

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

* 12+4 = 16 TOTAL SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	128R-B-R	WILL	12	1
ILLINOIS			CONTRACT NO. 62C04	

FOR INDEX OF SHEETS, SEE SHEET NO.

THE IMPROVEMENT IS LOCATED
IN THE CITY OF JOLIET

TRAFFIC DATA

US 30 (JEFFERSON ST)

ADT (2015) = 7800

POSTED SPEED LIMIT = 30 MPH

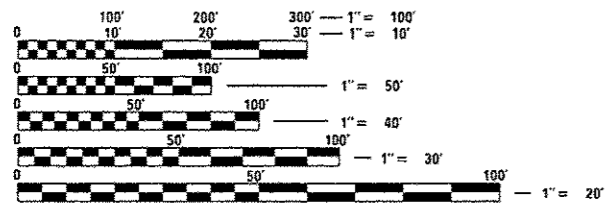
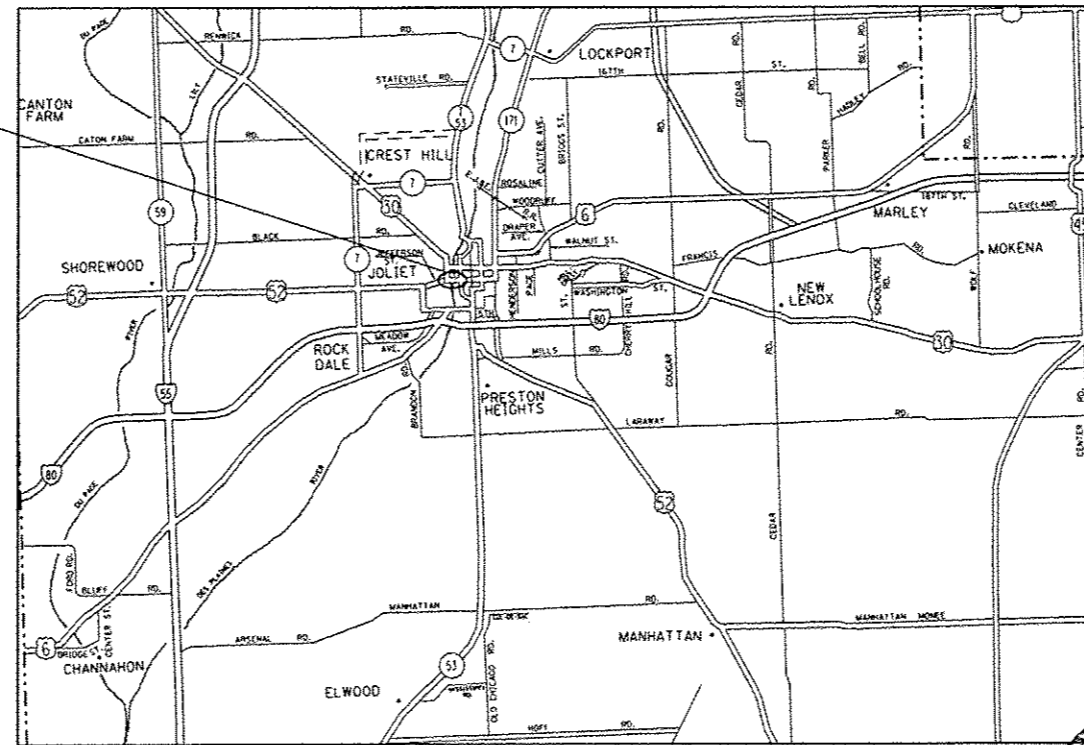
PROPOSED
HIGHWAY PLANS

FAP ROUTE 607(US 30 – JEFFERSON ST)
OVER DES PLAINES RIVER
SECTION: 128R-B-R
BRIDGE REPAIRS
WILL COUNTY
PROJECT: ACNHPP-0607(081)

C-91-237-16

R 10 E

LOCATION OF
IMPROVEMENT:
SN 099-0166



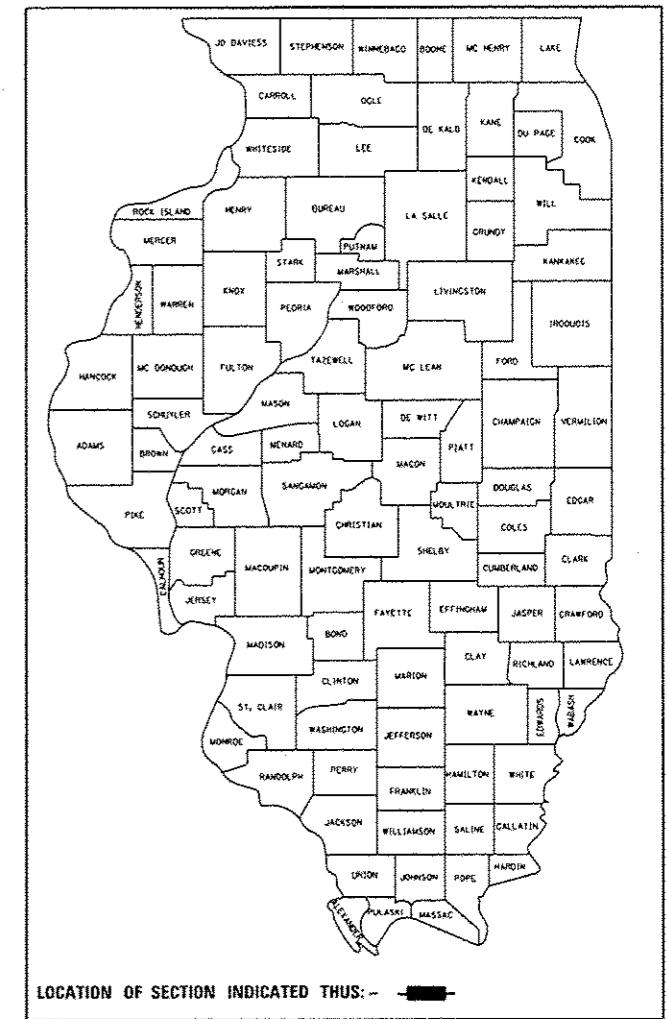
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: J. ALAIN MIDY (847) 221-3056
PROJECT MANAGER: ISSAM RAYYAN (847) 705-4178

CONTRACT NO. 62C04

GROSS LENGTH = 331.6 FT FT. = 0.063 MILE
NET LENGTH = 331.6 FT FT. = 0.063 MILE



LOCATION OF SECTION INDICATED THUS: - [black rectangle] -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED June 3, 2016

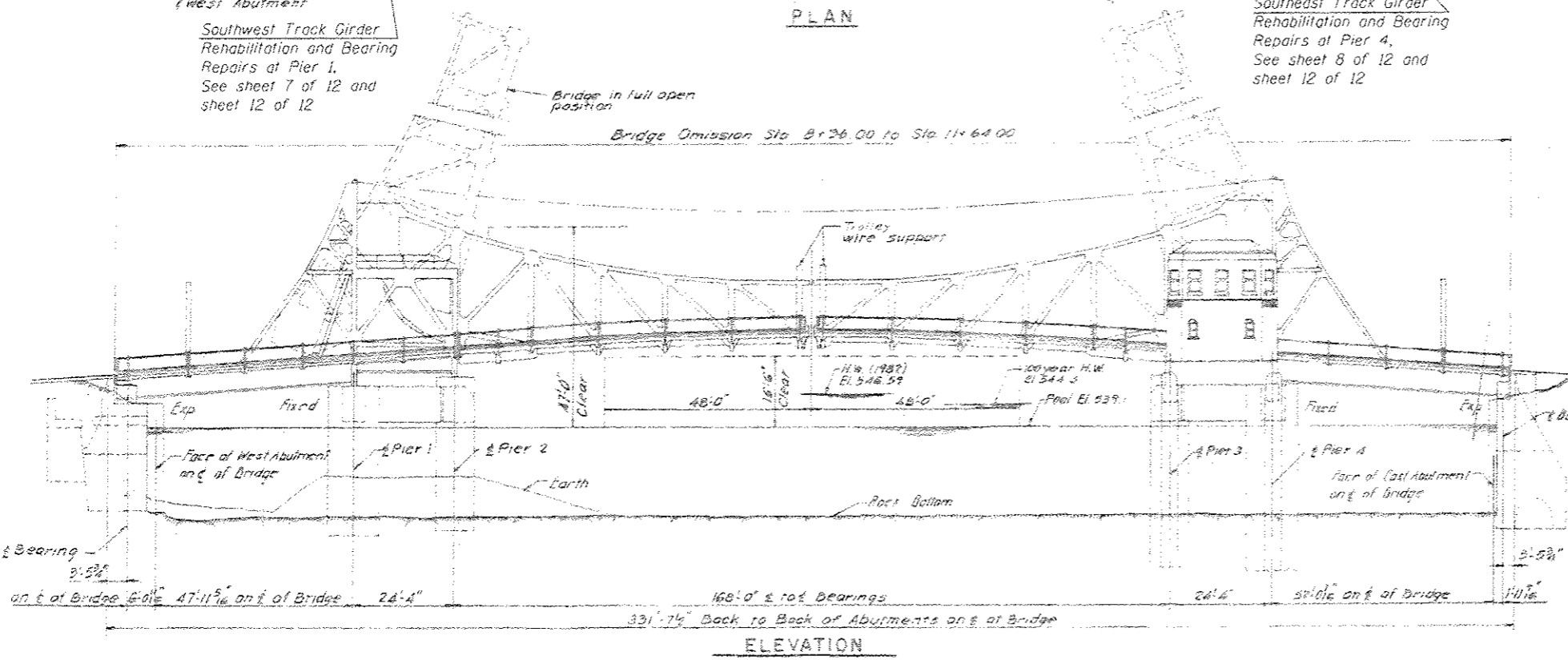
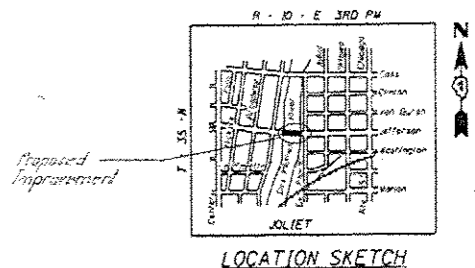
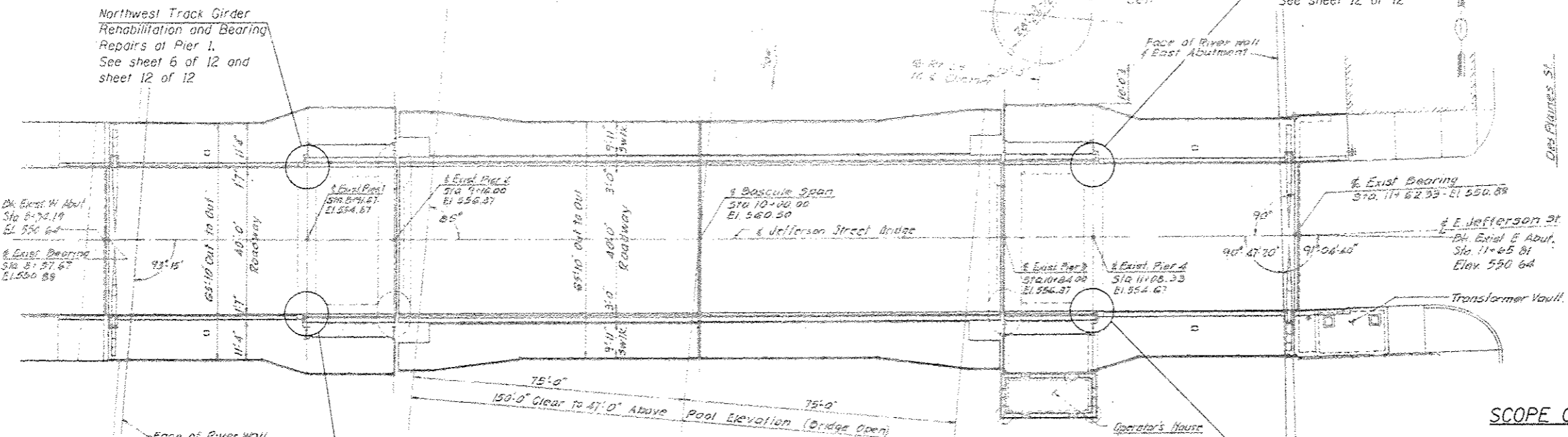
John F. [Signature]
REGIONAL ENGINEER

Julia [Signature]
ENGINEER OF DESIGN AND ENVIRONMENT

Travis [Signature]
DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

Bench Marks:
 Brass Plug in concrete wall, Sta. 0+S. 105+24.1 Right (MSL '12 Elev. = 544.146) N.G.V.D. El. = 543.708.
 Brass Disc in concrete wall, Sta. 0+S. 105+09.54 BR Right (MSL '12 Elev. = 544.082) N.G.V.D. El. = 543.644.



SCOPE OF WORK

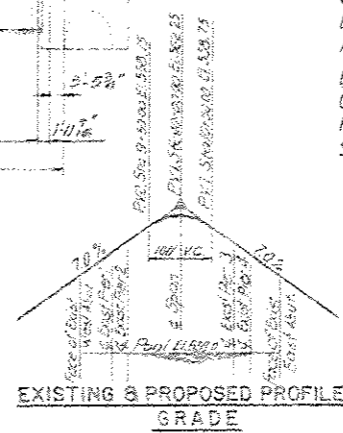
1. Removal of existing temporary support systems at pier 1
2. Install jacking systems
3. Remove existing bearing
4. Temporarily shore existing sidewalk beam and remove portion of sidewalk deck
5. Partial removal of bottom flange plates and angles
6. Repair end of track girder, install new angles and splice plates
7. Clean existing bearing
8. Reinstall existing bearing
9. Install anchor bolts and jack and adjust track girder level
10. Shim bearing
11. Remove sidewalk shoring

DESIGN DATA (BRIDGE REHAB)

Design Specifications:
 AASHTO Standard Specifications for Highway Bridges, 17th Edition, 2002 with 2005 Interims.
 AASHTO Standard Specifications for Movable Highway Bridges 1988 with Interim through 1995.

Design Loads:
 Dead load: Actual weights of the structure including structural steel, concrete, railings, electrical equipment, mechanical equipment, utilities and other permanent construction and fixtures.
 Live load: HS-20-44
 Wearing Surface: 25 psf
 Live load on sidewalk: 85 psf
 All other loads as per AASHTO Specifications

Design Stresses:
 Concrete: $f'_c = 3,500$ psi
 Reinforcement: $f_y = 60,000$ psi
 Structural Steel: $f_y = 50,000$ psi



Signed AHMAD M. HAMMAD
 AHMAD M. HAMMAD, S.E., Ill. Lic. No. 081-005467
 Expires 11-30-2016.
 Date 6.24.2016

GENERAL PLAN
JEFFERSON ST. OVER DES PLAINES RIVER
F.A.P. 607 (U.S. 30) - SEC. 12B R-B-I-1 (82)
WILL COUNTY
STATION 10+00.00
STRUCTURE NO. 099-0166

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	USER NAME : bloombergif	DESIGNED : LFB	REVISED :	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL PLAN & ELEVATION STRUCTURE NO. 099-0166	F.A.P. RTE. : 607	SECTION : 12BR-B-R	COUNTY : WILL	TOTAL SHEETS : 12	SHEET NO. : 2
	PLOT SCALE : N.F.S.	DRAWN : DCP	REVISED :			CONTRACT NO. : 62C04	ILLINOIS FED. AID PROJECT			
	PLOT DATE : 6/23/2016	CHECKED : AH	REVISED :	SHEET NO. 2 OF 12 SHEETS						

GENERAL NOTES

- Fasteners shall be ASTM A325 Type 1, mechanically galvanized bolts 7/8" φ, holes 15/16" φ, unless otherwise noted. Threads shall be excluded.
- All structural steel shall be AASHTO M270 Grade 50, unless otherwise noted.
- No field welding is permitted except as specified in the contract documents.
- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- The existing structural steel coating contains lead. The contractor shall take appropriate precautions to deal with the presence of lead on this project.
- All materials and construction methods shall conform to the Standard Specifications for Road and Bridge Construction adopted January 1, 2012 by the Illinois Department of Transportation including supplemental specifications and recurring special provisions unless otherwise specified.
- All elevation refer to mean sea level.
- River traffic shall be maintained at all times.
- The contractor shall obtain all necessary permits from the State of Illinois prior to commencing construction. The costs shall be included in the cost of the contract.
- Location and diameter of holes in new connecting members or pieces shall match holes in the existing structure. Holes in existing structure shall not be enlarged.
- It is the contractor's responsibility to take measurement in the field of the existing structure wherever new steel is to replace or to connect into existing material prior to ordering or fabricating any new steel. The contractor shall be responsible for the proper fitting and assembly of all parts of this work. The contractor shop drawings shall indicate which dimensions were obtained by actual field measurements.
- Members or pieces not specifically marked for removal or replacement are to remain in place.
- The contractor shall preform all work with care so that any materials that are to remain in place will not be damaged. If the Contractor damages any materials that are to remain in place, the damaged materials shall be repaired or replaced in a manner satisfactory to the engineer at the expense of the Contractor.
- Cost of cleaning, painting and reinstalling existing bearing is included in the cost of Jack, Remove and Reinstall Existing Bearings.
- Cost of drilling holes for anchor bolts is included in the cost of Remove and Replace Anchor Bolts.
- The Inorganic Zinc Rich Primer/Acrylic/Acrylic Paint System shall be used for shop and field painting of new structural steel. The color of the acrylic finish coat shall be Interstate Green, Munsell No. 7.5G 4/8.
- Core at least two 3" φ cores in existing pier at locations shown on the plans, to 15" depth to test and verify a minimum compressive strength of 3,500 psi. If minimum compressive strength is not achieved, concrete shall be removed and replaced with polymer concrete. Cost included in "Remove and Replace Anchor Bolts."
- All Structural Steel Repairs shall be cleaned and painted to meet the requirements of Primary Connections as specified in the special provisions for "Cleaning and Painting Contact Surface Areas of Existing Steel Structures."
- Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".
- Repairs to the West Track Girder at pier 1 should be done prior to East Track Girder repairs at pier 4, unless requirements by the Coast Guard inhibit this schedule.

CONSTRUCTION SEQUENCE WEST TRACK GIRDERS

REHABILITATION AT PIER 1

- Prior to jacking the track girder, remove connections connecting track girder to approach girder and rack support strut.
- Remove existing temporary support and install jacking system under each track girder at location as shown on plan.
- While the west leaf is in closed position, jack the track girders approximate 1/4" to allow the removal of existing bearings.
- Cut existing anchor bolts and remove existing bearings at pier 1 of pier track girders.
- Install temporary support under existing sidewalk stringer and remove portion of sidewalk deck and sidewalk stringer.
- Remove end cover plate, portion of bottom cover plate, bottom flange angles, stiffener angles, anchorage column connection plate and approach girder connection plate. Remove existing anchor bolt by core drilling.
- Attach new web plates and bottom flange angles and splice plate to existing webs.
- Connect outside stiffener angles furthest from the end of the track girder.
- Install connection to approach girder.
- Connect new WT stiffeners inside track girder and install connection to adjacent anchorage column.
- Connect bottom cover plate and splice plate.
- Install proposed end assembly of end plates and stiffener angles.
- Splice end of sidewalk stringer and weld support angle to new end plate. Clean and reuse existing reinforcement and patch deck.
- Remove temporary support under sidewalk stringer.
- Prepare bearing seat areas on top of pier 1 to be level grind existing anchor bolts smooth and seal with epoxy.
- Laser survey track girder elevation and jack each to the original elevation intended as directed by the Engineer.
* (NW Track girder approximate 1/2")
* (SW Track girder approximate 1/8")
- Slide existing bearing back into place and install new anchor bolts.
- Paint Track girder ends.

TRACK GIRDER TOLERANCES

- Each track girder shall be level from end to end and from side to side within ±0.005". Rack adjustment by shimming, or shim removal, may be necessary.
- Rolling surface elevations of north side girders to south side girders within ±0.01".
- Rolling surface elevations of east side girders to west side girders within ±0.06".

CENTER LOCKING TOLERANCES

- Elevations of the west side to the east side shall be within ±0.06".
- Center lock mesh should be checked and approved by Engineer upon completion of work to prevent crashing upon closure.

* Contractor shall laser survey existing condition and make necessary adjustment in accordance with track girder and center locking tolerances. Cost is included in the cost of the contract.

TOTAL BILL OF MATERIALS

Description	Unit	Total
Concrete Removal	Cy. Yd.	0.3
Concrete Superstructure	Cu. Yd.	0.3
Engineer's Field Office, Type A	Cal. Mo.	6
Mobilization	L. Sum	1
Changeable Message Sign	Cal. Mo.	2
Traffic Control and Protection, (Special)	L. Sum	1
Structural Steel Removal	L. Sum	1
Structural Steel Repair	Pound	8,010
Remove and Replace Anchor Bolts	Each	20
Temporary Traffic Signal Timing	Each	7
Jack, Remove and Reinstall Existing Bearings	Each	2

INDEX OF SHEETS

- 1 Cover Sheet
- 2 General Plan and Elevation
- * 3 General Notes and Bill of Material
- 4 Navigational Clearances
- 5 Jacking and Temporary Sidewalk Stringer Shoring
- 6 Plan and Elevation North Track Girder End at Pier 1
- 7 Plan and Elevation South Track Girder End at Pier 1
- 8 Plan and Elevation South Track Girder End at Pier 4
- 9 Sections and Details I
- 10 Sections and Details II
- 11 Sections and Details III
- 12 Anchor Bolt Repairs
- 12A Detour Plan
- 12B Detour Signing For Closing State Highway (TC-21)
- * 3A SUMMARY OF QUANTITIES
- 3B ADDITIONAL NOTES AND STANDARDS

LEGEND

-----	Existing	○	Proposed Bolt (Existing Hole)
+	Existing Rivet	⊗	Proposed Countersunk Bolt (New Hole)
⊗	Existing Bolt	●	Proposed Bolt (New Hole)
—	Proposed	▨	Removal

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	PLOT DATE = 6/29/2016	CHECKED = AH	REVISED =	SHEET NO. 3 OF 12 SHEETS		CONTRACT NO. 62C04				
ILLINOIS FED. AID PROJECT										

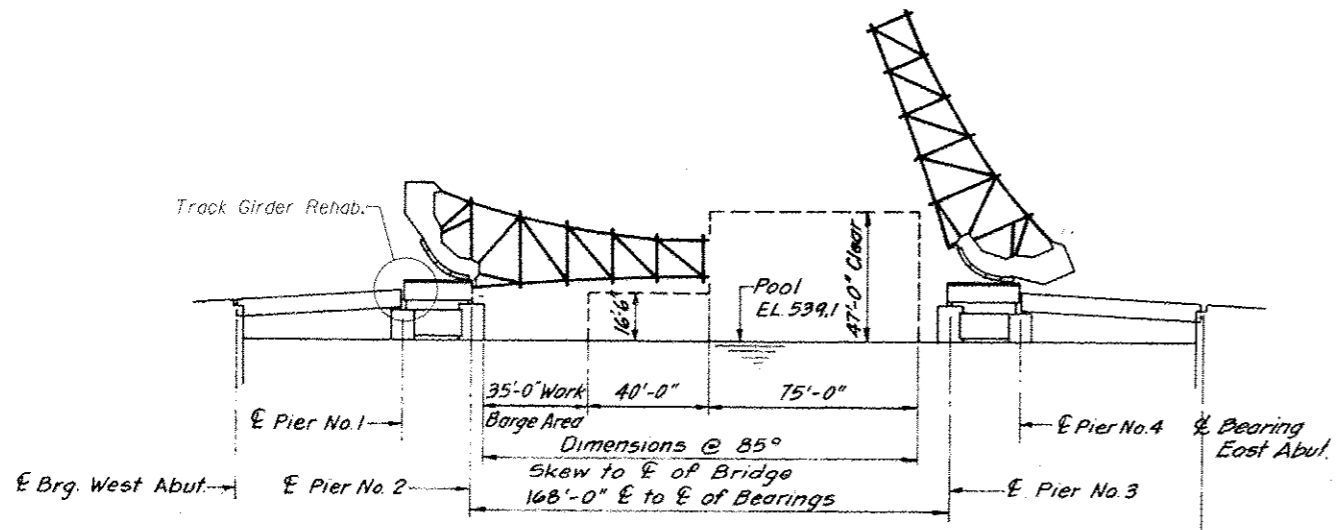
GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800)892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED).
2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE CITY OF JOLIET.
3. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
5. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847)705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
6. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE NOTIFICATION OF ALL EMERGENCY SERVICES, SCHOOL DISTRICTS, I.D.O.T.'S COMMUNICATIONS CENTER, SPRINGFIELD TRUCK PERMIT SECTION AND OTHER AGENCIES AFFECTED BY THE CLOSURE. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR POSTING SIGNS THAT WILL INDICATE THE DATES THE CLOSURE WILL BE IN PLACE.
8. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
9. THE DEPARTMENT HAS DETERMINED THAT IN STREAM WORK IS NOT REQUIRED FOR THE WORK SPECIFIED IN THIS CONTRACT. THE DEPARTMENT HAS NOT OBTAINED A 404 PERMIT. IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING AN USACE 404 PERMIT IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER USAGE PERMITS.
10. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO INSURE THAT NO DEBRIS WILL ENDAGER OR INTERFERE WITH ANY ACTIVITIES BENEATH THE BRIDGE ACCORDING TO ARTICLE 107.09 OF THE STANDARD SPECIFICATIONS AND SHALL PREVENT ANY DEBRIS FROM FALLING IN THE RIVER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE APPROPRIATE PAY ITEM INVOLVED AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

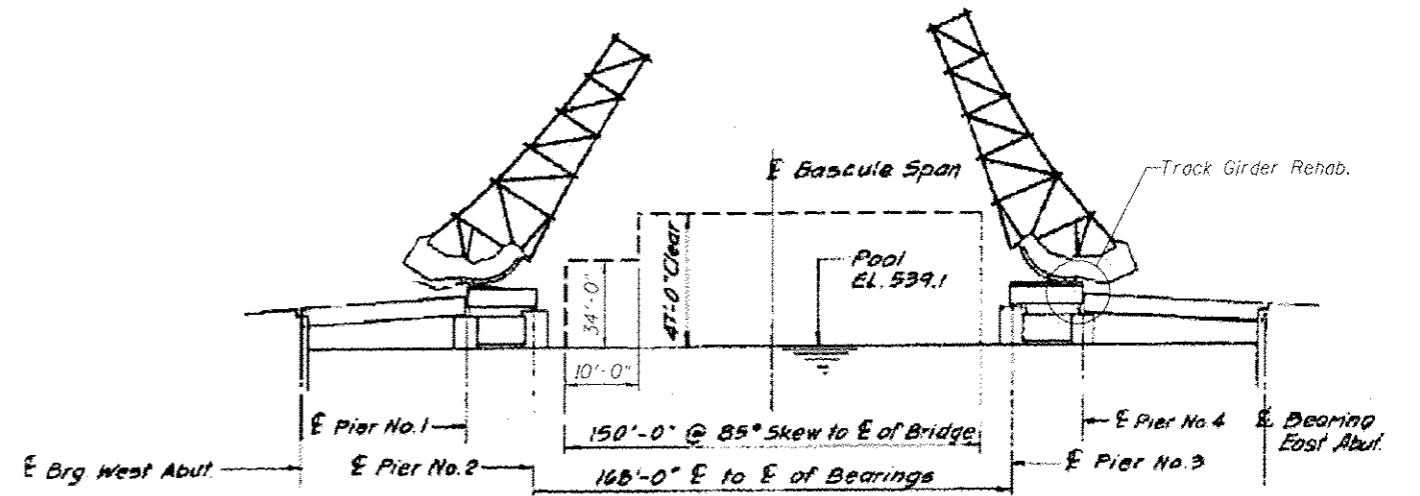
STATE STANDARDS

- 701801-06 SIDEWALK, CORNER OR CROSSWALK CLOSURE
- 701901-05 TRAFFIC CONTROL DEVICES

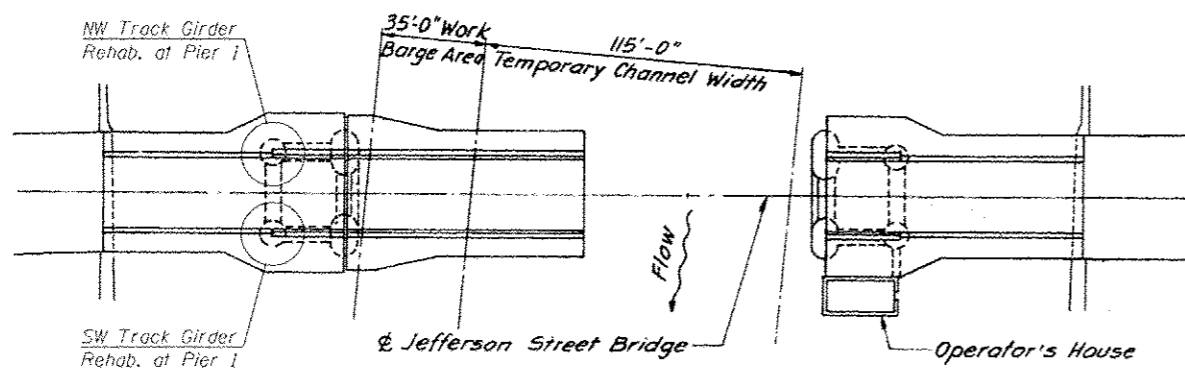
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p:\110846BID\INTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\011610\011610-1\Design\0116316-std-gennotes.dwg	pyrazanovskib	-	-			607	128R-B-R	WILL	12	30
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Default	PLOT DATE :	DATE :	REVISED :			SCALE:	SHEET 1 OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT



ELEVATION
(Looking North)



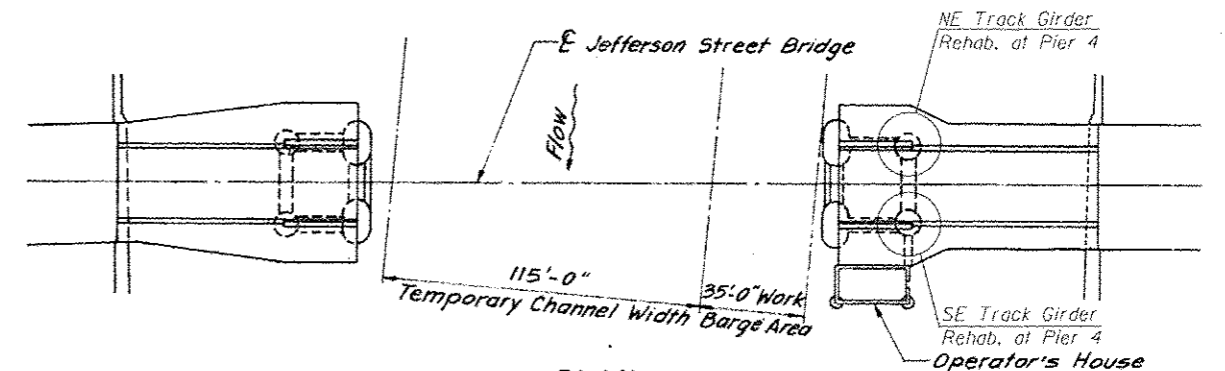
ELEVATION
(Looking North)



PLAN

POSITION 1

West leaf closed, East leaf full open (max. opening 71°)
 West leaf Track Girders at Pier 1
 West leaf shall remain closed while performing repairs at Pier 1 Track Girders. East leaf shown open for navigational clearances.



PLAN

POSITION 2

West leaf partially open (60° max. opening), East leaf fully open (max. opening 71°)
 East leaf Track Girders at Pier 4
 East leaf may be fully open to allow river traffic through while performing repairs at Pier 4 Track Girders.
 West leaf limited to 60° open until permanent repairs are completed.

Notes:

Maximum consecutive time for Position 1 is 1 week blockage or as approved by the Coast Guard. Contractor shall coordinate with the Coast Guard regarding navigational requirements, however, all correspondence from the Contractor to the Coast Guard shall be routed through and approved by IDOT and the Contractor is not permitted to contact USCG directly.

The roadway will be closed to traffic during rehabilitation. When the leaves of the bascule span are inoperable in the open position (60° max.) the leaves must be secured. The contractor shall submit the method and details for securing the leave in open position to the Engineer for review and approval.

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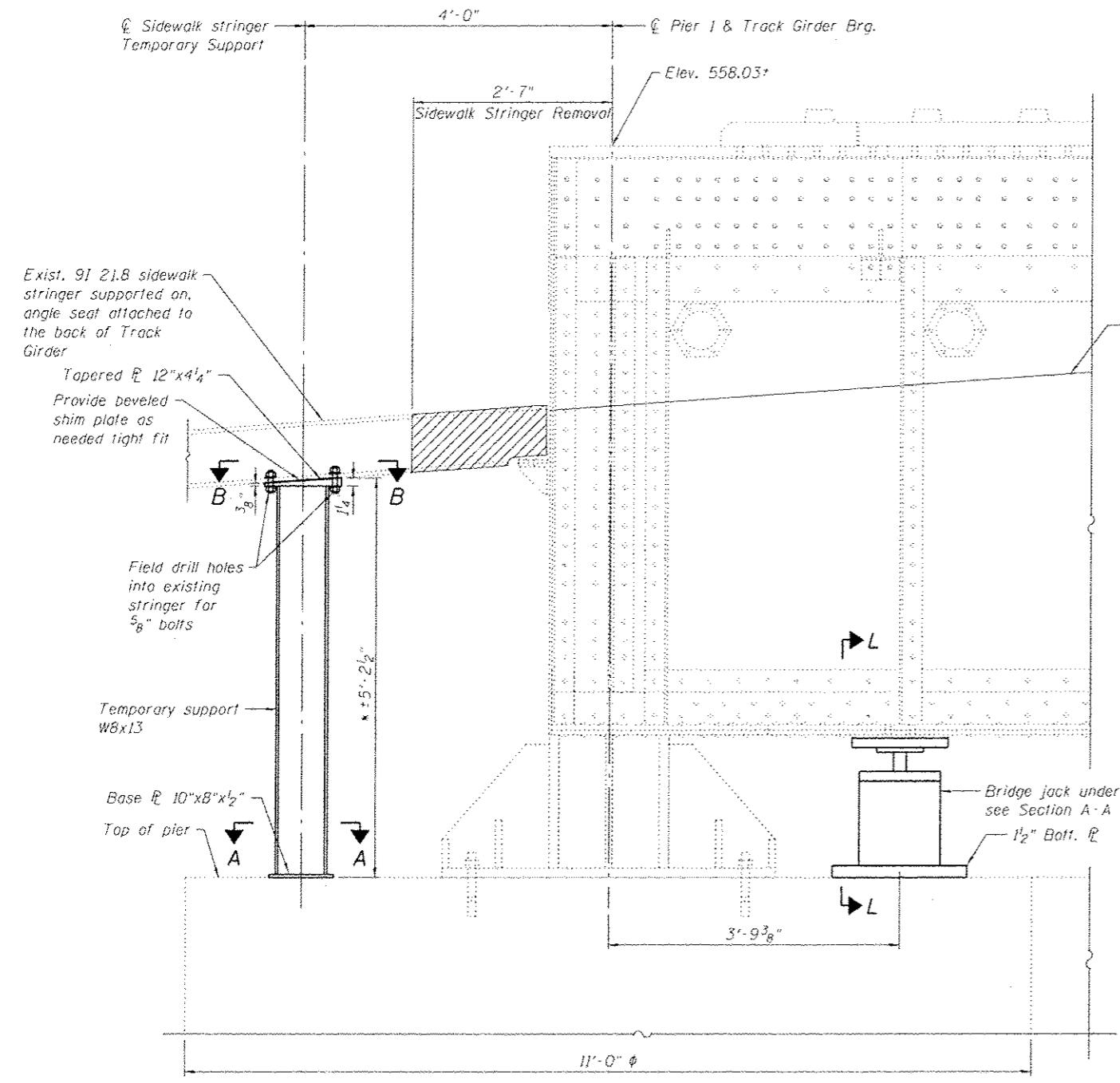
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NAVIGATIONAL CLEARANCES
STRUCTURE NO. 099-0166

SHEET NO. 4 OF 12 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	12BR-B-R	WILL	12	4
CONTRACT NO. 62C04			ILLINOIS FED. AID PROJECT	

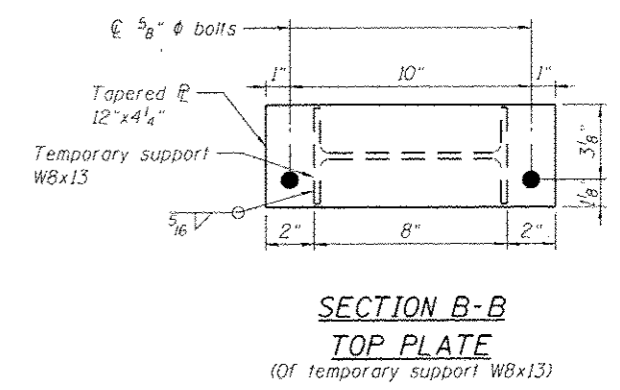
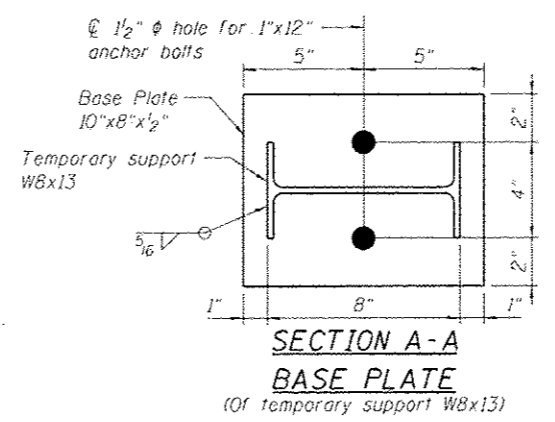
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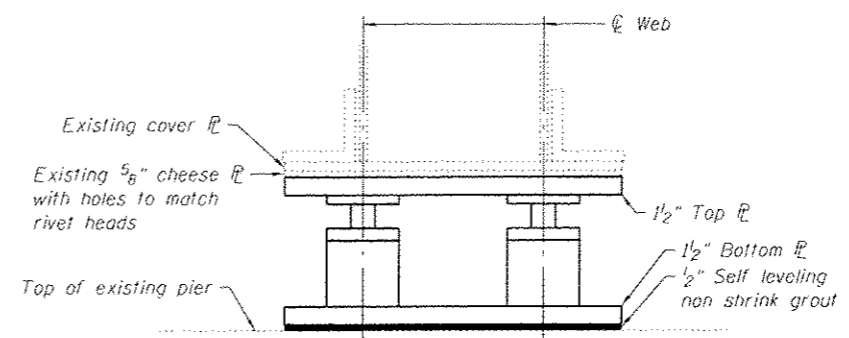
END ELEVATION

South Track Girder shown (Looking North Pier 1)
North Track Girder similar (Looking South Pier 1)

* To be verified in field before ordering material



Girder Reaction	
DL	45k
LL	0k

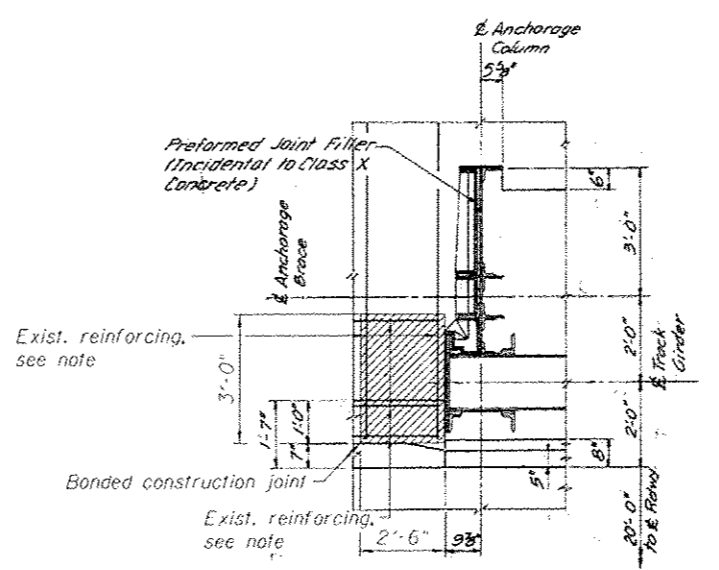


SECTION L-L

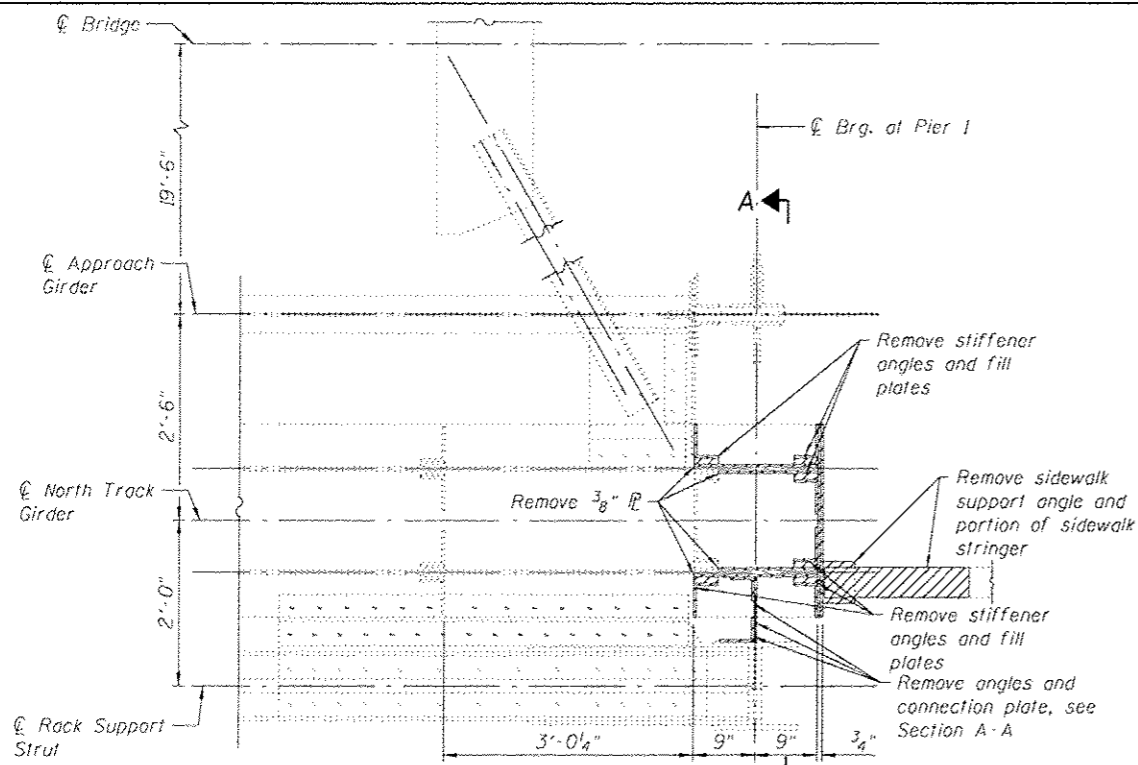
JACK AND REMOVE EXISTING BEARING PROCEDURE:

1. The Contractor shall submit for approval by the Engineer, plans for jacking track girder for the removal of existing bearing track girder end repairs and reinstalling cleaned existing bearing prior to commencing any related work. Minimum jacking capacity 70kip, 35 kip per jack. Jacks must be connected to the same manifold to provide even lift to both jacks.
2. Jack shall be installed below stiffener, as shown, after removing existing temporary support. Contractor may install timber post or other means necessary to transfer the load at existing stiffener angle.
3. Remove existing bearing after jacks have been installed and before temporary sidewalk support is in place.
4. After completing repairs, prepare bearing seat areas on top of pier to be level and grind existing anchor bolts smooth and seal with epoxy.
5. Jack each track to the proper elevation level as directed by the Engineer and as per Track Girder Tolerances on Sheet 3 of 12.
6. After temporary sidewalk support has been removed, slide cleaned, existing bearing back into place and install new anchor bolts.
7. Remove jacks and grout.

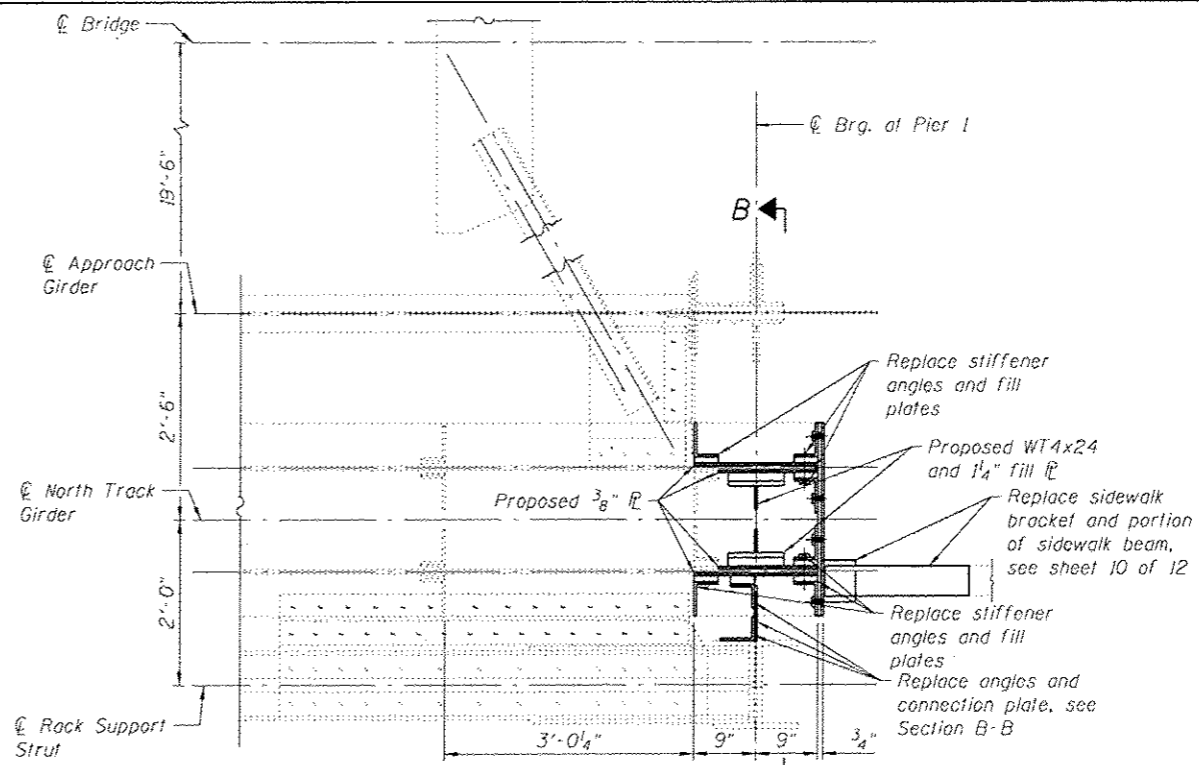
Notes:
Remove existing temporary support and install jacking system before removing track girders bearing at pier 1. See Sequence of Construction on Sheet 12 of 12.
The work shall be done in accordance with the special provision "Jack, Remove and Reinstall Existing Bearings".
Remove portion of existing sidewalk slab to complete structural repairs. Reinforcing bars to be cleaned and reused. Sidewalk slab shall be replaced in-kind. Cost included in the cost of Concrete Superstructure.
During rehabilitation, bridge shall be closed to live load traffic including construction loads.



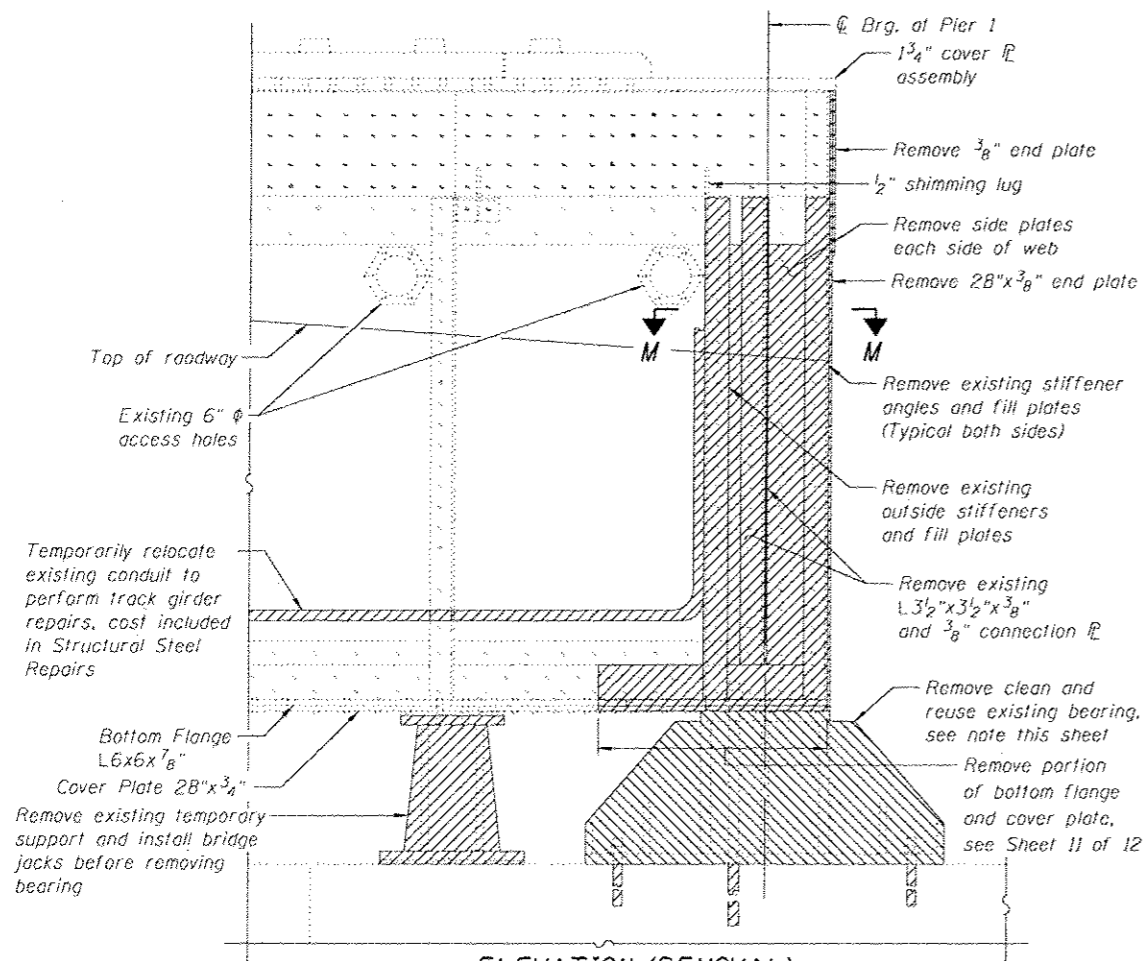
PLAN DETAIL AT END OF TRACK GIRDER SIDEWALK REMOVAL



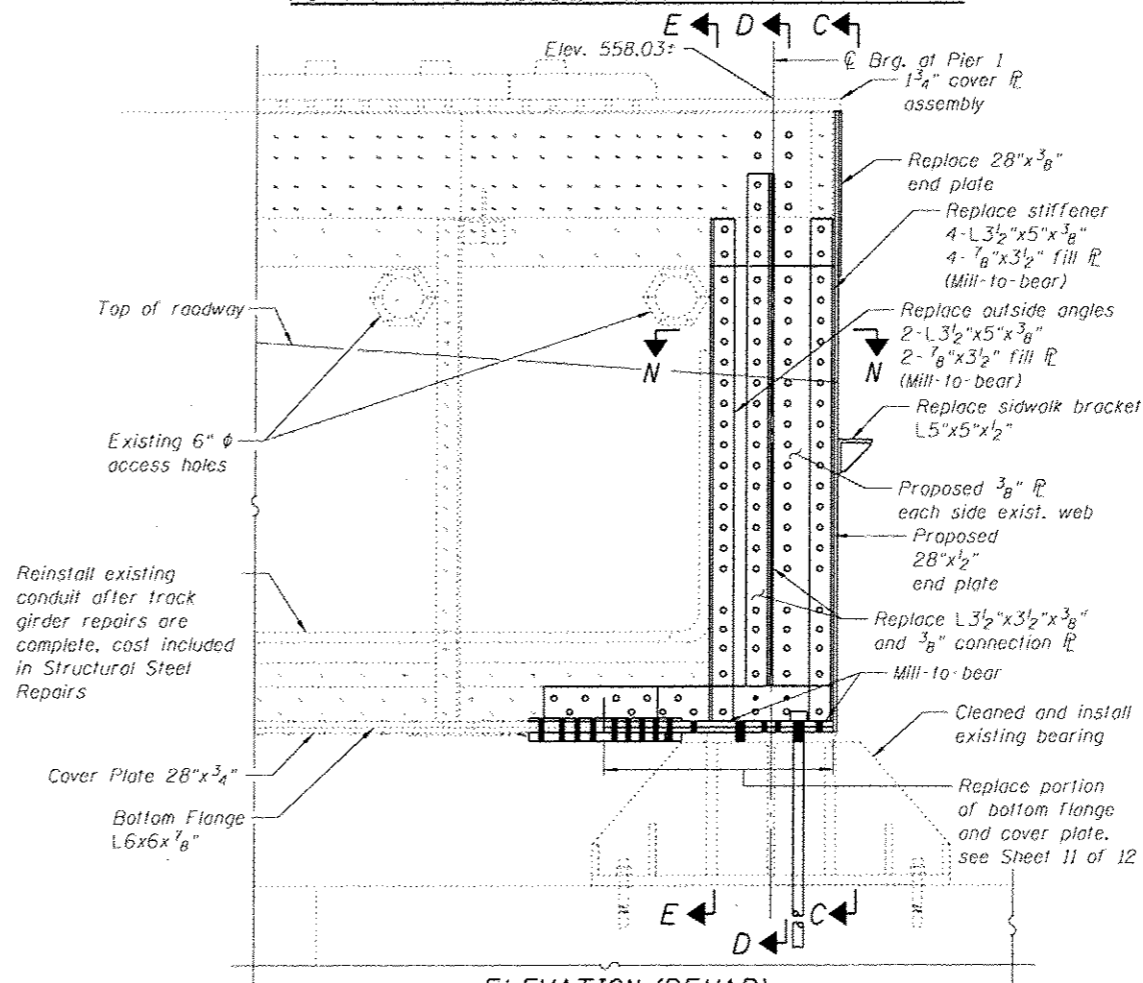
SECTIONAL PLAN M-M (REMOVAL)
NORTH TRACK GIRDER END AT PIER 1



SECTIONAL PLAN N-N (REHAB)
NORTH TRACK GIRDER END REPAIR AT PIER 1



ELEVATION (REMOVAL)
NORTH TRACK GIRDER END AT PIER 1
 (Looking South)



ELEVATION (REHAB)
NORTH TRACK GIRDER END REPAIR AT PIER 1
 (Looking South)

Notes:
 Use existing holes as template to drill new holes in new angles and plates.
 See sheet 9 of 12 for Section A-A and Section B-B
 See sheet 10 of 12 for Section C-C, Section D-D and Section E-E.
 Match-mark bearing so they are installed in same location and orientation.

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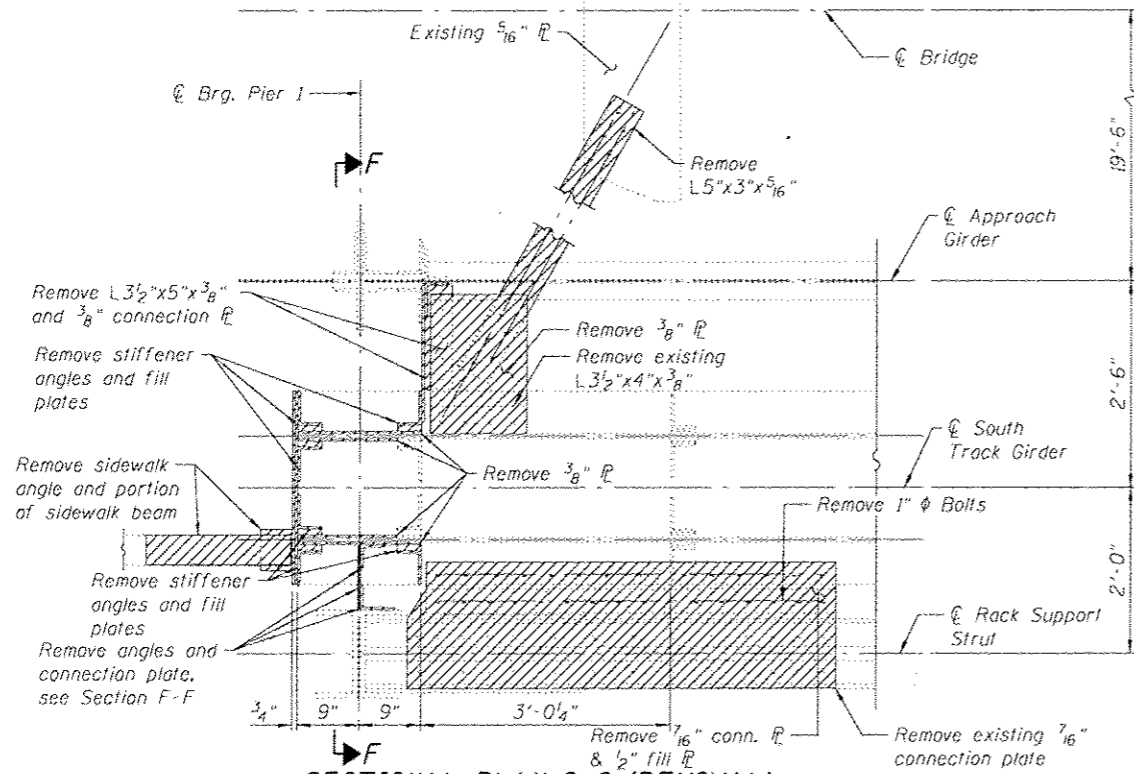
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PLOT DATE - 6/23/2016	CHECKED - AH	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

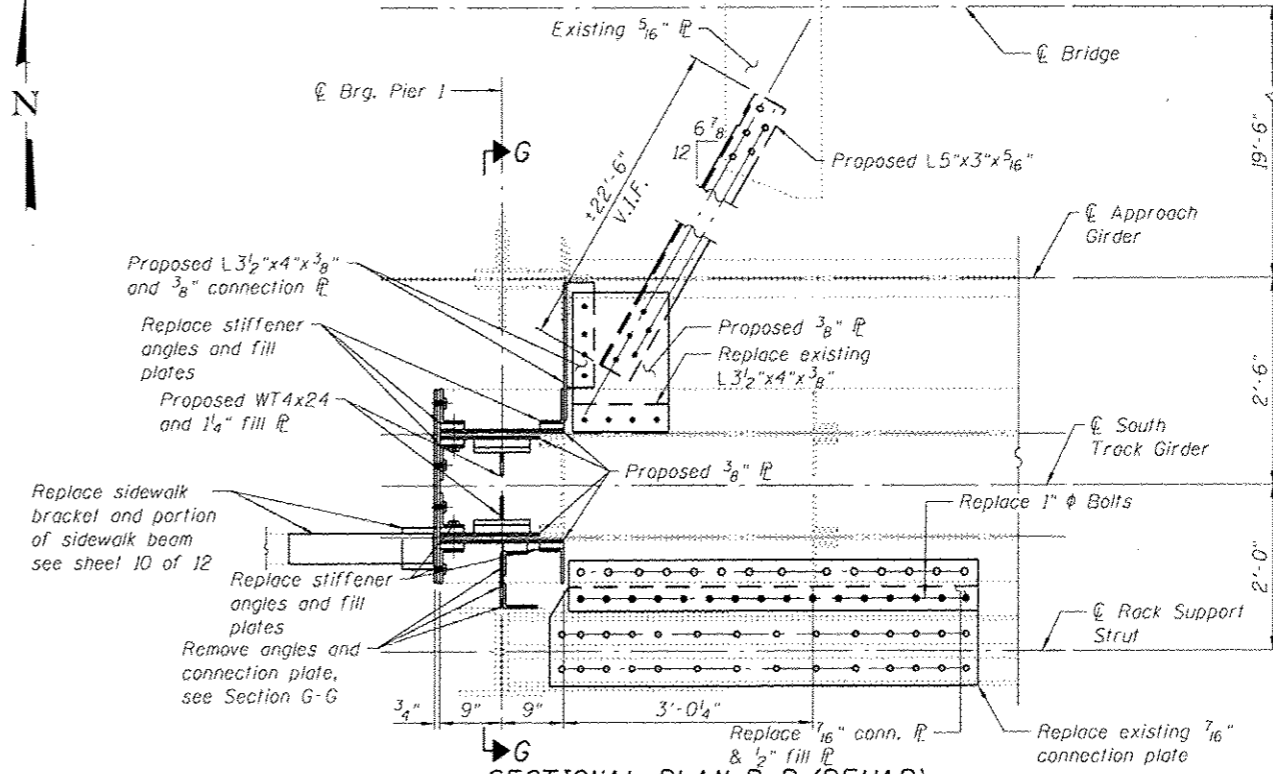
PLAN AND ELEVATION NORTH TRACK GIRDER END AT PIER 1
STRUCTURE NO. 099-0166

SHEET NO. 6 OF 12 SHEETS

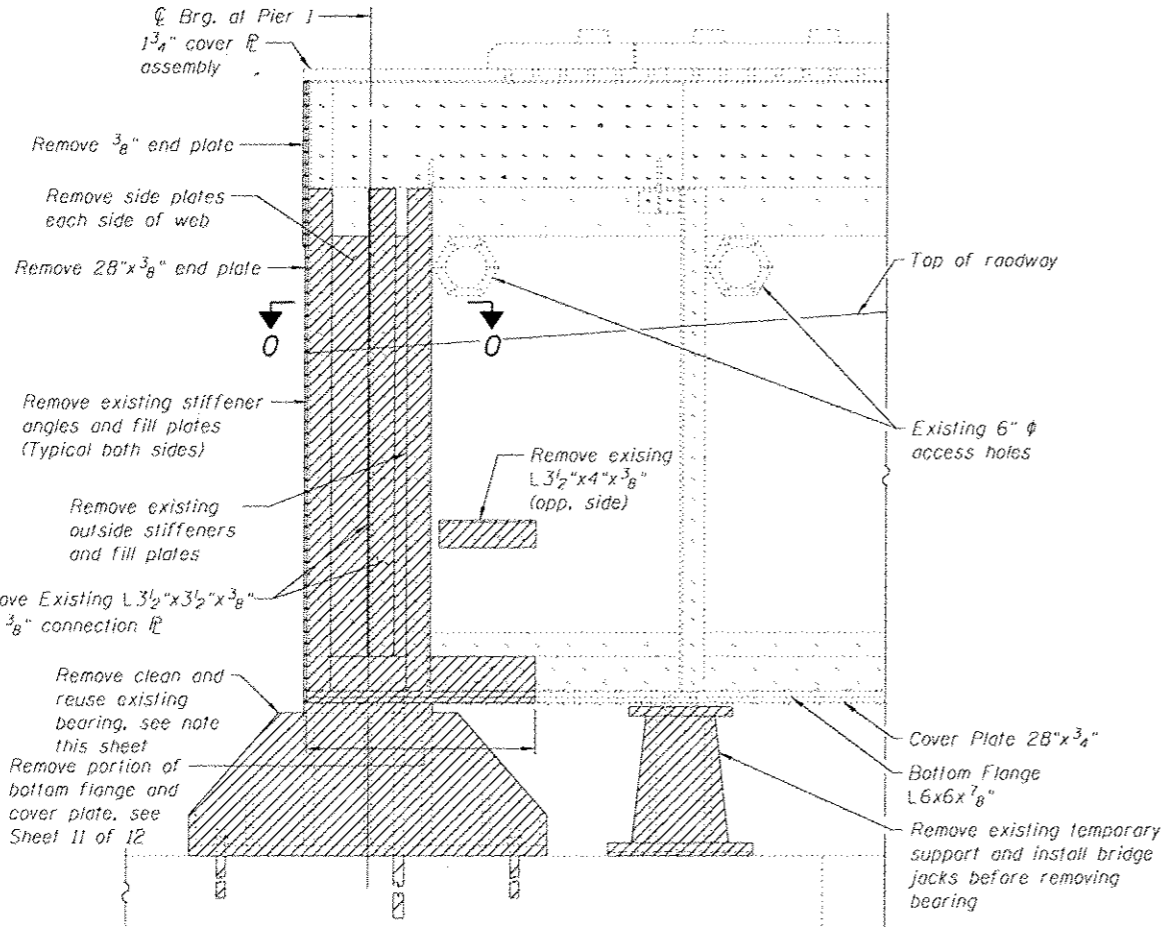
F.A.P. RTE. 607	SECTION 12BR-B-R	COUNTY WILL	TOTAL SHEETS 12	SHEET NO. 6
CONTRACT NO. 62C04			ILLINOIS FED. AID PROJECT	



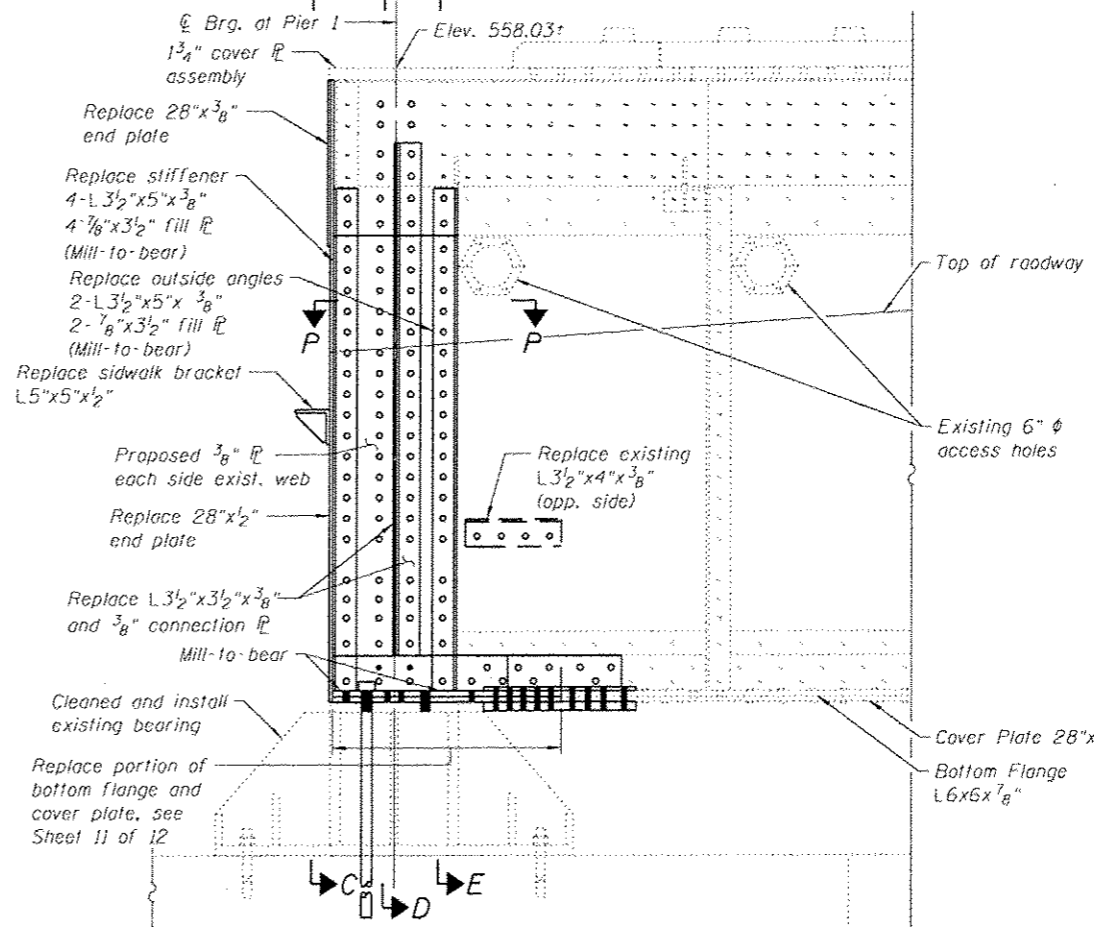
**SECTIONAL PLAN O-O (REMOVAL)
SOUTH TRACK GIRDER END AT PIER 1**



**SECTIONAL PLAN P-P (REHAB)
SOUTH TRACK GIRDER END REPAIR AT PIER 1**



**ELEVATION (REMOVAL)
SOUTH TRACK GIRDER END AT PIER 1
(Looking North)**



**ELEVATION (REHAB)
SOUTH TRACK GIRDER END REPAIR AT PIER 1
(Looking North)**

Notes:
Use existing holes as template to drill new holes in new angles and plates.
See sheet 9 of 12 for Section F-F and Section G-G.
See sheet 10 of 12 for Section C-C, Section D-D and Section E-E.
Match-mark bearing so they are installed in same location and orientation.

T:\169338-1\DOT Various Bridge Insp P\B 172-24\StructCadd\Sheets\0990186-16G-115-007-WTG.dgn 23-JUN-2016 16:44



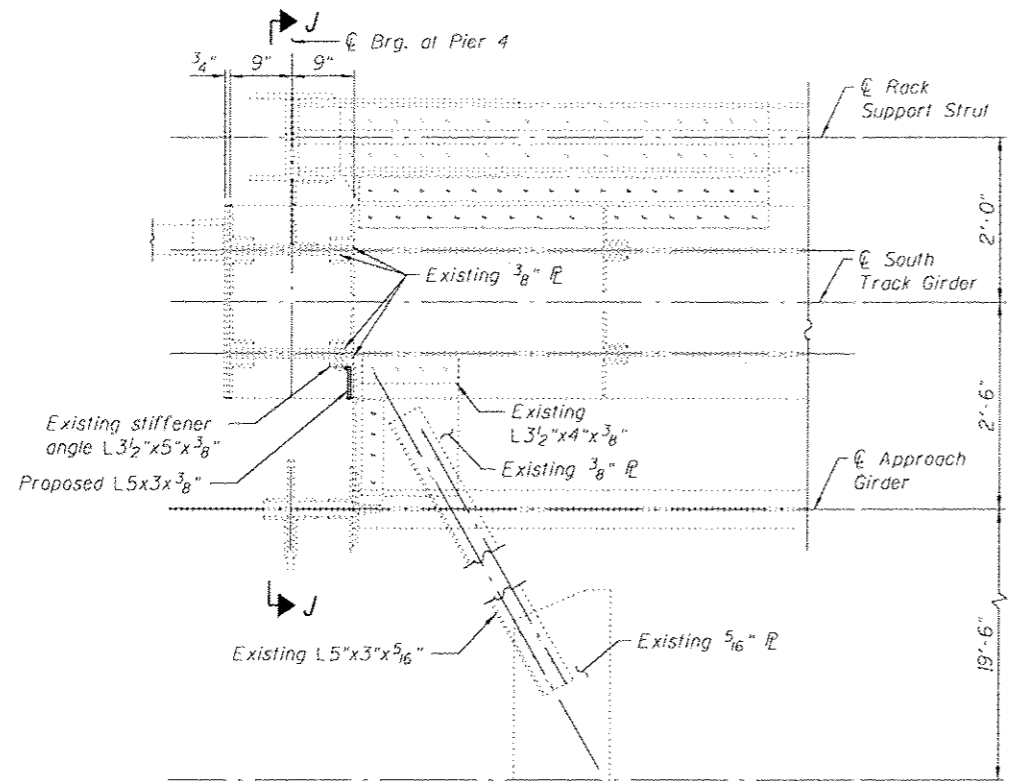
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PLOT DATE - 6/23/2016	DRAWN - LFB	REVISED -
	CHECKED - AH	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

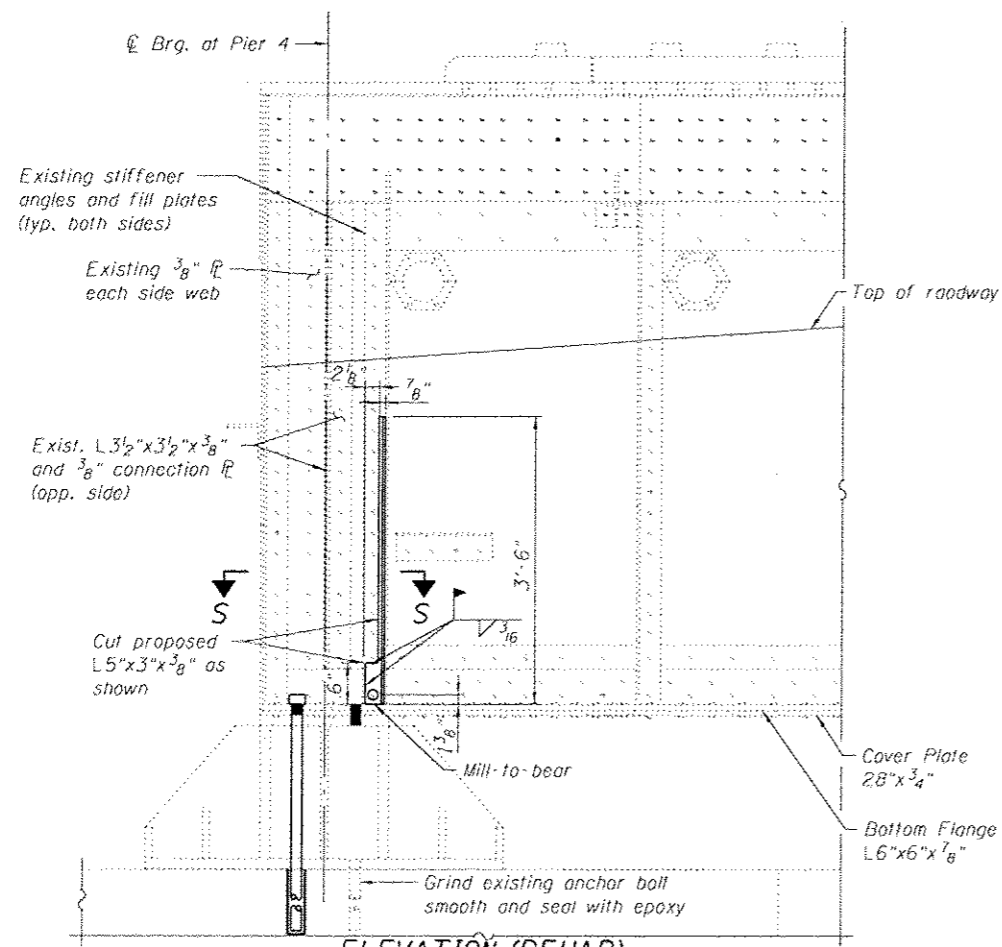
**PLAN AND ELEVATION SOUTH TRACK GIRDER END AT PIER 1
STRUCTURE NO. 099-0166**

SHEET NO. 7 OF 12 SHEETS

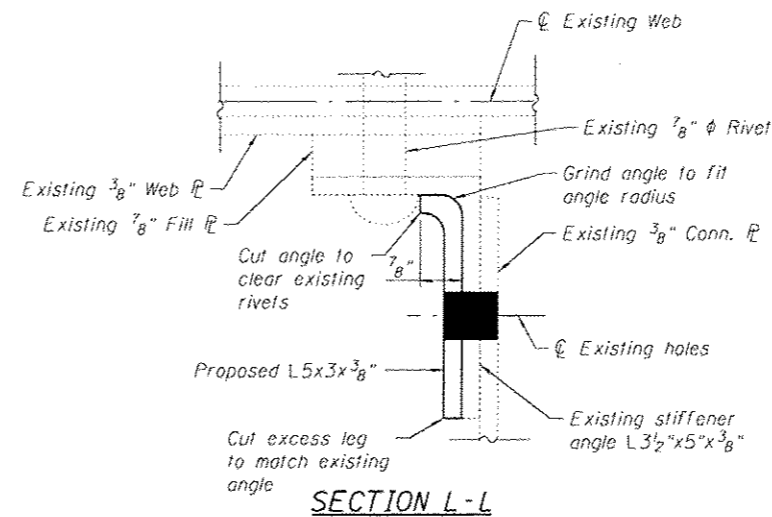
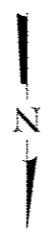
F.A.P. RTE. 607	SECTION 128R-B-R	COUNTY WILL	TOTAL SHEETS 12	SHEET NO. 7
CONTRACT NO. 62C04			ILLINOIS FED. AID PROJECT	



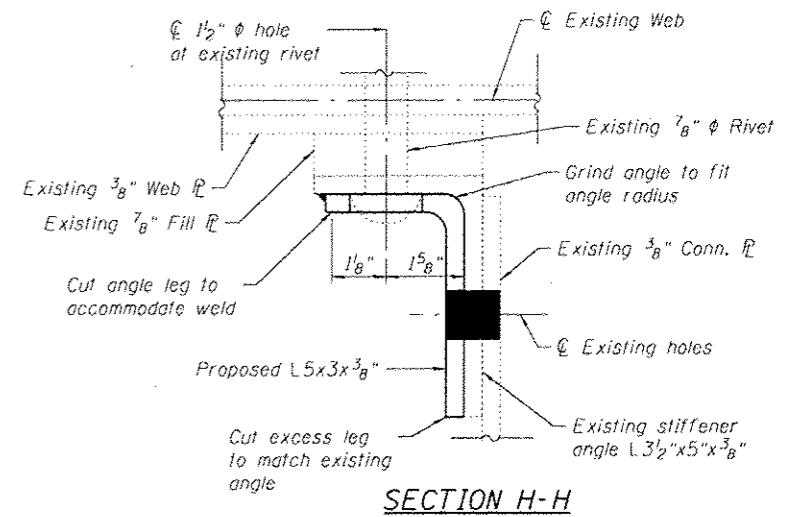
SECTIONAL PLAN S-S (REHAB)
SOUTH TRACK GIRDER END REPAIR AT PIER 4



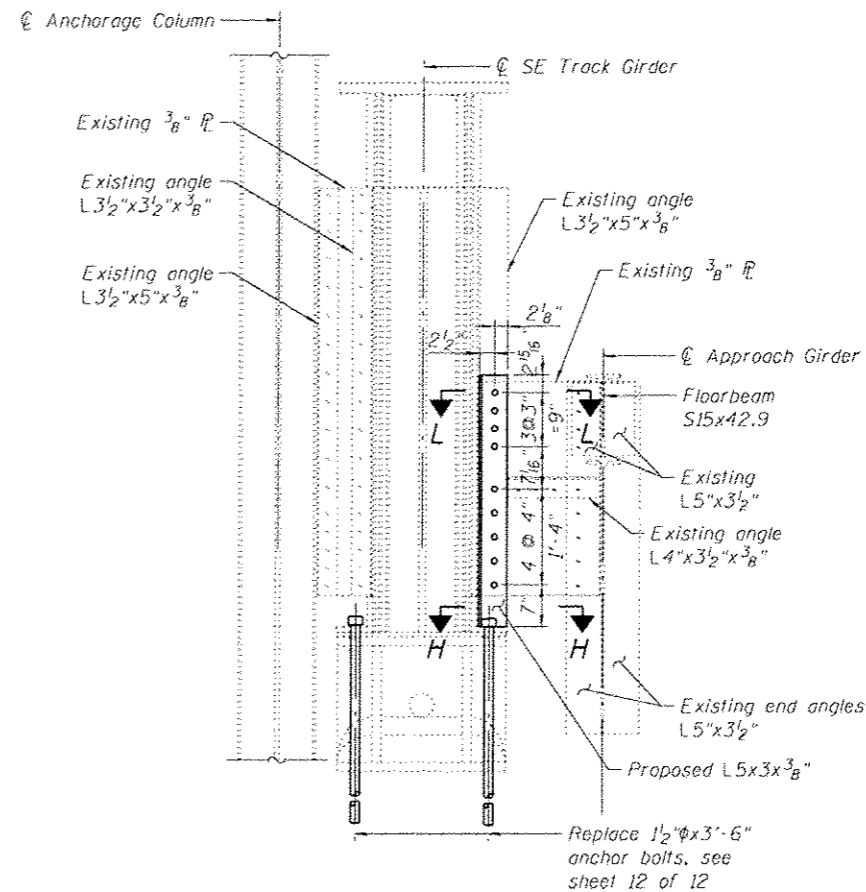
ELEVATION (REHAB)
SOUTH TRACK GIRDER END REPAIR AT PIER 4
 (Looking South)



SECTION L-L



SECTION H-H

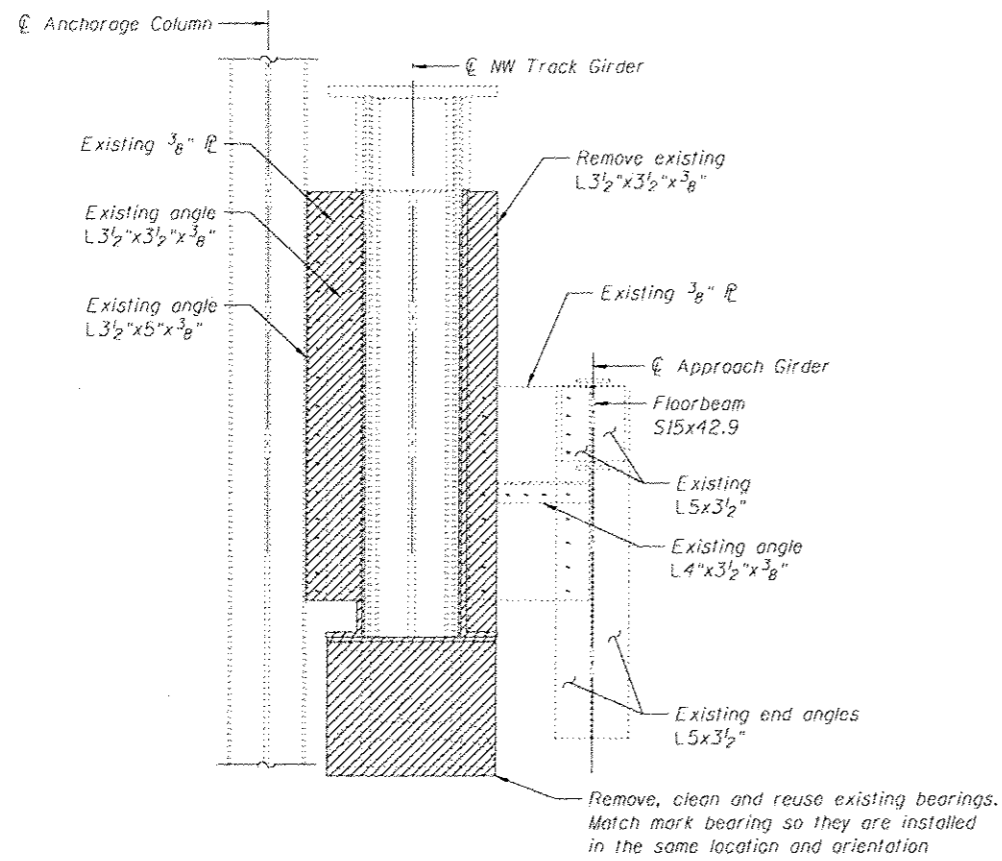


SECTION J-J
 (Repairs)

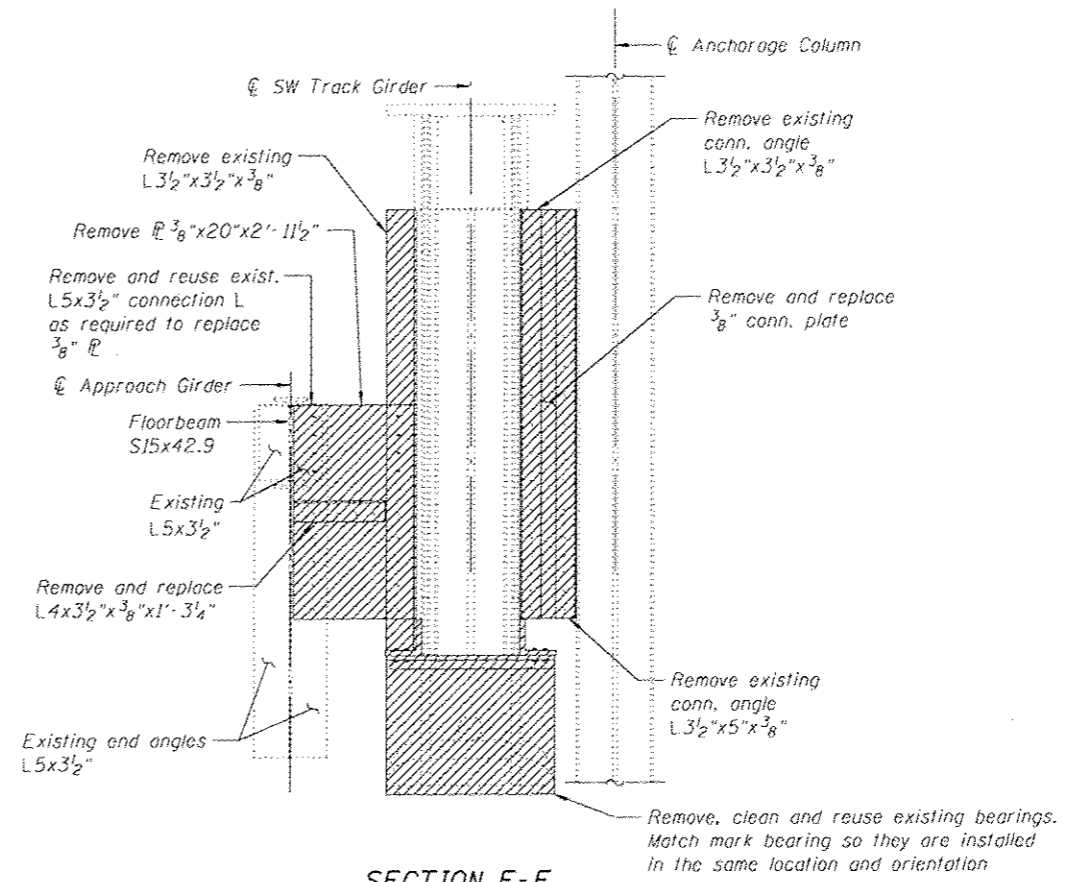
Notes:
 Use existing holes as template to drill new holes in new angle.

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 23 JUN 2016 16:44

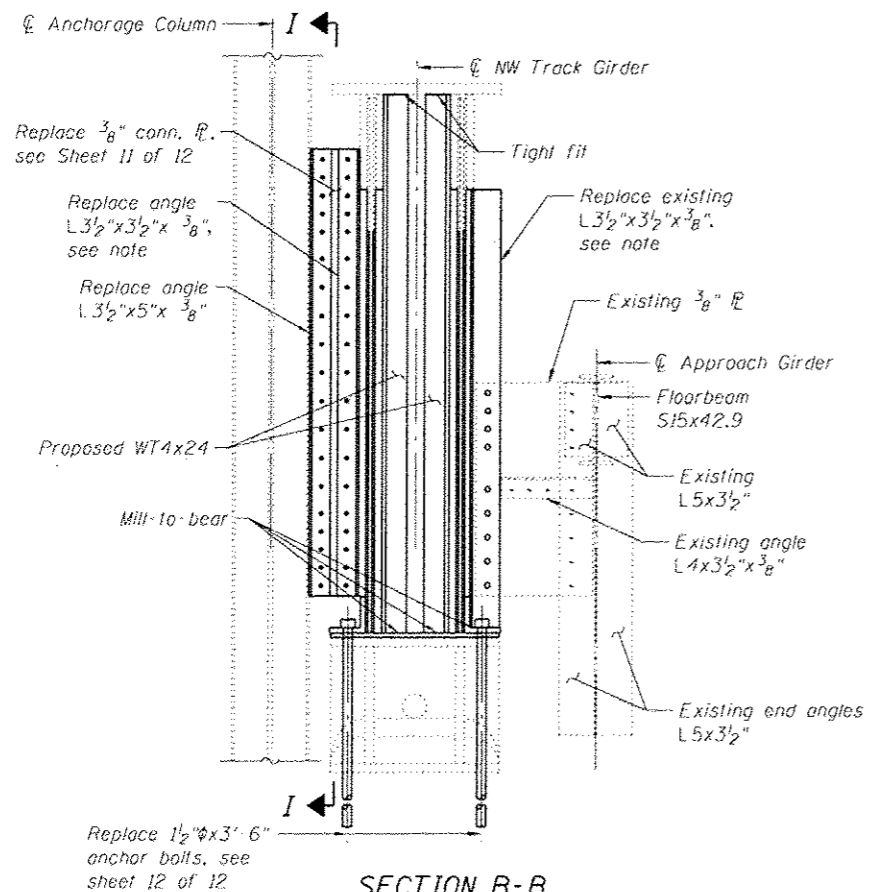
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	PL01 DATE = 6/23/2016	DRAWN = LFB	REVISED =							ILLINOIS FED. AID PROJECT



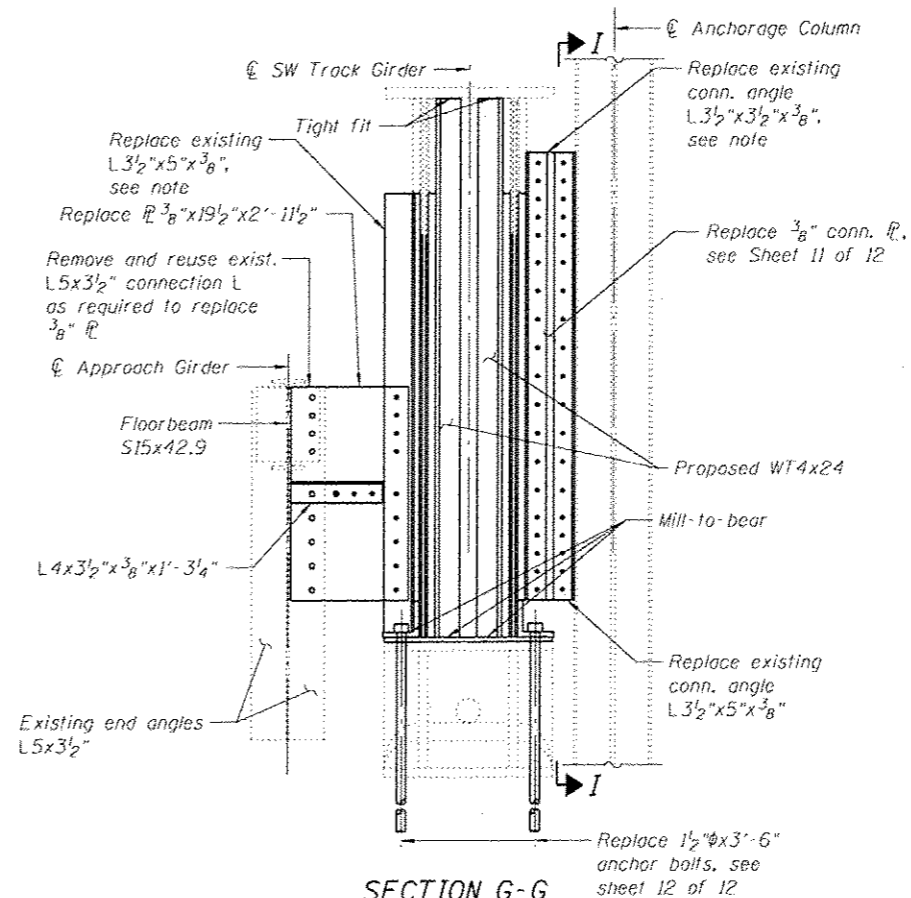
SECTION A-A
(Removal)



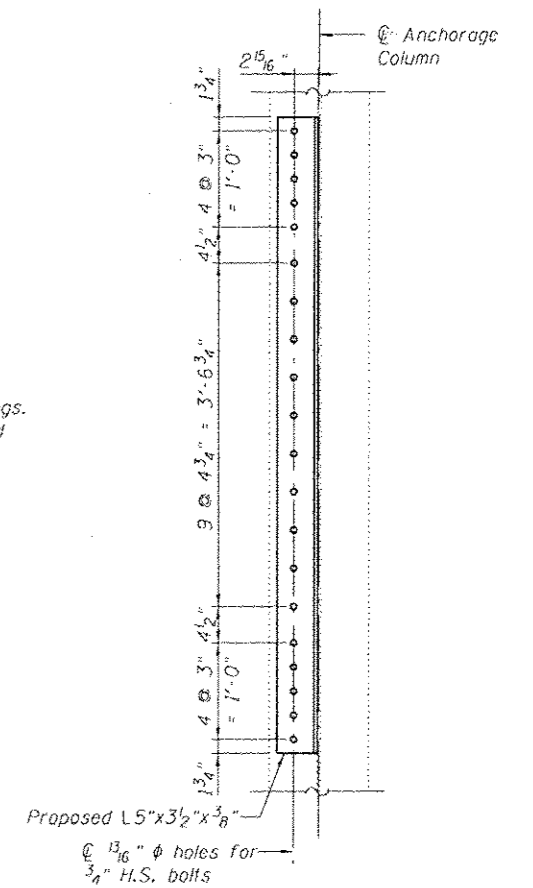
SECTION F-F
(Removal)



SECTION B-B
(Repairs)



SECTION G-G
(Repairs)



SECTION I-I

Notes:
For location of Section A-A & B-B, see sheet 6 of 12.
For location of Section F-F & G-G, see sheet 7 of 12.
Connect angle to connection plate after track girder has been set back in place on existing bearing.

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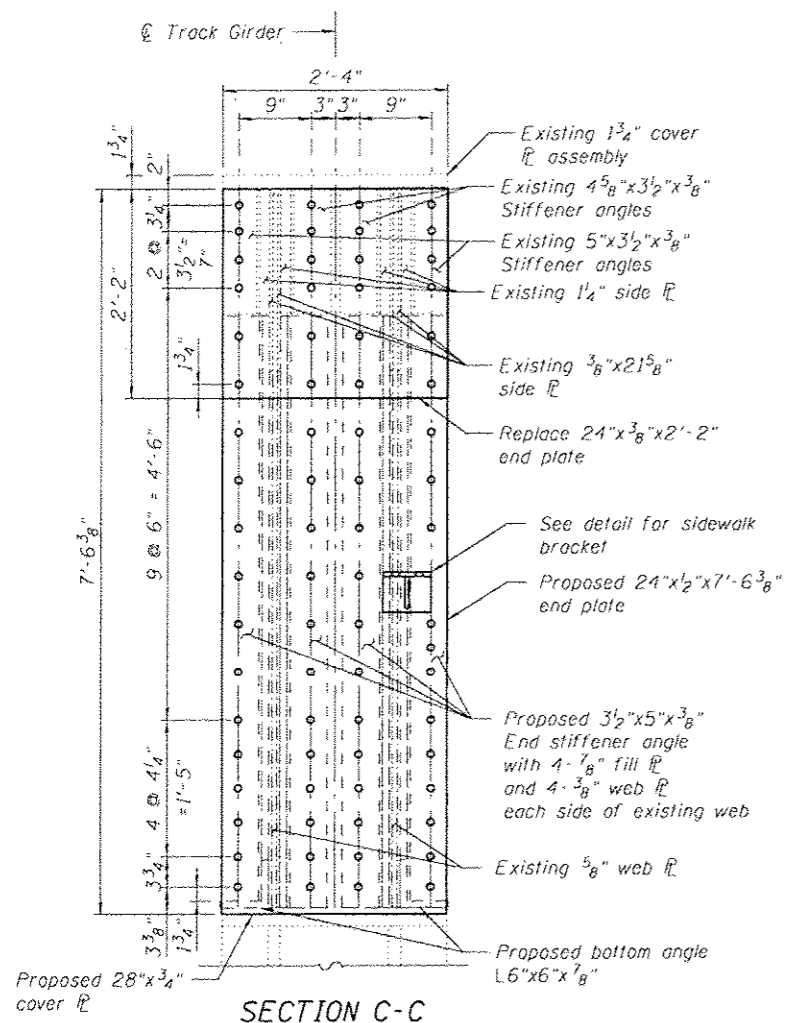
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	CHECKED - AH	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

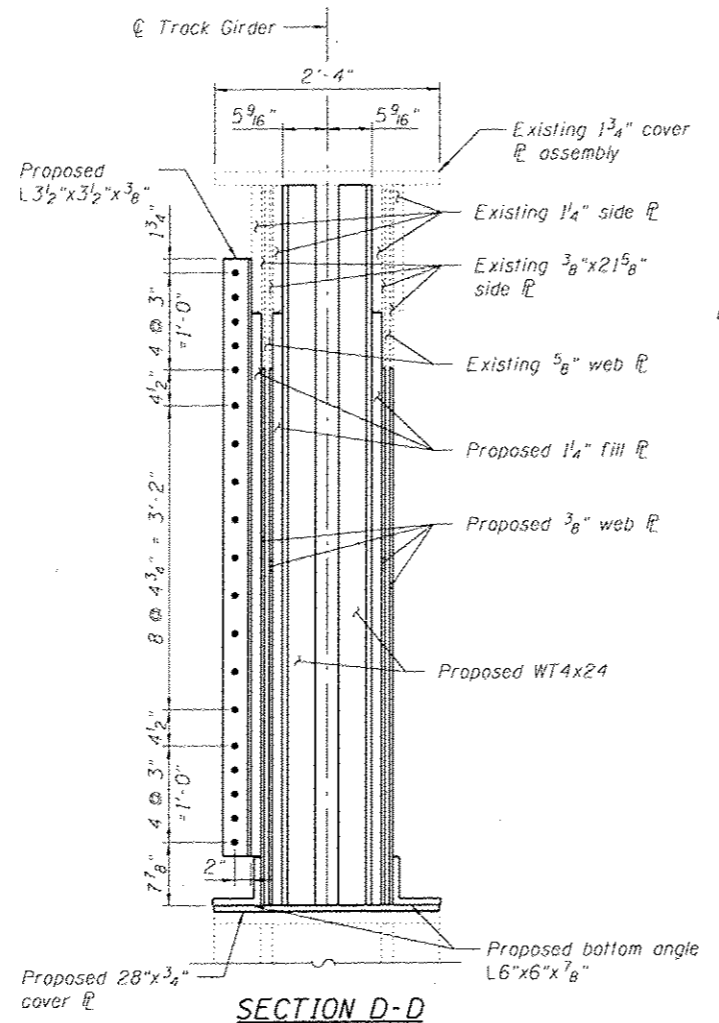
SECTIONS AND DETAILS I
STRUCTURE NO. 099-0166

SHEET NO. 9 OF 12 SHEETS

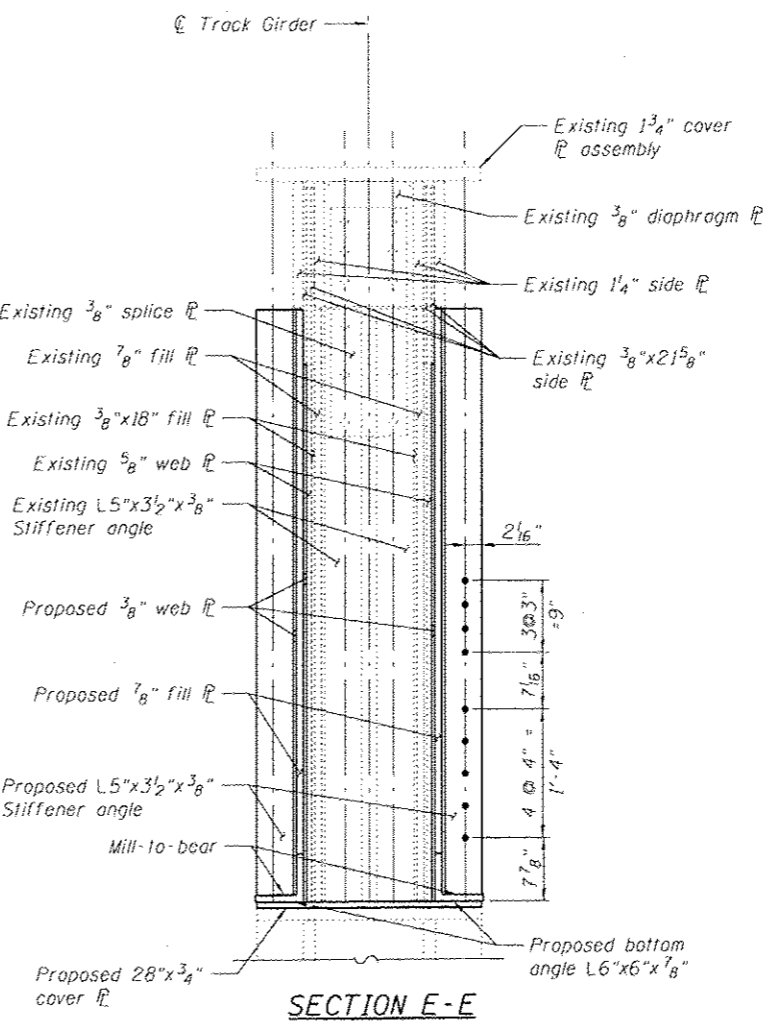
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CONTRACT NO. 62C04			ILLINOIS FED. AID PROJECT	



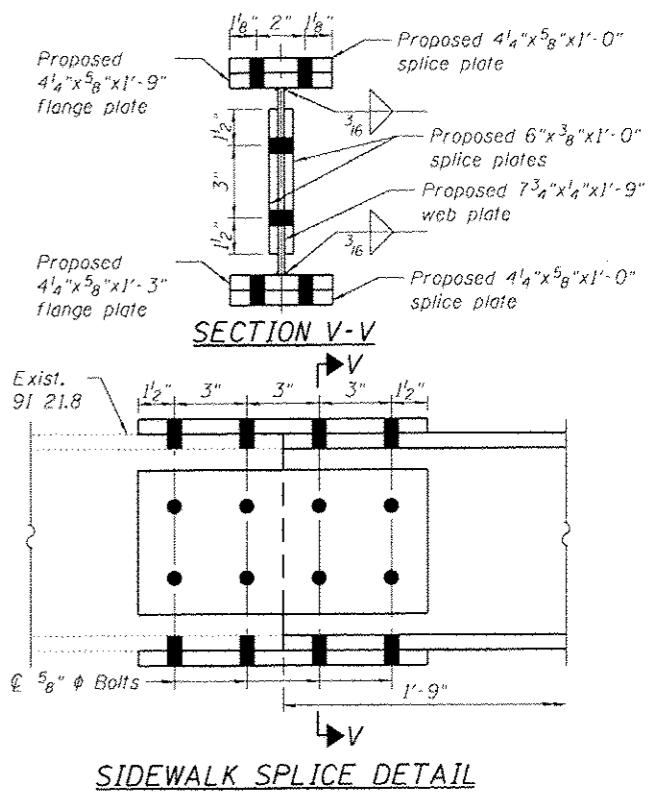
SECTION C-C
(South track girder shown at Pier 1,
north track girder opp. hand)



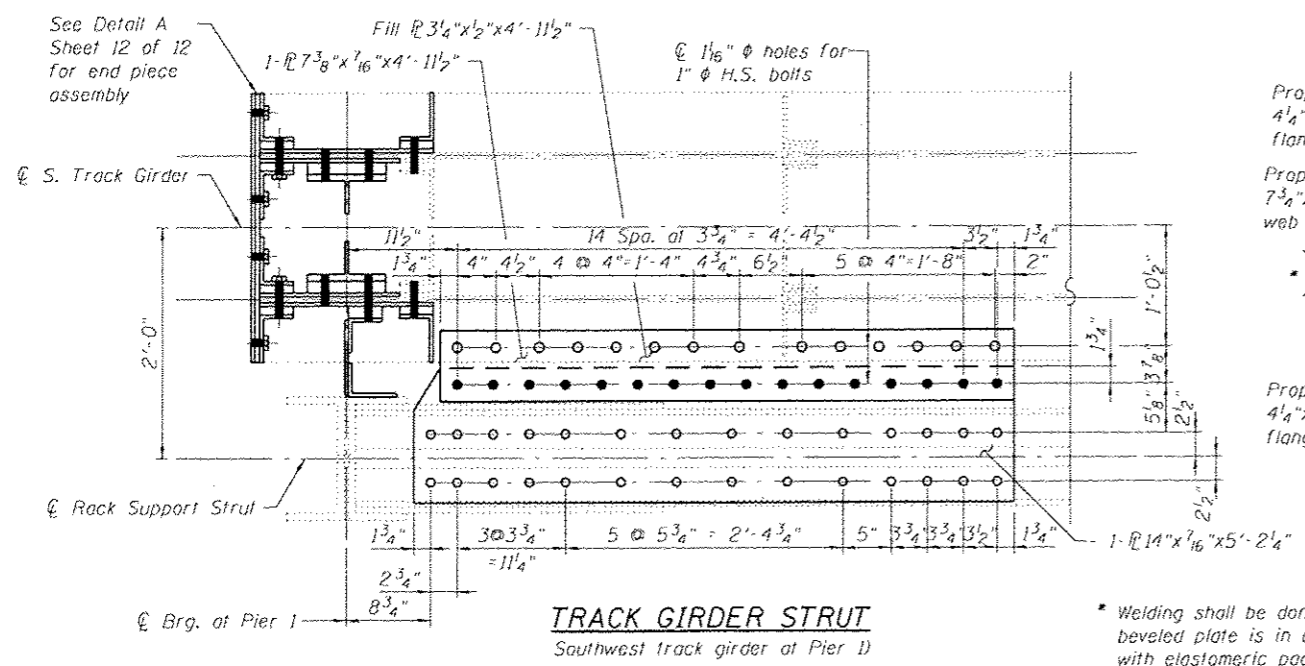
SECTION D-D



SECTION E-E

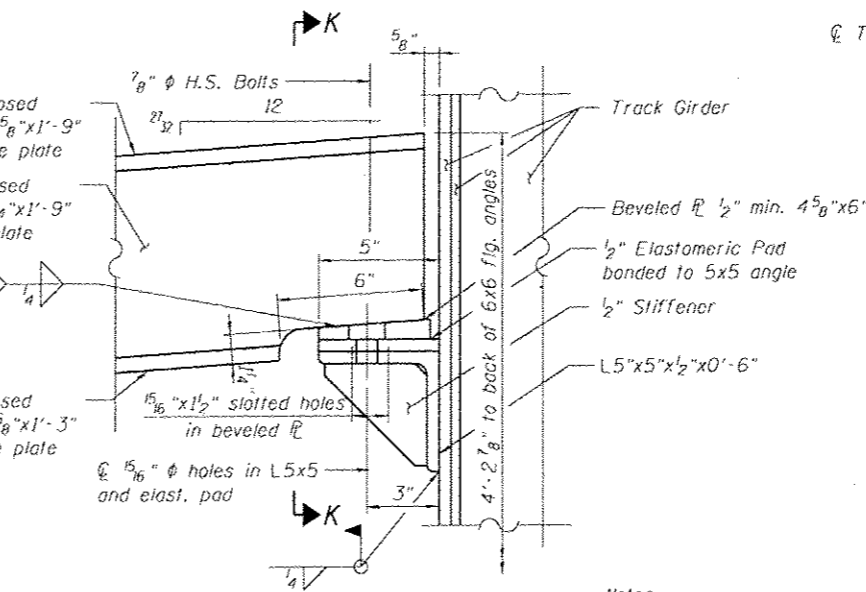


SIDEWALK SPLICE DETAIL

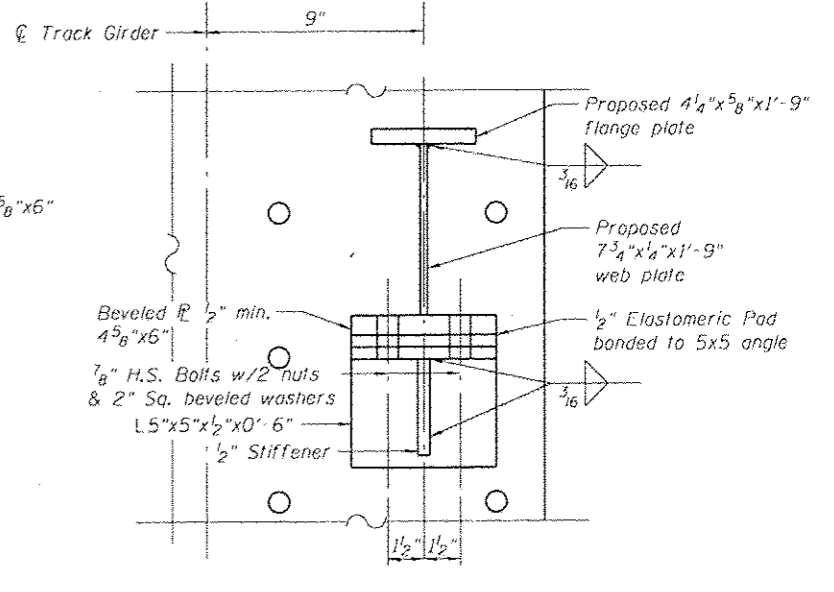


TRACK GIRDER STRUT
Southwest track girder at Pier 1

* Welding shall be done before
beveled plate is in contact
with elastomeric pad.



SIDEWALK BRACKET DETAIL

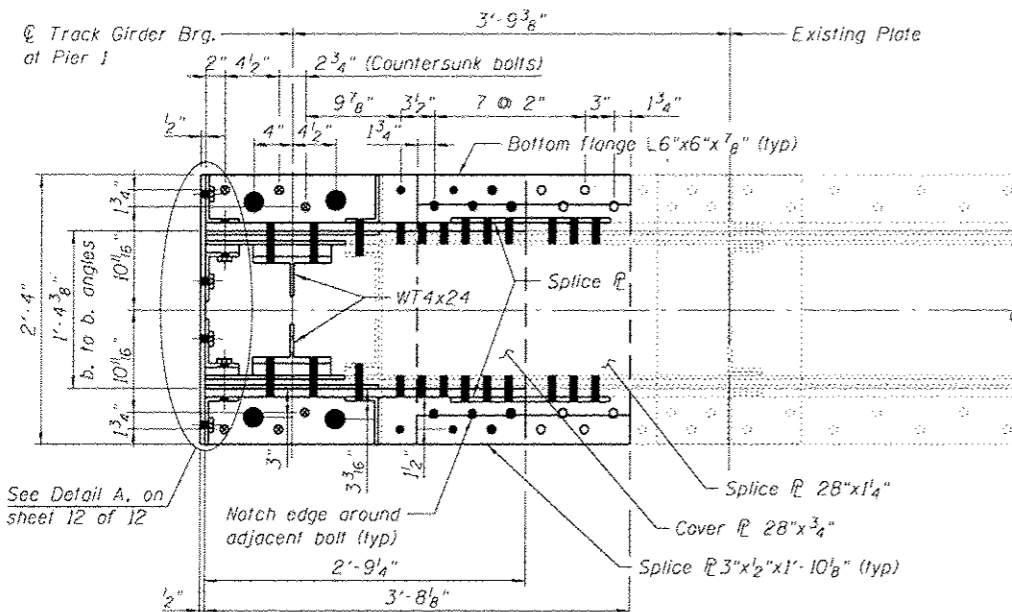


SECTION K-K

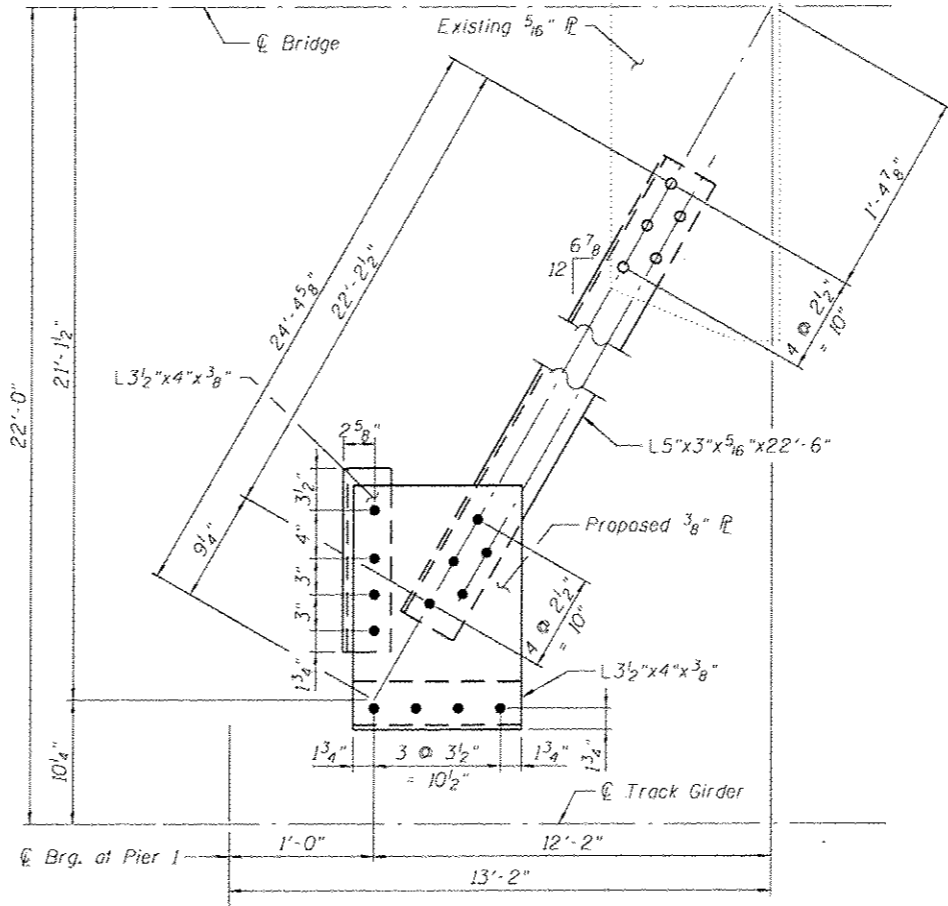
Notes:
For location of Section C-C, D-D &
E-E, see sheet 6 of 12.

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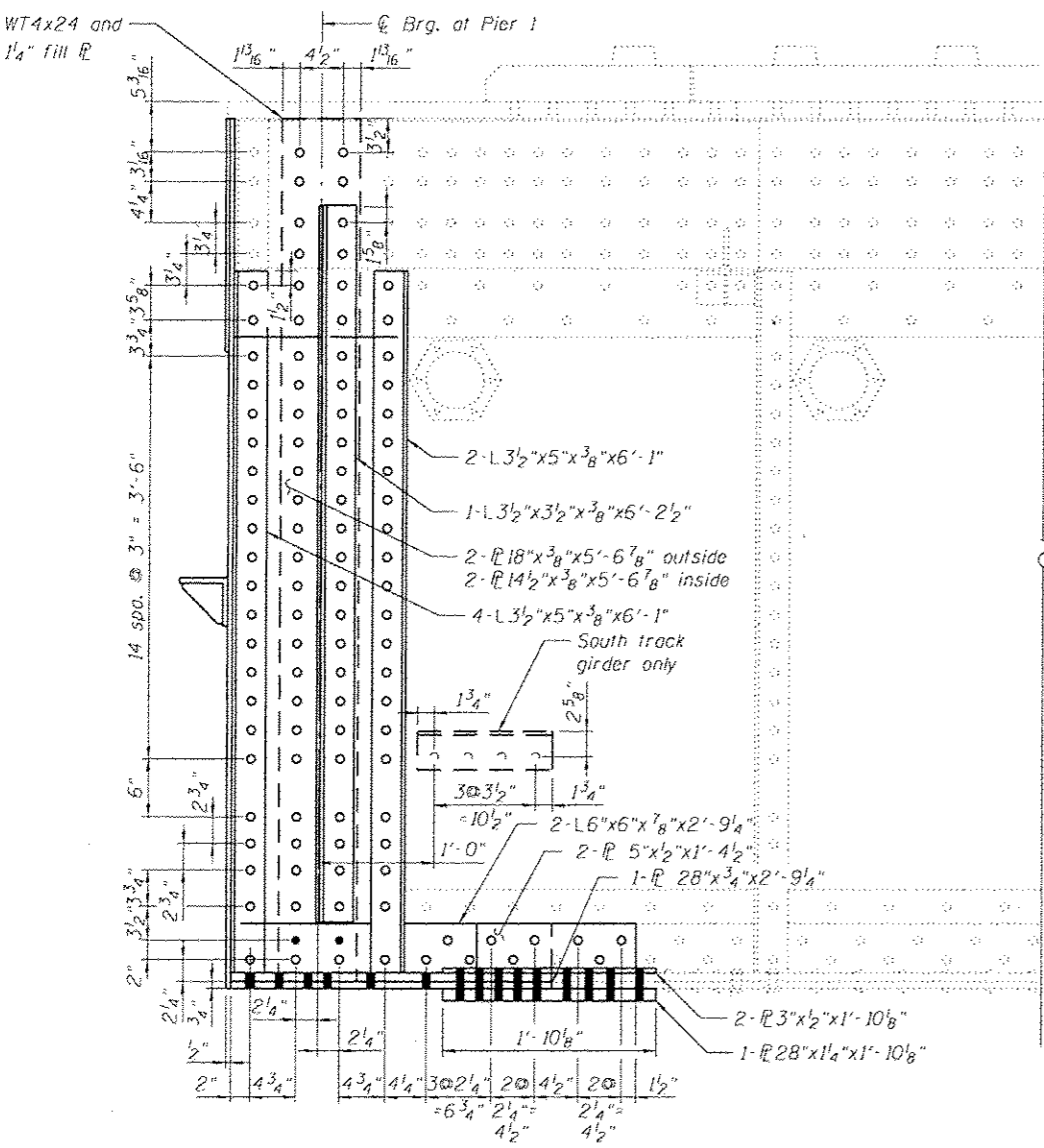
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	PLT SCALE - N.T.S.	CHECKED - MS	REVISD -			607	128R-B-R	WILL	12	10
	PLT DATE - 6/23/2016	CHECKED - AH	REVISD -			CONTRACT NO. 62C04		ILLINOIS FED. AID PROJECT		
						SHEET NO. 10 OF 12 SHEETS				



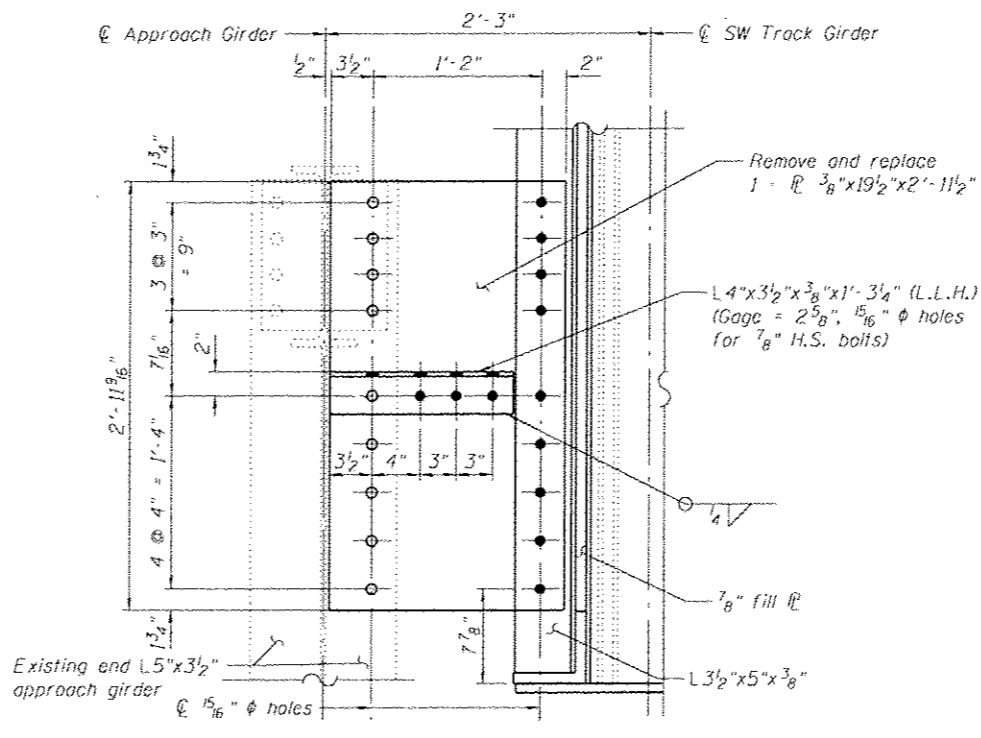
BOTTOM FLANGE DETAIL AT PIER 1



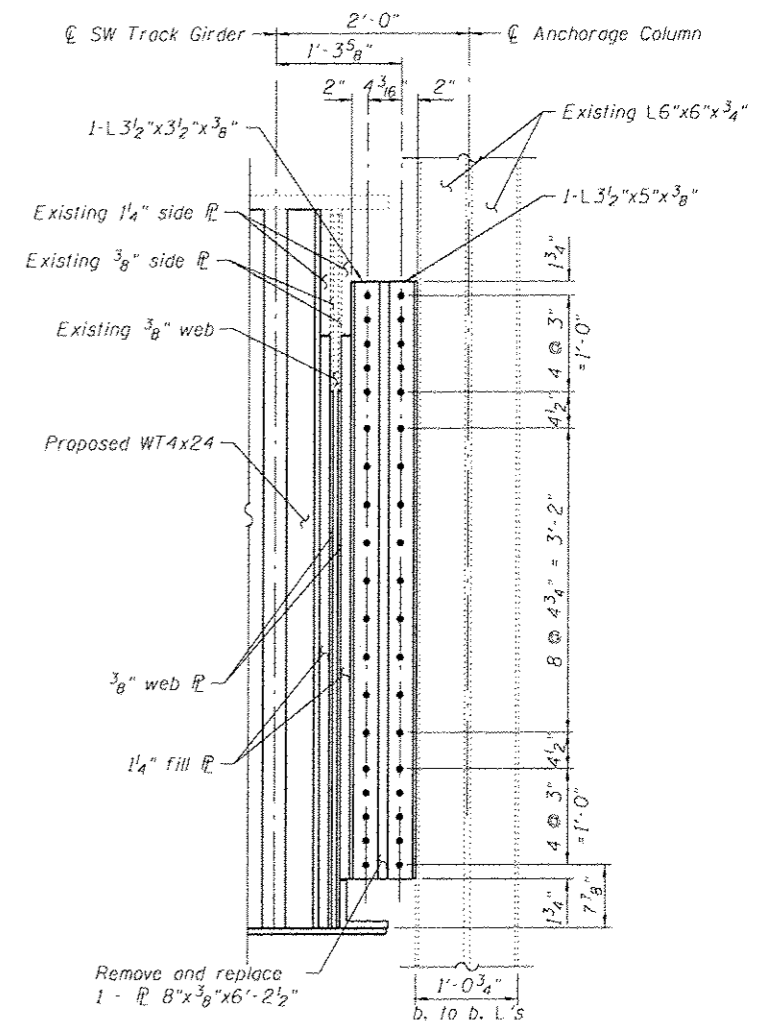
BRACE DETAIL



SOUTH TRACK GIRDER DETAIL AT PIER 1
(North Similar)



APPROACH GIRDER CONNECTION



SW ANCHORAGE COLUMN CONNECTION AT PIER 1
(NW Similar)

T:\168038B\DOT\Various Bridge Insp\PTB 172-24\StructCadd\Sheets\0990166-16G115-011-DET.dgn
 23-JUN-2016 16:44

ROUTE MARKERS

FOR U.S. ROUTES
M1-40-2424

FOR ILLINOIS ROUTES
M1-50-2424

R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

ARROWS SIGNS

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-1-2115

M6-3-2115

CARDINAL DIRECTION & DETOUR SIGNS

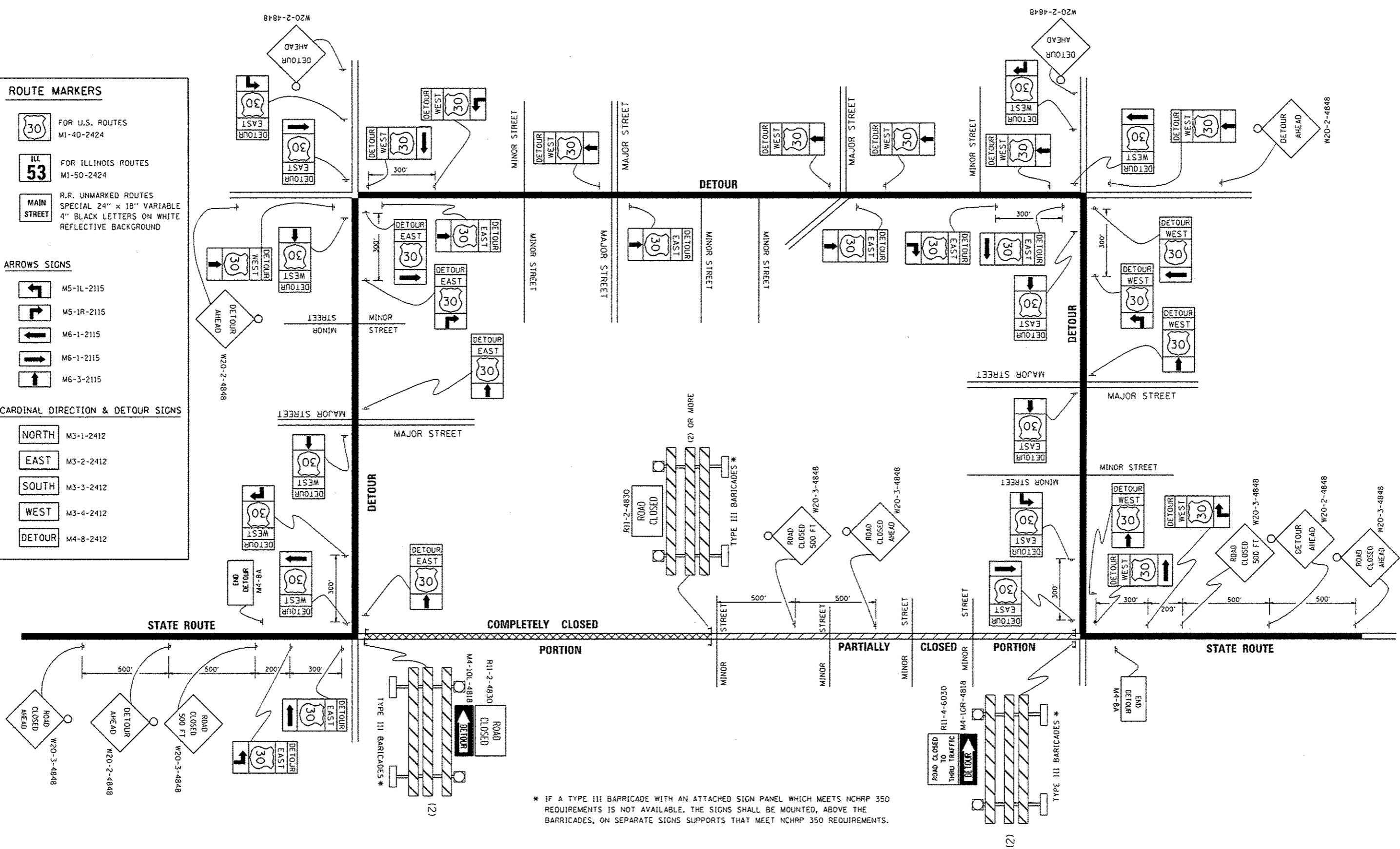
NORTH M3-1-2412

EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

DETOUR M4-8-2412



* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED - 10-18-02
pd:\110846BID\INTEG\Illinois.gov\PI007\00	summers\DOT Offices\Districts\1\Projects\01237\BR&M\Design\DistStd.dgn	CHECKED -	REVISED - R. BORO 09-14-09
PLOT SCALE = 100.0000 / in	DATE -	REVISIONS	
PLOT DATE = 6/2/2016			

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETOUR SIGNING		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FOR CLOSING STATE HIGHWAYS		60T	128R-B-R	WILL	12	128
SCALE: NONE		TC-21		CONTRACT NO. 62C04		
SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	