

PROPOSED ROADWAY LEGEND

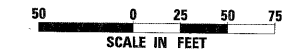
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|----------------------------------|----------------------------------|
| (A) CONT REINF PCC PVT 12 | (M) TOPSOIL EXC & PLAC |
| (B) HMA SC "D" N50 | (N) CONC BAR 2F 42HT |
| (C) HMA BC IL-12.5 N50 | (O) CONC BAR 1F 42HT SPL |
| (D) STABILIZED SUBBASE 4" | (P) MECH ST EARTH RET WL |
| (E) BIT SURF TREAT CL A-3 | (R) SPBGR TY A 6FT POSTS |
| (F) AGG BASE CSE A 8 | (S) PCC PVT 12 JOINTED |
| (G) SUB GRAN MAT A 12 | (T) HMA PAVT FD 13 |
| (H) PCC SHOULDERS 12 | (U) PROCESS MOD SOIL 12 AND LIME |
| (I) PCC SHOULDERS 12 SPL | (W) SUB GRAN MAT C |
| (J) HMA SHOULDERS 10 1/2 | (X) WOV W FENCE 4 |
| (K) SHLDR STABILIZATION (NOTE 1) | (Y) SPBGR TY A 9FT POST |
| (L) AGGREGATE SHLDS A 6 | (AA) SHOULDER RUMBLE STRIP |

NOTE 1: (K) PAID FOR AS (H) OR (J) TO MATCH ADJACENT SHOULDER LICENSE. (GPL)
 NOTE 2: SEE PLAT OF HIGHWAY FOR FULL EXTENT OF GENERAL PURPOSE LICENSE. (GPL)

PROP. CURVE PR-P1-3
 PI STA. =21175+23.95
 $\Delta = 55^\circ 49' 47''$ (RT)
 D =6° 59' 14"
 R =820.00'
 T =434.44'
 L =799.02'
 E =107.98'
 e = 7.96%
 T.R. = SEE SE TABLE
 S.E. RUN = SEE SE TABLE
 P.C. STA. =21170+89.51
 P.T. STA. =21178+88.53
 DESIGN SPEED = 50 MPH

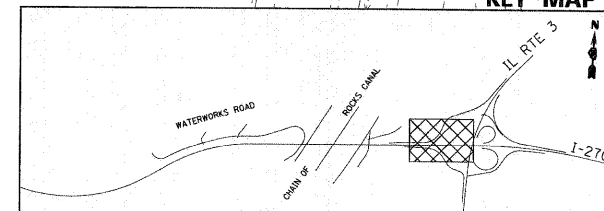
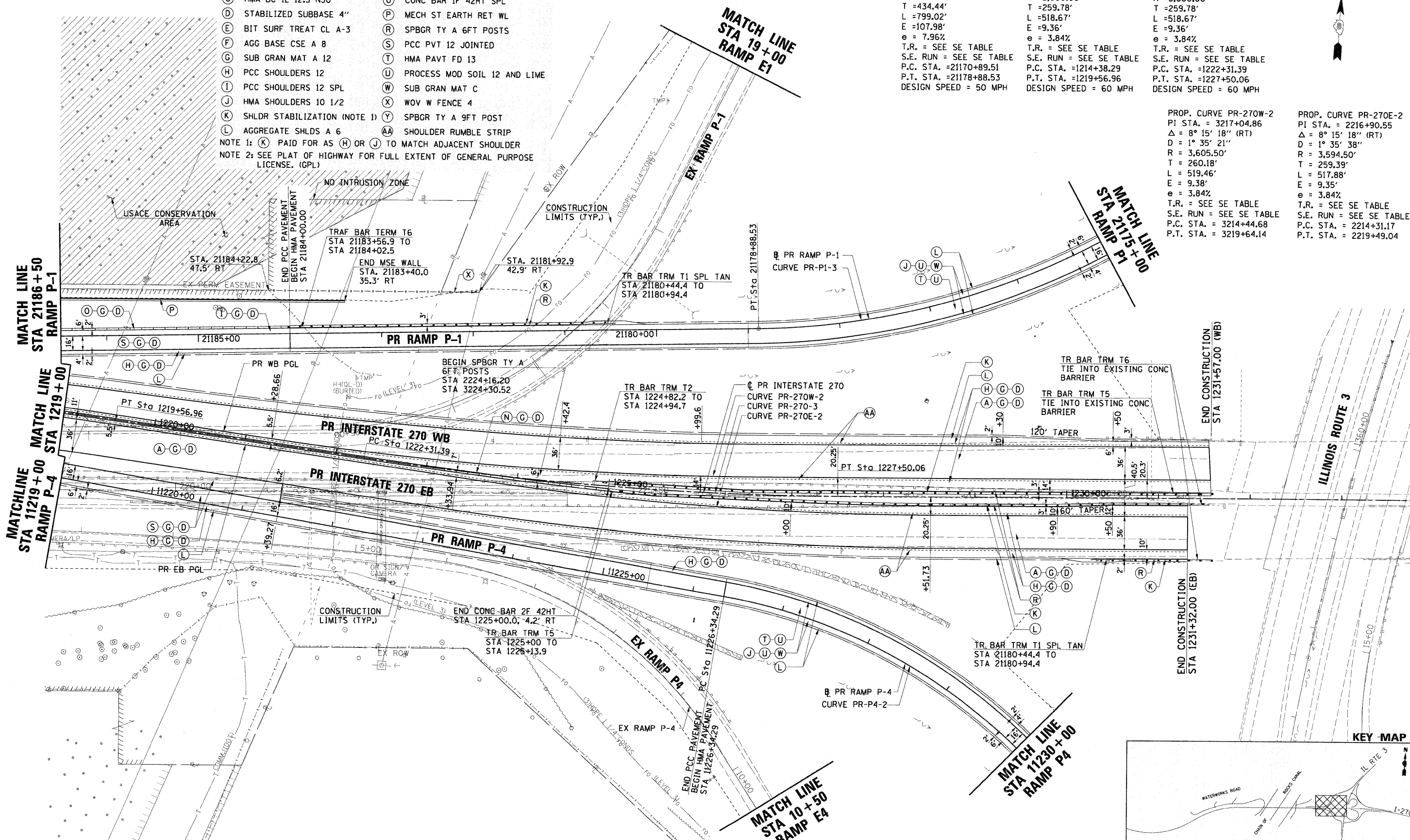
PROP. CURVE PR-270-2
 PI STA. =2126+98.07
 $\Delta = 8^\circ 15' 18''$ (RT)
 D =1° 35' 30"
 R =3,600.00'
 T =259.78'
 L =518.67'
 E =9.36'
 e = 3.84%
 T.R. = SEE SE TABLE
 S.E. RUN = SEE SE TABLE
 P.C. STA. =2124+38.29
 P.T. STA. =2129+56.96
 DESIGN SPEED = 60 MPH

PROP. CURVE PR-270-3
 PI STA. =1224+91.18
 $\Delta = 8^\circ 15' 18''$ (LT)
 D =1° 35' 30"
 R =3,600.00'
 T =259.78'
 L =518.67'
 E =9.36'
 e = 3.84%
 T.R. = SEE SE TABLE
 S.E. RUN = SEE SE TABLE
 P.C. STA. =1222+31.39
 P.T. STA. =1227+50.06
 DESIGN SPEED = 60 MPH



PROP. CURVE PR-270W-2
 PI STA. = 3217+04.86
 $\Delta = 8^\circ 15' 18''$ (RT)
 D = 1° 35' 21"
 R = 3,605.50'
 T = 260.18'
 L = 519.46'
 E = 9.38'
 e = 3.84%
 T.R. = SEE SE TABLE
 S.E. RUN = SEE SE TABLE
 P.C. STA. = 3214+44.68
 P.T. STA. = 3219+64.14

PROP. CURVE PR-270E-2
 PI STA. = 2216+90.55
 $\Delta = 8^\circ 15' 18''$ (RT)
 D = 1° 35' 38"
 R = 3,594.50'
 T = 259.39'
 L = 517.88'
 E = 9.35'
 e = 3.84%
 T.R. = SEE SE TABLE
 S.E. RUN = SEE SE TABLE
 P.C. STA. = 2214+31.17
 P.T. STA. = 2219+49.04



FILE NAME = DB76A91-Sht-Plan06.dgn	USER NAME = malopez	DESIGNED - SJM	REVISED - 01-04-11	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN INTERSTATE 270	F.A.I. RTE. = 270	SECTION = 60-1B-1	COUNTY = MADISON	TOTAL SHEETS = 712	SHEET NO. = 55	
PLOT SCALE = #SCALE#	CHECKED - SJM	REVISED -	SCALE: 1" = 50'			SHEET NO. 6 OF 12 SHEETS	STA. 1219+00 TO STA. 1233+00	CONTRACT NO. 76A91		ILLINOIS FED. AID PROJECT	
PLOT DATE = 3/17/2011	DATE = 06-30-10	REVISED -									