

034

F.A.P.	SECTION	COUNTY	O.A. SHEETS	SHEET
693	(L)RS-2	PEORIA	78	1
IL RTE 9		ILLINOIS	CONTRACT NO. 68D88	

INDEX OF SHEETS 07-12-2019 LETTING ITEM 034

- 1 COVER SHEET
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- 4-7 SUMMARY OF QUANTITIES
- 8-11 TYPICAL SECTIONS & SPECIAL DETAIL
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- 21-28 DISTRICT CADD STANDARDS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 693 IL ROUTE 9
SECTION (L)RS-2
PROJECT NHPP - WMT4(428)
TYPE of IMPROVEMENT: RESURFACING
PEORIA COUNTY
C-94-075-17

D-94-041-17



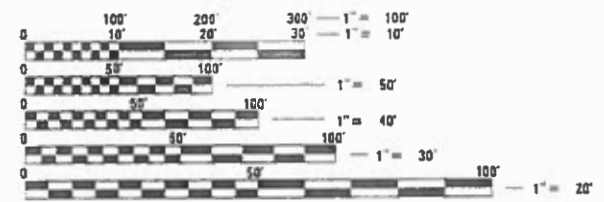
HIGHWAY STANDARDS

- 701426-09 701421-08
- 701602-08/10 701901-06 8
- 442001-04 442201-03
- 780001-05 781001-04
- 642006

CADD STANDARDS

- 406101-D4 440001-D4
- 630101-D4 780001-D4

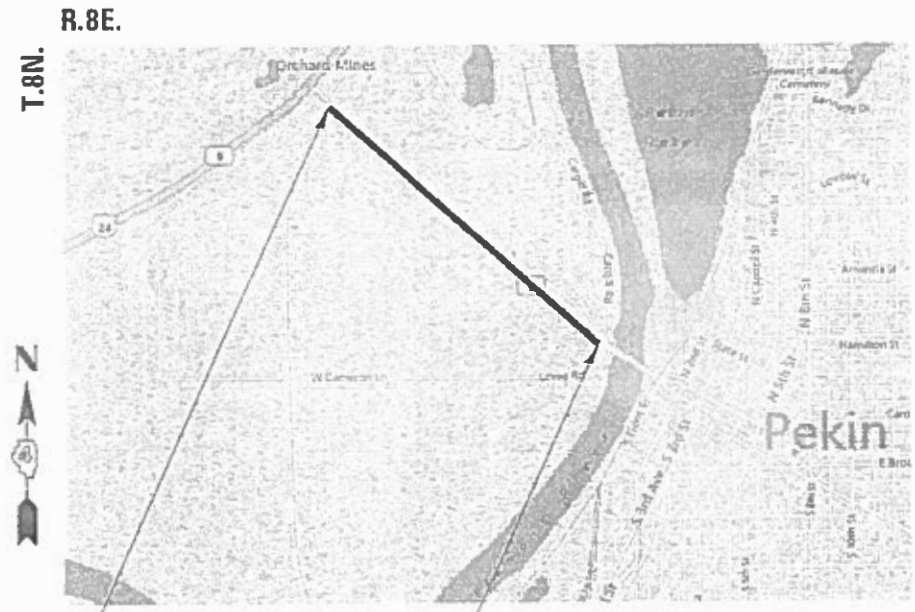
ADT = 14,600
MU = 5.3%



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: SAMPSON ADADE 309-671-3467
PROJECT MANAGER: CHRISTOPHER MAUSHARD 309-671-3453
CONTRACT NO. 68D88
CATALOG NO. 035498-00D



IMPROVEMENT BEGINS
STA. 20+20 LT
STA. 22+76 RT

IMPROVEMENT ENDS
STA. 78+20

DESCRIPTION OF WORK:
RESURFACING ON IL 9 FROM US 24 TO THE McNAUGHTON BRIDGE

GROSS LENGTH = 5,800 FT. = 1.10 MILE
NET LENGTH = 5,800 FT. = 1.10 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED May 31 2019
Kensil A. Barnett RSO
REGION THREE ENGINEER

June 14 2019
[Signature]
ENGINEER OF DESIGN AND ENVIRONMENT

June 14 2019
[Signature]
DIRECTOR OF HIGHWAY PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

AVAILABILITY OF ELECTRONIC FILES

Micro Station and GEOPAK files of this project will be made available to the Contractor after contract award. If there is a conflict between the electronic files and the printed contract plans and documents, the printed contract plans and documents shall take precedence over the electronic files. The Contractor shall accept all risk associated with using the electronic files and shall hold the Department harmless for any errors or omissions in the electronic files and the data contained therein. Errors or delays resulting from the use of the electronic files by the Contractor shall not result in an extension of time for any interim or final completion date or shall not be considered cause for additional compensation. The Contractor shall not use, share, or distribute these electronic files except for the purpose of constructing this contract. Any claims by third parties due to use or errors shall be the responsibility of the Contractor. The Contractor shall include this disclaimer with the transfer of these electronic files to any other parties and shall include appropriate language binding them to similar responsibilities.

ENVIRONMENTAL REVIEWS

Prior to the use of any proposed borrow areas, use areas (temporary access roads, detours, run-arounds, etc.) and/or waste areas, the Contractor shall file the required environmental resource request surveys according to Section 107.22 of the Standard Specifications. These surveys are required in order for the Department to conduct cultural and biological resource surveys for the proposed site.

Prior to any waste materials being removed from the construction site the required environmental resource surveys will need to be obtained and filed by the Contractor. Excess waste products removed from the construction site shall be disposed of as required in Section 202.03 of the Standard Specifications.

Any protruding metal bars shall be removed prior to the disposal of broken concrete at approved disposal sites.

The required environmental resource documentation shall include the following:

- * BDE Form 2289 (Environmental Survey Request)
- * BDE Form 2290 (Waste/Use Area Review)
- * A location map showing the size limits and location of the use area
- * Color photographs depicting the use area
- * Borrow Area Entry Agreement form-D4 PI0101

Please note that a minimum of four weeks shall be allowed for the District to obtain the required environmental clearances and six weeks for the required borrow site environmental clearances.

COMMITMENTS

Commitments are not to be altered without the written approval of all parties to which the commitment was made.
NO COMMITMENTS WERE MADE

PAVEMENT STATIONING NUMBERS & PLACEMENT

The Contractor shall provide labor and materials required to imprint pavement station numbers in the finished surface of the pavement and/or overlay. The numbers shall be approximately 3/4 inch (20mm) wide, 5 inches (125 mm) high and 5/8 inch (15 mm) deep.

The pavement station numbers shall be installed as specified herein:

Interval – 200 feet (English stationing) or 100 meters (metric stationing)

Bottom of Numbers – 6 inches (150 mm) from the inside edge of the pavement marking

Location:

- * 2,3, & 5 Lane Pavements – right edge of pavement in direction of increasing stations
- * Multi-Lane Divided Roadways – outside edge of pavement in both directions
- * Ramps – along baseline edge of pavement

Position – stations shall be placed so they can be read from the adjacent shoulder

Format – English (Metric) pavement stations shall use this format “XXX (XX + X00)” where X represents the pavement station

This work will not be paid for separately, but will be considered included in the cost of the associated pavement and/or overlay pay items.

BUTT JOINT CUTTING TIME RESTRICTION

Butt joints shall not be milled more than three (3) days prior to placement of the HMA surface course.

PAVING SURFACE COURSE

Continuous paving operations on the main roadway shall be maintained at all times during the construction of the hot-mix asphalt surface. No interruptions for side roads, entrances, turn lanes, etc. will be allowed.

CROSSING EXISTING STRUCTURES WITH EQUIPMENT

The following structure, SN 090-0114, may be crossed with the empty MTD. Any structures not listed above shall be verified by the resident prior to beginning work.

TRANSITION PAYMENT METHOD – NEW/OLD CONSTRUCTION

Three meter (10 ft.)(3m) transitions shall be used to match proposed items of work to existing items in the field unless otherwise shown. The transition shall be paid for at the contract unit price for the proposed item of work specified.

USER NAME = eliasgs	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL, JOB SPECIFIC NOTES, & COMMITMENTS	F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	DRAWN -	REVISED -			693	(L)RS-2	PEORIA	28	2	
PLOT SCALE = 115.0050' / in.	CHECKED -	REVISED -			IL RTE 9	CONTRACT NO. 68D88				
PLOT DATE = 5/30/2019	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO STA.

POLYMERIZED BITUMINOUS MATERIALS (TACK COAT) RATES

Surface Type	Residual Rate
Milled (HMA or PCC)	0.08 lb /sq ft
Existing Pavement	0.08 lb /sq ft
Fog Coat (between lifts)	0.08 lb /sq ft

The following mixture requirements are applicable for this project:

Mixture Use(s):	SMA Polymer Surface 1.75"	Polymer Binder 2"	Shoulders	Shoulder Inlay	Incidental Surface (Side Road)		
AC/PG:	SBS or SBR 76-28	SBS or SBR 76-28	PG 64-22	PG 64-22	PG 64-22		
Design Air Voids:	4.0% @ N=80	4.0% @ N=70	4.0% @ N=50	4.0% @ N=50	4.0% @ N=50		
Mixture Composition: (Mixture Gradation)	SMA 9.5	IL 9.5	IL 9.5	IL 9.5	IL 9.5		
Friction Aggregate:	Mix D	N.A.	Mix C	Mix C	Mix C		
Quality Management Program:	QCP	QCP	QCQA	QCQA	QCQA		

Note: 1) Individual lift thickness of each mix type will be no less than 3 times nominal maximum aggregate size and no more than 6 times nominal maximum aggregate size, unless otherwise approved by the Engineer.
 2) For design purposes, mixture weight for all mixes is determined to be 112.0 lb/s.y./in., unless otherwise noted.
 3) Sublot sizes for PFP and QCP mixes will be **600** tons, unless otherwise agreed to by the Engineer and the paving contractor.

JOB SPECIFIC NOTE

Two (2) changeable message signs shall be place five days prior to the start of construction. One for each direction of travel lane on IL 9 and this could be removed and used at the discretion of the Engineer for staging changes or any important message to be relayed to the travelling public. The cost of setting, removing, and maintaining shall be included in the pay item for a total of 20 days for the two changeable message signs.

USER NAME = eliasgs	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL, JOB SPECIFIC NOTES, & COMMITMENTS	F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 1:115,0050' / in.	DRAWN -	REVISED -			693	(L)RS-2	PEORIA	28	3
PLOT DATE = 5/30/2019	CHECKED -	REVISED -			IL RTE 9	CONTRACT NO. 68D88			
DATE -	REVISED -	SCALE:			SHEET	OF	SHEETS	STA.	TO
					ILLINOIS FED. AID PROJECT				

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONST. CODE	
				0005	URBAN
				PEORIA COUNTY	
				80% FED	20% STATE
40600295	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	POUND	75444	75444	
40600982	HOT - MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	320	320	
40600990	TEMPORARY RAMP	SQ YD	52	52	
40603208	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	TON	3518	3518	
40800050	INCIDENTAL HOT - MIX ASPHALT SURFACING	TON	88	88	
44000153	HOT - MIX ASPHALT SURFACE REMOVAL, 1"	SQ YD	4919	4919	
44000158	HOT - MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	16373	16373	
44000161	HOT - MIX ASPHALT SURFACE REMOVAL, 3"	SQ YD	235	235	
44200553	CLASS A PATCHES, TYPE II, 10 INCH	SQ YD	70	70	
44200557	CLASS A PATCHES, TYPE III, 10 INCH	SQ YD	42	42	
44200559	CLASS A PATCHES, TYPE IV, 10 INCH	SQ YD	140	140	
44200577	CLASS A PATCHES, TYPE II, 12 INCH	SQ YD	286	286	
44200581	CLASS A PATCHES, TYPE III, 12 INCH	SQ YD	84	84	
44200583	CLASS A PATCHES, TYPE IV, 12 INCH	SQ YD	238	238	

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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 5/31/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

F.A.P	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
693	(L)RS-2	PEORIA	28	4
IL RTE 9		CONTRACT NO. 68088		
ILLINOIS FED. AID PROJECT				

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONST. CODE	
				0005	URBAN
				PEORIA COUNTY	
				80% FED	20% STATE
44213000	PATCHING REINFORCEMENT	SQ YD	860	860	
44213200	SAW CUTS	FOOT	5479	5479	
44213204	TIE BARS 3/4"	EACH	50	50	
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	641	641	
48203100	HOT - MIX ASPHALT SHOULDERS	TON	6018	6018	
63301210	REMOVE AND RE-ERECT STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	365	365	
64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	21135	21135	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	2	2	
67100100	MOBILIZATION	L SUM	1	1	
70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	1	1	
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	5	5	
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	1991	1991	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	664	664	

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PLOT DATE = 5/31/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

F.A.P	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
693	ILRS-2	PEORIA	28	5
IL RTE 9		CONTRACT NO. 68088		
ILLINOIS FED. AID PROJECT				

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONST. CODE
				0005 URBAN PEORIA COUNTY 80% FED 20% STATE
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	125	125
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	33214	33214
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2789	2789
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	514	514
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1511	1511
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	17	17
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	429	429
X4060004	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80	TON	3078	3078
X4400196	HOT - MIX ASPHALT SURFACE REMOVAL, SPECIAL	SQ YD	1862	1862
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	14688	14688
X4421829	CLASS D PATCHES, TYPE III, 9' (SPECIAL)	SQ YD	64	64
7010725	CHANGEABLE MESSAGE SIGN	CAL DA	20	20
* X7830060	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS AND SYMBOLS	SQ FT	175	175
* X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	33214	33214

*= SPECIALTY ITEM

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PLOT DATE = 5/31/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

F.A.P	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
693	(L)RS-2	PEORIA	28	6
IL RTE 9		CONTRACT NO. 68088		

ILLINOIS FED. AID PROJECT

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CONST. CODE
0005
URBAN
PEORIA COUNTY
80% FED
20% STATE

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	
* X7830074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	2789	2789
* X7830076	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	514	514
* X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	1511	1511
* X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	17	17
XZ013798	CONSTRUCTION STATION LAYOUT	L SUM	1	1
Z0001002	GUARDRAIL AGGREGATE EROSION CONTROL	TON	79	79
Z0034105	MATERIAL TRANSFER DEVICE	TON	6596	6596

*= SPECIALTY ITEM

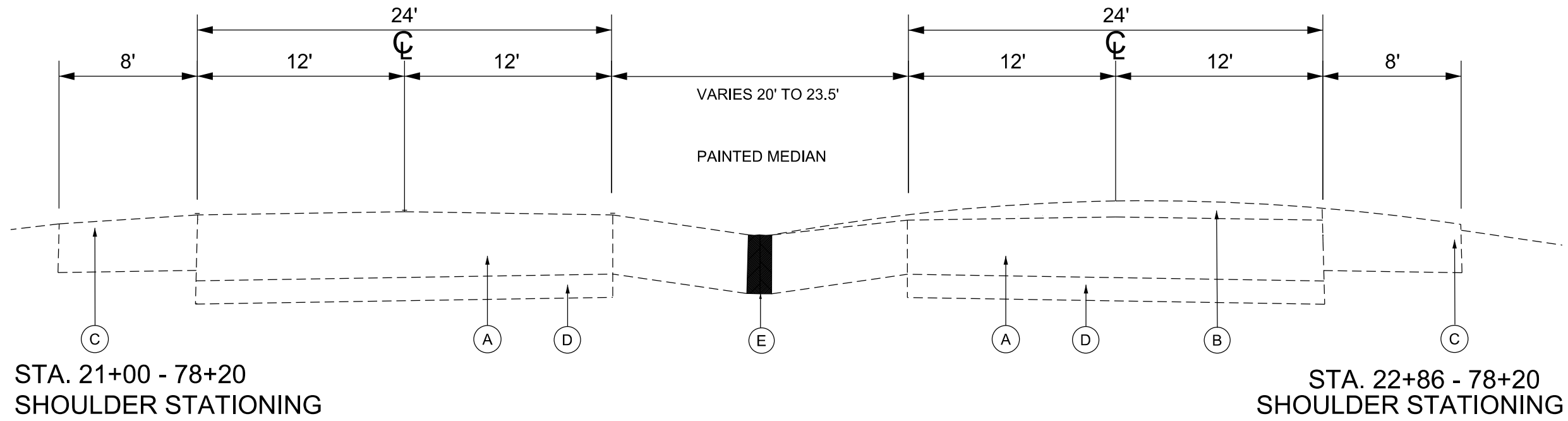
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
693	ILRS-2	PEORIA	28	7
IL RTE 9		CONTRACT NO. 68088		
ILLINOIS FED. AID PROJECT				

IL ROUTE 9



**TYPICAL SECTION #1 - EXISTING IL 9
STA. 20+90 TO STA. 77+25**

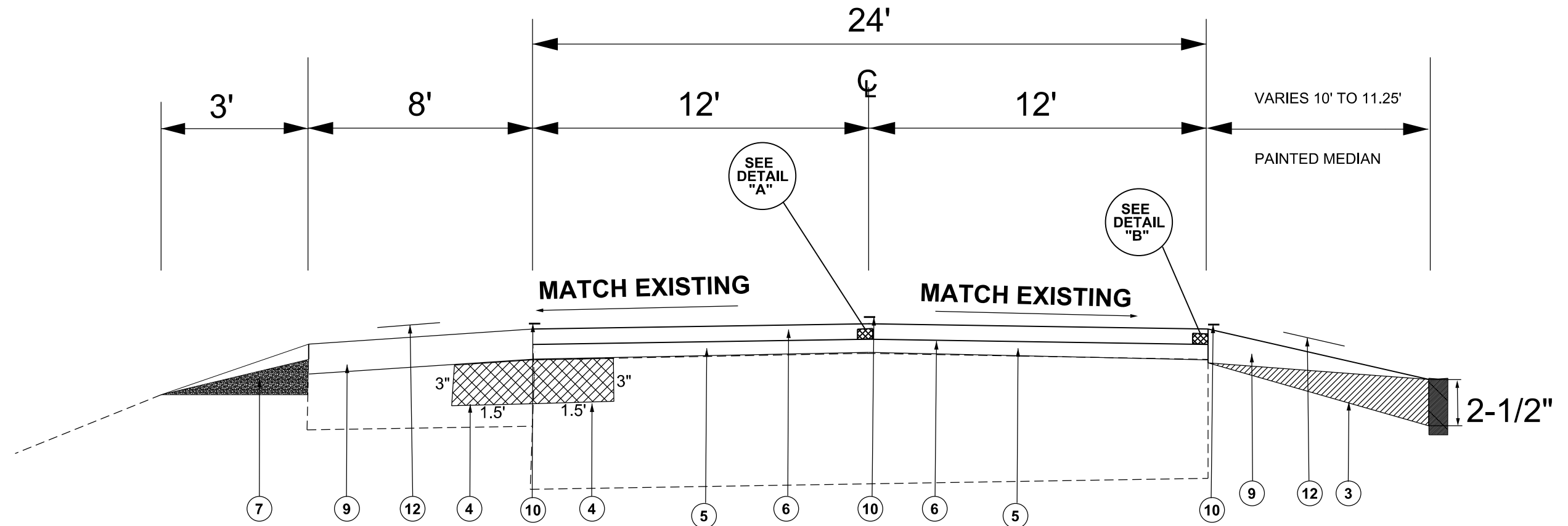
LEGEND - EXISTING

- (A) CRC PAVEMENT 10"
- (B) HMA OVERLAY 2" TO 2-1/4"
- (C) HMA SHOULDER
- (D) SUB-BASE GRANULAR MATERIAL 6"
- (E) SLOTTED DRAIN

NOT TO SCALE

USER NAME = eliasgs PLOT SCALE = 1/15,000 PLOT DATE = 5/30/2019		DESIGNED - DRAWN - CHECKED - DATE -		REVISED - REVISED - REVISED - REVISED -		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		IL ROUTE 9 TYPICAL SECTION		F.A.P. SECTION COUNTY TOTAL SHEETS SHEET NO. 693 (L)RS-2 PEORIA 28 8	
						SCALE: SHEET OF SHEETS STA. TO STA.		IL RTE 9		CONTRACT NO. 68D88 ILLINOIS FED. AID PROJECT	

IL ROUTE 9 WESTBOUND



STA. 21+00 TO STA. 78+20
SHOULDER STATIONING

TYPICAL SECTION #2 - PROPOSED WB IL9 STA. 20+20 TO STA. 77+25

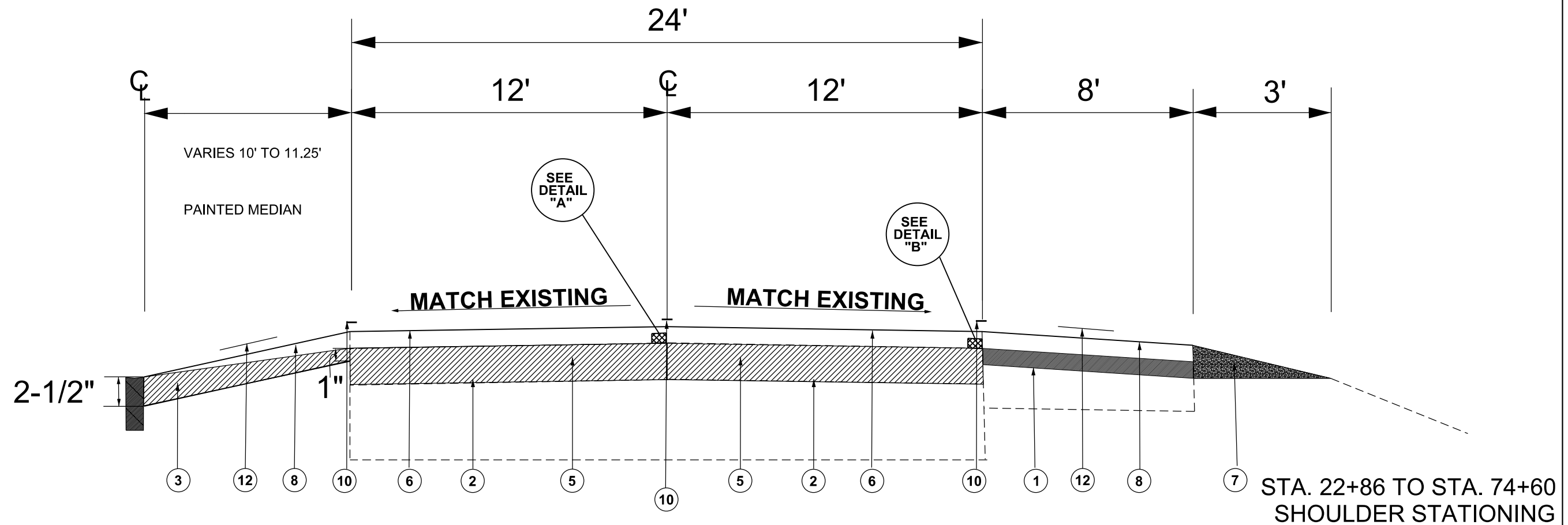
LEGEND - PROPOSED

- | | |
|--|---|
| ① HMA SURFACE REMOVAL, 1" | ⑦ AGGREGATE WEDGE SHLD., TYPE B |
| ② HMA SURFACE REMOVAL, 2 1/4" | ⑧ HOT-MIX ASPHALT SHOULDER 2- 1/2" |
| ③ HMA SURFACE REMOVAL, VAR. DEPTH | ⑨ HOT-MIX ASPHALT SHOULDER VAR 3 3/4" to 2 1/2" |
| ④ HMA SURFACE REMOVAL 3" | ⑩ PAVEMENT MARKING |
| ⑤ POLYMER BINDER 2"
SBS or SBR 76-28%@N70 | ⑪ GUARDRAIL AGGREGATE EROSION CONTROL |
| ⑥ SMA POLYMER SURFACE 1.75"
SBS or SBR 76-28%@N80 | ⑫ RUMBLE STRIPS |

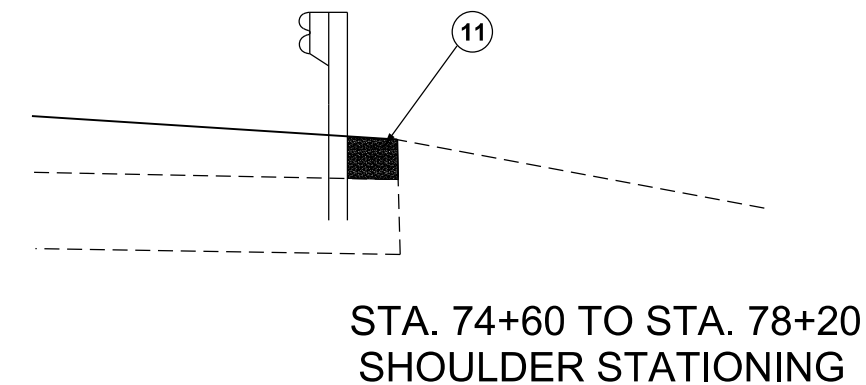
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PLOT SCALE = 1/16" = 115.0023' / in.	DRAWN -	REVISED -				693	(L)RS-2	PEORIA	28	9
PLOT DATE = 5/30/2019	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.		IL RTE 9		CONTRACT NO. 68D88		
	DATE -	REVISED -				ILLINOIS FED. AID PROJECT				

IL ROUTE 9 EASTBOUND



TYPICAL SECTION #3 - PROPOSED EB IL9
 STA. 22+76 TO STA. 77+25



LEGEND - PROPOSED

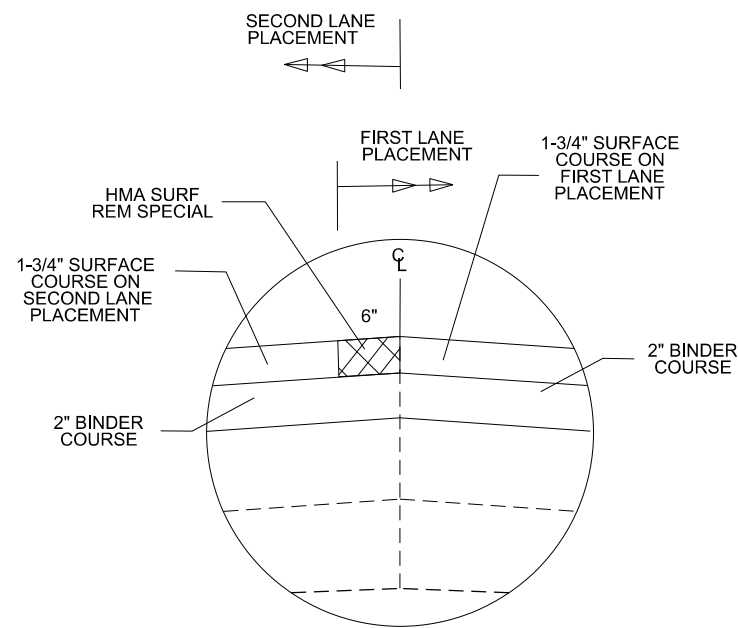
- ① HMA SURFACE REMOVAL, 1"
- ② HMA SURFACE REMOVAL, 2 1/4"
- ③ HMA SURFACE REMOVAL, VAR. DEPTH
- ④ HMA SURFACE REMOVAL 3"
- ⑤ POLYMER BINDER 2"
SBS or SBR 76-28%@N70
- ⑥ SMA POLYMER SURFACE 1.75"
SBS or SBR 76-28%@N80
- ⑦ AGGREGATE WEDGE SHLD., TYPE B
- ⑧ HOT-MIX ASPHALT SHOULDER 2- 1/2"
- ⑨ HOT-MIX ASPHALT SHOULDER VAR 3 3/4" to 2 1/2"
- ⑩ PAVEMENT MARKING
- ⑪ GUARDRAIL AGGREGATE EROSION CONTROL
- ⑫ RUMBLE STRIPS

*** THE EXSITING GUARDRAIL IS TO BE REMOVED AND RE-ADJUSTED IN HEIGHT TO COMPENSATE FOR THE ADDITIONAL PAVEMENT THICKNESS.**

NOT TO SCALE

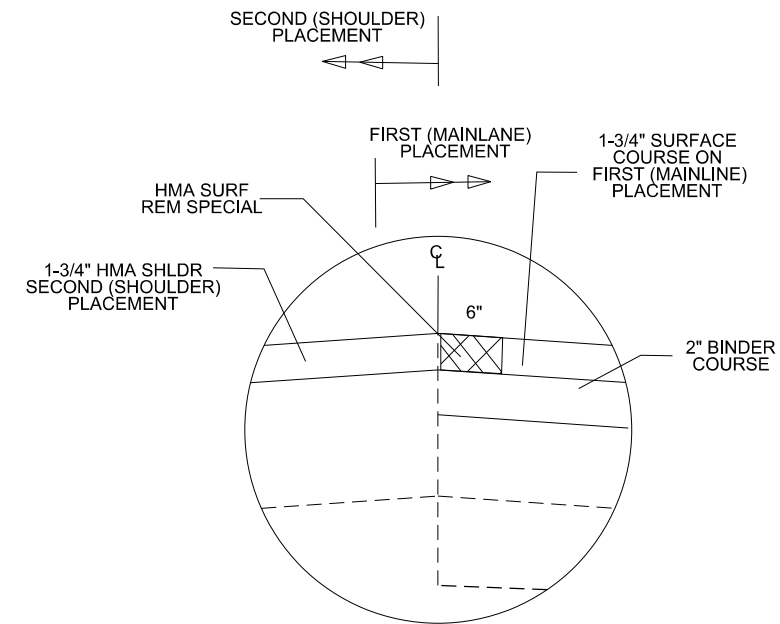
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DETAIL "A" - CENTERLINE & TURN LANES



SHOWING HMA SURFACE REMOVAL NEAR CENTERLINE
 SEE SPECIAL PROVISION FOR CONSTRUCTION
 SEQUENCE FOR MILLING AND PAVING

DETAIL "B" - EOP (ONLY WHERE SHOULDERS > 4')



SHOWING HMA SURFACE REMOVAL AT EDGE OF PAVEMENT (TYPICAL)
 SEE SPECIAL PROVISION FOR CONSTRUCTION
 SEQUENCE FOR MILLING AND PAVING

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PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 5/30/2019	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DETAIL A & B

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
693	(L)RS-2	PEORIA	28	11
IL RTE 9		CONTRACT NO. 68D88		
		ILLINOIS FED. AID PROJECT		

MILLING AND RESURFACING

LOCATION	STATION			LENGTH	WIDTH	AREA	HMA SURFACE REMOVAL					HMA SHOULDERS				SMA POLY SURF CSE, MIX "D", N80, 1-3/4"	POLY-BIT. MAT. (TACK CT)	MATERIAL TRANSFER DEVICE (MTD)	POLY BNDR (MM) IL-9.5, N70, 2" ,	INCIDENT HMA SURF 2-1/2"
							1"	3"	2.25"	VAR. DEPTH	SPECIAL	VAR.	3"	2-1/2"	1-3/4"					
							SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	TON	TON	TON	TON					
WB MAINLINE	20+00	TO	77+25	5,725	24	15,267		-	15,267	-			-	-	-	1,496	21,984	3,206	1,710	-
EB MAINLINE	22+76	TO	77+25	5,449	24	14,531		-	-	-			-	-	-	1,424	20,924	3,051	1,627	-
W. BOUND MEDIAN	21+00	TO	78+20	5,720	23.5	14,936					2,614						10,754			
E. BOUND MEDIAN	21+00	TO	77+25	5,625	23.5	14,688		-	-	14,688			-	2,056	-		10,575	-	-	-
WB SHOULDER	21+00	TO	78+20	5,720	8	5,084	-	-	-	-			-	-	498	-	3,661	-	-	-
EB SHOULDER	22+86	TO	78+20	5,534	8	4,919	4,919	-	-	-			-	689	-	-	3,542	-	-	-
EB TURNING LANE	59+01	TO	63+62	461	VARIES	475		-	475	-			-	-	-	47	684	100	53	-
SIDE ROAD	63+62	TO	64+71	58	VARIES	631		-	631	-			-	-	-	-	909	71	71	88
WB SHOULDER INLAY	63+62	TO	70+66	704	3	235		235	-	-			39	-	-	-	338	-	-	-
WB TURNING LANE	64+71	TO	69+32	461	VARIES	509		-	-	-			-	-	-	50	733	107	57	-
WB MILL WIDE PAVE WIDE SHLD & MEDIA	20+00	TO	77+25	5,725	1.0	636					636									
WB MILL WIDE PAVE WIDE CENTER LINE	20+00	TO	77+25	5,725	0.5	318					318					31	229	31		
EB MILL WIDE PAVE WIDE SHLD & MEDIA	22+76	TO	77+25	5,449	1.0	605					605									
EB MILL WIDE PAVE WIDE CENTER LINE	22+76	TO	77+25	5,449	0.5	303					303					30	218	30		
TOTAL							4,919	235	16,373	14,688	1,862	6,018				3,078	75,444	6,596	3,518	88

ENGINEER'S FIELD OFFICE, TYPE A	
LOCATION	CAL MO
JOBSITE	2
TOTAL	2

TRAFFIC CONTROL SURVEILLANCE	
LOCATION	CAL DAY
JOBSITE	5
TOTAL	5

CHANGEABLE MESSAGE SIGN	
LOCATION	CAL DA
JOBSITE	20
TOTAL	20

MOBILIZATION	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

GUARDRAIL AGGREGATE EROSION CONTROL			
LOCATION	STATION	TON	
EBL	74+55 TO 78+20	79	
TOTAL			79

CONSTRUCTION LAYOUT	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

AGGREGATE WEDGE SHOULDER, TYPE B			
LOCATION	STATION	TON	
EBL	22+86 TO 78+20	315	
WBL	21+00 TO 78+20	326	
TOTAL			641

PAVEMENT MARKINGS													
STATION			LENGTH		THERMOPLASTIC								
					4"			8"	12"	24"	LTR SYM AND NUM	6"	
					WHITE		YELLOW	WHITE	YELLOW	WHITE		WHITE	
					SOLID		SOLID	SOLID	SOLID	SOLID		SKIP	
LT	RT	DOUBLE	SOLID	SOLID	SOLID	LT	RT						
			FT	FT	FT	FT	FT	FT	FT	FT	FT		
20+20	TO	22+76	256	256	-	512	-	59	-	-	64	-	
22+76	TO	59+01	3,625	3,625	3,625	14,500	-	800	-	-	906	906	
59+01	TO	61+09	208	208	208	832	-	230	-	-	52	52	
61+09	TO	63+62	253	253	253	1,012	253	143	-	62.5	63	63	
63+62	TO	64+71	109	229	109	68	-	-	17	-	27	27	
64+71	TO	67+32	261	261	261	1,044	261	58	-	62.5	65	65	
67+32	TO	69+32	200	200	200	800	-	44	-	-	50	50	
69+32	TO	77+25	793	793	793	3,172	-	176	-	-	198	198	
SUBTOTAL				5,825	5,449	21,940	514	1,511	17	125	1,426	1,362	
TOTAL				33,214			514	1,511	17	125	2,789		

GROOVING FOR RECESSED PAVEMENT MARKINGS								
TYPE	5"			9"	13"	25"	LTR & SYM	7"
	FT			FT	FT	FT	SQ FT	FT
TOTAL	33,214			514	1,511	17	175	2,789

RUMBLE STRIPS (8")						
STATION			LT SHLD	LT MEDIAN	RT MEDIAN	RT SHLD
			FT	FT	FT	FT
21+00	TO	22+86	186	186	186	-
22+86	TO	59+01	3,615	3,615	3,615	3,615
59+01	TO	63+02	401	401	-	401
63+02	TO	64+71	-	-	-	169
64+71	TO	69+32	-	461	461	461
69+32	TO	77+25	793	793	793	793
77+25	TO	78+20	95	-	-	95
SUBTOTAL			5,090	5,456	5,055	5,534
TOTAL			21,135			

RAISED REFLECTIVE PAVEMENT MARKER					
LOCATION	STATION			CRYSTAL	AMBER
				EACH	EACH
WB MAINLINE	20+20	TO	63+62	55	109
	64+71	TO	77+25	16	32
EB MAINLINE	22+76	TO	63+62	52	103
	64+71	TO	77+25	16	32
EB TURN LN	61+09	TO	63+62	7	-
WB TURN LN	64+71	TO	67+32	7	-
SUBTOTAL				153	276
TOTAL				429	

CLASS A PATCHES									
LOCATION	CLASS A PATCHES 10 INCH			CLASS A PATCHES 12 INCH			SAW CUTS	TIE BARS 3/4"	PATCHING REINFORCEMENT
	TYPE II	TYPE III	TYPE IV	TYPE II	TYPE III	TYPE IV			
	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	FT	EACH	SQ YD
WB PASSING	21.3	20.0	80.0	-	-	-	470.0	-	121.3
WB DRIVING	45.3	20.0	53.3	-	-	-	562.0	-	118.6
EB PASSING	-	-	-	138.7	40.0	113.3	1398.0	23	292.0
EB DRIVING	-	-	-	133.3	40.0	113.3	2788.0	23	286.6
SUB-TOTAL	66.6	40.0	133.3	272.0	80.0	226.6	5218.0	46	818.5
5% WINTER BREAK UP	3.3	2.0	6.7	13.6	4.0	11.3	260.9	4	40.9
TOTAL	70	42	140	286	84	238	5479	50	860

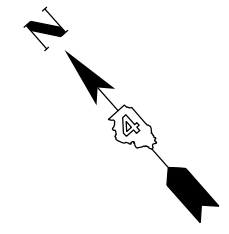
CLASS D PATCHES (AT REMOVAL OF WIDE FLANGE BEAM JOINTS)				
LOCATION	STATION	LENGTH	WIDTH	TYPE III, 9" (SPECIAL)
		FOOT	FOOT	SQ YD
WB	21+30	6	24	16.0
EB	68+30	6	24	16.0
WB	77+25	6	24	16.0
EB	77+25	6	24	16.0
TOTAL				64

SHORT TERM PAVEMENT MARKING				
STATION			MAINLINE	SHOULDER
			FT	FT
20+20	TO	78+20	1,094	897
TOTAL			1,991	

BUTT JOINTS AND TEMPORARY RAMPS					
LOCATION	STATION			BUTT JOINTS	TEMP RAMP
				SQ YD	SQ YD
START WBL	20+00	TO	21+00	267	45.0
START EBL	22+76	TO	23+46	187	32.0
END EBL	77+15	TO	77+25	27	13.0
END WBL	77+15	TO	77+25	27	13.0
TOTAL				508	103

SHORT TERM PAVEMENT MARKING REMOVAL				
STATION			MAINLINE	SHOULDER
			SQ FT	SQ FT
20+20	TO	78+20	365	299
TOTAL			664	

REMOVE AND REERECT SPB GUARDRAIL, TYPE A				
LOCATION	STATION			FT
EBL	74+55	TO	78+20	365
TOTAL				365



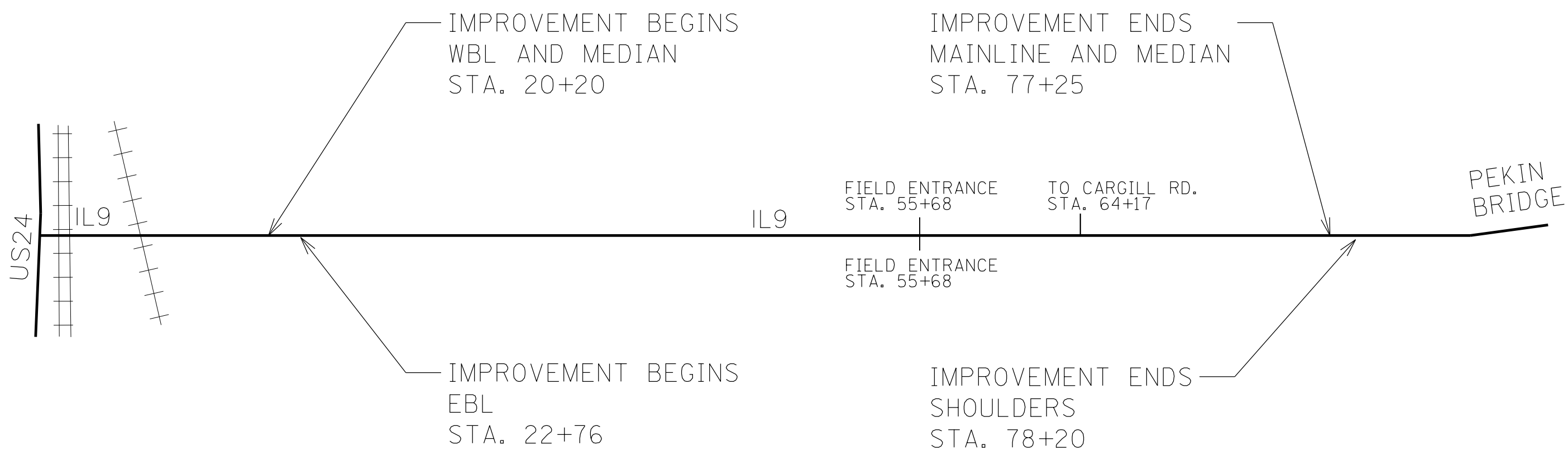
NOTES:

Project Speed Limit:
55 mph - Entire Project Limits

Significant Route Designation:
Entire Project Limits

Rural / Urban Split:
Urban - Entire Project Limits

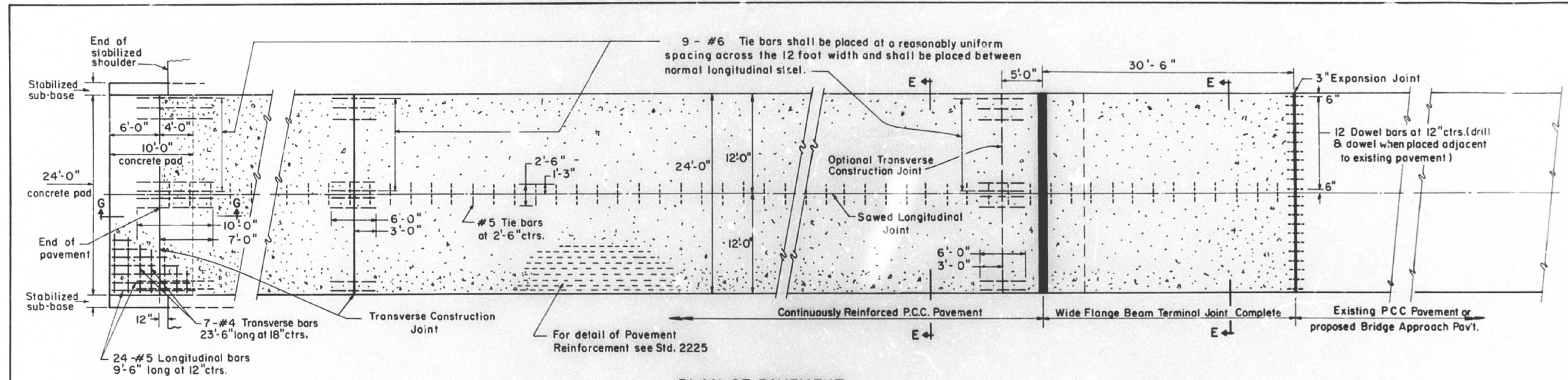
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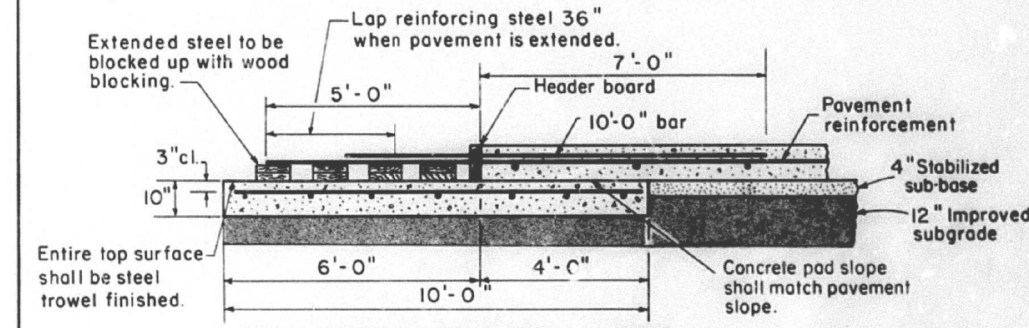
WIDE FLANGE BEAM TERMINAL JOINTS
 STA. 21+30 LT
 STA. 68+30 RT
 STA. 77+25 LT
 STA. 77+25 RT

NOT TO SCALE

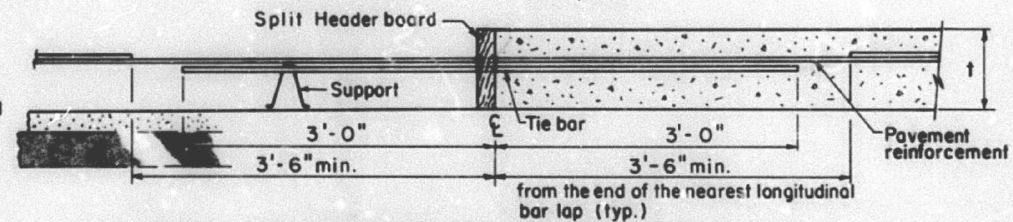
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Default	PLOT DATE = 5/30/2019	DATE -	REVISED -		IL RTE 9			CONTRACT NO. 68088								
ILLINOIS FED. AID PROJECT										SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.



PLAN OF PAVEMENT



TRANSVERSE TERMINAL JOINT SECTION G - G



DETAIL OF TRANSVERSE CONSTRUCTION JOINT

GENERAL NOTES

Details shown in Section G-G shall apply only at the end of the construction section. The 10-inch reinforced concrete pad, header board, wood blocking and the 5-feet of extended pavement reinforcement shall be included in the unit price bid each for the TRANSVERSE TERMINAL JOINT COMPLETE of the pavement width as shown on the plans.

Expansion joints and extra reinforcement in the pavement over concrete pads, sleeper slabs and at transverse construction joints shall be included in the price bid for C.R.P.C.C. PAVEMENT.

The concrete pavement on the expansion side of the wide flange beam shall be carefully finished at both the surface expansion joint trough and at the welded end plates to facilitate unrestrained pavement expansion.

When the Contractor places the reinforcement using a method which requires the tie bars for the longitudinal joint to be placed above the longitudinal reinforcement bars, the first three longitudinal bars on either side of the joint shall be placed such that the tie bars will be at the neutral axis.

Concrete for the Wide Flange Beam Terminal Joint sleeper slab and Transverse Terminal Joint concrete pad shall be either Class X or Portland Cement Concrete Pavement. It shall be placed in trench to the neat lines as shown. Forms will not be permitted. The concrete in the sleeper slab and concrete pad shall be cured in accordance with the methods specified for footings in Section 625 of the Standard Specifications, except that membrane curing will not be permitted for the sleeper slab.

The groove at the Wide Flange Beam Terminal Joint shall be sealed in accordance with the details shown. Sealant components shall be as follows or approved equals. Sealant shall be Dow Corning 888 Silicone Highway Joint Sealant. Tape shall be Polyethylene Tape No. 40. Primer, used on the metal only, shall be Dow Corning 1200. At the Contractor's option the joint may be sealed as shown in the optional groove detail.

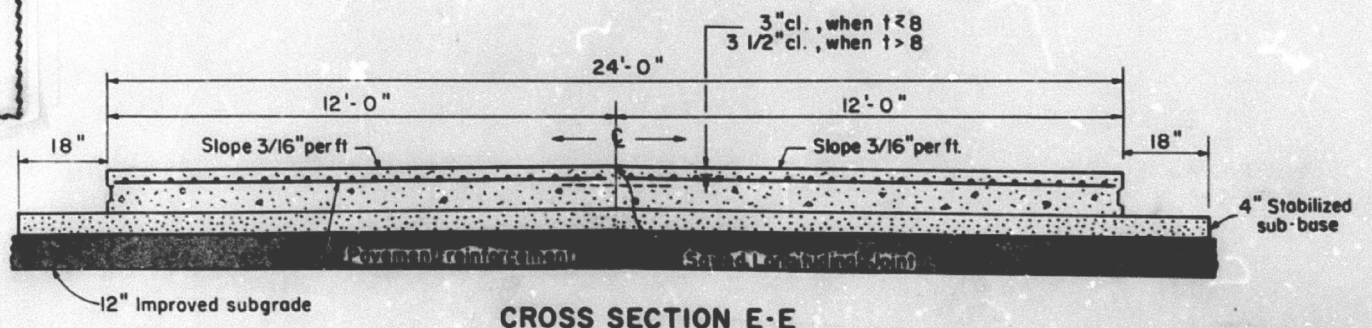
See Standard 2323 for details of joints and tie bars not shown.

DESIGN NOTES

When the slab length is less than 1500', only the 3" expansion joint should be used in lieu of the Wide Flange Beam Terminal Joint and expansion joint. When the slab length is between 1500' and 2000', the Bureau of Design shall be consulted for possible substitution of an expansion joint in lieu of the Wide Flange Beam Terminal Joint and expansion joint.

The length of the Wide Flange Beam Terminal Joint may be increased to a maximum length of 300' to avoid gore areas, ramps or other variable width pavement. Drawings showing the location, details and quantities shall be included in the plans.

Design Notes will not appear in the contract plans.



CROSS SECTION E-E

24 FT. - CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (WITH WIDE FLANGE BEAM TERMINAL JOINT) (Sheet 1 of 2)

STANDARD 2224-16

(FULL SIZE) D.W.N.5R

C-24-031

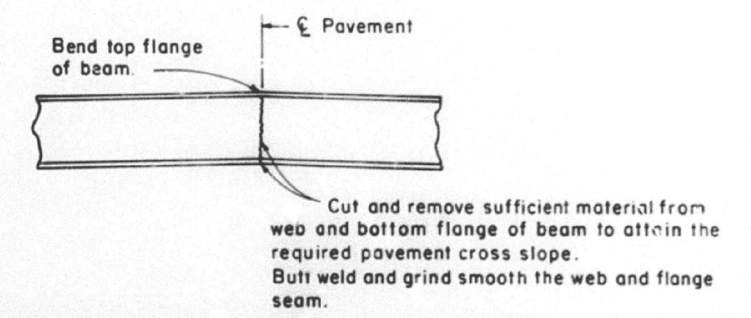
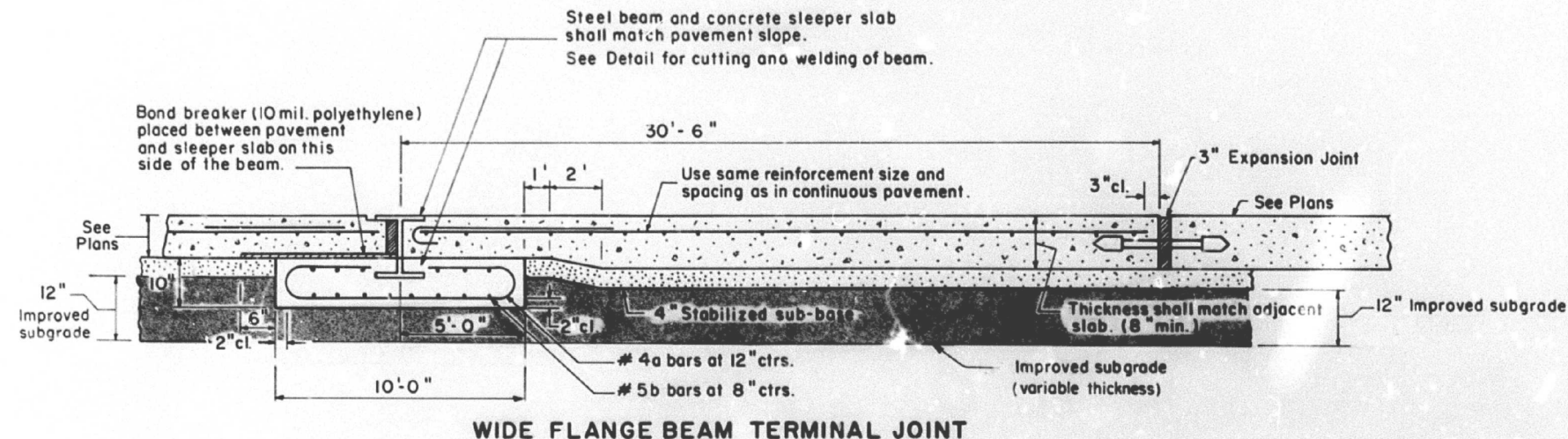
Illinois Department of Transportation

PASSED Mar. 19 1990
John E. Bass
Engineer of Policy and Procedures

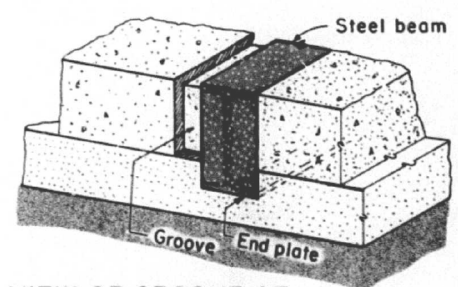
APPROVED Mar. 19 1990
D. H. Lewis
Engineer of Design

ISSUED 5-9-96

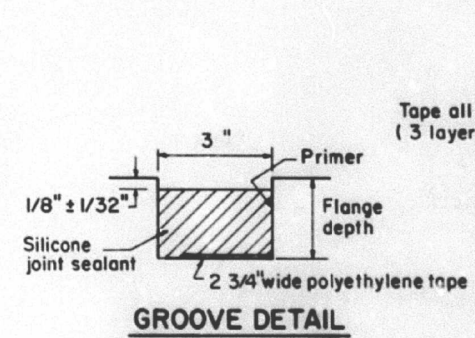
FILE NAME =	USER NAME = eliasg	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	WIDE FLANGE BEAM TERMINAL JOINTS FOR INFORMATION ONLY				F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
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		PLOT DATE = 5/30/2019	DATE -		REVISIONS								ILLINOIS FED. AID PROJECT			



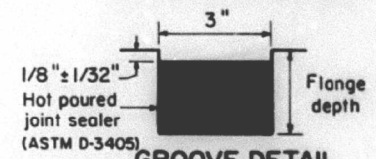
DETAIL OF CUTTING AND WELDING BEAM
 (After fabrication the entire beam assembly shall be galvanized in accordance with AASHTO M-111.)



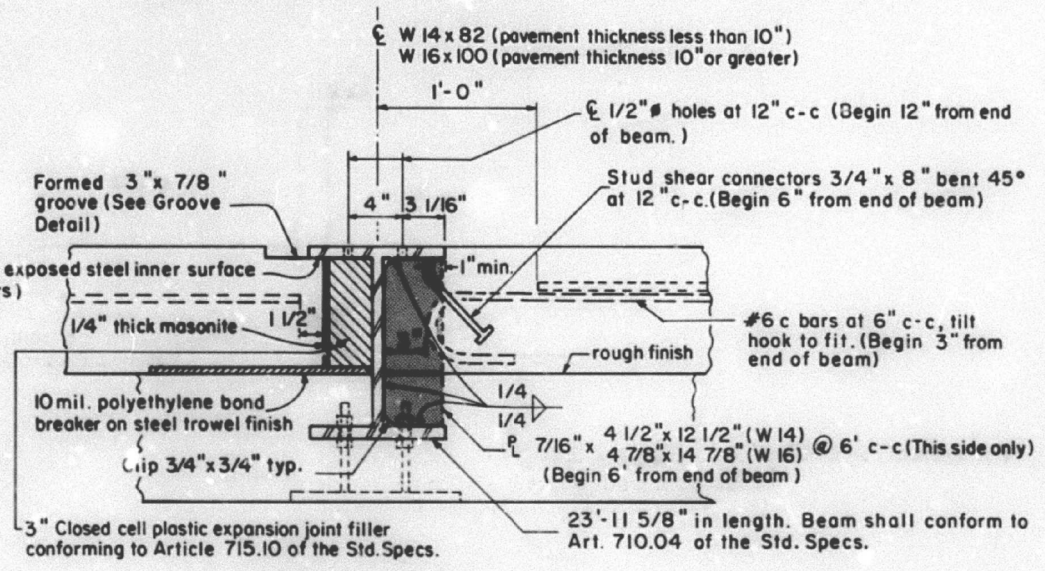
VIEW OF GROOVE AT EDGE OF PAVEMENT



GROOVE DETAIL



GROOVE DETAIL (OPTIONAL)



DETAIL AT BEAM

MATERIALS REQUIRED FOR (1) ONE WIDE FLANGE BEAM TERMINAL JOINT COMPLETE

Bar	No.	Size	Length	Shape
a	24	# 4	19'-0"	
b	29	# 5	23'-8"	
c	48	# 6	8'-6"	

PCC Pavt. or Class X Conc., Cu.Yds 7.4
 Reinforcement Bars, Lbs. 1630

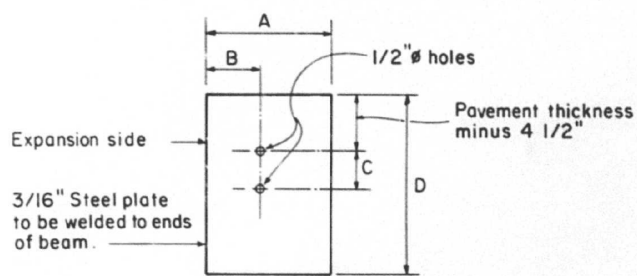
Structural Steel, Lbs. W 14 2030*
 W 16 2475*

* Weight includes beam, end plates, stiffener plates and studs.

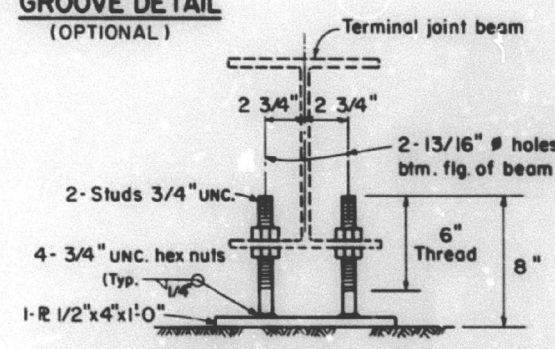
Pavement, Sq.Yds. 81.3
 Pavement Reinf., Sq.Yds. 81.3
 4" Stabilized Sub-base, Sq.Yds. 76.5
 Improved Subgrade, Sq.Yds. 76.5

MATERIALS REQUIRED FOR (1) ONE TRANSVERSE TERMINAL JOINT COMPLETE

PCC Pavt. or Class X Concrete, Cu.Yds.	7.4
Reinforcement Bars, Lbs.	348
Pavt. Reinforcement, Sq.Yds.	13.3

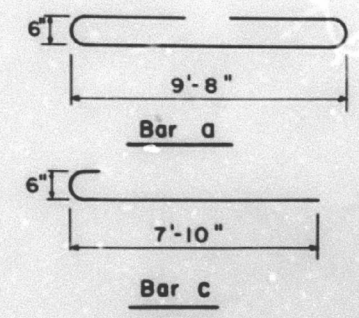


END PLATE



OPTIONAL ADJUSTABLE CHAIR

(The chair may be constructed of uncoated steel. These chairs, when used, should be located at approximate 6' centers beginning 3' from the end of the beam.)



Illinois Department of Transportation

PASSED Mar 19 1990
 John E. ...
 Engineer of Policy and Procedures

APPROVED Mar 19 1990
 ...
 Engineer of Design

ISSUED 5-9-96

BEAM SIZE		
DIM.	W 14	W 16
A	10 1/8"	10 3/8"
B	4 5/16"	4 7/16"
C	3"	4"
D	14 1/4"	17"

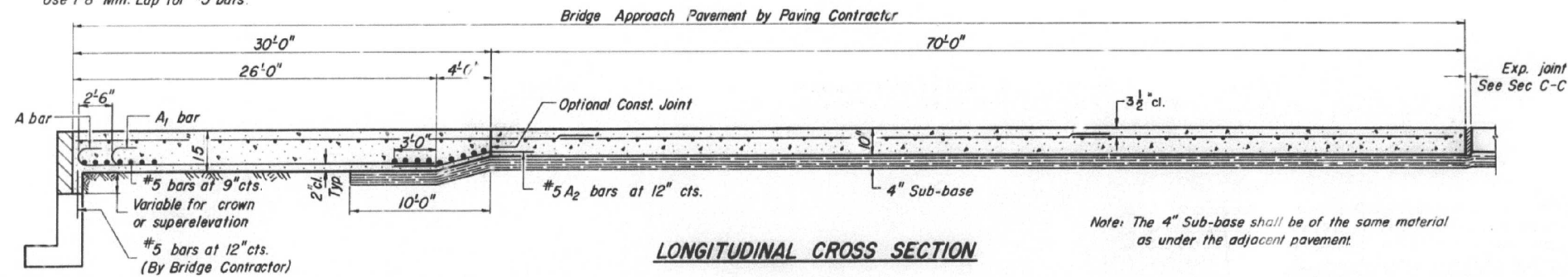
24 FT. - CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (WITH WIDE FLANGE BEAM TERMINAL JOINT) (Sheet 2 of 2)

STANDARD 2224-16

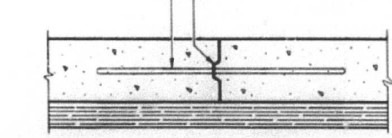
(FULL SIZE) DWWSR

C-2404

Note: Tilt hook of #9 bars for min. 3/2 cl. Use 1-4" Min. Lt. for #4 bars. Use 1-8" Min. Lap for #5 bars.

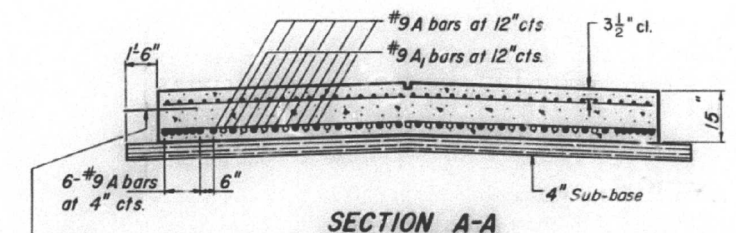
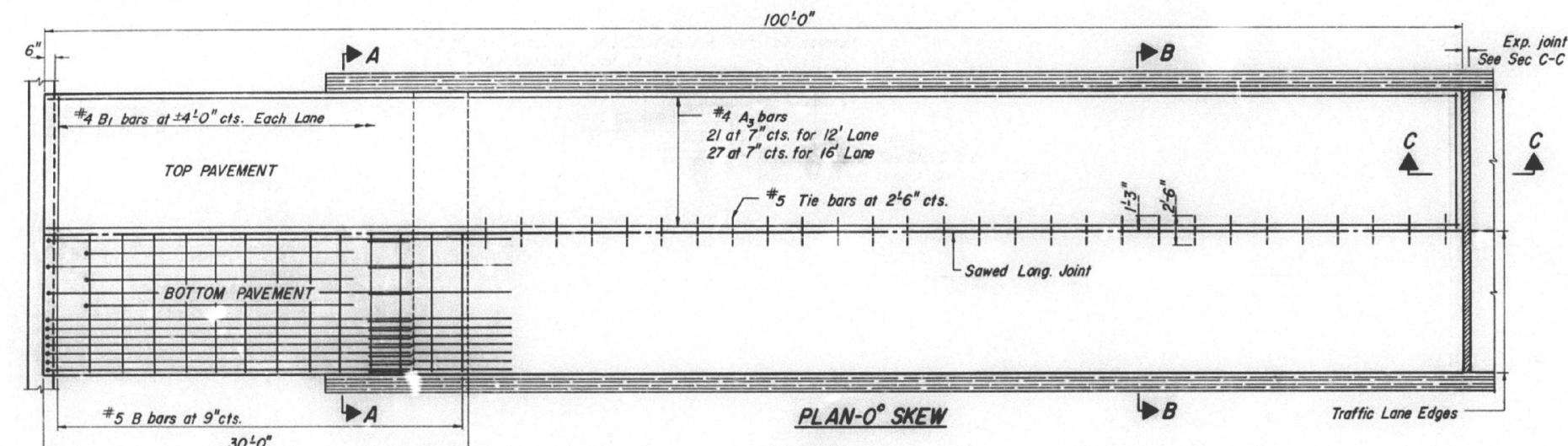


5/8" steel tie bars at 2'-6" cts. Keyed long. const. jt. in accordance with details shown on Standard 2323

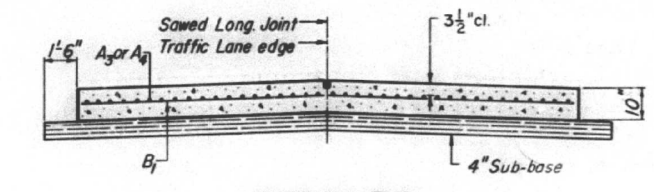
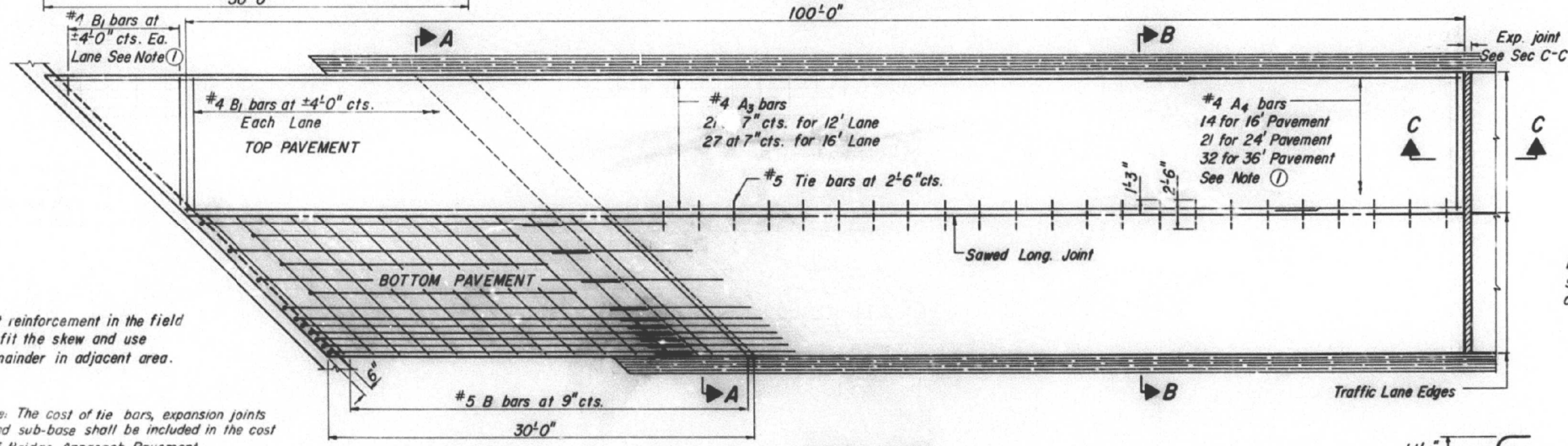


OPTIONAL LONG. CONST. JOINT

As approved by the Engineer, the contractor may elect to reduce the widths by use of the Optional Longitudinal Construction Joint shown. Joint shall be located at the edge of Traffic Lane.

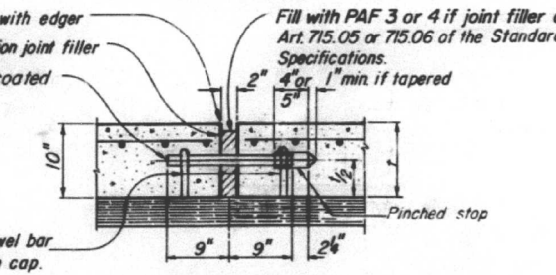


SECTION A-A



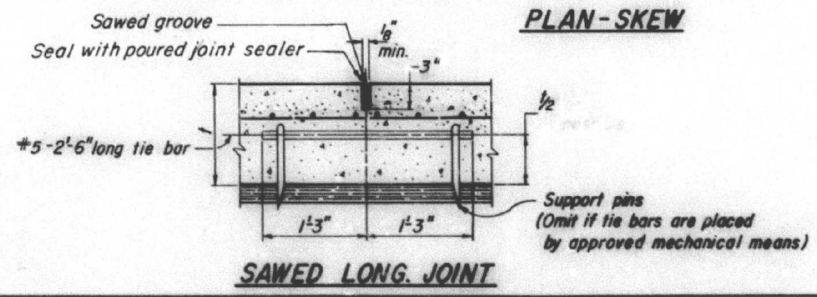
SECTION B-B

Finish corners with edge
2" Preformed expansion joint filler
1 1/4" dia x 18" long pre-coated smooth dowel bars at 12" cts.

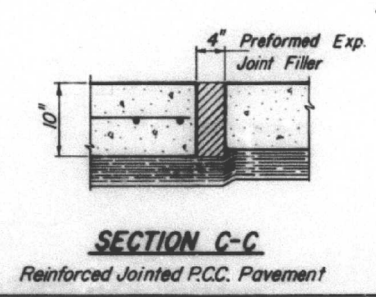


SECTION C-C

Continuous Reinforced P.C.C. Pavement

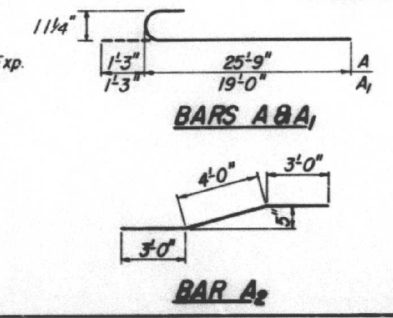


SAWED LONG JOINT



SECTION C-C

Reinforced Jointed P.C.C. Pavement



BARS A&A1

BAR A2

BRIDGE APPROACH PAVEMENT

(Sheet 1 of 2)
STANDARD 2353-5

Illinois Department of Transportation
PASSED May 20 1981
APPROVED May 20 1981
Engineer of Bridge and Traffic Structures
Engineer of Design

NOT TO SCALE

FILE NAME =	USER NAME = eliasgs	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING BRIDGE APPROACH PAVEMENTS FOR INFORMATION ONLY			F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLOT SCALE = 115.0000' / in.	CHECKED -	REVISED -					IL RTE 9		CONTRACT NO. 68088		
	PLOT DATE = 5/30/2019	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

H 5.20 e

QUANTITIES FOR 100-FOOT APPROACH SLAB

Skew Angle	Bottom Reinforcement			Top Reinforcement				Total Weight bars-lbs.
	#5 B bars		A	#4 A ₃ bars	#4 A ₄ bars	Total		
	No.	Length	A ₁ A ₂				No.	

Skew Angle	Bottom Reinforcement			Top Reinforcement				Total Weight bars-lbs.
	#5 B bars		A	#4 A ₃ bars	#4 A ₄ bars	Total		
	No.	Length	A ₁ A ₂				No.	

Skew Angle	Bottom Reinforcement			Top Reinforcement				Total Weight bars-lbs.
	#5 B bars		A	#4 A ₃ bars	#4 A ₄ bars	Total		
	No.	Length	A ₁ A ₂				No.	

16-FOOT WIDTH PAVEMENT

Skew Angle	No.	Length	Bottom Reinforcement	Top Reinforcement	Total Weight bars-lbs.
0°	40	15'-6"	24-#9 A bars	81 34'-3"	5980
5°	40	15'-7"	11-#9 A ₁ bars	81 34'-0" 14 3'-11"	6010
10°	40	15'-9"	24-#5 A ₂ bars	81 33'-10" 14 5'-2"	6020
15°	40	16'-1"		81 33'-7" 14 6'-5"	6030
20°	40	16'-6"		81 33'-5" 14 7'-9"	6050
25°	40	17'-1"		81 33'-2" 14 9'-2"	6070
30°	40	17'-11"		81 32'-11" 14 10'-9"	6110
35°	40	18'-11"		81 32'-7" 14 12'-6"	6150
40°	40	20'-3"		81 32'-3" 14 14'-5"	6200
45°	40	21'-11"		81 31'-11" 14 16'-8"	6280
50°	40	24'-1"		81 31'-5" 14 19'-4"	6370
55°	40	27'-0"		81 30'-11" 14 22'-8"	6490
60°	40	31'-0"		81 30'-2" 14 26'-11"	6660

Bridge Approach Pavement 178 Sq. Yds.

24-FOOT WIDTH PAVEMENT

Skew Angle	No.	Length	Bottom Reinforcement	Top Reinforcement	Total Weight bars-lbs.
0°	40	23'-6"	32-#9 A bars	126 34'-3"	8840
5°	40	23'-7"	19-#9 A ₁ bars	126 33'-10" 21 4'-9"	8780
10°	40	23'-10"	32-#5 A ₂ bars	126 33'-6" 21 6'-11"	8890
15°	40	24'-4"		126 33'-2" 21 9'-1"	8910
20°	40	25'-0"		126 32'-9" 21 11'-5"	8940
25°	40	25'-11"		126 32'-4" 21 13'-11"	8980
30°	40	27'-2"		126 31'-11" 21 16'-6"	9030
35°	40	28'-8"		126 31'-5" 21 19'-6"	9090
40°	40	30'-8"		126 30'-10" 21 22'-10"	9170
45°	40	33'-3"		126 30'-3" 21 26'-8"	9290
50°	80	19'-1"		126 29'-5" 21 31'-3"	9490
55°	80	21'-4"		126 28'-6" 21 36'-11"	9680
60°	80	24'-4"		126 27'-3" 42 22'-9"	9940

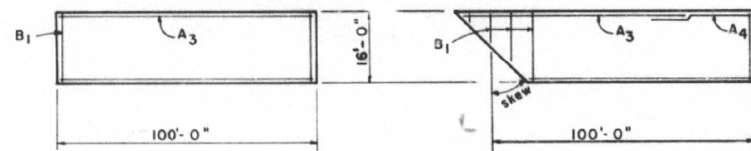
Bridge Approach Pavement 267 Sq. Yds.

36-FOOT WIDTH PAVEMENT

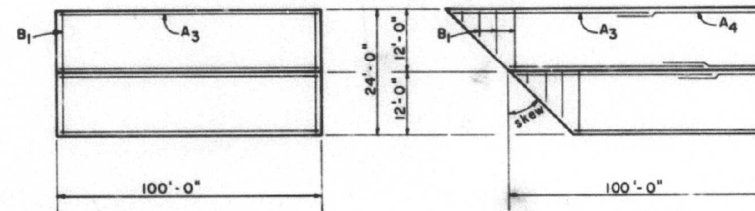
Skew Angle	No.	Length	Bottom Reinforcement	Top Reinforcement	Total Weight bars-lbs.
0°	40	35'-6"	44-#9 A bars	189 34'-3"	13040
5°	40	35'-8"	31-#9 A ₁ bars	189 33'-8" 32 5'-10"	13090
10°	40	36'-0"	44-#5 A ₂ bars	189 33'-2" 32 9'-0"	13110
15°	80	19'-4"		189 32'-8" 32 12'-4"	13230
20°	80	19'-10"		189 32'-0" 32 15'-9"	13260
25°	80	20'-7"		189 31'-5" 32 19'-6"	13330
30°	80	21'-6"		189 30'-9" 32 23'-6"	13410
35°	80	22'-8"		189 30'-0" 32 27'-10"	13510
40°	80	24'-2"		189 29'-2" 32 32'-10"	13630
45°	80	26'-2"		189 28'-3" 64 20'-0"	13840
50°	80	28'-8"		189 27'-1" 64 23'-6"	14050
55°	80	32'-0"		189 25'-8" 64 27'-9"	14330
60°	80	36'-7"		189 23'-10" 64 33'-2"	14710

Bridge Approach Pavement 400 Sq. Yds.

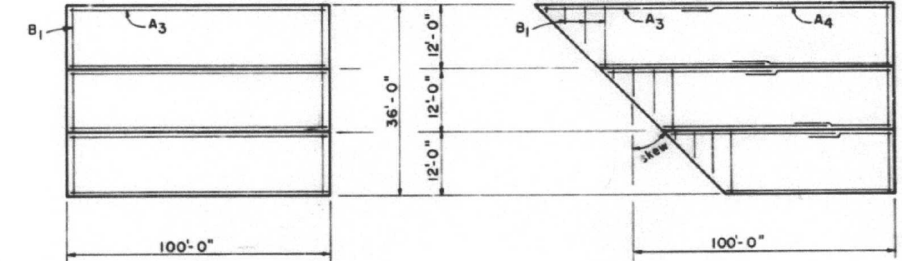
TOP OF SLAB BAR ARRANGEMENT



TOP OF SLAB BAR ARRANGEMENT



TOP OF SLAB BAR ARRANGEMENT



Illinois Department of Transportation

PASSED May 20, 1981

APPROVED May 20, 1981

Thomas A. Bought

Engineer of Design

ISSUED 10-17-78

BRIDGE APPROACH PAVEMENT

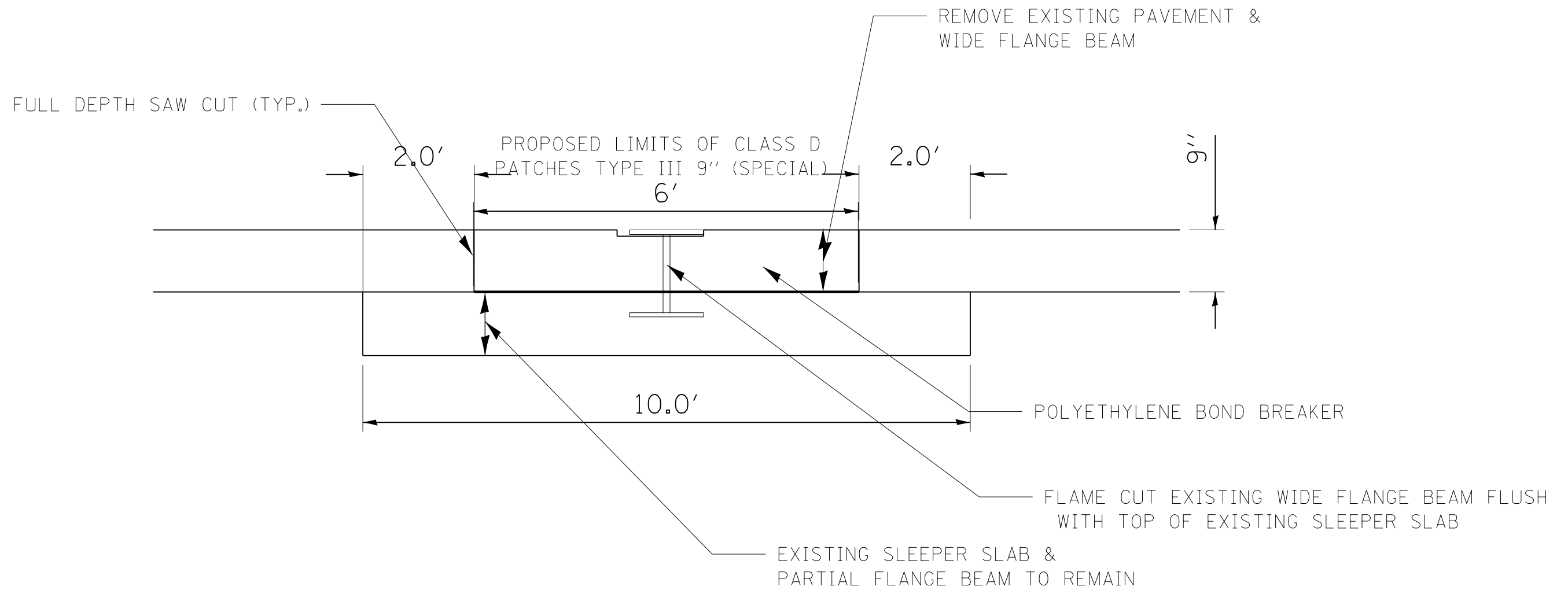
(Sheet 2 of 2)

STANDARD 2353-5

Full Size DWG. Sr.

NOT TO SCALE

FILE NAME =	USER NAME = eliasg	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING BRIDGE APPROACH PAVEMENTS FOR INFORMATION ONLY	F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom\dot\illinois\gov\PWIDOT\Documents\IDOT Offices\District 4\Projects\04.6808\Drawings\CADD\Drawings\046808B-CADD.dgn	PLOT SCALE = 1/16" = 1' / in.	CHECKED -	REVISED -			693	(L)RS-2	PEORIA	28	19
Default	PLOT DATE = 5/30/2019	DATE -	REVISED -			IL RTE 9	CONTRACT NO. 68088		ILLINOIS FED. AID PROJECT	
						SCALE:	SHEET OF SHEETS	STA.	TO STA.	



NOTES:

1. EXISTING WIDE FLANGE BEAM JOINTS SHALL BE REMOVED AND REPLACED WITH CLASS D PATCHES TYPE II 9" (SPECIAL)
2. WORK SHALL BE DONE IN ACCORDANCE TO APPLICABLE SECTIONS OF ARTICLE 442.08 OF STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.
3. ALL WORK, MATERIAL AND THE PLACEMENT OF POLYETHYLENE BOND BREAKER AND REMOVAL OF THE WIDE FLANGE BEAM SHALL BE INCLUDED IN THE UNIT PRICE FOR "CLASS D PATCHES TYPE II 9" (SPECIAL)"

NOT TO SCALE

MODEL: Default
 FILE NAME: p:\broom\clor_illinois.gov\PIV\DOT\Documents\DOT_Offices\District_4\Projects\04_68D88\CADD\Drawings\0468D88-CADD.dgn

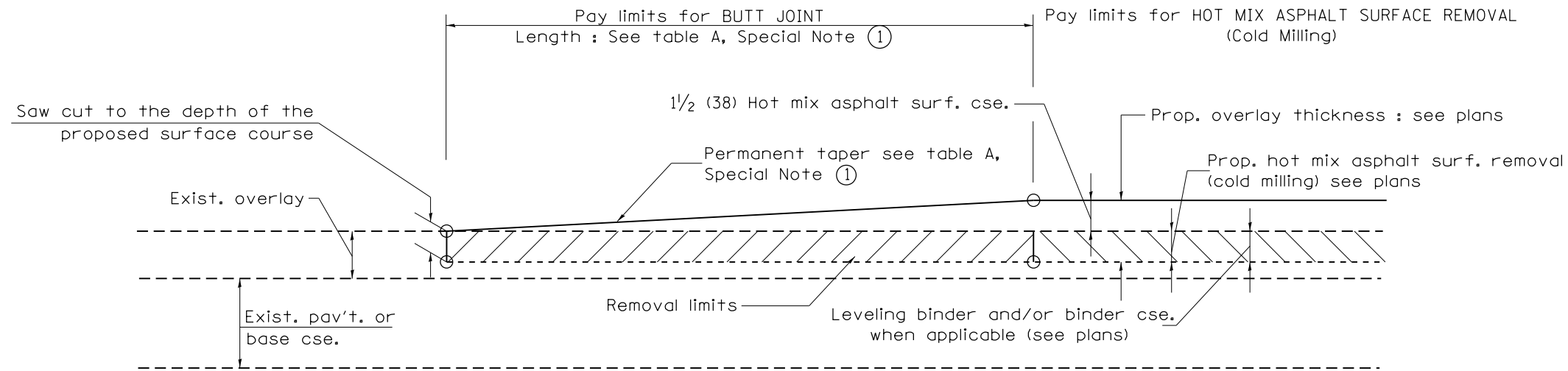
USER NAME = eliasgs	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 1/15.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 5/30/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**WIDE FLANGE BEAM JT REMOVAL
CLASS D PATCHES (SPECIAL)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
693	(L)RS-2	PEORIA	28	20
IL RTE 9		CONTRACT NO. 68D88		
ILLINOIS FED. AID PROJECT				



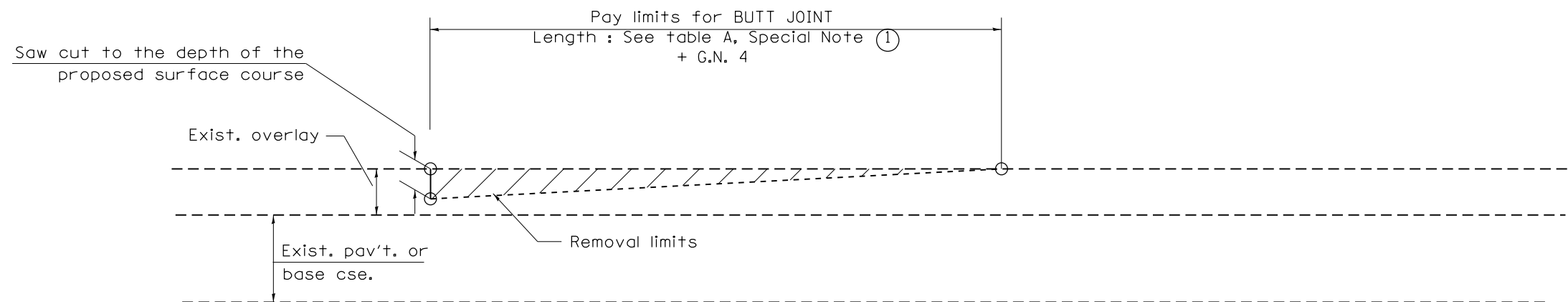
CASE 1 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

**TABLE A
TAPER RATES**

SPECIAL NOTE NUMBER	ELEMENT	MAINLINE INTERSTATES & 4-LANE EXPRESSWAYS	ALL OTHERS
①	BUTT JOINT TAPER RATE	1:480	1:240
②	TEMPORARY RAMP TAPER RATE	1:80	1:40

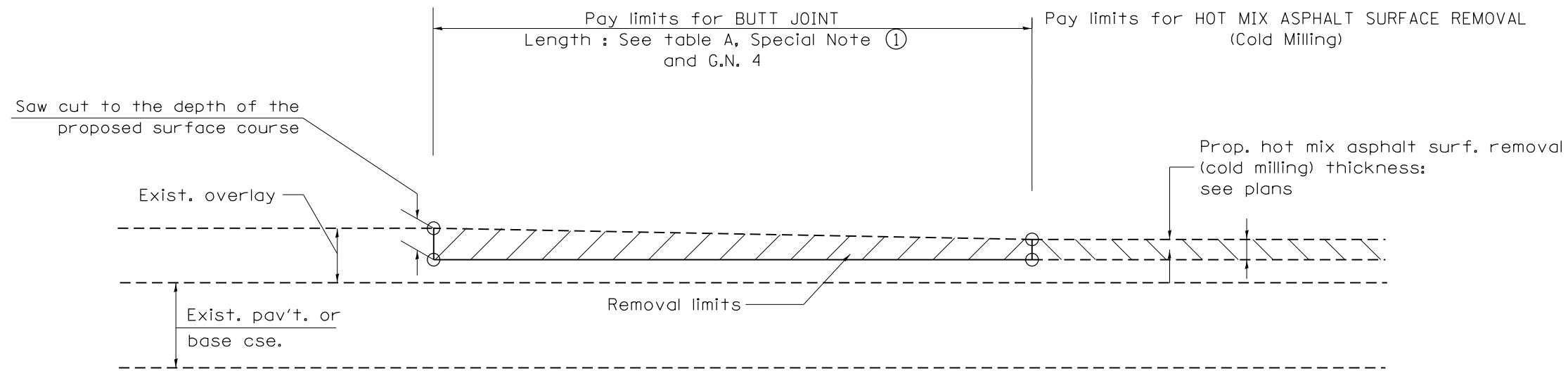
GENERAL NOTES

1. The work shall be done in accordance with Article 406.08 and the Special Provision for Butt Joints.
2. The pavement surface to be removed may be either bituminous or P.C. concrete. The work shall be performed in accordance with Article 440.04 and the Special Provisions for Butt Joints.
3. The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.05.
4. The length of butt joint is based on the taper rate times change in cold milling depth within the butt joint pay limits, unless otherwise indicated.
5. Temporary ramps are paid for separately and not included in the cost of the butt joints.

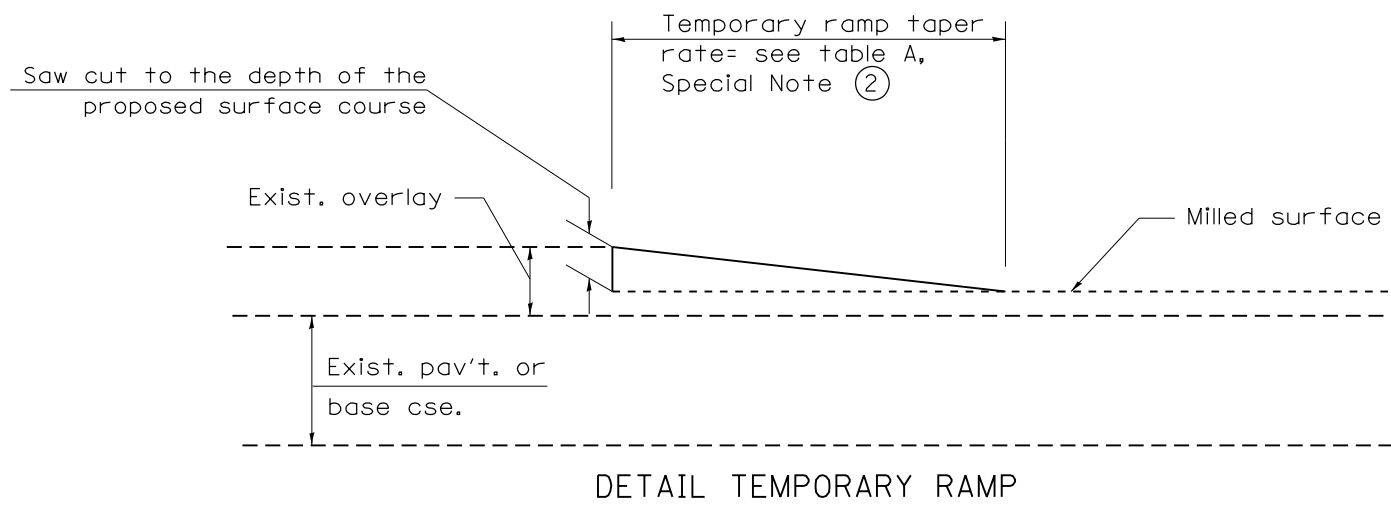


CASE 2 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

All dimensions are in inches (millimeters) unless otherwise noted.



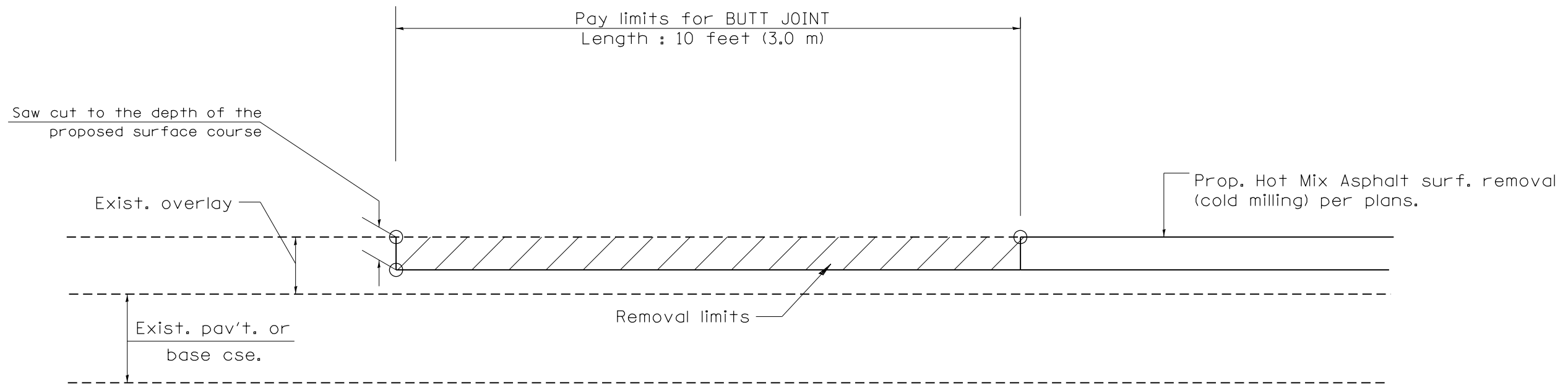
**CASE 3 : HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER**



DETAIL TEMPORARY RAMP

All dimensions are in inches (millimeters) unless otherwise noted.

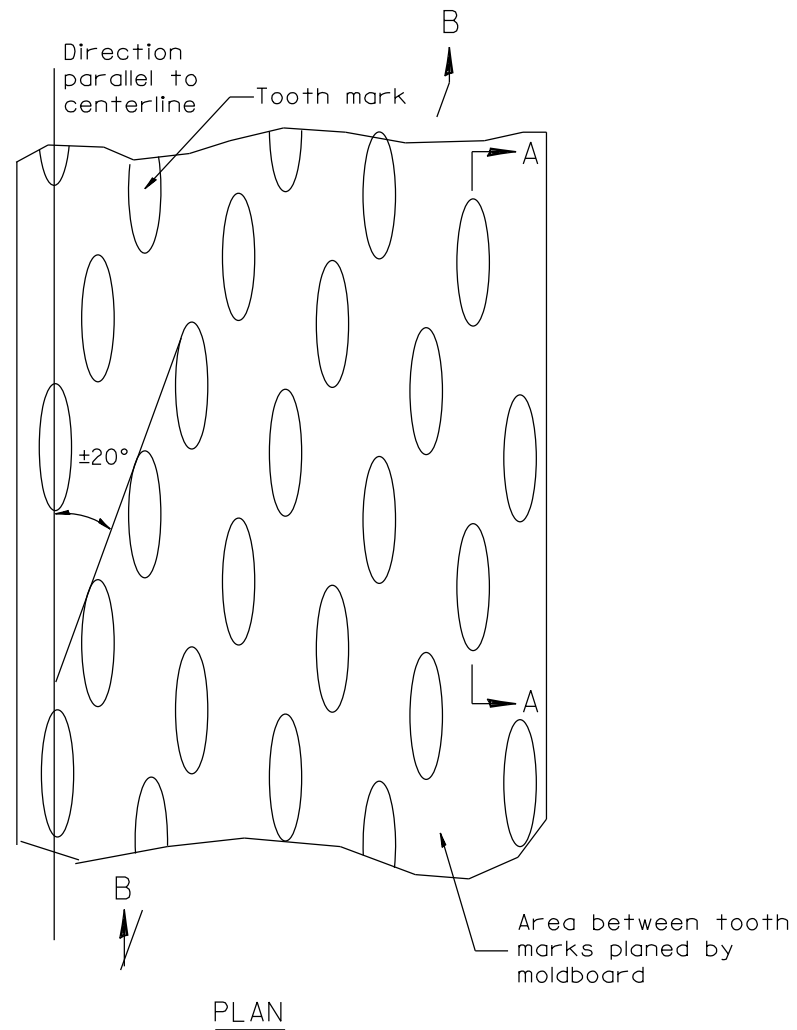
				STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINTS	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
						NOT TO SCALE	ILJRS-2		PEORIA	28
				SHT. 2 OF 3	CADD STD. 406101-D4	FAP 693 RTE IL 9		CONTRACT NO. 68D88		
						FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



CASE 4 : SINGLE LIFT OVERLAY WITH EQUIVALENT DEPTH
HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER

All dimensions are in inches (millimeters) unless otherwise noted.

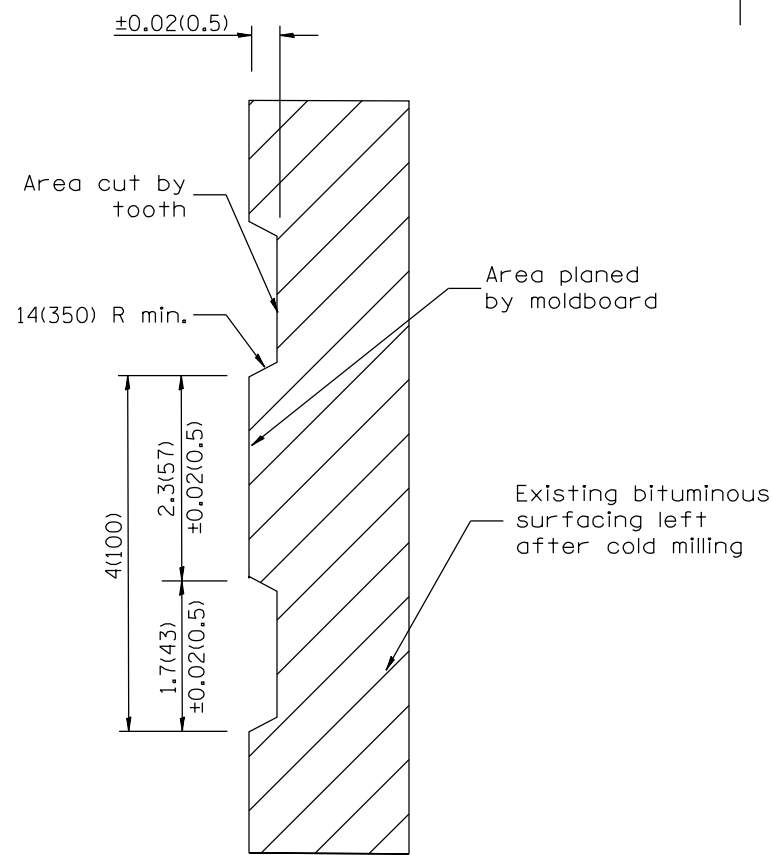
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINTS	SHT. 3 OF 3 CADD STD. 406101-D4	SECTION (L)RS-2 COUNTY PEORIA TOTAL SHEETS 28 SHEET NO. 23 CONTRACT NO. 68D88
NOT TO SCALE		<small>FAP 693 RTE IL 9 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT</small>	



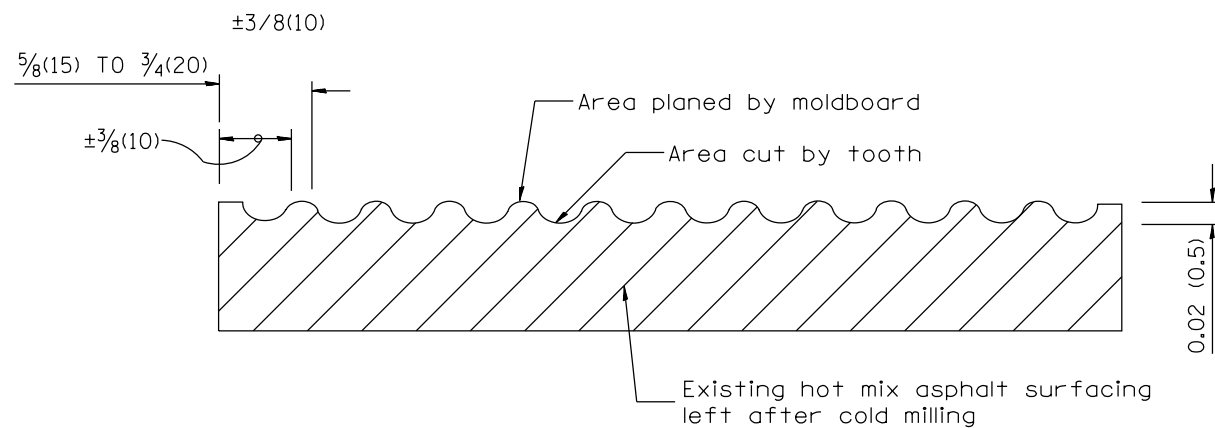
PLAN

General notes:

1. Coldmilling shall consist of two processes: Cutting with carbide teeth mounted on a rotating drum, and planing with a moldboard mounted immediately behind the cutting drum.
2. Other similar patterns will be acceptable if they consist of a smooth, flat, planed surface interspersed with a pattern of discontinuous longitudinal striations.



SECTION A-A



SECTION B-B PROJECTED
PERPENDICULAR TO CENTERLINE

All dimensions are in inches (millimeters) unless otherwise noted.

DESIGNER NOTES:
1. INCLUDE DISTRICT SPECIAL PROVISION, IF APPLICABLE.

01-01-97	RENUM. C-104.01, NEW REVISION BOX	T.P.
04-20-98	REMOVED MILLING DETAIL FROM STANDARD	J.A.
09-08-98	CORRECT NOTE LEADER PLACEMENT	R.W.
10-16-06	REVISED TO 2007 SPEC.	M.A.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

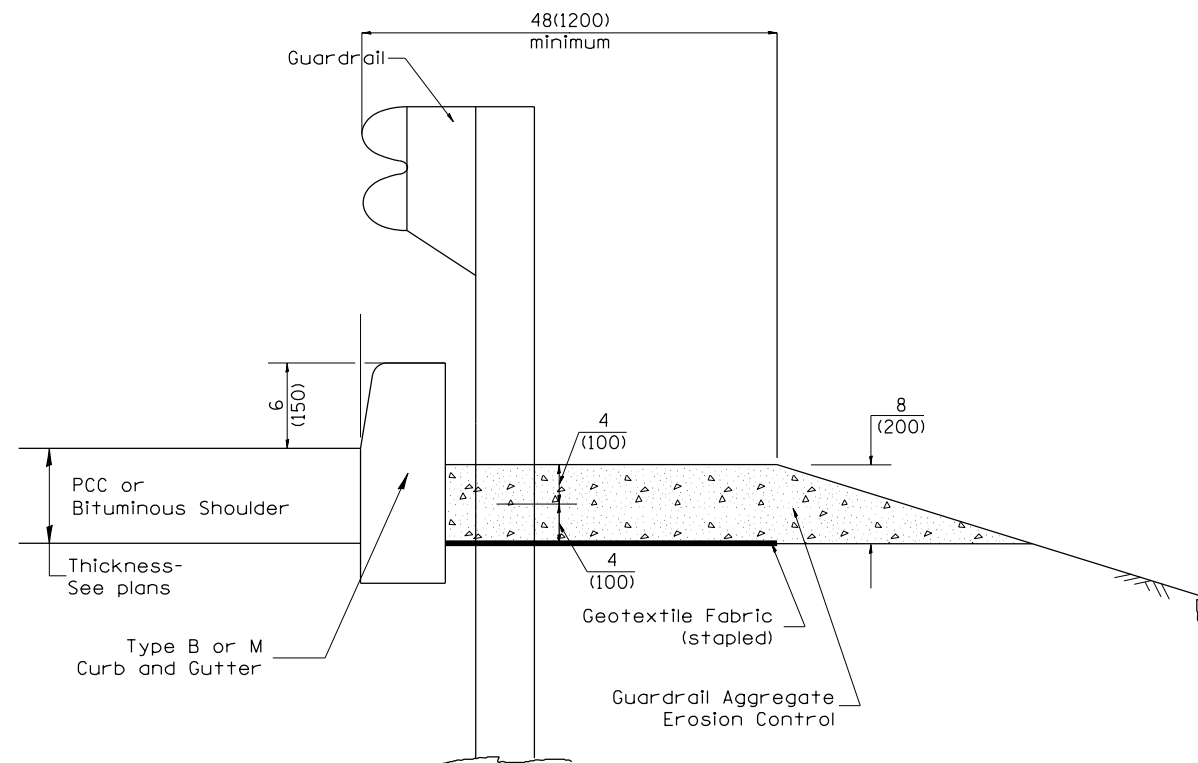
HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

NOT TO SCALE

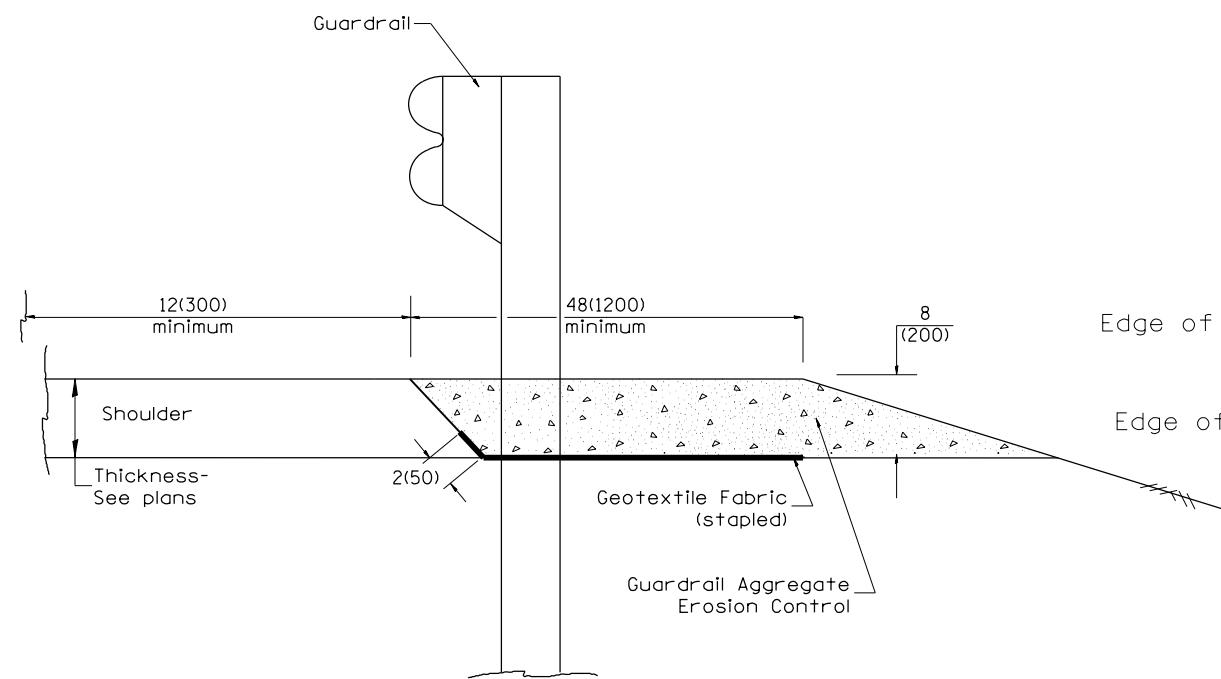
CADD STD. 440001-D4

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
(L)RS-2	PEORIA	28	24
FAP 693 RTE IL 9		CONTRACT NO. 68D88	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	

CONSIDER USING A "B" CURB PAY ITEM AT GUARDRAIL INSTALLATIONS WHERE GRADES ARE EQUAL TO OR GREATER THAN 1% AND AT INLETS. (INCLUDE DISTRICT SPECIAL PROVISION 1.1).
 USE "GUARDRAIL AGGREGATE EROSION CONTROL" AT GUARDRAIL INSTALLATIONS WHERE GRADES ARE LESS THAN 1% (INCLUDE DISTRICT SPECIAL PROVISION 1.1).
 INCLUDE THE FOLLOWING DISTRICT CADD STANDARDS AS NEEDED: SLOPE DRAINS FOR EXPOSED PIPES; SEE PAGE COLLARS FOR BURIED PIPES; SLOPE DRAINS FOR BURIED PIPES; SEE PAGE COLLARS FOR BURIED PIPES.
 SEE PAGE COLLARS FOR EXPOSED PIPES; CONCRETE THRUST BLOCKS AND PIPE ELBOW.
 INCLUDE DISTRICT SPECIAL PROVISION - "AGGREGATE QUALITY" FOR PROJECTS LOCATED IN THE WESTERN AREA OF THE DISTRICT - APPROX. DIVIDING LINE IS IL 97.
 DELETE DESIGNER NOTES WHEN INSERTING INTO PLAN FILES.
 OPERATIONS PREFERS USE OF PIPE OUTLETTING ONTO FORESLOPE WITH RIPRAP. USE NON-METALLIC PIPE WHEN POSSIBLE BECAUSE OF FUTURE CORROSION ISSUES.
 IF NO OTHER SEEDING IS PAID FOR ON THE CONTRACT, USE DISTRICT SPECIAL PROVISION FOR SEEDING, MINOR AREAS



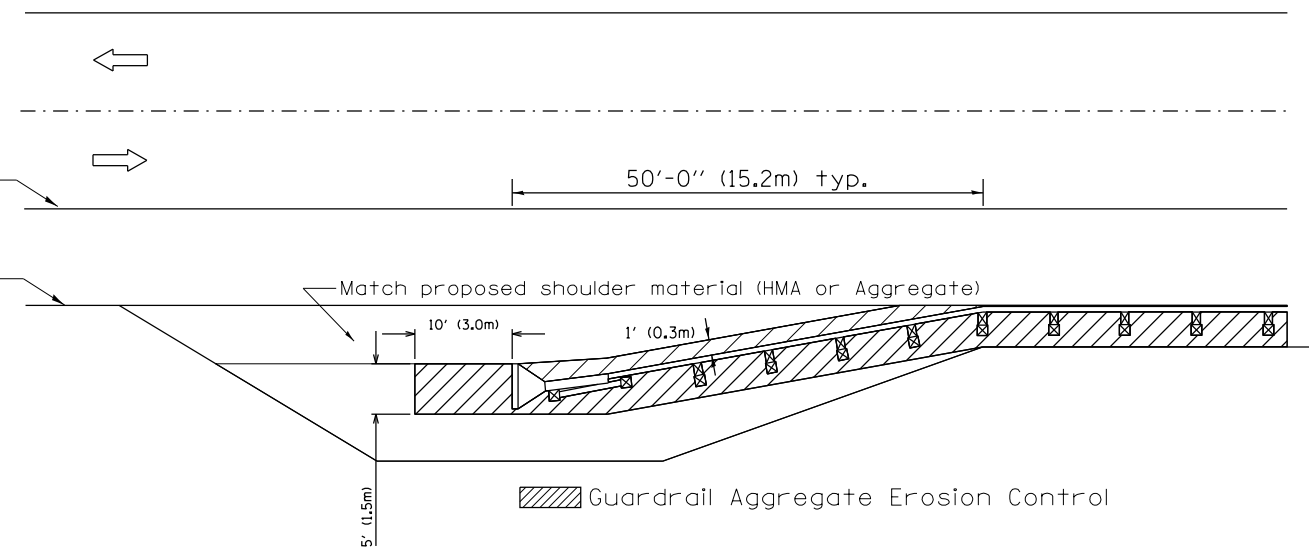
TYPICAL SECTION WITH EROSION CONTROL CURB



TYPICAL SECTION WITHOUT EROSION CONTROL CURB

GENERAL NOTES: GUARDRAIL AGGREGATE EROSION CONTROL

1. This work shall consist of grading as needed, furnishing and installing geotextile fabric and staples, and furnishing, placing and shaping crushed aggregate around and behind Steel Plate Beam Guardrail posts in accordance with Plan Details.
2. Before placing the aggregate and the Geotextile Fabric, weeds and grass shall be removed from the area to be covered.
3. After the area has been prepared, and in a dry condition, the Geotextile fabric shall be placed with a 12(300) minimum overlap. A knife cut for guardrail post installation is necessary.
4. The aggregate shall be deposited, compacted and shaped by either mechanical or hand methods, in a manner reasonably true to line and grade.
5. The Contractor shall have the option of placing the guardrail before or after the Geotextile Fabric and Aggregate are in place. If the guardrail is placed after the Geotextile Fabric and Aggregate, then any voids must be filled and the aggregate returned to line and grade.
6. Materials shall meet the following requirements:
 - A. The crushed aggregate shall be CA1 gradation in accordance with Article 1004.01(c) of the Standard Specifications.
 - B. The Geotextile Fabric shall be nonwoven fabric in accordance with Article 1080.02 of the Standard Specifications.



All dimensions are in inches (millimeters) unless otherwise noted.

DESIGNER NOTES:

01-01-97	RENUM. C-22.01, NEW REVISION BOX	T.P.	03-07-11	ADDED DETAIL SHOWING PLAN VIEW	R.D.
03-01-97	CORRECT STD. NUMBERS IN NOTES PG. 2	J.A.	08-10-12	REVISED CURB "B" AND AGGREGATE	R.D.
11-03-00	CORRECTION TO NOTES	M.A.	07-15-15	ADDRESSED SHOULDER INLET CURB	R.D.
10-16-06	REVISED TO 2007 SPEC.	M.A.	01-26-17	REVISED	R.D.

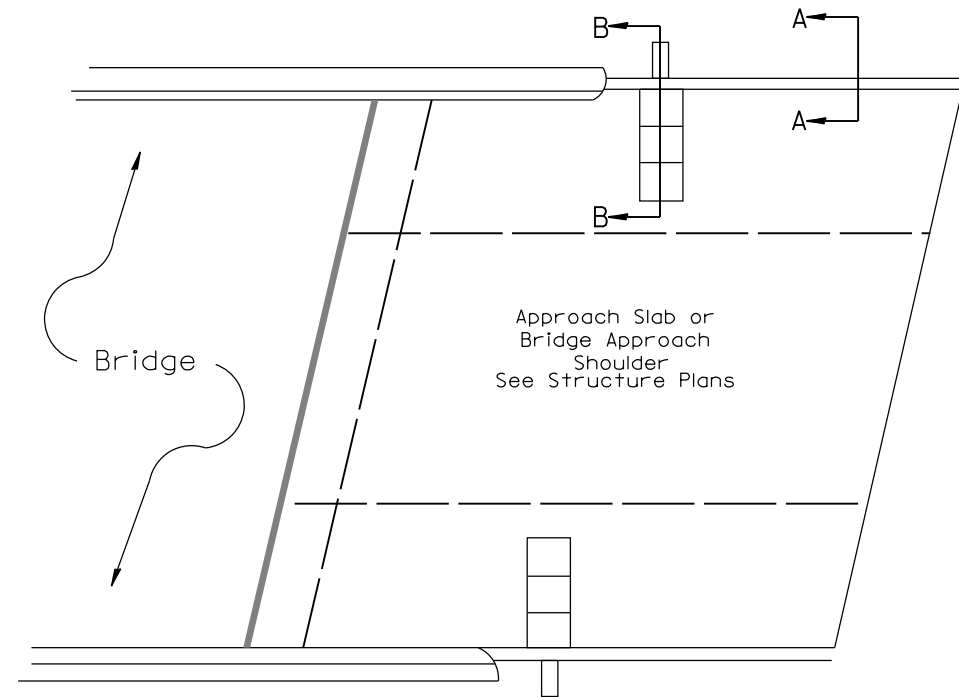
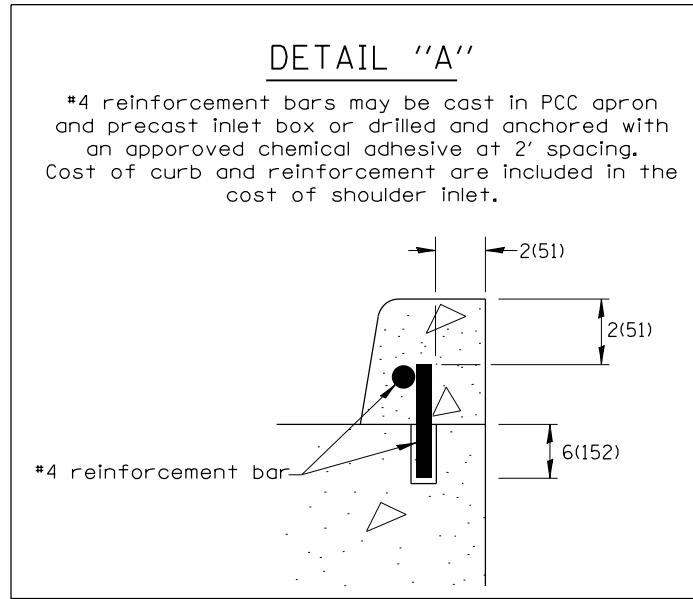
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GUARDRAIL EROSION CONTROL TREATMENTS

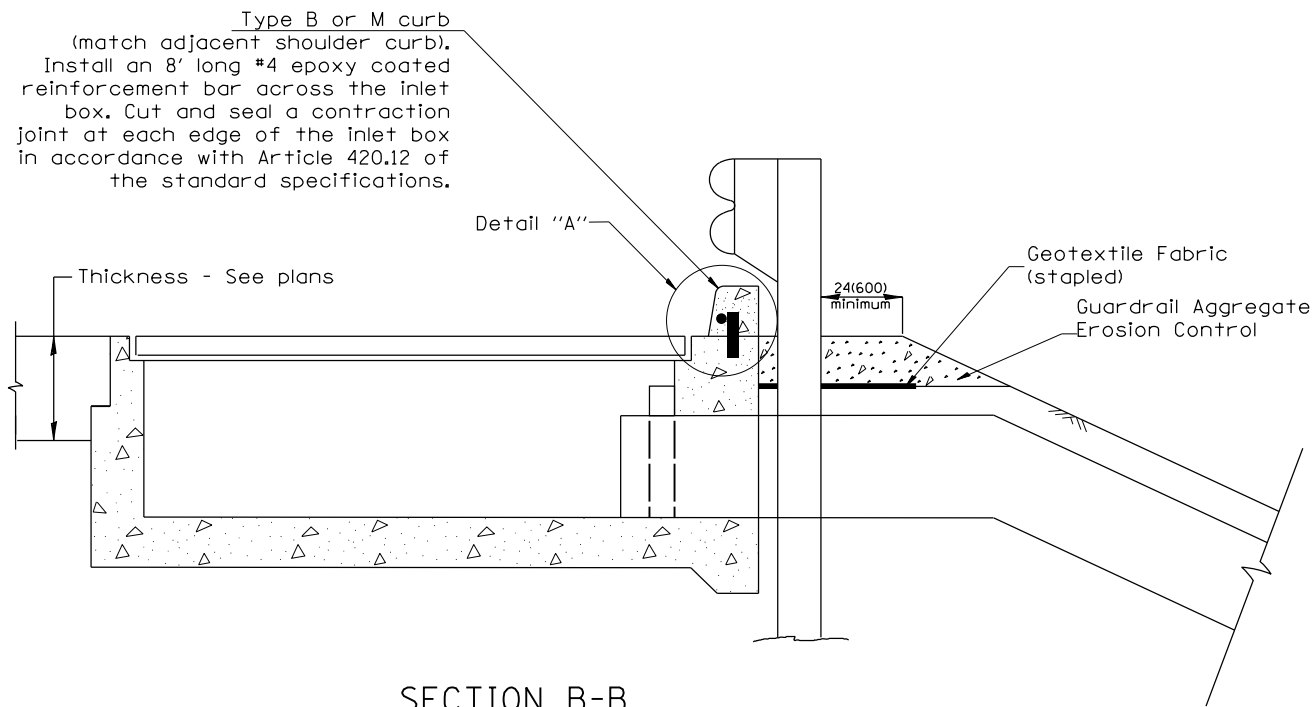
NOT TO SCALE

SHT. 1 OF 2
CADD STD. 630101-D4

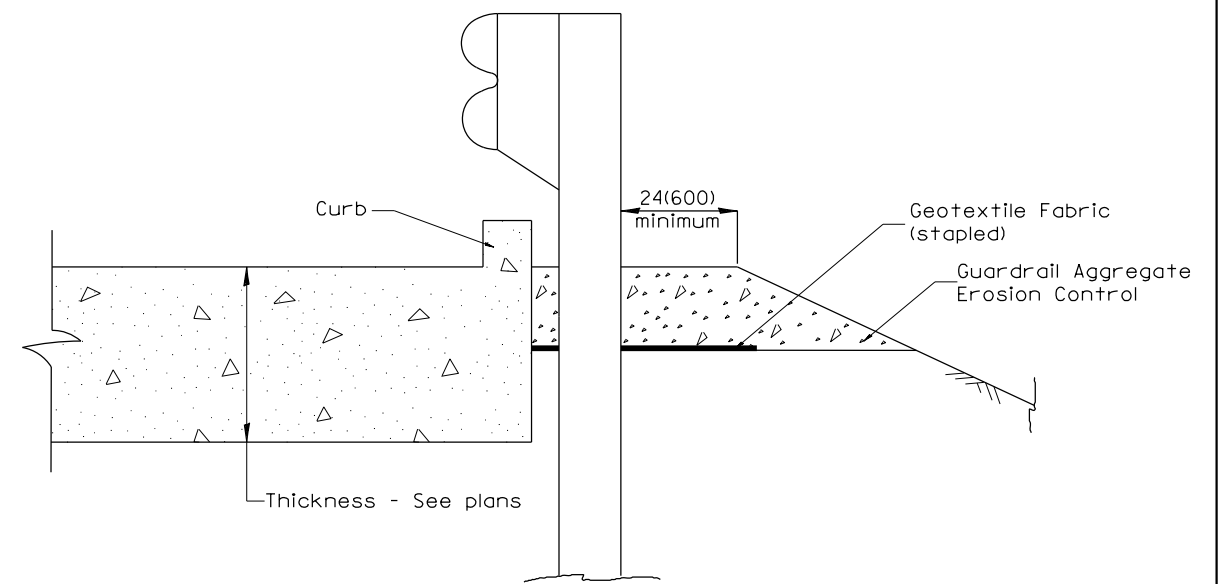
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
(L)RS-2	PEORIA	28	25
FAP 693 RTE IL 9		CONTRACT NO. 68D88	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	



PLAN VIEW
APPROACH SLAB OR SHOULDER PLACEMENT



SECTION B-B
TYPICAL SECTION AT INLETS
TYPE E, F & G (HIGHWAY STANDARD 610001)

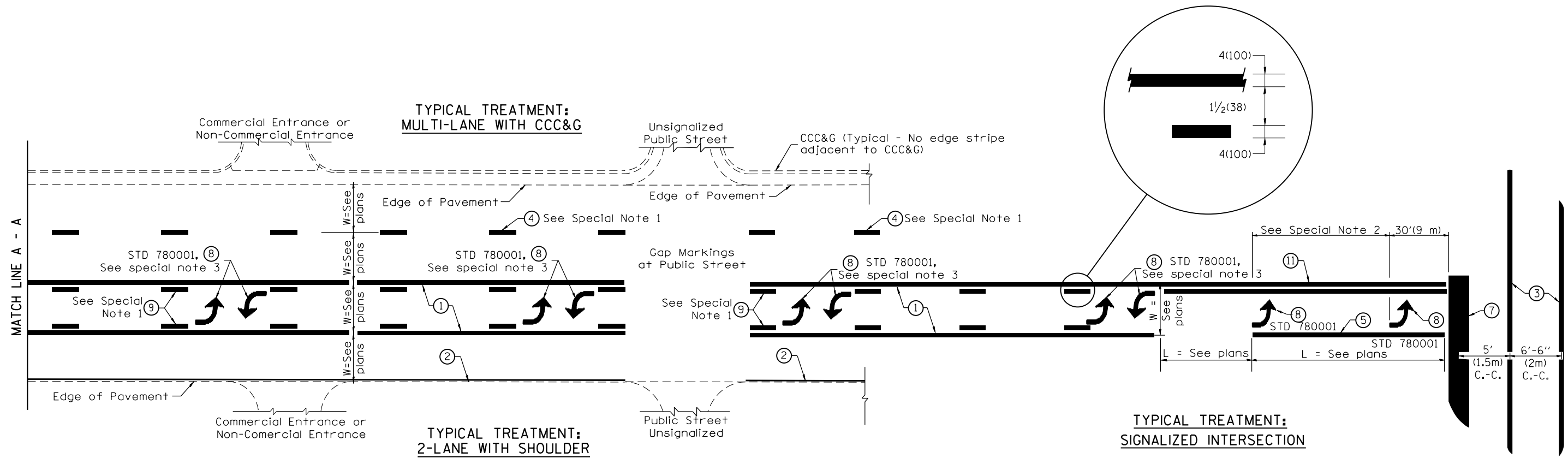


SECTION A-A
TYPICAL SECTION WITH BRIDGE APPROACH CURB

All dimensions are in inches (millimeters) unless otherwise noted.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				GUARDRAIL EROSION CONTROL TREATMENTS				SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
								(L)RS-2	PEORIA	28	26
				SHT. 2 OF 2				CONTRACT NO. 68D88			
				NOT TO SCALE				CADD STD. 630101-D4			
								FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			

DESIGNER NOTES:
1. Include State Standard 780001 (Typical Pavement Markings)



FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION

TYPICAL PAVEMENT MARKING LEGEND

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A) (See Table A)
- ⑪ 4(100) Double Solid (Yellow) (See Table A)

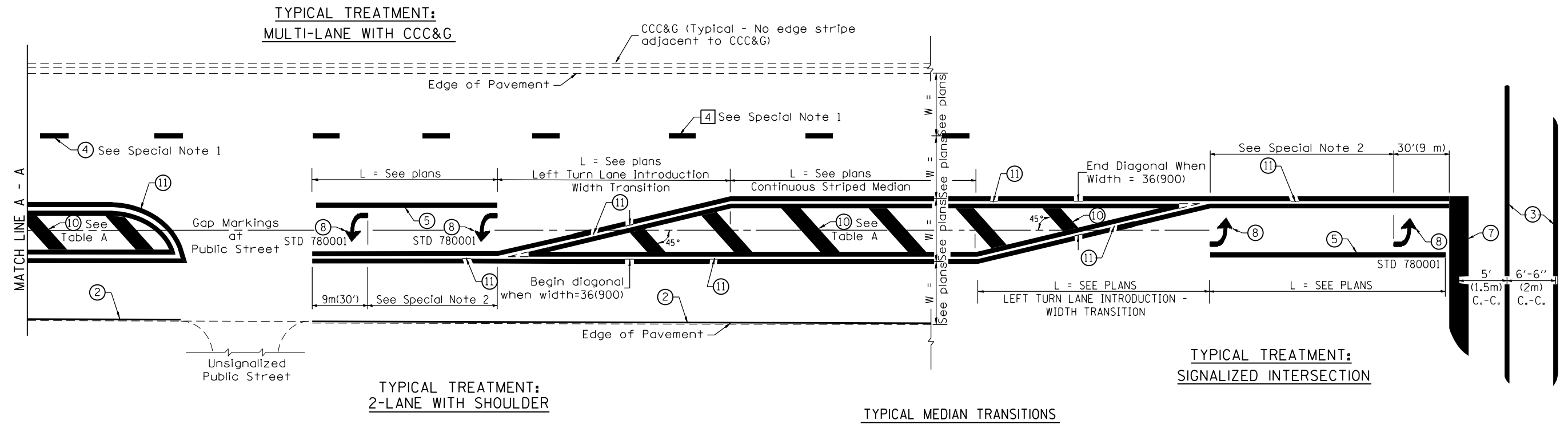
SPECIAL NOTES

1. Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
2. The following shall apply to arrows located in one-way left turn lanes:
 - A. A minimum of two (2) arrows is required.
 - B. The maximum spacing between arrows is 80' (24 m).
 - C. Arrows shall be evenly spaced if three (3) or more are required.
3. The following shall apply to arrow pairs located in two-way left turn lanes:
 - A. A minimum of two (2) arrow pairs is required.
 - B. The maximum spacing between arrow pairs is 200' (61 m).
 - C. Arrows shall be evenly spaced if three (3) or more are required.
 - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

GENERAL NOTES

1. Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
2. See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
3. Refer to Article 780.13 for letter, number and symbol areas (sq. ft.)
4. Areas are grooved 1" beyond each edge for the following symbols:
 - Through Arrow= 14.8 sq. ft.
 - Large Left or Right Arrow= 21.9 sq. ft.
 - 2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.
 - Wrong Way Arrow= 29.5 sq. ft.
 - Railroad Crossing Symbol= 69.8 sq. ft.
 (For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

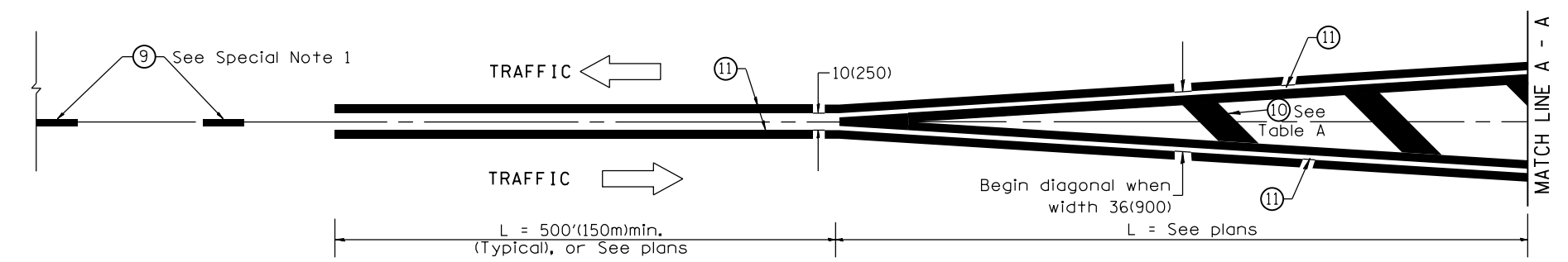
01-01-97	RENUM. F-8.03, NEW REVISION BOX	T.P.	10-16-06	REVISED TO 2007 SPEC.		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL PAVEMENT MARKINGS		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
02-07-97	ADD BI DIRECTIONAL DIMENSION	J.A.	2/29/16	ADDED GROOVING AREAS	R.D.			(L)RS-2	PEORIA	28	27		
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.											
08-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.											
NOT TO SCALE							SHT. 1 OF 2	CADD STD. 780001-D4	CONTRACT NO. 68D88				
							FAP 693 RTE IL 9		ILLINOIS FED. AID PROJECT				



FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE

TABLE A
RECOMMENDED SPACING BETWEEN DIAGONAL LINES

SPEED LIMIT RANGE	INTERSECTION CHANNELIZATION (Includes Width Transitions for Median and Left Turn Lane Introductions)	
	CONTINUOUS	
Less Than 30 mph (50 km/h)	50' (15m)	15' (5m)
30 - 45 mph (50 - 70 km/h)	75' (23m)	20' (6m)
Over 45 mph (70 km/h)	150' (46m)	30' (9m)



All dimensions are in inches (millimeters) unless otherwise noted.