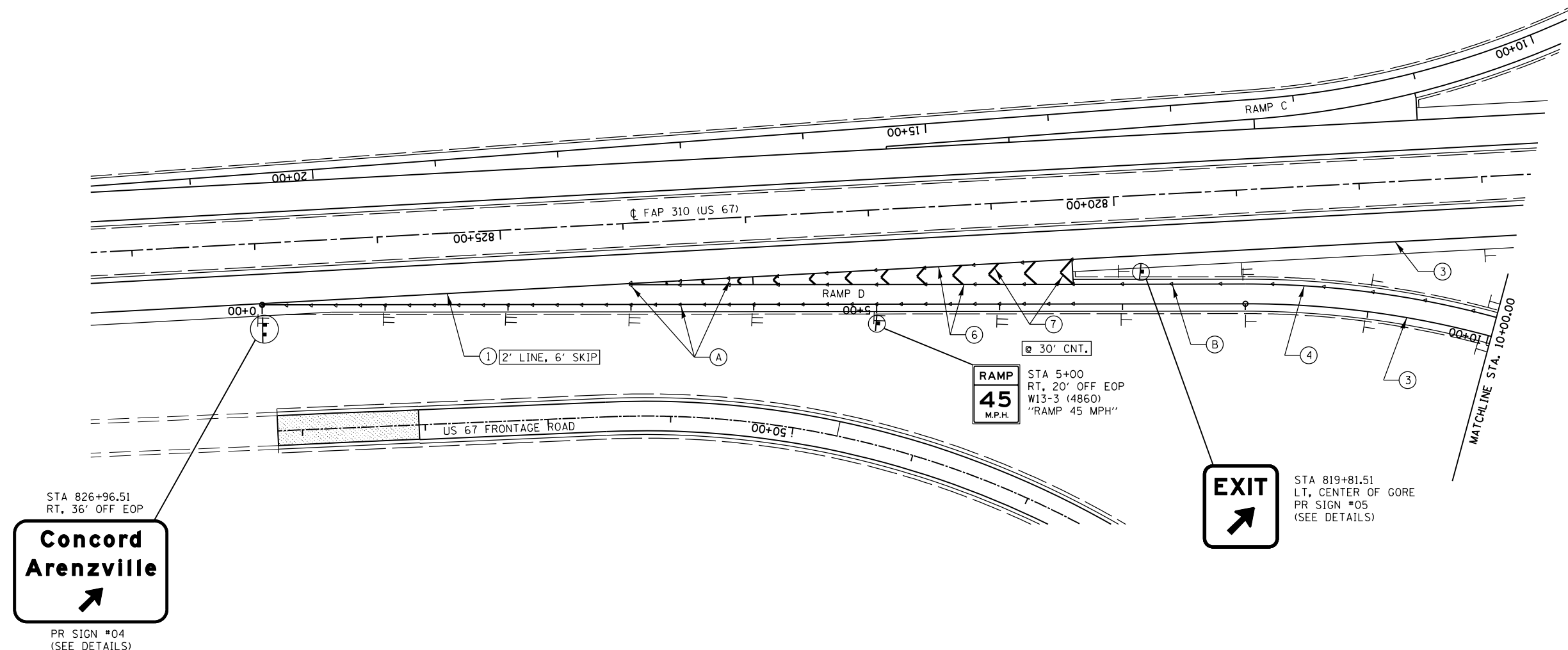
PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
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- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)



FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS</b>				F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ei:\pwork\pwork\sparksgw\0264881\P&P	Sideroads15-27.dgn	DRAWN -	REVISED -		<b>RAMP C</b>							793	280
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	<b>CONTRACT NO. 72667</b>			
	PLOT DATE = Mar-30-2011 08:49:39AM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								





- PAVEMENT MARKING LEGEND**
- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
  - (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
  - (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
  - (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
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  - (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

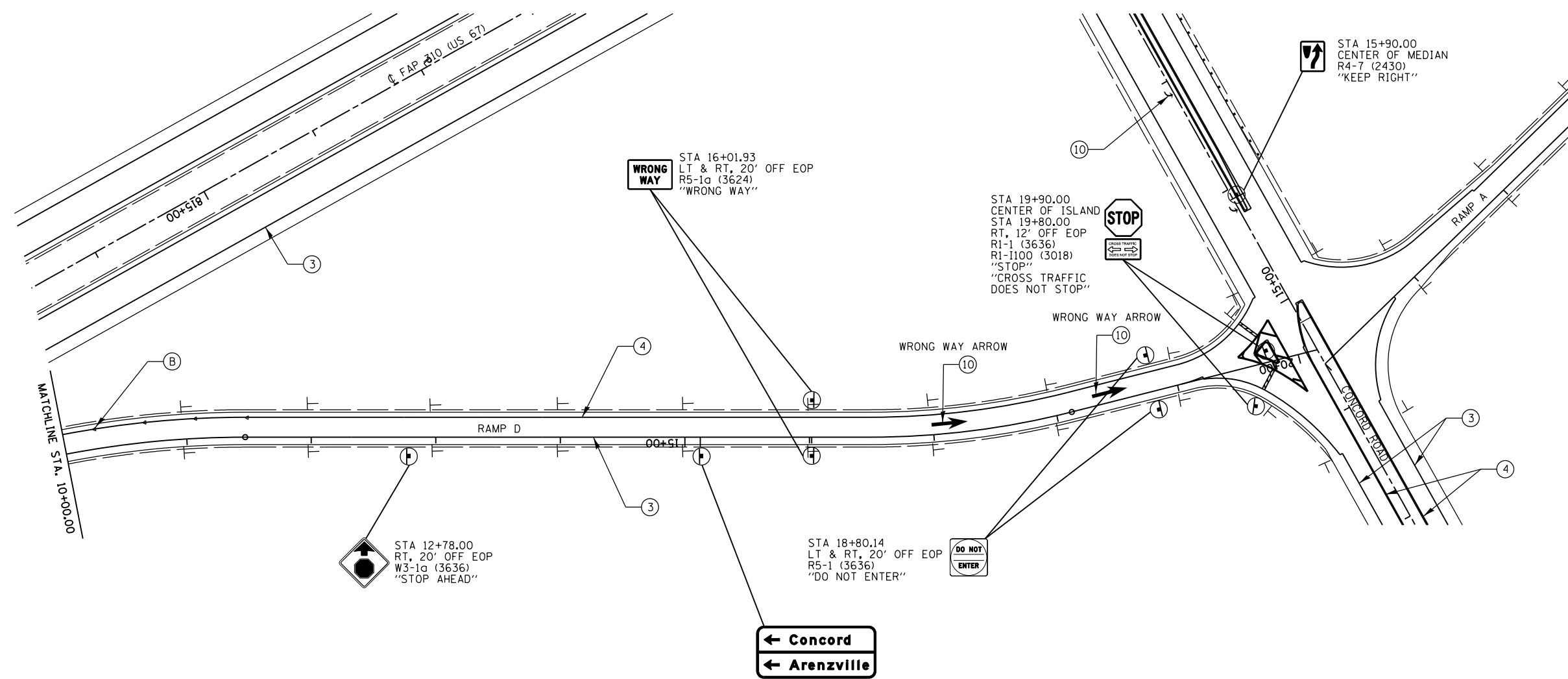
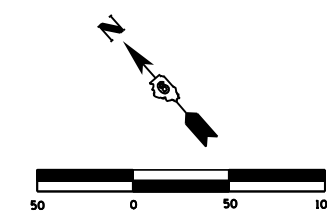
**TYPICAL SIDE STREET STOP SIGN INSTALLATION**

R6-1L (3612) "ONE WAY LT" BACK TO BACK  
 R61R (3612) "ONE WAY RT"  
 R1-1 (3636) "STOP"  
 R6-3A (2418) "DIVIDED HIGHWAY"  
 INSTALL ON 2 TELESCOPING STEEL POST

**TYPICAL SIDE STREET YIELD SIGN INSTALLATION**

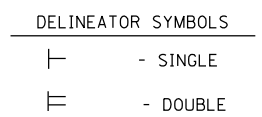
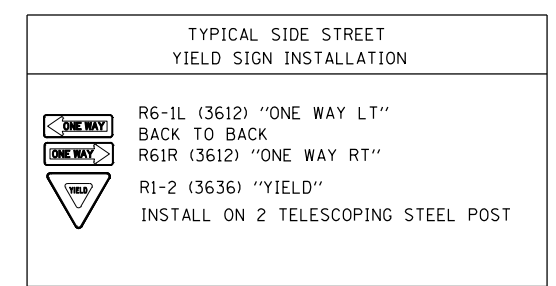
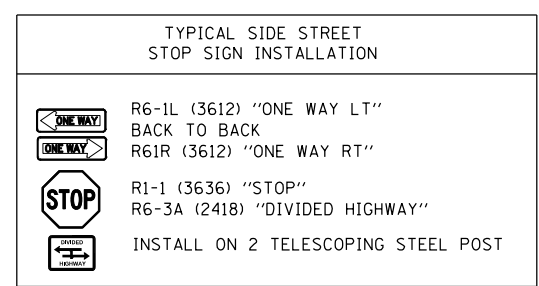
R6-1L (3612) "ONE WAY LT" BACK TO BACK  
 R61R (3612) "ONE WAY RT"  
 R1-2 (3636) "YIELD"  
 INSTALL ON 2 TELESCOPING STEEL POST

- DELINEATOR SYMBOLS**
- SINGLE
  - DOUBLE

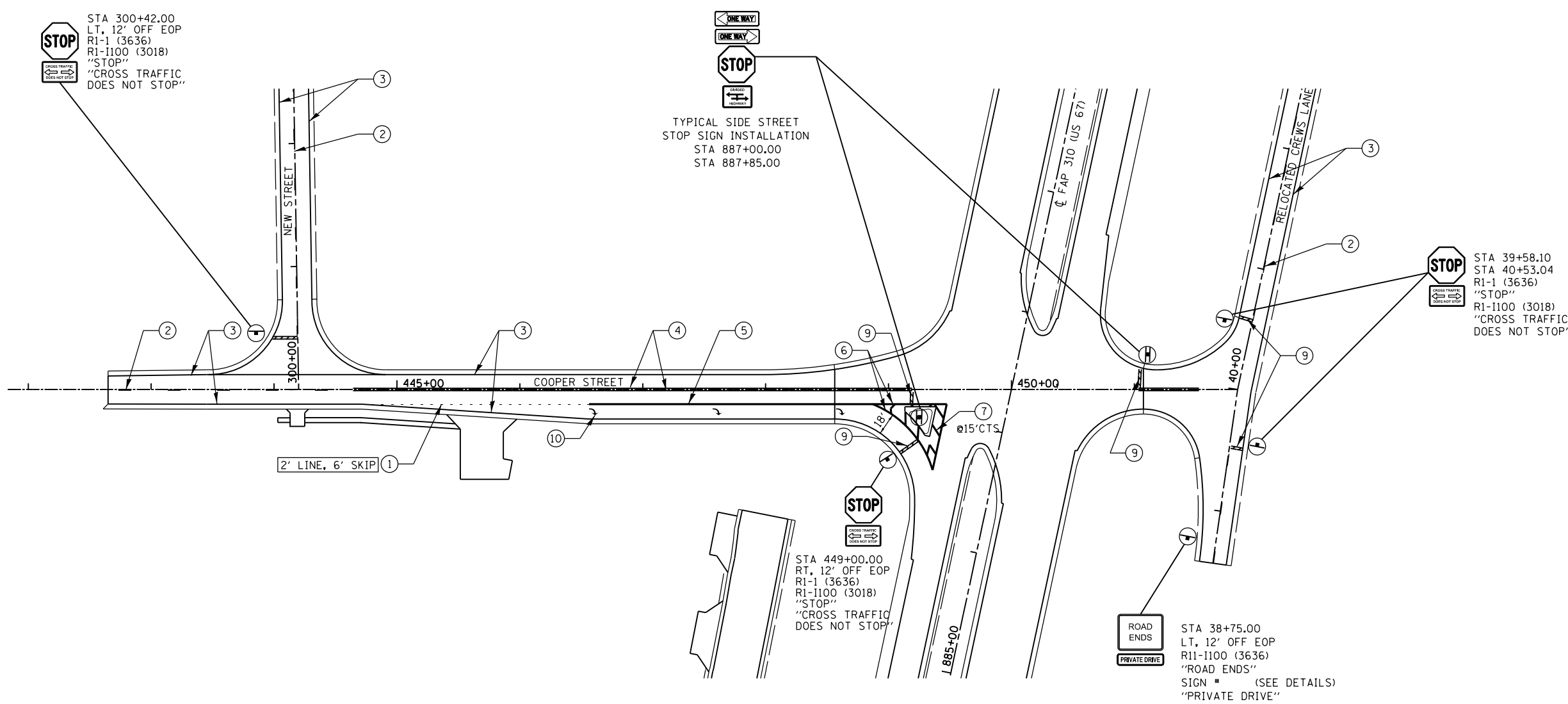
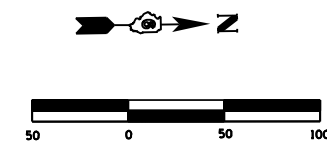


**PAVEMENT MARKING LEGEND**

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
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- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)



FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS RAMP D</b>				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ei:\pw\work\p1dot\sparksgw\10264881\P&P	Sideroods15-27.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.			793	282
	PLOT SCALE = 100.0000' / 1".	CHECKED -	REVISED -		CONTRACT NO. 72667								
	PLOT DATE = Mar-30-2011 08:49:40AM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								



**PAVEMENT MARKING LEGEND**

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
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- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

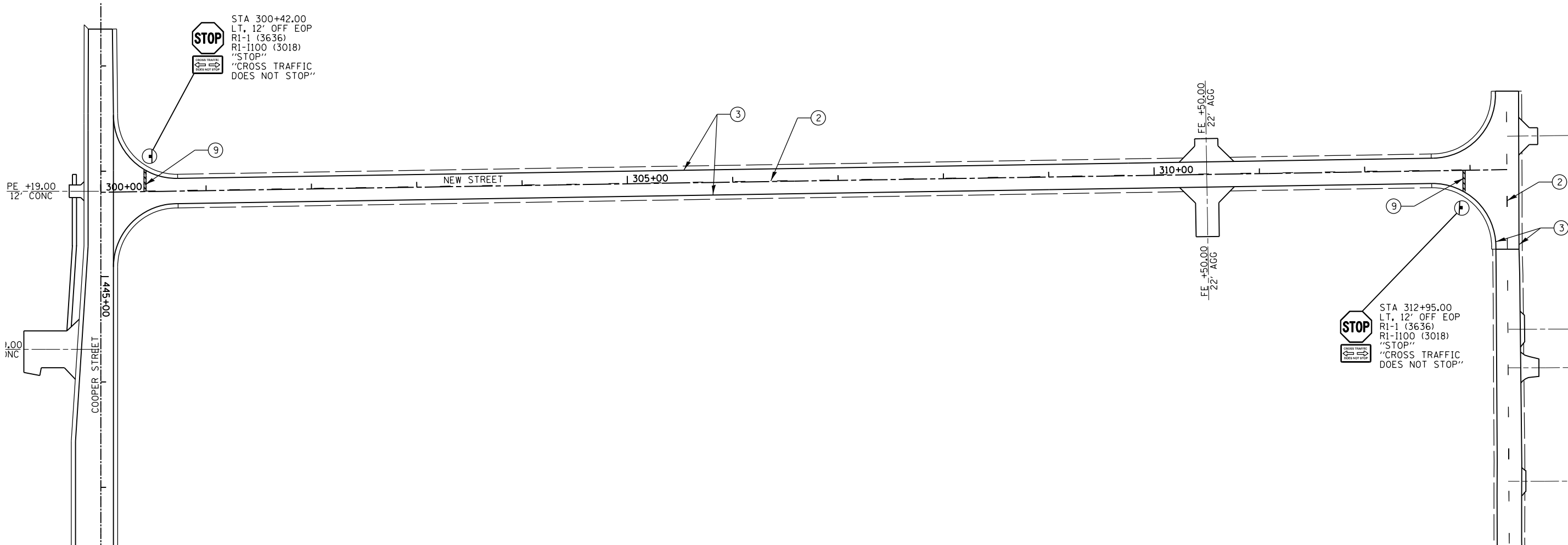
**TYPICAL SIDE STREET STOP SIGN INSTALLATION**

	R6-1L (3612) "ONE WAY LT" BACK TO BACK
	R61R (3612) "ONE WAY RT"
	R1-1 (3636) "STOP" R6-3A (2418) "DIVIDED HIGHWAY"
	INSTALL ON 2 TELESCOPING STEEL POST

**TYPICAL SIDE STREET YIELD SIGN INSTALLATION**

	R6-1L (3612) "ONE WAY LT" BACK TO BACK
	R61R (3612) "ONE WAY RT"
	R1-2 (3636) "YIELD"
	INSTALL ON 2 TELESCOPING STEEL POST

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS COOPER STREET</b>				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ei:\pw\work\p\idot\sparksgw\10264881\P&P	Sideroads15-27.dgn	DRAWN -	REVISED -		SCALE:      SHEET NO.      OF      SHEETS      STA.      TO      STA.							793	283
	PLOT SCALE = 100.0000' / 1".	CHECKED -	REVISED -						CONTRACT NO. 72667				
	PLOT DATE = Mar-30-2011 08:49:41AM	DATE -	REVISED -						ILLINOIS FED. AID PROJECT				



**PAVEMENT MARKING LEGEND**

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
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- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

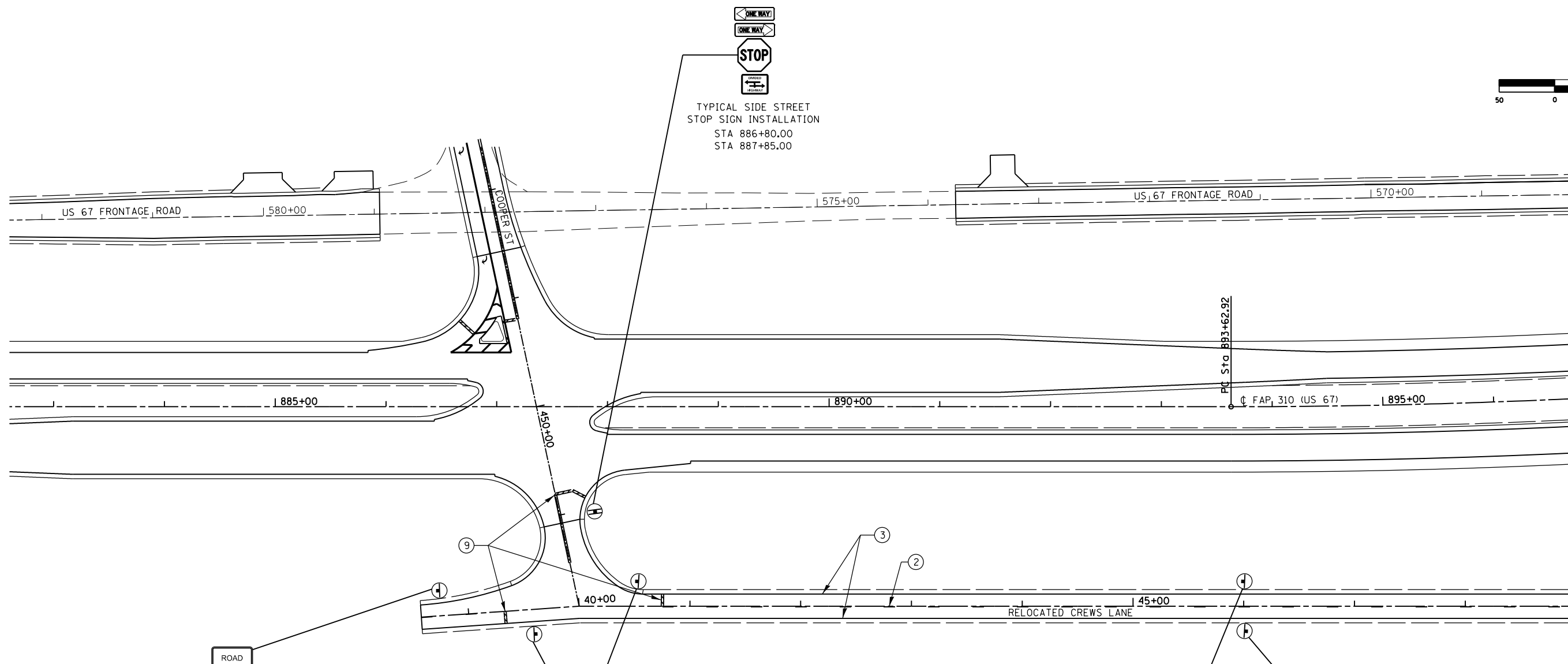
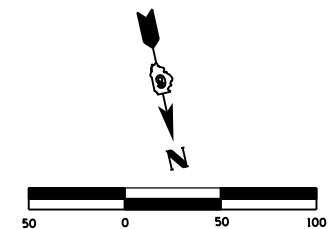
**TYPICAL SIDE STREET  
STOP SIGN INSTALLATION**

	R6-1L (3612) "ONE WAY LT"
	R6-1R (3612) "ONE WAY RT"
	R1-1 (3636) "STOP"
	R6-3A (2418) "DIVIDED HIGHWAY"
INSTALL ON 2 TELESCOPING STEEL POST	

**TYPICAL SIDE STREET  
YIELD SIGN INSTALLATION**

	R6-1L (3612) "ONE WAY LT"
	R6-1R (3612) "ONE WAY RT"
	R1-2 (3636) "YIELD"
INSTALL ON 2 TELESCOPING STEEL POST	

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
et:\pw\work\p\dot\sparksgw\0264881\P&P	Sideroads15-27.dgn	DRAWN -	REVISED -		<b>NEW STREET</b>								
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	<b>CONTRACT NO. 72667</b>			
	PLOT DATE = Mar-30-2011 08:49:41AM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								



ROAD ENDS  
PRIVATE DRIVE

STA 38+75.00  
CENTERLINE  
R11-1100 (3636)  
"ROAD ENDS"  
SIGN #  
"PRIVATE DRIVE"  
(SEE DETAILS)

STOP

STA 39+58.10  
RT, 12' OFF EOP  
STA 40+53.04  
LT, 12' OFF EOP  
R1-1 (3636)  
"STOP"  
R1-1100 (3018)  
"CROSS TRAFFIC  
DOES NOT STOP"

STA 46+00.00  
LT, 20' OFF EOP  
W3-1a (3636)  
"STOP AHEAD"



25  
MPH

STA 46+00.00  
RT, 20' OFF EOP  
W1-1R (3636)  
W13-1 (2424)  
"CURVE RIGHT AHEAD"  
"25 MPH"

PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

TYPICAL SIDE STREET STOP SIGN INSTALLATION

R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
R61R (3612) "ONE WAY RT"  
R1-1 (3636) "STOP"  
R6-3A (2418) "DIVIDED HIGHWAY"  
INSTALL ON 2 TELESCOPING STEEL POST

TYPICAL SIDE STREET YIELD SIGN INSTALLATION

R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
R61R (3612) "ONE WAY RT"  
R1-2 (3636) "YIELD"  
INSTALL ON 2 TELESCOPING STEEL POST

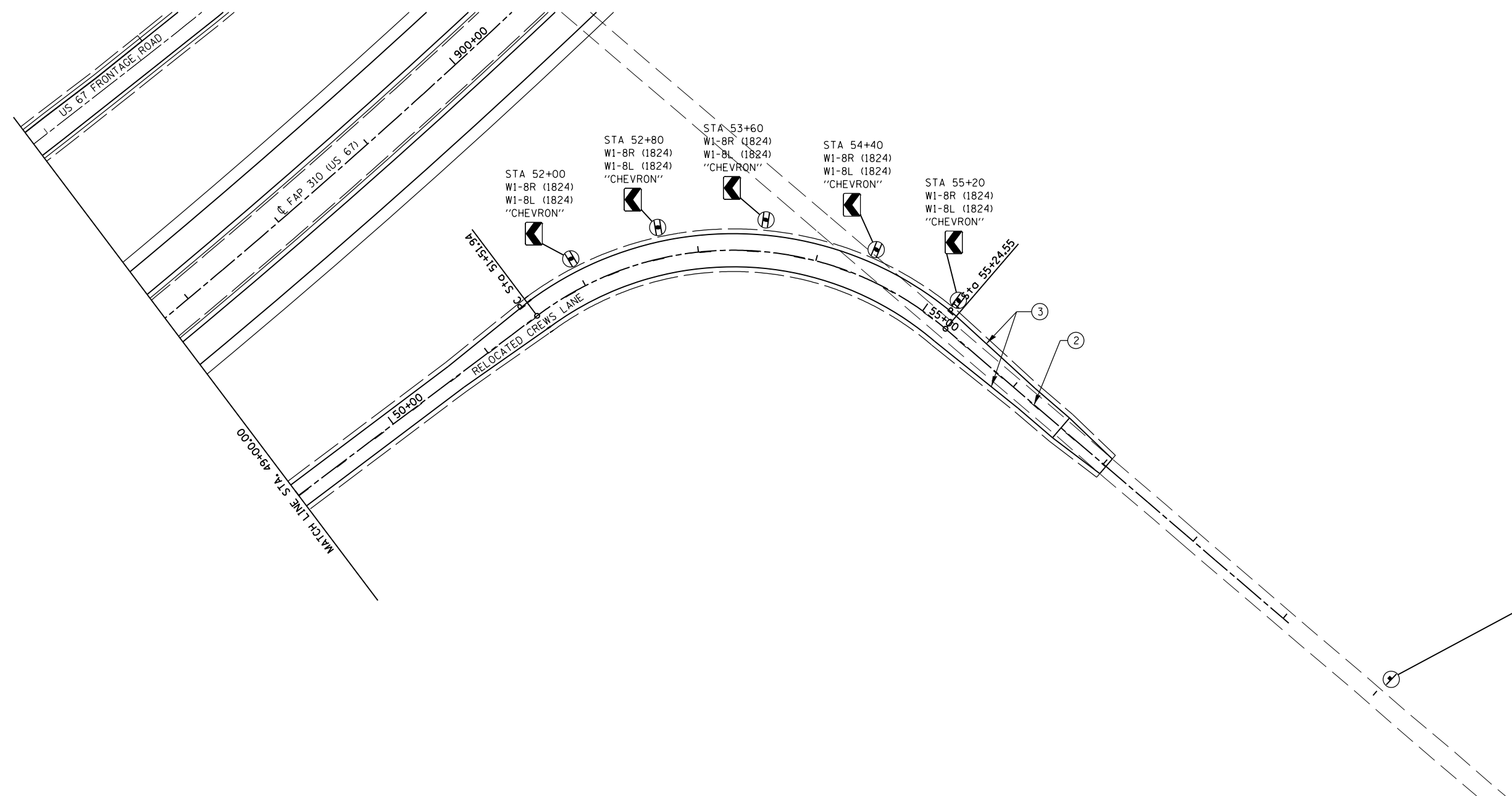
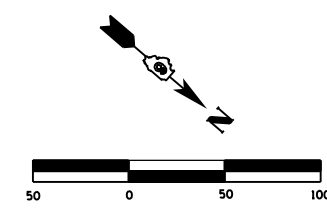
FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
c:\pwork\work\pwork\sparksgw\10264881\P&P	Sideroads1-14.dgn	DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED -
	PLOT DATE = Mar-30-2011 08:49:49AM	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

STRIPING & SIGNING PLAN SHEETS  
RELOCATED CREWS LANE

SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	Morgan	793	285
CONTRACT NO. 72667			ILLINOIS FED. AID PROJECT	



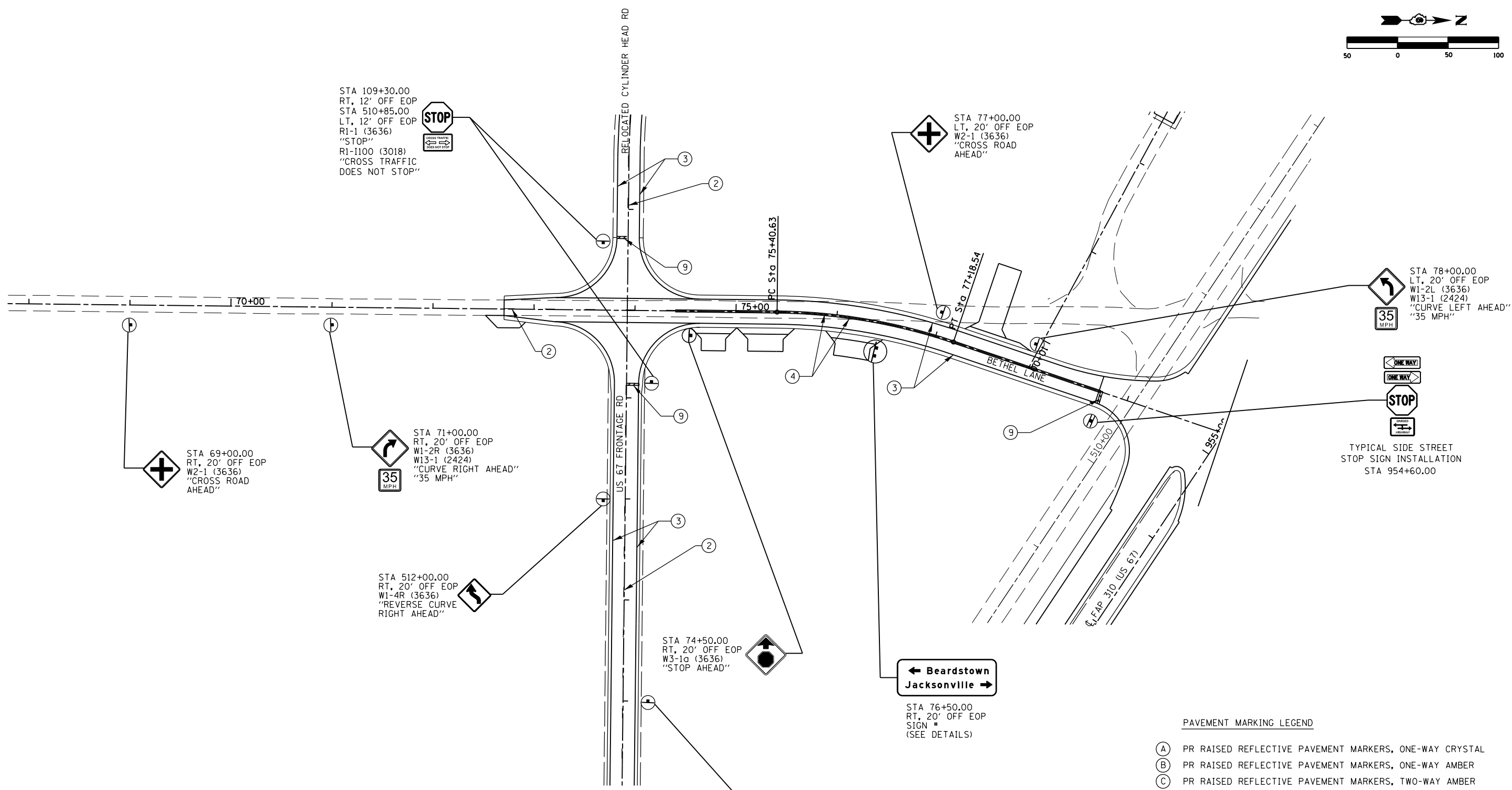
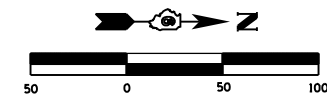
- PAVEMENT MARKING LEGEND**
- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
  - (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
  - (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
  - (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
  - (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
  - (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
  - (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
  - (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
  - (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
  - (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
  - (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
  - (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
  - (I) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
c:\pw\work\p\dot\sparksgw\0264881\p&p	Sideroads1-14.dgn	DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Mar-30-2011 08:49:50AM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STRIPING & SIGNING PLAN SHEETS  
RELOCATED CREWS LANE**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	Morgan	793	286
SCALE:			CONTRACT NO. 72667	
SHEET NO. OF SHEETS		ILLINOIS FED. AID PROJECT		



STA 109+30.00  
RT, 12' OFF EOP  
STA 510+85.00  
LT, 12' OFF EOP  
R1-1 (3636)  
"STOP"  
R1-1100 (3018)  
"CROSS TRAFFIC  
DOES NOT STOP"

STA 77+00.00  
LT, 20' OFF EOP  
W2-1 (3636)  
"CROSS ROAD  
AHEAD"

STA 78+00.00  
LT, 20' OFF EOP  
W1-2L (3636)  
W13-1 (2424)  
"CURVE LEFT AHEAD"  
"35 MPH"

STA 69+00.00  
RT, 20' OFF EOP  
W2-1 (3636)  
"CROSS ROAD  
AHEAD"

STA 71+00.00  
RT, 20' OFF EOP  
W1-2R (3636)  
W13-1 (2424)  
"CURVE RIGHT AHEAD"  
"35 MPH"

TYPICAL SIDE STREET  
STOP SIGN INSTALLATION  
STA 954+60.00

STA 512+00.00  
RT, 20' OFF EOP  
W1-4R (3636)  
"REVERSE CURVE  
RIGHT AHEAD"

STA 74+50.00  
RT, 20' OFF EOP  
W3-1a (3636)  
"STOP AHEAD"

← Beardstown  
Jacksonville →  
STA 76+50.00  
RT, 20' OFF EOP  
SIGN #  
(SEE DETAILS)

TO  
67  
TO  
ILLINOIS  
104  
STA 514+00.00  
LT, 20' OFF EOP  
M4-5 (2412)  
M1-1100 (2424)  
M6-1 (2115)  
"TO" "US 67" "ARROW"  
M4-5 (2412)  
M1-1100 (2424)  
M6-1 (2115)  
"TO" "ILLINOIS 104"  
"ARROW"

**TYPICAL SIDE STREET STOP SIGN INSTALLATION**

R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
R61R (3612) "ONE WAY RT"  
R1-1 (3636) "STOP"  
R6-3A (2418) "DIVIDED HIGHWAY"  
INSTALL ON 2 TELESCOPING STEEL POST

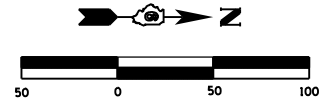
**TYPICAL SIDE STREET YIELD SIGN INSTALLATION**

R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
R61R (3612) "ONE WAY RT"  
R1-2 (3636) "YIELD"  
INSTALL ON 2 TELESCOPING STEEL POST

**PAVEMENT MARKING LEGEND**

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS BETHEL LANE</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\pwork\sparksgw\10264881\P&P	Sideroads1-14.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	310	69-3	Morgan	793	287
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -						CONTRACT NO. 72667					
	PLOT DATE = Mar-30-2011 08:49:51AM	DATE -	REVISED -						ILLINOIS FED. AID PROJECT					



STA 593+90.00  
CENTERLINE  
R11-1100 (3636)  
2-0M4-1 (1818)  
"ROAD ENDS"  
w/ 2- OBJECT MARKERS

STA 87+50.00  
LT, 20' OFF EOP  
W3-1a (3636)  
"STOP AHEAD"

STA 90+00.00  
LT, 20' OFF EOP  
W2-2R (3636)  
"SIDE ROAD SYMBOL 90 RT"

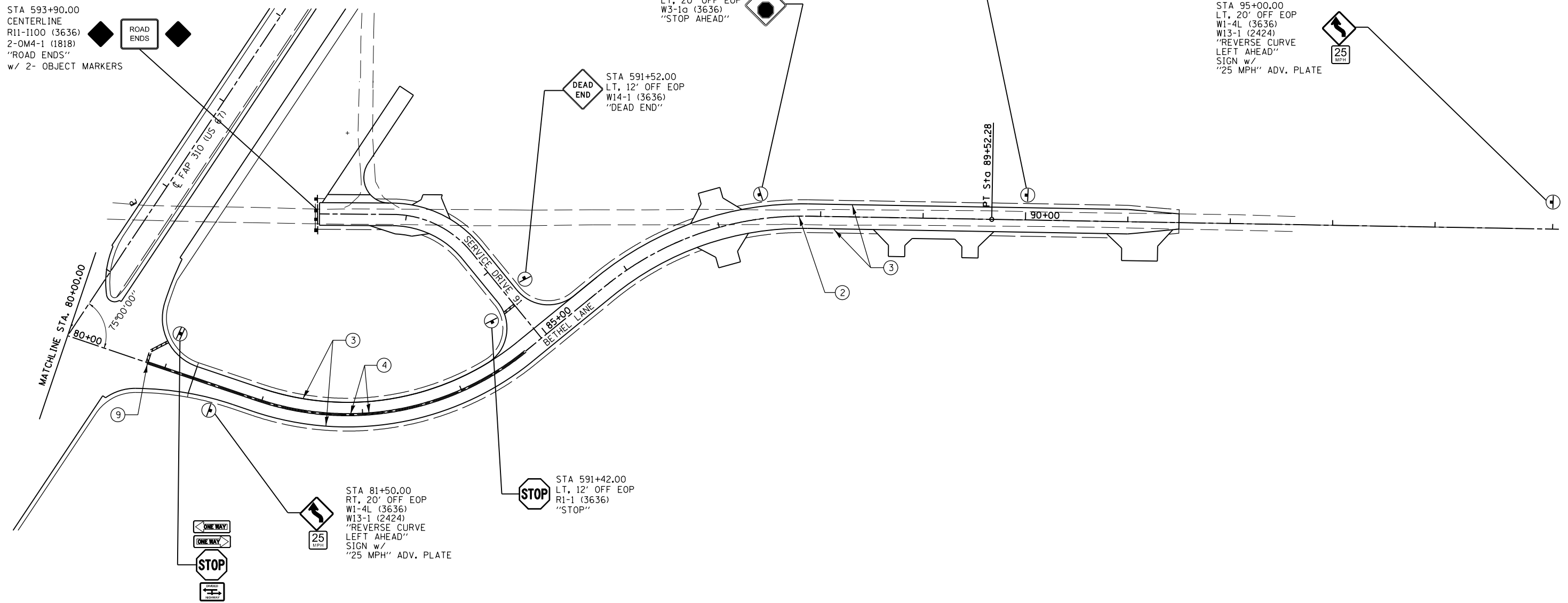
STA 95+00.00  
LT, 20' OFF EOP  
W1-4L (3636)  
W13-1 (2424)  
"REVERSE CURVE  
LEFT AHEAD"  
SIGN w/  
"25 MPH" ADV. PLATE

STA 591+52.00  
LT, 12' OFF EOP  
W14-1 (3636)  
"DEAD END"

STA 81+50.00  
RT, 20' OFF EOP  
W1-4L (3636)  
W13-1 (2424)  
"REVERSE CURVE  
LEFT AHEAD"  
SIGN w/  
"25 MPH" ADV. PLATE

STA 591+42.00  
LT, 12' OFF EOP  
R1-1 (3636)  
"STOP"

TYPICAL SIDE STREET  
STOP SIGN INSTALLATION  
STA 955+82.00



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

TYPICAL SIDE STREET  
STOP SIGN INSTALLATION

R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
R6-1R (3612) "ONE WAY RT"  
R1-1 (3636) "STOP"  
R6-3A (2418) "DIVIDED HIGHWAY"  
INSTALL ON 2 TELESCOPING STEEL POST

TYPICAL SIDE STREET  
YIELD SIGN INSTALLATION

R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
R6-1R (3612) "ONE WAY RT"  
R1-2 (3636) "YIELD"  
INSTALL ON 2 TELESCOPING STEEL POST

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
ct:\pw\work\p\idot\sparksgw\10264881\p&p	Sideroads1-14.dgn	DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Mar-30-2011 08:49:51AM	DATE -	REVISED -

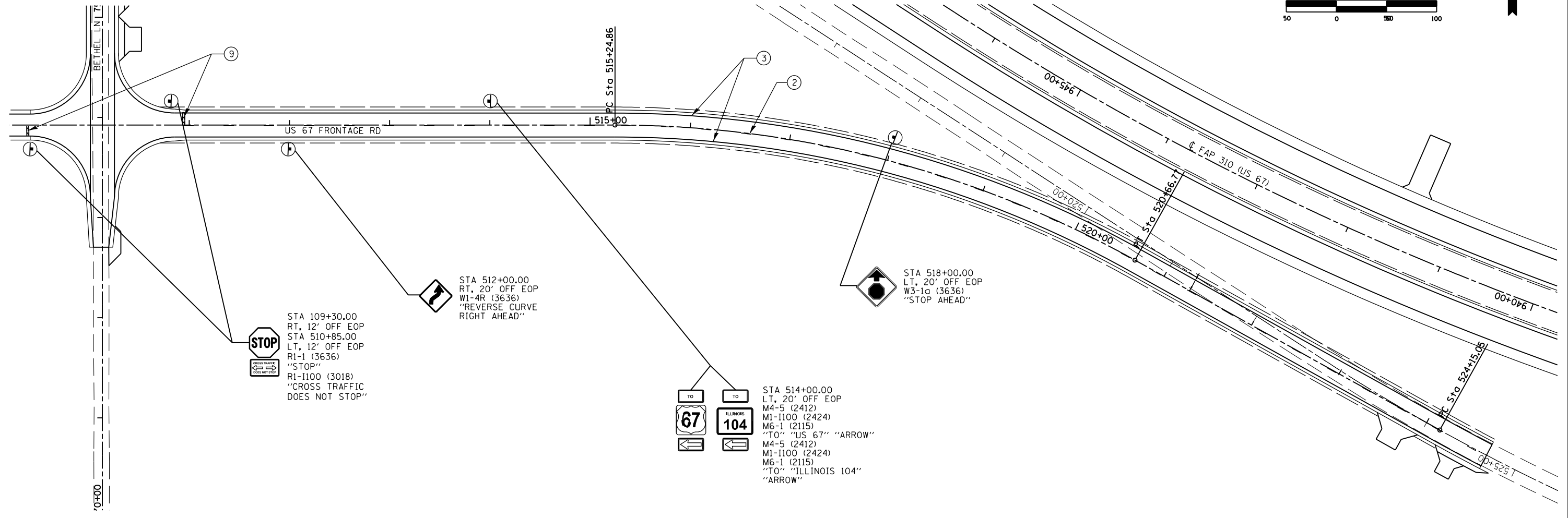
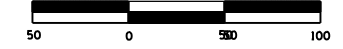
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

STRIPING & SIGNING PLAN SHEETS  
BETHEL LANE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	Morgan	793	288
CONTRACT NO. 72667			ILLINOIS FED. AID PROJECT	

SCALE: SHEET NO. OF SHEETS STA. TO STA.





STA 109+30.00  
 RT, 12' OFF EOP  
 STA 510+85.00  
 LT, 12' OFF EOP  
 R1-1 (3636)  
 "STOP"  
 R1-1100 (3018)  
 "CROSS TRAFFIC  
 DOES NOT STOP"

STA 512+00.00  
 RT, 20' OFF EOP  
 W1-4R (3636)  
 "REVERSE CURVE  
 RIGHT AHEAD"

STA 514+00.00  
 LT, 20' OFF EOP  
 M4-5 (2412)  
 M1-1100 (2424)  
 M6-1 (2115)  
 "TO" "US 67" "ARROW"  
 M4-5 (2412)  
 M1-1100 (2424)  
 M6-1 (2115)  
 "TO" "ILLINOIS 104"  
 "ARROW"

STA 518+00.00  
 LT, 20' OFF EOP  
 W3-1a (3636)  
 "STOP AHEAD"

PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

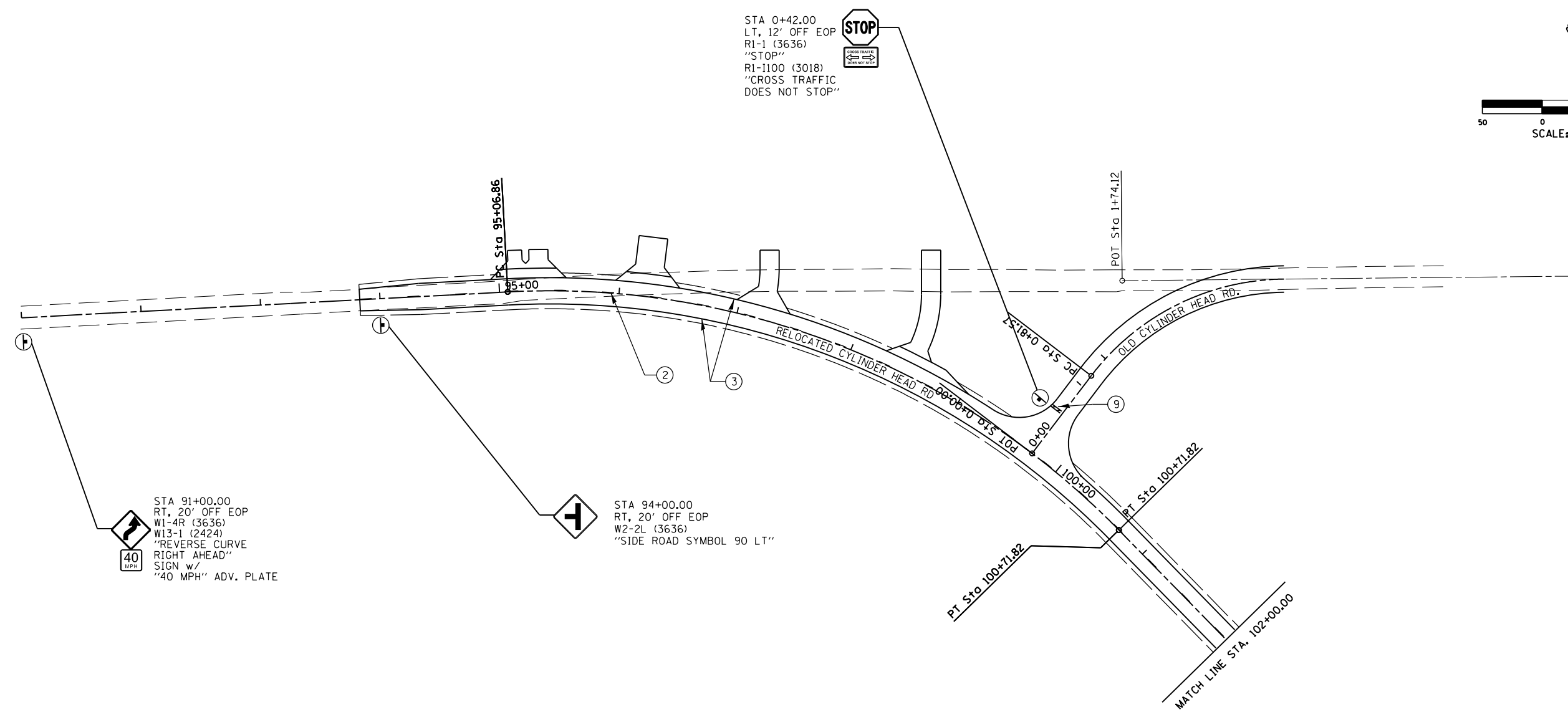
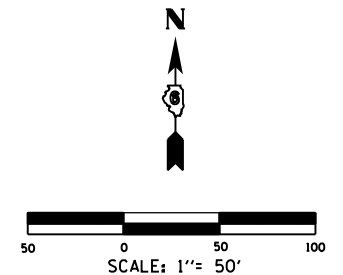
FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
c:\pw\work\p\dot\sparksgw\0264881\P&P	Sideroads1-14.dgn	DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Mar-30-2011 08:49:52AM	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**STRIPING & SIGNING PLAN SHEETS**  
**US 67 FRONTAGE ROAD**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	Morgan	793	289
CONTRACT NO. 72667				
ILLINOIS FED. AID PROJECT				



STA 91+00.00  
 RT, 20' OFF EOP  
 W1-4R (3636)  
 W13-1 (2424)  
 "REVERSE CURVE  
 RIGHT AHEAD"  
 SIGN w/  
 "40 MPH" ADV. PLATE

STA 94+00.00  
 RT, 20' OFF EOP  
 W2-2L (3636)  
 "SIDE ROAD SYMBOL 90 LT"

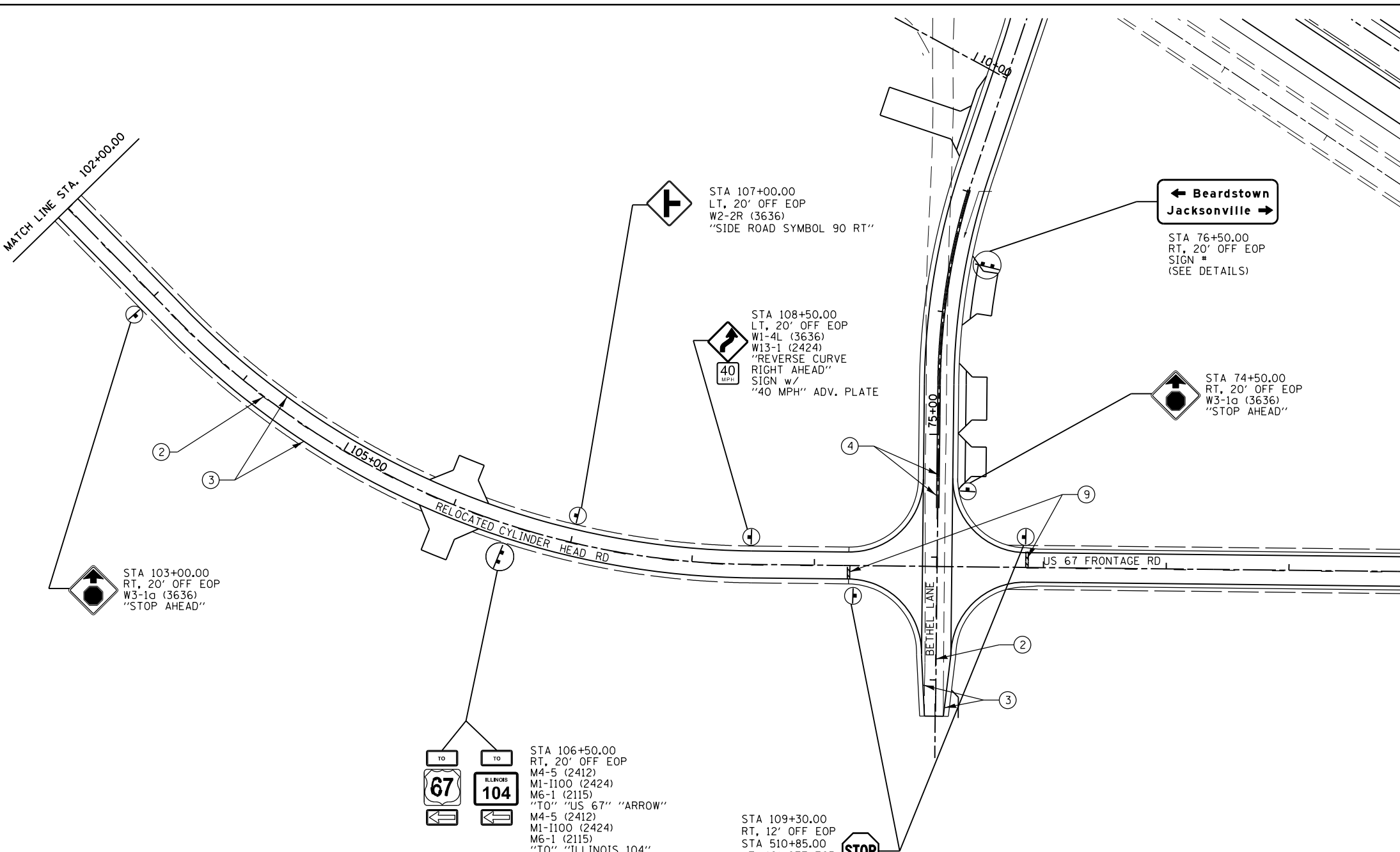
STA 0+42.00  
 LT, 12' OFF EOP  
 R1-1 (3636)  
 "STOP"  
 R1-1100 (3018)  
 "CROSS TRAFFIC  
 DOES NOT STOP"

**PAVEMENT MARKING LEGEND**

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS RELOCATED CYLINDER HEAD ROAD</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ei:\pw_work\pwidot\sparksgw\0264881\P&P	Sideroads1-14.dgn	DRAWN -	REVISED -		310	69-3	Morgan	793	290			
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 72667							
	PLOT DATE = Mar-30-2011 08:49:52AM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
				SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.				

MATCH LINE STA. 102+00.00



STA 103+00.00  
RT, 20' OFF EOP  
W3-1a (3636)  
"STOP AHEAD"

STA 107+00.00  
LT, 20' OFF EOP  
W2-2R (3636)  
"SIDE ROAD SYMBOL 90 RT"

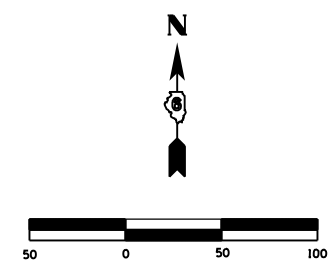
STA 108+50.00  
LT, 20' OFF EOP  
W1-4L (3636)  
W13-1 (2424)  
"REVERSE CURVE  
RIGHT AHEAD"  
SIGN w/  
"40 MPH" ADV. PLATE

← Beardstown  
Jacksonville →  
STA 76+50.00  
RT, 20' OFF EOP  
SIGN #  
(SEE DETAILS)

STA 74+50.00  
RT, 20' OFF EOP  
W3-1a (3636)  
"STOP AHEAD"

TO TO  
67 ILLINOIS 104  
M4-5 (2412)  
M1-I100 (2424)  
M6-1 (2115)  
"TO" "US 67" "ARROW"  
M4-5 (2412)  
M1-I100 (2424)  
M6-1 (2115)  
"TO" "ILLINOIS 104"  
"ARROW"

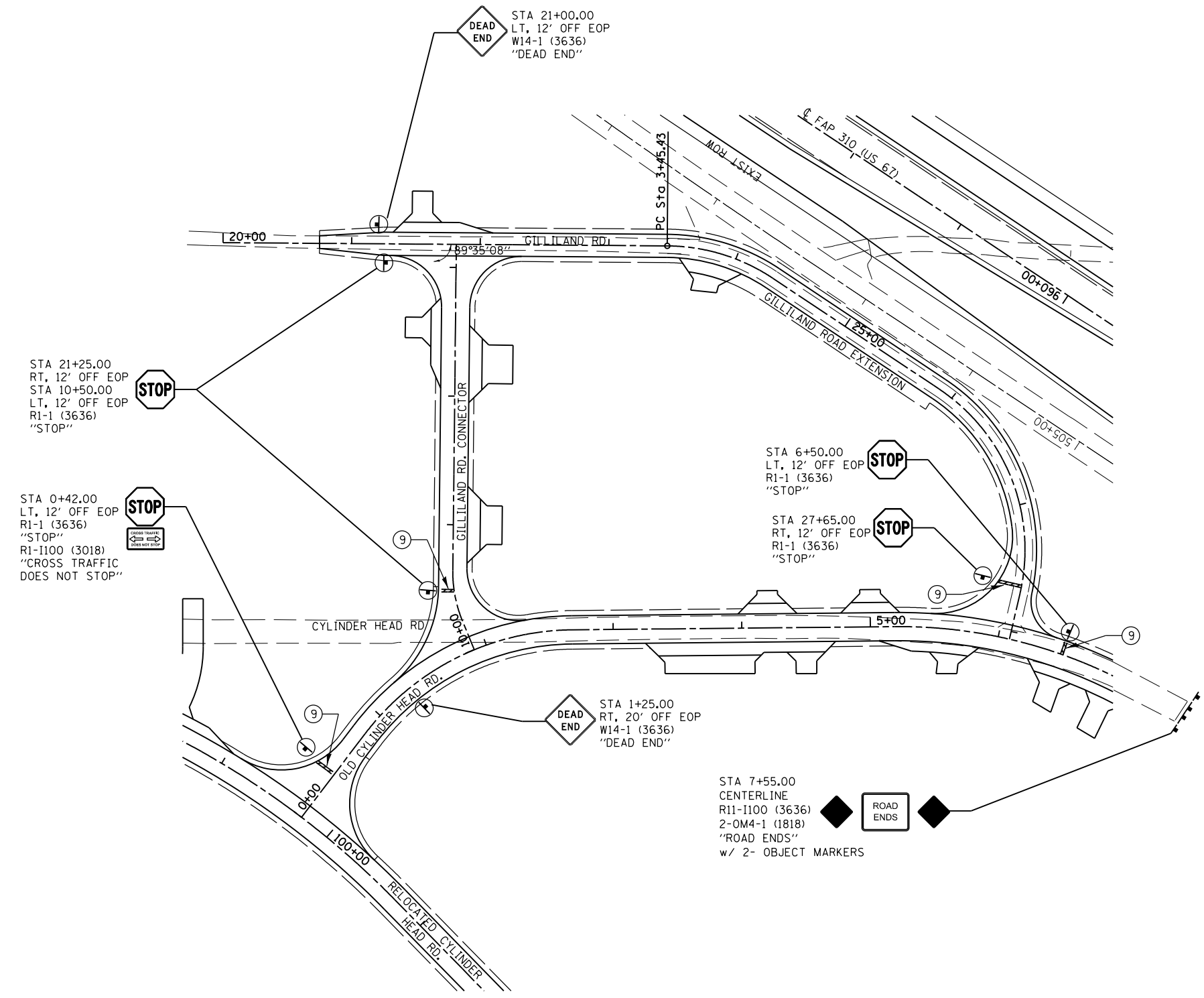
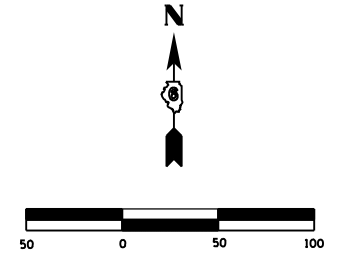
STA 109+30.00  
RT, 12' OFF EOP  
STA 510+85.00  
LT, 12' OFF EOP  
R1-1 (3636)  
"STOP"  
R1-I100 (3018)  
"CROSS TRAFFIC  
DOES NOT STOP"



PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS RELOCATED CYLINDER HEAD ROAD</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
ci:\pw\work\p\idot\sparksgw\10264881\P&P	Sideroads1-14.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	310	69-3	Morgan	793	291
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 72667									
	PLOT DATE = Mar-30-2011 08:49:53AM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT									



STA 7+55.00  
 CENTERLINE  
 R11-1100 (3636)  
 2-0M4-1 (1818)  
 "ROAD ENDS"  
 w/ 2- OBJECT MARKERS

**PAVEMENT MARKING LEGEND**

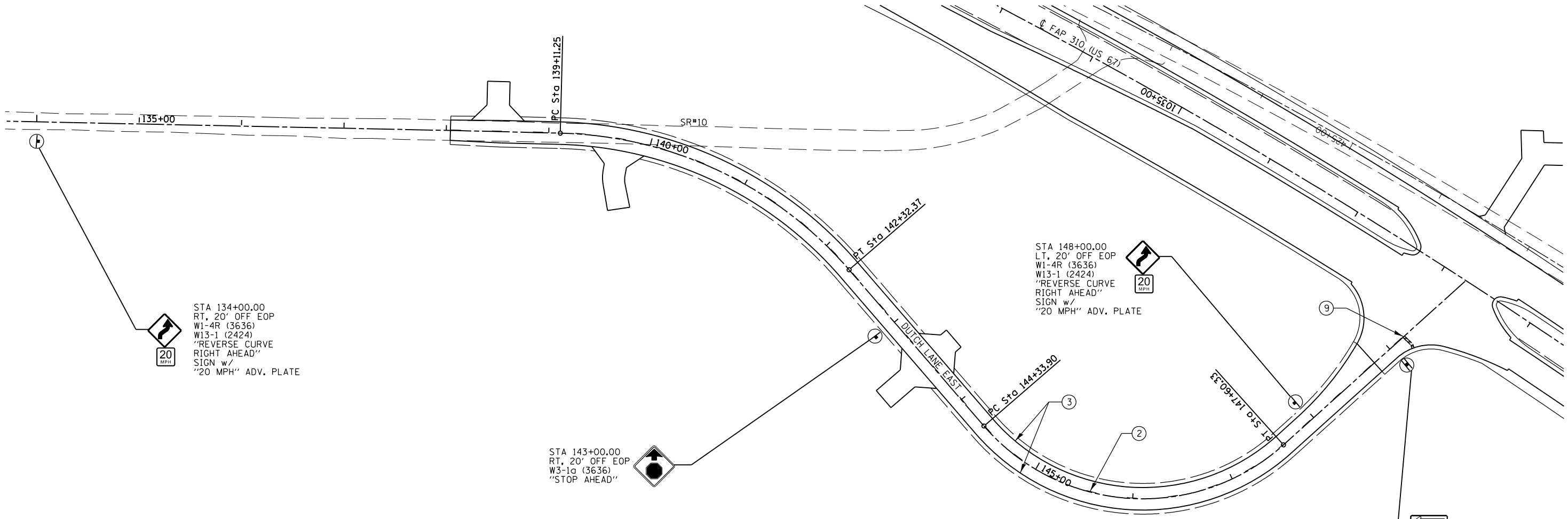
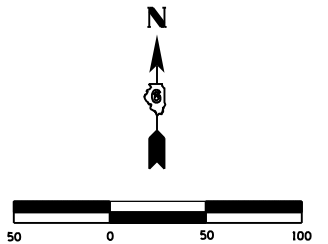
- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
c:\pwork\work\pwork\sparksgw\0264881\P&P	Sideroads1-14.dgn	DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Mar-30-2011 08:49:54AM	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**STRIPING & SIGNING PLAN SHEETS  
 OLD CYLINDER HEAD ROAD & GILLILAND ROAD**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	Morgan	793	292
SCALE:			CONTRACT NO. 72667	
SHEET NO. OF SHEETS		ILLINOIS FED. AID PROJECT		



STA 134+00.00  
RT, 20' OFF EOP  
W1-4R (3636)  
W13-1 (2424)  
"REVERSE CURVE  
RIGHT AHEAD"  
SIGN w/  
"20 MPH" ADV. PLATE

STA 143+00.00  
RT, 20' OFF EOP  
W3-1a (3636)  
"STOP AHEAD"

STA 148+00.00  
LT, 20' OFF EOP  
W1-4R (3636)  
W13-1 (2424)  
"REVERSE CURVE  
RIGHT AHEAD"  
SIGN w/  
"20 MPH" ADV. PLATE

**PAVEMENT MARKING LEGEND**

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

**TYPICAL SIDE STREET STOP SIGN INSTALLATION**

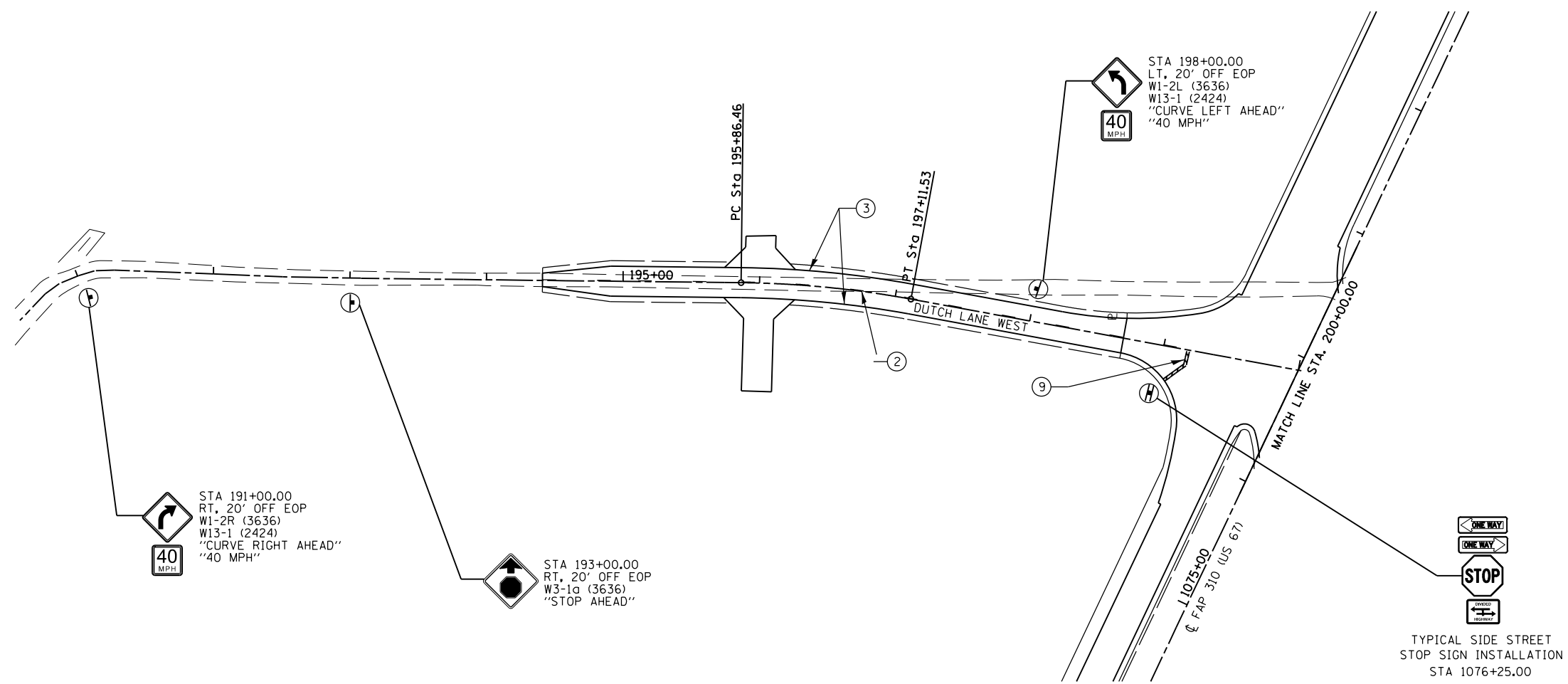
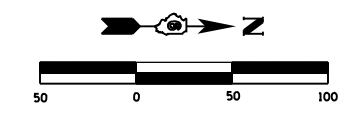
R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
R61R (3612) "ONE WAY RT"  
R1-1 (3636) "STOP"  
R6-3A (2418) "DIVIDED HIGHWAY"  
INSTALL ON 2 TELESCOPING STEEL POST

**TYPICAL SIDE STREET YIELD SIGN INSTALLATION**

R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
R61R (3612) "ONE WAY RT"  
R1-2 (3636) "YIELD"  
INSTALL ON 2 TELESCOPING STEEL POST

**TYPICAL SIDE STREET STOP SIGN INSTALLATION**  
STA 1031+60.00

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS DUTCH LANE EAST</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pw\work\p\idot\sparksgw\10264881\P&P	Sideroads1-14.dgn	DRAWN -	REVISED -		310	69-3	Morgan	793	293				
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -	CONTRACT NO. 72667		ILLINOIS FED. AID PROJECT								
PLOT DATE = Mar-30-2011 08:49:54AM	DATE -	REVISED -	SCALE:		SHEET NO.	OF SHEETS	STA.	TO STA.					



L POSTPAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
- (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
- (6) PR URETHANE PAVEMENT MARKING- LINE 8" (SOLID WHITE)
- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

TYPICAL SIDE STREET  
STOP SIGN INSTALLATION

R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
R61R (3612) "ONE WAY RT"  
R1-1 (3636) "STOP"  
R6-3A (2418) "DIVIDED HIGHWAY"  
INSTALL ON 2 TELESCOPING STEEL POST

TYPICAL SIDE STREET  
YIELD SIGN INSTALLATION

R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
R61R (3612) "ONE WAY RT"  
R1-2 (3636) "YIELD"  
INSTALL ON 2 TELESCOPING STEEL POST

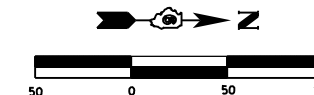
FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -
ei:\pw\work\p\dot\sparksgw\10264881\P&P	Sideroads1-14.dgn	DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = Mar-30-2011 08:49:55AM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

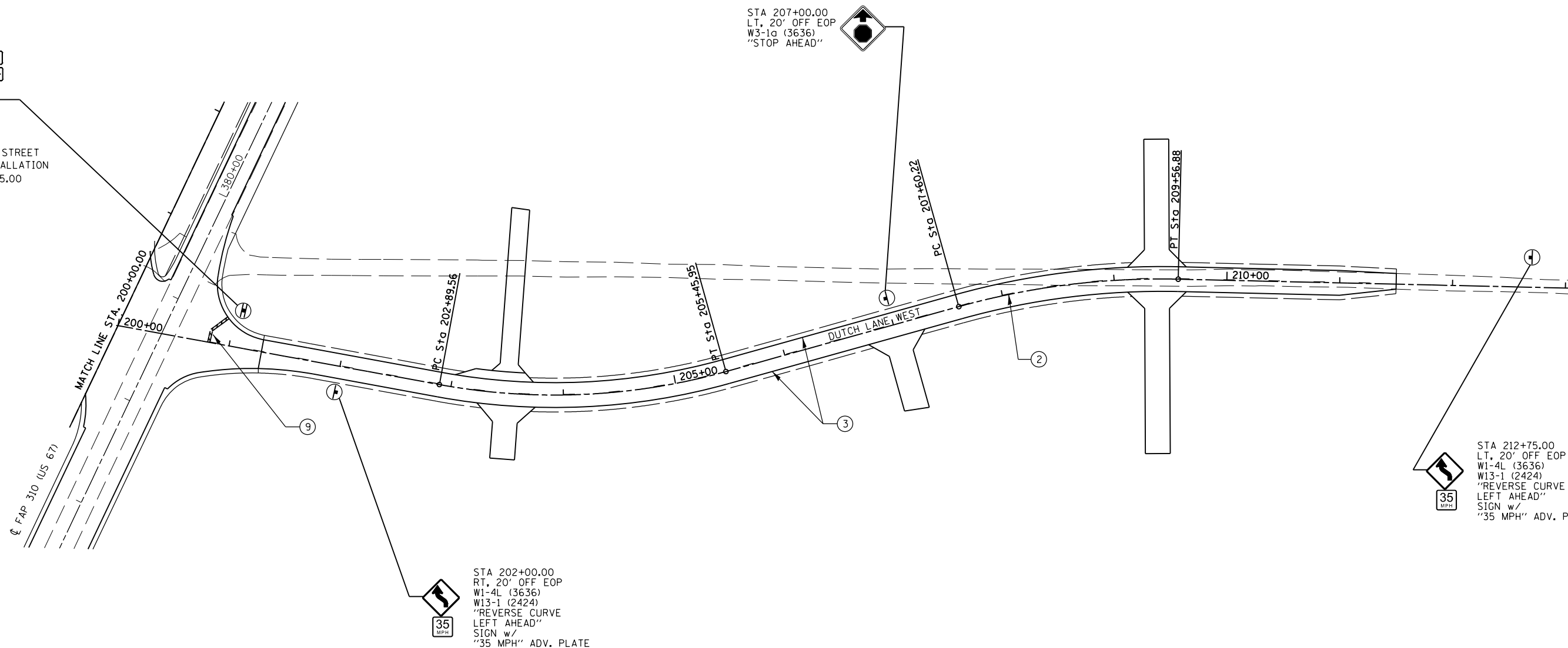
**STRIPING & SIGNING PLAN SHEETS  
DUTCH LANE WEST**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3	Morgan	793	294
CONTRACT NO. 72667				
ILLINOIS FED. AID PROJECT				

SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.
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TYPICAL SIDE STREET  
STOP SIGN INSTALLATION  
STA 1077+45.00



STA 202+00.00  
RT, 20' OFF EOP  
W1-4L (3636)  
W13-1 (2424)  
"REVERSE CURVE  
LEFT AHEAD"  
SIGN w/  
"35 MPH" ADV. PLATE

STA 212+75.00  
LT, 20' OFF EOP  
W1-4L (3636)  
W13-1 (2424)  
"REVERSE CURVE  
LEFT AHEAD"  
SIGN w/  
"35 MPH" ADV. PLATE

PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
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- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

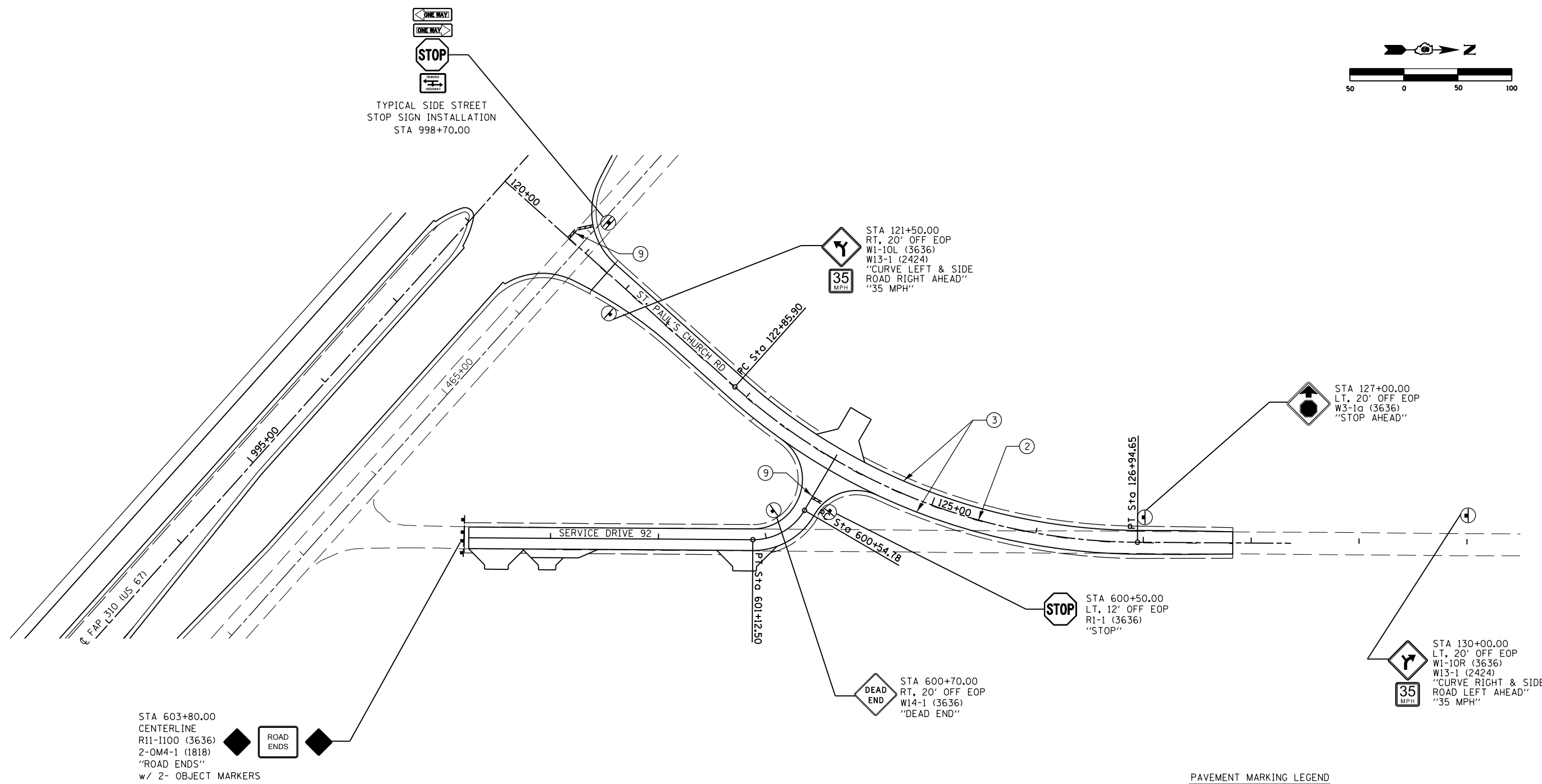
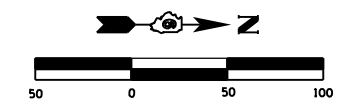
TYPICAL SIDE STREET  
STOP SIGN INSTALLATION

R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
R61R (3612) "ONE WAY RT"  
R1-1 (3636) "STOP"  
R6-3A (2418) "DIVIDED HIGHWAY"  
INSTALL ON 2 TELESCOPING STEEL POST

TYPICAL SIDE STREET  
YIELD SIGN INSTALLATION

R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
R61R (3612) "ONE WAY RT"  
R1-2 (3636) "YIELD"  
INSTALL ON 2 TELESCOPING STEEL POST

FILE NAME = c:\pwork\pwork\pwork\sparksgw\10264881\p&p	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS DUTCH LANE WEST</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	Sideroads1-14.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	310	69-3	Morgan	793
	PLOT SCALE = 100.0000' / 1".	CHECKED -	REVISED -						CONTRACT NO. 72667				
	PLOT DATE = Mar-30-2011 08:49:55AM	DATE -	REVISED -						ILLINOIS FED. AID PROJECT				



STA 603+80.00  
CENTERLINE  
R11-I100 (3636)  
2-OM4-1 (1818)  
"ROAD ENDS"  
w/ 2- OBJECT MARKERS

TYPICAL SIDE STREET  
STOP SIGN INSTALLATION  
STA 998+70.00

STA 121+50.00  
RT, 20' OFF EOP  
W1-10L (3636)  
W13-1 (2424)  
"CURVE LEFT & SIDE  
ROAD RIGHT AHEAD"  
"35 MPH"

STA 127+00.00  
LT, 20' OFF EOP  
W3-1a (3636)  
"STOP AHEAD"

STOP STA 600+50.00  
LT, 12' OFF EOP  
R1-1 (3636)  
"STOP"

DEAD END STA 600+70.00  
RT, 20' OFF EOP  
W14-1 (3636)  
"DEAD END"

STA 130+00.00  
LT, 20' OFF EOP  
W1-10R (3636)  
W13-1 (2424)  
"CURVE RIGHT & SIDE  
ROAD LEFT AHEAD"  
"35 MPH"

PAVEMENT MARKING LEGEND

- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
- (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
- (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
- (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
- (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
- (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
- (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
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- (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
- (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
- (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
- (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

TYPICAL SIDE STREET  
STOP SIGN INSTALLATION

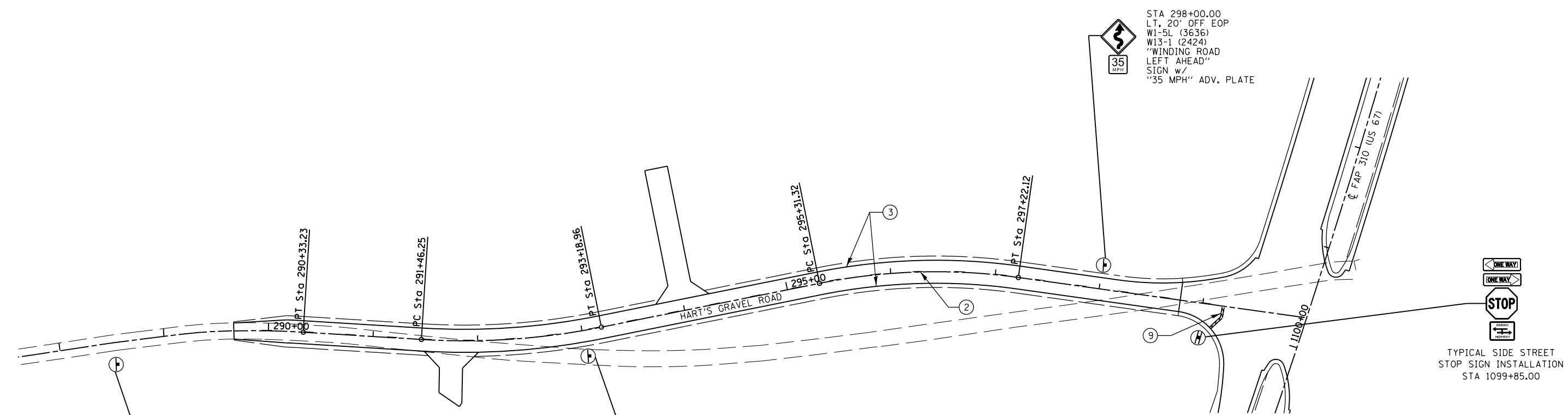
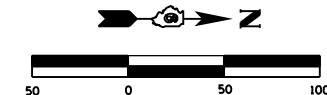
R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
R61R (3612) "ONE WAY RT"  
R1-1 (3636) "STOP"  
R6-3A (2418) "DIVIDED HIGHWAY"  
INSTALL ON 2 TELESCOPING STEEL POST

TYPICAL SIDE STREET  
YIELD SIGN INSTALLATION

R6-1L (3612) "ONE WAY LT"  
BACK TO BACK  
R61R (3612) "ONE WAY RT"  
R1-2 (3636) "YIELD"  
INSTALL ON 2 TELESCOPING STEEL POST

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS ST. PAUL'S CHURCH ROAD</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\idot\sparksgw\10264881\P&P	Sideroads1-14.dgn	DRAWN -	REVISED -		310	69-3	Morgan	793	296				
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.				CONTRACT NO. 72667				
	PLOT DATE = Mar-30-2011 08:49:56AM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								





STA 288+75.00  
RT, 20' OFF EOP  
W1-5R (3636)  
W13-1 (2424)  
"WINDING ROAD  
RIGHT AHEAD"  
SIGN w/  
"35 MPH" ADV. PLATE

STA 293+00.00  
LT, 20' OFF EOP  
W3-1a (3636)  
"STOP AHEAD"

STA 298+00.00  
LT, 20' OFF EOP  
W1-5L (3636)  
W13-1 (2424)  
"WINDING ROAD  
LEFT AHEAD"  
SIGN w/  
"35 MPH" ADV. PLATE

TYPICAL SIDE STREET  
STOP SIGN INSTALLATION  
STA 1099+85.00

- PAVEMENT MARKING LEGEND**
- (A) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY CRYSTAL
  - (B) PR RAISED REFLECTIVE PAVEMENT MARKERS, ONE-WAY AMBER
  - (C) PR RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER
  - (1) PR URETHANE PAVEMENT MARKING- LINE 5" (WHITE SKIP DASH)
  - (2) PR URETHANE PAVEMENT MARKING- LINE 5" (YELLOW SKIP DASH)
  - (3) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID WHITE)
  - (4) PR URETHANE PAVEMENT MARKING- LINE 5" (SOLID YELLOW)
  - (5) PR URETHANE PAVEMENT MARKING- LINE 6" (SOLID WHITE)
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  - (7) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID WHITE)
  - (8) PR URETHANE PAVEMENT MARKING- LINE 12" (SOLID YELLOW)
  - (9) PR PREFORMED PLASTIC PAVEMENT MARKING- LINE 24" (SOLID WHITE)
  - (10) PR PREFORMED PLASTIC PAVEMENT MARKING- LETTERS & SYMBOLS (LARGE ARROWS= 15.6 SF/EACH)

**TYPICAL SIDE STREET STOP SIGN INSTALLATION**

R6-1L (3612) "ONE WAY LT" BACK TO BACK  
 R6-1R (3612) "ONE WAY RT"  
 R1-1 (3636) "STOP"  
 R6-3A (2418) "DIVIDED HIGHWAY"  
 INSTALL ON 2 TELESCOPING STEEL POST

**TYPICAL SIDE STREET YIELD SIGN INSTALLATION**

R6-1L (3612) "ONE WAY LT" BACK TO BACK  
 R6-1R (3612) "ONE WAY RT"  
 R1-2 (3636) "YIELD"  
 INSTALL ON 2 TELESCOPING STEEL POST

FILE NAME =	USER NAME = Sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRIPING &amp; SIGNING PLAN SHEETS HART'S GRAVEL ROAD</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw\work\p\idot\sparksgw\10264881\P&P	Sideroads1-14.dgn	DRAWN -	REVISED -		310	69-3	Morgan	793	297				
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 72667								
	PLOT DATE = Mar-30-2011 08:49:57AM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

