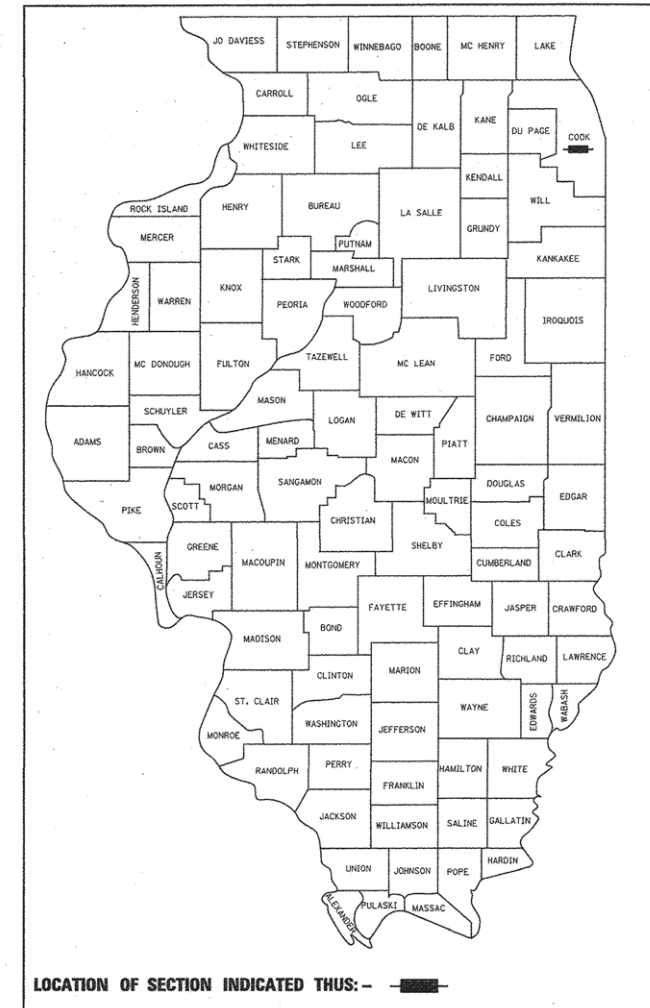


INDEX OF SHEETS (SEE SHEET 2 )  
HIGHWAY STANDARDS (SEE SHEET 2 )

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
**PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY**

FAU 1693 (EAST QUINCY STREET) LONGCOMMON ROAD TO HARLEM AVENUE RESURFACING  
FAU 3569 (LONGCOMMON ROAD) EAST BURLINGTON ROAD TO HARLEM AVENUE RESURFACING  
SECTION No. 16-00083-00-RS  
PROJECT No. M-4003(724)  
VILLAGE OF RIVERSIDE  
COOK COUNTY  
JOB No: C-91-259-16

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1693	16-00083-00-RS	COOK	31	1
3569		ILLINOIS	CONTRACT NO.	61C96



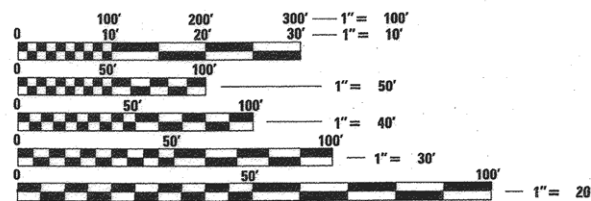
**TRAFFIC DATA**

LONGCOMMON ROAD  
ADT (YEAR) = 2100 (2014)  
SPEED LIMIT = 25 MPH

DESIGN DESIGNATION: MAJOR COLLECTOR (URBAN)

EAST QUINCY STREET  
ADT (YEAR) = 1350 (2014)  
SPEED LIMIT = 25 MPH

DESIGN DESIGNATION: MAJOR COLLECTOR (URBAN)



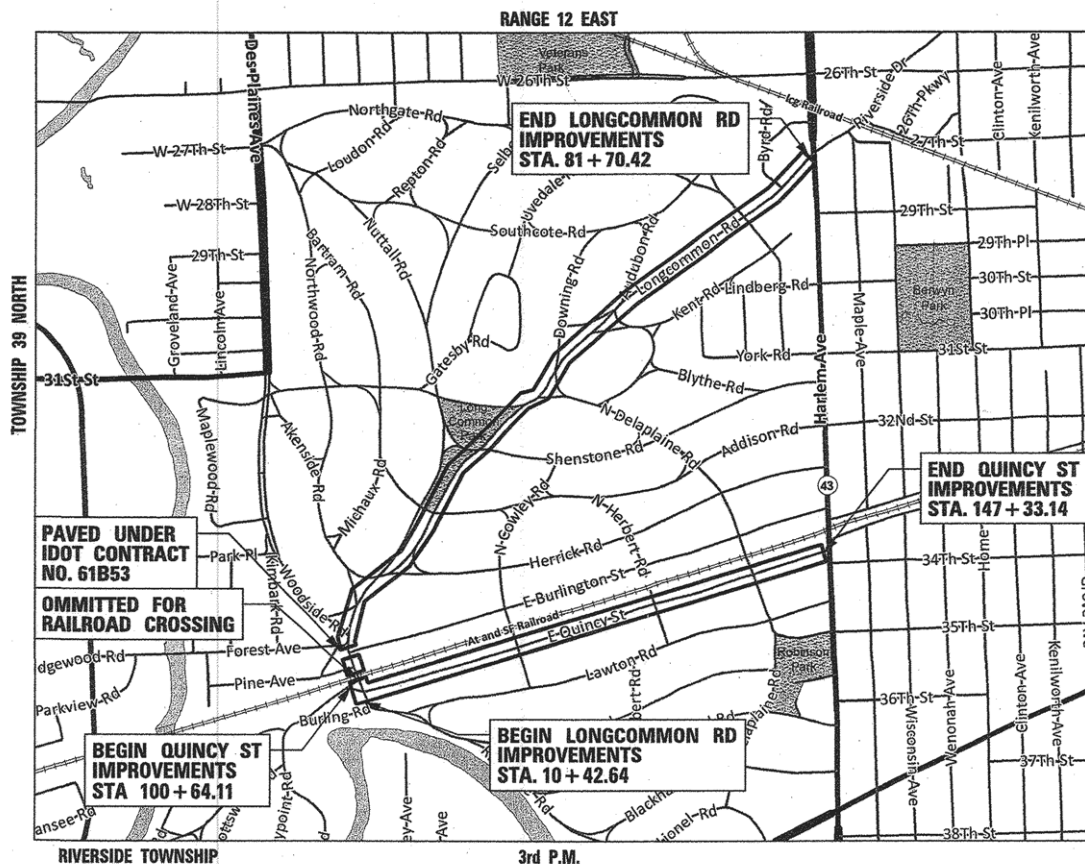
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

**CB** CHRISTOPHER B. BURKE ENGINEERING, LTD.  
9575 W. Higgins Road, Suite 600  
Rosemont, Illinois 60018  
(847) 823-0500

PROFESSIONAL DESIGN FIRM NO. 184-001175  
EXPIRATION DATE: 04/30/17

**CONTRACT NO. 61C96**



**LOCATION MAP**

LONGCOMMON ROAD  
GROSS LENGTH OF PROJECT = 7069 LINEAL FEET (1.34 MILES)  
NET LENGTH OF PROJECT = 6950 LINEAL FEET (1.31 MILES)  
EAST QUINCY STREET  
GROSS LENGTH OF PROJECT = 4369 LINEAL FEET (0.88 MILES)  
NET LENGTH OF PROJECT = 4369 LINEAL FEET (0.88 MILES)

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED May 2 2016  
*Edward Bailey*  
PUBLIC WORKS DIRECTOR, VILLAGE OF RIVERSIDE

PASSED May 22 2016  
*Carl Christensen*  
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW May 23 2016  
*John Featherman*  
REGIONAL ENGINEER



May 2 2016  
*Orion C. Galet*  
ORION C. GALET  
ILLINOIS REGISTRATION No. 062-060829  
EXPIRATION DATE: 11/30/2017

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. (847) 705-4406, SCHAUMBURG, IL.

GENERAL NOTES

- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE CURRENT EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTIONS IN ILLINOIS."
- ANY EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR AT NO COST TO THE VILLAGE.
- ALL DOMESTIC WATER SERVICE BOXES SHALL BE ADJUSTED TO THE PROPOSED GRADE AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE PAID FOR AS DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED.
- EXPOSED SUBGRADE MUST BE COVERED WITHIN 24 HOURS OF EXCAVATION. UNSTABLE SUBGRADE AREAS, AS DETERMINED BY THE ENGINEER, RESULTING FROM THE CONTRACTOR'S FAILURE TO COVER THE SUBGRADE SHALL BE EXCAVATED AND BACKFILLED WITH POROUS GRANULAR EMBANKMENT, SUBGRADE AT NO COST TO THE OWNER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND/OR ADJUSTED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS.
- THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR AT HIS OWN EXPENSE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER OR OWNER, OR REPLACED. SUCH WORK WILL BE AT THE CONTRACTOR'S EXPENSE.
- WHEN IT IS NECESSARY TO SHUT OFF A WATER MAIN, THE CONTRACTOR SHALL PROVIDE THE VILLAGE A MINIMUM 48 HOUR ADVANCE NOTICE, AND THE CONTRACTOR WILL BE RESPONSIBLE FOR NOTIFYING ALL AFFECTED RESIDENTS. THE VILLAGE WILL PROVIDE THE NECESSARY FORMS AND DETERMINE THE LIMITS OF AFFECTED AREAS.
- THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. IF WATER IS NEEDED DURING CONSTRUCTION ACTIVITIES THE CONTRACTOR MUST CONTACT THE RIVERSIDE WATER DEPARTMENT AT (708) 442-3590.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.U.L.I.E." AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, CABLE AND GAS FACILITIES AND THE VILLAGE OF RIVERSIDE PUBLIC WORKS DEPT. AT (708) 442-3590 FOR FIELD LOCATIONS OF BURIED WATER, SANITARY AND STORM FACILITIES (48-HOUR ADVANCE NOTIFICATION IS REQUIRED).
- DURING CONSTRUCTION OPERATIONS, WHENEVER ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINES OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION, ALL DRAINAGE AND UTILITY STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS.
- ALL SEWER AND WATER SERVICES CROSSED BY NEW STORM SEWERS SHALL BE PROPERLY LOCATED AND PROTECTED DURING CONSTRUCTION. ANY DAMAGE DONE TO SAID SERVICES NOT CONSIDERED TO BE IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.
- THE COST OF CONNECTING PROPOSED STRUCTURES AND THE FIRST 5 FEET OF ASSOCIATED PIPE TO THE EXISTING DRAINAGE SYSTEM SHALL BE CONSIDERED INCLUDED IN THE COST OF THE STRUCTURE.
- THE ENDS OF EXISTING DRAINAGE LINES AND HOLES IN EXISTING MANHOLES WHICH ARE NOT TO BE INCORPORATED INTO THE PROPOSED IMPROVEMENTS DESIGNATED BY THE ENGINEER SHALL BE SEALED WITH A PORTLAND CEMENT MORTAR TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF THE STRUCTURE IMPROVEMENT.
- THE CONTRACTOR SHALL CONFIRM ALL EXISTING STORM SEWER PIPE SIZES AND INVERTS PRIOR TO ORDERING STRUCTURES. ANY MODIFICATION OF STRUCTURES DUE TO THE FAILURE OF THE CONTRACTOR TO PERFORM THIS TASK SHALL BE AT THE CONTRACTOR'S EXPENSE AND MAY LEAD TO THE REJECTION OF THE STRUCTURE IN THE FIELD. PIPE TYPES INDICATED ON THESE PLANS AT THE STRUCTURE CALL-OUTS ARE THE EXISTING TYPES IDENTIFIED IN THE FIELD. PROPOSED PIPE IS PVC.
- THE CONTRACTOR SHALL PROVIDE ACCESS TO ADJUTING PROPERTIES AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT EXCEPT FOR PERIODS OF SHORT DURATION AS APPROVED BY THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
- ALL SAWCUTTING SHALL BE INCLUDED IN THE COST OF ITEM BEING REMOVED AND SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR RE-ERECTING ANY SIGNS AND POSTS REMOVED DURING CONSTRUCTION. RELOCATION OF EXISTING SIGNS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT. IF DURING CONSTRUCTION THE CONTRACTOR DAMAGES ANY EXISTING SIGNS, HE WILL REPLACE THE SIGN AT NO COST TO THE VILLAGE.
- THE CONTRACTOR SHALL SAWCUT AND REMOVE ONLY THE NECESSARY AREA OF EXISTING PAVEMENT NEEDED TO INSTALL PROPOSED STORM SEWER. THIS AREA SHALL BE BACKFILLED TO EXISTING PAVEMENT GRADE USING TRENCH BACKFILL, SPECIAL.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING ALL RESIDENTS AND THE VILLAGE OF RIVERSIDE 48 HOURS PRIOR TO ANY ROAD CLOSURE.
- ANY DEFECTS OF THE CONCRETE CURB & GUTTER AS IDENTIFIED BY THE RESIDENT ENGINEER SHALL BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE PRIOR TO PLACEMENT OF BITUMINOUS MATERIALS.
- AT THE END OF EACH DAY, IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO ASSURE THAT ALL STREETS ADJACENT TO THE PROJECT ARE FREE OF ALL CONSTRUCTION RELATED DEBRIS INCLUDING DIRT, STONE, NAILS ETC. THIS WORK SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER AND THE VILLAGE OF RIVERSIDE.
- THE CONTRACTOR SHALL COMPLETE ALL UTILITY WORK PRIOR TO PAVEMENT REMOVAL. THE CONTRACTOR SHALL ONLY REMOVE THE PORTION OF PAVEMENT NECESSARY TO INSTALL PROPOSED UTILITIES. PAVEMENT REMOVED FOR UTILITY WORK SHALL BE RESTORED AS INDICATED ON THE PLANS. ALL PAVEMENT REMOVED FOR UTILITY WORK MUST BE SAWCUT.

- THE CONTRACTOR AND ENGINEER SHALL FIELD VERIFY ALL DRAINAGE STRUCTURES TO DETERMINE WHETHER EACH STRUCTURE REQUIRES REMOVAL AND REPLACEMENT, RECONSTRUCTION, OR ADJUSTMENT.
- THE CONTRACTOR AND ENGINEER SHALL FIELD VERIFY ALL THE EXISTING PAVEMENT AFTER THE EXISTING BITUMINOUS SURFACE IS REMOVED TO DETERMINE THE AREAS THAT PATCHING IS REQUIRED.
- PAVEMENT MARKINGS TO BE FIELD LOCATED BY ENGINEER
- REMOVAL OF ASPHALT ON CURB AND GUTTER SHALL BE IN THE COST OF CURB AND GUTTER REMOVAL AND REPLACEMENT.
- CURB HEIGHTS SHOWN ON THE PLANS ARE ESTIMATED. CONTRACTOR SHALL LOWER THE CURB HEIGHT AS NECESSARY TO ACHIEVE POSITIVE DRAINAGE FROM THE FRONT OF WALK TO THE BACK OF CURB. NO CURB HEAD HEIGHT SHALL BE POURED LESS THAN 3 INCHES.
- SIDEWALK REMOVAL SHALL INCLUDE THICKNESS FOR NEW EXPOSED AGGREGATE SURFACE AND NEW SUBBASE TO BE REPLACED. ANY ADDITIONAL EARTH EXCAVATION NEEDED TO MEET ADA REQUIREMENTS SHALL BE INCLUDED.

MWRD TYPICAL GENERAL NOTES

- THE MWRD LOCAL SEWER SYSTEMS SECTION FIELD OFFICE MUST BE NOTIFIED AT LEAST TWO (2) WORKING DAYS PRIOR TO THE COMMENCEMENT OF ANY WORK (CALL 708-588-4055).
- ELEVATION DATUM IS CCD.
- NO FLOOR DRAINS
- NO FOOTING DRAINS AND DOWNSPOUTS
- ALL SANITARY SEWER PIPE MATERIALS AND JOINTS (AND STORM SEWER PIPE MATERIALS AND JOINTS IN A COMBINED SEWER AREA) SHALL CONFORM TO:
 

PIPE MATERIAL SPEC.	JOINT SPEC.
VITRIFIED CLAY PIPE	
VCP C-700	C-425
VCP (NO-BEL) C-700	C-425
JOINT	D-1784
COLLAR	
CONCRETE PIPE C-14	C-443
RCP C-76	C-443
ACP C-428	D-1869
ABS SEWER PIPE	
SOLID WALL 6" DIA.	SDR 23.5
ABS D-2751	D-2751
ABS COMPOSITE/TRUSS PIPE	
8"-15" DIA.	D-2680
ABS D-2680	
PVC GRAVITY SEWER PIPE	
6"-15" DIA. SDR 26	D-3212 OR
D-3034	
D-2855	
18"-27" DIA. F/DY=46	D-3212 OR
F-679	
D-2855	
CISP A-74	C-564
DIP A-21.51	A-21.11

(NOTE: THE DISTRICT HAS APPROVED LESS COMMON PIPE MATERIALS ON A QUALIFIED BASIS IN ADDITION TO THOSE ABOVE. PLEASE CONTACT THE DISTRICT IF CONSIDERING USING PIPE NOT LISTED ABOVE.)

- ALL SANITARY SEWER CONSTRUCTION (AND STORM SEWER CONSTRUCTION IN COMBINED SEWER AREAS), REQUIRES STONE BEDDING WITH STONE 1/4" TO 1" IN SIZE, WITH MINIMUM BEDDING THICKNESS EQUAL TO THE OUTSIDE DIAMETER OF THE SEWER PIPE, BUT NOT LESS THAN FOUR (4) INCHES NOR MORE THAN EIGHT (8) INCHES. MATERIAL SHALL BE CA-11 OR CA-13 AND SHALL BE EXTENDED AT LEAST 12" ABOVE THE TOP OF THE PIPE WHEN USING PVC.
- COUPLINGS THAT CREATE A WATERTIGHT SEAL SHALL BE USED IN THE CONNECTION OF SEWER PIPES OF DISSIMILAR MATERIALS.
- WHEN CONNECTING TO AN EXISTING SEWER MAIN BY MEANS OTHER THAN AN EXISTING WYE, TEE, OR AN EXISTING MANHOLE, ONE OF THE FOLLOWING METHODS SHALL BE USED:
  - CIRCULAR SAW-CUT OF SEWER MAIN BY PROPER TOOLS AND PROPER INSTALLATION OF A SADDLE.
  - REMOVE AN ENTIRE SECTION OF PIPE (BREAKING ONLY THE TOP OF ONE BELL) AND REPLACE WITH A WYE OR TEE BRANCH SECTION.
  - WITH PIPE CUTTER, NEATLY AND ACCURATELY CUT OUT DESIRED LENGTH OF PIPE FOR INSERTION OF PROPER FITTING, USING WATER TIGHT SEAL COUPLINGS TO HOLD IT FIRMLY IN PLACE.
- WHENEVER A SANITARY/COMBINED SEWER CROSSES UNDER A WATERMAIN, THE MINIMUM VERTICAL DISTANCE FROM THE TOP OF THE SEWER TO THE BOTTOM OF THE WATERMAIN SHALL BE 18 INCHES. FURTHERMORE, A MINIMUM HORIZONTAL DISTANCE OF 10 FEET BETWEEN SANITARY/COMBINED SEWERS AND WATERMANS SHALL BE MAINTAINED UNLESS: THE SEWER IS LAID IN A SEPARATE TRENCH, KEEPING A MINIMUM 18" VERTICAL SEPARATION; OR THE SEWER IS LAID IN THE SAME TRENCH WITH THE WATERMAIN LOCATED AT THE OPPOSITE SIDE ON A BENCH OF UNDISTURBED EARTH, KEEPING A MINIMUM 18" VERTICAL SEPARATION. IF EITHER THE VERTICAL OR HORIZONTAL DISTANCES DESCRIBED ABOVE CAN NOT BE MAINTAINED, OR THE SEWER CROSSES ABOVE THE WATERMAIN, THE SEWER SHALL BE CONSTRUCTED TO WATERMAIN STANDARDS.
- ALL EXISTING SEPTIC SYSTEMS SHALL BE ABANDONED. ABANDONED TANKS SHALL BE FILLED WITH GRANULAR MATERIAL OR REMOVED.
- ALL SANITARY MANHOLES, (AND STORM MANHOLES IN COMBINED SEWER AREAS), SHALL HAVE A MINIMUM INSIDE DIAMETER OF 48 INCHES, AND SHALL BE CAST IN PLACE OR PRE-CAST REINFORCED CONCRETE.
- ALL INLET AND OUTLET PIPES OF SANITARY SEWER MANHOLES AND OTHER UNDERGROUND STRUCTURES (AND IN COMBINED SEWER AREAS, ALSO ALL COMBINED/STORM SEWER MANHOLES, CATCH BASINS, INLETS, AND UNDERGROUND DETENTION STORAGE STRUCTURES) SHALL BE JOINED WITH WATERTIGHT FLEXIBLE RUBBER CONNECTORS CONFORMING TO A.S.T.M. C-4443 AND C-923 WITH STAINLESS STEEL BAND.

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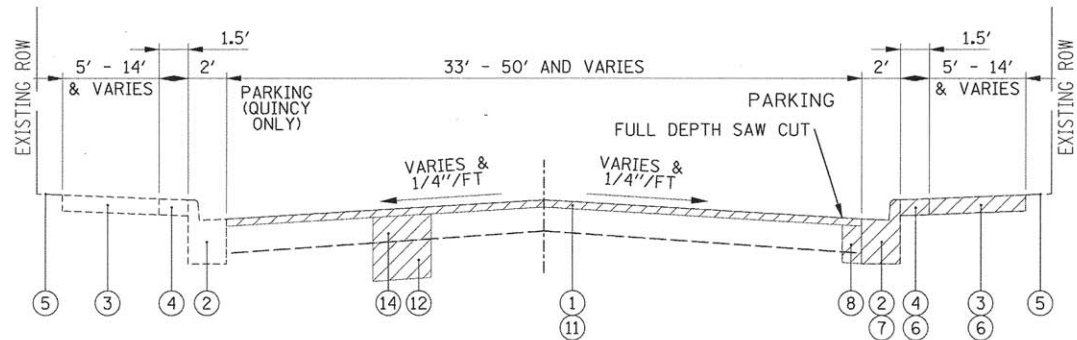
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SUMMARY OF QUANTITIES

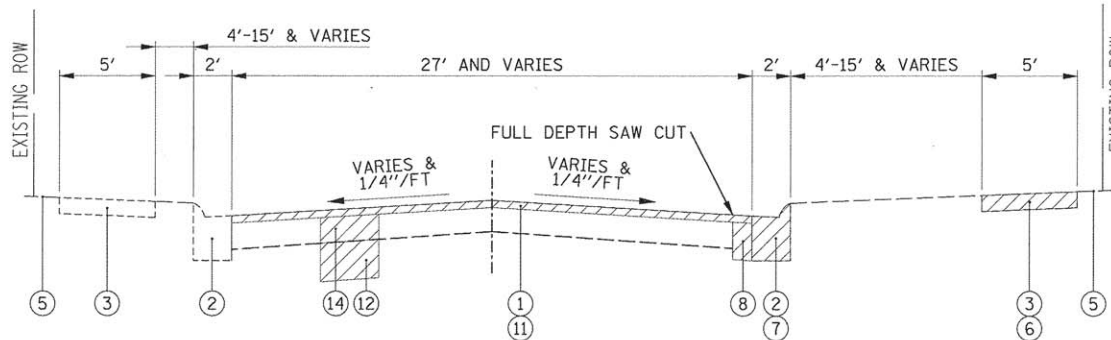
ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	0005	0005
				QUINCY STREET	LONGCOMMON ROAD
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS	CU YD	50	20	30
25200200	SUPPLEMENTAL WATERING	UNIT	45	20	25
28000510	INLET FILTERS	EACH	40	20	20
30300112	AGGREGATE SUBGRADE IMPROVEMENT, 12"	SQ YD	150	60	90
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	100	50	50
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	26181	9469	16712
40600400	MIXTURE FOR CRACKS, JOINTS AND FLANGWAYS	TON	58	21	37
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75 N50	TON	1711	619	1092
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	417	79	338
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	3422	1238	2184
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	150	50	100
42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SQ FT	350	250	100
42400800	DETECTABLE WARNINGS	SQ FT	801	359	442
44000157	HOT MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	38786	14028	24758
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	150	50	100
44000600	SIDEWALK REMOVAL	SQ FT	13708	5205	8503
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	292	106	186
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	874	316	558
* 56500600	DOMESTIC WATER SERVICE BOX TO BE ADJUSTED	EACH	20	10	10
60200105	CATCH BASIN, TYPE A, 4' DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	18	15	3
* 60300410	VALVE BOX FRAMES TO BE ADJUSTED WITH NEW FRAMES	EACH	8	8	0
60404940	FRAMES AND GRATES, TYPE 23	EACH	14	4	10
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	12	10	2
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	20	10	10
60600605	CONCRETE CURB, TYPE B	FOOT	300	150	150
INDICATES A SPECIAL PROVISION					

ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	0005	0005
				QUINCY STREET	LONGCOMMON ROAD
67100100	MOBILIZATION	L SUM	1	.5	.5
70102620	TRAFFIC CONTROL AND PROTECTION STANDARD 701501	L SUM	1	.5	.5
70102635	TRAFFIC CONTROL AND PROTECTION STANDARD 701701	L SUM	1	.5	.5
70102640	TRAFFIC CONTROL AND PROTECTION STANDARD 701801	L SUM	1	.5	.5
70300100	SHORT TERM PAVEMENT MARKING	FOOT	1250	500	750
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	417	167	250
Δ 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	330	98	232
Δ 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4166	1778	2388
Δ 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3051	1138	1913
Δ 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	854	0	854
Δ 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	829	190	639
Δ * 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1	1	0
Δ 87900200	DRILL EXISTING HANDHOLE	EACH	1	0	1
Δ * 88600600	DETECTOR LOOP REPLACEMENT	FOOT	118	0	118
* X0326862	STRUCTURES TO BE ADJUSTED	EACH	89	33	56
* X2520700	SODDING, (SPECIAL)	SQ YD	1797	866	931
* X6022810	MANHOLES, SANITARY, 4' DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	3	3	0
* X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	98	51	47
* Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	4	2	2
* Z0018700	DRAINAGE STRUCTURE TO BE REMOVED	EACH	27	23	4
* Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	104	52	52
* XX000406	BRICK PAVER REMOVAL AND REPLACEMENT	SQ FT	125	75	50
* XX001109	PORTLAND CEMENT CONCRETE ALLEY PAVEMENT, 8 INCH	SQ YD	14	0	14
* XX005701	ALLEY APRON APPROACH PAVEMENT REMOVAL	SQ YD	14	0	14
* XX008910	PAVEMENT MARKING (SPECIAL)	SQ FT	1756	966	790
* XX009049	REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)	FOOT	4252	2403	1849
* XX024100	EXPOSED AGGREGATE SURFACE	SQ FT	14144	5207	8937
INDICATES A SPECIAL PROVISION					

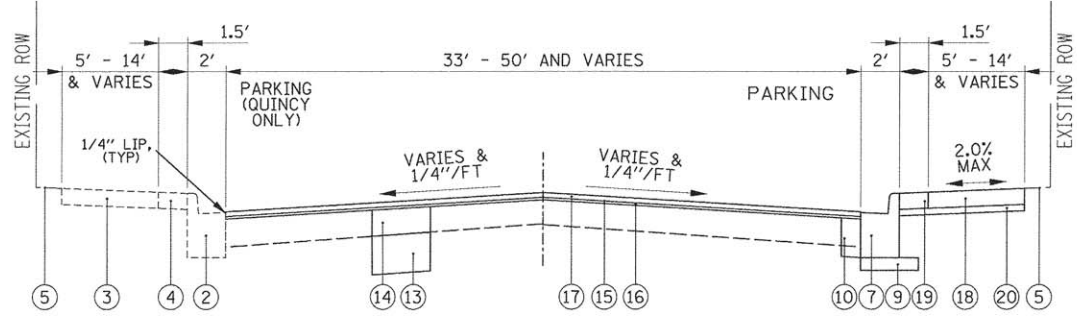
Δ SPECIALTY ITEMS



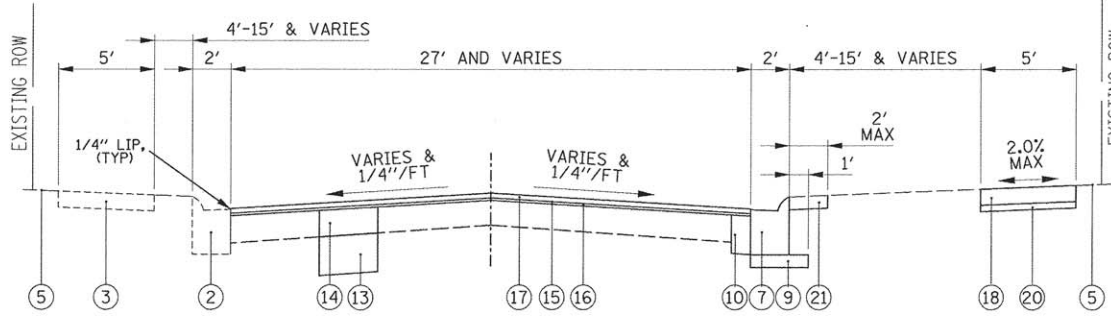
EXISTING TYPICAL SECTION  
QUINCY STREET  
STA 100+64.11 TO STA 107+05 (CURB: B-6.18)  
LONGCOMMON DRIVE  
STA 10+42.64 TO STA 17+00 (CURB: M-6.18)



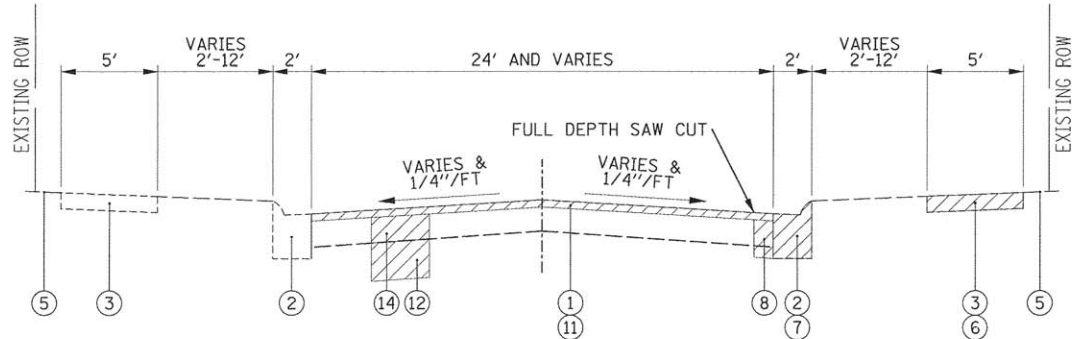
EXISTING TYPICAL SECTION  
LONGCOMMON DRIVE  
STA 45+00 TO STA 81+70.42 (CURB: M-6.18)



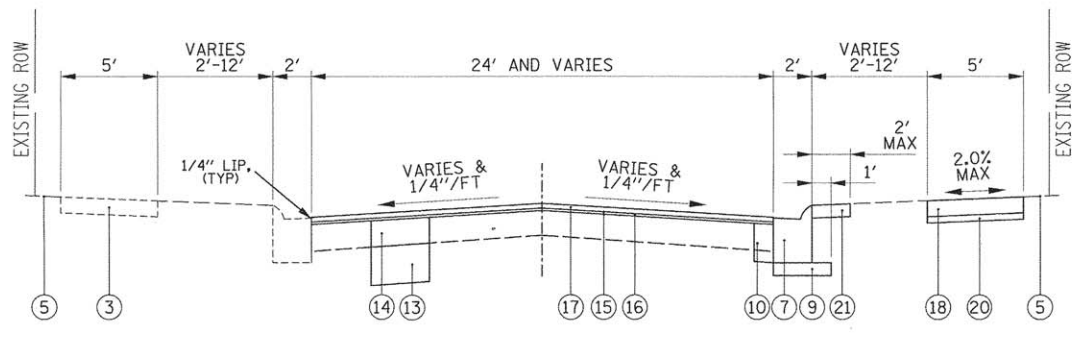
PROPOSED TYPICAL SECTION  
QUINCY STREET  
STA 100+64.11 TO STA 107+05 (CURB: B-6.18)  
LONGCOMMON DRIVE  
STA 10+42.64 TO STA 17+00 (CURB: M-6.18)



PROPOSED TYPICAL SECTION  
LONGCOMMON DRIVE  
STA 45+00 TO STA 81+70.42 (CURB: M-6.18)



EXISTING TYPICAL SECTION  
QUINCY STREET  
STA 107+05 TO STA 147+33.14 (CURB: M-6.18)  
LONGCOMMON DRIVE  
STA 17+00 TO STA 45+00 (CURB: M-6.18)



PROPOSED TYPICAL SECTION  
QUINCY STREET  
STA 107+05 TO STA 147+33.14 (CURB: M-6.18)  
LONGCOMMON DRIVE  
STA 17+00 TO STA 45+00 (CURB: M-6.18)

- LEGEND
- ITEMS TO BE REMOVED
- ① EXISTING BITUMINOUS BINDER AND SURFACE COURSE, 10"
  - ② EXISTING COMBINATION CONCRETE CURB AND GUTTER
  - ③ EXISTING EXPOSED AGGREGATE SIDEWALK
  - ④ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
  - ⑤ EXISTING SOIL AND GROUND COVER
  - ⑥ SIDEWALK REMOVAL (AS DIRECTED BY THE ENGINEER IN THE FIELD)
  - ⑦ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, SPECIAL (AS DIRECTED BY THE ENGINEER IN THE FIELD)
  - ⑧ EXISTING PAVEMENT REMOVAL (INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, SPECIAL)
  - ⑨ SUBBASE GRANULAR MATERIAL, TYPE B, 4" (INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, SPECIAL)
  - ⑩ P.C.C. BASE COURSE (INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, SPECIAL)
  - ⑪ HOT MIX ASPHALT SURFACE REMOVAL, (2")
  - ⑫ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS (AS DIRECTED BY THE ENGINEER IN THE FIELD)
  - ⑬ AGGREGATE SUBGRADE IMPROVEMENT, 12" (AS DIRECTED BY THE ENGINEER IN THE FIELD)
  - ⑭ CLASS D PATCHING, 8 INCH (AS DIRECTED BY THE ENGINEER IN THE FIELD)
  - ⑮ BITUMINOUS MATERIALS (TACK COAT)
  - ⑯ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 (3/4" MIN.)
  - ⑰ HMA SURFACE COURSE, MIX "D", N50 - 1.5"
  - ⑱ EXPOSED AGGREGATE SURFACE (AS DIRECTED BY THE ENGINEER IN THE FIELD)
  - ⑲ PORTLAND CEMENT CONCRETE SIDEWALK, 5" (AS DIRECTED BY THE ENGINEER IN THE FIELD)
  - ⑳ 2" AGGREGATE BASE COURSE (INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE SIDEWALK, 5" AND EXPOSED AGGREGATE SURFACE)
  - ㉑ SODDING, SALT TOLERANT (SPECIAL)

NOTE: DRIVEWAY PAVEMENT REMOVAL AND ALLEY APRON APPROACH PAVEMENT REMOVAL SHALL OCCUR IF THE ADJACENT CURB AND GUTTER IS TO BE REMOVED. ALL LIMITS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE ITEM	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5mm), 1.5"	4% @ 50 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 4.75, N50, 3/4"	3.5% @ 50 GYR
CLASS D PATCHING, 8" (IN 2 LIFTS)	
HOT MIX ASPHALT BINDER (HMA BINDER IL-19mm)	4% @ 70 GYR

NOTES:  
THE UNIT WEIGHT USED TO CALCULATE ALL HMA MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.  
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.  
FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS.  
FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS

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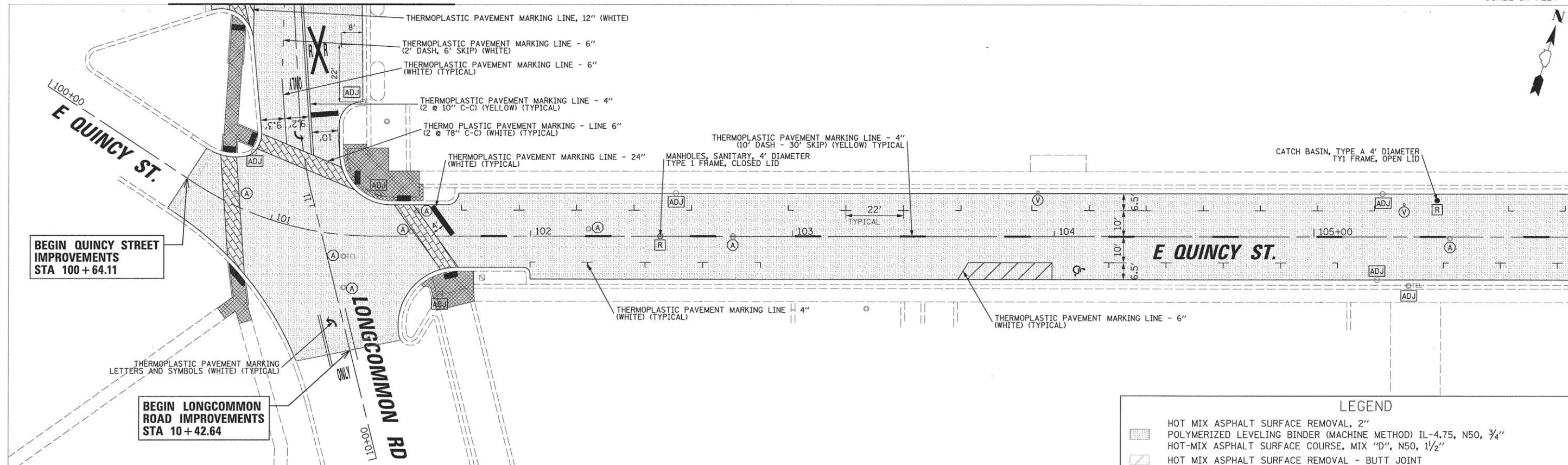
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS  
LONGCOMMON ROAD AND QUINCY STREET

SCALE: 20' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 3569	SECTION 16-00083-00-RS	COUNTY COOK	TOTAL SHEETS 31	SHEET NO. 4
CONTRACT NO. 61C96			ILLINOIS FED. AID PROJECT	

MATCH LINE STA. 11 + 77.40



BEGIN QUINCY STREET IMPROVEMENTS STA 100 + 64.11

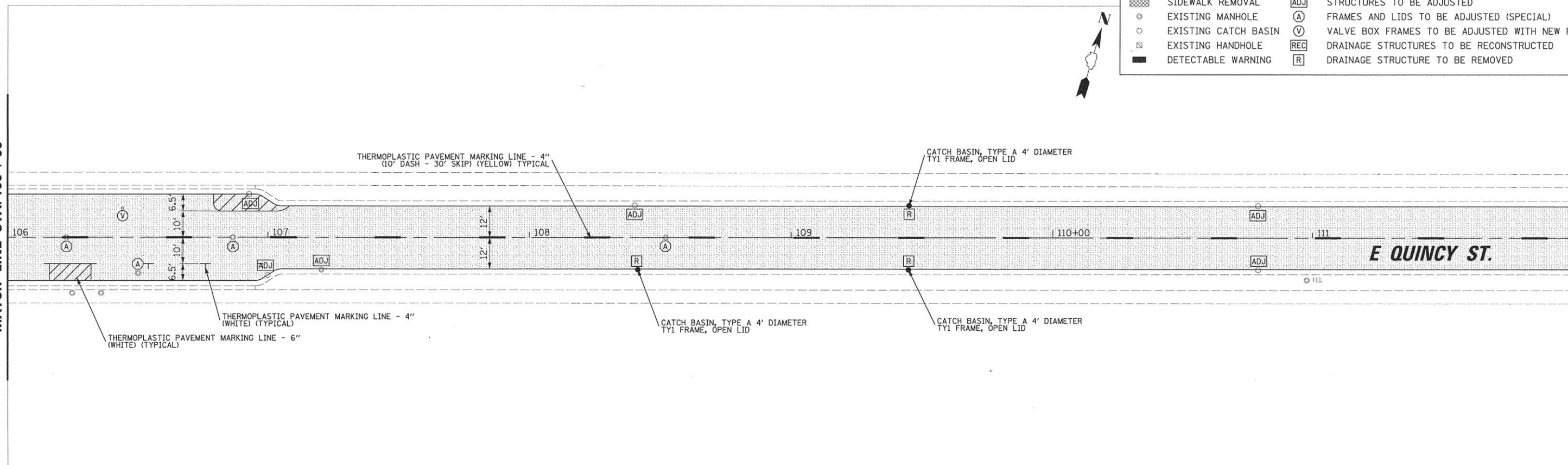
BEGIN LONGCOMMON ROAD IMPROVEMENTS STA 10 + 42.64

MATCH LINE STA. 106 + 00

LEGEND

[Pattern]	HOT MIX ASPHALT SURFACE REMOVAL, 2"	[Symbol]	STRUCTURES TO BE ADJUSTED
[Pattern]	POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, 3/4"	[Symbol]	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
[Pattern]	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"	[Symbol]	VALVE BOX FRAMES TO BE ADJUSTED WITH NEW FRAMES
[Pattern]	HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	[Symbol]	DRAINAGE STRUCTURES TO BE RECONSTRUCTED
[Pattern]	EXPOSED AGGREGATE SURFACE	[Symbol]	DRAINAGE STRUCTURE TO BE REMOVED
[Pattern]	PAVEMENT MARKING (SPECIAL)		
[Pattern]	SIDEWALK REMOVAL		
[Symbol]	EXISTING MANHOLE		
[Symbol]	EXISTING CATCH BASIN		
[Symbol]	EXISTING HANDHOLE		
[Symbol]	DETECTABLE WARNING		

MATCH LINE STA. 106 + 00

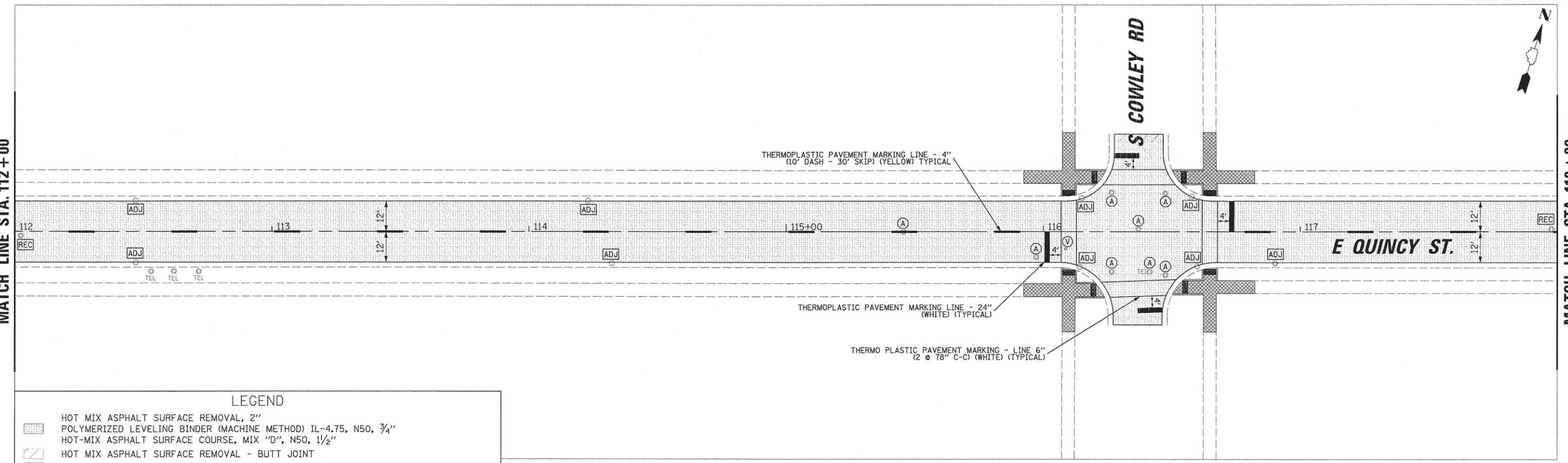


MATCH LINE STA. 112 + 00



MATCH LINE STA. 112+00

MATCH LINE STA. 118+00

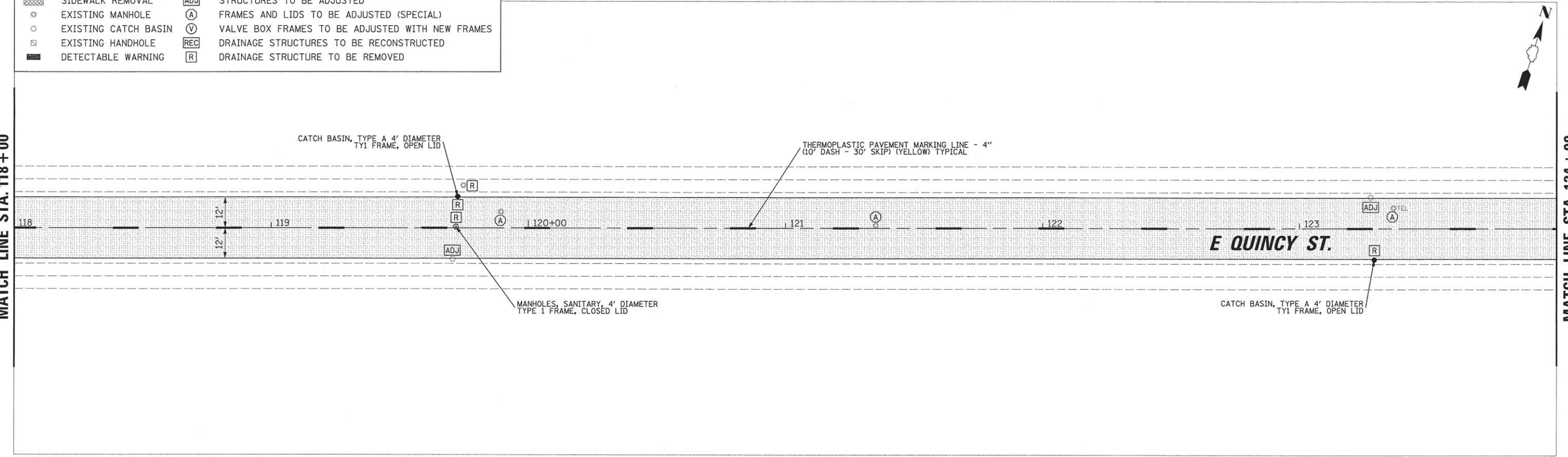


**LEGEND**

- HOT MIX ASPHALT SURFACE REMOVAL, 2"
- POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, 3/4"
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
- HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- EXPOSED AGGREGATE SURFACE
- PAVEMENT MARKING (SPECIAL)
- SIDEWALK REMOVAL
- EXISTING MANHOLE (A) STRUCTURES TO BE ADJUSTED
- EXISTING CATCH BASIN (V) FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
- EXISTING CATCH BASIN (V) VALVE BOX FRAMES TO BE ADJUSTED WITH NEW FRAMES
- EXISTING HANDHOLE (R) DRAINAGE STRUCTURES TO BE RECONSTRUCTED
- EXISTING HANDHOLE (R) DRAINAGE STRUCTURE TO BE REMOVED

MATCH LINE STA. 118+00

MATCH LINE STA. 124+00

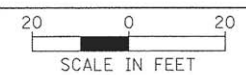


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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

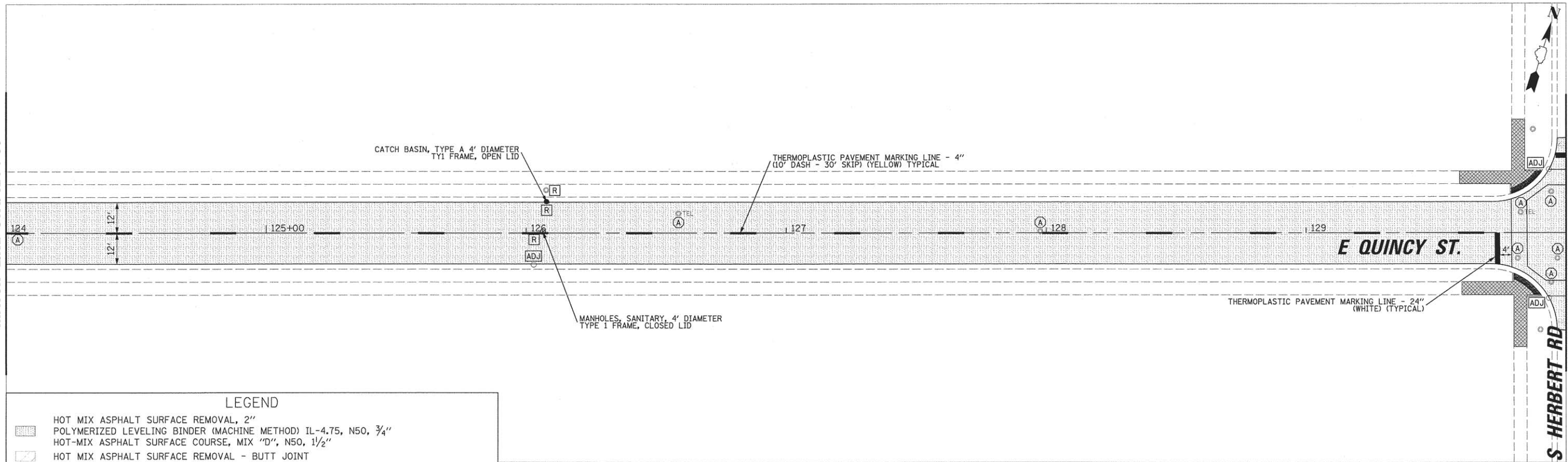
<b>ROADWAY PLAN</b>			
<b>LONGCOMMON ROAD AND QUINCY STREET</b>			
SCALE: 20'	SHEET 2 OF 11 SHEETS	STA. 112+00 TO STA. 124+00	

F.A.U. RTE. 3569	SECTION 16-00083-00-RS	COUNTY COOK	TOTAL SHEETS 31	SHEET NO. 6
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61C96	



MATCH LINE STA. 124+00

MATCH LINE STA. 130+00

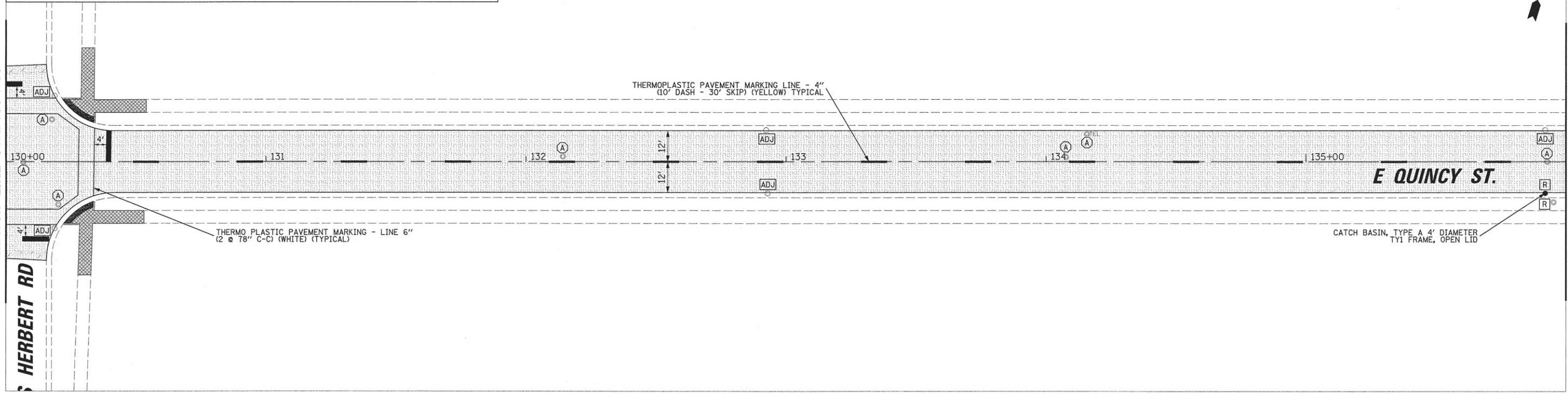


**LEGEND**

- HOT MIX ASPHALT SURFACE REMOVAL, 2"
- POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, 3/4"
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
- HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- EXPOSED AGGREGATE SURFACE
- PAVEMENT MARKING (SPECIAL)
- SIDEWALK REMOVAL
- EXISTING MANHOLE
- EXISTING CATCH BASIN
- EXISTING HANDHOLE
- DETECTABLE WARNING
- STRUCTURES TO BE ADJUSTED
- FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
- VALVE BOX FRAMES TO BE ADJUSTED WITH NEW FRAMES
- DRAINAGE STRUCTURES TO BE RECONSTRUCTED
- DRAINAGE STRUCTURE TO BE REMOVED

MATCH LINE STA. 130+00

MATCH LINE STA. 136+00



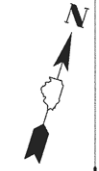
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
LONGCOMMON ROAD AND QUINCY STREET**

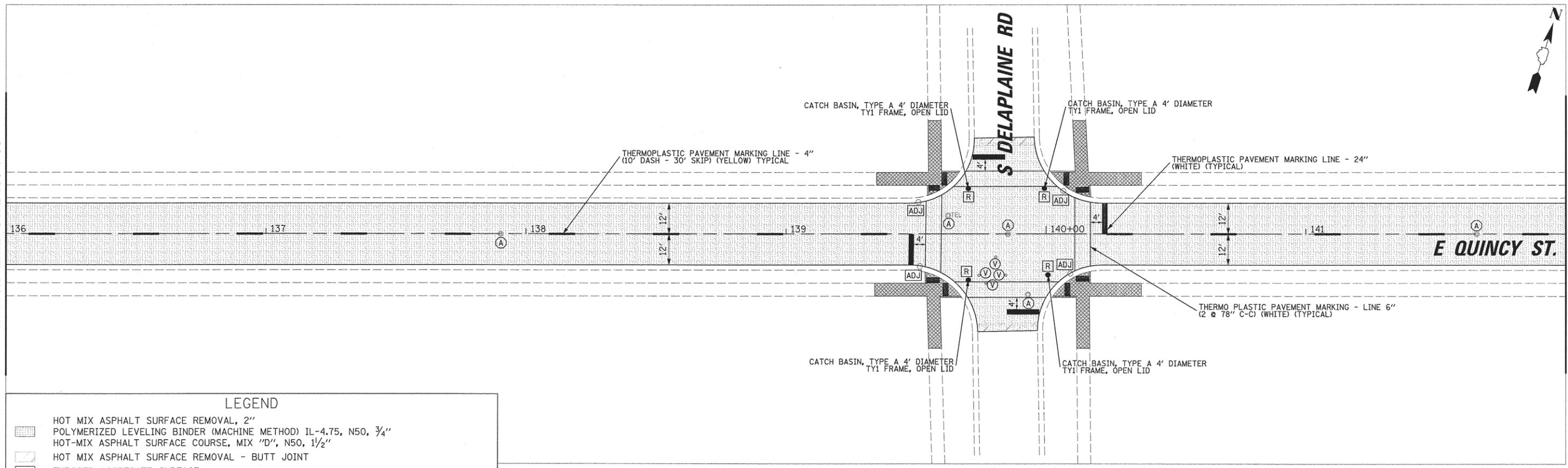
SCALE: 20'      SHEET 3 OF 11 SHEETS      STA. 124+00 TO STA. 136+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3569	16-00083-00-RS	COOK	31	7
CONTRACT NO. 61C96				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 136+00

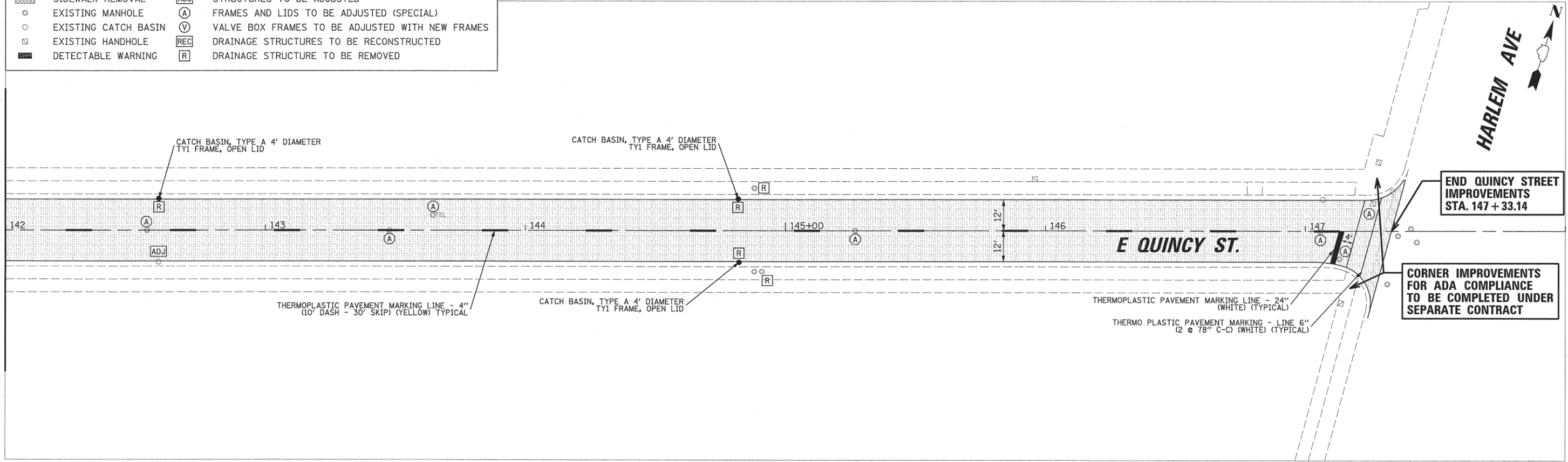
MATCH LINE STA. 142+00



**LEGEND**

	HOT MIX ASPHALT SURFACE REMOVAL, 2"		
	POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, 3/4"		
	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"		
	HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT		
	EXPOSED AGGREGATE SURFACE		
	PAVEMENT MARKING (SPECIAL)		
	SIDEWALK REMOVAL	<b>ADJ</b>	STRUCTURES TO BE ADJUSTED
	EXISTING MANHOLE	<b>A</b>	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
	EXISTING CATCH BASIN	<b>V</b>	VALVE BOX FRAMES TO BE ADJUSTED WITH NEW FRAMES
	EXISTING HANDHOLE	<b>REC</b>	DRAINAGE STRUCTURES TO BE RECONSTRUCTED
	DETECTABLE WARNING	<b>R</b>	DRAINAGE STRUCTURE TO BE REMOVED

MATCH LINE STA. 142+00



HARLEM AVE



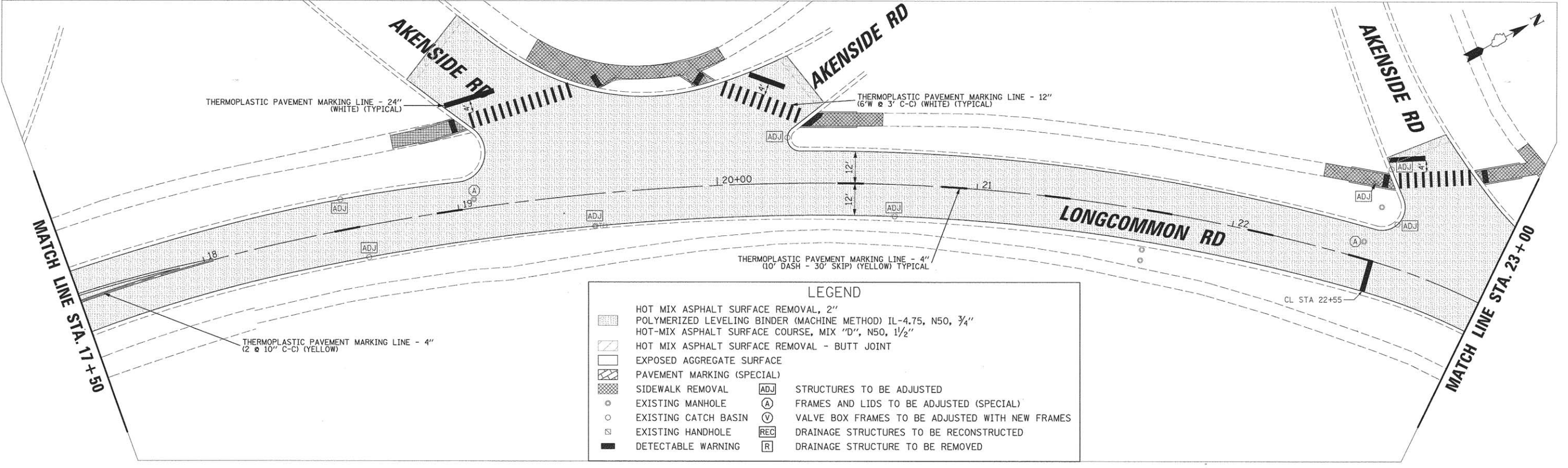
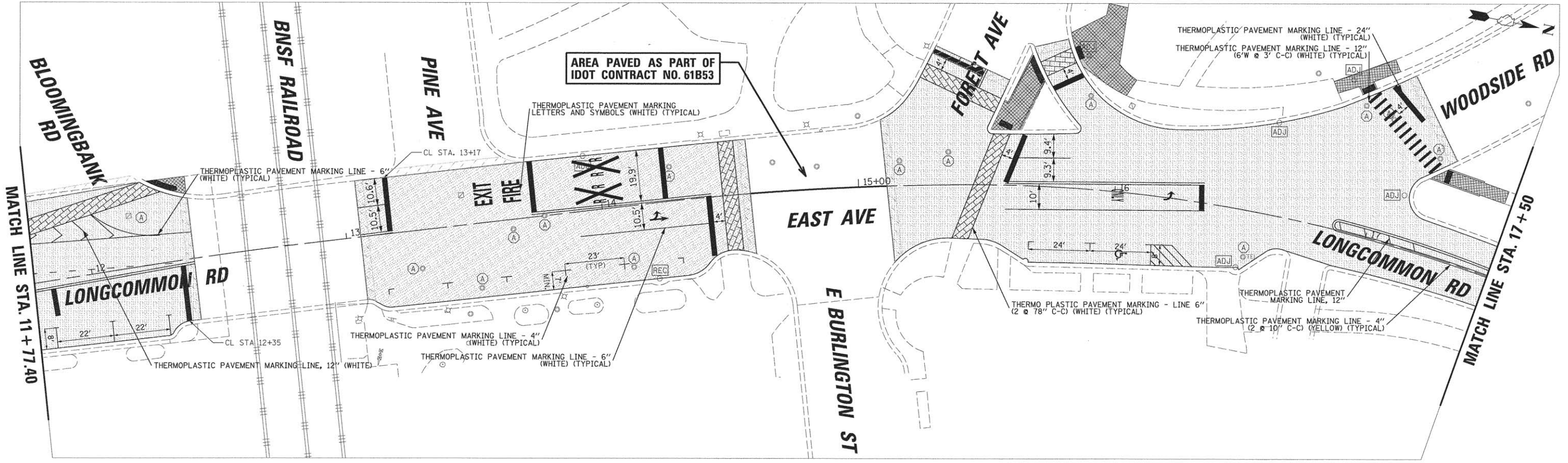
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>ROADWAY PLAN</b>	
<b>LONGCOMMON ROAD AND QUINCY STREET</b>	
SCALE: 20'	SHEET 4 OF 11 SHEETS STA. 136+00 TO STA. 148+00

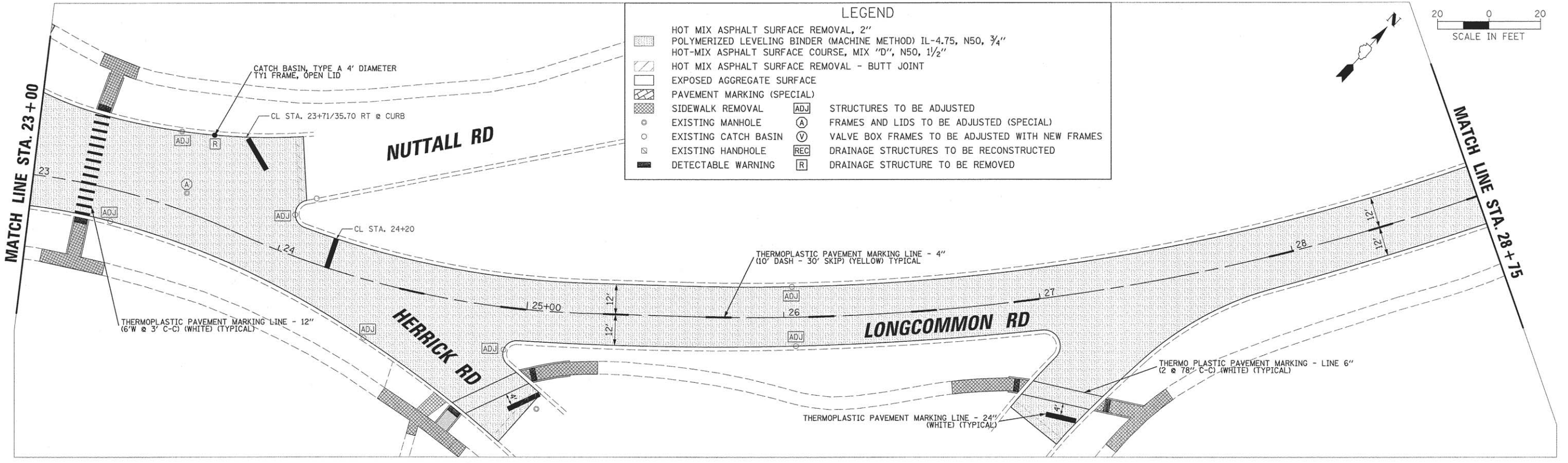
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3569	16-00083-00-RS	COOK	31	8
CONTRACT NO. 61C96			ILLINOIS FED. AID PROJECT	





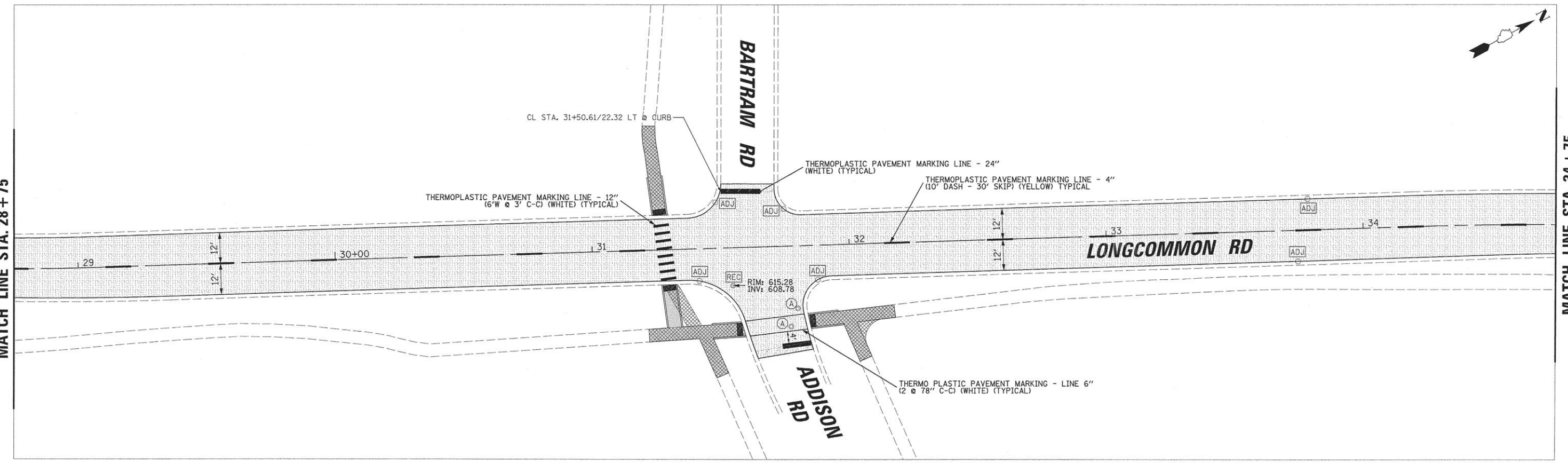
**LEGEND**

	HOT MIX ASPHALT SURFACE REMOVAL, 2"		STRUCTURES TO BE ADJUSTED
	POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, 3/4"		FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"		VALVE BOX FRAMES TO BE ADJUSTED WITH NEW FRAMES
	HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT		DRAINAGE STRUCTURES TO BE RECONSTRUCTED
	EXPOSED AGGREGATE SURFACE		DRAINAGE STRUCTURE TO BE REMOVED
	PAVEMENT MARKING (SPECIAL)		
	SIDEWALK REMOVAL		
	EXISTING MANHOLE		
	EXISTING CATCH BASIN		
	EXISTING HANDHOLE		
	DETECTABLE WARNING		



**LEGEND**

	HOT MIX ASPHALT SURFACE REMOVAL, 2"		STRUCTURES TO BE ADJUSTED
	POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, 3/4"		FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"		VALVE BOX FRAMES TO BE ADJUSTED WITH NEW FRAMES
	HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT		DRAINAGE STRUCTURES TO BE RECONSTRUCTED
	EXPOSED AGGREGATE SURFACE		DRAINAGE STRUCTURE TO BE REMOVED
	PAVEMENT MARKING (SPECIAL)		
	SIWALK REMOVAL		
	EXISTING MANHOLE		
	EXISTING CATCH BASIN		
	EXISTING HANDHOLE		
	DETECTABLE WARNING		



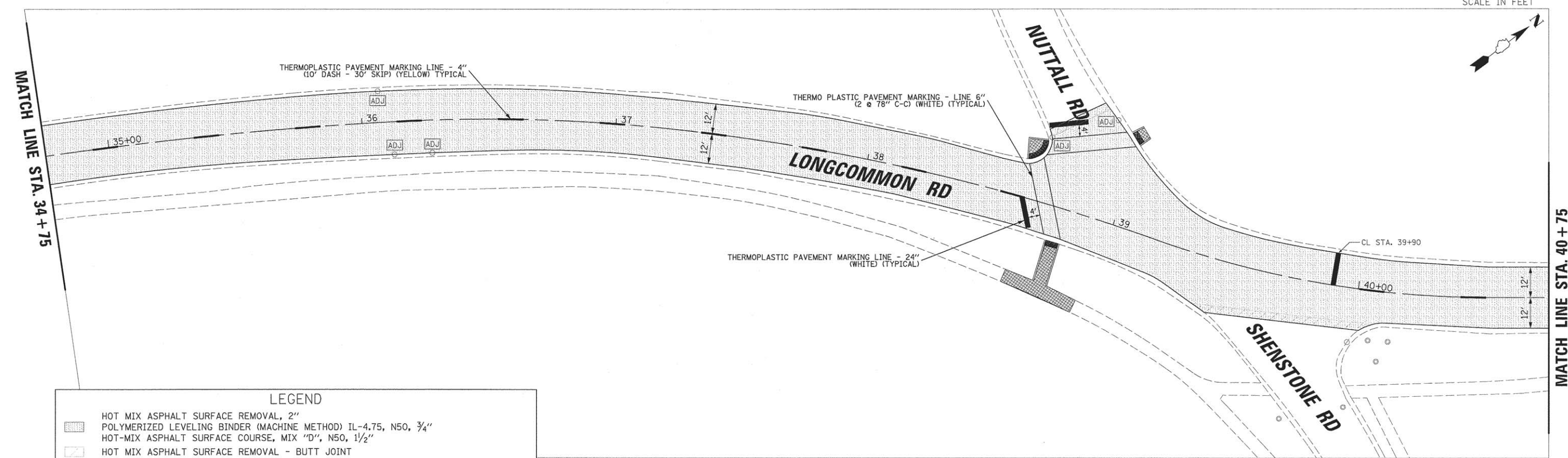
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
LONGCOMMON ROAD AND QUINCY STREET**

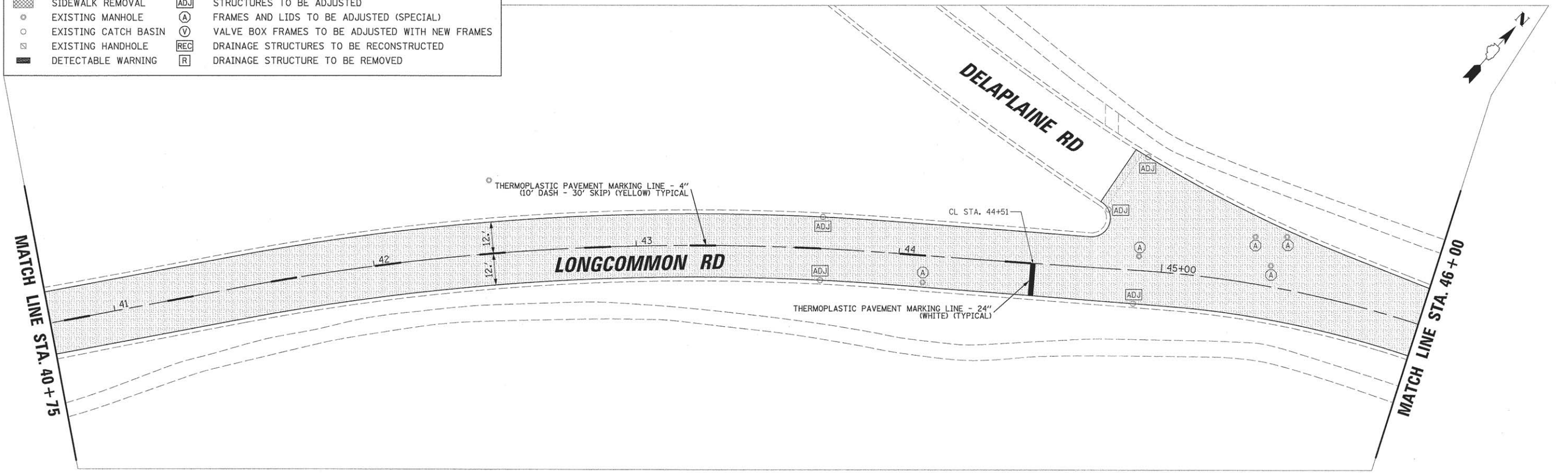
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3569	16-00083-00-RS	COOK	31	10
CONTRACT NO. 61C96			ILLINOIS FED. AID PROJECT	



**LEGEND**

	HOT MIX ASPHALT SURFACE REMOVAL, 2"		
	POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, 3/4"		
	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"		
	HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT		
	EXPOSED AGGREGATE SURFACE		
	PAVEMENT MARKING (SPECIAL)		
	SIDEWALK REMOVAL		STRUCTURES TO BE ADJUSTED
	EXISTING MANHOLE		FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
	EXISTING CATCH BASIN		VALVE BOX FRAMES TO BE ADJUSTED WITH NEW FRAMES
	EXISTING HANDHOLE		DRAINAGE STRUCTURES TO BE RECONSTRUCTED
	DETECTABLE WARNING		DRAINAGE STRUCTURE TO BE REMOVED



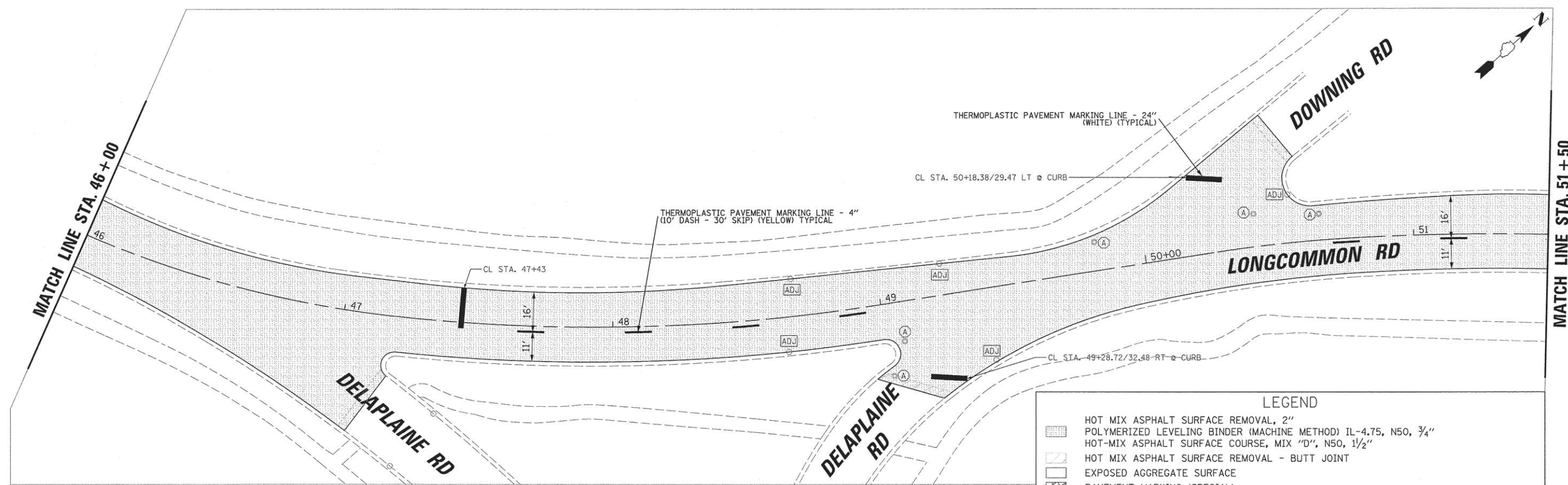
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
LONGCOMMON ROAD AND QUINCY STREET**

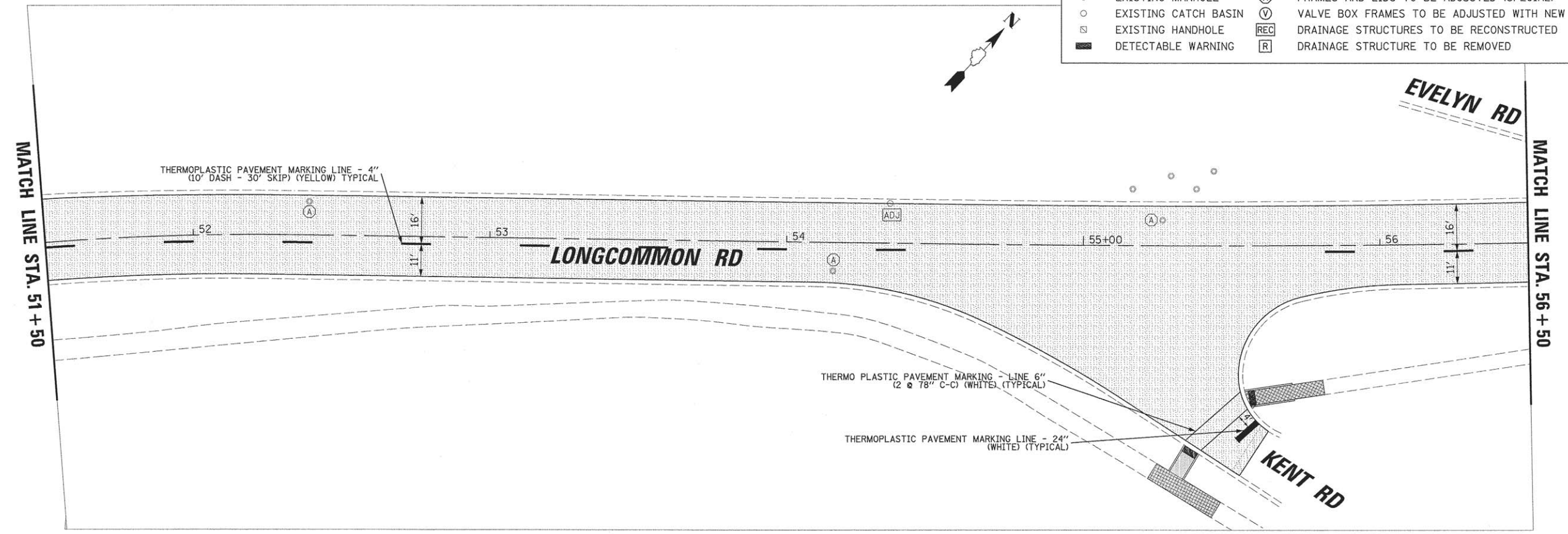
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3569	16-00083-00-RS	COOK	31	11
CONTRACT NO. 61C96			ILLINOIS FED. AID PROJECT	

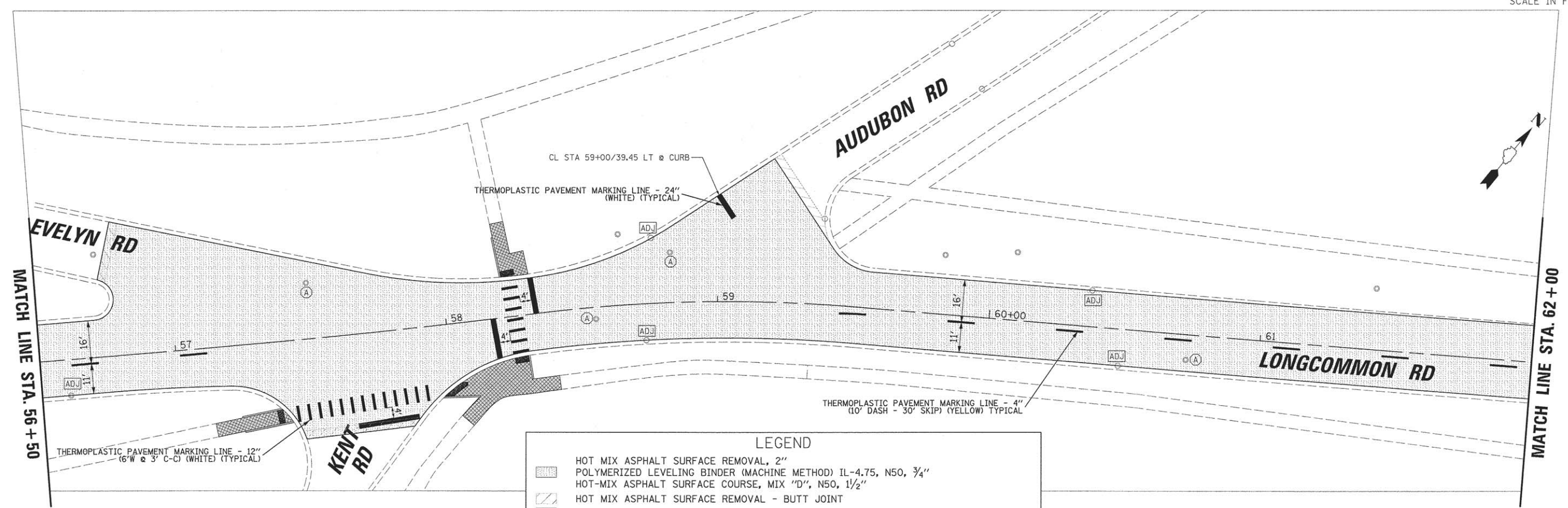


**LEGEND**

	HOT MIX ASPHALT SURFACE REMOVAL, 2"		STRUCTURES TO BE ADJUSTED
	POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, 3/4"		FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"		VALVE BOX FRAMES TO BE ADJUSTED WITH NEW FRAMES
	HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT		DRAINAGE STRUCTURES TO BE RECONSTRUCTED
	EXPOSED AGGREGATE SURFACE		DRAINAGE STRUCTURE TO BE REMOVED
	PAVEMENT MARKING (SPECIAL)		
	SIDEWALK REMOVAL		
	EXISTING MANHOLE		
	EXISTING CATCH BASIN		
	EXISTING HANDHOLE		
	DETECTABLE WARNING		

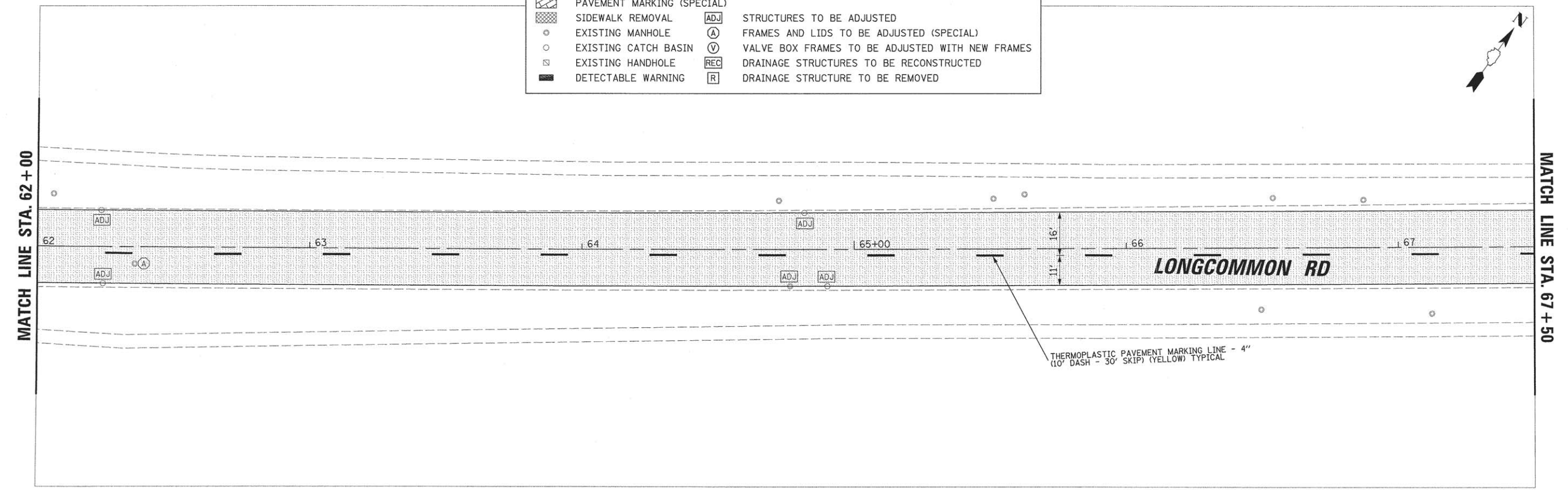


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Default	PLOT SCALE = 20'	CHECKED - OCG	REVISED -		ILLINOIS FED. AID PROJECT					
	PLOT DATE = 5/5/2016	DATE -	REVISED -							



**LEGEND**

	HOT MIX ASPHALT SURFACE REMOVAL, 2"		STRUCTURES TO BE ADJUSTED
	POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, 3/4"		FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"		VALVE BOX FRAMES TO BE ADJUSTED WITH NEW FRAMES
	HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT		DRAINAGE STRUCTURES TO BE RECONSTRUCTED
	EXPOSED AGGREGATE SURFACE		DRAINAGE STRUCTURE TO BE REMOVED
	PAVEMENT MARKING (SPECIAL)		
	SIDEWALK REMOVAL		
	EXISTING MANHOLE		
	EXISTING CATCH BASIN		
	EXISTING HANDHOLE		
	DETECTABLE WARNING		



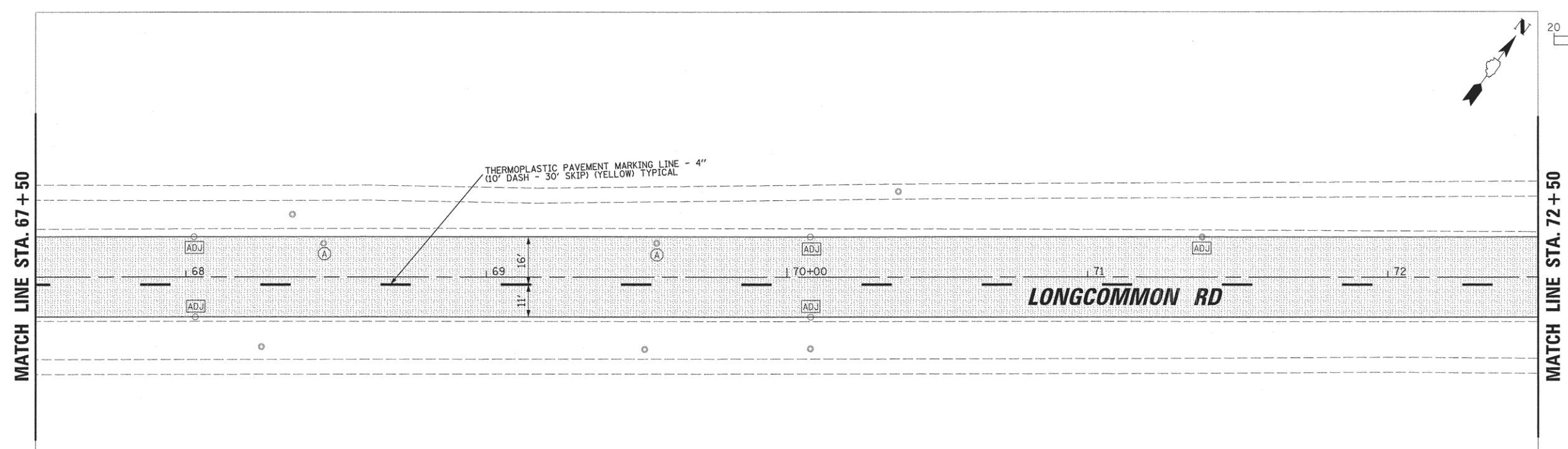
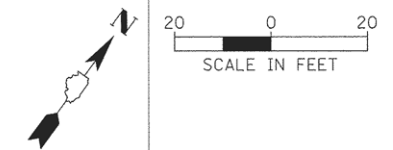
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
LONGCOMMON ROAD AND QUINCY STREET**

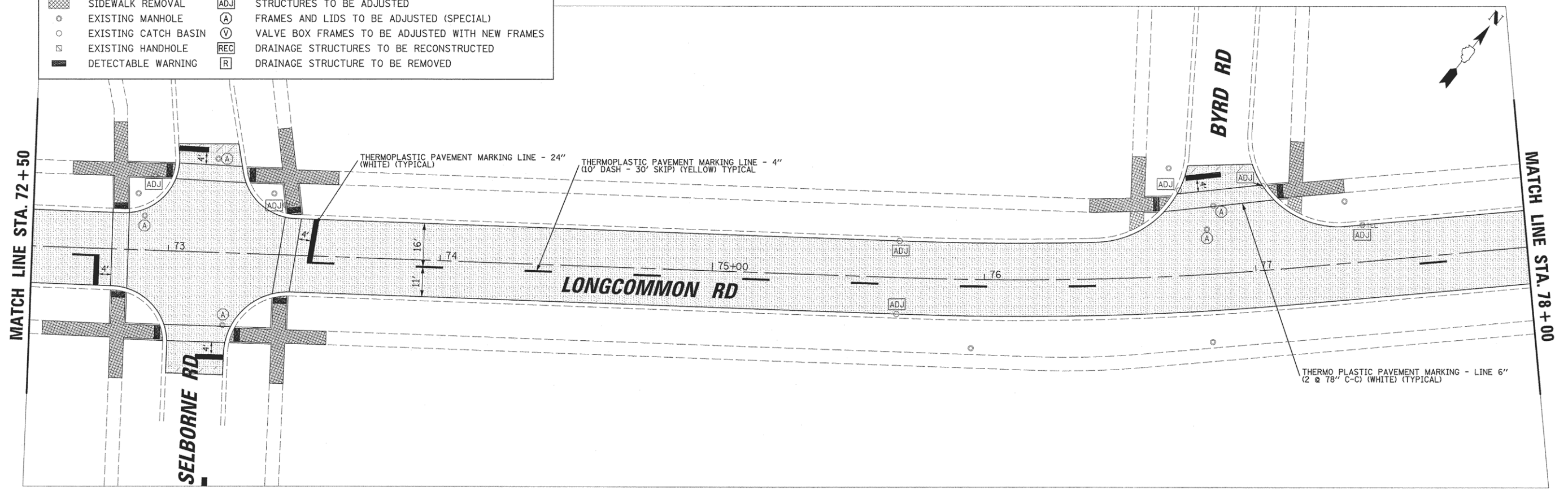
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3569	16-00083-00-RS	COOK	31	13
CONTRACT NO. 61C96				
ILLINOIS FED. AID PROJECT				



**LEGEND**

	HOT MIX ASPHALT SURFACE REMOVAL, 2"		STRUCTURES TO BE ADJUSTED
	POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, 3/4"		FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"		VALVE BOX FRAMES TO BE ADJUSTED WITH NEW FRAMES
	HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT		DRAINAGE STRUCTURES TO BE RECONSTRUCTED
	EXPOSED AGGREGATE SURFACE		DRAINAGE STRUCTURE TO BE REMOVED
	PAVEMENT MARKING (SPECIAL)		
	SIDEWALK REMOVAL		
	EXISTING MANHOLE		
	EXISTING CATCH BASIN		
	EXISTING HANDHOLE		
	DETECTABLE WARNING		



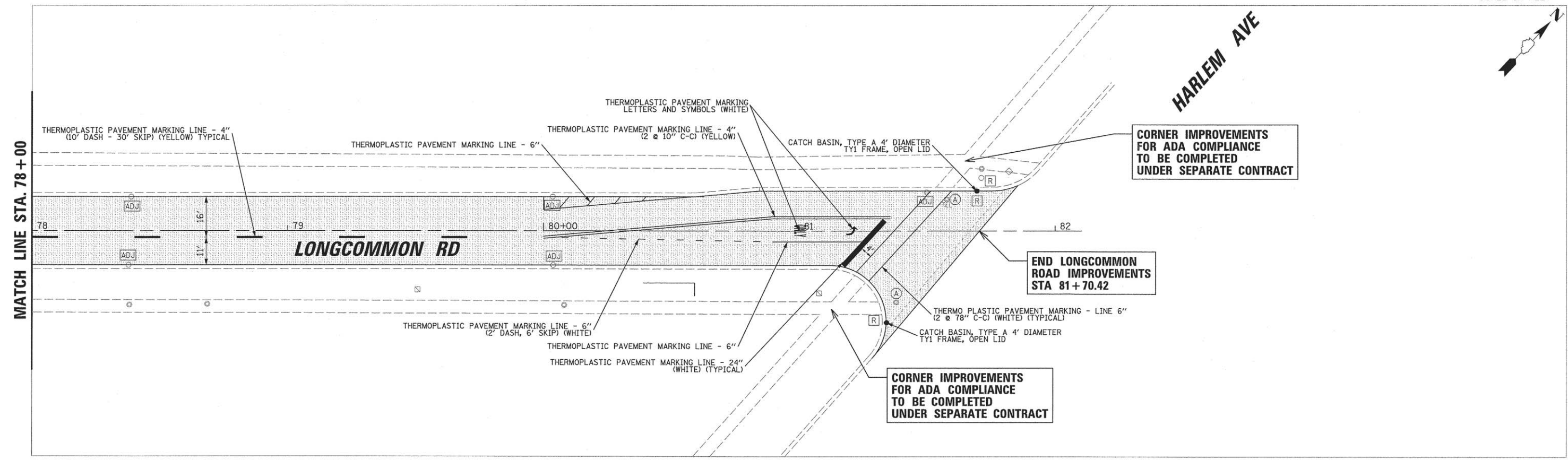
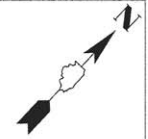
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	PLOT DATE = 5/5/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
LONGCOMMON ROAD AND QUINCY STREET**

SCALE: 20'      SHEET 10 OF 11 SHEETS      STA. 67+50 TO STA. 78+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3569	16-00083-00-RS	COOK	31	14
CONTRACT NO. 61C96			ILLINOIS FED. AID PROJECT	



**LEGEND**

	HOT MIX ASPHALT SURFACE REMOVAL, 2"
	POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, 3/4"
	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
	HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
	EXPOSED AGGREGATE SURFACE
	PAVEMENT MARKING (SPECIAL)
	SIDEWALK REMOVAL
	EXISTING MANHOLE
	EXISTING CATCH BASIN
	EXISTING HANDHOLE
	DETECTABLE WARNING
	STRUCTURES TO BE ADJUSTED
	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
	VALVE BOX FRAMES TO BE ADJUSTED WITH NEW FRAMES
	DRAINAGE STRUCTURES TO BE RECONSTRUCTED
	DRAINAGE STRUCTURE TO BE REMOVED

FILE NAME =	USER NAME = mgoldenberg	DESIGNED - OCG	REVISED -
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	PLOT DATE = 5/9/2016	DATE -	REVISED -

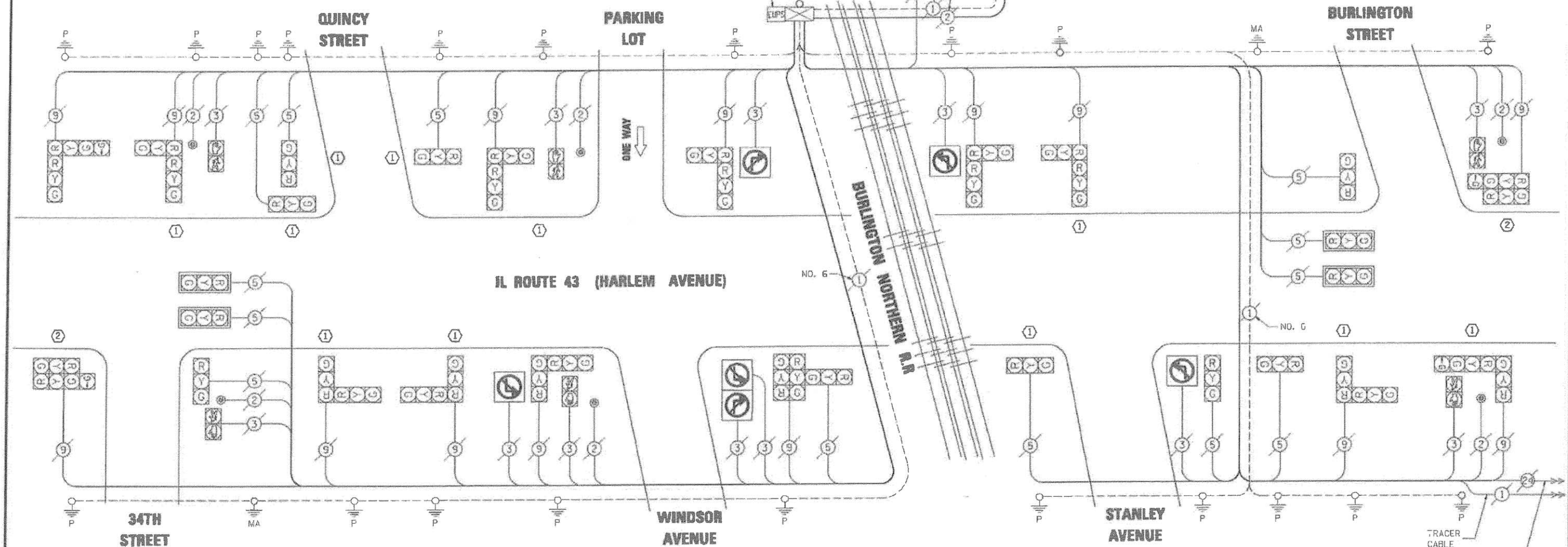
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
LONGCOMMON ROAD AND QUINCY STREET**

SCALE: 20' SHEET 11 OF 11 SHEETS STA. 78+00 TO STA. 81+12

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3569	16-00083-00-RS	COOK	31	15
				CONTRACT NO. 61C96
ILLINOIS FED. AID PROJECT				

SEE NEXT SHEET FOR CONTROLLER SEQUENCE



- ① REPLACE 14 FT. PAINTED POST, RE-USE EXISTING TYPE A CONCRETE FOUNDATION
- ② REPLACE 15 FT. PAINTED POST, RE-USE EXISTING TYPE A CONCRETE FOUNDATION (SEE GENERAL NOTES)

**CABLE PLAN**



**RECORD DRAWINGS**  
11/16/2015

Date (MM/DD/YYYY)  
Signed *Lenny Venezia*  
Print(First, Last) *Lenny Venezia*

**REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT**

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

13 EACH TRAFFIC SIGNAL POST

**PLANS FOR INFORMATION ONLY**

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE (INCAND.)	LED	OPERATION	
SIGNAL (RED)	41	135	17	0.50	348.5
(YELLOW)	41	135	25	0.25	258.75
(GREEN)	41	135	15	0.25	153.75
ARROW	4	135	12	0.10	4.8
PED. SIGNAL	6	30	25	1.00	180
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN		84	100	0.50	
FLASHER		135	25	0.50	
ENERGY COSTS TO: ILLINOIS DEPARTMENT OF TRANSPORTATION 201 WEST CENTER COURT SCHAUMBURG, IL 60196-1096					TOTAL = 1013.3
ENERGY SUPPLY CONTACT: X					
PHONE: X					
COMPANY: X					

**SCHEDULE OF QUANTITIES**

ITEM	QUANTITY	UNIT
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION		
TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	1	EACH
TRAFFIC SIGNAL POST, GALVANIZED STEEL 15 FT.	1	EACH
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT		

*Pedestrian Push Button*

ITEM	QUANTITY	UNIT
TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	1	EACH
TRAFFIC SIGNAL POST, GALVANIZED STEEL 15 FT.	1	EACH
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT		

FILE NAME = N:\RIVERSIDE\160012\Civil\traffic signalization_160012.sht	USER NAME = JGC	DESIGNED - BPT	REVISED -
83-7947.LDGN	DESIGNED - BPT	DRAWN - JGC/RDS	REVISED -
	PLOT SCALE = NOT TO SCALE	CHECKED - BPT	REVISED -
	PLOT DATE = X	DATE - 12-04-13	REVISED -

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF QUANTITIES AND CABLE PLAN**  
**IL ROUTE 43 (HARLEM AVENUE) FROM 34TH STREET TO EAST BURLINGTON ROAD**  
SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE. 3569	SECTION 16-00083-00-RS	COUNTY COOK	TOTAL SHEETS 31	SHEET NO. 16
CONTRACT NO. 61C96			CONTRACT NO. 61C96	

FILE NAME = N:\RIVERSIDE\160012\Civil\traffic signalization_160012.sht	USER NAME = dschroeder	DESIGNED - OCG	REVISED -
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	PLOT DATE = 5/18/2016	CHECKED - OCG	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**QUINCY ST SIGNALIZATION PLAN**  
**LONGCOMMON ROAD AND QUINCY STREET**  
SCALE: 20' SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE. 3569	SECTION 16-00083-00-RS	COUNTY COOK	TOTAL SHEETS 31	SHEET NO. 16
CONTRACT NO. 61C96			CONTRACT NO. 61C96	





RECORD DRAWINGS  
11/16/2015

Date (MM/DD/YYYY)

Signed *Leony Veneziano*

Print(First Last) *Leony Veneziano*

# TRAFFIC SIGNAL SEQUENCE OF OPERATION

F.A.U. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	2001-054 TS	COOK	14	11
TO STA.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

MOVEMENT	1												2		F L A S H	
	1	2A	2B	2C	2D	2E	2F	2G	2H	2J	2K	3	4	5A		5B
PHASE	1												2			
INTERVAL	1												2			
CHANGE TO	2												1			
ILL. RTE. 43 (HARLEM AV.) AT E. BURLINGTON RD. ALL SIGNALS	S/B	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R
ILL. RTE. 43 (HARLEM AV.) AT E. BURLINGTON RD. END MAST ARM AND FAR LEFT SIGNALS	N/B	G	G	G	G	G	G	G	G	G	G	G	G	Y	R	R
ILL. RTE. 43 (HARLEM AV.) AT E. BURLINGTON RD. NEAR AND FAR RIGHT SIGNALS	N/B	G	G	G	G	G	G	G	G	G	G	G	G	Y	R	R
ILL. RTE. 43 (HARLEM AV.) AT STANLEY AVE. ALL SIGNALS	S/B	G	G	G	Y	R	R	R	R	R	R	R	R	R	R	R
ILL. RTE. 43 (HARLEM AV.) AT STANLEY AVE. ALL SIGNALS	N/B	G	G	G	G	G	G	G	G	G	G	G	G	Y	R	R
ILL. RTE. 43 (HARLEM AV.) AT WINDSOR AVE. ALL SIGNALS	S/B	G	G	G	G	G	Y	R	R	R	R	R	R	R	R	R
ILL. RTE. 43 (HARLEM AV.) AT WINDSOR AVE. ALL SIGNALS	N/B	G	G	G	G	G	Y	R	R	R	R	R	R	R	R	R
ILL. RTE. 43 (HARLEM AV.) AT QUINCY STREET ALL SIGNALS	S/B	G	G	G	G	G	G	G	Y	R	R	R	R	R	R	R
ILL. RTE. 43 (HARLEM AV.) AT QUINCY STREET ALL SIGNALS	N/B	G	G	G	Y	R	R	R	R	R	R	R	R	R	R	R
ILL. RTE. 43 (HARLEM AV.) AT 34TH STREET FAR LEFT AND FAR RIGHT SIGNALS	S/B	G	G	G	G	G	G	G	G	G	G	G	G	Y	R	R
ILL. RTE. 43 (HARLEM AV.) AT 34TH STREET NEAR RIGHT SIGNALS	S/B	G	G	G	G	G	G	G	G	G	G	G	G	Y	R	R
ILL. RTE. 43 (HARLEM AV.) AT 34TH STREET ALL SIGNALS	N/B	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R
E. BURLINGTON ROAD ALL SIGNALS	E/B	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y
STANLEY AVE. ALL SIGNALS	W/B	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y
WINDSOR AVE. ALL SIGNALS	E/B	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y
WINDSOR AVE. ALL SIGNALS	W/B	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y
QUINCY STREET ALL SIGNALS	E/B	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y
34TH STREET ALL SIGNALS	W/B	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y
PEDESTRIAN SIGNALS CROSSING ILL. RTE. 43 (HARLEM AV.)		H	H	H	H	H	H	H	H	H	H	H	H	P	PH	H

PHASE 1 SHALL BE PLACED ON RECALL MODIFICATION OF THE CONTROLLER SHALL CONSIST OF ADDITION OF PEDESTRIAN MOVEMENTS EQUIPMENT.

P = ILLUMINATED PERSON WALK  
PH = ILLUMINATED FLASHING HAND - FLASHING DON'T WALK  
H = ILLUMINATED SOLID HAND - DON'T WALK

TO APPEAR ONLY UPON PUSHBUTTON ACTUATION  
FLASHING TO TERMINATE AT THE COMPLETION OF THE PEDESTRIAN INTERVAL CLEARANCE.

THIS OR FLASHING INTERVAL MAY FINISH TIME IN THE BIDIRECTIONAL STRAIGHT THROUGH MOVEMENT IF THE LEFT ARROW TIME IS NOT SUFFICIENT TO COMPLETE OR FLASHING INTERVALS.  
AND FLASHING TIMES TO BE SET ONLY ON PHASES WHERE AND FLASHING ARE INDICATED IN THE SEQUENCE OF OPERATION.

REVISIONS	NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
SEQUENCE OF OPERATION AND RAILROAD PREEMPTION SEQUENCE OF OPERATION  
ILL. ROUTE 43 (HARLEM AV.) AND 34TH ST./QUINCY ST./WINDSOR AVE./STANLEY/EAST BURLINGTON RD.  
SCALE: NONE  
DATE: 12/03/2002  
DRAWN BY: BCK  
DESIGNED BY: BCK  
CHECKED BY: BCK

09:29:42 12/03/2002

SEE NEXT SHEET FOR RAILROAD PREEMPTION SEQUENCE OF OPERATION

EXISTING SEQUENCES - INCLUDED FOR REFERENCE ONLY

PLANS FOR INFORMATION ONLY

TS# 7947 RIVERSIDE

PREPARED BY:  
**CEMCON, L.L.C.**  
Consulting Engineers, Land Surveyors & Planners  
2200 White Oak Circle, Suite 100  
Aurora, Illinois 60504-9025  
PH: 630.556.2100 FAX: 630.562.2189  
E-Mail: cemcon@cemcon.com Website: www.cemcon.com

FILE NAME = M:\PROJECTS\160012\Civil\Traffic\signalization_160012.sht	USER NAME = JGC	DESIGNED - BPT	REVISED -
83-7947_2.DGN		DRAWN - JGC/RDS	REVISED -
	PLOT SCALE = NOT TO SCALE	CHECKED - BPT	REVISED -
	PLOT DATE = X	DATE = 12-04-13	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL SEQUENCE OF OPERATION IL ROUTE 43 (HARLEM AVENUE) FROM 34TH STREET TO EAST BURLINGTON ROAD			
SCALE: N.T.S.	SHEET NO. OF SHEETS	STA. TO STA.	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2013-054TS	COOK	92	88
CONTRACT NO. 60X29				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

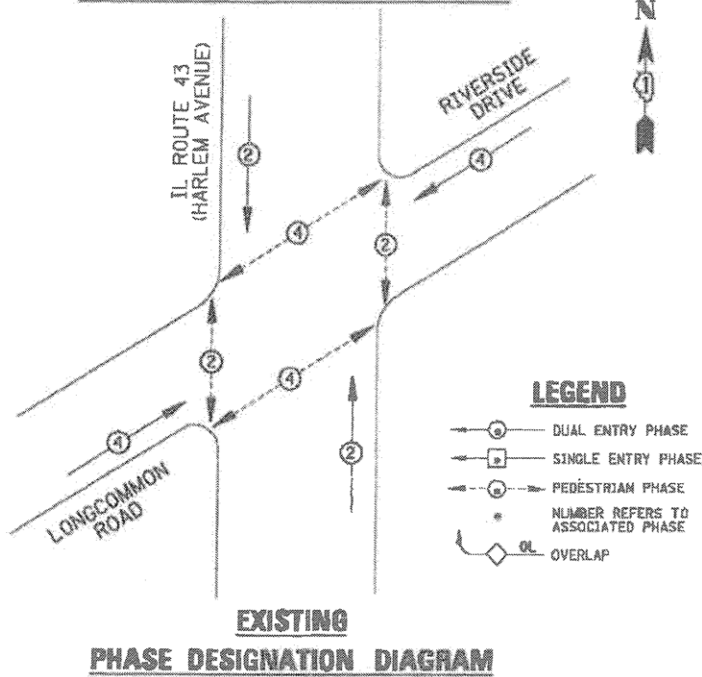
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	PLOT SCALE = 20'	CHECKED - OCG	REVISED -
	PLOT DATE = 5/18/2016	DATE =	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

QUINCY ST SIGNALIZATION PLAN LONGCOMMON ROAD AND QUINCY STREET			
SCALE: 20'	SHEET 2 OF 2 SHEETS	STA. TO STA.	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3569	16-00083-00-RS	COOK	31	17
CONTRACT NO. 61C96				
ILLINOIS FED. AID PROJECT				

**EXISTING CONTROLLER SEQUENCE**



**REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT**

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 5 EACH TRAFFIC SIGNAL POST
- 12 EACH SIGNAL HEAD, 3-SECTION
- 2 EACH TRAFFIC SIGNAL BACKPLATE
- 8 EACH PEDESTRIAN SIGNAL HEAD
- 8 EACH PEDESTRIAN PUSH-BUTTON

- ① REPLACE 14 FT. PAINTED POST, RE-USE EXISTING TYPE A CONCRETE FOUNDATION (SEE GENERAL NOTES)

**SCHEDULE OF QUANTITIES**

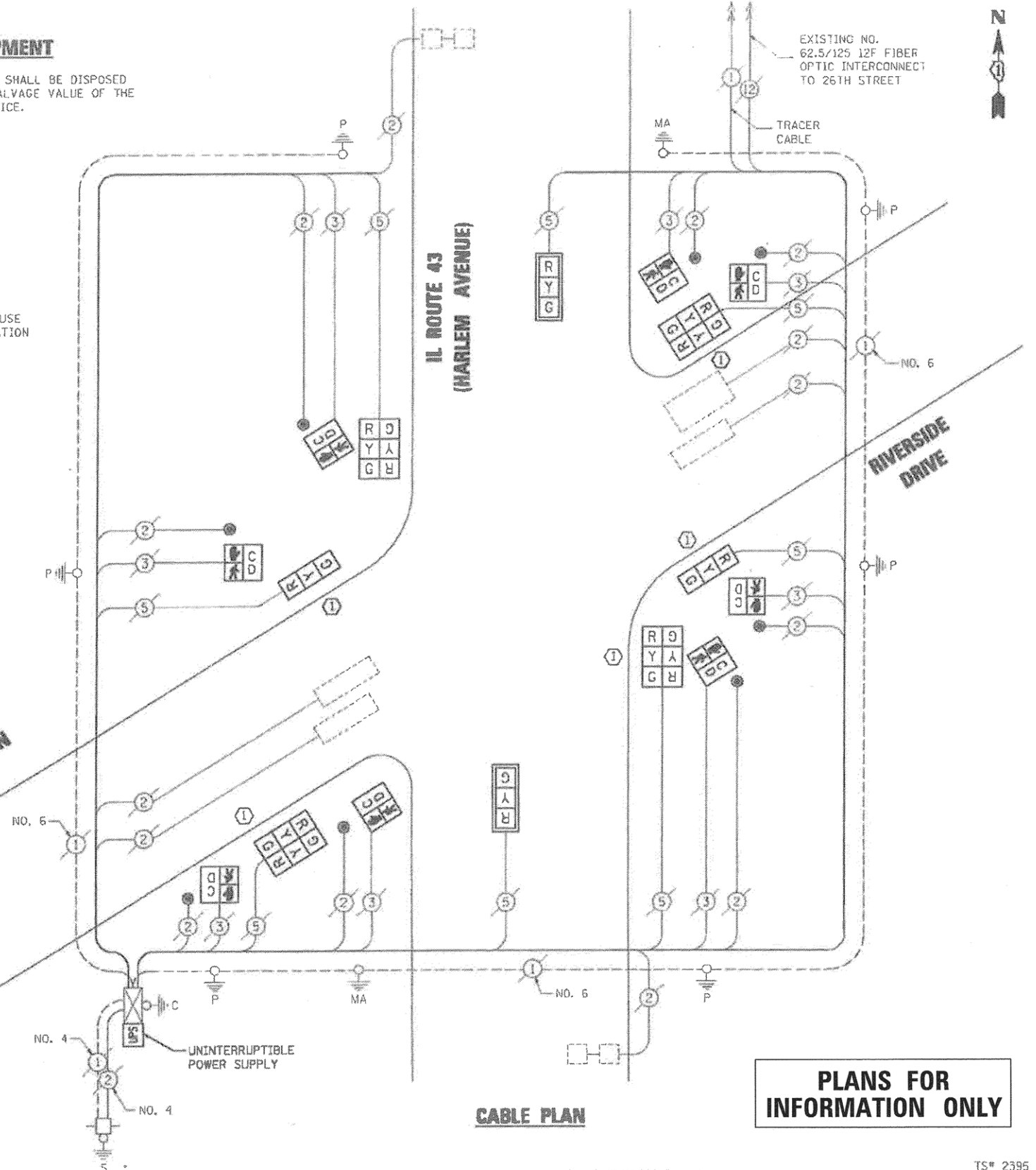
ITEM	UNIT	QUANTITY
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	5
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	2
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED	EACH	4
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8
TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	2
PEDESTRIAN PUSH-BUTTON	EACH	8
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
UNINTERRUPTIBLE POWER SUPPLY, SPECIAL	EACH	1

ITEM	UNIT	QUANTITY
TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	5
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	2
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED	EACH	4
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8
TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	2
PEDESTRIAN PUSH-BUTTON	EACH	8
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
UNINTERRUPTIBLE POWER SUPPLY, SPECIAL	EACH	1

**LONGCOMMON ROAD**

**IL ROUTE 43 (HARLEM AVENUE)**

**RIVERSIDE DRIVE**



**CABLE PLAN**

**PLANS FOR INFORMATION ONLY**

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE (INCAND.)	LED	%OPERATION	
SIGNAL (RED)	12	135	17	0.50	102
(YELLOW)	12	135	25	0.25	75
(GREEN)	12	135	15	0.25	45
ARROW		135	12	0.10	
PED. SIGNAL	8	90	25	1.00	200
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN		84	100	0.50	
FLASHER		135	25	0.50	
ENERGY COSTS TO: ILLINOIS DEPARTMENT OF TRANSPORTATION 201 WEST CENTER COURT SCHAUMBURG, IL 60196-1096					TOTAL = 522

**RECORD DRAWINGS**

Date (MM/DD/YYYY) 11/16/2015  
 Signed *Lenny Venezia*  
 Print(First Last) Lenny Venezia

FILE NAME: \\N\RIVERSIDE\160012\Civil\traffic signalization\160012.dgn	USER NAME: JGC	DESIGNED: BPT	REVISED:
		DRAWN: JGC/RDS	REVISED:
		CHECKED: BPT	REVISED:
		DATE: 12-04-13	REVISED:

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF QUANTITIES, CABLE PLAN AND PHASE DESIGNATION DIAGRAM**  
**IL ROUTE 43 (HARLEM AVENUE) AT RIVERSIDE DR. / LONGCOMMON RD.**

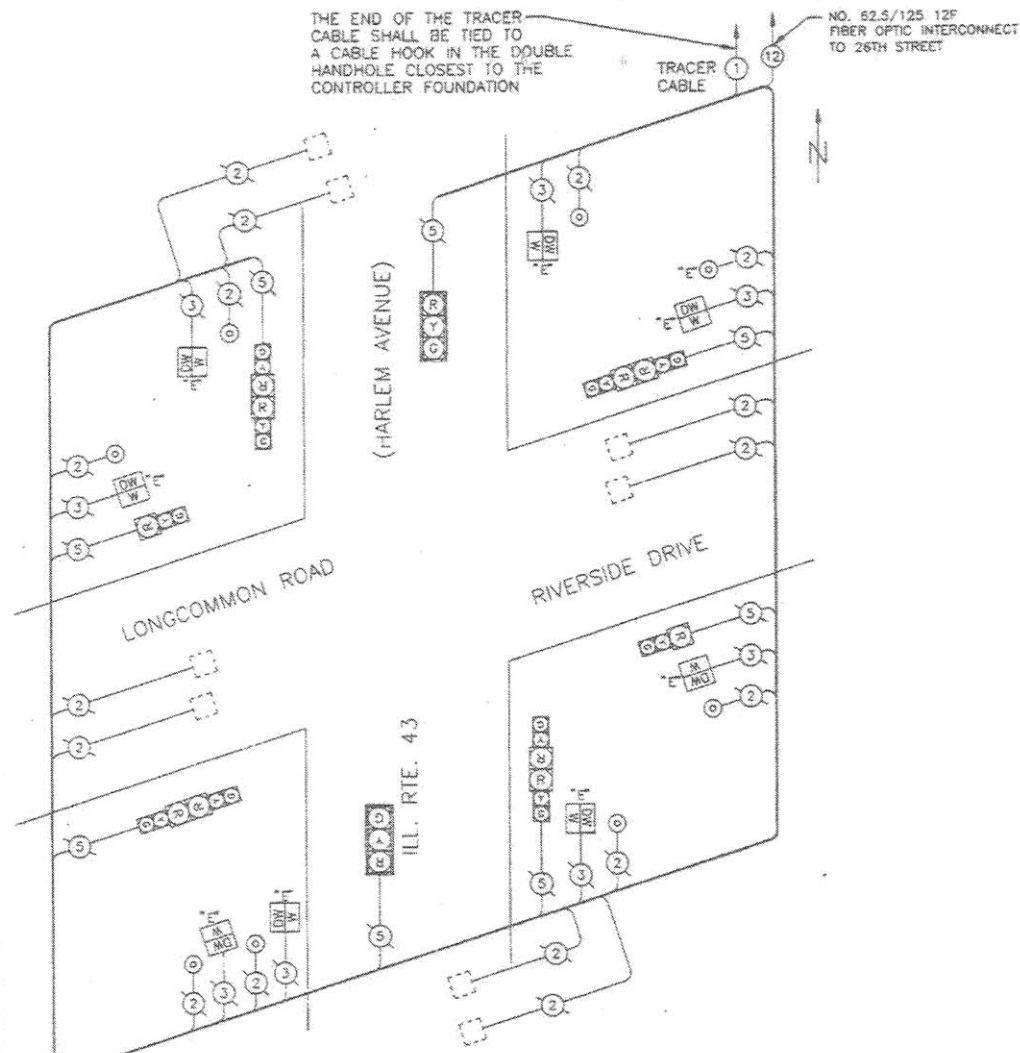
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3569	16-00083-00-RS	COOK	92	18
CONTRACT NO. 61C96				

FILE NAME: Default	USER NAME: dschroeder	DESIGNED: OCG	REVISED:
		DRAWN: RML	REVISED:
		CHECKED: OCG	REVISED:
		DATE: 5/16/2016	REVISED:

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**LONGCOMMON AVE SIGNALIZATION PLAN**  
**LONGCOMMON ROAD AND QUINCY STREET**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3569	16-00083-00-RS	COOK	31	18
CONTRACT NO. 61C96				



**CABLE PLAN LEGEND**

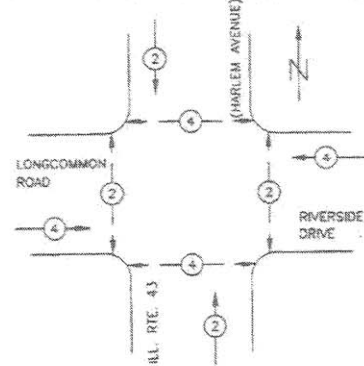
	EXISTING	PROPOSED
8" TRAFFIC SIGNAL SECTION		
12" TRAFFIC SIGNAL SECTION		
12" PEDESTRIAN SIGNAL SECTION		
CONTROLLER CABINET		
SERVICE INSTALLATION		
VEHICLE DETECTOR, INDUCTION LOOP		
CONFIRMATION BEACON		
EMERGENCY VEHICLE LIGHT DETECTOR		
PUSHBUTTON DETECTOR		
DENOTES NUMBER OF CONDUCTORS ALL CABLE NO. 14 EXCEPT AS INDICATED.		
ALL LOOP DETECTOR CABLE TO BE SHIELDED		
SIGNAL FACE WITH BACKPLATE		
"P" INDICATES PROGRAMMED HEAD.		

**SCHEDULE OF QUANTITIES**

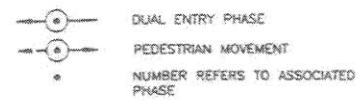
ITEM	UNIT	QUANTITY
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET (SPECIAL)	EACH	1
INDUCTIVE LOOP DETECTOR	EACH	8
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
TRANSCIVER - FIBER OPTIC	EACH	1

**CONTROLLER SEQUENCE**

REFERRING TO STANDARD 239.5, THE VEHICULAR AND PEDESTRIAN PHASES USED ARE DESIGNATED BELOW.



**LEGEND**



**PHASE DESIGNATION DIAGRAM**

DUAL ENTRY - ALL LEGS  
PROTECTED/PERMITTED LEFT TURN PHASING

1. REMOVE THE EXISTING CONTROLLER AND CABINET. INSTALL A FULL-ACTUATED CONTROLLER AND TYPE IV CABINET ON THE EXISTING FOUNDATION. INSTALL A FIBER OPTIC TRANSCIVER IN THE NEW CABINET. INSTALL EIGHT (8) INDUCTIVE LOOP DETECTORS IN THE NEW CABINET.

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE STATE, AND SHALL BE STORED AS STATED IN THE SPECIAL PROVISIONS FOR PICK UP BY STATE FORCES AS DIRECTED BY THE ENGINEER.

- 1 EACH CONTROLLER AND CABINET COMPLETE

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

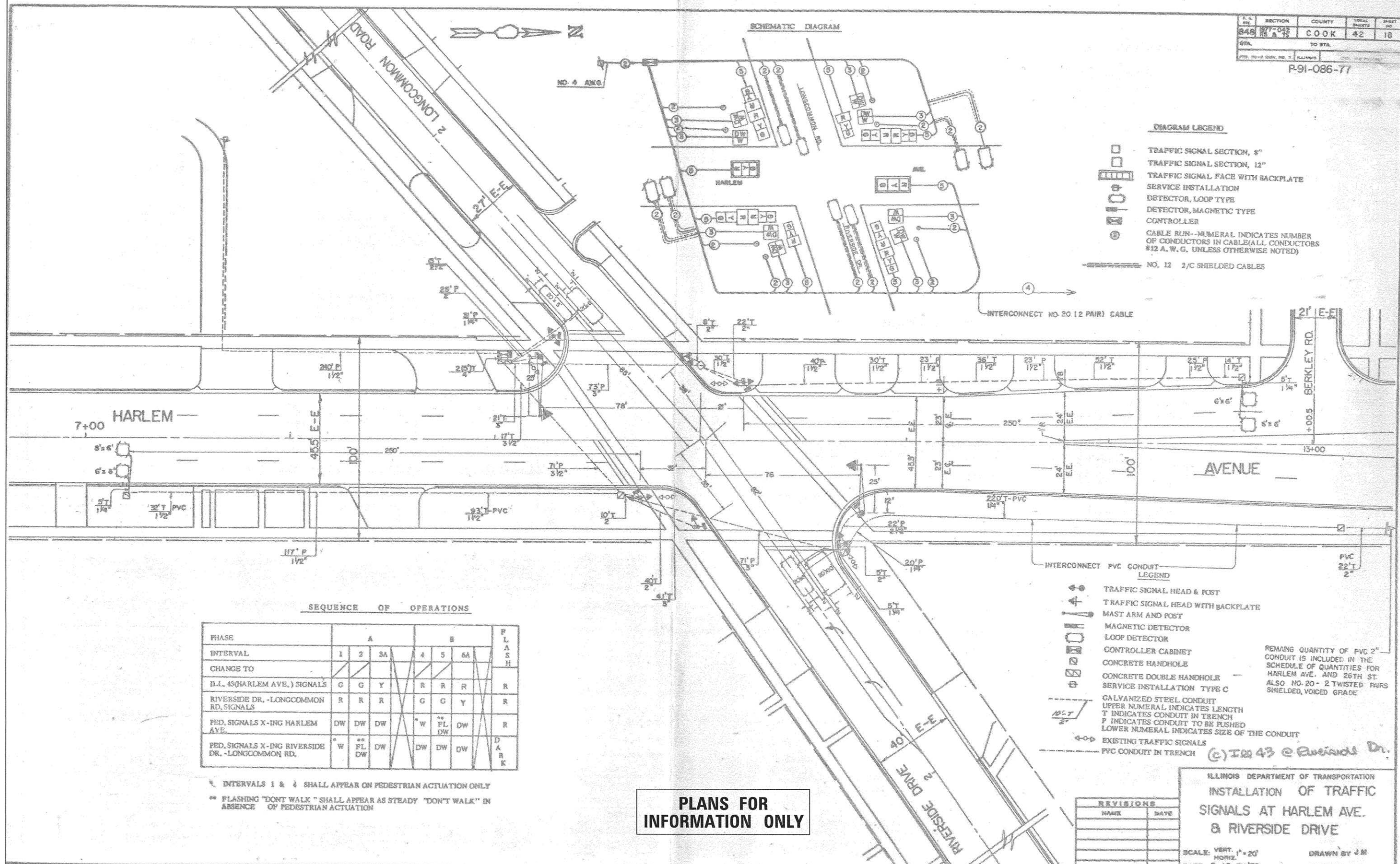
**PLANS FOR INFORMATION ONLY**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
CABLE PLAN,  
PHASE DESIGNATION DIAGRAM AND  
SCHEDULE OF QUANTITIES  
ILL. RTE. 43 (HARLEM AVENUE) AT  
RIVERSIDE DRIVE / LONGCOMMON ROAD  
DRAWN BY : RMP  
DESIGNED BY : JFW  
CHECKED BY : KMM  
NOT TO SCALE  
DATE : 12-15-95

F.A. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
848	16-0083-00-RS	COOK	42	18
STA.	TO STA.			

P-91-086-77



**DIAGRAM LEGEND**

- TRAFFIC SIGNAL SECTION, 8"
- TRAFFIC SIGNAL SECTION, 12"
- TRAFFIC SIGNAL FACE WITH BACKPLATE
- SERVICE INSTALLATION
- DETECTOR, LOOP TYPE
- DETECTOR, MAGNETIC TYPE
- CONTROLLER
- CABLE RUN--NUMERAL INDICATES NUMBER OF CONDUCTORS IN CABLE(ALL CONDUCTORS #12 A. W. G. UNLESS OTHERWISE NOTED)
- NO. 12 1/C SHIELDED CABLES

**INTERCONNECT PVC CONDUIT LEGEND**

- TRAFFIC SIGNAL HEAD & POST
- TRAFFIC SIGNAL HEAD WITH BACKPLATE
- MAST ARM AND POST
- MAGNETIC DETECTOR
- LOOP DETECTOR
- CONTROLLER CABINET
- CONCRETE HANDHOLE
- CONCRETE DOUBLE HANDHOLE
- SERVICE INSTALLATION TYPE C
- GALVANIZED STEEL CONDUIT
- UPPER NUMERAL INDICATES LENGTH
- T INDICATES CONDUIT IN TRENCH
- F INDICATES CONDUIT TO BE PUSHED
- LOWER NUMERAL INDICATES SIZE OF THE CONDUIT
- EXISTING TRAFFIC SIGNALS
- PVC CONDUIT IN TRENCH

REMAINING QUANTITY OF PVC 2" CONDUIT IS INCLUDED IN THE SCHEDULE OF QUANTITIES FOR HARLEM AVE. AND 25TH ST. ALSO NO. 20 - 2 TWISTED PAIRS SHIELDED, VOICED GRADE

**SEQUENCE OF OPERATIONS**

PHASE	A						B			FLASH
	1	2	3A	4	5	6A				
INTERVAL										
CHANGE TO										
ILL. 43(HARLEM AVE.) SIGNALS	G	G	Y	R	R	R				R
RIVERSIDE DR. -LONGCOMMON RD. SIGNALS	R	R	R	G	G	Y				R
PED. SIGNALS X-ING HARLEM AVE.	DW	DW	DW	*	** FL DW	DW				R
PED. SIGNALS X-ING RIVERSIDE DR. -LONGCOMMON RD.	*	** FL DW	DW	DW	DW	DW				DARK

\* INTERVALS 1 & 4 SHALL APPEAR ON PEDESTRIAN ACTUATION ONLY  
 \*\* FLASHING "DON'T WALK" SHALL APPEAR AS STEADY "DON'T WALK" IN ABSENCE OF PEDESTRIAN ACTUATION

**PLANS FOR INFORMATION ONLY**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 INSTALLATION OF TRAFFIC SIGNALS AT HARLEM AVE. & RIVERSIDE DRIVE  
 SCALE: VERT. 1" = 20'  
 HORIZ. DATE 2-18-FY 78  
 DRAWN BY J.M.  
 CHECKED BY

K.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
848	1877-042 RS & TS.	COOK	42	19

**SCHEDULE OF QUANTITIES**

2	EACH	SIGNAL HEAD, ALUMINUM, 1-FACE, 3 SECTION, BRACKET MOUNTED
2	EACH	SIGNAL HEAD, ALUMINUM, 1-FACE, 3 SECTION, MAST ARM MOUNTED
4	EACH	SIGNAL HEAD, ALUMINUM, 2-FACE, 3 SECTION, BRACKET MOUNTED
8	EACH	PEDESTRIAN SIGNAL HEAD, ALUMINUM, 1-FACE, BRACKET MOUNTED
2	EACH	TRAFFIC SIGNAL BACKPLATE
6	EACH	TRAFFIC SIGNAL POST, FERROUS 14 FT.
2	EACH	ALUMINUM MAST ARM ASSEMBLY AND POLE 25 FT. (19'-9" POLE)
8	EACH	INDUCTION LOOP DETECTOR AMPLIFIER, DIGITAL DESIGN
459	LN. FT.	DETECTOR LOOP
8	EACH	PEDESTRIAN PUSH-BUTTON
15	LN. FT.	GALVANIZED STEEL CONDUIT IN TRENCH 1-1/4"
110	LN. FT.	GALVANIZED STEEL CONDUIT IN TRENCH 2"
15	LN. FT.	GALVANIZED STEEL CONDUIT IN TRENCH 2-1/2"
62	LN. FT.	GALVANIZED STEEL CONDUIT IN TRENCH 3"
17	LN. FT.	GALVANIZED STEEL CONDUIT IN TRENCH 3-1/2"
10	LN. FT.	GALVANIZED STEEL CONDUIT IN TRENCH 4"
51	LN. FT.	GALVANIZED STEEL CONDUIT PUSHED 1-1/4"
468	LN. FT.	GALVANIZED STEEL CONDUIT PUSHED 1-1/2"
25	LN. FT.	GALVANIZED STEEL CONDUIT PUSHED 2"
220	LN. FT.	PVC CONDUIT IN TRENCH 1-1/4"
144	LN. FT.	GALVANIZED STEEL CONDUIT PUSHED 3"
71	LN. FT.	GALVANIZED STEEL CONDUIT PUSHED 3-1/2"
241	LN. FT.	ELECTRIC CABLE IN CONDUIT NO. 4 2/C
119	LN. FT.	ELECTRIC CABLE IN CONDUIT NO. 12 2/C
119	LN. FT.	ELECTRIC CABLE IN CONDUIT NO. 12 3/C
1169	LN. FT.	ELECTRIC CABLE IN CONDUIT NO. 12 3/C
1870	LN. FT.	ELECTRIC CABLE IN CONDUIT NO. 12 2 /C, SHIELDED
1	EACH	FULL ACTUATED CONTROLLER, 2 PHASES VOLUME DENSITY FOR ONE PHASE LOCAL COORDINATION, DIGITAL TIMING, IN TYPE IV CABINET SERVICE INSTALLATION TYPE C
1	EACH	CONCRETE FOUNDATION, TYPE A
18	LN. FT.	CONCRETE FOUNDATION, TYPE D
3.5	LN. FT.	CONCRETE FOUNDATION, TYPE E 24-INCH DIAMETER
20	LN. FT.	CONCRETE FOUNDATION, TYPE E 24-INCH DIAMETER
6	EACH	CONCRETE HANDHOLE
1	EACH	CONCRETE DOUBLE HANDHOLE
662	LN. FT.	TRENCH AND BACKFILL
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
4	EACH	ABANDON EXISTING HANDHOLE
5	EACH	CONCRETE FOUNDATION REMOVAL
492	LN. FT.	ELECTRIC CABLE IN CONDUIT NO. 20 A.W.G 2 TWISTED PAIRS, SHIELDED, VOICE GRADE
22	LN. FT.	P.V.C. CONDUIT IN TRENCH 2"
16	WEEK	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
125	LN. FT.	PVC CONDUIT IN TRENCH 1/2"
162	LN. FT.	GALVANIZED STEEL CONDUIT IN TRENCH 1/2"
22	LN. FT.	GALVANIZED STEEL CONDUIT, PUSHED 2-1/2"

**PROPOSED SCHEDULE OF TRAFFIC SIGNAL HEADS**

2	EACH	SIGNAL HEAD, ALUMINUM, 1-FACE, 3 SECTION WITH 12" RED LENS, BRACKET MOUNTED
2	EACH	SIGNAL HEAD, ALUMINUM, 1-FACE, 3 SECTION WITH 12" LENSES MAST ARM MOUNTED
4	EACH	SIGNAL HEAD, ALUMINUM, 2-FACE, 3 SECTION WITH 12" RED LENSES, BRACKET MOUNTED
8	EACH	PEDESTRIAN SIGNAL HEAD, ALUMINUM, 1-FACE, 2 SECTION WITH 12" LENSES, BRACKET MOUNTED

THE " REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT" INCLUDES THE REMOVAL OF THE FOLLOWING:

1	EACH	TRAFFIC SIGNAL CONTROLLER 2 PHASES, VOLUME DENSITY MODEL 1022; SERIAL P6938
2	EACH	SIGNAL RELAY, SR-4
2	EACH	MAGNETIC DETECTOR RELAY MR-9 SERIAL 82414 MR-9 SERIAL: 290659
1	EACH	CONTROLLER CABINET WITH PEDESTAL, TYPE II
4	EACH	MAGNETIC DETECTOR

THE ABOVE LISTED EQUIPMENT SHALL REMAIN IN OPERATION DURING THE CONSTRUCTION PERIOD AND WHEN NO LONGER NEEDED IT SHALL BE REMOVED BY THE CONTRACTOR AND STORED IN A LOCATION DESIGNATED BY THE ENGINEER FOR A LATER PICK UP BY THE CITY OF BERWYN AND THE VILLAGE OF RIVERSIDE.

4	EACH	SIGNAL HEAD, TRAFFIC CONTROL 2-FACE, 3 SECTION POST-TOP MOUNTED
4	EACH	TRAFFIC CONTROL POST 5 FT.

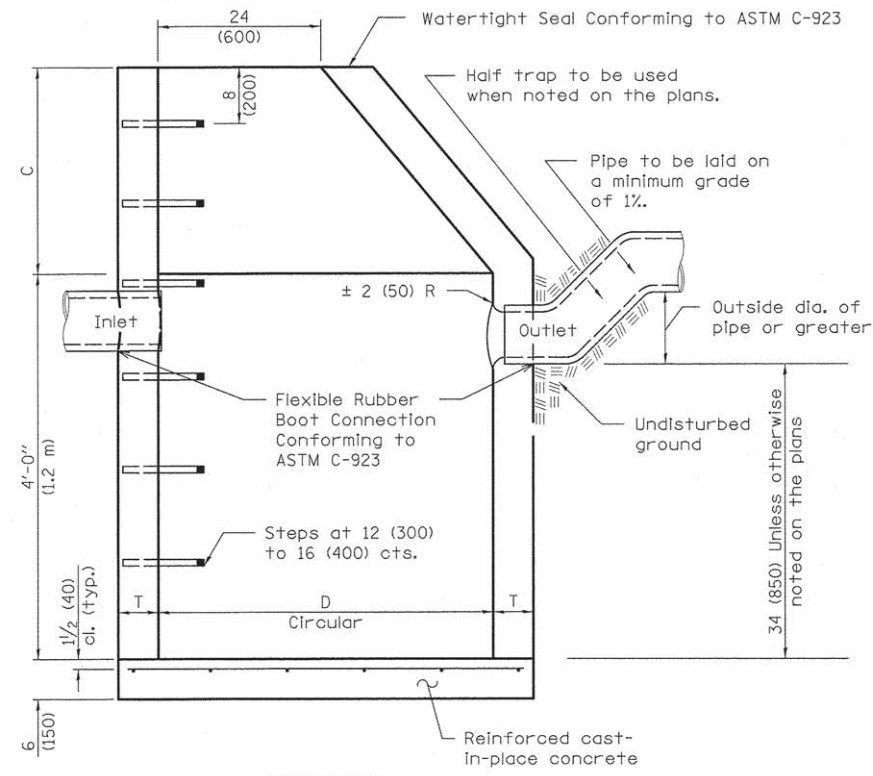
ADD THESE TWO ITEMS TO THE ABOVE LIST

**GENERAL NOTES**

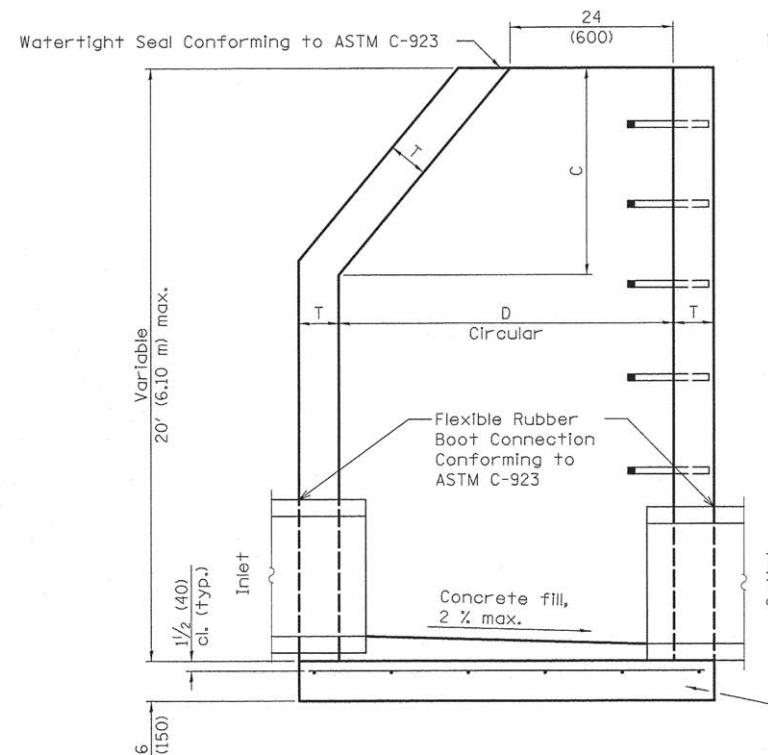
- ALL DETECTOR LOOPS SHALL CONSIST OF THE NUMBER OF TURNS AS RECOMMENDED BY THE MANUFACTURER. DETECTOR LOOPS AND LEAD-IN WIRING SHALL BE INSTALLED IN STRICT CONFORMITY WITH THE MANUFACTURERS RECOMMENDATIONS.
- THE 1/2" NO. 12 SHIELDED CABLE TO BE USED FOR THE DETECTOR LOOP LEAD-IN SHALL BE MEASURED FROM THE SPLICE TO THE CONTROLLER AS SPECIFIED IN SECTION T421.04 OF THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS EXCEPT NO SLACK SHALL BE ALLOWED. THE DETECTOR LOOP SHALL BE MEASURED FOR THAT PORTION OF SAW CUT BEYOND THE SPLICE AS SPECIFIED IN SECTION T418.04 OF THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS. FLAT CABLE WILL NOT BE PERMITTED.
- ELECTRIC CABLE THAT IS FURNISHED BY THE CONTRACTOR SHALL BE PROTECTED BY POLYETHYLENE INSULATION WITH A POLYVINYLCHLORIDE JACKET, UNLESS OTHERWISE SPECIFIED.
- ALL 12" AMBER SIGNAL SECTIONS SHALL BE FURNISHED WITH A PHOTO-ELECTRIC CONTROLLER DIMMER UNIT AS INCIDENTAL TO THE SIGNAL SECTION.
- THE REMOVAL AND REPLACEMENT OF SIDEWALK, DRIVEWAY, MEDIAN AND ISLAND SURFACE PAVING AT HANDHOLES, JACKING FITS, INSPECTION OPENINGS AND CONCRETE JUNCTION BOXES SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT WILL BE MADE. REPLACEMENT SHALL BE MADE WITH A LIKE MATERIAL OF LIKE THICKNESS TO THE EXISTING SURFACE.
- THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE THE INSTALLATION OF ANY COMPONENTS OF THE TRAFFIC SIGNAL SYSTEM.
- ALL SIGNAL POST AND MAST ARM POLES SHALL BE LOCATED WITH THEIR CENTERLINES A MINIMUM OF FOUR (4) AND SIX (6) FEET RESPECTIVELY FROM THE BACK OF CURB UNLESS NOTED OR DIMENSIONED TO THE CONTRARY ON THE DRAWINGS.
- THE BOTTOM OF THE BRACKET MOUNTED SIGNAL HEAD SHALL BE AT A MINIMUM HEIGHT OF 10 FEET FROM THE TOP OF THE PAVEMENT CROWN.
- ALL LEAD WIRE FROM THE LOOP DETECTOR TO THE FIRST HANDHOLE SHALL BE INCIDENTAL TO THE COST OF THE "DETECTOR LOOP".
- A SEPARATE TEMPORARY TRAFFIC SIGNAL SYSTEM WILL NOT BE INSTALLED. THE EXISTING SIGNAL SYSTEM WILL REMAIN IN OPERATION DURING THE CONSTRUCTION PERIOD.
- ABANDON ALL EXISTING CONDUIT AND CABLE UNLESS OTHERWISE SPECIFIED.
- CENTER ALL 6'x6' LOOPS IN THRU LANES

**PLANS FOR INFORMATION ONLY**

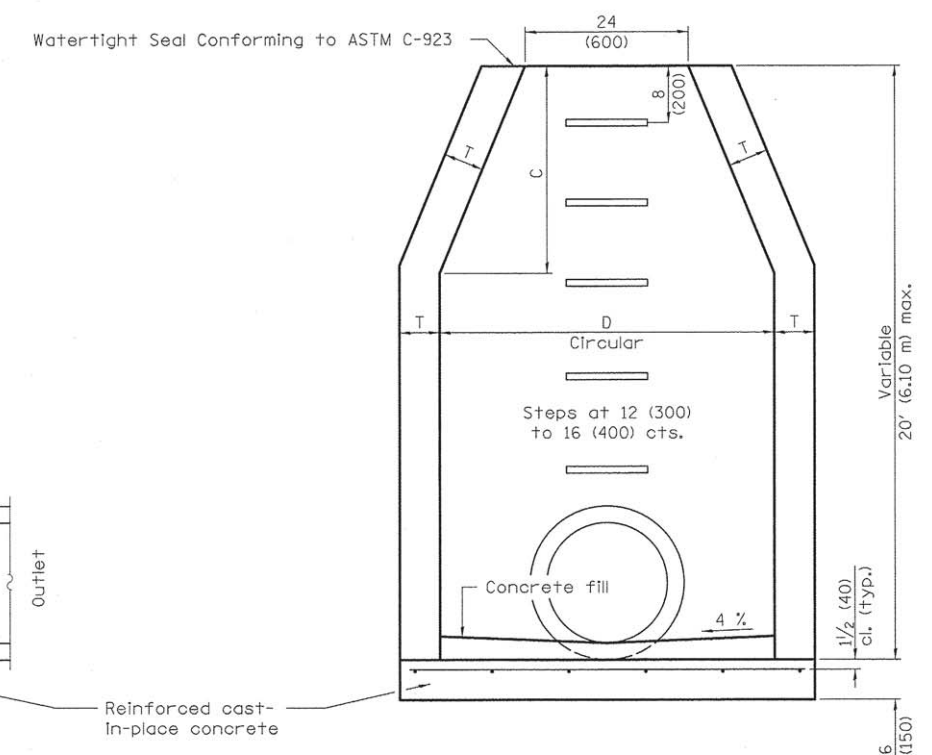
ILLINOIS DEPARTMENT OF TRANSPORTATION				
DISTRICT 1				
SCHEDULE OF QUANTITIES, AND				
GENERAL NOTES FOR HARLEM				
AVE. AND RIVERSIDE DR.--LONGCOMMON				
SCALE NONE DRAWN BY:				
DATE: FEB. 20 FY'78 CHECKED BY: JAW				



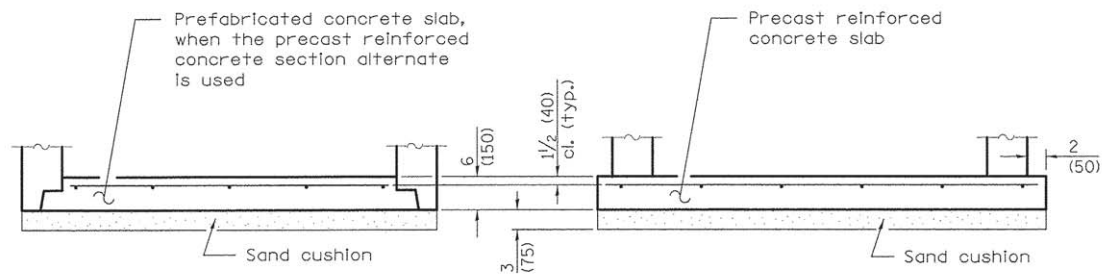
**ELEVATION**  
(Standard Outlet)



**ELEVATION - ECCENTRIC**



**ELEVATION - CONCENTRIC**



**ALTERNATE BOTTOM SLAB**

**GENERAL NOTES**

Bottom slabs shall be reinforced with a minimum of 0.20 sq. in./ft. (420 sq. mm/m) in both directions with a maximum spacing of 12 (300).

Bottom slabs may be connected to the riser as determined by the fabricator; however, only a single row of reinforcement around the perimeter may be utilized.

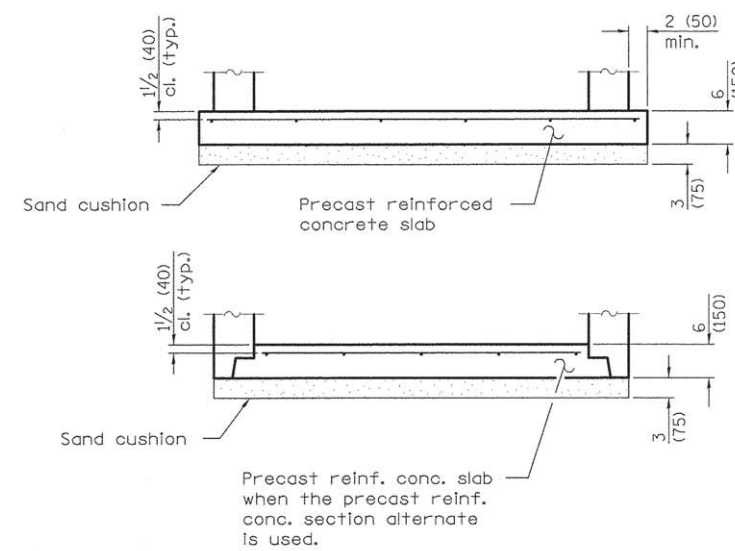
See Standard 602601 for optional precast reinforced concrete flat slab top.

See Standard 602701 for details of steps.

All dimensions are in inches (millimeters) unless otherwise shown.

ALTERNATE MATERIALS FOR WALLS	D	C*	T (min.)
Precast Reinforced Concrete Section	4'-0" (1.2 m) 5'-0" (1.5 m)	30 (750) 3'-9" (1.15 m)	4 (100) 5 (125)
Cast-in-place Concrete	4'-0" (1.2 m) 5'-0" (1.5 m)	30 (750) 3'-9" (1.15 m)	6 (150) 6 (150)

\* For precast reinforced concrete sections, dimension "C" may vary from the dimension given to plus 6 (150).



**ALTERNATE BOTTOM SLAB**

ALTERNATE MATERIALS FOR WALLS	D	C*	T (min.)
Precast Reinforced Concrete Section	4'-0" (1.2 m) 5'-0" (1.5 m)	30 (750) 3'-9" (1.15 m)	4 (100) 5 (125)
Cast-in-place Concrete	4'-0" (1.2 m) 5'-0" (1.5 m)	30 (750) 3'-9" (1.15 m)	6 (150) 6 (150)

\* For precast reinforced concrete sections, dimension "C" may vary from the dimension given to plus 6 (150).

**GENERAL NOTES**

Bottom slabs shall be reinforced with a minimum of 0.31 sq. in./ft. (660 sq. mm/m) in both directions with a maximum spacing of 12 (300).

Bottom slabs may be connected to the riser as determined by the fabricator; however, only a single row of reinforcement around the perimeter may be utilized.

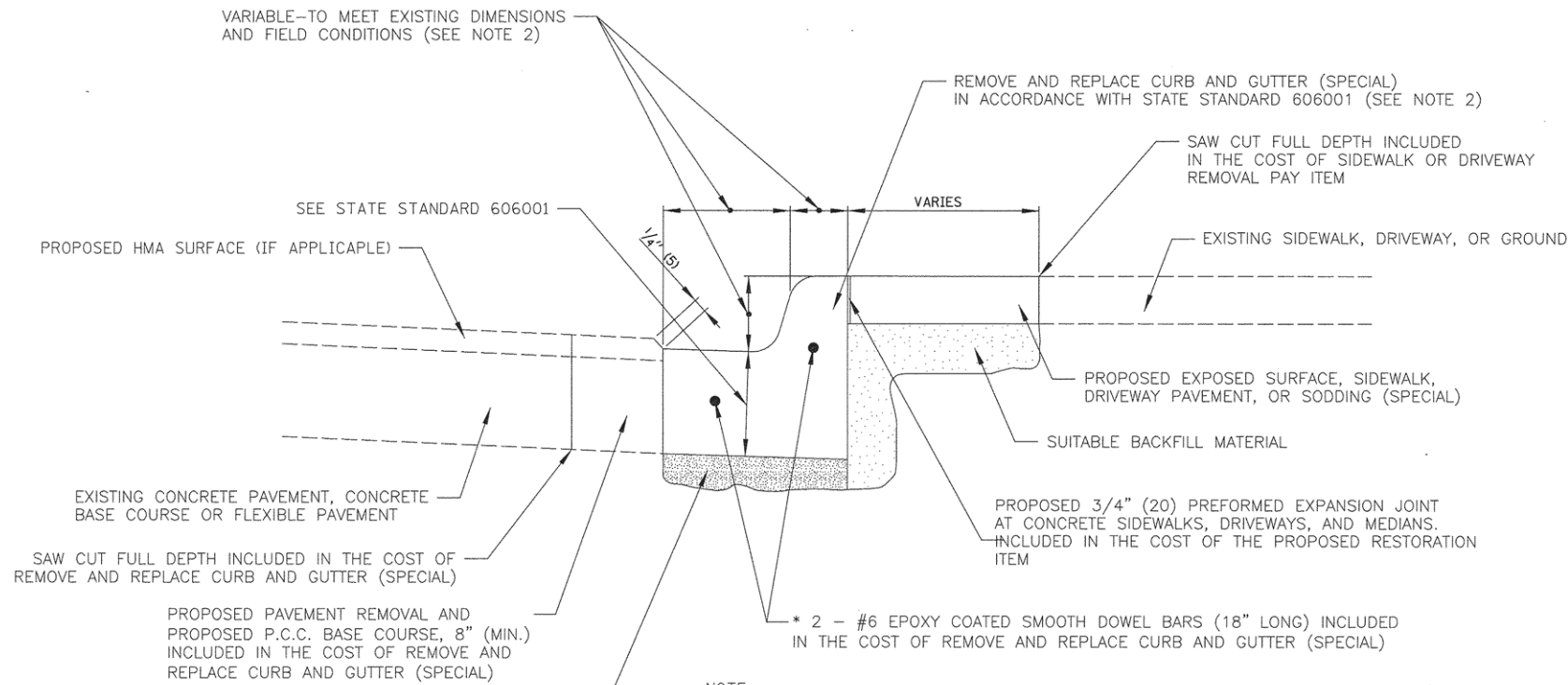
See Standard 602701 for details of steps.

See Standard 602601 for optional Precast Reinforced Concrete Flat Slab Top.

All dimensions are in inches (millimeters) unless otherwise shown.

**CATCH BASIN, TYPE A**

**MANHOLES, TYPE A**



**NOTE:**

1. SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
2. CURB OR CURB AND GUTTER REMOVAL SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE DIRECTED BY THE ENGINEER IN THE FIELD
4. LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
5. THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
6. THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
7. THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

PROPOSED PAVEMENT REMOVAL AND PROPOSED P.C.C. BASE COURSE, 8" (MIN.) INCLUDED IN THE COST OF REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)

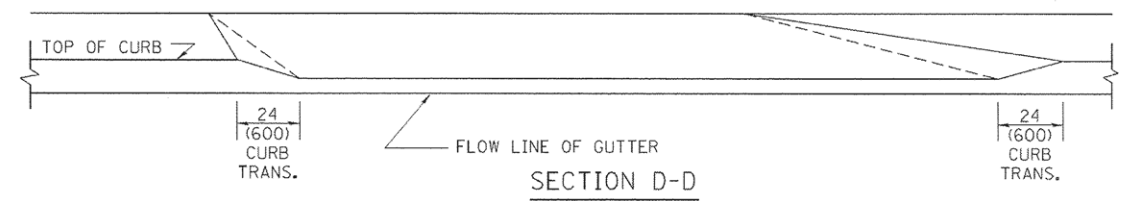
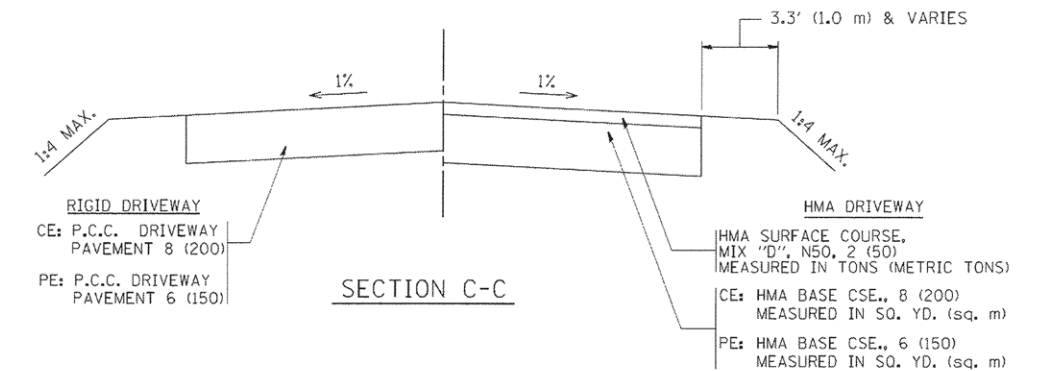
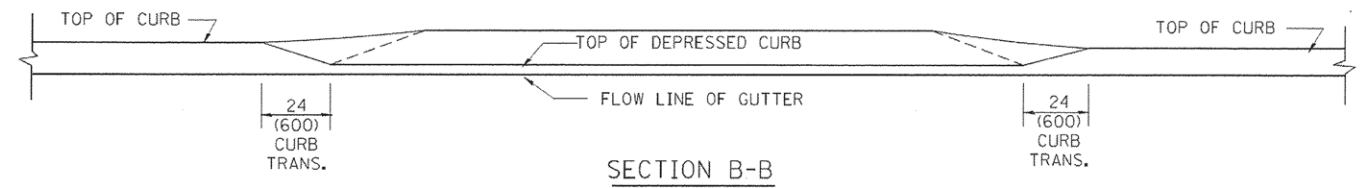
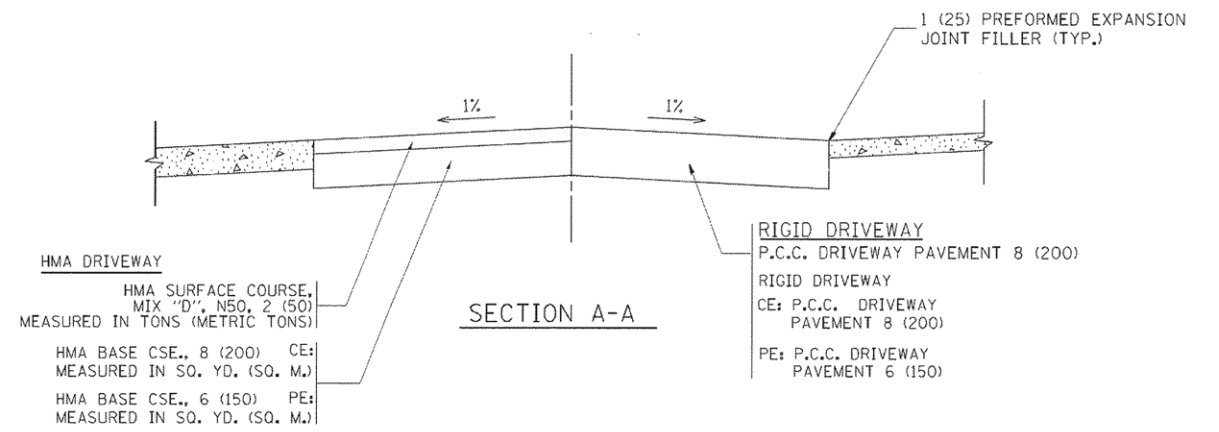
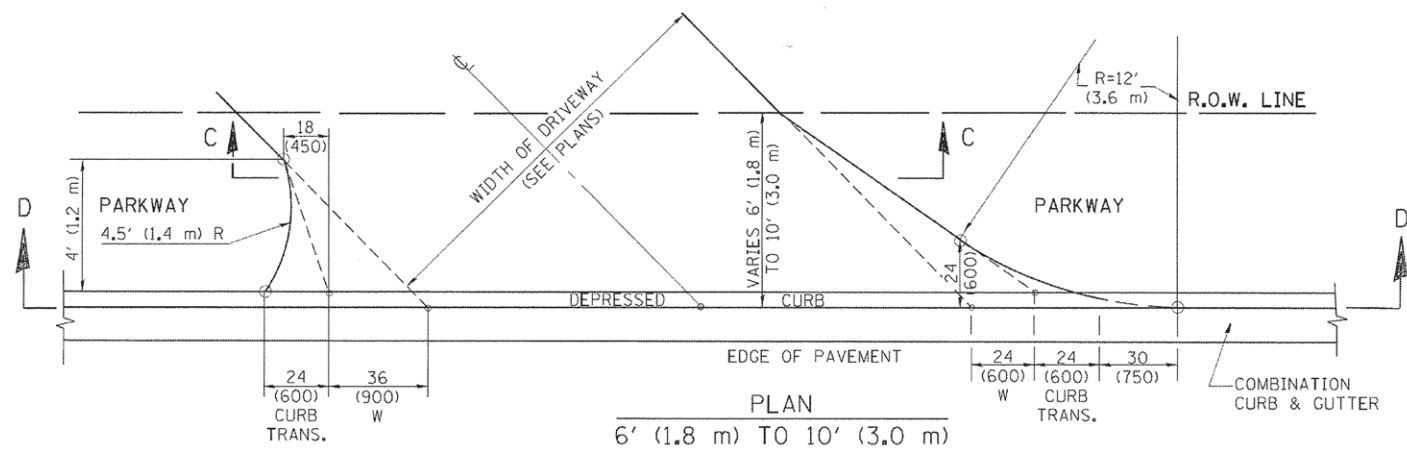
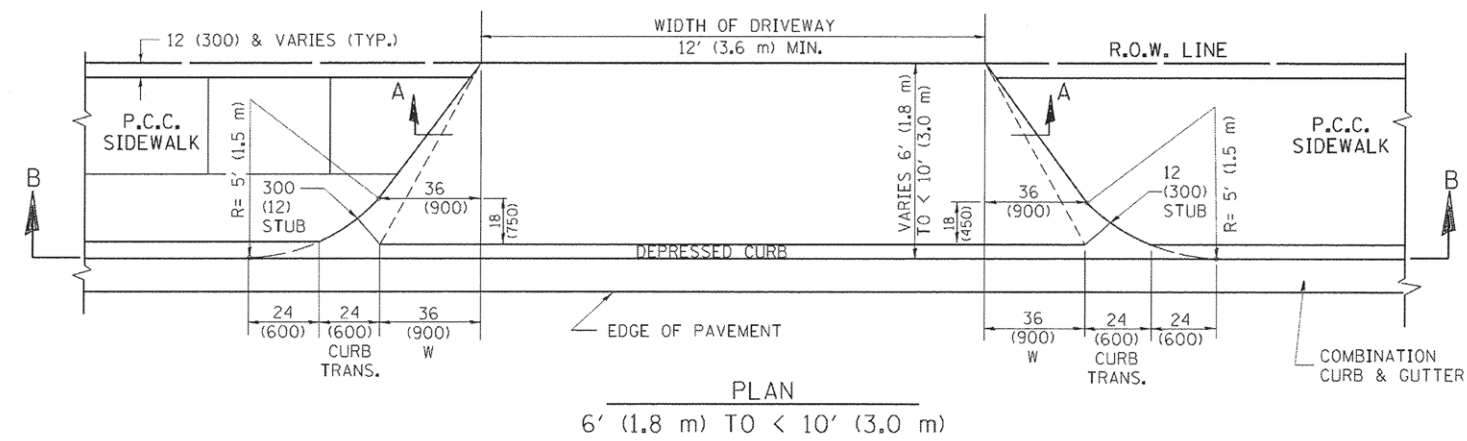
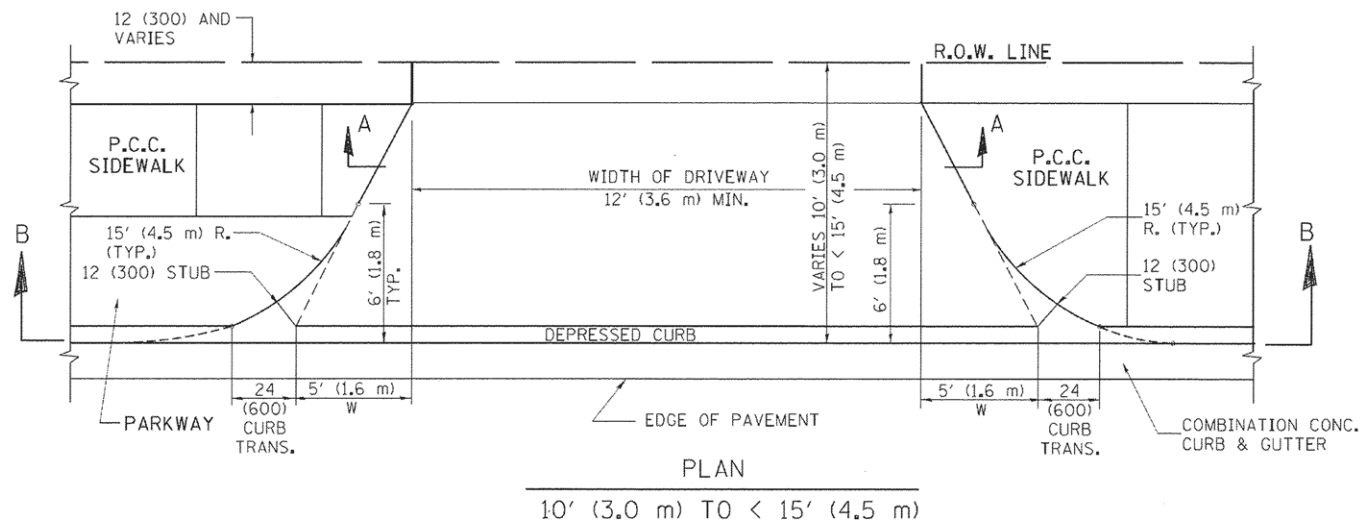
REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH THE ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)"

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

**REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)**

FILE NAME =	USER NAME = dschroeder	DESIGNED - OCG	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CONSTRUCTION DETAILS LONGCOMMON ROAD AND QUINCY STREET</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\RIVERSIDE\160012\Civil\DET_160012_02.SHT	DRAWN - RML	REVISIED -	REVISIED -			3569	16-00083-00-RS	COOK	31	23	
Default	PLOT SCALE = 20'	CHECKED - OCG	REVISIED -			CONTRACT NO. 61C96					
	PLOT DATE = 5/18/2016	DATE -	REVISIED -			ILLINOIS FED. AID PROJECT					
					SCALE: 20'	SHEET 2 OF 2 SHEETS		STA.	TO STA.		



**GENERAL NOTES**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = lcyso	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
cd:\pw\work\pstdot\lcyso\d010315\bd02.dwg		DRAWN -	REVISED - P. LOFLEUR 04-15-03
PLOT SCALE = 50.0000 / 1 in.		CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 10/29/2011		DATE - 11-06-95	REVISED - R. BORO 09-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

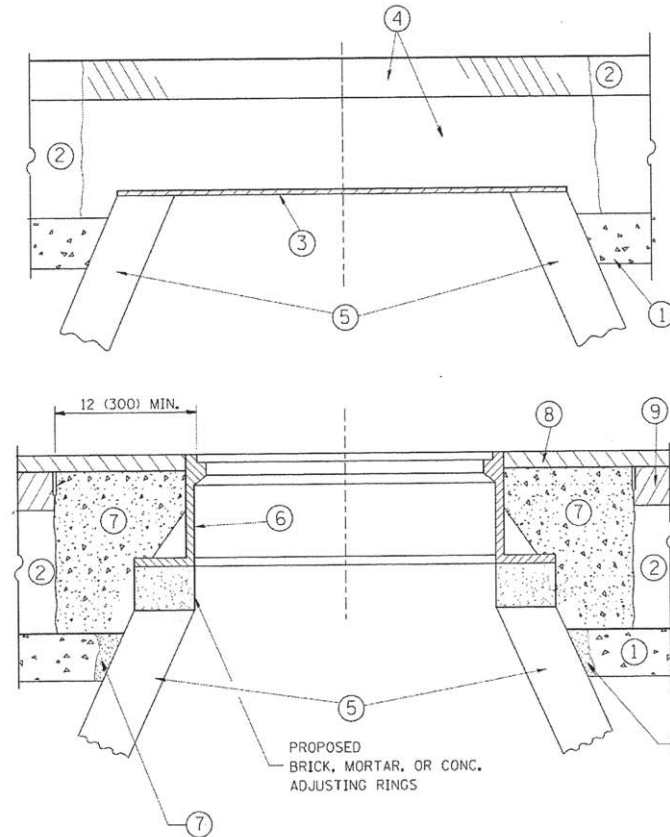
**DRIVEWAY DETAILS**

**DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3569	16-00083-00-RS	COOK	31	24
<b>BD400-02 (BD-02)</b>			<b>CONTRACT NO. 61C96</b>	
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID-PROJECT				





**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- \* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

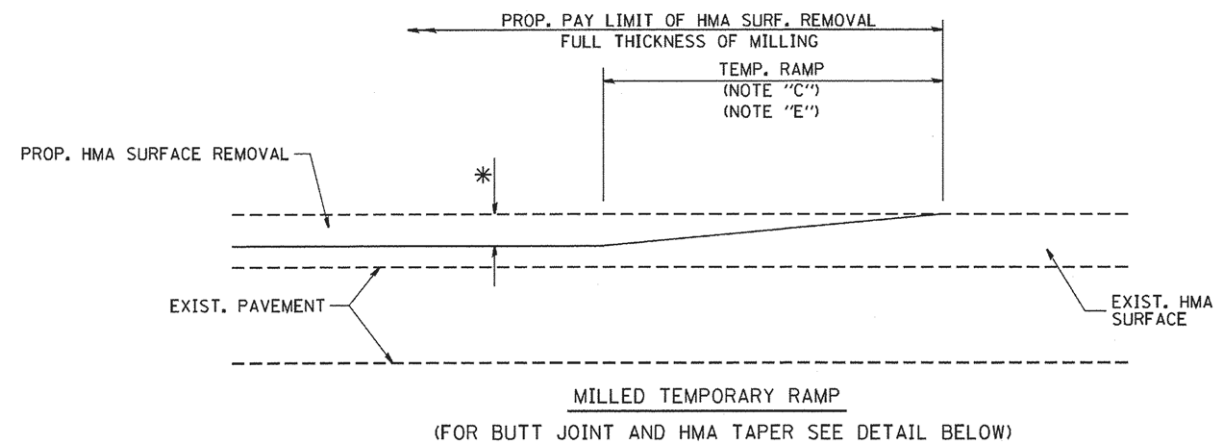
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et\pwwork\pwwork\bauredl\d0189315\bd08.dgn		DRAWN -	REVISED - R. BORO 01-01-07
		CHECKED -	REVISED - R. BORO 03-09-11
		DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

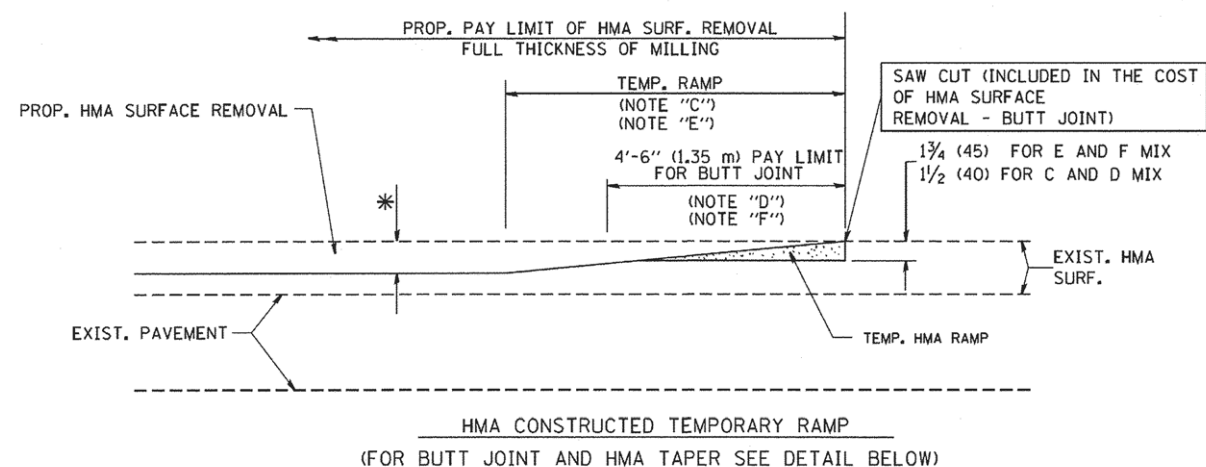
<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3569	16-00083-00-RS	COOK	31	25
<b>BD600-03 (BD-8)</b>			CONTRACT NO. 61C96	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

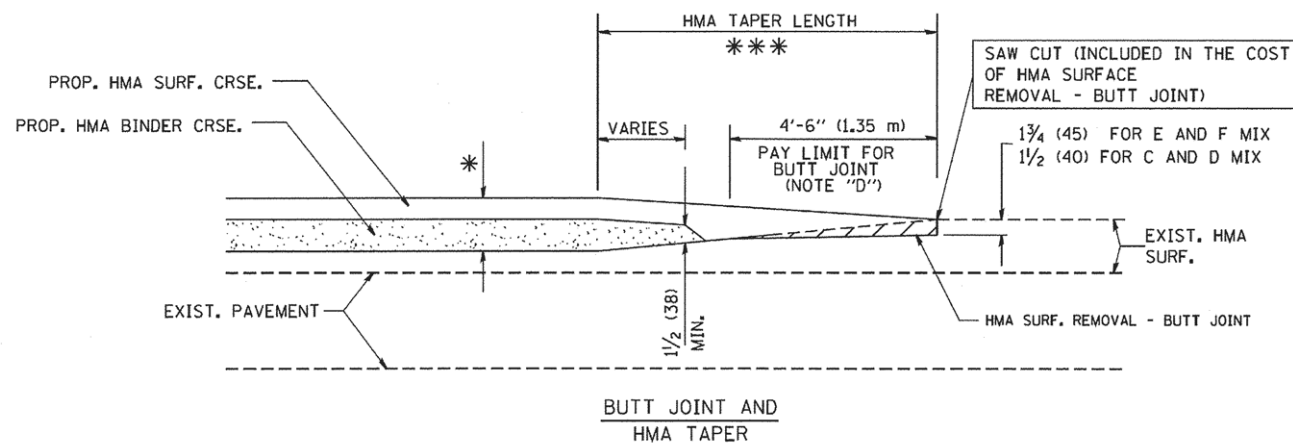




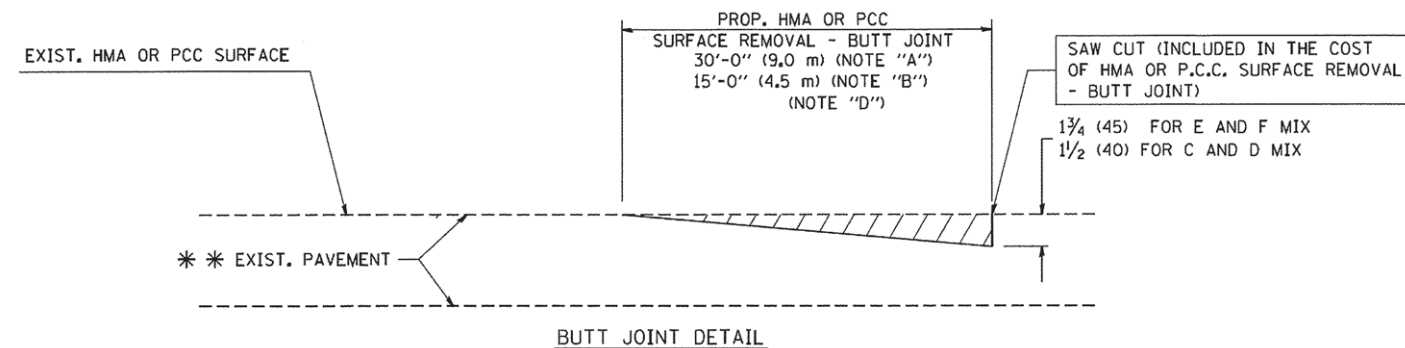
**OPTION 1**



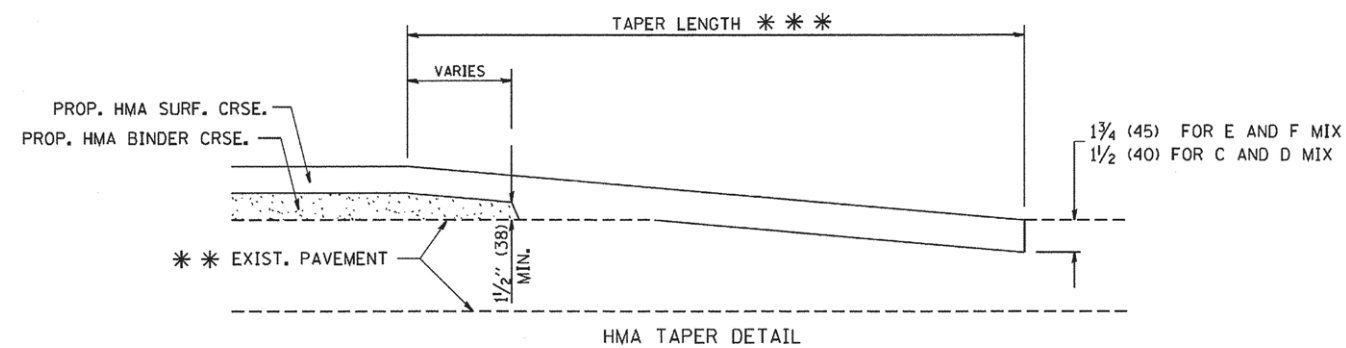
**OPTION 2  
TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

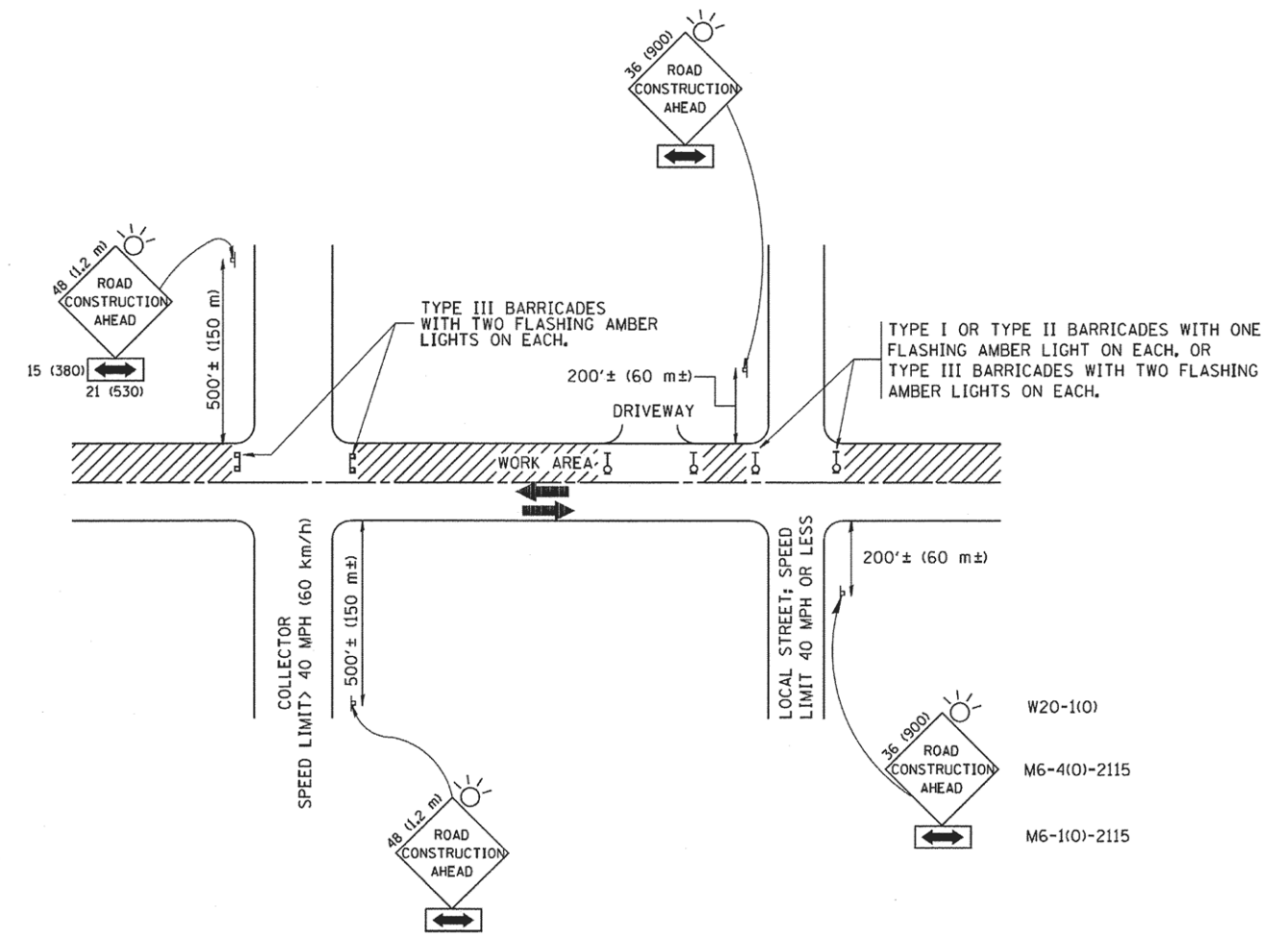
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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3569	16-00083-00-RS	COOK	31	27
BD400-05 BD32		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (Inches) unless otherwise shown.

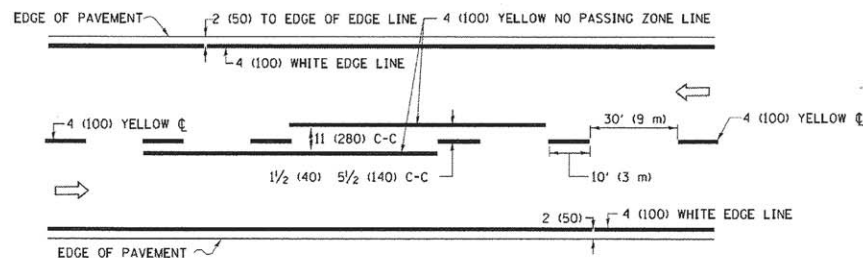
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		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

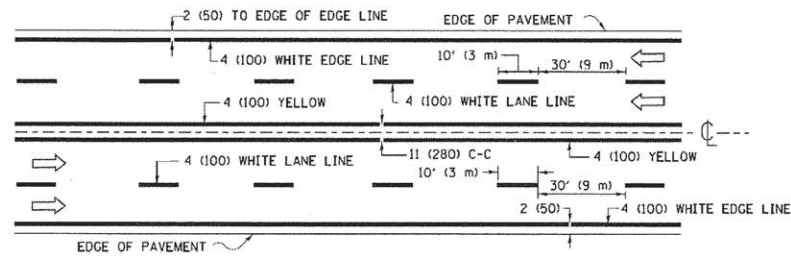
TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

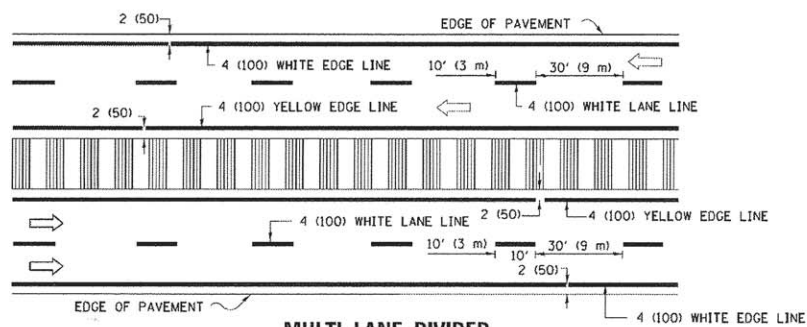
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3569	16-00083-00-RS	COOK	31	28
TC-10			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**2-LANE ROADWAY**

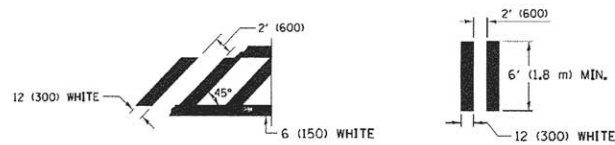
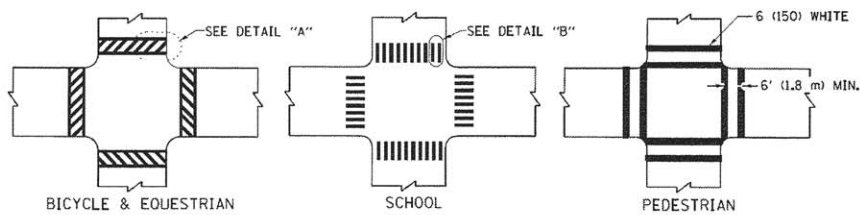


**MULTI-LANE UNDIVIDED**



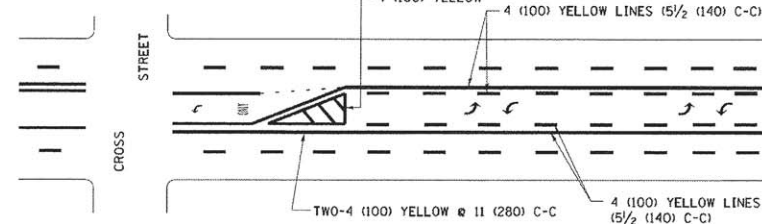
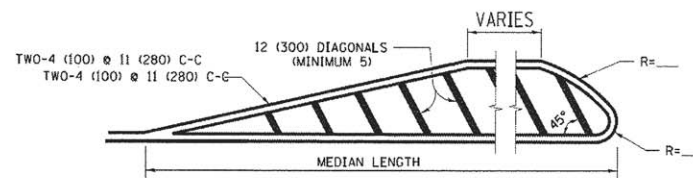
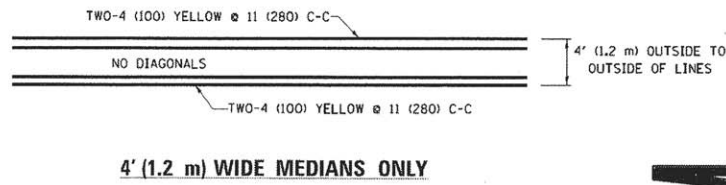
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

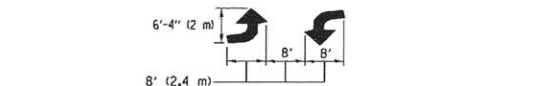


**TYPICAL CROSSWALK MARKING**

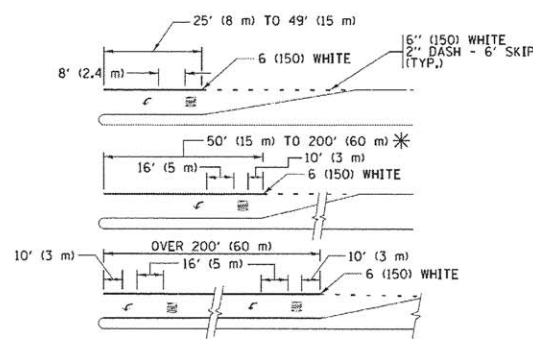
\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**TYPICAL PAINTED MEDIAN MARKING**

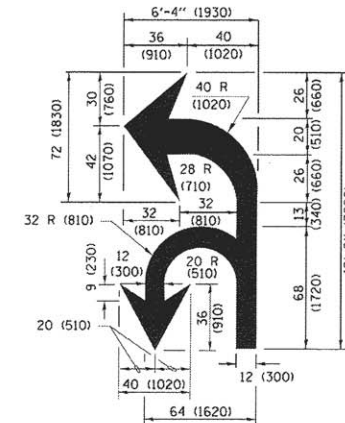
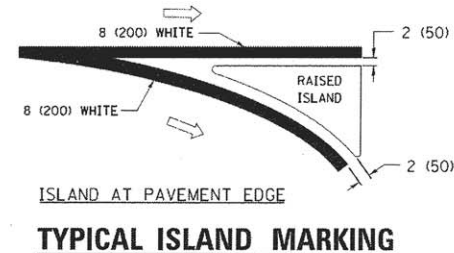
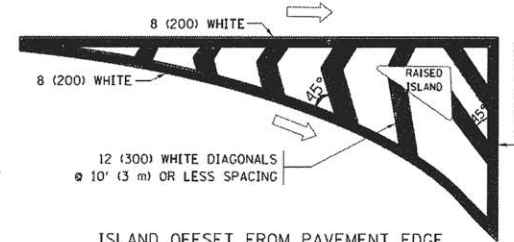


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

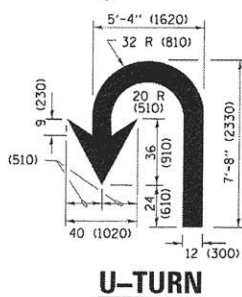
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**



**COMBINATION LEFT AND U-TURN**



**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES; FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

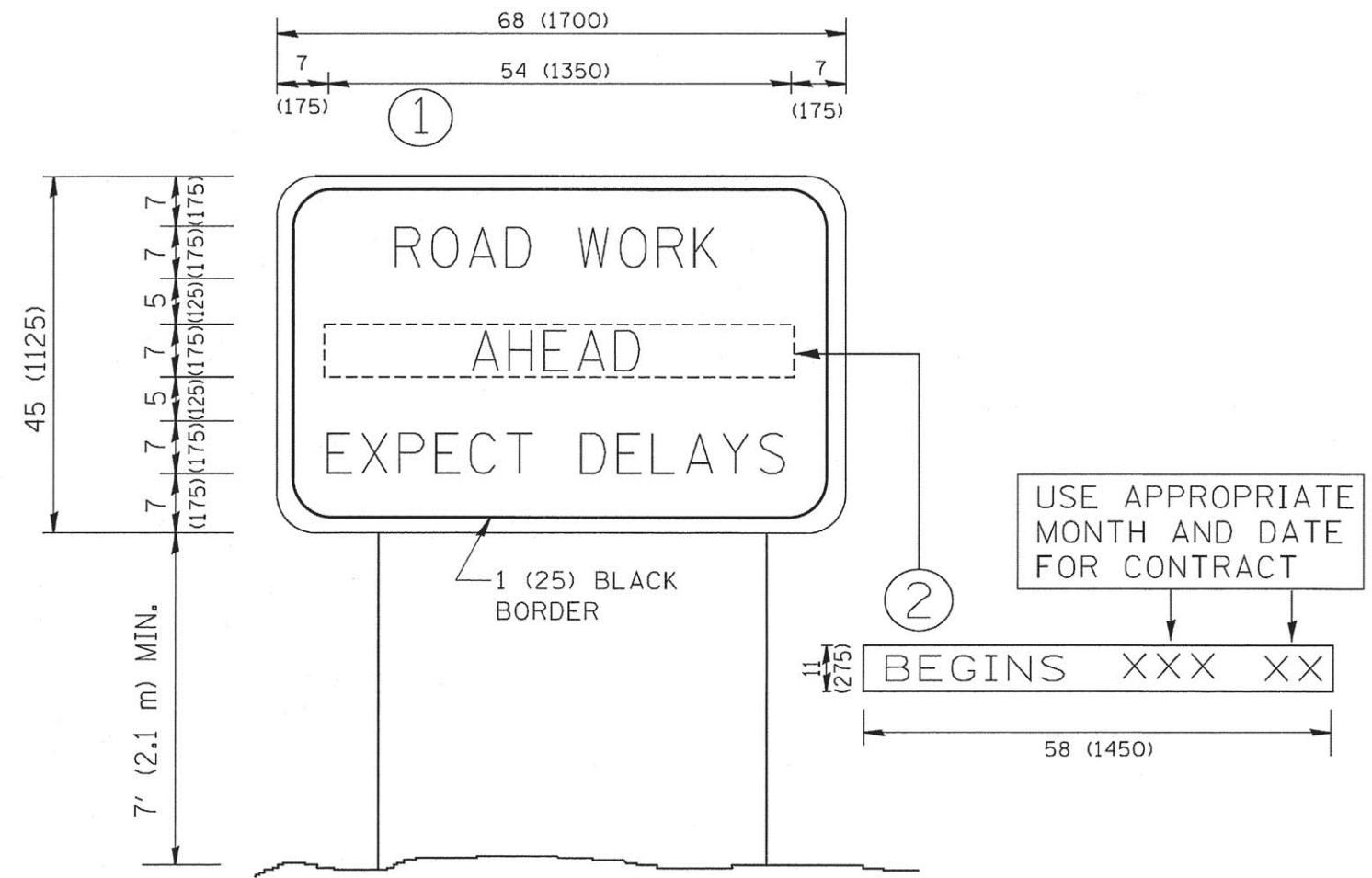
All dimensions are in inches (millimeters) unless otherwise shown.

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g:\il\04EBID\INTEG\11\notes\gov\PWIDOT\Documents\1007\0ffices\District\X\Projects\Dist\DRAWN\CAD\0to\CAD\sheta\1\13.dgn		CHECKED -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 50.000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 4/13/2016		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT ONE</b>			
<b>TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1472	12-00080-00-RS	COOK	31	29
<b>TC-13</b>			CONTRACT NO. 61C96	
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\distatd\22x34\to22.dgn	USER NAME = gaglionobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCLUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

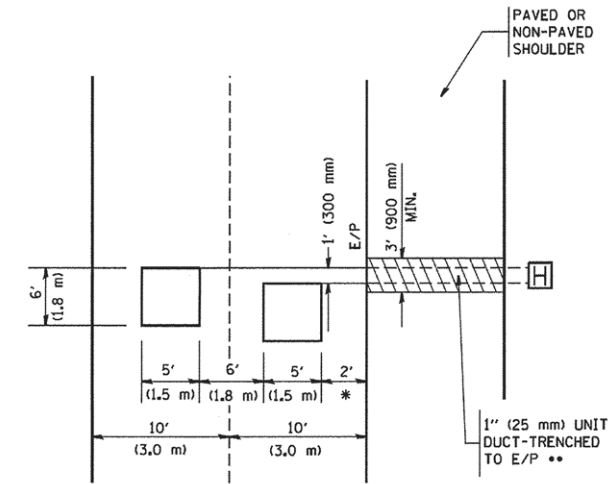
**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1472	12-00080-00-RS	COOK	31	30
TC-22			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



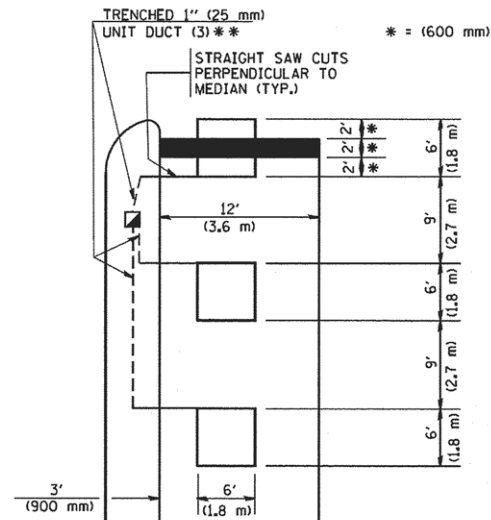
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



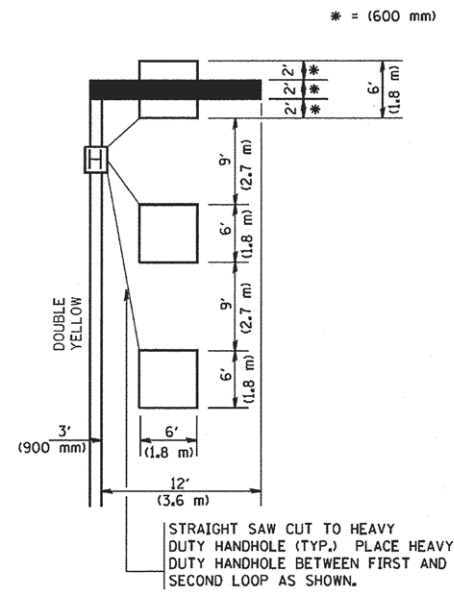
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

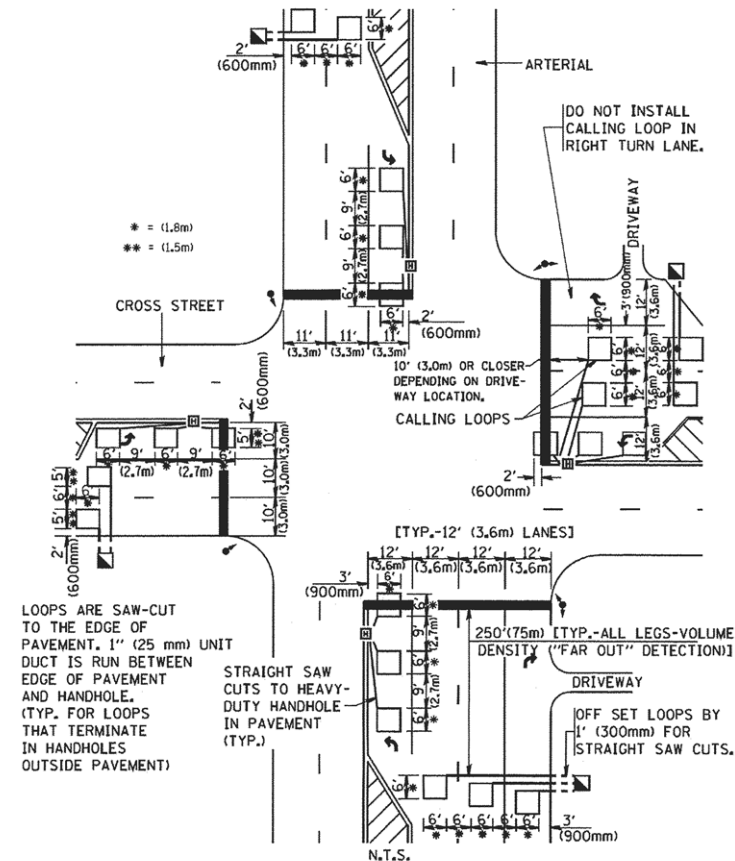
(PROTECTED / PERMITTED LEFT TURN PHASING)



\* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**

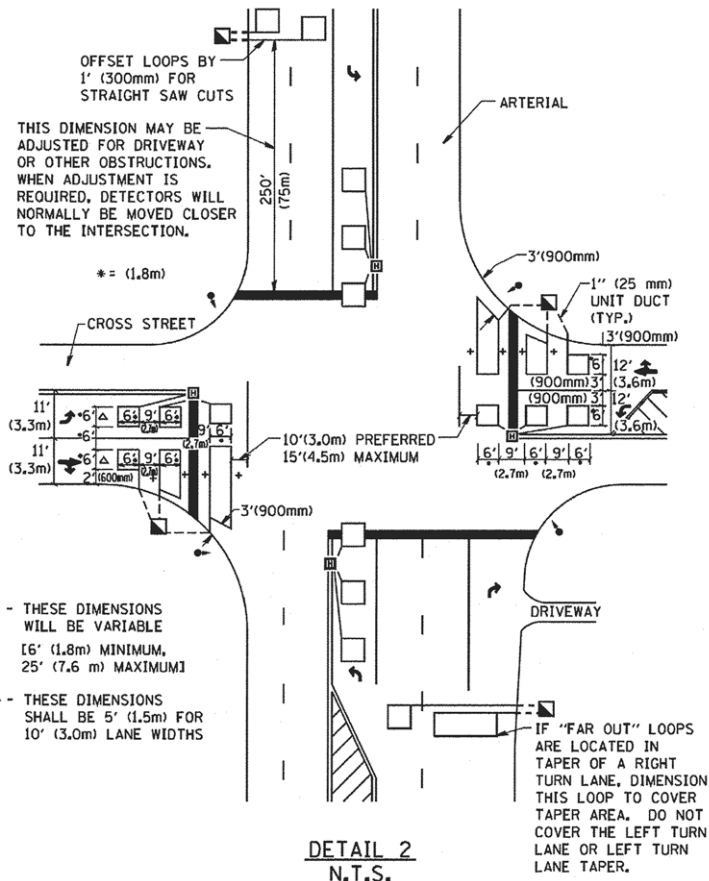


LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

DETAIL 1  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

△ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2  
N.T.S.

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\diststd\22x34\ts07.dgn	USER NAME = geglino	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>	F.A. RTE. 1472	SECTION 12-00080-00-RS	COUNTY COOK	TOTAL SHEETS 31	SHEET NO. 31	
PLOT SCALE = 50.0000' / IN.	CHECKED - R.K.F.	REVISED -	REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	CONTRACT NO.		
PLOT DATE = 1/4/2008	DATE -	REVISED -	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					