

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-RS-SW&SR	COOK	19	1
ILLINOIS			CONTRACT NO. 62L11	

\* 19 + 5 = 24 TOTAL SHEETS

FOR INDEX OF SHEETS, SEE SHEET NO. 2

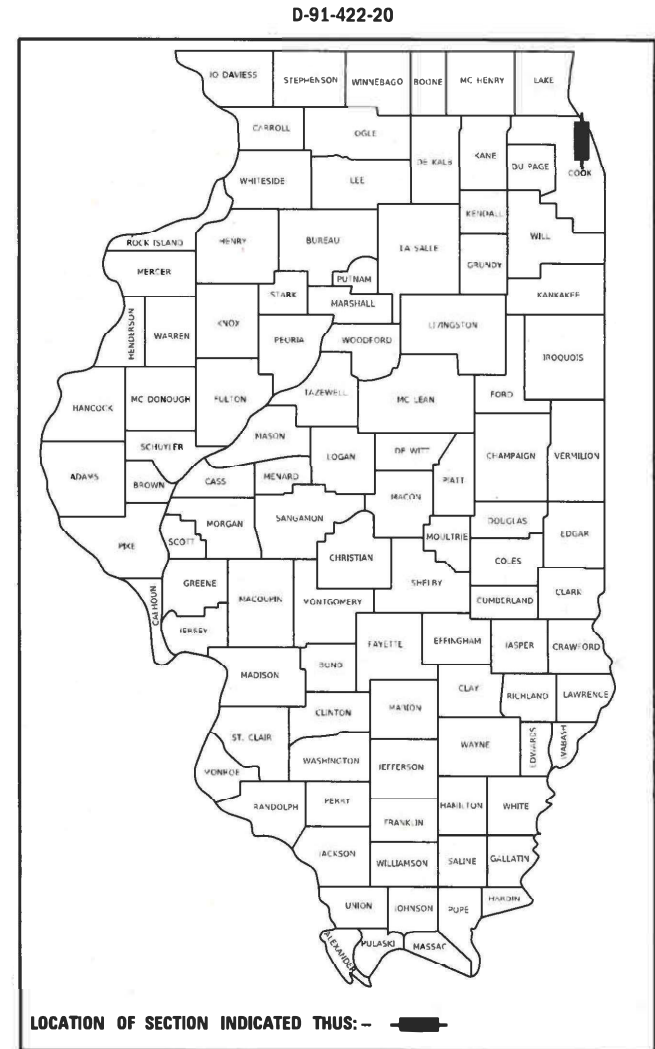
THIS PROJECT IS LOCATED IN THE VILLAGES OF NORTHBROOK AND GLENVIEW

PROPOSED  
HIGHWAY PLANS

OR 42: SHERMER ROAD  
WILLOW ROAD TO OLD WILLOW ROAD  
SECTION: 2020-039-RS-SW&SR  
STANDARD OVERLAY, NEW SHOULDERS,  
AND ADA IMPROVEMENTS  
COOK COUNTY

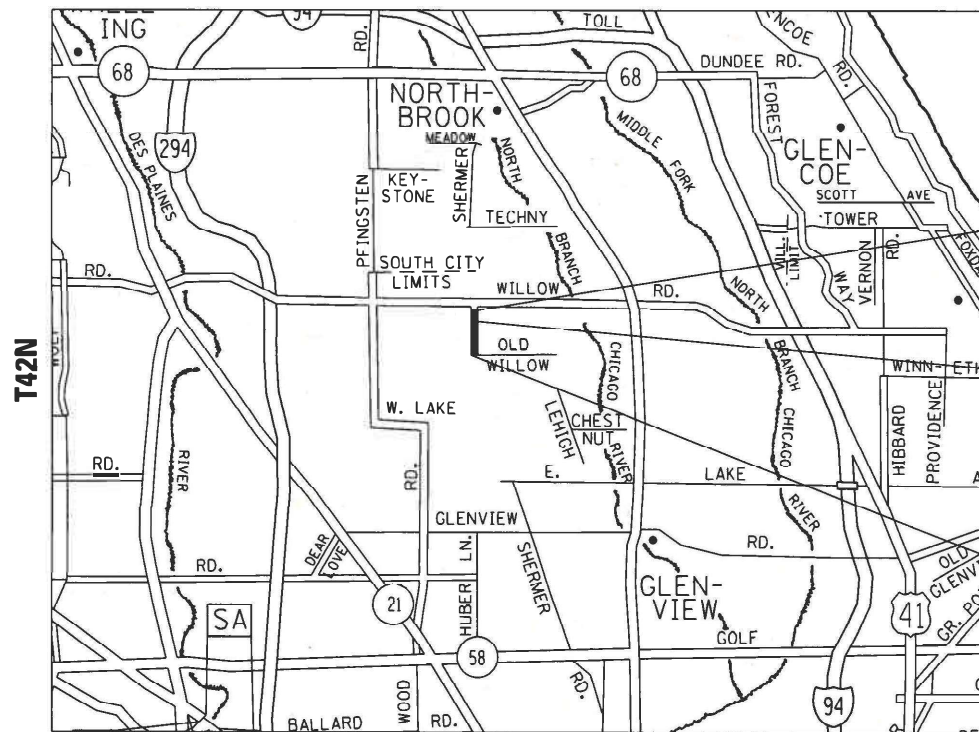
TRAFFIC DATA

EXISTING ADT = 9,200 (2018)  
POSTED SPEED LIMIT = 35 MPH



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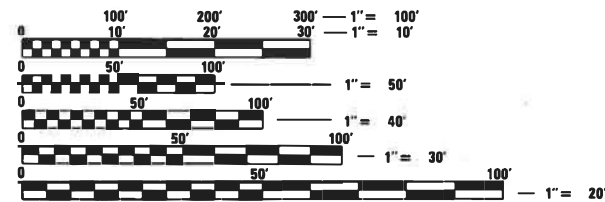
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PROJECT ENDS  
STATION 45 + 60

RESURFACING ENDS  
STATION 36 + 87

PROJECT BEGINS  
STATION 19 + 92



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS  
1-800-892-0123  
OR 811

NORTHFIELD TOWNSHIP

GROSS LENGTH = 2,568 FT. = 0.49 MILES  
NET LENGTH = 1695 FT. = 0.32 MILES

PROJECT ENGINEER: LUKASZ POCIECHA (847) 705-4255  
PROJECT MANAGER: FAWAD AQUEEL

CONTRACT NO. 62L11

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED March 16 20 22

Jose Pios REGIONAL ENGINEER

May 13, 2022 [Signature]  
ENGINEER OF DESIGN AND ENVIRONMENT

May 13, 2022 [Signature]  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

**INDEX OF SHEETS**

**STATE STANDARDS**

**GENERAL NOTES**

**SHEET NO.**

**DESCRIPTION**

**STANDARD NO.**

**DESCRIPTION**

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL J.U.L.I.E. AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
2. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE VILLAGES OF GLENVIEW AND NORTHBROOK.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5. SIDEWALK RAMPS MODIFICATIONS WITHIN THE LIMITS OF THE PROJECT SHALL CONFORM TO THE APPLICABLE HIGHWAY STANDARDS INCLUDED IN THE PLANS.
6. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
7. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
8. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
9. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
10. LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT [OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)], WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
11. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
12. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
13. SIDEWALK REMOVAL AND P.C.C SIDEWALK LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.
14. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
15. THE ENGINEER SHALL CONTACT FADI SULTAN, AREA TRAFFIC FIELD ENGINEER, AT FADI.SULTAN@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
16. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

1	COVER SHEET	000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
2-3	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES	424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
4-6	SUMMARY OF QUANTITIES	442201-03	CLASS C AND D PATCHES
7-7A	TYPICAL SECTIONS	482011-03	HMA SHLD. STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
8-9	ROADWAY AND PAVEMENT MARKING PLAN	606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
9A	DETECTOR LOOP REPLACEMENT PLAN	701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
9B	PATCHING (SPECIAL) PLAN IL 21 NORTH OF SANDERS	701311-03	LANE CLOSURE, 2W, 2L, MOVING OPERATIONS, DAY ONLY
10A	BD-08: DETAILS FOR FRAMES AND LIDS ADJUSTMENT	701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
10	BD-22: PAVEMENT PATCHING FOR HMA SURFACE REMOVAL	701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
11	BD-24: CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
12	BD-32: BUTT JOINT AND HMA TAPER DETAILS	701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
13	TC-10: TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, DRIVEWAYS	701901-08	TRAFFIC CONTROL DEVICES
14	TC-11: TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)		
15	TC-13: DISTRICT ONE TYPICAL PAVEMENT MARKINGS		
16	TC-16: SHORT TERM PAVEMENT MARKINGS LETTERS AND SYMBOLS		
17	TC-22: ARTERIAL ROAD INFORMATION SIGN		
18	TC-26: DRIVEWAY ENTRANCE SIGN		
19	PD-01: PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS		
19A	TS-07: DISTRICT 1- DETECTOR LOOP DETAILS FOR ROADWAY RESURFACING		

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, STATE STANDARDS,  
AND GENERAL NOTES**

SCALE: SHEET 1 OF 2 SHEETS STA. 19+92 TO STA. 45+60

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-R5-SW&SR	COOK	19	2
			CONTRACT NO. 62L11	
		ILLINOIS	FED. AID PROJECT	

**GENERAL NOTES CONT.**

17. THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT.
18. THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.
19. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
20. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
21. DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) SHOWN IN THE PLANS.
22. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
23. WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H) OR A NOTCHED LONGITUDINAL WEDGE IS USED.
24. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
25. THE CONTRACTOR MUST USE EXTREME CARE AND CAUTION WHEN MILLING AND PAVING THE PAVEMENT UNDER THE RAILROAD BRIDGE SO AS TO AVOID ACCIDENTLY HITTING THE BRIDGE WITH DUMP TRUCKS OR MILLING MACHINES. IT IS RECCOMENDED THAT MILLING UNDER THE RR STRUCTURE BE DONE USING A GRINDER MOUNTED ON A SKID-STEER/ BOBCAT LOADER"
26. MILLING AND RESURFACING UNDER THE RAILROAD STRUCTURE MUST NOT CHANGE OR NEGATIVELY IMPACT THE MINIMUM VERTICAL CLEARANCE UNDER THE STRUCTURE.
27. THE AGGREGATE GRADATION FOR THE AGGREGATE SUBGRADE IMPROVEMENT 12" LOWER LIFT SHALL BE CS 1 OR RR 1
28. ALL MILLED SURFACES SHALL BE AT A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES. ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT
29. AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAVE BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ABOVE ITEM WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND IDOT SUBGRADE STABILITY MANNUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

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DRAWN -	REVISED -	42					2020-039-RS-SW&SR	COOK	19	3	
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PLOT DATE = 4/29/2022	DATE -	REVISED -		SCALE:	SHEET 2 OF 2 SHEETS	STA. 19+92 TO STA. 45+60					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 100% STATE					
20200100	EARTH EXCAVATION	CU YD	358	358					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	103	103					
21101625	TOPSOIL FURNISH AND PLACE, 6"	SO YD	113	113					
25200110	SODDING, SALT TOLERANT	SO YD	216	216					
25200200	SUPPLEMENTAL WATERING	UNIT	2.2	2.2					
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	40	40					
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SO YD	478	478					
31101000	SUBGRADE GRANULAR MATERIAL, TYPE B	TON	216	216					
35600712	HOT-MIX ASPHALT BASE COURSE WIDENING, 9"	SO YD	478	478					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	5250	5250					
40600370	LONGITUDINAL JOINT SEALANT	FOOT	3759	3759					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	12	12					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	349	349					
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	30	30					
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	321	321					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 100% STATE					
40604172	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70	TON	763	763					
42001300	PROTECTIVE COAT	SO YD	114	114					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	250	250					
42400800	DETECTABLE WARNINGS	SO FT	30	30					
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	7778	7778					
44000600	SIDEWALK REMOVAL	SO FT	250	250					
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SO YD	165	165					
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SO YD	120	120					
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SO YD	45	45					
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	29	29					
60250200	CATCH BASINS TO BE ADJUSTED	EACH	5	5					
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	175	175					
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2					

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SHERMER RD (SOUTH OF WILLOW TO OLD WILLOW)  
SUMMARY OF QUANTITIES**

SCALE: SHEET NO. 1 OF 3 SHEETS STA. 19+92 TO STA. 45+60

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-R5-SW&SR	COOK	19	4
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT			CONTRACT NO. 62L11	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 100% STATE	0005			
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1				
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1				
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	2	2				
67100100	MOBILIZATION	L SUM	1	1				
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1				
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	4052	4052				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	1351	1351				
70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	1013	1013				
70300211	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS - PAINT	SQ FT	37	37				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 100% STATE	0005			
70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	380	380				
70300261	TEMPORARY PAVEMENT MARKING - LINE 12"- PAINT	FOOT	438	438				
70300281	TEMPORARY PAVEMENT MARKING - LINE 24"- PAINT	FOOT	78	78				
70306120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE III TAPE	FOOT	3596	3596				
72000100	SIGN PANEL - TYPE 1	SQ FT	16.5	16.5				
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3950	3950				
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	380	380				
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	438	438				
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	78	78				
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	20	20				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	20	20				
78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	37	37				

\* = SPECIALTY ITEMS

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

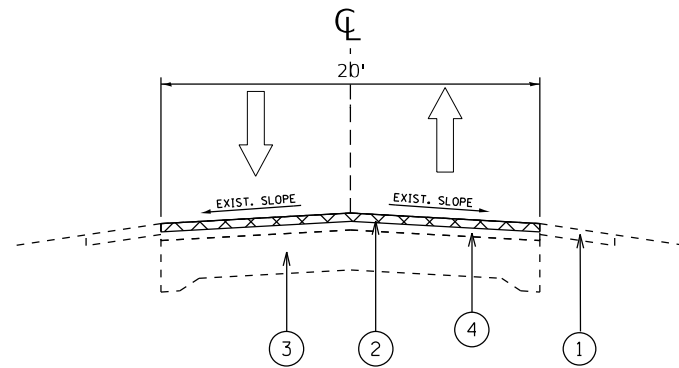
SHERMER RD (SOUTH OF WILLOW TO OLD WILLOW)  
SUMMARY OF QUANTITIES

SCALE: SHEET NO. 2 OF 3 SHEETS STA. 19+92 TO STA. 45+60

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-R5-SW&SR	COOK	19	5
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62L11	



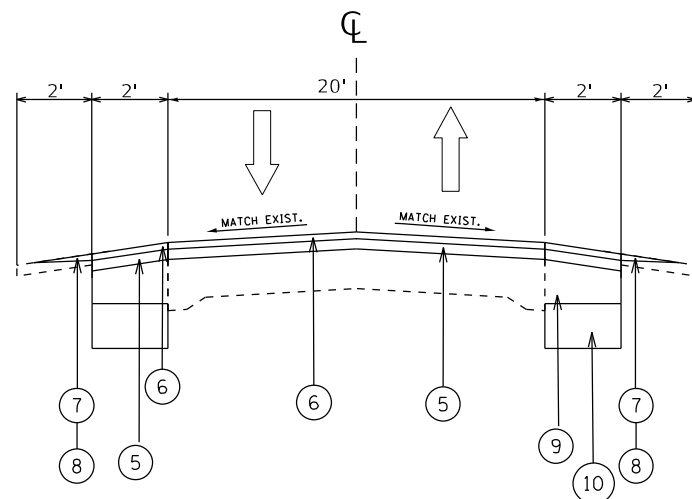
# SHERMER RD



## EXISTING TYPICAL SECTION

STA 19+92 TO STA 36+87

# SHERMER RD



## PROPOSED TYPICAL SECTION

STA 19+92 TO STA 36+87

### LEGEND

- ① EXISTING AGGREGATE SHOULDER
- ② PROPOSED HMA SURFACE REMOVAL, 2 1/2 "
- ③ EXISTING PCC BASE COURSE, ±9"
- ④ EXISTING HMA COURSE, ±3"
- ⑤ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50-0.75"
- ⑥ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70- 1.75"
- ⑦ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑧ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑨ PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING, 9"
- ⑩ PROPOSED AGGREGATE SUBGRADE IMPROVEMENT 12"
- ⑪ EXISTING CURB AND GUTTER

### NOTES

1. THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.
2. THE COST OF MILLING OVER THE GUTTER IS INCLUDED IN THE COST OF SURFACE REMOVAL.
3. THE COST OF C&G REMOVAL SHALL INCLUDE THE COST OF HMA OVERLAY REMOVAL OVER THE GUTTER.
4. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED HMA BINDER COURSE IL-4.75, N50.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS			
MIXTURE USES	MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70- 1 3/4"	4.0% @ 70 GYR	QC/OA
	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50- 3/4"	3.5% @ 50 GYR	QC/OA
PATCHING	CLASS D PATCHES (HMA BINDER IL-19.0 mm)	4.0% @ 70 GYR	QC/OA
	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES, 3" -CLASS D PATCHES (HMA BINDER IL-19.0 mm)	4.0% @ 70 GYR	QC/OA
PATCHING (SPECIAL)	HMA SURFACE COURSE, IL-9.5, MIX D N70, 2"	4.0% @ 70 GYR	QC/OA
	HMA BINDER COURSE, IL-19.0, N70, 8"	4.0% @ 70 GYR	QC/OA
HMA SHOULDER	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70- 1 3/4 "	4.0% @ 70 GYR	QC/OA
	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50- 3/4"	3.5% @ 50 GYR	QC/OA
	HOT MIX ASPHALT BASE COURSE WIDENING, 9"- (HMA BINDER IL-19.0)	4.0% @ 70 GYR	QC/OA

QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)

NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/50 YD/IN.

NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

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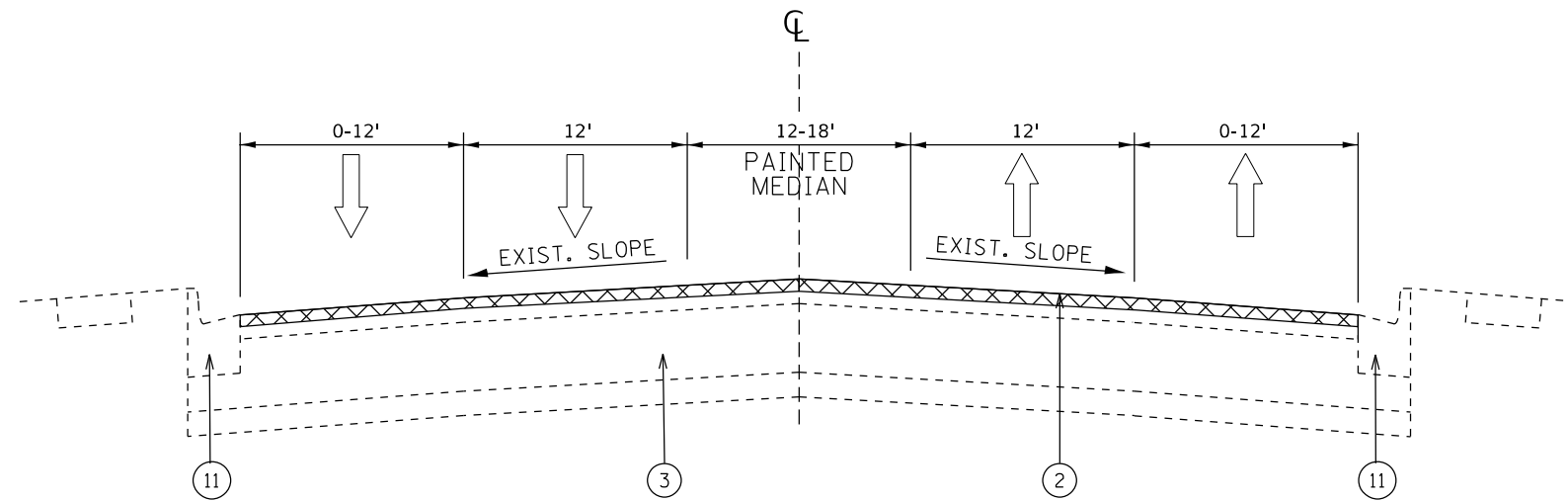
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED TYPICAL SECTIONS  
SHERMER RD - WILLOW RD TO OLD WILLOW RD**

SCALE: SHEET 1 OF 1 SHEETS STA. 19+92 TO STA. 45+60

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-R5-SW&SR	COOK	19	7
CONTRACT NO. 62L11				
ILLINOIS FED. AID PROJECT				

## SHERMER RD



### EXISTING TYPICAL SECTION

STA 42+11 TO STA 45+60

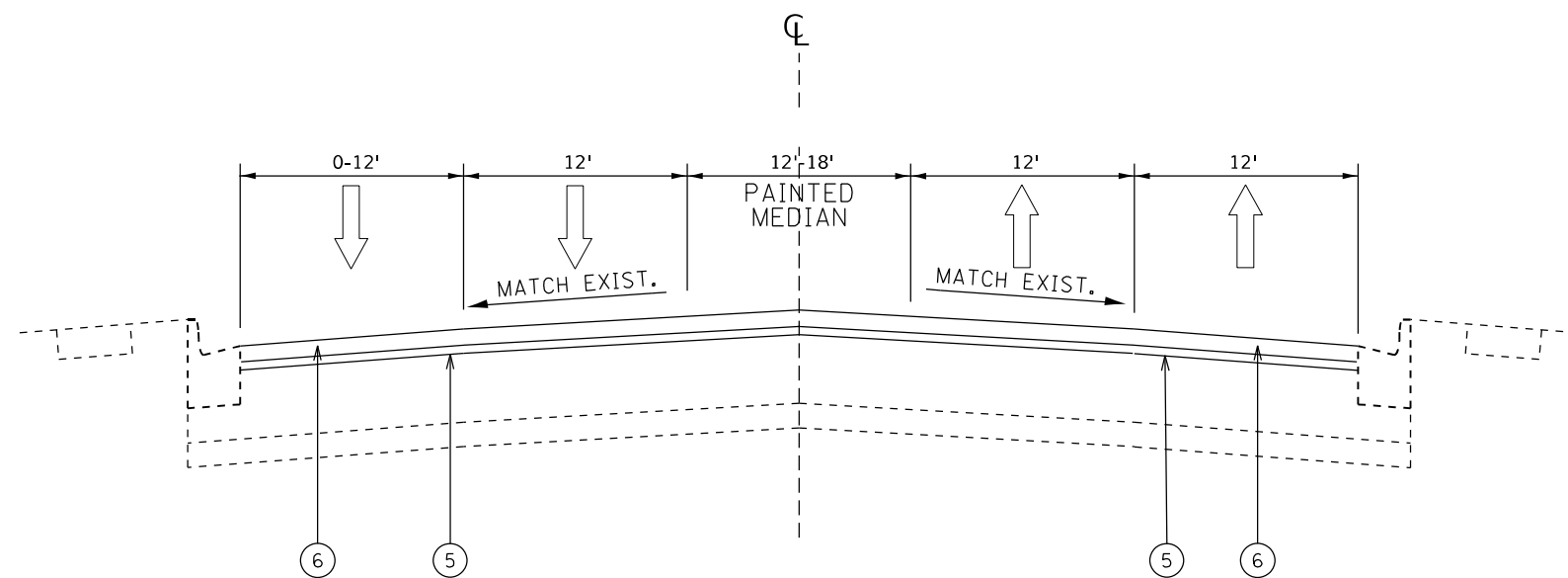
### LEGEND

- ① EXISTING AGGREGATE SHOULDER
- ② PROPOSED HMA SURFACE REMOVAL, 2 1/2 "
- ③ EXISTING PCC BASE COURSE, ±9"
- ④ EXISTING HMA COURSE, ±3"
- ⑤ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50-0.75"
- ⑥ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70- 1.75"
- ⑦ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑧ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑨ PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING, 9"
- ⑩ PROPOSED AGGREGATE SUBGRADE IMPROVEMENT 12"
- ⑪ EXISTING CURB AND GUTTER

### NOTES

1. THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.
2. THE COST OF MILLING OVER THE GUTTER IS INCLUDED IN THE COST OF SURFACE REMOVAL.
3. THE COST OF C&G REMOVAL SHALL INCLUDE THE COST OF HMA OVERLAY REMOVA OVER THE GUTTER.
4. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED HMA BINDER COURSE IL-4.75, N50.

## SHERMER RD



### PROPOSED TYPICAL SECTION

STA 42+11 TO STA 45+60

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED TYPICAL SECTIONS  
SHERMER RD – WILLOW RD TO OLD WILLOW RD**

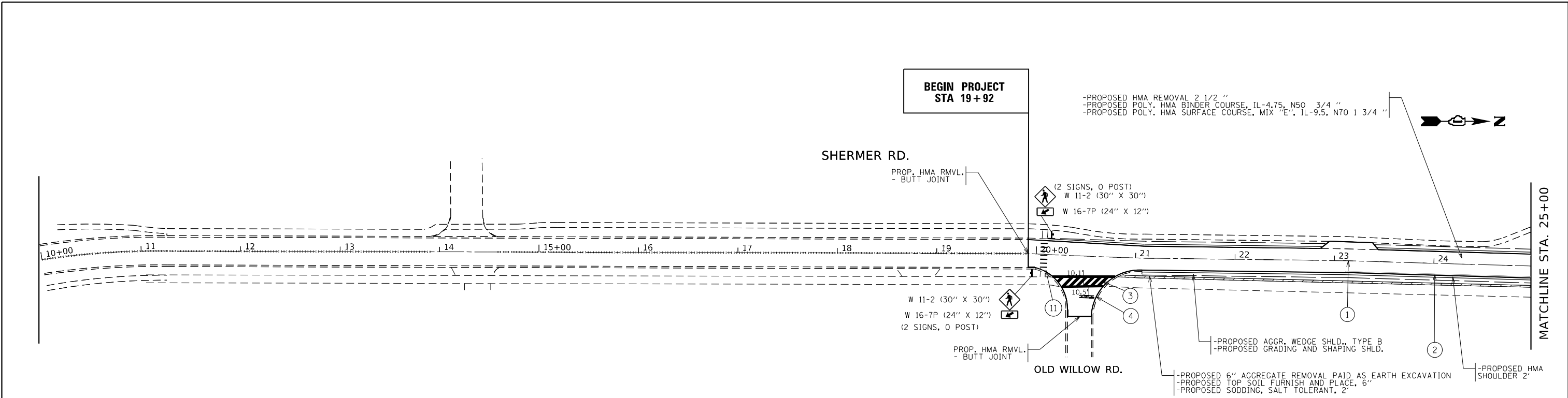
O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-R5-SW&SR	COOK	19	7A
CONTRACT NO. 62L11				

SCALE: SHEET 1 OF 1 SHEETS STA. 19+92 TO STA. 45+60

ILLINOIS FED. AID PROJECT

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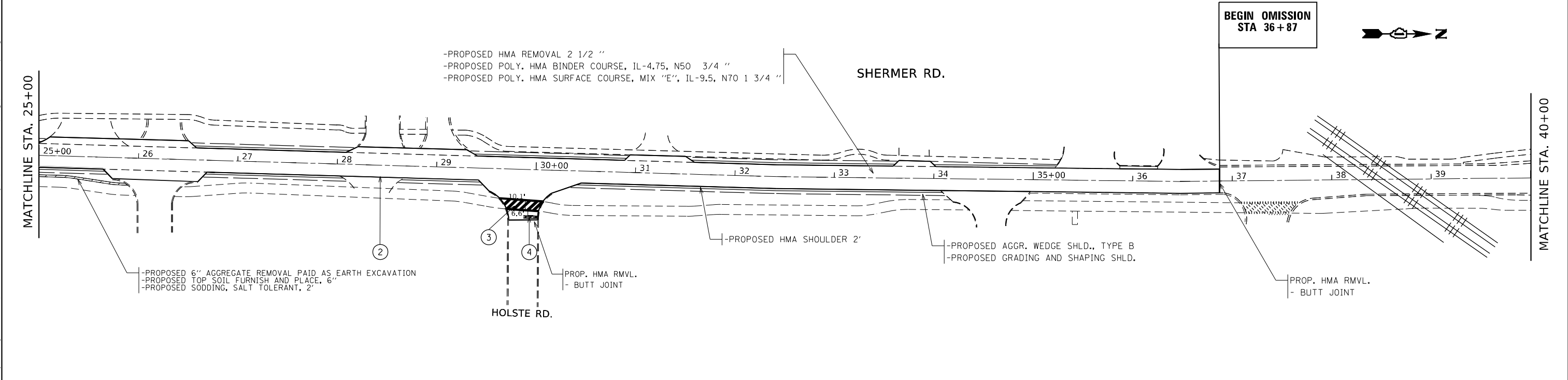




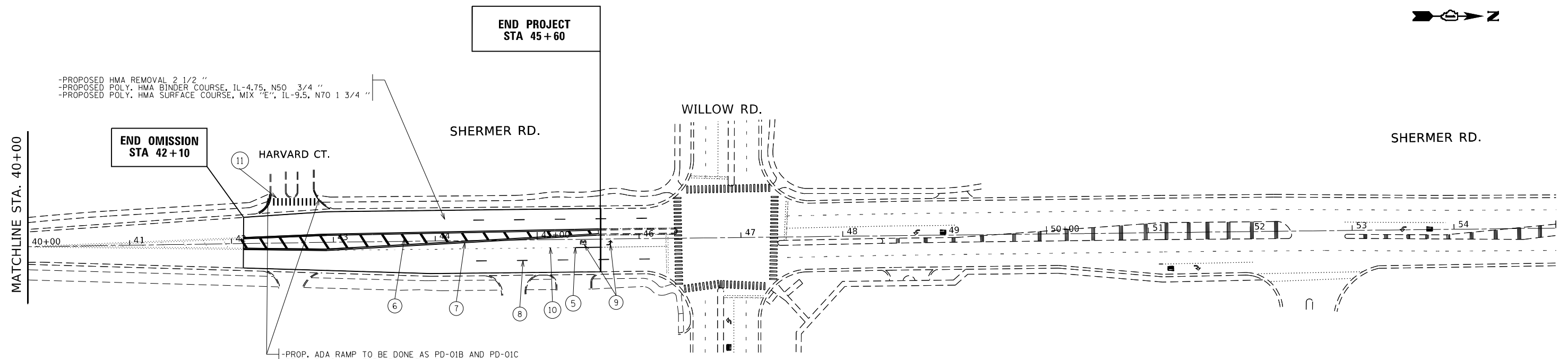
**PAVEMENT MARKING LEGEND**

① PROP. THERMOPLASTIC PVMT MARKING LINE 4", 10' DASH 30' SKIP, YELLOW (TYP.)	⑥ PROP. THERMOPLASTIC PVMT MARKING LINE 12", DIAGONALS, YELLOW (TYP.)
② PROP. THERMOPLASTIC PVMT MARKING LINE 4", EDGE LINE, WHITE (TYP.)	⑦ PROP. THERMOPLASTIC PVMT MARKING LINE 4", DOUBLE YELLOW @ 11" C-C (TYP.)
③ PROP. THERMOPLASTIC PVMT MARKING BICYCLE & EQUESTRIAN CROSSWALK, 12" DIAGONALS, 6" CROSSWALK, WHITE (TYP.)	⑧ PROP. THERMOPLASTIC PVMT MARKING LINE 4", 10' DASH 30' SKIP, WHITE (TYP.)
④ PROP. THERMOPLASTIC PVMT MARKING LINE 24", STOPBAR, WHITE (TYP.)	⑨ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS, WHITE (TYP.)
⑤ PROP. THERMOPLASTIC PVMT MARKING LINE 6", WHITE (TYP.)	⑩ PROP. THERMOPLASTIC PVMT MARKING LINE 6", 2' DASH 6' SKIP, WHITE (TYP.)
⑪ PROP. THERMOPLASTIC PVMT MARKING LINE 12", CROSSWALK, WHITE (TYP.)	

NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE  
DETAILS TC-11 AND TC-13



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SCALE: 1"=50'					SHEET 1 OF 2 SHEETS	STA. 19+92 TO STA. 45+60	



-PROPOSED HMA REMOVAL 2 1/2"  
 -PROPOSED POLY. HMA BINDER COURSE, IL-4.75, N50 3/4"  
 -PROPOSED POLY. HMA SURFACE COURSE, MIX "E", IL-9.5, N70 1 3/4"

END OMISSION  
 STA 42+10

END PROJECT  
 STA 45+60

-PROP. ADA RAMP TO BE DONE AS PD-01B AND PD-01C  
 -PROP. CONC. SW (FOR DET. WARNING)  
 •MAINTAIN EXISTING SLOPES  
 •SEE EXHIBIT 1 FOR MORE DETAILS

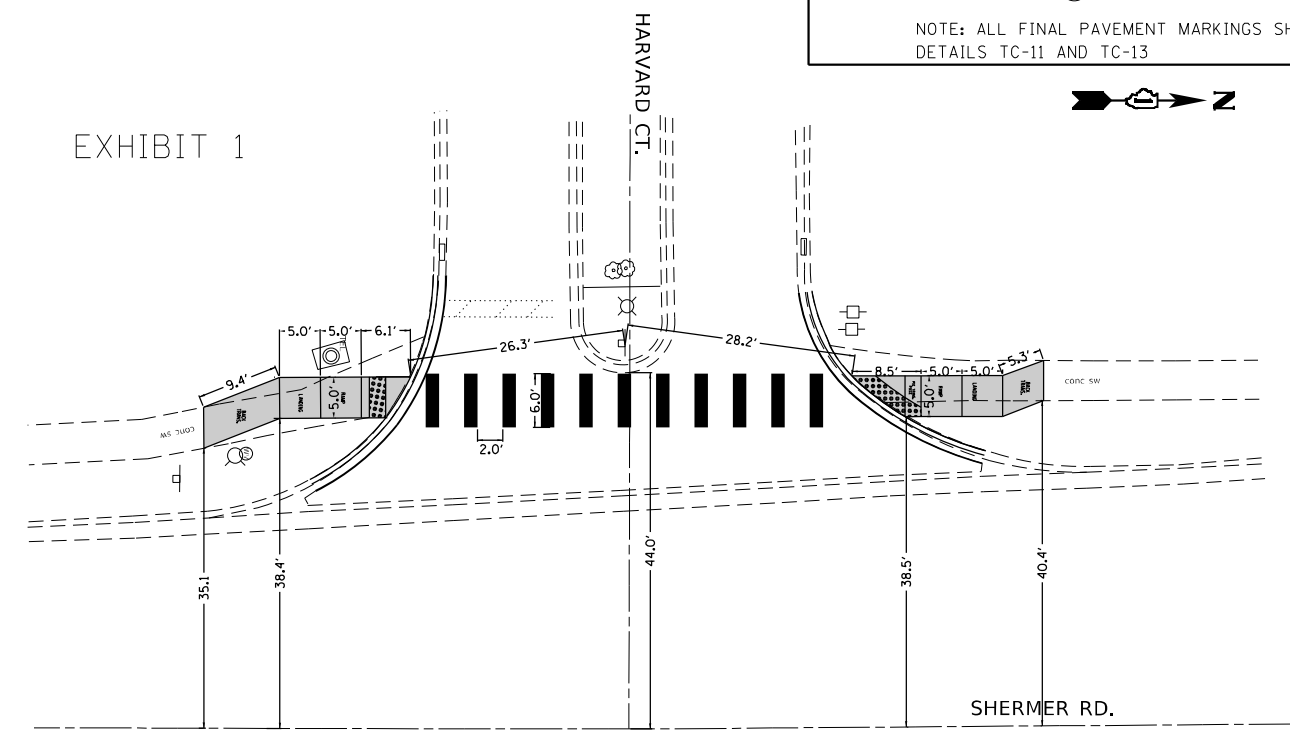
PAVEMENT MARKING LEGEND	
① PROP. THERMOPLASTIC PVMT MARKING LINE 4", 10' DASH 30' SKIP, YELLOW (TYP.)	⑥ PROP. THERMOPLASTIC PVMT MARKING LINE 12", DIAGONALS, YELLOW (TYP.)
② PROP. THERMOPLASTIC PVMT MARKING LINE 4", EDGELINE, WHITE (TYP.)	⑦ PROP. THERMOPLASTIC PVMT MARKING LINE 4", DOUBLE YELLOW @ 11" C-C (TYP.)
③ PROP. THERMOPLASTIC PVMT MARKING BICYCLE & EQUESTRIAN CROSSWALK, 12" DIAGONALS, 6" CROSSWALK, WHITE (TYP.)	⑧ PROP. THERMOPLASTIC PVMT MARKING LINE 4", 10' DASH 30' SKIP, WHITE (TYP.)
④ PROP. THERMOPLASTIC PVMT MARKING LINE 24", STOPBAR, WHITE (TYP.)	⑨ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS, WHITE (TYP.)
⑤ PROP. THERMOPLASTIC PVMT MARKING LINE 6", WHITE (TYP.)	⑩ PROP. THERMOPLASTIC PVMT MARKING LINE 6", 2' DASH 6' SKIP, WHITE (TYP.)
⑪ PROP. THERMOPLASTIC PVMT MARKING LINE 12", CROSSWALK, WHITE (TYP.)	

NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE DETAILS TC-11 AND TC-13

**NOTE:**

CURB AND GUTTER REMOVAL IS REQUIRED BETWEEN STA 42+00 AND 43+00 FOR ADA WORK. ANY RESTORATION THAT IS REQUIRED ADJACENT TO THE CURB AND GUTTER WILL BE PAID FOR UNDER SURFACE REMOVAL 2 1/2" AND REPLACED WITH SURFACE MIX "E"

**EXHIBIT 1**



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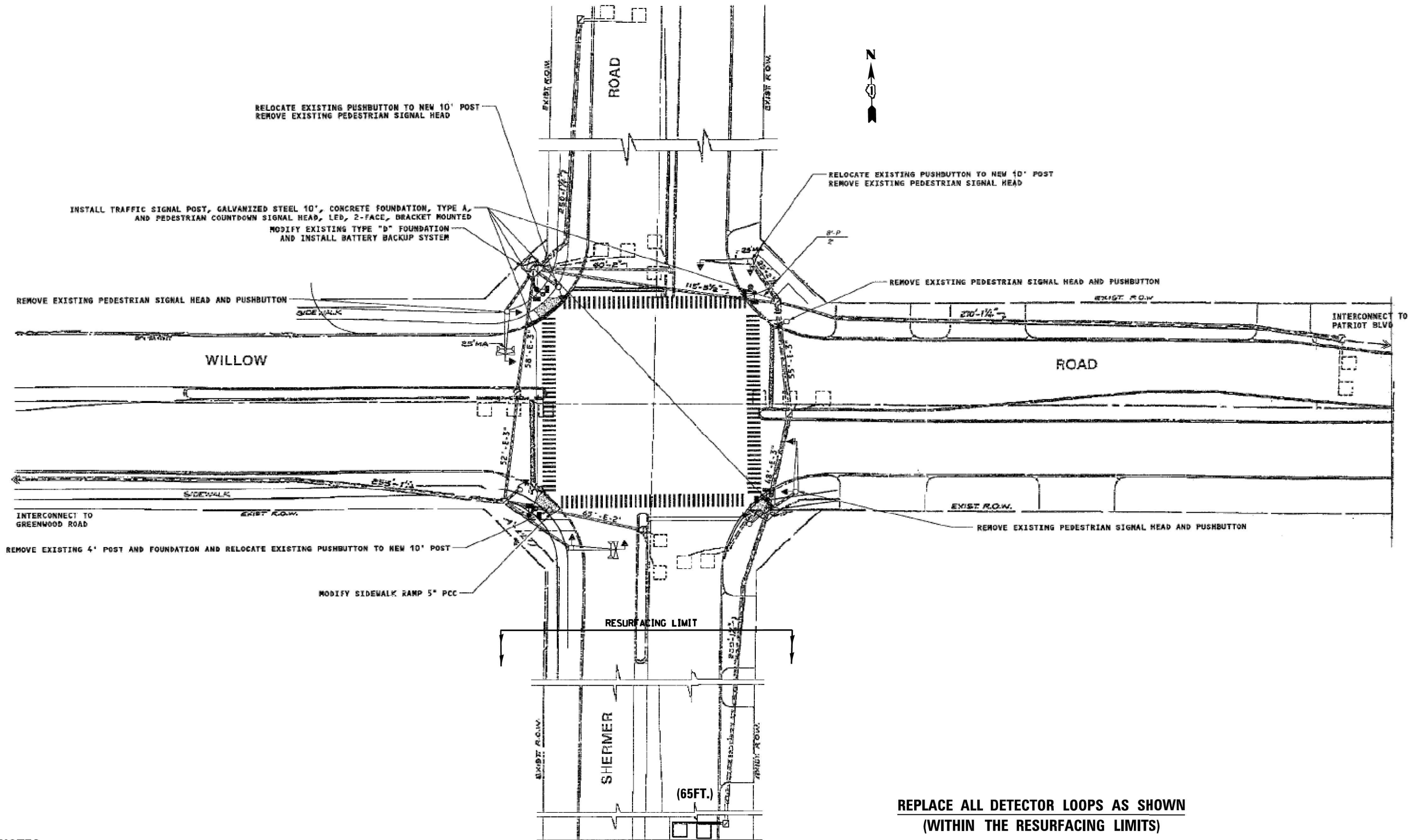
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	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN  
 SHERMER RD. (WILLOW RD. - OLD WILLOW RD.)**

SCALE: 1"=50' SHEET 2 OF 2 SHEETS STA. 19+92 TO STA. 45+60

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-R5&SW&SR	COOK	19	9
			CONTRACT NO. 62L11	
			ILLINOIS FED. AID PROJECT	



**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	65	FOOT

**TS#5770  
ECON 5**

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

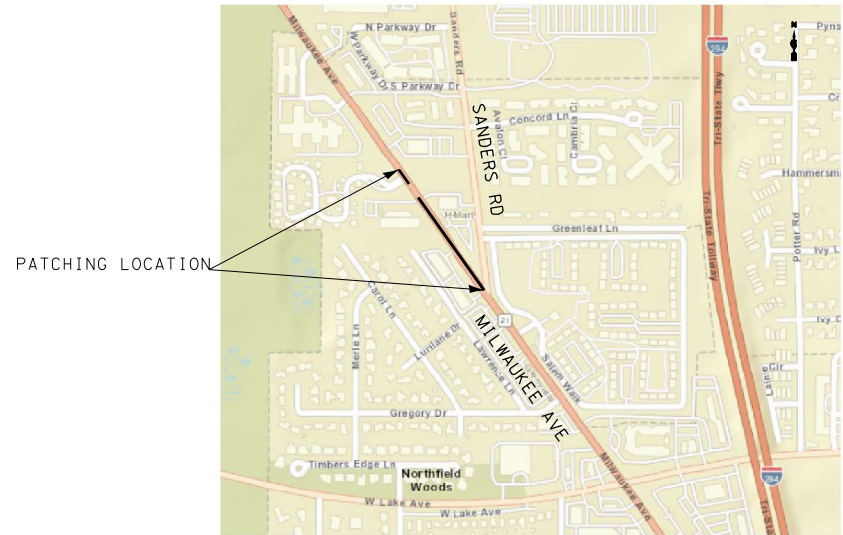
**DETECTOR LOOP REPLACEMENT PLAN  
SHERMER RD. (WILLOW RD. - OLD WILLOW RD.)**

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PLOT DATE = 4/29/2022	DATE -	REVISED -

SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. TO STA.

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-R5&SW&SR	COOK	19	9A
CONTRACT NO. 62L11				
ILLINOIS FED. AID PROJECT				

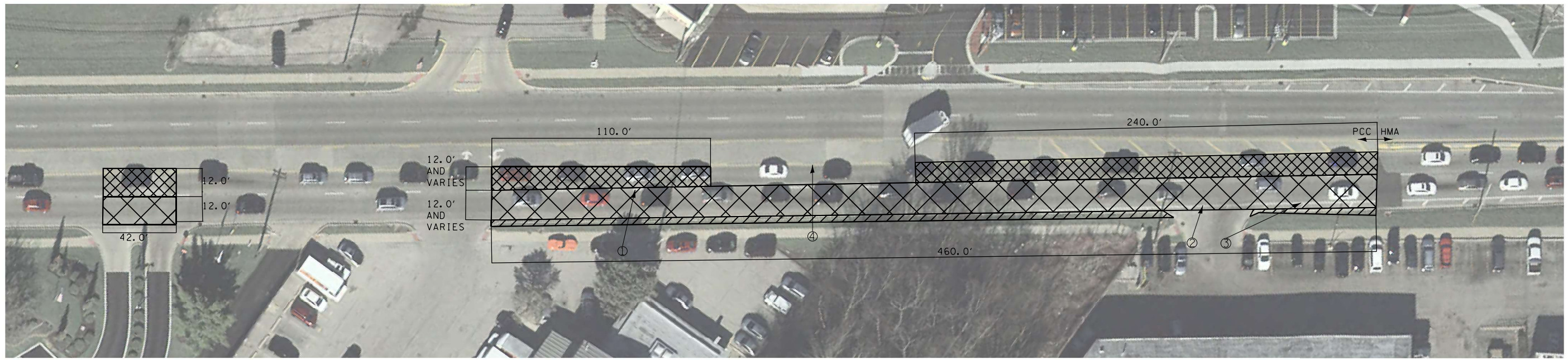
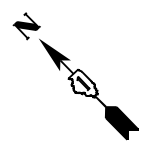
IL 21 PATCHING (SPECIAL) LOCATION MAP



**PAVEMENT MARKING LEGEND**

① PROP. THERMOPLASTIC PVMT MARKING LINE 4", 10' DASH 30' SKIP, YELLOW (TYP.)	③ PROP. THERMOPLASTIC PVMT MARKING LINE 12", DIAGONAL, WHITE (TYP.)
② PROP. THERMOPLASTIC PVMT MARKING LINE 4", EDGE LINE, WHITE (TYP.)	④ PROP. THERMOPLASTIC PVMT MARKING LINE 4", YELLOW (TYP.)

NOTE: ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE DETAILS TC-11 AND TC-13



- LANE 1 PATCH | -PROPOSED CLASS D PATCHES, TYPE IV, 10" (SPECIAL)  
 | -PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B (WHERE NECESSARY)
- LANE 2 PATCH | -PROPOSED CLASS D PATCHES, TYPE IV, 10" (SPECIAL)  
 | -PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B (WHERE NECESSARY)
- CURB AND GUTTER WORK | -CURB AND GUTTER REMOVAL AND REPLACEMENT  
 | -TOPSOIL FURNISH AND PLACE, 4"  
 | -SODDING, SALT TOLERANT  
 | -SUPPLEMENTAL WATERING

**NOTE:**  
EXACT PATCHING LOCATIONS TO BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER

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	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>PATCHING (SPECIAL) PLAN IL 21 AND NORTH OF SANDERS RD</b>			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

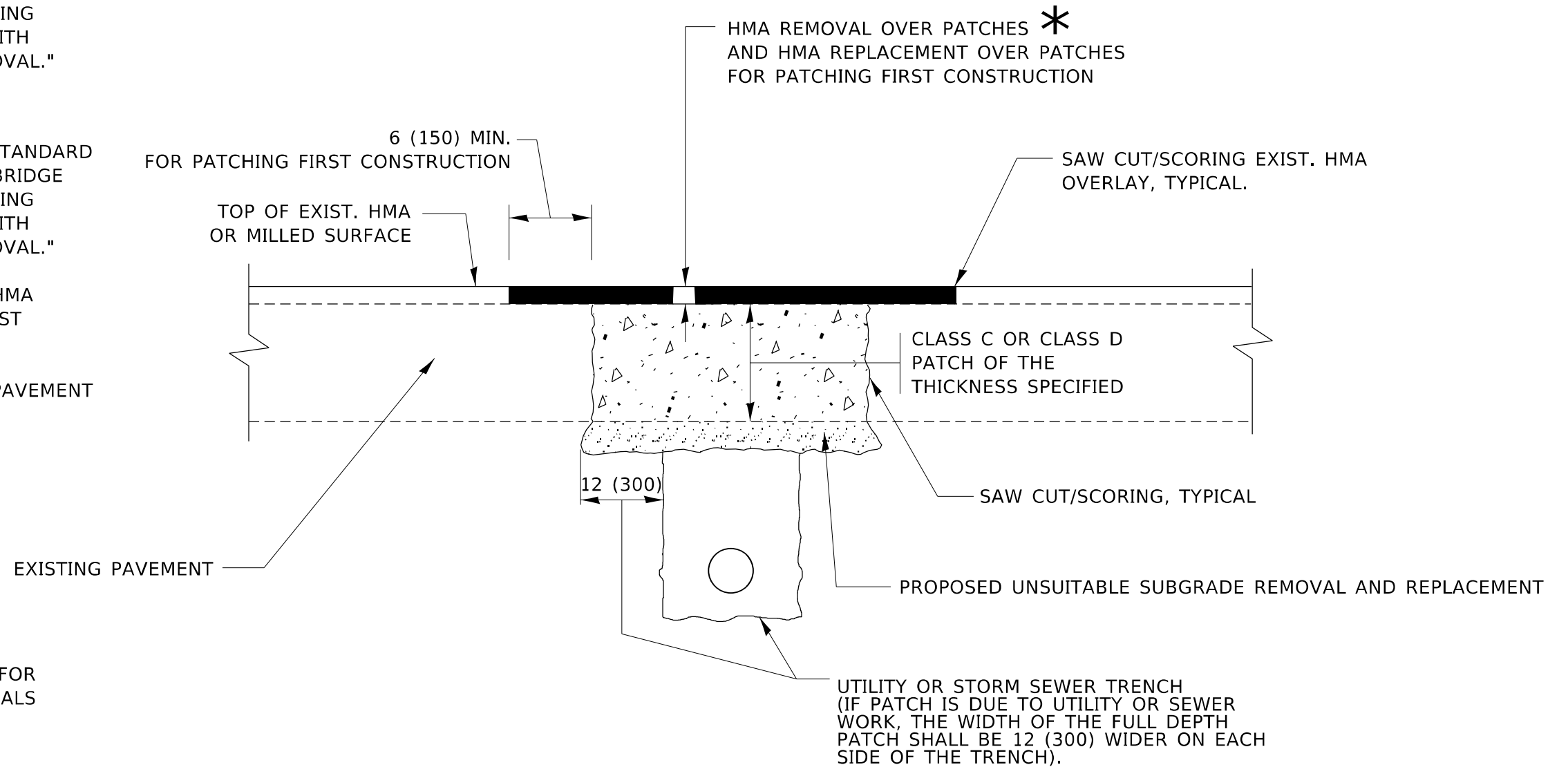
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	2020-039-R5-SW&SR	COOK	19	98
			CONTRACT NO. 62L11	
		ILLINOIS	FED. AID PROJECT	

**METHOD OF MEASUREMENT**

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

**BASIS OF PAYMENT**

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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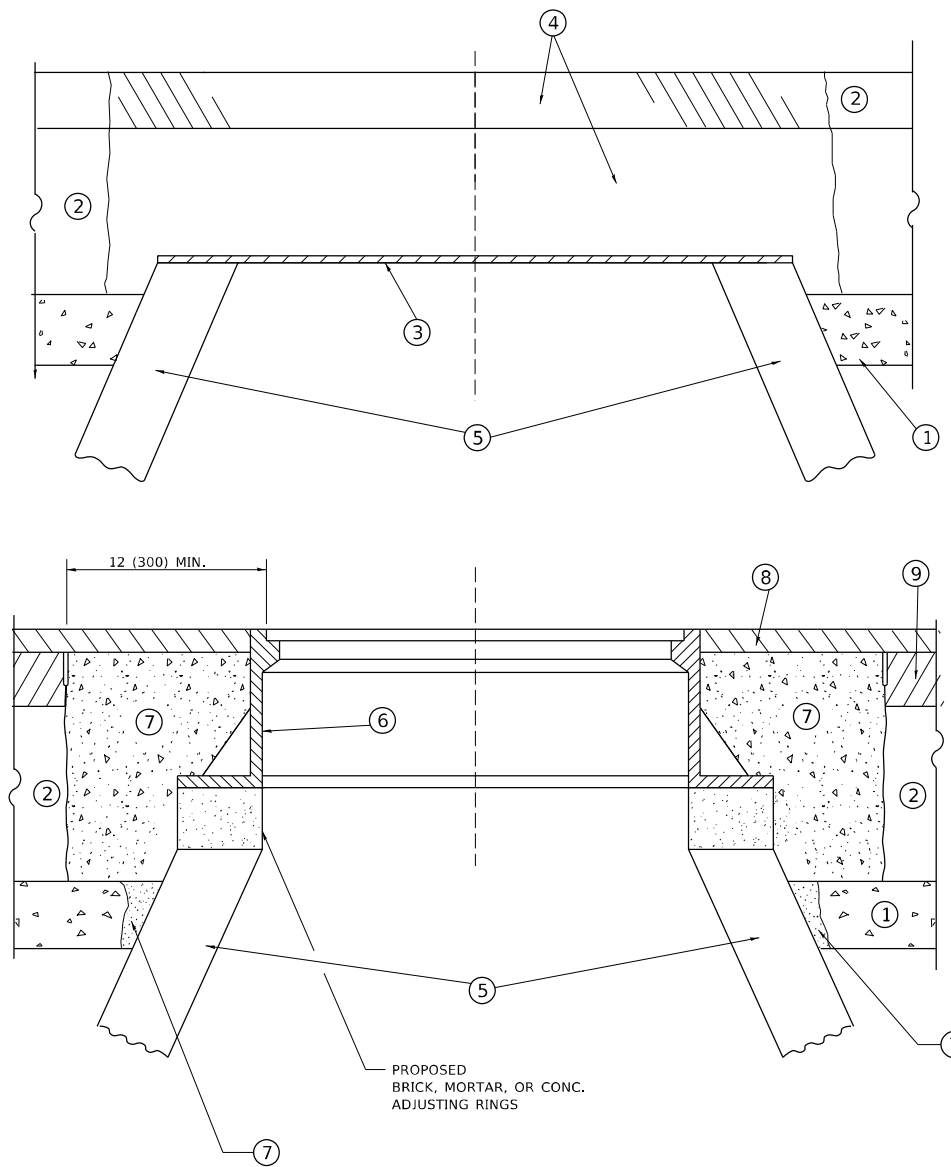
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PLOT DATE = 3/25/2022	DATE - 10-25-94	REVISED - K. SMITH 02-01-22

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR  
HMA SURFACED PAVEMENT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. 19+92 TO STA. 45+60

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-R5-SW&SR	COOK	19	10
BD400-04 (BD-22)			CONTRACT NO. 62L11	
ILLINOIS FED. AID PROJECT				



**DETAILS FOR FRAMES AND LIDS ADJUSTMENT  
WITH MILLING**

**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 1 1/2 (40) HMA TO REMAIN AFTER MILLING).

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS\*PP-1 CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- |  |                               |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL                 | ⑥ FRAME AND LID (SEE NOTES)   |
| ② EXISTING PAVEMENT                          | ⑦ CLASS*PP-1 CONCRETE         |
| ③ 36 (900) DIAMETER METAL PLATE              | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE  |
| ⑤ EXISTING STRUCTURE                         |                               |

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT**

- 1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- 2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- 3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- 4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

**NOTES**

- 1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- 2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- 3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- 4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

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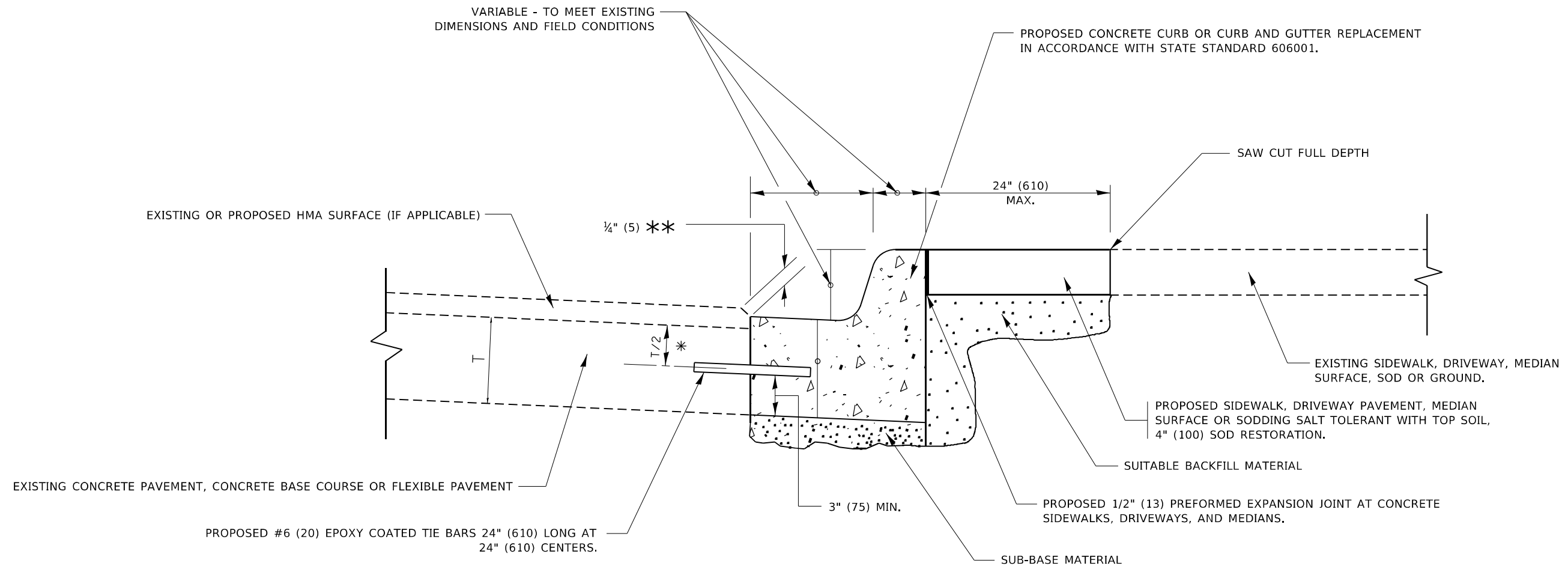
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PLOT DATE = 4/29/2022	DATE - 10-25-94	REVISED - K. SMITH 02-01-22

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. 19+92 TO STA. 45+60

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-R5-SW&SR	COOK	19	10A
BD600-03 (BD-08)		CONTRACT NO. 62L11		
ILLINOIS FED. AID PROJECT				



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

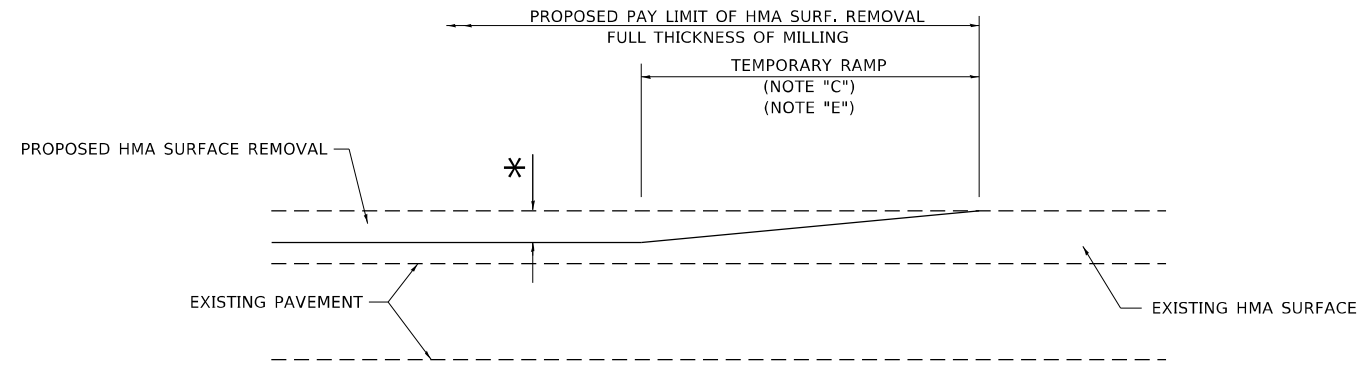
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. 19+92	TO STA. 45+60

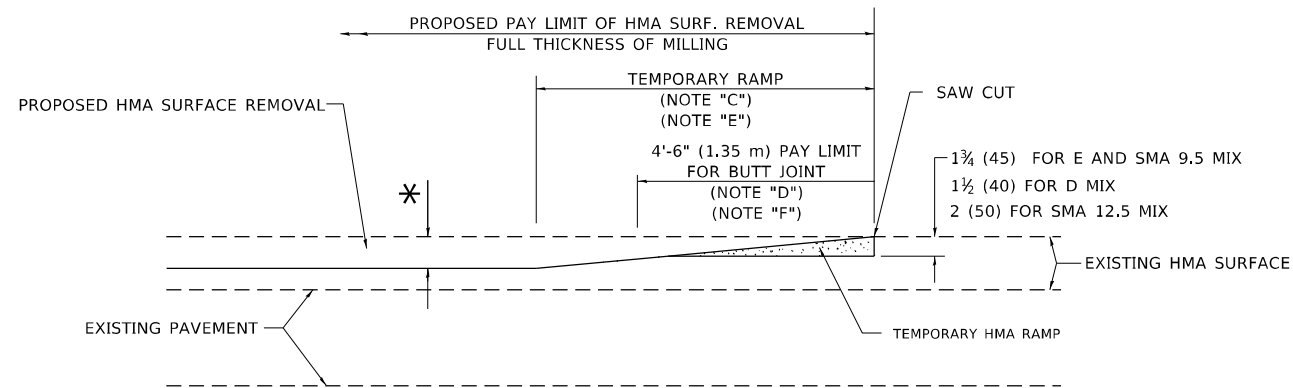
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42	2020-039-R5-SW&SR	COOK	19	11
<b>BD600-06 (BD-24)</b>		CONTRACT NO. 62L11		
ILLINOIS FED. AID PROJECT				



**MILLED TEMPORARY RAMP**

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 1**

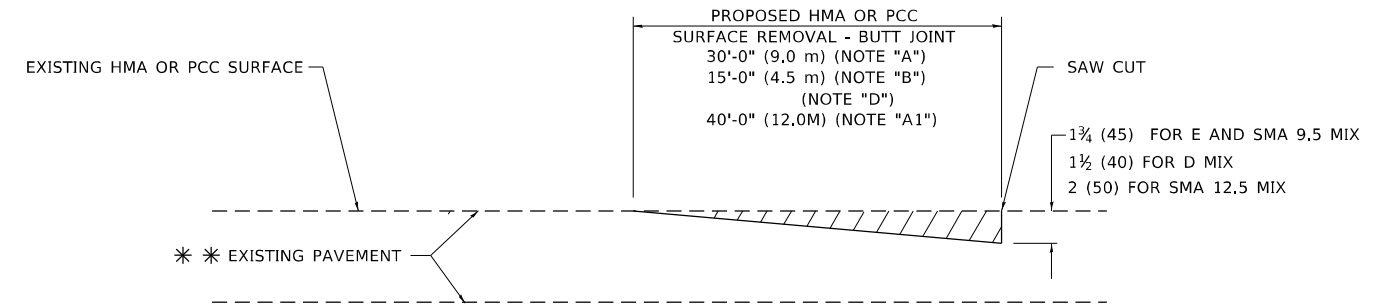


**HMA CONSTRUCTED TEMPORARY RAMP**

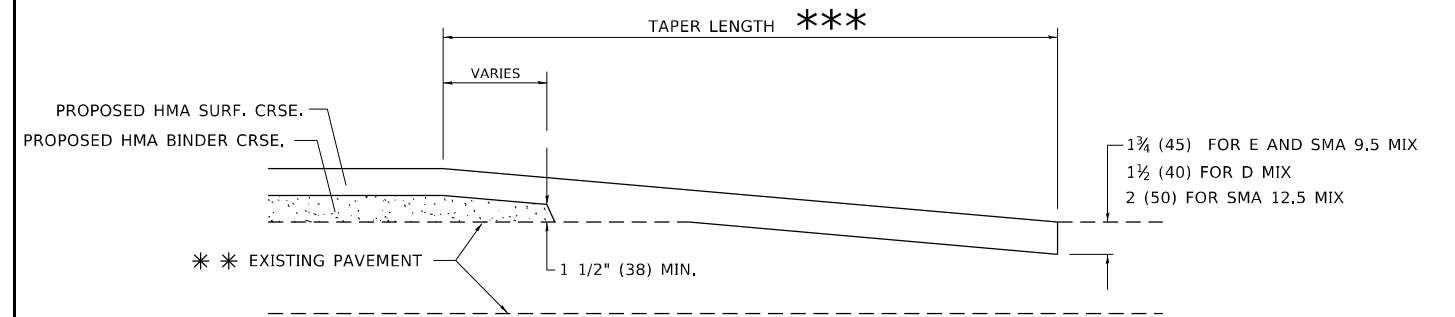
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

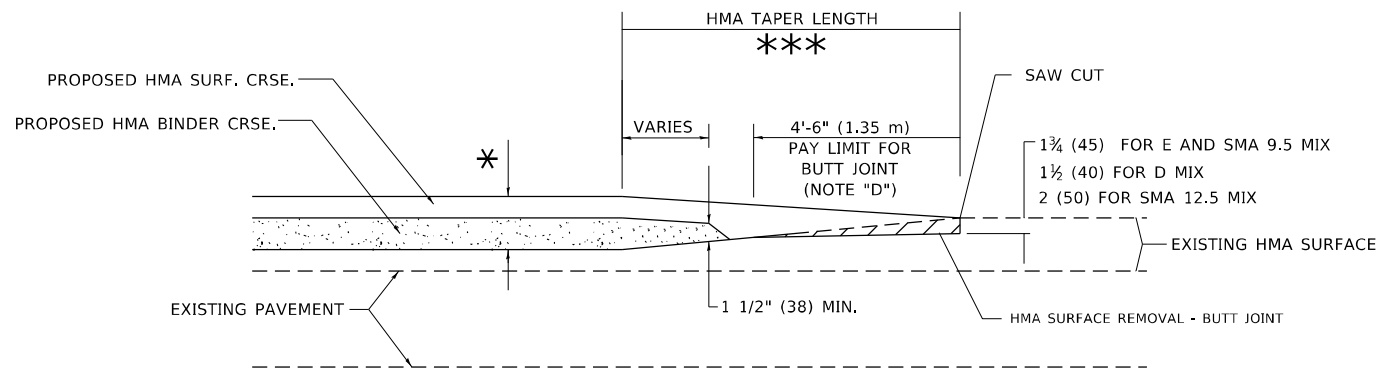
**GENERAL NOTES**

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**BUTT JOINT AND HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**BUTT JOINT AND HMA TAPER DETAILS**

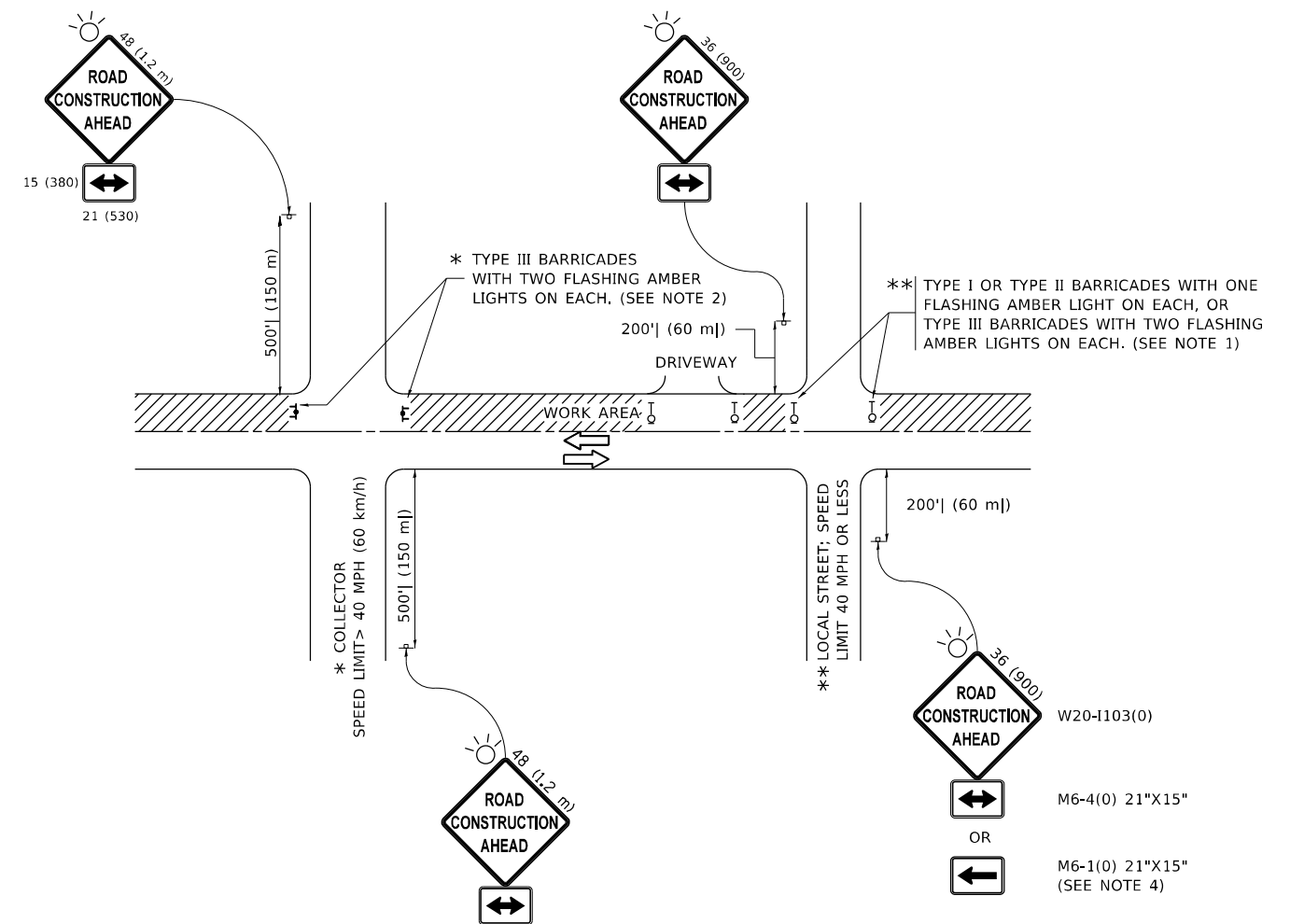
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O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-R5-SW&SR	COOK	19	12
<b>BD400-05 BD-32</b>		CONTRACT NO. 62L11		
ILLINOIS FED. AID PROJECT				

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PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 3/25/2022	CHECKED -	REVISED - R. BORO 01-01-07
	DATE - 06-13-90	REVISED - K. SMITH 02-01-22





**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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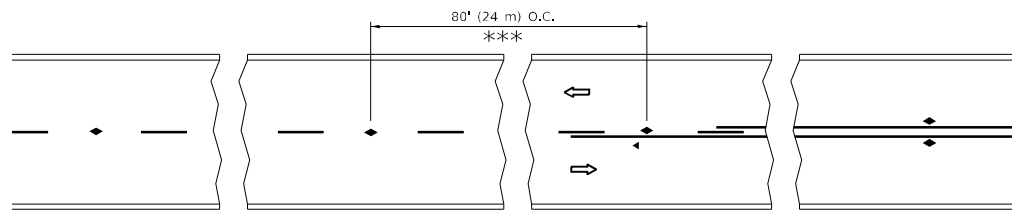
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PLOT DATE = 3/25/2022	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

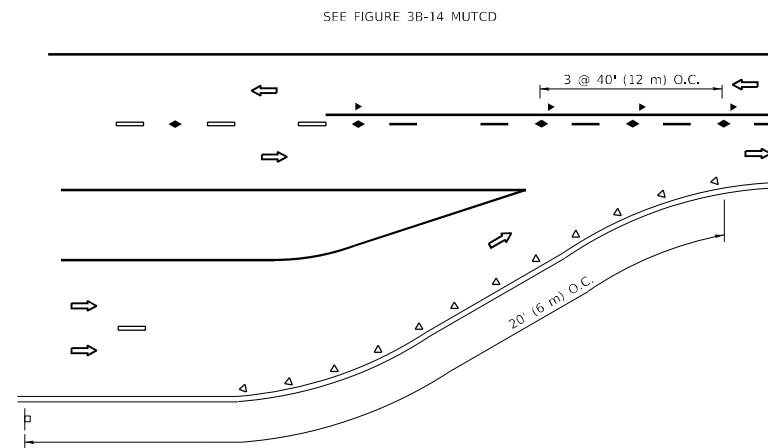
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O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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<b>TC-10</b>			CONTRACT NO. 62L11	
ILLINOIS FED. AID PROJECT				

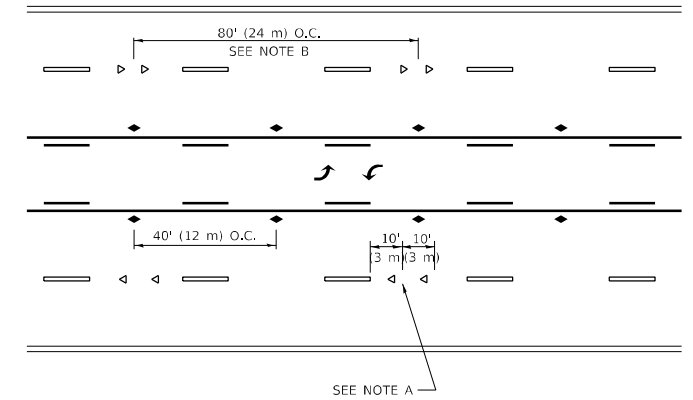


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

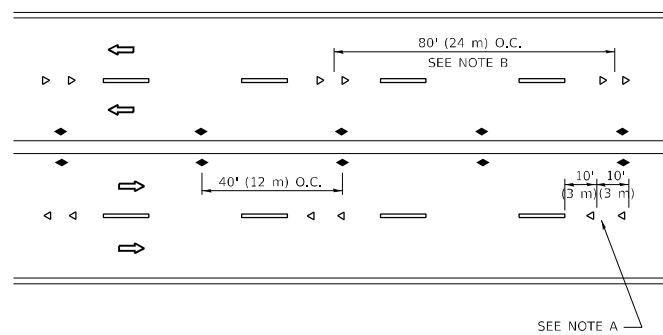
**TWO-LANE/TWO-WAY**



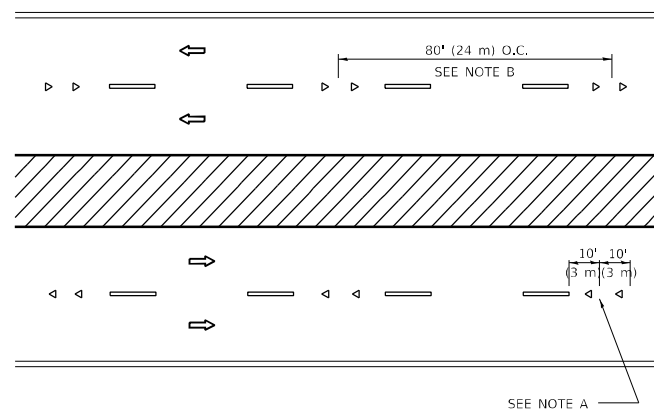
**LANE REDUCTION TRANSITION**



**TWO-WAY LEFT TURN**



**MULTI-LANE/UNDIVIDED**



**MULTI-LANE/DIVIDED**

**GENERAL NOTES**

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

**SYMBOLS**

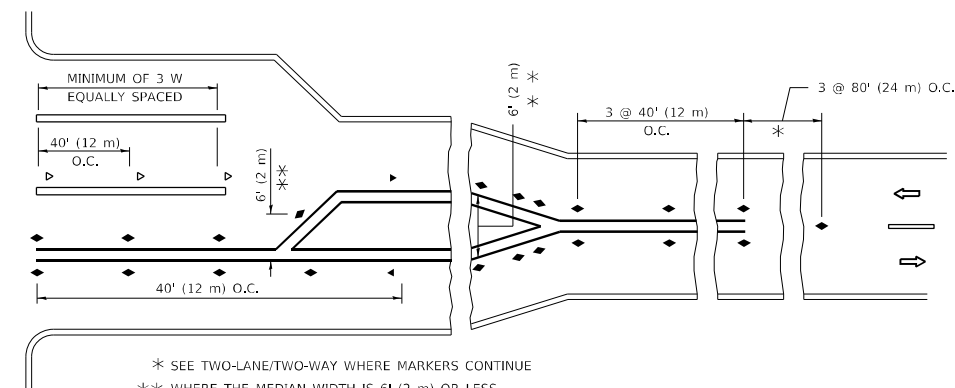
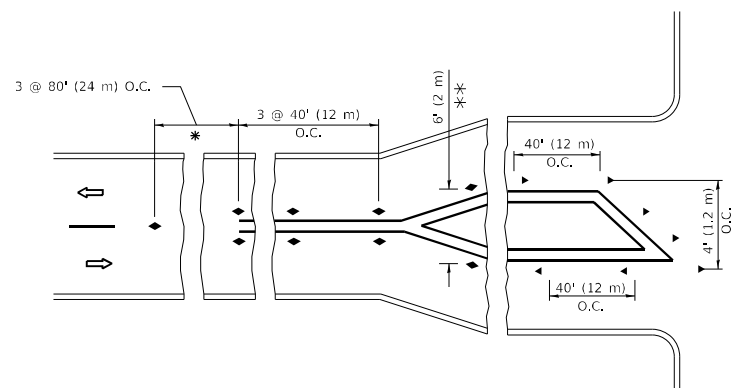
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

**LANE MARKER NOTES**

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

**DESIGN NOTES**

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



**TURN LANES**

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

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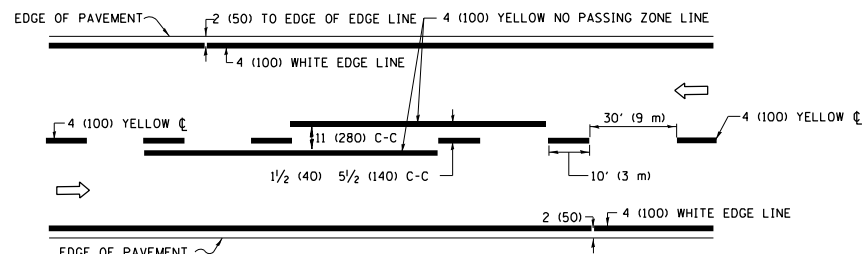
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PLOT DATE = 3/25/2022	CHECKED -	REVISED - C. JUCIUS 09-09-09
	DATE -	REVISED - C. JUCIUS 07-01-13

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

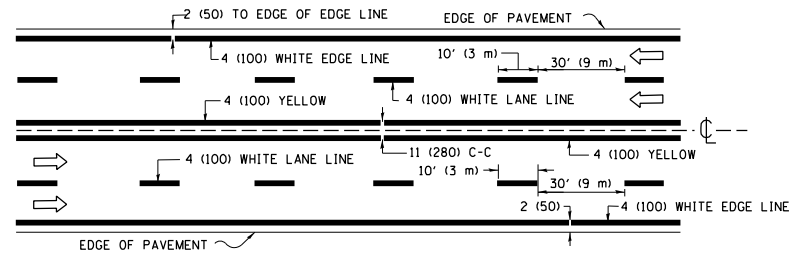
**TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. 19+92 TO STA.

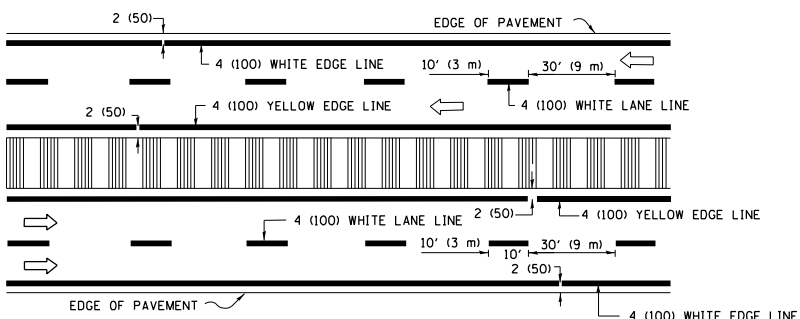
O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-RS-SW&SR	COOK	19	14
TC-11			CONTRACT NO. 62L11	
ILLINOIS FED. AID PROJECT				



**2-LANE ROADWAY**

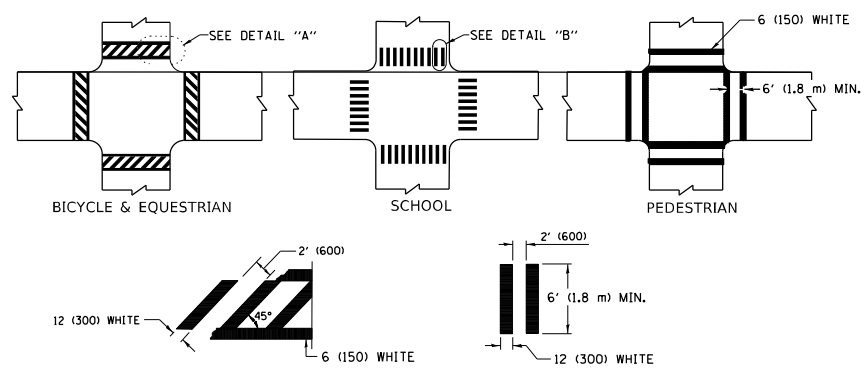


**MULTI-LANE UNDIVIDED**



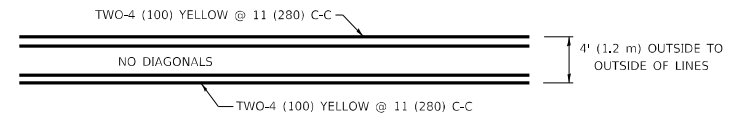
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

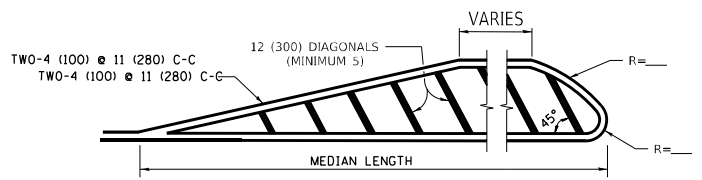


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



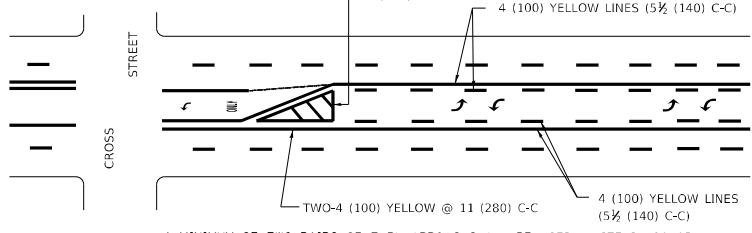
**4' (1.2 m) WIDE MEDIANS ONLY**



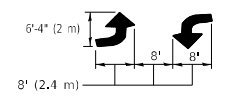
**MEDIANS OVER 4' (1.2 m) WIDE**

FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

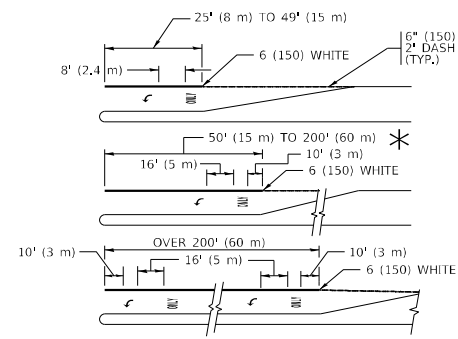


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

**TYPICAL PAINTED MEDIAN MARKING**

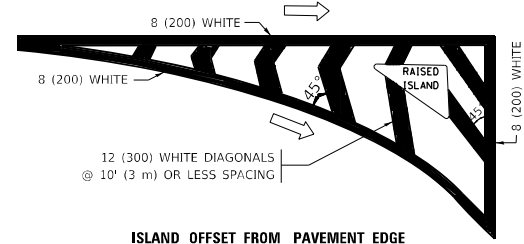


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

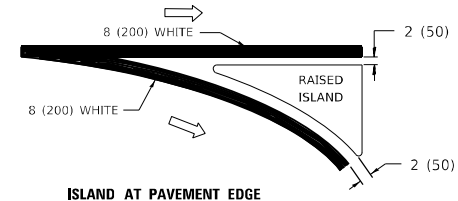
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

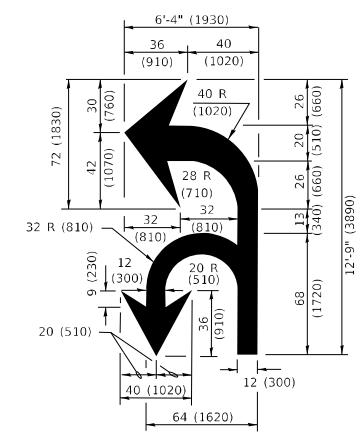
**TYPICAL TURN LANE MARKING**



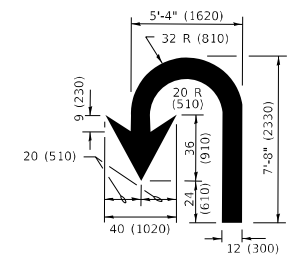
**ISLAND OFFSET FROM PAVEMENT EDGE**



**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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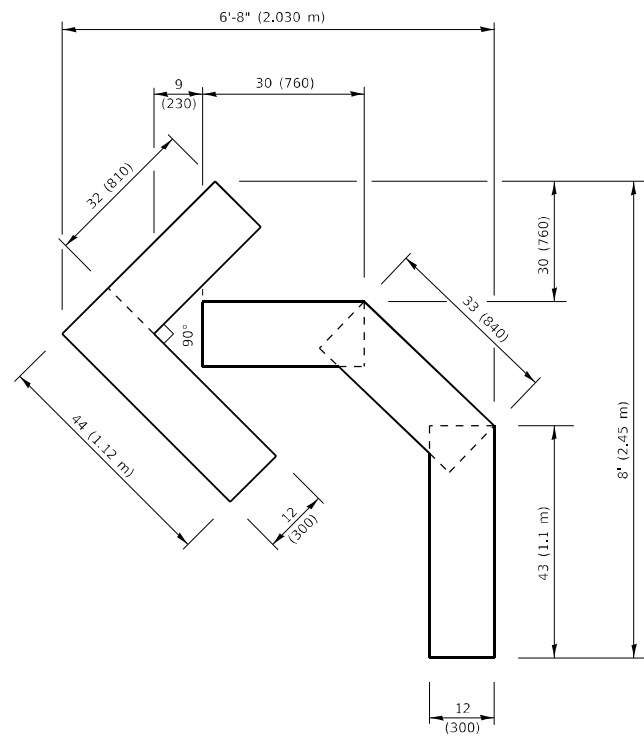
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PLOT DATE = 3/25/2022	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

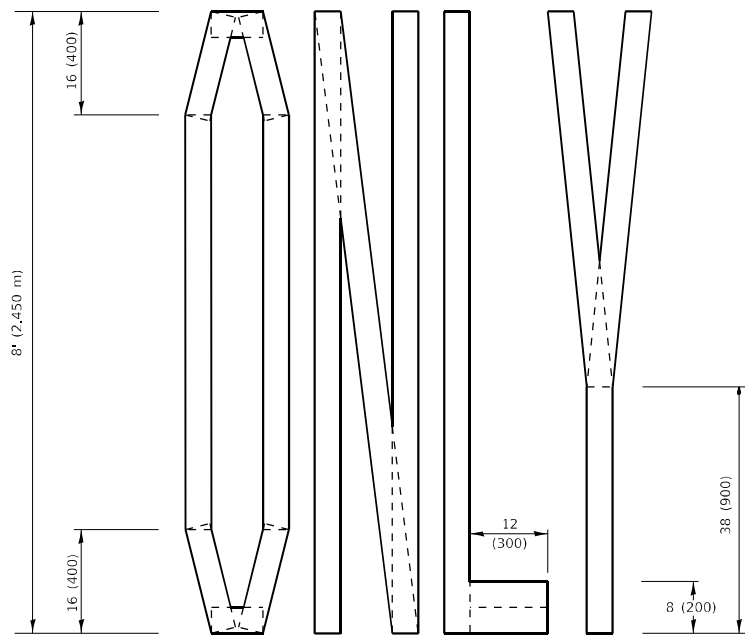
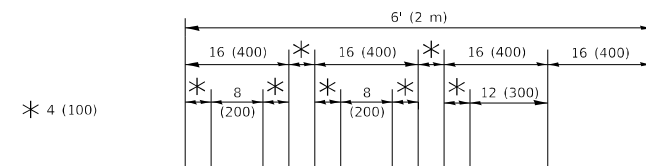
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O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-RS-SW&SR	COOK	19	15
<b>TC-13</b>		<b>CONTRACT NO. 62L11</b>		
ILLINOIS		FED. AID PROJECT		



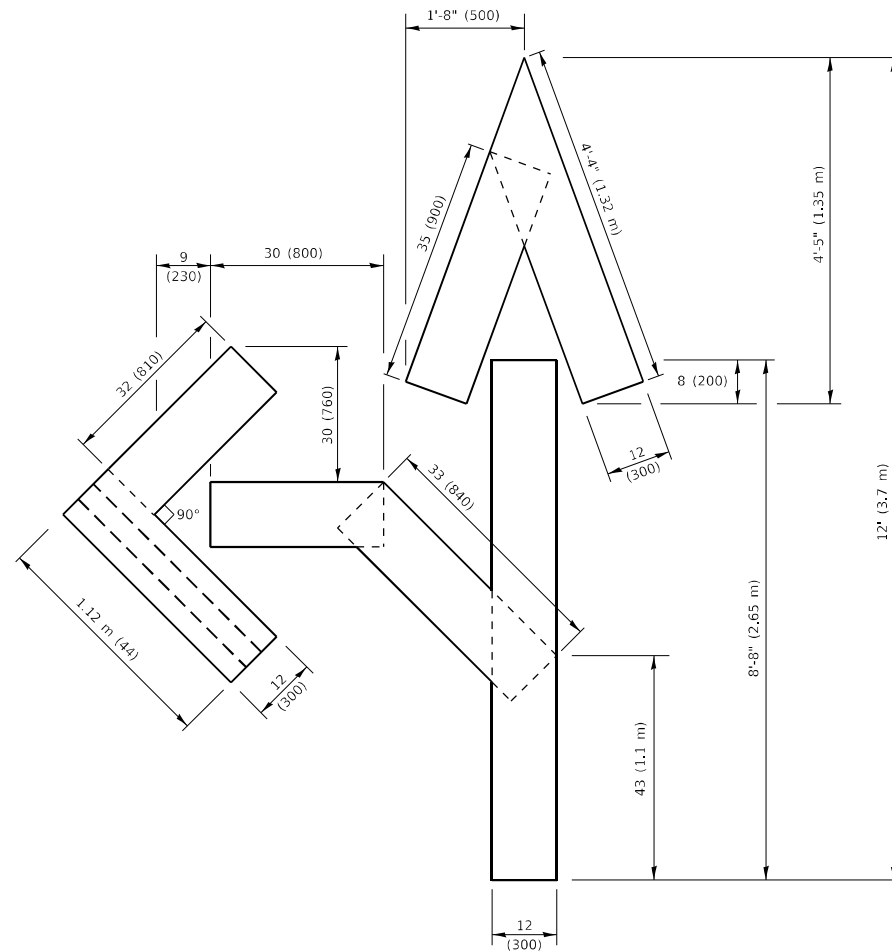
**QUANTITY**

4 (100) LINE = 45.5 ft. (13.9 m)  
15.2 sq. ft. (1.41 sq. m)



**QUANTITY**

4 (100) LINE = 64.1 ft. (19.5 m)  
21.4 sq. ft. (1.99 sq. m)

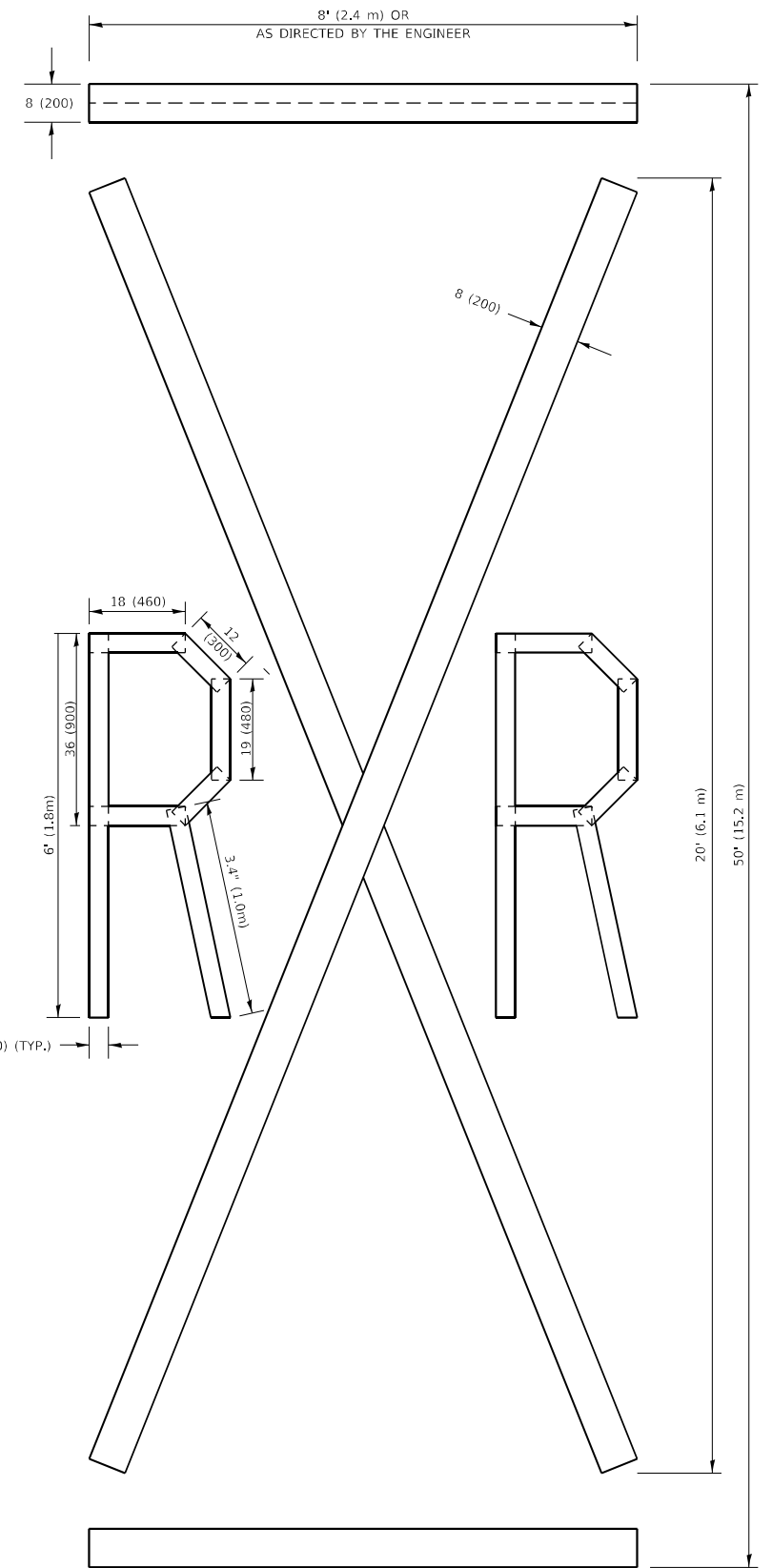


**QUANTITY**

4 (100) LINE = 82.5 ft. (25.1 m)  
27.5 sq. ft. (2.53 sq. m)

**NOTE:**

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**

4 (100) LINE = 225.9 ft. (68.9 m)  
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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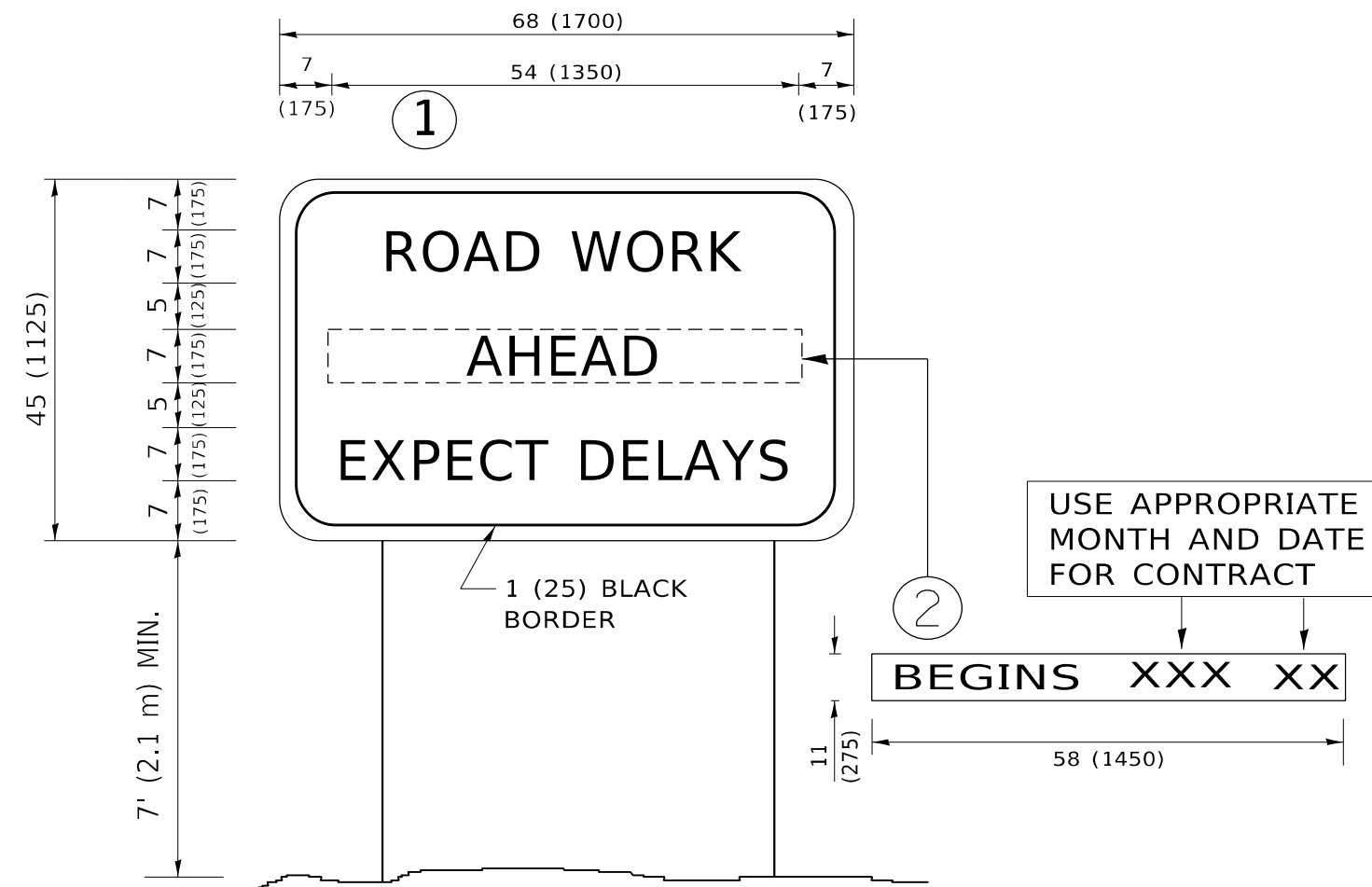
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PLOT DATE = 3/25/2022	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. 19+92 TO STA. 45+60

O.R. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-R5-SW&5R	COOK	19	16
<b>TC-16</b>		CONTRACT NO. 62L11		
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

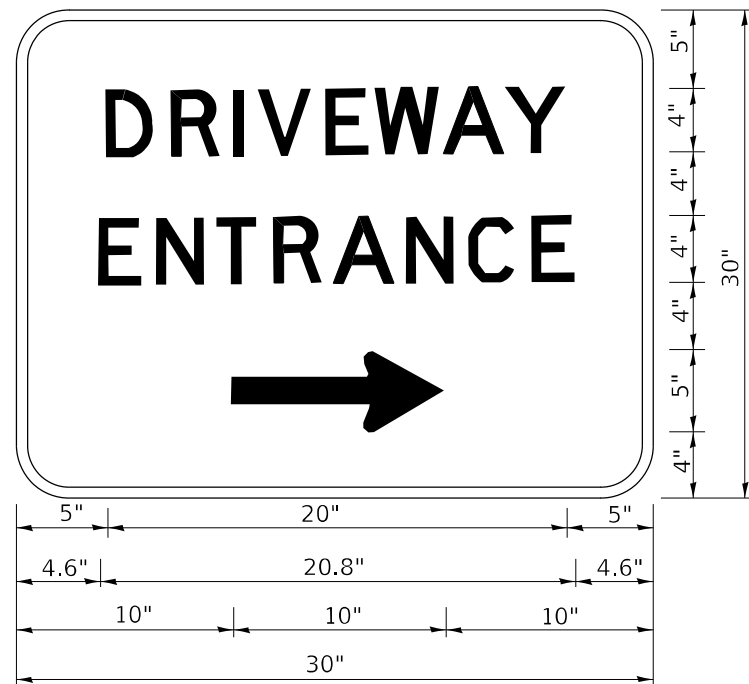
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	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 3/25/2022	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>ARTERIAL ROAD INFORMATION SIGN</b>			
SCALE: NONE	SHEET 1	OF 1 SHEETS	TO STA. 45+60

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-R5-SW&SR	COOK	19	17
<b>TC-22</b>		CONTRACT NO. 62L11		
ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

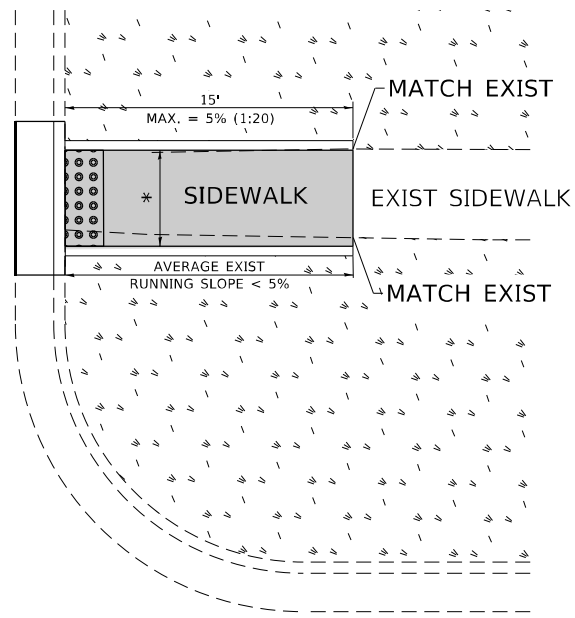
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. 19+92 TO STA. 45+60

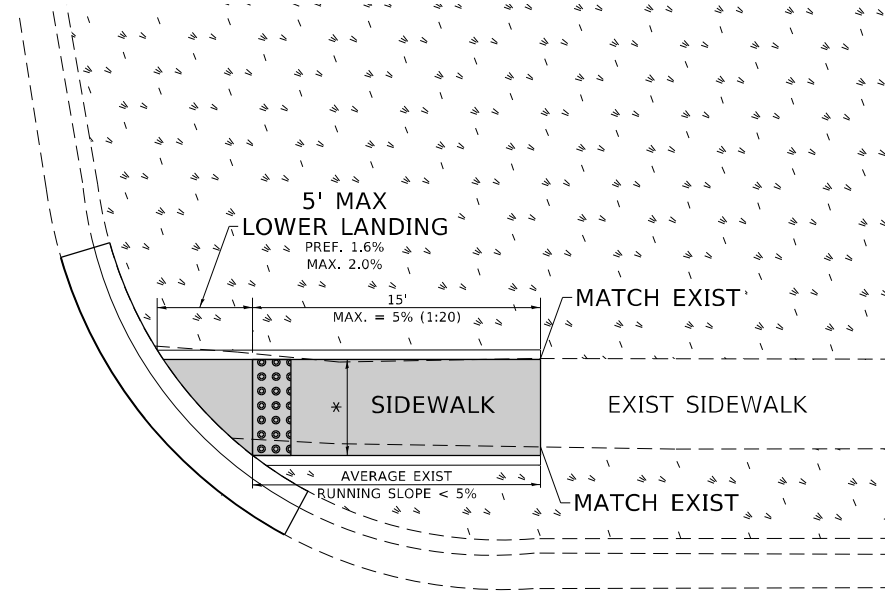
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42	2020-039-R5-SW&SR	COOK	19	18
<b>TC-26</b>			CONTRACT NO. 62L11	
ILLINOIS FED. AID PROJECT				

# ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

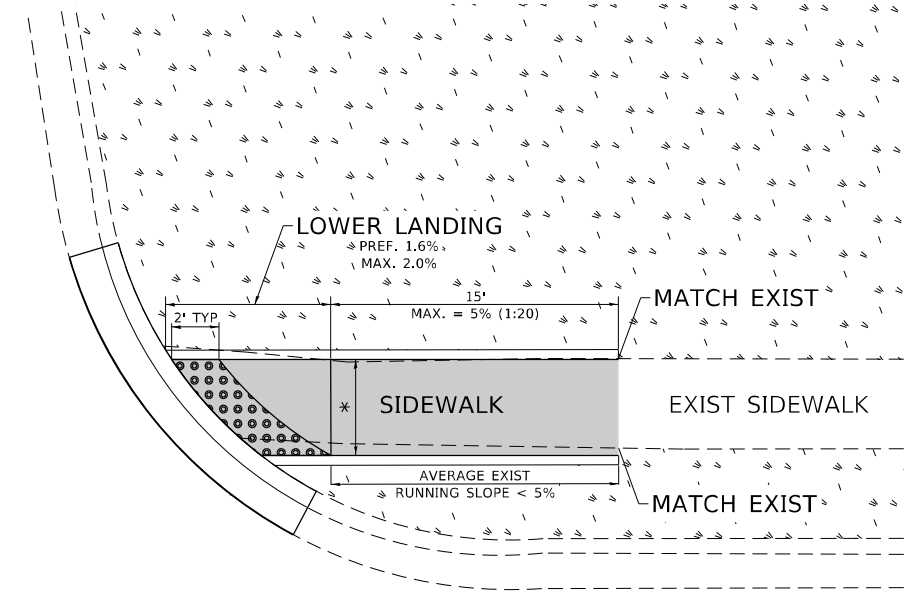
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
**PD-01B**



**PD-01C**



**LEGEND**

-  PROPOSED SIDE CURB
-  EXIST. GRASS
-  PROPOSED SIDEWALK
-  DETECTABLE WARNINGS

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

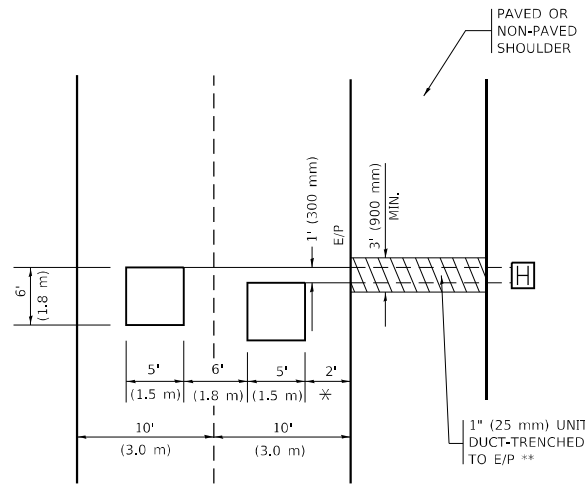
**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS  
(PD-01)**

SCALE: NONE SHEET OF SHEETS STA. 19+92 TO STA. 45+60

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-R5-SW&SR	COOK	19	19
PD-01			CONTRACT NO. 62L11	
ILLINOIS FED. AID PROJECT				

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

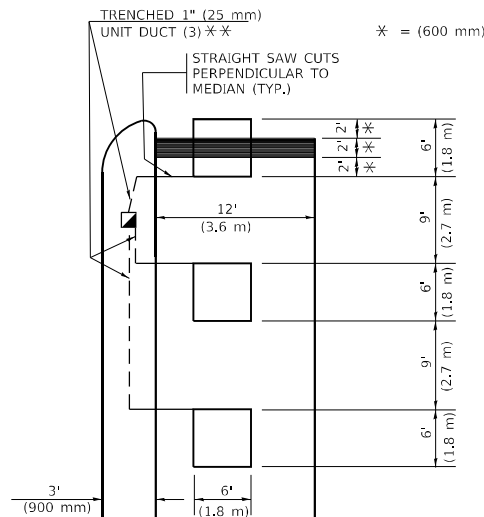


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS**  
**VOLUME DENSITY ("FAR OUT" DETECTION)**  
**ON SAME APPROACH**  
**(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

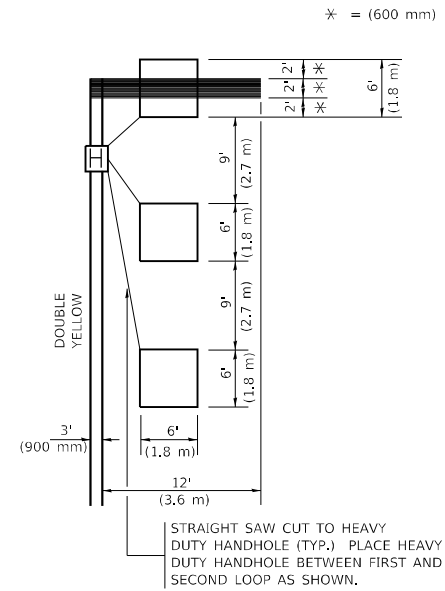


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS**  
**VOLUME DENSITY ("FAR OUT" DETECTION)**  
**ON SAME APPROACH**  
**(PROTECTED / PERMITTED LEFT TURN PHASING)**



\* = (600 mm)

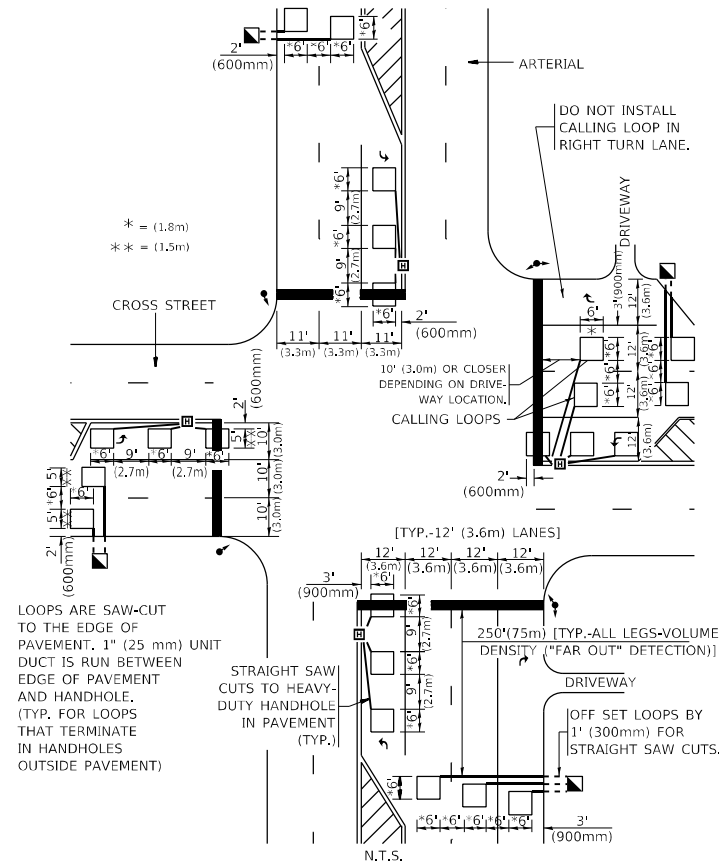
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

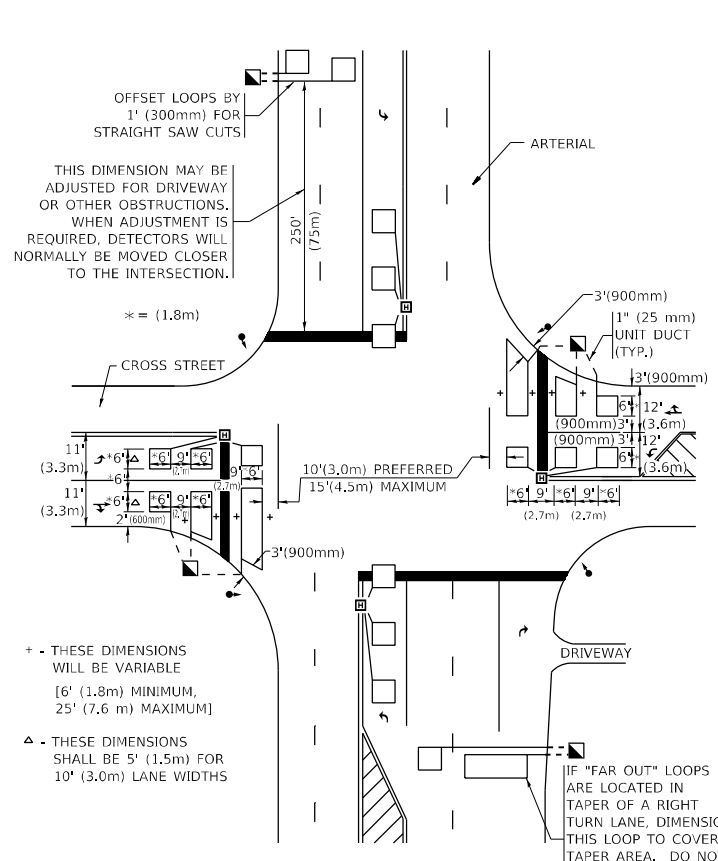
**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)**  
**CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)**



\* = (1.8m)  
 \*\* = (1.5m)

**DETAIL 1**  
 N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)**  
**CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

+ THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]  
 Δ THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

**DETAIL 2**  
 N.T.S.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE: ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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PLOT DATE = 4/29/2022	CHECKED - R.K.F.	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION**  
**DETAILS FOR ROADWAY RESURFACING**

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
42	2020-039-R5-SW&SR	COOK	19	19A
TS-07		CONTRACT NO. 62L11		
SCALE: NONE		ILLINOIS FED. AID PROJECT		

SCALE: NONE SHEET 1 OF 1 SHEETS STA. 19+92 TO STA. 45+60