

018

07-12-2019 LETTING ITEM 078

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6;N	PEORIA	48	1
		ILLINOIS	CONTRACT NO. 68E93	

INDEX OF SHEETS, SEE SHEET NO. 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED HIGHWAY PLANS

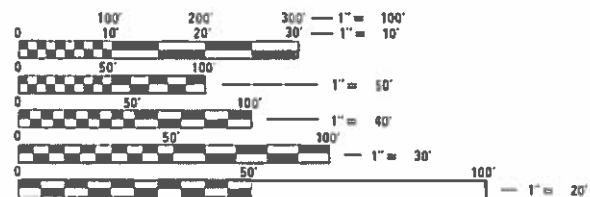
FAP ROUTE 646 (IL 40)
SECTION (Y)RS-6;N
PROJECT NHPP-STP-UDAU(932)
PAVEMENT RESURFACING (PPP)
PEORIA COUNTY

C-94-042-19

HIGHWAY STANDARDS

- 420001-09
- 442201-03
- 604001-04
- 606001-07
- 630001-12
- 630301-09
- 631011-10
- 631031-15
- 701101-05
- 701106-02
- 701411-09
- 701426-09
- 701451-05
- 701456-05
- 701601-09
- 701602-10
- 701701-10
- 701901-08
- 725001-01
- 780001-05
- 781001-04
- 782006
- 862001-01

ADT 24200 (2017)
MU 0.93%
SU 2.69%

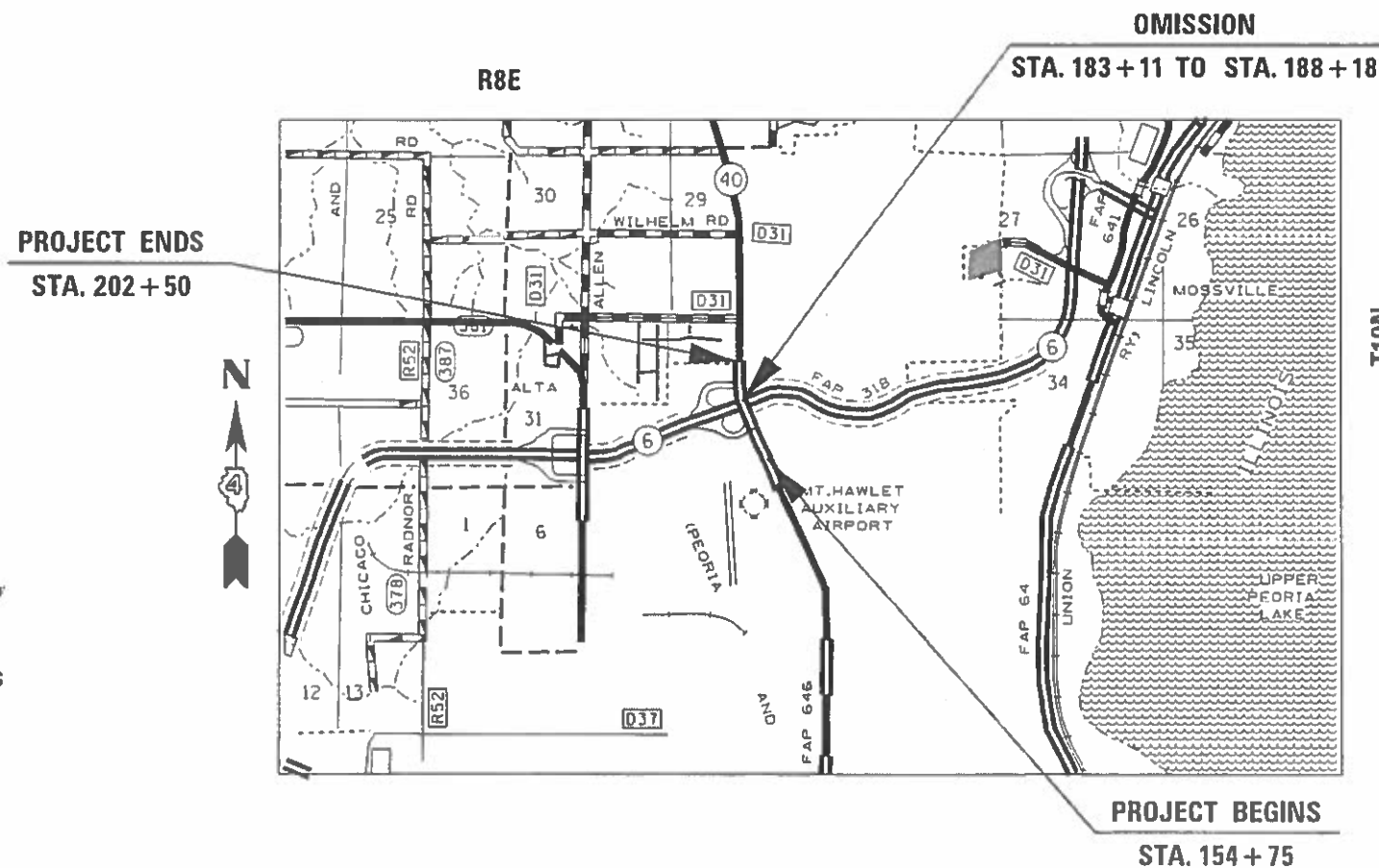


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER NICOLE FAYANT /KEVIN HORST (309)671-3472
PROJECT MANAGER RICHARD DOTSON (309)671-3455
CATALOG NO. 035736-00D
CONTRACT NO. 68E93

GROSS LENGTH = 4775 FT. = 0.904 MILE
NET LENGTH = 4268 FT. = 0.808 MILE



PROJECT DESCRIPTION
THE PROJECT CONSISTS OF RESURFACING ON IL 40 (KNOXVILLE AVE.) FROM RAVINSWOOD ROAD TO NORTH OF TIMBER LANE IN PEORIA.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED May 31 2019
Kennel A. Haunett KSD
REGION THREE ENGINEER

June 19 2019
E.A. Elk
ENGINEER OF DESIGN AND ENVIRONMENT

June 19 2019
Paul C. [Signature]
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION 3

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INDEX OF SHEETS

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- 40. CONCRETE CURB, TYPE B (SPECIAL) DETAIL
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GENERAL NOTES

ADDITIONAL SUPPLEMENTAL TRAFFIC CONTROL

The Department reserves the right at any time to add additional Traffic Control Systems or devices within the active contract limits, by means of an additional contract. All terms of Article 105.08 of the Standard Specifications shall be followed by each Contractor.

COMMITMENTS

Commitments are not to be altered without the written approval of all parties to which the commitment was made. No Commitments have been made for this project.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

Mixture Use(s):	POLYMER SMA SURFACE COURSE 1.75"	POLYMER BINDER COURSE 2"	INCIDENTAL SURFACE	SHOULDERS (SURFACE LIFT)	CLASS D PATCH (3" LIFTS)
AC/PG:	SBS OR SBR 76-28	SBS OR SBR 76-28	PG 64-22	PG 64-22	PG 64-22
Design Air Voids:	4.0% AT N=80	4.0% AT N=70	4.0% AT N=50	4.0% AT N=50	4.0% AT N=50
Mixture Composition: (Gradation Mixture):	SMA 9.5	IL 9.5	IL 9.5	IL 9.5FG	IL 9.5
Friction Aggregate:	MIX D	N.A.	MIX C	MIX C	N.A.
Quality Management:	QCP	QCP	QCQA	QCQA	QCQA

Notes:

- 1) Individual lift thickness of each mix type will be no less than 3 times nominal maximum aggregate size and no more than 6 times nominal maximum.
- 2) For design purposes, mixture weight for all mixes is determined to be 112.0 lb/s.y/in., unless otherwise noted.
- 3) Sublot sizes for PFP and QCP mixes will be 600 tons, unless otherwise agreed to by the Engineer and the paving Contractor.

GENERAL NOTES (CONTINUED)

PAVEMENT STATIONING NUMBERS & PLACEMENT

The Contractor shall provide labor and materials required to imprint pavement station numbers in the finished surface of the pavement and/or overlay. The numbers shall be approximately 3/4 inch (20 mm) wide, 5 inches (125 mm) high and 5/8 inch (15 mm) deep.

The pavement station numbers shall be installed as specified herein:

Interval – 200 feet (English stationing) or 100 meters (metric stationing)

Bottom of Numbers – 6 inches (150 mm) from the inside edge of the pavement marking

Location:

- 2, 3, & 5 Lane Pavements – right edge of pavement in direction of increasing stations
- Multi-Lane Divided Roadways – outside edge of pavement in both directions
- Ramps – along baseline edge of pavement

Position – stations shall be placed so they can be read from the adjacent shoulder

Format – English (Metric) pavement stations shall use this format "XXX (XX + X00)", where X represents the pavement station

This work will not be paid for separately, but will be included in the cost of the associated pavement and/or overlay pay items.

PAVING SURFACE COURSE

Continuous paving operations on the main roadway shall be maintained at all times during the construction of the hot-mix asphalt surface. No interruptions for side roads, entrances, turn lanes, etc. will be allowed.

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PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 5/31/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, GENERAL NOTES, COMMITMENTS,
AND PROJECT SPECIFIC NOTES**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6:N	PEORIA	48	2
			CONTRACT NO. 68E93	
		ILLINOIS FED. AID PROJECT		

GENERAL NOTES (CONTINUED)

ENVIRONMENTAL REVIEWS

Prior to the use of any proposed borrow areas, use areas (temporary access roads, detours, run-arounds, etc.) and/or waste areas, the Contractor shall file the required environmental resource request surveys according to Section 107.22 of the Standard Specifications. These surveys are required in order for the Department to conduct cultural and biological resource surveys for the proposed site.

The required environmental resource documentation shall include the following:

- * BDE Form 2289 (Cultural and Natural Resources Review of Borrow Areas)
- * BDE Form 2290 (Waste/Use Area Review)
- * A location map showing the size limits and location of the use area
- * Color photographs depicting the use area
- * Borrow Area Entry Agreement form – D4 PI0101

Prior to any waste materials being removed from the construction site the required environmental resource surveys shall be obtained and filed by the Contractor. Excess waste products removed from the construction site shall be disposed of as required in Section 202.03 of the Standard Specifications.

Any protruding metal bars shall be removed prior to the disposal of broken concrete at approved disposal sites.

Please note that a minimum of four weeks shall be allowed for the District to obtain the required environmental clearances and six weeks for the required borrow site environmental clearances.

WINTER SHUTDOWN RESTRICTIONS ON COLD MILLED PROJECTS

Prior to winter shutdown the following steps shall be taken:

- All cold milled surfaces shall be overlaid.
- All lanes shall be reopened to traffic.
- Manholes, where applicable, shall be adjusted to the elevation of the binder course/leveling binder to ease in plowing snow, and re-adjusted to finished grade in the Spring. The initial manhole adjustment will be paid for at the contract unit price and any re-adjustment, as directed by the Engineer, will be paid for in accordance with Article 109.04.
- Temporary or permanent pavement marking shall be placed as applicable.

PROJECT SPECIFIC NOTES

BEFORE THE HMA MATERIAL IS PLACED IN THE PARTIAL DEPTH PATCHES AT THE BRIDGE, THE CONTRACTOR SHALL CORE HOLES IN THE PAVEMENT IN THESE PARTIAL DEPTH PATCH AREAS TO DETERMINE IF THERE ARE VOIDS BELOW THE PAVEMENT. IF VOIDS ARE FOUND, THEY SHALL BE FILLED WITH PUMPED GROUT. THIS WORK SHALL BE DONE AS DIRECTED BY THE ENGINEER, AND IT WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

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DEPARTMENT OF TRANSPORTATION**

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646	(Y)RS-6:N	PEORIA	48	3
CONTRACT NO. 68E93			ILLINOIS FED. AID PROJECT	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
				NHPP	STP	NHPP	STP
				80/20	80/20	80/20	80/20
				(FED./STATE)	(FED./STATE)	(FED./STATE)	(FED./STATE)
				ROADWAY	ROADWAY	SAFETY	SAFETY
				0005	0005	0021	0021
70100820	TRAFFIC CONTROL AND PROTECTION, STANDARD 701451	L SUM	1	0.7	0.3		
70100825	TRAFFIC CONTROL AND PROTECTION, STANDARD 701456	L SUM	1	0.7	0.3		
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	0.7	0.3		
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	0.7	0.3		
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	0.7	0.3		
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	5	3.5	1.5		
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	63	44	19		
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	16395	10821	5574		
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1989	1313	676		
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	6	3	3		
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	510	265	245		
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	18162	11892	6270		
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2140	1420	720		
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	4929	2957	1972		

*= SPECIALTY ITEM

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PLOT DATE = 6/3/2019	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-62N	PEORIA	48	7
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE					
				URBAN		NHPP	STP	NHPP	STP
						80/20	80/20	80/20	80/20
						(FED./STATE)	(FED./STATE)	(FED./STATE)	(FED./STATE)
		ROADWAY	ROADWAY	SAFETY	SAFETY				
		0005	0005	0021	0021				
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	821	562	259				
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	531	179	352				
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	286	184	102				
* 78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	28	16	12				
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	143	92	51			
* 88200510	TRAFFIC SIGNAL BACKPLATE, RETROREFLECTIVE	EACH	8			4	4		
	X0300004	BITUMINOUS MATERIALS (TACK COAT), SPECIAL	POUND	55918	36315	19603			
* X0320023	CLOSED CIRCUIT TELEVISION DOME CAMERA, HD	EACH	2			2			
	X0322494	CURB CUT	FOOT	144		144			
	X0556100	PARTIAL DEPTH PATCHING (SPECIAL)	SQ YD	103	34	69			
	X4400196	HOT - MIX ASPHALT SURFACE REMOVAL, SPECIAL	SQ YD	208	137	71			
	X4421000	PARTIAL DEPTH PATCHING	TON	12	4	8			
	X6061005	CONCRETE CURB, TYPE B (SPECIAL)	FOOT	30	15	15			
* X7830068	GROOVING FOR RECESSED PAVEMNT MARKING, LETTERS, NUMBERS AND SYMBOLS	SQ FT	510	265	245				

*= SPECIALTY ITEM

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6;N	PEORIA	48	8
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE					
				URBAN		NHPP	STP	NHPP	STP
						80/20	80/20	80/20	80/20
						(FED./STATE)	(FED./STATE)	(FED./STATE)	(FED./STATE)
		ROADWAY	ROADWAY	SAFETY	SAFETY				
		0005	0005	0021	0021				
* X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	18162	11892	6270				
* X7830074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	2140	1420	720				
* X7830076	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	4929	2957	1972				
* X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	821	562	259				
* X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	531	179	352				
XZ013798	CONSTRUCTION STATION LAYOUT	L SUM	1	0.7	0.3				
Z0001002	GUARDRAIL AGGREGATE EROSION CONTROL	TON	263	121	142				
Z0033068	TRAFFIC SIGNAL BATTERY BACKUP SYSTEM	EACH	2			1	1		
Z0033072	VIDEO VEHICLE DETECTION SYSTEM	EACH	2			2			
Z0034105	MATERIAL TRANSFER DEVICE	TON	6811	4489	2322				

*= SPECIALTY ITEM

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-62N	PEORIA	48	9
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

RESURFACING SCHEDULE																		
LOCATION					40600982	44000152	44000155	44000157	44000159	X4401198	X4400196	48203100	X0300004	40603153	40603208	40800050	Z0034105	
					HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL	HOT-MIX ASPHALT SHOULDERS	BITUMINOUS MATERIALS (TACK COAT), SPECIAL	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	INCIDENTAL HOT-MIX ASPHALT SURFACING	MATERIAL TRANSFER DEVICE	
					SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	TON	POUND	TON	TON	TON	TON		
IL 40 STA	154+75	TO	155+05	LT/RT	297.9							429.0		29.2	33.4		62.6	
IL 40 STA	155+05	TO	162+00	RT		777.7					87.1	560.0						
IL 40 STA	155+05	TO	162+00	LT		790.7					69.1	569.3			19.4			
IL 40 STA	155+05	TO	162+00	LT/RT				5084.6				7321.8	498.3	569.5			1,067.8	
LINDBERGH DRIVE					75.6			1036.5		359.6		1059.6				164.8		
IL 40 STA	162+00	TO	177+75	RT		1813.7		5650.5			203.1	9442.6	553.7	632.9			1,186.6	
IL 40 STA	162+00	TO	177+75	LT		1783.2		4849.7			150.5	8267.4	475.3	543.2	49.2		1,018.4	
RAMP E					62.3							89.7	6.1	7.0			13.1	
RAMP D					151.0							217.4	14.8	16.9			31.7	
RAMP C					70.2							101.0	6.9	7.9			14.7	
IL 40 STA	177+75	TO	183+21	RT		614.1					68.8	442.1						
IL 40 STA	177+75	TO	183+11	LT	44.9	426.2		486.6			47.7	1072.1	52.1	59.5			111.6	
IL 40 STA	177+75	TO	183+21	LT/RT	161.5			3821.7				5735.9	390.4	446.1			836.5	
RAMP B					113.8							163.9	11.2	12.7			23.9	
RAMP A					140.4							202.2	13.8	15.7			29.5	
IL 40 STA	188+18	TO	193+50	RT	83.9	590.0		2025.2			66.1	3461.8	206.7	236.2			442.9	
IL 40 STA	188+06	TO	193+50	LT	146.3	538.3		2135.1			60.3	3672.8	223.6	255.5			479.1	
IL 40 STA	193+50	TO	198+75	RT		694.9		1823.8			47.8	3126.7	178.7	204.3	30.0		383.0	
IL 40 STA	193+50	TO	198+75	LT		479.2		1883.0			53.7	3056.6	184.5	210.9			395.4	
IL 40 STA	198+75	TO	199+05	RT				126.7	53.3		1.5	220.8	12.4	8.9	4.5		21.3	
IL 40 STA	198+75	TO	199+05	LT				86.7	31.3		3.5	147.3	8.5	6.1			14.6	
IL 40 STA	199+05	TO	202+20	RT			355.7				18.4	256.1			21.4			
IL 40 STA	199+05	TO	202+20	LT			498.7				10.1	359.0			45.8			
IL 40 STA	199+05	TO	202+20	LT/RT				2405.6				3464.1	235.7	168.4			404.1	
W. RICHMAR ROAD					42.5			275.3		129.5		322.1				50.1		
W. RAVINSWOOD ROAD					40.4			125.9		79.7		177.2				27.6		
IL 40 STA	202+20	TO	202+50	LT/RT	315.8							454.7	30.9	22.1			53.1	
ADDITIONAL QUANTITIES FOR MILL WIDE / PAVE WIDE																		
ALONG MAINLINE LONGITUDINAL JOINTS FOR ENTIRE LENGTH OF JOB					25.0				930.0		208.0	1525.0	114.0	107.0			220.9	
TOTALS					1,772	8,508	854	1,438	31,309	653	208	888	55,918	3,247	3,564	413		6,811

AGGREGATE WEDGE SHOULDER SCHEDULE	
LOCATION	48102100 AGGREGATE WEDGE SHOULDER, TYPE B
IL 40	TON
STA. 154+75 TO 183+21 RT	36.0
STA. 154+75 TO 183+11 LT	35.9
STA. 188+18 TO 202+50 RT	18.1
STA. 188+06 TO 202+50 LT	18.3
TOTAL	108

TEMPORARY RAMP SCHEDULE	
40600990 TEMPORARY RAMP	
LOCATION	SQ YD
JOBSITE	2,600
TOTAL	2,600

MOBILIZATION	
67100100 MOBILIZATION	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

CONSTRUCTION LAYOUT	
XZ013798 CONSTRUCTION STATION LAYOUT	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

TRAFFIC CONTROL SCHEDULE								
LOCATION	70100420 TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	70100820 TRAFFIC CONTROL AND PROTECTION, STANDARD 701451	70100825 TRAFFIC CONTROL AND PROTECTION, STANDARD 701456	70102630 TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	70102632 TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	70102635 TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	70103815 TRAFFIC CONTROL SURVEILLANCE	70107025 CHANGEABLE MESSAGE SIGN
IL 40	EACH	L SUM	L SUM	L SUM	L SUM	L SUM	CAL DA	CAL DA
JOBSITE	4	1	1	1	1	1	5	63
TOTALS	4	1	1	1	1	1	5	63

NOTE: 3 CHANGEABLE MESSAGE SIGNS AT 21 DAYS EACH

DRAINAGE SCHEDULE			
LOCATION	60260100 INLETS TO BE ADJUSTED	60260300 INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	21400100 GRADING AND SHAPING DITCHES
IL 40	EACH	EACH	FOOT
STA. 181+30 LT	1		
STA. 187+49 LT		1	
STA. 171+14 TO STA. 173+14 RT (ALONG OUTSIDE OF RAMP TO NB IL 6)			200
TOTAL	1	1	200

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USER NAME = fayantnd	DESIGNED -	REVISED -
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PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 5/31/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE. 646	SECTION (Y)RS-6-N	COUNTY PEORIA	TOTAL SHEETS 48	SHEET NO. 10
CONTRACT NO. 68E93			ILLINOIS FED. AID PROJECT	

CLASS D PATCHES														
LOCATION	LENGTH	WIDTH	44201803 TYPE II, 13"				44201807 TYPE III, 13"				44201809 TYPE IV, 13"			
			NORTHBOUND		SOUTHBOUND		NORTHBOUND		SOUTHBOUND		NORTHBOUND		SOUTHBOUND	
			# OF PATCHES	SQ YD	# OF PATCHES	SQ YD	# OF PATCHES	SQ YD	# OF PATCHES	SQ YD	# OF PATCHES	SQ YD	# OF PATCHES	SQ YD
STA. 154+75 TO STA. 202+50	6	12	25	200.0	75	600.0								
	15	12					2	40.0						
	40	12									1	53.3		
	46	12								2	122.7			
	46	8								1	40.9			
SUBTOTAL			200		600		40			164		53		
TOTAL				800			40	40			217			

PARTIAL DEPTH PATCHING		
LOCATION	X0556100 PARTIAL DEPTH PATCHING (SPECIAL)	X4421000 PARTIAL DEPTH PATCHING
	SQ. YD.	TON
STA. 183+98 TO STA. 184+11 RT (TO CORRECT DROP-OFF IN PCC SHOULDER)	14.3	1.6
STA. 187+22 TO STA. 187+37 RT (TO CORRECT DROP-OFF IN PCC APPROACH PAVT. AND SHLDR.)	59.0	6.6
STA. 183+21 TO STA. 184+11.5 AND STA. 187+11.5 TO STA. 188+18 LT AND RT (TO REPAIR LONGITUDINAL JOINTS IN PCC APPROACH PAVEMENT)	30.0	3.4
TOTAL	103	12

CURB AND GUTTER SCHEDULE				
LOCATION	44000500 COMBINATION CURB AND GUTTER REMOVAL	60605000 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	X0322494 CURB CUT	X6061005 CONCRETE CURB, TYPE B (SPECIAL)
	FOOT	FOOT	FOOT	FOOT
IL 40				
STA. 183+12 TO 183+27 LT	15	15		
STA. 187+10 TO 188+54 LT			144	
STA. 183+96.6 TO 184+11.6 RT				15
STA. 187+21.4 TO 187+36.4 RT				15
TOTAL	15	15	144	30

GUARDRAIL SCHEDULE									
LOCATION	63000001 STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	63100169 TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) FLARED	63100045 TRAFFIC BARRIER TERMINAL, TYPE 2	63100085 TRAFFIC BARRIER TERMINAL, TYPE 6	64300370 IMPACT ATTENUATORS (FULLY REDIRECTIVE, WIDE), TEST LEVEL 3	78200005 GUARDRAIL REFLECTORS, TYPE A	72501000 TERMINAL MARKER - DIRECT APPLIED	Z0001002 GUARDRAIL AGGREGATE EROSION CONTROL	63200310 GUARDRAIL REMOVAL
	FOOT	EACH	EACH	EACH	EACH	EACH	EACH	TON	FOOT
IL 40									
STA. 162+22.4 TO STA. 162+72.2 RT	87.5	1	1			4	1	30.4	
STA. 169+12.4 TO STA. 170+12.4 RT	37.5	1	1			4	1	20.3	
STA. 170+18 (SIGN FOUND. IN MEDIAN)					2				
STA. 182+06.3 TO STA. 184+13.2 LT	162.5		1	1		4		42.9	
STA. 182+26 TO STA. 184+12 RT									186
STA. 181+91 TO STA. 184+11 LT									220
STA. 182+71.0 TO STA. 184+13.5 RT	50	1		1		4	1	27.7	
STA. 186+96.3 TO STA. 188+29.8 LT	50	1		1		4	1	27.7	
STA. 187+21 TO STA. 193+66 RT									645
STA. 186+98 TO STA. 188+77 LT									179
STA. 187+19.5 TO STA. 188+23.5 RT	50		1	1		4		20.1	
STA. 187+30 (SIGN FOUND. IN MEDIAN)					2				
STA. 189+81.2 TO STA. 193+43.5 RT	300	1	1				1	73.4	
STA. 195+02 (SIGN FOUND. IN MEDIAN)					2				
STA. 195+02.8 TO STA. 196+02.7 LT	37.5	1	1			4	1	20.3	
TOTAL	775	6	6	4	6	28	6	263	1,230

FIELD OFFICE	
67000500 ENGINEER'S FIELD OFFICE, TYPE B	
LOCATION	CAL MO
JOBSITE	3
TOTAL	3

NOTE: FOR GUARDRAIL AGG. EROS. CONTR., THE QUANTITY CALCULATIONS ASSUMED A CROSS SECTIONAL AREA OF 4' WIDTH X 8" DEPTH AND 2.05 TONS/CU. YD.

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PLOT DATE = 5/31/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6:N	PEORIA	48	11
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

PAVEMENT MARKING														
LOCATION				78000100 THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	78000200 THERMOPLASTIC PAVEMENT MARKING - LINE 4"				78000400 THERMOPLASTIC PAVEMENT MARKING - LINE 6"	78000500 THERMOPLASTIC PAVEMENT MARKING - LINE 8"		78000600 THERMOPLASTIC PAVEMENT MARKING - LINE 12"		78000650 THERMOPLASTIC PAVEMENT MARKING - LINE 24"
IL 40				SQ FT	FOOT				FOOT	FOOT		FOOT		FOOT
					SOLID WHITE	SOLID YELLOW	DOUBLE SOLID YELLOW	SKIP DASH YELLOW		SOLID WHITE	DOTTED WHITE	SOLID YELLOW	SOLID WHITE	
STA	154+75	TO	167+00	234.0	2640.0	2566.0		160.0	600.0	1095.0	45.0		175.0	101.0
STA	167+00	TO	183+21	31.2	3289.0	3237.0			820.0	1571.0	246.0		387.0	78.0
STA	188+06	TO	202+50	244.4	2895.0	2451.0	924.0		720.0	1915.0	57.0	88.0	171.0	352.0
TOTALS				510	18,162				2,140	4,929		821		531

GROOVING FOR PAVEMENT MARKING										
LOCATION				X7830068 GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS	X7830070 GROOVING FOR RECESSED PAVEMENT MARKING, 5"	X7830074 GROOVING FOR RECESSED PAVEMENT MARKING, 7"	X7830076 GROOVING FOR RECESSED PAVEMENT MARKING, 9"	X7830078 GROOVING FOR RECESSED PAVEMENT MARKING, 13"	X7830090 GROOVING FOR RECESSED PAVEMENT MARKING, 25"	
IL 40				SQ FT	FOOT	FOOT	FOOT	FOOT	FOOT	
STA	154+75	TO	167+00	234.0	5366.0	600.0	1140.0	175.0	101.0	
STA	167+00	TO	183+21	31.2	6526.0	820.0	1817.0	387.0	78.0	
STA	188+06	TO	202+50	244.4	6270.0	720.0	1972.0	259.0	352.0	
TOTALS				510	18,162	2,140	4,929	821	531	

PAVEMENT MARKER SCHEDULE		
LOCATION	78300200 RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	78100100 RAISED REFLECTIVE PAVEMENT MARKER
IL 40	EACH	EACH
STA 154+75 TO 183+21	92.0	184.0
STA 188+06 TO 202+50	51.0	102.0
TOTALS	143	286

SHORT-TERM PAVEMENT MARKING		
LOCATION	70300100 SHORT-TERM PAVEMENT MARKING	70300150 SHORT-TERM PAVEMENT MARKING REMOVAL
IL 40	FOOT	SQ FT
STA 154+75 TO 202+50	16395.0	1989.0
TOTALS	16395	1989

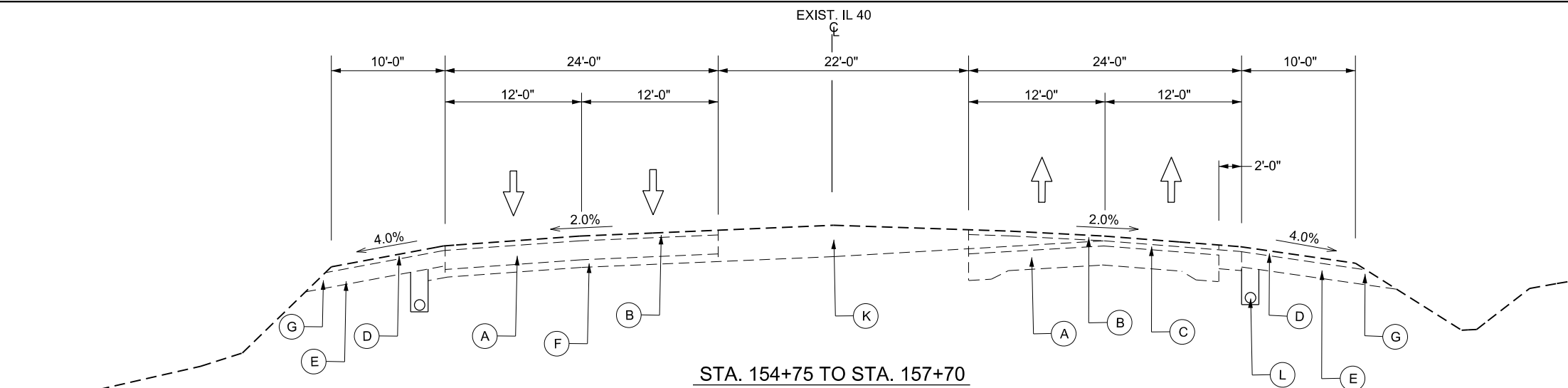
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PLOT DATE = 5/31/2019	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

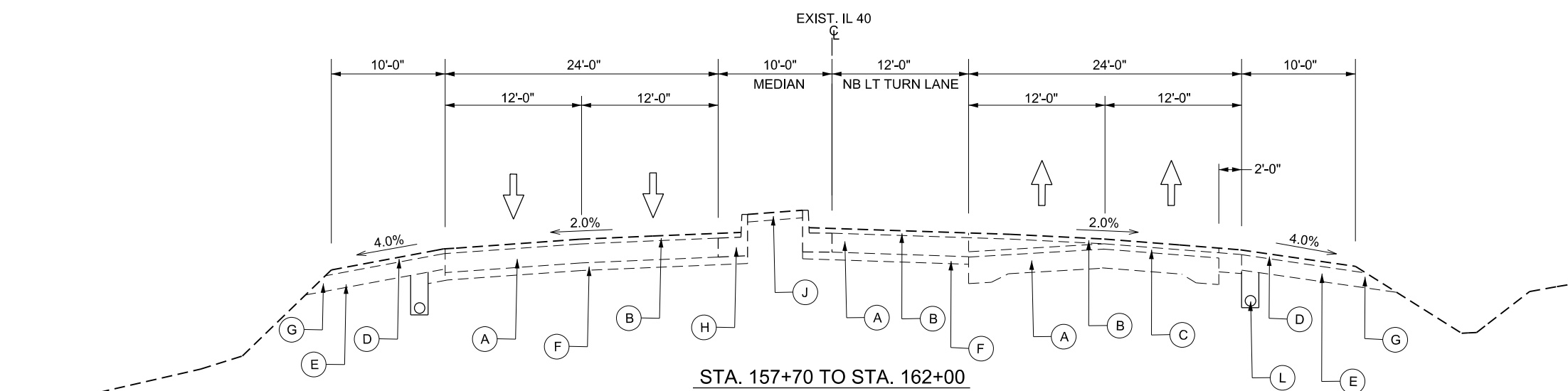
SCHEDULE OF QUANTITIES				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6:N	PEORIA	48	12
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

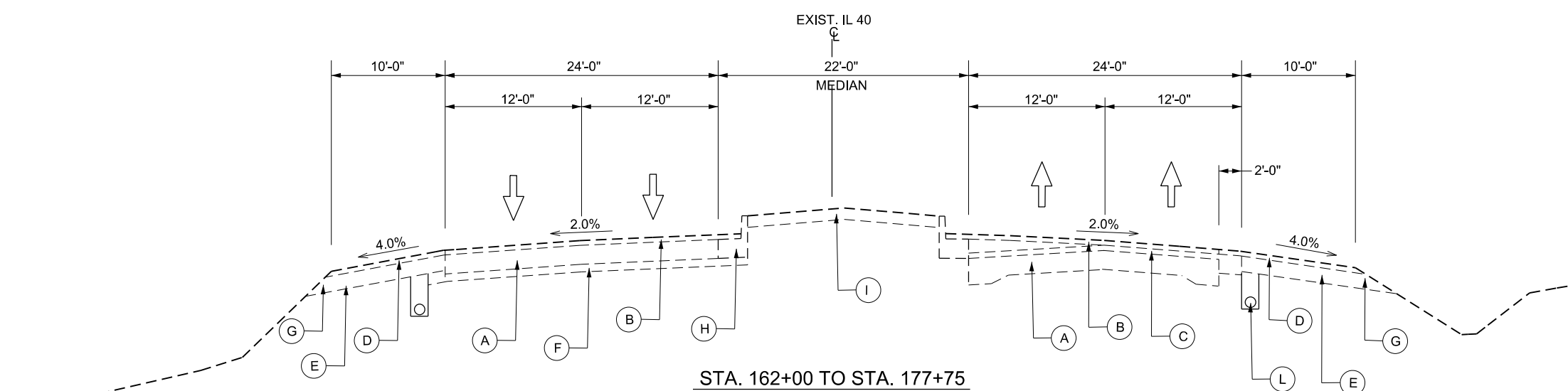


STA. 154+75 TO STA. 157+70

- LEGEND:**
- (A) EXISTING P.C.C. PAVEMENT, 10"
 - (B) EXISTING H.M.A. OVERLAY, 2.5"
 - (C) EXISTING H.M.A. OVERLAY, 2.75"
 - (D) EXISTING H.M.A. OVERLAY, 1.5"
 - (E) EXISTING H.M.A. SHOULDERS, 10"
 - (F) EXISTING STABILIZED SUB BASE, 4"
 - (G) EXISTING AGGREGATE SHOULDER TYPE B
 - (H) EXISTING TYPE B-6.24 C.C.C&G
 - (I) EXISTING GRASS MEDIAN
 - (J) EXISTING P.C.C. MEDIAN, 4"
 - (K) EXISTING H.M.A. PAVEMENT, VARIABLE THICKNESS
 - (L) EXISTING 4" PIPE UNDERDRAIN



STA. 157+70 TO STA. 162+00



STA. 162+00 TO STA. 177+75

NOT TO SCALE

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PLOT DATE = 5/31/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 40 (KNOXVILLE AVE.)
EXISTING TYPICAL SECTIONS**

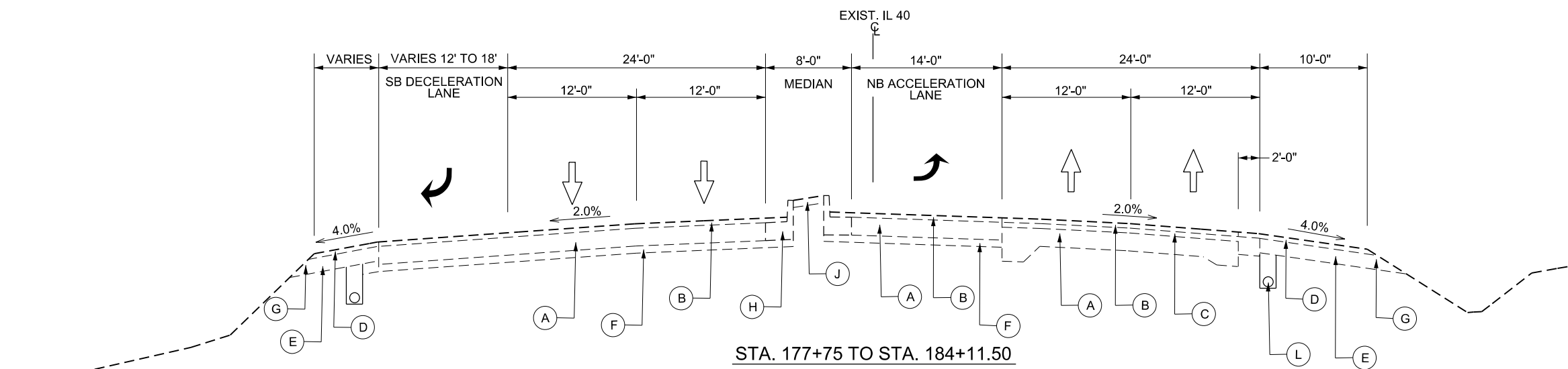
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CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.

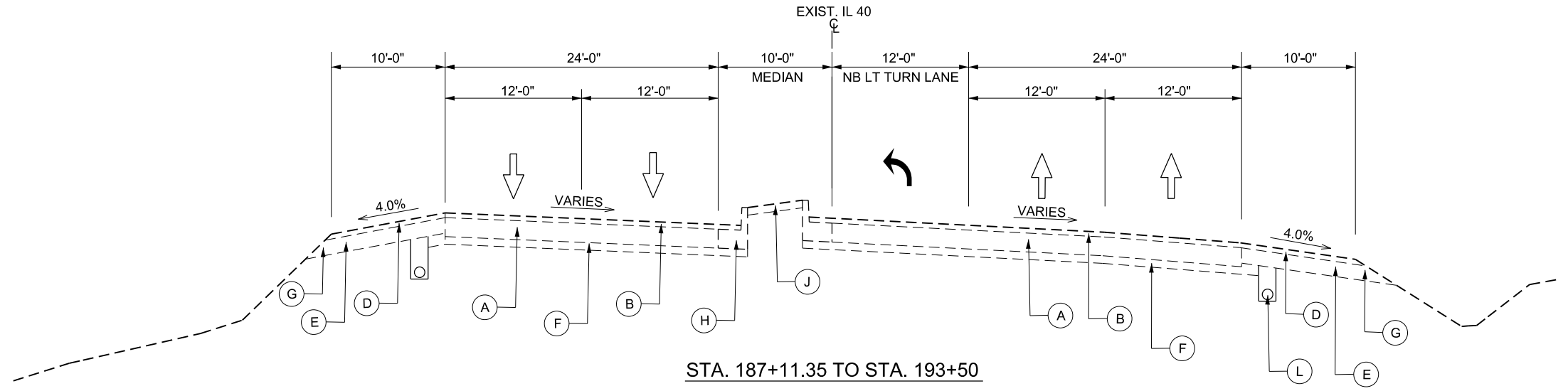
LEGEND:

- (A) EXISTING P.C.C. PAVEMENT, 10"
- (B) EXISTING H.M.A. OVERLAY, 2.5"
- (C) EXISTING H.M.A. OVERLAY, 2.75"
- (D) EXISTING H.M.A. OVERLAY, 1.5"
- (E) EXISTING H.M.A. SHOULDERS, 10"
- (F) EXISTING STABILIZED SUB BASE, 4"
- (G) EXISTING AGGREGATE SHOULDER TYPE B
- (H) EXISTING TYPE B-6.24 C.C.C&G
- (I) EXISTING GRASS MEDIAN
- (J) EXISTING P.C.C. MEDIAN, 4"
- (K) EXISTING H.M.A. PAVEMENT, VARIABLE THICKNESS
- (L) EXISTING 4" PIPE UNDERDRAIN

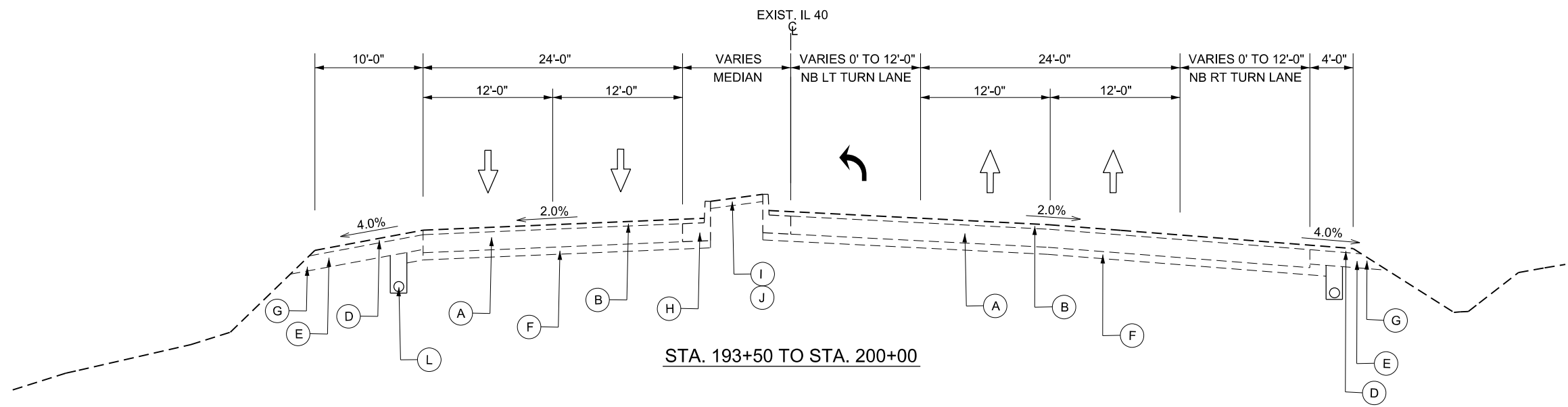
**BRIDGE OMISSION
STA. 184+11.50 TO 187+11.35**



STA. 177+75 TO STA. 184+11.50



STA. 187+11.35 TO STA. 193+50



STA. 193+50 TO STA. 200+00

NOT TO SCALE

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PLOT DATE = 5/31/2019	DATE -	REVISED -

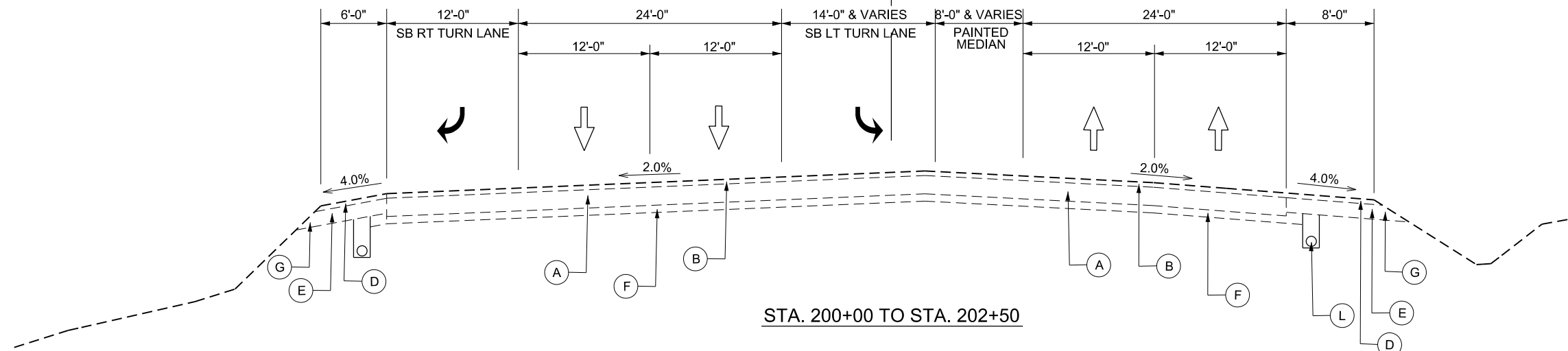
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 40 (KNOXVILLE AVE.)
EXISTING TYPICAL SECTIONS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 646	SECTION (Y)RS-6-N	COUNTY PEORIA	TOTAL SHEETS 48	SHEET NO. 14
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

EXIST. IL 40



STA. 200+00 TO STA. 202+50

- LEGEND:**
- (A) EXISTING P.C.C. PAVEMENT, 10"
 - (B) EXISTING H.M.A. OVERLAY, 2.5"
 - (C) EXISTING H.M.A. OVERLAY, 2.75"
 - (D) EXISTING H.M.A. OVERLAY, 1.5"
 - (E) EXISTING H.M.A. SHOULDERS, 10"
 - (F) EXISTING STABILIZED SUB BASE, 4"
 - (G) EXISTING AGGREGATE SHOULDER TYPE B
 - (H) EXISTING TYPE B-6.24 C.C.C&G
 - (I) EXISTING GRASS MEDIAN
 - (J) EXISTING P.C.C. MEDIAN, 4"
 - (K) EXISTING H.M.A. PAVEMENT, VARIABLE THICKNESS
 - (L) EXISTING 4" PIPE UNDERDRAIN

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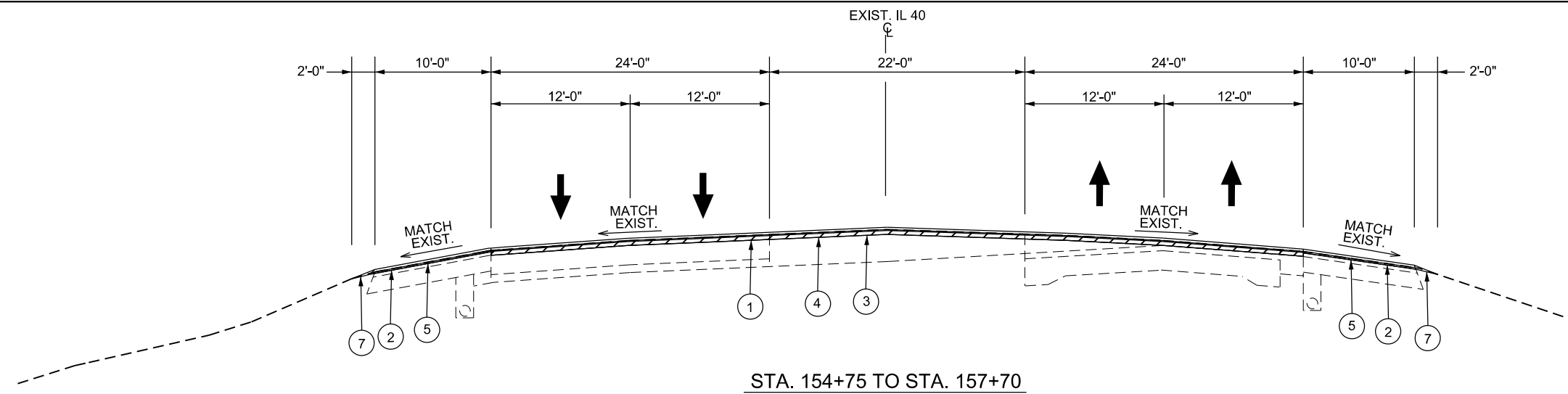
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PLOT DATE = 5/31/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

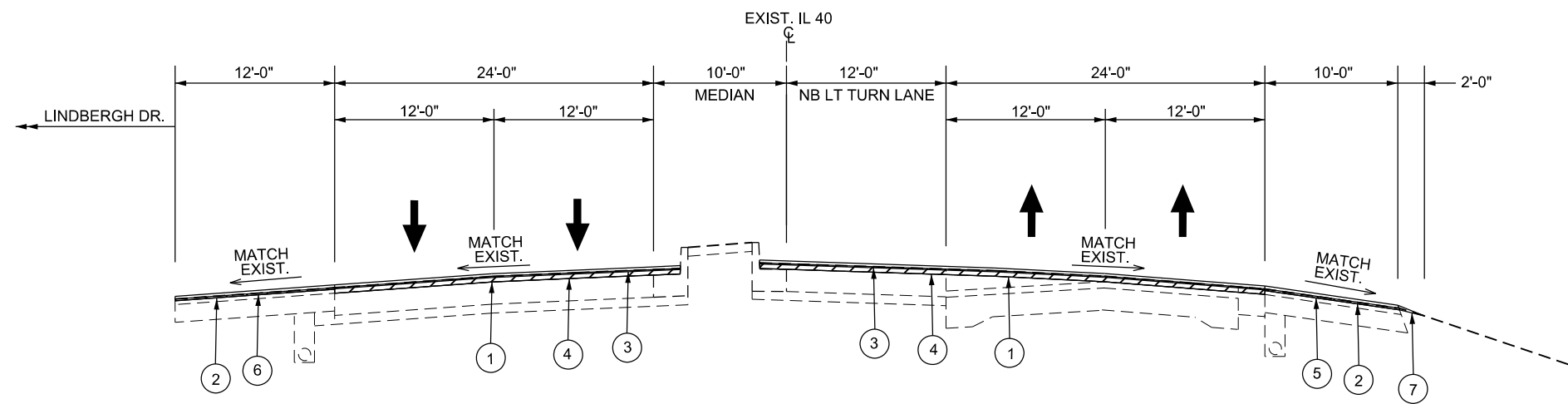
IL 40 (KNOXVILLE AVE.)
EXISTING TYPICAL SECTIONS

SCALE: SHEET OF SHEETS STA. TO STA.

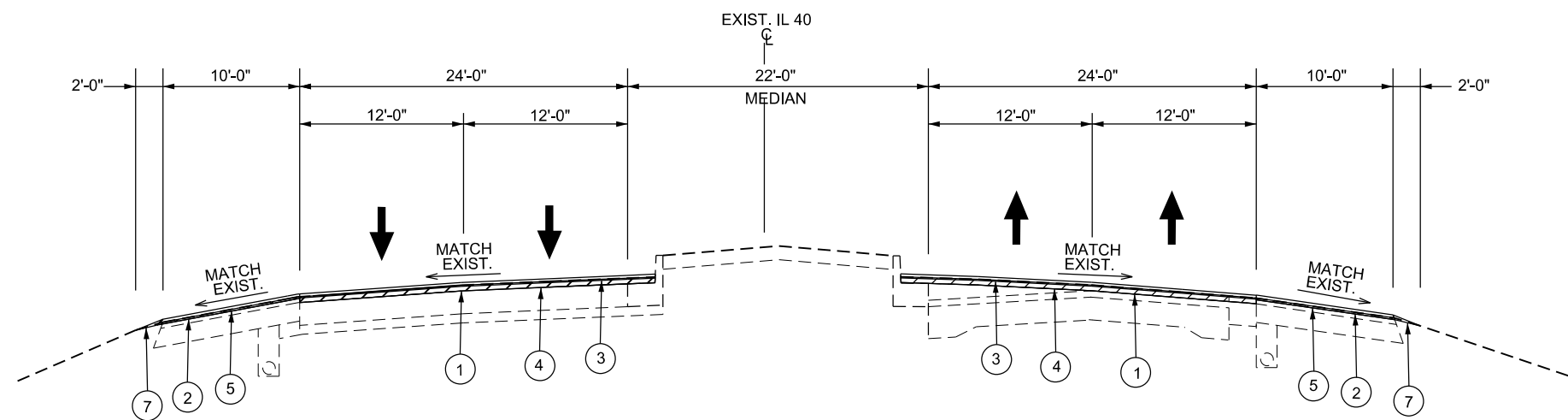
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			CONTRACT NO. 68E93	
		ILLINOIS FED. AID PROJECT		



STA. 154+75 TO STA. 157+70



STA. 157+70 TO STA. 162+00



STA. 162+00 TO STA. 177+75

LEGEND:

- ① HOT-MIX ASPHALT SURFACE REMOVAL, 2.5"
- ② HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
- ③ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 (1.75")
- ④ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (2")
- ⑤ HOT-MIX ASPHALT SHOULDERS (2")
- ⑥ INCIDENTAL HOT-MIX ASPHALT SURFACING (2")
- ⑦ AGGREGATE WEDGE SHOULDER, TYPE B
- ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, 1.5"
- ⑨ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (1.25")

NOT TO SCALE

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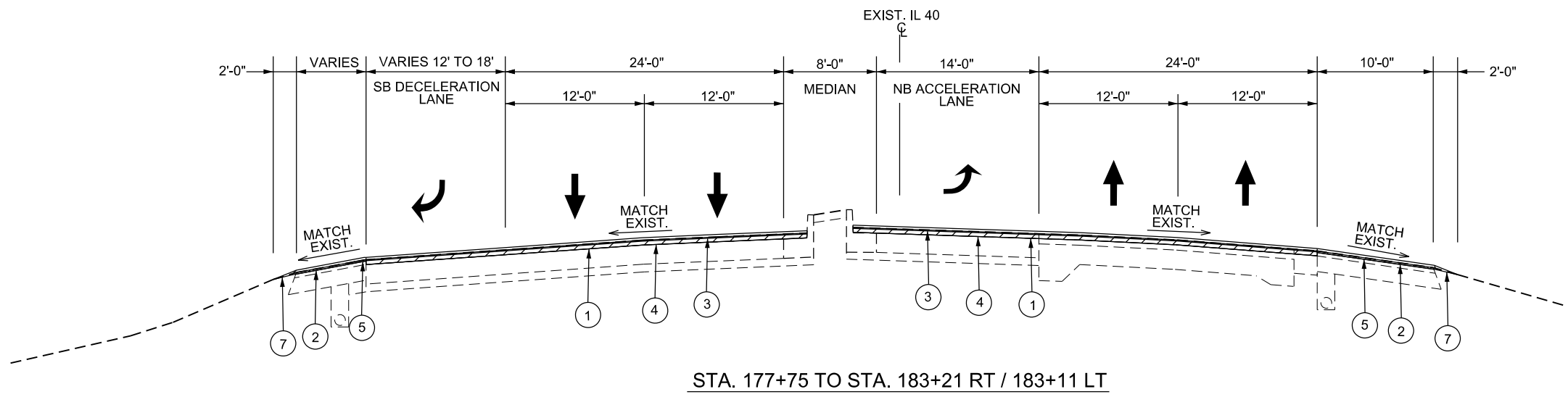
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 40 (KNOXVILLE AVE.)
PROPOSED TYPICAL SECTIONS**

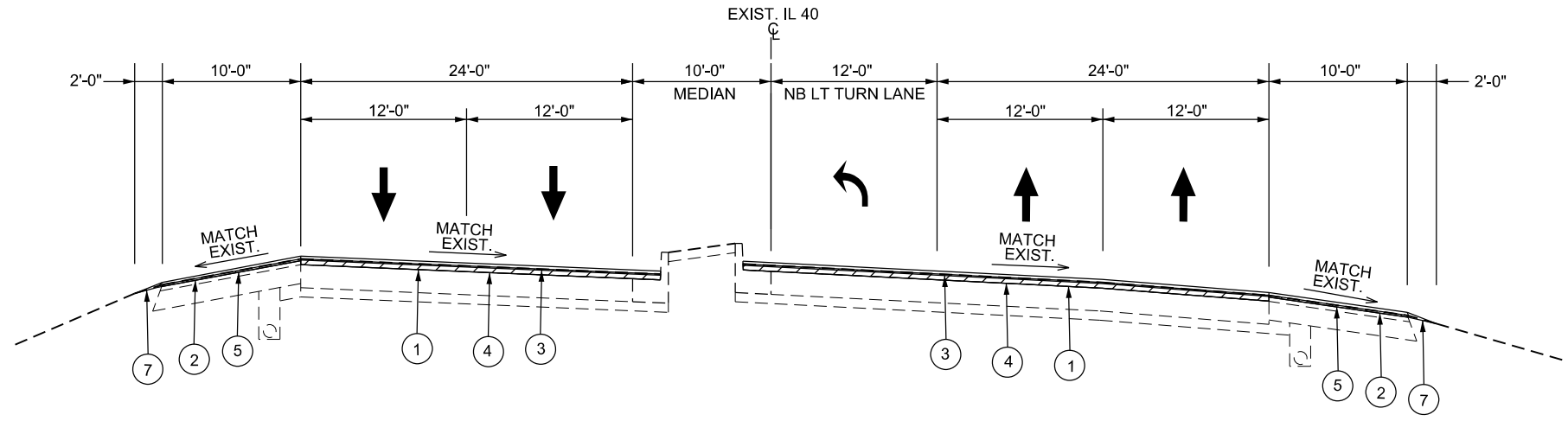
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6:N	PEORIA	48	16
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				



STA. 177+75 TO STA. 183+21 RT / 183+11 LT

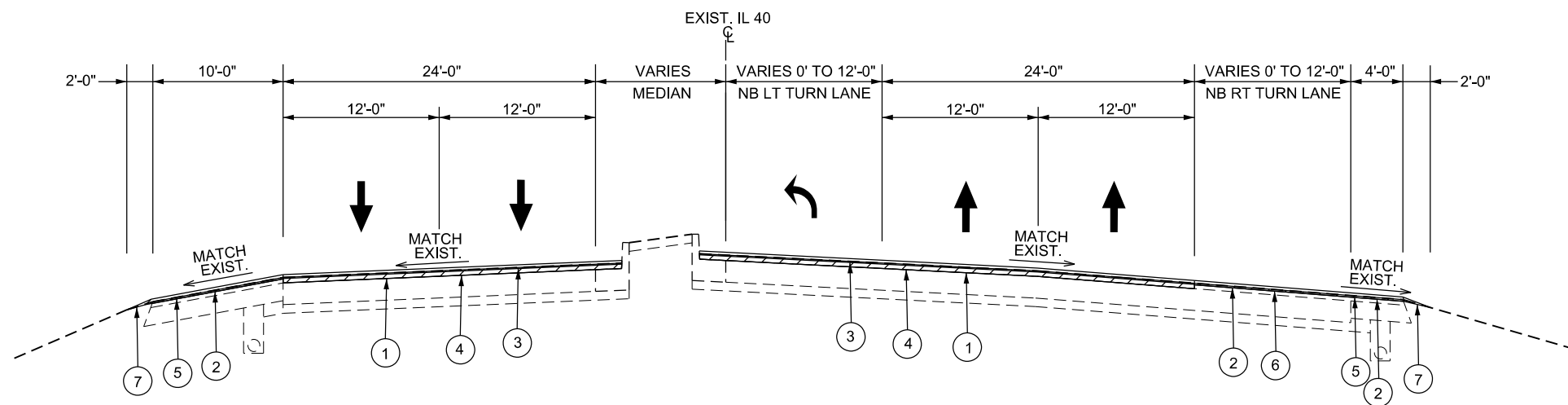
- LEGEND:**
- ① HOT-MIX ASPHALT SURFACE REMOVAL, 2.5"
 - ② HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
 - ③ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 (1.75")
 - ④ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (2")
 - ⑤ HOT-MIX ASPHALT SHOULDERS (2")
 - ⑥ INCIDENTAL HOT-MIX ASPHALT SURFACING (2")
 - ⑦ AGGREGATE WEDGE SHOULDER, TYPE B
 - ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, 1.5"
 - ⑨ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (1.25")



STA. 188+18 RT / 188+06 LT TO STA. 193+50

BRIDGE OMISSION
STA. 184+11.50 TO 187+11.35

BRIDGE APPROACH PAVEMENT OMISSION
STA. 183+21 TO 184+11.50 RT
STA. 183+11 TO 184+11.50 LT
STA. 187+11.35 TO 188+18 RT
STA. 187+11.35 TO 188+06 LT



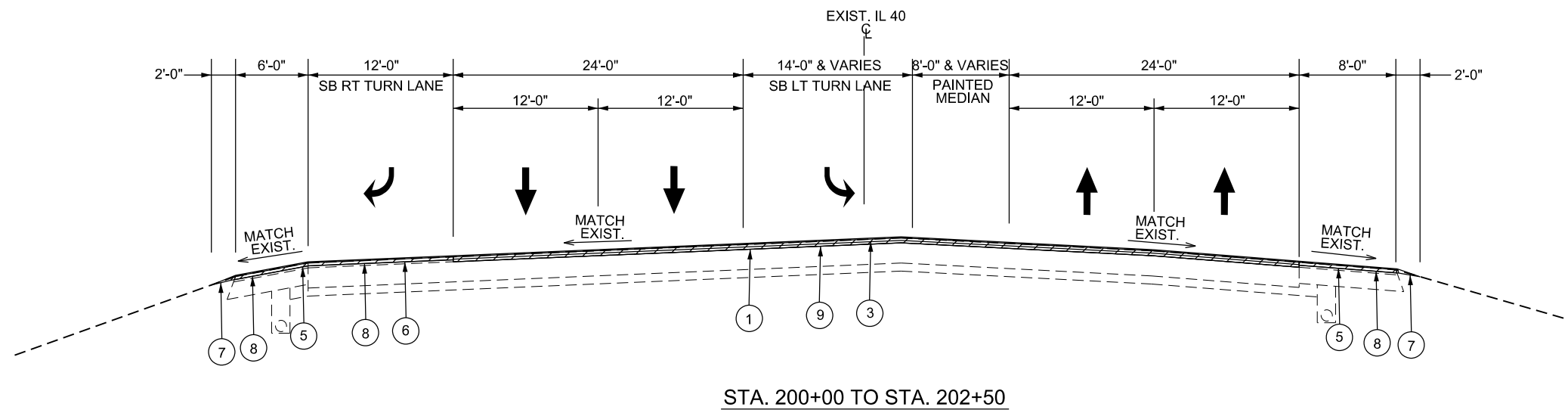
STA. 193+50 TO STA. 200+00

- PLEASE NOTE THE FOLLOWING BETWEEN STA. 198+75 TO 199+05:**
1. NB AND SB DRIVING LANES AND NB LEFT TURN LANE - TRANSITION THE POLYMERIZED HMA BINDER COURSE (IL-9.5, N70) DEPTH FROM 2" TO 1.25"
 2. NB RIGHT TURN LANE AND NB/SB SHOULDERS - TRANSITION THE HMA SURFACE REMOVAL DEPTH FROM 3/4" TO 1.5"

NOT TO SCALE

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PLOT SCALE = 1/2" = 12.0000' / in.	DRAWN -	REVISED -		SCALE:	SHEET OF SHEETS	STA. TO STA.	CONTRACT NO. 68993			
PLOT DATE = 5/31/2019	CHECKED -	REVISED -					ILLINOIS FED. AID PROJECT			
	DATE -	REVISED -								



- LEGEND:**
- ① HOT-MIX ASPHALT SURFACE REMOVAL, 2.5"
 - ② HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
 - ③ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 (1.75")
 - ④ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (2")
 - ⑤ HOT-MIX ASPHALT SHOULDERS (2")
 - ⑥ INCIDENTAL HOT-MIX ASPHALT SURFACING (2")
 - ⑦ AGGREGATE WEDGE SHOULDER, TYPE B
 - ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, 1.5"
 - ⑨ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (1.25")

STA. 200+00 TO STA. 202+50

NOT TO SCALE

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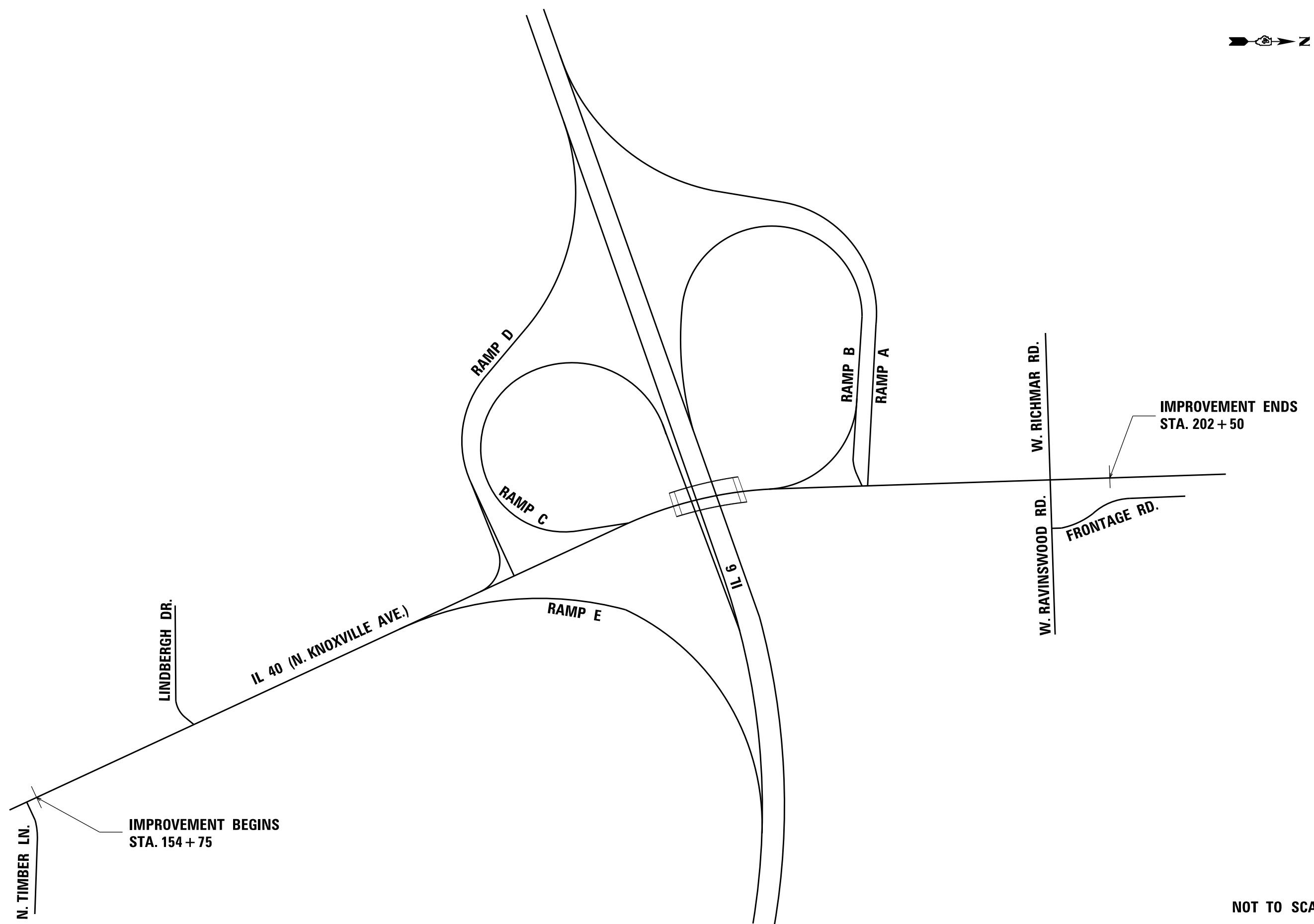
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PLOT DATE = 5/31/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 40 (KNOXVILLE AVE.)
PROPOSED TYPICAL SECTIONS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6:N	PEORIA	48	18
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				



NOT TO SCALE

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







**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

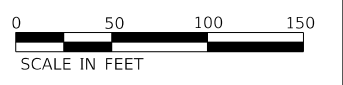
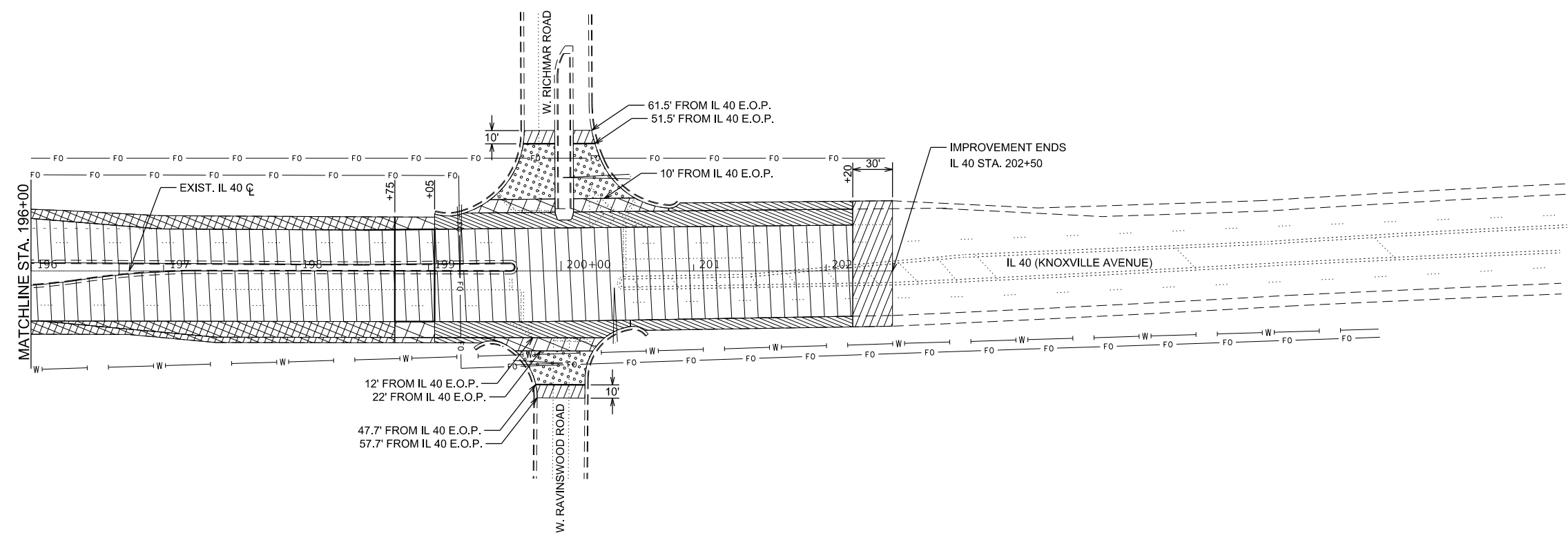
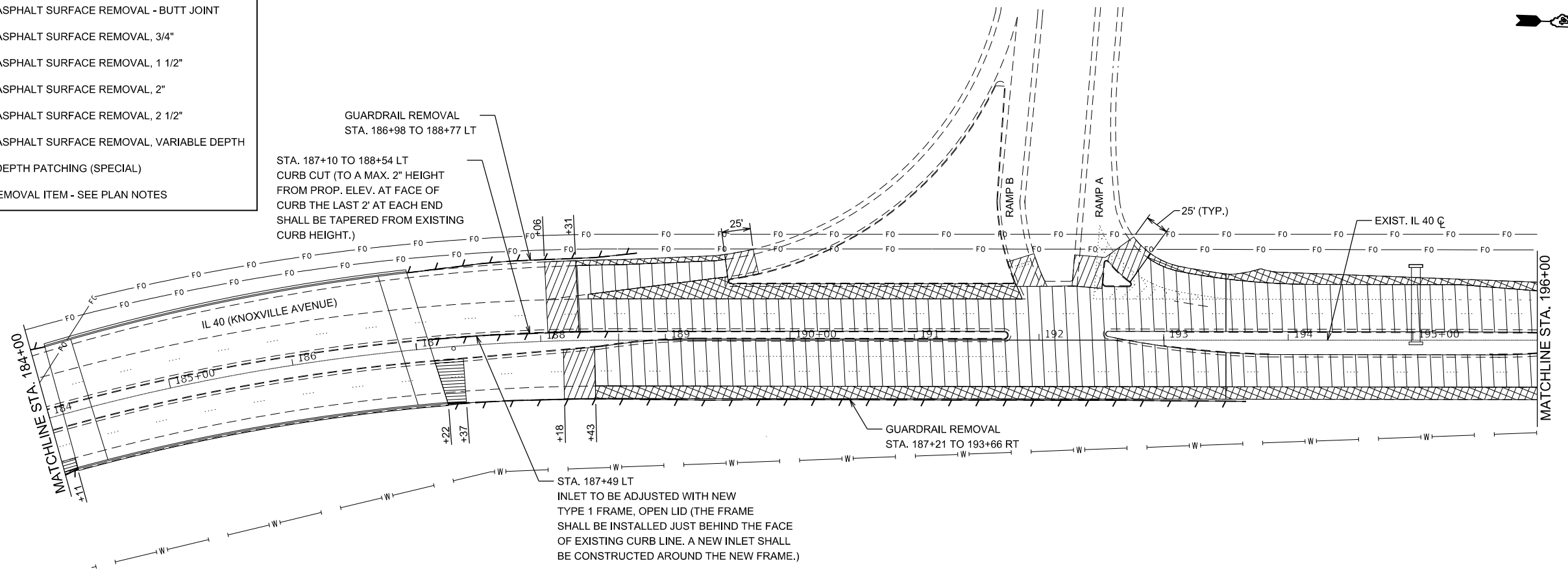
LINE DIAGRAM

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6:N	PEORIA	48	19
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

LEGEND

-  HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
-  HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
-  HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
-  HOT-MIX ASPHALT SURFACE REMOVAL, 2"
-  HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
-  HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
-  PARTIAL DEPTH PATCHING (SPECIAL)
-  LINEAR REMOVAL ITEM - SEE PLAN NOTES



MODEL: Remove of Plan Sheets
 FILE NAME: S:\ENR\11\11\GSD\STDP\PLUS\5\sum0368E93_Knoxville_Curbcut\Drawings\68E93_Design.dgn

USER NAME = fayantnd	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 5/31/2019	DATE -	REVISED -







**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

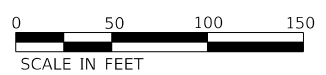
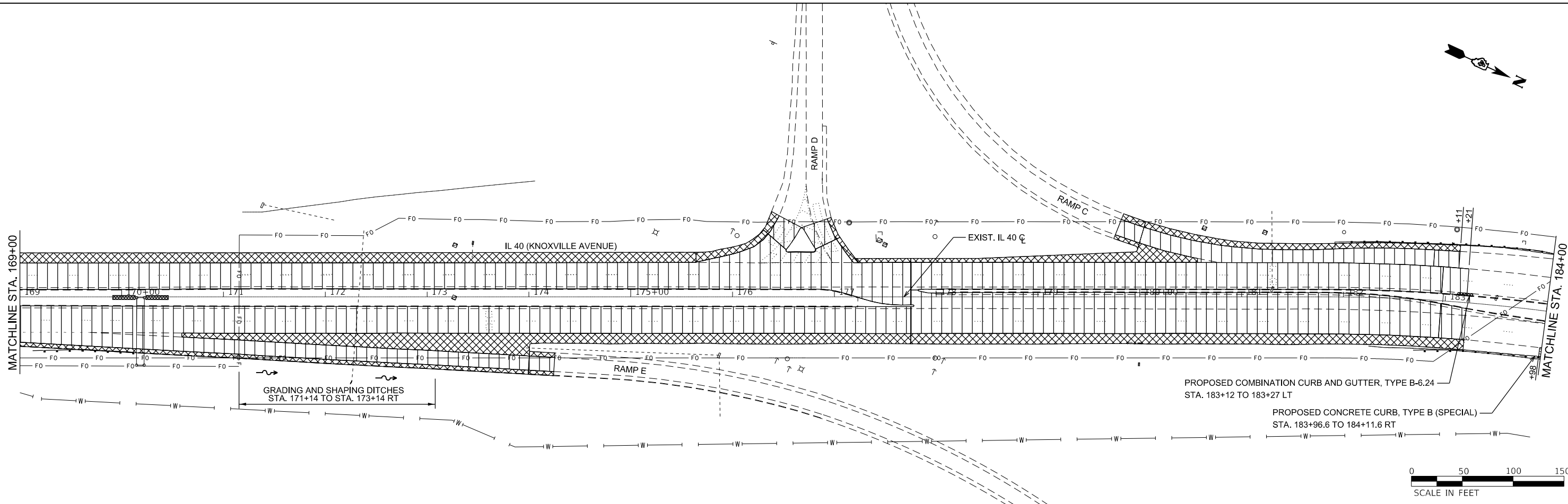
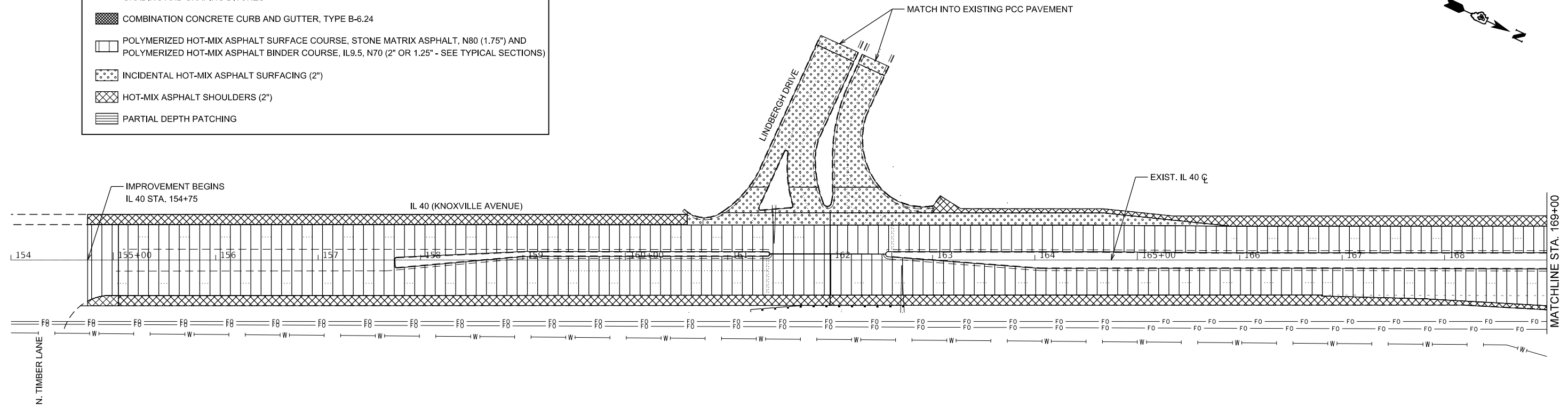
**IL 40 (KNOXVILLE AVE.)
REMOVAL PLAN**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6:N	PEORIA	48	21
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

LEGEND

-  GRADING AND SHAPING DITCHES
-  COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
-  POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 (1.75") AND POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL9.5, N70 (2" OR 1.25" - SEE TYPICAL SECTIONS)
-  INCIDENTAL HOT-MIX ASPHALT SURFACING (2")
-  HOT-MIX ASPHALT SHOULDERS (2")
-  PARTIAL DEPTH PATCHING



MODEL: Proposed Resurfacing Sheets
 FILE NAME: S:\ENR\171023\TDR\15\5000\03\68E93_Knoxville_Curb\Drawings\68E93_Dr\prop.dgn

USER NAME = fayantnd	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 5/31/2019	DATE -	REVISED -







**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

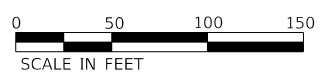
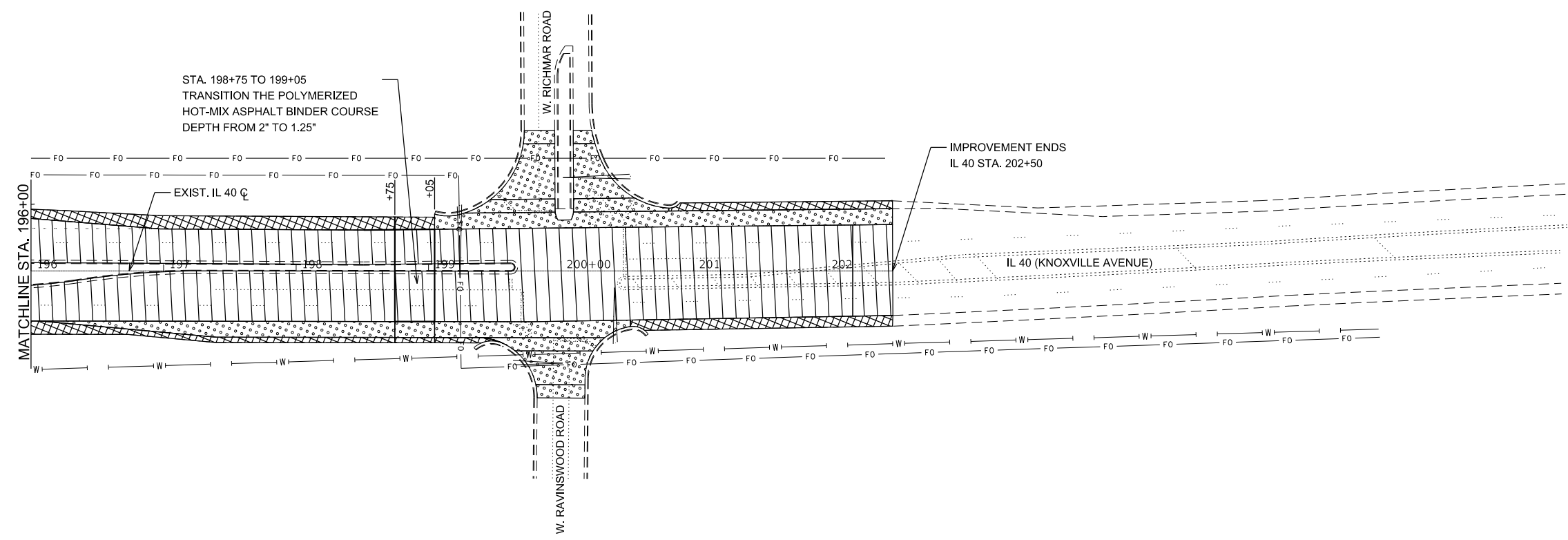
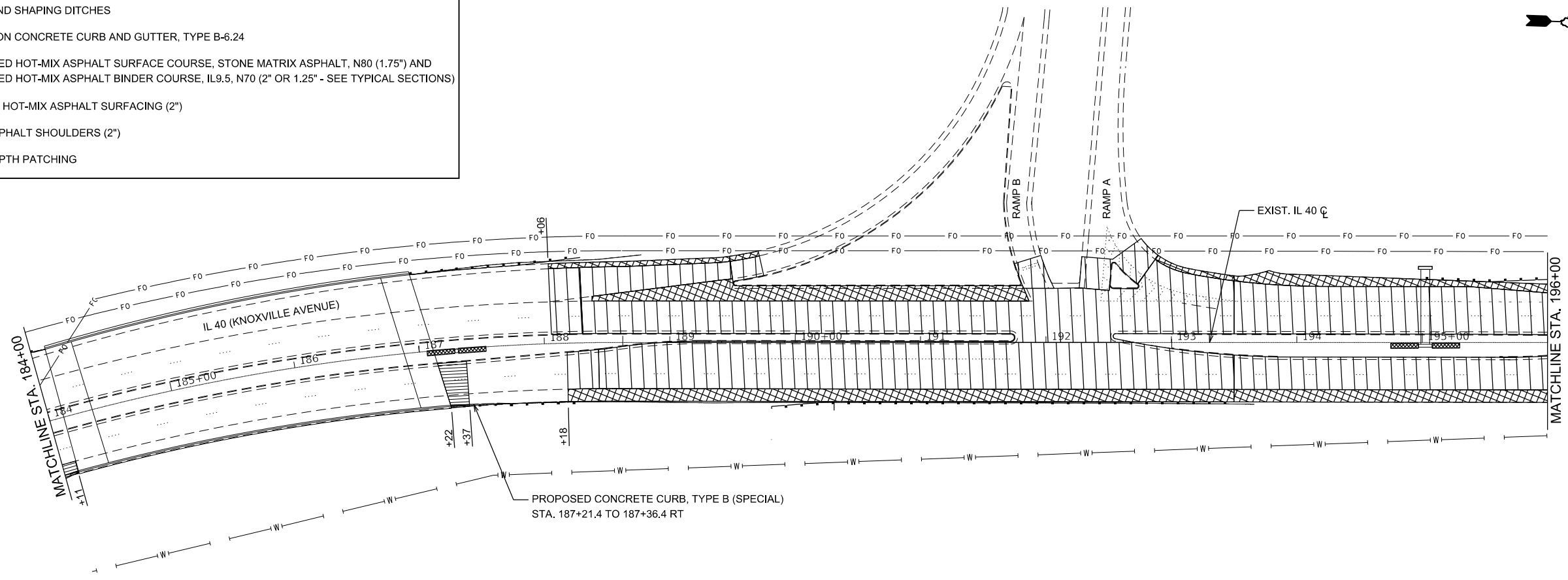
**IL 40 (KNOXVILLE AVE.)
PROPOSED RESURFACING PLAN**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 646	SECTION (Y)RS-6:N	COUNTY PEORIA	TOTAL SHEETS 48	SHEET NO. 22
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

LEGEND

-  GRADING AND SHAPING DITCHES
-  COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
-  POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 (1.75") AND POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL9.5, N70 (2" OR 1.25" - SEE TYPICAL SECTIONS)
-  INCIDENTAL HOT-MIX ASPHALT SURFACING (2")
-  HOT-MIX ASPHALT SHOULDERS (2")
-  PARTIAL DEPTH PATCHING



MODEL: Proposed Resurfacing Sheets
FILE NAME: S:\ENR\11\03\17\68E93\Knoxville County\Drawings\68E93_Design.dgn

USER NAME = fayantnd	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 5/31/2019	CHECKED -	REVISED -
	DATE -	REVISED -

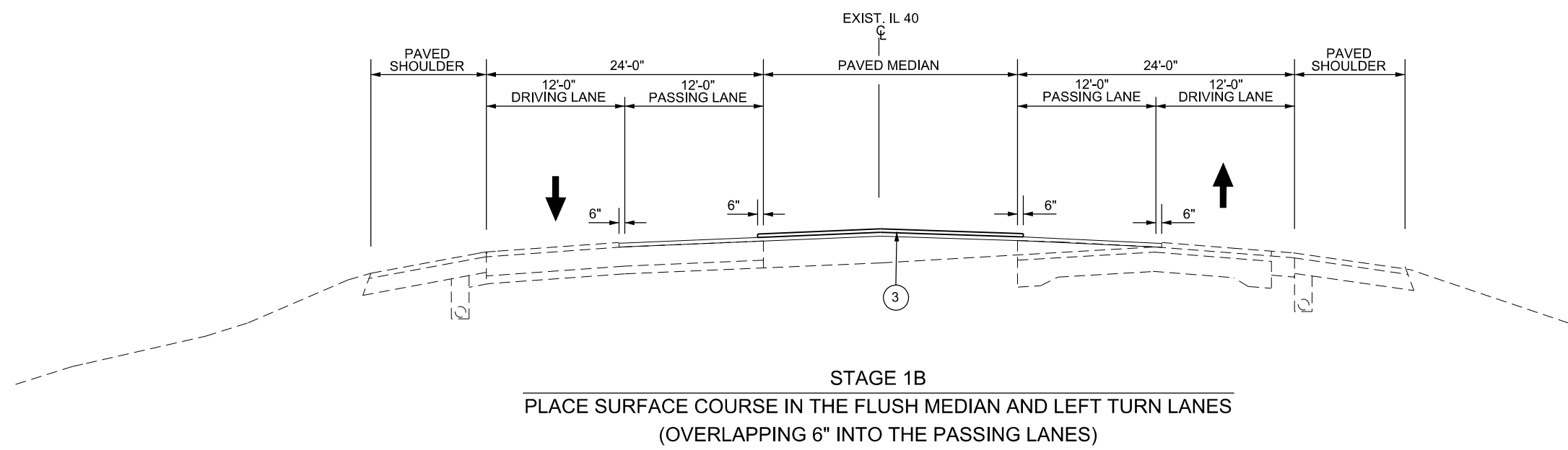
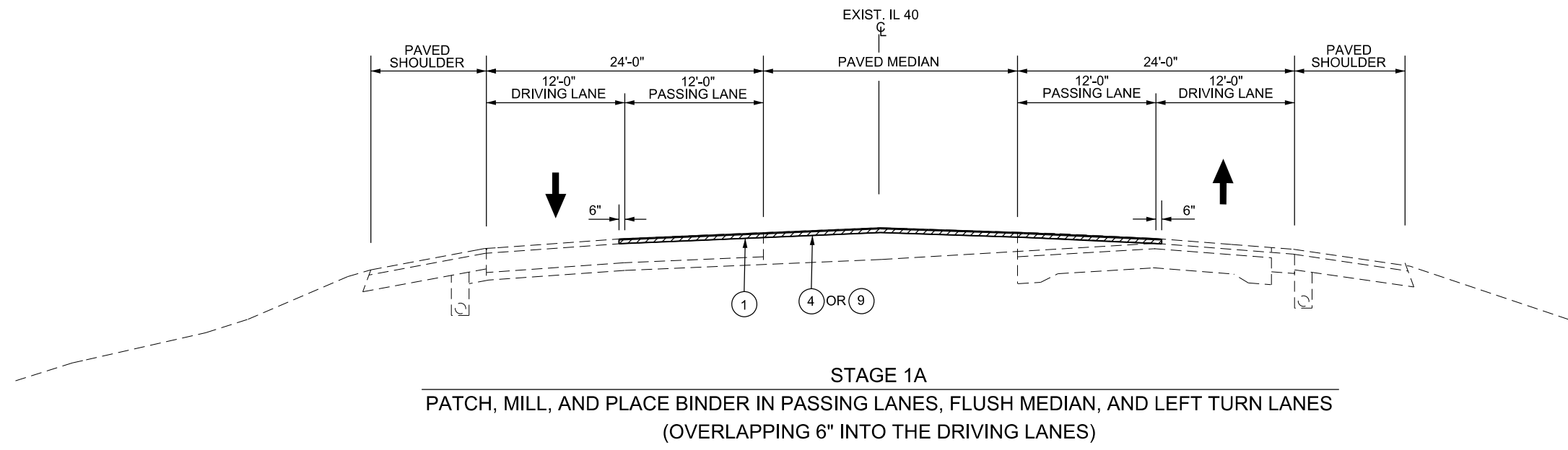
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 40 (KNOXVILLE AVE.)
PROPOSED RESURFACING PLAN**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6:N	PEORIA	48	23
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

- LEGEND:**
- ① HOT-MIX ASPHALT SURFACE REMOVAL, 2.5"
 - ② HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
 - ③ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 (1.75")
 - ④ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (2")
 - ⑤ HOT-MIX ASPHALT SHOULDERS (2")
 - ⑥ INCIDENTAL HOT-MIX ASPHALT SURFACING (2")
 - ⑦ AGGREGATE WEDGE SHOULDER, TYPE B
 - ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, 1.5"
 - ⑨ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (1.25")



NOT TO SCALE

MODEL STAGING
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USER NAME = fayantnd	DESIGNED -	REVISED -
	DRAWN -	REVISED -
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PLOT DATE = 5/31/2019	DATE -	REVISED -

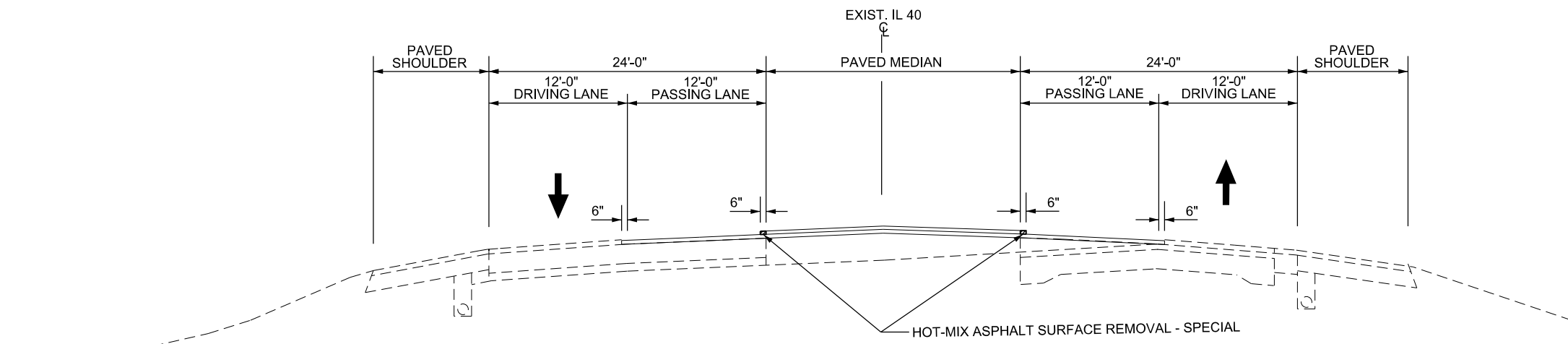
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 40 (KNOXVILLE AVE.)
STAGING DETAILS

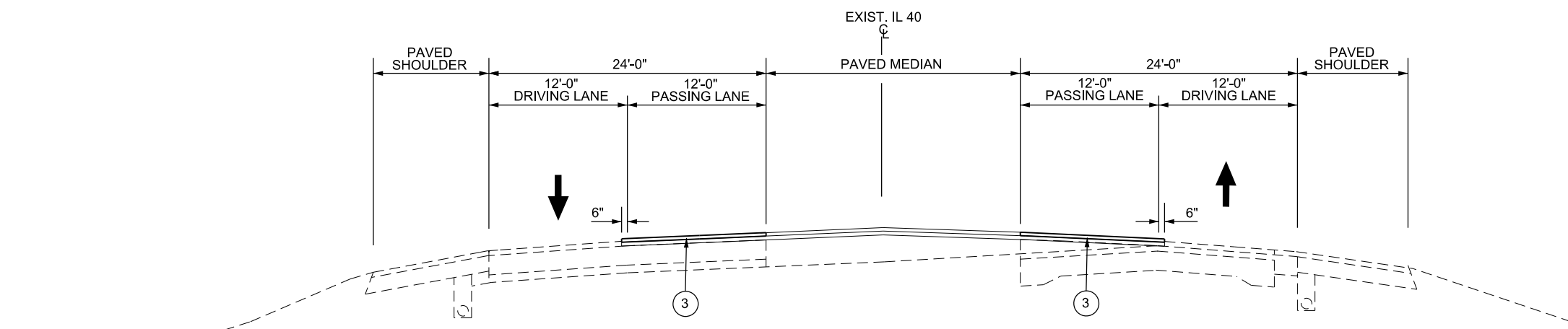
SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6:N	PEORIA	48	24
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

- LEGEND:**
- ① HOT-MIX ASPHALT SURFACE REMOVAL, 2.5"
 - ② HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
 - ③ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 (1.75")
 - ④ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (2")
 - ⑤ HOT-MIX ASPHALT SHOULDERS (2")
 - ⑥ INCIDENTAL HOT-MIX ASPHALT SURFACING (2")
 - ⑦ AGGREGATE WEDGE SHOULDER, TYPE B
 - ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, 1.5"
 - ⑨ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (1.25")



STAGE 1C
MILL OVERLAPPED 6" WIDE STRIPS IN PASSING LANES



STAGE 1D
PLACE SURFACE COURSE IN PASSING LANES
(OVERLAPPING 6" INTO THE DRIVING LANES)

NOT TO SCALE

MODEL STAGING
FILE NAME: S:\CEN\WORK\STDP\PLUS\5\5\2019\Drawings\188E93 - Guardrail and Impact Attenuators.dgn

USER NAME = fayantnd	DESIGNED -	REVISED -
	DRAWN -	REVISED -
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PLOT DATE = 5/31/2019	DATE -	REVISED -

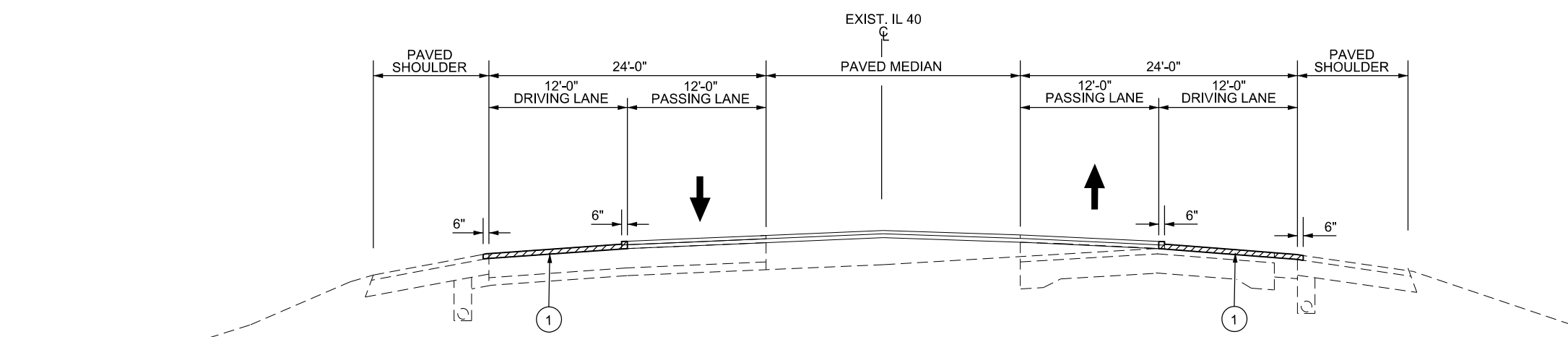
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 40 (KNOXVILLE AVE.)
STAGING DETAILS

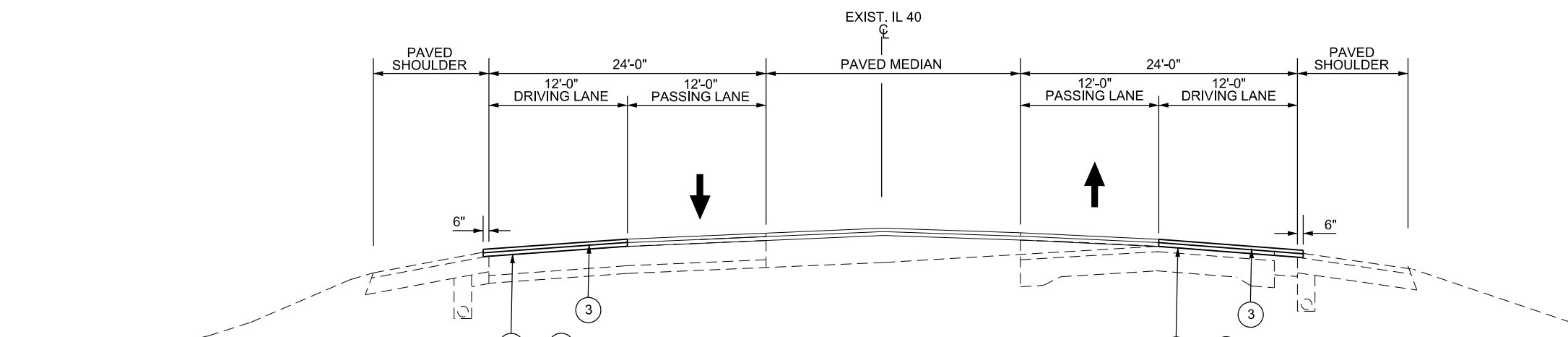
SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6:N	PEORIA	48	25
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

- LEGEND:**
- ① HOT-MIX ASPHALT SURFACE REMOVAL, 2.5"
 - ② HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
 - ③ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 (1.75")
 - ④ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (2")
 - ⑤ HOT-MIX ASPHALT SHOULDERS (2")
 - ⑥ INCIDENTAL HOT-MIX ASPHALT SURFACING (2")
 - ⑦ AGGREGATE WEDGE SHOULDER, TYPE B
 - ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, 1.5"
 - ⑨ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (1.25")



STAGE 2A
PATCH AND MILL DRIVING LANES
(OVERLAPPING 6" INTO THE SHOULDERS)



STAGE 2B
PLACE BINDER COURSE AND SURFACE COURSE IN DRIVING LANES
(OVERLAPPING 6" INTO THE SHOULDERS)

NOT TO SCALE

MODEL STAGING
 FILE NAME: S:\CEN\WORK\STDP\PLUS\5\sub\0368E93_Knoxville_Corridor\Drawings\68E93_Guardrail_and_Impact_Atenuators.dgn

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	DRAWN -	REVISED -
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PLOT DATE = 5/31/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

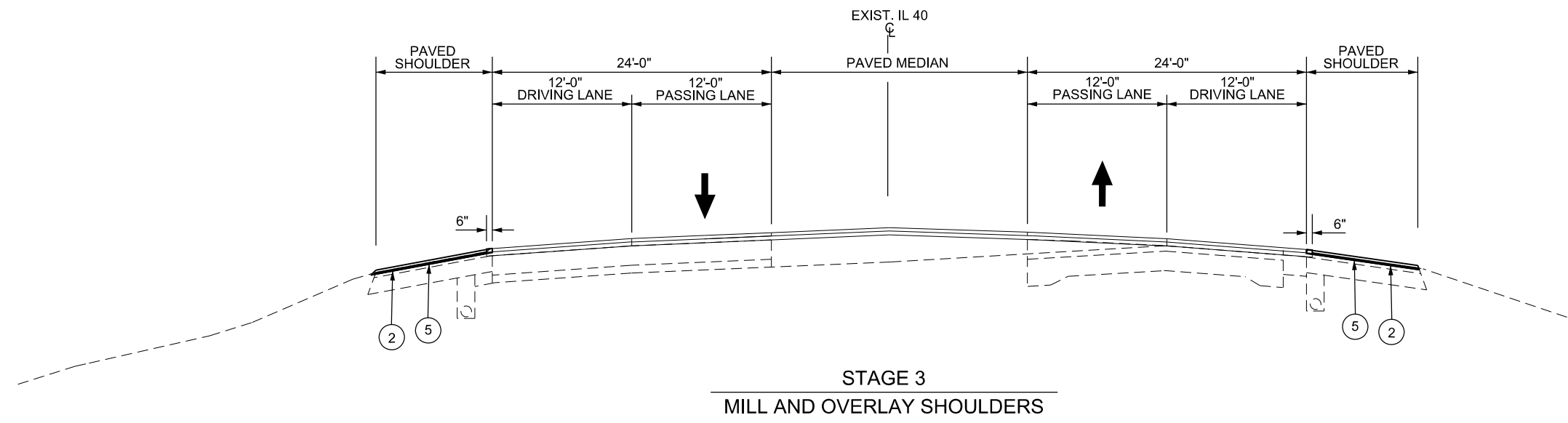
IL 40 (KNOXVILLE AVE.)
STAGING DETAILS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6:N	PEORIA	48	26
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

LEGEND:

- ① HOT-MIX ASPHALT SURFACE REMOVAL, 2.5"
- ② HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
- ③ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 (1.75")
- ④ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (2")
- ⑤ HOT-MIX ASPHALT SHOULDERS (2")
- ⑥ INCIDENTAL HOT-MIX ASPHALT SURFACING (2")
- ⑦ AGGREGATE WEDGE SHOULDER, TYPE B
- ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, 1.5"
- ⑨ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (1.25")



NOT TO SCALE

MODEL STAGING
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	DRAWN -	REVISED -
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PLOT DATE = 5/31/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

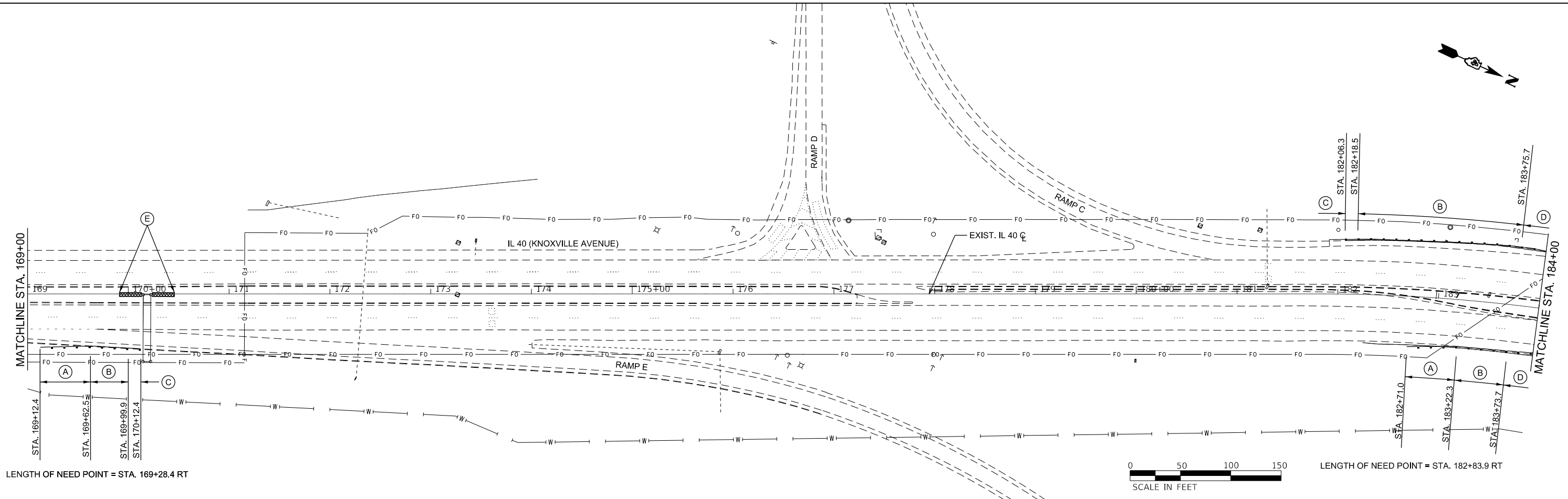
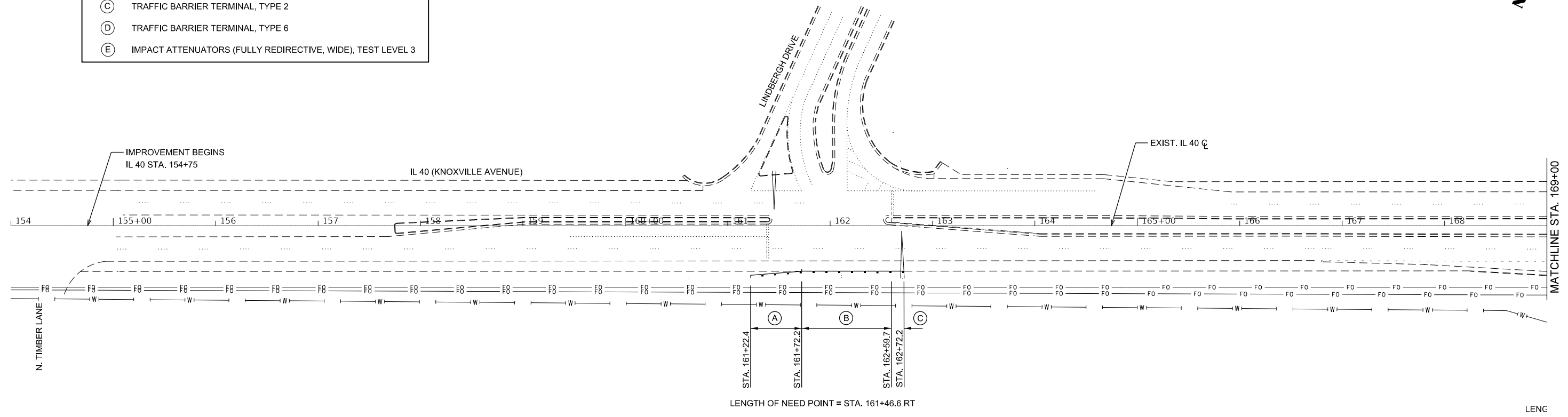
IL 40 (KNOXVILLE AVE.)
STAGING DETAILS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6:N	PEORIA	48	27
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

LEGEND

- (A) TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) FLARED
- (B) STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
- (C) TRAFFIC BARRIER TERMINAL, TYPE 2
- (D) TRAFFIC BARRIER TERMINAL, TYPE 6
- (E) IMPACT ATTENUATORS (FULLY REDIRECTIVE, WIDE), TEST LEVEL 3



MODEL: Guardrail.dwg; Details
 FILE: Model_Spec/IL40/IL40Ramp.dwg; Knoxville_Corridor/Drawings/68E93_Design.dwg
 MODEL: Guardrail.dwg; Details
 FILE: Model_Spec/IL40/IL40Ramp.dwg; Knoxville_Corridor/Drawings/68E93_Design.dwg

USER NAME = fayantnd	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 5/31/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

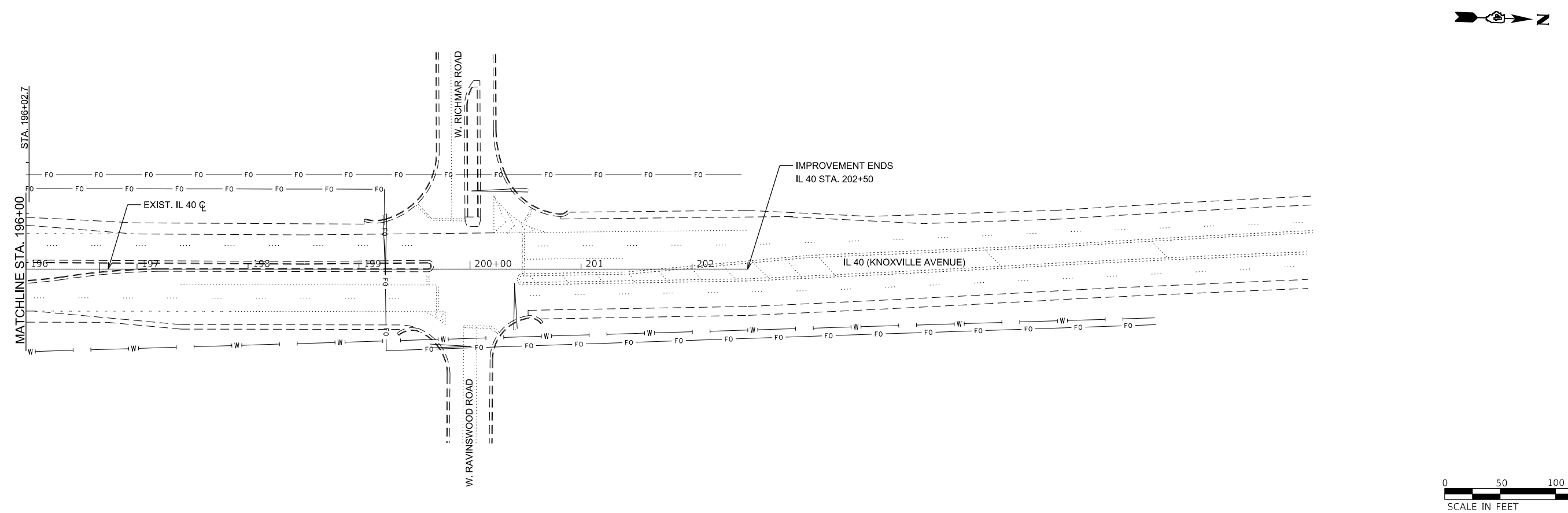
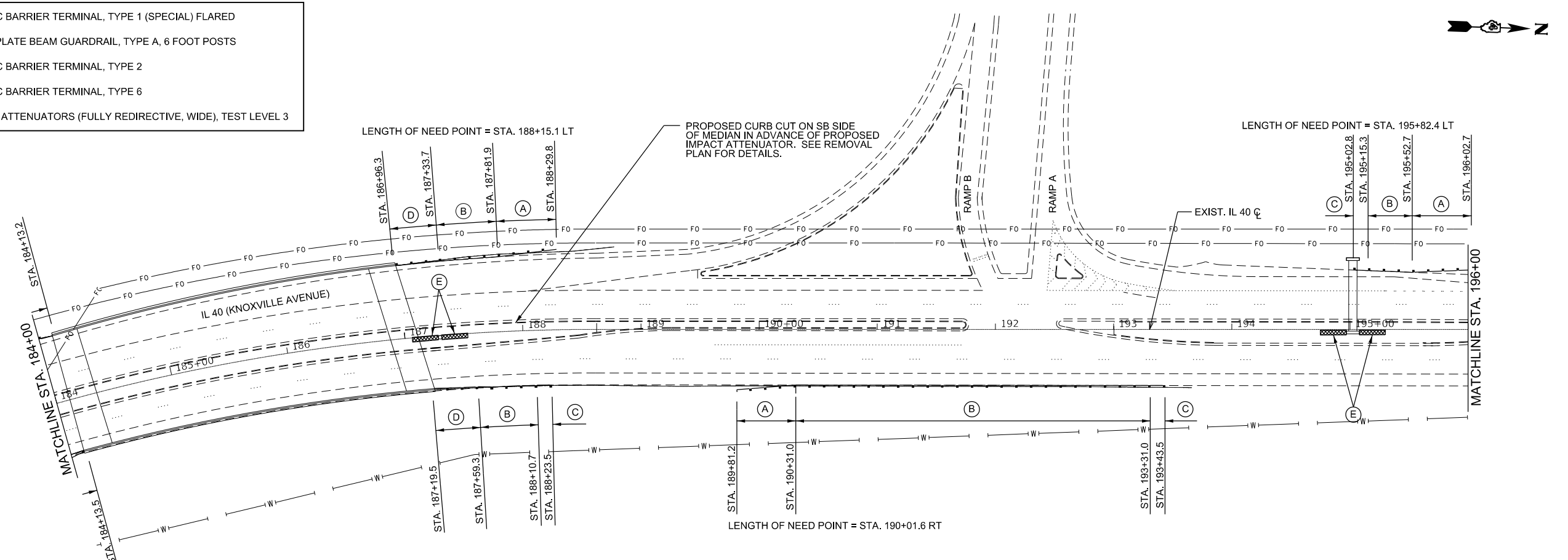
IL 40 (KNOXVILLE AVE.)
GUARDRAIL DETAILS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 646	SECTION (Y)RS-6:N	COUNTY PEORIA	TOTAL SHEETS 48	SHEET NO. 28
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

LEGEND

- (A) TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) FLARED
- (B) STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
- (C) TRAFFIC BARRIER TERMINAL, TYPE 2
- (D) TRAFFIC BARRIER TERMINAL, TYPE 6
- (E) IMPACT ATTENUATORS (FULLY REDIRECTIVE, WIDE), TEST LEVEL 3



MODEL: Guardrail Details
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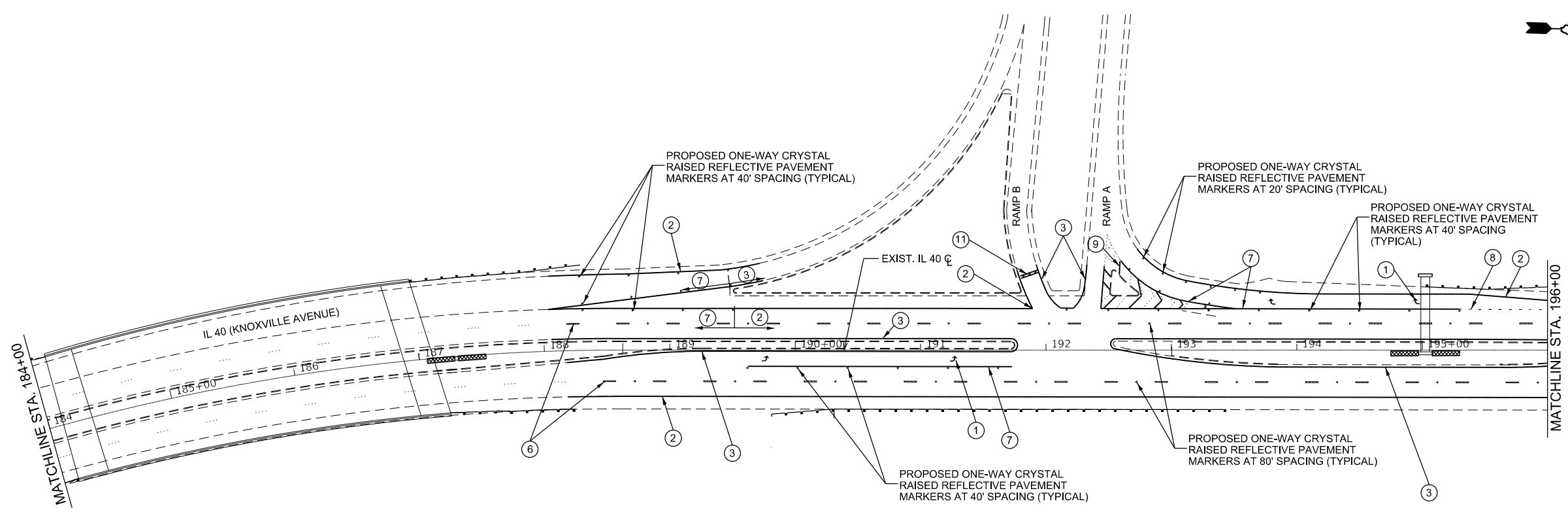
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PLOT DATE = 5/31/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 40 (KNOXVILLE AVE.)
GUARDRAIL DETAILS**

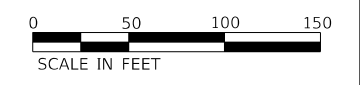
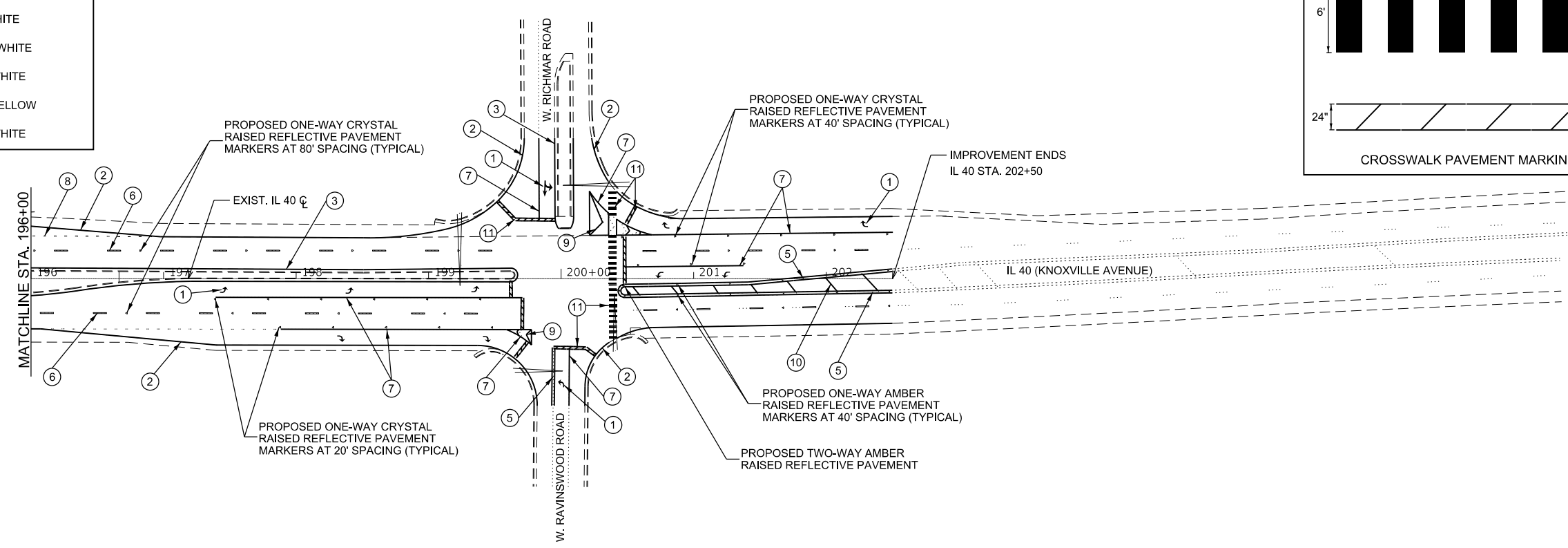
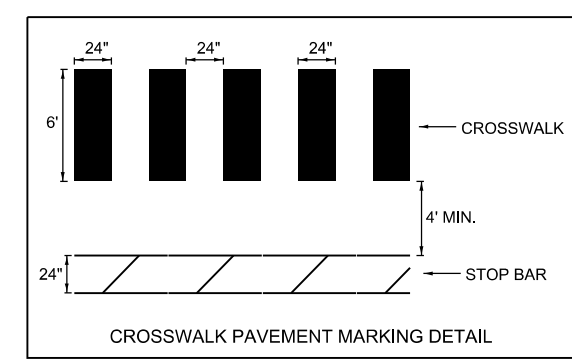
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6-N	PEORIA	48	29
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				



THERMOPLASTIC LEGEND

- ① LETTERS & SYMBOLS
- ② 4" SOLID WHITE
- ③ 4" SOLID YELLOW
- ④ 4" SKIP-DASH YELLOW
- ⑤ 4" SOLID DOUBLE YELLOW
- ⑥ 6" SKIP-DASH WHITE
- ⑦ 8" SOLID WHITE
- ⑧ 8" DOTTED WHITE
- ⑨ 12" SOLID WHITE
- ⑩ 12" SOLID YELLOW
- ⑪ 24" SOLID WHITE



MODEL: Pavement Marking Sheet
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	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 6/3/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 40 (KNOXVILLE AVE.)
PAVEMENT MARKING PLAN**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 646	SECTION (Y)RS-6:N	COUNTY PEORIA	TOTAL SHEETS 48	SHEET NO. 31
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL SCHEDULE OF QUANTITIES					
DESCRIPTION	UNIT	TOTAL QTY.	IL 40 & LINDBERGH DR.	IL 40 & IL 6 INTERCHANGE	IL 40 & RAVINSWOOD RD./ RICHMAR RD.
TRAFFIC SIGNAL BACKPLATE, RETROREFLECTIVE	EACH	8.0	3.0	1.0	4.0
CLOSED CIRCUIT TELEVISION DOME CAMERA, HD	EACH	2.0	1.0	1.0	
TRAFFIC SIGNAL BATTERY BACKUP SYSTEM	EACH	2.0	1.0		1.0
VIDEO VEHICLE DETECTION SYSTEM	EACH	2.0	1.0	1.0	

CONSTRUCTION NOTES

1. EXISTING UTILITY LOCATION INFORMATION IS NOT SHOWN ON THE PLAN SHEETS. THE CONTRACTOR SHALL FIELD VERIFY THE LOCATION OF ALL UTILITIES AND PRIVATELY OWNED FACILITIES PRIOR TO THE INSTALLATION OF ANY COMPONENTS. THE CONTRACTOR SHALL VERIFY EXISTING FIELD CONDITIONS AND TERRAIN PRIOR TO COMMENCING WORK ON THE PROJECT.
2. THE LOCATION OF ALL UTILITIES AND PRIVATELY OWNED FACILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO THE INSTALLATION OF ANY COMPONENTS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING EXISTING IDOT ELECTRICAL FACILITIES AT HIS/HER OWN EXPENSE IF REQUIRED. THE CONTRACTOR SHALL ALSO BE LIABLE FOR ANY DAMAGE TO IDOT FACILITIES RESULTING FROM INACCURATE LOCATING.
4. ELECTRICAL WORK SHALL CONFORM WITH NATIONAL, STATE, AND LOCAL CODES.
5. THE CONTRACTOR SHALL PROVIDE ELECTRICAL CABLE SLACK IN ACCORDANCE WITH ARTICLE 873.03 UNLESS SPECIFIED OTHERWISE.
6. ALL SURPLUS MATERIALS SHALL BE DISPOSED OF IN ACCORDANCE WITH ARTICLE 202.03 OF THE STANDARD SPECIFICATION.
7. ANY MAINTENANCE OF EXISTING ELECTRICAL FACILITIES WILL BE CONSIDERED EXTRA WORK IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
8. THE EXISTING TRAFFIC SIGNALS AND VEHICLE DETECTION SHALL REMAIN IN OPERATION DURING THE INSTALLATION OF THE PROPOSED VEHICLE DETECTION COMPONENTS.
9. REMOVAL OF EXISTING ITEMS (CCTV CAMERAS, VEHICLE DETECTION CAMERAS, VEHICLE DETECTION EQUIPMENT, ETC.) WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES FOR THE PROPOSED ITEMS.
10. THE CONTRACTOR SHALL VERIFY FIELD CONDITIONS PRIOR TO BIDDING. THERE WILL BE NO ADDITIONAL COMPENSATION PAID FOR CLAIMS THAT ARISE FROM A FAILURE TO FULLY INVESTIGATE EXISTING FIELD CONDITIONS.

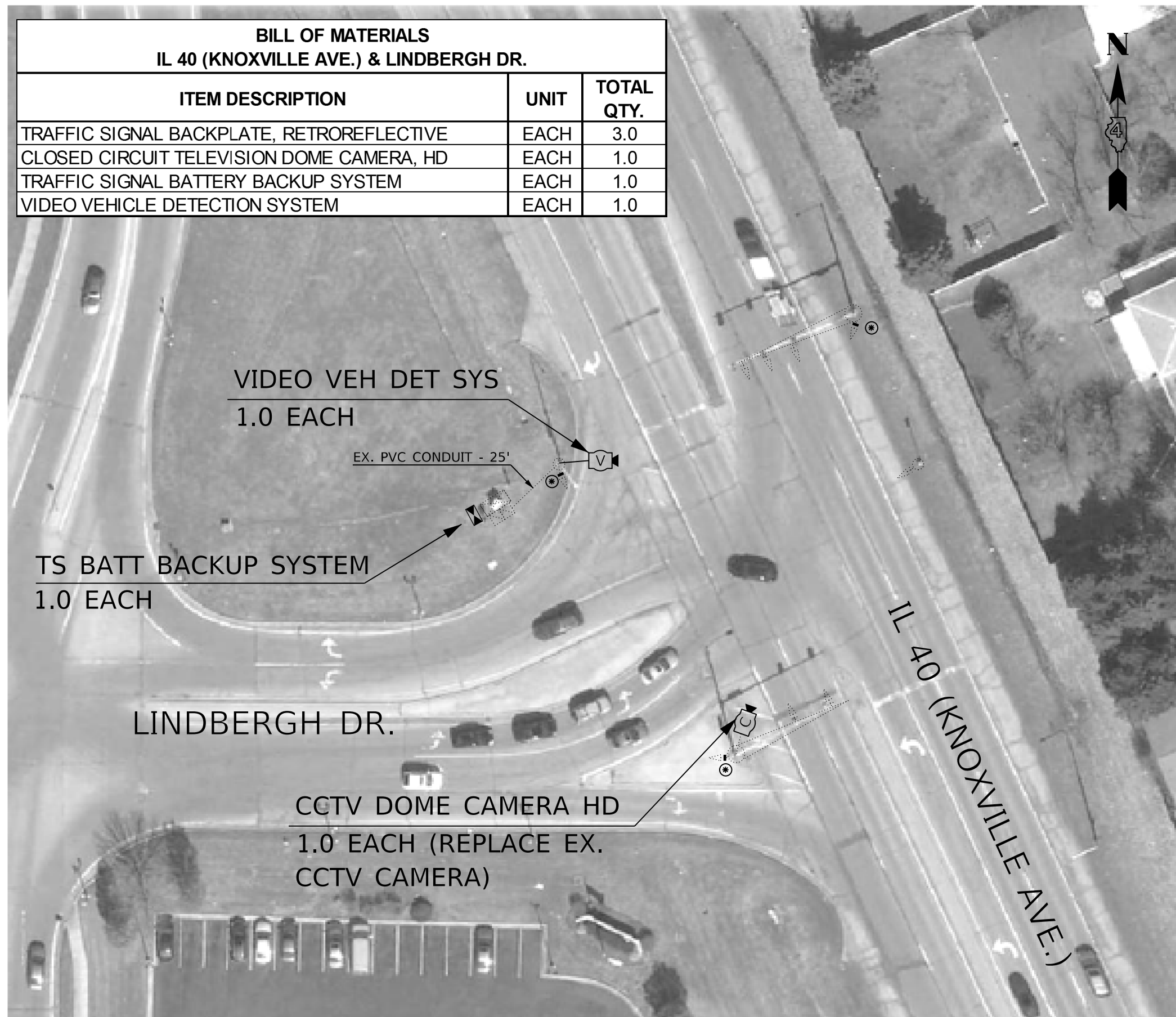
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TRAFFIC SIGNALS
SHEET 1 OF 7

USER NAME = fayantnd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL SCHEDULE OF QUANTITIES AND CONSTRUCTION NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE = 1/8" = 178.8071' / in.	DRAWN -	REVISED -			646	(Y)RS-6;N	PEORIA	48	32	
PLOT DATE = 5/30/2019	CHECKED -	REVISED -			CONTRACT NO. 68E93					
	DATE -	REVISED -			ILLINOIS	FED. AID PROJECT				
					SCALE:	SHEET	OF	SHEETS	STA.	TO STA.

**BILL OF MATERIALS
IL 40 (KNOXVILLE AVE.) & LINDBERGH DR.**

ITEM DESCRIPTION	UNIT	TOTAL QTY.
TRAFFIC SIGNAL BACKPLATE, RETROREFLECTIVE	EACH	3.0
CLOSED CIRCUIT TELEVISION DOME CAMERA, HD	EACH	1.0
TRAFFIC SIGNAL BATTERY BACKUP SYSTEM	EACH	1.0
VIDEO VEHICLE DETECTION SYSTEM	EACH	1.0



TRAFFIC SIGNAL LEGEND

	EXISTING DOUBLE HANDHOLE
	EXISTING MAST ARM
	EXISTING TRAFFIC SIGNAL POST
	EXISTING TRAFFIC SIGNAL HEAD W/ RETROREFLECTIVE BACKPLATE
	EXISTING CONDUIT
	EXISTING TRAFFIC SIGNAL CONTROLLER CABINET
	PROPOSED RETROREFLECTIVE BACKPLATE ON EXISTING TRAFFIC SIGNAL HEAD
	PROPOSED VIDEO VEHICLE DETECTION SYSTEM
	PROPOSED CCTV DOME CAMERA, HD (REPLACE EX. CCTV CAMERA)
	PROPOSED BATTERY BACKUP SYSTEM

TRAFFIC SIGNALS
SHEET 2 OF 7
NOT TO SCALE

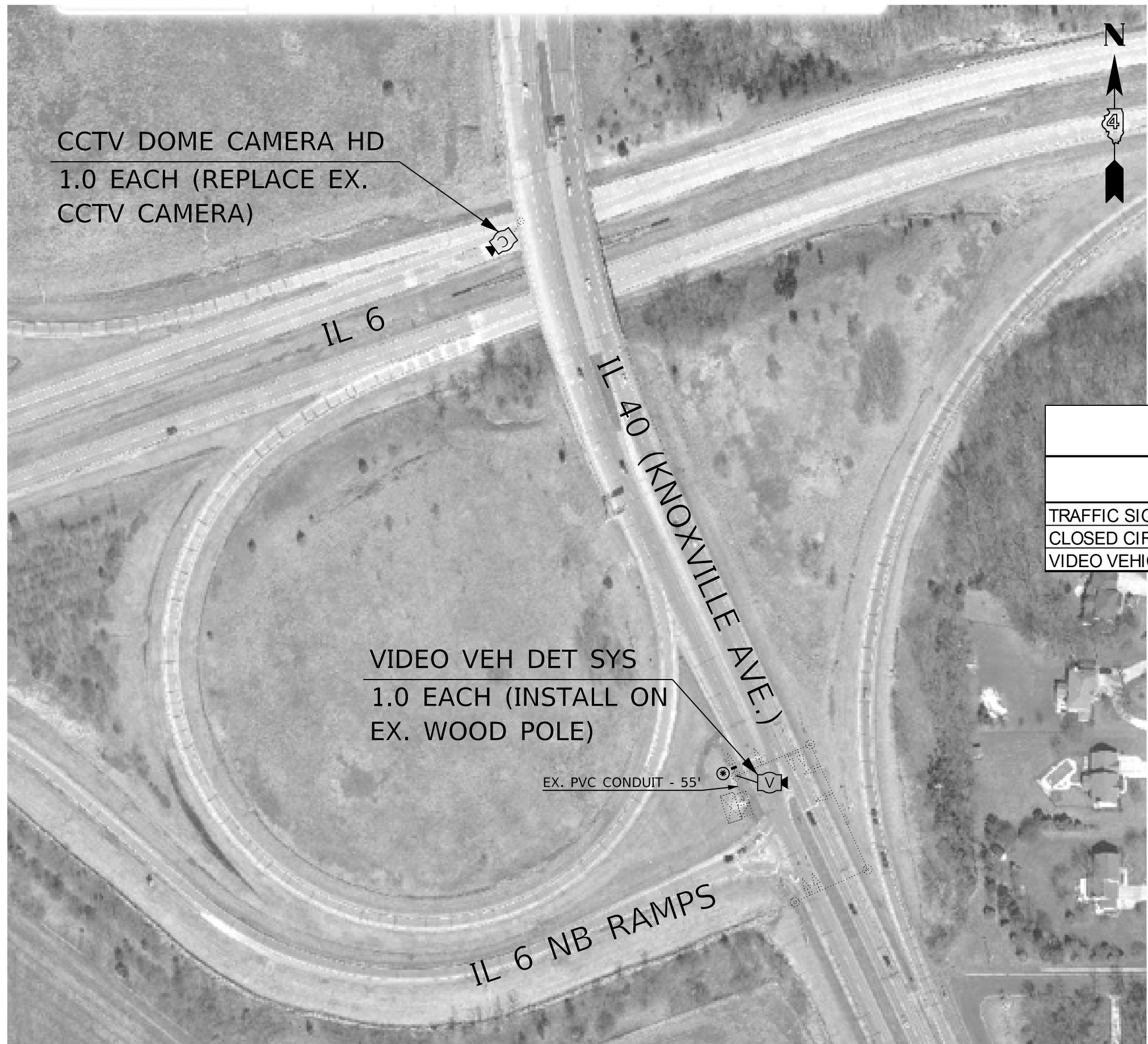
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USER NAME = fayantnd	DESIGNED -	REVISED -
PLOT SCALE = 178.8071' / in.	DRAWN -	REVISED -
PLOT DATE = 5/30/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED TRAFFIC SIGNAL PLANS IL 40 (KNOXVILLE AVE.) & LINDBERGH DR.	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6;N	PEORIA	48	33
			CONTRACT NO. 68E93	
		ILLINOIS	FED. AID PROJECT	



BILL OF MATERIALS IL 40 (KNOXVILLE AVE.) & IL 6		
ITEM DESCRIPTION	UNIT	TOTAL QTY.
TRAFFIC SIGNAL BACKPLATE, RETROREFLECTIVE	EACH	1.0
CLOSED CIRCUIT TELEVISION DOME CAMERA, HD	EACH	1.0
VIDEO VEHICLE DETECTION SYSTEM	EACH	1.0

TRAFFIC SIGNAL LEGEND	
	EXISTING DOUBLE HANDHOLE
	EXISTING WOOD POLE
	EXISTING ITS CAMERA POLE
	EXISTING TRAFFIC SIGNAL HEAD W/ RETROREFLECTIVE BACKPLATE
	EXISTING CONDUIT
	EXISTING TRAFFIC SIGNAL CONTROLLER CABINET
	PROPOSED RETROREFLECTIVE BACKPLATE ON EXISTING TRAFFIC SIGNAL HEAD
	PROPOSED VIDEO VEHICLE DETECTION SYSTEM
	PROPOSED CCTV DOME CAMERA, HD (REPLACE EX. CCTV CAMERA)
	PROPOSED BATTERY BACKUP SYSTEM

TRAFFIC SIGNALS
SHEET 3 OF 7
NOT TO SCALE

MODEL: Default
FILE NAME: S:\CEN\WORK\03\STDP\PLUS\55\0368E93_Knoxville Overby\Traffic Signal Plans\68E93 - Knoxville, MIH & Overby (Final 5-29-19).dgn

USER NAME = fayantnd	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 1/8" = 178.8071' / in.	CHECKED -	REVISED -
PLOT DATE = 5/30/2019	DATE -	REVISED -

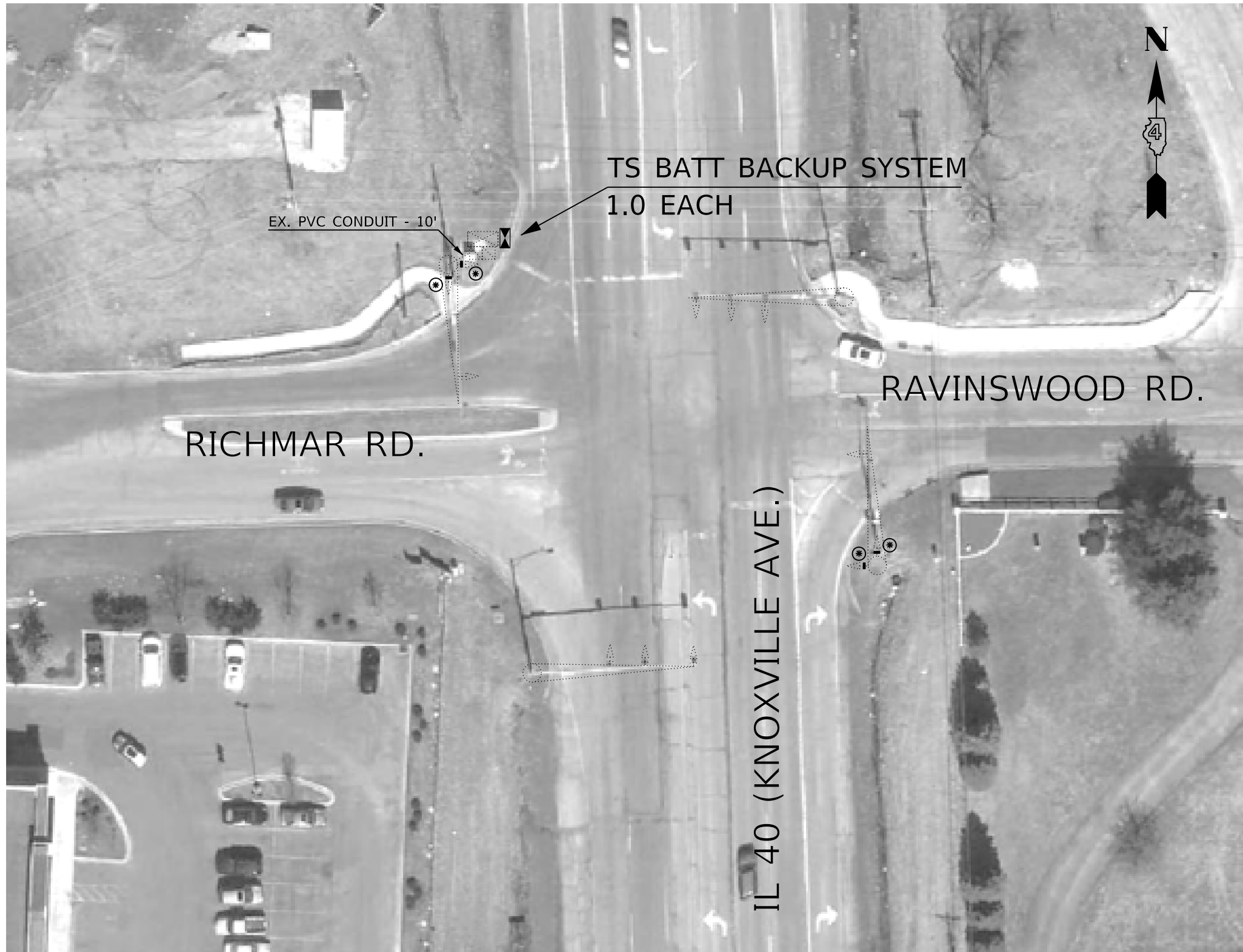
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED TRAFFIC SIGNAL PLANS
IL 40 (KNOXVILLE AVE.) & IL 6 INTERCHANGE**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6;N	PEORIA	48	34
			CONTRACT NO. 68E93	
		ILLINOIS	FED. AID PROJECT	

BILL OF MATERIALS IL 40 (KNOXVILLE AVE.) & RAVINSWOOD RD./ RICHMAR RD.		
ITEM DESCRIPTION	UNIT	TOTAL QTY.
TRAFFIC SIGNAL BACKPLATE, RETROREFLECTIVE	EACH	4.0
TRAFFIC SIGNAL BATTERY BACKUP SYSTEM	EACH	1.0



TRAFFIC SIGNAL LEGEND

	EXISTING DOUBLE HANDHOLE
	EXISTING MAST ARM
	EXISTING TRAFFIC SIGNAL HEAD W/ RETROREFLECTIVE BACKPLATE
	EXISTING CONDUIT
	EXISTING TRAFFIC SIGNAL CONTROLLER CABINET
	PROPOSED RETROREFLECTIVE BACKPLATE ON EXISTING TRAFFIC SIGNAL HEAD
	PROPOSED BATTERY BACKUP SYSTEM

TRAFFIC SIGNALS
SHEET 4 OF 7
NOT TO SCALE

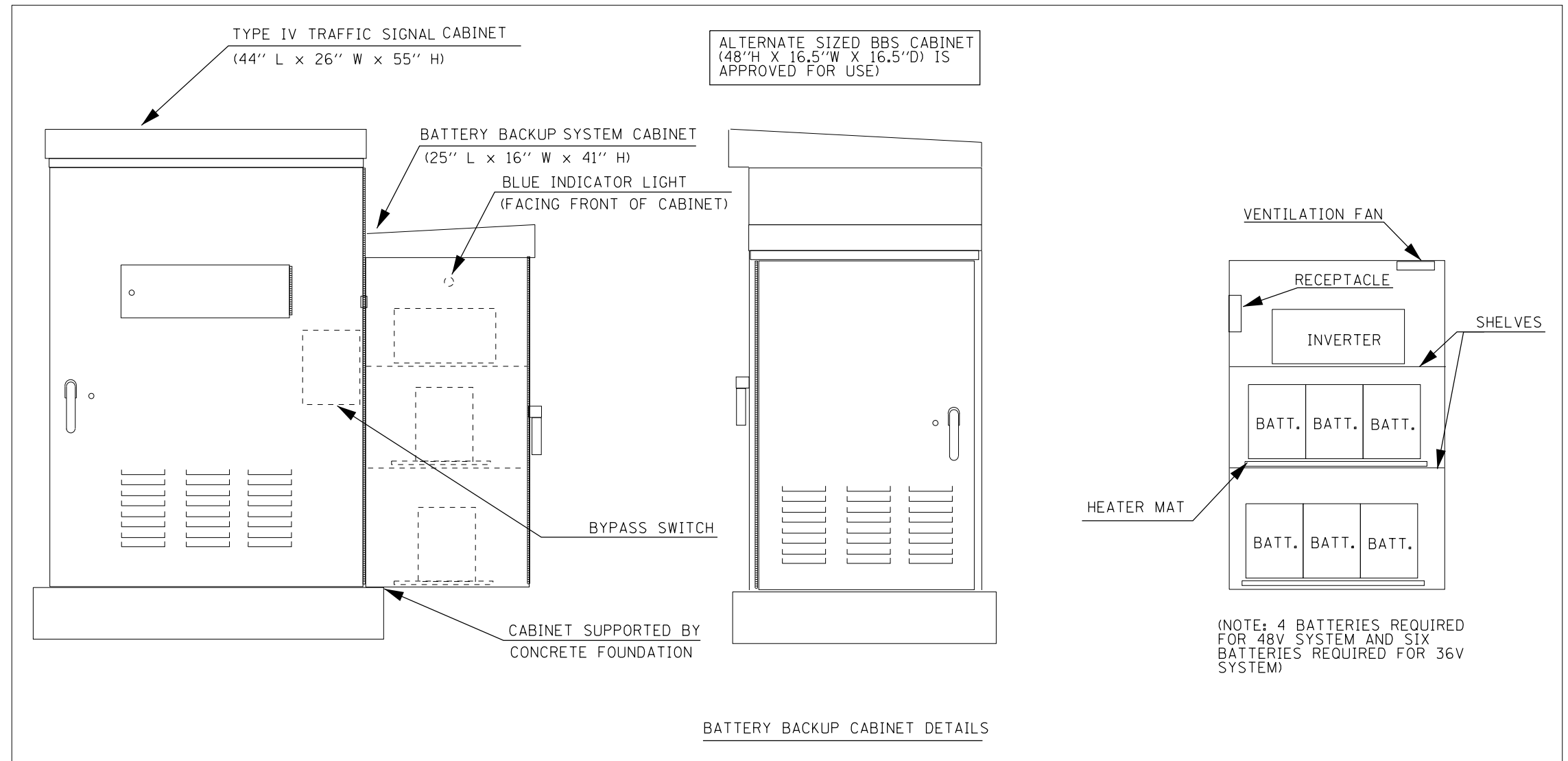
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USER NAME = fayantnd	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 178.8071' / in.	CHECKED -	REVISED -
PLOT DATE = 5/30/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED TRAFFIC SIGNAL PLANS IL 40 (KNOXVILLE AVE.) & RAVINSWOOD RD./RICHMAR RD.			
SCALE:	SHEET	OF	SHEETS
	STA.		TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6;N	PEORIA	48	35
				CONTRACT NO. 68E93
		ILLINOIS	FED. AID PROJECT	



NOTES

1. THE BATTERY BACKUP SYSTEM CABINET SHALL BE A NEMA TYPE 3R CABINET WITH MINIMUM OUTSIDE DIMENSIONS OF 41" (H) X 25" (W) X 16" (D). THE CABINET SHALL BE EQUIPPED WITH A THREE POINT LATCHING MECHANISM, TWO SHELVES, THERMOSTATICALLY CONTROLLED VENTILATION FAN, AND A POWER RECEPTACLE. THE CABINET SHALL BE MOUNTED TO THE SIDE OF THE PROPOSED TYPE IV CABINET WITH THE BOTTOM OF THE CABINET SUPPORTED BY THE CONCRETE FOUNDATION.
2. ALL CABINET LIGHTS, HEATER STRIPS, VENTILATION FANS, AND SERVICE RECEPTACLES SHALL BE BYPASSED WHEN THE BATTERY BACKUP UNIT IS OPERATING IN BATTERY MODE.
3. THE BATTERY BACKUP UNITS CONTACTS SHALL BE WIRED TO PROVIDE LOCAL CONTROLLER ALARMS (AS AVAILABLE IN THE PROPOSED CABINETS).
4. THE BYPASS SWITCH SHALL BE AUTOMATIC AND SHALL BE INSTALLED IN THE BBS CABINET.

MODEL: Default
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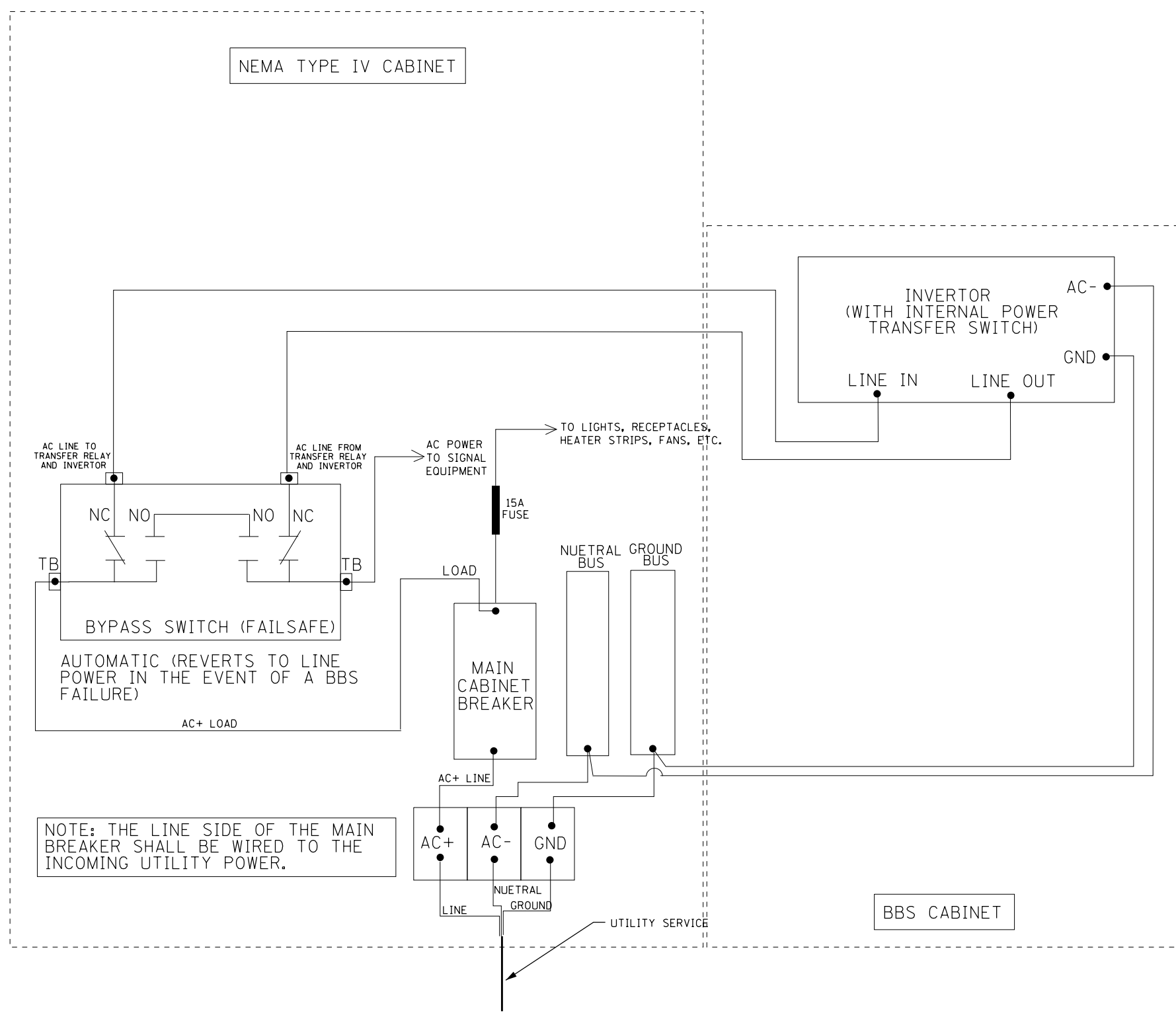
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PLOT SCALE = 1/8" = 1'-0"	DRAWN -	REVISED -
PLOT DATE = 5/30/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BATTERY BACKUP SYSTEM CABINET DETAIL

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-6:N	PEORIA	48	36
CONTRACT NO. 68E93			ILLINOIS FED. AID PROJECT	



NOTE: THE LINE SIDE OF THE MAIN BREAKER SHALL BE WIRED TO THE INCOMING UTILITY POWER.

MODEL: Default
 FILE NAME: S:\GEN\WORK\ORD\STD&PLUS\Sua0368E93 - Knoxville Overby/Traffic Signal Plans\68E93 - Knoxville, MI & Overby (Final 5-29-19).dgn

USER NAME = fayantnd	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 1/8" = 1/16" / in.	CHECKED -	REVISED -
PLOT DATE = 5/30/2019	DATE -	REVISED -

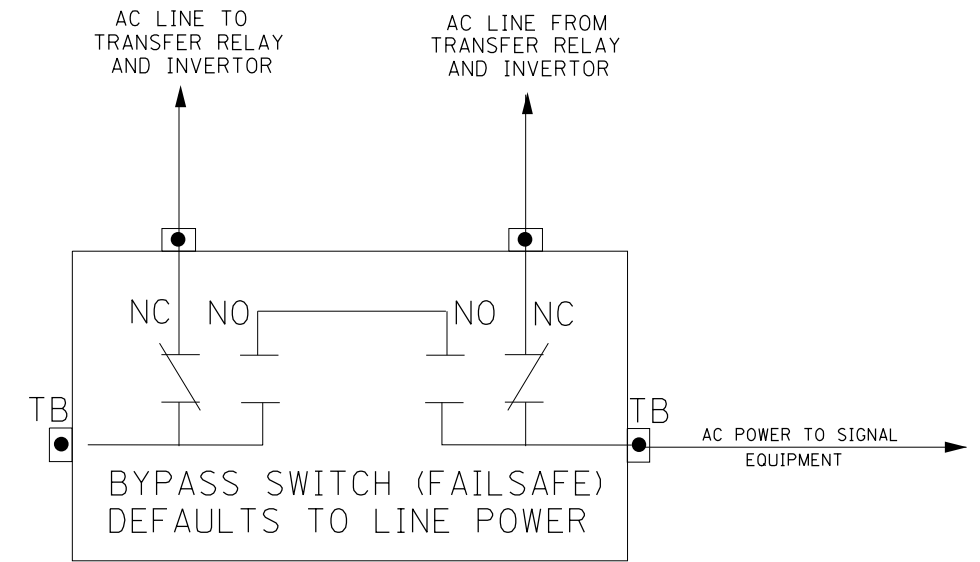
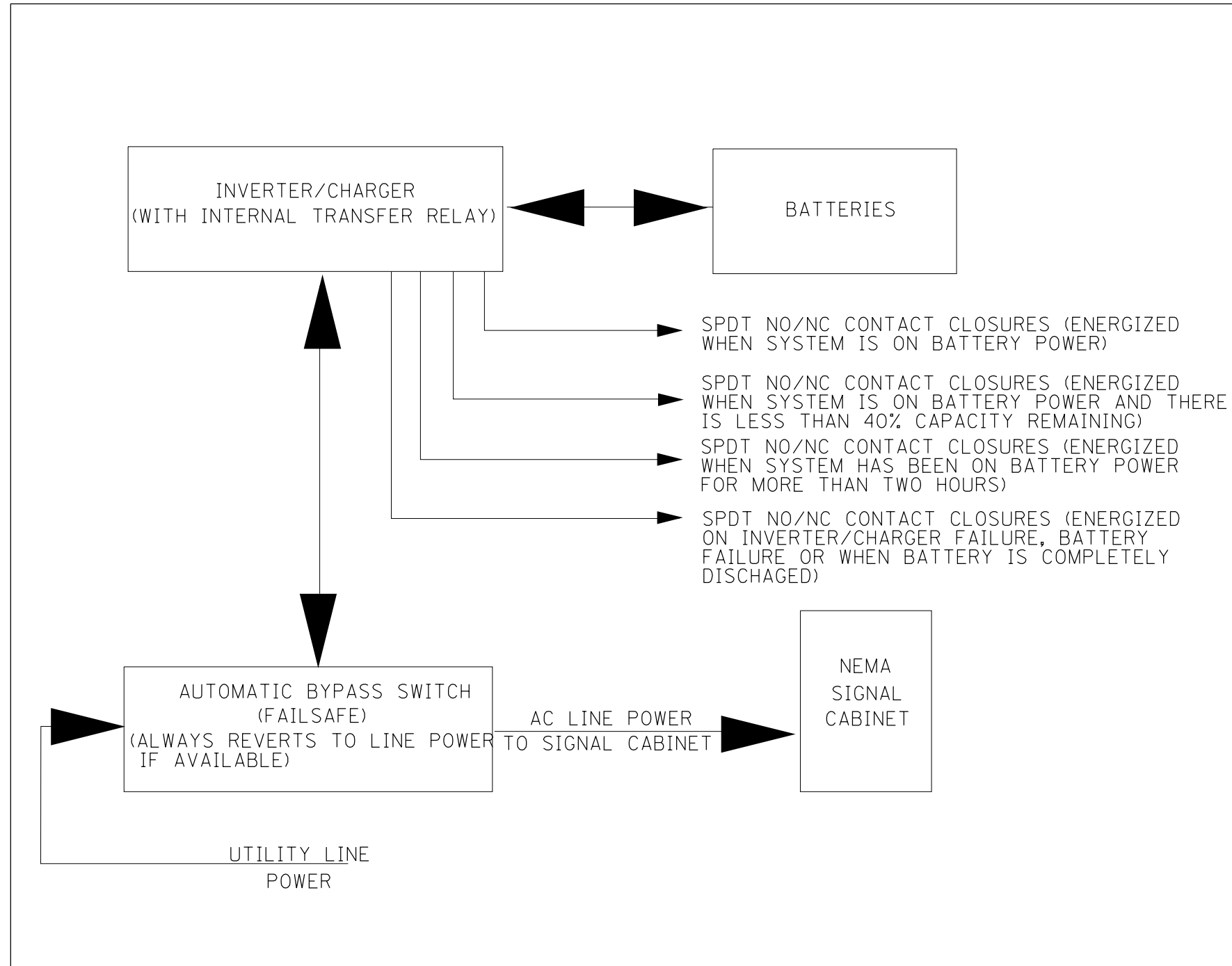
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BATTERY BACKUP SYSTEM CABINET WIRING DIAGRAM

SCALE: SHEET OF SHEETS STA. TO STA.

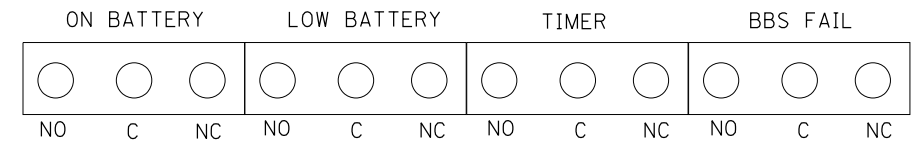
F.A.P. RTE. 646	SECTION (Y)RS-6;N	COUNTY PEORIA	TOTAL SHEETS 48	SHEET NO. 37
CONTRACT NO. 68E93			ILLINOIS FED. AID PROJECT	

MODEL: Default
 FILE NAME: S:\CEN\WORK\ORD\STDP\PLUS\Sua0368E93 - Knoxville Overby\Traffic Signal Plans\68E93 - Knoxville Mill & Overby (Final 5-29-19).dgn



NOTES:

1. TB - #8 TERMINAL BLOCKS
2. NO - NORMALLY OPEN
3. NC - NORMALLY CLOSED
4. NO/NC CONTACTS SHALL ALL TOGGLE SIMULTANEOUSLY WITH A SINGLE MANUALLY OPERATED SWITCH.
5. THE MANUAL BYPASS SWITCH SHALL ONLY SWITCH LINE POWER. THE NEUTRAL AND EQUIPMENT GROUND SHALL NOT BE SWITCHED AND SHALL BE CONNECTED TO THE NEMA CABINET BUSES.



NO/NC CONTACTS MAY SHARE OR USE SEPARATE COMMONS.

USER NAME = fayantnd	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
PLOT SCALE = 1/8" = 1' / in.	CHECKED -	REVISED -	
PLOT DATE = 5/30/2019	DATE -	REVISED -	

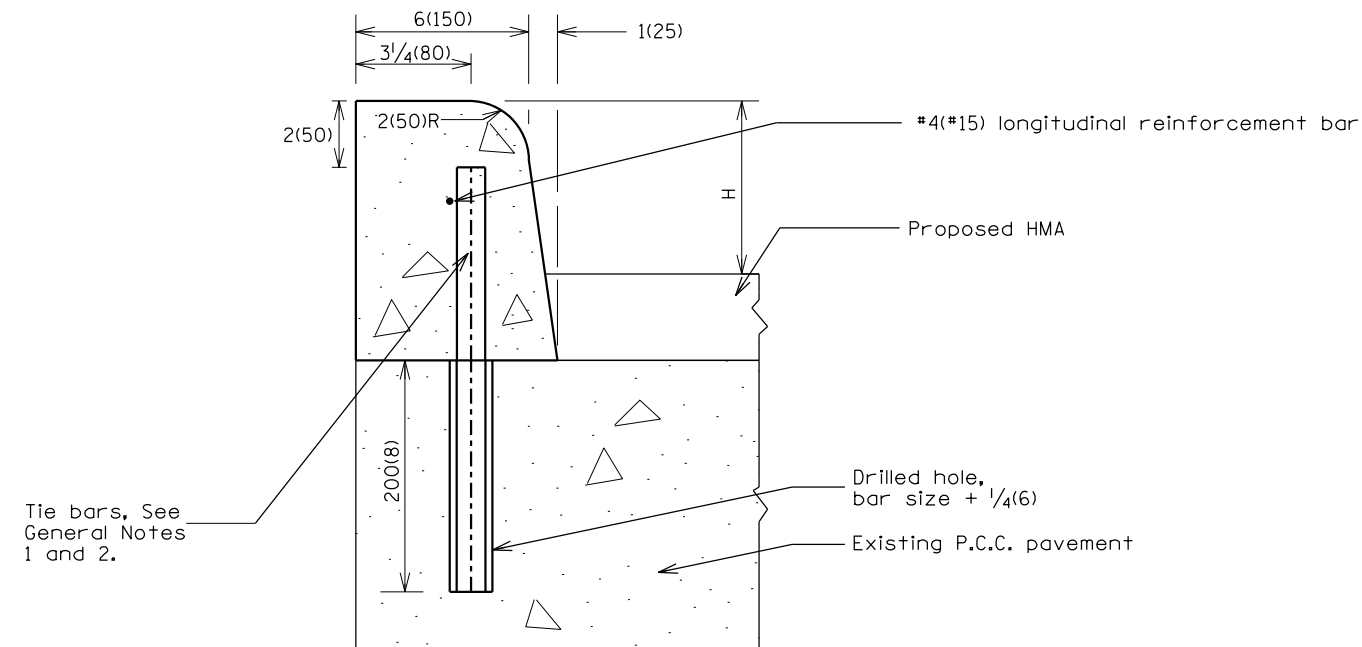
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BATTERY BACKUP SYSTEM BLOCK
AND BYPASS SWITCH DIAGRAMS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 646	SECTION (Y)RS-6;N	COUNTY PEORIA	TOTAL SHEETS 48	SHEET NO. 38
CONTRACT NO. 68E93				
ILLINOIS FED. AID PROJECT				

TYPICAL CURB INSTALLATION
ON TOP OF EXISTING PAVEMENT



LEGEND

H = curb height: 4(100)

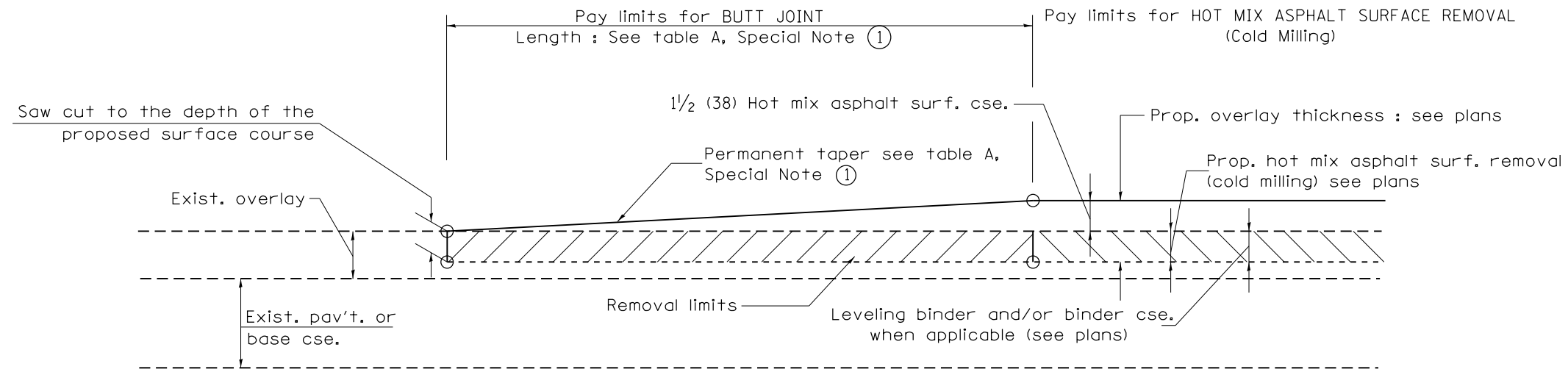
T = thickness of existing P.C.C. pavement.
(see plans for thickness)

GENERAL NOTES:

1. Tie bars shall be #6(*20) at 24(600mm) center in accordance with details for longitudinal construction joint shown in Standard 420001.
2. A minimum clearance of 2(50mm) between the end of the tie bar and the back or top of the curb shall be maintained.
3. CONCRETE CURB, TYPE B (SPECIAL) shall be constructed in accordance with the applicable portions of Standards 420001 and 606001, and in accordance with the applicable portions of Section 606 of the Standard Specifications except as noted herein.
4. The curb location shall be as shown on Highway Standard 63101.
5. This work will be paid for at the Contract unit price per foot for CONCRETE CURB, TYPE B (SPECIAL).

All dimensions are in inches (millimeters) unless otherwise noted.

				STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		CONCRETE CURB, TYPE B (SPECIAL) DETAIL			
				NOT TO SCALE					
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.					
646	(Y)RS-62N	PEORIA	48	40					
				CONTRACT NO. 68E93					
FED. ROAD DIST. NO.		ILLINOIS		FED. AID PROJECT					



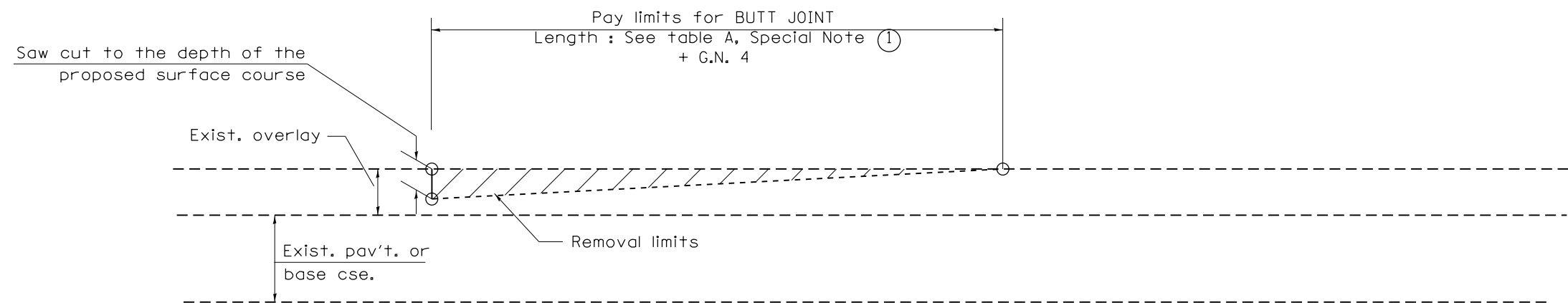
CASE 1 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

**TABLE A
TAPER RATES**

SPECIAL NOTE NUMBER	ELEMENT	MAINLINE INTERSTATES & 4-LANE EXPRESSWAYS	ALL OTHERS
①	BUTT JOINT TAPER RATE	1:480	1:240
②	TEMPORARY RAMP TAPER RATE	1:80	1:40

GENERAL NOTES

1. The work shall be done in accordance with Article 406.08 and the Special Provision for Butt Joints.
2. The pavement surface to be removed may be either bituminous or P.C. concrete. The work shall be performed in accordance with Article 440.04 and the Special Provisions for Butt Joints.
3. The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.05.
4. The length of butt joint is based on the taper rate times change in cold milling depth within the butt joint pay limits, unless otherwise indicated.
5. Temporary ramps are paid for separately and not included in the cost of the butt joints.



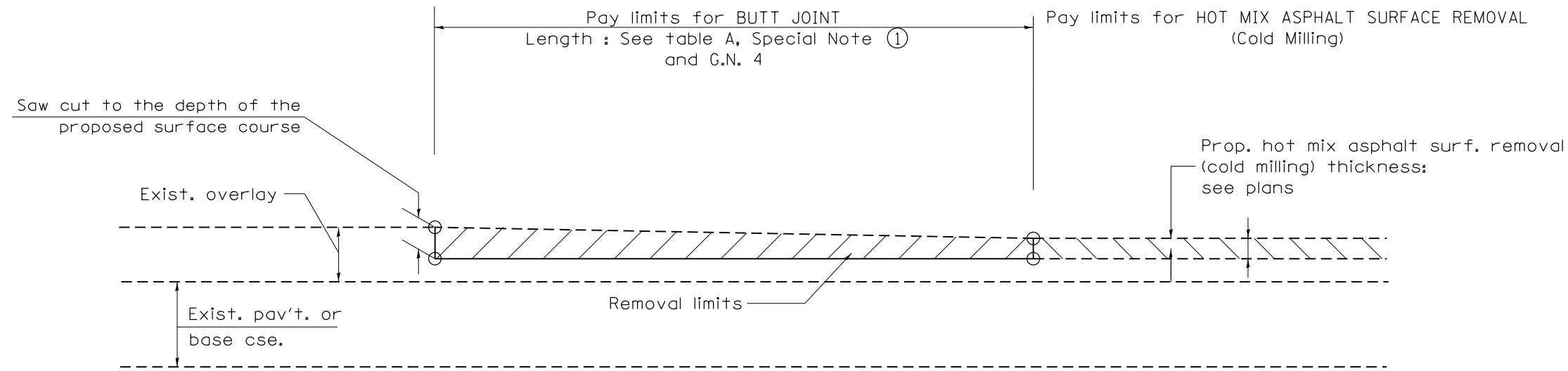
CASE 2 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

All dimensions are in inches (millimeters) unless otherwise noted.

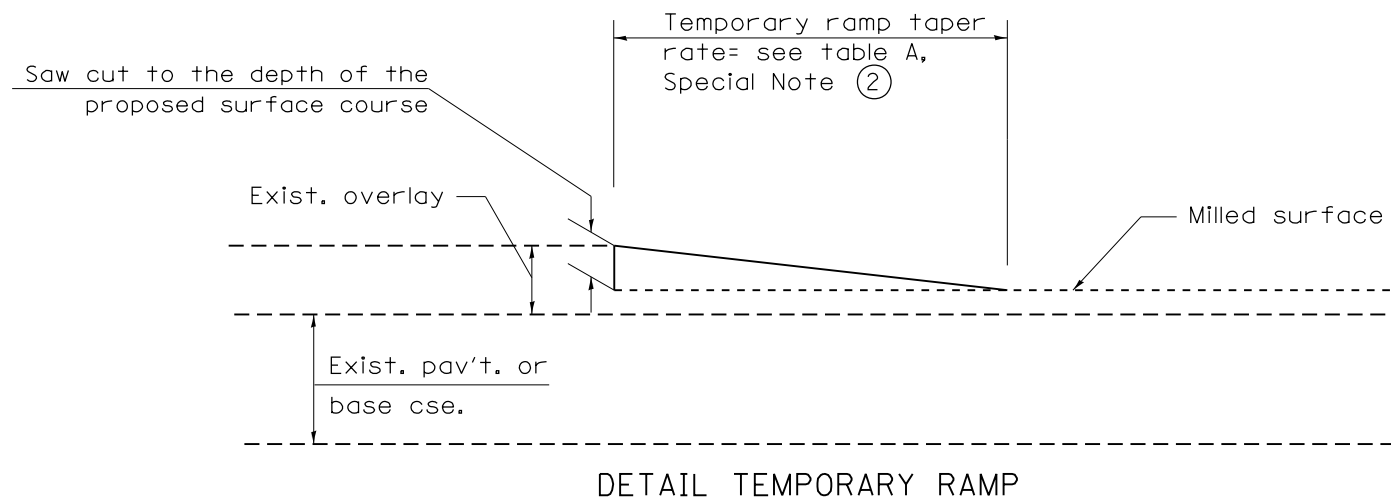
01-01-97	RENUM. C-23.01, NEW REVISION BOX	T.P.	08-21-13	MAJOR MODIFICATIONS	R.D.	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINTS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
04-01-97	CORRECTION TO DEPTH	J.A.	02-29-16	MINOR CORRECTIONS	R.D.			646	(Y)RS-62N	PEORIA	48	41
09-15-05	REVISED DESIGNER NOTE	M.M.A.	04-12-16	MINOR CORRECTIONS	R.D.			SHT. 1 OF 3 CADD STD. 406101-D4				
10-16-06	REVISED TO 2007 SPEC.	M.A.	02-14-17	ADDED NOTE 5	R.D.			CONTRACT NO. 68E93				

NOT TO SCALE

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

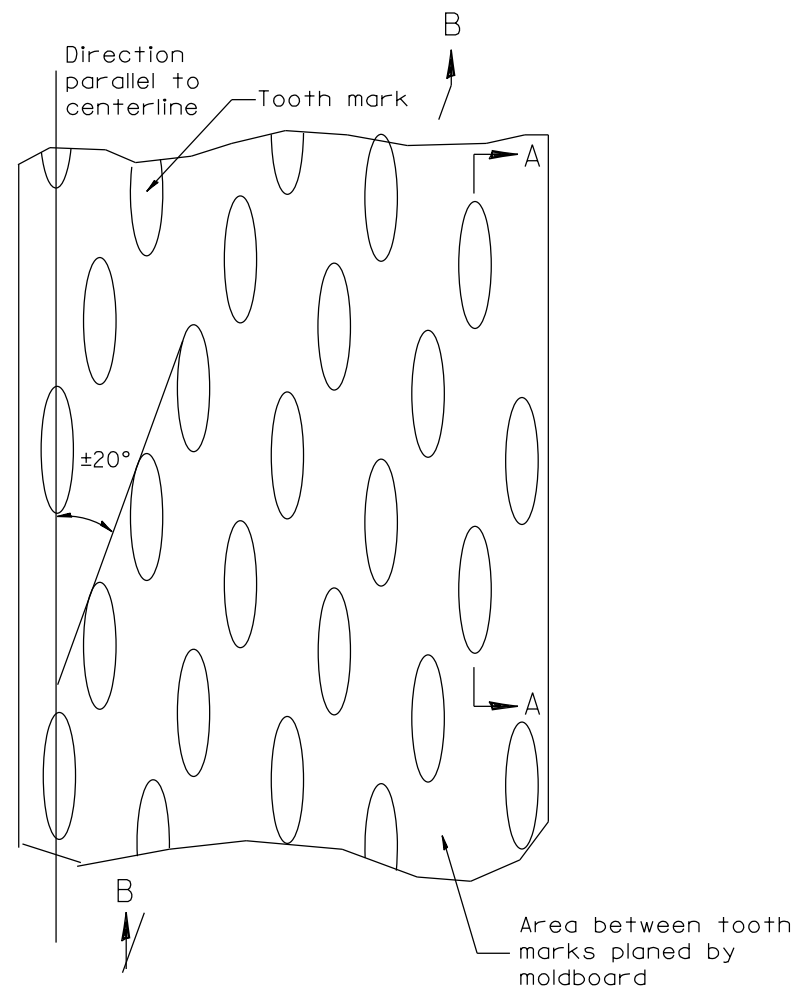


CASE 3 : HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER

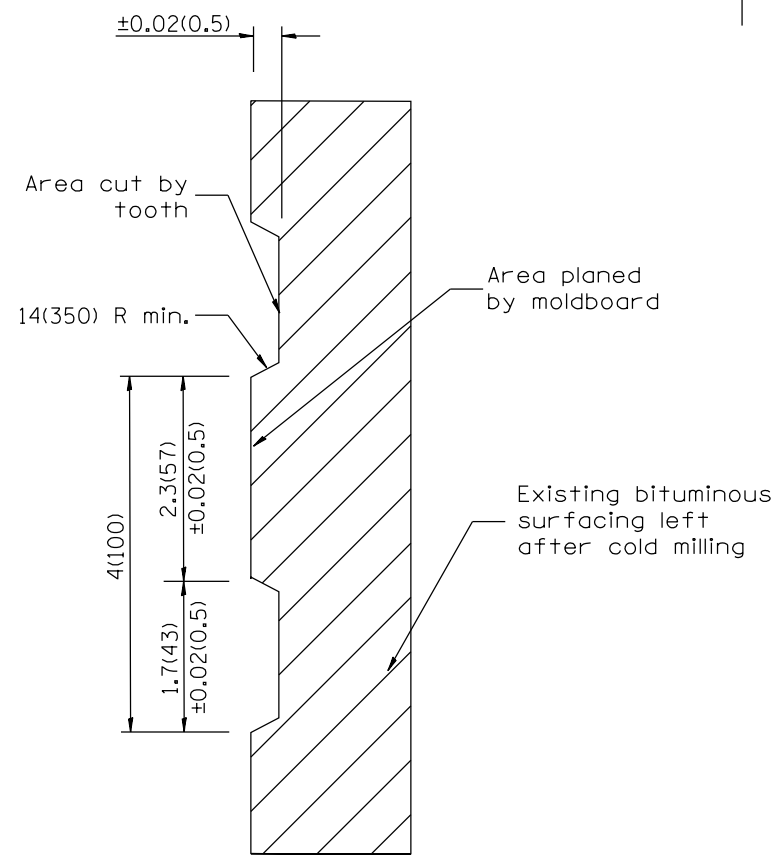


All dimensions are in inches (millimeters) unless otherwise noted.

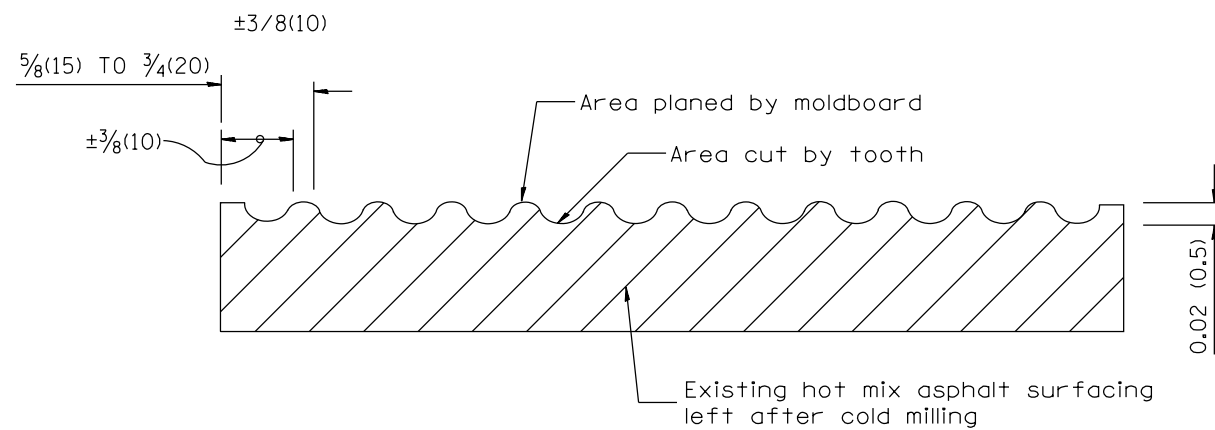
				STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		BUTT JOINTS		SHT. 2 OF 3 CADD STD. 406101-D4	
				NOT TO SCALE				CONTRACT NO. 68E93	
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.					
646	(Y)RS-62N	PEORIA	48	42					
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT							



PLAN



SECTION A-A



SECTION B-B PROJECTED
PERPENDICULAR TO CENTERLINE

General notes:

1. Coldmilling shall consist of two processes: Cutting with carbide teeth mounted on a rotating drum, and planing with a moldboard mounted immediately behind the cutting drum.
2. Other similar patterns will be acceptable if they consist of a smooth, flat, planed surface interspersed with a pattern of discontinuous longitudinal striations.

All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-104.01, NEW REVISION BOX	T.P.
04-20-98	REMOVED MILLING DETAIL FROM STANDARD	J.A.
09-08-98	CORRECT NOTE LEADER PLACEMENT	R.W.
10-16-06	REVISED TO 2007 SPEC.	M.A.

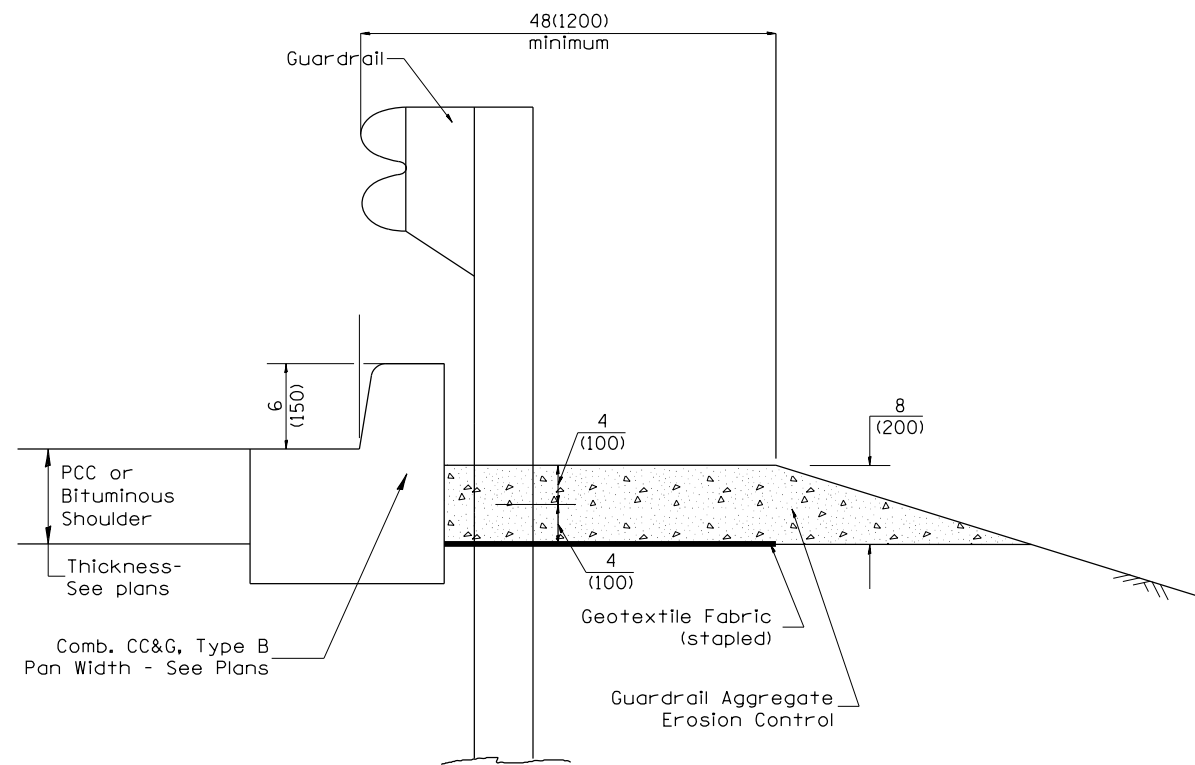
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

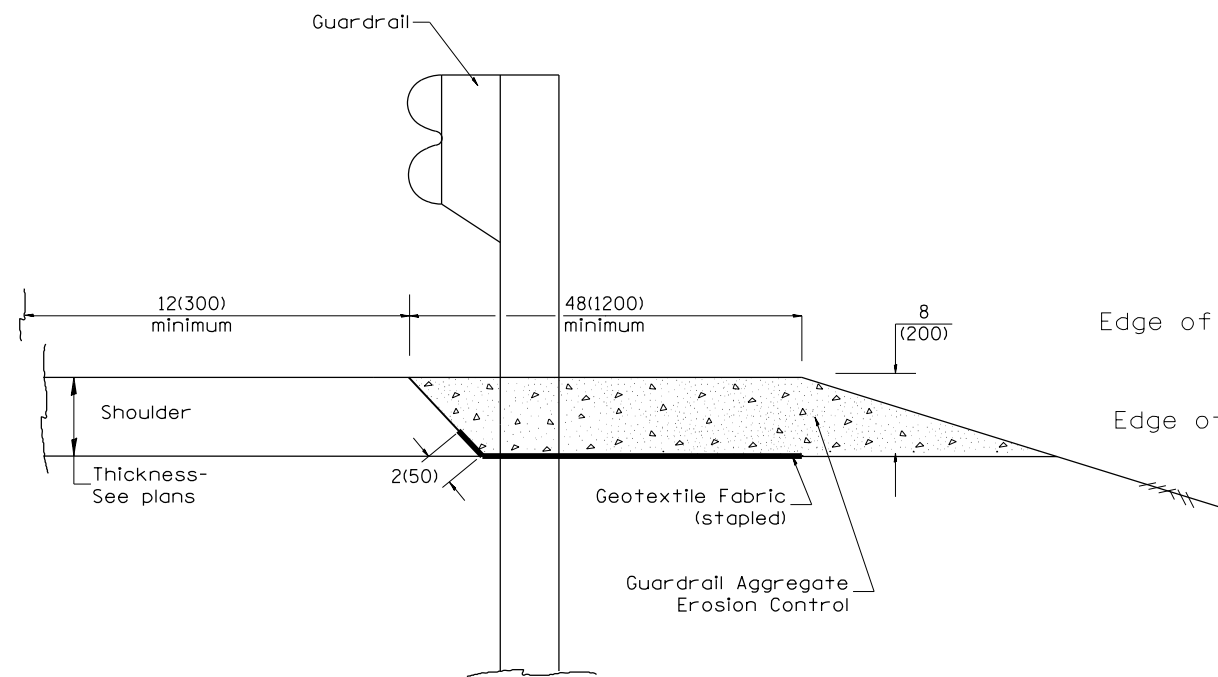
NOT TO SCALE

CADD STD. 440001-D4

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-62N	PEORIA	48	44
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68E93	



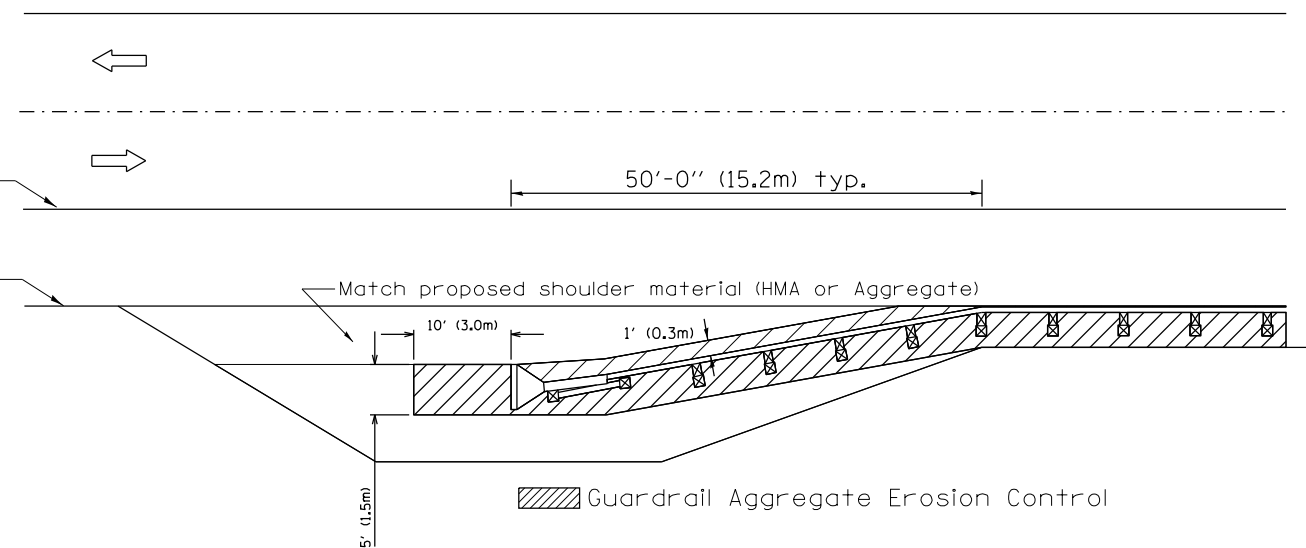
TYPICAL SECTION WITH COMBINATION CONCRETE CURB & GUTTER



TYPICAL SECTION WITHOUT EROSION CONTROL CURB

GENERAL NOTES: GUARDRAIL AGGREGATE EROSION CONTROL

1. This work shall consist of grading as needed, furnishing and installing geotextile fabric and staples, and furnishing, placing and shaping crushed aggregate around and behind Steel Plate Beam Guardrail posts in accordance with Plan Details.
2. Before placing the aggregate and the Geotextile Fabric, weeds and grass shall be removed from the area to be covered.
3. After the area has been prepared, and in a dry condition, the Geotextile fabric shall be placed with a 12(300) minimum overlap. A knife cut for guardrail post installation is necessary.
4. The aggregate shall be deposited, compacted and shaped by either mechanical or hand methods, in a manner reasonably true to line and grade.
5. The Contractor shall have the option of placing the guardrail before or after the Geotextile Fabric and Aggregate are in place. If the guardrail is placed after the Geotextile Fabric and Aggregate, then any voids must be filled and the aggregate returned to line and grade.
6. Materials shall meet the following requirements:
 - A. The crushed aggregate shall be CA1 gradation in accordance with Article 1004.01(c) of the Standard Specifications.
 - B. The Geotextile Fabric shall be nonwoven fabric in accordance with Article 1080.02 of the Standard Specifications.



All dimensions are in inches (millimeters) unless otherwise noted.

03-07-11	ADDED DETAIL SHOWING PLAN VIEW	R.D.	5-30-18	CHANGE B CURB TO CC&G	R.D.
08-10-12	REVISED CURB "B" AND AGGREGATE	R.D.			
07-15-15	ADDRESSED SHOULDER INLET CURB	R.D.			
01-26-17	REVISED	R.D.			

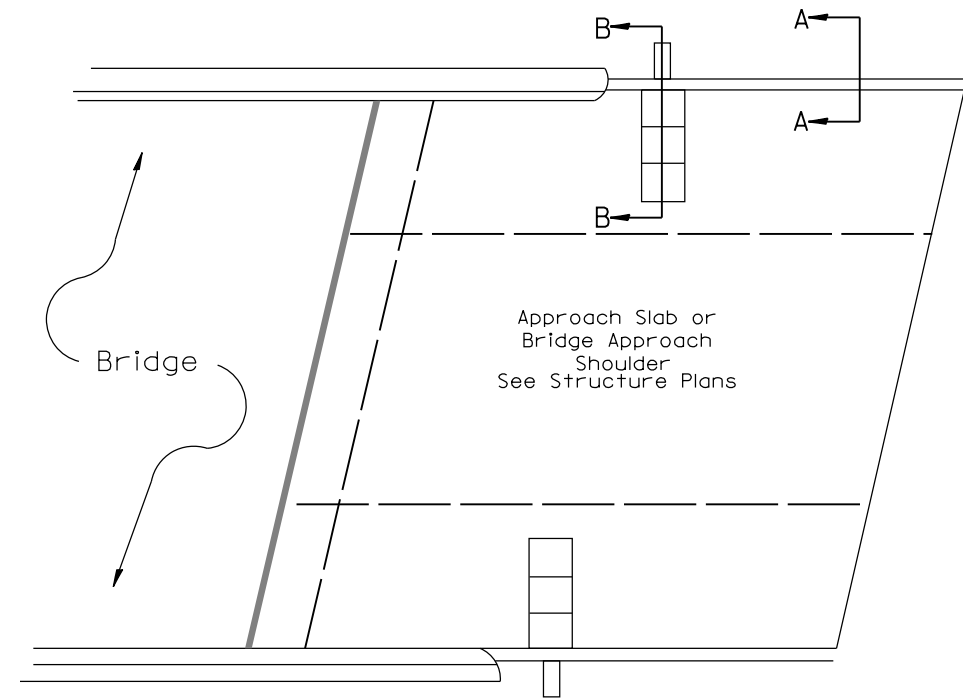
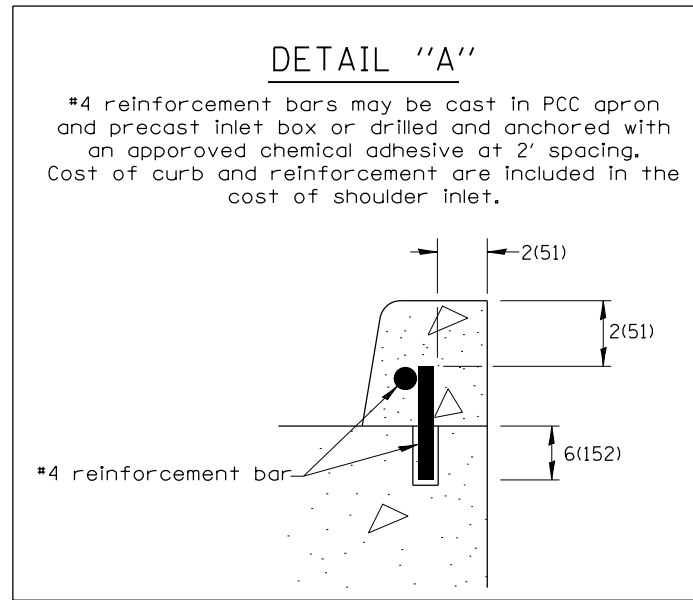
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

NOT TO SCALE

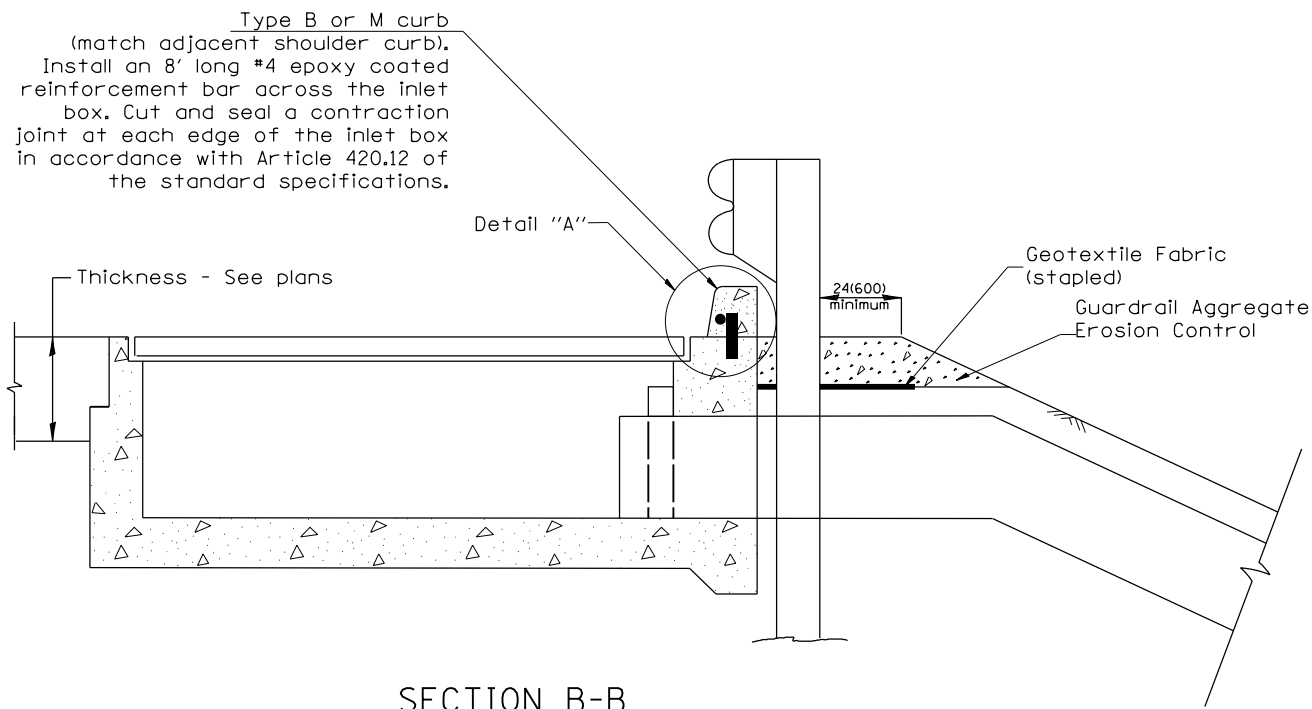
GUARDRAIL EROSION CONTROL TREATMENTS

SHT. 1 OF 2
CADD STD. 630101-D4

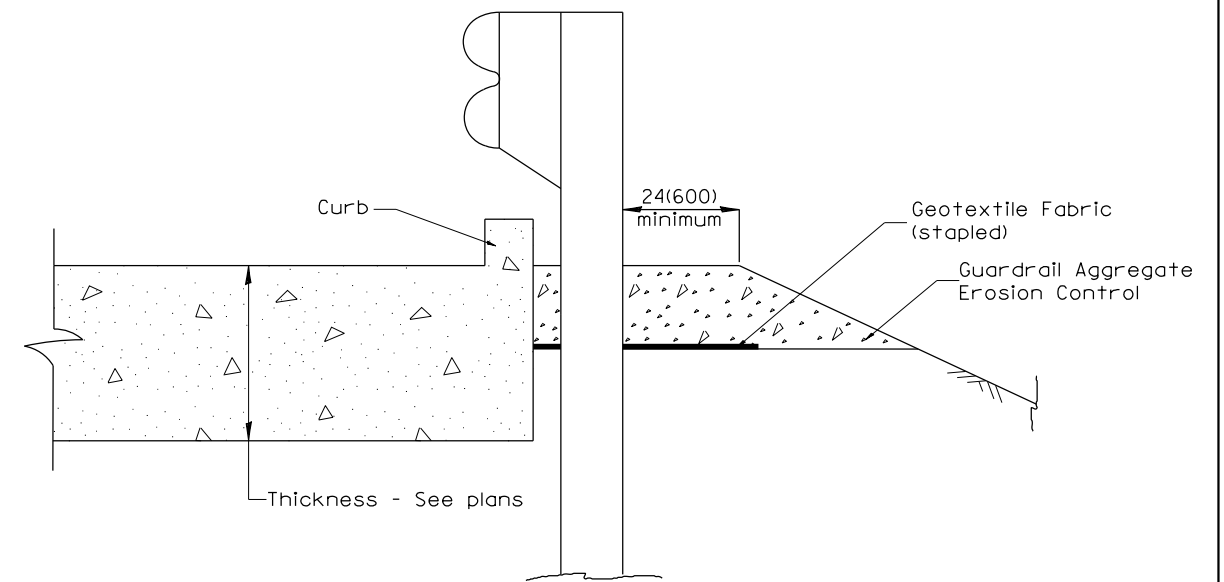
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-62N	PEORIA	48	45
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68E93	



PLAN VIEW
APPROACH SLAB OR SHOULDER PLACEMENT



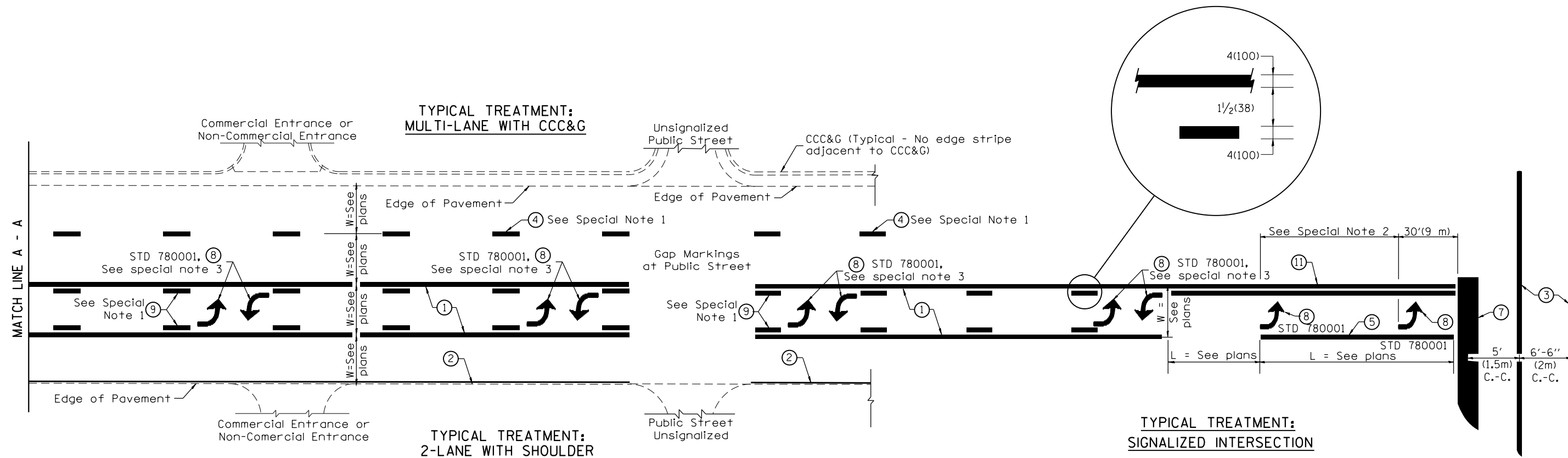
SECTION B-B
TYPICAL SECTION AT INLETS
TYPE E, F & G (HIGHWAY STANDARD 610001)



SECTION A-A
TYPICAL SECTION WITH BRIDGE APPROACH CURB

All dimensions are in inches (millimeters) unless otherwise noted.

STATE OF ILLINOIS				GUARDRAIL EROSION CONTROL TREATMENTS				SHT. 2 OF 2	
DEPARTMENT OF TRANSPORTATION				NOT TO SCALE				CADD STD. 630101-D4	
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.					
646	(Y)RS-62N	PEORIA	48	46	CONTRACT NO. 68E93				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT							



FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION

TYPICAL PAVEMENT MARKING LEGEND

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A) (See Table A)
- ⑪ 4(100) Double Solid (Yellow) (See Table A)

SPECIAL NOTES

1. Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
2. The following shall apply to arrows located in one-way left turn lanes:
 - A. A minimum of two (2) arrows is required.
 - B. The maximum spacing between arrows is 80' (24 m).
 - C. Arrows shall be evenly spaced if three (3) or more are required.
3. The following shall apply to arrow pairs located in two-way left turn lanes:
 - A. A minimum of two (2) arrow pairs is required.
 - B. The maximum spacing between arrow pairs is 200' (61 m).
 - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
 - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

GENERAL NOTES

1. Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
2. See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
3. Refer to Article 780.13 for letter, number and symbol areas (sq. ft.)
4. Areas are grooved 1" beyond each edge for the following symbols:
Through Arrow= 14.8 sq. ft.
Large Left or Right Arrow= 21.9 sq. ft.
2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.
Wrong Way Arrow= 29.5 sq. ft.
Railroad Crossing Symbol= 69.8 sq. ft.
(For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

01-01-97	RENUM. F-8.03, NEW REVISION BOX	T.P.	10-16-06	REVISED TO 2007 SPEC.	
02-07-97	ADD BI DIRECTIONAL DIMENSION	J.A.	2/29/16	ADDED GROOVING AREAS	R.D.
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.			
08-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.			

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL PAVEMENT MARKINGS

NOT TO SCALE

SHT. 1 OF 2
CADD STD. 780001-D4

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
646	(Y)RS-62N	PEORIA	48	47
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 68E93	

