

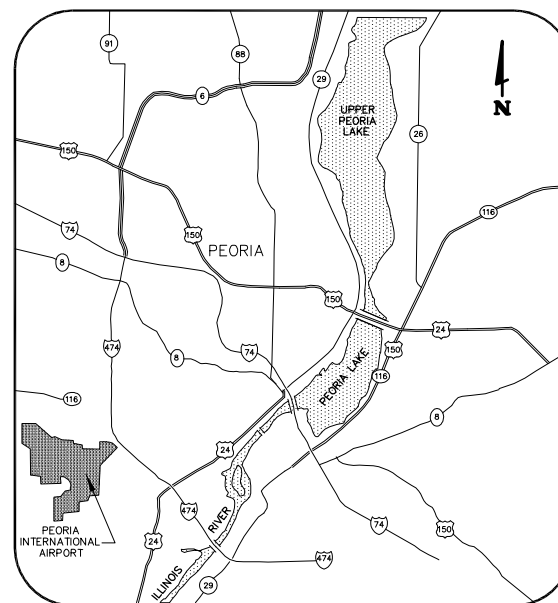
**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**

A.I.P. PROJECT NO.: 3-17-0080-XX  
 ILLINOIS PROJECT NO.: PIA-4079

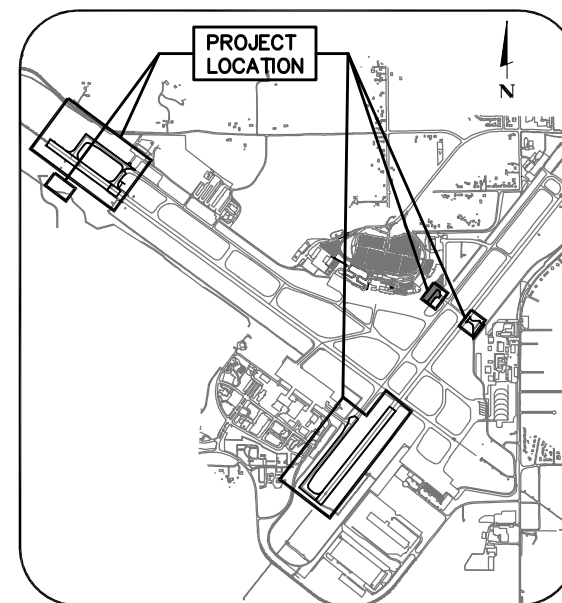
**REHABILITATE NORTHWEST AND**  
**SOUTHWEST QUADRANT PAVEMENTS**

SCOPE TO INCLUDE: MILL AND OVERLAY THE NORTHWEST 1,500' OF RUNWAY 13/31 INCLUDING MARKING, GROOVING AND REMOVAL OF THE NORTHWEST BAK PAVEMENTS; MILL AND OVERLAY TAXIWAY E; MILL AND OVERLAY THE SOUTHWEST 2,000' OF RUNWAY 4/22 INCLUDING GROOVING AND MARKING; MILL AND OVERLAY TAXIWAY A; RECONSTRUCT TAXIWAY A-4 ADJACENT TO GENERAL AVIATION RAMP; REMOVE TAXIWAY A-4 EAST OF RUNWAY 4/22; INSTALL DUCT AND CABLE FROM TERMINAL TO ILANG FOR FIRE ALARM SYSTEM

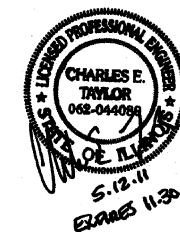
MAY 13, 2011



LOCATION MAP



SITE PLAN



**811** Know what's below. Call before you dig. COMMON GROUND ALLIANCE  
 www.call811.com or Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

**CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.**

**DESIGN INFORMATION**

GEOMETRIC CRITERIA  
 AIRPLANE DESIGN GROUP IV  
 AIRCRAFT APPROACH CATEGORY D  
 CRITICAL AIRCRAFT DC-8-63

PAVEMENT DESIGN CRITERIA  
 155,000 LB. AIRCRAFT WITH 8,417 ANNUAL DEPARTURES  
 210,000 LB. AIRCRAFT WITH 2,339 ANNUAL DEPARTURES  
 336,000 LB. AIRCRAFT WITH 404 ANNUAL DEPARTURES

MAXIMUM EQUIPMENT HEIGHT = 25'  
 GROUND FREQUENCY 121.85

GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT

TOWNSHIP: 8 NORTH  
 RANGE: 7 EAST  
 COUNTY: PEORIA  
 TOWNSHIP: LIMESTONE

GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT

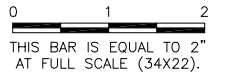
APPROVED: *Charles E. Taylor*  
 DATE: May 12, 2011

**CMT**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613

SUBMITTED BY: *W.S. Jr.*  
 DATE: 5-12-11  
 CM&T JOB NUMBER 10061-03-00

**PE092**

| REVISIONS |    |      |
|-----------|----|------|
| NUMBER    | BY | DATE |
|           |    |      |
|           |    |      |
|           |    |      |



**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 INDEX TO SHEETS - SUMMARY OF QUANTITIES**

**CMT**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613

|   |              |
|---|--------------|
| DESIGN BY:  | TJH          |
| DRAWN BY:   | CMT          |
| CHECKED BY:   | CET          |
| APPROVED BY:  | CET          |
| DATE:   | MAY 13, 2011 |
| JOB No:   | 100610300    |
| IL PROJ. NO. PIA-4079<br>AIP PROJ. NO. 3-17-0080-XX |              |
| SHEET 02 OF 84 SHEETS                               |              |

| ITEM NO. | ITEM DESCRIPTION                       | UNIT | QUANT  |
|----------|--|------|--------|
| AR108158 | 1/C #8 5KV UG CABLE IN UNIT DUCT       | L.F. | 2305   |
| AR108806 | 6 PAIR CONTROL CABLE                   | L.F. | 4750   |
| AR110014 | 4" DIRECTIONAL BORE                    | L.F. | 315    |
| AR110501 | 1-WAY CONCRETE ENCASED DUCT            | L.F. | 1800   |
| AR110610 | ELECTRICAL HANDHOLE                    | EACH | 2      |
| AR125444 | TAXI GUIDANCE SIGN, 4 CHARACTER        | EACH | 2      |
| AR125445 | TAXI GUIDANCE SIGN, 5 CHARACTER        | EACH | 3      |
| AR125447 | TAXI GUIDANCE SIGN, 7 CHARACTER        | EACH | 3      |
| AR125415 | MITL, BASE MOUNTED                     | EACH | 1      |
| AR125515 | HIRL, BASE MOUNTED                     | EACH | 5      |
| AR125902 | REMOVE BASE MOUNTED LIGHT              | EACH | 6      |
| AR125904 | REMOVE TAXI GUIDANCE SIGN              | EACH | 10     |
| AR150540 | HAUL ROUTE                             | L.S. | 1      |
| AR152411 | UNCLASSIFIED EXCAVATION                | L.S. | 1      |
| AR152480 | SHOULDER ADJUSTMENT                    | S.Y. | 18285  |
| AR156510 | SILT FENCE                             | L.F. | 2000   |
| AR156520 | INLET PROTECTION                       | EACH | 2      |
| AR201670 | CRACK CONTROL FABRIC                   | S.Y. | 3740   |
| AR208540 | OVERSIZE AGGREGATE                     | TON  | 5150   |
| AR209600 | GEOTEXTILE FABRIC                      | S.Y. | 3850   |
| AR209606 | CRUSHED AGG. BASE COURSE - 6"          | S.Y. | 3850   |
| AR401610 | BITUMINOUS SURFACE COURSE              | TON  | 23255  |
| AR401630 | BITUMINOUS SURFACE TEST SECTION        | EACH | 1      |
| AR401640 | BITUMINOUS PAVEMENT GROOVING           | S.Y. | 58955  |
| AR401650 | BITUMINOUS PAVEMENT MILLING            | S.Y. | 106840 |
| AR401910 | REMOVE AND REPLACE BITUMINOUS PAVEMENT | S.Y. | 25     |
| AR401921 | REMOVE PAVEMENT                        | S.Y. | 6000   |
| AR403610 | BITUMINOUS BASE COURSE                 | TON  | 8875   |
| AR403630 | BITUMINOUS BASE COURSE TEST SECTION    | EACH | 1      |
| AR501550 | PCC PAVEMENT MILLING                   | S.Y. | 2840   |
| AR501900 | REMOVE PCC PAVEMENT                    | S.Y. | 4300   |
| AR510900 | REMOVE TIE DOWN                        | EACH | 17     |
| AR510905 | REMOVE GROUND ROD                      | EACH | 5      |
| AR602510 | BITUMINOUS PRIME COAT                  | GAL  | 1540   |
| AR603510 | BITUMINOUS TACK COAT                   | GAL  | 27345  |
| AR620510 | PAVEMENT MARKING                       | S.F. | 299300 |
| AR620900 | PAVEMENT MARKING REMOVAL               | S.F. | 825    |
| AR701212 | 12" CMP                                | L.F. | 120    |
| AR705524 | 4" PERFORATED UNDERDRAIN W/SOCK        | L.F. | 1650   |
| AR705640 | UNDERDRAIN CLEANOUT                    | EACH | 2      |
| AR751410 | INLET                                  | EACH | 1      |
| AR751900 | REMOVE INLET                           | EACH | 1      |
| AR752212 | METAL END SECTION - 12"                | EACH | 1      |
| AR800273 | 6-STRAND FIBER OPTIC DATA CABLE        | L.F. | 4750   |
| AR800372 | REMOVE BITUMINOUS AND PCC PAVEMENT     | S.Y. | 3750   |
| AR801208 | BAK REMOVAL                            | LS   | 1      |
| AR801211 | REMOVE ROADWAY PAVEMENT                | SY   | 800    |
| AR801212 | REMOTE FIRE ALARM ANNUNCIATOR PANEL    | LS   | 1      |
| AR801954 | L-893 LIGHTED RUNWAY CLOSURE MARKER    | EACH | 2      |
| AR901510 | SEEDING                                | ACRE | 8      |
| AR904510 | SODDING                                | S.Y. | 18540  |
| AR908510 | MULCHING                               | ACRE | 6      |
| AR908520 | EXCELSIOR BLANKET                      | S.Y. | 7420   |

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| 05              | CONSTRUCTION ACTIVITY PLAN 1C                           |
| 06              | CONSTRUCTION ACTIVITY PLAN 2                            |
| 07              | CONSTRUCTION ACTIVITY PLAN 3                            |
| 08              | CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS            |
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| 10              | EXISTING CONDITIONS AND REMOVALS PLAN 2                 |
| 11              | EXISTING CONDITIONS AND REMOVALS PLAN 3                 |
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| 13              | EXISTING PAVEMENT STRUCTURE 2                           |
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| 18              | PAVEMENT REMOVAL PLAN 4                                 |
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| 21              | BAK REMOVAL DETAILS 3                                   |
| 22              | BAK REMOVAL DETAILS 4                                   |
| 23              | BAK REMOVAL DETAILS 5                                   |
| 24              | BAK REMOVAL DETAILS 6                                   |
| 25              | BAK REMOVAL DETAILS 7                                   |
| 26              | BAK REMOVAL DETAILS 8                                   |
| 27              | BAK REMOVAL DETAILS 9                                   |
| 28              | BAK REMOVAL DETAILS 10                                  |
| 29              | BAK REMOVAL DETAILS 11                                  |
| 30              | BAK REMOVAL DETAILS 12                                  |
| 31              | BAK REMOVAL DETAILS 13                                  |
| 32              | PROPOSED IMPROVEMENTS 1                                 |
| 33              | PROPOSED IMPROVEMENTS 2                                 |
| 34              | PROPOSED IMPROVEMENTS 3                                 |
| 35              | TYPICAL SECTIONS 1                                      |
| 36              | TYPICAL SECTIONS 2                                      |
| 37              | TYPICAL SECTIONS 3                                      |
| 38              | RUNWAY 13 PLAN AND PROFILE STA. 500+00 TO STA. 511+00   |
| 39              | RUNWAY 13 PLAN AND PROFILE STA. 511+00 TO STA. 516+75   |
| 40              | TAXIWAY E PLAN AND PROFILE STA. 1505+00 TO STA. 1516+50 |
| 41              | TAXIWAY E7 PLAN AND PROFILE STA. 0+75 TO STA. 5+38      |

| INDEX OF SHEETS |   |
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| 43              | RUNWAY 4 PLAN AND PROFILE SHEET STA. 500+00 TO STA. 511+00    |
| 44              | RUNWAY 4 PLAN AND PROFILE SHEET STA. 511+00 TO STA. 518+67    |
| 45              | TAXIWAY A6 PLAN AND PROFILE SHEET STA. 0+75 TO STA. 4+50      |
| 46              | TAXIWAY A PLAN AND PROFILE STA. 1500+00 TO STA. 1512+00       |
| 47              | TAXIWAY A PLAN AND PROFILE SHEET STA. 1512+00 TO STA. 1516+90 |
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| 50              | STAKING PLAN 3  |
| 51              | STAKING PLAN 4  |
| 52              | STAKING PLAN 5  |
| 53              | GROOVING PLAN AND DETAILS                                     |
| 54              | PAVING AND MISCELLANEOUS DETAILS 1                            |
| 55              | FIRE ALARM AND CRASH PHONE CABLING PLAN                       |
| 56              | FIRE ALARM AND CRASH PHONE DETAILS                            |
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| 58              | ELECTRICAL SIGN PLAN 1  |
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| 60              | ELECTRICAL DETAILS SHEET 1                                    |
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| 66              | MARKING PLAN 4  |
| 67              | MARKING PLAN 5  |
| 68              | MARKING PLAN 6  |
| 69              | MARKING DETAILS 1   |
| 70              | MARKING DETAILS 2   |
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| 72              | TURFING PLAN 1  |
| 73              | TURFING PLAN 2  |
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| 75              | TURFING PLAN 4  |
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| 78              | GRADING AND STABILIZATION PLAN 1                              |
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| 80              | INLET DETAILS   |
| 81              | DRAINAGE DETAILS  |
| 82              | UNDERDRAIN DETAILS  |
| 83              | FENCE DETAILS 1   |
| 84              | EROSION CONTROL DETAILS 1                                     |

| ESTIMATED EARTHWORK QUANTITIES |  |                  |  |   |
|--------------------------------|--|------------------|--|---|
|                                | UNDERCUT FOR FULL DEPTH RUNWAY REMOVAL | BAK REMOVAL AREA | ROADWAY PAVEMENT REMOVAL AND GRADING INCLUDING DETENTION BASIN | TAXIWAY A4 GRADING INCLUDING GRADING FOR NEW FLARED END SECTION |
| CUT                            | 3,650 C.Y.                             | --               | 700 C.Y.   | 240 C.Y.  |
| FILL                           | --                                     | 700 C.Y.         | 200 C.Y.   | 1,370 C.Y.  |



| CONTROL POINTS |                  |               |               |           |
|----------------|------------------|---------------|---------------|-----------|
| POINT ID       | FIELD OBJECT     | NORTHING      | EASTING       | ELEVATION |
| A              | NGS MON. (PIA C) | 1,458,146.116 | 2,423,579.842 | 617.76'   |
| B              | IPIN             | 1,458,709.670 | 2,423,121,160 | 622.77'   |
| C              | PK NAIL          | 1,459,060.312 | 2,422,642.140 | 627.60'   |
| D              | IPIN             | 1,459,479.852 | 2,422,891.205 | 627.08'   |
| E              | IPIN             | 1,459,180.667 | 2,423,845.613 | 623.32'   |
| F              | IPIN             | 1,456,755.533 | 2,430,100.731 | 650.78'   |
| G              | IPIN             | 1,456,454.176 | 2,429,858.994 | 650.85'   |
| H              | NGS MON. (PIA A) | 1,456,245.241 | 2,430,782.054 | ---       |
| I              | IPIN             | 1,455,870.070 | 2,430,582.725 | 652.99'   |
| J              | IPIN             | 1,455,648.387 | 2,430,583.472 | 651.76'   |
| K              | IPIN             | 1,453,900.299 | 2,428,465.564 | 642.72'   |
| L              | IPIN             | 1,452,803.227 | 2,427,596.218 | 641.09'   |
| M              | IPIN             | 1,453,343.232 | 2,427,558.316 | 639.88'   |
| N              | IPIN             | 1,453,937.503 | 2,428,003.422 | 639.93'   |

**LEGEND**

- AREA OF CONSTRUCTION
- CONTRACTOR'S STAGING AND STORAGE AND PARKING AREA
- FAA SERVICE ROADWAYS
- CONTRACTOR'S ACCESS
- CONTROL POINT
- CRITICAL POINT

CONTRACTOR'S STAGING, STORAGE & PARKING  
 CONTRACTOR'S ACCESS THROUGH NEW GATE

CONTRACTOR'S ACCESS THROUGH EXISTING GATE

CONTRACTOR'S ACCESS THROUGH EXISTING GATE

CONTRACTOR'S STAGING, STORAGE & PARKING

CONTRACTOR'S STAGING & STORAGE

PRIMARY BORROW AREA

SEE NOTE 14

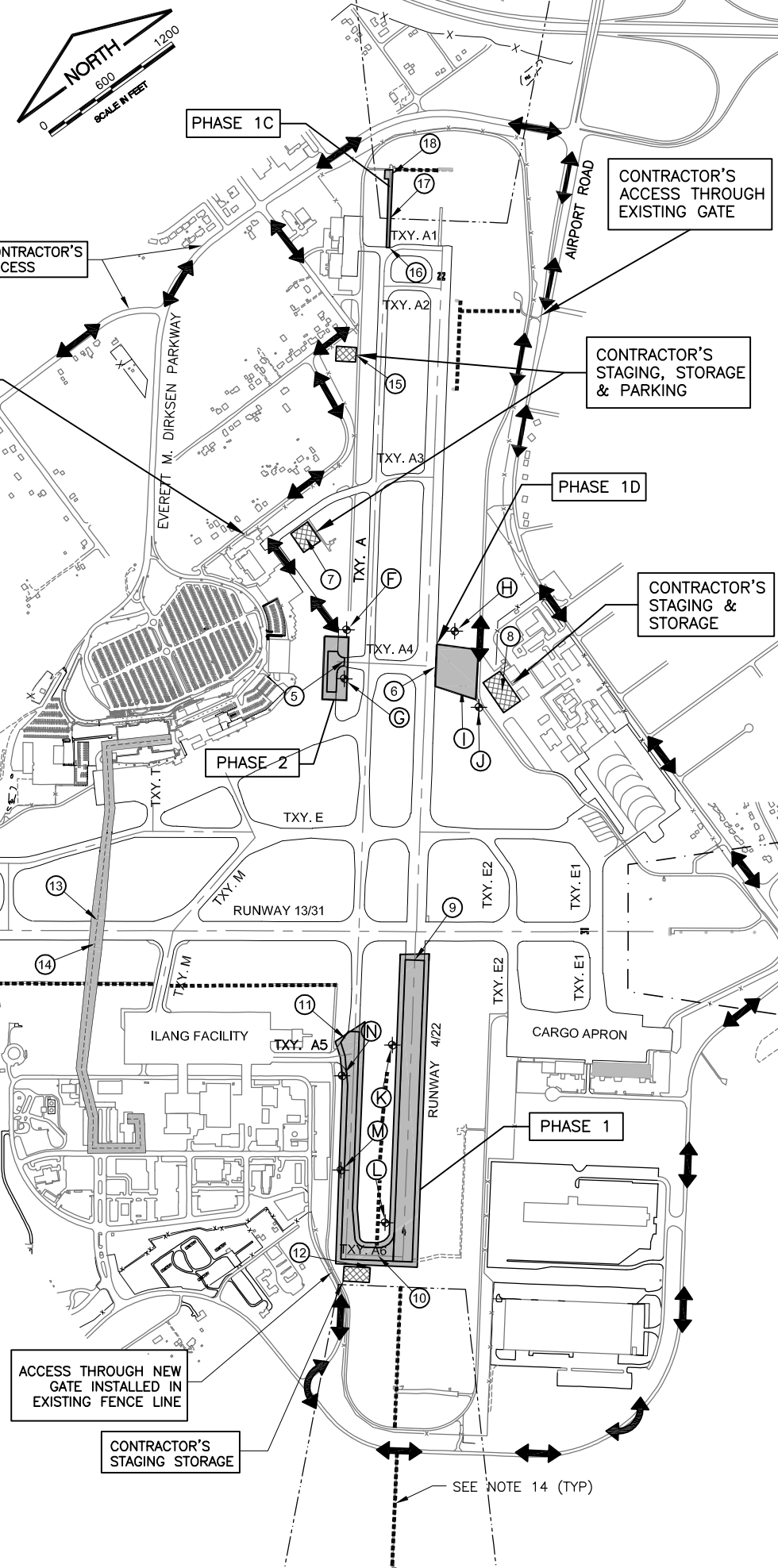
SEE NOTE 14 (TYP)

SEE NOTE 14 (TYP)

| CRITICAL POINTS |              |              |                  |                    |                   |
|-----------------|--------------|--------------|------------------|--------------------|-------------------|
| POINT ID        | LATITUDE     | LONGITUDE    | GROUND ELEVATION | MAX. EQUIP. HEIGHT | OVERALL ELEVATION |
| 1               | 40°40'14.17" | 89°42'30.06" | 623.25'          | 25'                | 648.80'           |
| 2               | 40°40'24.49" | 89°42'41.88" | 629.20'          | 25'                | 654.20'           |
| 3               | 40°40'18.64" | 89°42'31.49" | 620.40'          | 25'                | 645.40'           |
| 4               | 40°40'24.16" | 89°42'19.26" | 650.00'          | 25'                | 675.00'           |
| 5               | 40°39'54.77" | 89°41'09.44" | 651.75'          | 25'                | 676.75'           |
| 6               | 40°39'50.24" | 89°41'02.22" | 656.75'          | 25'                | 681.75'           |
| 7               | 40°40'04.98" | 89°41'04.55" | 656.00'          | 25'                | 681.00'           |
| 8               | 40°39'46.31" | 89°40'58.14" | 652.00'          | 25'                | 677.00'           |
| 9               | 40°39'32.85" | 89°41'21.73" | 648.50'          | 25'                | 670.90'           |
| 10              | 40°39'16.76" | 89°41'43.63" | 639.00'          | 25'                | 644.00'           |
| 11              | 40°39'31.45" | 89°41'31.51" | 642.50'          | 25'                | 667.50'           |
| 12              | 40°39'15.68" | 89°41'44.80" | 636.00'          | 25'                | 661.00'           |
| 13              | 40°39'50.09" | 89°41'45.29" | 648.00'          | 25'                | 673.00'           |
| 14              | 40°39'48.70" | 89°41'46.97" | 645.00'          | 25'                | 670.00'           |
| 15              | 40°40'13.49" | 89°40'49.82" | 650.00'          | 25'                | 675.00'           |
| 16              | 40°40'18.70" | 89°40'40.63" | 654.00'          | 25'                | 679.00'           |
| 17              | 40°40'20.45" | 89°40'38.81" | 650.00'          | 25'                | 675.00'           |
| 19              | 40°40'23.19" | 89°40'35.91" | 642.00'          | 25'                | 667.00'           |

ACCESS THROUGH NEW GATE INSTALLED IN EXISTING FENCE LINE

CONTRACTOR'S STAGING STORAGE



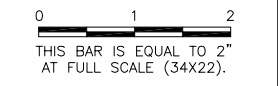
**GENERAL NOTES**

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS, AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF AN ESCORT OR A FLAGMAN IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED MAAP/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT OPERATIONS DIRECTOR.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
- EXCESS EARTH, BROKEN ASPHALT AND CONCRETE SHALL BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 130' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
- ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
- THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC.
- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- SEE CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS SHEET FOR NOTES REGARDING THE CONTRACTOR'S ACCESS.
- CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE LIGHTED, MARKED, AND FLAGGED PER SECTION 50-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
- COST OF TEMPORARY EDGE LIGHTING AND CABLING IN ORDER TO MAINTAIN AIRFIELD CIRCUITS WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL REMOVE BITUMINOUS MILLINGS FROM AIRPORT PROPERTY AND DISPOSE OF MILLINGS IN ACCORDANCE WITH FEDERAL, STATE AND LOCAL REQUIREMENTS. ANY SALVAGE VALUE RESULTING FROM SALE OF THE MILLINGS SHALL BE CREDITED TOWARD THE PRICE BID FOR THE WORK. AT THE AIRPORT'S OPTION, THE CONTRACTOR MAY IN LIEU OF HAULING THE MILLINGS OFF OF THE AIRPORT PROPERTY, THE MILLINGS MAY BE SPREAD, GRADED AND COMPACTED ON EXISTING AGGREGATE ROADWAYS LOCATED ON AIRPORT PROPERTY. THE MAJORITY OF THESE ROADWAYS ARE UTILIZED BY THE FAA IN SUPPORT OF THEIR FACILITIES ON THE PROPERTY. THE AIRPORT SHALL COORDINATE THE PLACEMENT OF THE MILLINGS WITH THE FAA. THE CONTRACTOR SHALL PROVIDE SPREADING AND COMPACTION EQUIPMENT FOR THE PLACEMENT OF THESE MILLING. THE WORK SHALL RESULT IN A SMOOTH, FUNCTIONAL ROADWAY WHICH DOES NOT DISTURB EXISTING DRAINAGE PATTERNS. ANY WORK NOT COMPLETED TO THE SATISFACTION OF THE LOCAL FAA REPRESENTATIVE SHALL BE REMOVED AND THE AREA RESTORED TO ITS ORIGINAL CONDITION.
- THE PRIMARY BORROW AREA SHALL BE AS SHOWN ON THIS SHEET. ACCESS TO THIS AREA SHALL BE LIMITED TO THE TIMES WHERE THE RUNWAY 13 GLIDESLOPE IS NOT ACTIVE. THE CONTRACTOR SHALL OBTAIN BORROW FILL MATERIAL FROM THE LAND MASS THAT HAS EXPERIENCED SLOPE FAILURE. THE CONTRACTOR SHALL ONLY USE MATERIAL FROM THIS AREA THAT HAS EXPERIENCE SLOPE FAILURE AND HAS DETACHED FROM THE EXISTING HILL. THE CONTRACTOR SHALL NOT REMOVE UNDISTURBED MATERIAL FROM THIS AREA. WHEN OBTAINING THE MATERIAL, THE CONTRACTOR SHALL USE CAUTION NOT TO STEEPEN THE SLOPE AND CAUSE FURTHER INSTABILITY. THE PRIMARY BORROW AREA SHALL BE SEED AND THEN MULCHED USING AN EXCELSIOR BLANKET FABRIC TO PREVENT EROSION, ADDITIONAL QUANTITY HAS BEEN INCLUDED IN THE CONTRACT FOR THESE ITEMS. SILT FENCE SHALL BE PLACED ALONG THE EXISTING FENCE LINE FOR A LENGTH OF 300'. SEEDING, MULCHING, AND SILT FENCE SHALL BE PAID FOR UNDER THEIR RESPECTIVE PAY ITEMS. ALL COSTS TO EXCAVATE, HAUL MATERIAL, COMPACT, AND RESHAPE THE BORROW AREA SHALL BE CONSIDERED PART OF THE LUMP SUM PAYMENT FOR THE UNCLASSIFIED EXCAVATION PAY ITEM.
- THE SECONDARY BORROW AREA SHALL BE IN THE LOCATION OF THE CONTRACTOR'S ACCESS FOR PHASE 3 AS SHOWN ON THIS SHEET. ALL COSTS TO EXCAVATE, HAUL MATERIAL, COMPACT, AND RESHAPE THE SECONDARY BORROW AREA SHALL BE CONSIDERED PART OF THE LUMP SUM PAYMENT FOR THE UNCLASSIFIED EXCAVATION PAY ITEM. SEEDING AND MULCHING FOR THE SECONDARY BORROW AREA SHALL BE CONSIDERED INCIDENTAL TO THE HAUL ROUTE PAY ITEM AND SHALL BE COMPLETED ALONG WITH THE RESTORATION OF THE HAUL ROUTE AREA.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITIES PRIOR TO COMPLETING EXCAVATION OR TRENCHING ITEMS. ANY UTILITIES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED IMMEDIATELY BY THE CONTRACTOR AND AT HIS/HER OWN COST.

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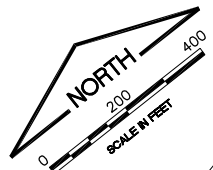
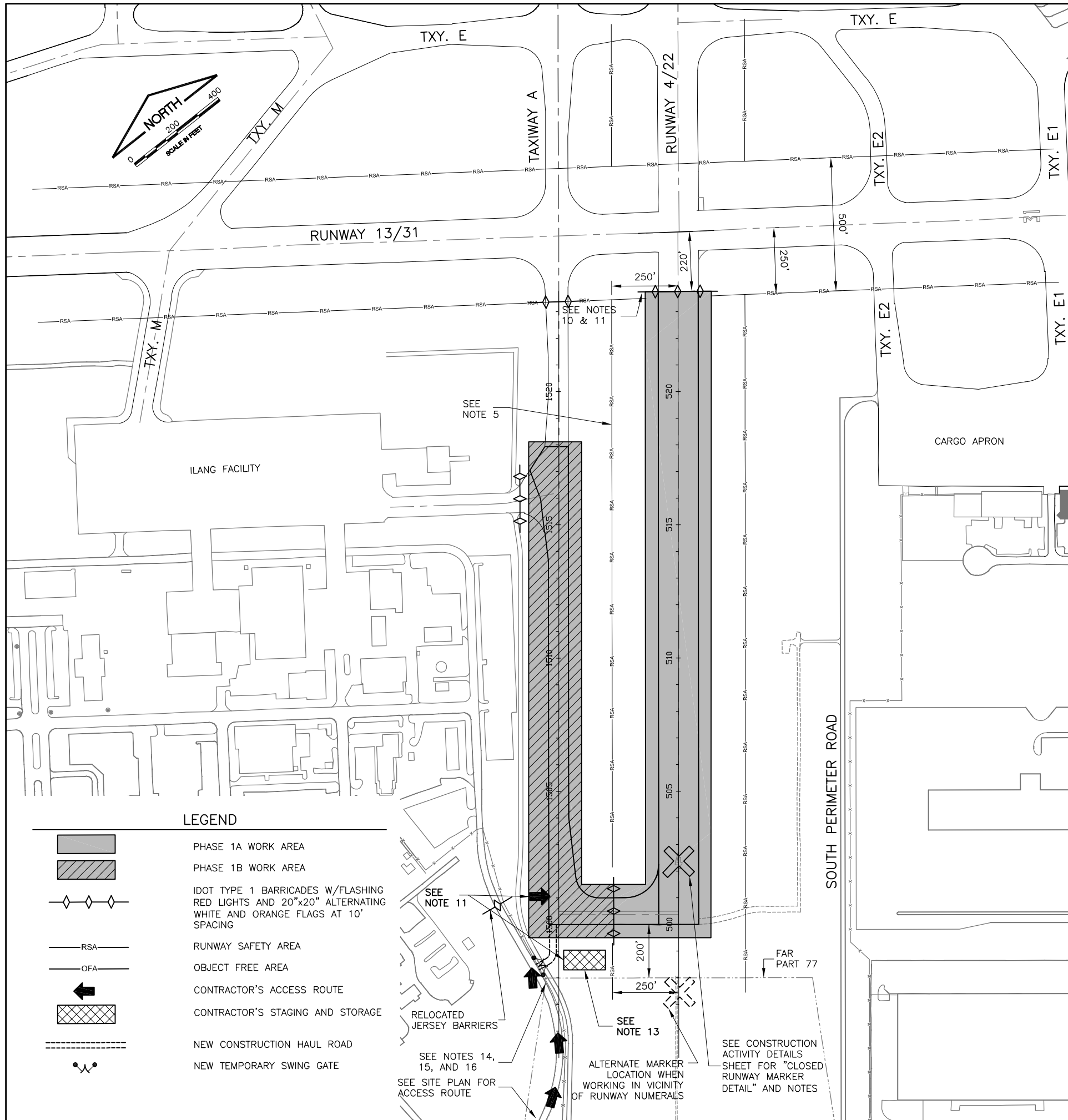
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GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS

REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 SITE PLAN

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| APPROVED BY:               | CET          |
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**LEGEND**

- PHASE 1A WORK AREA
- PHASE 1B WORK AREA
- IDOT TYPE 1 BARRICADES W/FLASHING RED LIGHTS AND 20"x20" ALTERNATING WHITE AND ORANGE FLAGS AT 10' SPACING
- RUNWAY SAFETY AREA
- OBJECT FREE AREA
- CONTRACTOR'S ACCESS ROUTE
- CONTRACTOR'S STAGING AND STORAGE
- NEW CONSTRUCTION HAUL ROAD
- NEW TEMPORARY SWING GATE

**SEQUENCE OF CONSTRUCTION NOTES**

1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON THIS PHASE. PHASE 1D IS SHOWN ON CONSTRUCTION ACTIVITY PLAN 2.
2. THE AIRPORT WILL REQUIRE 14 DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING WORK IN PHASE 1A AND 1B TO ISSUE ALL APPROPRIATE NOTAM'S.
3. THE CONTRACTOR SHALL EXPEDITE WORK WITHIN 250' OF THE RUNWAY CENTERLINE. UPON COMPLETION OF THE PAVING FOR THE RUNWAY WORK, THE SHOULDER SHALL BE INSTALLED AND THE PAVEMENT MARKING COMPLETED. AIRPORT OPERATIONS AND THE ENGINEER SHALL REVIEW THE CONDITION OF THE CONSTRUCTION AREA AND IF FOUND TO BE IN CONFORMANCE WITH STANDARDS, THE RUNWAY SHALL BE OPENED. FOLLOWING THE SPECIFIED CURE TIME, THE RUNWAY SHALL BE CLOSED AGAIN FOR COMPLETION OF THE RUNWAY GROOVING AND FINAL PAVEMENT MARKING. THIS WORK SHALL BE CLOSELY COORDINATED WITH THE WORK WHICH REQUIRES THE CLOSURE OF RUNWAY 13/31. AT NO TIME SHALL BOTH RUNWAYS OR ILS SYSTEMS ON MULTIPLE RUNWAYS BE CLOSED AT THE SAME TIME.
4. AFTER THE COMPLETION OF PHASE 1A, RUNWAY 4/22 SHALL BE RE-OPENED. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT ALL WORK REQUIRING THAT THE RUNWAY BE TEMPORARILY CLOSED. AFTER PHASE 1A, RUNWAY 4/22 SHALL BE OPENED AT THE END OF EACH WORKDAY WHEN WORK ON ADJACENT PAVEMENTS REQUIRE TEMPORARY RUNWAY CLOSURES.
5. INSTALL LATHE LINE/CONSTRUCTION SET BACK LINE ALONG THE RUNWAY 4/22 RSA FOR THE FULL LENGTH OF THE PHASE 1 WORK AREA PLUS 500' BEYOND EACH END.
6. WORK IN PHASES 1A AND 1B MAY BE COMPLETED AT THE SAME TIME.
7. AT NO TIME WILL THE CONTRACTOR BE ALLOWED TO CLOSE BOTH RUNWAY 4/22 AND RUNWAY 13/31 AT THE SAME TIME.
8. CONTRACTOR'S ACCESS FOR THIS PHASE WILL BE FROM WEST SMITHVILLE ROAD TO EAST LOOP DRIVE. THE CONTRACTOR SHALL RELOCATE THE JERSEY BARRIERS AFTER COORDINATING WITH THE ILANG. BARRIERS SHALL BE PLACED BACK IN THEIR ORIGINAL POSITION AFTER THIS PHASE IS COMPLETED. SEE SITE PLAN FOR ACCESS ROUTE.
9. THE CONTRACTOR SHALL DETERMINE IF THE EXISTING PAVEMENT STRUCTURES FOR THE HAUL ROUTE IS SUFFICIENT FOR THE CONSTRUCTION EQUIPMENT. IF THE EXISTING STRUCTURES ARE DETERMINED NOT TO BE SUFFICIENT, THE CONTRACTOR SHALL UTILIZE THE TURF AREA AS A HAUL/ACCESS ROUTE. THIS ALTERNATE ROUTE SHALL BE CONSTRUCTED AND MAINTAINED AT THE CONTRACTOR'S EXPENSE AND SHALL BE RETURNED TO IT'S ORIGINAL CONDITION AT THE COMPLETION OF THE PROJECT. THE COST TO RESTORE THE ALTERNATE ROUTE SHALL BE AT THE CONTRACTOR'S EXPENSE AND SHALL BE TO THE SATISFACTION OF THE RESIDENT ENGINEER. NO EXTENSION TO CALENDAR DAYS WILL BE GIVEN FOR LACK OF ADEQUATE ACCESS DUE TO WEATHER OR ANY OTHER CONDITIONS. EXISTING PAVEMENTS/ROUTES SAID TO HAVE BEEN DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND AT NO ADDITIONAL COST TO THE CONTRACT.
10. THE CONTRACTOR SHALL REMAIN CLEAR OF THE RUNWAY 13/31 SAFETY AREA AND AT NO TIME SHALL CONSTRUCTION VEHICLES DRIVE ON RUNWAY 13/31. CONSTRUCTION OPERATIONS MAY OPERATE UP TO 200' OF THE RUNWAY CENTERLINE IN CLOSE COORDINATION WITH THE AIR TRAFFIC CONTROL TOWER. THE CONTRACTOR SHALL PLAN OPERATION SUCH THAT CONSTRUCTION VEHICLES ENTER AND EXIT THROUGH THE TEMPORARY GATE SHOWN USING THE ONLY THE CONSTRUCTION AREA. THE CONTRACTOR SHALL NOT BE ALLOW TO CLOSE RUNWAY 13/31 FOR ANY REASON.
11. FOR CONSTRUCTION OPERATIONS SUCH AS HAUL ROUTES AND/OR CONSTRUCTION STAGING AND STORAGE, ALL EXISTING AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COSTS TO RESTORE THESE AREAS SHALL BE CONSIDERED INCIDENTAL TO THE HAUL ROUTE ITEM.
12. SEE CONSTRUCTION ACTIVITY DETAILS FOR BASELINE INFORMATION.
13. CONTRACTOR'S STAGING AND STORAGE AREA SHALL REMAIN CLEAR OF THE RUNWAY AND TAXIWAY SAFETY AREAS AT ALL TIME. CONTRACTOR SHALL ALSO STORE CONSTRUCTION EQUIPMENT IN SUCH A MANNER SO AS NOT TO PENETRATE FAR PART 77 SURFACES.
14. THE CONTRACTOR SHALL CONSTRUCT A NEW HAUL ROUTE IN THIS AREA FOR CONSTRUCTION ACTIVITY USING MILLINGS FROM THE PROPOSED CONSTRUCTION OPERATIONS. ANY PAVEMENT DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED BY THE CONTRACTOR AT HIS/HER OWN COST.
15. AT THE END OF CONSTRUCTION, THE HAUL ROUTE SHALL BE COMPLETELY REMOVED AND THE AREA SHALL BE RESTORED TO ITS ORIGINAL CONDITIONS. ALL COSTS TO REMOVE AND RESTORE SHALL BE CONSIDERED INCIDENTAL TO HAUL ROAD CONSTRUCTION.
16. A TEMPORARY GATE SHALL BE INSTALLED IN THE FENCELINE AT THE LOCATION SHOWN OR AS DIRECTED BY THE RESIDENT ENGINEER. ALL ITEMS ASSOCIATED WITH THE REMOVAL OF THE EXISTING FENCE, INSTALLATION OF THE TEMPORARY GATE, FENCE MAINTENANCE, AND RESTORATION OF THE FENCE AT THE END OF THE PROJECT SHALL BE CONSIDERED PART OF THE HAUL ROUTE PAY ITEM. CONSTRUCTION OF NEW FENCE TO RESTORE TO THE ORIGINAL CONDITIONS SHALL MEET THE REQUIREMENTS OF ITEM 162 FOR 10' CLASS E FENCE.
17. AT THE END OF THE CONSTRUCTION PHASE, ANY EXISTING PAVEMENTS DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED BY THE CONTRACTOR AT HIS/HER OWN COST.
18. IT IS IN THE BEST INTEREST OF THE AIRPORT TO COMPLETE THIS PHASE OF WORK FIRST. ANY CONCERNS REGARDING THE CONSTRUCTION PHASING SHALL BE DISCUSSED DURING THE PRE-CONSTRUCTION MEETING AND PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SHOULD NO QUESTIONS OR CONCERNS BE RAISED REGARDING THE CONSTRUCTION PHASING, THE ITEMS SHOWN ON THIS SHEET SHALL BE THE INITIAL PRIORITY.

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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

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**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**

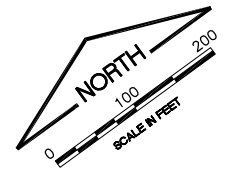
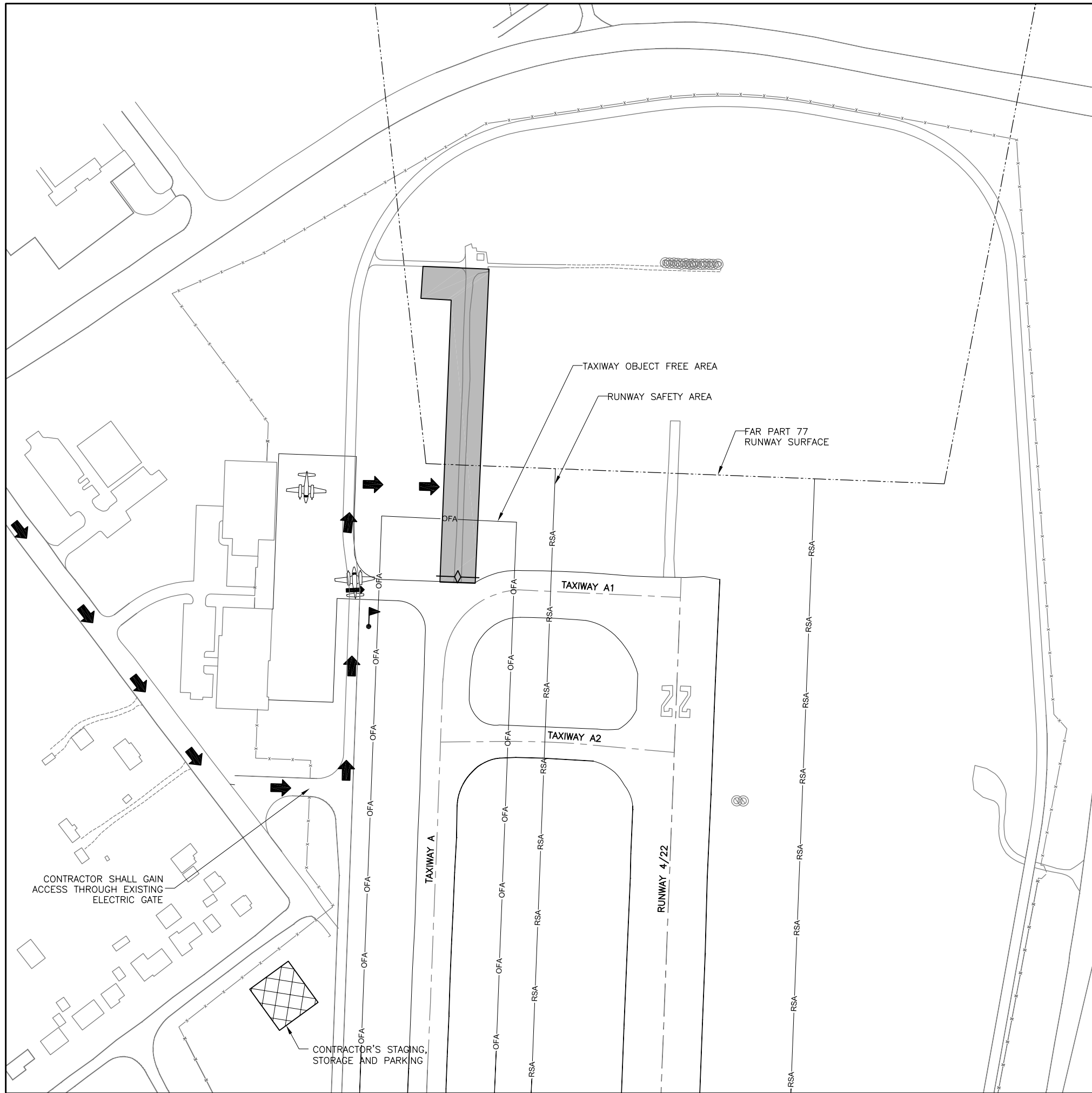
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**CONSTRUCTION ACTIVITY PLAN 1**

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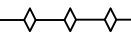










SEQUENCE OF CONSTRUCTION NOTES

1. ALL WORK IN THIS PHASE SHALL BE COMPLETED CONCURRENT WITH PHASE 1A OR OTHER SCHEDULED RUNWAY 4/22 CLOSURES. ALL CONSTRUCTION OPERATIONS SHALL BE UNDER THE CONTROL OF THE AIR TRAFFIC CONTROL TOWER. THE CONTRACTOR SHALL BE IN CONSTANT COMMUNICATIONS WITH THE ATCT AND SHALL YIELD TO ALL AIRCRAFT OPERATIONS.
2. THE CONTRACTOR SHALL DESIGNATE A FLAGMAN TO CONTROL ALL ACCESS TO ACTIVE AIRFIELD PAVEMENT. THE FLAGMAN SHALL BE RESPONSIBLE FOR STOPPING ALL CONSTRUCTION TRAFFIC SHORT OF THE TAXIWAY OBJECT FREE AREA AND OBTAINING CLEARANCE FROM THE ATCT TO PROCEED PRIOR TO ALLOWING ANY CONSTRUCTION VEHICLES ONTO AIRFIELD PAVEMENTS.
3. THE CONTRACTOR SHALL GIVE THE AIRPORT 7 DAYS NOTICE PRIOR TO THE INITIATION OF THIS PHASE TO COORDINATE WITH TENANTS. ACCESS TO THE MAUI JIM AND OSF HANGARS SHALL REMAIN CONTINUOUS THROUGHOUT THIS PHASE.
4. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN AIRFIELD PAVEMENT THAT IS CROSSED BY THE CONSTRUCTION ACTIVITY OR THAT IS DIRECTLY ADJACENT TO CONSTRUCTION OPERATIONS. ALL DEBRIS, DIRT, OR ANY FOD TYPE MATERIAL SHALL BE IMMEDIATELY SWEEPED OFF OF THE PAVEMENT SO AS TO PREVENT DAMAGE TO TAXIING AIRCRAFT.
5. THE MAJORITY OF THE WORK FOR THIS PHASE IS EITHER WITHIN THE RUNWAY PART 77 CRITICAL SURFACE AREA OR WITHIN THE TAXIWAY A OBJECT FREE AREA. THE CONTRACTOR SHALL BE IN CLOSE CONTACT WITH THE ATCT THROUGHOUT THIS PHASE AND SHALL MINIMIZE EQUIPMENT HEIGHT TO BE UNDER THE MAXIMUM 25' HEIGHT WHENEVER POSSIBLE.

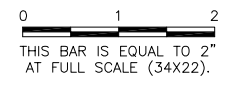
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-  PHASE 1C WORK AREA
-  CONTRACTOR'S STAGING AND STORAGE
-  IDOT TYPE 1 BARRICADES W/FLASHING RED LIGHTS AND 20"x20" ALTERNATING WHITE AND ORANGE FLAGS AT 10' SPACING
-  RSA
-  OFA
-  CONTRACTOR'S ACCESS ROUTE
-  AIRCRAFT ACCESS
-  CONTRACTOR AND AIRCRAFT ACCESS
-  CONTRACTOR'S FLAGMAN FOR CONTROLLING CONSTRUCTION TRAFFIC ON AIRPORT MOVEMENT AREA PAVEMENT

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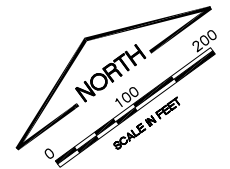
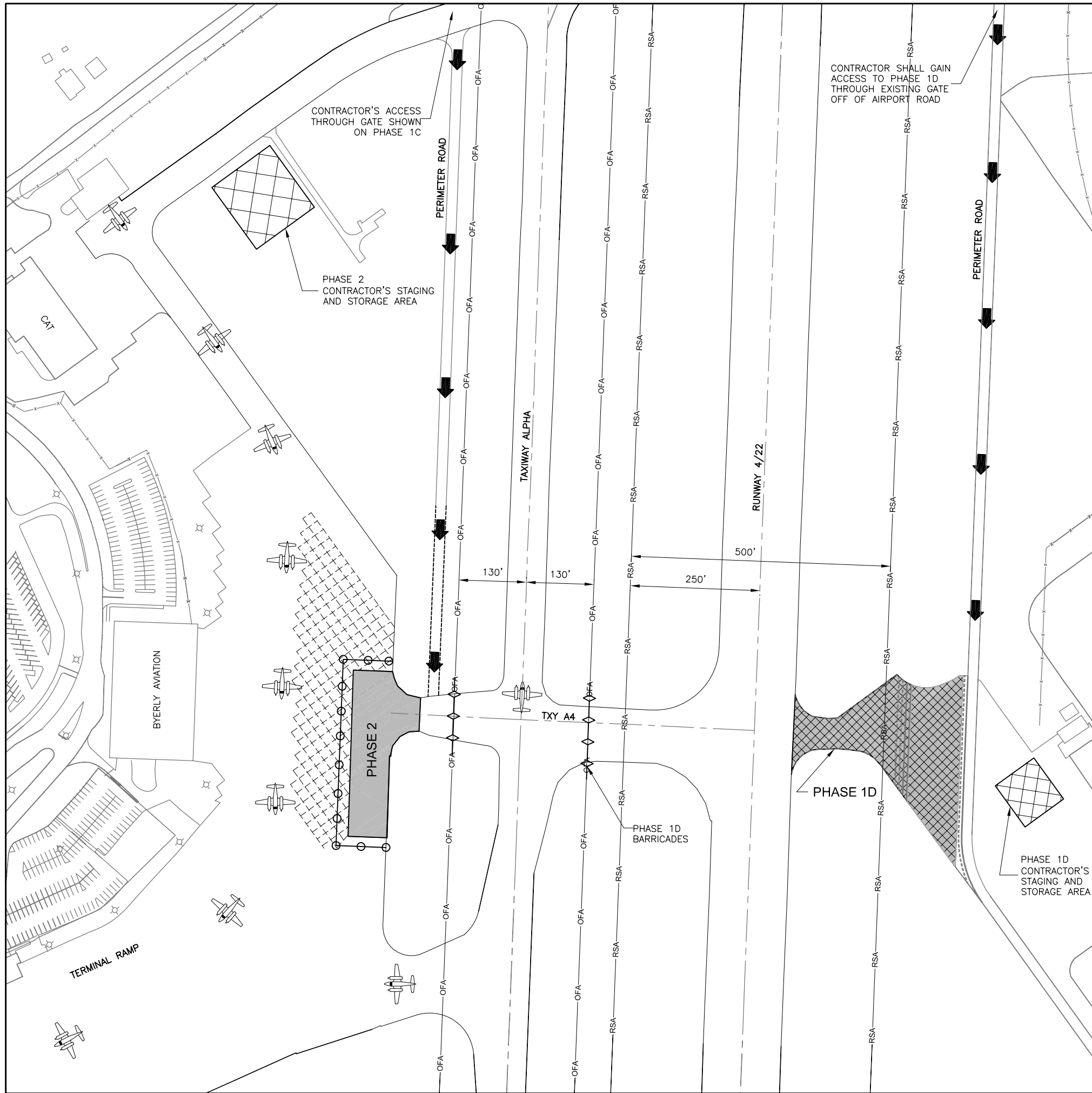
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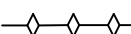
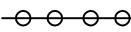
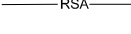
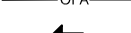


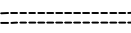
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**SEQUENCE OF CONSTRUCTION NOTES**

1. CONTRACTOR SHALL INSTALL ALL BARRICADES PRIOR TO INITIATING WORK IN THIS PHASE.
2. THE LOW PROFILE BARRICADES FOR PHASE 2 SHALL BE PLACED 10' FROM THE WORK LIMITS. BARRICADES CAN BE PLACED FURTHER WHEN CONFLICTING WITH CONSTRUCTION OPERATIONS, BUT SHALL BE PLACED BACK AT 10' WHEN OPERATING AIRCRAFTS ARE NEAR.
3. WORK IN PHASE 1D SHALL BE COMPLETED SIMULTANEOUSLY WITH PHASE 1A AND SHALL REQUIRE THE TEMPORARY CLOSURE OF RUNWAY 4/22. THE CONTRACTOR SHALL ADHERE TO THE FOLLOWING:
  - A. WORK IN PHASE 1D SHALL NOT BE COMPLETED DURING A TIME THAT RUNWAY 13/31 IS CLOSED.
  - B. ALL PAVEMENT REMOVED SHALL BE REPLACED WITH SELECTIVE FILL MATERIAL TO EXISTING GRADE TO ELIMINATE ANY DROP OFFS FROM EXISTING PAVEMENT.
  - C. WORK IN PHASE 1D SHALL BE COMPLETED IN NO MORE THAN FIVE CONSECUTIVE CALENDAR DAYS.
  - D. AT THE END OF EACH WORKING DAY, THE RUNWAY SHALL BE SWEEPED CLEAR, ALL REMOVALS SHALL BE FILLED AND THE RUNWAY SAFETY AREA SHALL BE GRADED AS SHOWN IN THE TYPICAL SECTION.
  - E. RUNWAY 4/22 SHALL BE RE-OPENED AT THE END OF EACH WORK DAY (WHEN THE RUNWAY IS NOT CLOSED FOR WORK IN PHASE 1A)
  - F. IF THE CONTRACTOR SHOULD ELECT TO COMPLETE THE WORK IN PHASE 1D CONCURRENT WITH WORK IN PHASE 1, THE RUNWAY SHALL NOT BE REQUIRED TO BE REOPENED AT THE END OF EACH WORKING DAY.
4. STOCK PILING OF MATERIALS ON RUNWAYS SHALL NOT BE ALLOWED AT ANY TIME.
5. ALL PAVEMENTS ADJACENT TO WORK AREAS 2 AND 1D SHALL BE CONTINUOUSLY CLEANED BY THE CONTRACTOR TO ELIMINATE DEBRIS.
6. AT NO TIME WILL THE CONTRACTOR BE ALLOWED TO CLOSE BOTH RUNWAY 4/22 AND RUNWAY 13/31 AT THE SAME TIME.

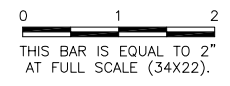
**LEGEND**

-  PHASE 2 WORK AREA
-  PHASE 1D WORK AREA
-  CONTRACTOR'S STAGING AND STORAGE
-  IDOT TYPE 1 BARRICADES W/FLASHING RED LIGHTS AND 20"x20" ALTERNATING WHITE AND ORANGE FLAGS AT 10' SPACING
-  LOW PROFILE BARRICADES
-  RUNWAY SAFETY AREA
-  OBJECT FREE AREA
-  CONTRACTOR'S ACCESS ROUTE
-  AIRCRAFT ACCESS
-  PLACE AND COMPACT MILLINGS

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**PEORIA, ILLINOIS**

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**CONSTRUCTION ACTIVITY PLAN 2**

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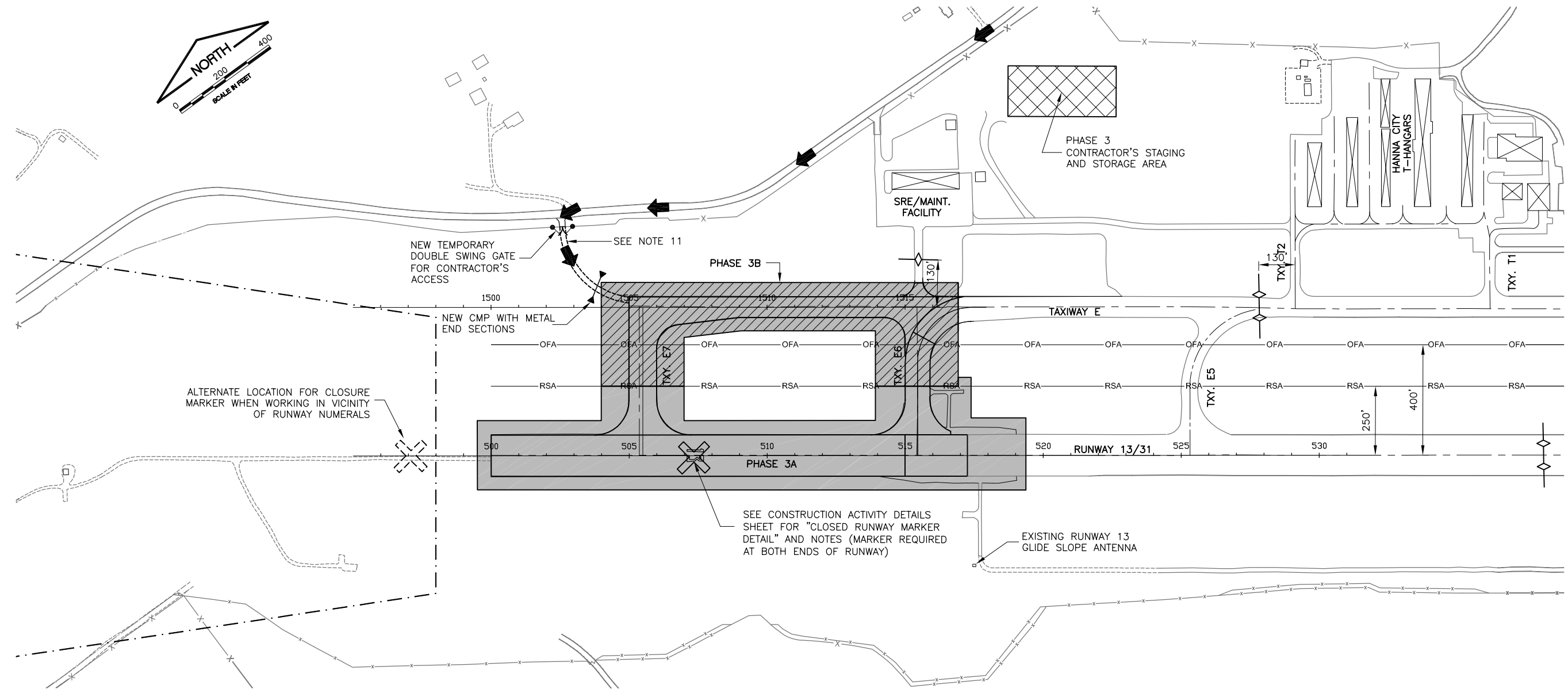
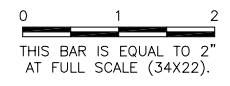


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**LEGEND**

- PHASE 3A WORK AREA
- PHASE 3B WORK AREA
- IDOT TYPE 1 BARRICADES W/FLASHING RED LIGHTS AND 20'x20" ALTERNATING WHITE AND ORANGE FLAGS AT 10' SPACING
- RUNWAY SAFETY AREA/ CONSTRUCTION SETBACK LINE
- OBJECT FREE AREA
- CONTRACTOR'S ACCESS ROUTE
- CONTRACTOR'S STAGING, STORAGE, AND PARKING
- EXISTING AIRFIELD BUILDING
- NEW CONSTRUCTION HAUL ROAD
- NEW TEMPORARY DOUBLE SWING GATE
- NEW TEMPORARY DRAINAGE PIPE

**SEQUENCE OF CONSTRUCTION NOTES**

1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON THIS PHASE.
2. THE AIRPORT WILL REQUIRE 14 DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING WORK IN PHASE 3A AND 3B TO ISSUE ALL APPROPRIATE NOTAM'S.
3. WORK IN THIS PHASE SHALL NOT BE STARTED UNTIL RUNWAY 4/22 HAS BEEN GROOVED AND IS ACCEPTED BY THE ENGINEER. AFTER THE COMPLETION OF PHASES 1 AND 2 INCLUDING ALL WORK REQUIRING THAT RUNWAY 4/22 CLOSE, IT IS THE INTENTION OF THE AIRPORT TO CEASE WORK ON THE PROJECT AND SCHEDULE A WINTER SHUTDOWN. NEITHER RUNWAY SHALL BE ALLOWED TO BE CLOSED THROUGHOUT THE WINTER. WORK IN THIS PHASE SHALL BEGIN WHEN WEATHER IS EXPECTED TO ALLOW THE CONTRACTOR TO WORK IN A TIMELY MANNER SO AS TO PREVENT PROLONGED CLOSURES.
4. THE CONTRACTOR SHALL EXPEDITE WORK WITHIN 250' OF THE RUNWAY CENTERLINE. UPON COMPLETION OF THE PAVING FOR THE RUNWAY WORK, THE SHOULDER SHALL BE INSTALLED AND THE PAVEMENT MARKING COMPLETED. AIRPORT OPERATIONS AND THE ENGINEER SHALL REVIEW THE CONDITION OF THE CONSTRUCTION AREA AND IF FOUND TO BE IN CONFORMANCE WITH STANDARDS, THE RUNWAY SHALL BE OPENED. FOLLOWING THE SPECIFIED CURE TIME, THE RUNWAY SHALL BE CLOSED AGAIN FOR COMPLETION OF THE RUNWAY GROOVING AND FINAL PAVEMENT MARKING. THIS WORK SHALL BE CLOSELY COORDINATED WITH THE WORK WHICH REQUIRES THE CLOSURE OF RUNWAY 4/22. AT NO TIME SHALL BOTH RUNWAYS OR ILS SYSTEMS ON MULTIPLE RUNWAYS BE CLOSED AT THE SAME TIME.
5. AFTER THE COMPLETION OF PHASE 3A, RUNWAY 13/31 SHALL BE RE-OPENED. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT ALL WORK REQUIRING THAT THE RUNWAY BE TEMPORARILY CLOSED. AFTER PHASE 3A, RUNWAY 13/31 SHALL BE OPENED AT THE END OF EACH WORKDAY.
6. DURING RUNWAY CLOSURES, CLOSED RUNWAY MARKERS ARE TO BE PLACED AT BOTH ENDS OF RUNWAY 13/31 AND SHALL BE PLACED AS SHOWN ON THE CONSTRUCTION ACTIVITY DETAILS SHEET.
7. WORK IN PHASES 3A AND 3B MAY BE COMPLETED CONCURRENTLY PROVIDED THAT THE WORK REQUIRING RUNWAY CLOSURES IS NOT DELAYED.
8. THE LATHE LINE/CONSTRUCTION SET BACK LINE SHALL BE SET IN PLACE FOR WORK IN PHASE 3B WHEN RUNWAY 13/31 IS ACTIVE. THE LATHE LINE SHALL EXTEND 500' EACH WAY BEYOND THE LIMITS OF CONSTRUCTION.
9. SEE CONSTRUCTION ACTIVITY DETAILS FOR BASELINE INFORMATION.
10. THE CONTRACTOR SHALL CONSTRUCT A NEW HAUL ROUTE FOR CONSTRUCTION ACTIVITY USING THE MILLINGS FROM THE PROPOSED CONSTRUCTION OPERATIONS. ANY PAVEMENT DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED BY THE CONTRACTOR AT HIS/HER OWN COST.
11. A TEMPORARY GATE SHALL BE INSTALLED IN THE FENCELINE AT THE LOCATION SHOWN OR AS DIRECTED BY THE RESIDENT ENGINEER. ALL ITEMS ASSOCIATED WITH THE REMOVAL OF EXISTING FENCE, INSTALLATION OF THE TEMPORARY GATE, FENCE MAINTENANCE, AND RESTORATION OF THE FENCE AT THE END OF THE PROJECT SHALL BE CONSIDERED PART OF THE HAUL ROUTE PAY ITEM. CONSTRUCTION OF NEW FENCE TO RESTORE TO THE ORIGINAL CONDITIONS SHALL MEET THE REQUIREMENTS OF ITEM 162 FOR 10' CLASS E FENCE.
12. THE CONTRACTOR SHALL INSTALL NEW DRAINAGE PIPE TO PROMOTE DRAINAGE UNDER THE NEW HAUL ROAD. THE PIPE MUST BE INSTALLED OUTSIDE OF THE TAXIWAY SAFETY AREA.
13. AT THE END OF CONSTRUCTION, THE HAUL ROUTE SHALL BE COMPLETELY REMOVED AND THE AREA SHALL BE RESTORED TO ITS ORIGINAL CONDITIONS. ALL COSTS TO REMOVE AND RESTORE SHALL BE CONSIDERED INCIDENTAL TO HAUL ROUTE PAY ITEM.
14. IT IS IN THE BEST INTEREST OF THE AIRPORT TO COMPLETE THIS PHASE OF WORK AFTER THE COMPLETION OF THE RUNWAY 4 AND TAXIWAY A REHABILITATION. NO WORK SHALL BE INITIATED ON THIS PHASE UNTIL ALL WORK REQUIRING THE CLOSURE OF RUNWAY 4 AND/OR TAXIWAY A HAVE BEEN COMPLETED.

**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**CONSTRUCTION ACTIVITY PLAN 3**

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**PEORIA INTERNATIONAL AIRPORT**

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| DESIGN BY:                 | TJH          |
| DRAWN BY:                  | CMT          |
| CHECKED BY:                | CET          |
| APPROVED BY:               | CET          |
| DATE:                      | MAY 13, 2011 |
| JOB No:                    | 100610300    |
| IL PROJ. NO. PIA-4079      |              |
| AIP PROJ. NO. 3-17-0080-XX |              |
| SHEET 07 OF 84 SHEETS      |              |



**CONSTRUCTION ACTIVITY PLAN GENERAL NOTES**

1. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK SO AS TO NOT INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS.
2. THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS.
3. IT SHALL BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECT ARE THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS.
4. WHEN CONFLICTS ARISE, RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT THROUGH THE RESIDENT ENGINEER IN THE BEST INTEREST OF THE AIRPORT AND THE PROJECT.
5. DELAYS, CHANGES IN SCHEDULING OR THE EXPEDITION OF WORK UNDER THIS CONTRACT TO PROVIDE FOR THE TIMELY PROSECUTION OF THE PROJECT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
6. VEHICLE ACCESS ON PERIMETER ROADS SHALL BE MAINTAINED THROUGH OUT THE PROJECT.
7. THE CONTRACTOR SHALL BE AWARE THAT DAILY OPERATION WILL CONTINUE OUT OF THE EXISTING MAINTENANCE FACILITY THROUGHOUT THIS PROJECT. AS A RESULT, DAILY COORDINATION WILL BE NECESSARY TO LIMIT DISRUPTION TO AIRPORT/TENTANT AND CONTRACTOR OPERATIONS.
8. CONTRACTOR'S STAGING, STORAGE, AND PARKING WILL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS.
9. THE CONTRACTOR SHALL PLACE ALL BARRICADES, CONSTRUCTION SETBACK LINES, AND EROSION CONTROL ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
10. CONSTRUCTION PHASING IS OF CRITICAL IMPORTANCE TO THE AIRPORT FOR THIS PROJECT. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE ALLOWED TO CLOSE BOTH RUNWAYS AT THE SAME TIME.
11. PHASE 1 AND PHASE 3 SHALL BE COMPLETED AT DIFFERENT TIMES WITH NO OVERLAPPING OF PHASES TO ENSURE THAT ONE RUNWAY REMAINS OPEN AT ALL TIMES. IF, FOR TIMING PURPOSES, THE CONTRACTOR REQUESTS THAT THE WORK SHOWN IN PHASE 3 BE COMPLETED PRIOR TO THE WORK IN PHASE 1, HE SHALL MAKE THE REQUEST IN WRITING TO THE AIRPORT THROUGH THE RESIDENT ENGINEER AND SHALL INCLUDE JUSTIFICATION AND A DETAILED PROJECT SCHEDULE.
12. CONSTRUCTION RELATED ITEMS REQUIRING THE CLOSURE OF RUNWAYS AND/OR TAXIWAYS AND/OR SHUT DOWN OF ILS EQUIPMENT SHALL REQUIRE CLOSE COORDINATION WITH THE AIRPORT AND FAA. NO EXTENSION TO CONTRACT TIME WILL BE GIVEN FOR DELAYS CAUSED BY LACK OF ADEQUATE COORDINATION.
14. CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
  - A. THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
  - B. THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE AIRPORT OPERATIONS BY THE CONTRACTOR.
  - C. THE CONTRACTOR SHALL USE AN EXISTING GATE(S), OR NEW GATES AS CALLED OUT IN THE PLANS, FOR ACCESS TO THE AIRFIELD.
  - D. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
  - E. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
  - F. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
  - G. THE CONTRACTOR SHALL CLOSE AND SECURE THE ACCESS GATE(S) UPON LEAVING THE SITE.
  - H. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
  - I. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
  - J. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE USED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.

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 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

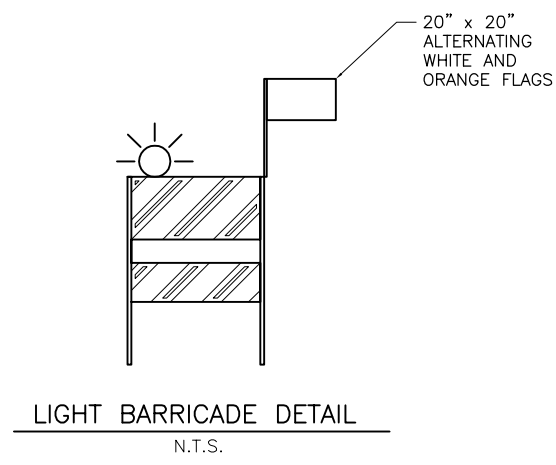
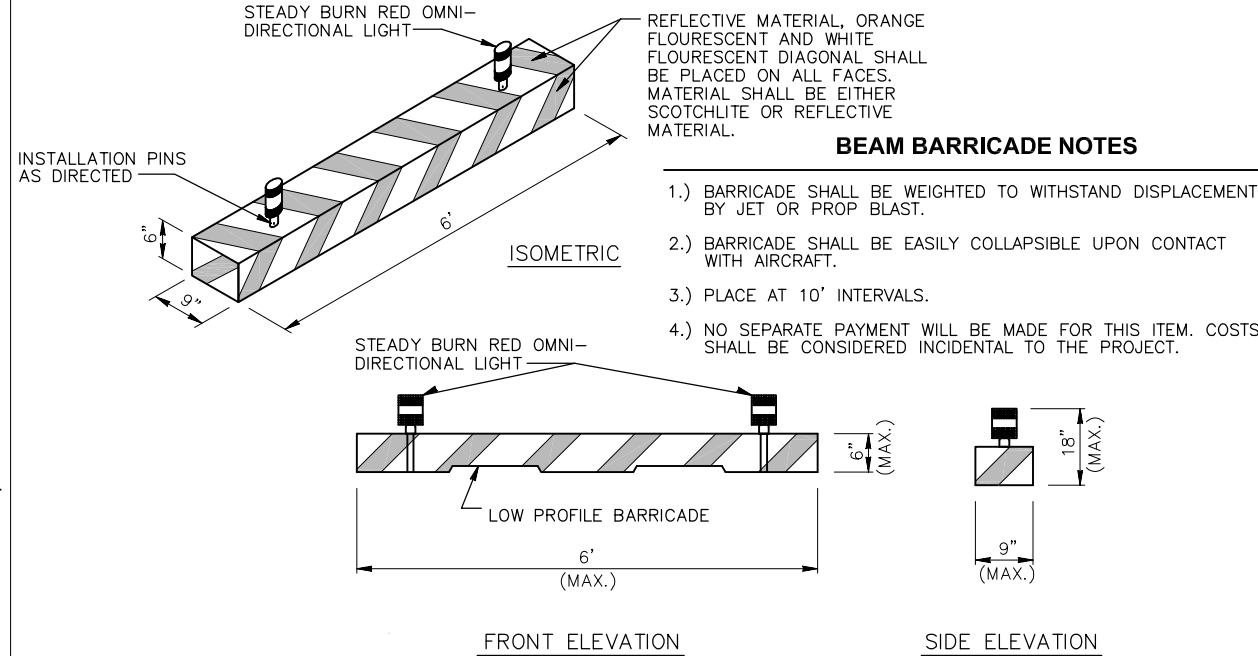
GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS

REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 CONSTRUCTION ACTIVITY PLAN NOTES  
 AND DETAILS

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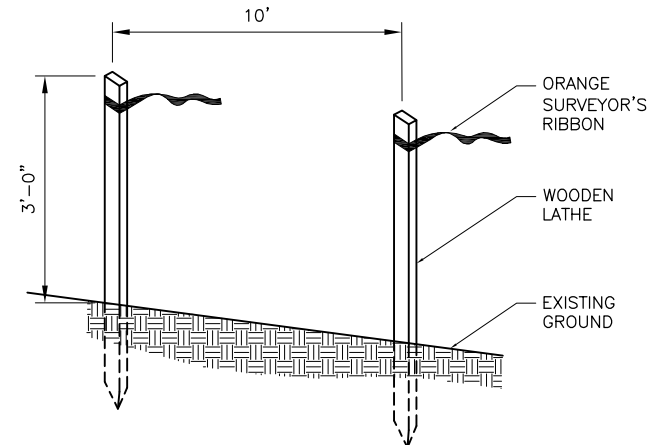
- LIGHT BARRICADE NOTES**
1. LIGHTS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
  2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
  3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
  4. PLACE AT 10' INTERVALS.

**SECURITY NOTES**

1. MAINTAINING THE SECURITY REQUIREMENTS OF THE AIRPORT SHALL BE A PRIMARY CONCERN FOR THE CONTRACTOR.
2. AT NO TIME ARE UNSUPERVISED GAPS OR OPENINGS PERMITTED IN THE AIRPORT FENCE LINE. IF THE PROPOSED WORK REQUIRES AN OPENING OR REMOVAL OF THE FENCE, THE CONTRACTOR SHALL PROVIDE A GUARD TO PREVENT UNAUTHORIZED ACCESS. THE CONTRACTOR SHALL RE-INSTALL THE FENCE AT THE COMPLETION OF THE WORK DAY.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING AIRPORT SECURITY BY SUPERVISING OPENINGS OR MAINTAINING THE AIRPORT PERIMETER FENCE LINE AT ALL TIMES DURING THE COURSE OF THE WORK.
4. PRIOR TO REMOVING A SEGMENT OF THE AIRPORT PERIMETER FENCE, THE PROPOSED FENCE SHALL BE ERECTED. THE FENCE SHALL BE ERECTED AT THE LOCATIONS SHOWN IN THE PLANS SO THE PERIMETER FENCE LINE IS ALWAYS CONTINUOUS AND THERE ARE NO OPENINGS OR BREAKS IN THE AIRPORT PERIMETER FENCE LINE.
5. FINES CAN BE LEVIED AGAINST THE CONTRACTOR BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) FOR NEGLIGENCE IF THE AIRPORT SECURITY IS COMPROMISED AND THE AIRPORT PERIMETER FENCE LINE IS NOT MAINTAINED AS SPECIFIED ABOVE. FINES CAN ALSO BE LEVIED AGAINST THE CONTRACTOR FOR FAILURE TO COOPERATE WITH THE AIRPORT MANAGEMENT AS REQUIRED TO MAINTAIN AIRPORT SECURITY.

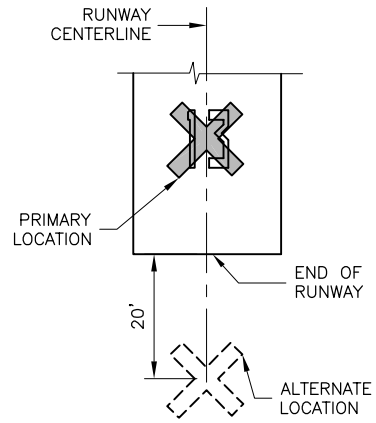
**BASELINE INFORMATION**

| BASELINE | STATION      | NORTHING      | EASTING       |
|----------|--------------|---------------|---------------|
| RWY. 13  | 495+00       | 1,459,908.139 | 2,421,774.122 |
| RWY. 13  | 520+00       | 1,458,432.171 | 2,423,791.922 |
| TXY. E7  | 0+00         | 1,459,288.402 | 2,422,621.366 |
| TXY. E   | 5+37.63      | 1,459,722.331 | 2,422,938.774 |
| TXY. E   | 1500+00      | 1,460,047.024 | 2,422,495.199 |
| TXY. E6  | 1516+50      | 1,459,072.436 | 2,423,826.619 |
| TXY. E6  | 0+00         | 1,458,700.762 | 2,423,424.730 |
| TXY. E6  | 3+37.28 (PC) | 1,458,972.931 | 2,423,623.927 |
| TXY. E6  | 6+51.45 (PT) | 1,459,016.195 | 2,423,903.451 |
| RWY. 4   | 495+00       | 1,452,082.734 | 2,427,217.351 |
| RWY. 4   | 520+00       | 1,454,042.812 | 2,428,769.155 |
| TXY. A6  | 0+00         | 1,452,513.951 | 2,427,558.748 |
| TXY. A6  | 4+50         | 1,452,793.276 | 2,427,205.934 |
| TXY. A   | 1499+00      | 1,452,675.672 | 2,427,112.826 |
| TXY. A   | 1520+00      | 1,454,322.137 | 2,428,416.341 |



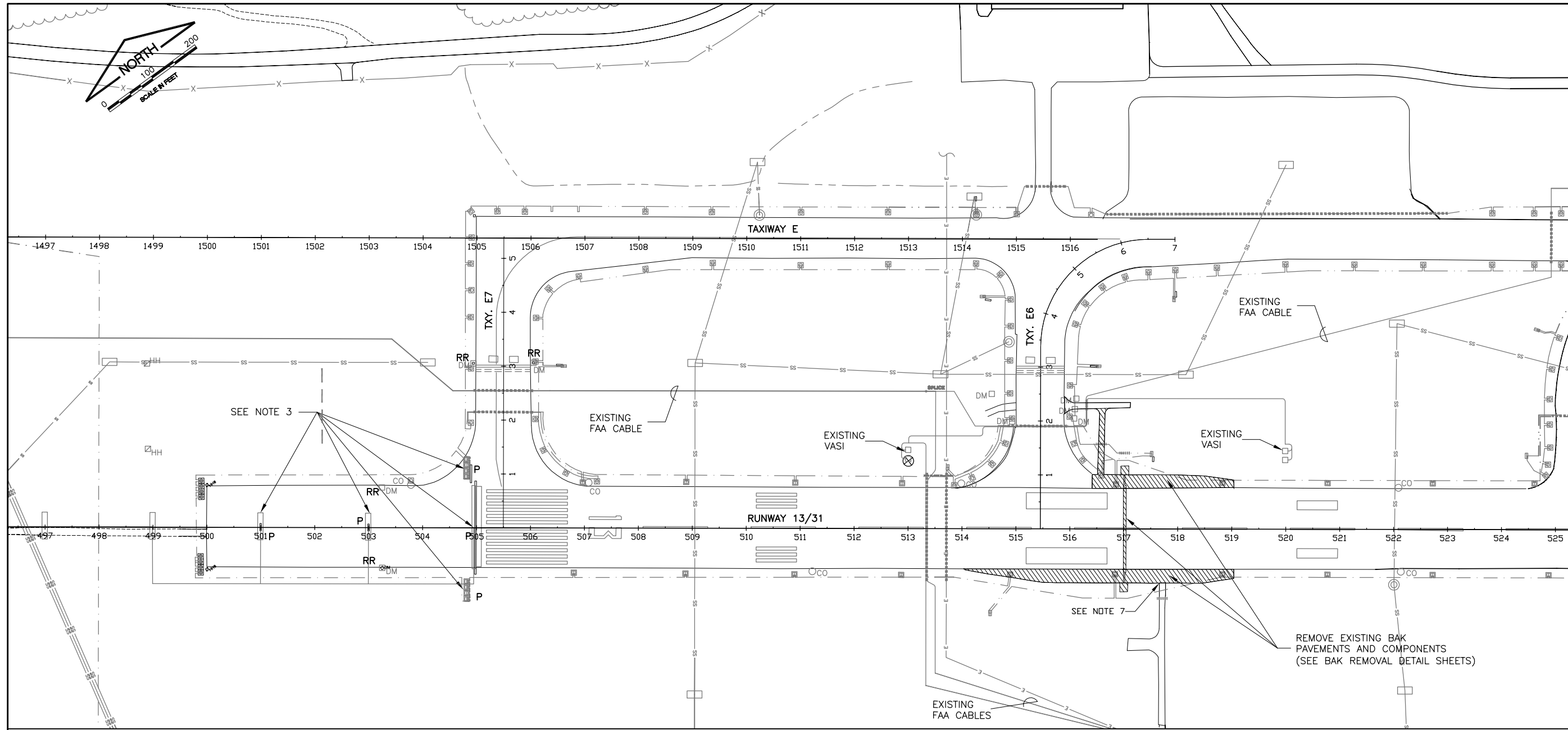
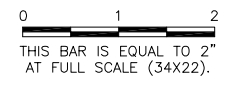
**NOTES:**

1. MARKERS SHALL BE PORTABLE, LIGHTED RUNWAY CLOSURE MARKERS IN ACCORDANCE WITH FAA AC150/5345-55 FOR FAA-L893 AND MANUFACTURER MUST BE ON THE FAA APPROVED LIST.
2. CONTRACTOR SHALL PROVIDE AND MAINTAIN MARKERS THROUGHOUT THE DURATION OF CONSTRUCTION.
3. AT THE END OF CONSTRUCTION, THE MARKERS SHALL BE TURNED OVER TO THE AIRPORT IN NEW CONDITION FOR OWNERSHIP.
4. COST OF INSTALLING, MAINTAINING, FUELING, RELOCATING, AND REMOVING MARKERS SHALL BE INCIDENTAL TO THE RUNWAY CLOSURE MARKER ITEM.
5. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS. WHEN CONSTRUCTION OPERATIONS CONFLICT, THE CLOSURE MARKERS SHALL BE MOVED TO THE ALTERNATE LOCATION.



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**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**

**EXISTING CONDITIONS AND REMOVALS PLAN 1**

**LEGEND**

|  |                                     |
|--|-------------------------------------|
|  | EXISTING STORM SEWER                |
|  | EXISTING UNDERGROUND ELECTRIC       |
|  | EXISTING FENCE                      |
|  | EXISTING ELECTRICAL DUCT            |
|  | EXISTING BASE MOUNTED TAXIWAY LIGHT |
|  | EXISTING BASE MOUNDED RUNWAY LIGHT  |
|  | EXISTING INLET                      |
|  | EXISTING DUCT MARKER                |
|  | EXISTING CLEANOUT                   |
|  | EXISTING HANDHOLE                   |
|  | EXISTING FLARED END SECTION         |
|  | EXISTING PAVEMENT MARKING           |
|  | EXISTING CIRCUIT 13/31              |
|  | EXISTING CIRCUIT #3                 |
|  | PROTECT EXISTING STRUCTURE          |
|  | REMOVE AND REINSTALL                |
|  | EXISTING BAK TO BE REMOVED          |

**NOTES:**

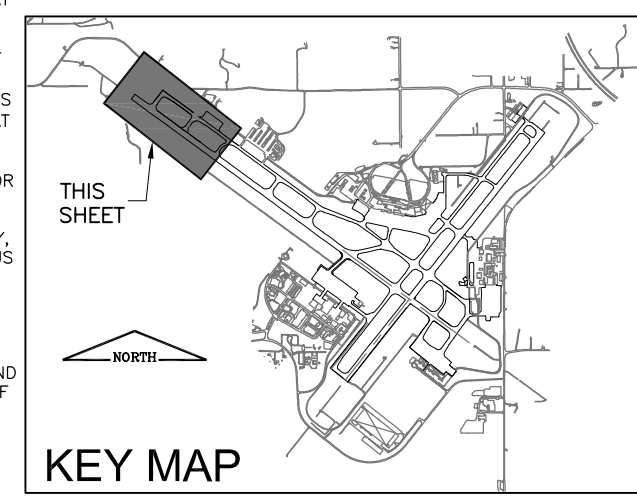
1. ALL COSTS NECESSARY TO PROTECT ALL ITEMS TO REMAIN WITHIN THE LIMITS OF CONSTRUCTION SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
2. ALL UTILITIES, NAVIADS, STRUCTURES AND ANY OTHER MISCELLANEOUS ITEMS DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AT THE CONTRACTOR'S EXPENSE.
3. CONTRACTOR SHALL USE CAUTION DURING MILLING OPERATIONS NOT TO DAMAGE THESE EXISTING LIGHT FIXTURES OR THE CONCRETE THEY ARE ENCASED IN. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPLACED/REPAIRED BY THE CONTRACTOR AT HIS/HER EXPENSE.
4. PRIOR TO REMOVING THE EXISTING DUCT MARKERS, THE CONTRACTOR SHALL SURVEY THE EXACT LOCATION OF THE MARKERS.
5. AFTER THE IMPROVEMENTS HAVE BEEN COMPLETED ON THE RUNWAY, THE REMOVED DUCT MARKERS SHALL BE PLACED IN THEIR PREVIOUS LOCATION OVER THE EXISTING DUCT. THE COST TO REMOVE AND REINSTALL THIS ITEM SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
6. THE BAK REMOVAL SHALL CONSIST OF REMOVING ALL ITEMS ASSOCIATED WITH THE BAK INCLUDING PAVEMENTS, FOUNDATIONS AND INTERNAL COMPONENTS. SEE BAK REMOVAL DETAILS FOR DETAILS OF THE EXISTING ITEMS AND REMOVAL NOTES.
7. CONTRACTOR SHALL USE 12" OF MILLINGS AND 4" OF BITUMINOUS SURFACE TO RECONNECT ACCESS ROAD TO THE RUNWAY. EARTH EXCAVATION AND PLACEMENT AND COMPACTION OF THE MILLINGS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE 4" BITUMINOUS SURFACE SHALL BE PAID FOR UNDER ITEM 401.

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 Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

**CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.**



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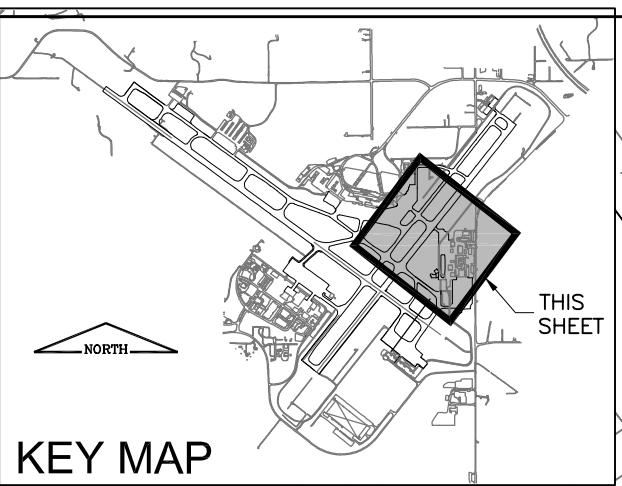
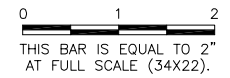
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SHEET 9 OF 84 SHEETS

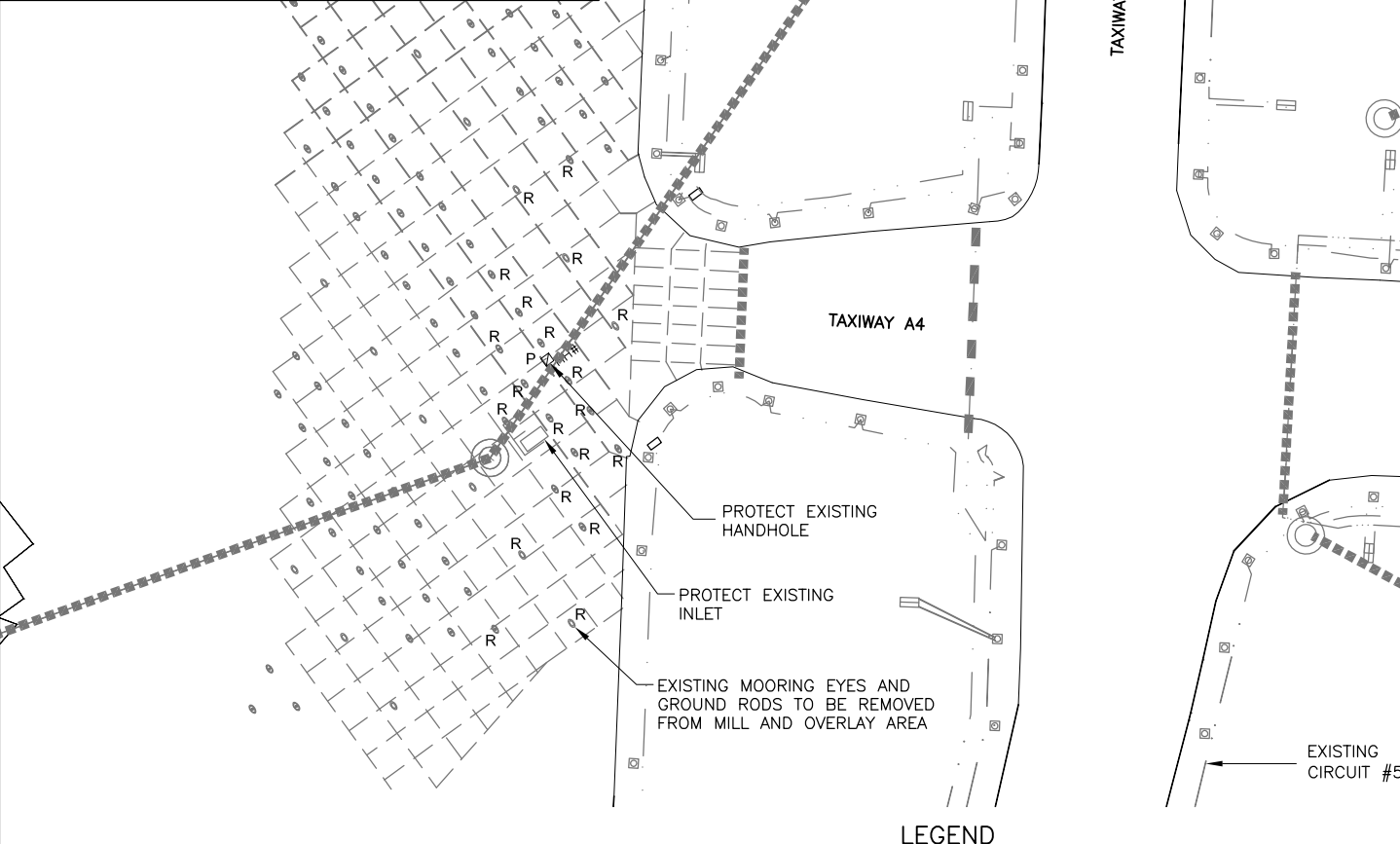


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**KEY MAP**



**NOTES:**

1. ALL COSTS NECESSARY TO PROTECT ALL ITEMS TO REMAIN WITHIN THE LIMITS OF CONSTRUCTION SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
2. ALL UTILITIES, NAVIADS, STRUCTURES AND ANY OTHER MISCELLANEOUS ITEMS DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AT THE CONTRACTOR'S EXPENSE.
3. THE CONTRACTOR SHALL USE CAUTION NOT TO DAMAGE EXISTING AIRFIELD CABLES. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED BY A QUALIFIED ELECTRICIAN AT THE CONTRACTOR'S COST.
4. ALL UTILITIES SHOWN ARE APPROXIMATE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PERFORM UTILITY LOCATES TO SATISFY HIMSELF/HERSELF THAT ALL UTILITIES ARE OUTSIDE OF THE LIMITS OF CONSTRUCTION PRIOR TO PROCEEDING WITH THE DEMOLITION.
5. AS SHOWN ON SHEET PROPOSED IMPROVEMENTS 2, NEW 1/C 5KV CABLE IN UNIT DUCT SHALL BE INSTALLED BETWEEN THE TWO RUNWAY EDGE LIGHTS SHOWN. PRIOR TO REMOVING THE SIGNS, THE CONTRACTOR SHALL LOCATE THE EXISTING CIRCUIT TO VERIFY THE CONNECTIONS CAN BE MADE IN THE LOCATION SHOWN ON PROPOSED IMPROVEMENTS 2.
6. THE CONTRACTOR SHALL SAWCUT TO A 3" DEPTH ON FOUR SIDES OF TIE DOWNS TO BE REMOVED. THE PCC AROUND THE TIE DOWN SHALL BE "CHIPPED" OUT TO THE DEPTH OF THE TIE DOWN. IN THE EVENT THAT ANCHOR BARS WERE USED, THE ANCHOR BAR SHALL BE BURNT OFF. AFTER MILLING THE REMOVAL AREA, THE CONTRACTOR SHALL USE BITUMINOUS MATERIAL TO PATCH THE TIE DOWN AND GROUND ROD REMOVALS.
7. GROUND RODS SHALL BE REMOVED BY "CHIPPING" OUT PAVEMENT AROUND THE GROUND ROD AND CUTTING OR BURNING OFF BELOW THE SPECIFIED MILLING DEPTH.

**LEGEND**

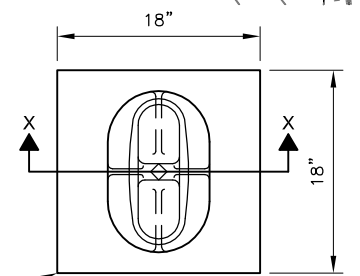
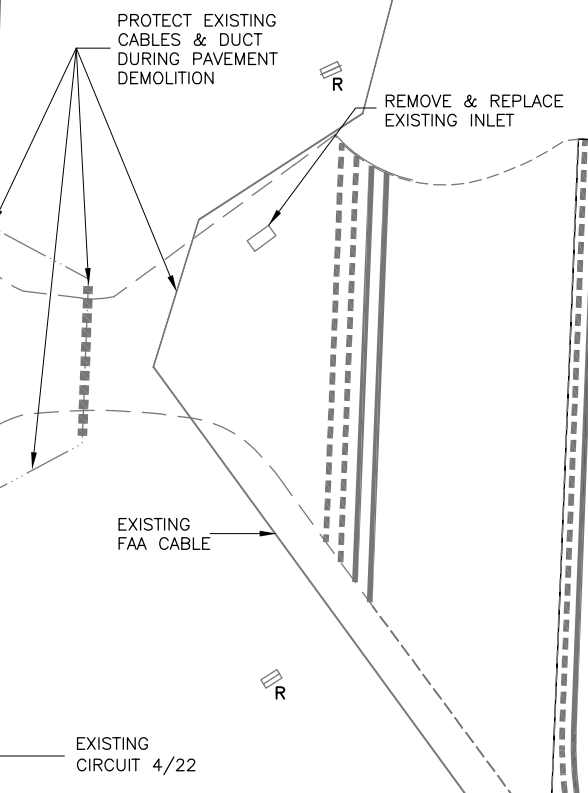
|      |                                     |
|------|-------------------------------------|
| ---  | EXISTING JOINTING                   |
| —E—  | EXISTING UNDERGROUND ELECTRIC       |
| —SS— | EXISTING STORM SEWER                |
| ---  | EXISTING ELECTRICAL DUCT            |
| ---  | EXISTING CIRCUIT #5                 |
| ---  | EXISTING CIRCUIT 4/22               |
| ---  | EXISTING MARKING                    |
| ⊠    | EXISTING BASE MOUNTED TAXIWAY LIGHT |
| ⊠    | EXISTING BASE MOUNTED RUNWAY LIGHT  |
| ⊠    | EXISTING AIRFIELD SIGN              |
| P    | PROTECTIVE EXISTING STRUCTURE       |
| R    | REMOVE                              |
| ⊠HH  | EXISTING ELECTRICAL HANDHOLE        |
| ⊠    | EXISTING MOORING EYE                |
| ⊠    | EXISTING GROUND ROD                 |
| ⊠    | EXISTING INLET                      |

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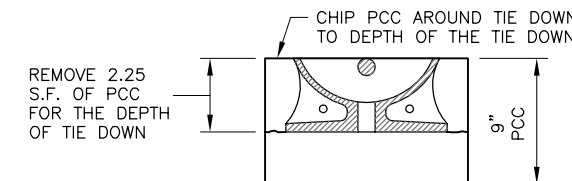
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THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

**CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.**



SAWCUT PARTIAL DEPTH (TYPICAL OF 4 SIDES)



**MOORING EYE/TIE DOWN REMOVAL DETAIL**

N.T.S.

**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 EXISTING CONDITIONS AND REMOVALS PLAN 2**

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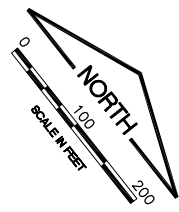
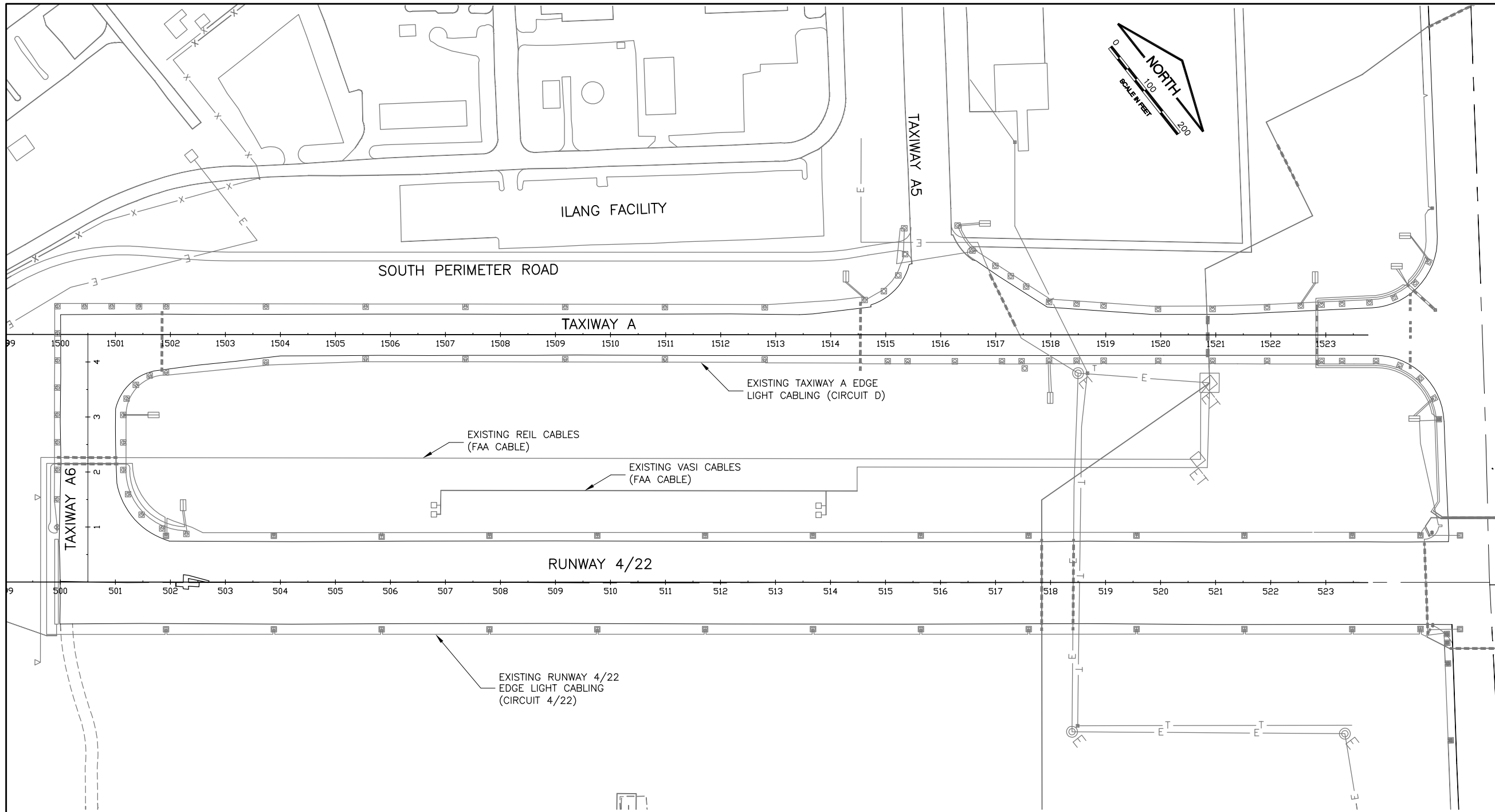
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| DATE:                      | MAY 13, 2011 |
| JOB No:                    | 100610300    |
| IL PROJ. NO. PIA-4079      |              |
| AIP PROJ. NO. 3-17-0080-XX |              |
| SHEET 10 OF 84 SHEETS      |              |



**PE092**

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 AT FULL SCALE (34X22).



**LEGEND**

|  |  |
|--|--|
|  | EXISTING BITUMINOUS PAVEMENT TO BE REMOVED |
|  | EXISTING TELEPHONE                         |
|  | EXISTING UNDERGROUND ELECTRIC              |
|  | EXISTING SANITARY                          |
|  | EXISTING STORM SEWER                       |
|  | EXISTING FENCE                             |
|  | EXISTING ELECTRICAL DUCT                   |
|  | EXISTING CIRCUIT D                         |
|  | EXISTING CIRCUIT 4/22                      |
|  | EXISTING BASE MOUNTED TAXIWAY LIGHT        |
|  | EXISTING BASE MOUNTED RUNWAY LIGHT         |
|  | EXISTING AIRFIELD SIGN                     |

**NOTES:**

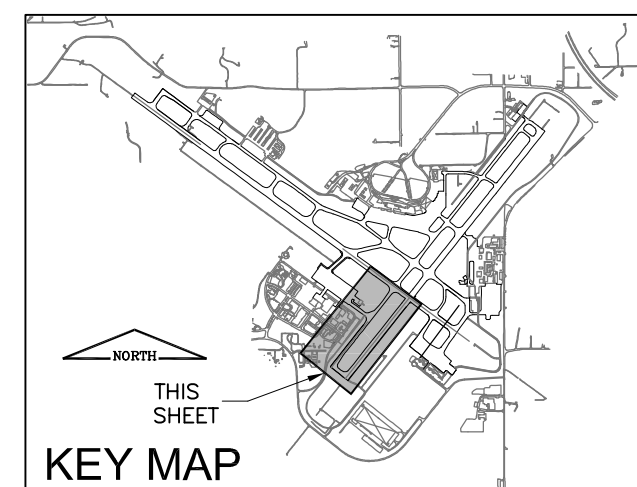
1. ALL COSTS NECESSARY TO PROTECT ALL ITEMS TO REMAIN WITHIN AND/OR NEAR THE LIMITS OF CONSTRUCTION SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
2. ALL UTILITIES, NAVIADS, STRUCTURES AND ANY OTHER MISCELLANEOUS ITEMS DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AT THE CONTRACTOR'S EXPENSE.
3. ALL AIRFIELD STRUCTURES (LIGHTS, SIGNS, MARKERS, ETC...) ADJACENT TO THE PROPOSED IMPROVEMENTS ARE TO REMAIN IN PLACE AND SHALL BE PROTECTED BY THE CONTRACTOR..

**811** Know what's below. Call before you dig.

**COMMON GROUND ALLIANCE**  
 www.call811.com or  
 Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

**CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.**



**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
 PEORIA, ILLINOIS

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**EXISTING CONDITIONS AND REMOVALS PLAN 3**

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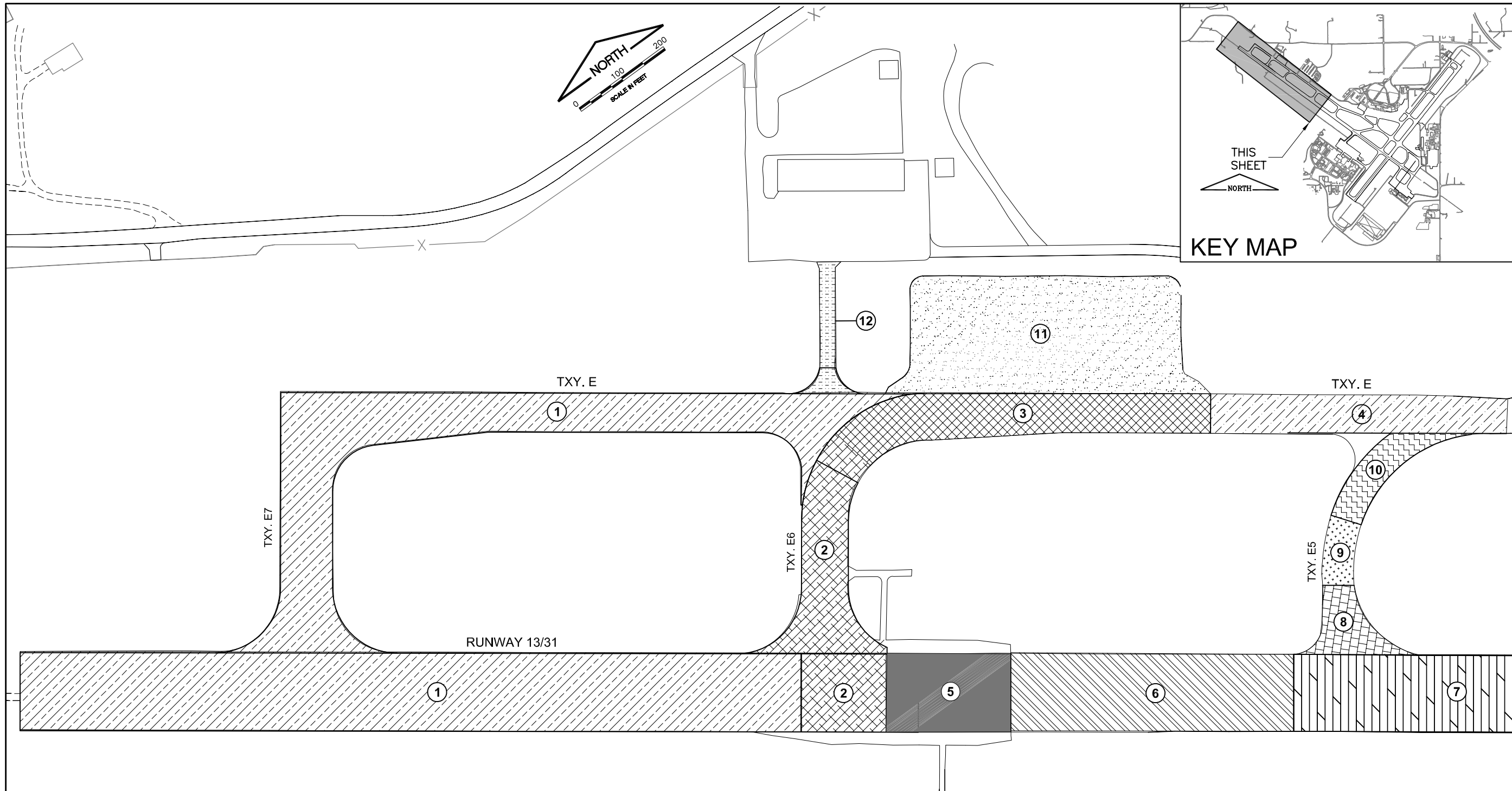
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| DATE:         | MAY 13, 2011    |
| JOB No:       | 100610300       |
| IL PROJ. NO.  | PIA-4079        |
| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 11 OF 84 SHEETS |

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**KEY MAP**

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 NORTH

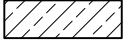
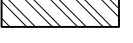
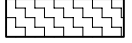





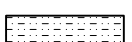
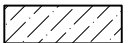
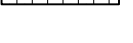

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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 EXISTING PAVEMENT STRUCTURE 1**

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**LEGEND**

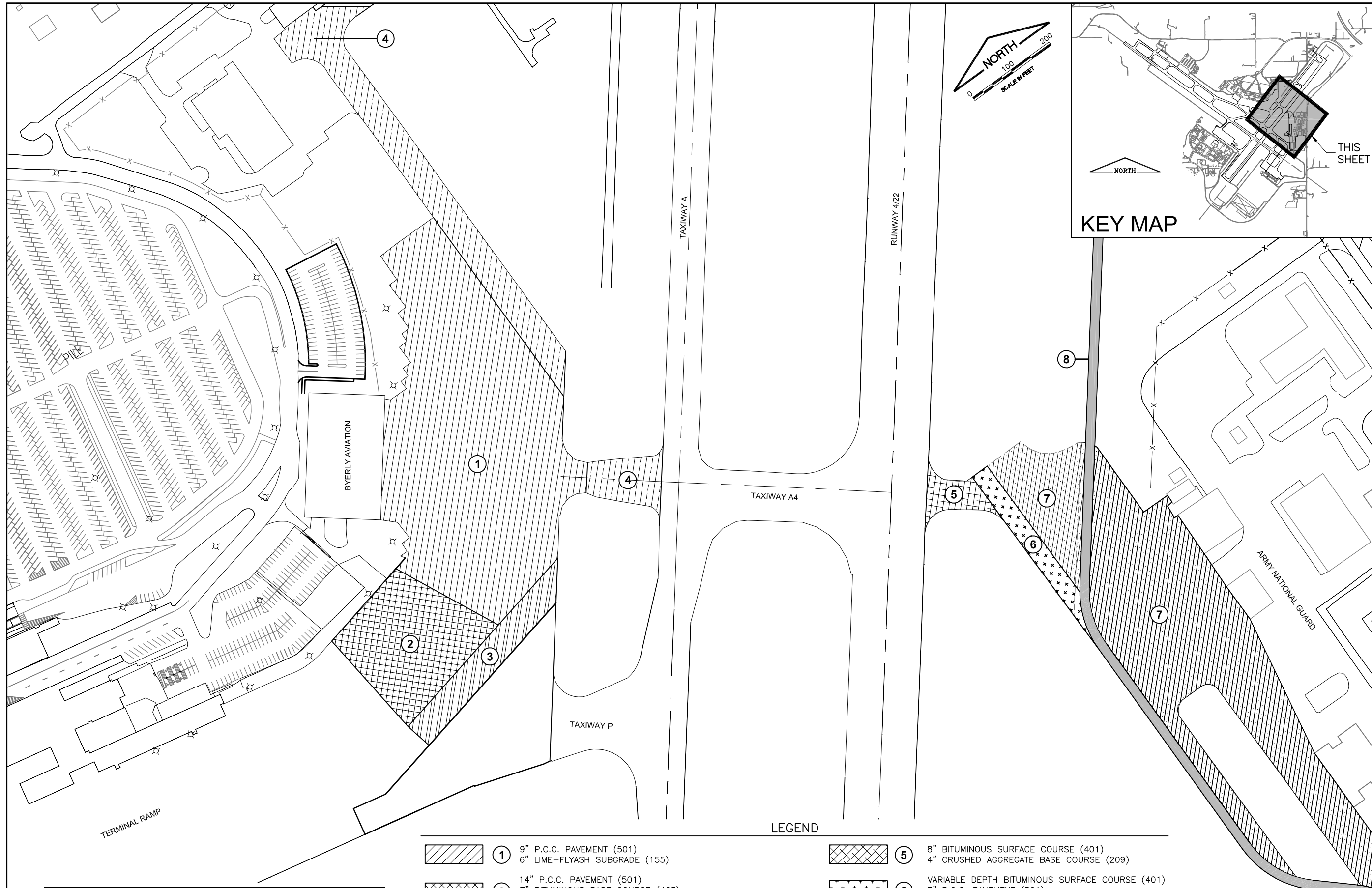
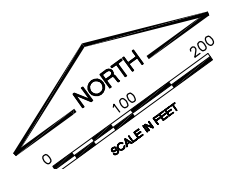
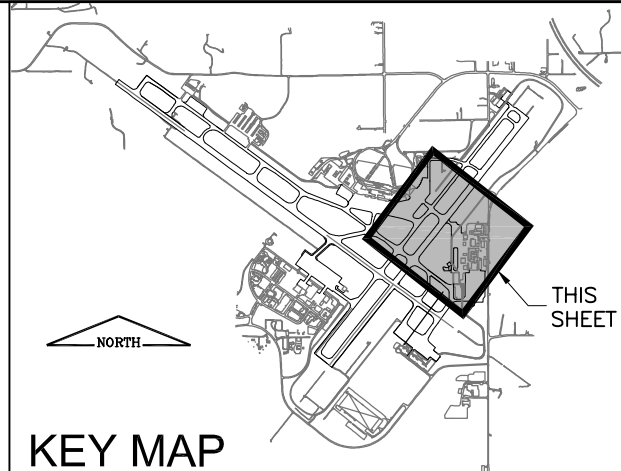
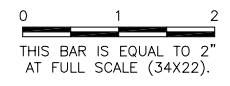
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|---|---|--|---|---|---|
|  ① | 3" BITUMINOUS SURFACE COURSE (401)<br>25" BITUMINOUS BASE COURSE (403)<br>4" CRUSHED AGGREGATE BASE COURSE (209)<br>8" LIME MODIFIED SUBGRADE (195) |  ⑥ | 10" P.C.C. PAVEMENT (501)<br>VARIABLE DEPTH BITUMINOUS BASE COURSE (403)<br>11" P.C.C. PAVEMENT (501)   |  ⑩ | 11" & VARIABLE BITUMINOUS SURFACE COURSE (401)<br>3" BITUMINOUS BASE COURSE (403)<br>3" BITUMINOUS SURFACE COURSE (401)<br>6" CRUSHED AGGREGATE BASE COURSE (209) |
|  ② | 12" P.C.C. PAVEMENT (501)<br>11" BITUMINOUS BASE COURSE (403)<br>6" LIME-FLYASH SUBGRADE (155)  |  ⑦ | 10" P.C.C. PAVEMENT (501)<br>VARIABLE DEPTH BITUMINOUS BASE COURSE (403)<br>3.5" BITUMINOUS SURFACE COURSE (401)<br>3" BITUMINOUS BASE COURSE (403)<br>9" P.C.C. PAVEMENT (501) |  ⑪ | 5" BITUMINOUS SURFACE COURSE (401)<br>23" BITUMINOUS BASE COURSE (403)<br>6" LIME-FLYASH SUBGRADE (155)   |
|  ③ | 6" BITUMINOUS SURFACE COURSE (401)<br>22" BITUMINOUS BASE COURSE (403)<br>6" LIME-FLYASH SUBGRADE (155)   |  ⑧ | 6.5" & VARIABLE BITUMINOUS SURFACE COURSE (401)<br>3" BITUMINOUS BASE COURSE (403)<br>3" BITUMINOUS SURFACE COURSE (401)<br>6" CRUSHED AGGREGATE BASE COURSE (209)              |  ⑫ | 2" BITUMINOUS SURFACE COURSE (401)<br>6" CRUSHED AGGREGATE BASE COURSE (209)  |
|  ④ | 7.5" BITUMINOUS SURFACE COURSE (401)<br>23" BITUMINOUS BASE COURSE (201)<br>6" LIME-FLYASH SUBGRADE (155)   |  ⑨ | 9" & VARIABLE DEPTH BITUMINOUS SURFACE COURSE (401)<br>3" BITUMINOUS BASE COURSE (403)<br>3" BITUMINOUS SURFACE COURSE (401)<br>6" CRUSHED AGGREGATE BASE COURSE (209)          |   |   |
|  ⑤ | 9" P.C.C. PAVEMENT (501)<br>VARIABLE DEPTH BITUMINOUS BASE COURSE (403)<br>11" P.C.C. PAVEMENT (501)  |  |   |   |   |

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| DRAWN BY:                  | CMT          |
| CHECKED BY:                | CET          |
| APPROVED BY:               | CET          |
| DATE:                      | MAY 13, 2011 |
| JOB No:                    | 100610300    |
| IL PROJ. NO. PIA-4079      |              |
| AIP PROJ. NO. 3-17-0080-XX |              |
| SHEET 12 OF 84 SHEETS      |              |

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**LEGEND**


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| <p>① 9" P.C.C. PAVEMENT (501)<br/>6" LIME-FLYASH SUBGRADE (155)</p> <p>② 14" P.C.C. PAVEMENT (501)<br/>7" BITUMINOUS BASE COURSE (403)<br/>6" LIME TREATED SUBGRADE (155)</p> <p>③ 3"-4" &amp; VARIABLE DEPTH BITUMINOUS SURFACE COURSE (401)<br/>10" P.C.C. PAVEMENT (501)</p> <p>④ 3" BITUMINOUS SURFACE COURSE (403)<br/>10" P.C.C. PAVEMENT (501)</p> | <p>⑤ 8" BITUMINOUS SURFACE COURSE (401)<br/>4" CRUSHED AGGREGATE BASE COURSE (209)</p> <p>⑥ VARIABLE DEPTH BITUMINOUS SURFACE COURSE (401)<br/>7" P.C.C. PAVEMENT (501)<br/>8" GRANULAR SUBBASE BLANKET COURSE (154)</p> <p>⑦ VARIABLE DEPTH BITUMINOUS SURFACE COURSE (401)<br/>6" P.C.C. PAVEMENT (501)<br/>6" GRANULAR SUBGRADE (154)</p> <p>⑧ 2" BITUMINOUS SURFACE COURSE (401)<br/>2" BITUMINOUS BASE COURSE (403)<br/>12" CRUSHED AGGREGATE BASE COURSE (209)</p> |
|---|--|

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**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 EXISTING PAVEMENT STRUCTURE 2**

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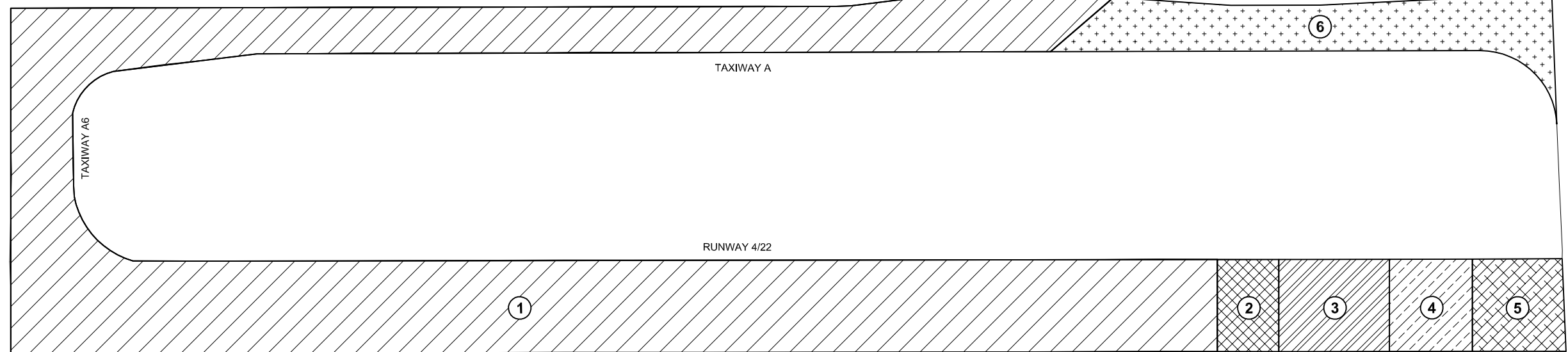
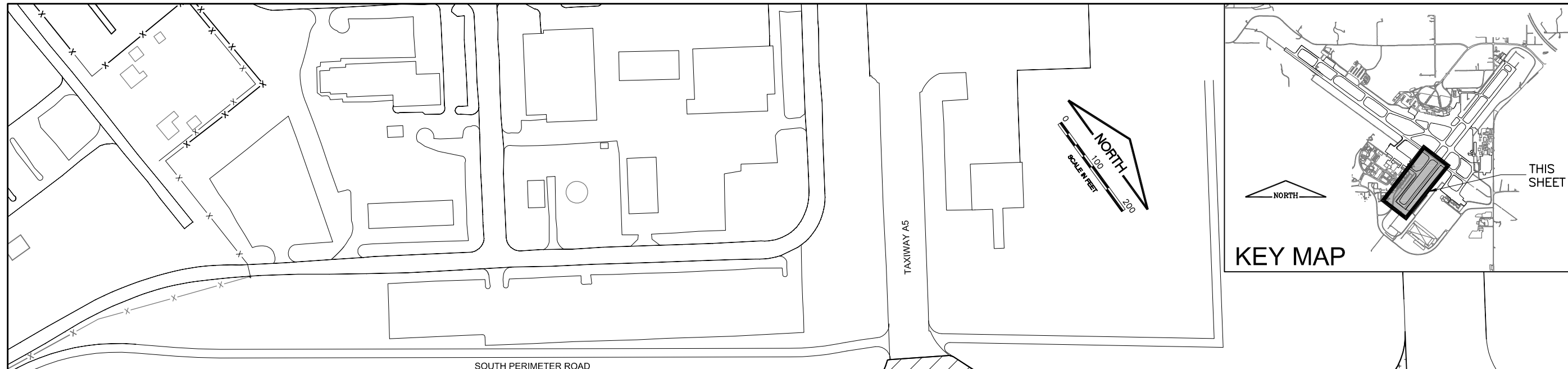
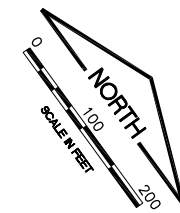
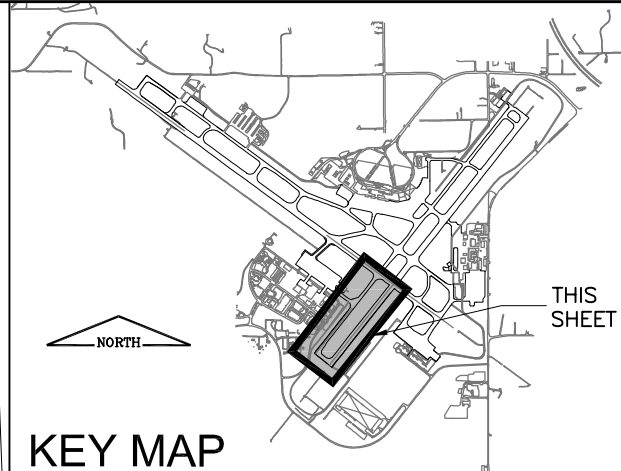
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| JOB No:       | 100610300       |
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| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 13 OF 84 SHEETS |



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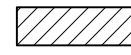
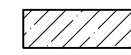
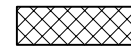
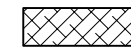
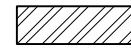
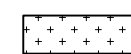
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 EXISTING PAVEMENT STRUCTURE 3**

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| IL PROJ. NO.  | PIA-4079        |
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| SHEET         | 14 OF 84 SHEETS |

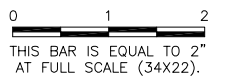
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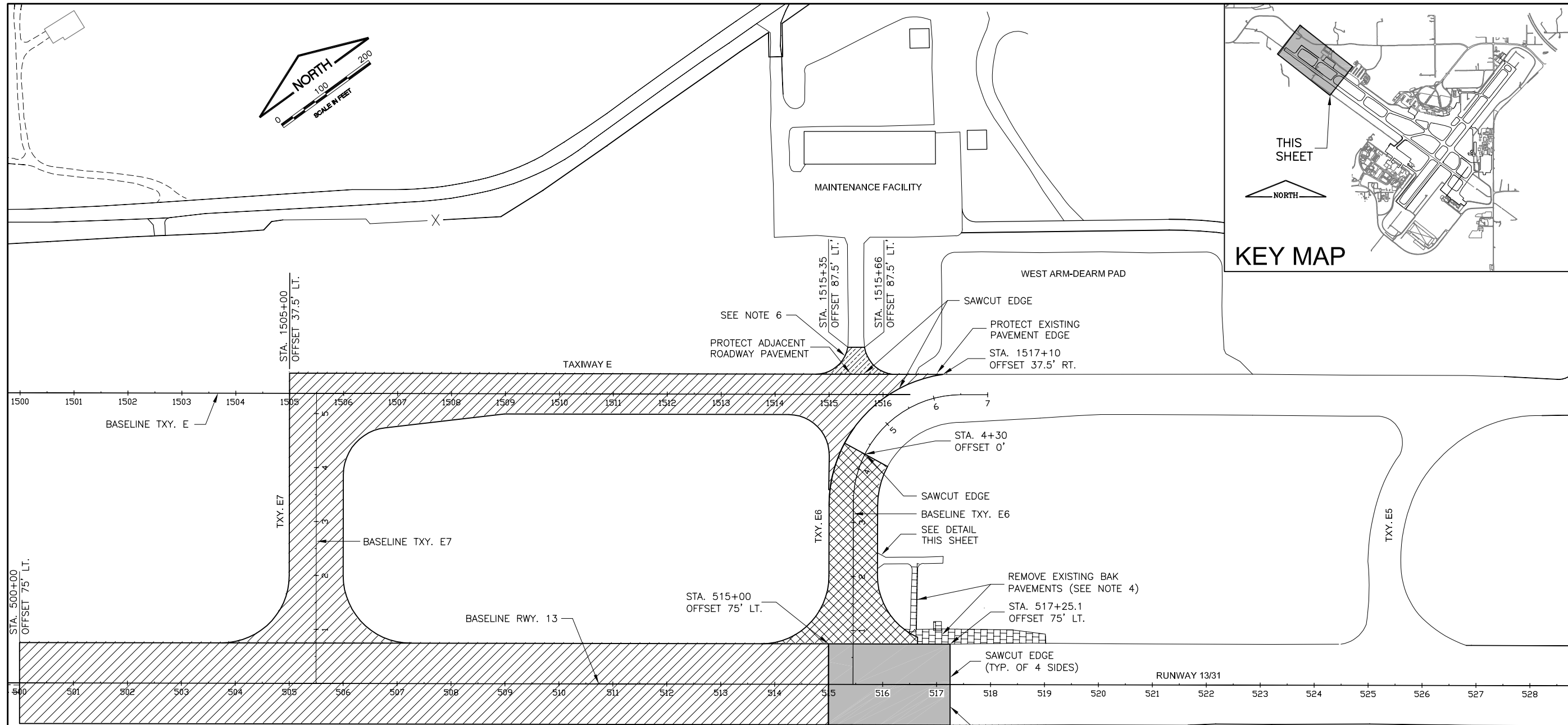
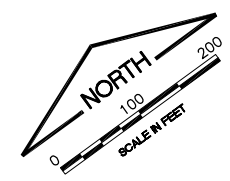
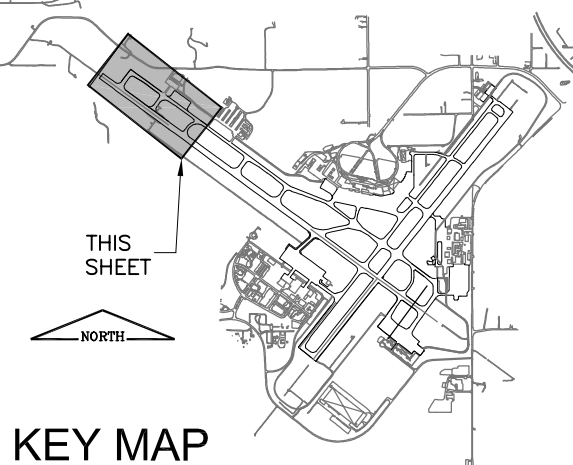
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|--|--|---|---|
|  ① | 6" BITUMINOUS SURFACE COURSE (401)<br>20" BITUMINOUS BASE COURSE (403)<br>4" CRUSHED AGGREGATE BASE COURSE (209)                                     |  ④ | 3" & VARIABLE DEPTH BITUMINOUS SURFACE COURSE (401)<br>19" & VARIABLE DEPTH BITUMINOUS BASE COURSE (403)<br>8" CRUSHED AGGREGATE BASE COURSE (209)    |
|  ② | 3" & VARIABLE DEPTH BITUMINOUS SURFACE COURSE (401)<br>8.5" & VARIABLE DEPTH BITUMINOUS BASE COURSE (403)<br>8" CRUSHED AGGREGATE BASE COURSE (209)  |  ⑤ | 10" P.C.C. PAVEMENT (501)<br>VARIABLE DEPTH BITUMINOUS BASE COURSE (403)<br>4" CRUSHED AGGREGATE BASE COURSE (209)                                    |
|  ③ | 3" & VARIABLE DEPTH BITUMINOUS SURFACE COURSE (401)<br>15.5" & VARIABLE DEPTH BITUMINOUS BASE COURSE (403)<br>8" CRUSHED AGGREGATE BASE COURSE (209) |  ⑥ | 3" BITUMINOUS SURFACE COURSE (401)<br>11" BITUMINOUS BASE COURSE (403)<br>17" CRUSHED AGGREGATE BASE COURSE (209)<br>12" LIME MODIFIED SUBGRADE (155) |

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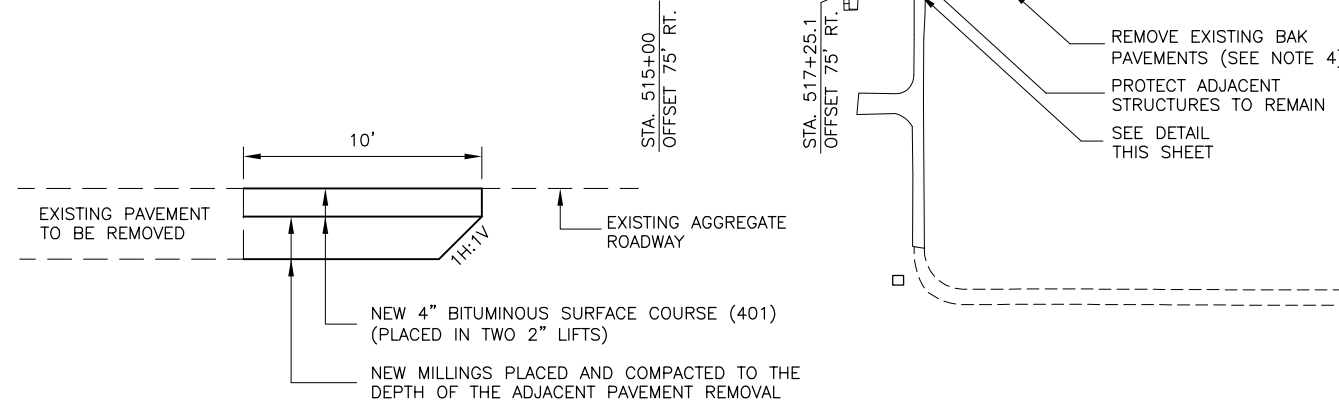


**KEY MAP**



**NOTES:**

1. WHEN REMOVING EXISTING PAVEMENT ADJACENT TO AN EXISTING STRUCTURE TO REMAIN IN PLACE, THE CONTRACTOR SHALL USE CAUTION NOT TO DAMAGE THE ADJACENT STRUCTURES. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED BY THE CONTRACTOR TO THE AIRPORT'S SATISFACTION AND ALL COSTS SHALL BE BORNE TO THE CONTRACTOR.
2. SAW CUTTING SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAVEMENT REMOVAL ITEM.
3. FULL DEPTH PAVEMENT REMOVAL INCLUDES THE REMOVAL OF ANY AND ALL COMBINATIONS OF PAVEMENT STRUCTURES, SUBBASE, AND SUBGRADE AS REQUIRED TO REMOVE THE AREA SHOWN TO THE LIMITS SHOWN IN THE PLANS. THIS WORK SHALL BE PAID FOR UNDER ITEM 800372 - REMOVE BITUMINOUS AND P.C.C. PAVEMENT.
4. THE EXISTING BAK PAVEMENTS TO BE REMOVED SHALL BE PAID FOR AS PART OF THE BAK REMOVAL PAY ITEM. SEE EXISTING CONDITIONS AND REMOVALS SHEET 1 AND THE BAK REMOVAL DETAILS SHEETS.
5. THE 4" OF BITUMINOUS SURFACE COURSE AS SHOWN IN THE "ADJACENT ROADWAY DETAIL" SHALL BE PAID FOR UNDER ITEM 401. THE EARTH EXCAVATION AND PLACEMENT AND COMPACTION OF THE MILLINGS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. THE BITUMINOUS BUTT JOINT CONSTRUCTION SHALL CONSIST OF TRANSITIONAL AND VARIABLE DEPTH MILLING. THIS ITEM SHALL BE MEASURED AND PAID FOR PER SQUARE YARD UNDER THE BITUMINOUS PAVEMENT MILLING ITEM.



**ADJACENT ROADWAY DETAIL**

**LEGEND**

|  |  |
|--|--|
|  | 3" BITUMINOUS PAVEMENT MILLING                               |
|  | FULL DEPTH PAVEMENT REMOVAL                                  |
|  | REMOVE 12" P.C.C. PAVEMENT<br>3" BITUMINOUS PAVEMENT MILLING |
|  | BUTT JOINT CONSTRUCTION                                      |
|  | BAK PAVEMENT REMOVAL   |

**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**PAVEMENT REMOVAL PLAN 1**

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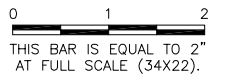
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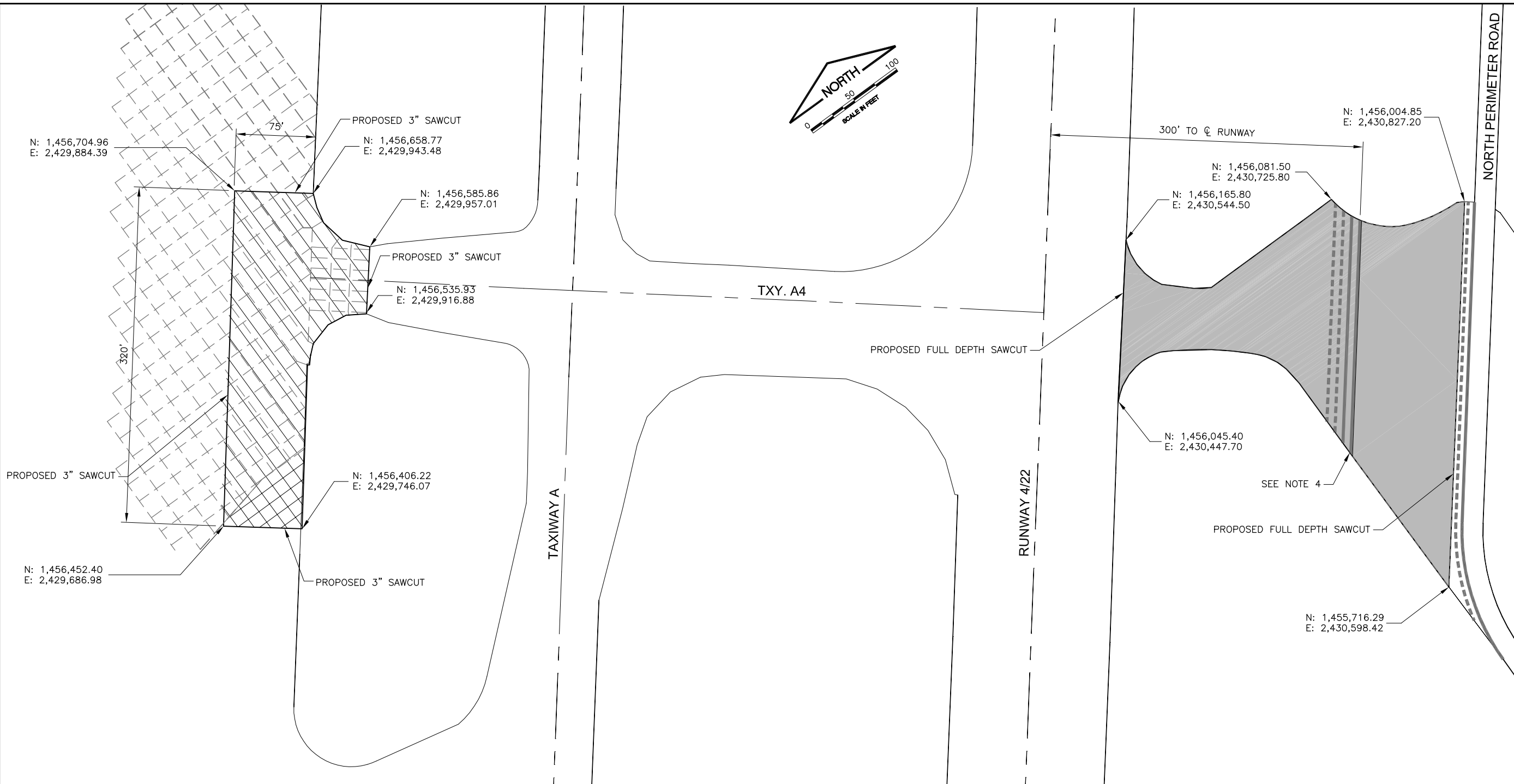
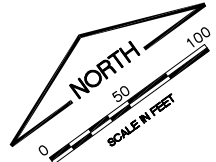
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 PAVEMENT REMOVAL PLAN 2**

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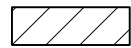
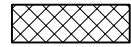

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| SHEET 16 OF 84 SHEETS                               |              |

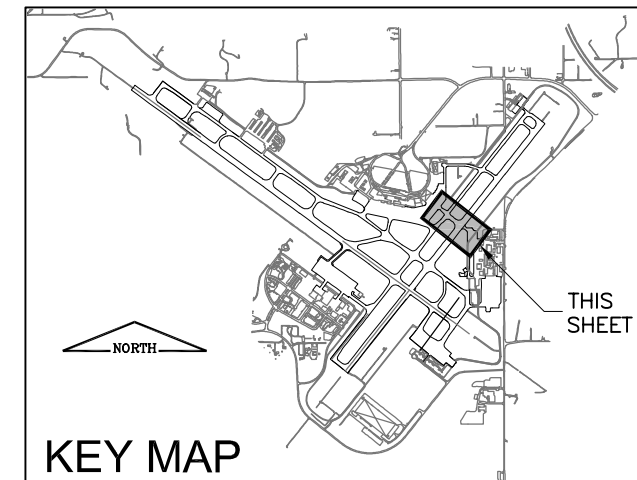


**LEGEND**

-  FULL DEPTH PAVEMENT REMOVAL
-  3" PCC PAVEMENT MILLING
-  3" BITUMINOUS PAVEMENT MILLING
-  EXISTING PAVEMENT JOINTING

**NOTES:**

- WHEN REMOVING EXISTING PAVEMENT ADJACENT TO AN EXISTING STRUCTURE TO REMAIN IN PLACE, THE CONTRACTOR SHALL USE CAUTION NOT TO DAMAGE THE ADJACENT STRUCTURES. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED BY THE CONTRACTOR TO THE AIRPORT'S SATISFACTION AND ALL COSTS SHALL BE BORNE TO THE CONTRACTOR.
- SAW CUTTING SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAVEMENT REMOVAL ITEM.
- FULL DEPTH PAVEMENT REMOVAL INCLUDES THE REMOVAL OF ANY AND ALL COMBINATIONS OF PAVEMENT STRUCTURES, SUBBASE, AND SUBGRADE AS REQUIRED TO REMOVE THE AREA SHOWN TO THE LIMITS SHOWN IN THE PLANS.
- THIS AREA CONSISTS OF VARIOUS PAVEMENT STRUCTURES. ALL REMOVALS IN THIS AREA SHALL BE CONSIDERED PART OF THE FULL DEPTH PAVEMENT REMOVAL AND NO SEPARATE PAYMENT WILL BE MADE FOR EACH INDIVIDUAL PAVEMENT STRUCTURE. ALL PAVEMENT REMOVAL IN THIS AREA SHALL BE PAID FOR UNDER ITEM 401921 - REMOVE PAVEMENT. ALL COSTS TO OBTAIN, PLACE, AND COMPACT SELECT BORROW MATERIAL TO THE LIMITS AND GRADES SHOWN ON THE GRADING AND STABILIZATION PLAN 1 SHALL BE CONSIDERED PART OF THE LUMP SUM UNCLASSIFIED EXCAVATION PAY ITEM. NO SEPARATE PAYMENT SHALL BE MADE FOR BORROW MATERIAL.

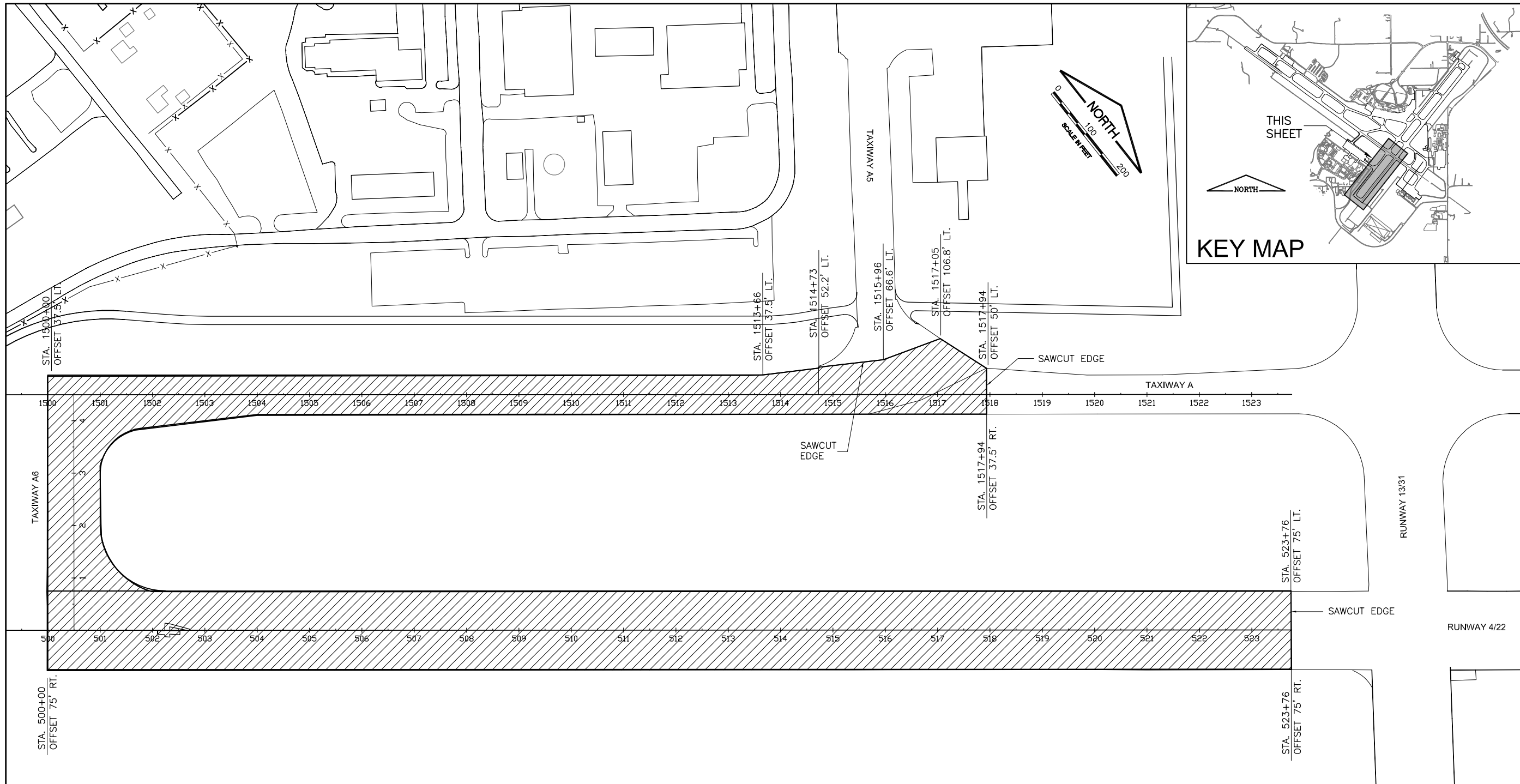
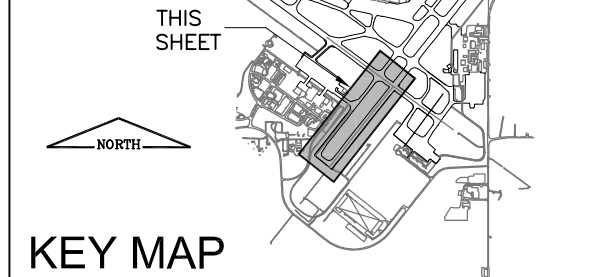




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0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).



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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 PAVEMENT REMOVAL PLAN 3**



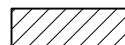
Know what's below.  
 Call before you dig.

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THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

**CALL 911 IN THE EVENT IN WHICH DAMAGE  
 RESULTS IN THE RELEASE OF NATURAL GAS.**

**LEGEND**

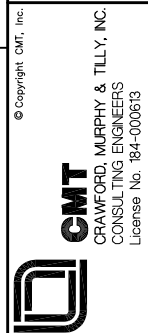


3" BITUMINOUS PAVEMENT MILLING

**NOTES:**

1. WHEN REMOVING EXISTING PAVEMENT ADJACENT TO AN EXISTING STRUCTURE TO REMAIN IN PLACE, THE CONTRACTOR SHALL USE CAUTION NOT TO DAMAGE THE ADJACENT STRUCTURES. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED BY THE CONTRACTOR TO THE AIRPORT'S SATISFACTION AND ALL COSTS SHALL BE BORNE TO THE CONTRACTOR.
2. SAW CUTTING SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAVEMENT REMOVAL ITEM.

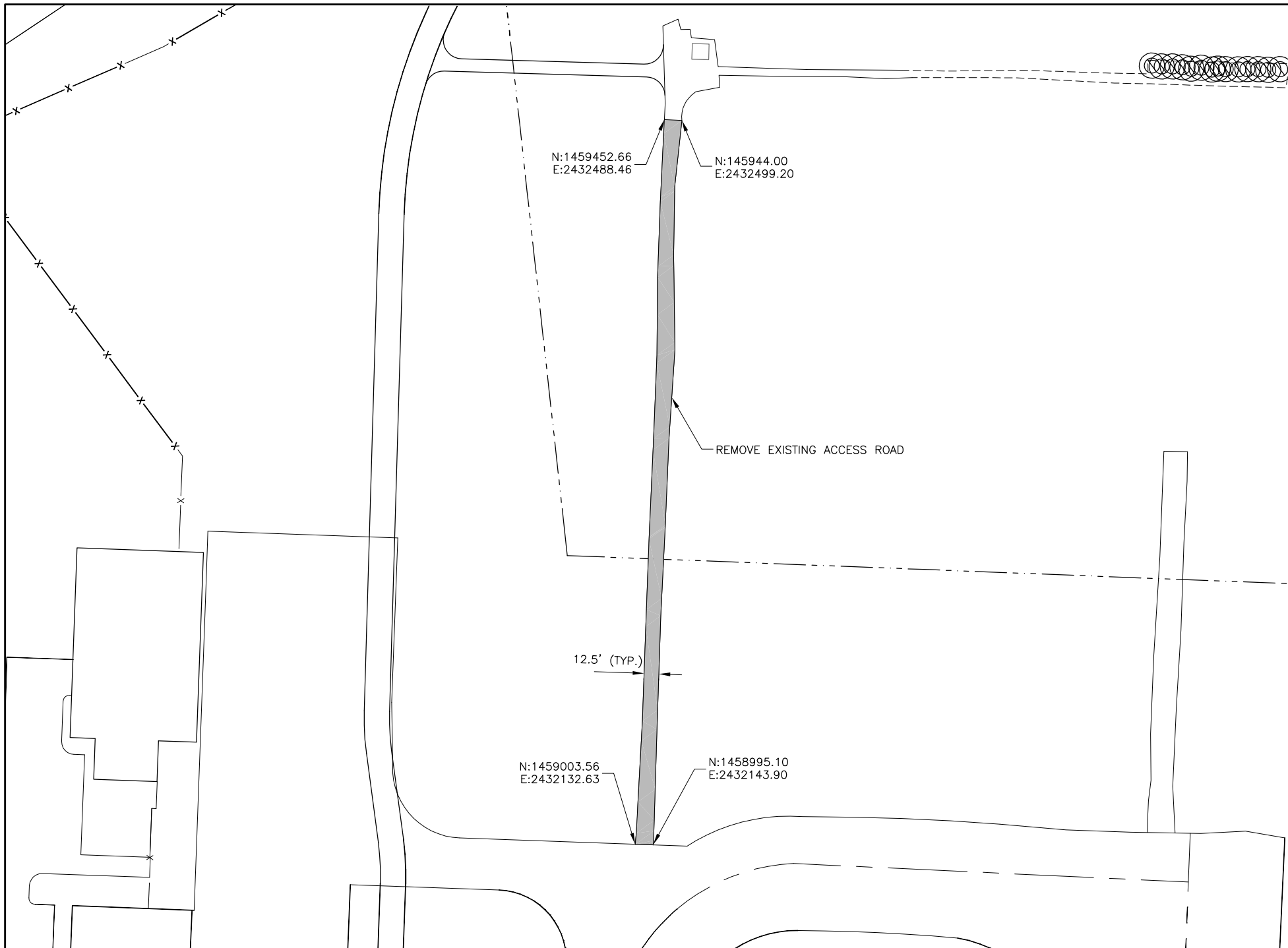
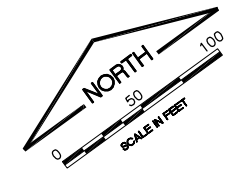
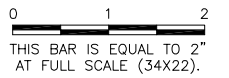
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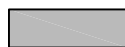
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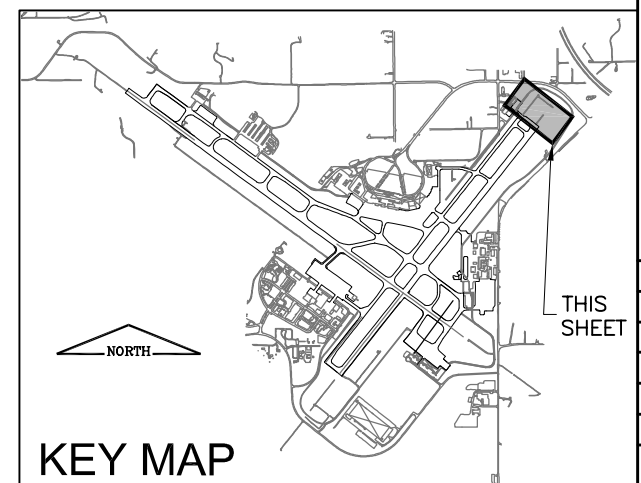
**LEGEND**



FULL DEPTH PAVEMENT REMOVAL

**NOTES:**

1. WHEN REMOVING EXISTING PAVEMENT ADJACENT TO AN EXISTING STRUCTURE TO REMAIN IN PLACE, THE CONTRACTOR SHALL USE CAUTION NOT TO DAMAGE THE ADJACENT STRUCTURES. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED BY THE CONTRACTOR TO THE AIRPORT'S SATISFACTION AND ALL COSTS SHALL BE BORNE TO THE CONTRACTOR.
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4. PAVEMENT REMOVAL AS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER ITEM AR801211 - ROADWAY REMOVAL. PAYMENT FOR THIS ITEM SHALL BE PER SQUARE YARD. ALL WORK ASSOCIATED WITH GRADING AND COMPACTING SHALL BE CONSIDERED PART OF THE UNCLASSIFIED EXCAVATION PAY ITEM.
5. IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE ALL UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS. ANY UTILITIES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED IMMEDIATELY BY THE CONTRACTOR AT HIS COST AND TO THE SATISFACTION OF THE ENGINEER.



**KEY MAP**

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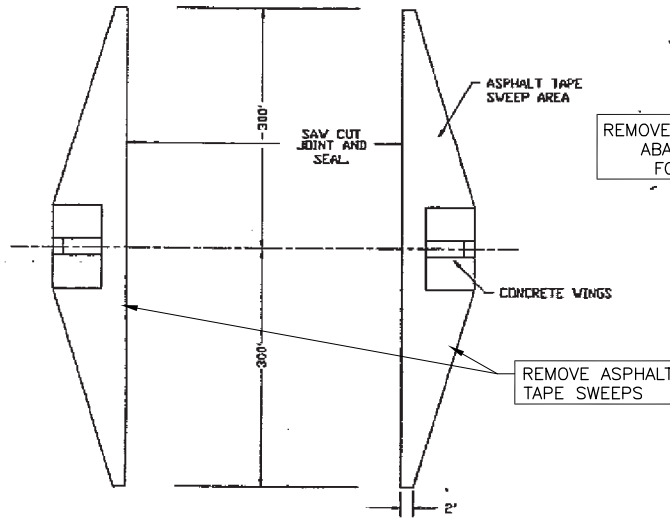
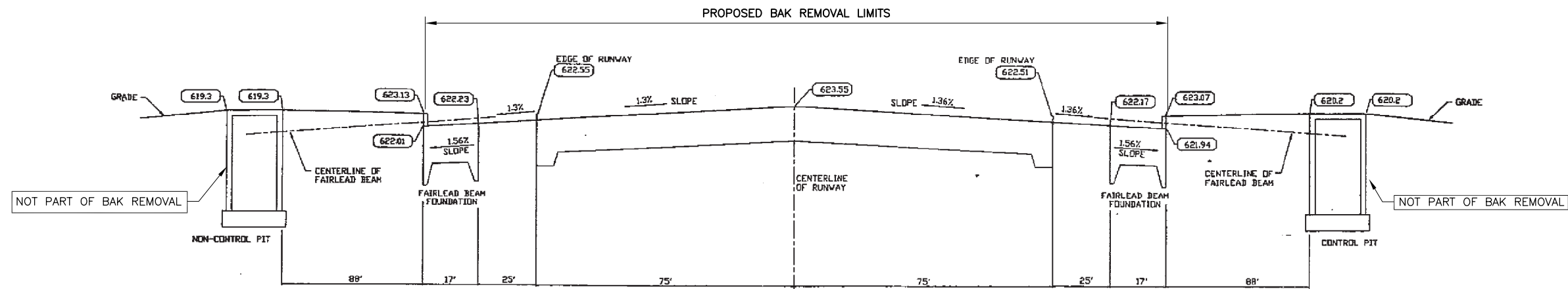
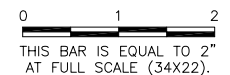
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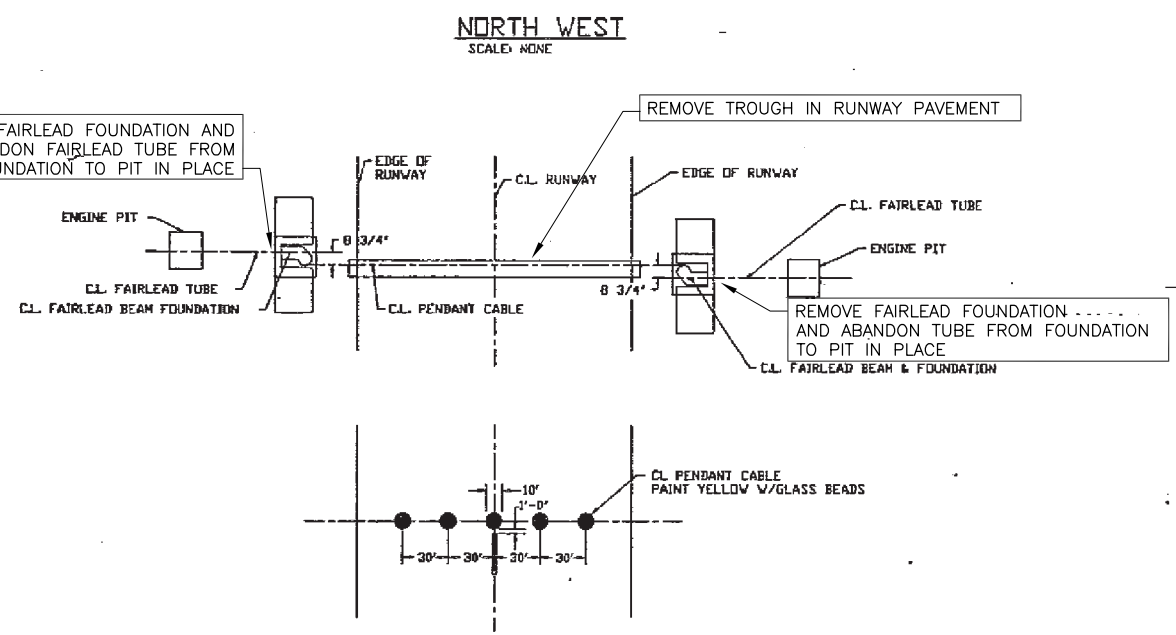
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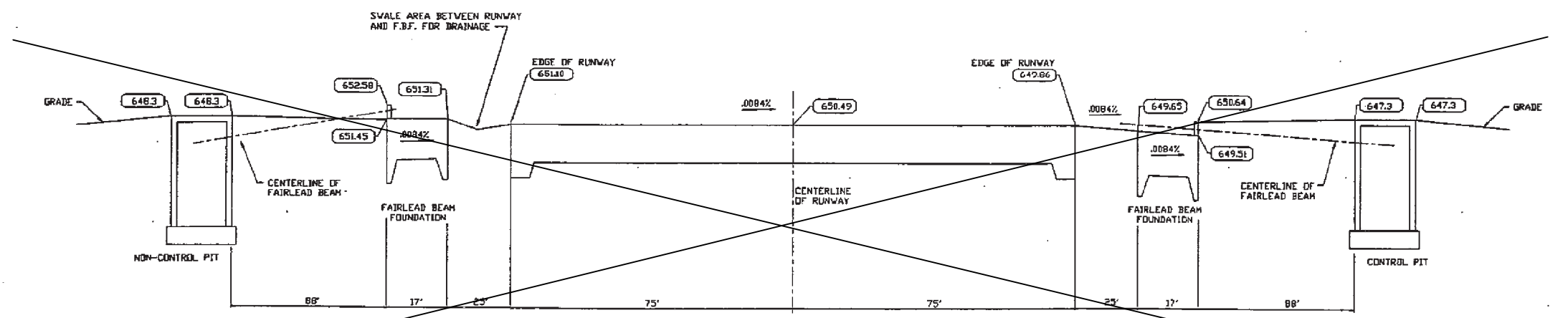
**TAPE SWEEP SIZE**  
SCALE: NONE



**LOCATION DIAGRAM**  
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**SOUTH EAST**  
SCALE: NONE

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SCALE  
 1"=40'  
 1"=80'  
 1"=160'

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 Peoria, IL 61614  
 (309) 673-1111  
 (309) 673-1112  
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 CHECKED BY: O.D.O.  
 APPROVED BY: O.D.O.

**GREATER PEORIA AIRPORT AUTHORITY**  
 PEORIA, ILLINOIS

**ELEVATION SECTIONS**



SCALE AS NOTED / JOB NO.  
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**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
 PEORIA, ILLINOIS

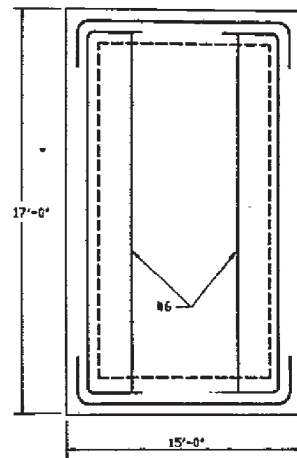
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**BAK REMOVAL DETAILS 1**

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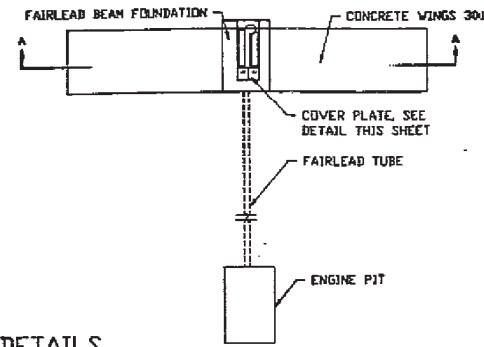
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 AIP PROJ. NO. 3-17-0080-XX



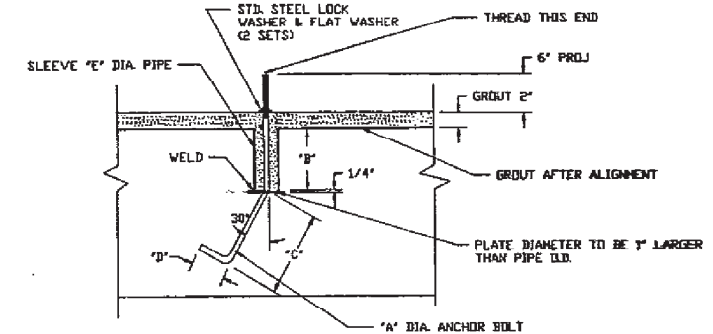
PLAN VIEW  
SCALE: NONE



SECTION A-A  
SCALE: NONE



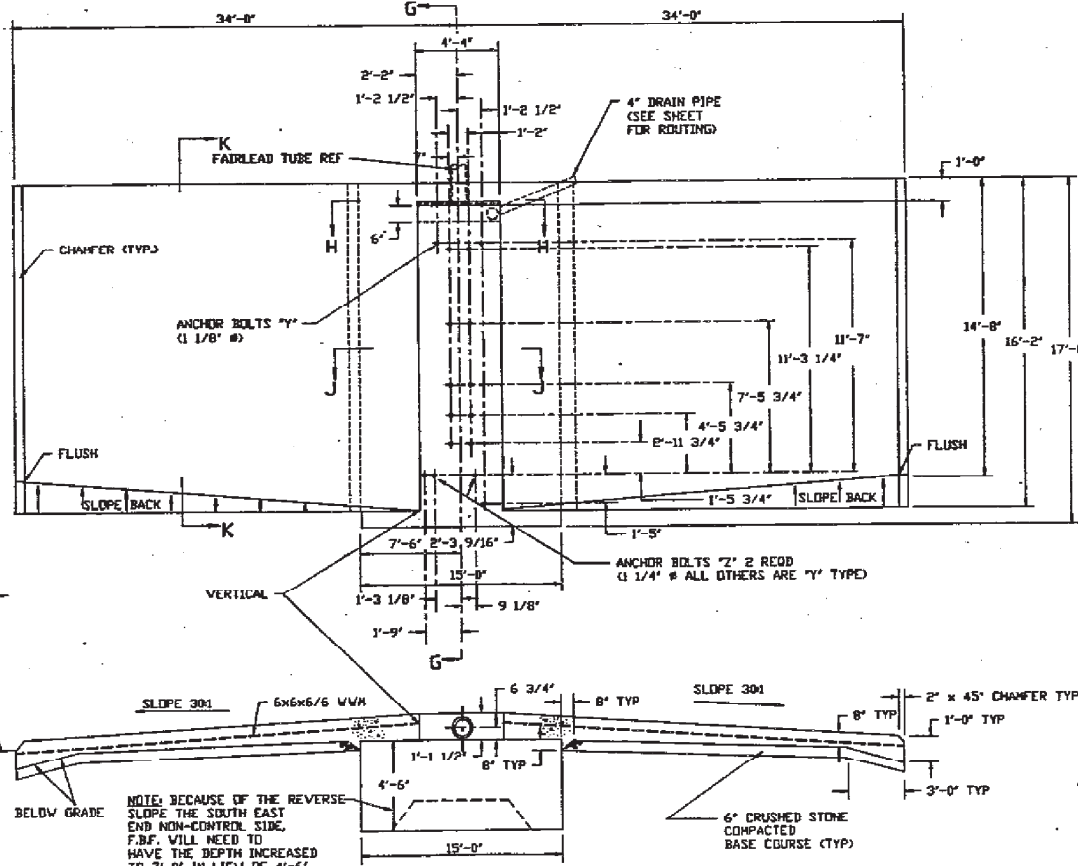
WEST END TYPICAL DETAILS  
(BOTH SIDES)  
SCALE: NONE



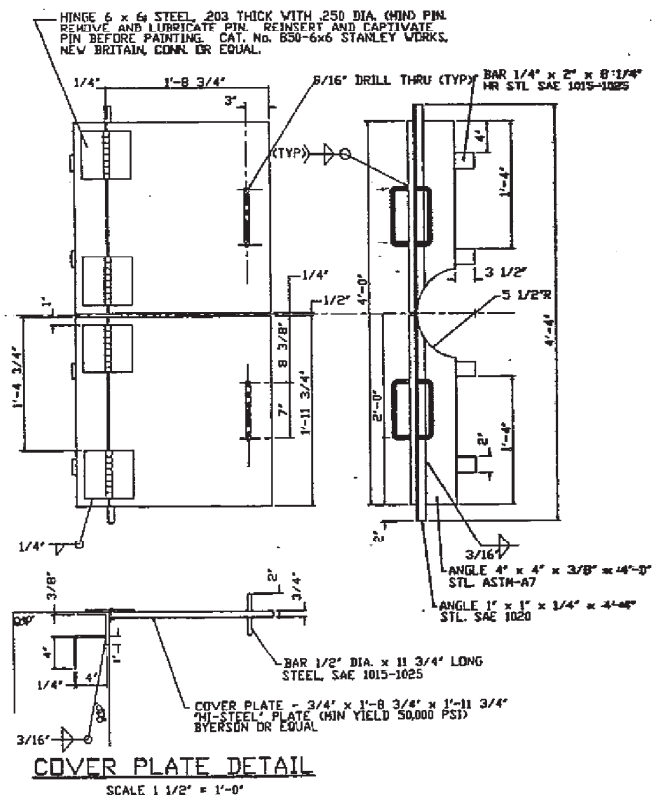
NOTES:  
 CONTRACTOR IS RESPONSIBLE TO VERIFY ANCHOR BOLT PATTERN OF EACH FAIRLEAD BEAM AND MAKE ADJUSTMENTS AS NECESSARY.

ANCHOR BOLT & FAIRLEAD BEAM DETAIL (TYP)  
SCALE: NONE

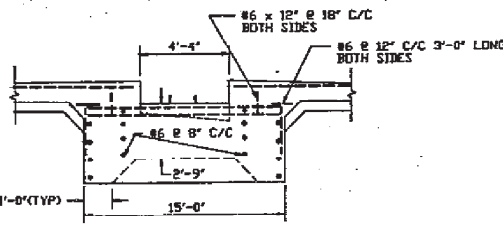
| ANCHOR BOLT SCHEDULE |        |       |       |     |     |               |
|----------------------|--------|-------|-------|-----|-----|---------------|
| BOLT                 | "A"    | "B"   | "C"   | "D" | "E" | PROJ QUANTITY |
| "1"                  | 1 1/8" | 1'-2" | 10'   | 4"  | 2"  | 56            |
| "2"                  | 1 1/4" | 2'-0" | 1'-0" | 5"  | 3"  | 8             |



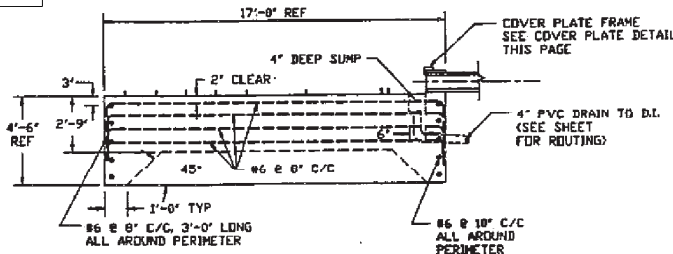
TYPICAL FAIRLEAD BEAM FOUNDATION  
VIEW FROM RUNWAY  
SCALE: NONE



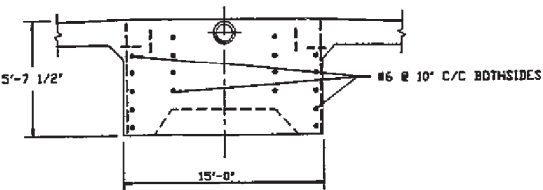
COVER PLATE DETAIL  
SCALE 1/2" = 1'-0"



SECTION J-J  
SCALE: NONE



SECTION G-G  
SCALE: NONE



SECTION H-H  
SCALE: NONE

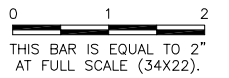
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GENERAL WAYNE A. DOWNING  
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 PEORIA, ILLINOIS

REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 BAK REMOVAL DETAILS 2

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GENERAL WAYNE A. DOWNING  
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SHEET 20 OF 84 SHEETS

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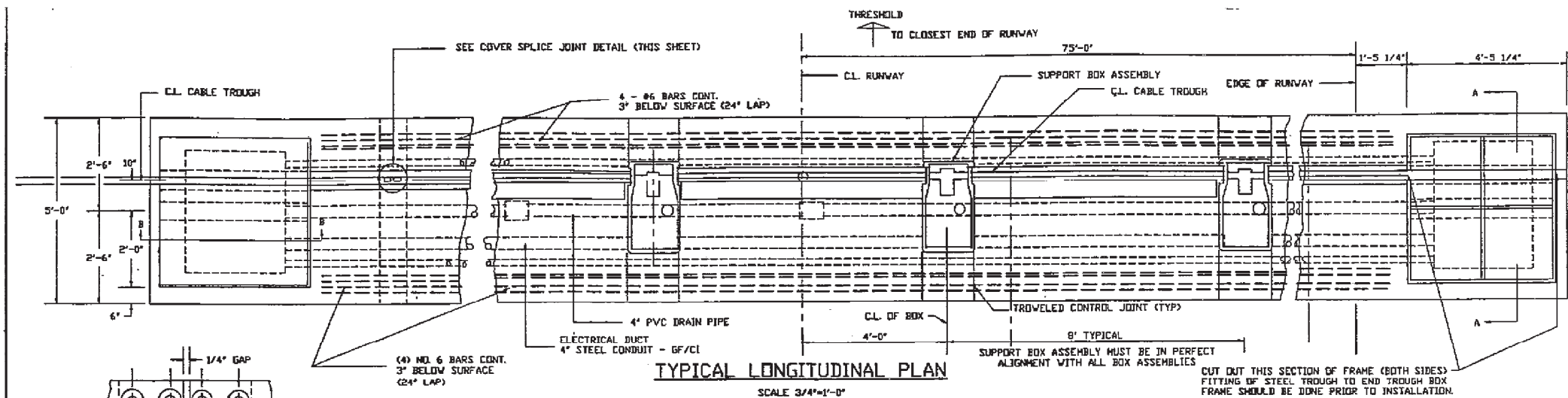
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 APPROVED BY: 0.0.0  
 GREATER PEORIA AIRPORT AUTHORITY  
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FAIRLEAD BEAM FOUNDATION DETAILS

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 SHEET 16 OF 150 SHEETS





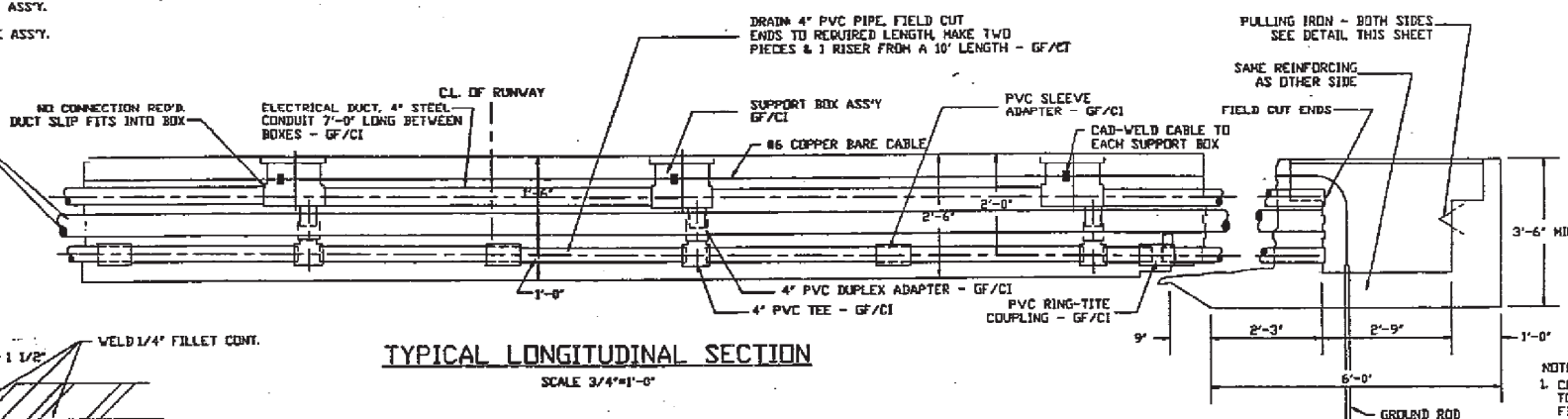
**REMOVAL NOTES:**

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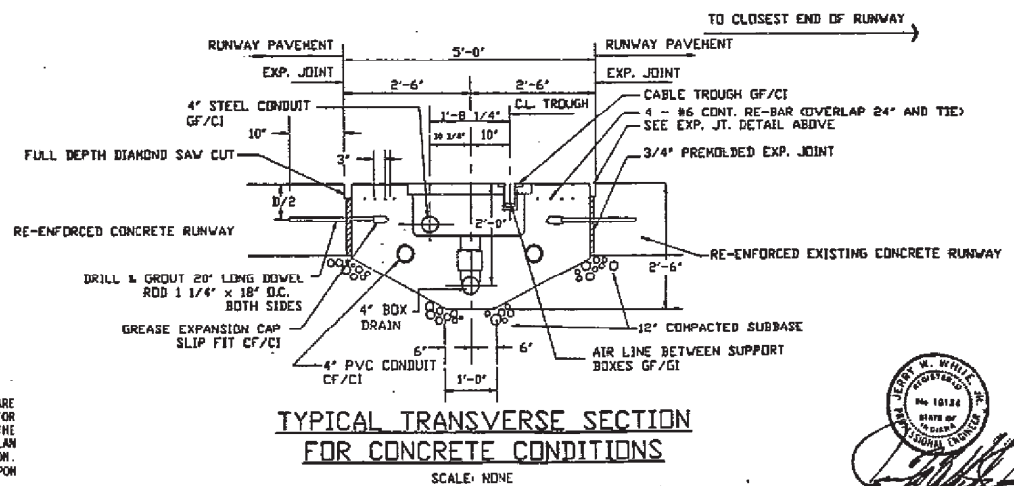
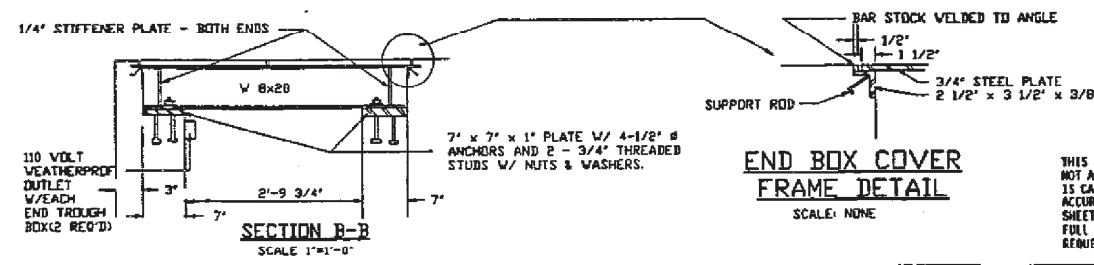
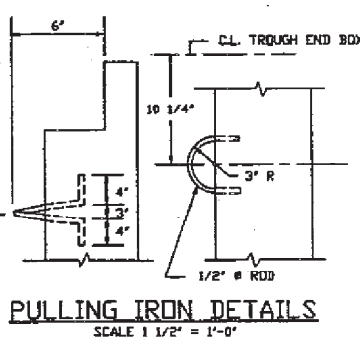
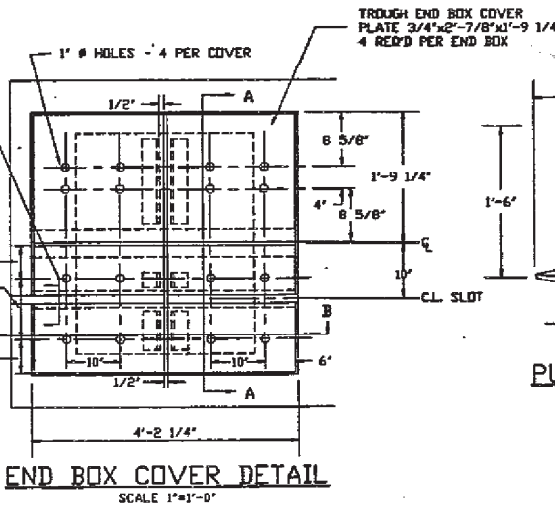
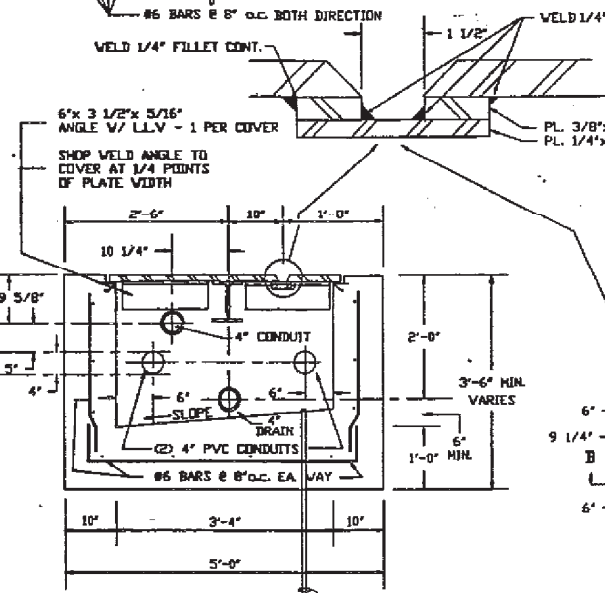
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0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



- NOTES:**
1. CONTRACTOR SHALL PROVIDE AND PLACE WEDGES IN TROUGH SECTION TO PREVENT TROUGH DISTORTION OR COLLAPSE DURING PLACING AND FINISHING OF CONCRETE.
  2. CONTRACTOR SHALL GREASE AND TAPE ALL BOLT HOLES, ENTIRE TROUGH SUPPORT BOXES AND ALL JOINTS PRIOR TO PLACING CONCRETE.
  3. CONTRACTOR TO REMOVE AIR LINES, HEATERS, AND TROUGH COVERS BEFORE INSTALLATION OF CABLE TROUGH.
  4. CONTRACTOR TO TAPE AND COVER SUPPORT BOXES AND LIDS DURING CONCRETE PLACEMENT.
  5. CONTRACTOR TO PROVIDE THREADED ENDS ON ALL CONDUITS.
  6. ALL EQUIPMENT SHALL BE FIRMLY ANCHORED TO ASSURE NO MOVEMENT DURING CONCRETE PLACEMENT.
  7. CONTRACTOR SHALL WORK AND MONITOR CONCRETE TO ASSURE THAT CONCRETE IS WORKED UNDER THE TROUGH AND AROUND ALL ITEMS UNTIL CONC. BEGINS TO DRAW AWAY FROM METAL. NO VOIDS WILL BE ACCEPTED.
  8. ALL STEEL SHALL HAVE FACTORY PRIME COAT.
  9. THE CONCRETE MUST BE POURED PERFECTLY LEVEL FROM SIDE TO SIDE, WITH A 14" STRAIGHT EDGE THERE SHOULD NOT BE ANY MORE THAN 1/8" DEVIATION ACROSS SURFACE.
  10. EMPTY CONDUITS, DRANS AND RACEWAYS HAVE PULL STRINGS INSTALLED.



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**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**BAK REMOVAL DETAILS 3**

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| DATE:        | MAY 13, 2011 |
| JOB No:      | 100610300    |

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 CONSULTING ENGINEERS  
 3000 S.W. 10TH AVE., SUITE 100  
 MIAMI, FL 33135

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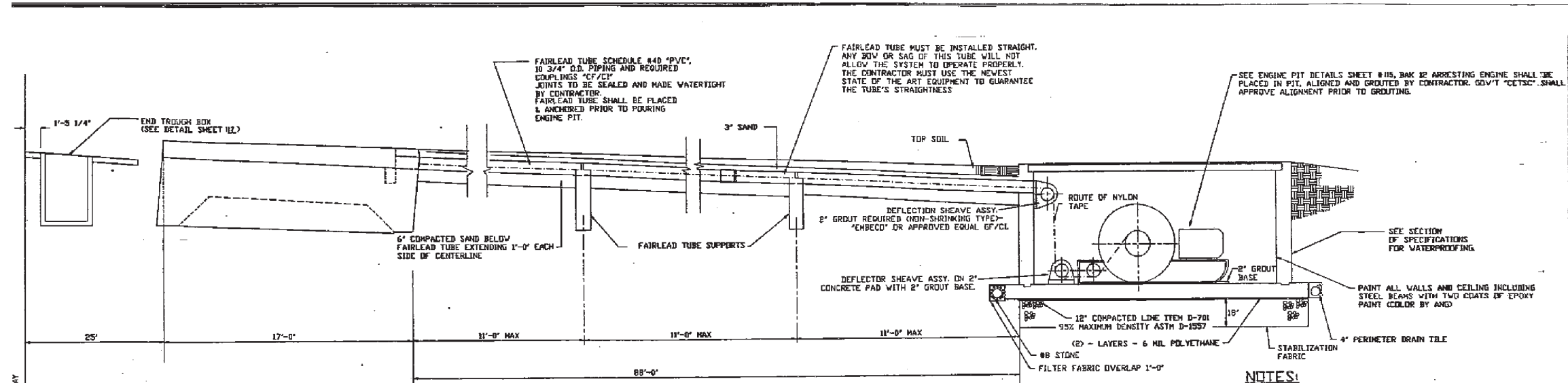
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SCALE AS NOTED  
 JOB NO. 100610300  
 DATE 0/0/1999 0000-00  
 SHEET 117 OF 130 SHEETS

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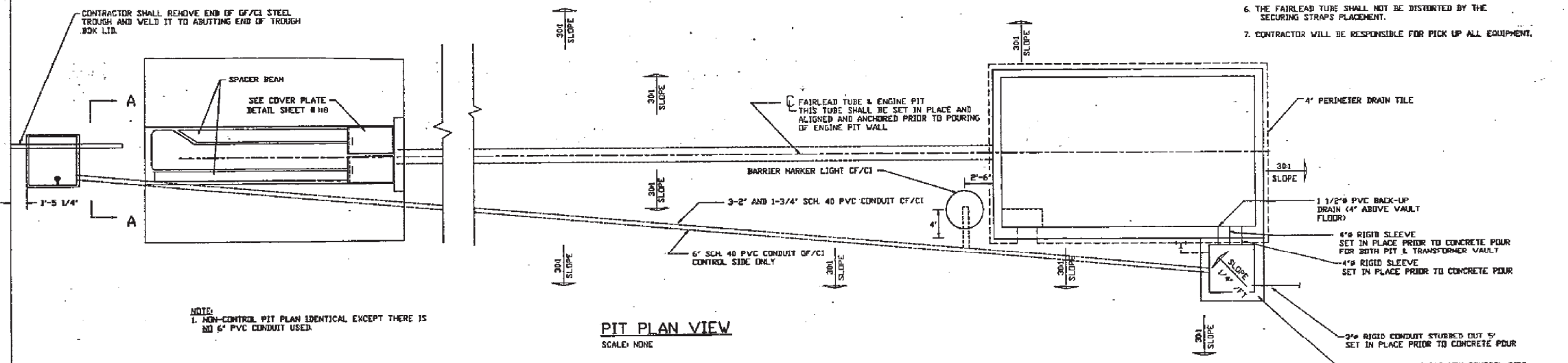
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 SCALE: NONE

**THIS SHEET FOR INFORMATIONAL PURPOSES ONLY. FAIRLEAD TUBE AND ENGINE PIT TO REMAIN IN PLACE**

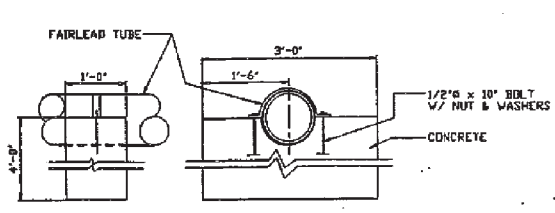
- NOTES:**
- CONTRACTOR SHALL INJECT EPOXY EXPANSION PRODUCT IN ALL OPENINGS IN PITS AND HANDHOLES FOR WATERTIGHT JOINTS.
  - ALL OPEN TRENCHING TO BE BACKFILLED WITH SELECT MATERIAL AND COMPACTED TO 95% MAXIMUM DENSITY AT OPTIMUM MOISTURE CONTENT. COMPACTION SHALL BE IN ACCORDANCE WITH P-152.
  - ALL CONDUIT SHALL BE ENCASED WHEN RUNNING UNDER ASPHALT SHOULDERS.
  - CONTRACTOR SHALL PLACE ENGINE BRAKING ASSEMBLY, FAIRLEAD BEAM AND DEFLECTOR SHEAVES. CETSC UNIT WILL APPROVE ALIGNMENT. CONTRACTOR WILL THEN GROUT WITH A NON-METALLIC NON-SHRINK GROUT.
  - CETSC PERSONNEL ARE GOV'T PEOPLE WHICH WILL PERFORM CERTAIN FUNCTIONS ON THIS PROJECT. AS SPELLED OUT IN SPECIFICATIONS.
  - THE FAIRLEAD TUBE SHALL NOT BE DISTORTED BY THE SECURING STRAPS PLACEMENT.
  - CONTRACTOR WILL BE RESPONSIBLE FOR PICK UP ALL EQUIPMENT.

**REMOVAL NOTES:**

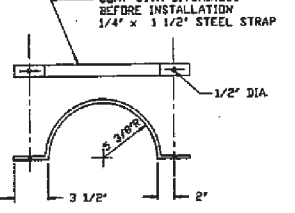
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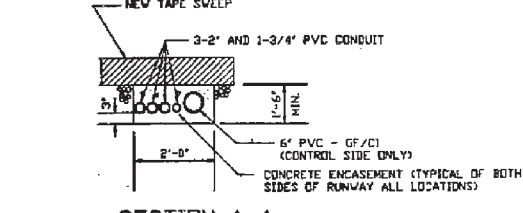
**PIT PLAN VIEW**  
 SCALE: NONE



**FAIRLEAD TUBE SUPPORT**  
 SCALE 1"=1'-0"



**STRAP DETAIL**  
 SCALE: NONE



**SECTION A-A ELECTRICAL DUCT ENCASEMENT**  
 SCALE 3/4"=1'-0"

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|  |  | CMT<br>CRAWFORD, MURPHY & TILLY, INC.<br>CONSULTING ENGINEERS<br>License No. 184-000613 | DESIGN BY: 0.0.0                     | GREATER PEORIA AIRPORT AUTHORITY<br>PEORIA, ILLINOIS | PIT SECTION & PLAN VIEWS | SCALE AS NOTED<br>DATE 0/0/1990<br>SHEET 216 OF 230 SHEETS |
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GENERAL WAYNE A. DOWNING  
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 REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 BAK REMOVAL DETAILS 4

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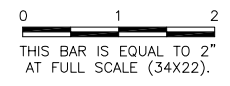
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| JOB No:       | 100610300       |
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| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 22 OF 84 SHEETS |



**THIS SHEET FOR INFORMATIONAL PURPOSES ONLY. ENGINE PIT TO REMAIN IN PLACE**

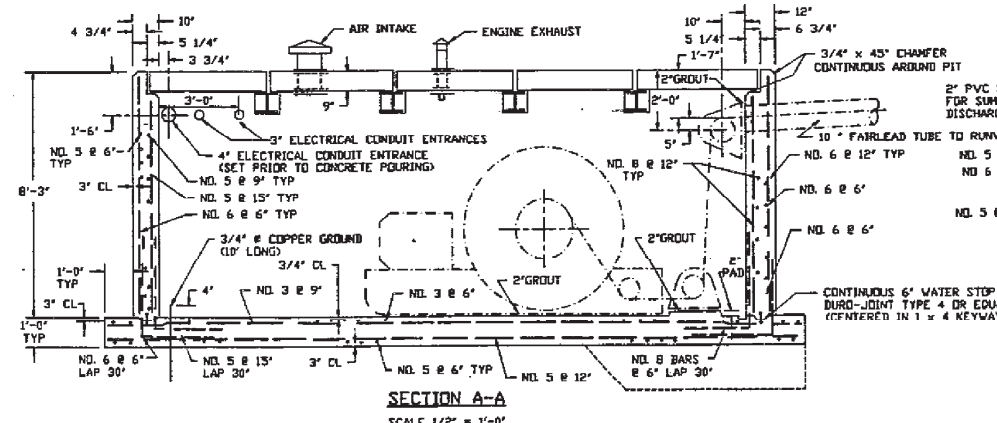
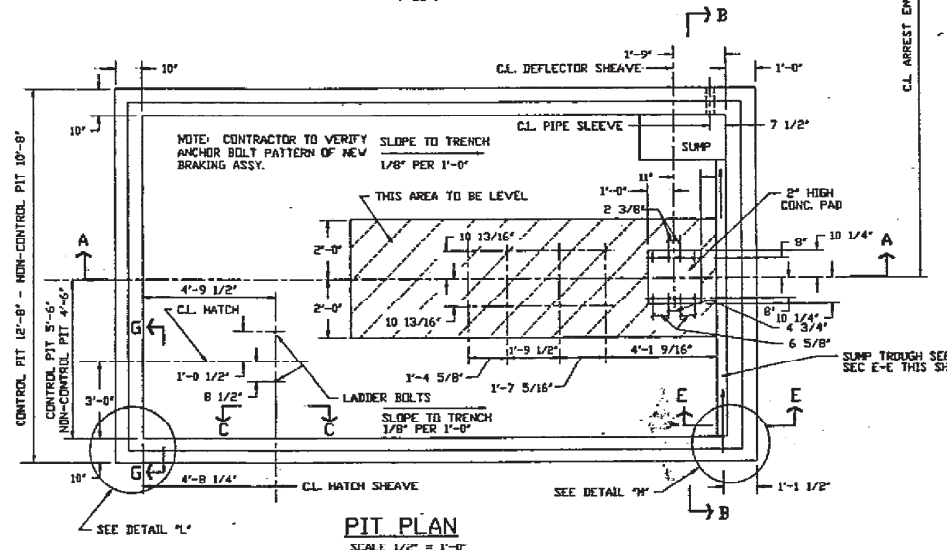
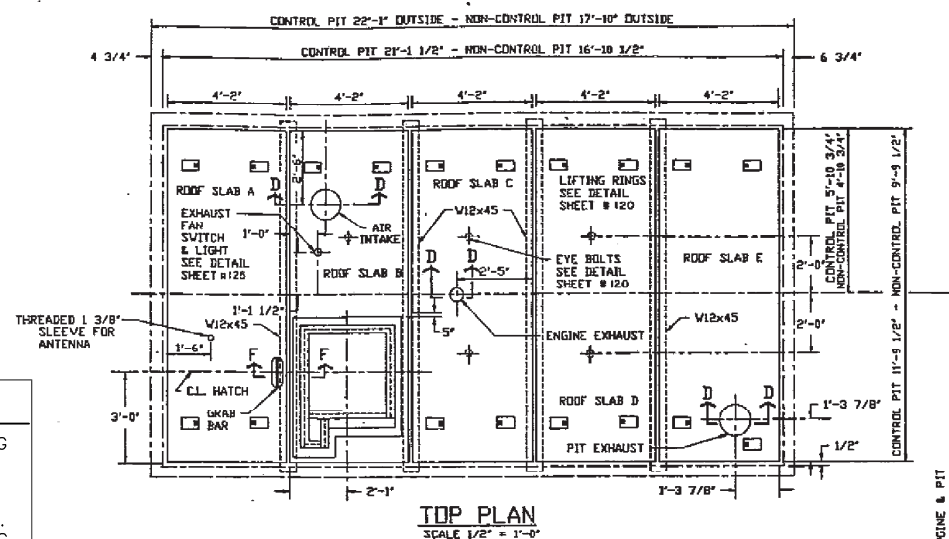
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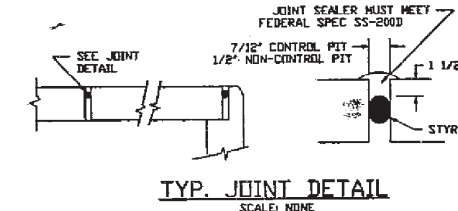
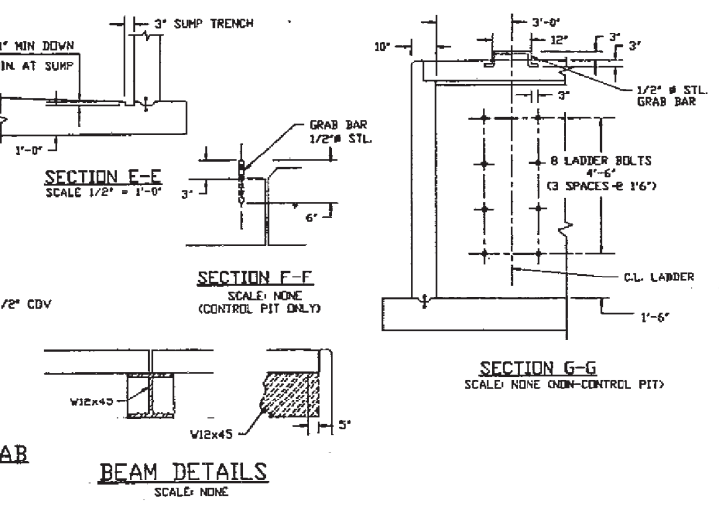


**REMOVAL NOTES:**

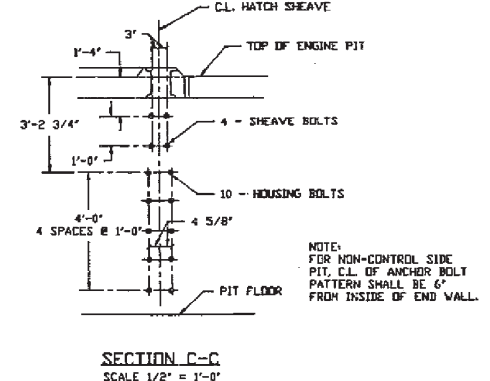
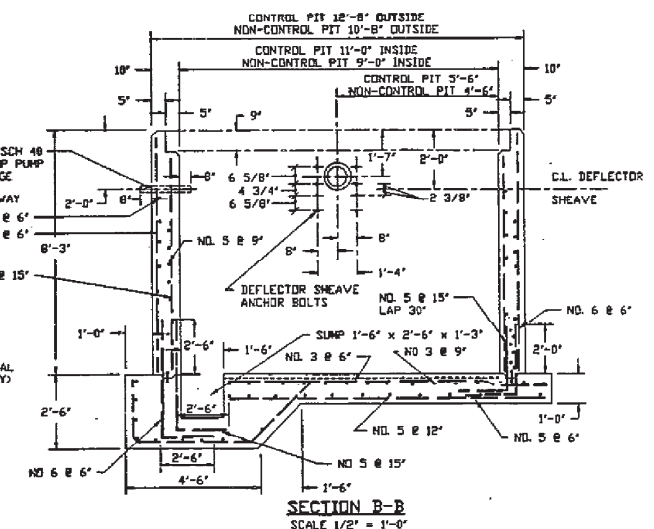
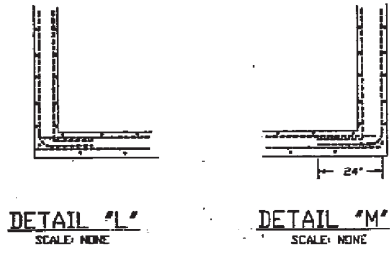
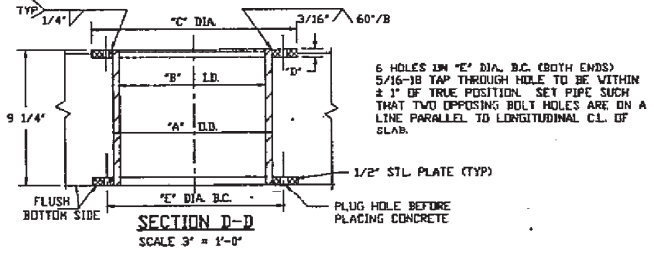
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- NOTES:**
1. THE NON-CONTROL PIT SHALL BE CONSTRUCTED IDENTICALLY AS THE CONTROL PIT EXCEPT OVERALL LENGTH IS 4'-3\"/>
  2. SECTION F-F IS FOR THE CONTROL PIT, SINCE ROOF SLAB 'A' IS DELETED FROM THE NON-CONTROL PIT AND THE OUTSIDE WALL ABUTS ROOF SLAB 'B' THE GRAB BAR SHALL BE INSERTED IN THE OUTSIDE WALL IN THE SAME LOCATION AS SHOWN PER SECTION F-F.
  3. INTERIOR WALLS OF PIT SHALL BE RUBBED SMOOTH AND FREE OF ANY HOLES OR HONEYCOMB.
  4. CONCRETE FLOOR SHALL HAVE A SMOOTH METAL TROVELED FINISH.



| HOLE             | PIPE SIZE                            | A      | B      | C       | D    | E      |
|------------------|--------------------------------------|--------|--------|---------|------|--------|
| FRESH AIR INTAKE | 10" DIA. SCH. 30 SEAMLESS STEEL PIPE | 10.750 | 10.136 | 15-3/4" | 1/2" | 12"    |
| AIR EXHAUST      | 10" DIA. SCH. 30 SEAMLESS STEEL PIPE | 10.750 | 10.136 | 15-3/4" | 1/2" | 12"    |
| ENGINE EXHAUST   | 4" DIA. SCH. 30 SEAMLESS STEEL PIPE  | 4.50   | 4.026  | 9-1/2"  | 1/2" | 5-3/4" |



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 PEORIA, ILLINOIS

ENGINE PIT DETAILS



SCALE AS NOTED JOB NO. 10000-000  
 DATE 0/0/1991  
 SHEET 115 OF 130 SHEETS

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 DRAWN BY: CMT  
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 APPROVED BY: CET  
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 IL PROJ. NO. PIA-4079  
 AIP PROJ. NO. 3-17-0080-XX  
 SHEET 23 OF 84 SHEETS

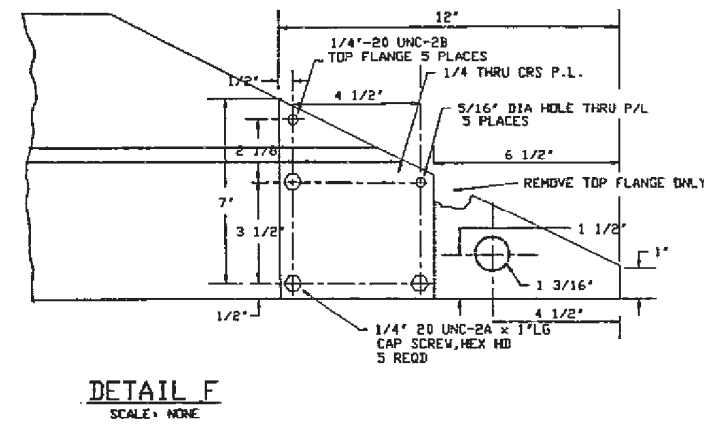
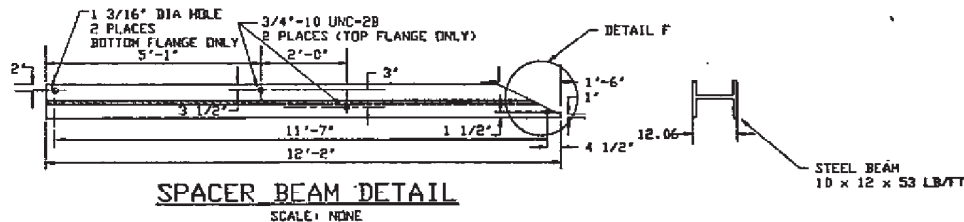
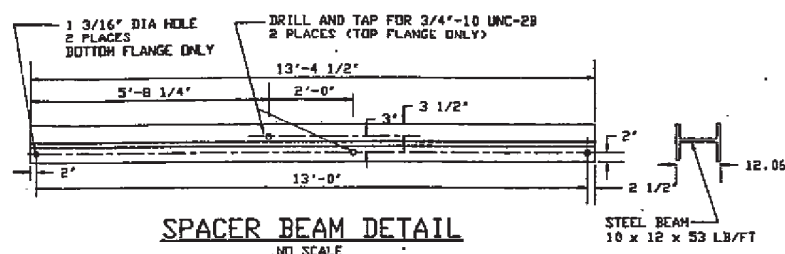
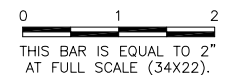
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REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 BAK REMOVAL DETAILS 5

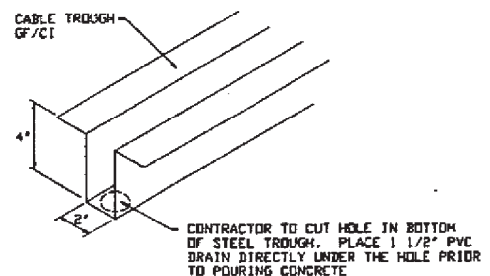
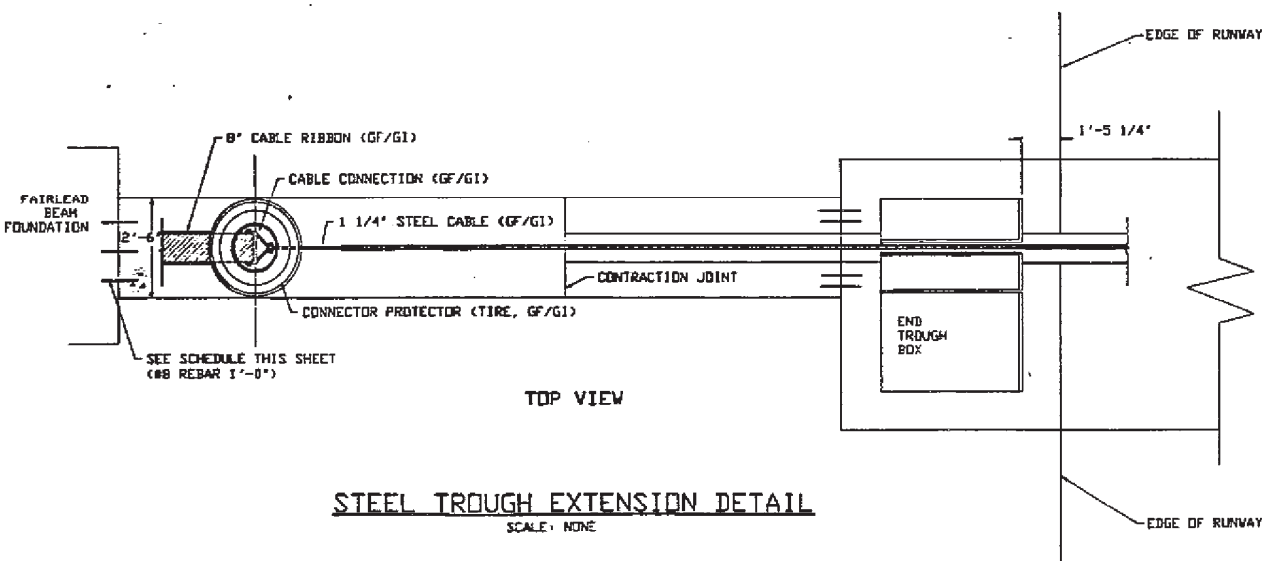
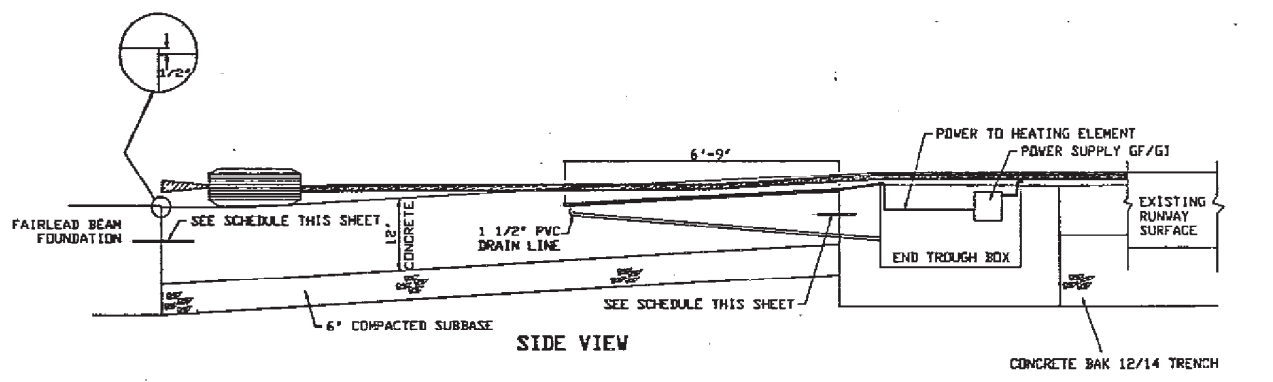
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**STEEL TROUGH SECTION**  
SCALE: NONE

**STEEL TROUGH EXTENSION DETAIL**  
SCALE: NONE

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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**BAK REMOVAL DETAILS 6**

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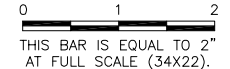
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| CHECKED BY:  | CET          |
| APPROVED BY: | CET          |
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| JOB No:      | 100610300    |

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**PEORIA, ILLINOIS**  
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**BAK REMOVAL DETAILS 7**

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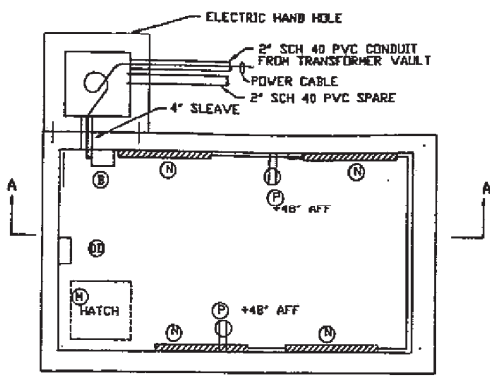
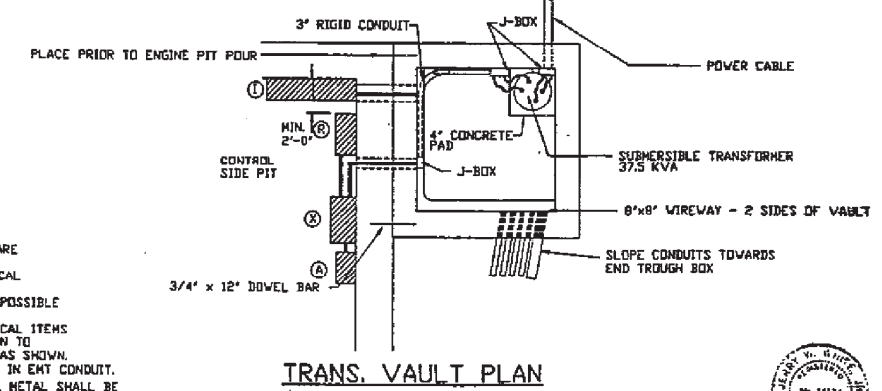
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| DATE:         | MAY 13, 2011    |
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| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 25 OF 84 SHEETS |

**PARTS**

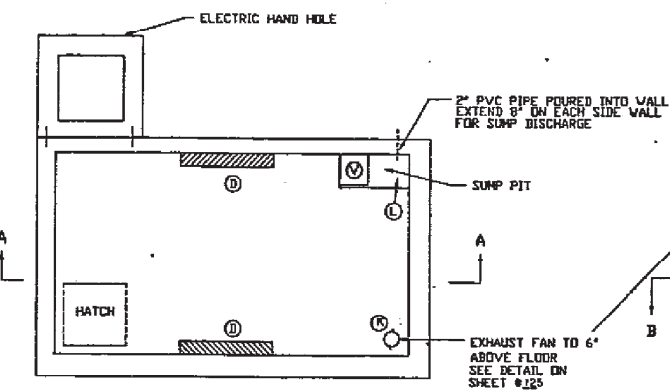
| SYMBOL | DESCRIPTION  | REMARKS   |
|--------|--|---|
| A      | DISTRIBUTION PANEL - 120/240 V 1 PHASE, 3 WIRE, 3 POLE WITH MAIN BREAKER   | MEMO 1 CF/CI  |
| B      | DISTRIBUTION PANEL - 120/240 V 1 PHASE, 3 WIRE, 100A   | MEMO 1 CF/CI  |
| C      | 37.5 KVA 480 1 PHASE VOLTS TO 120/240 1 PHASE SUBMERSIBLE TRANSFORMER (SEE RISER DIAGRAM)  | CF/CI AS PER SPECIFICATIONS   |
| D      | ELECTRIC CABINET UNIT HEATER 20V, 840W, IN SURFACE MOUNT BOTTOM INLET FRONT DISCHARGE INTERNAL THERMOSTAT                          | SEMI-LAR TO ENGINEER TYPE CU MODEL A  |
| E      | CABLE POSITION CONTROL BOX 430-2-0402-1  | CF/CI   |
| F      | ROLLER ELECTRICAL SWITCH AND ASSOCIATED PARTS  | THIS SWITCH SHALL BE MOUNTED TO THE HATCH FRAME AND WIRED TO ALL LIGHTS AND WHEN THE LID IS RAISED ALL LIGHTS AND THE EXHAUST FAN SHALL COME ON.  |
| G      | AIR COMPRESSOR   | COMPRESSOR IS 4" SET AND MOUNTED BY CONTRACTOR AND MOUNTED UP TO AIR LINES BY 60V. ELECTRICAL, HOOK UP BY CONTRACTOR.   |
| H      | SAFETY SWITCH-240 VAC 30A, 2-POLE, SINGLE THROW WITH 20A FUSE  | MEMO 1 CF/CI FOR AIR COMPRESSOR   |
| I      | 8"X8" ENCLOSED METAL WIREWAY   | CF/CI TO RUN ENTIRE LENGTH OF REAR WALL INSTALL AGAINST CEILING.  |
| J      | DELICIOUSSET DRIVER MODEL J-4, FILTER MODEL 602-4V & 25# BAG OF PELLETS ALL ITEMS CF/CI  | DRIVER TO BE PLACED ON AIR LID BY CONTRACTOR.   |
| K      | EXHAUST FAN 115 V, 400 CFM, 0.55" SP   | CF/CI - SECURE DUCTWORK TO WALL. EXHAUST FAN SHALL BE ACTIVATED BY INDICATOR LIGHT ON TOP OF PIT.   |
| L      | SUMP PUMP, SUBMERSIBLE TYPE, 45 GPM @ 10' HEAD, 115V, 40 CYCLE, 0.75 HP  | LEAK PUMP WELL SWITCH ON WHEN THE WATER LEVEL IN THE SUMP REACHES A DEPTH OF 3". THE LEAK PUMP WILL COME ON WHEN THE WATER LEVEL REACHES 10". THE SYSTEM SHALL ALSO BE WIRED TO FACILITATE AUTOMATIC ALTERNATIONS BETWEEN THE LEAK AND LEAK PUMP AFTER EACH CYCLE. CF/CI USE GASKET PUMPS DUPLEX SUMP CONTROL MODEL. ASSUMED BY EQUAL CONTROL MUST SIGNAL A HIGH WATER ALARM. |
| M      | ROLLER SWITCH AND ASSOCIATED RELAY   | HATCH TO SIDE OF HATCH FRAME AND CONNECT THRU RELAY TO THE LIGHTS COME ON WHEN THE HATCH IS OPENED. THIS SWITCH MUST ALSO BE INTERCONNECTED WITH THE EXHAUST FAN SWITCH SO EITHER THE ROLLER SWITCH OR THE EXHAUST FAN LIGHT SWITCH WILL OPERATE THE PIT LIGHTS AND EXHAUST FAN.  |
| N      | LIGHT FIXTURE - UL LISTED VAPOR PROOF, CLEAR GLASS LENS, FLUORESCENT, MOUNTED ON WALL WITH 4" BRACKETS - TIC TO EXHAUST FAN SWITCH | TOPPER GLASS CO-BULBS, 4" FIXTURES, CF/CI USE MODEL 100V124840  |
| P      | DUPLEX RECEPTACLE 115V   | CF/CI WEATHER PROOF. ALL RECEPTACLES SHALL BE GROUND FAULT.   |
| R      | SAFETY SWITCH 100A 240V FUSED 2 POLE   | CF/CI   |
| S      | PRESSURE SWITCH - ASKO 85A204-TA1041   | CF/CI - SWITCH TO HOLD CONTACTS OPEN WHILE UNDER AIR PRESSURE AND CLOSE CONTACTS WHEN PRESSURE IS LOST - INSTALL AFTER EQUIPMENT INSTALLATION.  |
| T      | RELAY - ASKO 84AL 125A 05410C  | CF/CI OPERATED BY PRESSURE SWITCH "S". SEE ENGINEER FOR PRESSURE SETTINGS. THIS RELAY TO OPERATE BOTH BARRIERS MARCHER LIGHTS.  |
| U      | WEATHERPROOF EXHAUSTION LIGHT & FAN WITH 2-WAY SWITCH - HUSCO 3703-3000 AND 10 GAL.  | CF/CI TIE TO RESPECTIVE EXHAUST FAN AND LIGHTS. SEE ITEM F FOR INTERCONNECTION WITH FRAME ROLLER SWITCH AND PIT LIGHTS.   |
| V      | DEHUMIDIFIER - WALL MOUNTED 4' ABOVE FINISHED FLOOR  | 25 PINT CAPACITY WITH HUMIDISTAT, STEEL CABINET ENCLOSED TRIP FAN AUTOMATIC SHUT OFF, SUBJECT TO DRAIN HOSE CONNECTION, UL LISTED FOR 120V, 60 CYCLE AND A CORD LONG ENOUGH TO REACH A POWER SOURCE. CF/CI 3/4" X 1/2" COPPER DRAIN TO SUMP PIT.  |
| W      | GROUND ROD 3/4" DIA. X 10'-0" LG. C&D WELD WITH GROUND CONNECTOR   | CF/CI - SHALL BE MAX OF 1 1/2" FROM WALL.   |
| X      | SAFETY SWITCH - 200A 240VAC NON-FUSIBLE 3-POLE, DOUBLE THROW   | CF/CI CONNECT TO EMERGENCY RECEPTACLE. THIS SWITCH SHALL TRANSFER POWER LOADS TO AN EMERGENCY GENERATOR THRU PIG IN "Y".  |
| Y      | EMERGENCY POWER RECEPTACLE - 200A 240 VAC 4-WIRE WITH SPREAD COVER   | CF/CI RECEPTACLE TO MATCH AIR MATERIAL. CLASSIFIED STANDARD SPECIFICATIONS. WIRE UP BY WALL.  |
| BB     | JUNCTION BOX   | CF/CI   |
| DD     | LIGHT AND ALARM BUZZER ALLEN BRADLEY 800-1158 SIGNAL BUZZER BY DUPLEX  | CF/CI ONE LIGHT SHALL BE SUPPLIED FOR THE NON-CONTROL PIT AND WILL BE CONNECTED TO BAK 14 EQUIPMENT IN CONTROL PIT.   |

NOTE: THERE ARE 2 CONTROL PIT AND 2 NON-CONTROL PIT REQUIRED FOR THIS PROJECT.

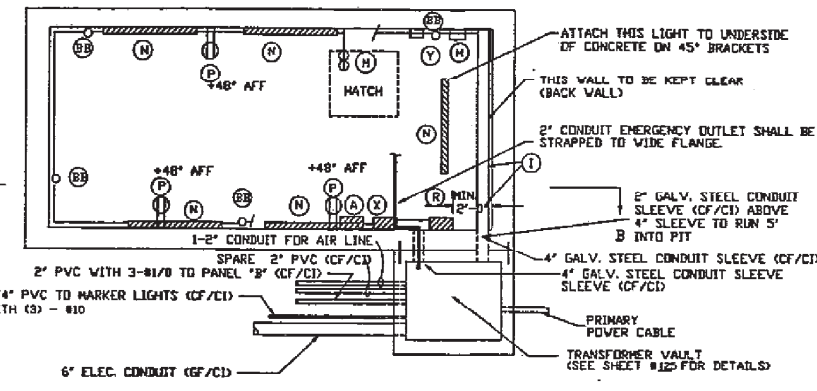
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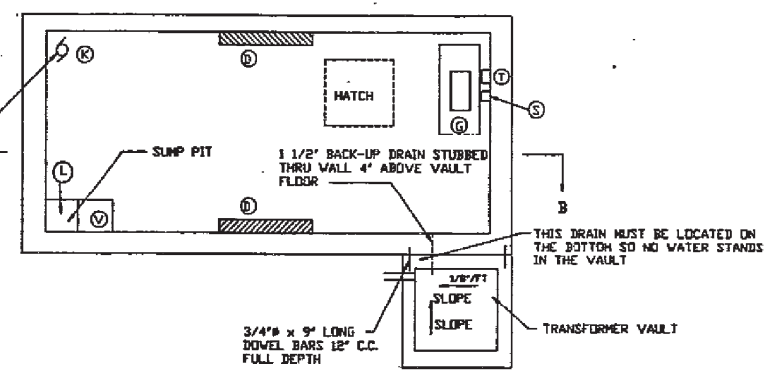
**NON-CONTROL PIT ELEC. PLAN**  
SCALE: NONE



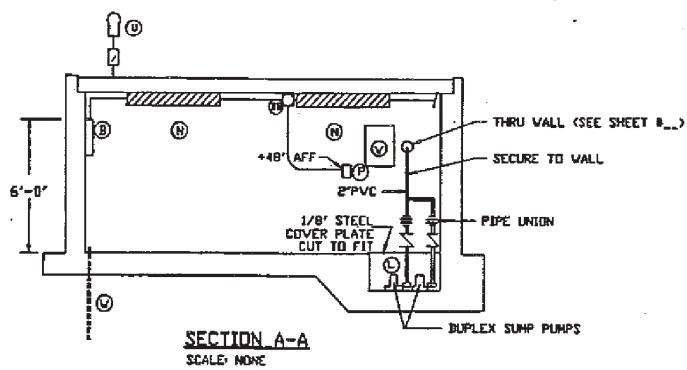
**NON-CONTROL PIT MECH. PLAN**  
SCALE: NONE



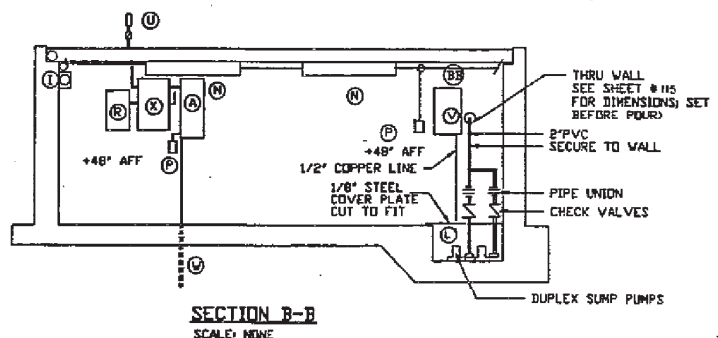
**CONTROL PIT ELEC. PLAN**  
SCALE: NONE



**CONTROL PIT MECH. PLAN**  
SCALE: NONE



**SECTION A-A**  
SCALE: NONE



**SECTION B-B**  
SCALE: NONE

- NOTES:**
- ALL ELECTRICAL WORK SHALL BE IN STRICT COMPLIANCE WITH THE NATIONAL ELECTRICAL CODE, LATEST EDITION AND ALL LOCAL CODES AND ORDINANCES HAVING JURISDICTION.
  - ALL WIRING TO BE COPPER UNLESS OTHERWISE SPECIFIED. WIRE SHALL BE A MINIMUM OF #12 A.W.G. EXCEPT FOR CONTROL WIRING.
  - ALL CONDUITS AND PIPES ENTERING THE ARRESTOR ENGINE AND TRANSFORMER PITS BELOW GRADE SHALL BE POURED IN PLACE.
  - CONNECT 1/2" COPPER TUBE TO DRAIN LINE FROM DEHUMIDIFIER TO SUMP PIT.

- ALL QUANTITIES AND DIMENSIONS ARE FOR REFERENCE ONLY AND SHALL BE FIELD CHECKED.
- ANY ITEMS NOT LISTED AS CI SHALL BE ASSUMED AS CI.
- ALL WIRE CONNECTIONS IN ALL ELECTRICAL MANHOLES TO BE COVERED WITH SHRINK TYPE CONNECTORS TO SEAL AGAINST CORROSION.
- CONTRACTOR TO PROVIDE NEOPRENE ISOLATION PADS FOR AIR COMPRESSOR.
- TRANSFORMER PIT AND MANHOLES TO BE WATERPROOFED AS PER SPECIFICATIONS.

- PIT EXHAUSTS ARE WIRED TO SWITCH ON EXHAUST FAN INDICATOR LIGHTS WHICH ARE MOUNTED ON TOP OF ENGINE PIT.
- BACK WALL TO REMAIN FREE OF ELECTRICAL EQUIPMENT ON CONTROL SIDE PIT.
- LOCATE ALL ITEMS IN PITS AS HIGH AS POSSIBLE EXCEPT AS MARKED.
- CONTRACTOR SHALL LAYOUT ALL ELECTRICAL ITEMS IN PIT PRIOR TO BEGINNING INSTALLATION TO ASSURE THE SPECIFIED ITEMS WILL FIT AS SHOWN.
- ALL WIRING EXCEPT AS NOTED SHALL BE IN EMT CONDUIT.
- ALL WALLS AND CEILINGS INCLUDING ALL METAL SHALL BE PAINTED WITH TWO COATS OF WHITE EPOXY.
- SEAL CONCRETE FLOOR WITH CLEAR SEALANT.

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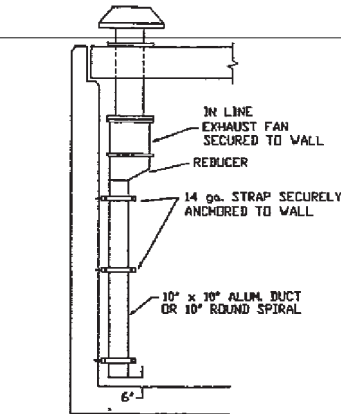
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MECHANICAL AND ELECTRICAL  
 SCHEDULES AND DETAILS

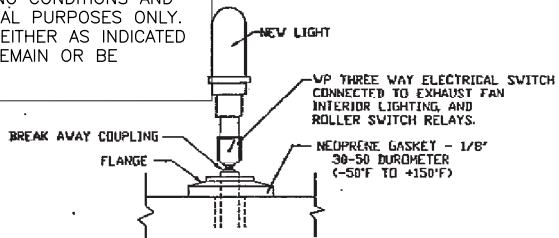
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 DATE 0/0/1999 DDD0-000  
 SHEET 24 OF 130 SHEETS

**REMOVAL NOTES:**

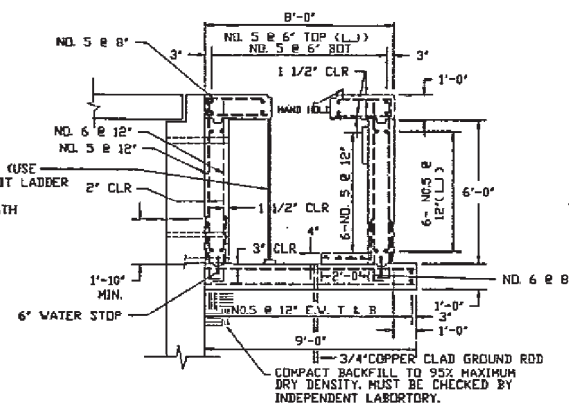
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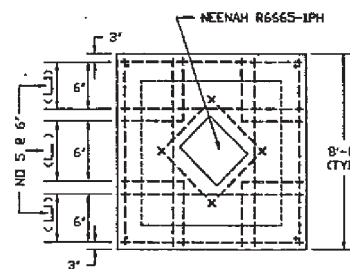
**FAN EXHAUST DETAIL**  
SCALE: NONE



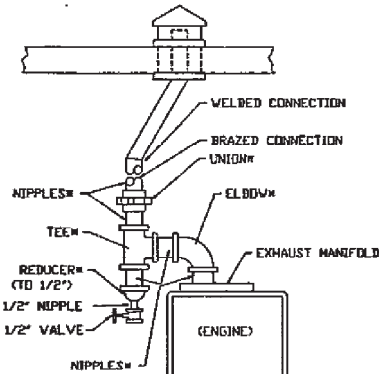
**EXHAUST FAN SWITCH & LIGHT**  
SCALE: NONE



**TRANS. VAULT SECTION**  
SCALE: NONE



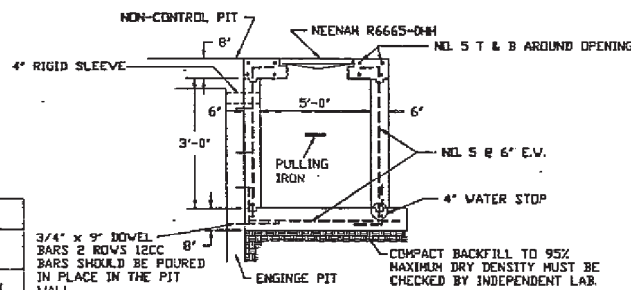
**CONC. COVER STEEL**  
SCALE: NONE



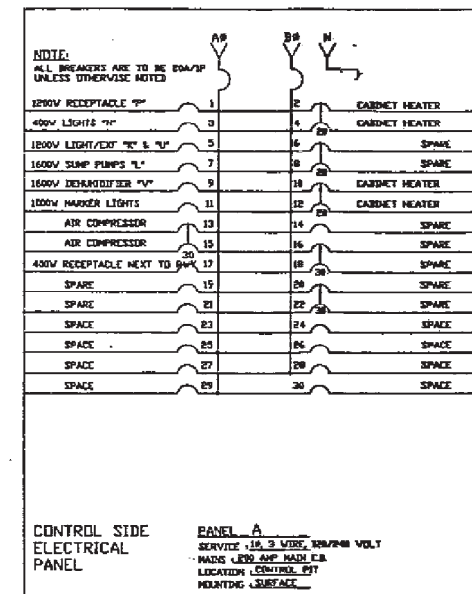
ALL MATERIAL TO BE GALVANIZED PIPE

| ENGINE TYPE                                      | SIZE FITTINGS |
|--|---------------|
| ALL MV64B WISCONSIN ENGINES AND PACKETIE ENGINES | 1 1/2\"/>     |
| FOR V-465D WISCONSIN ENGINE                      | 2\"/>         |

**ENGINE EXHAUST SYSTEM MOISTURE TRAP INSTALLATION**  
SCALE: NONE

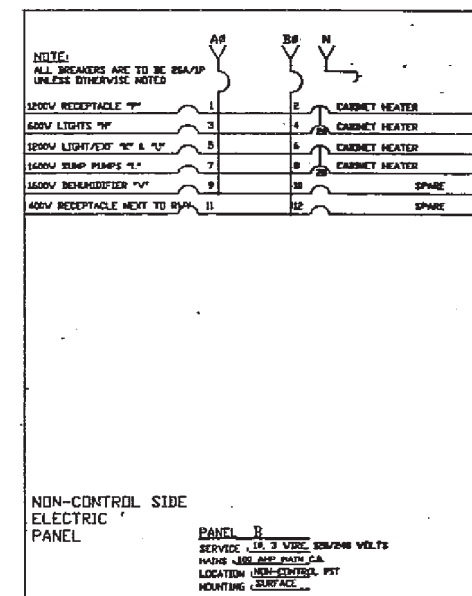


**ELEC. HANDHOLE**  
SCALE: NONE



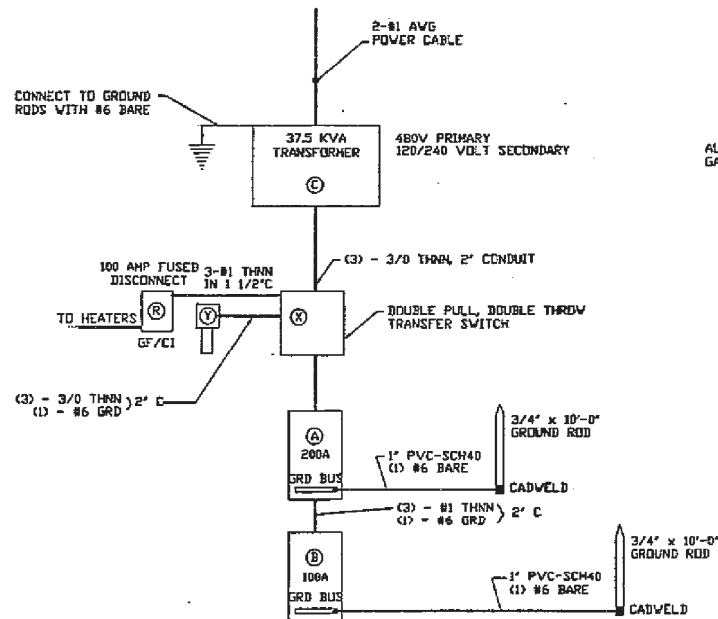
**CONTROL SIDE ELECTRICAL PANEL**

**PANEL A**  
SERVICE - 120 VOLT, 200/240 VOLTS  
MANS JES. AND PATH C.A.  
LOCATION - TRANS. VAULT  
MOUNTING SURFACE



**NON-CONTROL SIDE ELECTRICAL PANEL**

**PANEL B**  
SERVICE - 120 VOLT, 200/240 VOLTS  
MANS JES. AND PATH C.A.  
LOCATION - TRANS. VAULT  
MOUNTING SURFACE



**ELECTRIC DISTRIBUTION DIAGRAM**  
SCALE: NONE

**NOTE:**

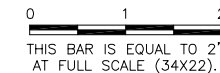
1. ANY POWER OUTAGES REQUIRED FOR ELECTRICAL CONNECTIONS MUST BE COORDINATED & APPROVED 3 DAYS IN ADVANCE.
2. TRANSFORMER VAULT AND ELECTRIC HANDHOLE SHALL BE POURED. PRECAST IS NOT ACCEPTABLE.
3. CONTRACTOR MUST PROVIDE TEMPORARY POWER UNTIL SUCH TIME THE TRANSFORMER ARRIVES.
4. ALL ELECTRICAL RACES AND CONDUITS TO HAVE BELLS OR BUSHING INSTALLED.
5. THE TRANSFORMER MUST BE PLACED AS FAR AS POSSIBLE IN THE CORNER OF THE PIT. BOTH PRIMARY AND SECONDARY CABLES MUST STAY CLEAR OF THE LADDER INGRESS AND EGRESS AREA.

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REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
BAK REMOVAL DETAILS 8

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| JOB No:                    | 100610300    |
| IL PROJ. NO. PIA-4079      |              |
| AIP PROJ. NO. 3-17-0080-XX |              |
| SHEET 26 OF 84 SHEETS      |              |

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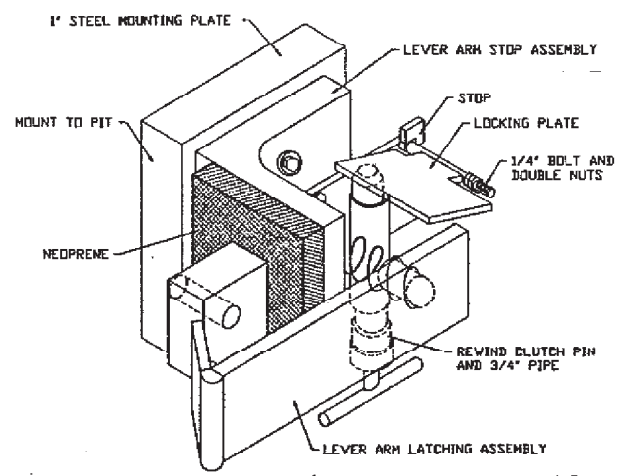
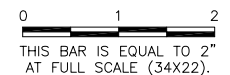
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STRUCTURAL DETAILS

SCALE AS NOTED  
DATE: 05/13/11  
JOB NO. 100610300  
SHEET 26 OF 84 SHEETS

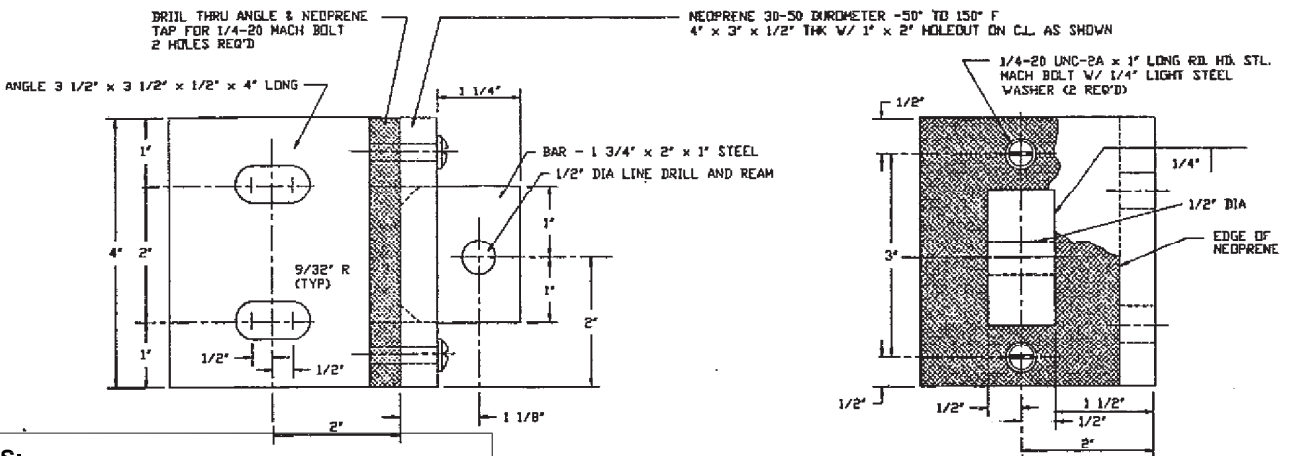


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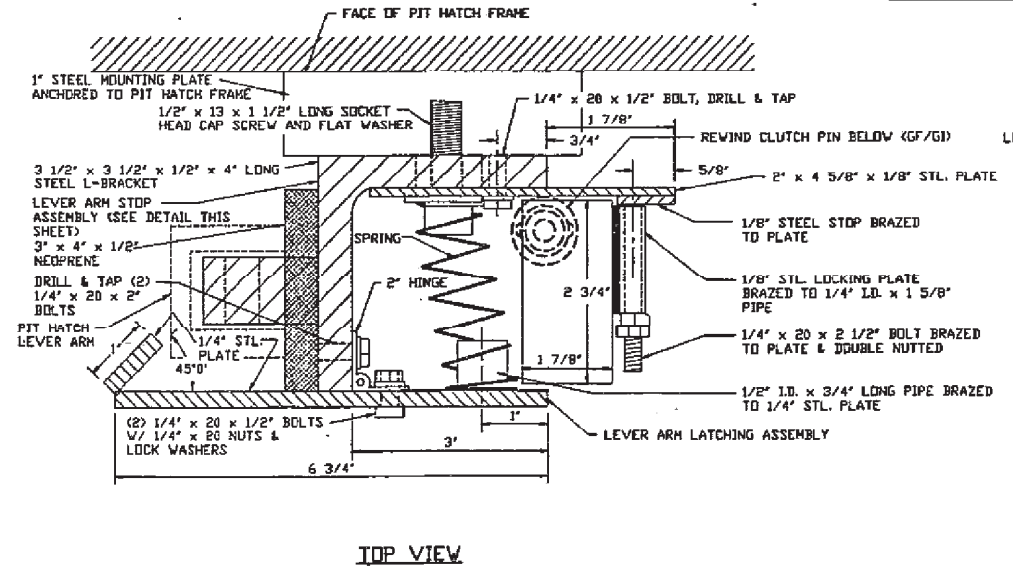
**ISOMETRIC VIEW OF ASSEMBLED  
 ENGINE PIT HATCH LATCH**  
 SCALE: NONE



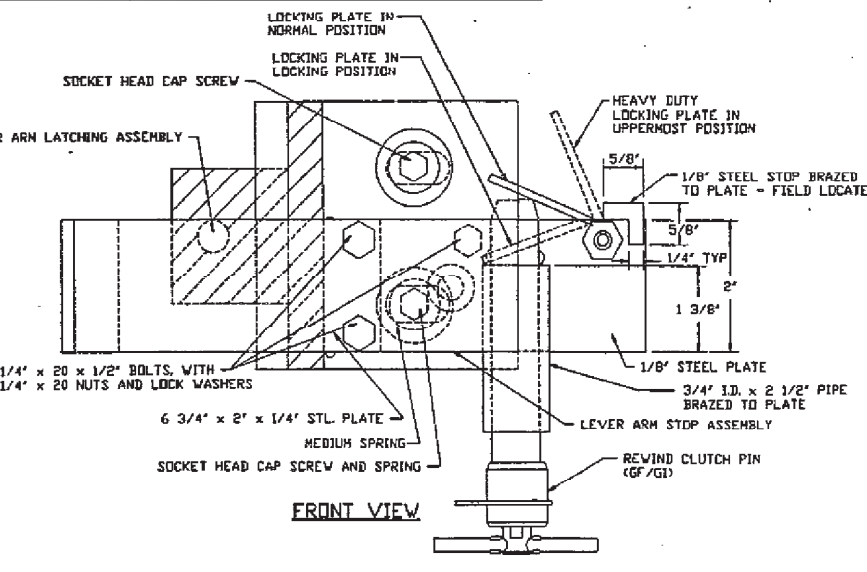
**HATCH STOP/HOLD OPEN LATCH**  
 FULL SIZE  
 (SEE SECTION B-B SHEET 16)

**REMOVAL NOTES:**

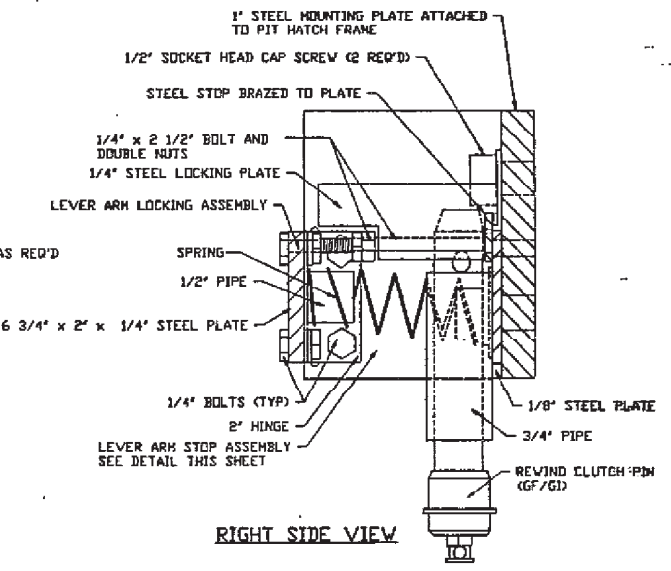
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**TOP VIEW**



**PIT DOOR LATCH**  
 FULL SIZE



**RIGHT SIDE VIEW**

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| JOB No:      | 100610300    |

ES  
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 1/4"  
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 3/4"  
 1"  
 1 1/4"  
 1 1/2"  
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 3"  
 4"

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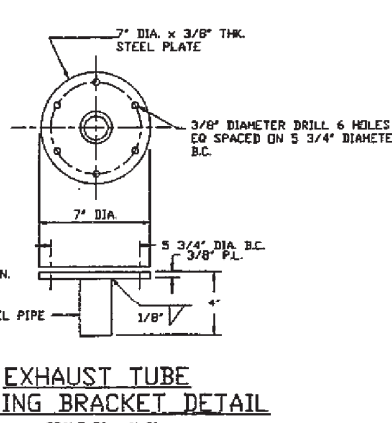
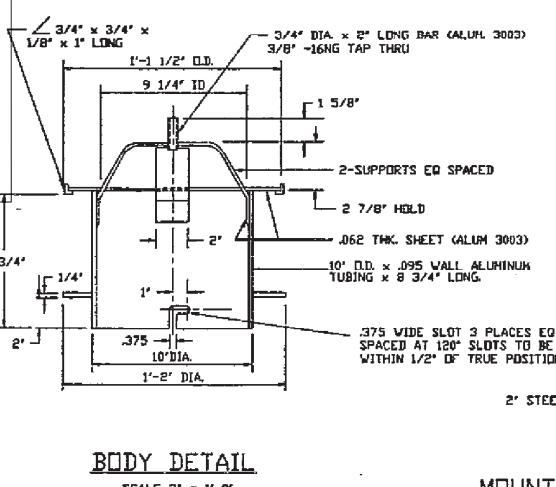
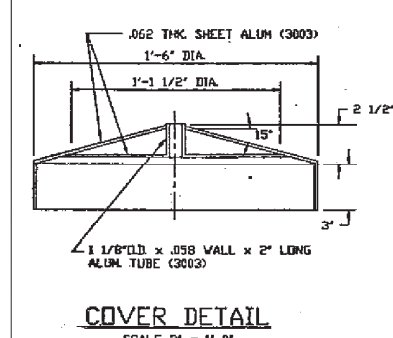
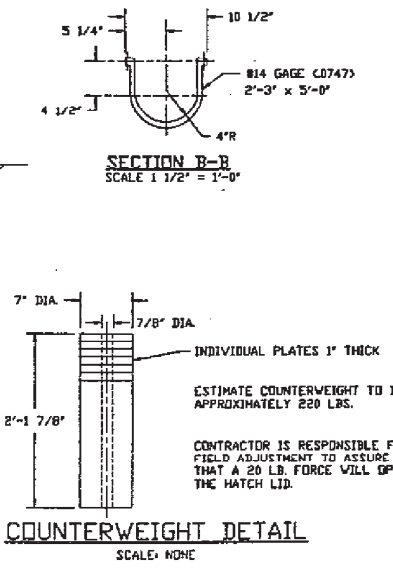
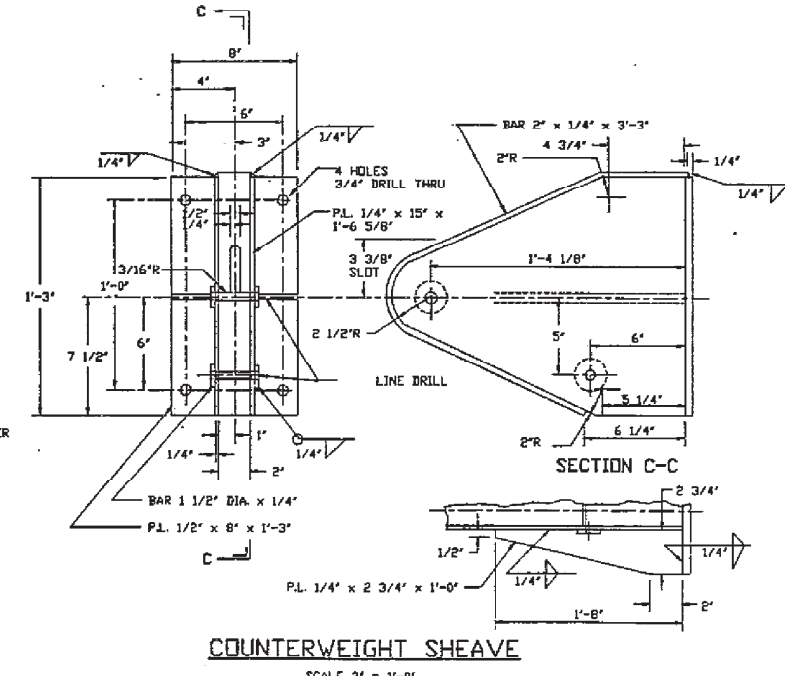
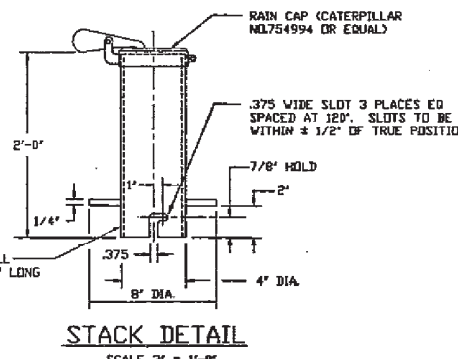
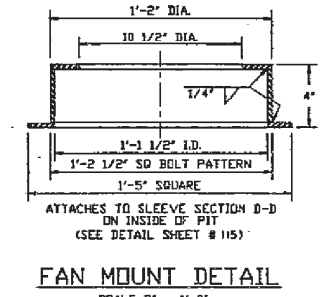
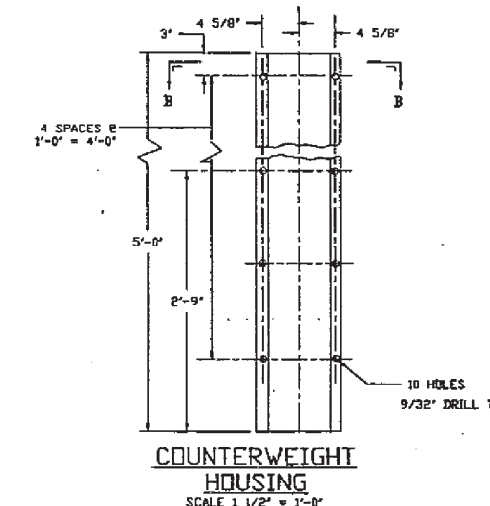
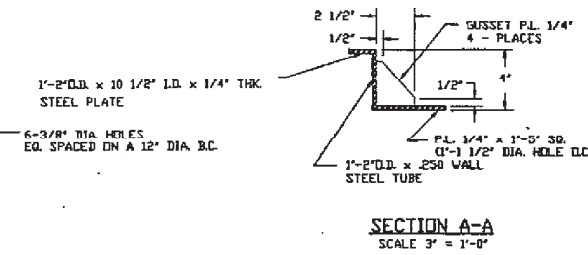
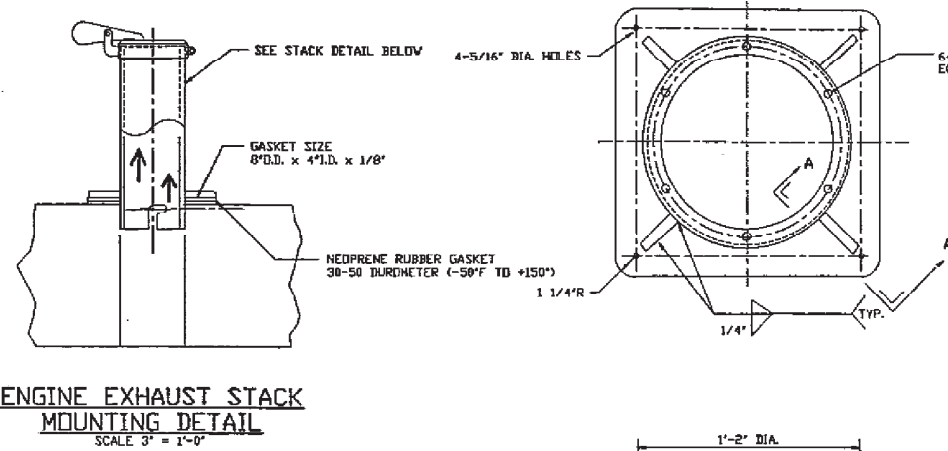
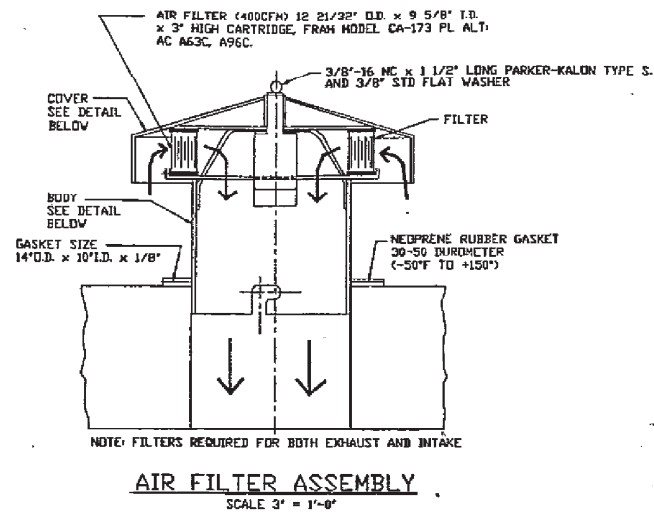
**HATCH DETAILS**

SCALE AS NOTED JOB NO. 100610300  
 DATE 0/0/1991 0000-00  
 SHEET 173 OF 130 SHEETS

**PE092**

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**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**BAK REMOVAL DETAILS 10**

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**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**

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| DATE:        | MAY 13, 2011 |
| JOB No:      | 100610300    |

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GREATER PEORIA AIRPORT AUTHORITY  
 PEORIA, ILLINOIS

MISC. METAL DETAILS

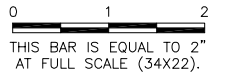


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 DATE 0/0/1990  
 SHEET 119 OF 123 SHEETS

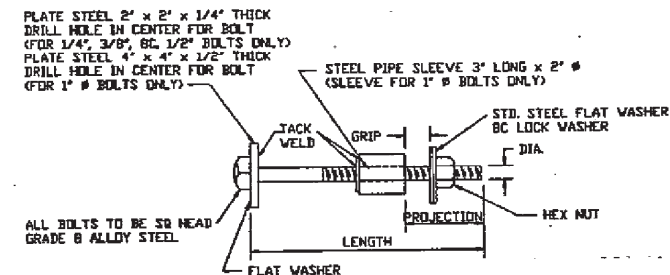


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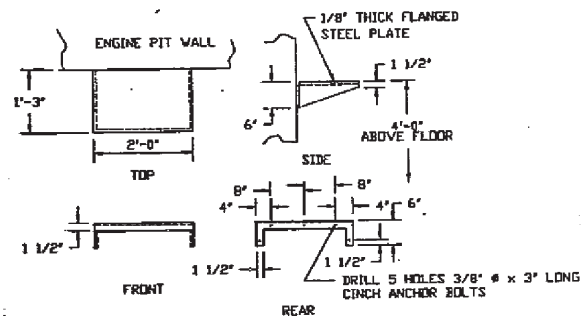
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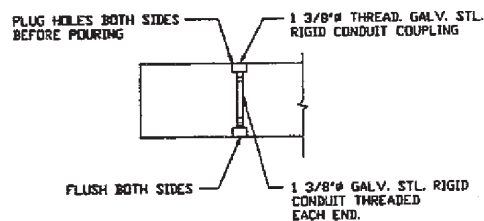
| ANCHOR BOLT CHART         |          |        |       |        |        |
|---------------------------|----------|--------|-------|--------|--------|
| USE                       | DIAMETER | GRIP   | GROUT | PROJ.  | LENGTH |
| LADDER - CONTROL SIDE     | 3/4"     | 3/8"   | ---   | 1-3/4" | 6"     |
| LADDER - NON-CONTROL SIDE | 3/4"     | 3/8"   | ---   | 2"     | 6"     |
| WEIGHT HOUSING            | 1/4"     | ---    | ---   | 3/4"   | 3"     |
| COUNTERWEIGHT SHEAVES     | 1/2"     | 1/2"   | ---   | 2"     | 6"     |
| AIR COMPRESSOR            | 1/2"     | ---    | 2"    | 2-1/2" | 3"     |
| DEFLECTOR SHEAVE          | 1"       | 1"     | 1"    | 4-1/2" | 14"    |
| ENGINES                   | 1"       | 1-1/8" | 2"    | 6"     | 15"    |
| NUTS                      | 1"       | ---    | ---   | ---    | ---    |
| LOCK WASHERS              | 1"       | ---    | ---   | ---    | ---    |
| FLAT WASHERS              | 1"       | ---    | ---   | ---    | ---    |



**ANCHOR BOLT DETAIL**  
 SCALE: NONE

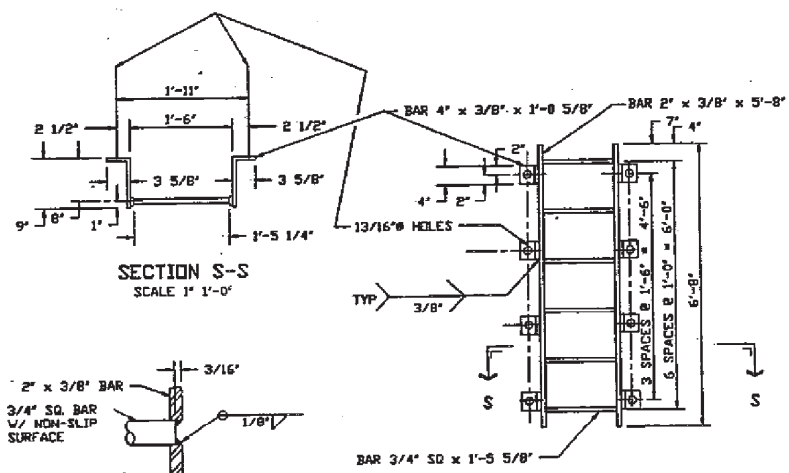


**DEHUMIDIFIER RACK DETAIL**  
 SCALE: NONE



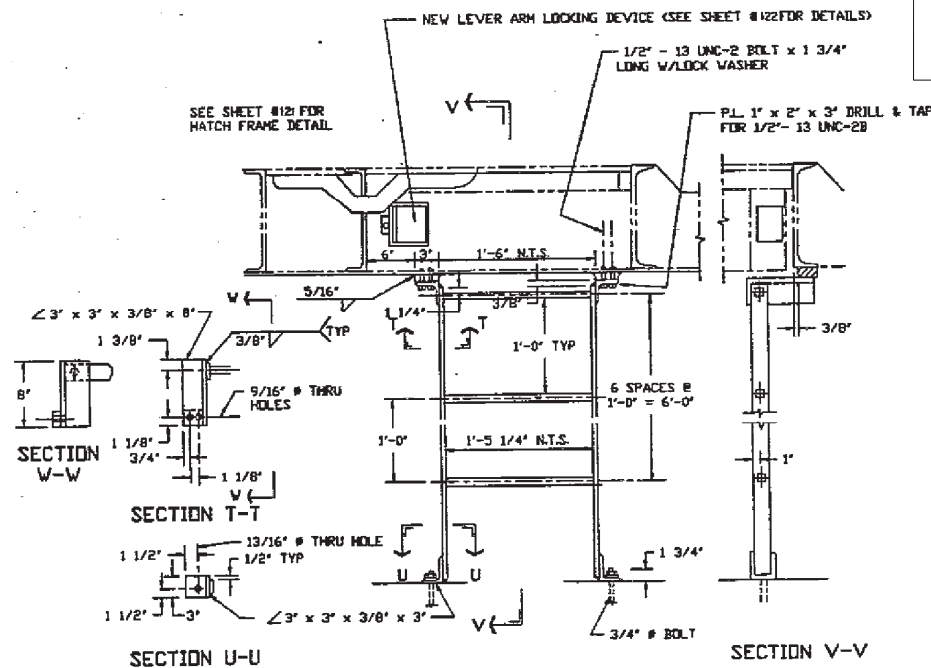
**FAN INDICATOR LIGHT PENETRATION DETAIL**  
 SCALE 1 1/2" = 1'-0"

(1) FOR EACH CONTROL PIT)  
 (1) FOR EACH NON-CONTROL SIDE PIT)



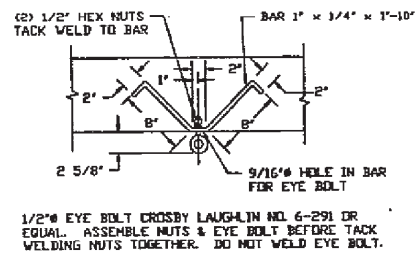
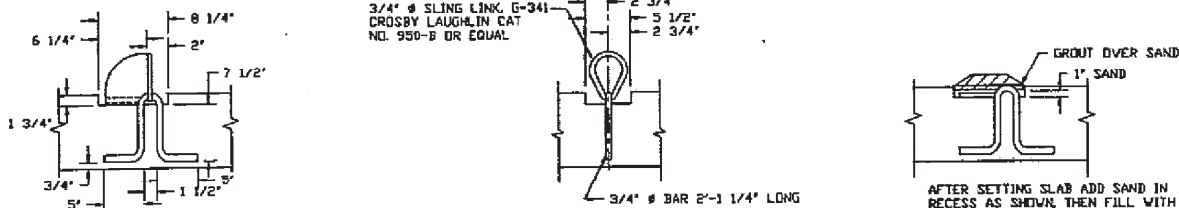
**TYPICAL RUNG CONNECTION**  
 SCALE: NONE

**NON-CONTROL PIT LADDER DETAIL**  
 SCALE 3/4" = 1'-0"



**CONTROL PIT LADDER DETAIL**  
 SCALE: NONE

**LIFTING RING DETAILS**  
 SCALE 1 1/2" = 1'-0"



**TYP. EYE BOLT DETAIL**  
 SCALE 1 1/2" = 1'-0"

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DATE: 07/19/11  
 SHEET 120 OF 130 SHEETS

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|  |  | DIRECTORY NAME                       |  |  | DATE: 07/19/11<br>SHEET 120 OF 130 SHEETS                                  |  |               |   |

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 REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 BAK REMOVAL DETAILS 11

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0 1 2  
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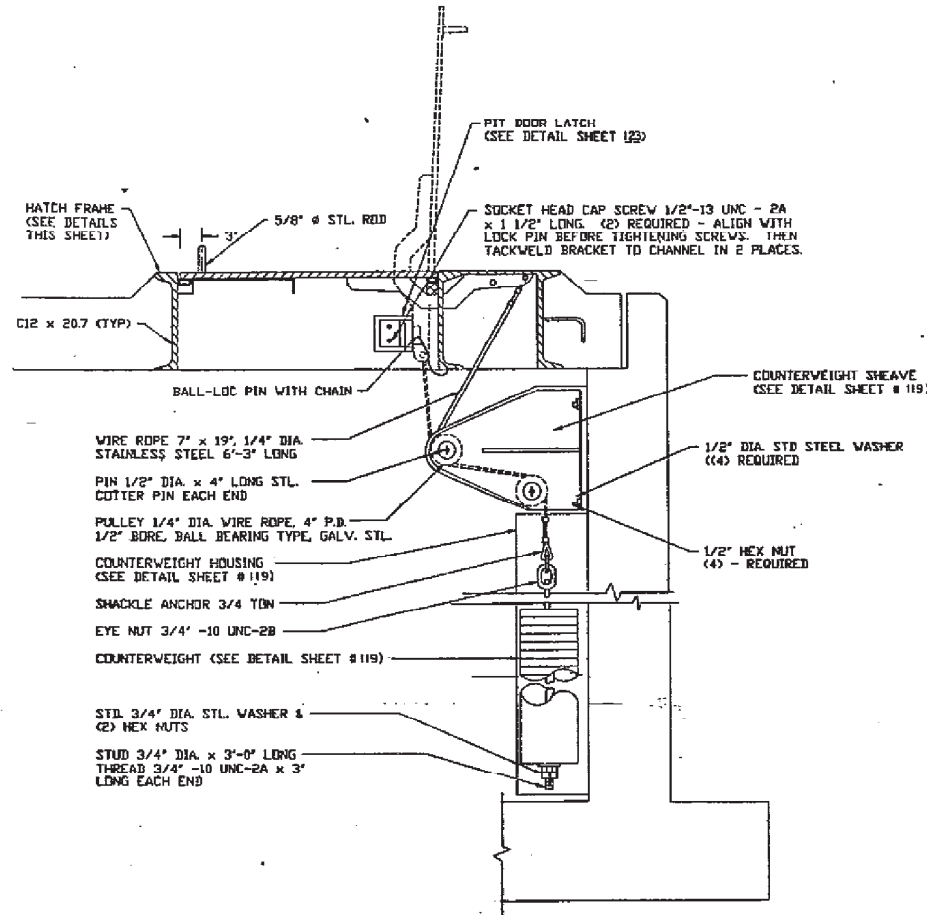
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 BAK REMOVAL DETAILS 12**

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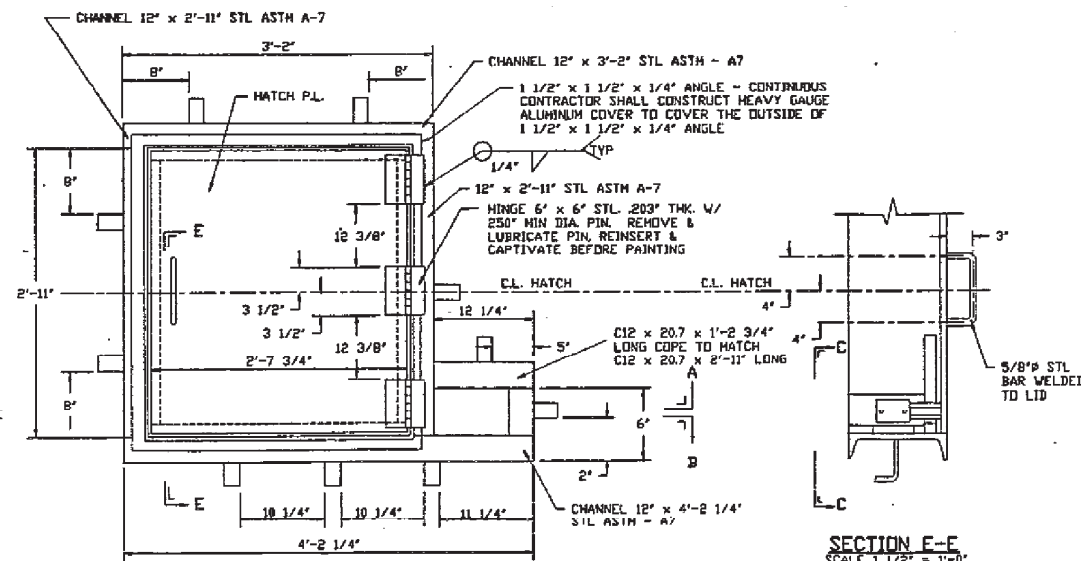
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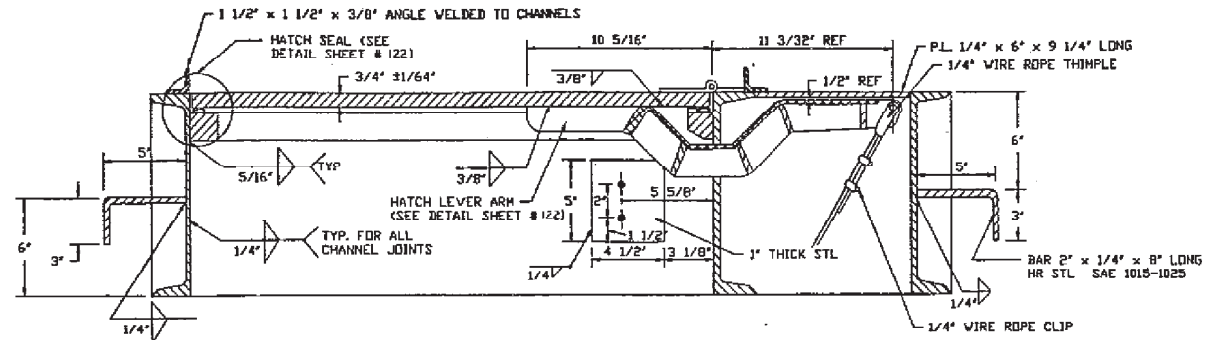
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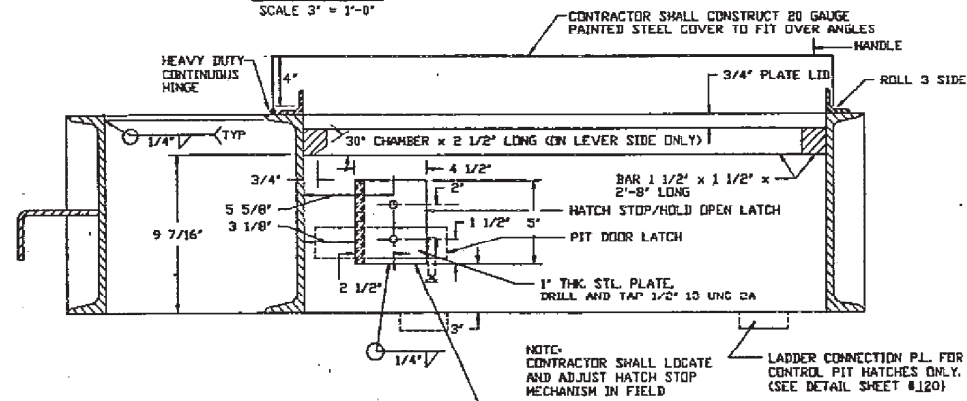
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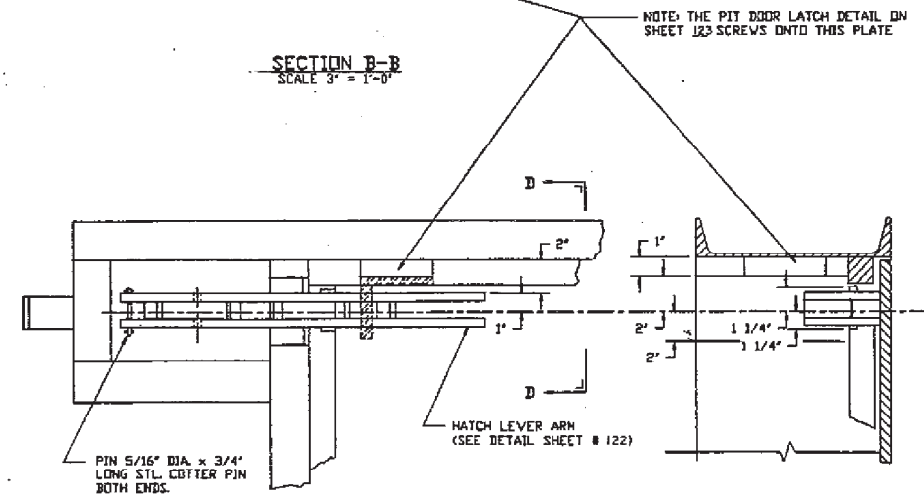
**HATCH PLAN**  
 SCALE 1 1/2" = 1'-0"



**SECTION A-A**  
 SCALE 3\"/>



**SECTION B-B**  
 SCALE 3\"/>



**SECTION D-D**  
 SCALE 3\"/>

**BOTTOM OF HATCH VIEW C-C**  
 SCALE 3\"/>

**SECTION E-E**  
 SCALE 1 1/2\"/>

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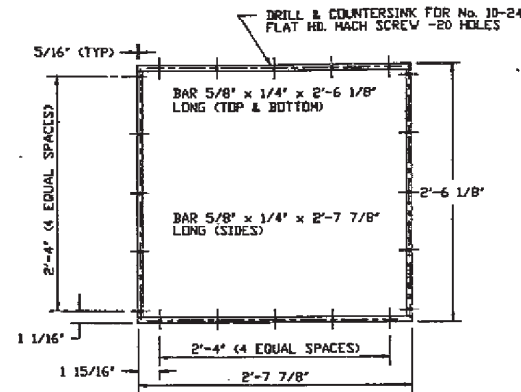
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*J. Heavysides*  
 SCALE AS NOTED  
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 SHEET 30 OF 120 SHEETS

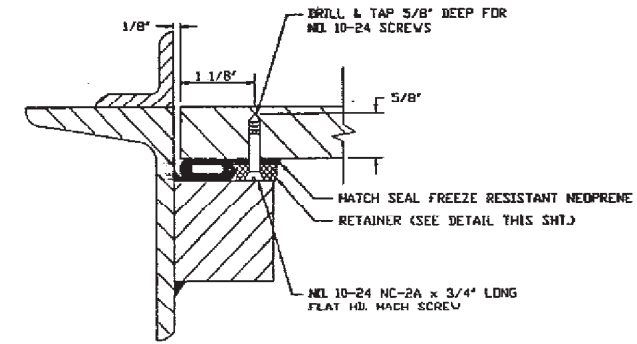
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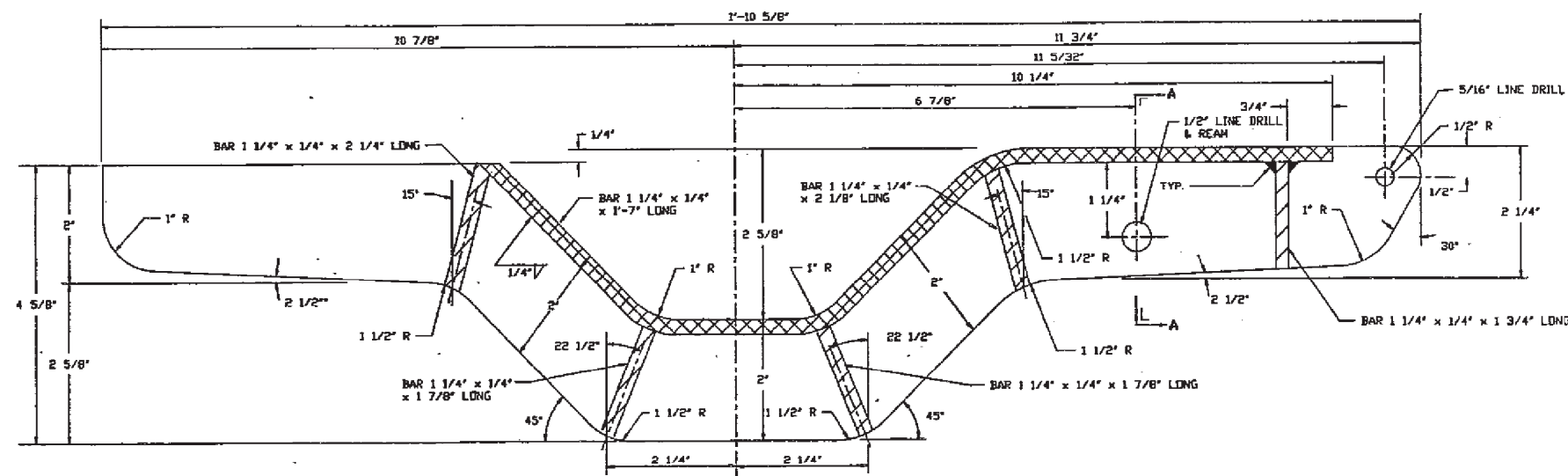
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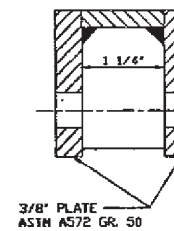
**RETAINER DETAIL**  
SCALE: NONE



**HATCH SEAL DETAIL**  
SCALE: NONE



**PIT HATCH LEVER ARM SECTION**  
FULL SCALE



**SECTION A-A**  
FULL SCALE

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0 1 2  
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GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS

REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 BAK REMOVAL DETAILS 13

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| JOB No:      | 100610300    |

IL PROJ. NO. PIA-4079  
 AIP PROJ. NO. 3-17-0080-XX

1125

**ENGINEERING CONSULTANTS, INC.**  
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 PEORIA, ILLINOIS 61604  
 TEL: 309.673.1234  
 FAX: 309.673.1235  
 WWW: WWW.ECICON.COM

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 APPROVED BY: O.D.O.

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 PEORIA, ILLINOIS

STEEL DETAILS

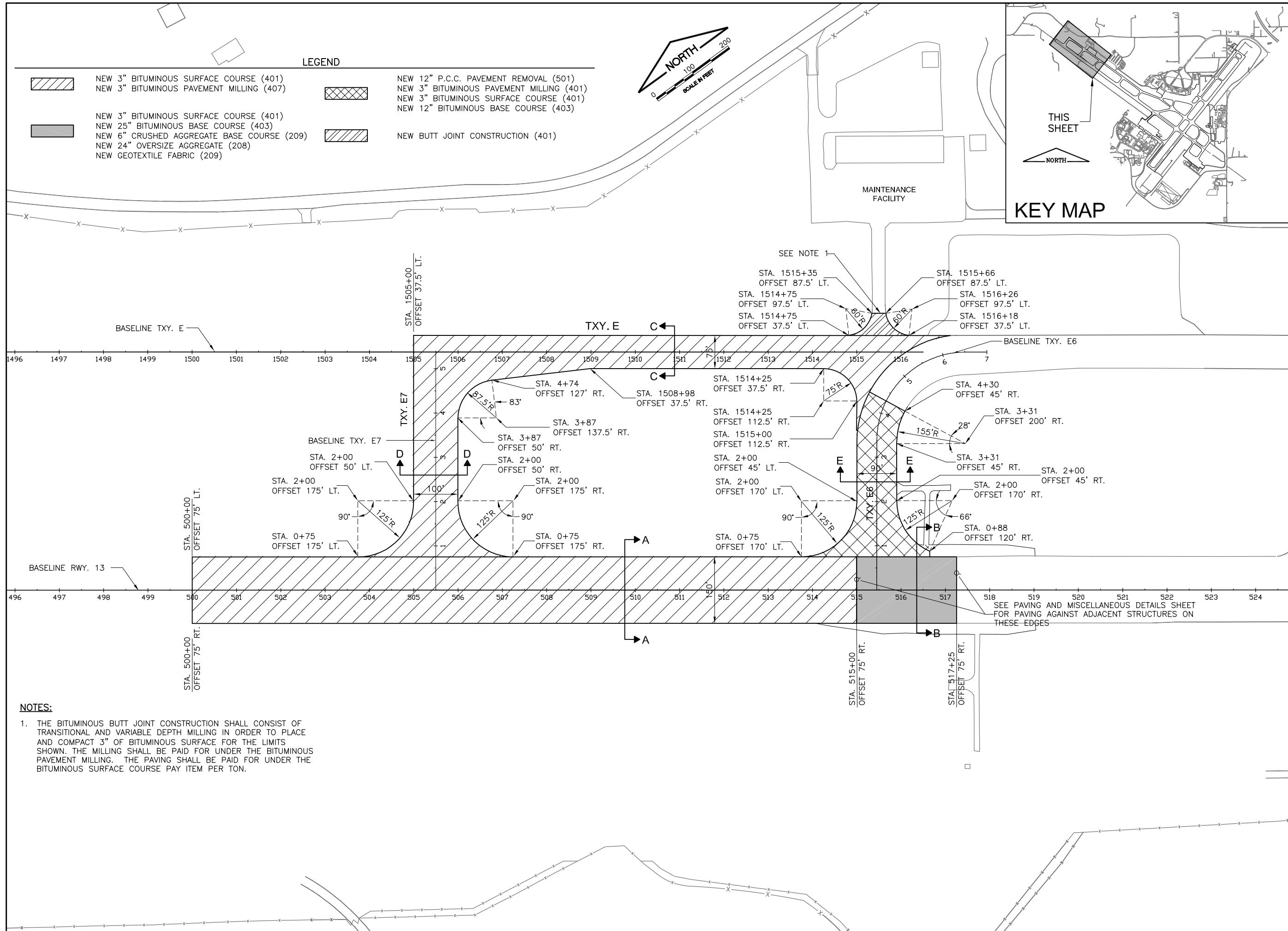
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 SHEET 31 OF 84 SHEETS



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0 1 2  
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 AT FULL SCALE (34X22).



**LEGEND**

|  |  |  |  |
|--|--|--|--|
|  | NEW 3" BITUMINOUS SURFACE COURSE (401)     |  | NEW 12" P.C.C. PAVEMENT REMOVAL (501)    |
|  | NEW 3" BITUMINOUS PAVEMENT MILLING (407)   |  | NEW 3" BITUMINOUS PAVEMENT MILLING (401) |
|  | NEW 3" BITUMINOUS SURFACE COURSE (401)     |  | NEW 3" BITUMINOUS SURFACE COURSE (401)   |
|  | NEW 25" BITUMINOUS BASE COURSE (403)       |  | NEW 12" BITUMINOUS BASE COURSE (403)     |
|  | NEW 6" CRUSHED AGGREGATE BASE COURSE (209) |  | NEW BUTT JOINT CONSTRUCTION (401)        |
|  | NEW 24" OVERSIZE AGGREGATE (208)           |  |  |
|  | NEW GEOTEXTILE FABRIC (209)                |  |  |

**NOTES:**

1. THE BITUMINOUS BUTT JOINT CONSTRUCTION SHALL CONSIST OF TRANSITIONAL AND VARIABLE DEPTH MILLING IN ORDER TO PLACE AND COMPACT 3" OF BITUMINOUS SURFACE FOR THE LIMITS SHOWN. THE MILLING SHALL BE PAID FOR UNDER THE BITUMINOUS PAVEMENT MILLING. THE PAVING SHALL BE PAID FOR UNDER THE BITUMINOUS SURFACE COURSE PAY ITEM PER TON.

**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 PROPOSED IMPROVEMENTS 1**

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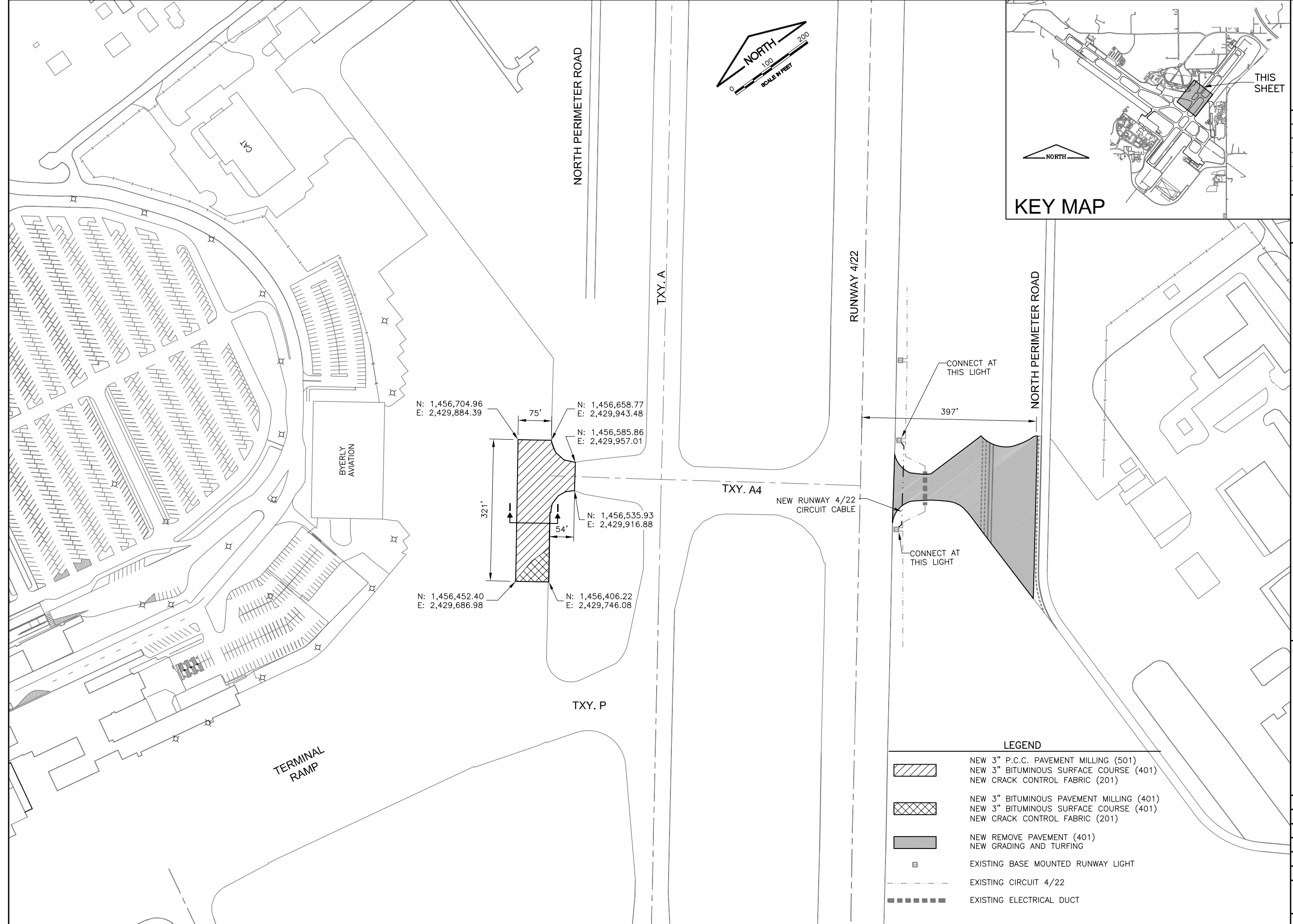
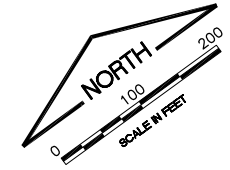
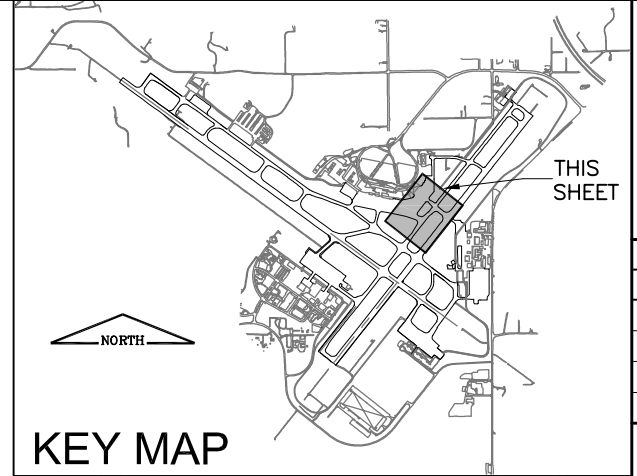
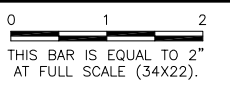
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| IL PROJ. NO.  | PIA-4079        |
| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 32 OF 84 SHEETS |

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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 PROPOSED IMPROVEMENTS 2**

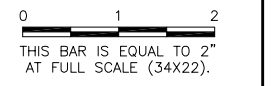
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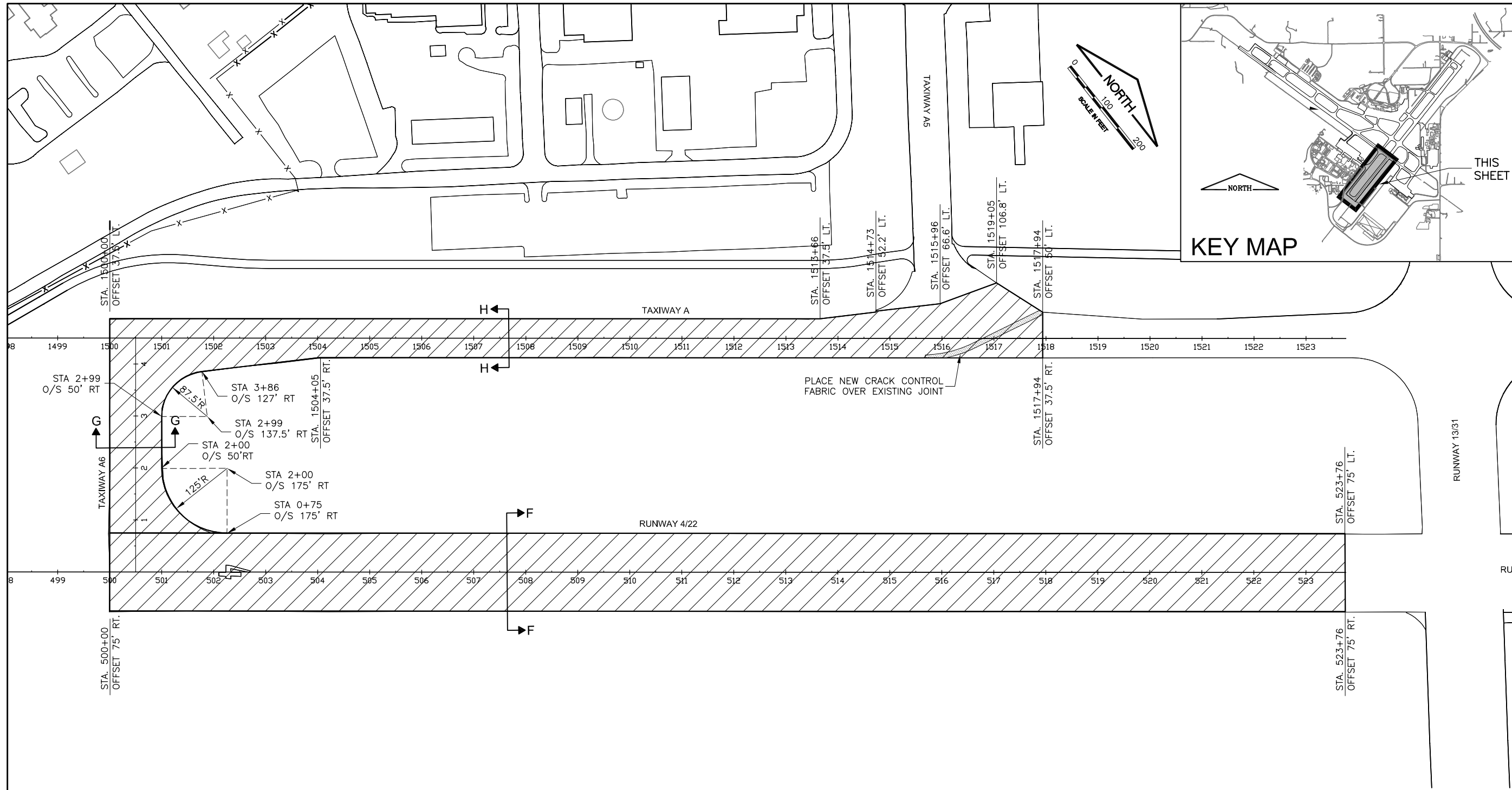
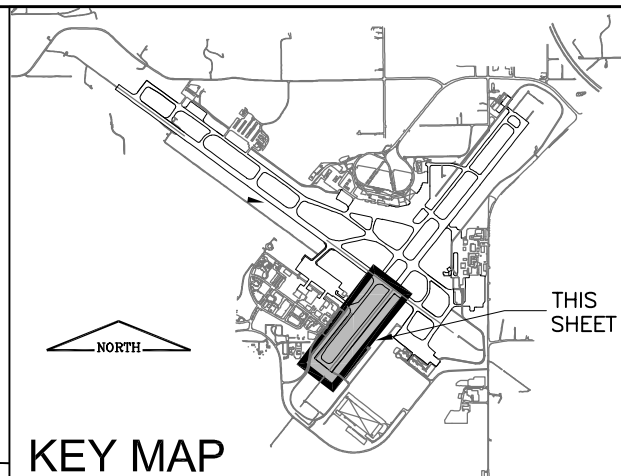
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| DATE:                      | MAY 13, 2011    |
| JOB No:                    | 100610300       |
| IL PROJ. NO. PIA-4079      |                 |
| AIP PROJ. NO. 3-17-0080-XX |                 |
| SHEET                      | 33 OF 84 SHEETS |

**PE092**

| REVISIONS |    |      |
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| NUMBER    | BY | DATE |
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**KEY MAP**



**LEGEND**

|  |  |
|--|--|
|  | NEW 3" BITUMINOUS SURFACE COURSE (401)   |
|  | NEW 3" BITUMINOUS PAVEMENT MILLING (401) |
|  | NEW CRACK CONTROL FABRIC (201)           |

**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 PROPOSED IMPROVEMENTS 3**

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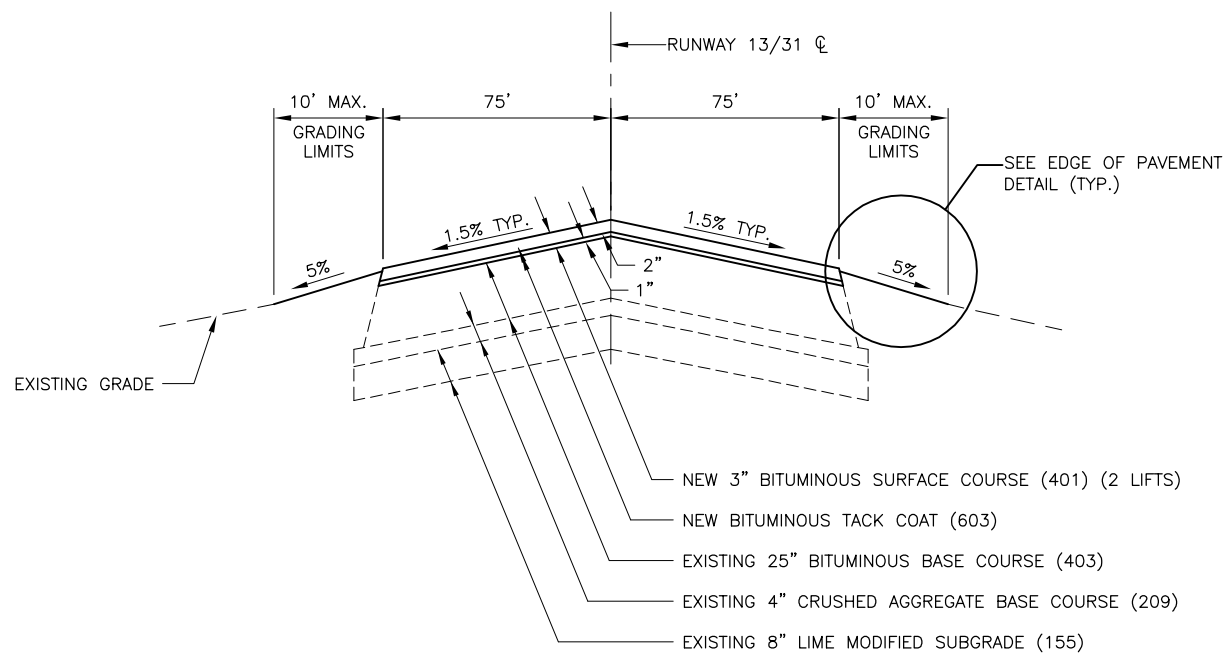
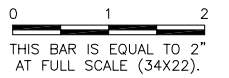
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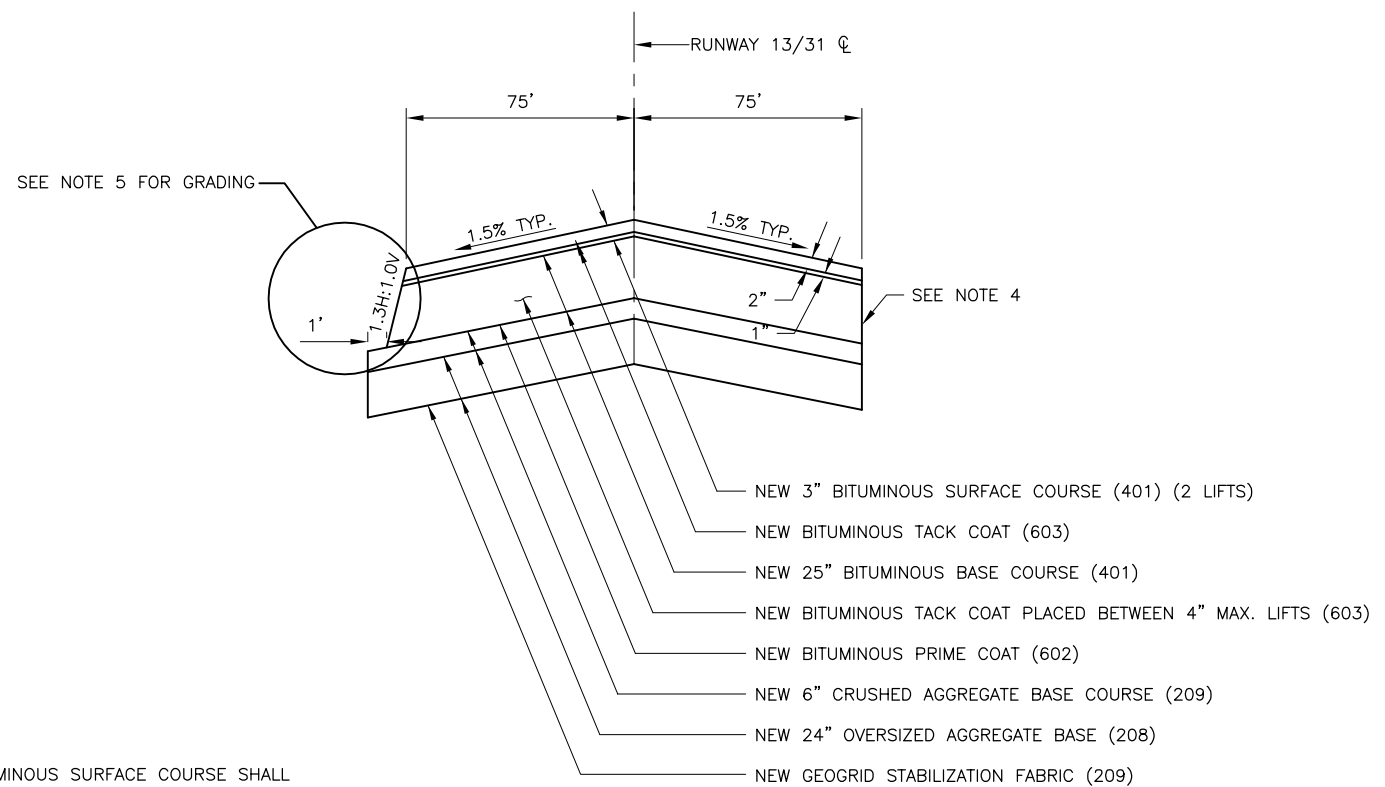
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| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 34 OF 84 SHEETS |

**PE092**

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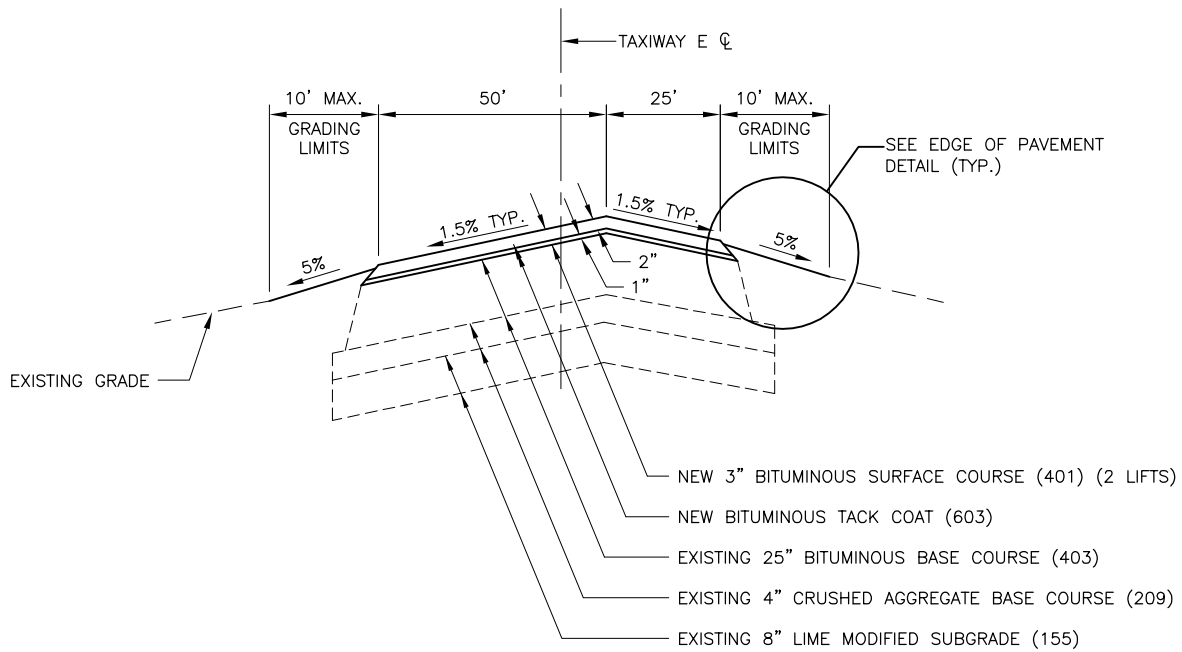
**SECTION A-A (PHASE 3)**  
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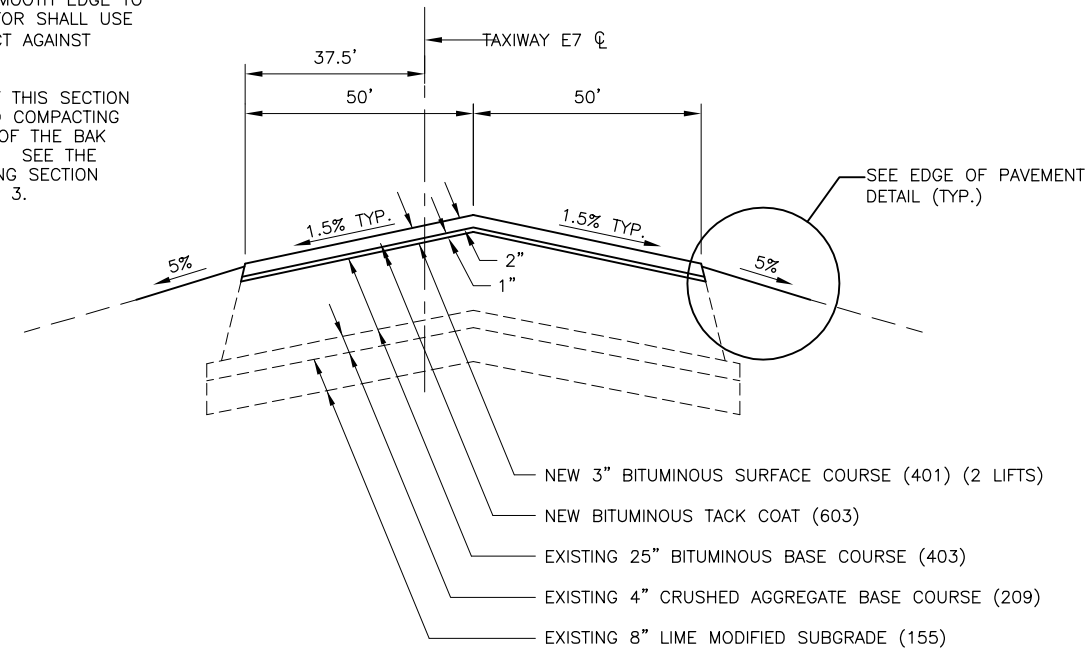
**SECTION B-B (PHASE 3)**  
N.T.S.

**NOTES:**

1. NEW BITUMINOUS SURFACE COURSE SHALL BE PLACED IN 2 LIFTS. THE FIRST SHALL BE 1" THICK FOR ESTABLISHING GRADE AND THE FINAL LIFT SHALL BE 2" THICK.
2. NEW BITUMINOUS BASE COURSE SHALL BE PLACED IN MAXIMUM 4" LIFTS.
3. BITUMINOUS TACK COAT SHALL BE PLACED BETWEEN EACH LIFT.
4. THIS EDGE ABUTS TXY E6. CONTRACTOR SHALL SAWCUT ADJACENT EDGE AS NECESSARY TO PROVIDE A SMOOTH EDGE TO COMPACT AGAINST. CONTRACTOR SHALL USE A "PUP" ROLLER TO COMPACT AGAINST ADJACENT STRUCTURES.
5. GRADING FOR THE EDGES OF THIS SECTION SHALL INCLUDE PLACING AND COMPACTING FILL MATERIAL IN THE AREA OF THE BAK PAVEMENTS TO BE REMOVED. SEE THE RUNWAY SAFETY AREA GRADING SECTION DETAIL ON TYPICAL SECTIONS 3.



**SECTION C-C (PHASE 3)**  
N.T.S.



**SECTION D-D (PHASE 3)**  
N.T.S.

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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 TYPICAL SECTIONS 1**

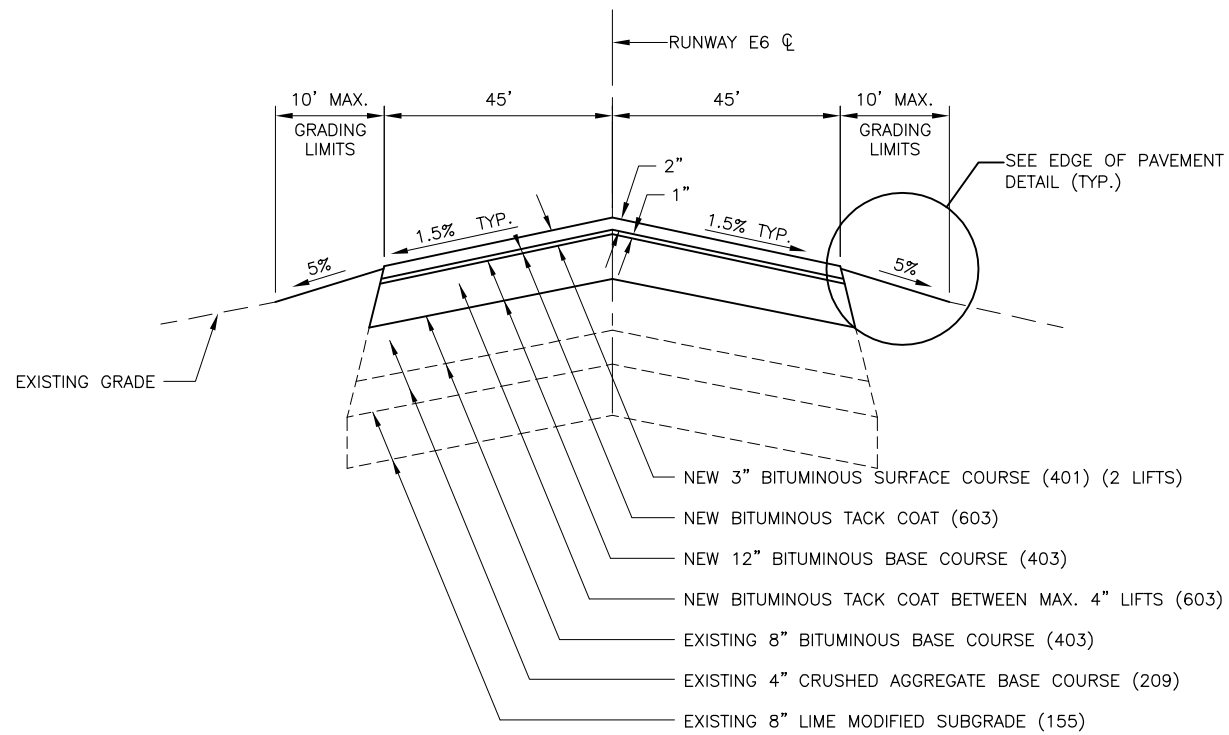
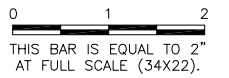
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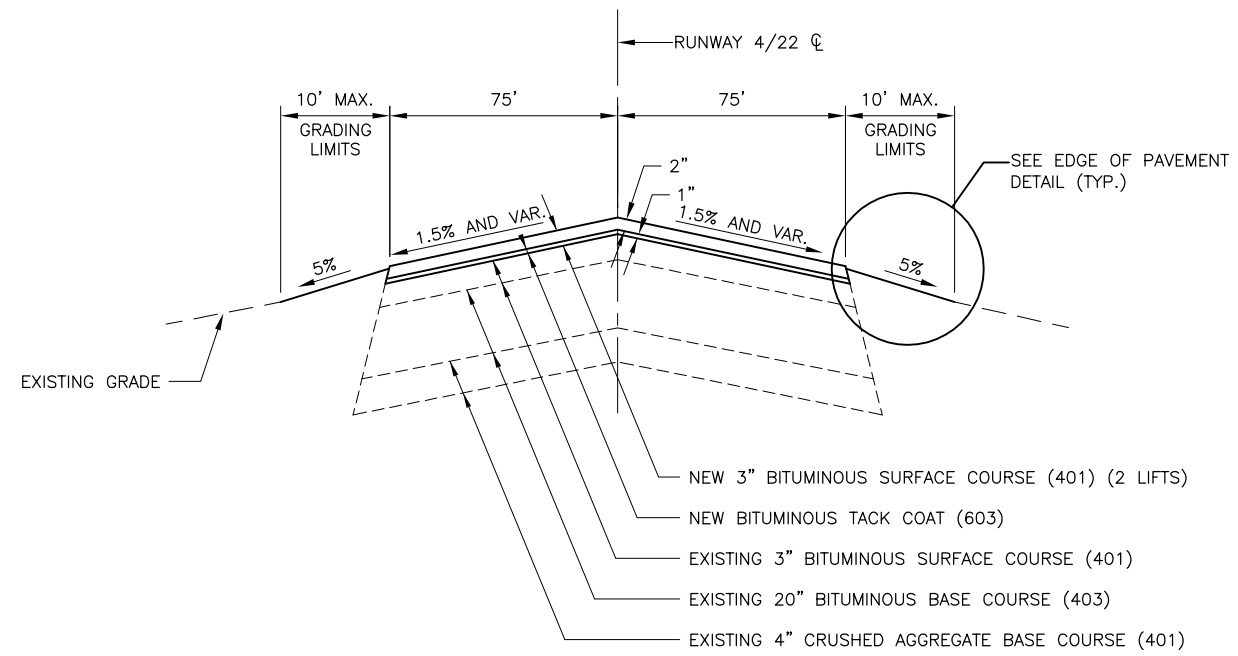


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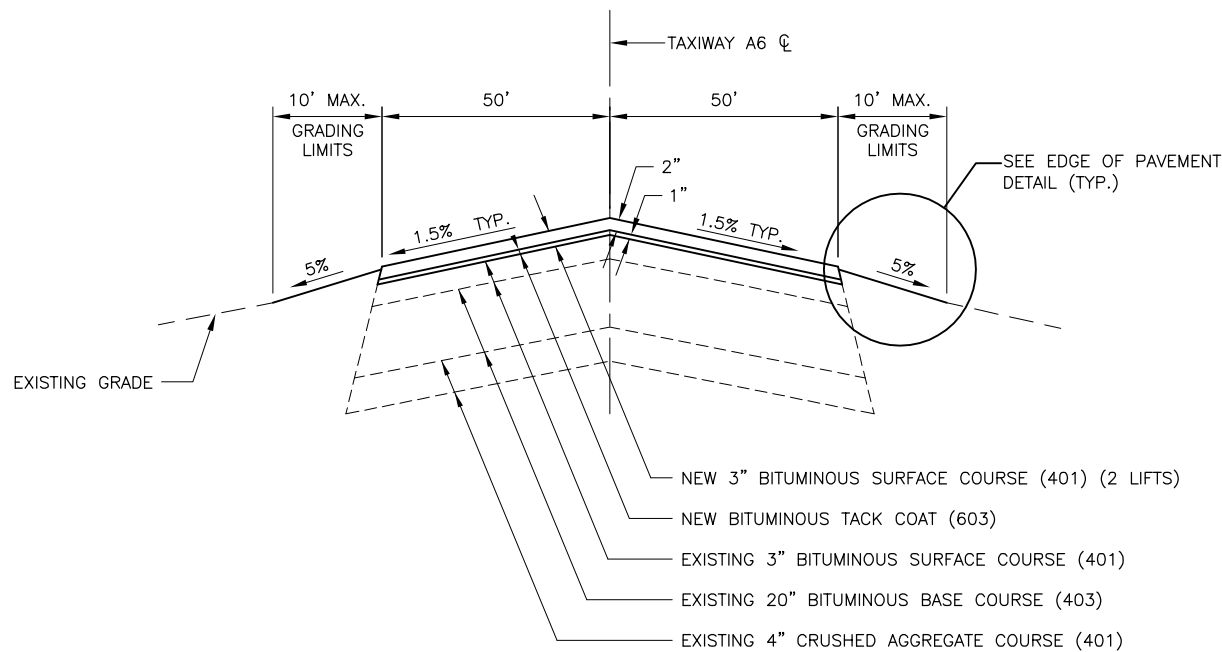
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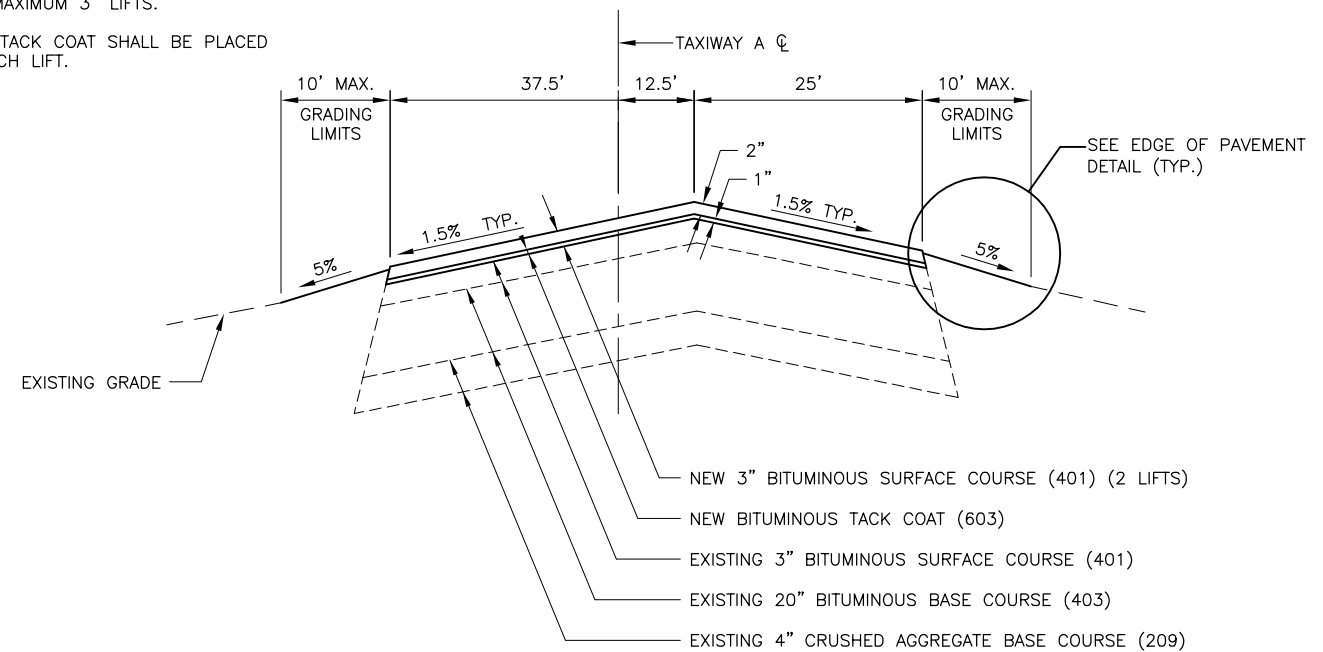
**SECTION F-F (PHASE 1)**  
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**NOTES:**

1. NEW BITUMINOUS SURFACE COURSE SHALL BE PLACED IN 2 LIFTS. THE FIRST LIFT SHALL BE 1" THICK FOR ESTABLISHING GRADE AND THE FINAL LIFT SHALL BE 2" THICK.
2. NEW BITUMINOUS BASE COURSE SHALL BE PLACED IN MAXIMUM 3" LIFTS.
3. BITUMINOUS TACK COAT SHALL BE PLACED BETWEEN EACH LIFT.



**SECTION G-G (PHASE 1)**  
N.T.S.



**SECTION H-H (PHASE 1)**  
N.T.S.

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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 TYPICAL SECTIONS 2**

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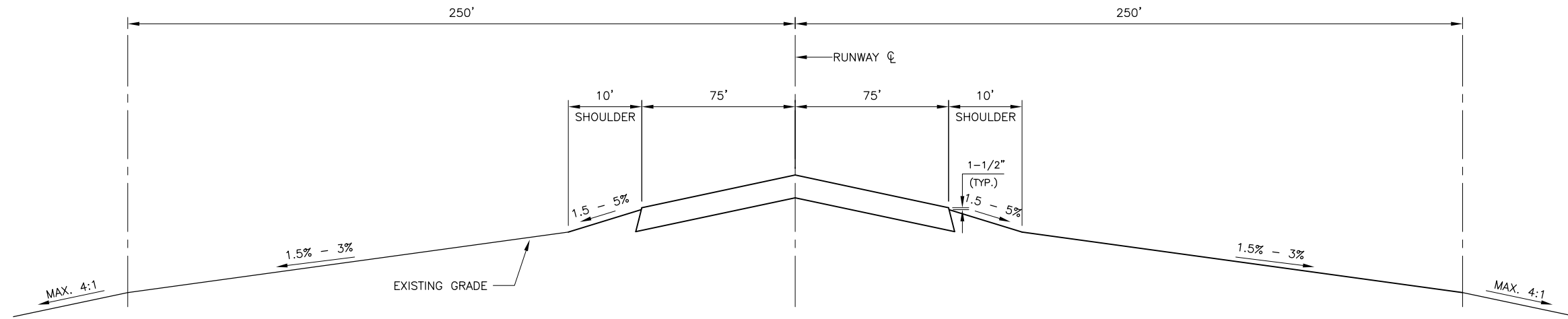
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| SHEET         | 36 OF 84 SHEETS |

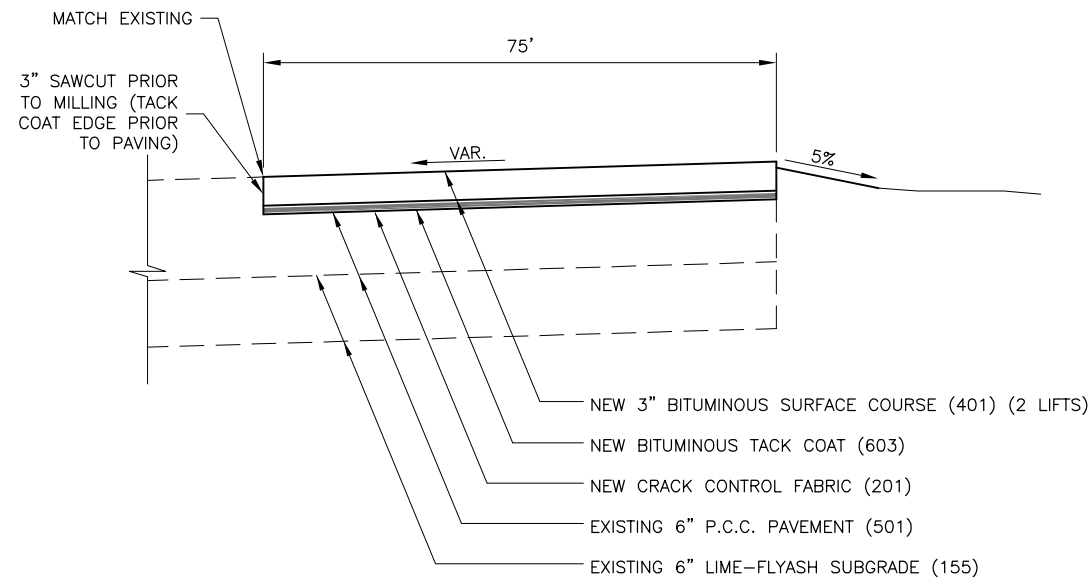
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0 1 2  
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 AT FULL SCALE (34X22).



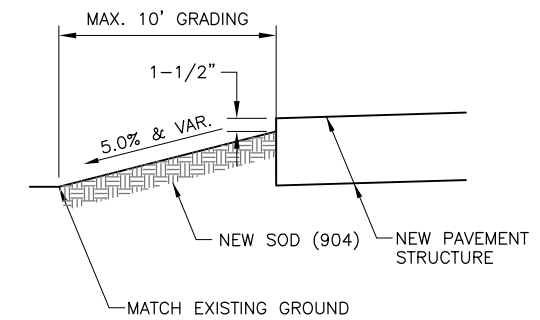
**RUNWAY SAFETY AREA GRADING SECTION**  
 N.T.S.



**SECTION I-I (TAXILANE) - PHASE 2**  
 N.T.S.

**NOTES:**

1. THE TAXILANE A4 PAVEMENT REMOVAL AREA SHALL BE GRADED TO DRAIN. ALL GRADING WITHIN THE RUNWAY SAFETY AREA SHALL BE WITHIN THE TOLERANCES SHOWN ON THE RUNWAY SAFETY AREA GRADING SECTION.
2. NEW BITUMINOUS SURFACE COURSE SHALL BE PLACED IN 2 LIFTS. THE FIRST LIFT SHALL BE 1" THICK FOR ESTABLISHING GRADE AND THE FINAL LIFT SHALL BE 2" THICK.
3. BITUMINOUS TACK COAT SHALL BE PLACED BETWEEN EACH LIFT.



**EDGE OF PAVEMENT DETAIL**  
 N.T.S.

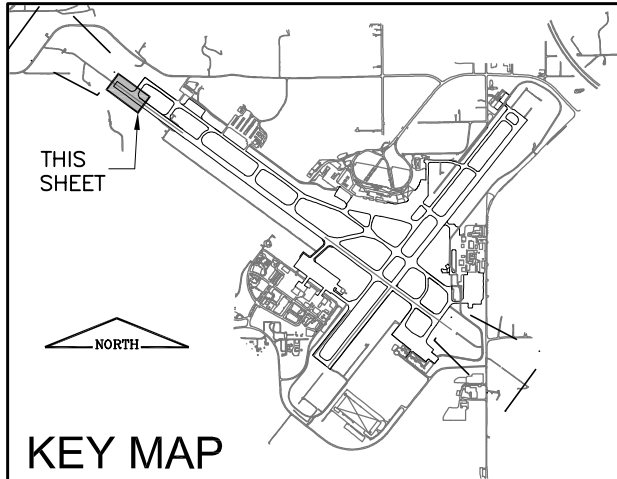
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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXILANE**  
**TYPICAL SECTIONS 3**

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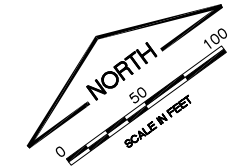
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| SHEET 37 OF 84 SHEETS                               |              |



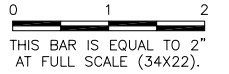
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 THE CENTERLINE PROFILE REFERS TO THE BASELINE OF THE ASSOCIATED TAXIWAY OR RUNWAY. PAVEMENTS SHALL BE CONSTRUCTED WITH THE CROWN AS SHOWN ON THE TYPICAL SECTION SHEET. PAVEMENT MARKING SHALL BE PLACED AS SHOWN ON THE MARKING PLAN SHEETS.



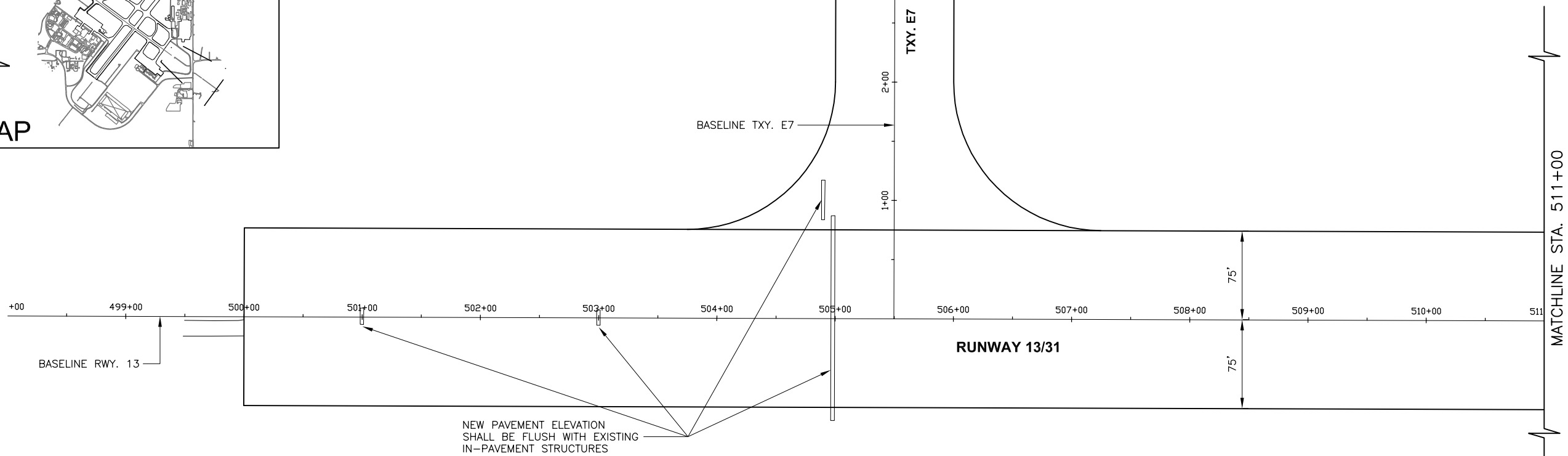
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 PIA\_ALIGN  
 BASE\_PROP\_GEO  
 PIA\_STAKE  
 KEYMAP

**PE092**

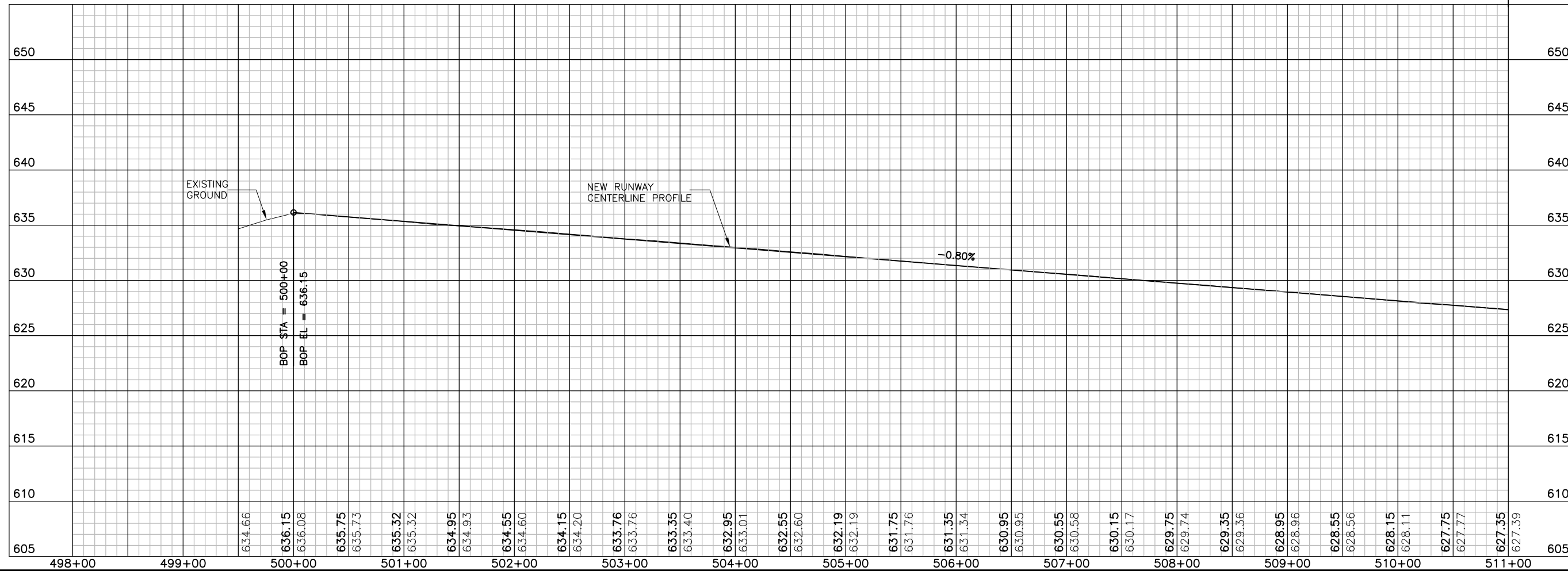
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**KEY MAP**



SCALES:  
 1" = 50' HOR  
 1" = 5' VER



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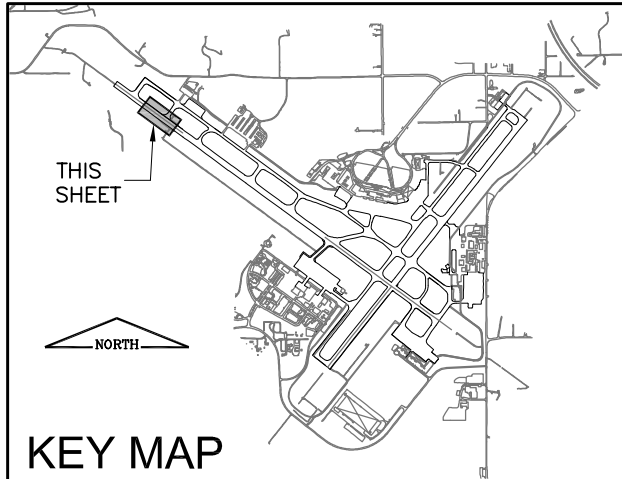
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 STA. 500+00 TO STA. 511+00**

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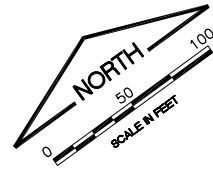


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 AIP PROJ. NO. 3-17-0080-XX  
 SHEET 38 OF 84 SHEETS





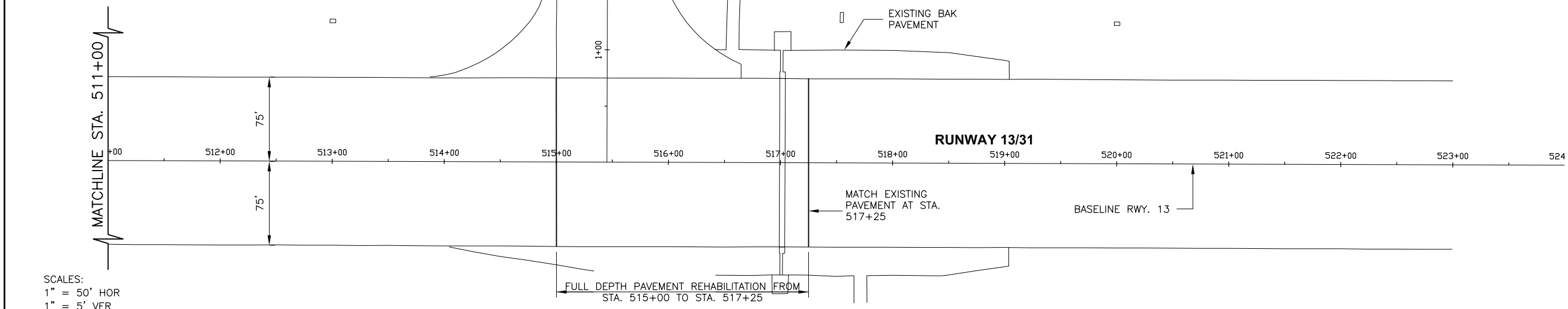
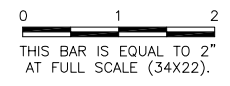
**NOTE:**  
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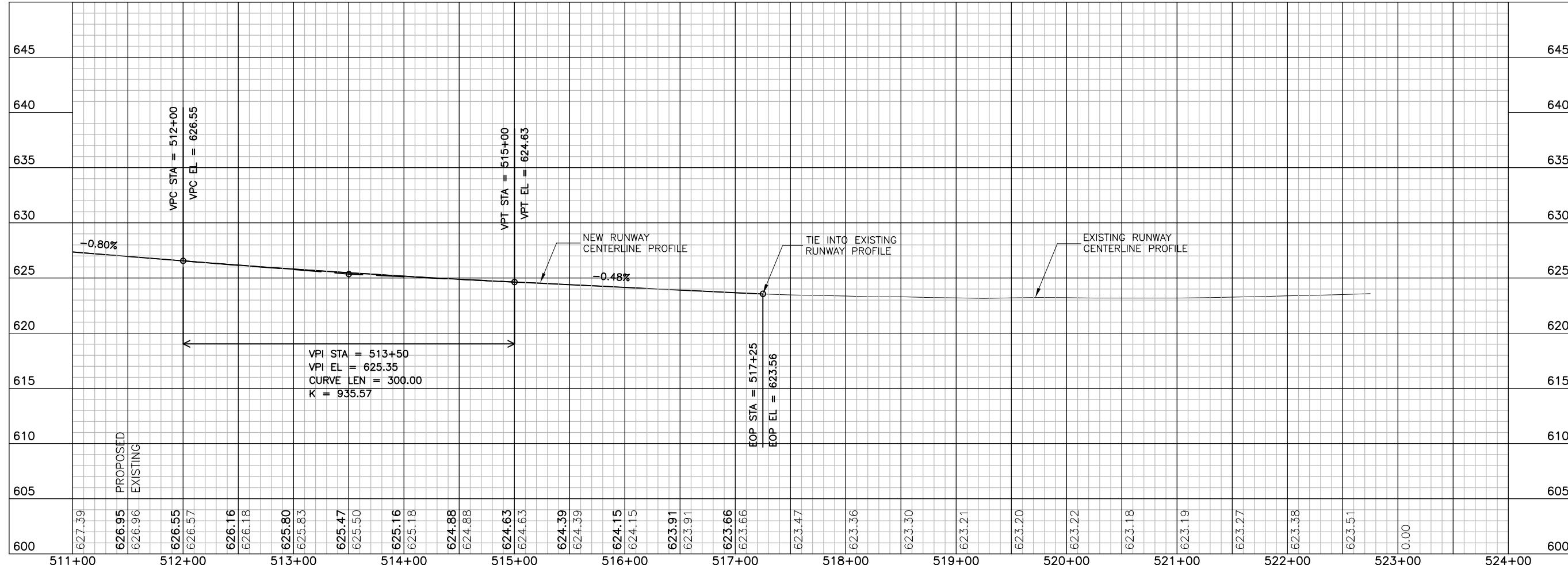
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 PIA\_STAKE  
 KEYMAP

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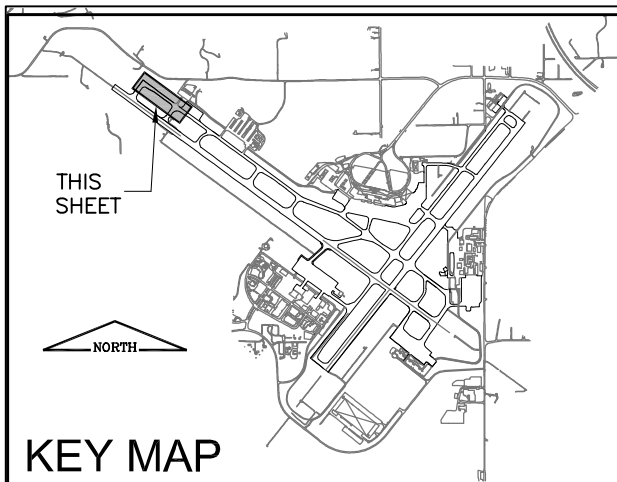
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 1" = 50' HOR  
 1" = 5' VER



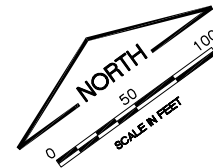
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**RUNWAY 13 PLAN AND PROFILE**  
**STA. 511+00 TO STA. 516+75**

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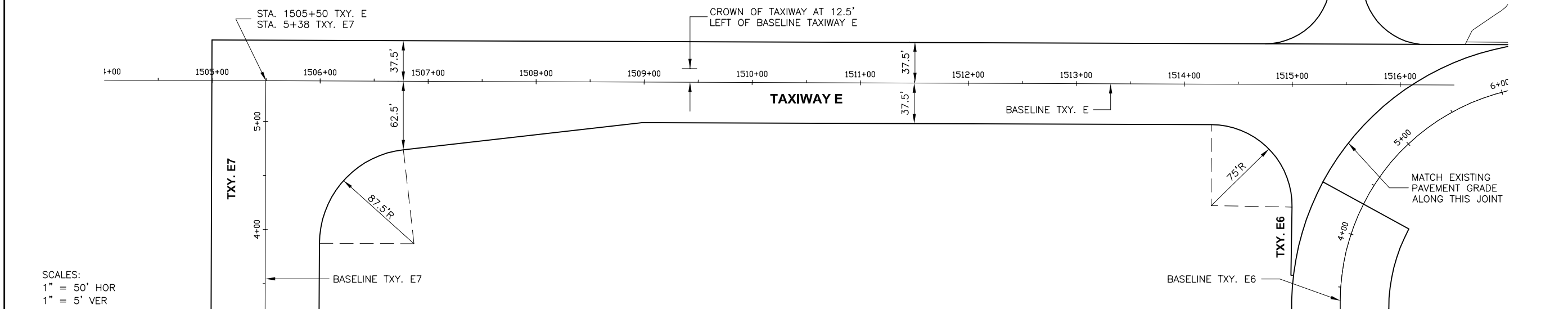
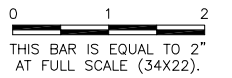
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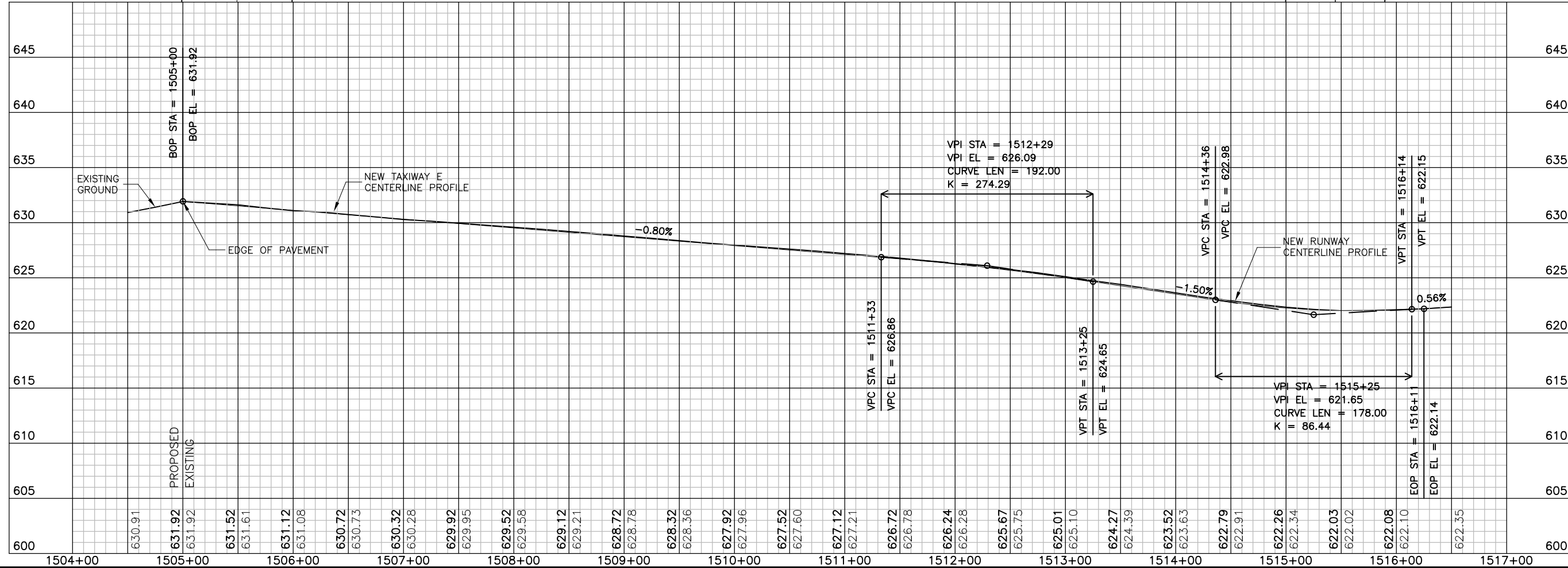
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 PIA\_ALIGN  
 PIA\_STAKE  
 KEYMAP

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SCALES:  
 1" = 50' HOR  
 1" = 5' VER



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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**

**TAXIWAY E PLAN AND PROFILE**  
**STA. 1505+00 TO STA. 1516+50**

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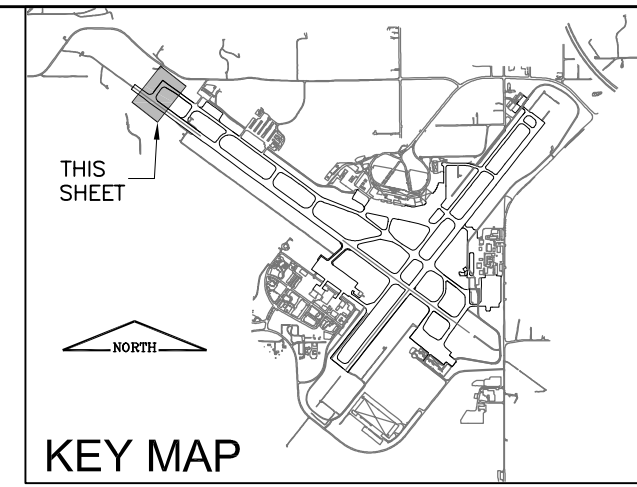
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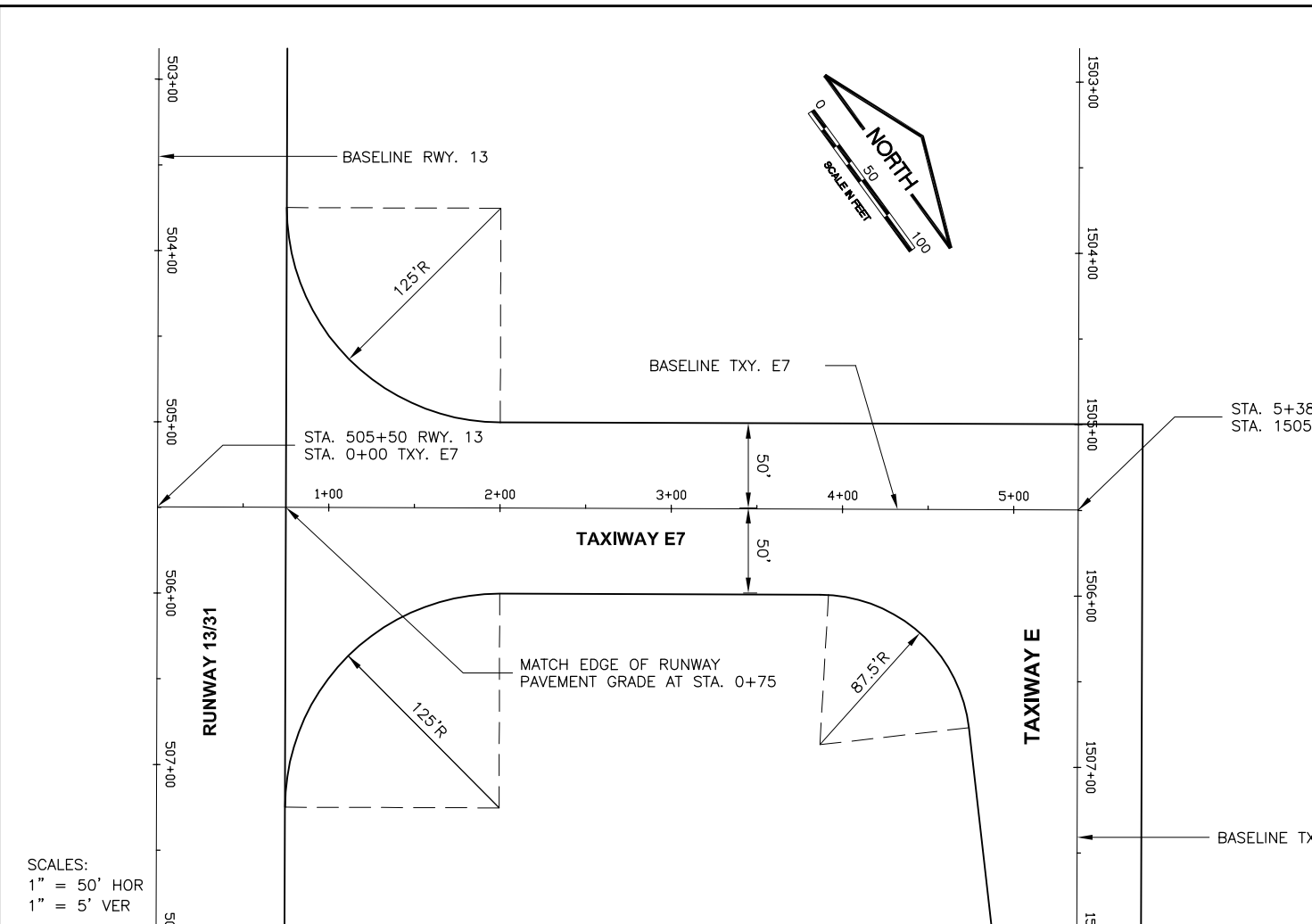
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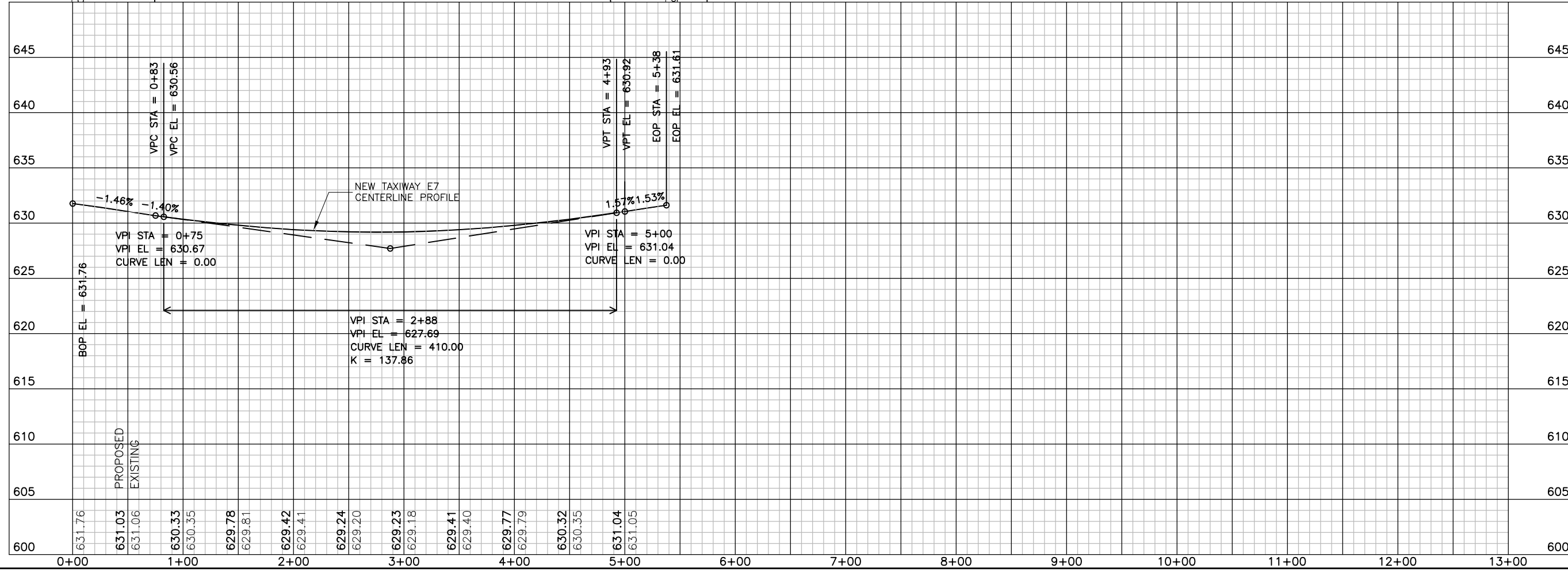
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**NOTE:**  
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 SHALL BE CONSTRUCTED WITH THE CROWN AS SHOWN  
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 SHALL BE PLACED AS SHOWN ON THE MARKING PLAN  
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SCALES:  
 1" = 50' HOR  
 1" = 5' VER



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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 TAXIWAY E7 PLAN AND PROFILE  
 STA. 0+75 TO STA. 5+38**

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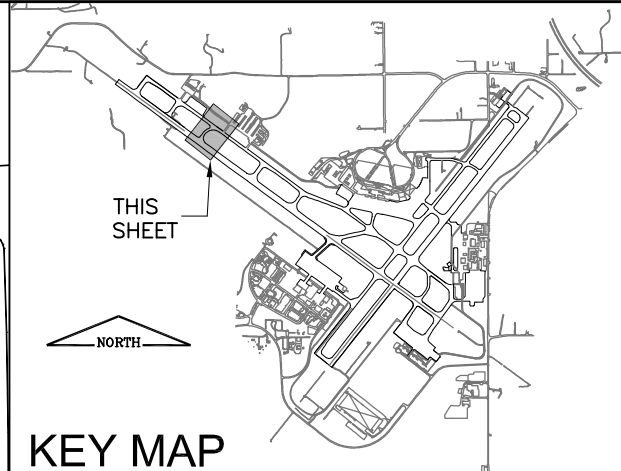
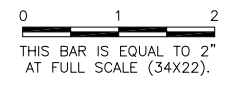
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| SHEET         | 41 OF 84 SHEETS |

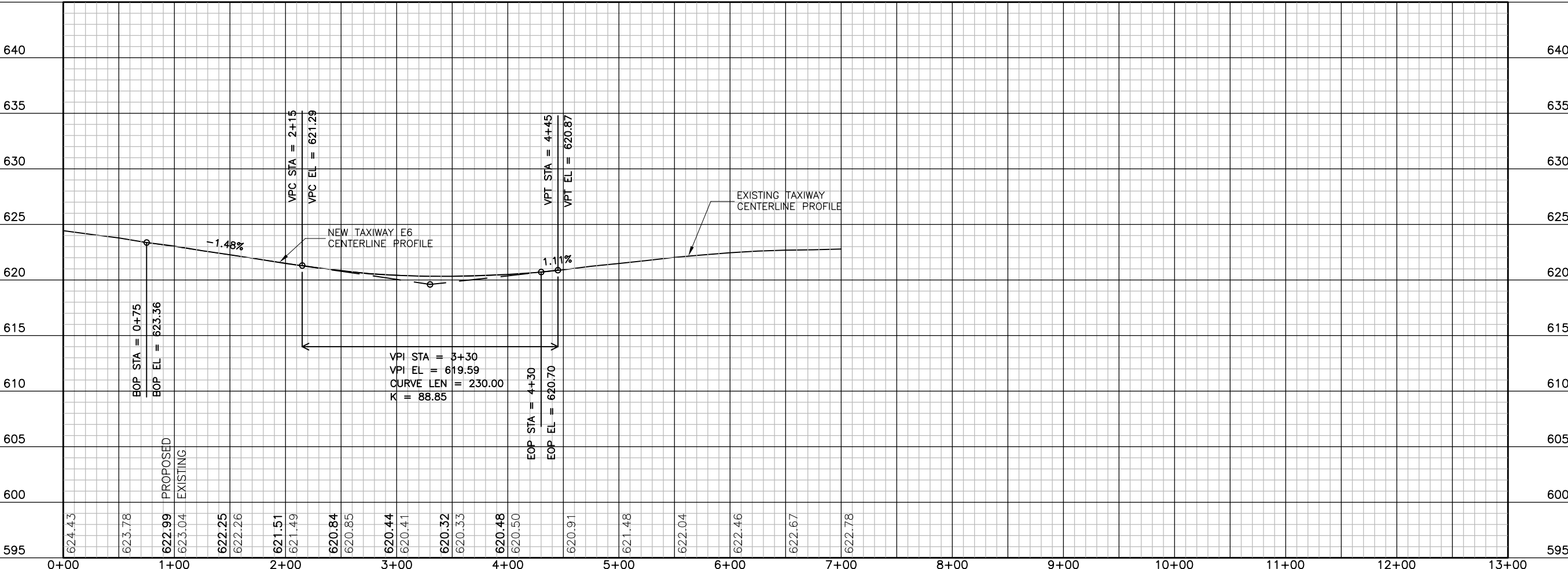
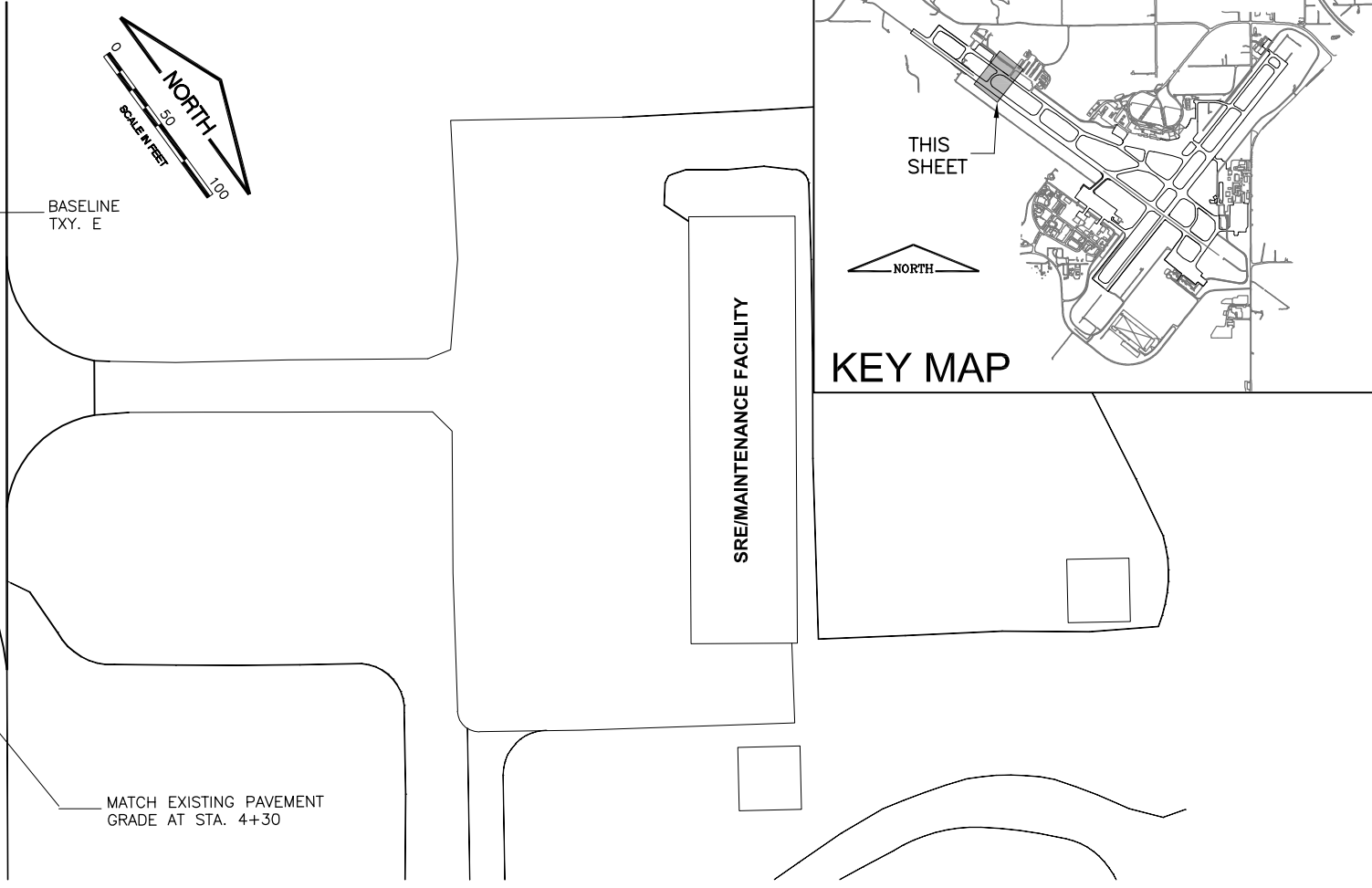
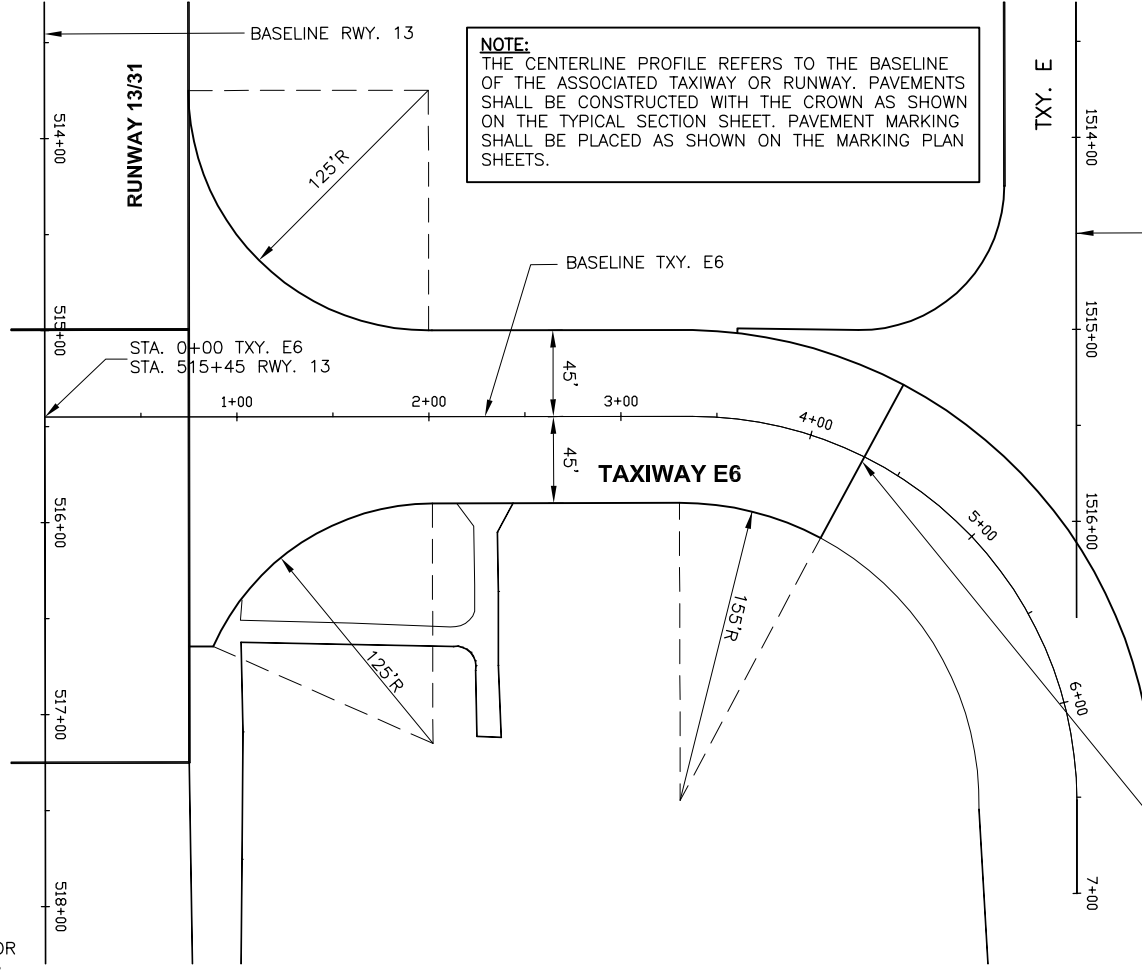


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SCALES:  
 1" = 50' HOR  
 1" = 5' VER



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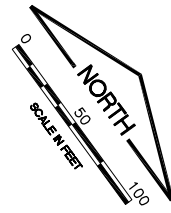
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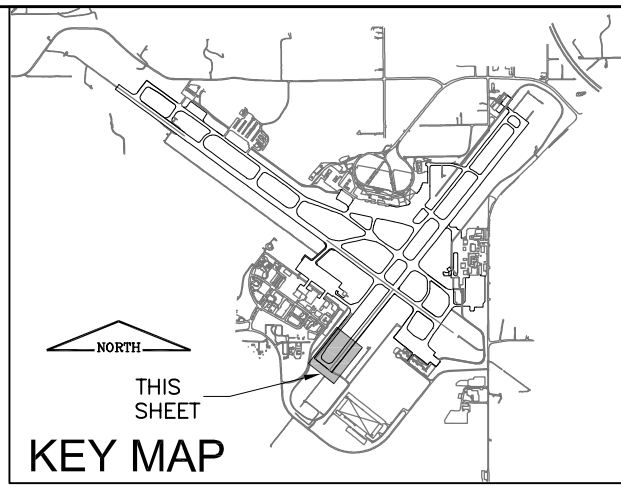
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**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**

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| DESIGN BY:    | TJH          |
| DRAWN BY:     | CMT          |
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| DATE:         | MAY 13, 2011 |
| JOB No:       | 100610300    |
| IL PROJ. NO.  | PIA-4079     |
| AIP PROJ. NO. | 3-17-0080-XX |



**NOTE:**  
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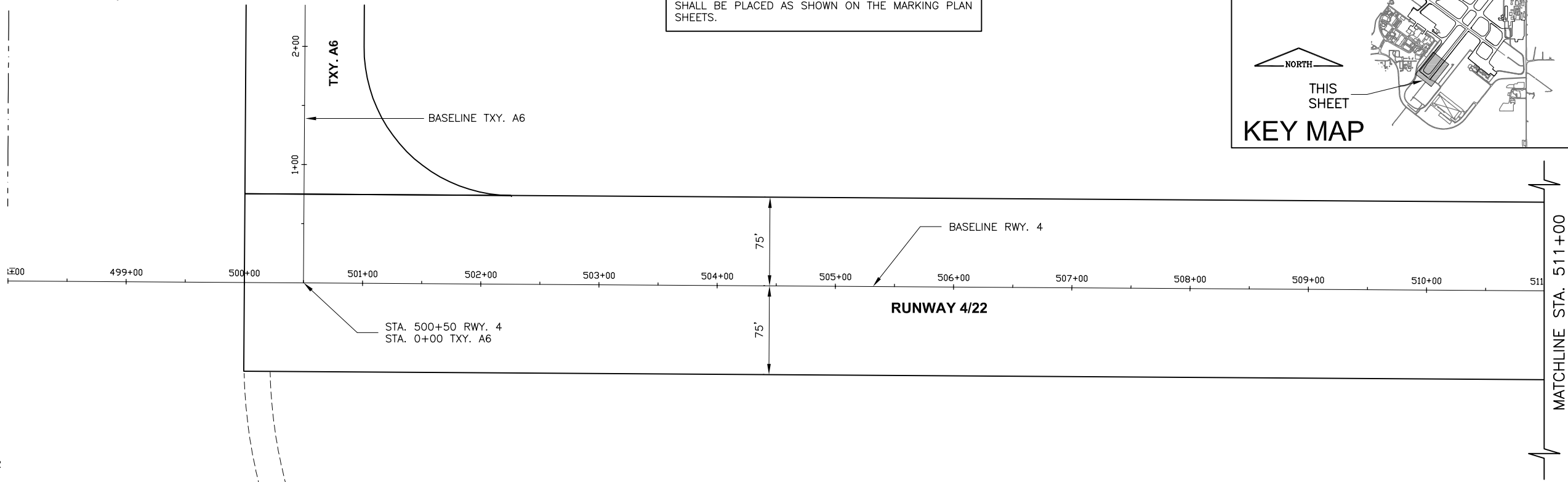


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 PLOT DATE: 5/18/2011 8:24 AM  
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 PIA\_ALIGN  
 BASE\_PROP\_GEO  
 PIA-Base\_Updated  
 PIA\_STAKE  
 KEYMAP

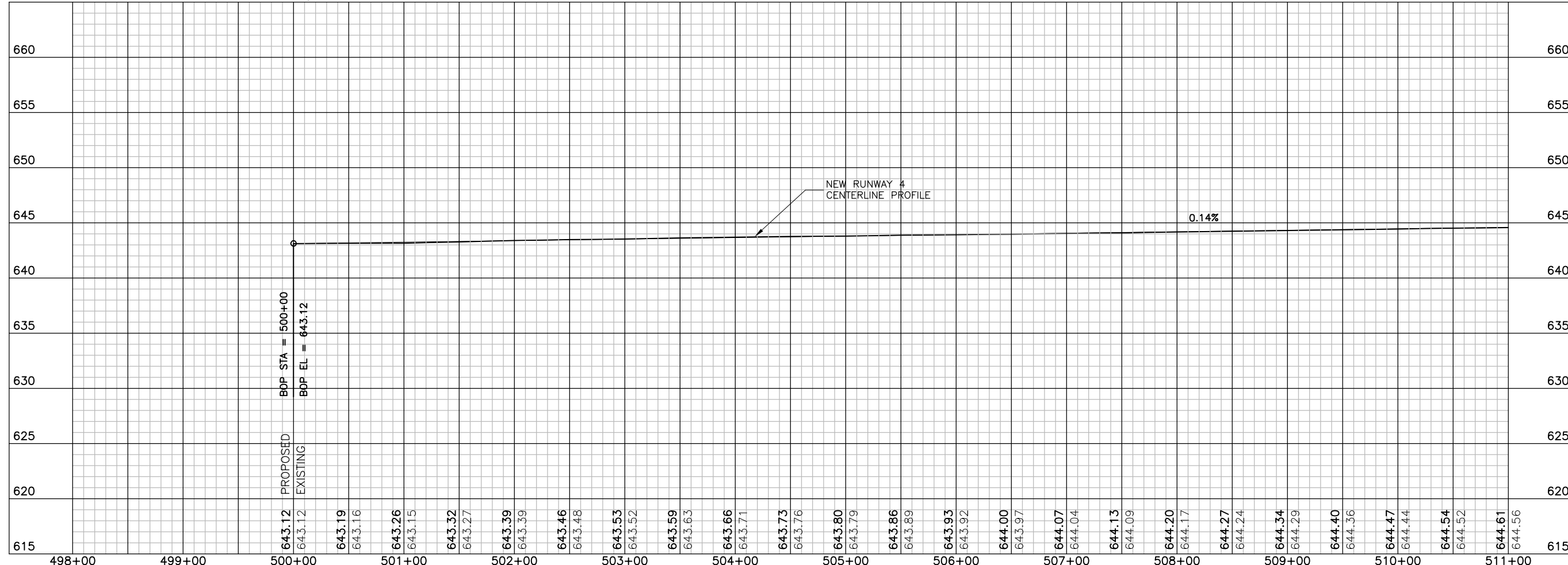
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0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).



SCALES:  
 1" = 50' HOR  
 1" = 5' VER



**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**RUNWAY 4 PLAN AND PROFILE SHEET**  
**STA. 500+00 TO STA. 511+00**

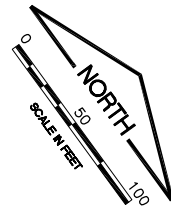
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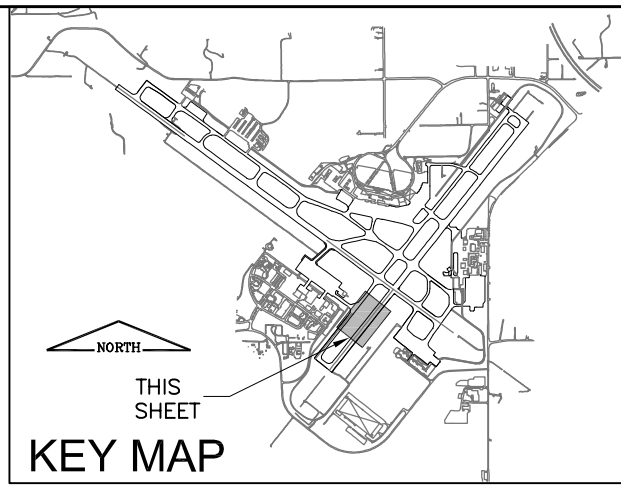
**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**

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| DESIGN BY:    | TJH          |
| DRAWN BY:     | CMT          |
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| DATE:         | MAY 13, 2011 |
| JOB No:       | 100610300    |
| IL PROJ. NO.  | PIA-4079     |
| AIP PROJ. NO. | 3-17-0080-XX |

SHEET 43 OF 84 SHEETS



**NOTE:**  
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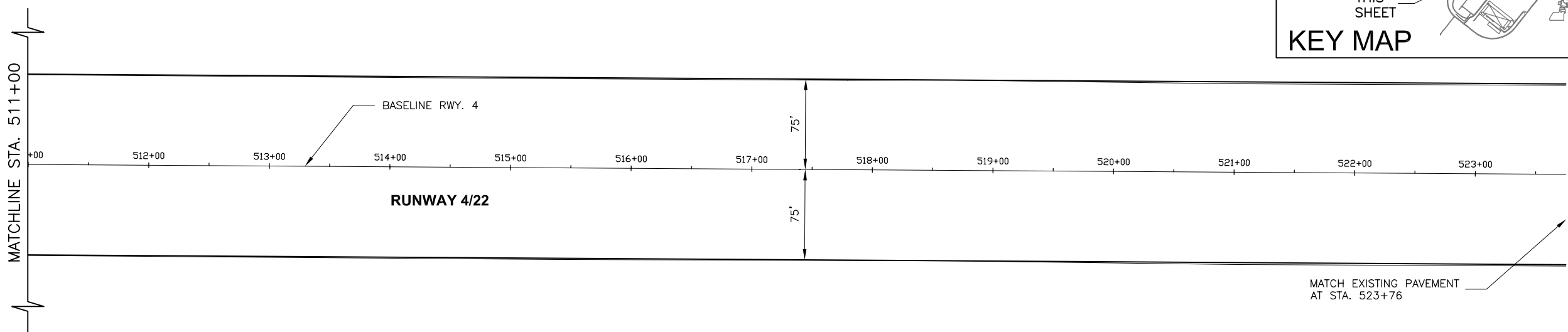


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 BASE\_PROP\_GEO  
 Pia-Base\_Updated  
 PIA\_ALIGN  
 PIA\_STAKE  
 KEYMAP

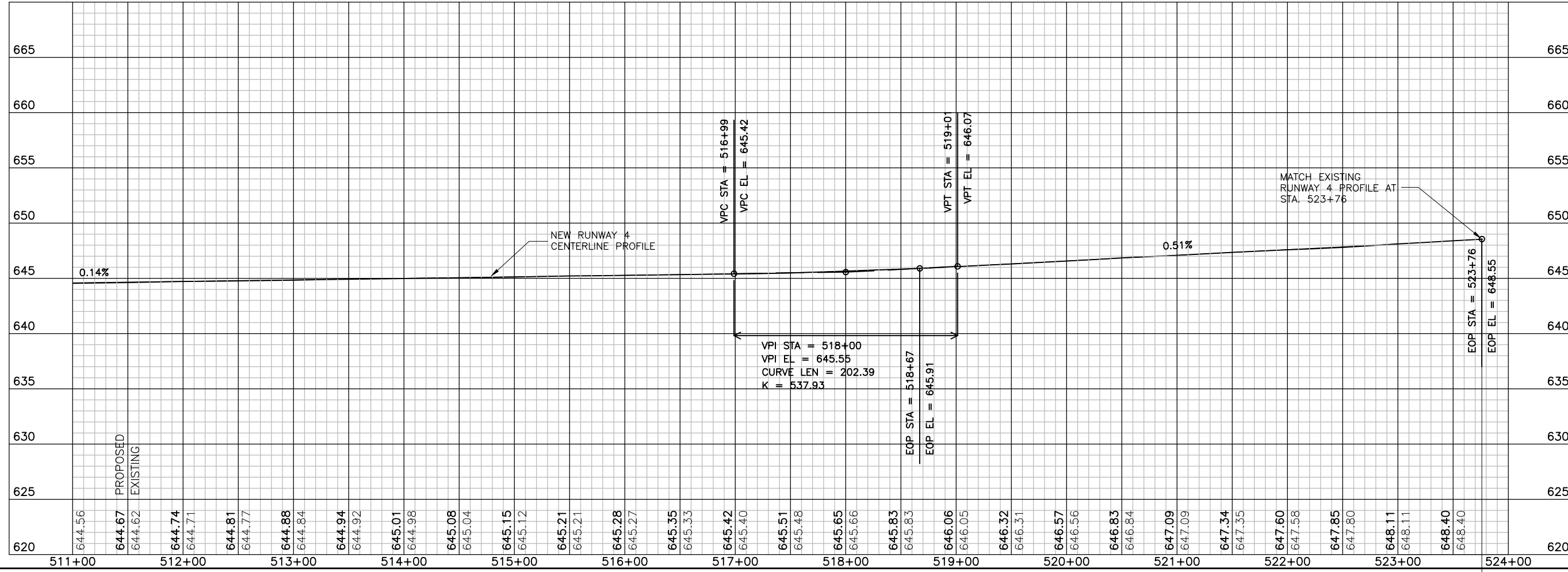
**PE092**

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0 1 2  
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 AT FULL SCALE (34X22).



SCALES:  
 1" = 50' HOR  
 1" = 5' VER



**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**

**RUNWAY 4 PLAN AND PROFILE SHEET**  
**STA. 511+00 TO STA. 518+67**

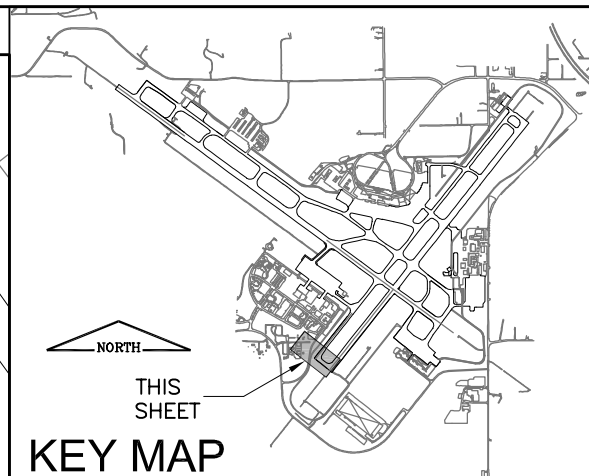
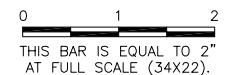
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| IL PROJ. NO.  | PIA-4079        |
| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 44 OF 84 SHEETS |

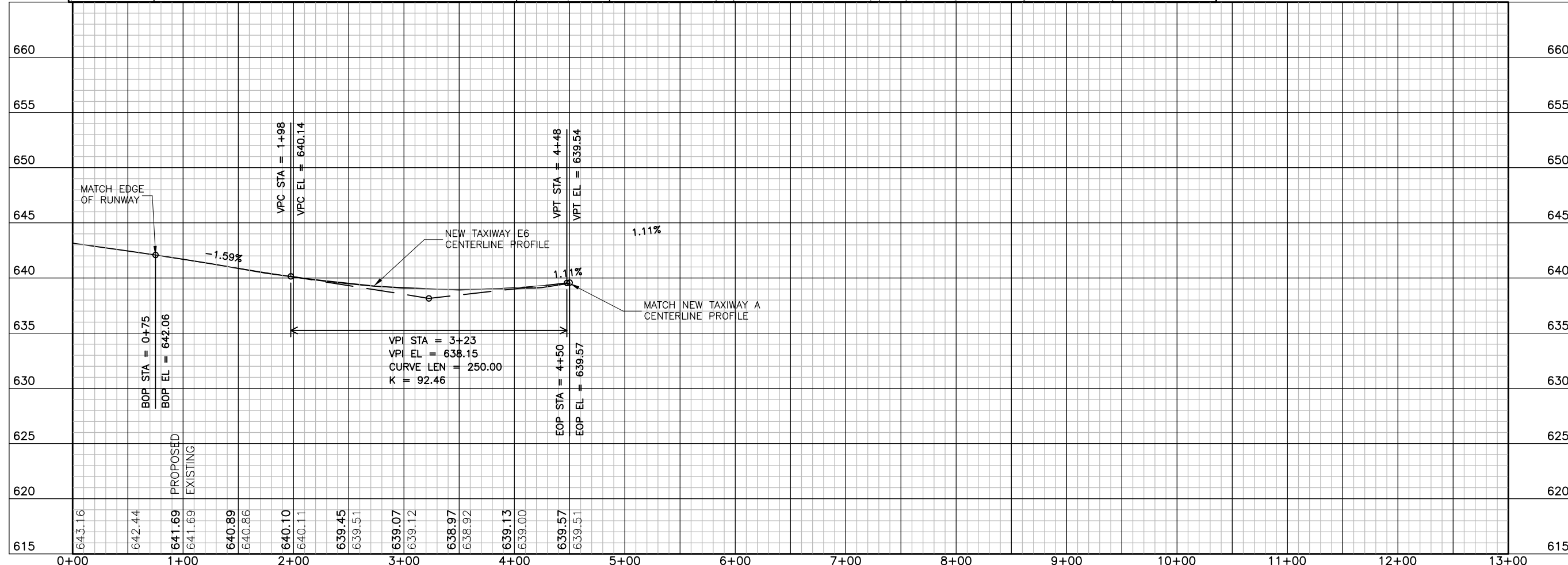
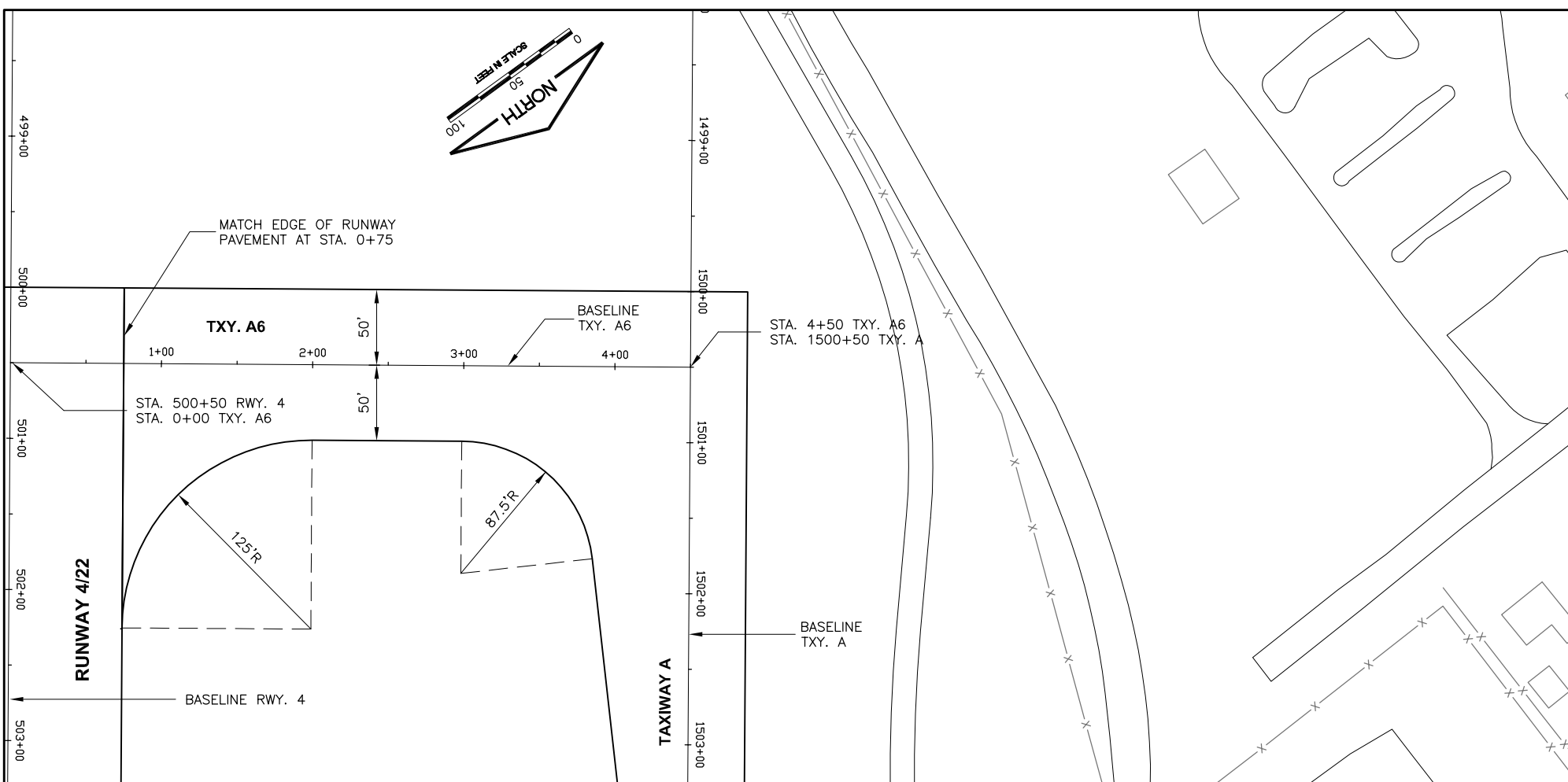
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**NOTE:**  
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SCALES:  
 1" = 50' HOR  
 1" = 5' VER



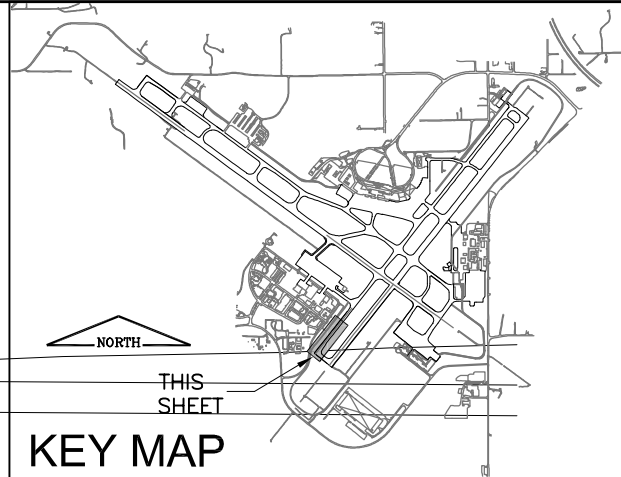
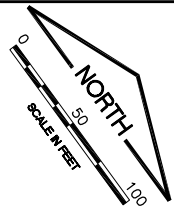
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**PEORIA, ILLINOIS**  
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**TAXIWAY A6 PLAN AND PROFILE SHEET**  
**STA. 0+75 TO STA. 4+50**

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| IL PROJ. NO.  | PIA-4079     |
| AIP PROJ. NO. | 3-17-0080-XX |



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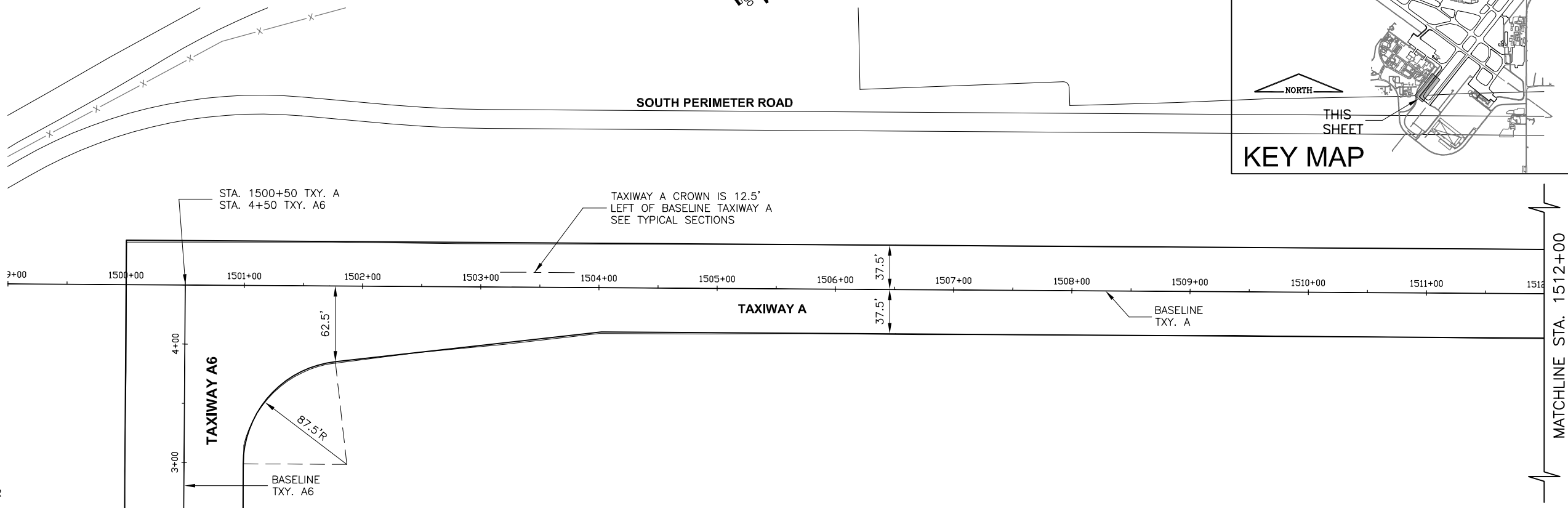


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 FILE: TAXIWAY A PLAN AND PROFILE STA.  
 UPDATE BY: TJ Heavisides  
 PLOT DATE: 5/18/2011 8:25 AM  
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 PIA\_STAKE  
 KEYMAP

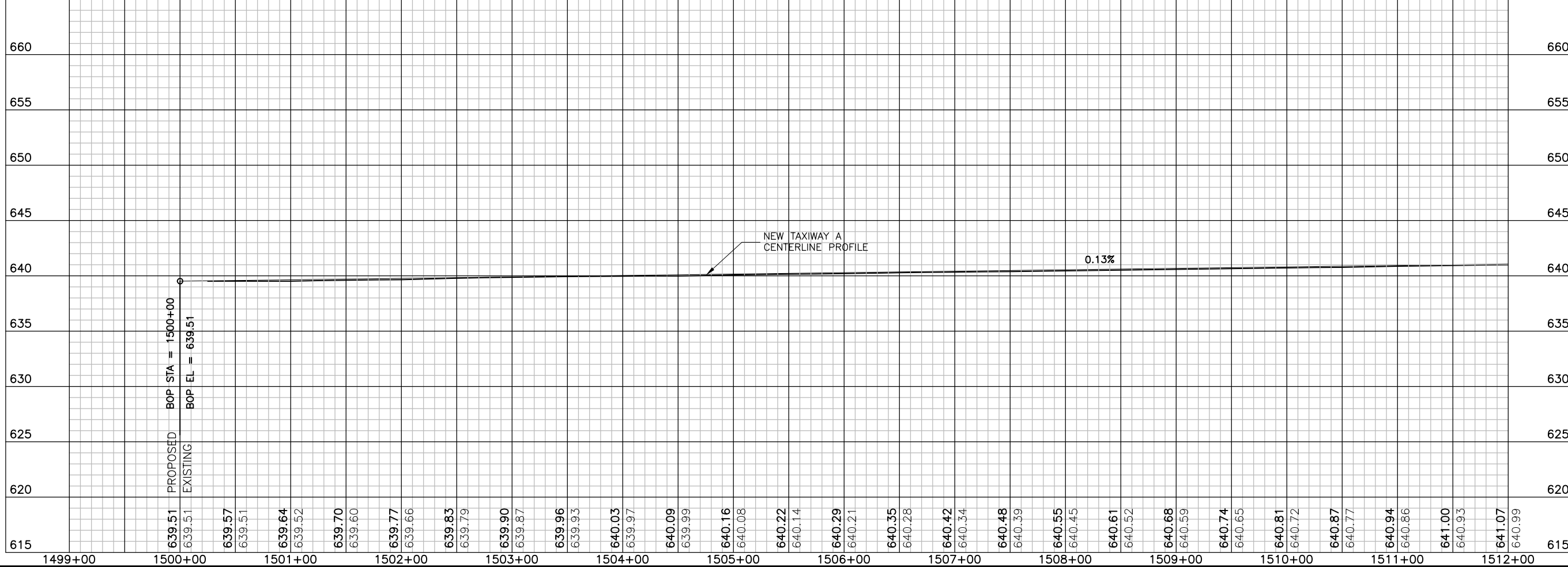
**PE092**

| REVISIONS |    |      |
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0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).



SCALES:  
 1" = 50' HOR  
 1" = 5' VER



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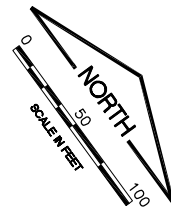
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**TAXIWAY A PLAN AND PROFILE**  
**STA. 1500+00 TO STA. 1512+00**

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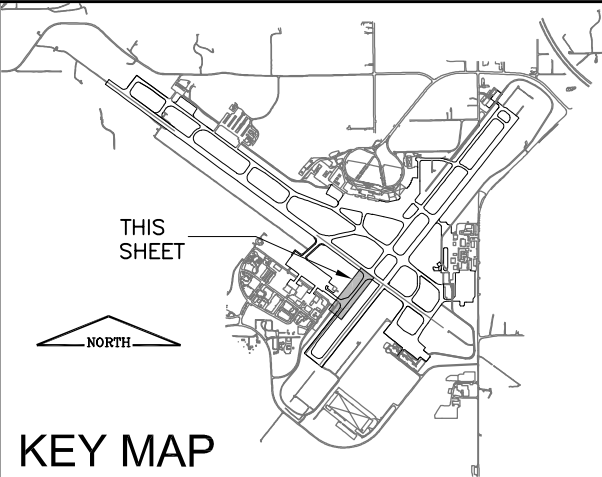
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| JOB No:                    | 100610300    |
| IL. PROJ. NO. PIA-4079     |              |
| AIP PROJ. NO. 3-17-0080-XX |              |
| SHEET 46 OF 84 SHEETS      |              |



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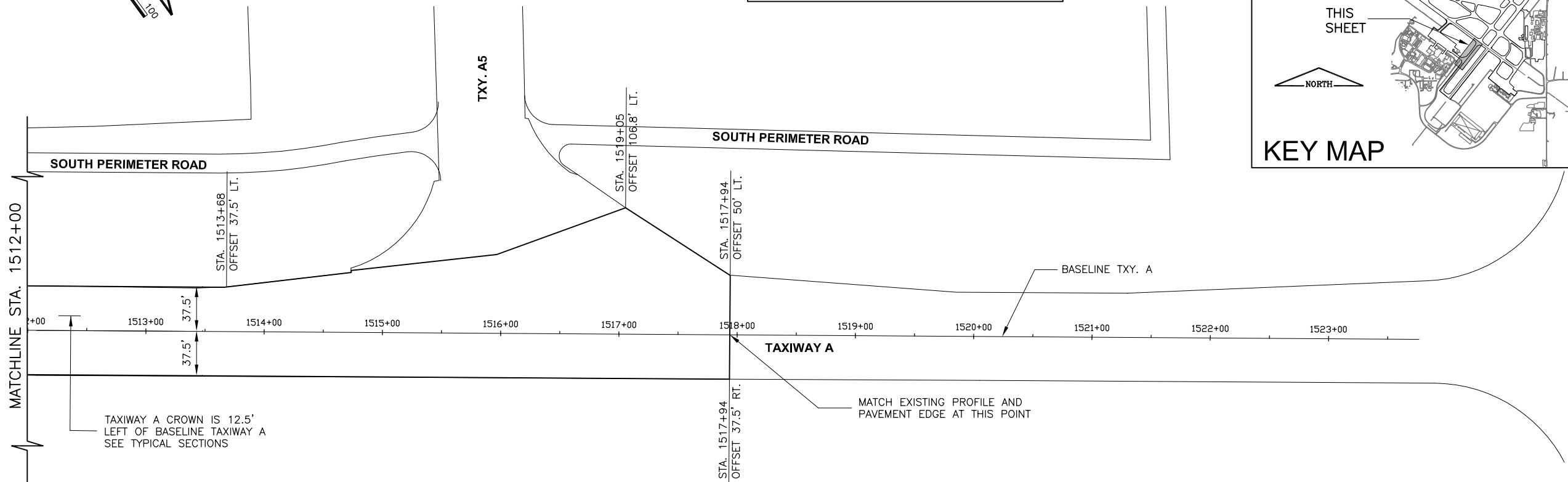


\\SPL\_CELERRA\A\PeoriaAP\100610300\Draw\Sheets  
 FILE: TAXIWAY A PLAN AND PROFILE SHEET  
 UPDATE BY: TJ Heavisides  
 PLOT DATE: 5/18/2011 8:25 AM  
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 EGPTBASE\_SURVEY  
 PIA\_ALIGN  
 BASE\_PROP\_GEO  
 PIA-Base\_Updated  
 PIA\_STAKE  
 KEYMAP

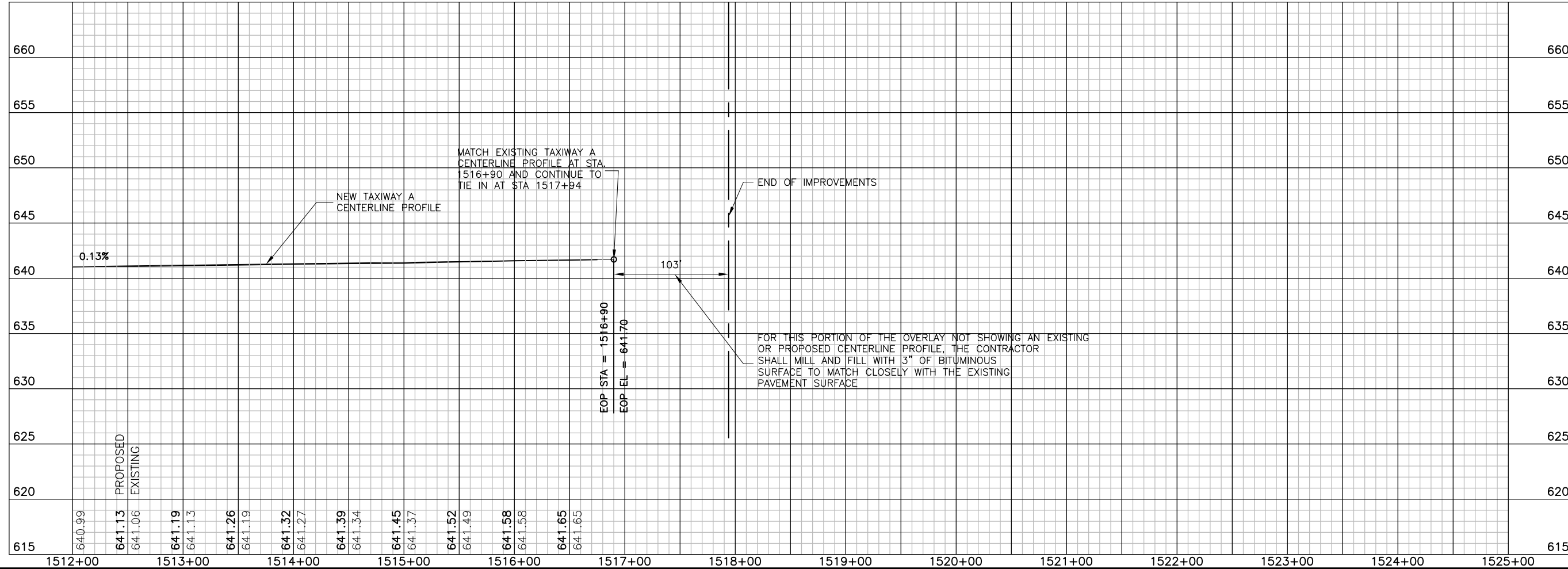
**PE092**

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0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



SCALES:  
 1" = 50' HOR  
 1" = 5' VER



**GENERAL WAYNE A. DOWNING**  
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**PEORIA, ILLINOIS**

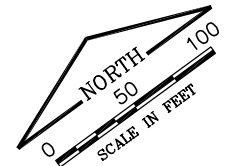
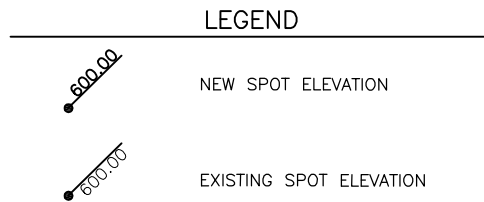
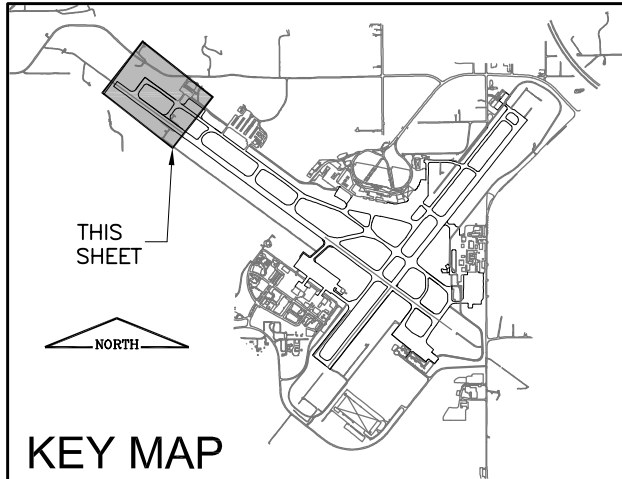
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**TAXIWAY A PLAN AND PROFILE SHEET**  
**STA. 1512+00 TO STA. 1516+90**

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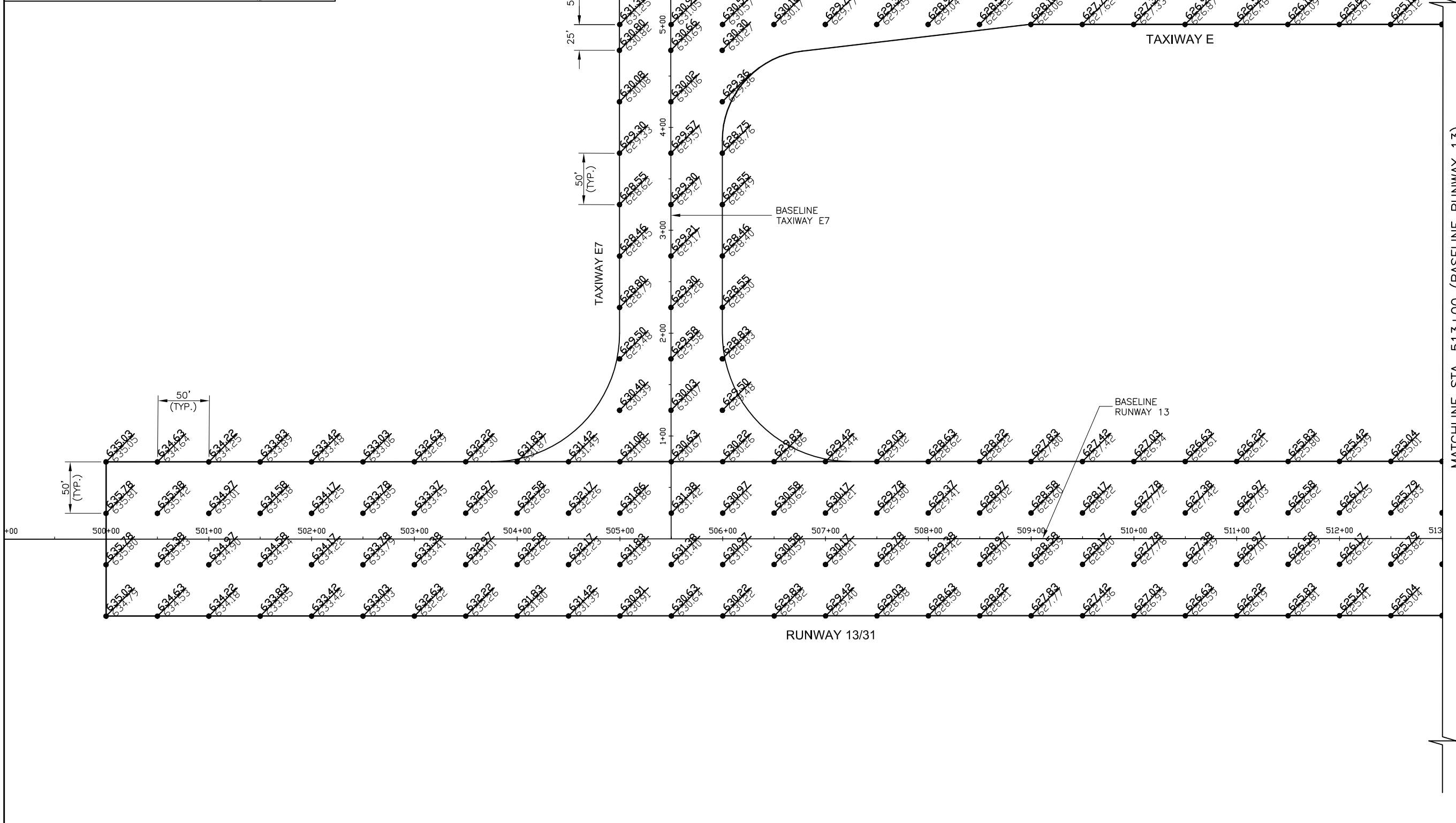
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 JOB No: 100610300  
 IL PROJ. NO. PIA-4079  
 AIP PROJ. NO. 3-17-0080-XX  
 SHEET 47 OF 84 SHEETS



KEY MAP

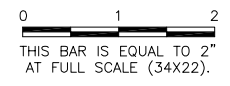


MATCHLINE STA. 513+00 (BASELINE RUNWAY 13)

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 FILE: STAKING PLAN 1.dwg  
 UPDATE BY: TJ Heavisides  
 PLOT DATE: 5/18/2011 8:25 AM  
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 KEYMAP:  
 PIA\_STAKE  
 PIA\_ALIGN  
 BASE\_PROP\_GEO

**PE092**


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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**STAKING PLAN 1**

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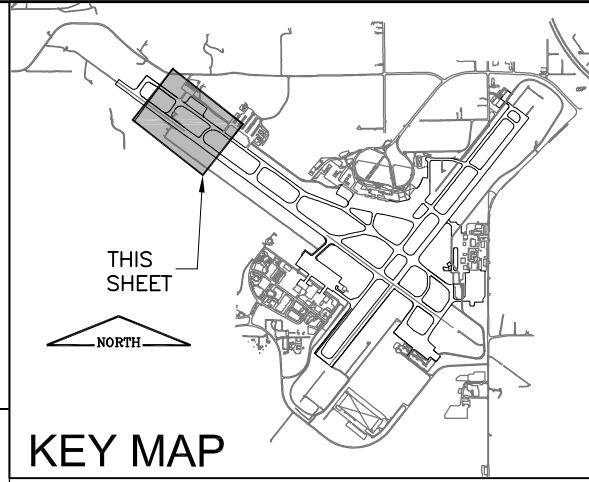
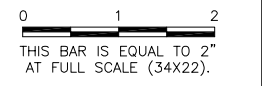
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| CHECKED BY:   | CET             |
| APPROVED BY:  | CET             |
| DATE:         | MAY 13, 2011    |
| JOB No:       | 100610300       |
| IL PROJ. NO.  | PIA-4079        |
| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 48 OF 84 SHEETS |



**PE092**

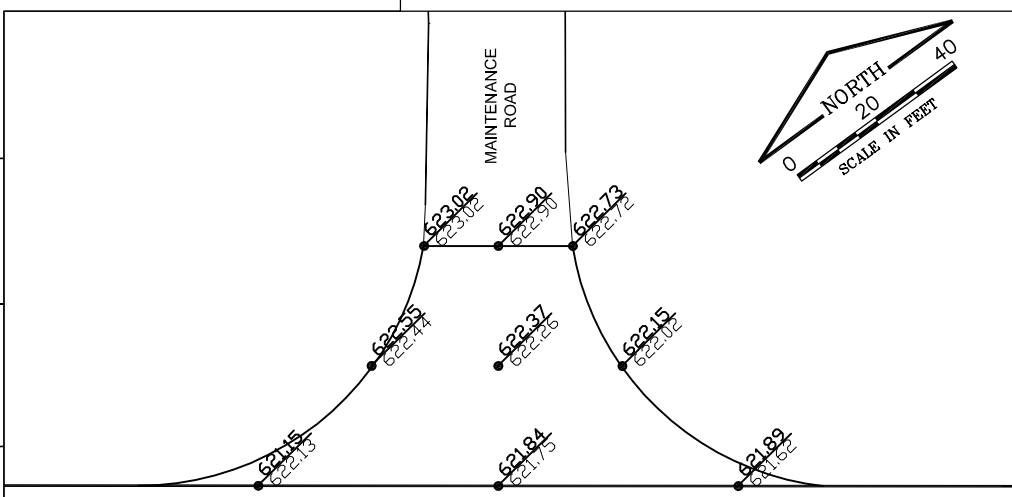
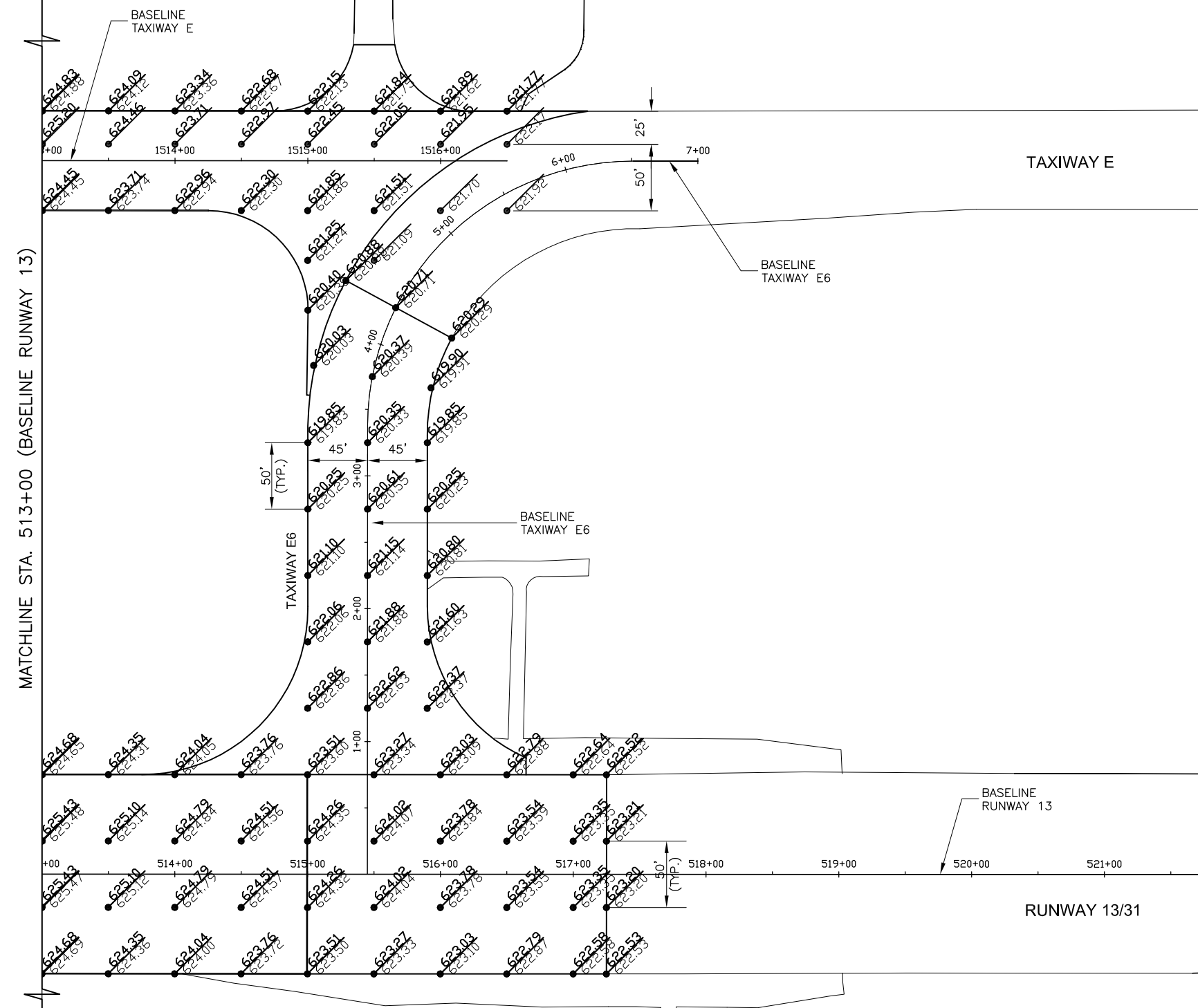
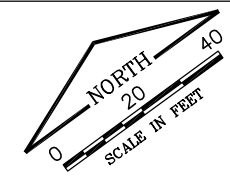
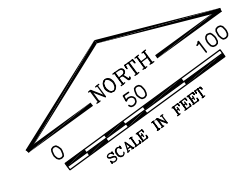
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**KEY MAP**

**LEGEND**

- NEW SPOT ELEVATION
- EXISTING SPOT ELEVATION



MATCHLINE STA. 513+00 (BASELINE RUNWAY 13)

TAXIWAY E

RUNWAY 13/31

TAXIWAY E

1515+00

1516+00


BUTT JOINT - STAKING PLAN

**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**STAKING PLAN 2**

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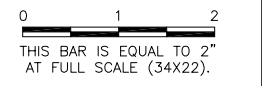


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| DRAWN BY:     | CMT             |
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| APPROVED BY:  | CET             |
| DATE:         | MAY 13, 2011    |
| JOB No:       | 100610300       |
| IL PROJ. NO.  | PIA-4079        |
| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 49 OF 84 SHEETS |

**PE092**

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 PEORIA, ILLINOIS**

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 STAKING PLAN 3**

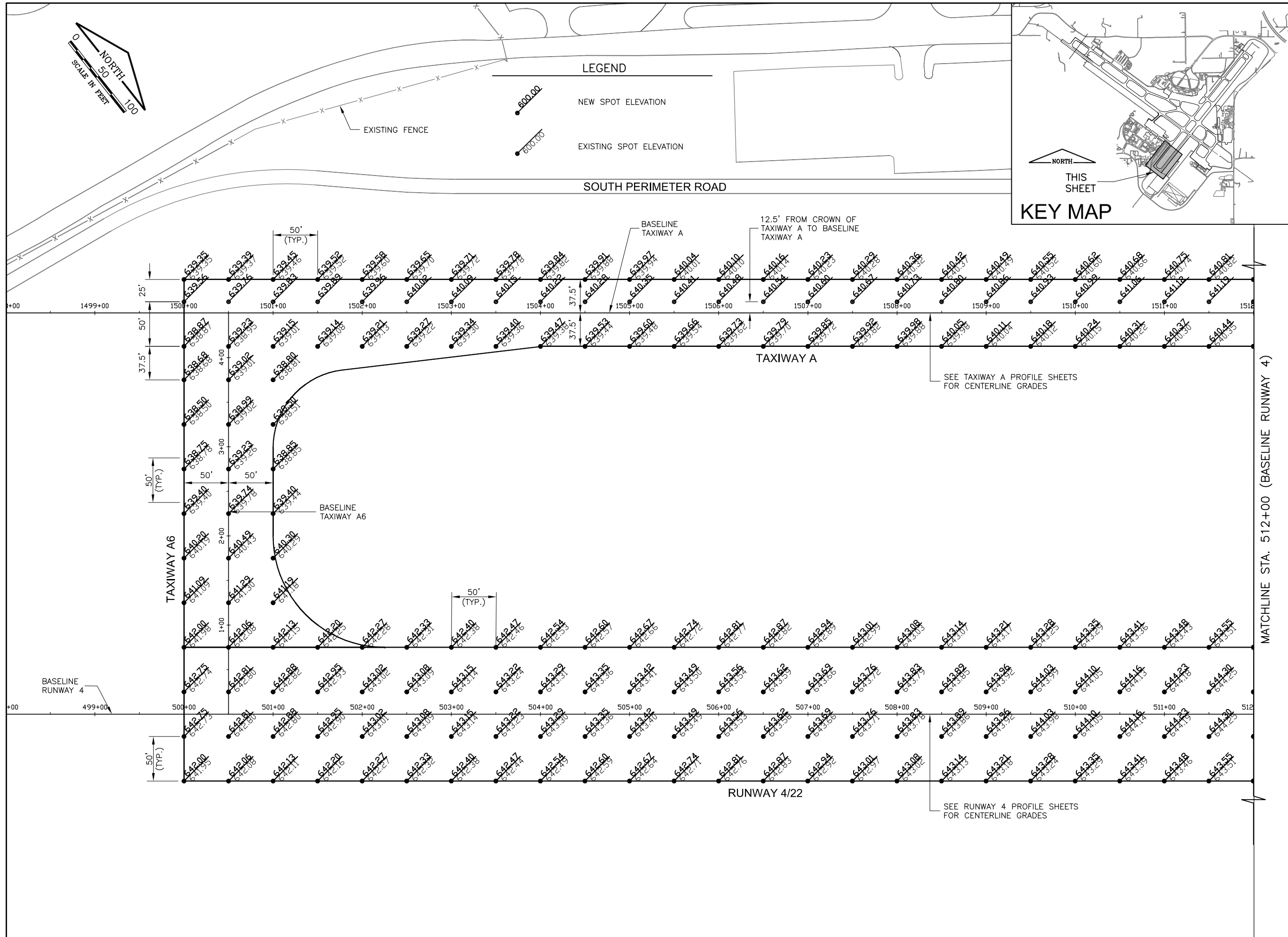
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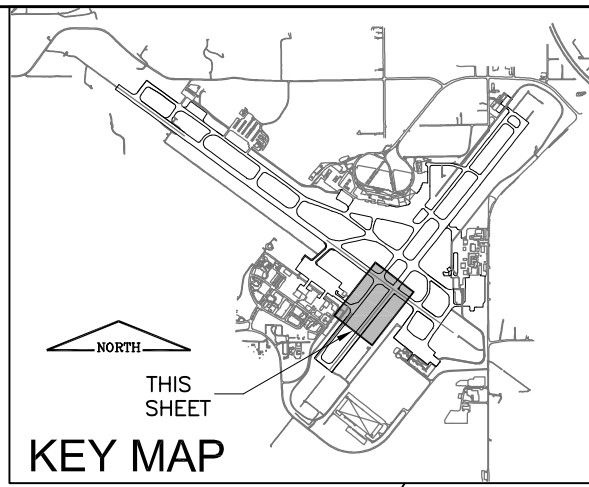
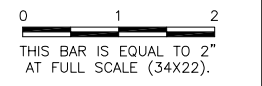


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| CHECKED BY:   | CET             |
| APPROVED BY:  | CET             |
| DATE:         | MAY 13, 2011    |
| JOB No:       | 100610300       |
| IL PROJ. NO.  | PIA-4079        |
| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 50 OF 84 SHEETS |

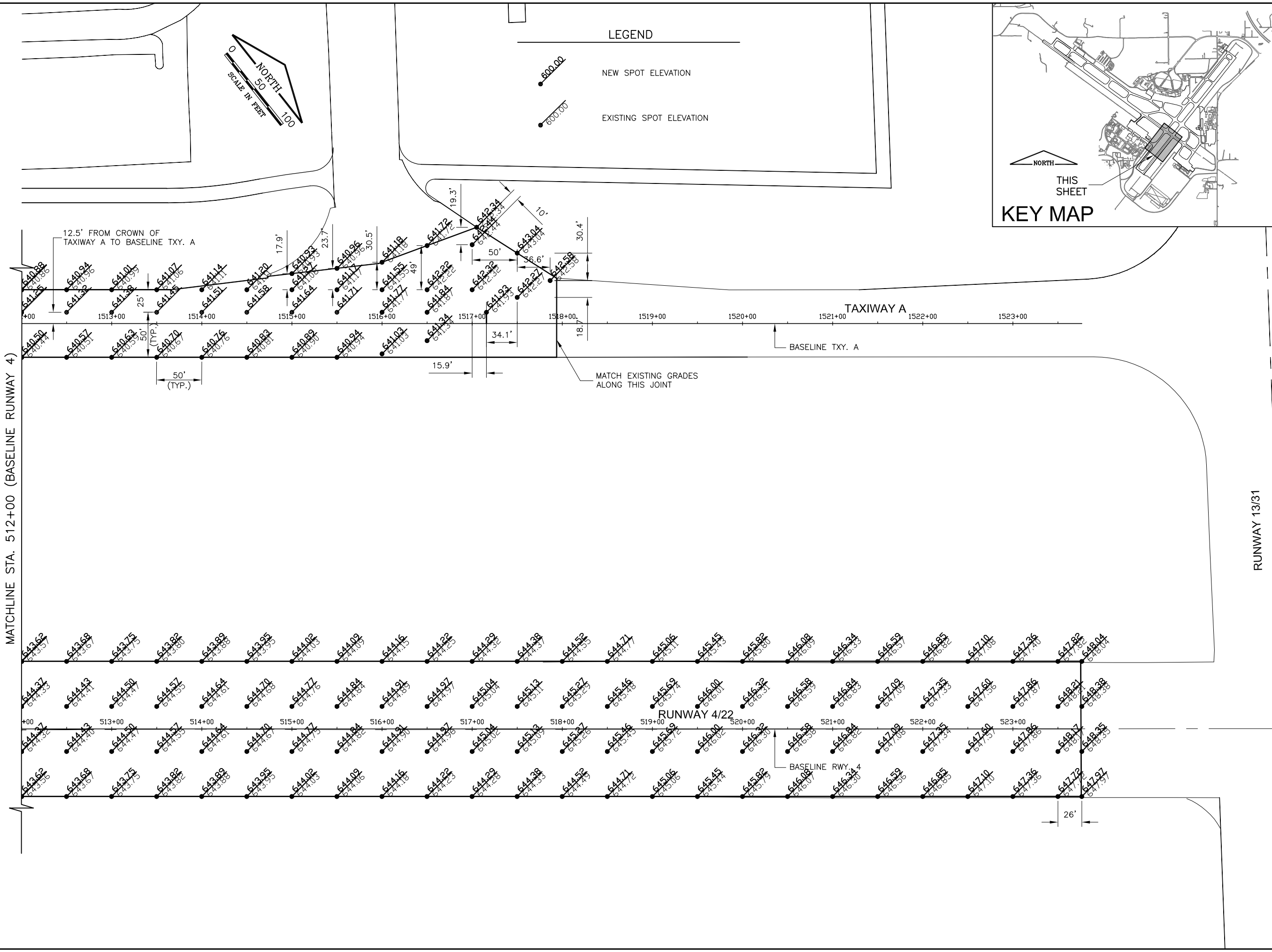
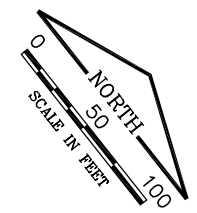
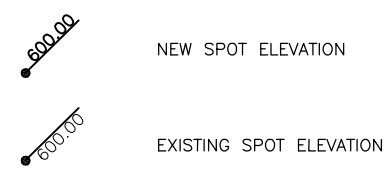


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**LEGEND**




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**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**STAKING PLAN 4**

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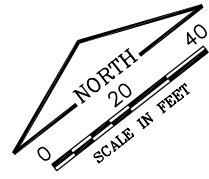


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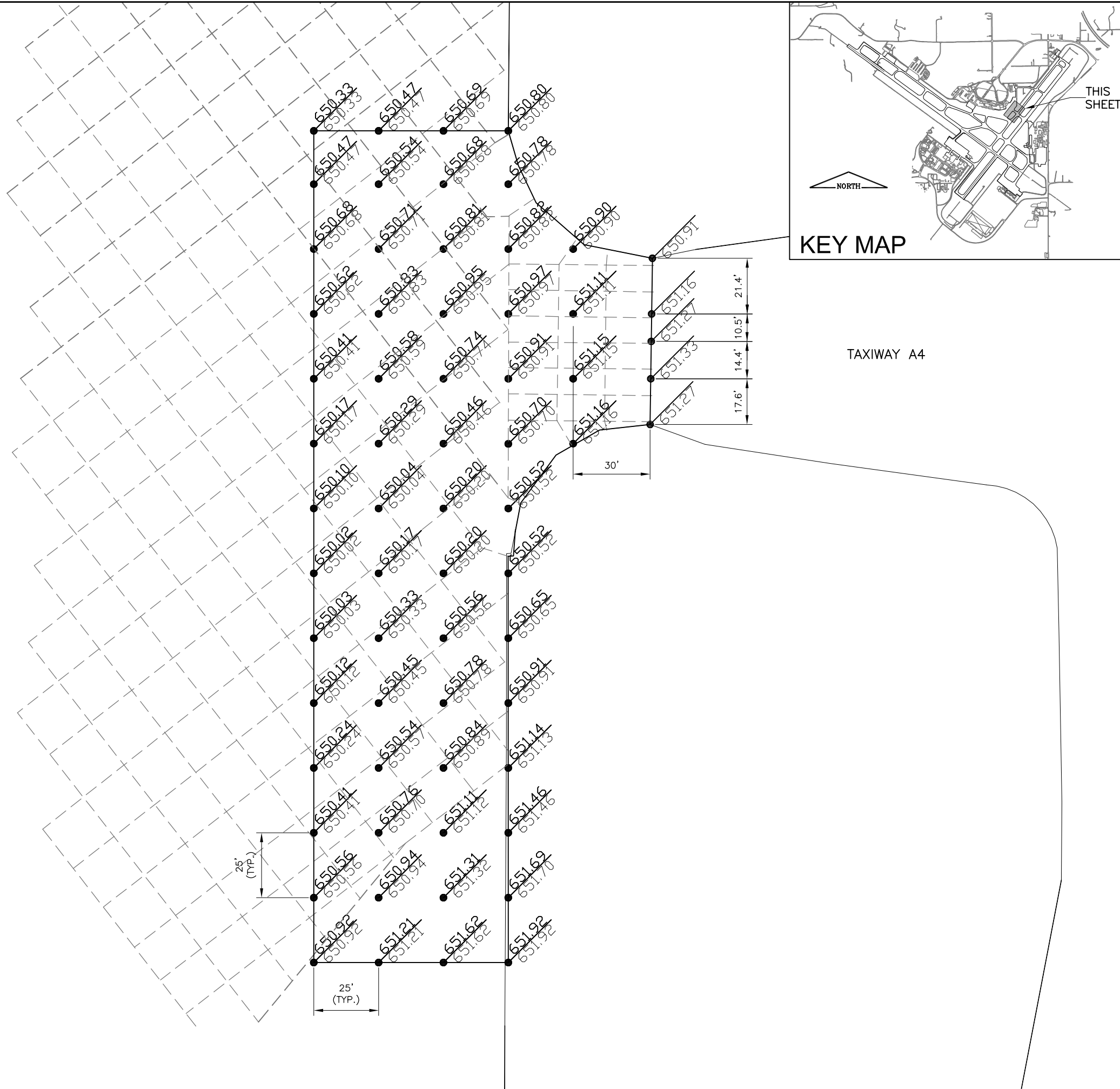
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| CHECKED BY:                | CET          |
| APPROVED BY:               | CET          |
| DATE:                      | MAY 13, 2011 |
| JOB No:                    | 100610300    |
| IL PROJ. NO. PIA-4079      |              |
| AIP PROJ. NO. 3-17-0080-XX |              |
| SHEET 51 OF 84 SHEETS      |              |



LEGEND

- NEW SPOT ELEVATION
- EXISTING SPOT ELEVATION



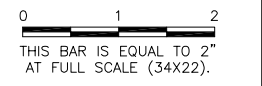
KEY MAP



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 PLOT DATE: 5/18/2011 8:26 AM  
 Pia-Bose\_Updated  
 KEYMAP  
 PIA\_ALIGN  
 PIA\_STAKE  
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REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 STAKING PLAN 5

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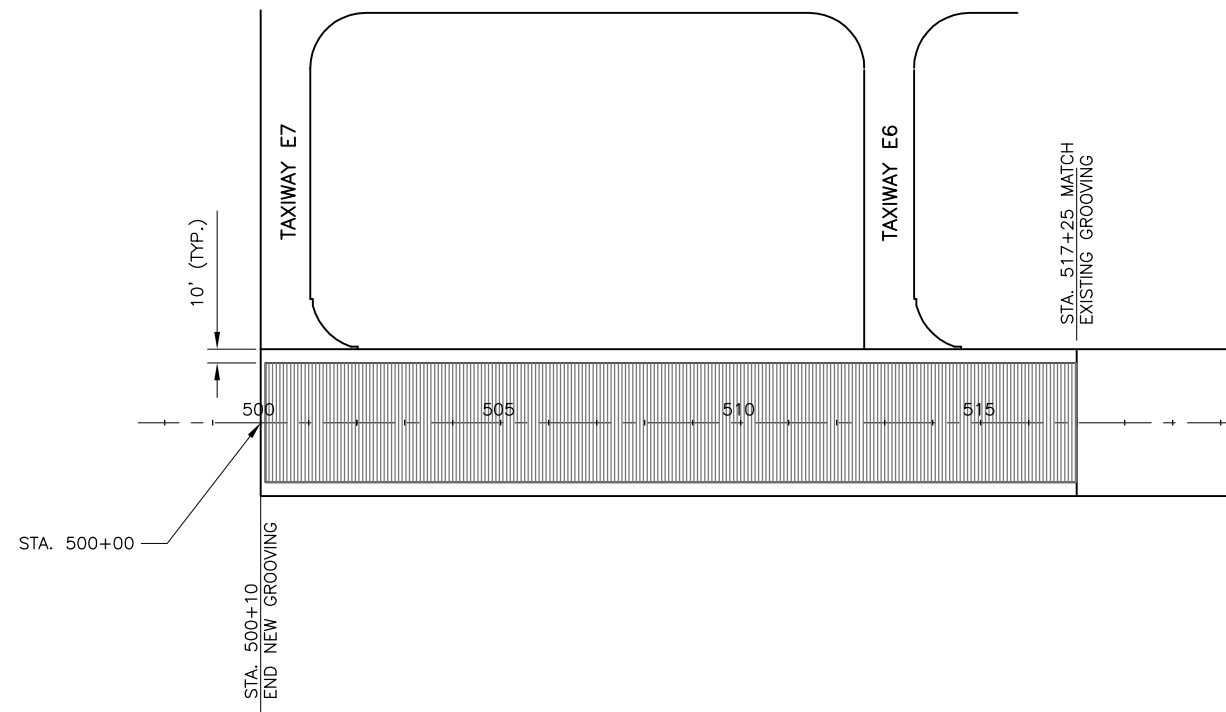
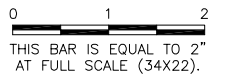
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| SHEET 52 OF 84 SHEETS                               |              |

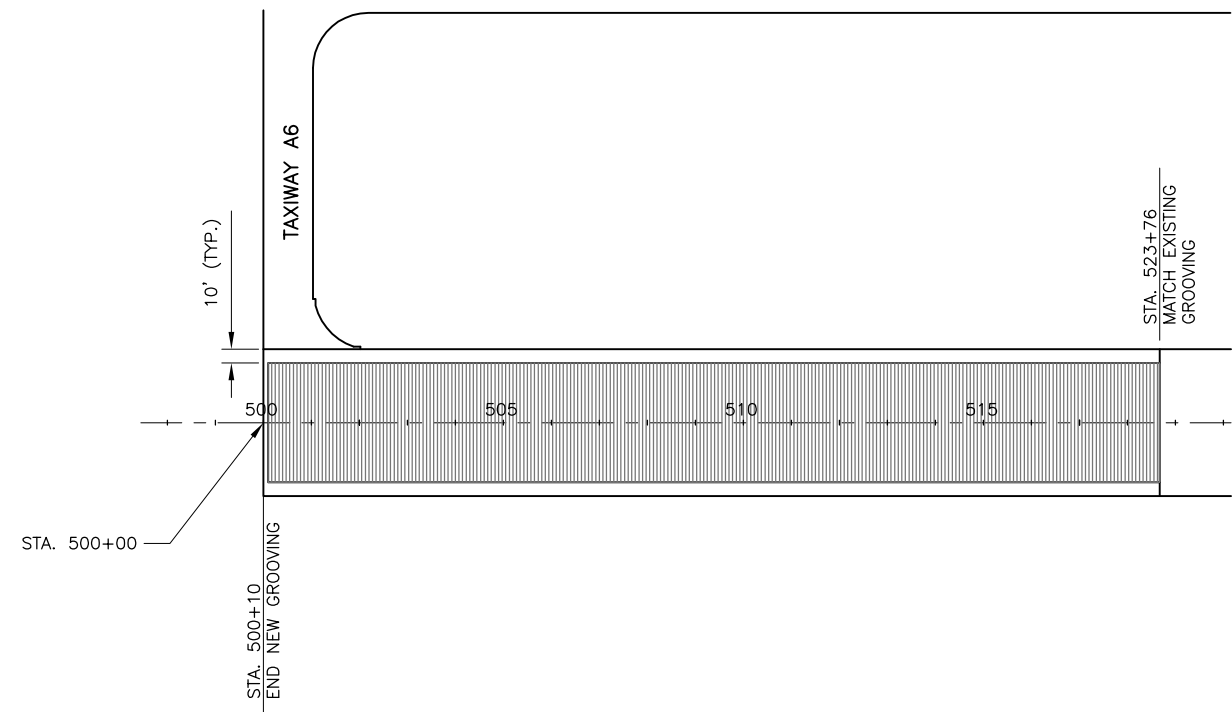


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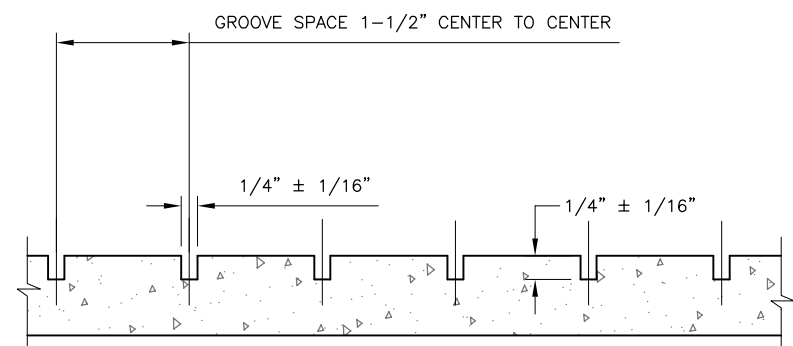
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**GROOVING PLAN — RUNWAY 13**  
 N.T.S.



**GROOVING PLAN — RUNWAY 4**  
 N.T.S.



**GROOVE DIMENSIONS**  
 N.T.S.

**NOTES:**

1. RUNWAY PAVEMENT GROOVING SHALL NOT COMMENCE UNTIL THE BITUMINOUS SURFACE COURSE HAS BEEN GIVEN AN ADEQUATE CURE TIME OF AT LEAST 2 WEEKS.
2. SEE CONSTRUCTION ACTIVITY DETAIL SHEETS FOR NOTES ON CONSTRUCTION OPERATIONS AFTER COMPLETING PAVING AND PRIOR TO BEGINNING GROOVING.

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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**GROOVING PLAN AND DETAILS**

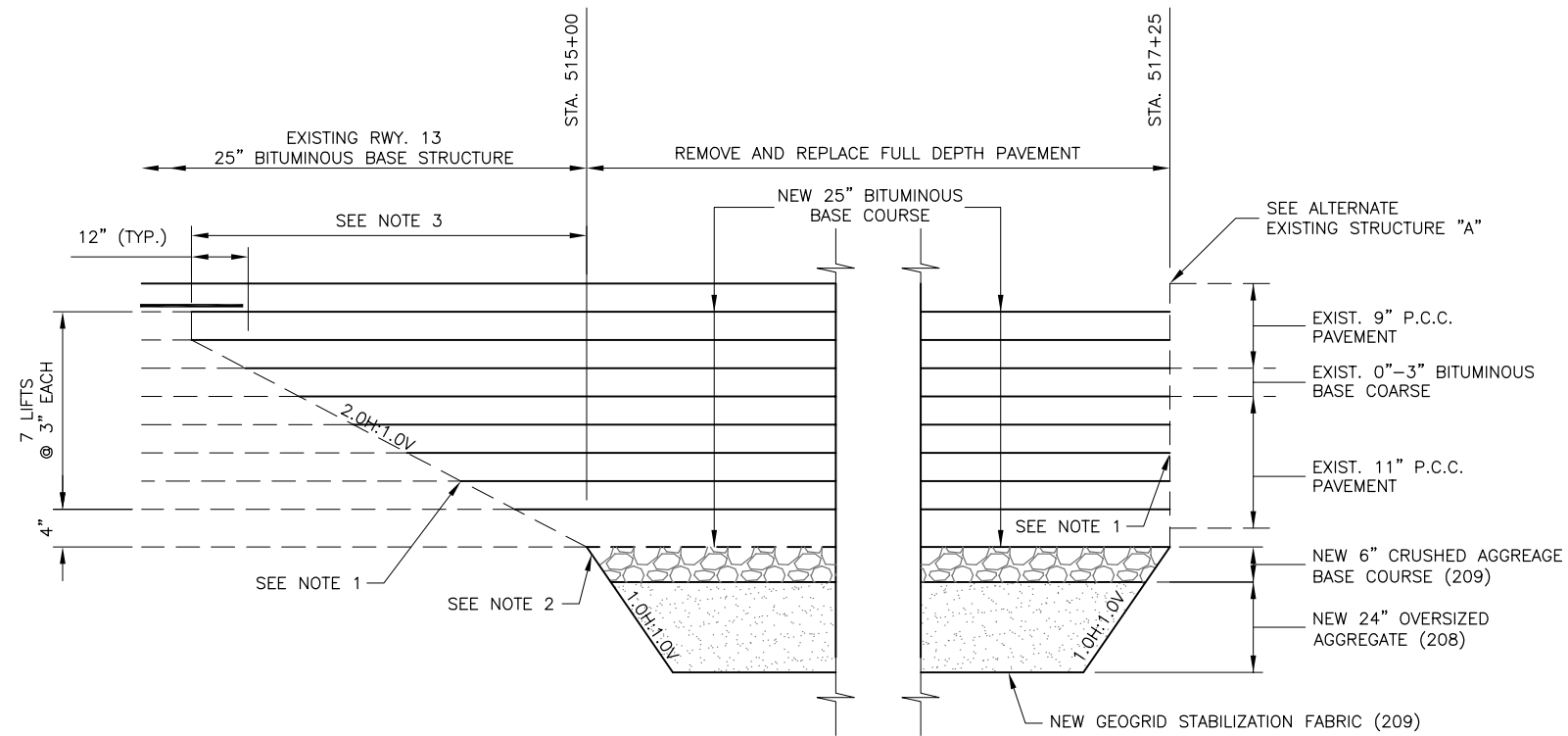
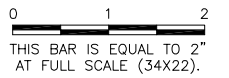
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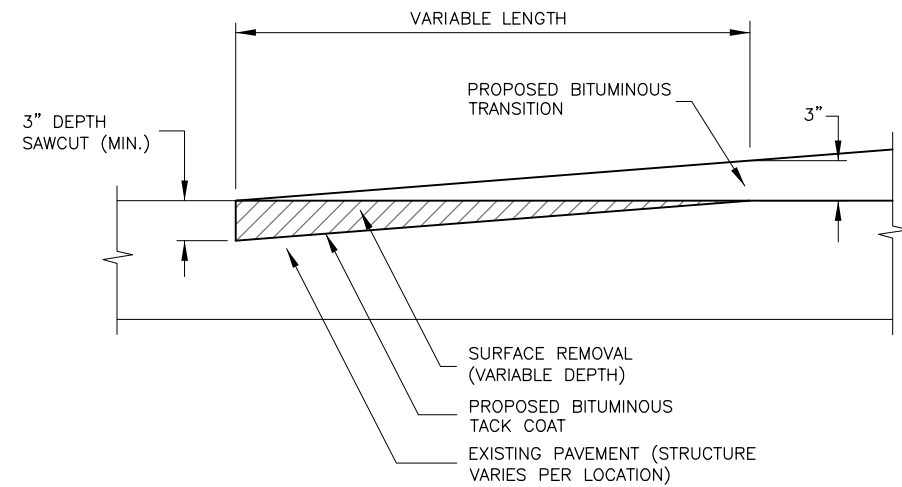
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| SHEET  | 53 OF 84 SHEETS |

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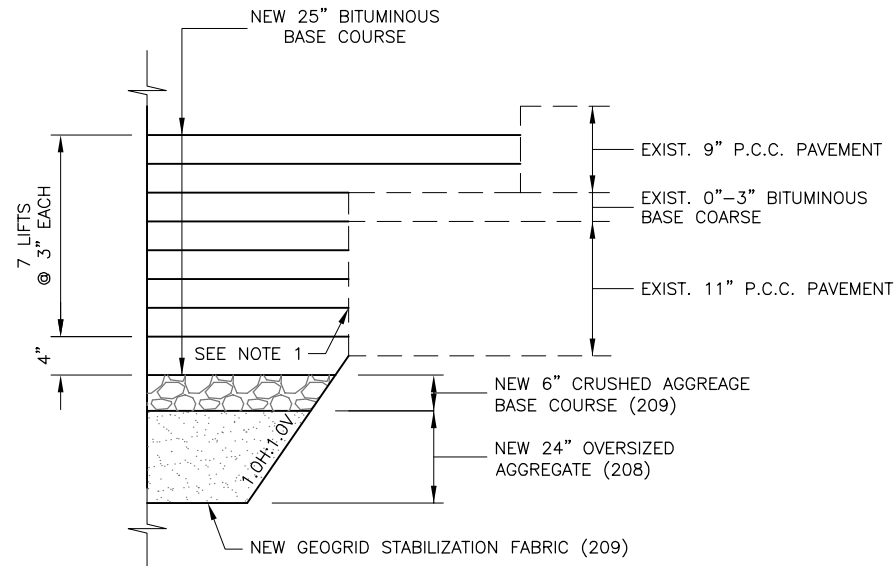
**FULL DEPTH RUNWAY 13 - CONSTRUCTION SECTION**  
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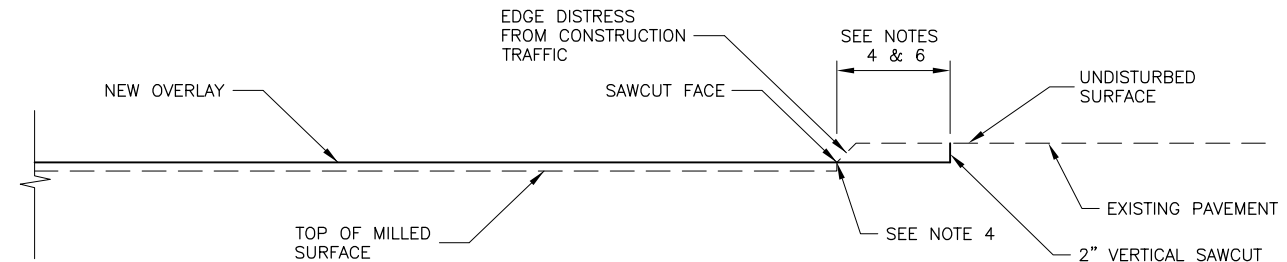
**BUTT JOINT DETAIL**  
 N.T.S.

**NOTES:**

1. CONTRACTOR SHALL USE A SMALLER "PUP" ROLLER TO COMPACT AGAINST ADJACENT STRUCTURES AND TO ROLL PERPENDICULAR TO THE PAVING DIRECTION.
2. CONTRACTOR SHALL EXCAVATE THE SUBGRADE AS SHOWN AGAINST THE EXISTING PAVEMENT BASE TO KEEP THE SUBGRADE COMPACTED UNDER THE EXISTING PAVEMENT STRUCTURE.
3. THE AREA SHOWN HERE AS A BITUMINOUS TRANSITION SHALL BE INCIDENTAL TO THE BITUMINOUS PAVEMENT MILLING PAY ITEM.
4. PRIOR TO PLACING AND COMPACTING THE FINAL LIFT OF THE BITUMINOUS SURFACE, THE CONTRACTOR SHALL MILL THE EXISTING EDGE OF PAVEMENT TO BE MATCHED FOR THE DEPTH OF THE FINAL LIFT AND FOR A MINIMUM WIDTH OF 2'. THE WIDTH SHALL BE EXPANDED AS NECESSARY TO ELIMINATE THE EDGE DISTRESS CAUSED BY CONSTRUCTION TRAFFIC.
5. CRACK CONTROL FABRIC SHALL BE PLACED OVER THE JOINT CREATED WITH THE INITIAL LIFT.
6. THE ADDITIONAL MILLING REQUIRED TO CREATE A SMOOTH FACE TO MATCH SHALL BE CONSIDERED INCIDENTAL TO THE PAVING OPERATION. THE ADDITIONAL BITUMINOUS SURFACE COURSE, BITUMINOUS TACK COAT, AND CRACK CONTROL FABRIC SHALL BE MEASURED INDIVIDUALLY FOR PAYMENT.
7. TACK COAT SHALL BE PLACED ON ALL VERTICAL EDGES AND BETWEEN ANY LIFTS.



**ALTERNATE EXISTING STRUCTURE "A"**  
 N.T.S.



**MATCHING BITUMINOUS PAVEMENT DETAIL**  
 N.T.S.

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**PEORIA INTERNATIONAL AIRPORT**  
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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**PAVING AND MISCELLANEOUS DETAILS 1**

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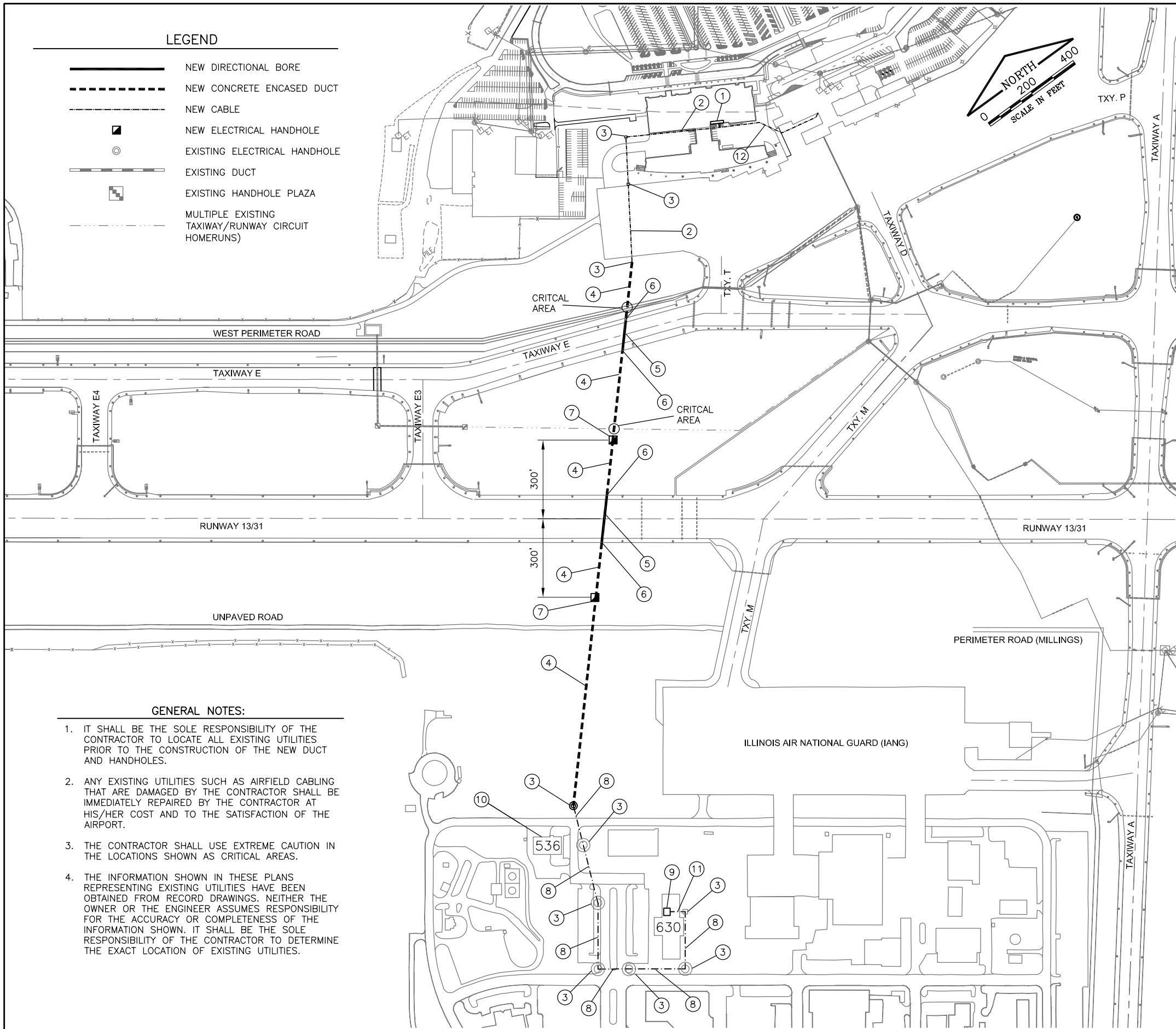


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| SHEET 54 OF 84 SHEETS      |              |

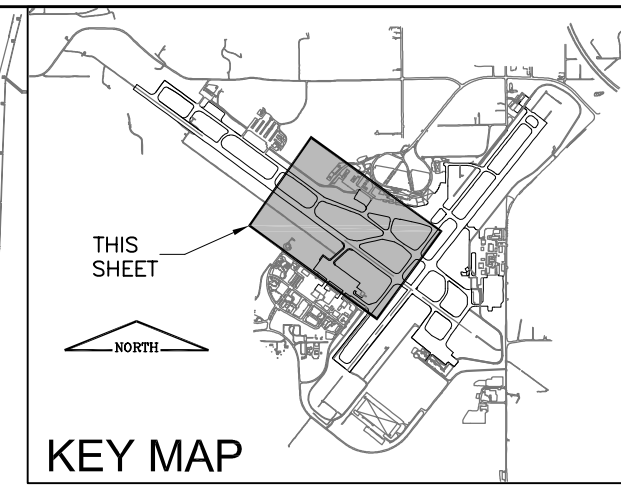
**LEGEND**

- NEW DIRECTIONAL BORE
- - - NEW CONCRETE ENCASED DUCT
- - - NEW CABLE
- NEW ELECTRICAL HANDHOLE
- ⊙ EXISTING ELECTRICAL HANDHOLE
- - - EXISTING DUCT
- EXISTING HANDHOLE PLAZA
- - - MULTIPLE EXISTING TAXIWAY/RUNWAY CIRCUIT HOMERUNS)



**GENERAL NOTES:**

1. IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE ALL EXISTING UTILITIES PRIOR TO THE CONSTRUCTION OF THE NEW DUCT AND HANDHOLES.
2. ANY EXISTING UTILITIES SUCH AS AIRFIELD CABLING THAT ARE DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED BY THE CONTRACTOR AT HIS/HER COST AND TO THE SATISFACTION OF THE AIRPORT.
3. THE CONTRACTOR SHALL USE EXTREME CAUTION IN THE LOCATIONS SHOWN AS CRITICAL AREAS.
4. THE INFORMATION SHOWN IN THESE PLANS REPRESENTING EXISTING UTILITIES HAVE BEEN OBTAINED FROM RECORD DRAWINGS. NEITHER THE OWNER OR THE ENGINEER ASSUMES RESPONSIBILITY FOR THE ACCURACY OR COMPLETENESS OF THE INFORMATION SHOWN. IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE EXACT LOCATION OF EXISTING UTILITIES.



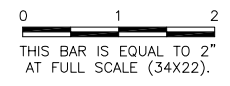
**KEYED NOTES:**

- ① LOCATION OF NEW TERMINAL BLDG COMMUNICATIONS ROOM. SEE DETAIL, SHEET 56.
- ② EXISTING 4-WAY 4" DUCT BANK INSTALLED WITH NEW TERMINAL BUILDING. USE ONE 4" CONDUIT FOR INSTALLATION OF NEW 6-STRAND SINGLE-MODE FIBER OPTIC CABLE (NEW TERMINAL BLDG FIRE ALARM) & 6-PAIR #19 AWG COMMUNICATION CABLE (CRASH PHONE).
- ③ EXISTING HANDHOLE.
- ④ NEW 4" CONCRETE ENCASED SCHEDULE 40 PVC CONDUIT WITH 6-STRAND SINGLE-MODE FIBER OPTIC CABLE (NEW TERMINAL BLDG FIRE ALARM) & 6-PAIR #19 AWG COMMUNICATION CABLE (CRASH PHONE).
- ⑤ AT TAXIWAY E AND RUNWAY 13/31, THE CONTRACTOR SHALL DIRECTIONALLY BORE NEW 4" DUCT UNDER THE EXISTING PAVEMENT STRUCTURE. THE DIRECTIONAL BORE SHALL EXTEND A MINIMUM OF 5' BEYOND THE EXISTING TAXIWAY/RUNWAY EDGE LIGHT CIRCUIT.
- ⑥ THE CONTRACTOR SHALL CONNECT THE END OF NEW DIRECTIONALLY BORED CONDUIT TO THE NEW 4" CONCRETE ENCASED CONDUIT USING APPROPRIATE CONNECTION FITTINGS. THE CONTRACTOR SHALL ALSO EXCAVATE BETWEEN THE END OF THE DIRECTIONAL BORE AND THE EDGE OF THE TAXIWAY/RUNWAY PAVEMENT AND ENCASE THIS DUCT IN CONCRETE. THE CONTRACTOR SHALL USE CAUTION SO AS NOT TO DAMAGE THE EDGE LIGHT CIRCUITS WHEN EXCAVATING. THE COST TO EXCAVATE, CONNECT, AND ENCASE IN CONCRETE SHALL BE CONSIDERED INCIDENTAL TO THE DIRECTIONAL BORE ITEM.
- ⑦ NEW 4'x4'x4' HANDHOLE.
- ⑧ EXISTING IANG DUCT BANK. USE ONE EXISTING CONDUIT FOR INSTALLATION OF NEW 6-STRAND SINGLE-MODE FIBER OPTIC CABLE (NEW TERMINAL BLDG FIRE ALARM) & 6-PAIR #19 AWG COMMUNICATION CABLE (CRASH PHONE).
- ⑨ EXISTING COMM ROOM (29) IN IANG BLDG 630. SEE DETAIL, SHEET 56.
- ⑩ EXISTING IANG BLDG 536. SEE DETAIL, SHEET 56.
- ⑪ INSTALL NEW 6-STRAND SINGLE-MODE FIBER OPTIC CABLE (NEW TERMINAL BLDG FIRE ALARM) & 6-PAIR #19 AWG COMMUNICATION CABLE (CRASH PHONE) IN EXISTING CONDUIT FROM EXISTING HANDHOLE TO EXISTING COMM ROOM (29).
- ⑫ EXISTING 4-WAY 4" COMMUNICATIONS DUCT BANK INSTALLED BETWEEN NEW TERMINAL BUILDING AND EXISTING TERMINAL BUILDING. USE ONE 4" CONDUIT FOR INSTALLATION OF NEW 6-PAIR #19 AWG COMMUNICATION CABLE (CRASH PHONE). SEE DETAIL, SHEET 56, FOR CONTINUATION IN EXISTING TERMINAL BUILDING.

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 FILE: FIRE ALARM AND CRASH PHONE  
 UPDATE BY: TJ Heavisides  
 PLOT DATE: 5/18/2011 8:26 AM  
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 BASE\_PROP\_ELEC  
 UTILITY\_Updated  
 BASE\_PROP\_GEO

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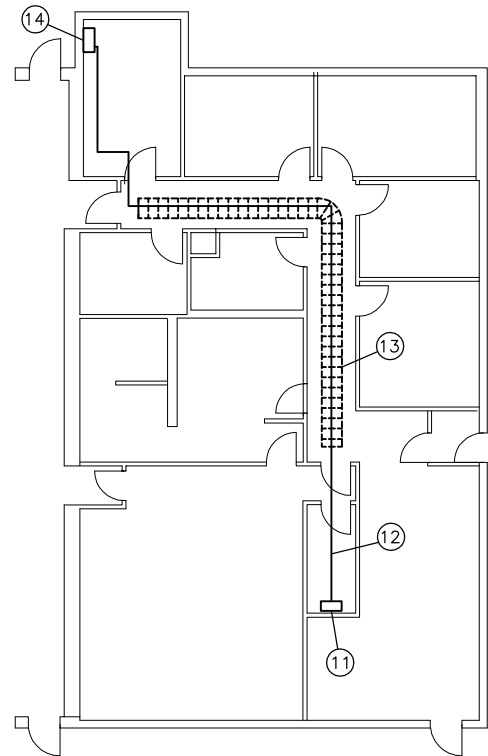
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**FIRE ALARM AND CRASH PHONE CABLING PLAN**

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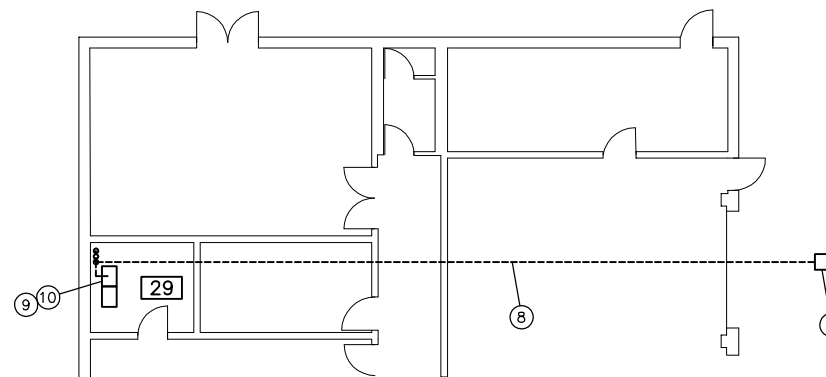
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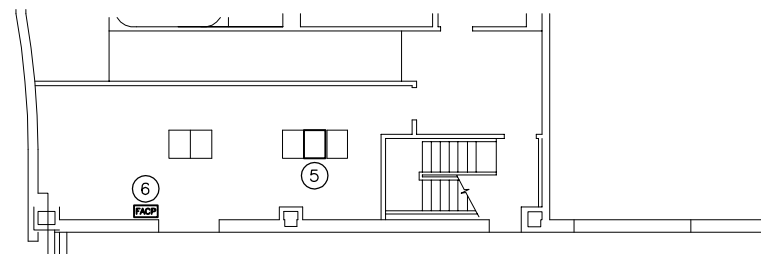
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| SHEET 55 OF 84 SHEETS                                |              |



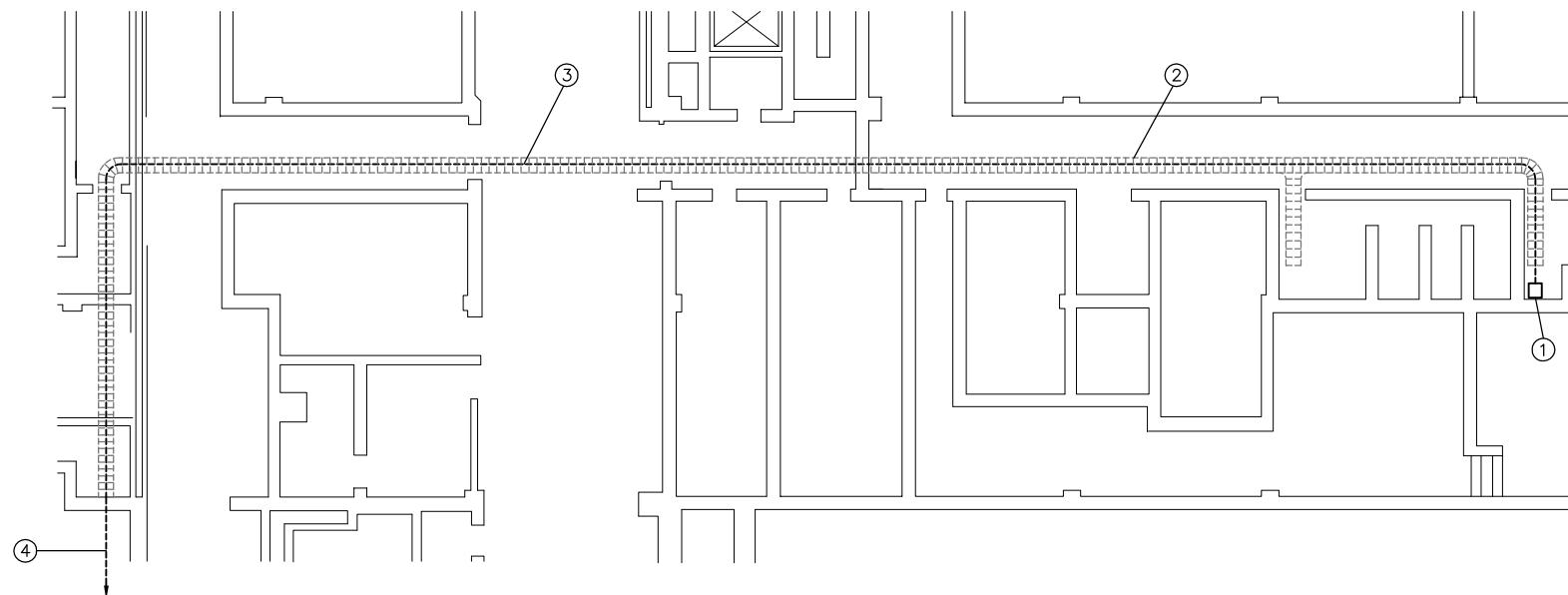
PARTIAL IANG BUILDING 536  
FLOOR PLAN  
SCALE 1/8" = 1'-0"



PARTIAL IANG BUILDING 630  
FLOOR PLAN  
SCALE 1/8" = 1'-0"



PARTIAL NEW TERMINAL  
COMM ROOM PLAN  
SCALE 1/8" = 1'-0"



PARTIAL EXISTING TERMINAL  
LOWER LEVEL FLOOR PLAN  
SCALE 1/8" = 1'-0"

KEYED NOTES

- ① IN EXISTING COMM ROOM IN LOWER LEVEL OF EXISTING TERMINAL BLDG, THE CONTRACTOR SHALL INTERCEPT EXISTING COMMUNICATION WIRING UP TO EXISTING CRASH PHONE IN ATCT AND CONNECT TO NEW 6-PAIR #19 AWG COMMUNICATION CABLE AS NECESSARY. UNUSED CONDUCTORS IN THIS NEW COMMUNICATION CABLE SHALL BE LABELED AS "SPARE".  
  
CONTRACTOR SHALL COORDINATE THIS WORK WITH THE AIRPORT AND THE FAA AND COMPLY WITH ANY REQUIREMENTS.
- ② EXISTING CABLE TRAY.
- ③ NEW 6-PAIR #19 AWG COMMUNICATION CABLE, INSTALLED IN EXISTING CABLE TRAY.
- ④ EXISTING 4-WAY 4" COMMUNICATIONS DUCT BANK INSTALLED BETWEEN NEW TERMINAL BUILDING AND EXISTING TERMINAL BUILDING. USE ONE 4" CONDUIT FOR INSTALLATION OF NEW 6-PAIR #19 AWG COMMUNICATION CABLE (ATCT CRASH PHONE). SEE SHEET 55 FOR CONTINUATION.
- ⑤ TERMINATE NEW 6-PAIR #19 AWG COMMUNICATION CABLE FROM EXISTING TERMINAL BUILDING (CRASH PHONE) AND NEW 6-PAIR #19 AWG COMMUNICATION CABLE FROM IANG (CRASH PHONE) ON EXISTING PLYWOOD BACK BOARD IN NEW TERMINAL COMM ROOM. UNUSED CONDUCTORS IN THESE NEW COMMUNICATION CABLES SHALL BE LABELED AS "SPARE". INSTALL THESE NEW COMMUNICATION CABLES IN 1" CONDUITS AS REQUIRED FROM PLYWOOD BACK BOARD TO TERMINATIONS OF EXISTING 4-WAY 4" COMMUNICATIONS DUCT BANKS FROM EXISTING TERMINAL BUILDING AND FROM IANG. SEE SHEET 55 FOR ADDITIONAL INFORMATION.
- ⑥ LOCATION OF EXISTING SIEMENS FIRE ALARM CONTROL PANEL (FACP) IN NEW TERMINAL BUILDING COMM ROOM. ADD THE FOLLOWING EQUIPMENT TO FACP:  
- MOUNTING PLATE (D2300MP)  
- FIBER MODULE (D2325CPS)  
- NETWORK CARD (NIC-C)  
- POWER SUPPLY (PSX-12)  
- 6-STRAND SINGLE-MODE FIBER OPTIC CABLE IN 1" CONDUIT TO EXISTING 4-WAY 4" COMMUNICATIONS DUCT BANK. SEE SHEET 55 FOR ADDITIONAL INFORMATION.
- ⑦ EXISTING HANDHOLE. SEE SHEET 55 FOR ADDITIONAL INFORMATION.
- ⑧ INSTALL NEW 6-STRAND SINGLE-MODE FIBER OPTIC CABLE (FIRE ALARM) AND NEW 6-PAIR #19 AWG COMMUNICATION CABLE (CRASH PHONE) IN EXISTING CONDUIT FROM EXISTING HANDHOLE TO EXISTING COMM ROOM (29).
- ⑨ PROVIDE NEW FIBER OPTIC PATCH PANEL (IF REQUIRED) IN EXISTING RACK FOR TERMINATION OF NEW 6-STRAND SINGLE-MODE FIBER OPTIC CABLE (FIRE ALARM) FROM NEW TERMINAL BLDG. COORDINATE WORK WITH IANG. PROVIDE ANY ACCESSORIES REQUIRED FOR A COMPLETE INSTALLATION.
- ⑩ TERMINATE NEW 6-PAIR #19 AWG COMMUNICATION CABLE (CRASH PHONE) IN EXISTING IANG COMMUNICATIONS CABINET. CONNECT TO EXISTING WIRING TO EXISTING IANG CRASH PHONE AS NECESSARY. UNUSED CONDUCTORS IN THIS NEW COMMUNICATION CABLE SHALL BE LABELED AS "SPARE".  
  
CONTRACTOR SHALL COORDINATE THIS WORK WITH THE AIRPORT AND THE IANG AND COMPLY WITH ANY REQUIREMENTS.
- ⑪ EXISTING IANG FIBER OPTIC PATCH PANEL.
- ⑫ NEW 3/4" CONDUIT WITH NEW 6-STRAND SINGLE-MODE FIBER OPTIC CABLE (FIRE ALARM).
- ⑬ EXISTING CABLE TRAY.
- ⑭ NEW REMOTE SIEMENS FIRE ALARM ANNUNCIATOR PANEL, INCLUDING BUT NOT LIMITED TO THE FOLLOWING:  
- ENCLOSURE (CAB-1)  
- POWER SUPPLY (PSC-12) PROVIDE 120V POWER FROM NEAREST SOURCE  
- NETWORK CARD (NIC-C)  
- DISPLAY (PMI)  
- CARD CAGE (CC-2)  
- MOUNTING PLATE (D2300MP)  
- FIBER MODULE (D2325CPS)  
- 18Ah BATTERIES  
- TERMINATION OF 6-STRAND SINGLE-MODE FIBER OPTIC CABLE.

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UPDATE BY: TJ Heavisides  
PLOT DATE: 5/18/2011 8:26 AM  
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BASE\_PROP\_ELEC  
UTILITY\_Updated

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AT FULL SCALE (34X22).

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PEORIA INTERNATIONAL AIRPORT  
PEORIA, ILLINOIS

REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
FIRE ALARM AND CRASH PHONE DETAILS

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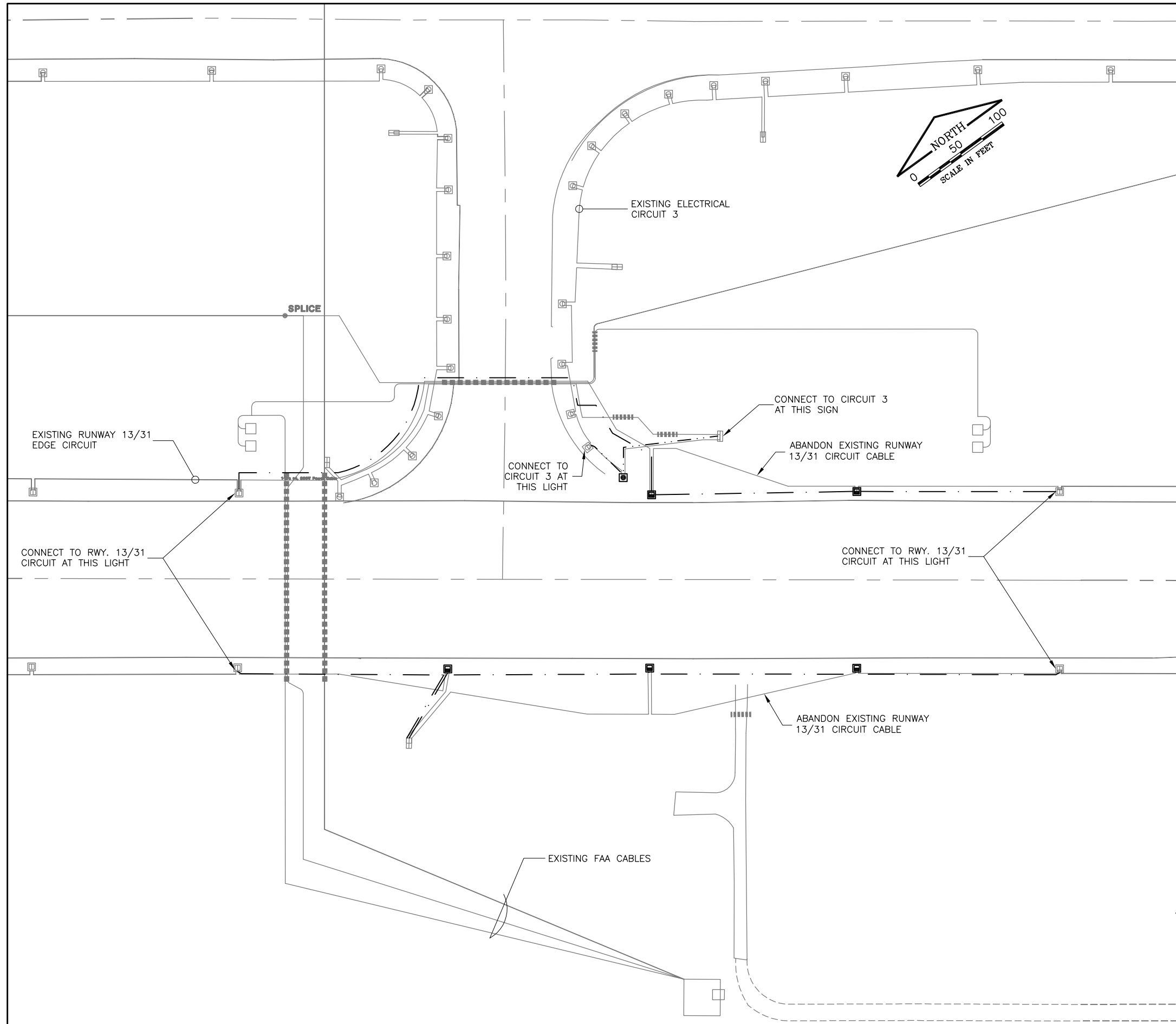
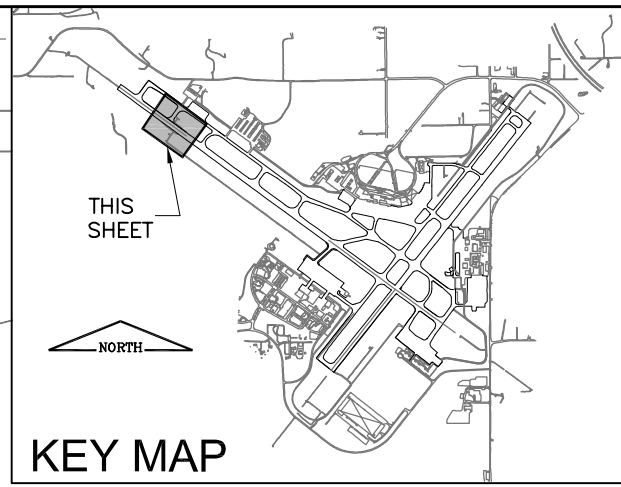
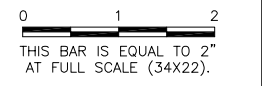
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**NOTES:**

1. CONTRACTOR SHALL LOCATE EXISTING CIRCUITS PRIOR TO DISCONNECTING THE EXISTING LIGHTS. COSTS TO LOCATE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
2. IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR FOR LOCATING ALL UTILITIES AND AIRFIELD CABLES IN THE AREAS WHERE NEW CABLES ARE TO BE TRENCHED. ANY CABLES OR UTILITIES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED IMMEDIATELY BY HIM AT HIS COST AND TO THE SATISFACTION OF THE AIRPORT.

**LEGEND**

|       |                                     |
|-------|-------------------------------------|
| ---   | NEW CIRCUIT 13/31 CABLE             |
| -.-.- | NEW CIRCUIT 3 CABLE                 |
| ■     | NEW BASE MOUNTED RUNWAY LIGHT       |
| □     | NEW BASE MOUNTED TAXIWAY LIGHT      |
| ■     | EXISTING BASE MOUNTED RUNWAY LIGHT  |
| □     | EXISTING BASE MOUNTED TAXIWAY LIGHT |
| ▭     | EXISTING AIRFIELD SIGN              |

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**PEORIA, ILLINOIS**

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**ELECTRICAL LIGHTING PLAN**

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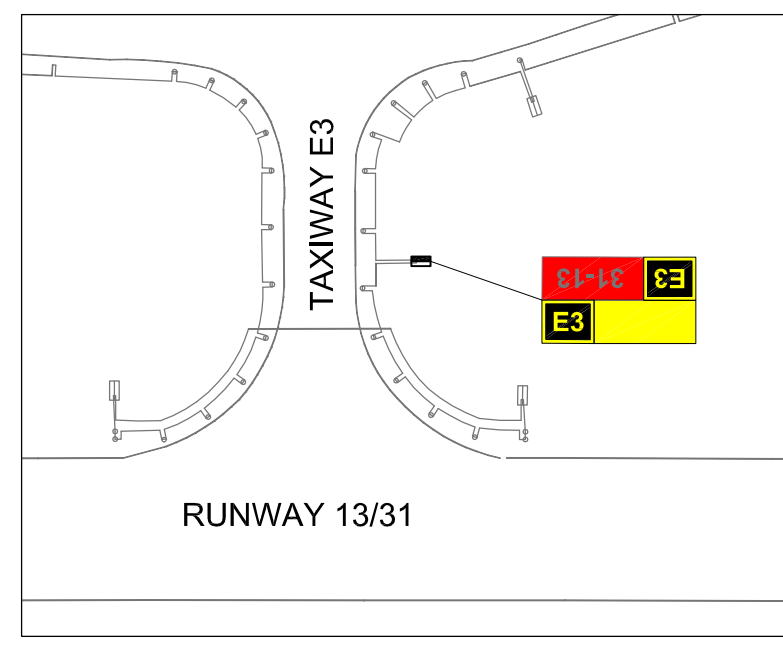
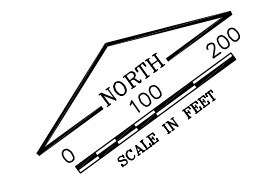
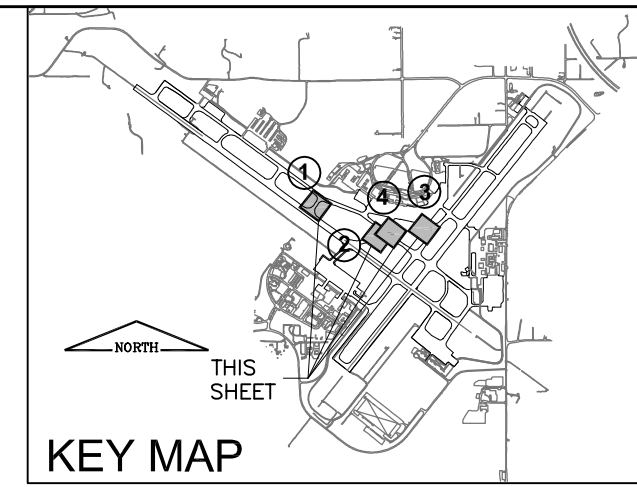
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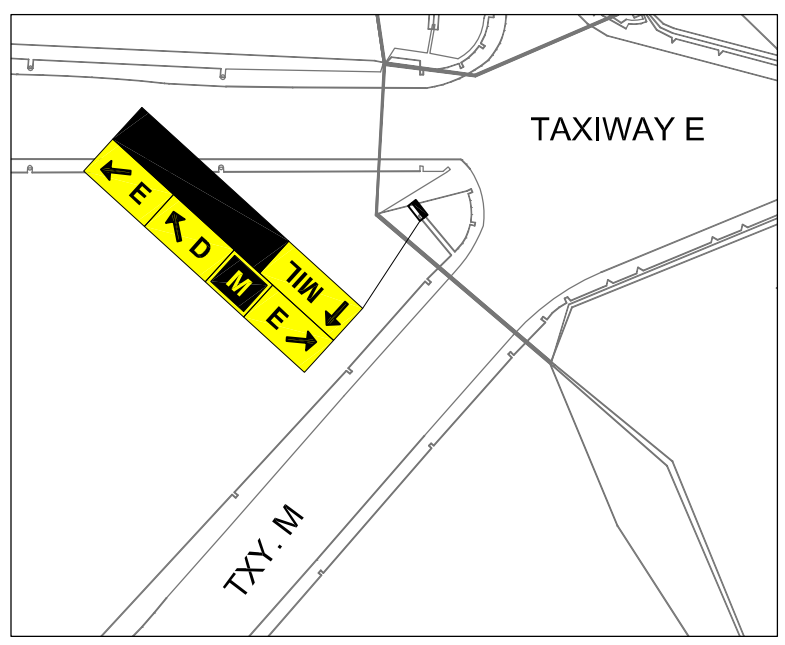
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 AT FULL SCALE (34X22).



**INTERSECTION OF TXY E3 AND RWY 13/31**

SCALE 1" = 100'

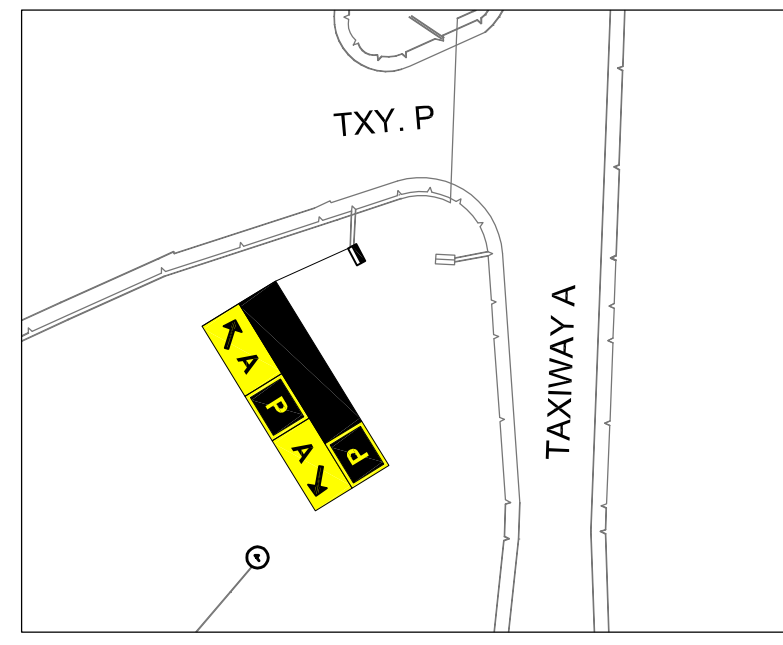
①



**INTERSECTION OF TXY M AND TXY E**

SCALE 1" = 100'

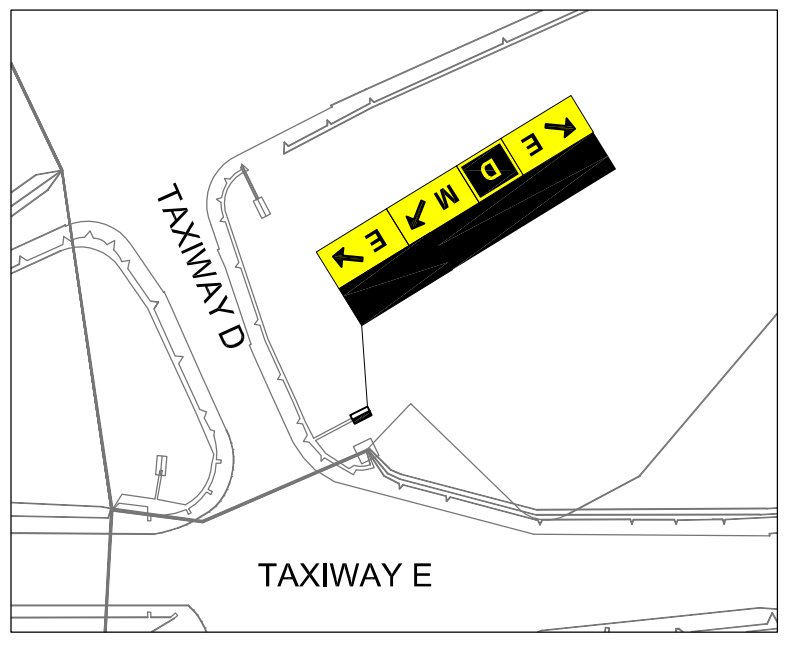
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**INTERSECTION OF TXY A AND TXY P**

SCALE 1" = 100'

③





**INTERSECTION OF TXY E AND TXY D**

SCALE 1" = 100'

④

**LEGEND**

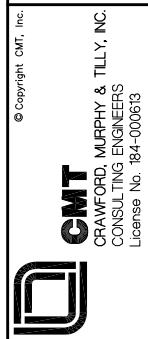
-  EXISTING AIRFIELD SIGN
-  REMOVE AND REPLACE EXISTING AIRFIELD SIGN

**NOTES:**

1. REMOVAL OF EXISTING SIGN SHALL INCLUDE THE REMOVAL OF THE CONCRETE FOUNDATION AND ALL SIGN COMPONENTS.
2. THE NEW SIGN SHALL BE CONSTRUCTED IN THE SAME LOCATION AND ORIENTATION AS THE EXISTING SIGN.
3. ANY CABLE REQUIRED TO CONNECT THE SIGN TO THE EXISTING CIRCUIT SHALL BE CONSIDERED INCIDENTAL TO THE SIGN.

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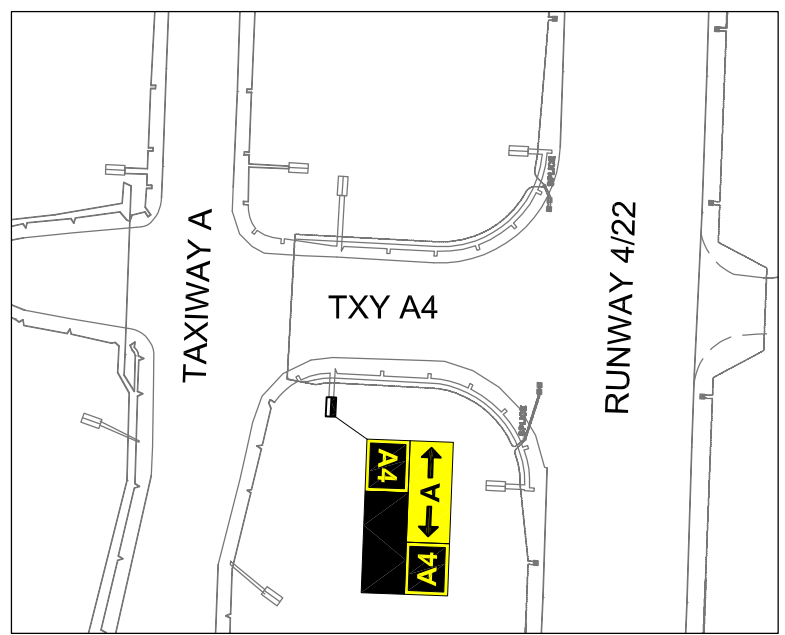
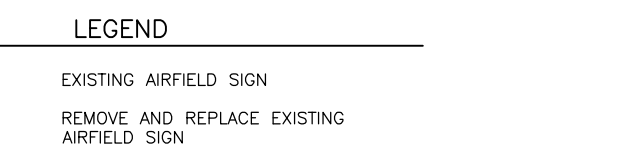
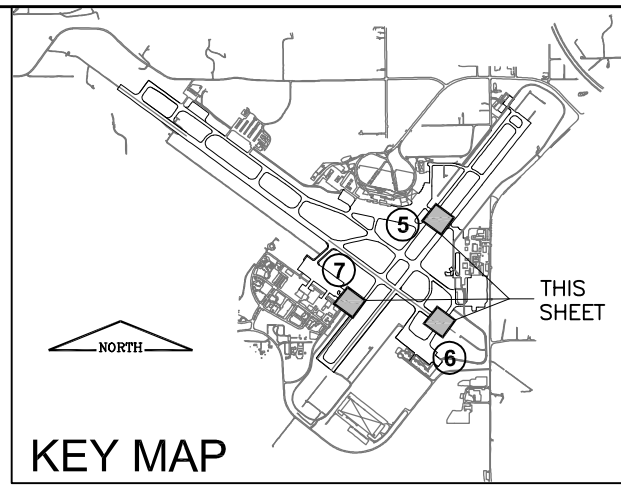
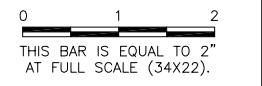
REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 ELECTRICAL SIGN PLAN 1



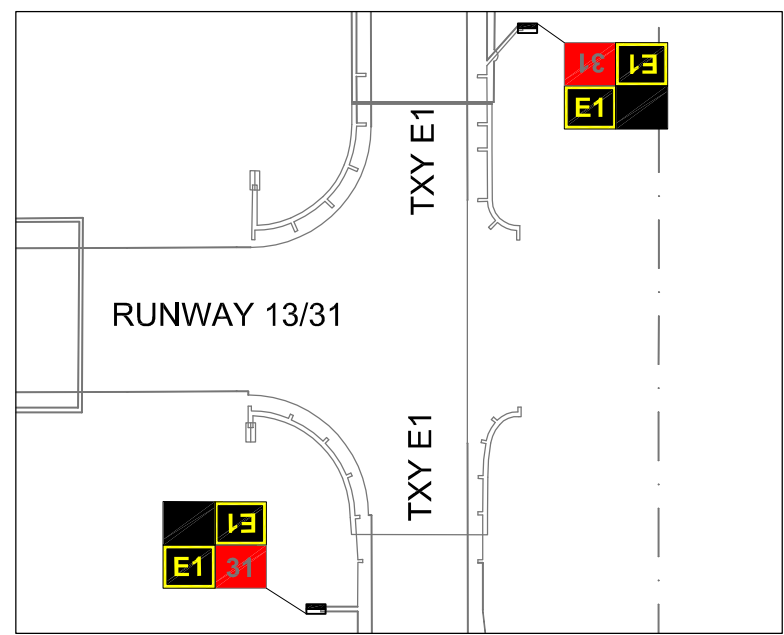
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| APPROVED BY:  | CET             |
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| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 58 OF 84 SHEETS |

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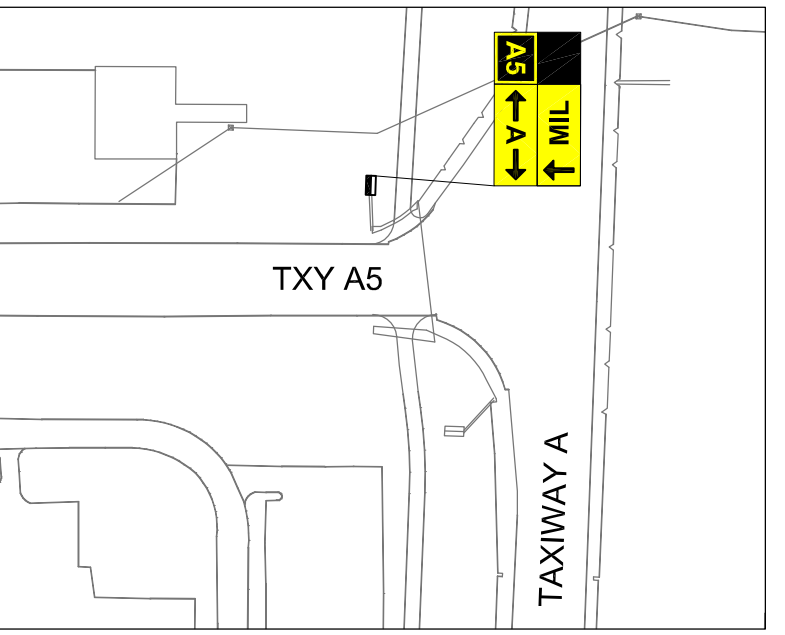
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**INTERSECTION OF TXY A4 AND RWY 4/22**  
 SCALE 1" = 100'  
 ⑤

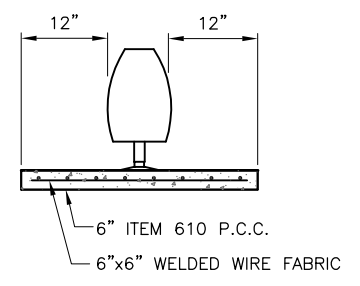
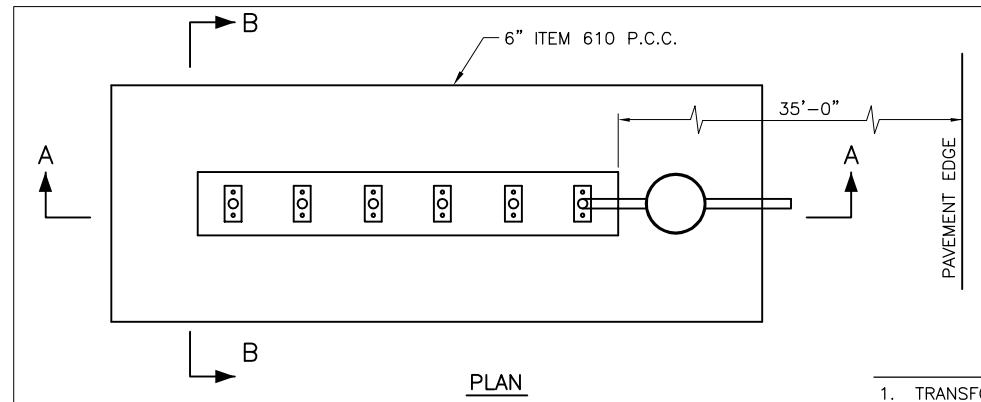


**INTERSECTION OF TXY E1 AND RWY 31**  
 SCALE 1" = 100'  
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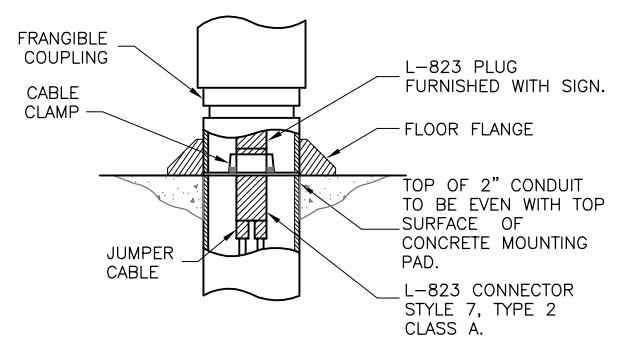
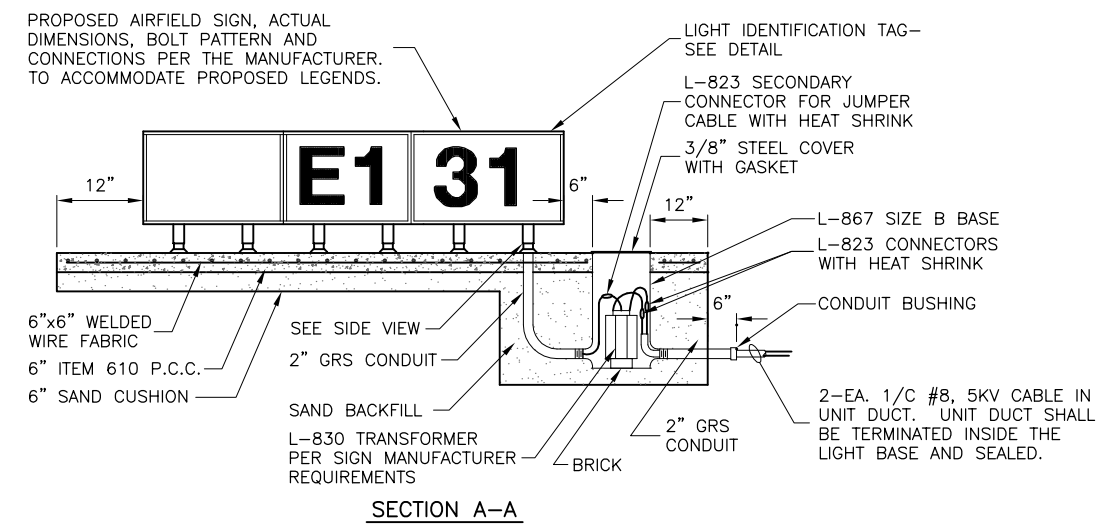


**INTERSECTION OF TXY A AND TXY A5**  
 SCALE 1" = 100'  
 ⑦

- NOTES:**
1. REMOVAL OF EXISTING SIGN SHALL INCLUDE THE REMOVAL OF THE CONCRETE FOUNDATION AND ALL SIGN COMPONENTS.
  2. THE NEW SIGN SHALL BE CONSTRUCTED IN THE SAME LOCATION AND ORIENTATION AS THE EXISTING SIGN.
  3. ANY CABLE REQUIRED TO CONNECT THE SIGN TO THE EXISTING CIRCUIT SHALL BE CONSIDERED INCIDENTAL TO THE SIGN.



- NOTES**
1. TRANSFORMER WATTAGE SHALL BE AS REQUIRED BY SIGN MANUFACTURER.
  2. SIGN LEGEND SHALL BE AS SHOWN IN THE PLANS.
  3. SIGNS SHALL BE SIZE 2 STYLE 2, CLASS 2.



**L-858 AIRFIELD SIGN DETAILS**  
 N.T.S.

**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**ELECTRICAL SIGN PLAN 2**

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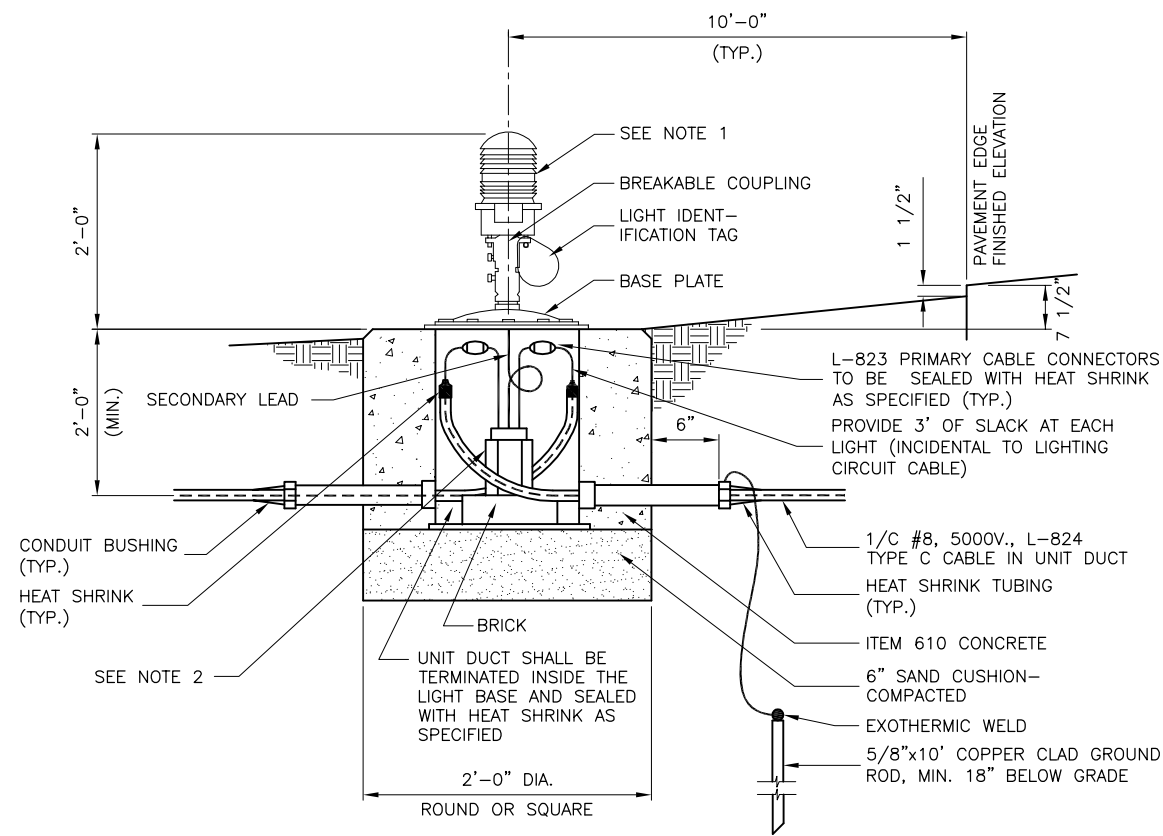
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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 ELECTRICAL DETAILS SHEET 1**

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| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 60 OF 84 SHEETS |

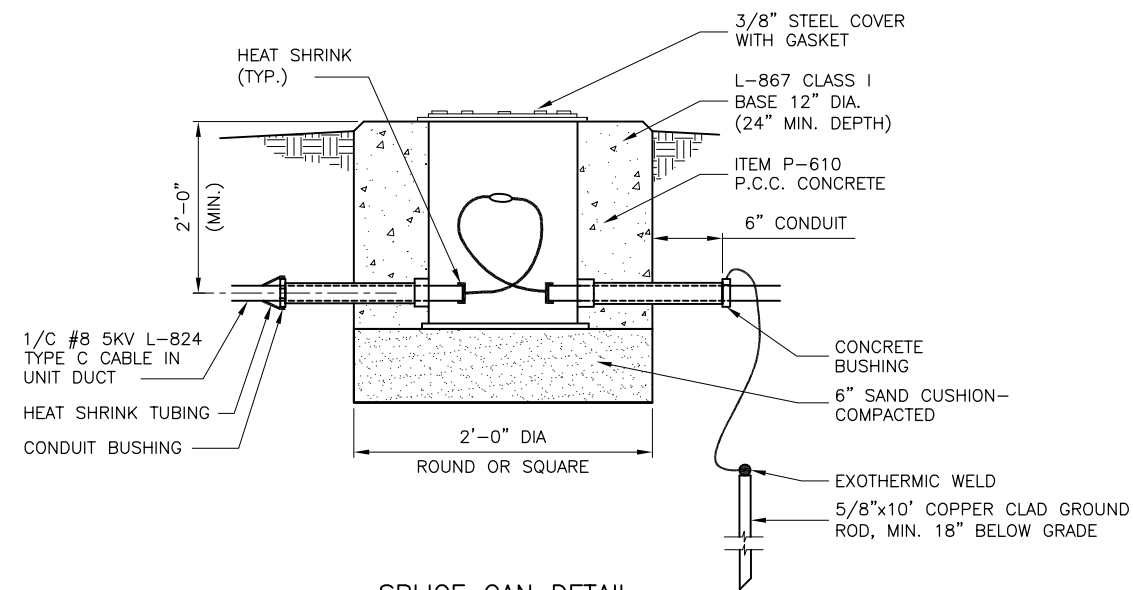


**BASE MOUNTED MARKER LIGHTS**

N.T.S.

**BASE MOUNTED LIGHT NOTES**

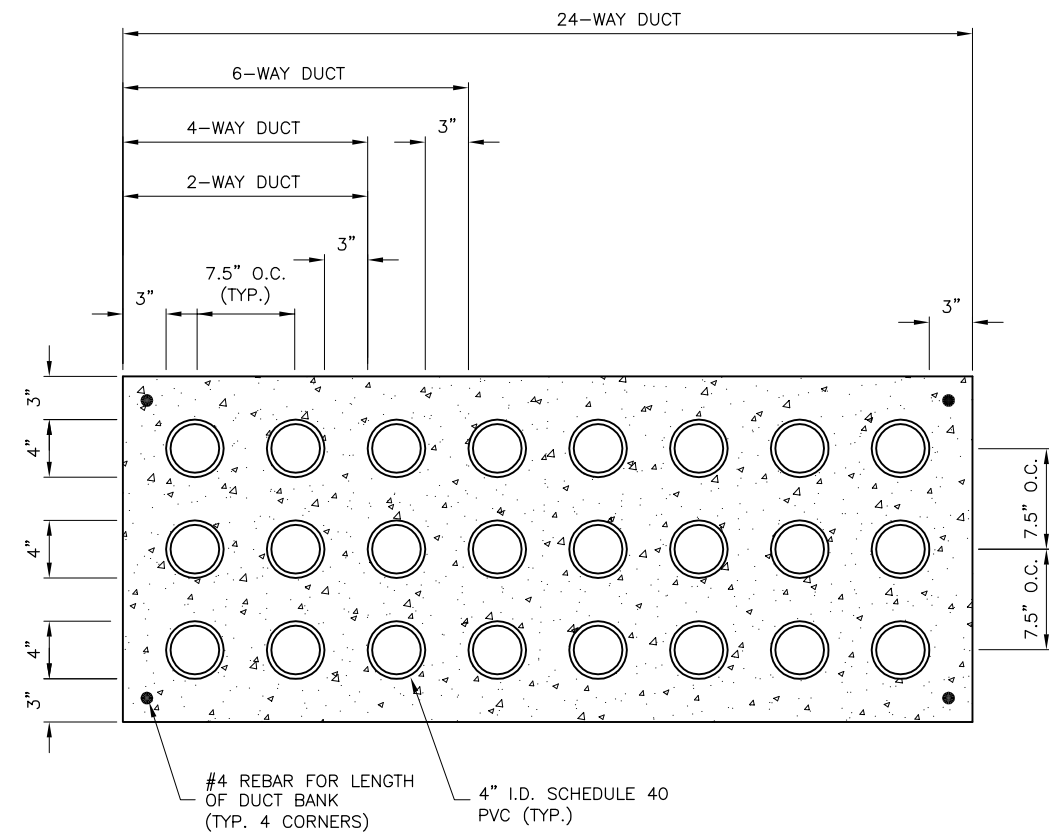
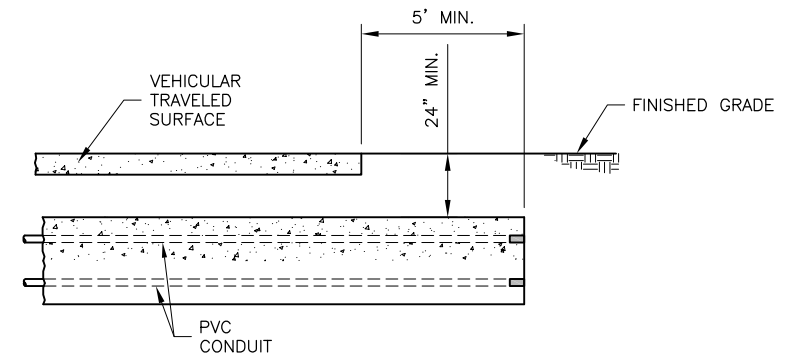
1. FOR MEDIUM INTENSITY TAXIWAY EDGE LIGHTS, THE LAMP SHALL BE A L-861 FIXTURE WITH A 30-45 WATT INCANDESCENT LAMP. FOR HIGH INTENSITY RUNWAY EDGE LIGHTS, THE LAMP SHALL BE A L-862 HIGH INTENSITY QUARTZ, 115 WATT LAMP.
2. FOR MEDIUM INTENSITY TAXIWAY EDGE LIGHTS, THE ISOLATION TRANSFORMER SHALL BE A L-830-1, 30/45 WATT, 6.6/6.6 AMP. FOR HIGH INTENSITY RUNWAY EDGE LIGHTS, THE ISOLATION TRANSFORMER SHALL BE A L-830-1, 100 WATT, 6.6 AMP.



**SPLICE CAN DETAIL**

N.T.S.

UNIT DUCT SHALL BE TERMINATED AT THE L-823 CONNECTORS AND SEALED WITH HEAT SHRINK AS SPECIFIED.



**DUCT BANK DETAILS - TYPICAL**

N.T.S.

**DUCT BANK NOTES**

1. DIMENSIONS SHOWN ARE MINIMUM.
2. TOP OF CONCRETE ENCASEMENT TO BE NOT LESS THAN 24" BELOW FINISHED SUBGRADE.
3. DUCT CONCRETE TO BE ITEM 610 STRUCTURAL P.C.C. CONCRETE.
4. ALL 1-WAY DUCT WILL BE 4" INSIDE DIAMETER.
5. LOCATIONS SHOWN ARE APPROXIMATE. DUCT BANKS TO BE INSTALLED AT LOCATIONS DESIGNATED BY THE ENGINEER.
6. CONTRACTOR WILL INSTALL DUCT BANKS AT A DEPTH WHICH WILL NOT CONFLICT WITH ELEVATION SENSITIVE UTILITIES.
7. A PULL WIRE MUST BE PROVIDED IN EACH UNUSED CONDUIT. UNUSED DUCT MUST BE SEALED TO THE SATISFACTION OF THE ENGINEER.



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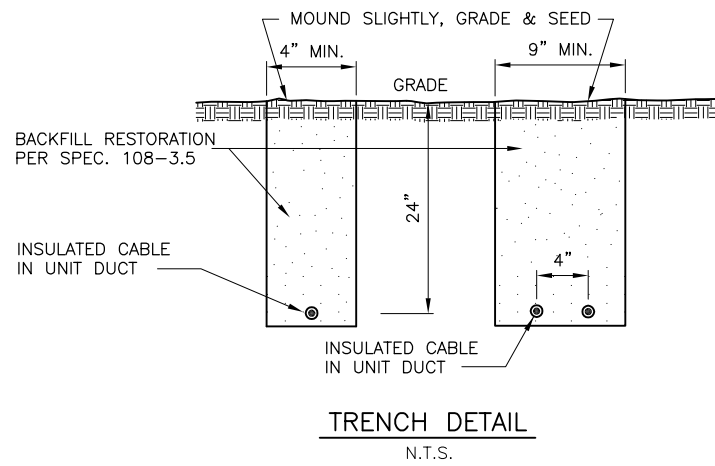
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REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 ELECTRICAL DETAILS SHEET 2

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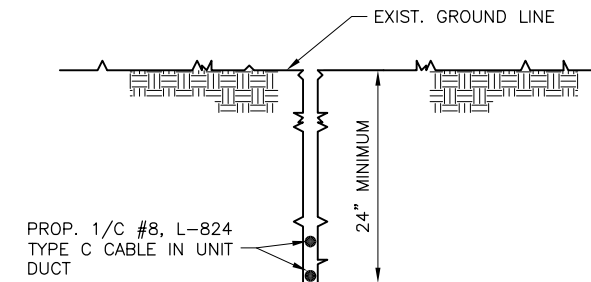
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| IL PROJ. NO.  | PIA-4079        |
| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 61 OF 84 SHEETS |



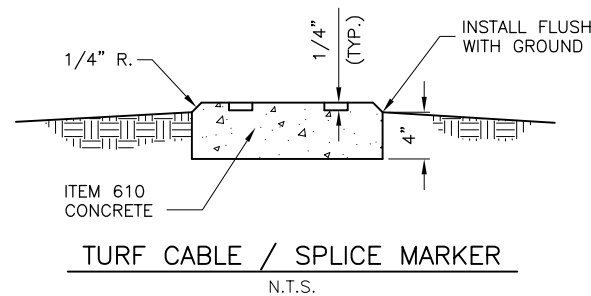
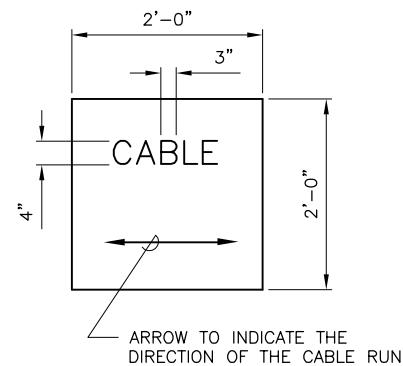
- NOTES**
- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
  - DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
  - SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
  - ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO ITEM 108.

NOTE:  
 AT CONTRACTOR'S OPTION, CABLE PLOWING MAY BE USED IN NON CONGESTED AREAS IN LIEU OF TRENCHING



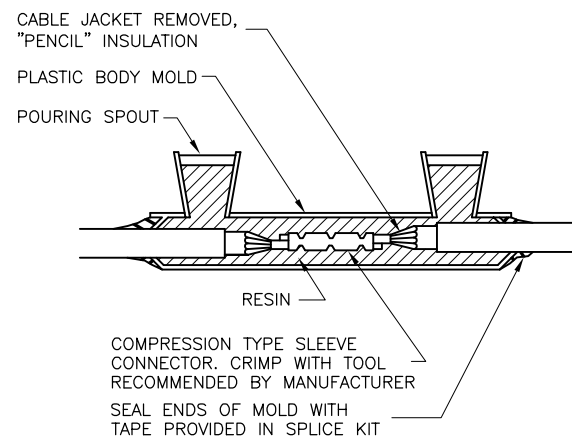
NOTE:  
 WHERE TWO UNIT DUCTS ARE TO BE INSTALLED, CONTRACTOR MAY PLOW UNIT DUCTS SEPARATELY (SIDE BY SIDE)

**CABLE IN UNIT-DUCT - PLOWED**  
 N.T.S.



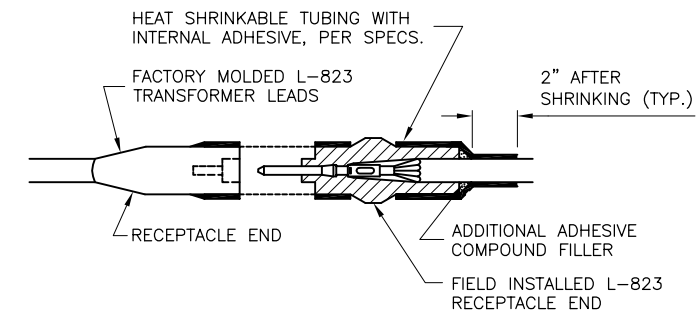
**NOTES**

- CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE HOMERUN.
- ITEM 610 CONCRETE SHALL BE USED.
- ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
- THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED ITEMS.
- 0.049 CU. YD. CONCRETE PER MARKER.
- A MARKER CONFORMING TO THIS DETAIL MARKED "SPLICE" SHALL BE INSTALLED AT ALL SPLICE LOCATIONS NOT IN LIGHT CANS OR MANHOLES.



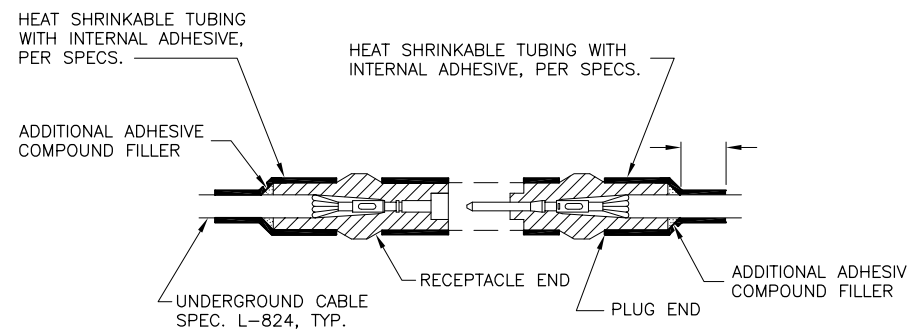
FOR IN-LINE CONNECTIONS OF EXISTING CABLES CUT DURING CONSTRUCTIONS.

**TYPE A**



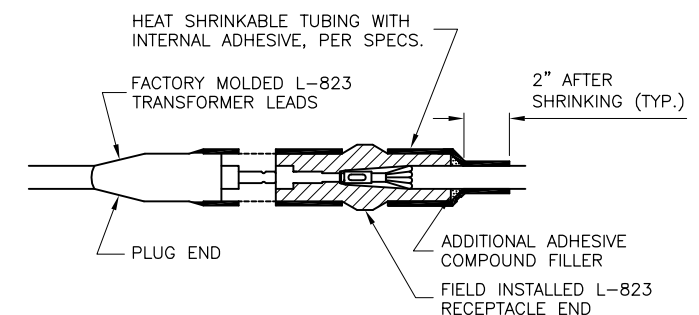
FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.

**TYPE C**



NOT TO BE USED IN THIS PROJECT UNLESS OTHERWISE DIRECTED BY ENGINEER

**TYPE B**



FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.

**TYPE D**

**CABLE SPLICES**

N.T.S.

**NOTES**

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.

**PE092**

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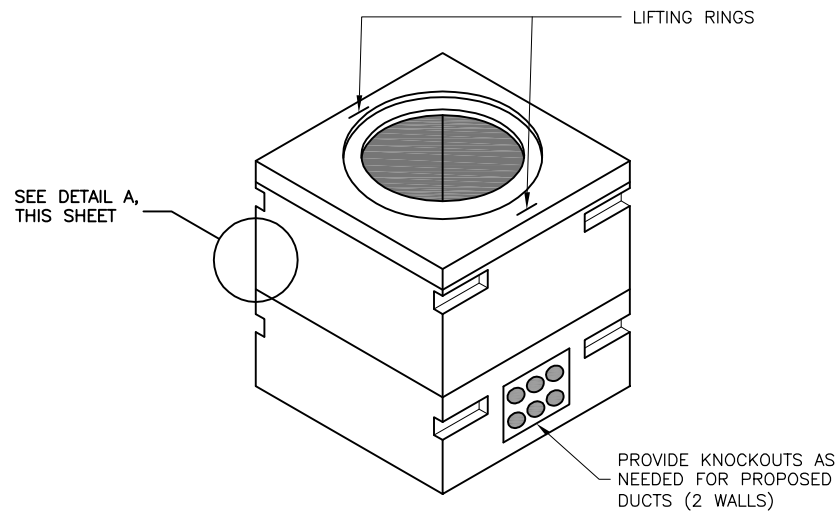
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REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 ELECTRICAL DETAILS SHEET 3

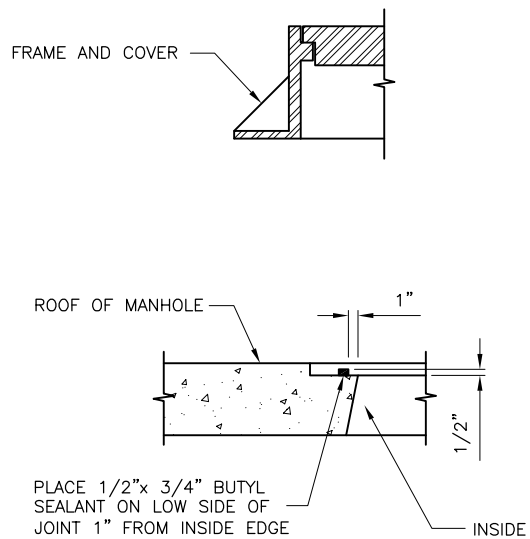
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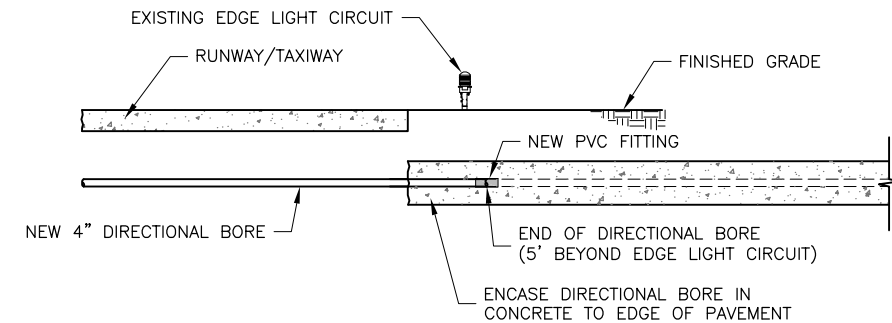
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| SHEET         | 62 OF 84 SHEETS |



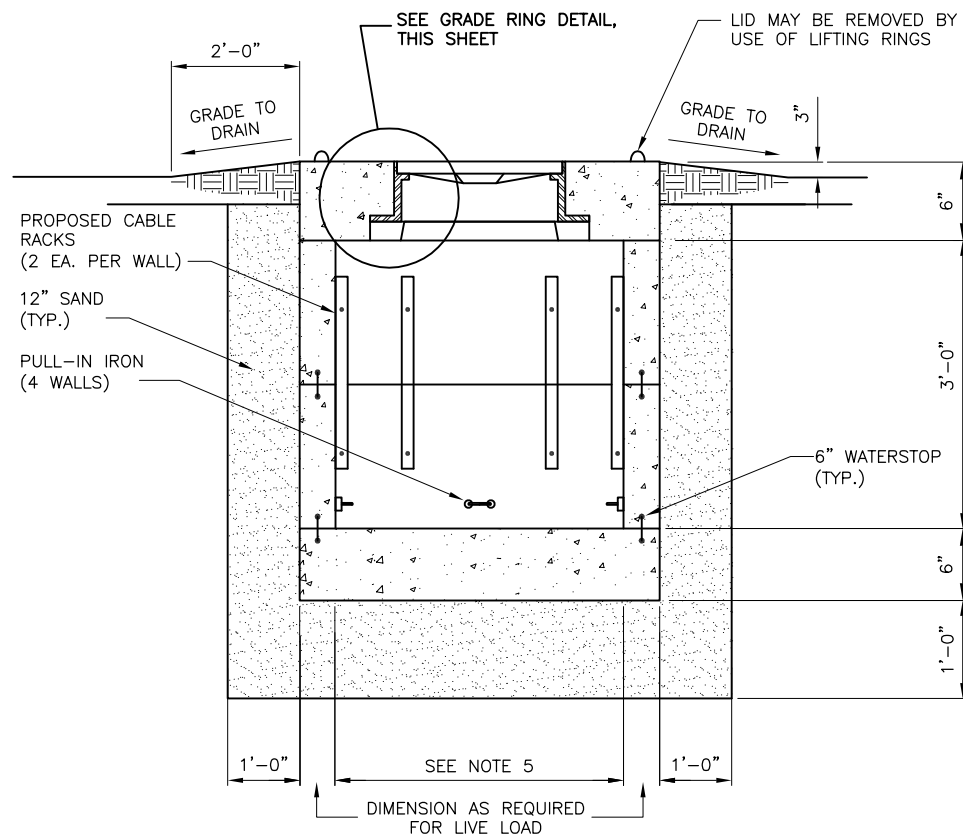
**ELECTRICAL HANDHOLE DETAIL**  
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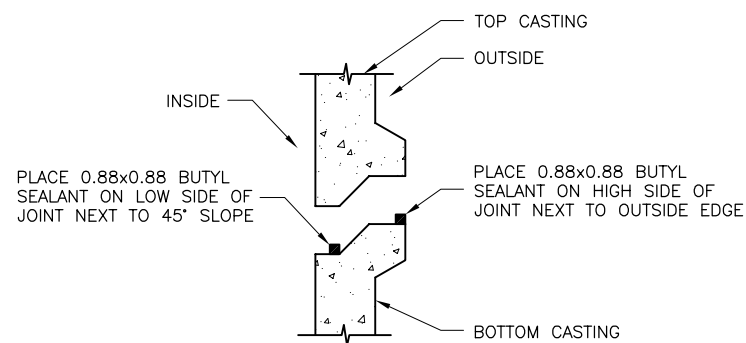
**GRADE RING DETAIL**  
 N.T.S.



**DIRECTIONAL BORE TO CONCRETE ENCASED DUCT CONNECTION DETAIL**  
 N.T.S.



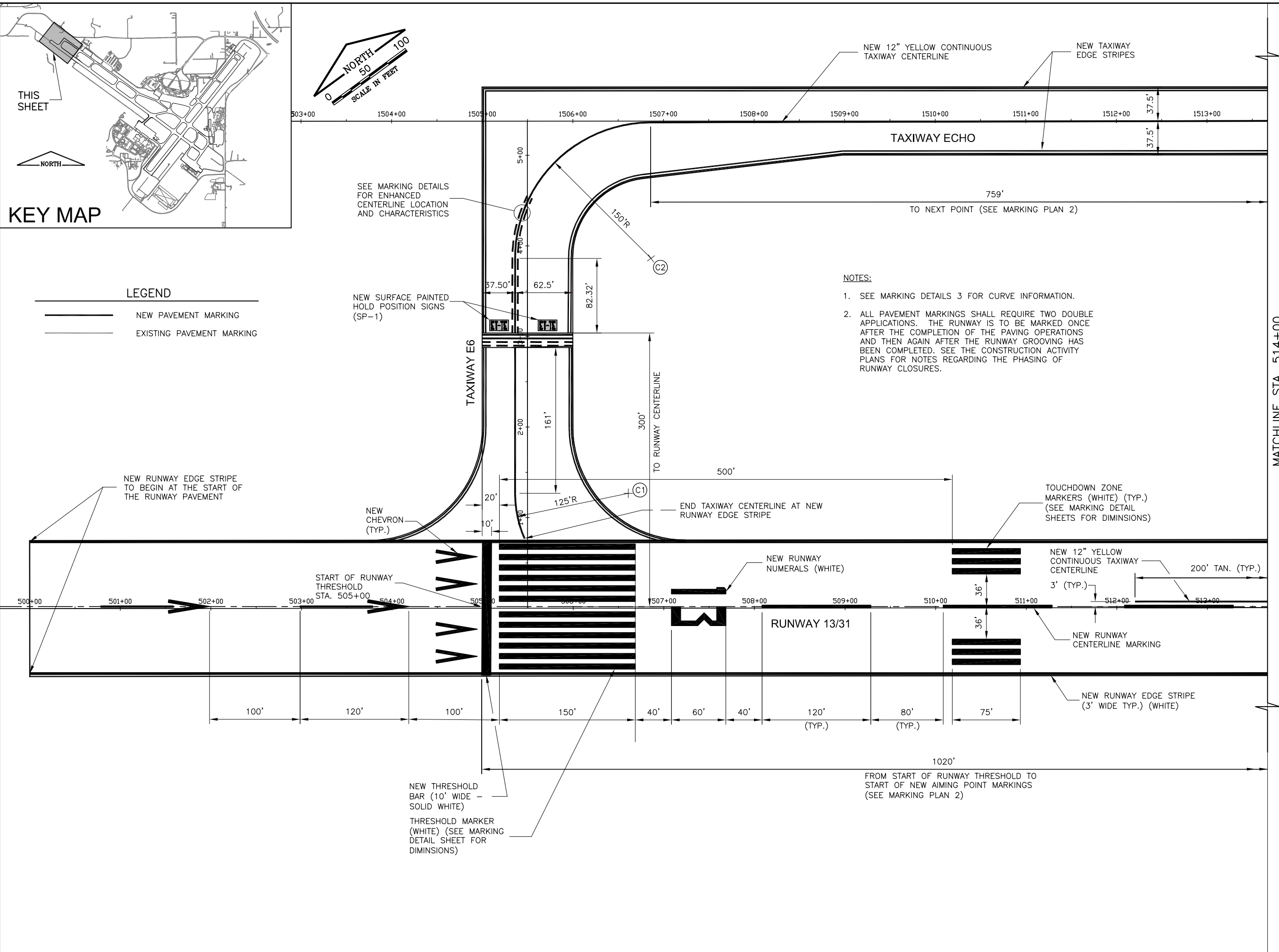
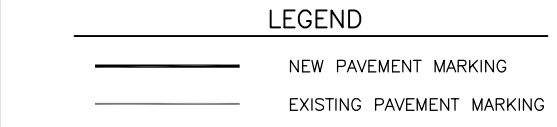
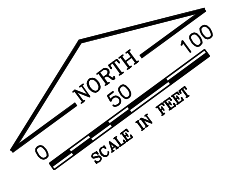
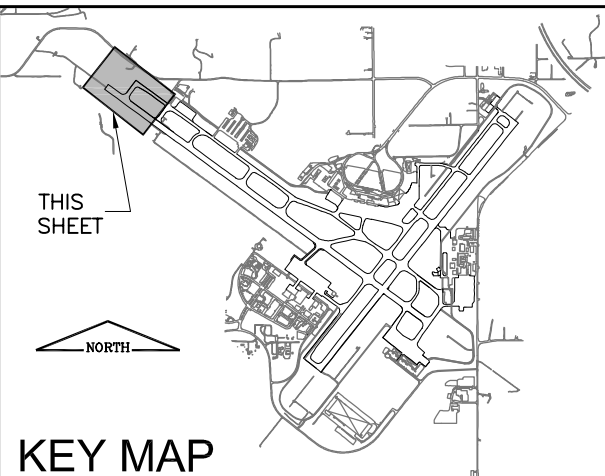
**ELECTRICAL HANDHOLE DETAIL**  
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**DETAIL A**  
 N.T.S.

**NOTES**

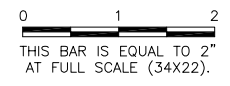
- THE HANDHOLE/GRADE RING/HANDHOLE LID ASSEMBLY SHALL BE CONSTRUCTED TO MEET OR EXCEED THE FOLLOWING LOADINGS:
  - EARTHLOAD = 2 FEET FILL AT 130 LBS/FT<sup>3</sup>.
  - SURCHARD = 2 FEET FILL AT 130 LBS/FT<sup>3</sup>.
  - LIVE LOAD = A.A.S.H.T.O. HS-20 TRUCK WITH 20% IMPACT
  - f'c = 4,500 P.S.I.
  - fy = 60,000 P.S.I.
  - ULTIMATE STRENGTH DESIGN METHOD
 THE SUPPLIER SHALL PROVIDE CERTIFICATION THAT THE HANDHOLES MEET OR EXCEED THESE REQUIREMENTS PRIOR TO INSTALLATION.
- THE HANDHOLE CONSTRUCTION AND INSTALLATION SHALL BE WATERTIGHT. ALL CONSTRUCTION JOINTS AND DUCTS SHALL BE SEALED TO PREVENT WATER ENTRY. ALL UNUSED DUCT BANK OPENINGS IN HANDHOLE SHALL BE SEALED WITH METAL PLATES TREATED FOR CORROSION RESISTANCE AND BOLTED INTO PLACE. MATING SURFACES SHALL BE SEALED USING BUTYL SEALANT.
- THE HANDHOLE LID ASSEMBLY SHALL BE INSTALLED SLIGHTLY ABOVE THE SURROUNDING FINAL GRADE AND THE EARTH SHALL BE GRADED UP TO IT.
- HANDHOLE FRAME AND LID SHALL BE HEAVY DUTY, NEENAH R-1750-C1SB, BOLTED LID, LABELED "COMMUNICATIONS", OR EQUIVALENT.
- THE HANDHOLE LID ASSEMBLY SHALL BE SET THAT IF DESIRED, THE LID MAY BE REMOVED BY USE OF THE LIFTING RINGS.



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 KEYMAP  
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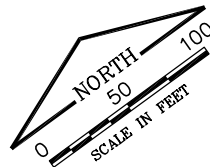
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 MARKING PLAN 1**

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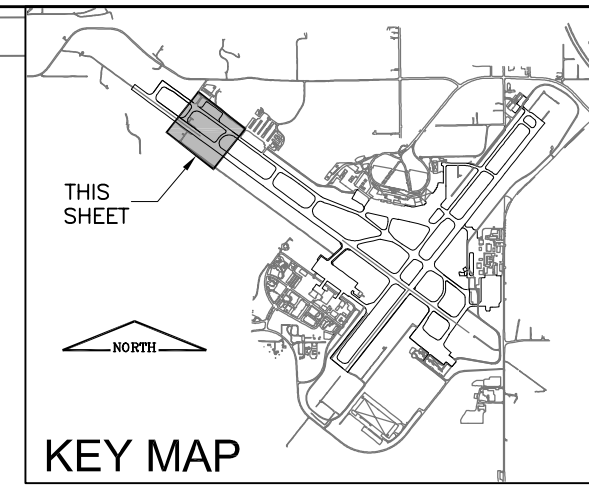
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| SHEET         | 63 OF 84 SHEETS |



**LEGEND**

- NEW PAVEMENT MARKING
- EXISTING PAVEMENT MARKING

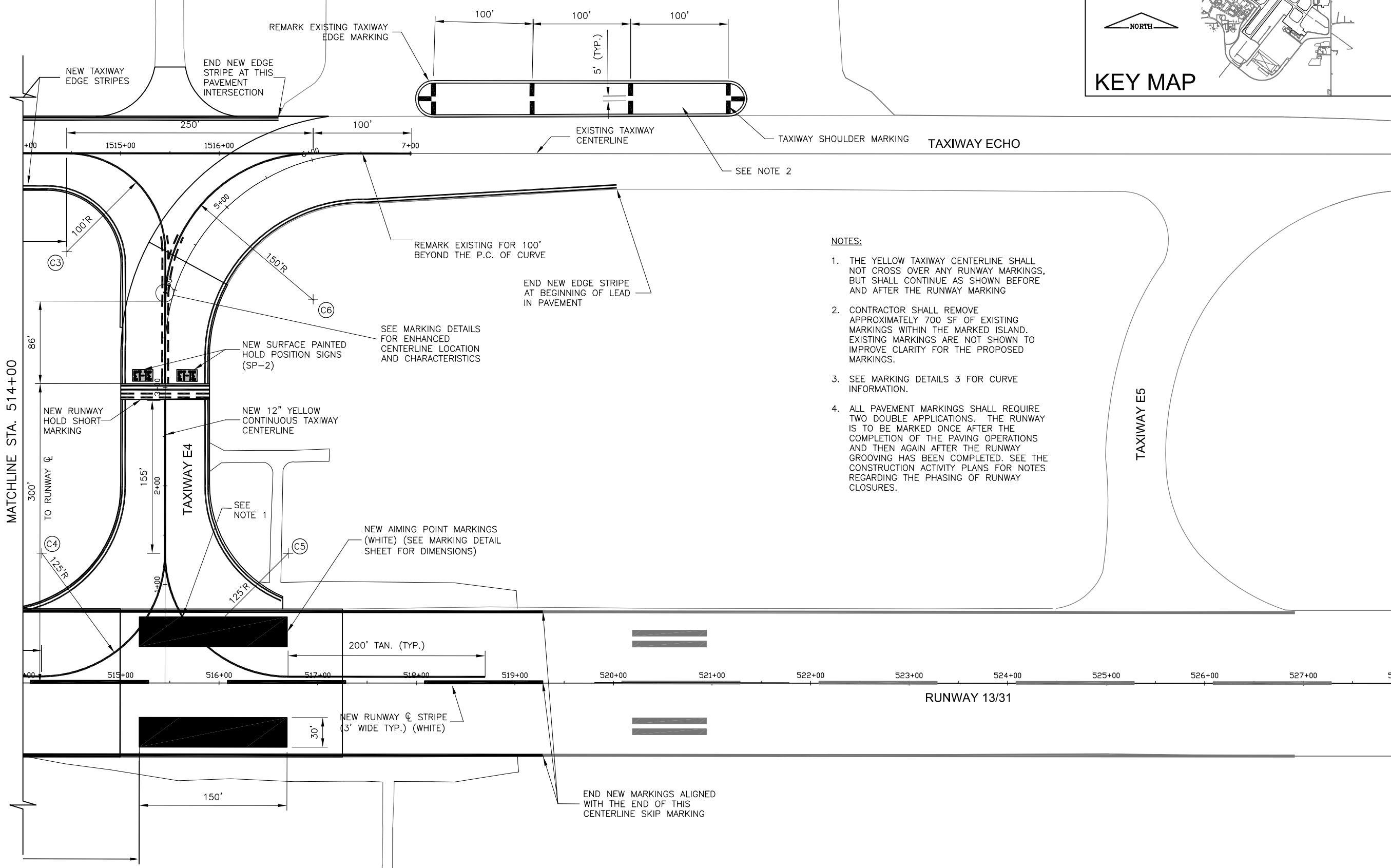


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 BASE\_PROJ\_GEO  
 PIA\_ALIGN  
 PROP\_MARKING

**PE092**

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**NOTES:**

1. THE YELLOW TAXIWAY CENTERLINE SHALL NOT CROSS OVER ANY RUNWAY MARKINGS, BUT SHALL CONTINUE AS SHOWN BEFORE AND AFTER THE RUNWAY MARKING
2. CONTRACTOR SHALL REMOVE APPROXIMATELY 700 SF OF EXISTING MARKINGS WITHIN THE MARKED ISLAND. EXISTING MARKINGS ARE NOT SHOWN TO IMPROVE CLARITY FOR THE PROPOSED MARKINGS.
3. SEE MARKING DETAILS 3 FOR CURVE INFORMATION.
4. ALL PAVEMENT MARKINGS SHALL REQUIRE TWO DOUBLE APPLICATIONS. THE RUNWAY IS TO BE MARKED ONCE AFTER THE COMPLETION OF THE PAVING OPERATIONS AND THEN AGAIN AFTER THE RUNWAY GROOVING HAS BEEN COMPLETED. SEE THE CONSTRUCTION ACTIVITY PLANS FOR NOTES REGARDING THE PHASING OF RUNWAY CLOSURES.

**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**  
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**MARKING PLAN 2**

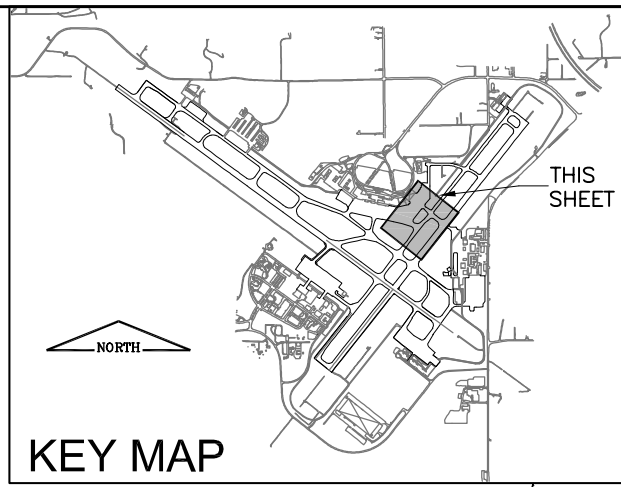
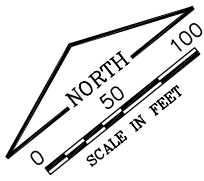
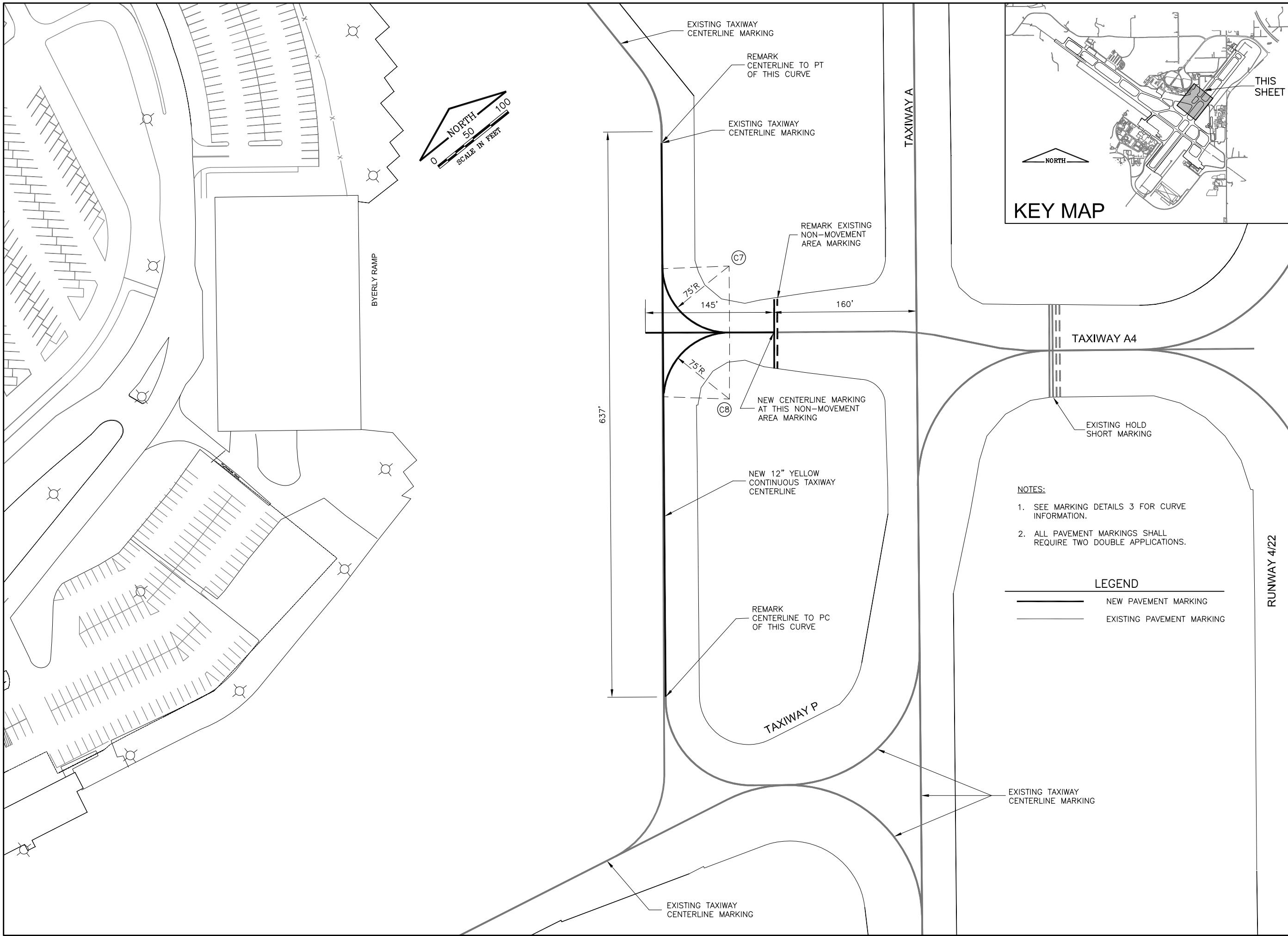
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| AIP PROJ. NO. 3-17-0080-XX |              |
| SHEET 64 OF 84 SHEETS      |              |

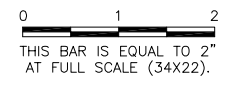




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 PLOT DATE: 5/18/2011 8:28 AM  
 KEYMAP  
 PIA-Bose\_Updated  
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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 MARKING PLAN 3**

- NOTES:**
- SEE MARKING DETAILS 3 FOR CURVE INFORMATION.
  - ALL PAVEMENT MARKINGS SHALL REQUIRE TWO DOUBLE APPLICATIONS.

**LEGEND**

|  |                           |
|--|---------------------------|
|  | NEW PAVEMENT MARKING      |
|  | EXISTING PAVEMENT MARKING |

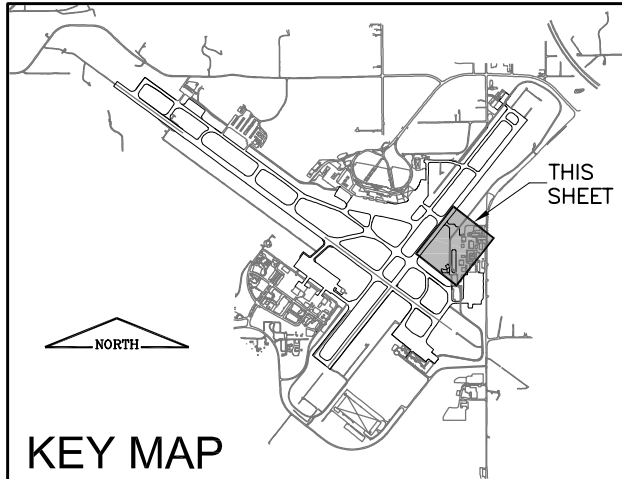
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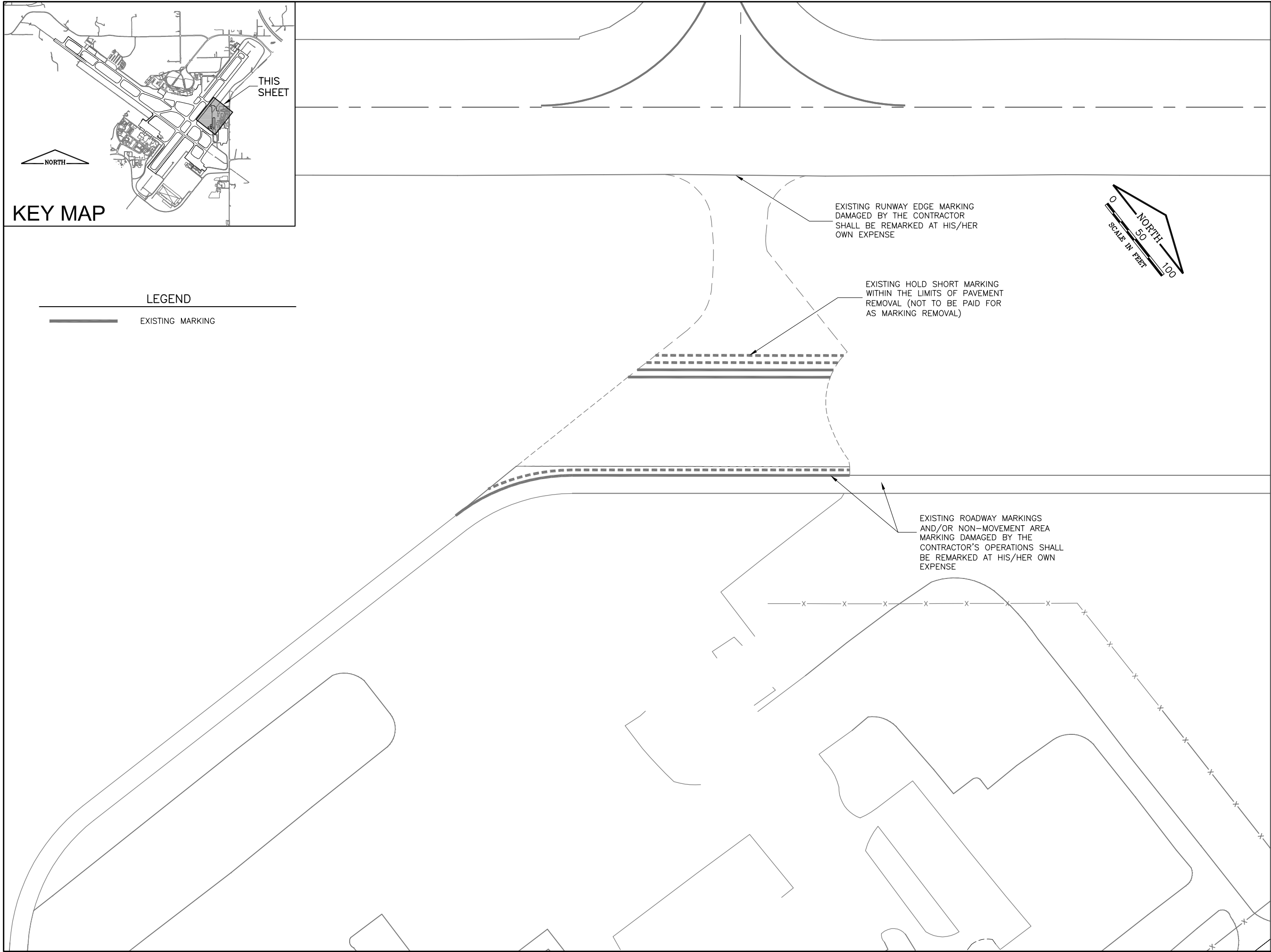
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| SHEET 65 OF 84 SHEETS                                |              |



KEY MAP

LEGEND

|   |                  |
|---|------------------|
|  | EXISTING MARKING |
|---|------------------|



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0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

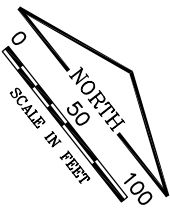
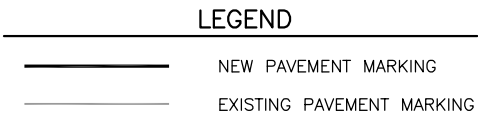
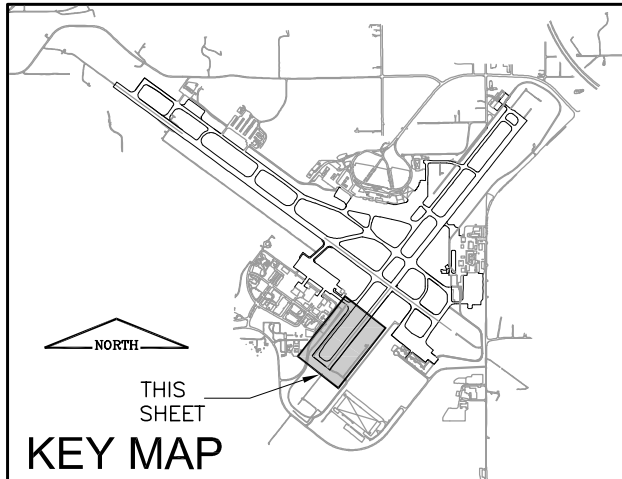
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REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 MARKING PLAN 4

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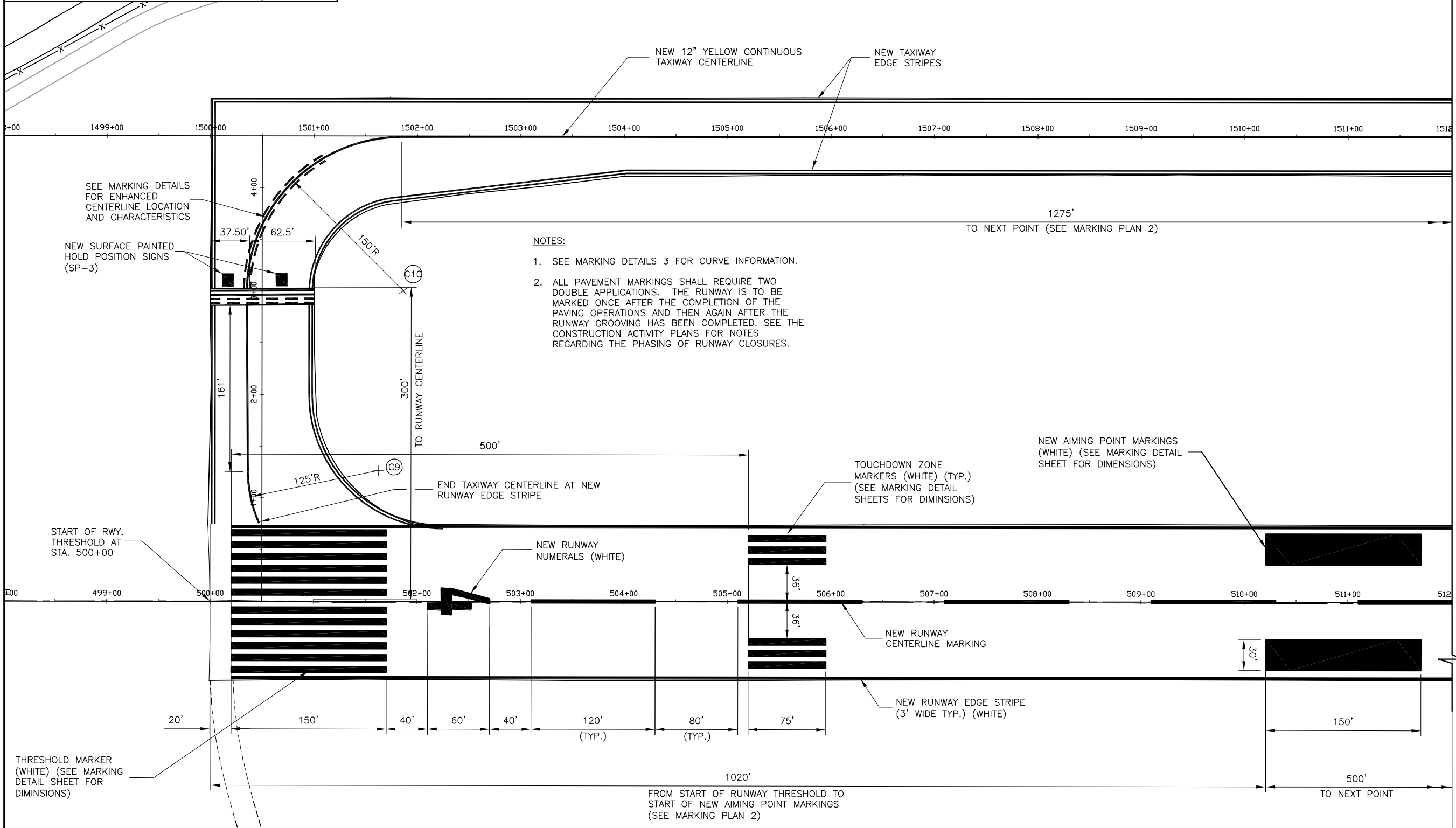
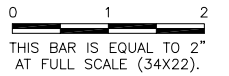
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| SHEET 66 OF 84 SHEETS                               |              |



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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 MARKING PLAN 5**

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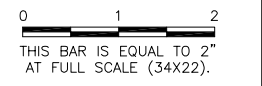
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| AIP PROJ. NO. 3-17-0080-XX |              |
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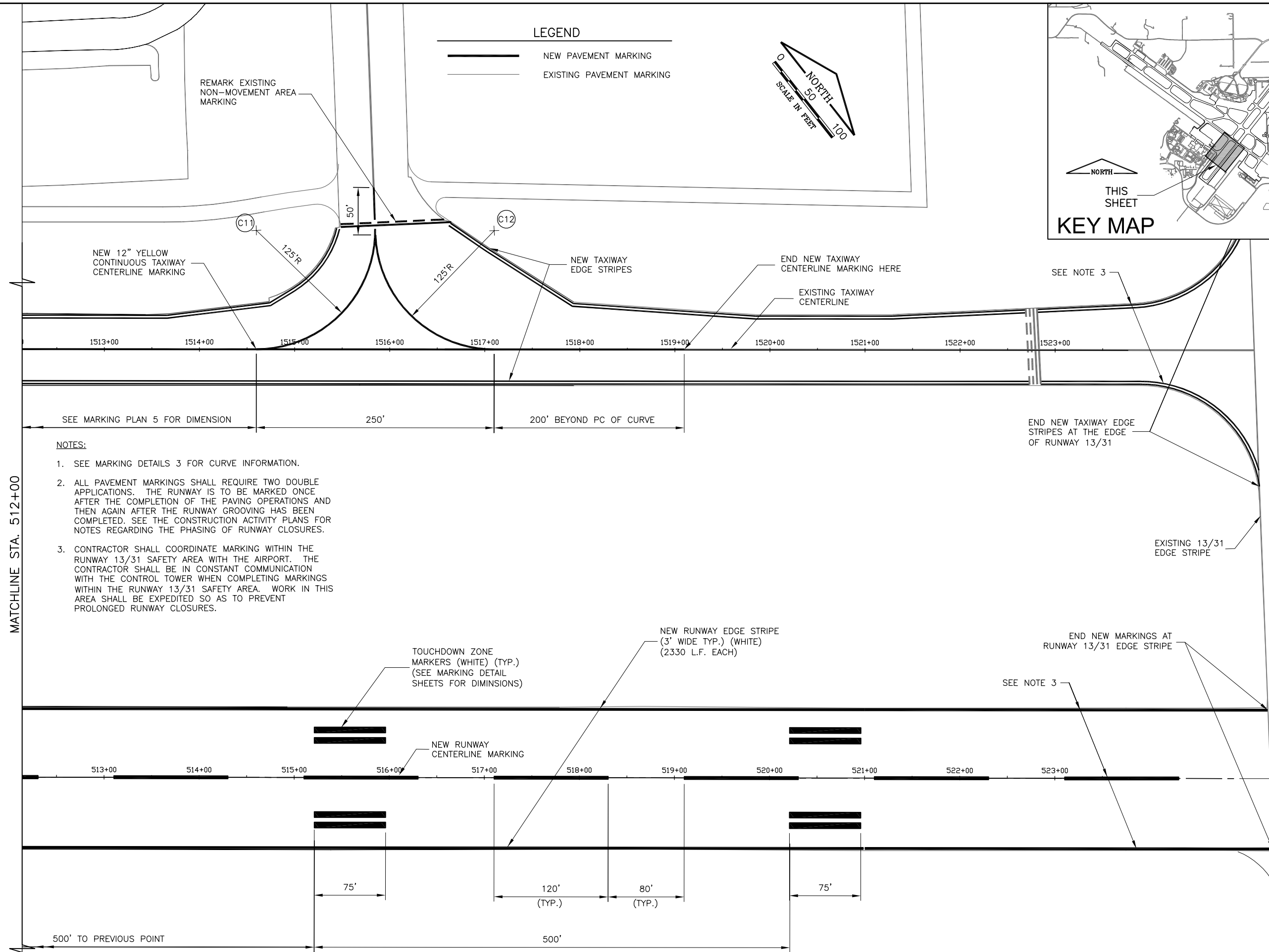
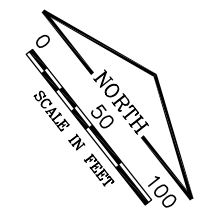
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**LEGEND**

- NEW PAVEMENT MARKING
- EXISTING PAVEMENT MARKING



**NOTES:**

1. SEE MARKING DETAILS 3 FOR CURVE INFORMATION.
2. ALL PAVEMENT MARKINGS SHALL REQUIRE TWO DOUBLE APPLICATIONS. THE RUNWAY IS TO BE MARKED ONCE AFTER THE COMPLETION OF THE PAVING OPERATIONS AND THEN AGAIN AFTER THE RUNWAY GROOVING HAS BEEN COMPLETED. SEE THE CONSTRUCTION ACTIVITY PLANS FOR NOTES REGARDING THE PHASING OF RUNWAY CLOSURES.
3. CONTRACTOR SHALL COORDINATE MARKING WITHIN THE RUNWAY 13/31 SAFETY AREA WITH THE AIRPORT. THE CONTRACTOR SHALL BE IN CONSTANT COMMUNICATION WITH THE CONTROL TOWER WHEN COMPLETING MARKINGS WITHIN THE RUNWAY 13/31 SAFETY AREA. WORK IN THIS AREA SHALL BE EXPEDITED SO AS TO PREVENT PROLONGED RUNWAY CLOSURES.

MATCHLINE STA. 512+00

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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 MARKING PLAN 6**

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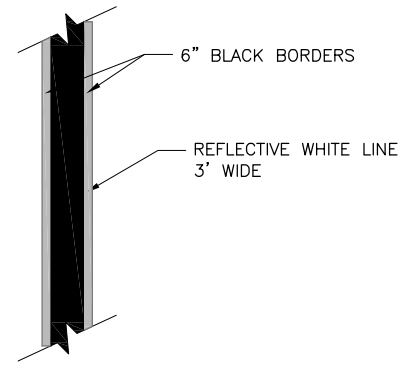
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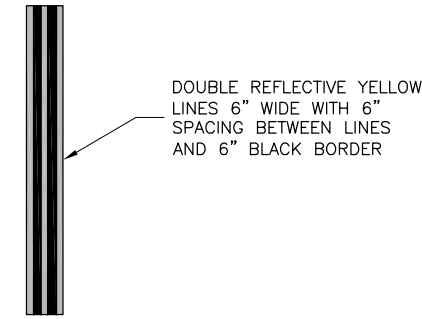
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**MARKING NOTES**

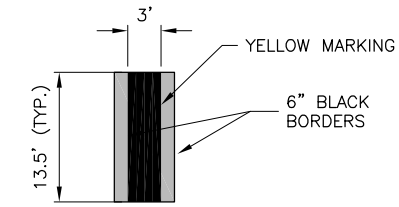
1. ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE BEADS & 6" BLACK BORDER.
2. BLACK BORDER DOES NOT INCLUDE REFLECTIVE BEADS.



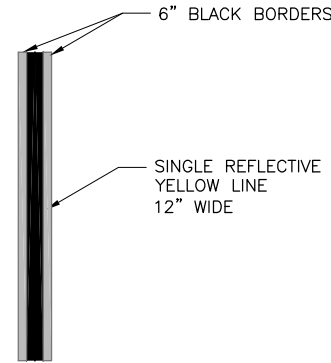
**RUNWAY EDGE STRIPE MARKING (CONTINUOUS)**  
N.T.S.



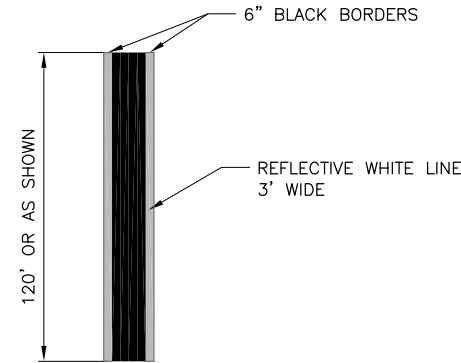
**TAXIWAY EDGE MARKINGS (CONTINUOUS)**  
N.T.S.



**TAXIWAY SHOULDER MARKING DETAIL**  
N.T.S.

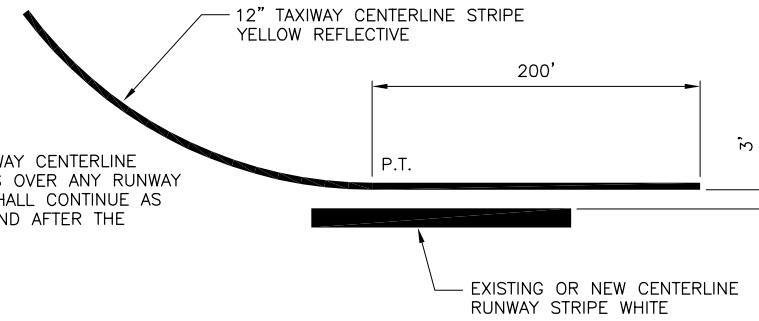


**TAXIWAY CENTERLINE MARKING (CONTINUOUS)**  
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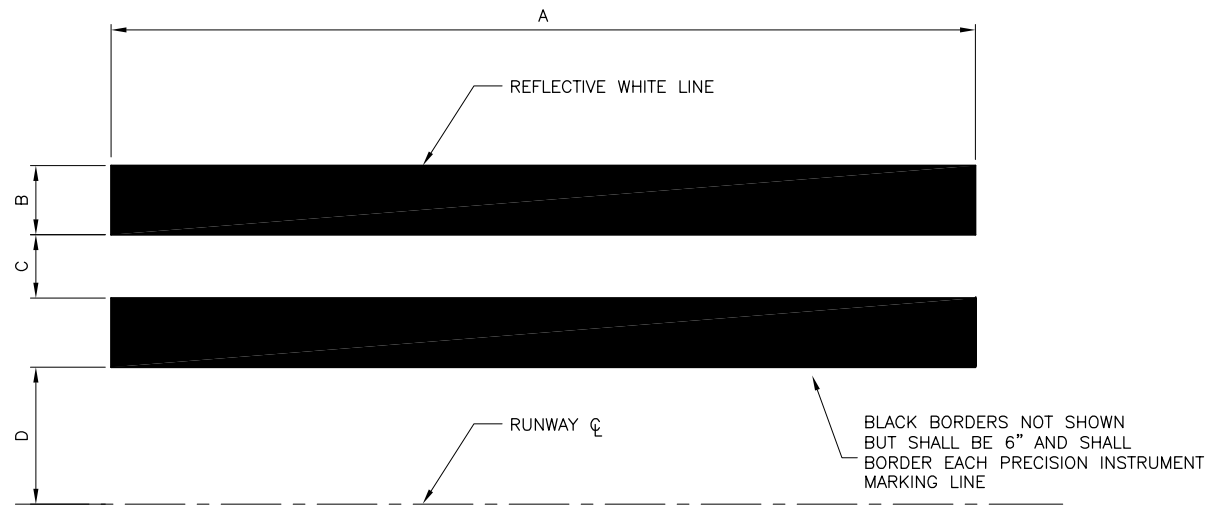


**RUNWAY CENTERLINE STRIPE MARKING DETAIL**  
N.T.S.

**NOTE**  
 1. THE YELLOW TAXIWAY CENTERLINE SHALL NOT CROSS OVER ANY RUNWAY MARKINGS, BUT SHALL CONTINUE AS SHOWN BEFORE AND AFTER THE RUNWAY MARKING

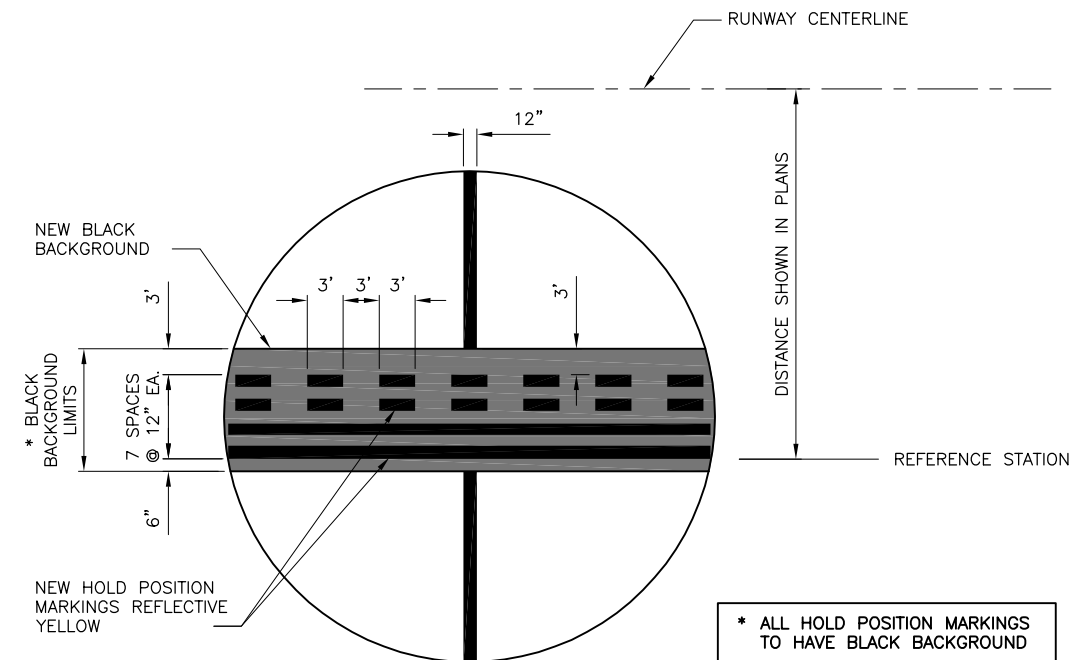


**TAXIWAY EXIT MARKING DETAIL**  
N.T.S.



| 150' RUNWAY              |             |       |       |       |
|--------------------------|-------------|-------|-------|-------|
| MARKING (PER RUNWAY END) | DIMENSION * |       |       |       |
|                          | A           | B     | C     | D     |
| THRESHOLD MARKER         | 150'        | 5.75' | 5.75' | 5.75' |
| AIMING POINT MARKING     | 150'        | 30'   | —     | 36'   |
| TOUCHDOWN ZONE MARKER    | 75'         | 6'    | 5'    | 36'   |

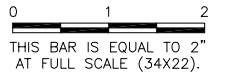
**PRECISION INSTRUMENT MARKING DETAIL**  
N.T.S.



**RUNWAY HOLD POSITION MARKING DETAIL**  
N.T.S.

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REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 MARKING DETAILS 1

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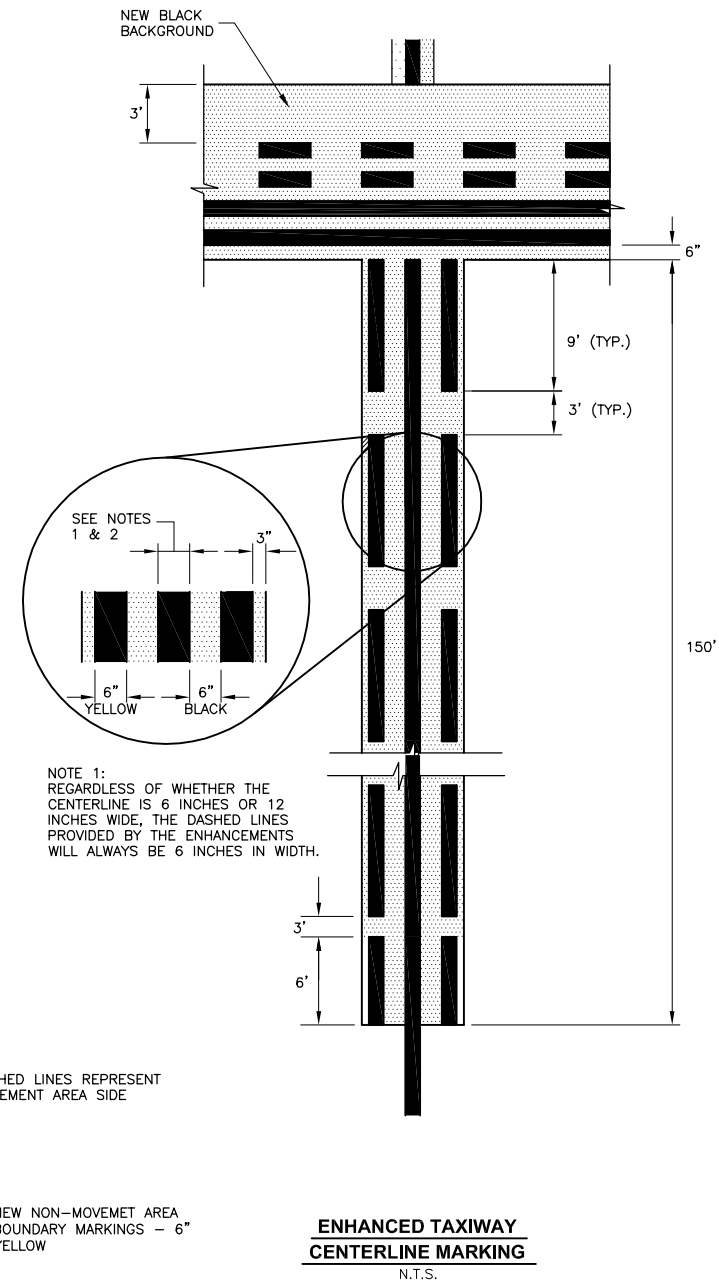
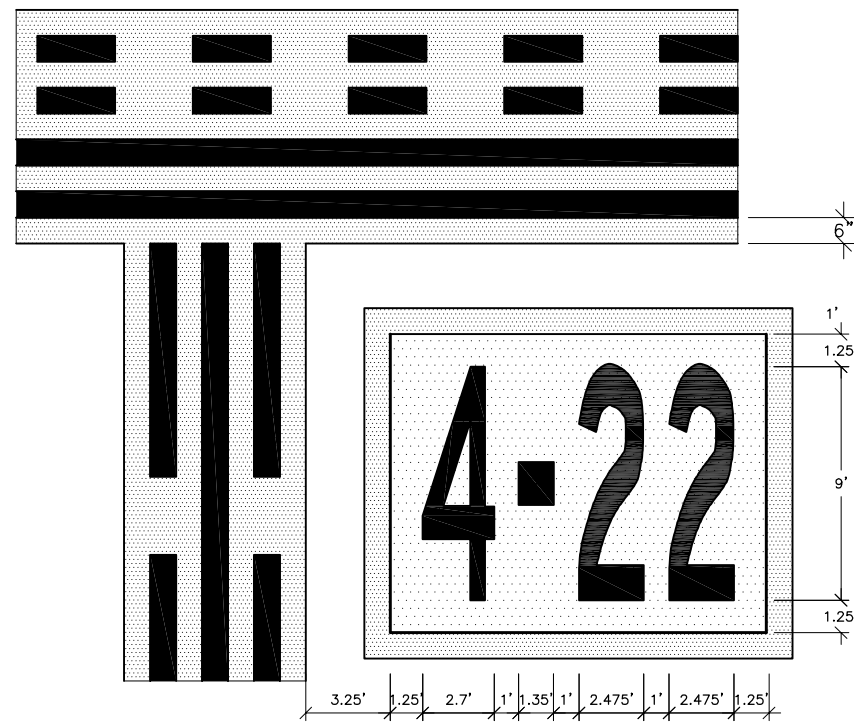
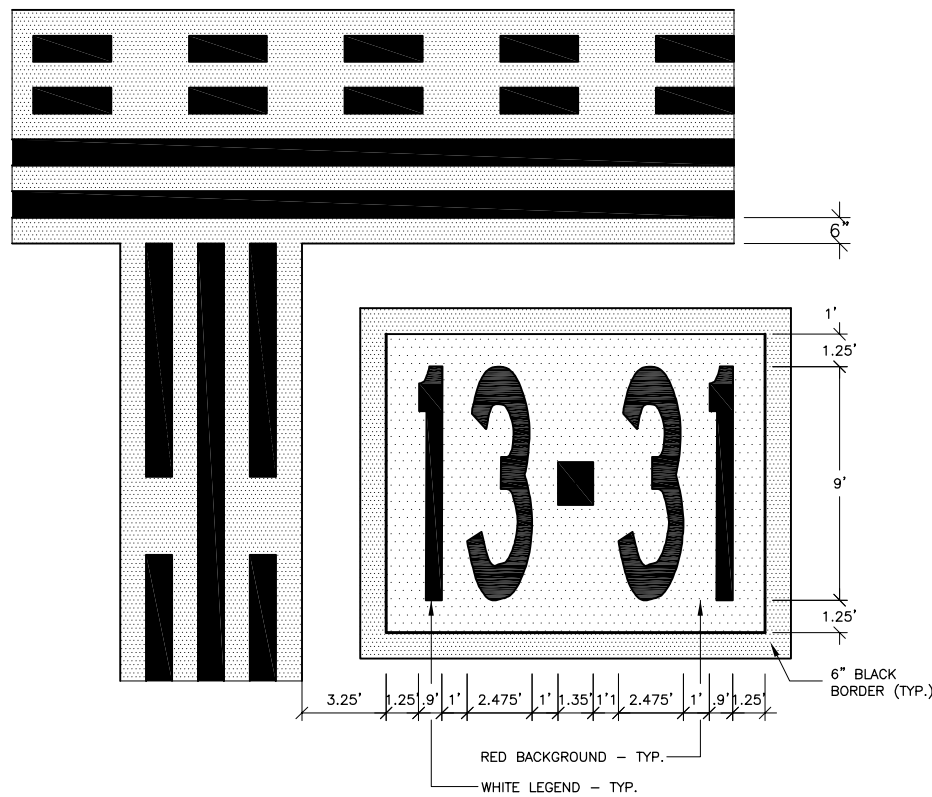
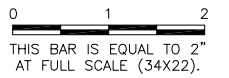
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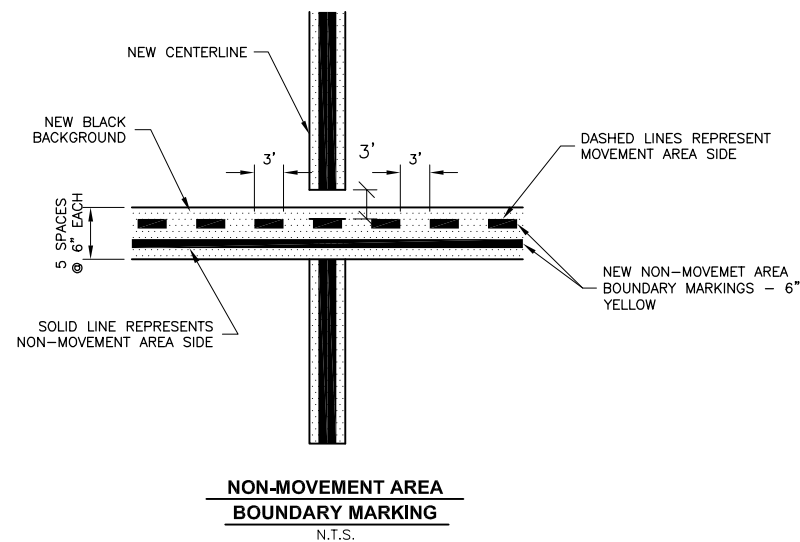
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NOTE 1:  
 REGARDLESS OF WHETHER THE CENTERLINE IS 6 INCHES OR 12 INCHES WIDE, THE DASHED LINES PROVIDED BY THE ENHANCEMENTS WILL ALWAYS BE 6 INCHES IN WIDTH.

- MARKING NOTES**
1. ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE BEADS & 6" BLACK BORDER.
  2. BLACK BORDER DOES NOT INCLUDE REFLECTIVE BEADS.
  3. WHEN PAINTING THE SURFACE PAINTED HOLD POSITION SIGNS, THE CONTRACTOR SHALL ENSURE THAT THE INFORMATION GIVEN IN THE MARKING SCHEDULE TABLE BELOW MATCHES THE ADJACENT TAXIWAY SIGN. THE NUMBERS DETAILED ABOVE ARE SHOWN FOR DIMENSIONING PURPOSES. THE CONTRACTOR SHOULD REFER TO THE MARKING LEGEND IN THE TABLE BELOW WHEN PAINTING THE SURFACE PAINTED HOLD POSITION SIGNS.



**ENHANCED TAXIWAY CENTERLINE MARKING**  
 N.T.S.

**NON-MOVEMENT AREA BOUNDARY MARKING**  
 N.T.S.

**TABLE NOTES**  
 \* MARKING DIMENSIONS IN TABLE DO NOT INCLUDE THE 6" BLACK PAINT THAT IS TO BORDER THE OUTSIDE OF THE RED PAINT.

**MARKING SCHEDULE**

| MARK # | MARKING LEGEND | TAXIWAY HOLDING SHORT | LOCATION ON TAXIWAY | WHITE LEGEND ON RED BACKGROUND WITH BLACK BORDER | NUMBER OF CHARACTERS | * MARKING DIMENSION | RUNWAY HOLDING | NOTES                     |
|--------|----------------|-----------------------|---------------------|--|----------------------|---------------------|----------------|---------------------------|
| SP-1   | 31-13          | TAXIWAY E7            | BOTH SIDES OF TXY ☉ | 31-13  | 5                    | 11.5'h x 13.6'w     | RWY 13/31      | NEW HOLD POSITION MARKING |
| SP-2   | 31-13          | TAXIWAY E6            | BOTH SIDES OF TXY ☉ | 31-13  | 5                    | 11.5'h x 113.6'w    | RWY 13/31      | NEW HOLD POSITION MARKING |
| SP-3   | 4              | TAXIWAY A             | BOTH SIDES OF TXY ☉ | 4  | 1                    | 11.5'h x 5.2'w      | RWY 4/22       | NEW HOLD POSITION MARKING |

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REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 MARKING DETAILS 2

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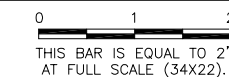
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**MARKING DETAILS 3**

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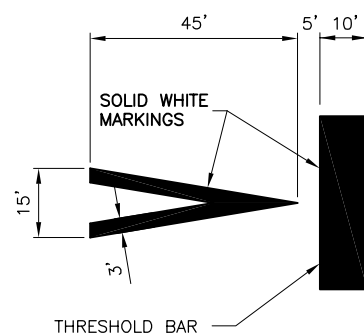
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| SHEET         | 71 OF 84 SHEETS |

| MARKING COORDINATES |       |              |              |          |           |            |  |
|---------------------|-------|--------------|--------------|----------|-----------|------------|--|
| CURVE               | POINT | NORTHING     | EASTING      | BASELINE | STATION   | OFFSET     | CURVE ELEMENTS   |
| C1                  | PC    | 1,459,352.51 | 2,422,664.05 | TXY. E7  | 0+76.9    | 3.4' LT.   | RADIUS = 125'<br>LENGTH = 51.3'<br>DELTA = 23°32'3.40"   |
|                     | RP    | 1,459,325.04 | 2,422,785.99 | TXY. E7  | 1+26.9    | 137.5' RT. |  |
|                     | PT    | 1,459,398.92 | 2,422,685.16 | TXY. E7  | 1+26.9    | 12.5' LT.  |  |
| C2                  | PC    | 1,459,608.15 | 2,422,838.36 | TXY. E7  | 3+86.2    | 12.5' LT.  | RADIUS = 150'<br>LENGTH = 137.5'<br>DELTA = 90°00'00"    |
|                     | RP    | 1,459,519.53 | 2,422,959.39 | TXY. E7  | 3+86.2    | 137.5' RT. |  |
|                     | PT    | 1,459,640.75 | 2,423,047.75 | TXY. E   | 1506+00   | 0.0        |  |
| C3                  | PC    | 1,459,195.05 | 2,423,662.77 | TXY. E   | 1514+45.4 | 0.0        | RADIUS = 100'<br>LENGTH = 157.1'<br>DELTA = 90°00'00"    |
|                     | RP    | 1,459,114.39 | 2,423,603.67 | TXY. E   | 1514+45.4 | 97.8' RT.  |  |
|                     | PT    | 1,459,055.29 | 2,423,684.33 | TXY. E6  | 4+31.8    | 24.5' LT.  |  |
| C4                  | PC    | 1,458,806.97 | 2,423,502.40 | TXY. E6  | 1+36.6    | 0.0'       | RADIUS = 125'<br>LENGTH = 196.3'<br>DELTA = 90°00'00"    |
|                     | RP    | 1,458,880.84 | 2,423,401.56 | TXY. E6  | 1+31.6    | 125.0' LT. |  |
|                     | PT    | 1,458,780.01 | 2,423,327.69 | RWY. 13  | 514+19.9  | 6.7' LT.   |  |
| C5                  | PC    | 1,458,806.97 | 2,423,502.40 | TXY. E6  | 1+31.6    | 0.0'       | RADIUS = 125'<br>LENGTH = 196.3'<br>DELTA = 90°00'00"    |
|                     | RP    | 1,458,733.09 | 2,423,603.23 | TXY. E6  | 1+31.6    | 125.0' RT. |  |
|                     | PT    | 1,458,632.26 | 2,423,529.35 | RWY. 13  | 516+69.9  | 6.7' LT.   |  |
| C6                  | PC    | 1,459,014.95 | 2,423,654.78 | TXY. E6  | 3+88.3    | 6.6' LT.   | RADIUS = 150'<br>LENGTH = 235.6'<br>DELTA = 90°00'00"    |
|                     | RP    | 1,458,926.30 | 2,423,775.78 | TXY. E6  | 4+98.6    | 150.0' RT. |  |
|                     | PT    | 1,459,047.30 | 2,423,864.43 | TXY. E6  | 6+03.1    | 8.1' LT.   |  |
| C7                  | PC    | 1,456,674.41 | 2,429,909.11 | N/A      | N/A       | N/A        | RADIUS = 75'<br>LENGTH = 115'<br>DELTA = 88°11'02.29"    |
|                     | RP    | 1,456,629.82 | 2,429,969.11 | N/A      | N/A       | N/A        |  |
|                     | PT    | 1,456,571.26 | 2,429,922.64 | N/A      | N/A       | N/A        |  |
| C8                  | PC    | 1,456,561.83 | 2,429,819.03 | N/A      | N/A       | N/A        | RADIUS = 75'<br>LENGTH = 105'<br>DELTA = 86°55'39.54"    |
|                     | RP    | 1,456,510.40 | 2,429,875.91 | N/A      | N/A       | N/A        |  |
|                     | PT    | 1,456,571.02 | 2,429,922.87 | N/A      | N/A       | N/A        |  |
| C9                  | PC    | 1,452,758.64 | 2,427,497.70 | TXY. A6  | 0+75.6    | 0.7' LT.   | RADIUS = 125'<br>LENGTH = 51.3'<br>DELTA = 23°32'03.40"  |
|                     | RP    | 1,452,679.68 | 2,427,528.94 | TXY. A6  | 1+26.2    | 112.5'     |  |
|                     | PT    | 1,452,581.18 | 2,427,451.97 | TXY. A6  | 1+25.4    | 12.5' LT.  |  |
| C10                 | PC    | 1,452,690.16 | 2,427,312.52 | TXY. A6  | 3+02.     | 12.5' LT.  | RADIUS = 150'<br>LENGTH = 232.2'<br>DELTA = 88°41'16.29" |
|                     | RP    | 1,452,805.70 | 2,427,408.19 | TXY. A   | 42+99.2   | 150' RT.   |  |
|                     | PT    | 1,452,899.30 | 2,427,289.89 | TXY. A   | 1502+00   | 0.0'       |  |
| C11                 | PC    | 1,453,898.79 | 2,428,060.75 | TXY. A   | 1514+59.8 | 0.0        | RADIUS = 125'<br>LENGTH = 200'<br>DELTA = 92°06'44.96"   |
|                     | RP    | 1,453,976.29 | 2,427,982.67 | TXY. A   | 1514+59.7 | 125' LT.   |  |
|                     | PT    | 1,454,077.16 | 2,428,056.50 | TXY. A   | 1515+84.6 | 129.7' LT. |  |
| C12                 | PC    | 1,454,071.59 | 2,428,064.11 | TXY. A   | 1514+84.6 | 125' LT.   | RADIUS = 125'<br>LENGTH = 191.6'<br>DELTA = 90°00'00"    |
|                     | RP    | 1,454,172.46 | 2,428,137.94 | TXY. A   | 1517+09.9 | 125' LT.   |  |
|                     | PT    | 1,454,094.88 | 2,428,235.95 | TXY. A   | 1517+09.9 | 0.0        |  |

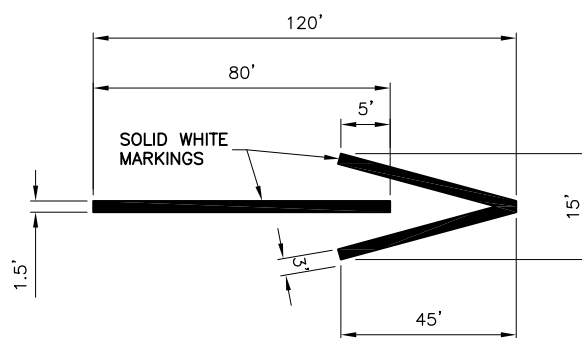
**ARROWHEAD NOTES:**

1. ARROWHEAD IS 295 SF WHICH INCLUDES BLACK BORDER.



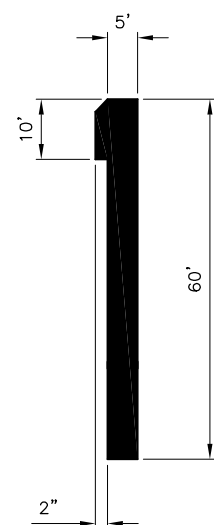
ARROWHEAD AND THRESHOLD BAR DETAILS

N.T.S.

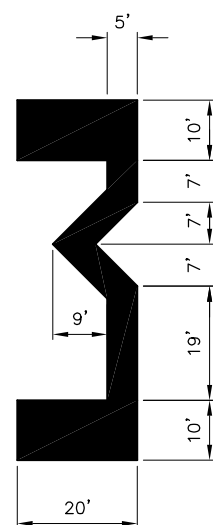


ARROW MARKING DETAIL

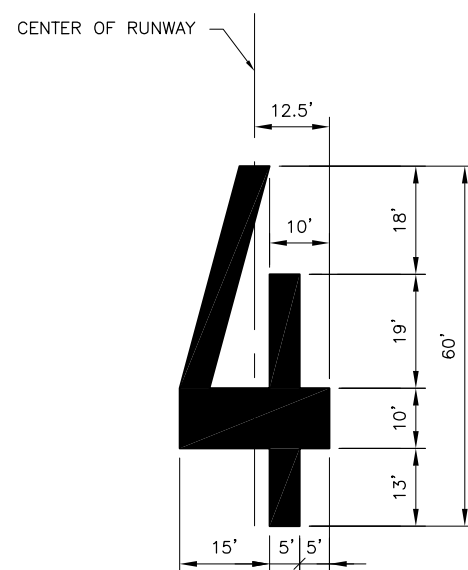
N.T.S.



(AREA = 385 SF)



(AREA = 735 SF)



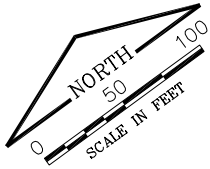
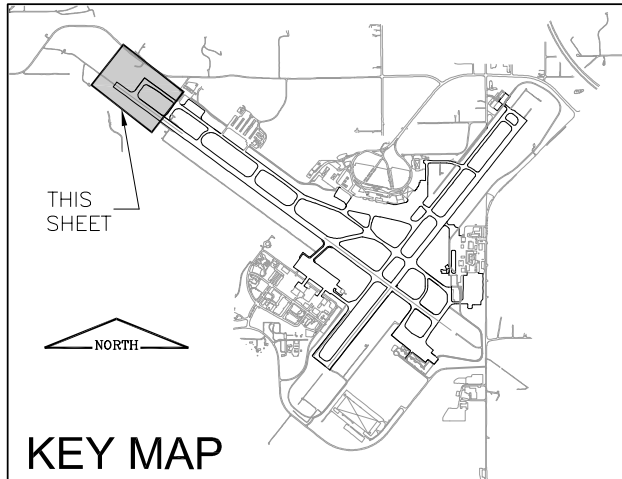
(AREA = 701 SF)

RUNWAY NUMERALS DETAIL

N.T.S.

**MARKING NOTES**

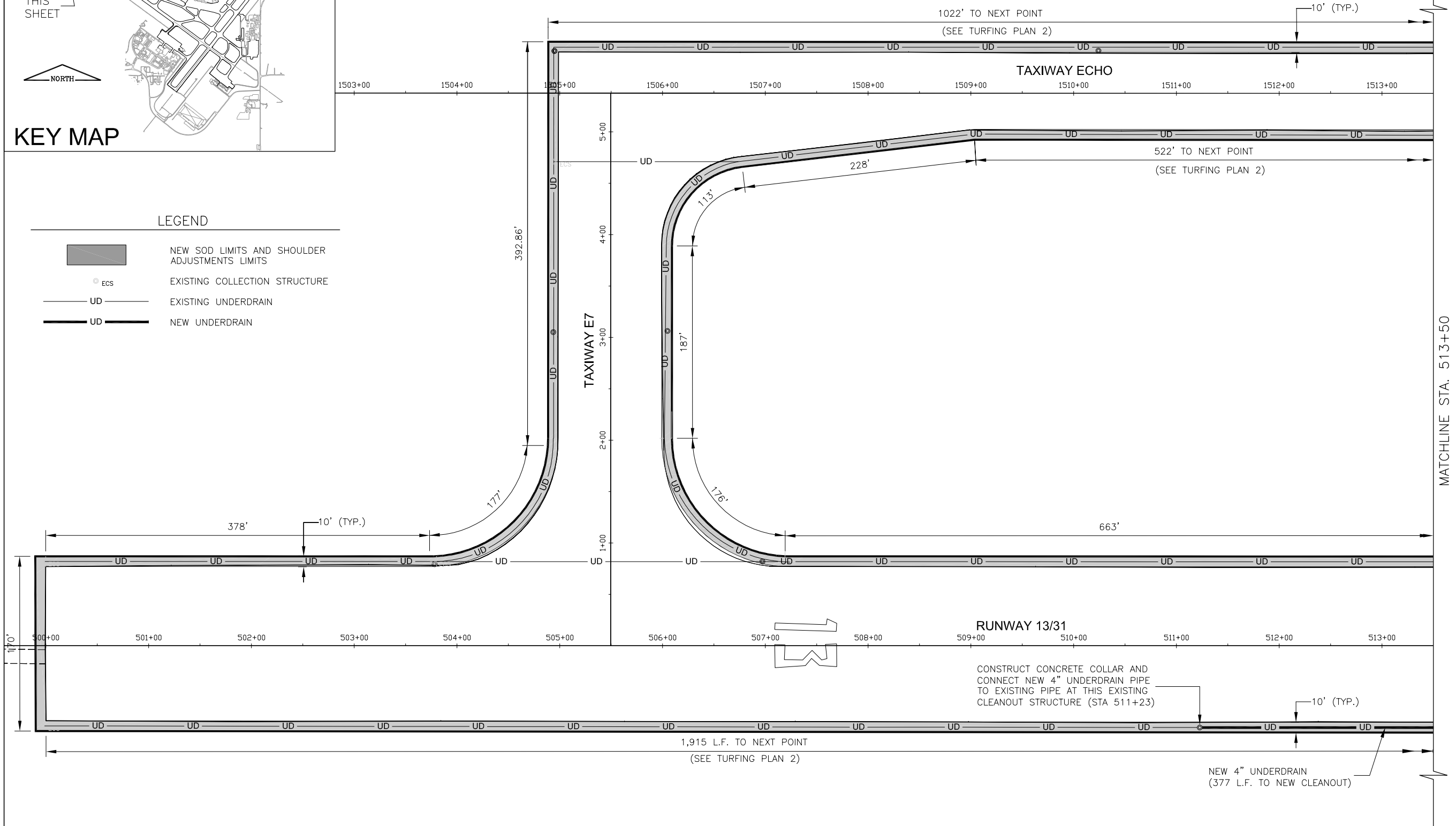
1. ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE BEADS & 6" BLACK BORDER.
2. BLACK BORDER DOES NOT INCLUDE REFLECTIVE BEADS.
3. AREA SHOWN FOR THE NUMERALS INCLUDES THE AREA OF THE BLACK BORDER TO BE PAINTED.



**KEY MAP**

**LEGEND**

- NEW SOD LIMITS AND SHOULDER ADJUSTMENTS LIMITS
- EXISTING COLLECTION STRUCTURE
- EXISTING UNDERDRAIN
- NEW UNDERDRAIN



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 PIA\_ALIGN  
 PIA\_TOPO  
 BASE\_PROP\_DRAIN  
 PIA BASE 9\_03\_08

**PE092**

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0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT  
 PEORIA, ILLINOIS**

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 TURFING PLAN 1**

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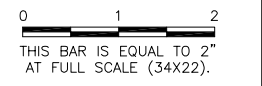
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| SHEET 72 OF 84 SHEETS                                |              |

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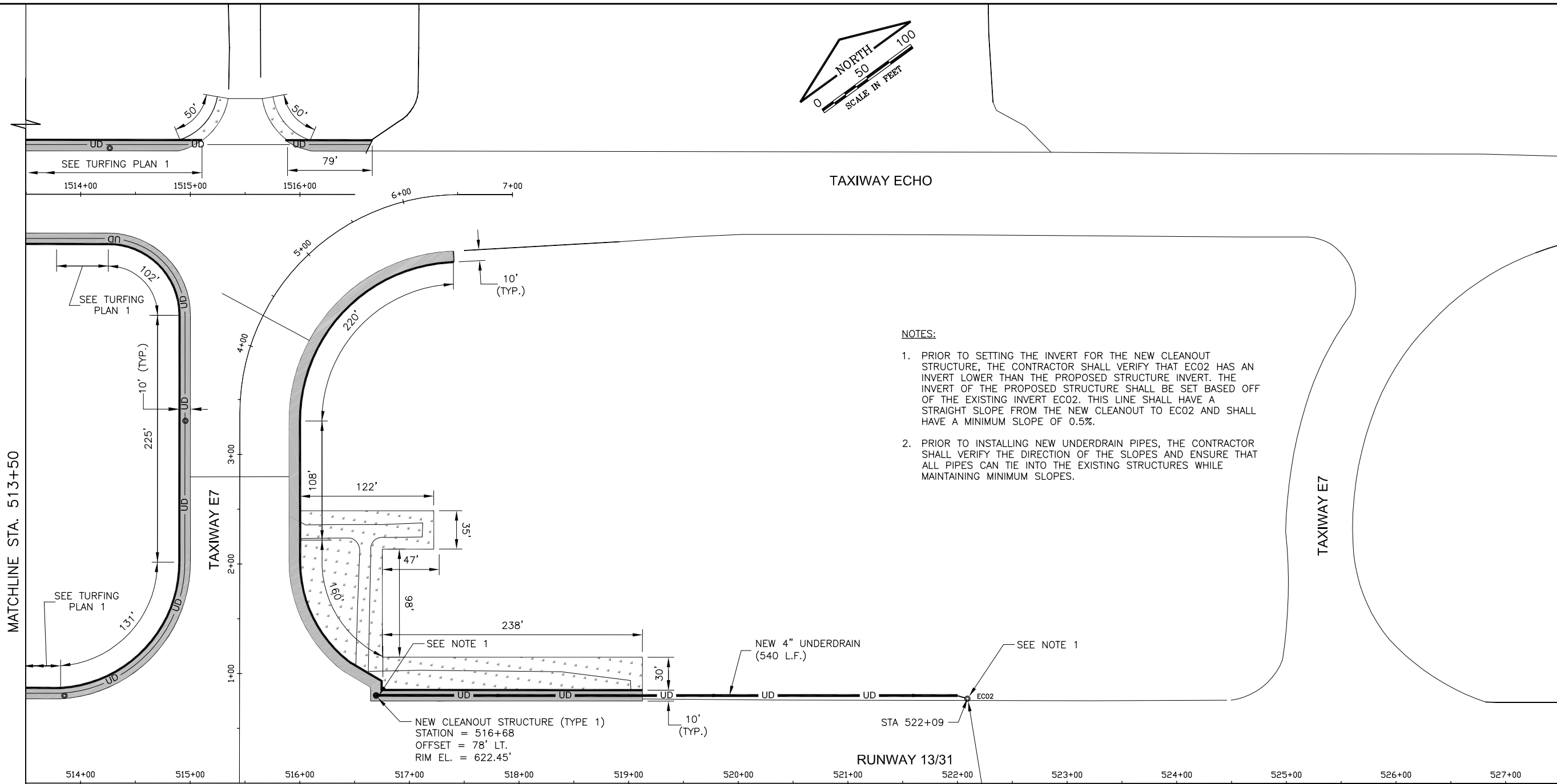
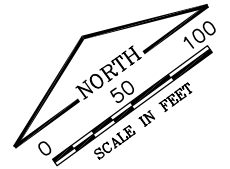
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**TURFING PLAN 2**

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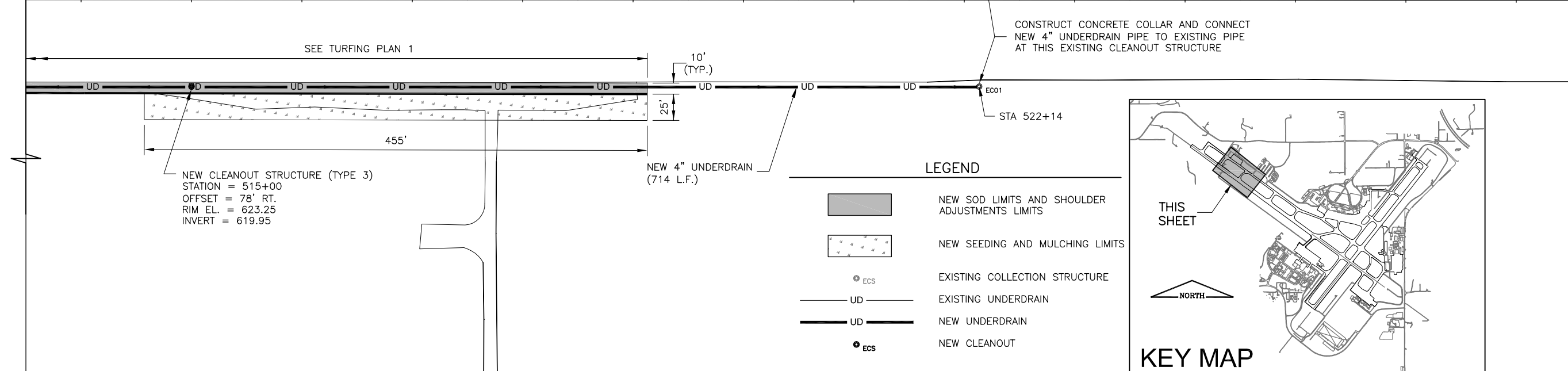
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| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 73 OF 84 SHEETS |

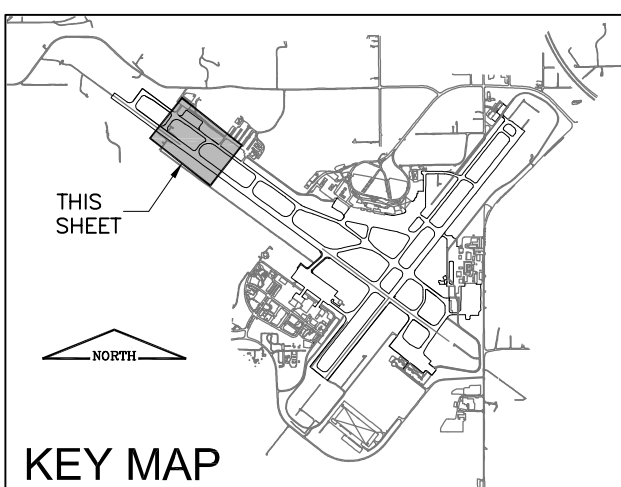


- NOTES:**
1. PRIOR TO SETTING THE INVERT FOR THE NEW CLEANOUT STRUCTURE, THE CONTRACTOR SHALL VERIFY THAT EC02 HAS AN INVERT LOWER THAN THE PROPOSED STRUCTURE INVERT. THE INVERT OF THE PROPOSED STRUCTURE SHALL BE SET BASED OFF OF THE EXISTING INVERT EC02. THIS LINE SHALL HAVE A STRAIGHT SLOPE FROM THE NEW CLEANOUT TO EC02 AND SHALL HAVE A MINIMUM SLOPE OF 0.5%.
  2. PRIOR TO INSTALLING NEW UNDERDRAIN PIPES, THE CONTRACTOR SHALL VERIFY THE DIRECTION OF THE SLOPES AND ENSURE THAT ALL PIPES CAN TIE INTO THE EXISTING STRUCTURES WHILE MAINTAINING MINIMUM SLOPES.



**LEGEND**

|  |  |
|--|--|
|  | NEW SOD LIMITS AND SHOULDER ADJUSTMENTS LIMITS |
|  | NEW SEEDING AND MULCHING LIMITS                |
|  | EXISTING COLLECTION STRUCTURE                  |
|  | EXISTING UNDERDRAIN                            |
|  | NEW UNDERDRAIN                                 |
|  | NEW CLEANOUT                                   |



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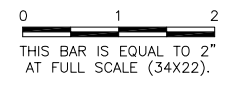
TAXIWAY E7

**RUNWAY 13/31**

**KEY MAP**

**PE092**

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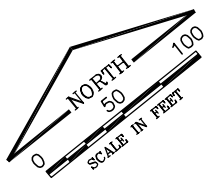
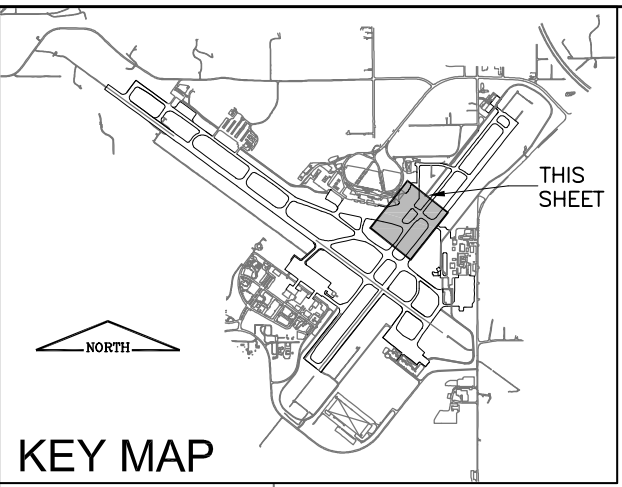
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 TURFING PLAN 3**

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| SHEET 74 OF 84 SHEETS                               |              |



TAXIWAY A

TAXIWAY A4

RUNWAY 4/22

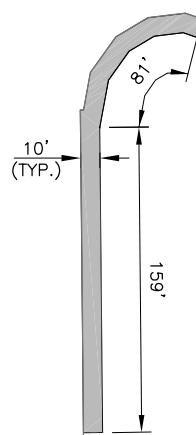
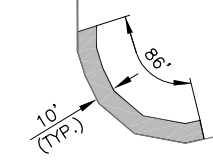
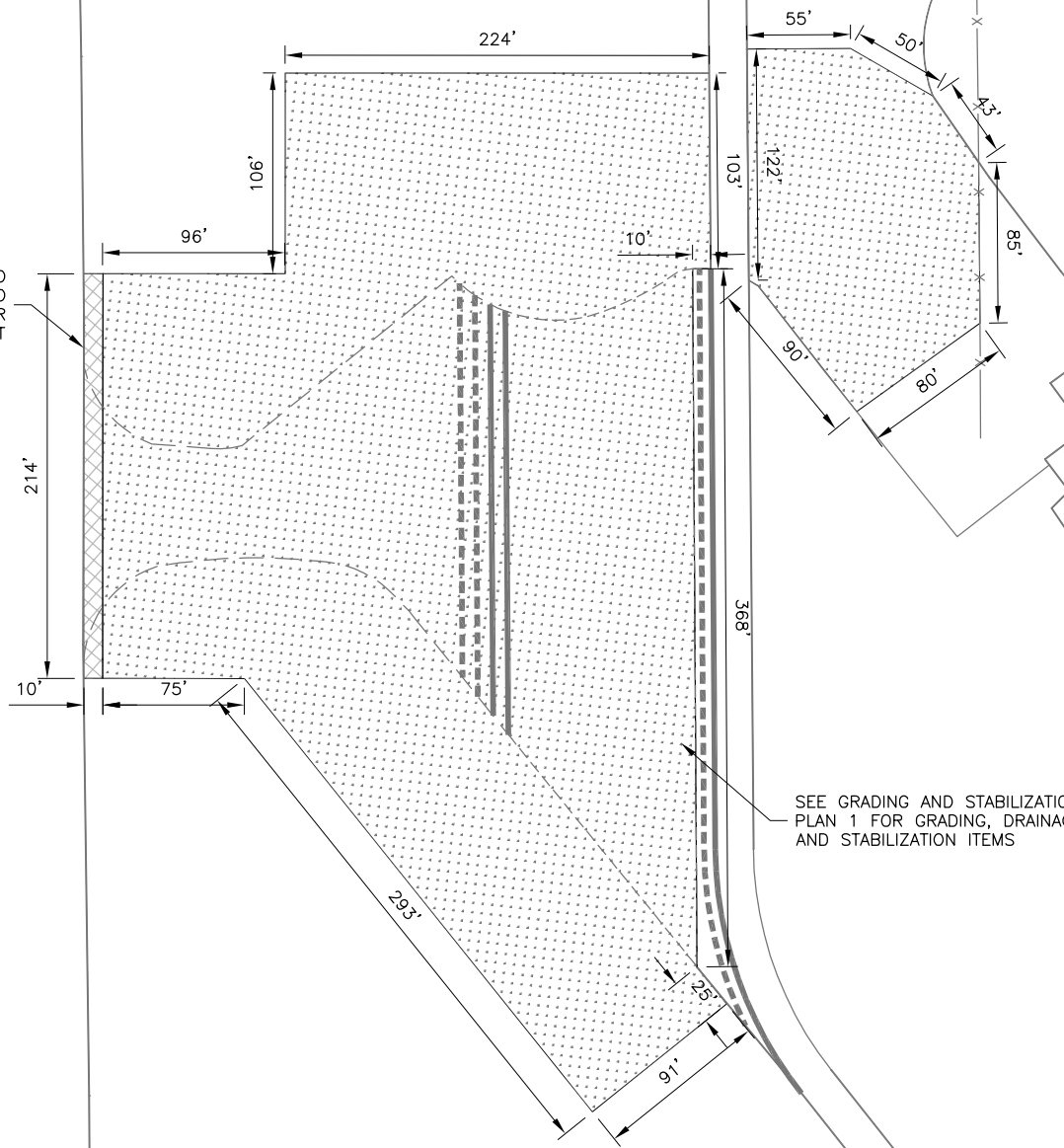
TAXIWAY P

GRADING ALONG RUNWAY TO BE CONSIDERED INCIDENTAL TO THE PAVEMENT REMOVAL AND SHALL NOT BE MEASURED AS SHOULDER ADJUSTMENT

SEE GRADING AND STABILIZATION PLAN 1 FOR GRADING, DRAINAGE AND STABILIZATION ITEMS

**LEGEND**

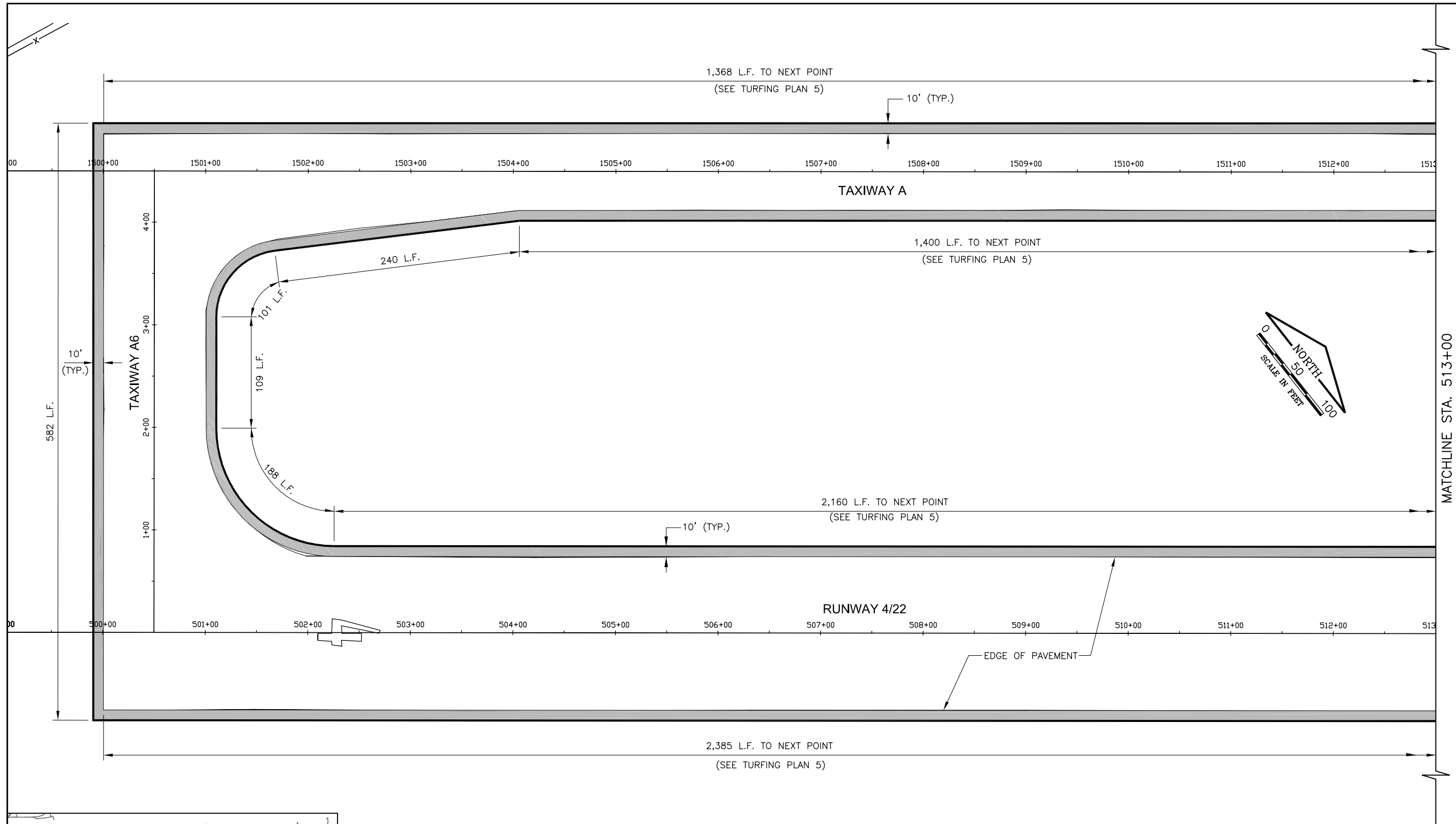
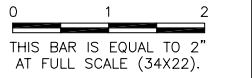
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|--|---|
|  | NEW SOD AND SHOULDER ADJUSTMENTS LIMITS   |
|  | NEW SOD AND GRADING LIMITS                |
|  | NEW SEEDING, MULCHING, AND GRADING LIMITS |
|  | EXISTING PAVEMENT TO BE REMOVED           |



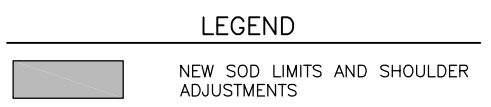
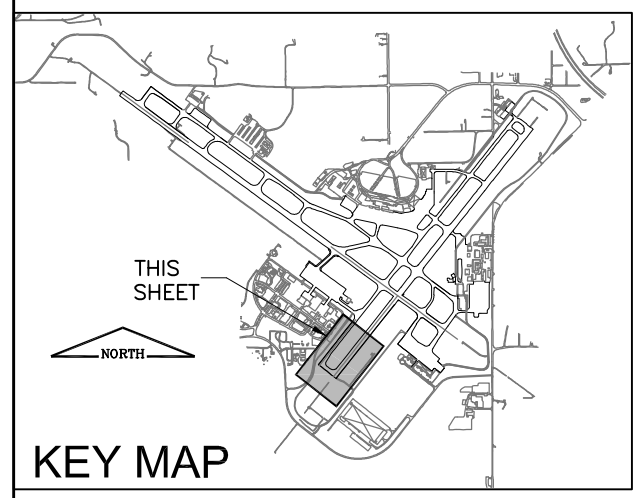


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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 TURFING PLAN 4**

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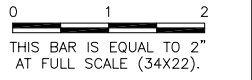
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| AIP PROJ. NO. 3-17-0080-XX |              |
| SHEET 75 OF 84 SHEETS      |              |

**PE092**

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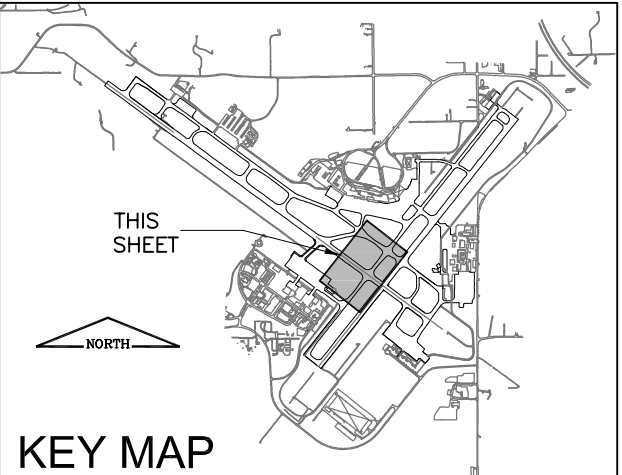
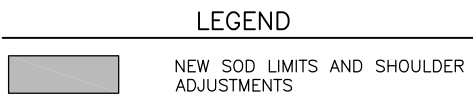
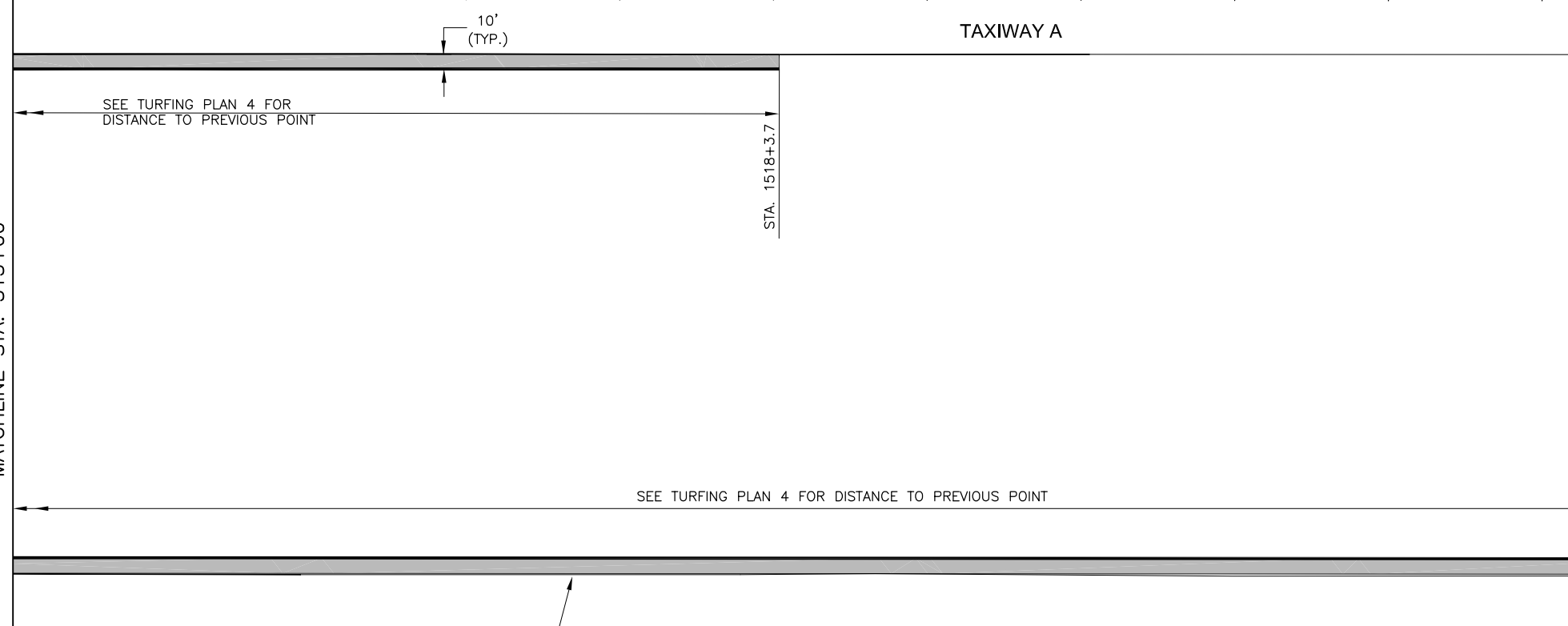
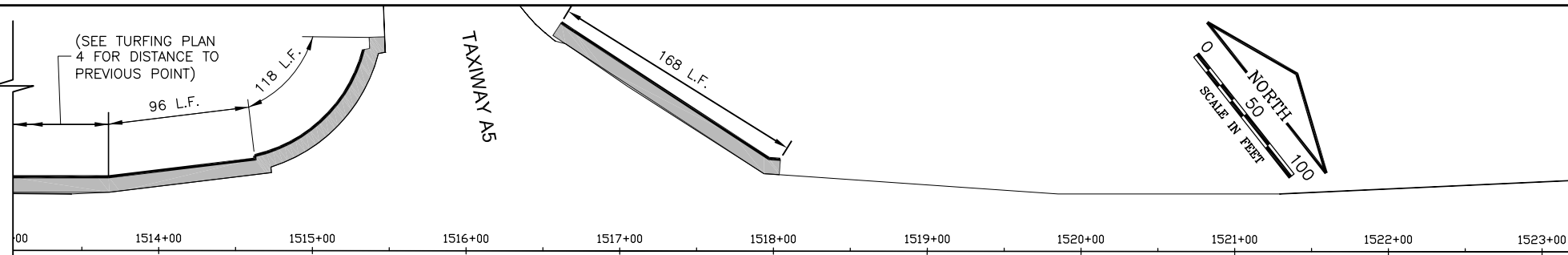
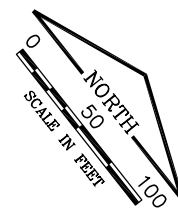
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**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 TURFING PLAN 5**

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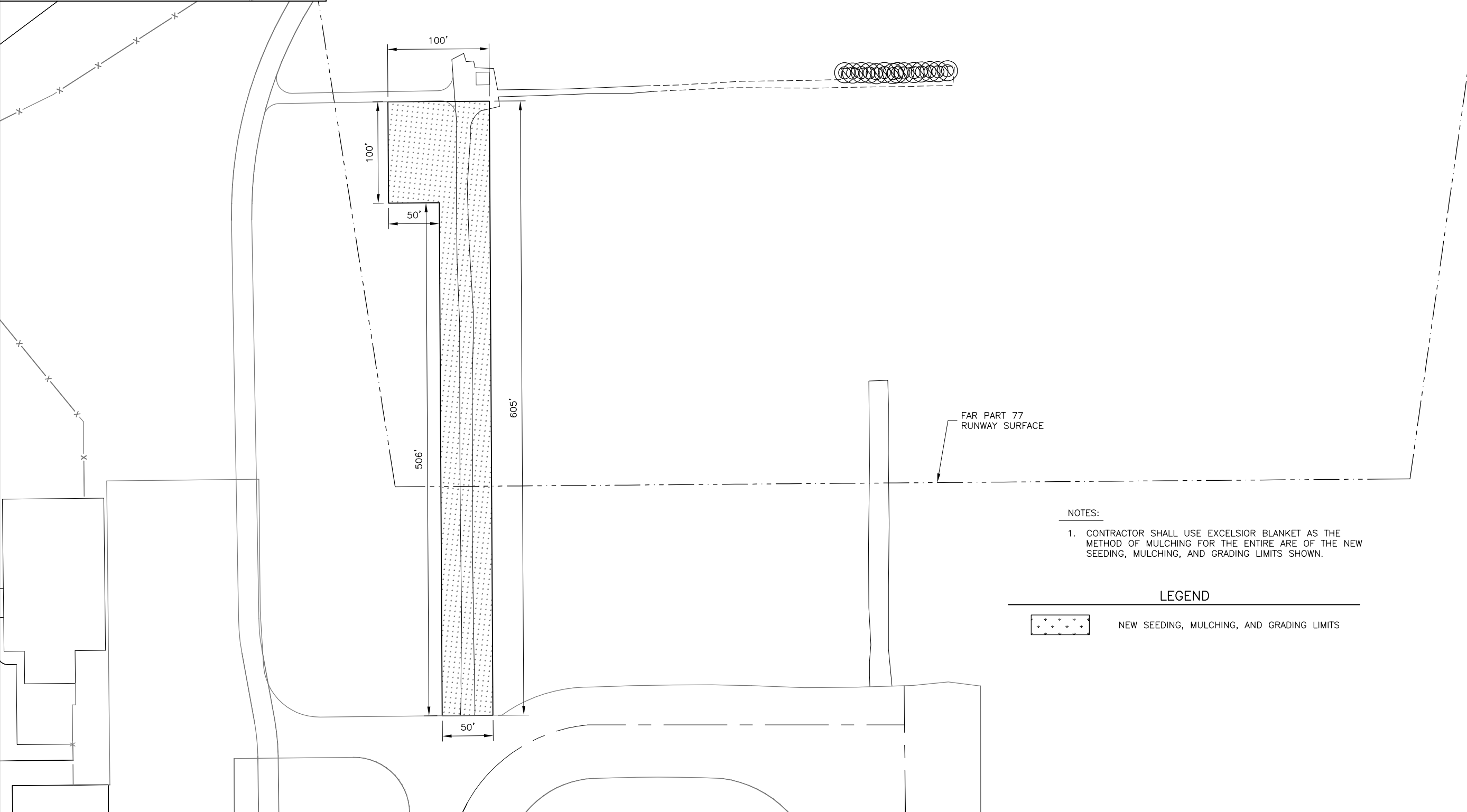
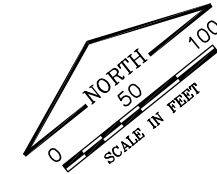
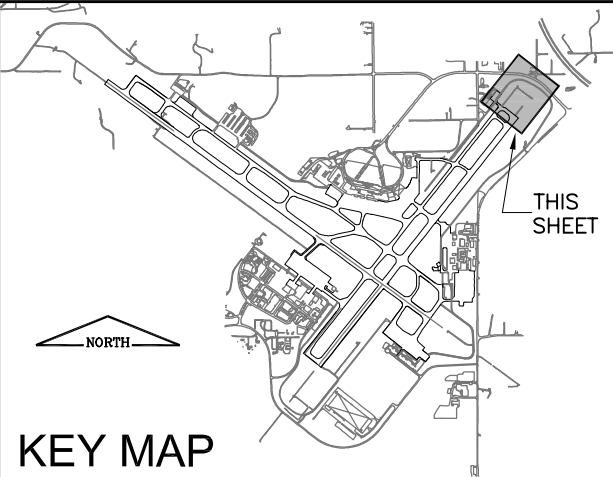
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| SHEET         | 76 OF 84 SHEETS |



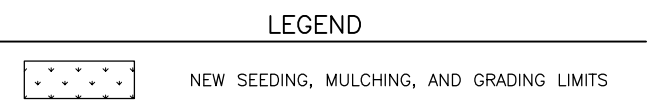
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0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).



- NOTES:**
- CONTRACTOR SHALL USE EXCELSIOR BLANKET AS THE METHOD OF MULCHING FOR THE ENTIRE ARE OF THE NEW SEEDING, MULCHING, AND GRADING LIMITS SHOWN.



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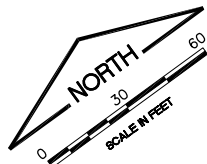
**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 TURFING PLAN 6**

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| AIP PROJ. NO. 3-17-0080-XX |              |
| SHEET 77 OF 84 SHEETS      |              |



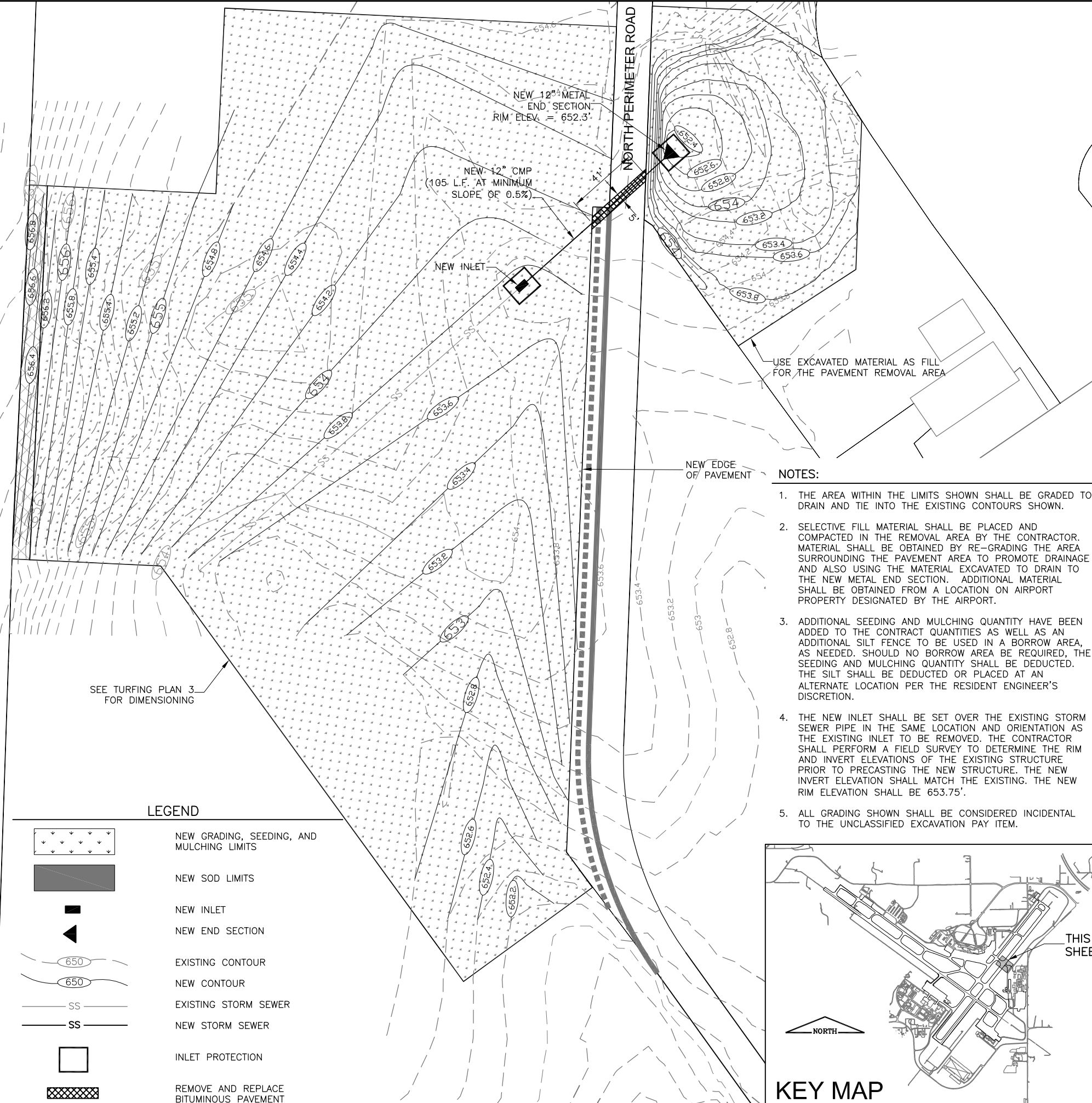
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TAXIWAY A4

RUNWAY 4/22






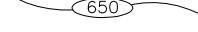

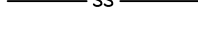




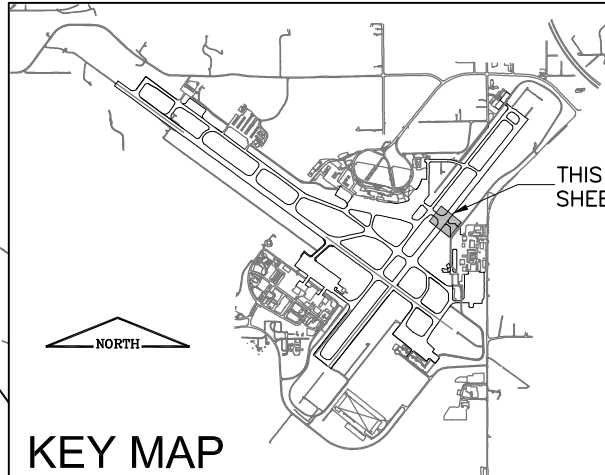
**NOTES:**

1. THE AREA WITHIN THE LIMITS SHOWN SHALL BE GRADED TO DRAIN AND TIE INTO THE EXISTING CONTOURS SHOWN.
2. SELECTIVE FILL MATERIAL SHALL BE PLACED AND COMPACTED IN THE REMOVAL AREA BY THE CONTRACTOR. MATERIAL SHALL BE OBTAINED BY RE-GRADING THE AREA SURROUNDING THE PAVEMENT AREA TO PROMOTE DRAINAGE AND ALSO USING THE MATERIAL EXCAVATED TO DRAIN TO THE NEW METAL END SECTION. ADDITIONAL MATERIAL SHALL BE OBTAINED FROM A LOCATION ON AIRPORT PROPERTY DESIGNATED BY THE AIRPORT.
3. ADDITIONAL SEEDING AND MULCHING QUANTITY HAVE BEEN ADDED TO THE CONTRACT QUANTITIES AS WELL AS AN ADDITIONAL SILT FENCE TO BE USED IN A BORROW AREA, AS NEEDED. SHOULD NO BORROW AREA BE REQUIRED, THE SEEDING AND MULCHING QUANTITY SHALL BE DEDUCTED. THE SILT SHALL BE DEDUCTED OR PLACED AT AN ALTERNATE LOCATION PER THE RESIDENT ENGINEER'S DISCRETION.
4. THE NEW INLET SHALL BE SET OVER THE EXISTING STORM SEWER PIPE IN THE SAME LOCATION AND ORIENTATION AS THE EXISTING INLET TO BE REMOVED. THE CONTRACTOR SHALL PERFORM A FIELD SURVEY TO DETERMINE THE RIM AND INVERT ELEVATIONS OF THE EXISTING STRUCTURE PRIOR TO PRECASTING THE NEW STRUCTURE. THE NEW INVERT ELEVATION SHALL MATCH THE EXISTING. THE NEW RIM ELEVATION SHALL BE 653.75'.
5. ALL GRADING SHOWN SHALL BE CONSIDERED INCIDENTAL TO THE UNCLASSIFIED EXCAVATION PAY ITEM.

SEE TURFING PLAN 3 FOR DIMENSIONING

**LEGEND**

-  NEW GRADING, SEEDING, AND MULCHING LIMITS
-  NEW SOD LIMITS
-  NEW INLET
-  NEW END SECTION
-  EXISTING CONTOUR
-  NEW CONTOUR
-  EXISTING STORM SEWER
-  NEW STORM SEWER
-  INLET PROTECTION
-  REMOVE AND REPLACE BITUMINOUS PAVEMENT



**KEY MAP**

**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**GRADING AND STABILIZATION PLAN 1**

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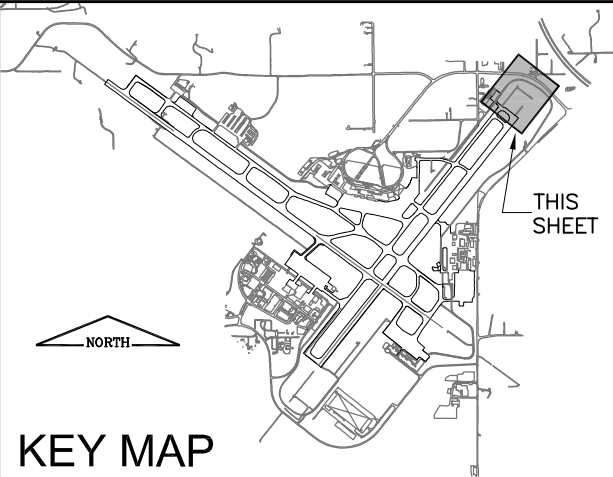
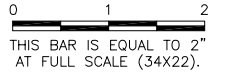
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**PEORIA INTERNATIONAL AIRPORT**

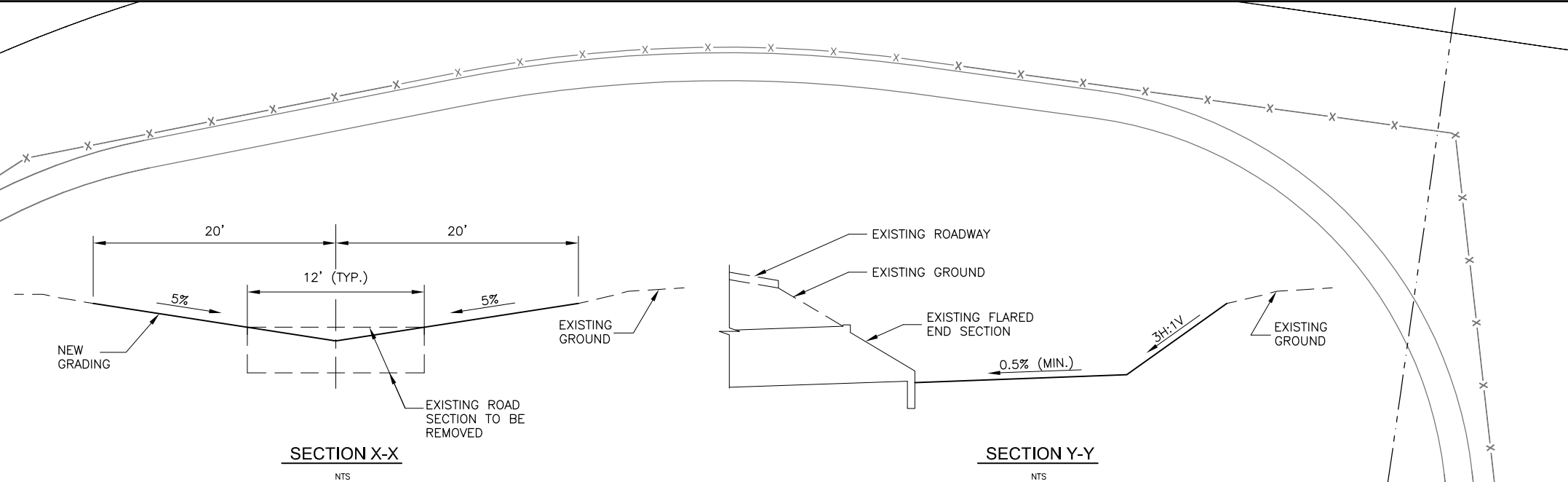
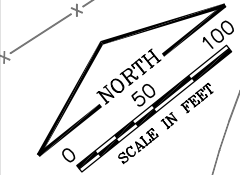
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| DATE:                      | MAY 13, 2011 |
| JOB No:                    | 100610300    |
| IL PROJ. NO. PIA-4079      |              |
| AIP PROJ. NO. 3-17-0080-XX |              |
| SHEET 78 OF 84 SHEETS      |              |

**PE092**

| REVISIONS |    |      |
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**KEY MAP**

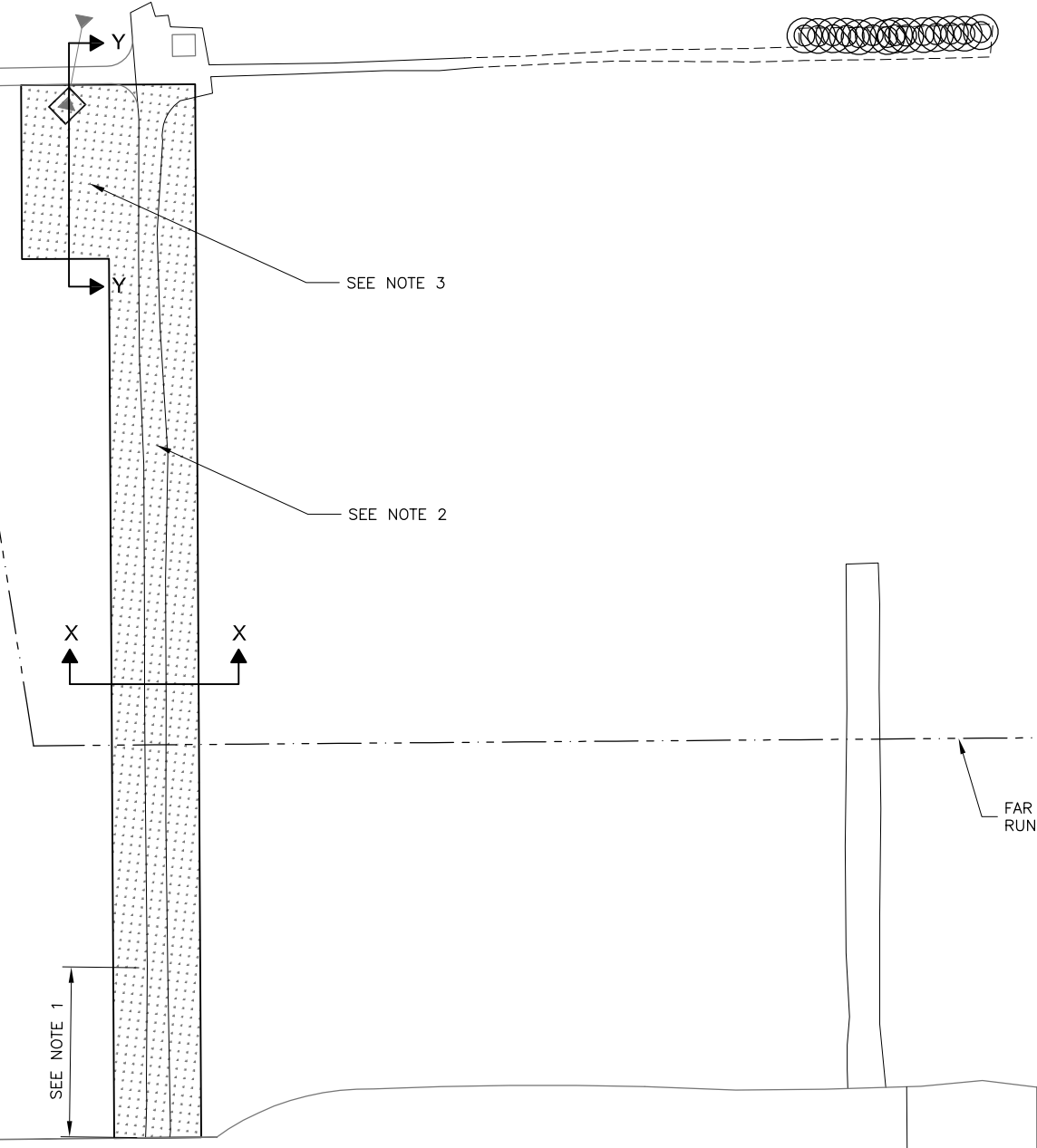


**SECTION X-X**  
NTS

**SECTION Y-Y**  
NTS

**NOTES:**

1. THE TAXIWAY SAFETY AREA (86' FROM THE CENTERLINE OF TAXIWAY A1) SHALL BE GRADED TO DRAIN AWAY FROM THE TAXIWAY PAVEMENT AT 1.5% - 3%. GRADING SHALL ALSO HAVE A 1.5" DROP FROM THE EDGE PAVEMENT TO THE NEW TURF. SEE EDGE OF PAVEMENT DETAIL ON SHEET TYPICAL SECTIONS 3.
2. NEW SWALE PROFILE SHALL BE GRADED TO DRAIN AT A MINIMUM SLOPE OF 0.5%. THE NEW SLOPE SHALL DRAIN TO THE EXISTING FLARED END SECTION WEST OF THE LOCALIZER SHACK.
3. CONTRACTOR SHALL EXPAND THE EXISTING DETENTION AREA IN FRONT OF THE EXISTING FLARED END SECTION. THE DETENTION AREA SHALL HAVE A MINIMUM SLOPE OF 0.5% IN ANY DIRECTION TOWARDS THE EXISTING FLARED END SECTION. MAXIMUM SLOPES AT THE EDGES OF THE DETENTION AREA SHALL BE 3:1 SO AS TO MAINTAIN A MOWABLE AREA. THE OVERALL DIMENSIONS OF THE DETENTION AREA SHALL BE THE 100' BY 100' AREA AS DIMENSIONED ON TURFING PLAN 6 FOR THE GRADING AND TURFING LIMITS.



SEE NOTE 3

SEE NOTE 2

SEE NOTE 1

FAR PART 77  
RUNWAY SURFACE

**LEGEND**

- NEW GRADING, SEEDING, AND MULCHING LIMITS
- INLET PROTECTION
- EXISTING STORM SEWER

**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**GRADING AND STABILIZATION PLAN 2**

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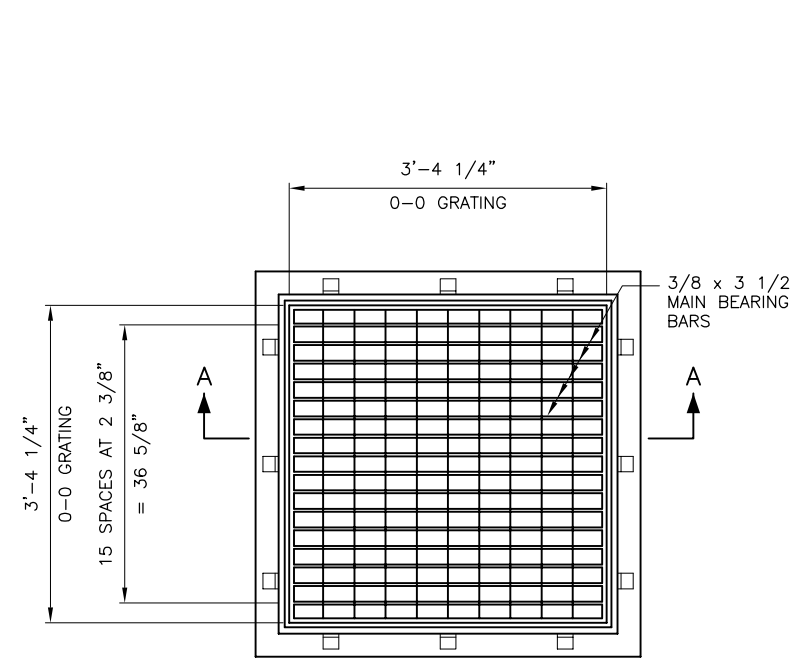
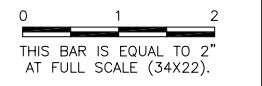


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| IL PROJ. NO. PIA-4079      |              |
| AIP PROJ. NO. 3-17-0080-XX |              |
| SHEET 79 OF 84 SHEETS      |              |

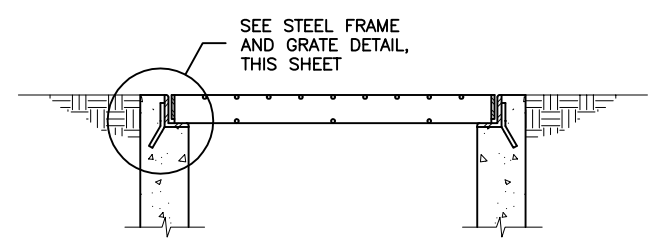


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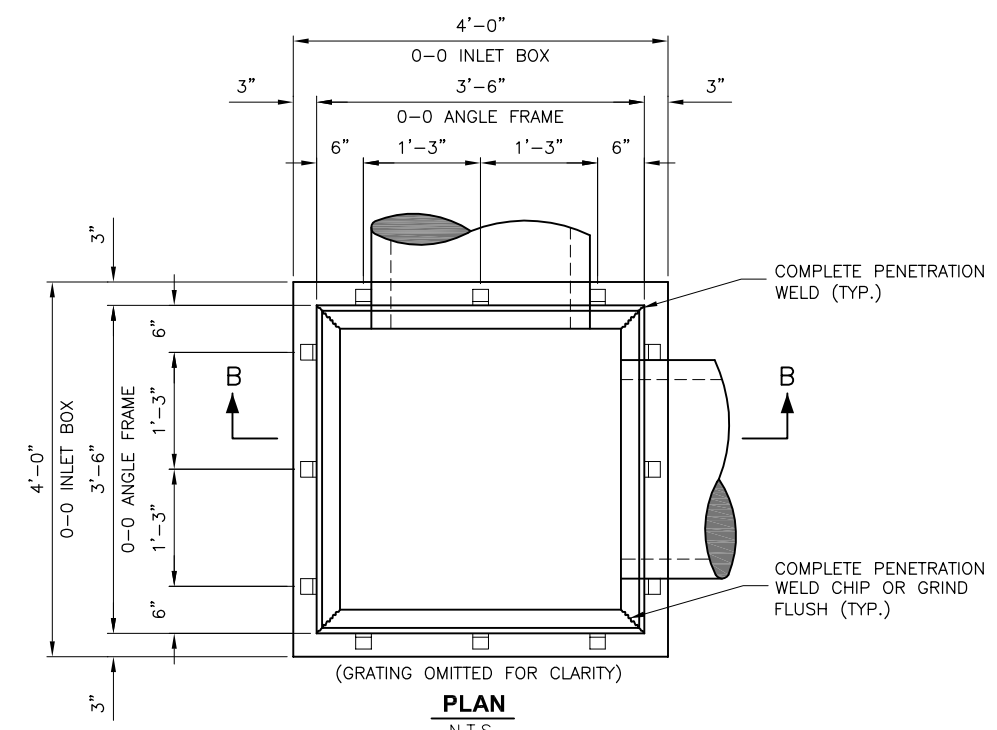
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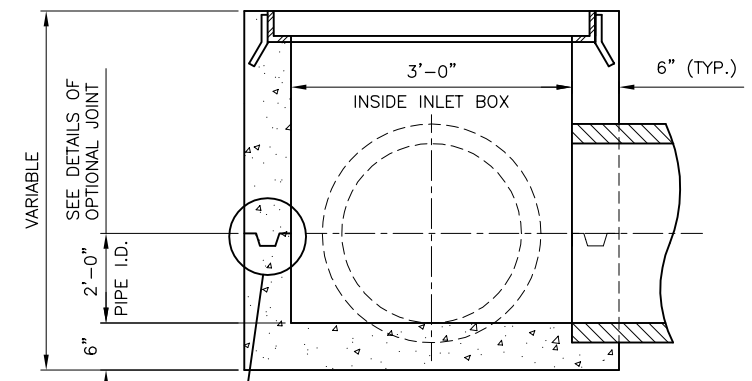
**PLAN**  
N.T.S.



**SECTION A-A**  
N.T.S.

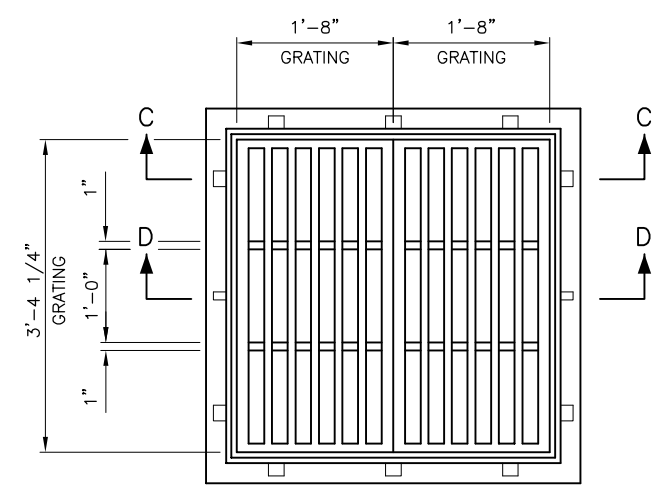


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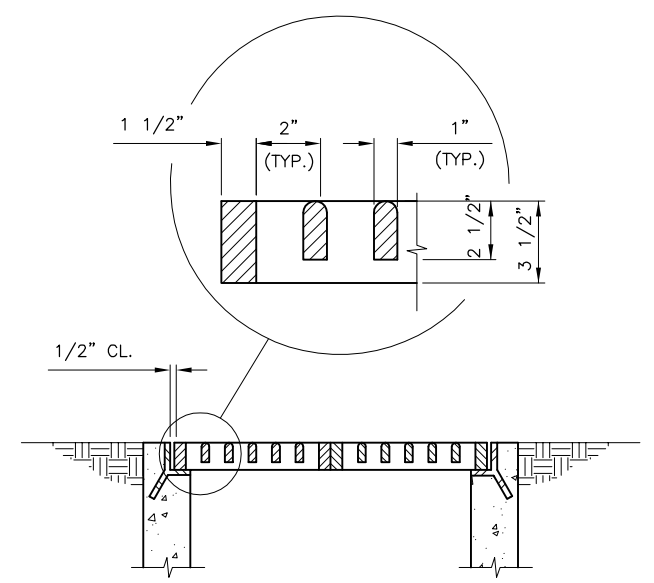


**SECTION B-B**  
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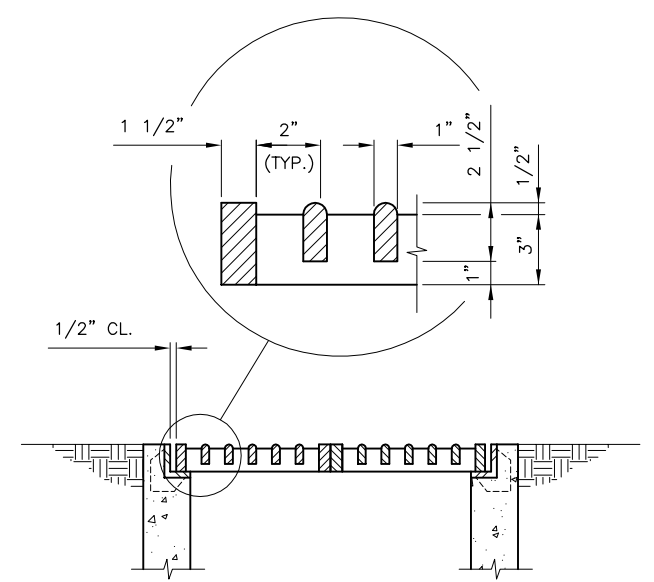
SEE OPTIONAL JOINT DETAIL, THIS SHEET



**PLAN**  
N.T.S.

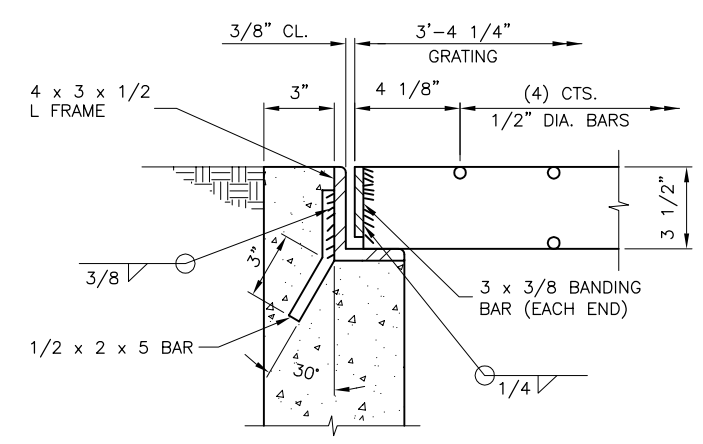


**SECTION C-C**  
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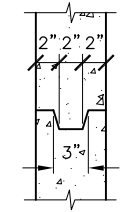


**SECTION D-D**

**CAST FRAME & GRATE DETAILS**  
N.T.S.



**STEEL FRAME & GRATE DETAIL**  
N.T.S.



**OPTIONAL JOINT DETAIL**  
N.T.S.

**INLET NOTE**  
 INLET IS AN IDOT STD. 542546, FLUSH INLET BOX FOR MEDIAN. FRAME & GRATES ARE NEENAH R-3807 OR APPROVED EQUAL.

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 PEORIA, ILLINOIS

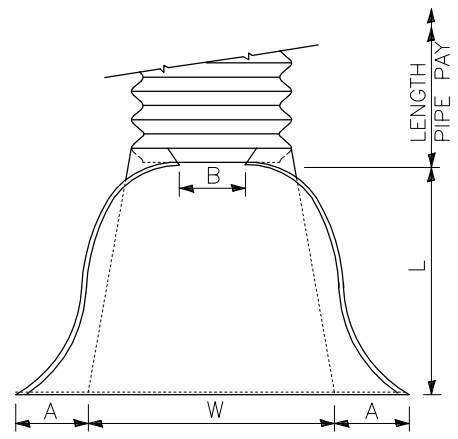
REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
**INLET DETAILS**

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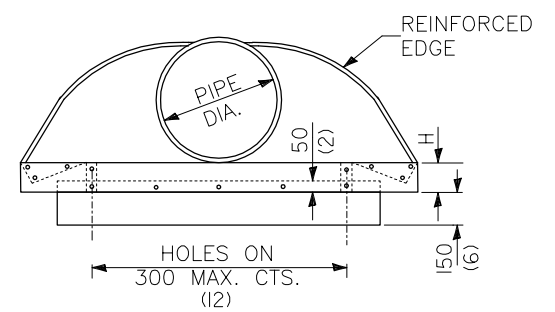
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| JOB No:       | 100610300       |
| IL PROJ. NO.  | PIA-4079        |
| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 80 OF 84 SHEETS |

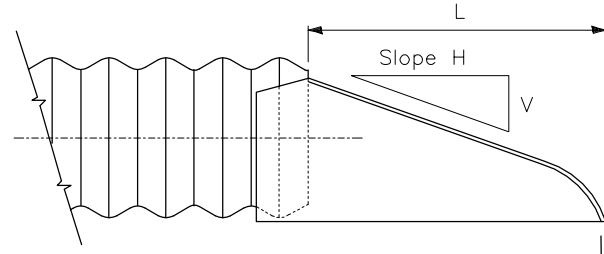
| PIPE DIA. | THICKNESS    | DIMENSIONS |               |          |               |                | SLOPE (Approx.) (V:H) | BODY  |
|-----------|--------------|------------|---------------|----------|---------------|----------------|-----------------------|-------|
|           |              | A          | B             | H        | L             | W              |                       |       |
| 300 (12)  | 1.63 (0.064) | 150 (6)    | 150 (6)       | 150 (6)  | 535 (21)      | 610 (24)       | 1:2-1/2               | 1 Pc. |
| 375 (15)  | 1.63 (0.064) | 180 (7)    | 205 (8)       | 150 (6)  | 660 (26)      | 760 (30)       | 1:2-1/2               | 1 Pc. |
| 450 (18)  | 1.63 (0.064) | 205 (8)    | 255 (10)      | 150 (6)  | 785 (31)      | 915 (36)       | 1:2-1/2               | 1 Pc. |
| 525 (21)  | 1.63 (0.064) | 230 (9)    | 305 (12)      | 150 (6)  | 915 (36)      | 1,065 m (42")  | 1:2-1/2               | 1 Pc. |
| 600 (24)  | 1.63 (0.064) | 255 (10)   | 330 (13)      | 150 (6)  | 1,040 m (41") | 1,220 m (48")  | 1:2-1/2               | 1 Pc. |
| 750 (30)  | 2.01 (0.079) | 305 (12)   | 405 (16)      | 205 (8)  | 1,295 m (51") | 1,525 m (60")  | 1:2-1/2               | 1 Pc. |
| 900 (36)  | 2.01 (0.079) | 355 (14)   | 480 (19)      | 230 (9)  | 1,525 m (60") | 1,830 m (72")  | 1:2-1/2               | 2 Pc. |
| 1050 (42) | 2.77 (0.109) | 405 (16)   | 560 (22)      | 280 (11) | 1,750 m (69") | 2,135 m (84")  | 1:2-1/2               | 2 Pc. |
| 1200 (48) | 2.77 (0.109) | 455 (18)   | 685 (27)      | 305 (12) | 1,980 m (78") | 2,285 m (90")  | 1:2-1/4               | 2 Pc. |
| 1350 (54) | 2.77 (0.109) | 455 (18)   | 760 (30)      | 305 (12) | 2,135 m (84") | 2,590 m (102") | 1:2                   | 2 Pc. |
| 1500 (60) | 2.77 (0.109) | 455 (18)   | 840 (33)      | 305 (12) | 2,210 m (87") | 2,895 m (114") | 1:1-3/4               | 3 Pc. |
| 1650 (66) | 2.77 (0.109) | 455 (18)   | 915 (36)      | 305 (12) | 2,210 m (87") | 3,050 m (120") | 1:1-1/2               | 3 Pc. |
| 1800 (72) | 2.77 (0.109) | 455 (18)   | 990 (39)      | 305 (12) | 2,210 m (87") | 3,200 m (126") | 1:1-1/3               | 3 Pc. |
| 1950 (78) | 2.77 (0.109) | 455 (18)   | 1,065 m (42") | 305 (12) | 2,210 m (87") | 3,355 m (132") | 1:1-1/4               | 3 Pc. |
| 2250 (84) | 2.77 (0.109) | 455 (18)   | 1,145 m (45") | 305 (12) | 2,210 m (87") | 3,505 m (138") | 1:1-1/8               | 3 Pc. |



PLAN



END VIEW

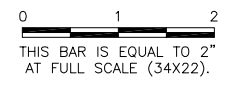


SIDE VIEW

END SECTION

PE092

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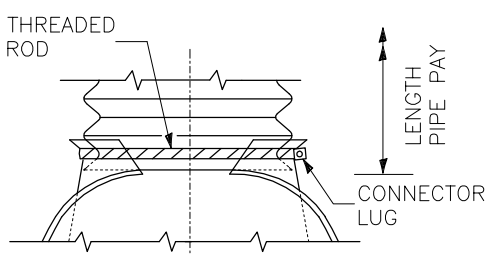
NOTES

FOR 1500 MM (60") THRU 2250 MM (84") SIZES, REINFORCED EDGES SHALL BE SUPPLEMENTED WITH STIFFENER ANGLES. THE ANGLES SHALL BE 51X51X6.4 MM (2X2X1/4") FOR 1500 MM (60") THRU 1800 MM (72") DIAMETER AND 64X64X6.4 MM (2-1/2X2-1/2X1/4") FOR 1950 MM (78") THRU 2250 MM (84") DIAMETER. THE ANGLES SHALL BE ATTACHED BY M10 (3/8") RIVETS OR BOLTS.

ALL SLOPE RATIOS ARE EXPRESSED AS UNITS OF VERTICAL DISPLACEMENT TO UNITS OF HORIZONTAL DISPLACEMENT (V:H).

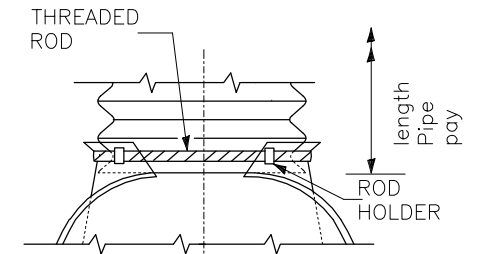
NOTES

1. TYPES 1 AND 2 FOR PIPES WITH ANNULAR ENDS ONLY.
2. TYPE 3 CONNECTION CAN BE USED FOR ALL PIPE SIZES AND INCLUDES 300 MM (12") OF THE PIPE LENGTH. THE CONNECTOR SECTION SHALL BE ATTACHED TO THE END SECTION BY RIVETS OR BOLTS AND SHALL BE THE SAME METAL THICKNESS AS THE END SECTION. STUB SHALL BE EITHER 68 MM (2-2/3") PITCH x 13 MM (1/2") DEPTH OR 75 MM (3") PITCH x 25 MM (1") DEPTH ANNULAR CORRUGATED PIPE.
3. TYPE 4 CONNECTION CAN BE USED FOR ALL PIPE SIZES. COUPLER SHALL BE 68 MM x 13 MM (2-2/3"x1/2") DIMPLE, HUGGER, OR ANNULAR BAND OF 75 MM x 25 MM (3"x1"). THE DIMPLE, HUGGER, OR ANNULAR BAND MAY BE USED WITH CORRUGATED METAL PIPES HAVING ANNULAR ENDS. FOR CORRUGATED METAL PIPES HAVING HELICAL ENDS, ONLY THE DIMPLE BAND WILL BE ALLOWED.
4. ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.



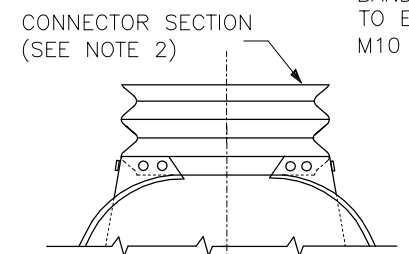
TYPE 1

For 300 (12) THRU 600 (24) only (See Note 1)



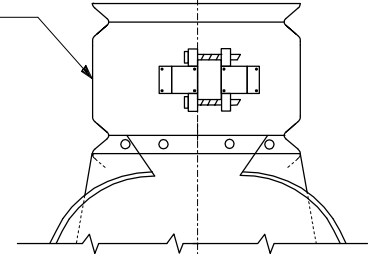
TYPE 2

For 750 (30) and 900 (36) ONLY (SEE NOTE 1)



TYPE 3

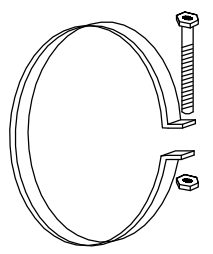
(SEE NOTE 2)



TYPE 4

(SEE NOTE 3)

CONNECTIONS OF END SECTIONS



29 (1) WIDE, 2.77 (0.109) THICK STRAP WITH STANDARD M12 x 150 (1/2 x 6) BAND BOLT AND NUT.

ALTERNATE STRAP CONNECTOR

(FOR TYPE 1 ONLY)

METAL END SECTION FOR PIPE CULVERTS  
**STANDARD 542401**

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REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
**DRAINAGE DETAILS**

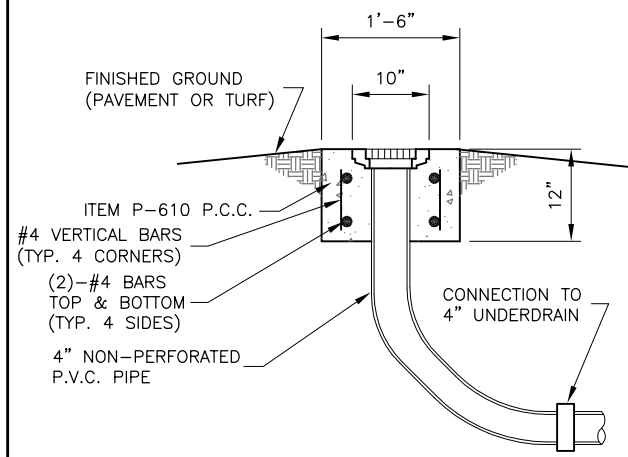
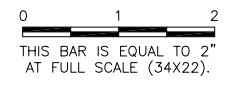
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| IL PROJ. NO. PIA-4079      |              |
| AIP PROJ. NO. 3-17-0080-XX |              |
| SHEET 81 OF 84 SHEETS      |              |

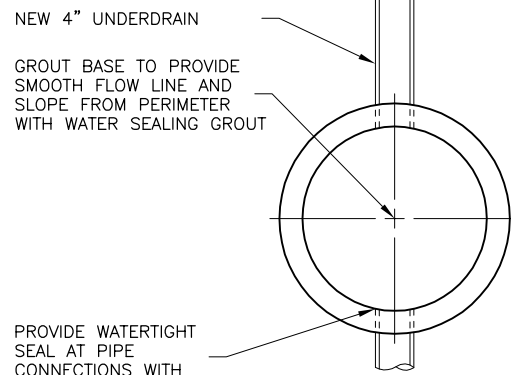
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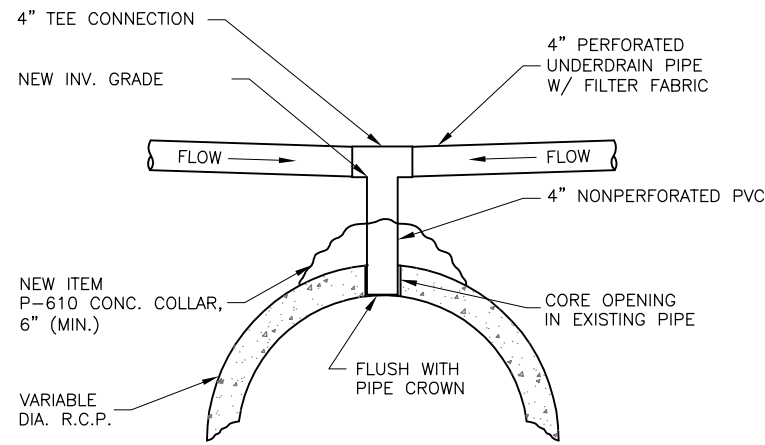


**SIDE VIEW**

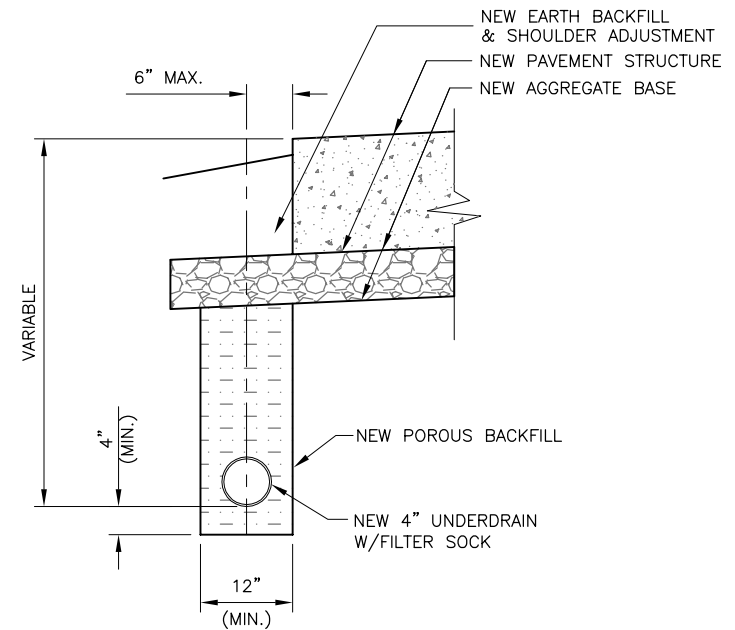
**UNDERDRAIN CLEAN-OUT DETAIL - TYPE 1**  
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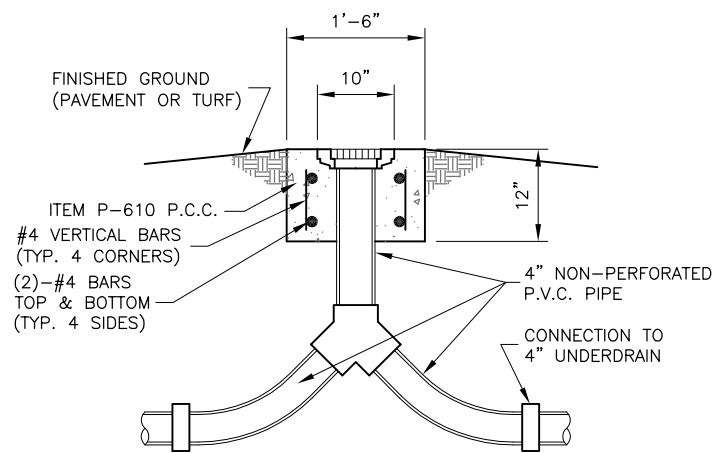
**PLAN**



**UNDERDRAIN DIRECT TOP CONNECTION DETAIL**  
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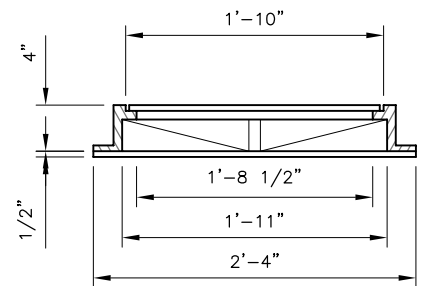


**TYPICAL UNDERDRAIN DETAIL PAVEMENT EDGE**  
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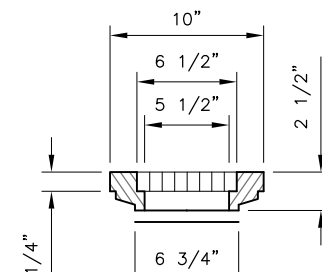


**SIDE VIEW**

**UNDERDRAIN CLEAN-OUT DETAIL - TYPE 2**  
N.T.S.



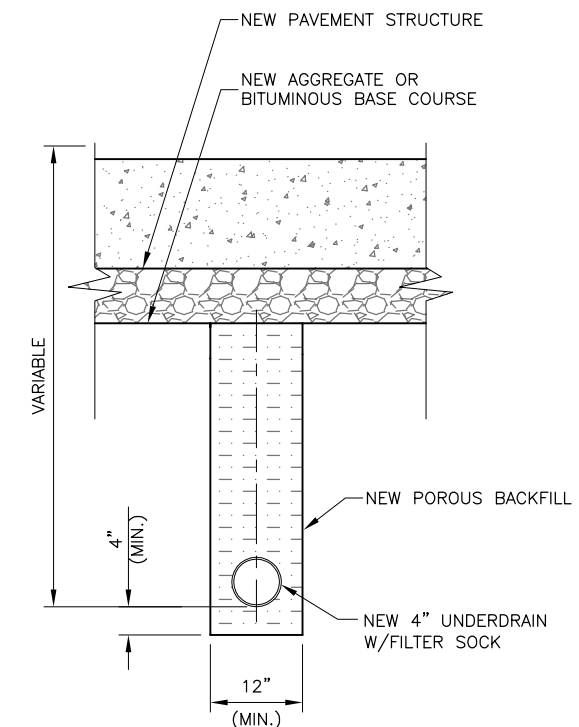
**FRAME AND LID**



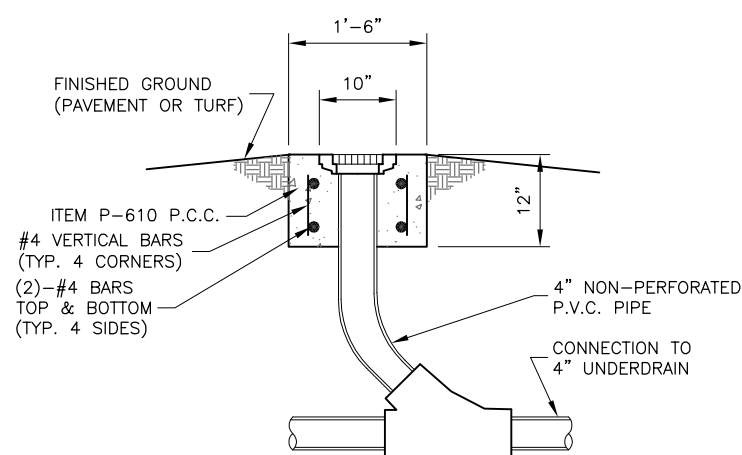
**FRAME AND COVER**

CAST IRON FRAME & LID (SIMILAR TO NEENAH R-1690-A HEAVY DUTY OR APPROVED EQUAL)

CAST IRON FRAME & COVER (SIMILAR TO NEENAH R-6013 OR APPROVED EQUAL)

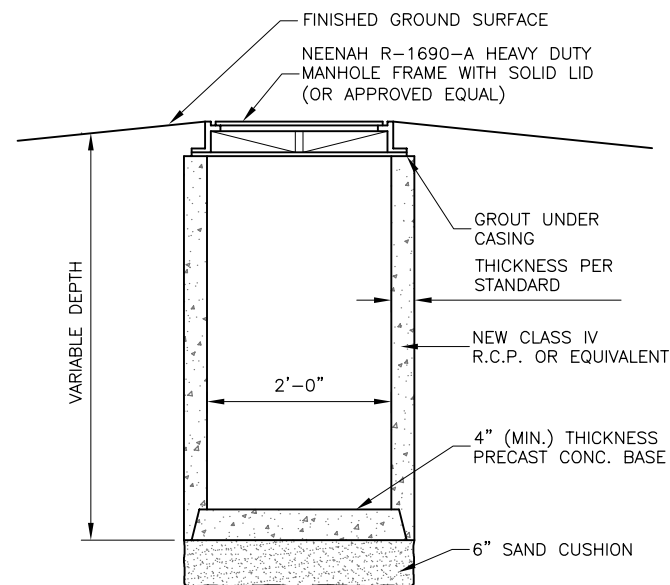


**TYPICAL UNDERDRAIN DETAIL BELOW PAVEMENT**  
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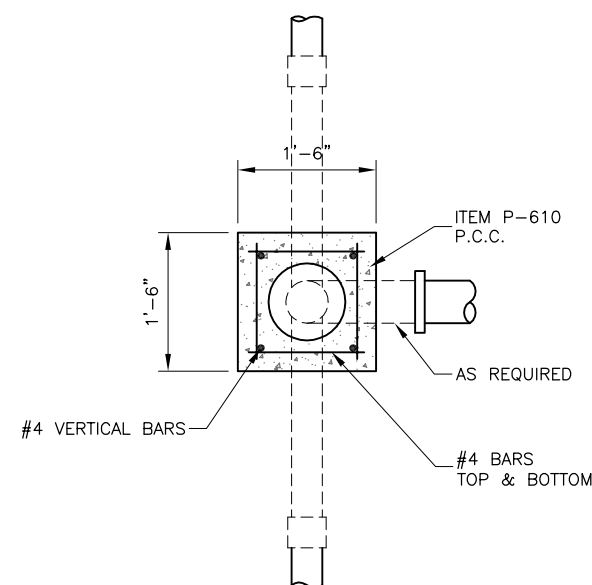
**SIDE VIEW**

**UNDERDRAIN CLEAN-OUT DETAIL - TYPE 3**  
N.T.S.



**SIDE VIEW**

**UNDERDRAIN COLLECTION STRUCTURE DETAIL**  
N.T.S.



**PLAN**

**UNDERDRAIN CLEAN-OUT DETAILS**  
N.T.S.

**NOTES**

DURING UNDERDRAIN CONSTRUCTION, CARE SHALL BE TAKEN TO ENSURE GOOD DRAINAGE BETWEEN THE SUB-BASE AND THE POROUS BACKFILL MATERIAL WHICH WILL AFFECT DRAINAGE TO THE UNDERDRAIN SHALL BE REMOVED PRIOR TO THE PLACEMENT OF PROPOSED POROUS BACKFILL. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL.

**GENERAL WAYNE A. DOWNING**  
**PEORIA INTERNATIONAL AIRPORT**  
**PEORIA, ILLINOIS**

**REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY**  
**UNDERDRAIN DETAILS**

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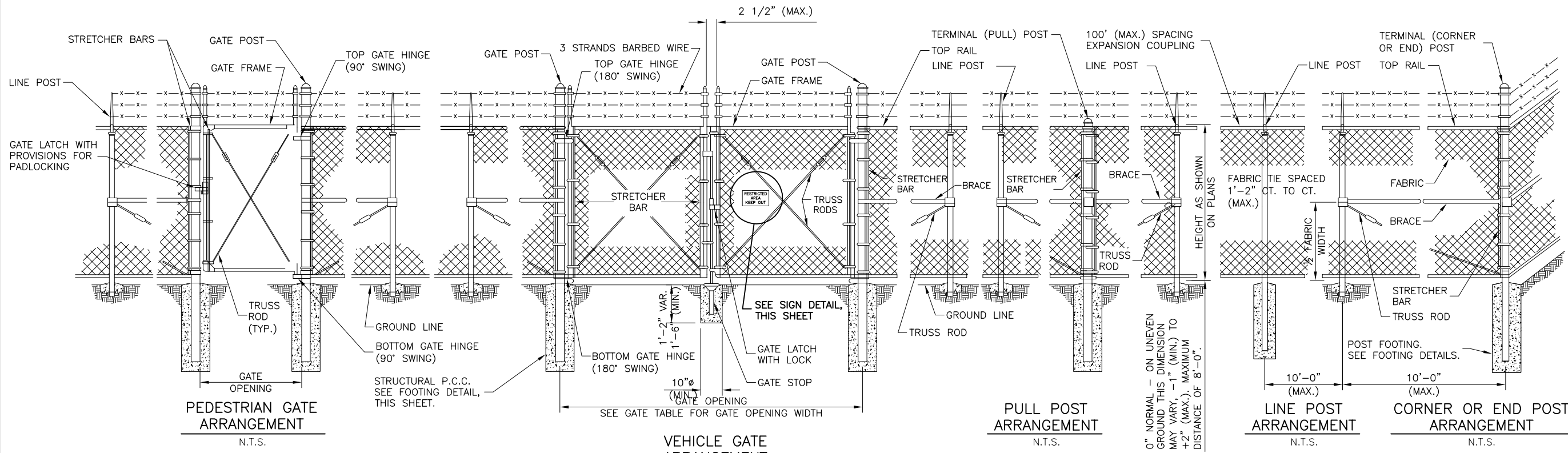
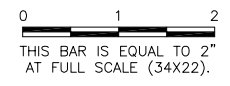
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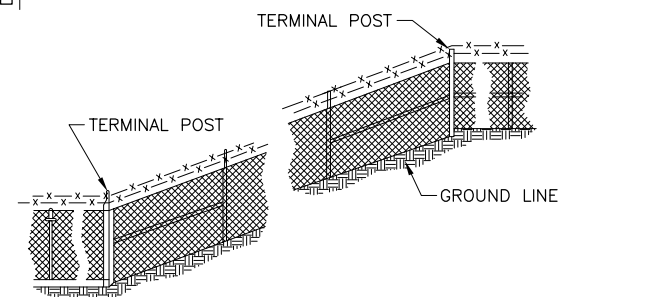
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| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 82 OF 84 SHEETS |

**PE092**

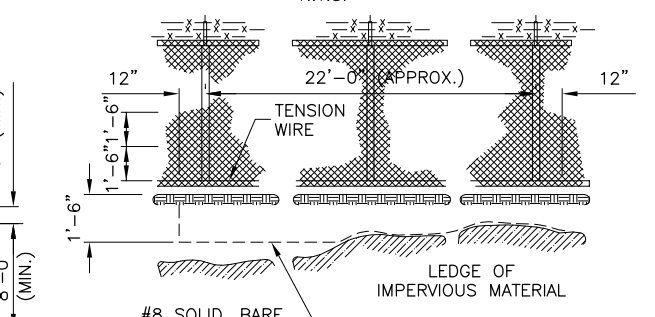
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PULL POSTS SHALL BE PLACED AT LOCATIONS DETERMINED BY THE ENGINEER. THEY SHALL BE PLACED AT 660 FT. INTERVALS BETWEEN POSTS TO WHICH THE ENDS OF THE FABRIC ARE CLAMPED OR MIDWAY BETWEEN SUCH POSTS WHEN THE DISTANCE IS LESS THAN 1320 FT. AND GREATER THAN 660 FT.

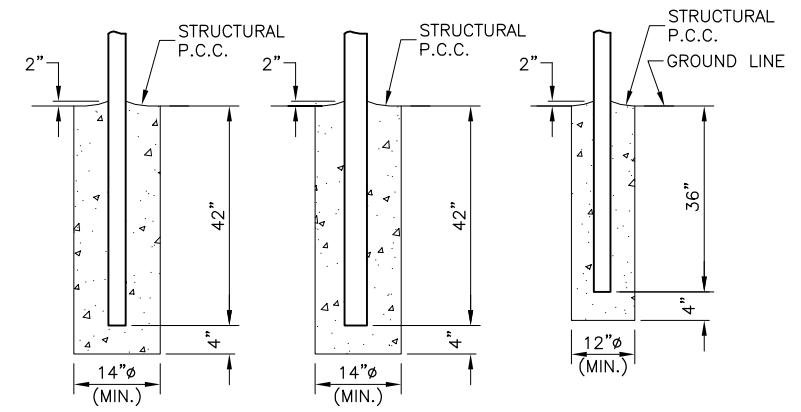


**FENCE INSTALLATION ON SLOPES**  
N.T.S.

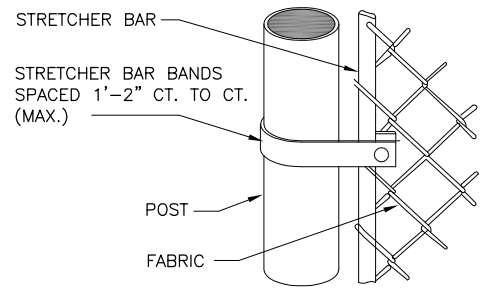


**COUNTERPOISE GROUND (ALTERNATE)**  
N.T.S.

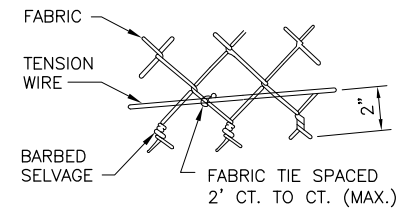
**PROTECTIVE ELECTRICAL GROUND DETAILS**  
N.T.S.



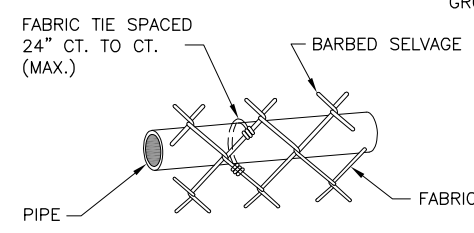
**FOOTING DETAILS**  
N.T.S.



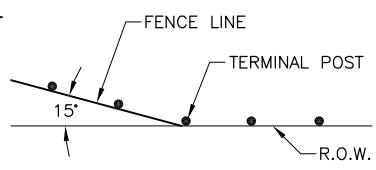
**FASTENING STRETCHER BAR TO POST**  
N.T.S.



**TYING FABRIC TO TENSION WIRE**  
N.T.S.

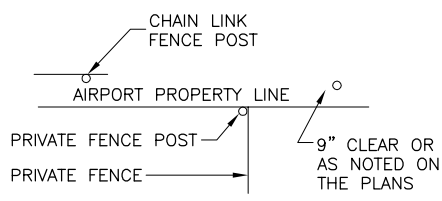


**TYING FABRIC TO PIPE**  
N.T.S.

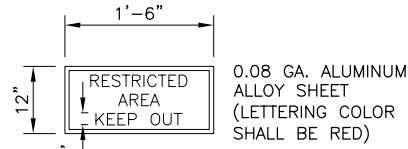


WHERE FENCE LINE HAS A CHANGE IN DIRECTION OF 15° OR MORE, A TERMINAL POST SHALL BE PLACED AS SHOWN ABOVE. WHERE ANGLE IS LESS THAN 15° AND EXISTING CONDITIONS REQUIRE A TERMINAL POST, THEY SHALL BE PLACED AS DIRECTED BY THE ENGINEER.

**INSTALLATION AT CORNERS**  
N.T.S.

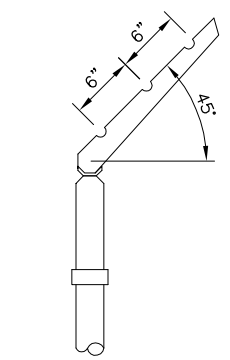


**PRIVATE FENCE TERMINAL**  
N.T.S.



EACH GATE SHALL REQUIRE TWO SIGNS. EVERY 100' OF FENCE SHALL REQUIRE ONE SIGN. EVERY STRAIGHT SECTION OF FENCE SHALL REQUIRE MINIMUM ONE SIGN.

**SIGN DETAIL**  
N.T.S.



**BARBED WIRE ARM ON LINE POST**  
N.T.S.

- NOTES**
1. CONTINUOUS FENCE SHALL BE GROUNDED AT INTERVALS NOT EXCEEDING 1000 FT. EXCEPT THERE SHALL BE A GROUND NOT EXCEEDING 100 FT. FROM GATE IN EACH SECTION OF THE FENCE ADJACENT TO THE GATE.
  2. FENCE UNDER POWER LINES SHALL BE GROUNDED BY 3 GROUNDS, ONE DIRECTLY UNDER THE CROSSING AND ONE ON EACH SIDE 25 TO 50 FT. AWAY. A SINGLE GROUND SHALL BE LOCATED DIRECTLY UNDER EACH TELEPHONE WIRE OR CABLE CROSSINGS.
  3. THE COUNTERPOISE SHALL BE USED ONLY WHERE IT IS IMPOSSIBLE TO DRIVE A GROUND ROD BECAUSE OF IMPERVIOUS EARTH STRUCTURES.
  4. THE GROUND WIRE SHALL BE CONNECTED TO THE FABRIC AND THE GROUND ROD BY A MECHANICAL CLAMP OF CAST BRONZE BODY AND BRONZE OR STAINLESS STEEL BOLTS AND WASHERS. WHEN A TENSION WIRE IS REQUIRED, THE BOTTOM CONNECTION OF THE GROUND WIRE SHALL BE MADE TO THE TENSION WIRE.
  5. ALL PROPOSED CLASS E FENCE SHALL MEET THE REQUIREMENTS OF IDOT STANDARD 664001-01.

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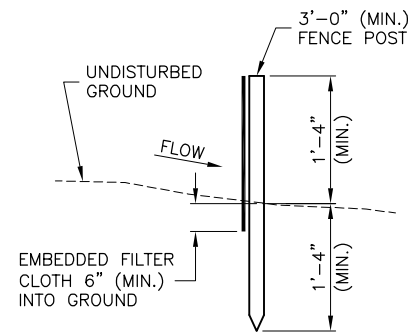
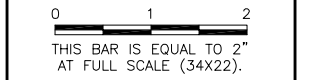
REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
**FENCE DETAILS 1**

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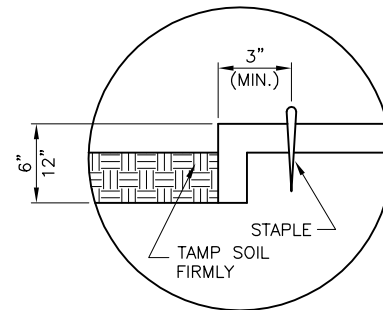
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| DRAWN BY:     | CMT             |
| CHECKED BY:   | CET             |
| APPROVED BY:  | CET             |
| DATE:         | MAY 13, 2011    |
| JOB No:       | 100610300       |
| IL PROJ. NO.  | PIA-4079        |
| AIP PROJ. NO. | 3-17-0080-XX    |
| SHEET         | 83 OF 84 SHEETS |

**PE092**

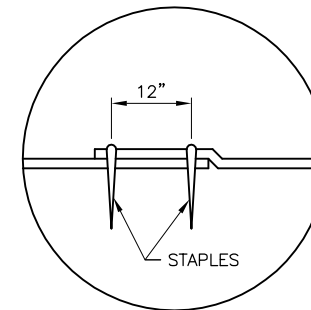
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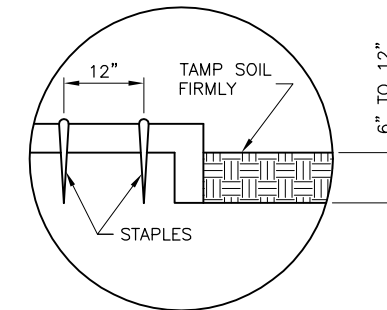
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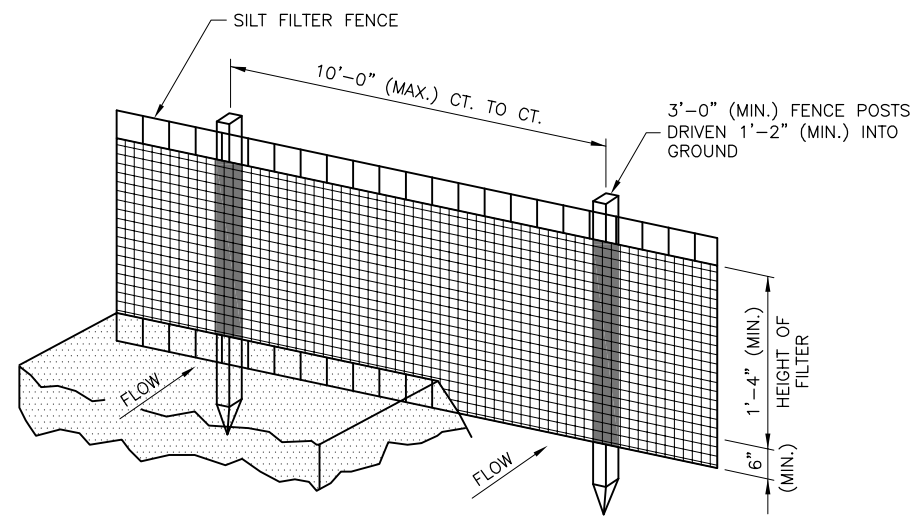
**DETAIL 1 - TERMINAL FOLD**



**DETAIL 2 - JUNCTION SLOT**



**DETAIL 3 - ANCHOR SLOT**



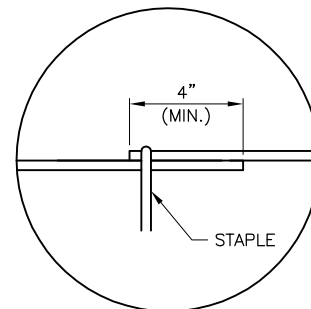
**PERSPECTIVE VIEW**

**EROSION CONTROL FABRIC FENCE DETAILS**

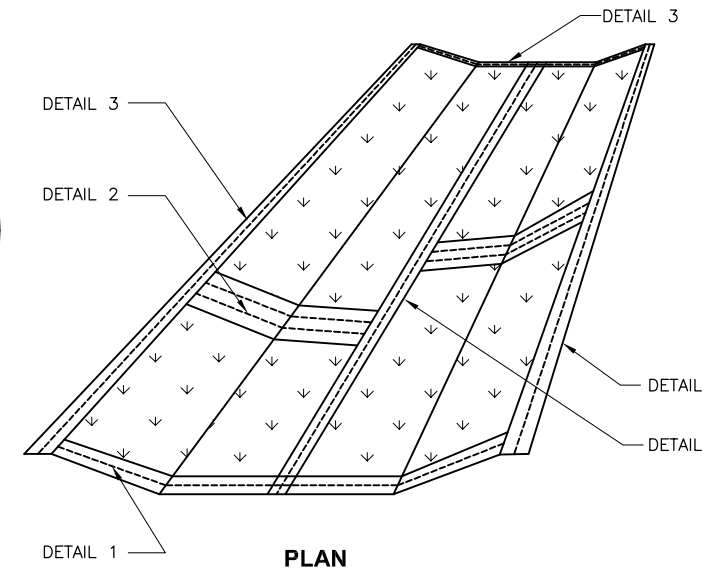
N.T.S.

**EROSION CONTROL FABRIC FENCE NOTES**

1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.



**DETAIL 4 - LAP JOINT**



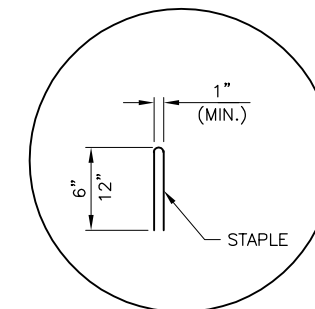
**PLAN**

**EXCELSIOR BLANKET DETAILS**

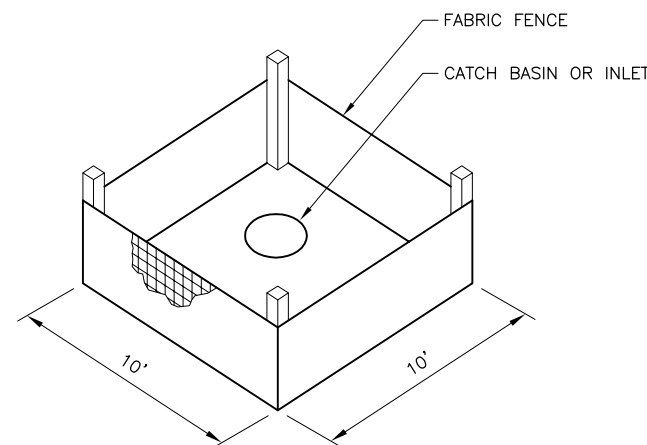
N.T.S.

**EXCELSIOR BLANKET NOTES**

1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
3. ALL TERMINAL ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.

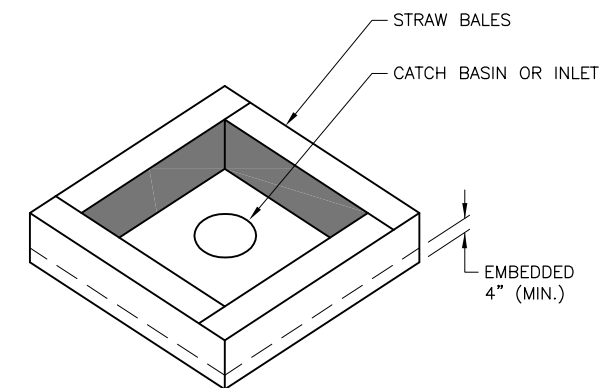


**DETAIL 5 - STAPLE DETAIL**



**INLET PROTECTION WITH FABRIC**

N.T.S.



**INLET PROTECTION WITH STRAW BALES**

N.T.S.

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REHABILITATE RUNWAY 13/31 & PARALLEL TAXIWAY  
 EROSION CONTROL DETAILS 1

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