

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY

VILLAGE OF ITASCA  
PEDESTRIAN IMPROVEMENTS  
SECTION 10-00059-01-SP  
PROJECT SRTS-M-4003(338)  
VILLAGE OF ITASCA  
DUPAGE COUNTY  
JOB NO. C-91-325-14

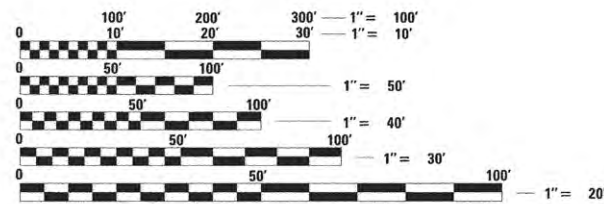
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	10-00059-01-SP	COOK	77	1
ILLINOIS CONTRACT NO. 61C82				



TRAFFIC DATA

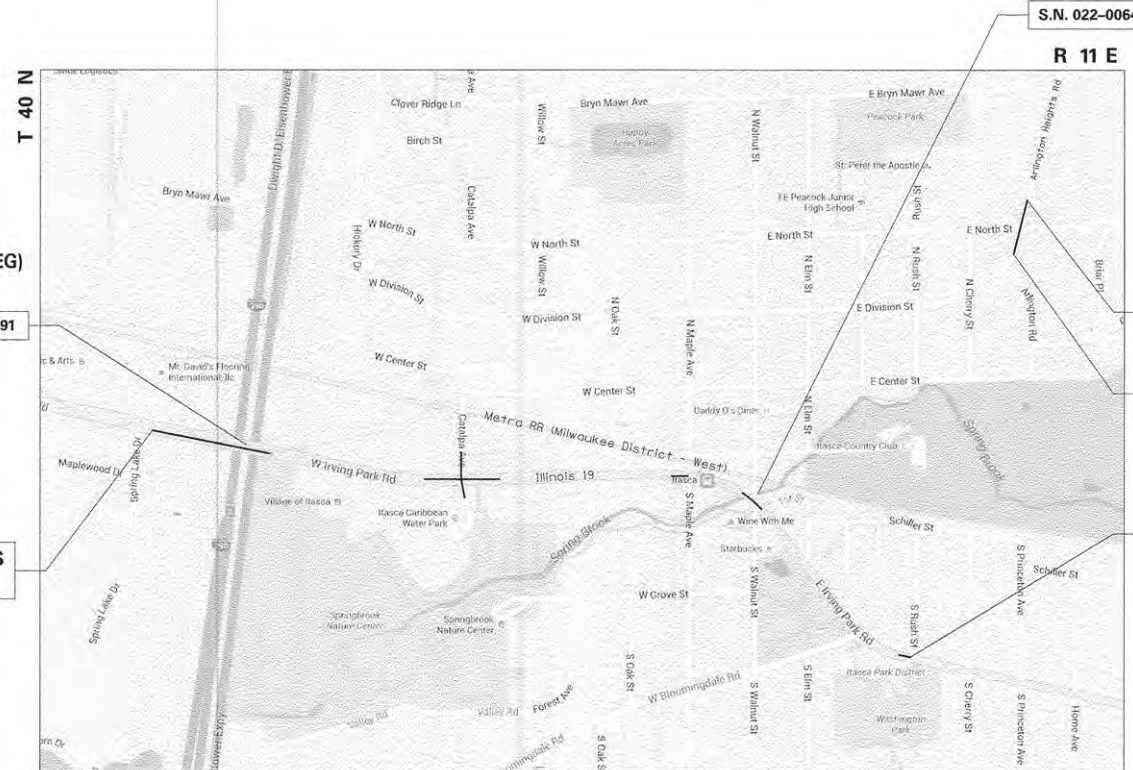
	ADT	POSTED SPEED	FUNCTIONAL CLASSIFICATION
ILLINOIS 19 (IRVING PARK RD)	12800 (2013)	35 WB /30 EB MPH	MINOR ARTERIAL
CATALPA AVENUE	1950 (2012)	25 MPH	LOCAL
ARLINGTON HEIGHTS ROAD	4900 (2011)	35 NB /30 SB MPH	COLLECTOR
WALNUT STREET	4250 (2013)	25 MPH	MAJOR COLLECTOR (N LEG)

FUNCTIONAL CLASSIFICATION  
MINOR ARTERIAL  
LOCAL  
COLLECTOR  
MAJOR COLLECTOR (N LEG)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811



PROJECT BEGINS  
STA. 10+12.74

PROJECT ENDS  
STA. 16+81.00

PROJECT BEGINS  
STA. 13+25.00

PROJECT ENDS  
STA. 62+31.94

GROSS LENGTH = 6049 FT. = 1.15 MILE  
NET LENGTH = 2683 FT. = 0.51 MILE

PROJECT IS LOCATED  
IN VILLAGE OF ITASCA

**DANIEL B. DREW**  
REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS  
Signed: [Signature]  
Daniel B. Drew, P.E. Il. Lic. No. 062-055312  
Expires 11-30-2017  
Date: 3-14-16

**SPIROS PANTAZIS**  
REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS  
Signed: [Signature]  
Spiros Pantazis, S.E. Il. Lic. No. 081-006448  
Expires 11-30-2016  
Date: 3/14/16  
For drawings 64 thru 68

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED: 3-28 2016  
[Signature]  
VILLAGE OF ITASCA, PUBLIC WORKS SUPERVISOR

PASSED: APRIL 8 2016  
[Signature]  
CHRISTOPHER HOLT  
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID  
BASED ON LIMITED  
REVIEW: April 11 2016  
[Signature]  
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

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OF THE STATE OF ILLINOIS

FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E., PTOE (847) 705-4021, SCHAUMBURG, IL

**GENERAL NOTES**

- THESE PLANS WERE PREPARED USING THE ENGLISH SYSTEM OF MEASUREMENTS. ANY REFERENCES TO THE METRIC SYSTEM OF MEASUREMENT, AS IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, SHALL NOT BE USED FOR THIS PROJECT.
- THE SCALES SHOWN ON THE DRAWINGS APPLY ONLY TO FULL SIZE PLANS, NOT TO THE INSETS AND DETAILS OR REDUCED SIZE PLANS. THE CONTRACTOR SHALL NOT SCALE THESE PLANS.
- ANY REFERENCE TO "STANDARD SPECIFICATIONS" THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO MEAN ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION," LATEST EDITION.
- THE INFORMATION SHOWN ON THE DRAWINGS IS BASED ON FIELD SURVEYS PROVIDED BY THE AMERICAN SURVEYING AND ENGINEERING, P.C. AND EXISTING PLAN DATA PROVIDED BY IDOT. THE ENGINEER AND LANDSCAPE ARCHITECT DO NOT GUARANTEE THE COMPLETENESS OR ACCURACY OF THE UTILITY INFORMATION INDICATED. THE CONTRACTOR SHALL MAKE ITS OWN INVESTIGATION REGARDING THE EXISTENCE AND LOCATION OF UTILITIES OR OTHER STRUCTURES. THE CONTRACTOR SHALL CALL J.U.L.I.E. AT 1-800-892-0123 AND THE VILLAGE OF ITASCA PUBLIC WORKS AT 630-773-2455 48 HOURS IN ADVANCE OF ANY CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.
- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED APRIL 1, 2016; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (IMUTCD); THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.
- NO PAYMENT WILL BE MADE FOR RESTORATION BEYOND THE LIMITS SHOWN ON THE PLANS.
- THE CONTRACTOR SHALL VISIT THE SITE AND BECOME FAMILIAR WITH THE WORK. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR FOR FIELD VERIFICATION AND NOTIFICATION TO THE ENGINEER OF ANY DISCREPANCY BETWEEN HIS FIELD NOTES AND THESE PLANS PRIOR TO SUBMITTING THE BID. IF DURING THE CONSTRUCTION, THE CONTRACTOR FINDS ANY DISCREPANCY BETWEEN THE FIELD AND THESE PLANS, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REPORT IT TO THE ENGINEER BEFORE PROCEEDING WITH THE WORK IN THAT AREA.
- THE CONTRACTOR SHALL OBTAIN THE VILLAGE OF ITASCA APPROVAL FOR HIS STORAGE AREA, TRAILER LOCATION, AND STAGING AREAS PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 AND THE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR AT (847) 705-4155 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT AND PROVIDE ACCESS TO ADJUTING PROPERTY, UTILITIES, PEDESTRIANS, AND VEHICULAR TRAFFIC.
- TRAFFIC SHALL BE MAINTAINED ON ALL STREETS AND PARKING SHALL BE PROHIBITED WITHIN FIFTY FEET OF THE CONSTRUCTION AREA AT ALL TIMES. THE CONTRACTOR SHALL CONTACT THE VILLAGE OF ITASCA PUBLIC WORKS AT 773-773-2455 48 HOURS BEFORE COMMENCING CONSTRUCTION. ON STREET AND ON SITE PARKING AFFECTED BY CONSTRUCTION ACTIVITIES SHALL BE COORDINATED BY THE CONTRACTOR THROUGHOUT THE CONSTRUCTION DURATION. THIS COORDINATION WILL NOT BE PAID FOR SEPARATELY AND SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
- THE RESIDENT ENGINEER SHALL CONTACT THE NORTH COOK FIELD ENGINEER, AREA TRAFFIC FIELD ENGINEER AT (773) 685-8386 AT LEAST (2) WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING AND PRESERVING THE PROJECT'S SURVEY CONTROL POINTS AND BENCHMARKS. RELOCATING AND REPLACING CONTROL POINTS AND BENCHMARKS SHALL BE THE CONTRACTORS RESPONSIBILITY AT HIS OWN EXPENSE.
- THE CONTRACTOR SHALL TAKE ALL NECESSARY SAFETY PRECAUTIONS TO PROTECT ADJUTING PROPERTY, UTILITIES, PEDESTRIANS, AND VEHICULAR TRAFFIC.
- DURING CONSTRUCTION OPERATIONS, IF ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DUST AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
- THE CONTRACTOR SHALL RESTORE ALL DISTURBED AREAS AND DEBRIS FROM THE SITE FOR COMPLETION OF THE CONTRACT.
- THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S OWN EXPENSE.

- THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING CURING TIME FOR VARIOUS HOT-MIX ASPHALT LIFTS.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- THE CONTRACTOR SHALL SAW CUT FULL DEPTH WITH A CONCRETE SAWING MACHINE, TO PREVENT THE SURFACE FROM SPALLING WHEN THE SURFACE COURSE AND THE BASE COURSE IS BROKEN. THE WORK SHALL BE DONE IN SUCH A MANNER THAT A STRAIGHT JOINT IS SECURED.
- ADA ACCESSIBLE SIDEWALK RAMPS, AS SHOWN IN THE DETAILS, AND DEPRESSED CURBS SHALL BE PROVIDED AT ALL CROSSWALKS.
- ALL MATERIALS FOR THE PROJECT SHALL BE PROVIDED BY THE CONTRACTOR. ALL MATERIALS SHALL BE NEW, WITH MATERIAL CERTIFICATION WITHIN THE PAST 12 MONTHS OF FABRICATION OR AS REQUIRED BY THE ENGINEER.
- CONTRACTOR SHALL TAKE CARE TO AVOID DISTURBING OR DAMAGING TO EXISTING ROADWAY THAT ARE TO REMAIN WITHIN THE VICINITY OF THE CONSTRUCTION OPERATIONS.

**GENERAL NOTES - SEDIMENT AND EROSION CONTROL**

- THE ENGINEER WILL ASSURE THAT EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY WAY. ALL EROSION CONTROL MEASURED SHALL BE INSTALLED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES WHICH WILL POTENTIALLY CREATE ERODIBLE CONDITIONS.
- EROSION CONTROL ITEMS ARE CONSIDERED TO BE HIGH PRIORITY ITEMS ON THIS CONTRACT. QUANTITIES OF THE FOLLOWING ITEMS HAVE BEEN ADDED TO THE CONTRACT FOR USE AT THE DISCRETION OF THE ENGINEER.
- 28000400 PERIMETER EROSION BARRIER (FOOT)
  - 28000510 INLET FILTERS (EACH)
  - 28000500 INLET AND PIPE PROTECTION (EACH)

**INDEX OF SHEETS**

SHEET NUMBER	SHEET TITLE
1	COVER SHEET
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11 - 17	TYPICAL SECTIONS
18	ALIGNMENT, TIES AND BENCHMARKS
19 - 24	REMOVAL PLANS
25 - 30	PROPOSED PLANS
31 - 34	MISCELLANEOUS DETAILS
35 - 40	PAVEMENT MARKING, SIGNING, & LANDSCAPING PLANS
41 - 63	TRAFFIC SIGNAL PLANS AND STANDARD DRAWINGS
64 - 68	STRUCTURAL PLANS
69	BD-01: DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND CURB OR EDGE GREATER THAN OR EQUAL TO 15' (4.5 m)
70	BD-07: STORM SEWER CONNECTION TO EXISTING SEWER
71	BD-24: CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
72	BD-32: BUTT JOINTS AND HMA TAPER
73	BD-34: DETAILS FOR DEPRESSED CURB & GUTTER AND SHOULDER TREATMENT AT TBT TY 1 SPL.
74	TC-10: TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
75	TC-13: DISTRICT ONE TYPICAL PAVEMENT MARKINGS
76	TC-16: PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
77	TC-22: ARTERIAL ROAD INFORMATION SIGN

**INDEX OF STATE STANDARDS**

STD. NO.	TITLE
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
420001-08	PAVEMENT JOINTS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-02	DIAGONAL CURB RAMPS FOR SIDEWALKS
424016-02	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424031-01	MEDIAN PEDESTRIAN CROSSINGS
483001-04	PCC SHOULDER
602011-02	CATCH BASIN, TYPE C
604001-04	FRAME AND LIDS, TYPE 1
604036-03	GRATE, TYPE 8
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
631033-06	TRAFFIC BARRIER TERMINAL, TYPE 6B
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701502-06	URBAN LANE CLOSURE, 2L, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-05	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS
720016-03	MAST ARM MOUNTED STREET NAME SIGNS
728001-01	TELESCOPING STEEL SIGN SUPPORT
731001-01	BASE FOR TELESCOPING STEEL SIGN SUPPORT
780001-05	TYPICAL PAVEMENT MARKINGS
782006-00	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS
805001-01	ELECTRICAL SERVICE INSTALLATION DETAILS
814001-03	HANDHOLES
814006-02	DOUBLE HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
862001-01	UNINTERRUPTABLE POWER SUPPLY (UPS)
873001-02	TRAFFIC SIGNAL GROUNDING AND BONDING
877001-06	STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
878001-10	CONCRETE FOUNDATION DETAILS
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS

FILE NAME = (H:ITASCA\GENERAL NOTES.dgn)	USER NAME = mersobands	DESIGNED = MPG	REVISED =	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS INDEX OF SHEETS, GENERAL NOTES, INDEX OF STATE STANDARDS</b>			F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 2
DATE = 5/20/2016	PLOT SCALE = 1/8"=1'-0"	CHECKED = MBR	REVISED =		SCALE: N.T.S.	SHEET 1	OF 1	SHEETS STA.	TO STA.	CONTRACT NO. 61C82		
					ILLINOIS FED. AID PROJECT							



CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				STP FUNDS	SRTS FUNDS
				80% FED 20% LOCAL	100% FED
				SAFETY 0021 URBAN	SAFETY 0021 URBAN
20200100	EARTH EXCAVATION	CU YD	120	36	84
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	49	48	1
21101505	TOPSOIL EXCAVATION AND PLACEMENT	CU YD	112	27	85
25200110	SODDING, SALT TOLERANT	SQ YD	1068	404	664
28000400	PERIMETER EROSION BARRIER	FOOT	100	100	
28000500	INLET AND PIPE PROTECTION	EACH	5		5
28000510	INLET FILTERS	EACH	10	10	
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	586	167	419
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	34		34
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	5026	1296	3730
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	67		67
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	46	46	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	105	81	24
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	64		64
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	126		126
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	68	68	
42001300	PROTECTIVE COAT	SQ YD	1481	1322	159
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	293		293
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	5410	4533	877

\* = SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				STP FUNDS	SRTS FUNDS
				80% FED 20% LOCAL	100% FED
				SAFETY 0021 URBAN	SAFETY 0021 URBAN
42400800	DETECTABLE WARNINGS	SQ FT	309	279	30
44000100	PAVEMENT REMOVAL	SQ YD	300	273	27
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	2002	809	1193
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	66		66
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	479	479	
44000600	SIDEWALK REMOVAL	SQ FT	2769	1959	810
44003100	MEDIAN REMOVAL	SQ FT	331	331	
44004250	PAVED SHOULDER REMOVAL	SQ YD	767	607	160
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	894	97	797
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	208	62	146
48101620	AGGREGATE SHOULDERS, TYPE B 10"	SQ YD	149	149	
48203021	HOT-MIX ASPHALT SHOULDERS, 6"	SQ YD	145		145
48300700	PORTLAND CEMENT CONCRETE SHOULDERS 12"	SQ YD	70	70	
* 50104000	BRIDGE RAIL REMOVAL	FOOT	259	259	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	34.4	34.4	
50300300	PROTECTIVE COAT	SQ YD	123	123	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	5580	5580	
* 50901730	BRIDGE FENCE RAILING	FOOT	259	259	
* 50901750	PARAPET RAILING	FOOT	519	519	

\* = SPECIALTY ITEM

FILE NAME =  
D1-ITASCA\_S00\_SHT02.dgn  
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DESIGNED - MBR  
DRAWN - MBR  
PLOT SCALE = 28.0000' / in.  
PLOT DATE = 5/20/2016

REVISED -  
REVISED -  
CHECKED - MPG  
DATE - 5/20/2016  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET 2 OF 8 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	10-00059-01-SP		77	4
			DUPAGE	CONTRACT NO. 61C82
ILLINOIS FED. AID PROJECT				



CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				STP FUNDS	SRTS FUNDS
				80% FED 20% LOCAL	100% FED
				SAFETY	SAFETY
				0021	0021
URBAN	URBAN				
550B0050	STORM SEWERS, CLASS B, TYPE 1 12"	FOOT	16		16
55100200	STORM SEWER REMOVAL 6"	FOOT	15		15
55100500	STORM SEWER REMOVAL 12"	FOOT	10	10	
60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	1	1	
60207005	CATCH BASINS, TYPE C, TYPE 1 FRAME, CLOSED LID	EACH	2		2
60207605	CATCH BASINS, TYPE C, TYPE 8 GRATE	EACH	1		1
60255500	MANHOLES TO BE ADJUSTED	EACH	5	1	4
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	1	1	
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	10	5	5
60300405	VALVE BOX FRAMES TO BE ADJUSTED	EACH	1	1	
60500050	REMOVING CATCH BASINS	EACH	3	1	2
60600605	CONCRETE CURB, TYPE B	FOOT	277.0	261.0	16.0
60603900	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (ABUTTING EXISTING PAVEMENT)	FOOT	386.0	358.5	27.5
60605100	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (ABUTTING EXISTING PAVEMENT)	FOOT	92.5	92.5	
60608600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06	FOOT	19.5	19.5	
60610400	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24	FOOT	110.0	110.0	
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	135	135	
60619200	CONCRETE MEDIAN, TYPE SB-6.06	SQ FT	166		166
60619600	CONCRETE MEDIAN, TYPE SB-6.12	SQ FT	1312	1312	

\* = SPECIALTY ITEM

FILE NAME =  
D1-ITASCA\_S00\_SHT03.dgn  
Default

USER NAME = mrichards  
DESIGNED - MBR  
DRAWN - MBR  
PLOT SCALE = 28.0000' / 1" =  
CHECKED - MPG  
DATE - 5/20/2016  
PLOT DATE = 5/20/2016

REVISED -  
REVISED -  
REVISED -  
REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET 3 OF 8 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	10-00059-01-SP	DUPAGE	77	5
			CONTRACT NO.	61C82
[ILLINOIS] FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				STP FUNDS	SRTS FUNDS
				80% FED 20% LOCAL	100% FED
				SAFETY 0021 URBAN	SAFETY 0021 URBAN
* 63100089	TRAFFIC BARRIER TERMINAL, TYPE 6B	EACH	1	1	
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	1	1	
63700155	CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT	FOOT	425	425	
63700900	CONCRETE BARRIER BASE	FOOT	425	425	
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	215	215	
* 66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1	
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	4	4	
67100100	MOBILIZATION	LSUM	1	1	
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	LSUM	1	1	
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	LSUM	1	1	
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1	
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1	
* 72000100	SIGN PANEL - TYPE 1	SO FT	152	106	46
* 72000200	SIGN PANEL - TYPE 2	SO FT	33	33	
* 72400310	REMOVE SIGN PANEL - TYPE 1	SO FT	22		22
* 72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	34	21	13
* 72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	EACH	58	37	21
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	1	1	
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	215	159	56

\* = SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				STP FUNDS	SRTS FUNDS
				80% FED 20% LOCAL	100% FED
				SAFETY 0021 URBAN	SAFETY 0021 URBAN
* 73100100	BASE FOR TELESCOPING STEEL SIGN SUPPORT	EACH	8	4	4
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	111	111	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	6116	4210	1906
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	733	733	
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	59	59	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1344	992	352
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	227	199	28
* 78200530	BARRIER WALL MARKERS, TYPE C	EACH	6	6	
* 80500020	SERVICE INSTALLATION - POLE MOUNTED	EACH	1	1	
* 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	755	755	
* 81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	139	139	
* 81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	54	54	
* 81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	296	296	
* 81400100	HANDHOLE	EACH	3	3	
* 81400200	HEAVY-DUTY HANDHOLE	EACH	5	5	
* 81400300	DOUBLE HANDHOLE	EACH	2	2	
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2	2	
* 85100500	PAINT NEW TRAFFIC SIGNAL POST	EACH	1	1	
* 85100600	PAINT NEW MAST ARM AND POLE, UNDER 40 FOOT	EACH	4	4	

\* = SPECIALTY ITEM

FILE NAME =  
DI-ITASCA\_S00\_SH105.dgn  
Default

USER NAME = mrichards	DESIGNED - MBR	REVISED -
	DRAWN - MBR	REVISED -
PLOT SCALE = 28.0000' / in.	CHECKED - MPC	REVISED -
PLOT DATE = 5/28/2016	DATE - 5/20/2016	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET 5 OF 8 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	10-00059-01-SP	DUPAGE	77	7
CONTRACT NO. 61C82			ILLINOIS FED. AID PROJECT	



CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				STP FUNDS 80% FED 20% LOCAL SAFETY 0021 URBAN	SRTS FUNDS 100% FED SAFETY 0021 URBAN
* 86400100	TRANSCEIVER - FIBER OPTIC	EACH	1	1	
* 87300925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	2091	2091	
* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1314	1314	
* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1851	1851	
* 87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	787	787	
* 87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1184	1184	
* 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	2680	2680	
* 87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	72	72	
* 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	1830	1830	
* 87502440	TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	3	3	
* 87700160	STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.	EACH	1	1	
* 87700170	STEEL MAST ARM ASSEMBLY AND POLE, 26 FT.	EACH	1	1	
* 87700190	STEEL MAST ARM ASSEMBLY AND POLE, 30 FT.	EACH	1	1	
* 87700210	STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.	EACH	1	1	
* 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	40	40	
* 87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4	4	
* 87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	47	47	
* 87900200	DRILL EXISTING HANDHOLE	EACH	3	3	
* 88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4	4	

\* = SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				STP FUNDS	SRTS FUNDS
				80% FED 20% LOCAL	100% FED
				SAFETY	SAFETY
				0021	0021
				URBAN	URBAN
* 88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2	2	
* 88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4	4	
* 88030240	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	2	2	
* 88102710	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED	EACH	5	5	
* 88102740	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED	EACH	1	1	
* 88102747	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4	4	
* 88102760	PEDESTRIAN SIGNAL HEAD, LED, 3-FACE, BRACKET MOUNTED	EACH	1	1	
* 88200410	TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	8	8	
* 88500100	INDUCTIVE LOOP DETECTOR	EACH	8	8	
* 88600100	DETECTOR LOOP, TYPE I	FOOT	953	953	
* 88700200	LIGHT DETECTOR	EACH	4	4	
* 88700300	LIGHT DETECTOR AMPLIFIER	EACH	1	1	
* 88800100	PEDESTRIAN PUSH-BUTTON	EACH	17	17	
* 89502200	MODIFY EXISTING CONTROLLER	EACH	1	1	
* 89502210	MODIFY EXISTING CONTROLLER CABINET	EACH	1	1	
* 89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	2083	2083	
* 89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	2	2	
* 89502376	REBUILD EXISTING HANDHOLE	EACH	1	1	
* 89502380	REMOVE EXISTING HANDHOLE	EACH	4	4	

\* = SPECIALTY ITEM

FILE NAME =  
DI-ITASCA\_S00\_SHT07.dgn  
Default

USER NAME = mrichards  
PLOT SCALE = 28.8228' / in.  
PLOT DATE = 5/20/2016

DESIGNED - MBR  
DRAWN - MBR  
CHECKED - MPG  
DATE - 5/20/2016

REVISED -  
REVISED -  
REVISED -  
REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
SUMMARY OF QUANTITIES**

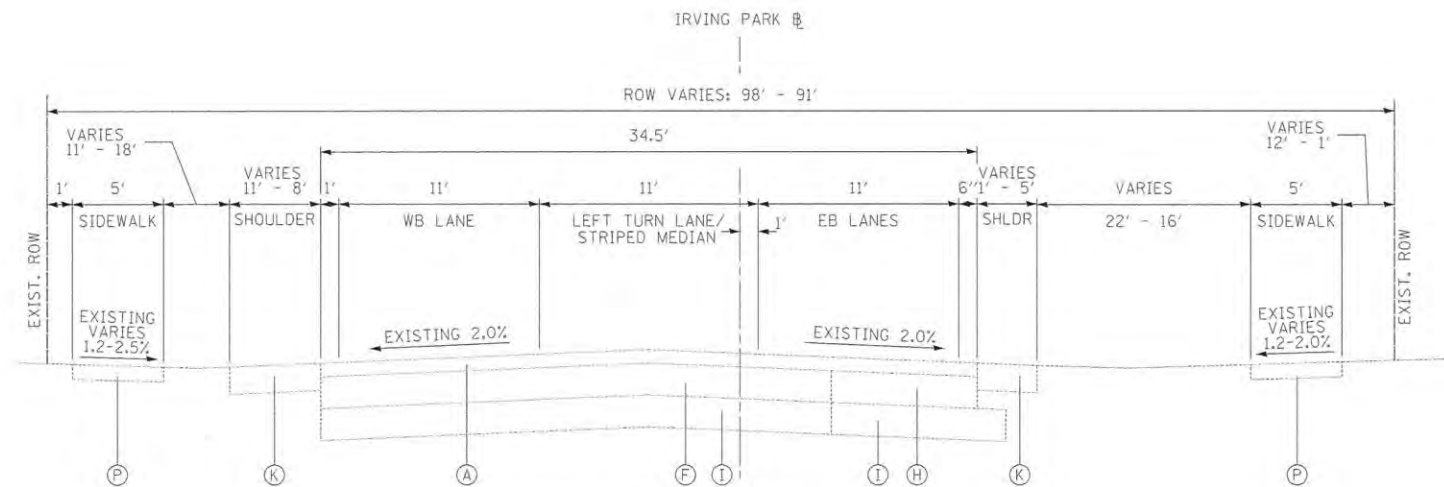
SCALE: N.T.S. SHEET 7 OF 8 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	10-00059-01-SP	DUPAGE	77	9
CONTRACT NO.			61C82	
ILLINOIS FED. AID PROJECT				

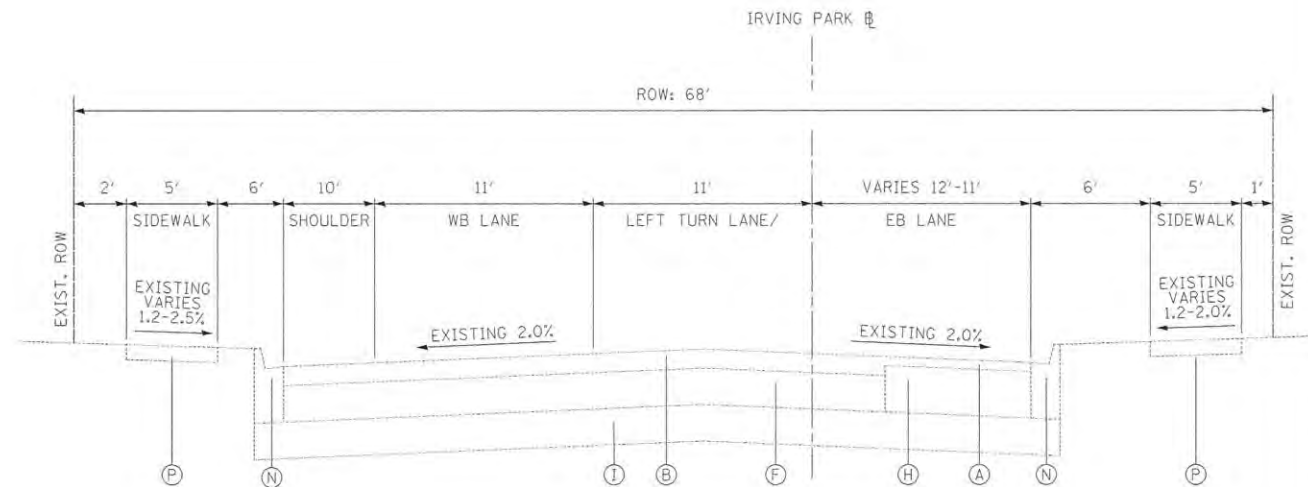




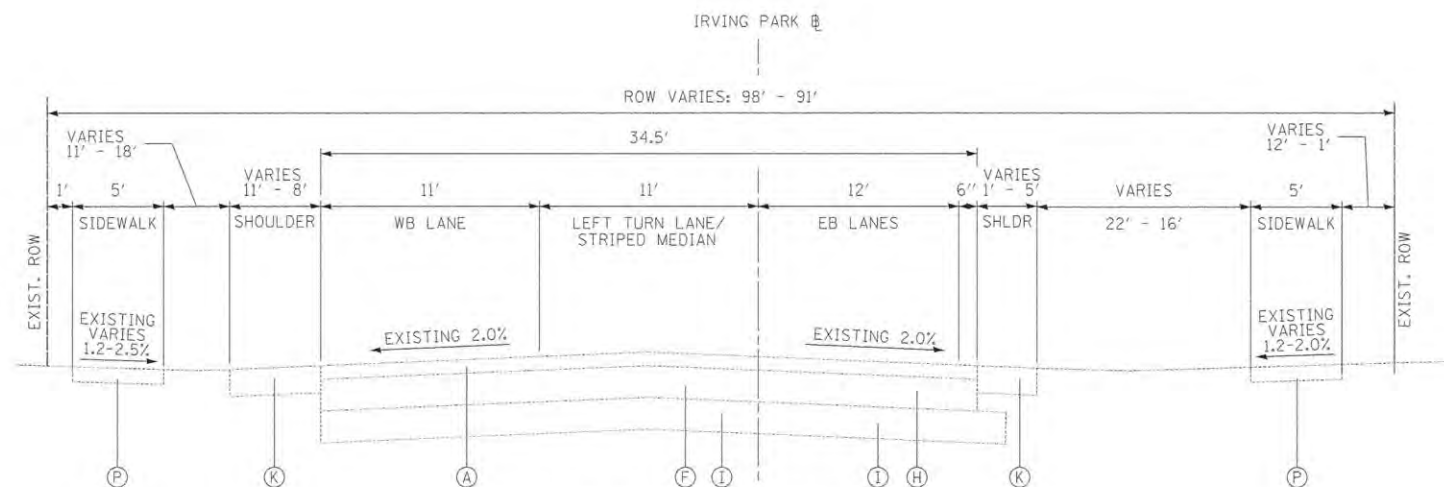




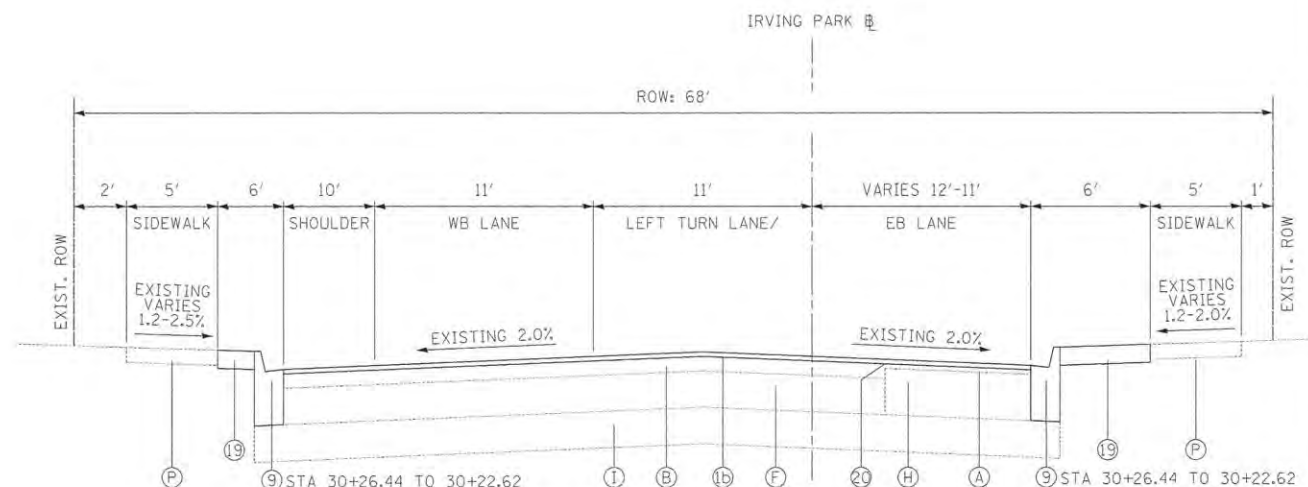
EXISTING TYPICAL SECTION  
 IRVING PARK ROAD AT CATALPA  
 (WEST LEG)  
 STA. 26+33.84 TO 29+74.34



EXISTING TYPICAL SECTION  
 IRVING PARK ROAD AT CATALPA  
 (EAST LEG)  
 STA. 29+74.34 TO STA 31+89.52



PROPOSED TYPICAL SECTION  
 IRVING PARK ROAD AT CATALPA  
 (WEST LEG)  
 STA. 26+69.34 TO 29+26.44



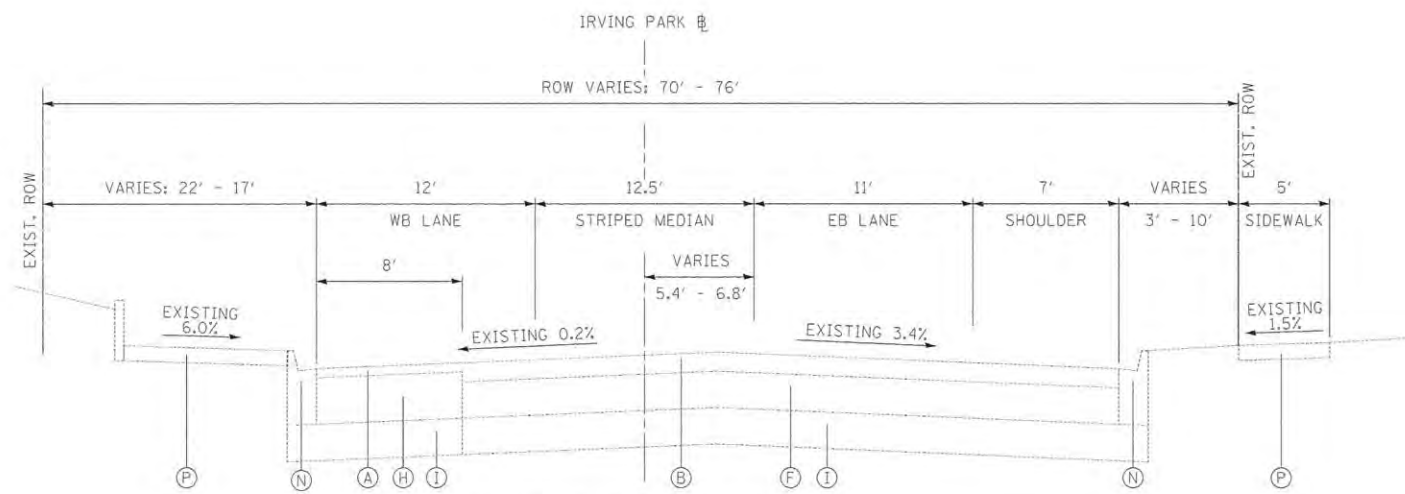
PROPOSED TYPICAL SECTION  
 IRVING PARK ROAD AT CATALPA  
 (EAST LEG)  
 STA. 29+26.44 TO STA 31+89.52

EXISTING LEGEND

- (A) HOT-MIX ASPHALT, 2"±
- (B) HOT-MIX ASPHALT, 4"±
- (C) HOT-MIX ASPHALT, 6"±
- (D) HOT-MIX ASPHALT, 8"±
- (E) PCC BASE COURSE, 6"±
- (F) PCC PAVEMENT, 9"±
- (G) PCC BASE COURSE, 10"±
- (H) HMA BASE COURSE, 10"±
- (I) AGGREGATE SUBGRADE, 12"±
- (J) SUBBASE GRANULAR MATERIAL, 6"±
- (K) HMA SHOULDER, 8"±
- (L) PCC MEDIAN SURFACE, 4"±
- (M) COMB CONC CURB & GUTTER, TYPE B-6.24
- (N) COMB CONC CURB & GUTTER, TYPE B-6.12
- (O) COMB CONC CURB & GUTTER, TYPE B-6.06
- (P) PCC SIDEWALK, 5"±
- (Q) STEEL PLATE BEAM GUARDRAIL & TERMINALS
- (R) AGGREGATE SHOULDER, 6"±

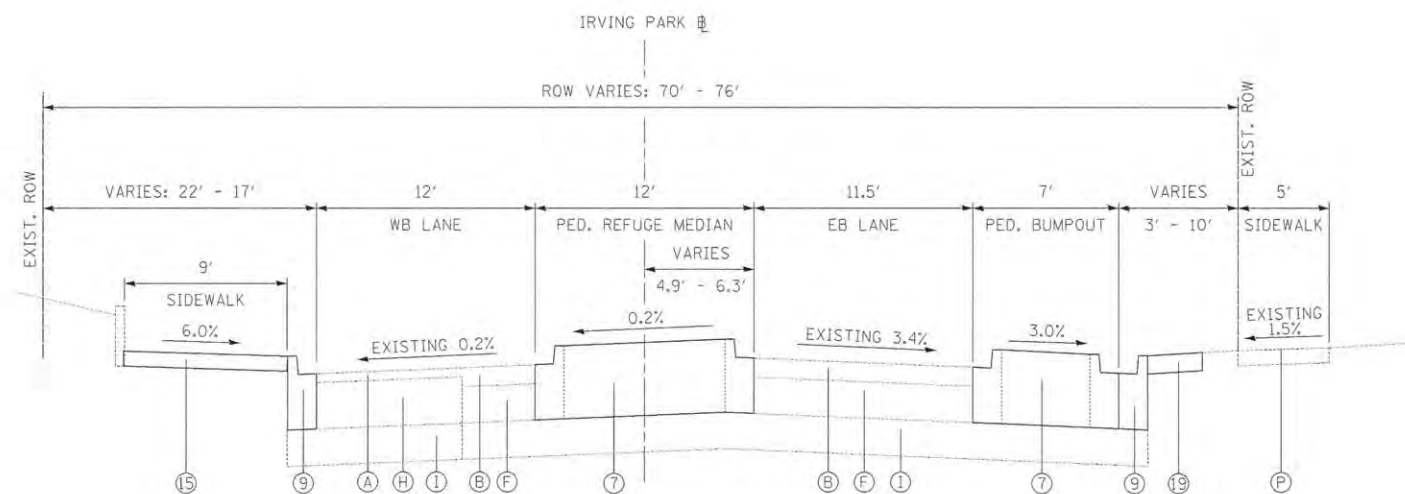
PROPOSED LEGEND

- (1) HMA SURFACE COURSE, MIX "D", N50 1 1/2" (40603335)
- (1b) HMA SURFACE COURSE, MIX "D", N70 1 1/2" (40603340)
- (2) HMA BINDER COURSE, IL-19.0, N50 4 1/2" (40603080)
- (3) PCC SHOULDERS 12" (48300700)
- (4) HMA SHOULDERS, 6" (48203021)
- (5) CONCRETE MEDIAN SURFACE, 4 INCH (60618300)
- (6) CONCRETE MEDIAN, TYPE SB-6.06 (606019200)
- (7) CONCRETE MEDIAN, TYPE SB-6.12 (606019600)
- (8) COMB CC&G, TYPE B-6.24 (ABUT EXISTING PVMT) (60605100)
- (9) COMB CC&G, TYPE B-6.12 (ABUT EXISTING PVMT) (60603900)
- (10) COMB CC&G, TYPE M-6.24 (60610400)
- (11) COMB CC&G, TYPE M-6.06 (60608600)
- (12) AGGREGATE SUBGRADE IMPROVEMENT (30300001) (SEE PLANS FOR DEPTHS)
- (13) CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (63700155)
- (14) CONCRETE BARRIER BASE (63700900)
- (15) PCC SIDEWALK 5 INCH (42400100)
- (16) AGGREGATE SHOULDERS, TYPE B 6" (48101500) OR AGGREGATE SHOULDERS, TYPE B 10" (48101620) (FOR GUARDRAIL STABILIZATION AT EXISTING/PROPOSED GUARDRAIL LOCATIONS)
- (17) LEVELING BINDER (MACHINE METHOD), N50 (40600625) OR LEVELING BINDER (MACHINE METHOD), N70 (40600635)
- (18) PARAPET RAILING (50901750)
- (19) LANDSCAPING ITEMS
- (20) STRIP REFLECTIVE CRACK CONTROL TREATMENT (44300200)



EXISTING TYPICAL SECTION

IRVING PARK ROAD AT MAPLE  
(WEST LEG)  
STA. 43+12.88 TO STA. 43+59.40



PROPOSED TYPICAL SECTION

IRVING PARK ROAD AT MAPLE  
(WEST LEG)  
STA. 43+12.88 TO STA. 43+59.40

EXISTING LEGEND

- (A) HOT-MIX ASPHALT, 2"±
- (B) HOT-MIX ASPHALT, 4"±
- (C) HOT-MIX ASPHALT, 6"±
- (D) HOT-MIX ASPHALT, 8"±
- (E) PCC BASE COURSE, 6"±
- (F) PCC PAVEMENT, 9"±
- (G) PCC BASE COURSE, 10"±
- (H) HMA BASE COURSE, 10"±
- (I) AGGREGATE SUBGRADE, 12"±
- (J) SUBBASE GRANULAR MATERIAL, 6"±
- (K) HMA SHOULDER, 8"±
- (L) PCC MEDIAN SURFACE, 4"±
- (M) COMB CONC CURB & GUTTER, TYPE B-6.24
- (N) COMB CONC CURB & GUTTER, TYPE B-6.12
- (O) COMB CONC CURB & GUTTER, TYPE B-6.06
- (P) PCC SIDEWALK, 5"±
- (Q) STEEL PLATE BEAM GUARDRAIL & TERMINALS
- (R) AGGREGATE SHOULDER, 6"±

PROPOSED LEGEND

- (13) HMA SURFACE COURSE, MIX "D", N50 1 1/2" (40603335)
- (14) HMA SURFACE COURSE, MIX "D", N70 1 1/2" (40603340)
- (15) HMA BINDER COURSE, IL-19.0, N50 4 1/2" (40603080)
- (16) PCC SHOULDERS 12" (48300700)
- (17) HMA SHOULDERS, 6" (48203021)
- (18) CONCRETE MEDIAN SURFACE, 4 INCH (60618300)
- (19) CONCRETE MEDIAN, TYPE SB-6.06 (606019200)
- (20) CONCRETE MEDIAN, TYPE SB-6.12 (606019600)
- (21) COMB CC&G, TYPE B-6.24 (ABUT EXISTING PVMT) (60605100)
- (22) COMB CC&G, TYPE B-6.12 (ABUT EXISTING PVMT) (60603900)
- (23) COMB CC&G, TYPE M-6.24 (60610400)
- (24) COMB CC&G, TYPE M-6.06 (60608600)
- (25) AGGREGATE SUBGRADE IMPROVEMENT (30300001)  
(SEE PLANS FOR DEPTHS)
- (26) CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (63700155)
- (27) CONCRETE BARRIER BASE (63700900)
- (28) PCC SIDEWALK 5 INCH (42400100)
- (29) AGGREGATE SHOULDERS, TYPE B 6" (48101500)  
-OR-  
AGGREGATE SHOULDERS, TYPE B 10" (48101620) (FOR GUARDRAIL  
STABILIZATION AT EXISTING/PROPOSED GUARDRAIL LOCATIONS)
- (30) LEVELING BINDER (MACHINE METHOD), N50 (40600625)
- (31) LEVELING BINDER (MACHINE METHOD), N70 (40600635)
- (32) PARAPET RAILING (50901750)
- (33) LANDSCAPING ITEMS
- (34) STRIP REFLECTIVE CRACK CONTROL TREATMENT (44300200)

FILE NAME =  
01-ITASCA-TYPSEC-IL-19-MAPLE\_SHT03.dgn  
(Deface)

USER NAME = mitchard  
PLOT SCALE = 1/8"=1'-0"  
PLOT DATE = 5/20/2016

DESIGNED - KRP  
DRAWN - KRP  
CHECKED - MPG  
DATE - 5/20/2016

REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
TYPICAL SECTIONS - IL-19 (IRVING PARK RD) AT MAPLE AVE

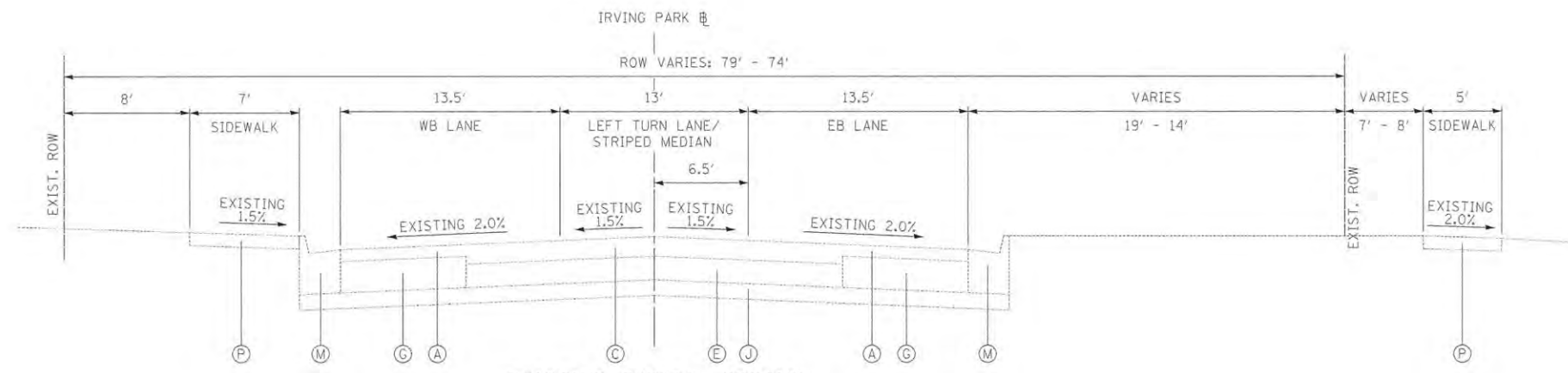
SCALE: N.T.S. SHEET 3 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	10-00059-01-SP	DUPAGE	77	13
			CONTRACT NO. 61C82	
ILLINOIS FED. AID PROJECT				



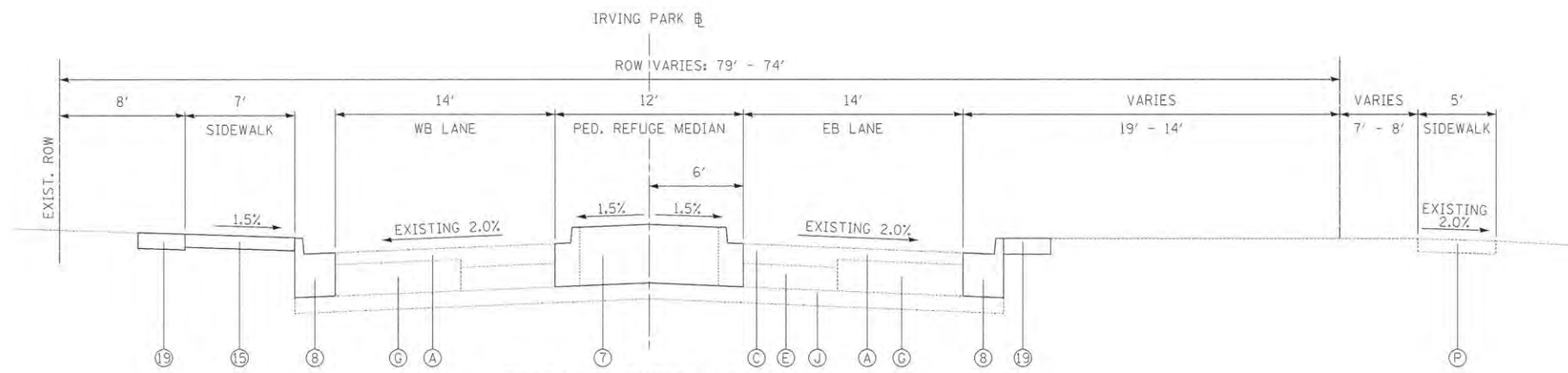






**EXISTING TYPICAL SECTION**

IRVING PARK ROAD AT RUSH  
(WEST LEG)  
STA. 61+57.68 TO 62+24.56



**PROPOSED TYPICAL SECTION**

IRVING PARK ROAD AT RUSH  
(WEST LEG)  
STA. 61+57.68 TO 62+24.56

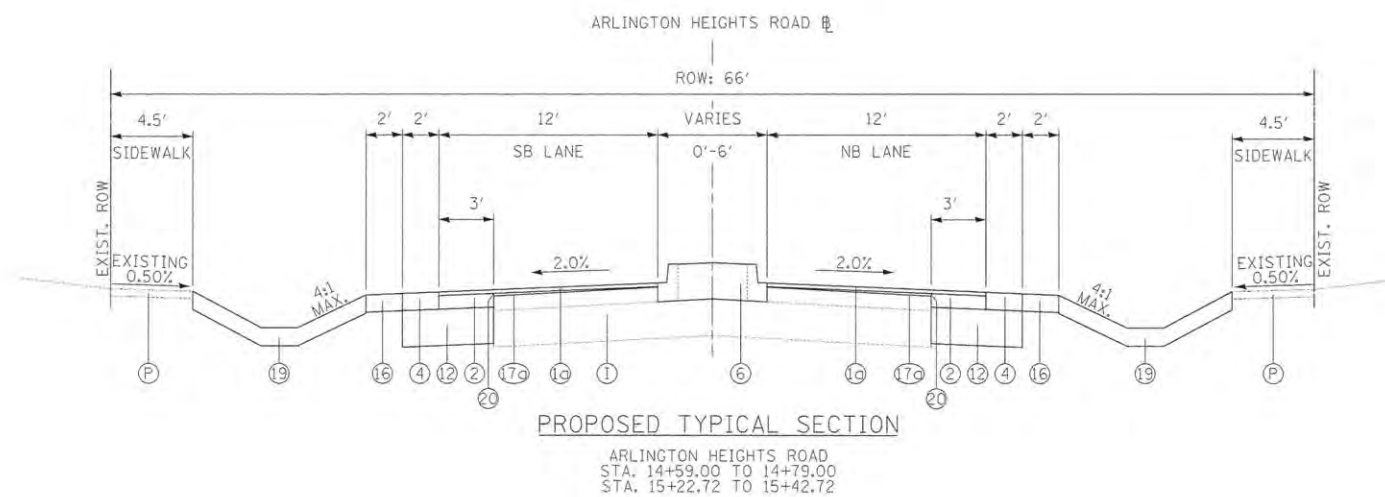
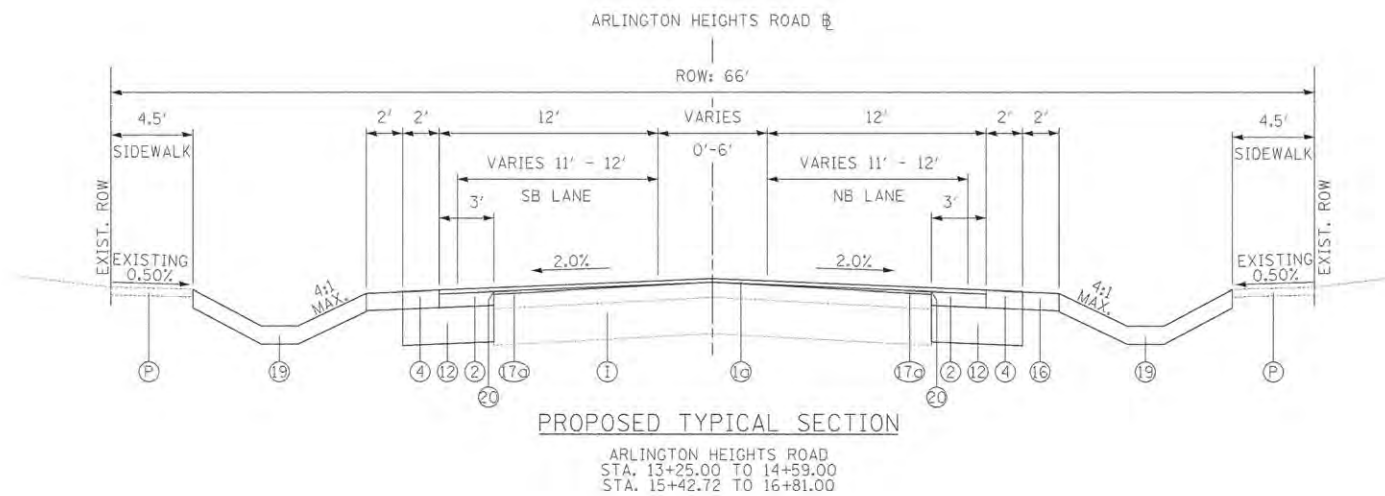
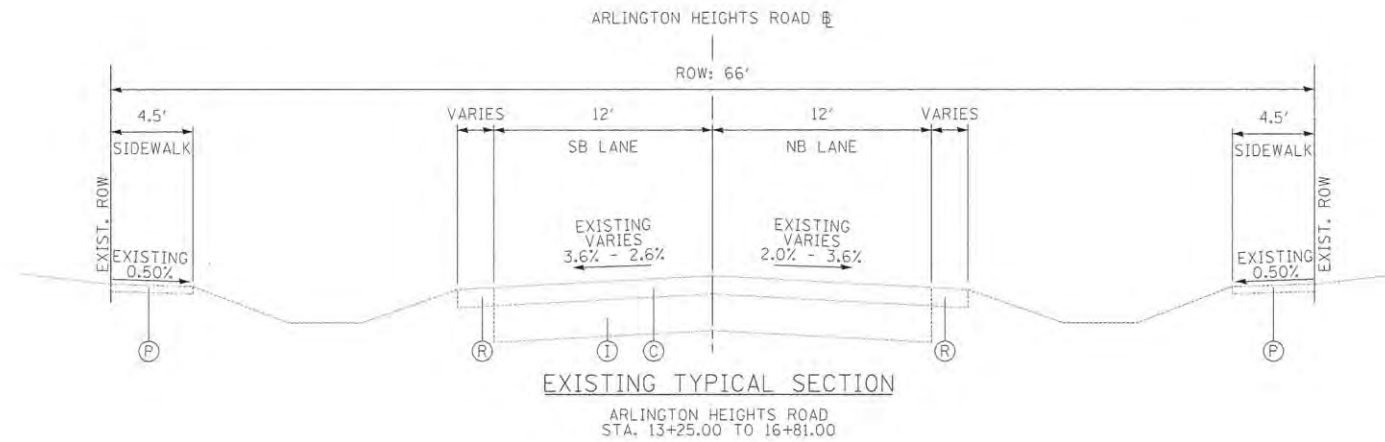
**EXISTING LEGEND**

- (A) HOT-MIX ASPHALT, 2"±
- (B) HOT-MIX ASPHALT, 4"±
- (C) HOT-MIX ASPHALT, 6"±
- (D) HOT-MIX ASPHALT, 8"±
- (E) PCC BASE COURSE, 6"±
- (F) PCC PAVEMENT, 9"±
- (G) PCC BASE COURSE, 10"±
- (H) HMA BASE COURSE, 10"±
- (I) AGGREGATE SUBGRADE, 12"±
- (J) SUBBASE GRANULAR MATERIAL, 6"±
- (K) HMA SHOULDER, 8"±
- (L) PCC MEDIAN SURFACE, 4"±
- (M) COMB CONC CURB & GUTTER, TYPE B-6.24
- (N) COMB CONC CURB & GUTTER, TYPE B-6.12
- (O) COMB CONC CURB & GUTTER, TYPE B-6.06
- (P) PCC SIDEWALK, 5"±
- (Q) STEEL PLATE BEAM GUARDRAIL & TERMINALS
- (R) AGGREGATE SHOULDER, 6"±

**PROPOSED LEGEND**

- (19) HMA SURFACE COURSE, MIX "D", N50 1 1/2" (40603335)
- (15) HMA SURFACE COURSE, MIX "D", N70 1 1/2" (40603340)
- (8) HMA BINDER COURSE, IL-19.0, N50 4 1/2" (40603080)
- (7) PCC SHOULDERS 12" (48300700)
- (4) HMA SHOULDERS, 6" (48203021)
- (5) CONCRETE MEDIAN SURFACE, 4 INCH (60618300)
- (6) CONCRETE MEDIAN, TYPE SB-6.06 (606019200)
- (7) CONCRETE MEDIAN, TYPE SB-6.12 (606019600)
- (8) COMB CC&G, TYPE B-6.24 (ABUT EXISTING PVMT) (60605100)
- (9) COMB CC&G, TYPE B-6.12 (ABUT EXISTING PVMT) (60603900)
- (10) COMB CC&G, TYPE M-6.24 (60610400)
- (11) COMB CC&G, TYPE M-6.06 (60608600)
- (12) AGGREGATE SUBGRADE IMPROVEMENT (30300001)  
(SEE PLANS FOR DEPTHS)
- (13) CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (63700155)
- (14) CONCRETE BARRIER BASE (63700900)
- (15) PCC SIDEWALK 5 INCH (42400100)
- (16) AGGREGATE SHOULDERS, TYPE B 6" (48101500)  
-OR-  
AGGREGATE SHOULDERS, TYPE B 10" (48101620) (FOR GUARDRAIL STABILIZATION AT EXISTING/PROPOSED GUARDRAIL LOCATIONS)
- (17) LEVELING BINDER (MACHINE METHOD), N50 (40600625)
- (17b) LEVELING BINDER (MACHINE METHOD), N70 (40600635)
- (18) PARAPET RAILING (50901750)
- (19) LANDSCAPING ITEMS
- (20) STRIP REFLECTIVE CRACK CONTROL TREATMENT (44300200)





**EXISTING LEGEND**

- (A) HOT-MIX ASPHALT, 2"±
- (B) HOT-MIX ASPHALT, 4"±
- (C) HOT-MIX ASPHALT, 6"±
- (D) HOT-MIX ASPHALT, 8"±
- (E) PCC BASE COURSE, 6"±
- (F) PCC PAVEMENT, 9"±
- (G) PCC BASE COURSE, 10"±
- (H) HMA BASE COURSE, 10"±
- (I) AGGREGATE SUBGRADE, 12"±
- (J) SUBBASE GRANULAR MATERIAL, 6"±
- (K) HMA SHOULDER, 8"±
- (L) PCC MEDIAN SURFACE, 4"±
- (M) COMB CONC CURB & GUTTER, TYPE B-6.24
- (N) COMB CONC CURB & GUTTER, TYPE B-6.12
- (O) COMB CONC CURB & GUTTER, TYPE B-6.06
- (P) PCC SIDEWALK, 5"±
- (Q) STEEL PLATE BEAM GUARDRAIL & TERMINALS
- (R) AGGREGATE SHOULDER, 6"±

**PROPOSED LEGEND**

- (1G) HMA SURFACE COURSE, MIX "D", N50 1 1/2" (40603335)
- (1B) HMA SURFACE COURSE, MIX "D", N70 1 1/2" (40603340)
- (2) HMA BINDER COURSE, IL-19.0, N50 4 1/2" (40603080)
- (3) PCC SHOULDERS 12" (48300700)
- (4) HMA SHOULDERS, 6" (48203021)
- (5) CONCRETE MEDIAN SURFACE, 4 INCH (60618300)
- (6) CONCRETE MEDIAN, TYPE SB-6.06 (606019200)
- (7) CONCRETE MEDIAN, TYPE SB-6.12 (606019600)
- (8) COMB CC&G, TYPE B-6.24 (ABUT EXISTING PVMT) (60605100)
- (9) COMB CC&G, TYPE B-6.12 (ABUT EXISTING PVMT) (60603900)
- (10) COMB CC&G, TYPE M-6.24 (60610400)
- (11) COMB CC&G, TYPE M-6.06 (60608600)
- (12) AGGREGATE SUBGRADE IMPROVEMENT (30300001) (SEE PLANS FOR DEPTHS)
- (13) CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (63700155)
- (14) CONCRETE BARRIER BASE (63700900)
- (15) PCC SIDEWALK 5 INCH (42400100)
- (16) AGGREGATE SHOULDERS, TYPE B 6" (48101500)  
-OR-  
AGGREGATE SHOULDERS, TYPE B 10" (48101620) (FOR GUARDRAIL STABILIZATION AT EXISTING/PROPOSED GUARDRAIL LOCATIONS)
- (17G) LEVELING BINDER (MACHINE METHOD), N50 (40600625)
- (17B) LEVELING BINDER (MACHINE METHOD), N70 (40600635)
- (18) PARAPET RAILING (50901750)
- (19) LANDSCAPING ITEMS
- (20) STRIP REFLECTIVE CRACK CONTROL TREATMENT (44300200)

**IDOT HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

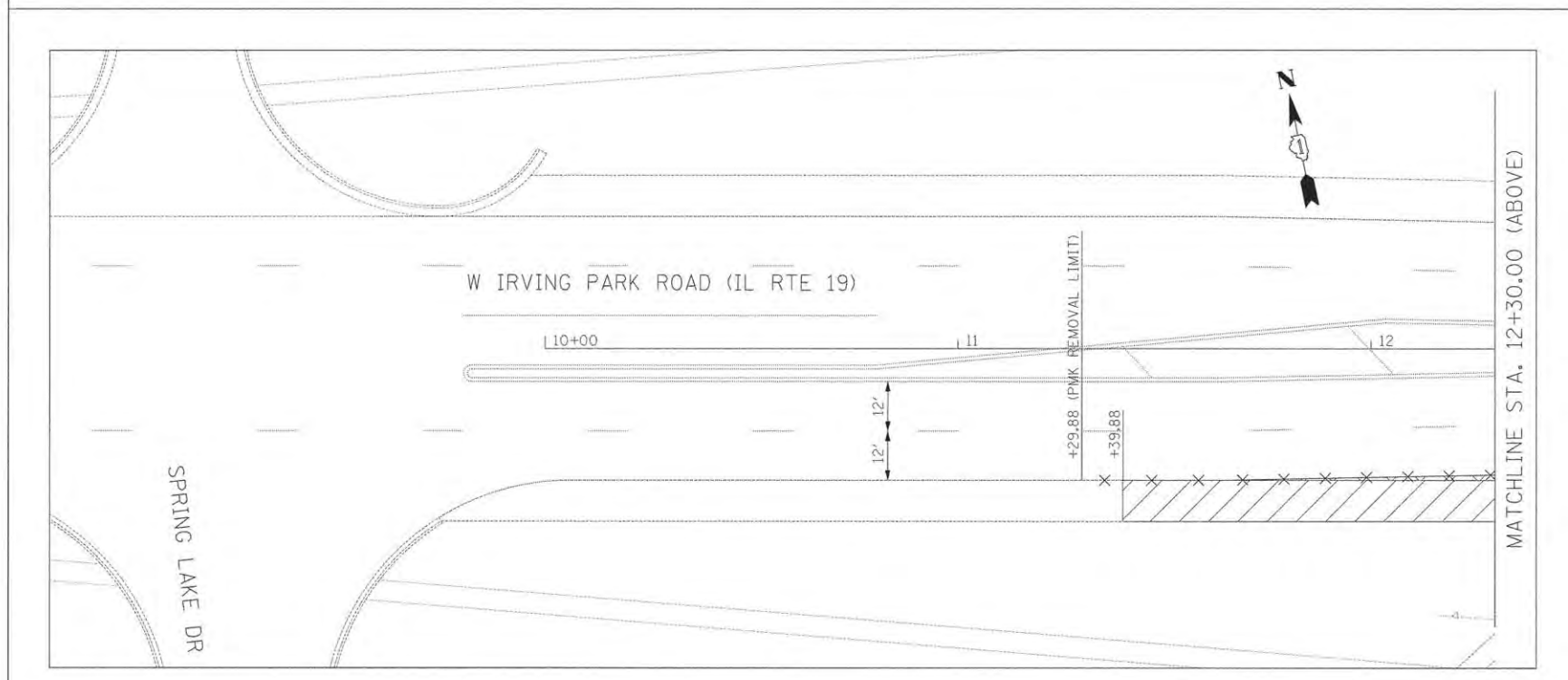
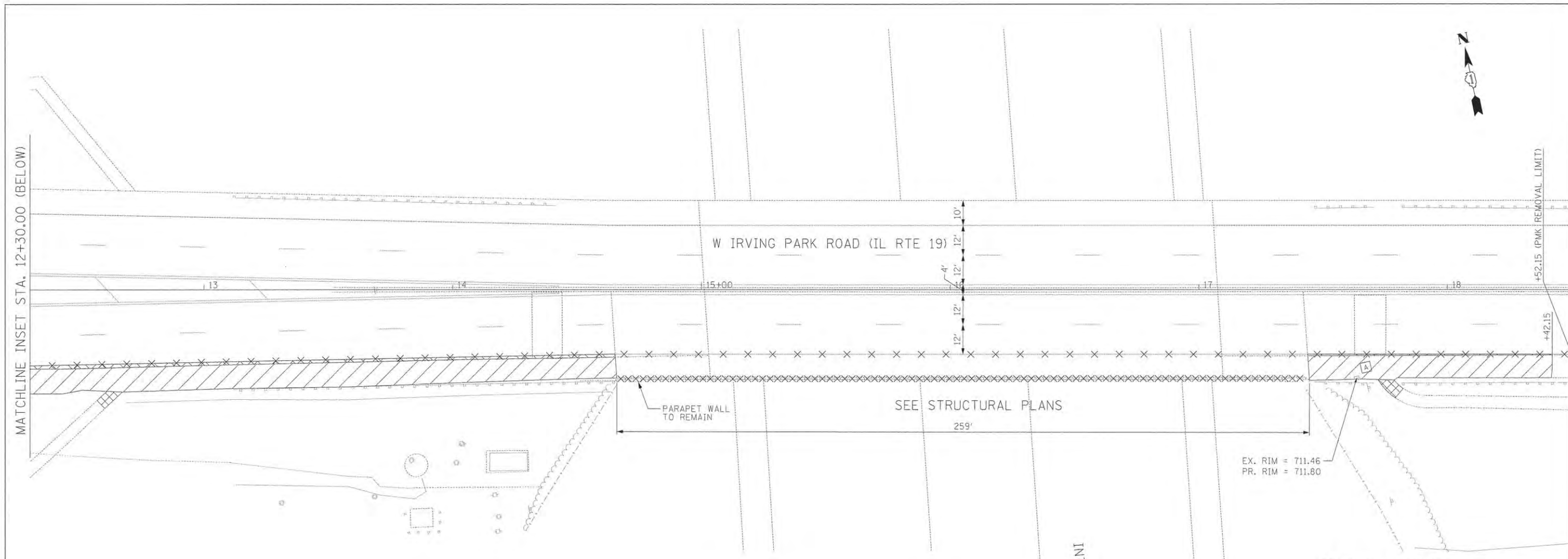
ITEM	AIR VOIDS @ NDES
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 1 1/2" LIFT	4% @ 50 Gyr.
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm), 1 1/2" LIFT	4% @ 70 Gyr.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 4 1/2" LIFT	4% @ 50 Gyr.
LEVELING BINDER (MACHINE METHOD), N50 (IL 9.5 mm), 1" LIFT (AVERAGE)	4% @ 50 Gyr.
LEVELING BINDER (MACHINE METHOD), N70 (IL 9.5 mm), 1" LIFT (AVERAGE)	4% @ 70 Gyr.
HOT-MIX ASPHALT SHOULDER, 6" (BINDER IL-19 mm)	4% @ 50 Gyr.
HMA DRIVEWAY ENTRANCE PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 2" LIFT	4% @ 50 Gyr.
HOT-MIX ASPHALT BASE COURSE, 6" (BINDER IL-19 mm)	4% @ 50 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LB/SQ YD/IN.

"THE AC TYPE FOR POLYMERIZED HMA MIXTURES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS."

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.





- REMOVAL LEGEND**
- COMB. CURB & GUTTER REMOVAL (44000500)
  - BRIDGE RAIL REMOVAL (50104000)
  - PAVEMENT MARKING REMOVAL (78300100)
  - STORM SEWER REMOVAL (55100200/55100500)
  - SIDEWALK REMOVAL (44000600)
  - MEDIAN REMOVAL (44003100)
  - DRIVEWAY PAVEMENT REMOVAL (44000200)
  - PAVEMENT REMOVAL (44000100) (RETAIN EXISTING SUBGRADES)
  - HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" (44000155)
  - PAVED SHOULDER REMOVAL (44004250)
  - FRAMES & GRATES TO BE ADJUSTED (60300105)
  - MANHOLES / VALVE VAULTS TO BE ADJUSTED (60265700)
  - VALVE BOX FRAMES TO BE ADJUSTED (60300405)
  - REMOVING CATCH BASINS (60500050)



FILE NAME = D:\ITASCA\REK\L-IL-1-290_SHT01.dgn	USER NAME = mrichards	DESIGNED - KRP	REVISED -
		DRAWN - KRP	REVISED -
		CHECKED - MPG	REVISED -
		DATE - 5/20/2016	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
REMOVAL PLAN - IL-19 (IRVING PARK RD) OVER I-290**















SCALE: 1" = 20'    SHEET 1 OF 6 SHEETS    STA.    TO STA.

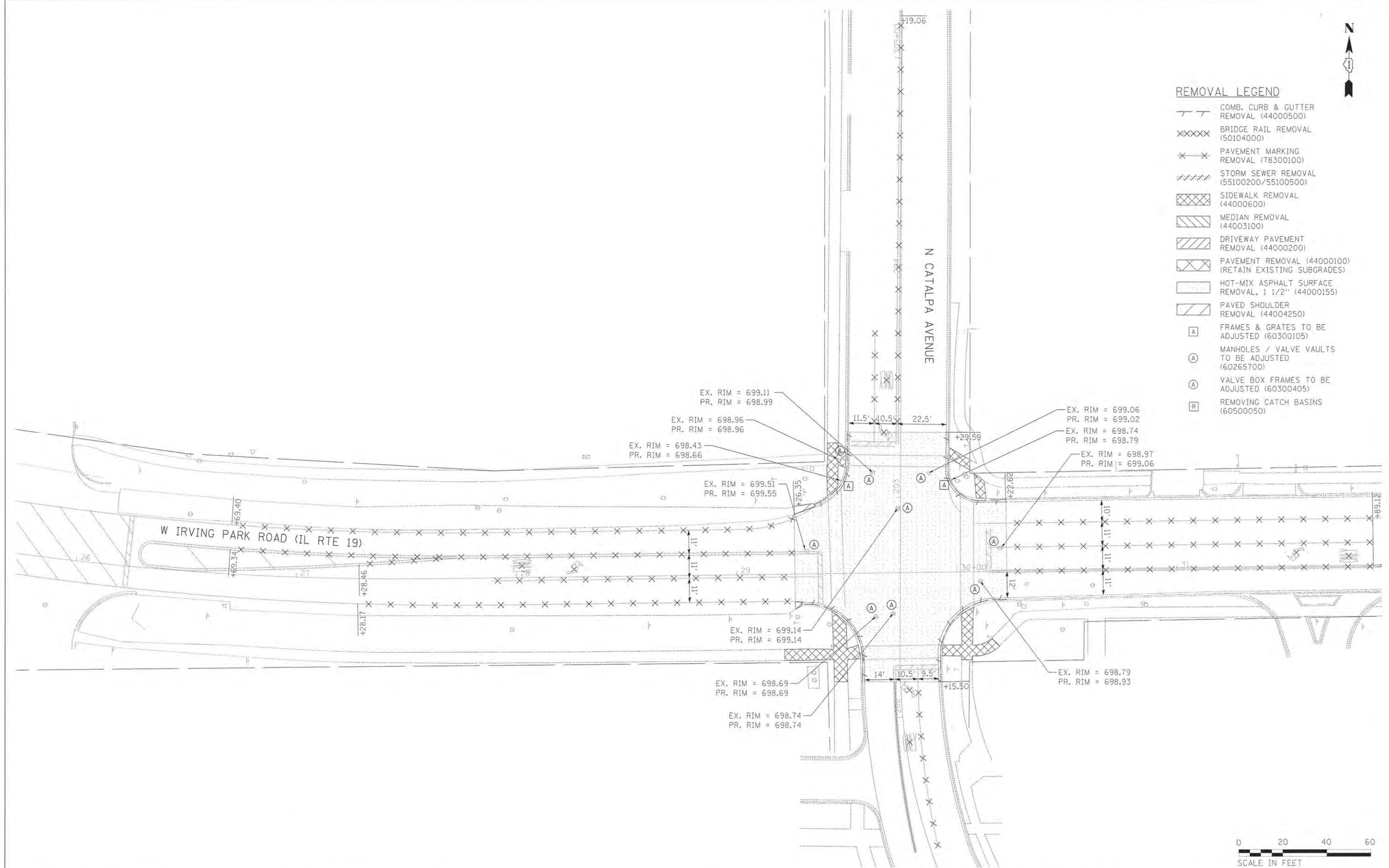
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	10-00059-01-SP	DUPAGE	77	19
CONTRACT NO. 61CB2			ILLINOIS FED. AID PROJECT	



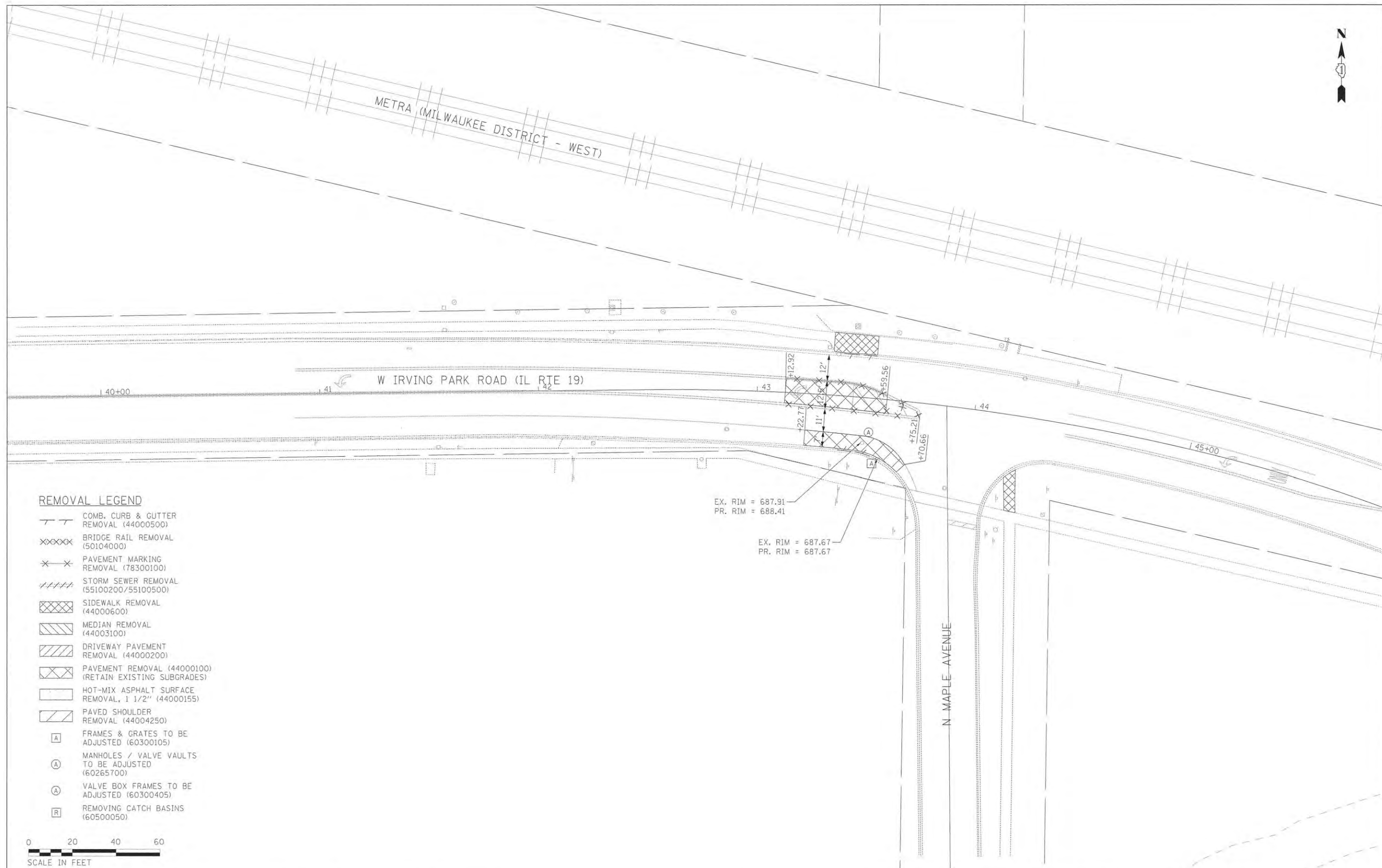


**REMOVAL LEGEND**

-  COMB. CURB & GUTTER REMOVAL (44000500)
-  BRIDGE RAIL REMOVAL (50104000)
-  PAVEMENT MARKING REMOVAL (78300100)
-  STORM SEWER REMOVAL (55100200/55100500)
-  SIDEWALK REMOVAL (44000600)
-  MEDIAN REMOVAL (44003100)
-  DRIVEWAY PAVEMENT REMOVAL (44000200)
-  PAVEMENT REMOVAL (44000100) (RETAIN EXISTING SUBGRADES)
-  HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" (44000155)
-  PAVED SHOULDER REMOVAL (44004250)
-  FRAMES & GRATES TO BE ADJUSTED (60300105)
-  MANHOLES / VALVE VAULTS TO BE ADJUSTED (60265700)
-  VALVE BOX FRAMES TO BE ADJUSTED (60300405)
-  REMOVING CATCH BASINS (60500050)



FILE NAME = 01-ITASCA_REP_LL-02_CATALPA_SHT01.dgn	USER NAME = merichards	DESIGNED - KRP	REVISED - -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS REMOVAL PLAN - IL-19 (IRVING PARK RD) AT CATALPA AVE</b>			F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 20
PROJECT SCALE = 20,000 / 1" = 200'	CHECKED - MPG	REVISOR - -	DATE - 5/20/2016		SCALE: 1" = 20'	SHEET 2	OF 6 SHEETS	STA.	TO STA.	CONTRACT NO. 61C82		
PLDW DATE = 5/20/2016	DATE - 5/20/2016	REVISOR - -	REVISOR - -		ILLINOIS FED. AID PROJECT							



**REMOVAL LEGEND**

- COMB. CURB & GUTTER REMOVAL (44000500)
- BRIDGE RAIL REMOVAL (50104000)
- PAVEMENT MARKING REMOVAL (78300100)
- STORM SEWER REMOVAL (55100200/55100500)
- SIDEWALK REMOVAL (44000600)
- MEDIAN REMOVAL (44003100)
- DRIVEWAY PAVEMENT REMOVAL (44000200)
- PAVEMENT REMOVAL (44000100) (RETAIN EXISTING SUBGRADES)
- HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" (44000155)
- PAVED SHOULDER REMOVAL (44004250)
- FRAMES & GRATES TO BE ADJUSTED (60300105)
- MANHOLES / VALVE VAULTS TO BE ADJUSTED (60265700)
- VALVE BOX FRAMES TO BE ADJUSTED (60300405)
- REMOVING CATCH BASINS (60500050)



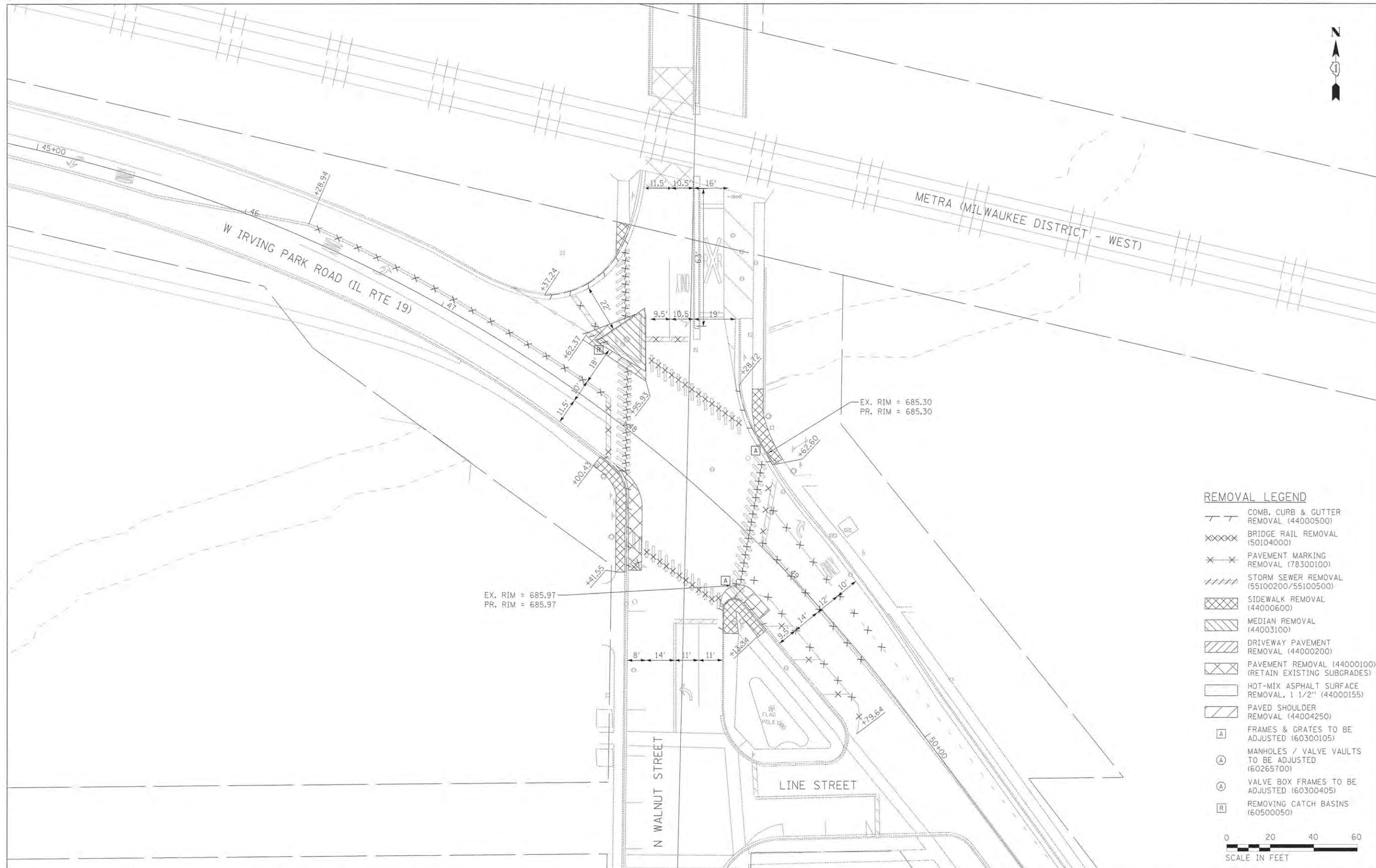
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		DRAWN - KRP	REVISED -
		CHECKED - MPG	REVISED -
		DATE - 5/20/2016	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
REMOVAL PLAN - IL-19 (IRVING PARK RD) AT MAPLE AVE**

SCALE: 1" = 20' SHEET 3 OF 6 SHEETS STA. TO STA.

F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 21
CONTRACT NO. 61C82			ILLINOIS FED. AID PROJECT	



- REMOVAL LEGEND**
- COMB. CURB & GUTTER REMOVAL (44000500)
  - BRIDGE RAIL REMOVAL (50104000)
  - PAVEMENT MARKING REMOVAL (78300100)
  - STORM SEWER REMOVAL (55100200/55100500)
  - SIDEWALK REMOVAL (44000600)
  - MEDIAN REMOVAL (44003100)
  - DRIVEWAY PAVEMENT REMOVAL (44000200)
  - PAVEMENT REMOVAL (44000100) (RETAIN EXISTING SUBGRADES)
  - HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" (44000155)
  - PAVED SHOULDER REMOVAL (44004250)
  - [A] FRAMES & GRATES TO BE ADJUSTED (60300105)
  - [A] MANHOLES / VALVE VAULTS TO BE ADJUSTED (60265700)
  - [A] VALVE BOX FRAMES TO BE ADJUSTED (60300405)
  - [R] REMOVING CATCH BASINS (60500050)



EX. RIM = 685.97  
PR. RIM = 685.97

EX. RIM = 685.30  
PR. RIM = 685.30

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

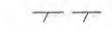





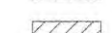
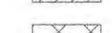
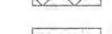
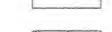
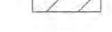


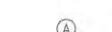
**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
REMOVAL PLAN - IL-19 (IRVING PARK RD) AT WALNUT ST**

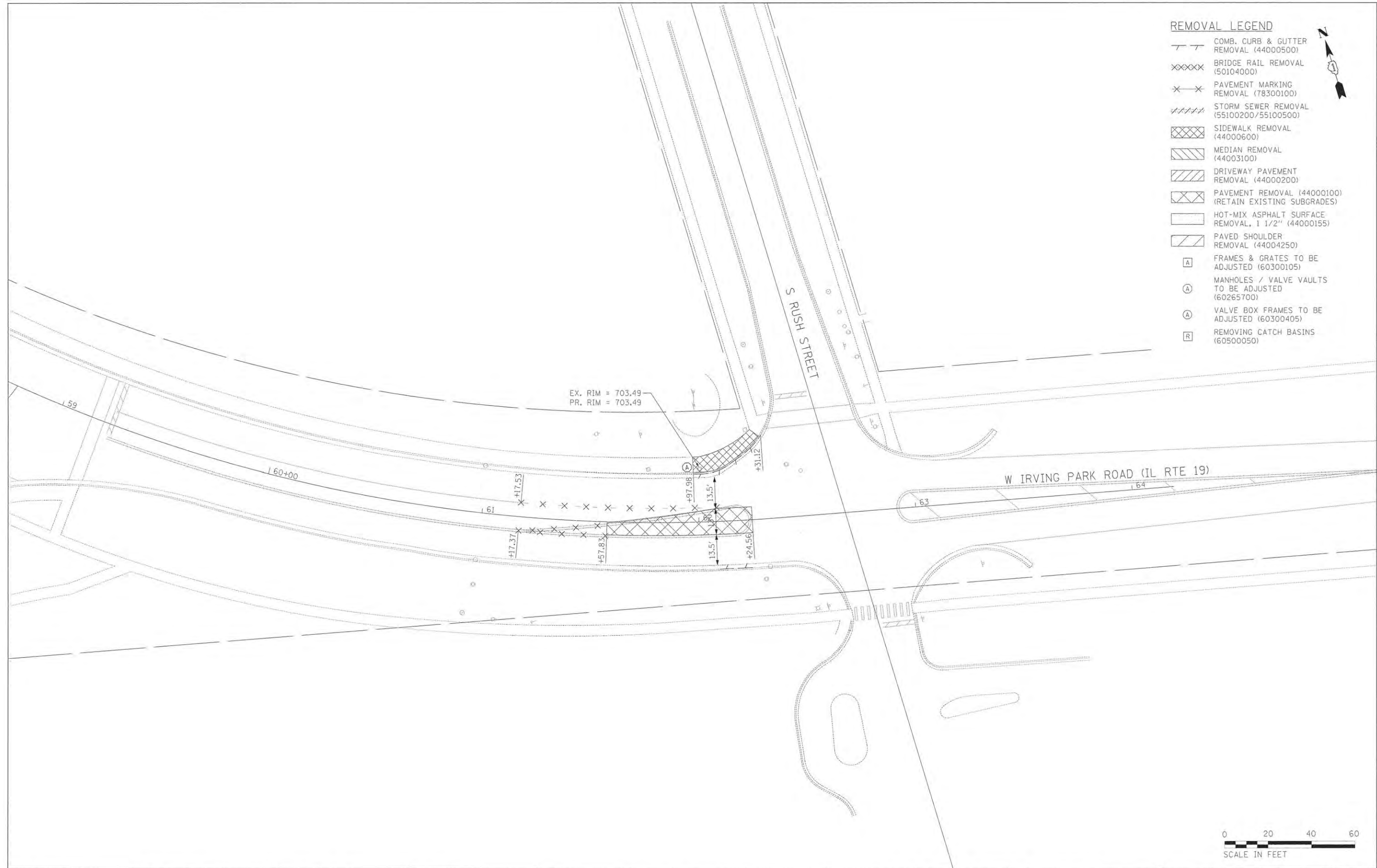
SCALE: 1" = 20'    SHEET 4 OF 6 SHEETS STA.    TO STA.

F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 22
CONTRACT NO. 61C82			ILLINOIS FED. AID PROJECT	



**REMOVAL LEGEND**

-  COMB. CURB & GUTTER REMOVAL (4400500)
-  BRIDGE RAIL REMOVAL (5010400)
-  PAVEMENT MARKING REMOVAL (7830010)
-  STORM SEWER REMOVAL (55100200/55100500)
-  SIDEWALK REMOVAL (4400600)
-  MEDIAN REMOVAL (4400310)
-  DRIVEWAY PAVEMENT REMOVAL (44000200)
-  PAVEMENT REMOVAL (44000100) (RETAIN EXISTING SUBGRADES)
-  HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" (44000155)
-  PAVED SHOULDER REMOVAL (44004250)
-  [A] FRAMES & GRATES TO BE ADJUSTED (60300105)
-  [A] MANHOLES / VALVE VAULTS TO BE ADJUSTED (60265700)
-  [A] VALVE BOX FRAMES TO BE ADJUSTED (60300405)
-  [R] REMOVING CATCH BASINS (60500050)



FILE NAME = DI-ITASCA_REM.L-85-RUSH_SHT01.dgn	USER NAME = mrichard	DESIGNED - KRP	REVISED -
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		DATE - 5/20/2016	REVISED -

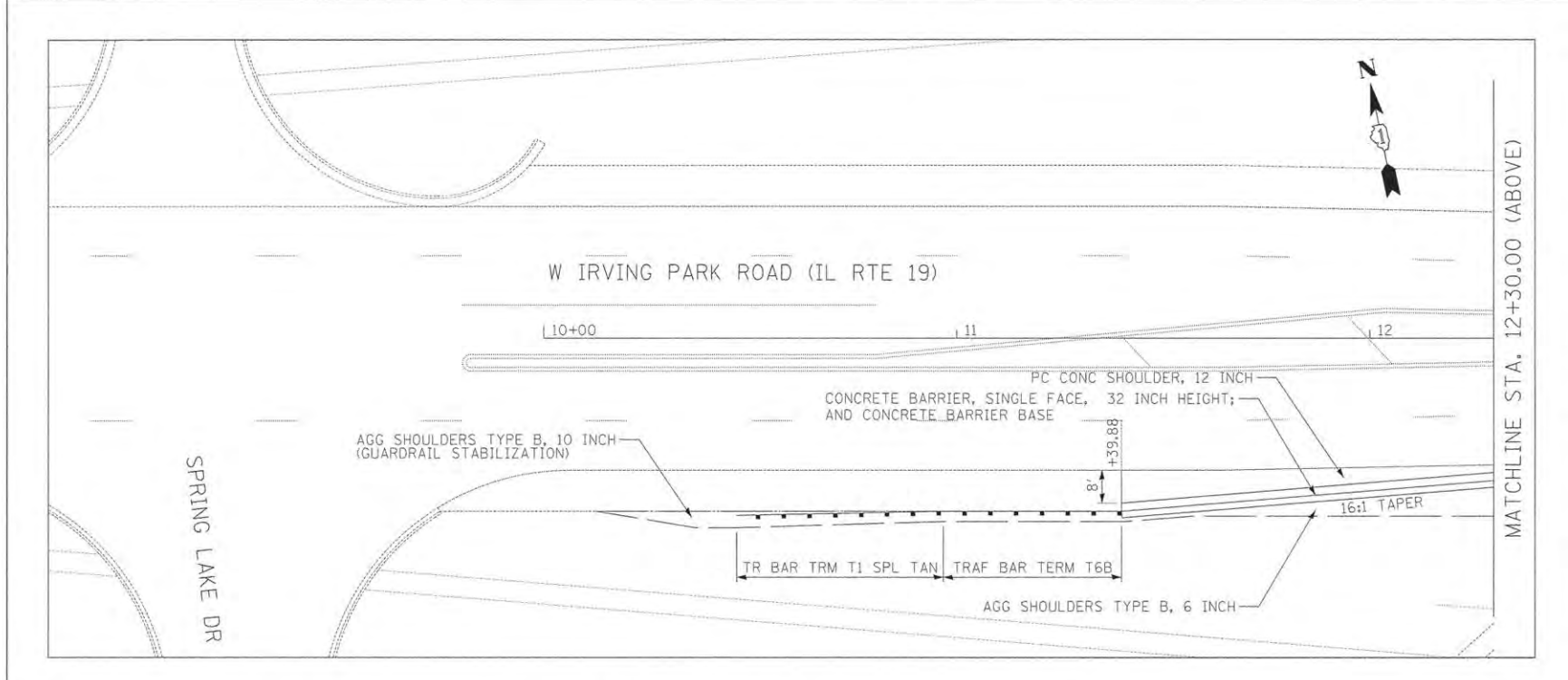
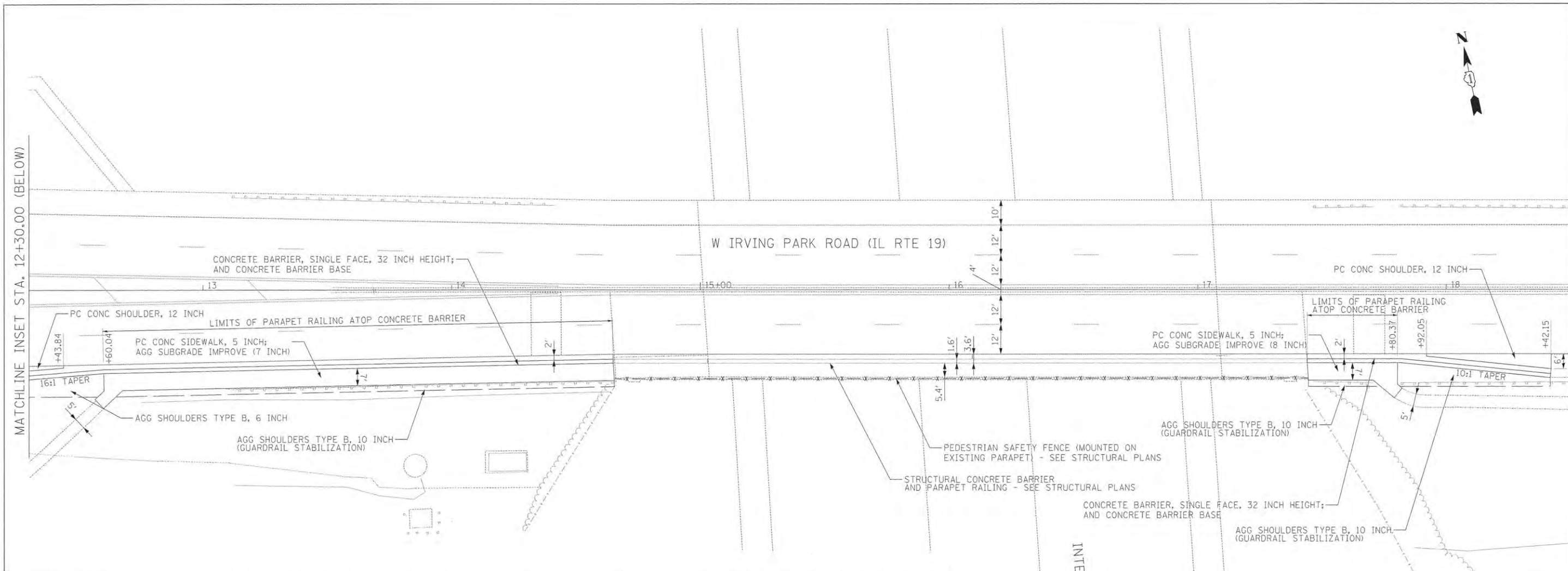
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
REMOVAL PLAN - IL-19 (IRVING PARK RD) AT RUSH ST**

SCALE: 1" = 20'    SHEET 5 OF 6 SHEETS    STA.    TO STA.

F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 23
CONTRACT NO. 61C82				
[ILLINOIS] FED. AID PROJECT				





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DESIGNED - KRP  
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 CHECKED - MPG  
 DATE - 5/20/2016

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

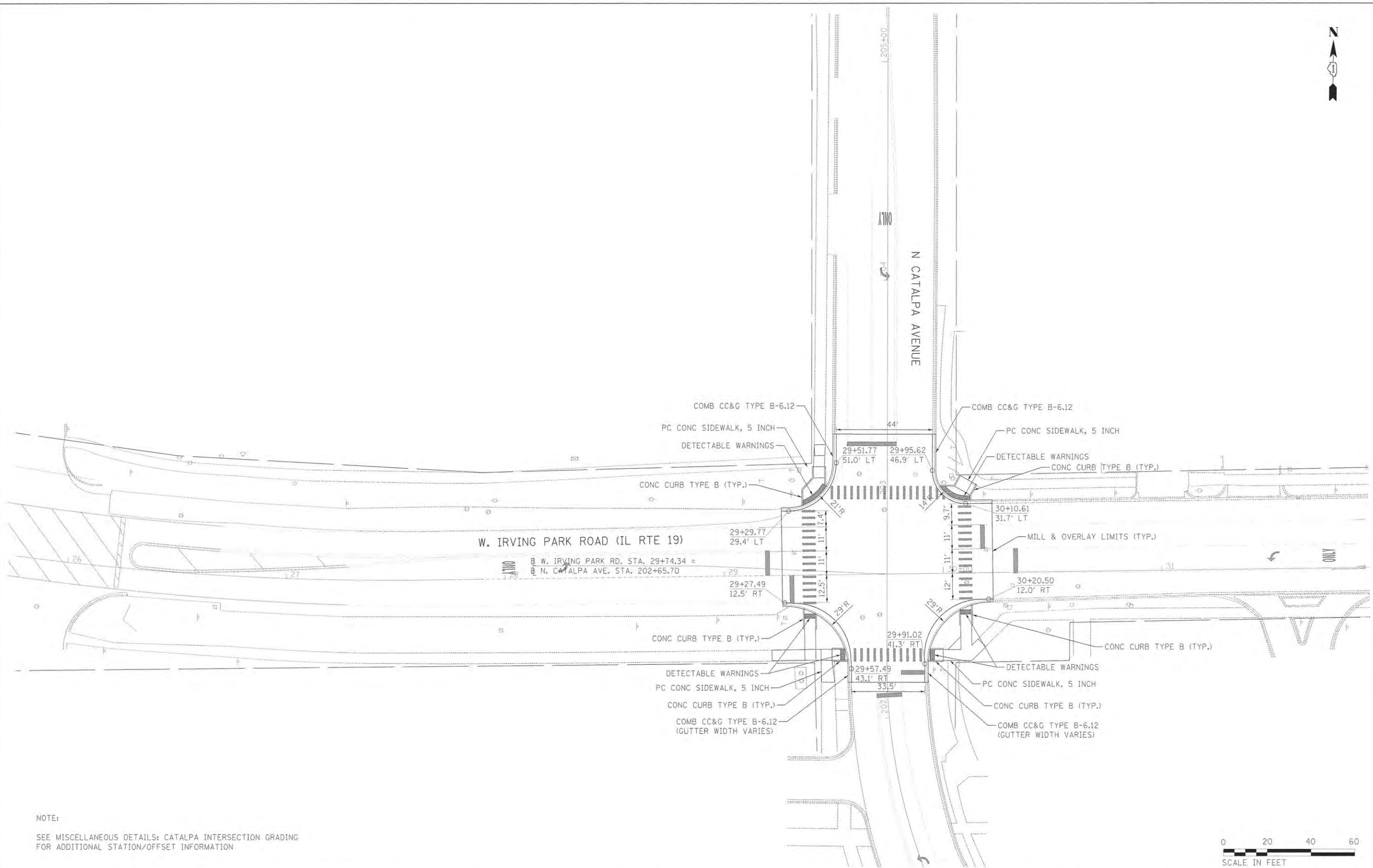
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
 PROPOSED PLAN - IL-19 (IRVING PARK RD) OVER I-290**

SCALE: 1" = 20' SHEET 1 OF 6 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	10-00059-01-SP	DUIPAGE	77	25
			CONTRACT NO. 61C82	
ILLINOIS FED. AID PROJECT				





NOTE:  
SEE MISCELLANEOUS DETAILS: CATALPA INTERSECTION GRADING  
FOR ADDITIONAL STATION/OFFSET INFORMATION



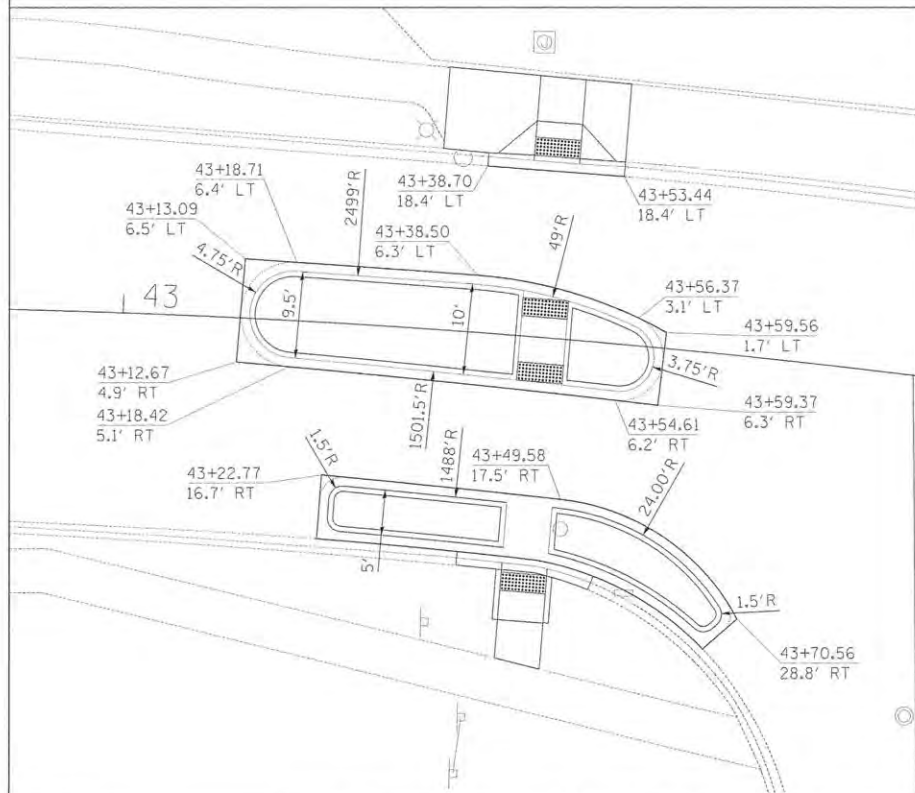
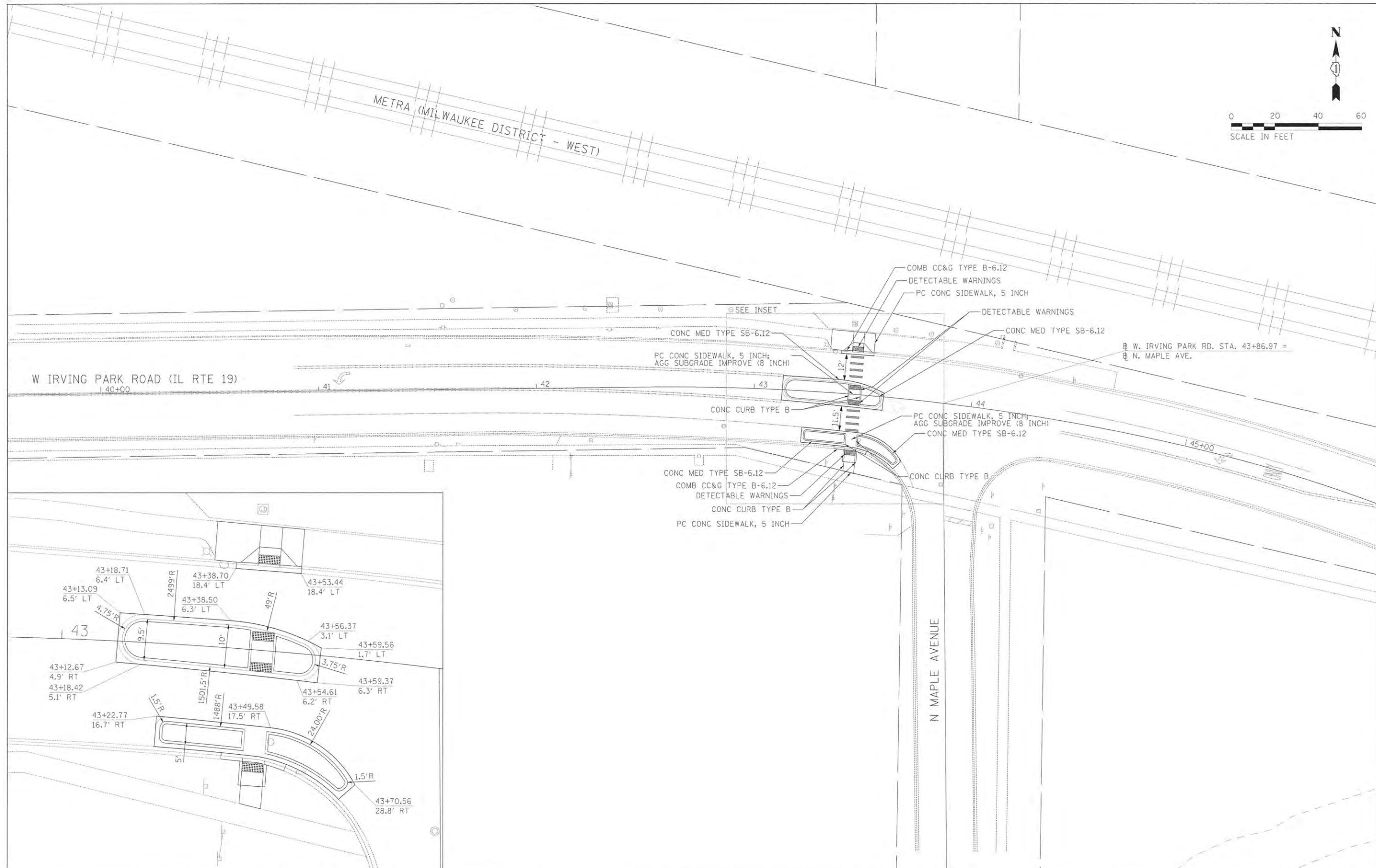
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
PROPOSED PLAN - IL-19 (IRVING PARK RD) AT CATALPA AVE**

SCALE: 1" = 20'    SHEET 2 OF 6 SHEETS    STA.    TO STA.

F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 26
CONTRACT NO. 61C82				
ILLINOIS FED. AID PROJECT				



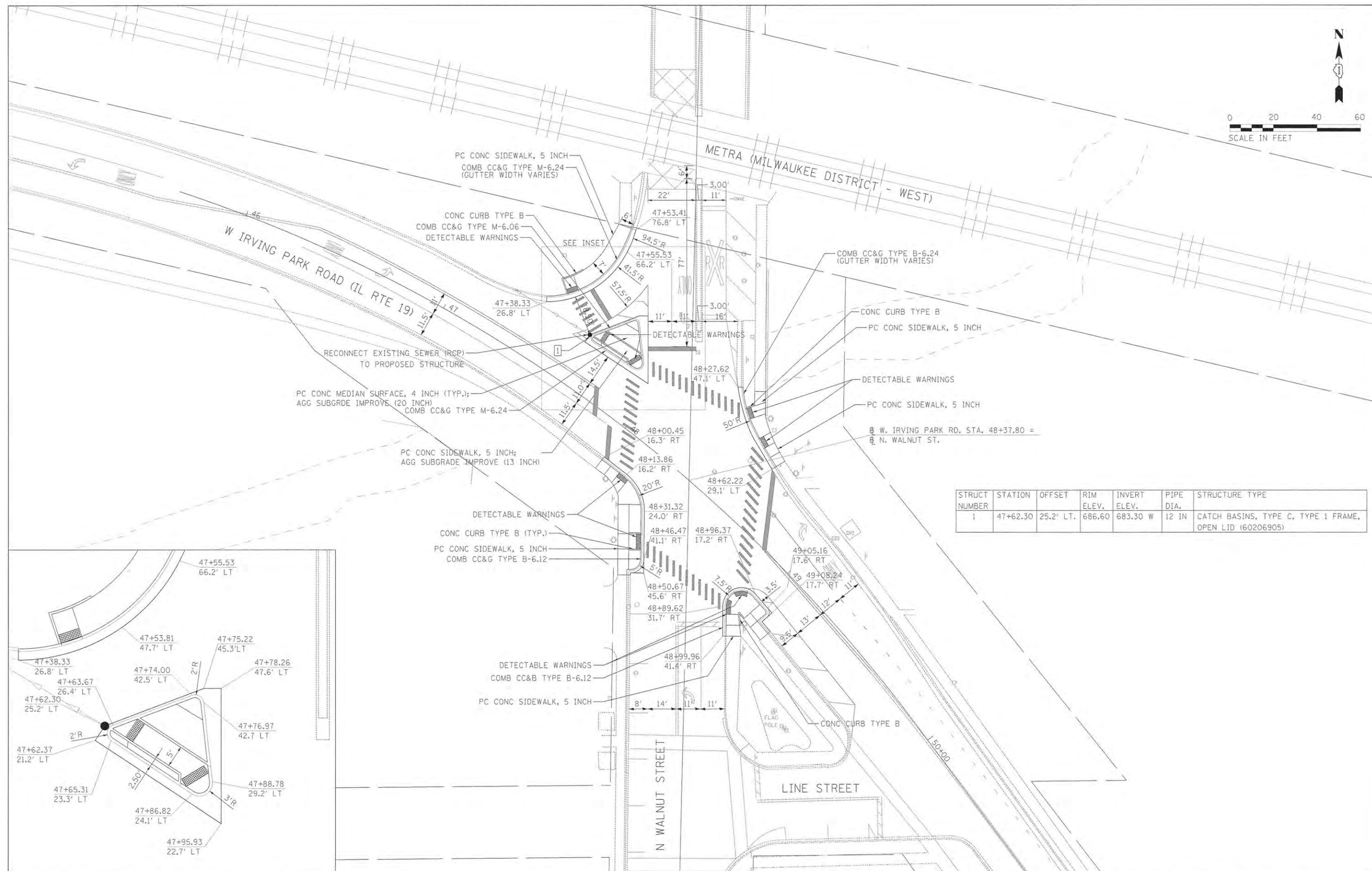
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

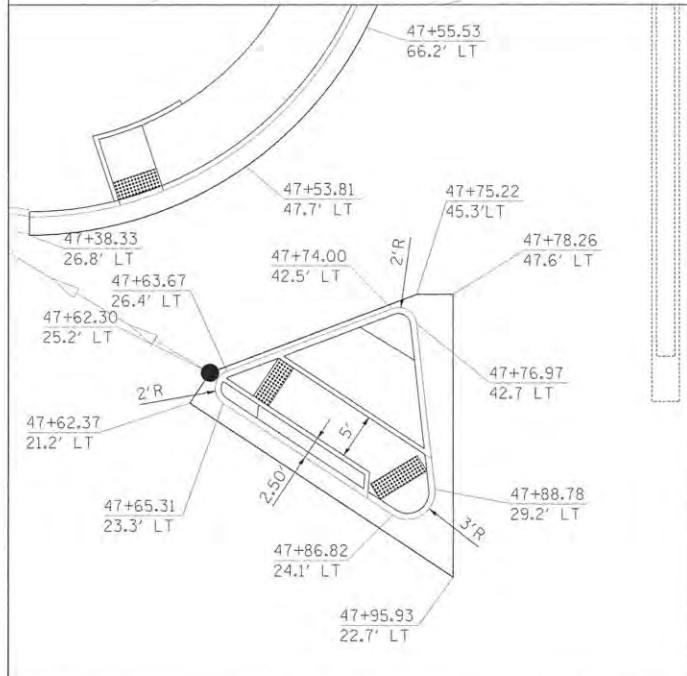
**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
PROPOSED PLAN - IL-19 (IRVING PARK RD) AT MAPLE AVE**

SCALE: 1" = 20'    SHEET 3    OF 6    SHEETS    STA.    TO STA.

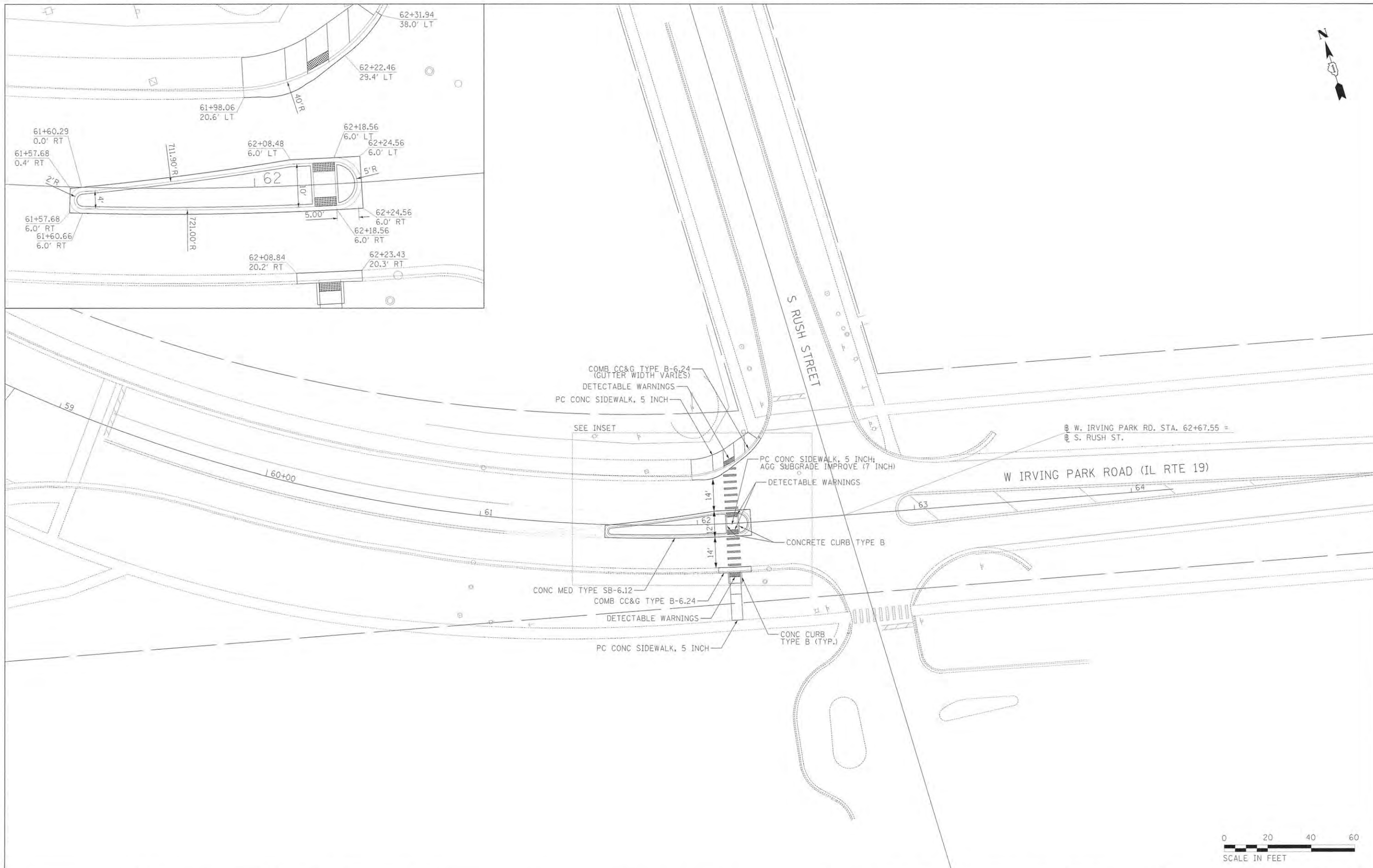
F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 27
CONTRACT NO. 61C82			ILLINOIS FED. AID PROJECT	



STRUCT NUMBER	STATION	OFFSET	RIM ELEV.	INVERT ELEV.	PIPE DIA.	STRUCTURE TYPE
1	47+62.30	25.2' LT.	686.60	683.30 W	12 IN	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID (60206905)





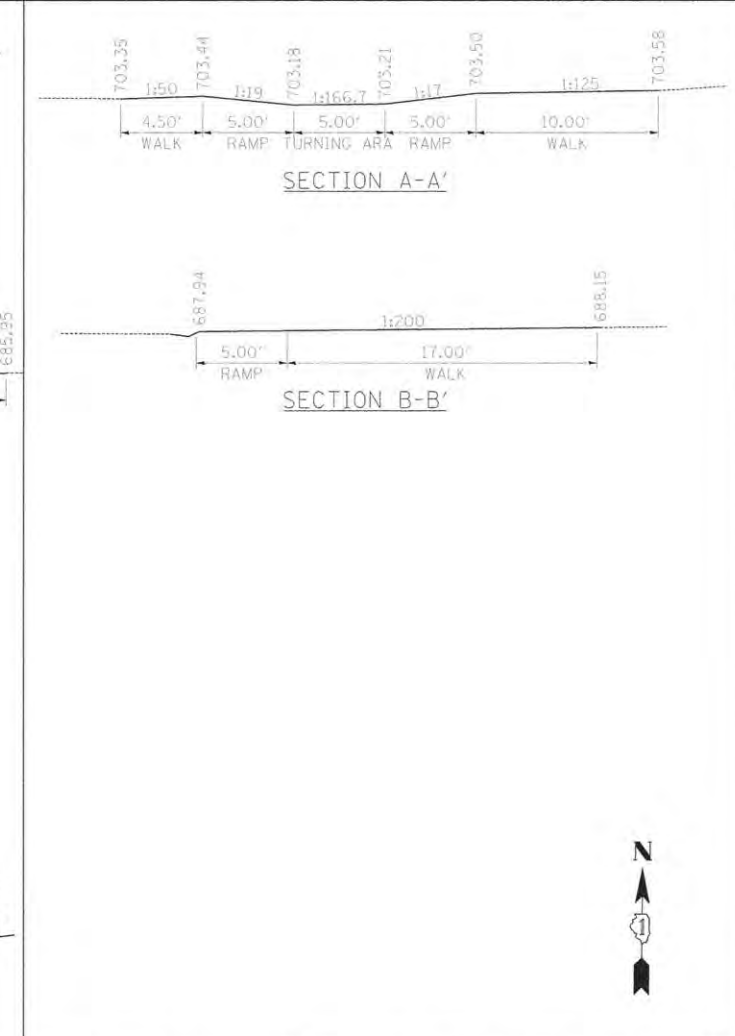
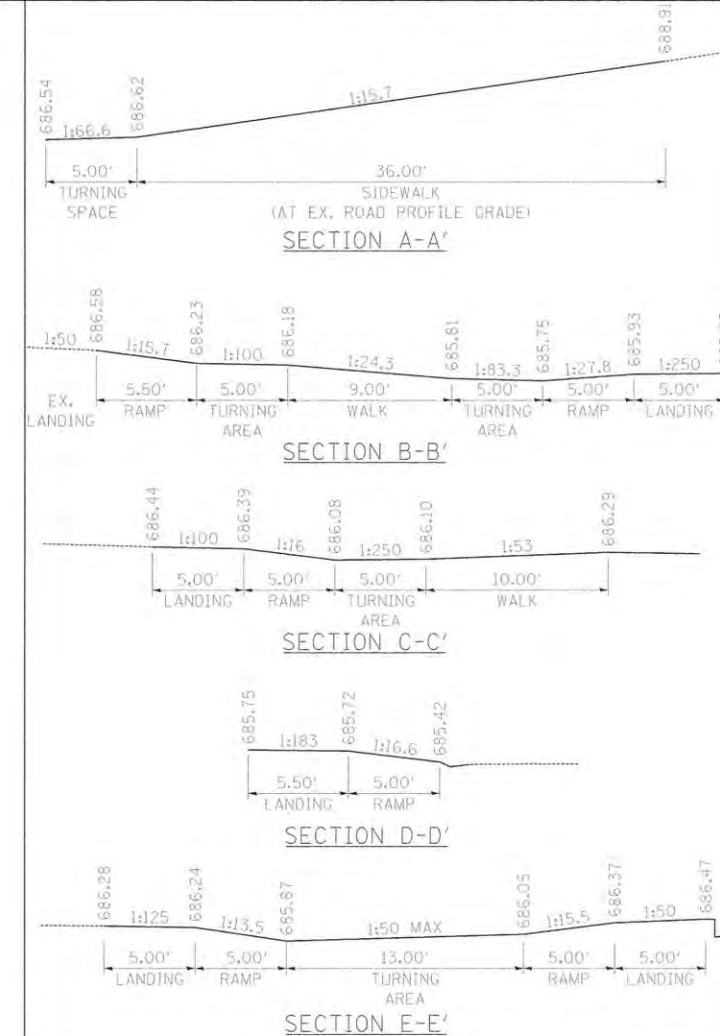
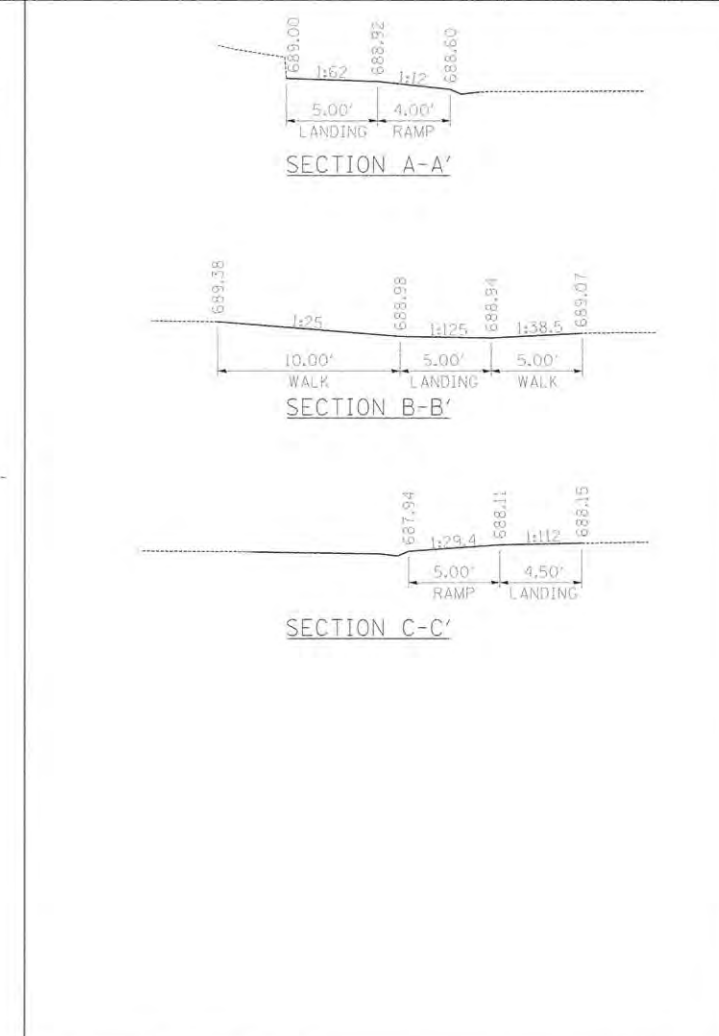
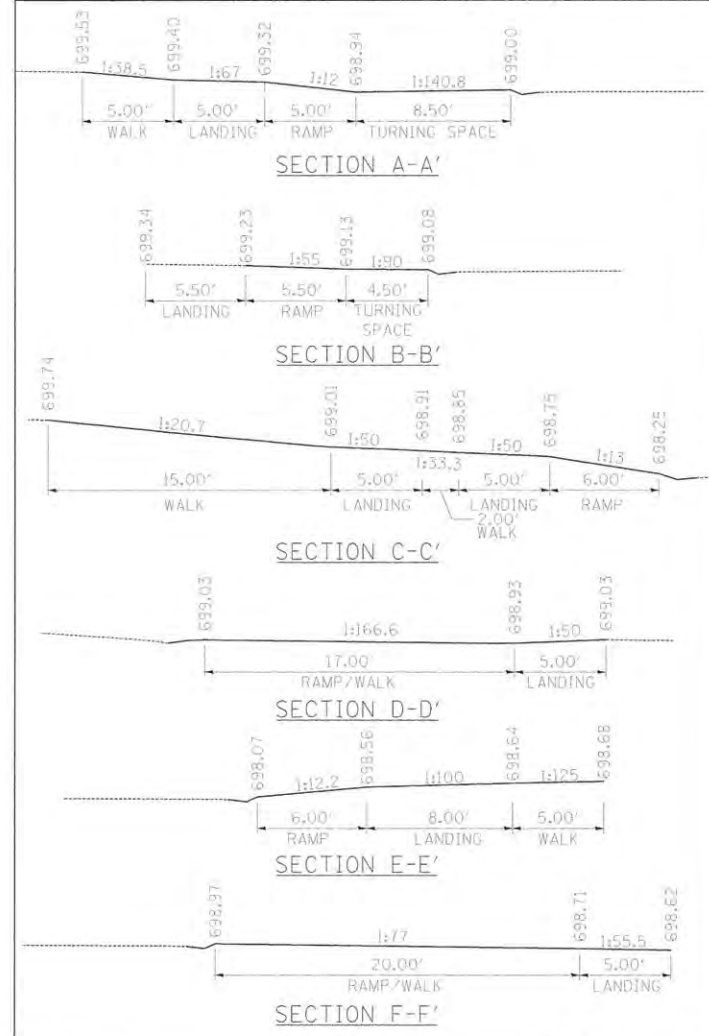
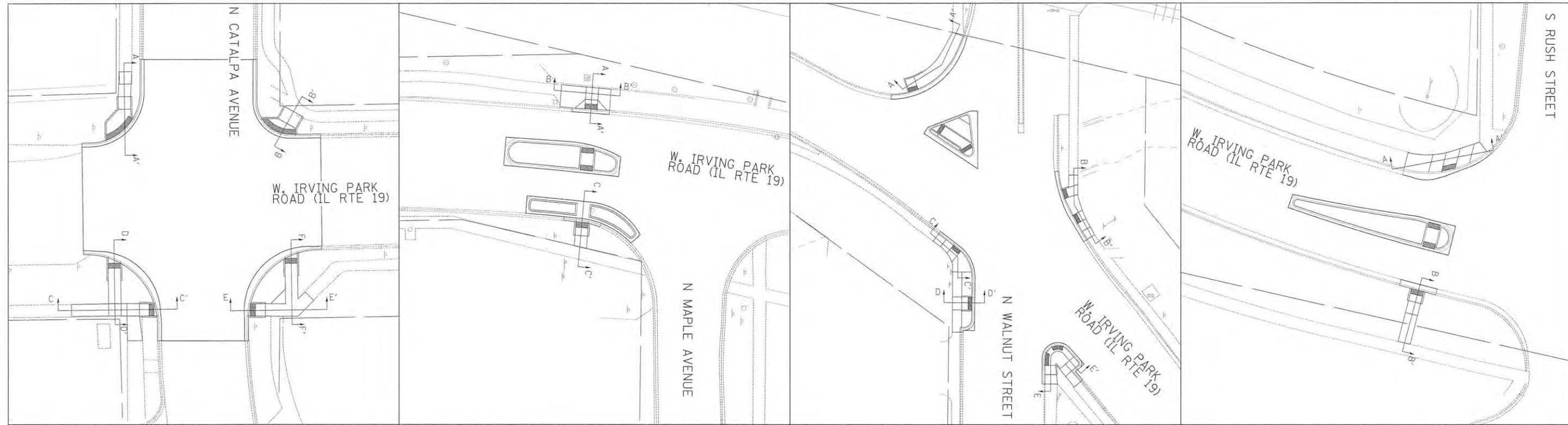


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	PILOT SCALE 20,000 1" = 20'	CHECKED MPG	REVISED -				SCALE: 1" = 20' SHEET 5 OF 6 SHEETS STA. TO STA.		CONTRACT NO. 61C82		ILLINOIS FED. AID PROJECT
Default	PILOT DATE 5/20/2016	DATE 5/20/2016	REVISED -								

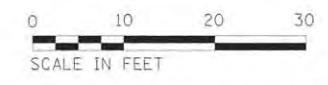
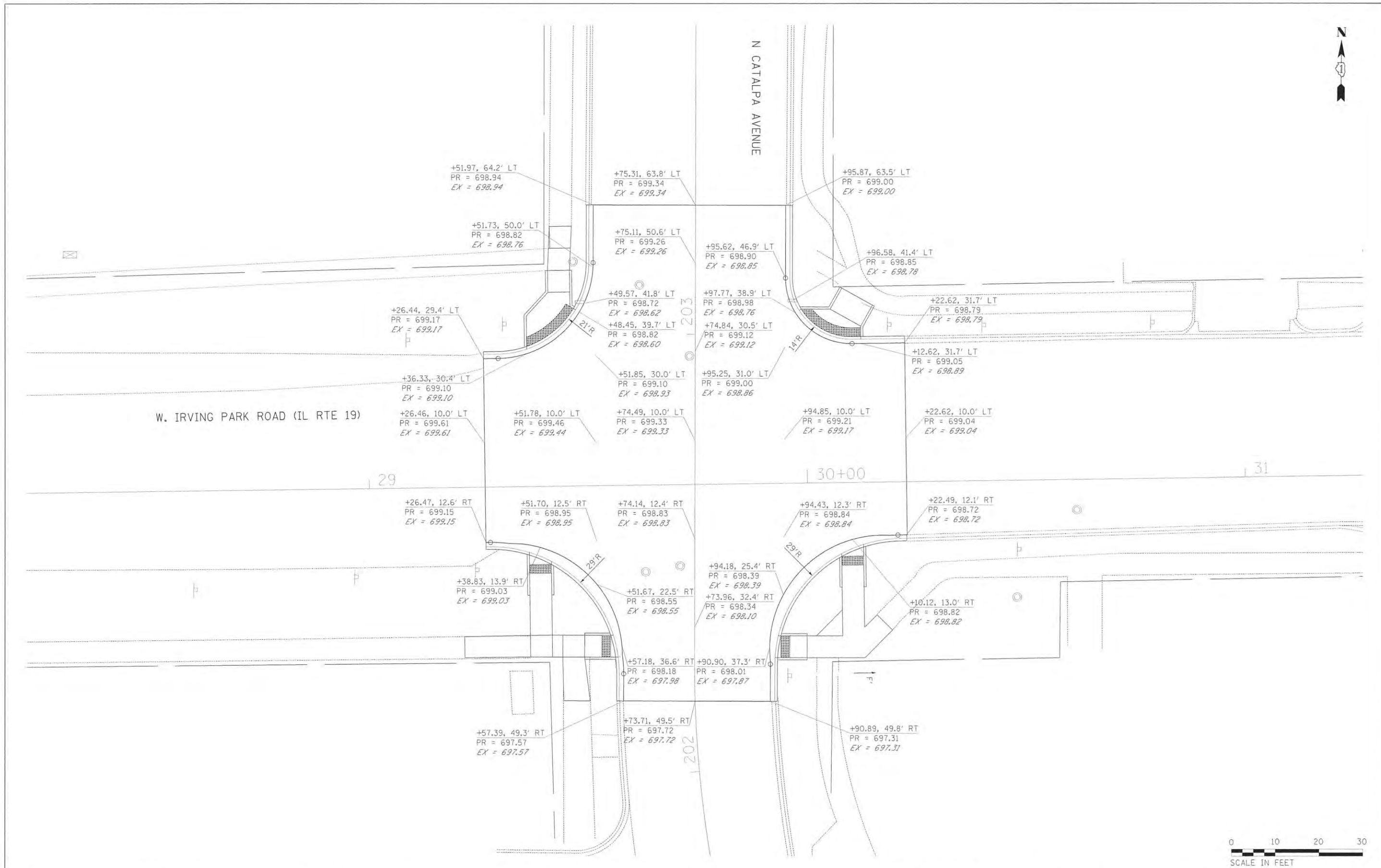








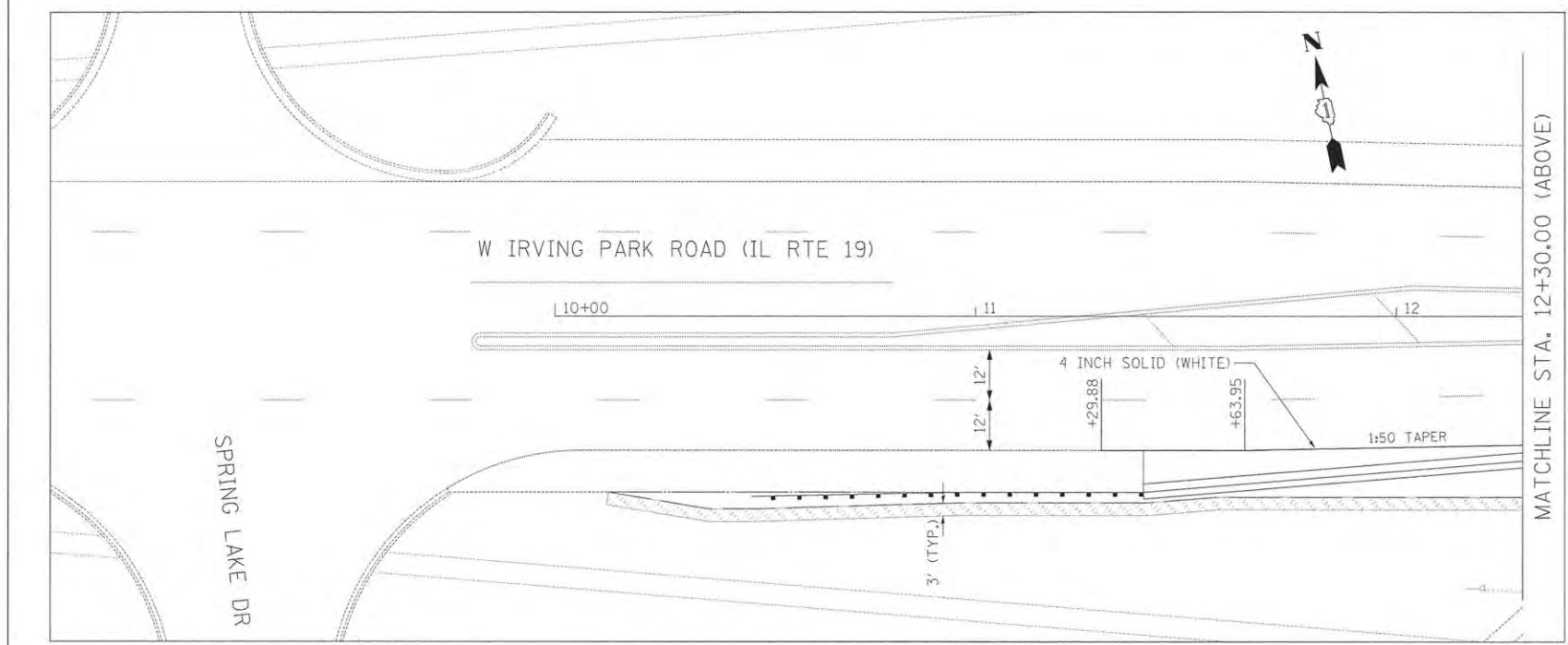
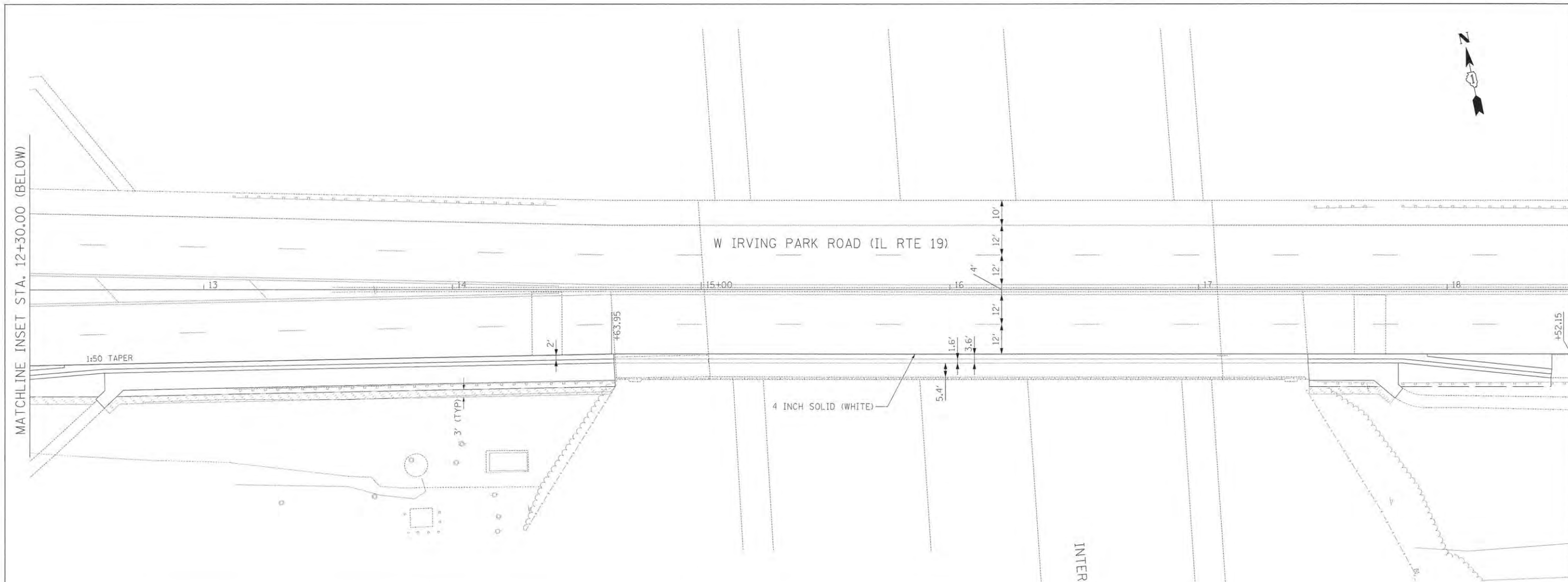
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Plot Scale = 20.0000 "/> <td>Checked - MPG</td> <td>Revised -</td> <td>Revised -</td> <td>SCALE: N.T.S.</td> <td>SHEET 2 OF 4 SHEETS STA.</td> <td>TO STA.</td> <td colspan="5" style="text-align: center;">CONTRACT NO. 61C82</td>	Checked - MPG	Revised -	Revised -		SCALE: N.T.S.	SHEET 2 OF 4 SHEETS STA.	TO STA.	CONTRACT NO. 61C82				
Plot Date = 5/20/2016	Date = 5/20/2016	Revised -	Revised -		ILLINOIS FED. AID PROJECT							



FILE NAME : D:\13AG00_DTL\01_CATALPA\DWG\13AG00.dgn	USER NAME : mrichard	DESIGNED - MPG	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS MISCELLANEOUS DETAILS: CATALPA INTERSECTION GRADING</b>			F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 33
Default	PLD SCALE = 1/8"=1'-0"	DRAWN - MPG	REVISED -		SCALE: N.T.S.	SHEET 3	OF 4 SHEETS	STA. TO STA.	CONTRACT NO. 61C82			
	PLOT DATE = 5/20/2016	CHECKED - DBD	REVISED -						ILLINOIS FED. AID PROJECT			
		DATE - 5/20/2016	REVISED -									







INTERSTATE 290

**LEGEND:**  
 LANDSCAPING:  
 TOPSOIL EXCAVATION & PLACEMENT (21101505)  
 SODDING, SALT TOLERANT (25200110)



FILE NAME =  
 01-ITASCA\_PMK\_L-01-I-290\_01T01.dgn  
 Defwilt

USER NAME = mrschords  
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 PLOT DATE = 5/20/2016

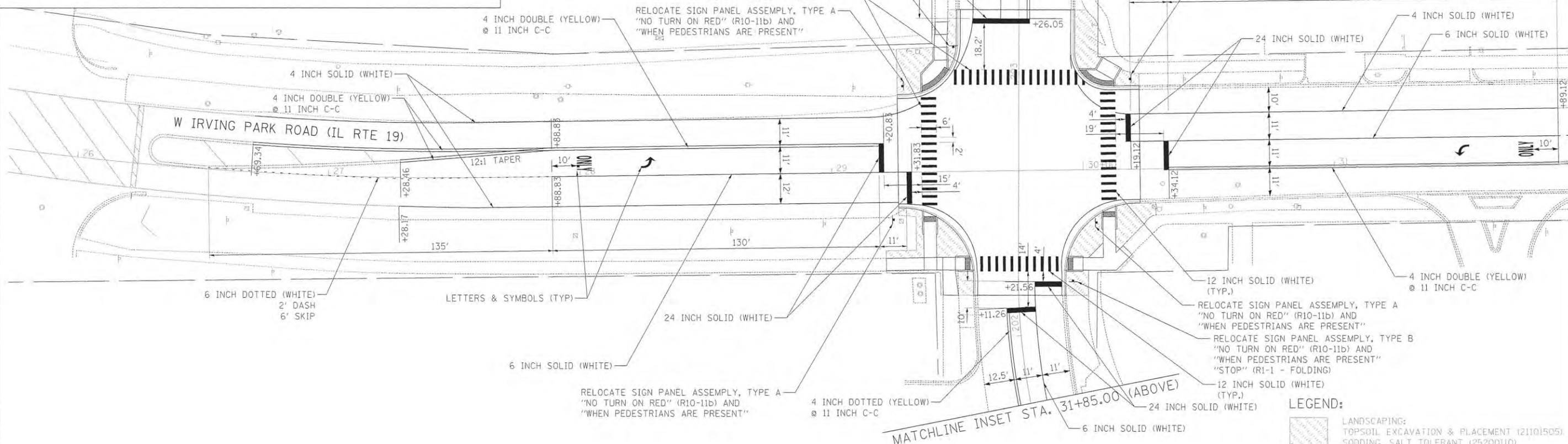
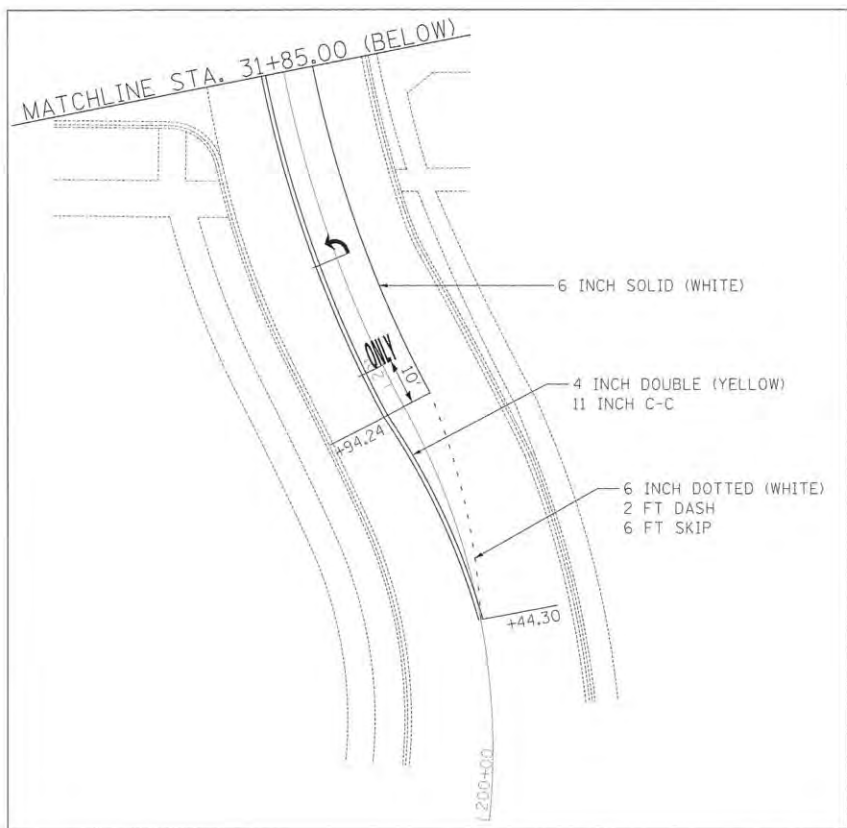
DESIGNED - KRP  
 DRAWN - KRP  
 CHECKED - MPG  
 DATE - 5/20/2016

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
 PAVEMENT MARKING, SIGNING, LANDSCAPING - IL-19 (IRVING PARK RD) OVER I-290**  
 SCALE: 1" = 20" SHEET 1 OF 6 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	10-00059-01-SP	DUPAGE	77	35
CONTRACT NO. 61C82			ILLINOIS FED. AID PROJECT	



**LEGEND:**  
 LANDSCAPING: TOPSOIL EXCAVATION & PLACEMENT (2110)505) SODDING, SALT TOLERANT (25200)10)



FILE NAME = D:\ITASCA_PMC\1-02_CATALPA_SHT01.dgn	USER NAME = mrichards	DESIGNED - KRP	REVISED -
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		DATE - 5/20/2016	REVISED -

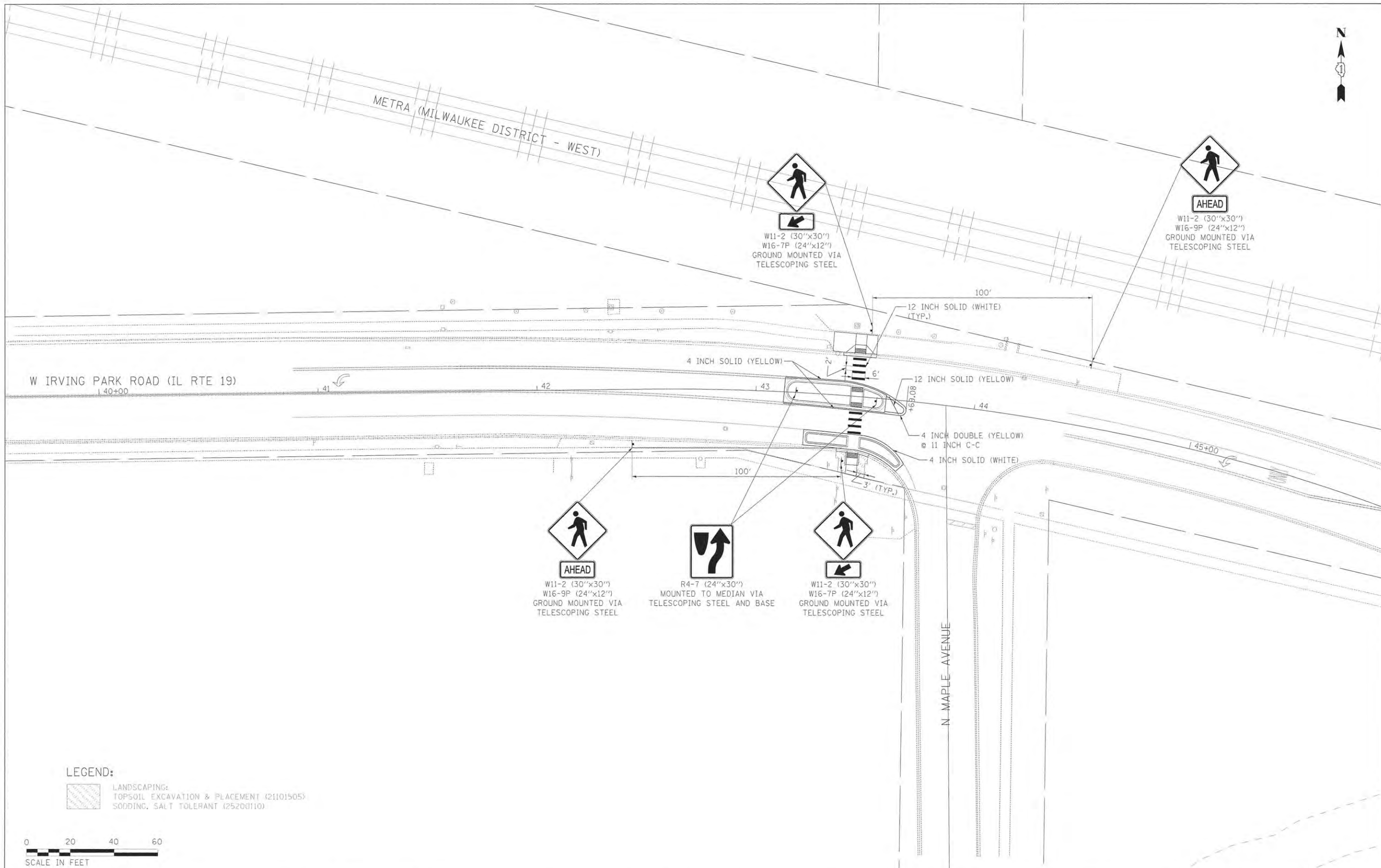
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
PAVEMENT MARKING, SIGNING, LANDSCAPING - IL-19 (IRVING PARK RD) AT CATALPA AVE**

SCALE: 1" = 20' SHEET 2 OF 6 SHEETS STA. TO STA.

F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 36
CONTRACT NO. 61C82			ILLINOIS FED. AID PROJECT	





**LEGEND:**

 LANDSCAPING:  
 TOPSOIL EXCAVATION & PLACEMENT (21101505)  
 SODDING, SALT TOLERANT (25200110)



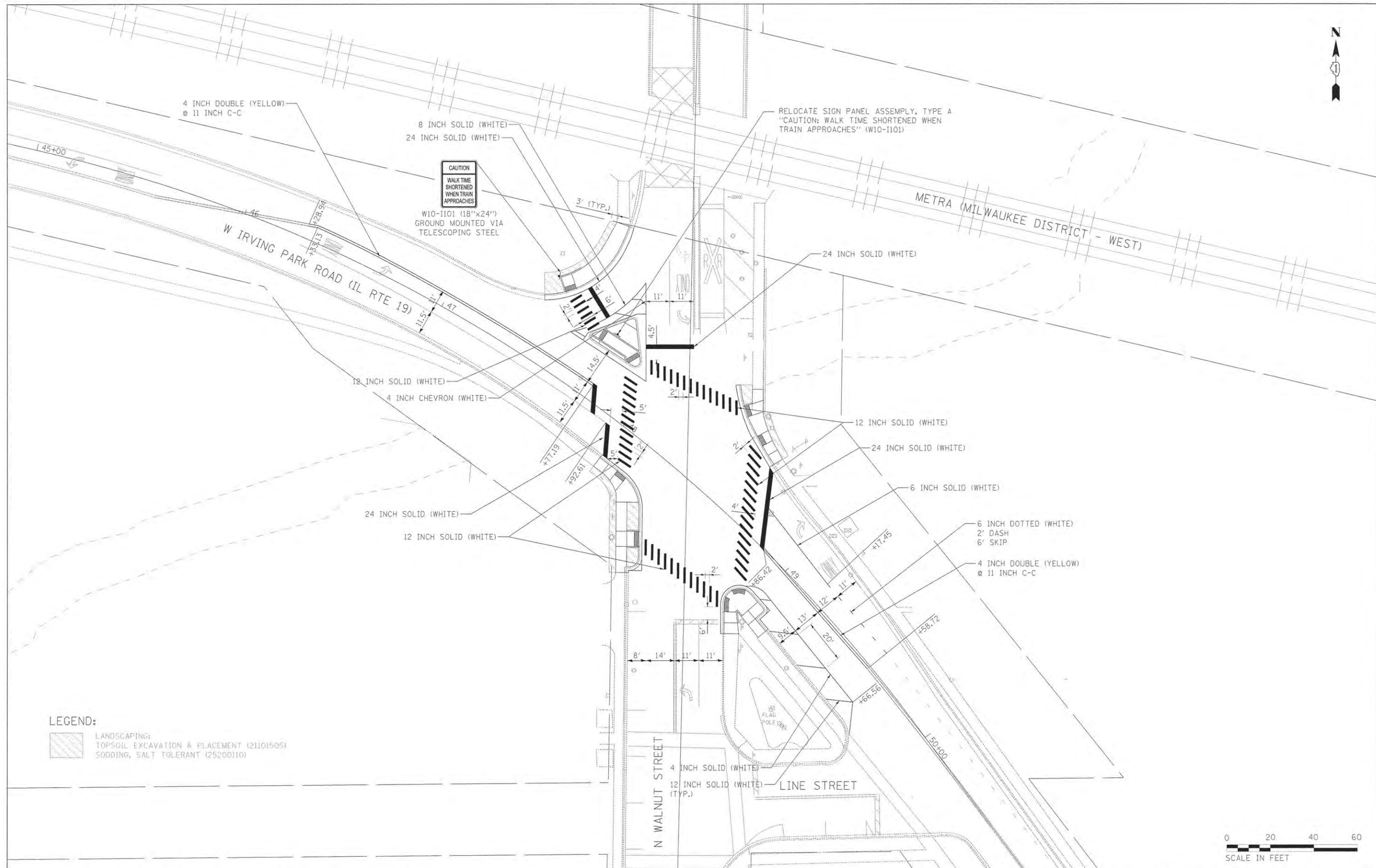
FILE NAME = DI-ITASCA-PKILL-R3-MAPLE-SHT3.dgn	USER NAME = mrichmond	DESIGNED - KRP	REVISED -
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	Plot DATE = 5/20/2016	DATE - 5/20/2016	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS**  
**PAVEMENT MARKING, SIGNING, LANDSCAPING - IL-19 (IRVING PARK RD) AT MAPLE AVE**  
 SCALE: 1" = 20' SHEET 3 OF 6 SHEETS STA. TO STA.

F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 37
CONTRACT NO. 61C82			[ILLINOIS] FED. AID PROJECT	





**LEGEND:**

- LANDSCAPING: TOPSOIL EXCAVATION & PLACEMENT (21101505)
- SODDING, SALT TOLERANT (25200110)

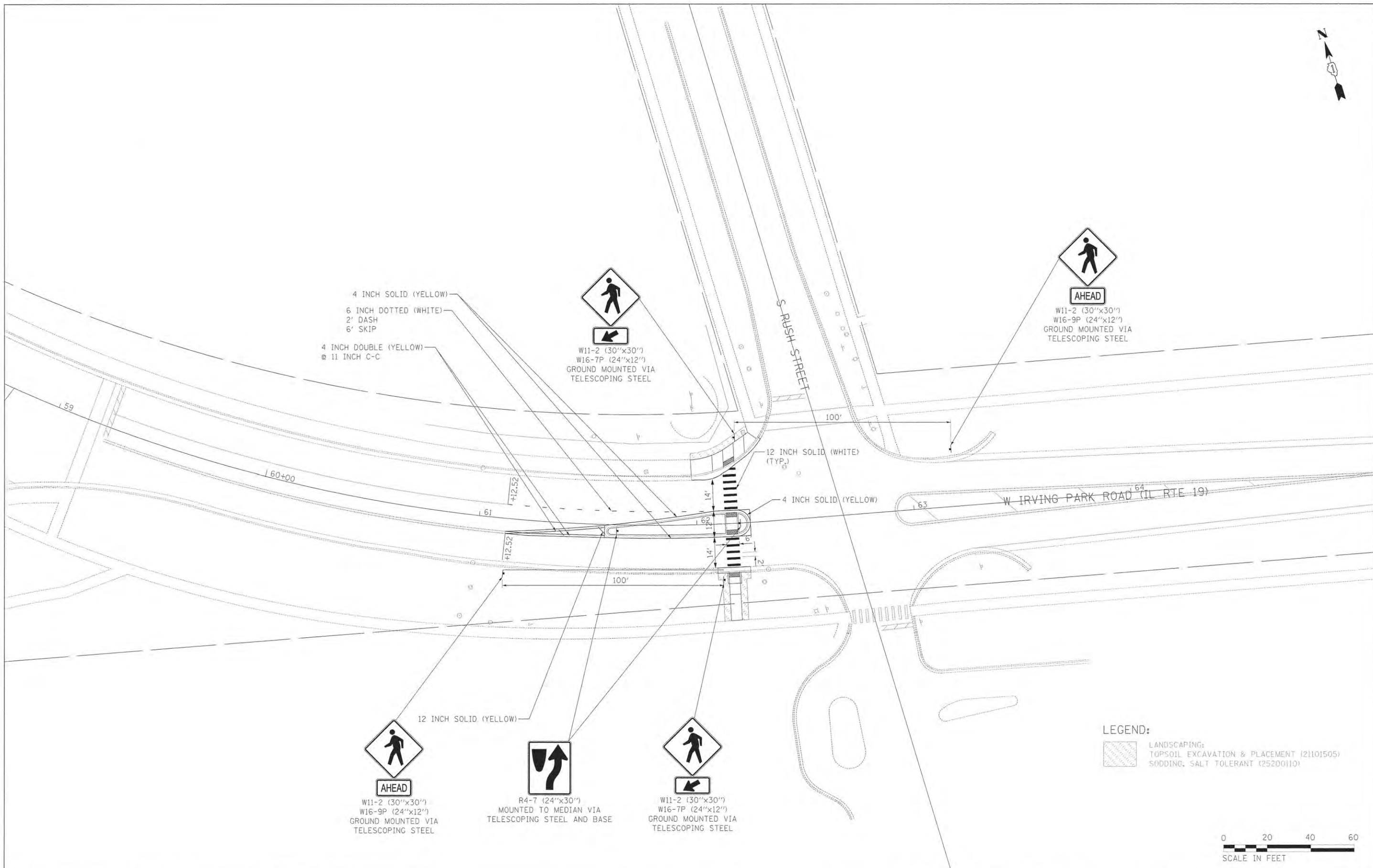


FILE NAME = 01-ITASCA_PMK-L-04-WALNUT_SHT01.dgn	USER NAME = mrjchard	DESIGNED - KRP	REVISED -
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		DATE - 5/20/2016	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS**  
**PAVEMENT MARKING, SIGNING, LANDSCAPING - IL-19 (IRVING PARK RD) AT WALNUT ST**  
 SCALE: 1" = 20' SHEET 4 OF 6 SHEETS STA. TO STA.

F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 38
CONTRACT NO. 61C82			ILLINOIS FED. AID PROJECT	



4 INCH SOLID (YELLOW)  
 6 INCH DOTTED (WHITE)  
 2' DASH  
 6' SKIP  
 4 INCH DOUBLE (YELLOW)  
 @ 11 INCH C-C

W11-2 (30"x30")  
 W16-7P (24"x12")  
 GROUND MOUNTED VIA  
 TELESCOPING STEEL

AHEAD  
 W11-2 (30"x30")  
 W16-9P (24"x12")  
 GROUND MOUNTED VIA  
 TELESCOPING STEEL

S RUSH STREET

12 INCH SOLID (WHITE)  
 (TYP.)

4 INCH SOLID (YELLOW)

W IRVING PARK ROAD (IL RTE 19)

12 INCH SOLID (YELLOW)

AHEAD  
 W11-2 (30"x30")  
 W16-9P (24"x12")  
 GROUND MOUNTED VIA  
 TELESCOPING STEEL

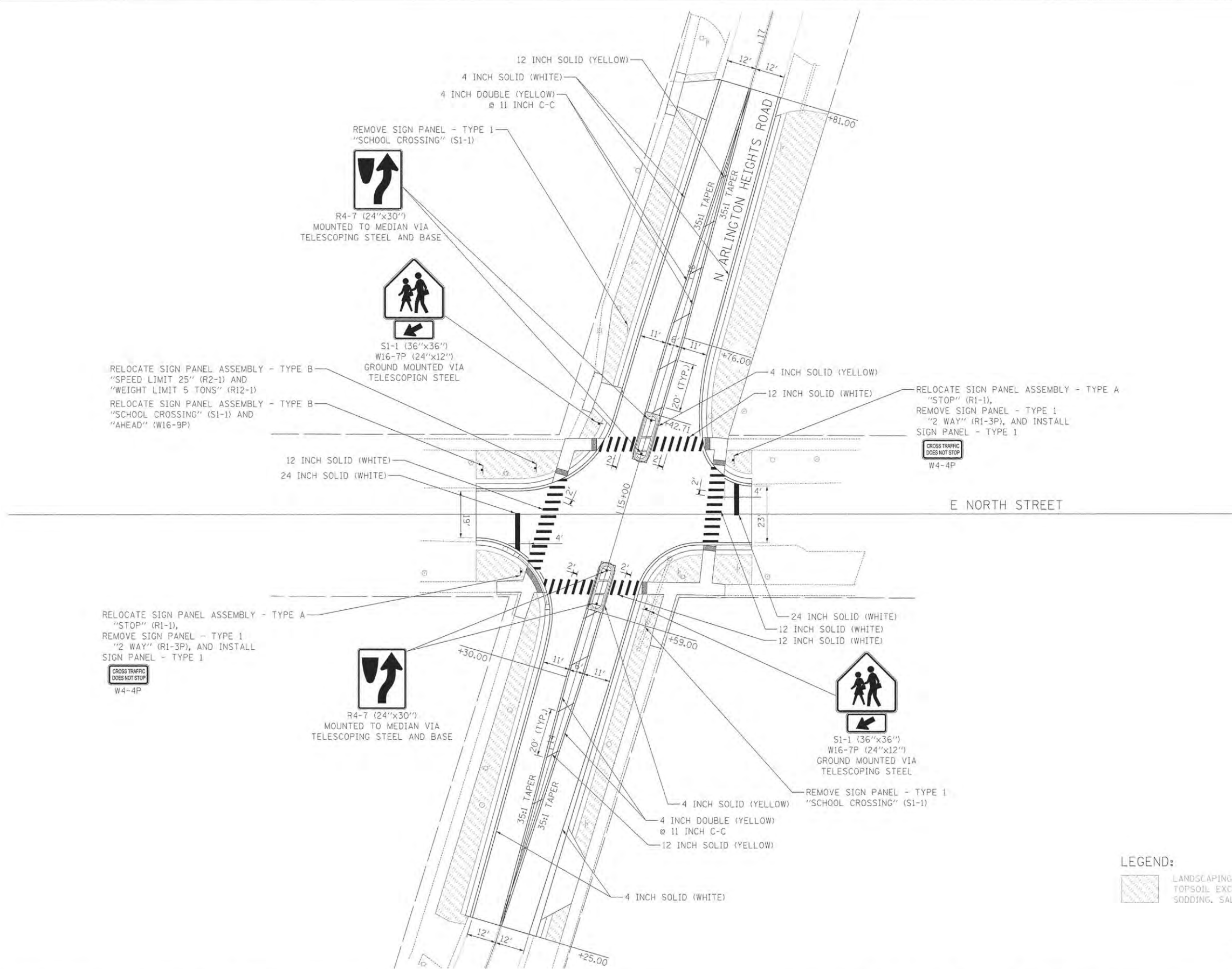
R4-7 (24"x30")  
 MOUNTED TO MEDIAN VIA  
 TELESCOPING STEEL AND BASE

W11-2 (30"x30")  
 W16-7P (24"x12")  
 GROUND MOUNTED VIA  
 TELESCOPING STEEL

LEGEND:  
 LANDSCAPING:  
 TOPSOIL EXCAVATION & PLACEMENT (21101505)  
 SODDING, SALT TOLERANT (25200110)



FILE NAME = 01-ITASCA_PMK_LL-B5L_RUSH_SHT01.dgn	USER NAME = mrichards	DESIGNED - KRP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS PAVEMENT MARKING, SIGNING, LANDSCAPING - IL-19 (IRVING PARK RD) AT RUSH ST</b>			F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 39
Default	PLOT SCALE = 20,0000 1" = 20'	CHECKED - MPG	REVISED -		SCALE: 1" = 20'	SHEET 5	OF 6	SHEETS STA.	TO STA.	CONTRACT NO. 61C82		
	PLOT DATE = 5/20/2016	DATE - 5/20/2016	REVISED -		ILLINOIS FED. AID PROJECT							



FILE NAME =	USER NAME = mrichards	DESIGNED = KRP	REVISED =
DI-ITASCA-FMK-L-06-ARLINGTON-NORTH-SH18	3jgn	DRAWN = KRP	REVISED =
Default	PLOT SCALE = 20.0000 / 1 in.	CHECKED = MPG	REVISED =
	PLOT DATE = 5/20/2016	DATE = 5/20/2016	REVISED =

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
PAVEMENT MARKING, SIGNING, LANDSCAPING - ARLINGTON HEIGHTS RD AT NORTH ST**

SCALE: 1" = 20' SHEET 6 OF 6 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2626	10-00059-01-SP	DUPAGE	77	40
				CONTRACT NO. 61C82
[ILLINOIS] FED. AID PROJECT SRTS-4009(087)				



# TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE			
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA			
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED			
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
UNINTERRUPTABLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F			
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				UNDERGROUND CONDUIT, GALVANIZED STEEL (UC)				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F			
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F			
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F			
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM		S		CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM		I		STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED			
SIGNAL POST				REMOVE ITEM	R			ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED			
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM	RL			STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED			
GUY WIRE				ABANDON ITEM	A			SIGNAL POST AND FOUNDATION TO BE REMOVED			
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				INTERSECTION & SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				QUEUE DETECTOR			
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD				PREFORMED QUEUE DETECTOR			
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				"RB" INDICATES REFLECTIVE BACKPLATE				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				PREFORMED SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED							
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID							
ILLUMINATED SIGN "NO LEFT TURN"				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER							
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO INTERCONNECT							
DETECTOR LOOP, TYPE I				RADIO REPEATER							
PREFORMED DETECTOR LOOP				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED							
MICROWAVE VEHICLE SENSOR				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)							
VIDEO DETECTION CAMERA											
VIDEO DETECTION ZONE											
PAN, TILT, ZOOM CAMERA											
WIRELESS DETECTOR SENSOR											
WIRELESS ACCESS POINT											

## RAILROAD SYMBOLS

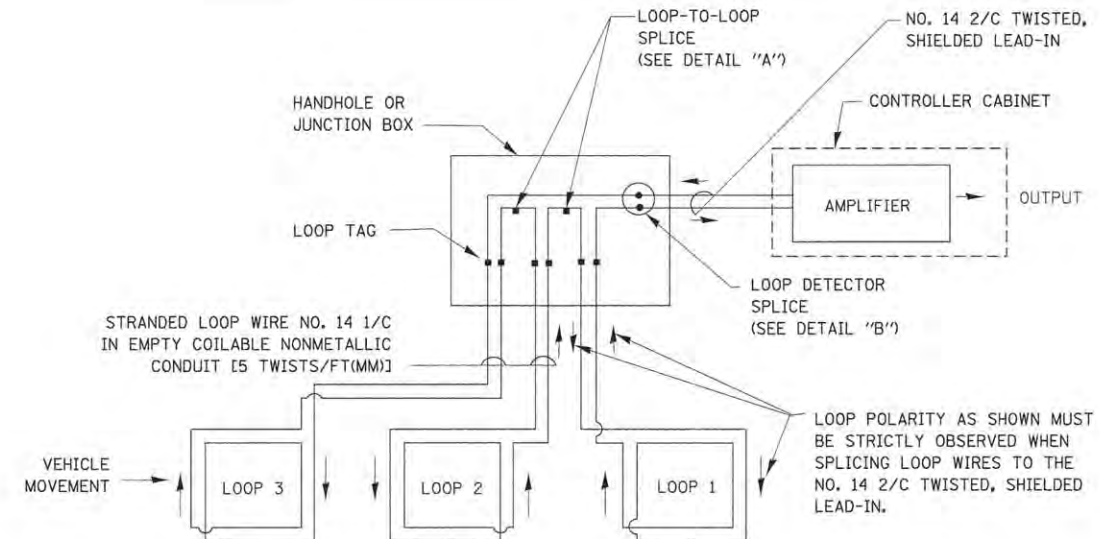
	EXISTING	PROPOSED
RAILROAD CONTROL CABINET		
RAILROAD CANTILEVER MAST ARM		
FLASHING SIGNAL		
CROSSING GATE		
CROSSBUCK		

TS SHT NO. 1



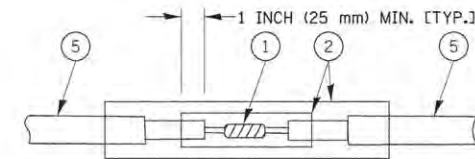
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

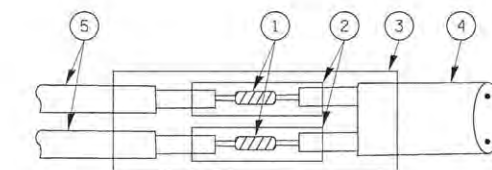


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

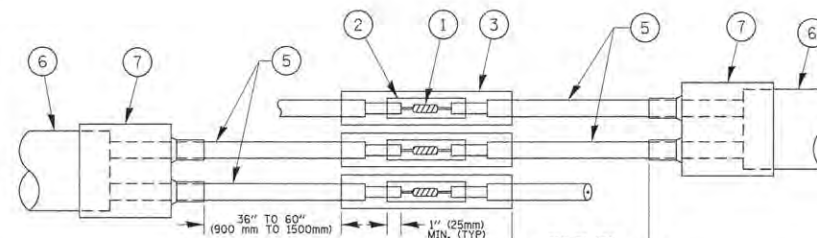


DETAIL "A"  
LOOP-TO-LOOP SPLICE

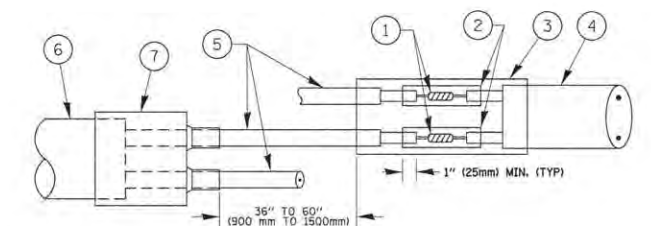


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**TYPE I LOOP**



DETAIL "A"  
LOOP-TO-LOOP SPLICE



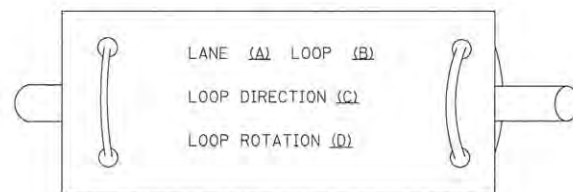
DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**PREFORMED LOOP**

**LOOP DETECTOR SPLICE**

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- ⑥ PREFORMED LOOP
- ⑦ XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

**LOOP LEAD-IN CABLE TAG**

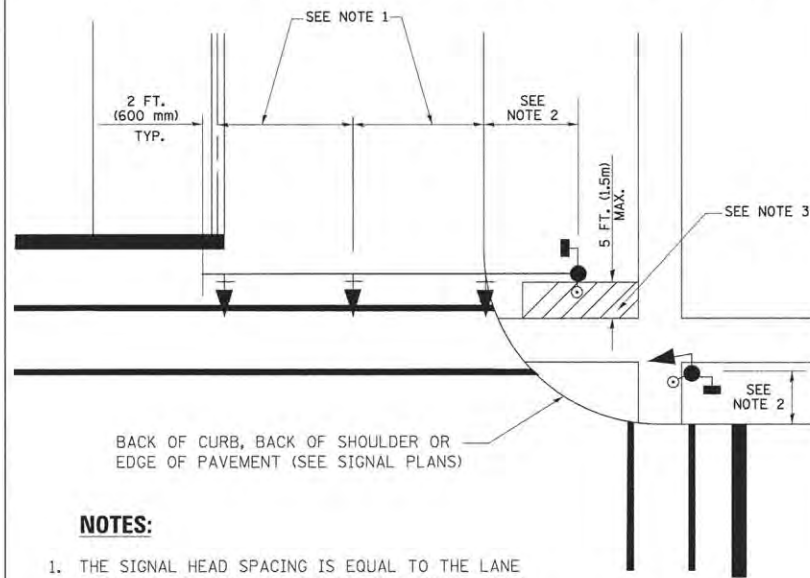


- A. LANE #1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

TS SHT NO. 2

FILE NAME =	USER NAME = foatemj	DESIGNED - DAD	REVISED - DAG 1-1-14	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>			F.A.I.L. RT. E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ts\p\work\p\w\dot\foatemj\00108315\ts05.dgn					DRAWN - BCK	SCALE: NONE	SHEET NO. 2 OF 7 SHEETS	STA.	1321	10-00059-01-SP	DUPAGE	77	42
PLOT SCALE = 50:0000 1" = 100'					CHECKED - DAD	TO STA.			<b>TS-05</b>		CONTRACT NO. <b>61C82</b>		
PLOT DATE = 1/13/2014					DATE - 10-28-09					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

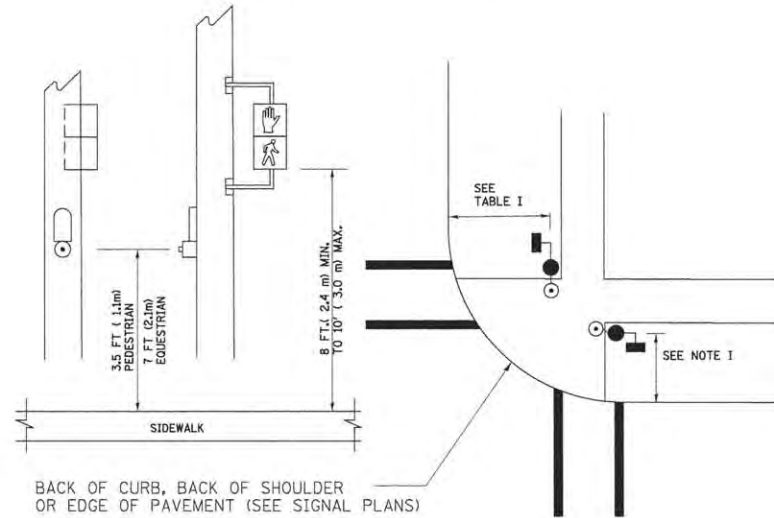
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST  
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR  
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN  
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.**



**NOTES:**

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

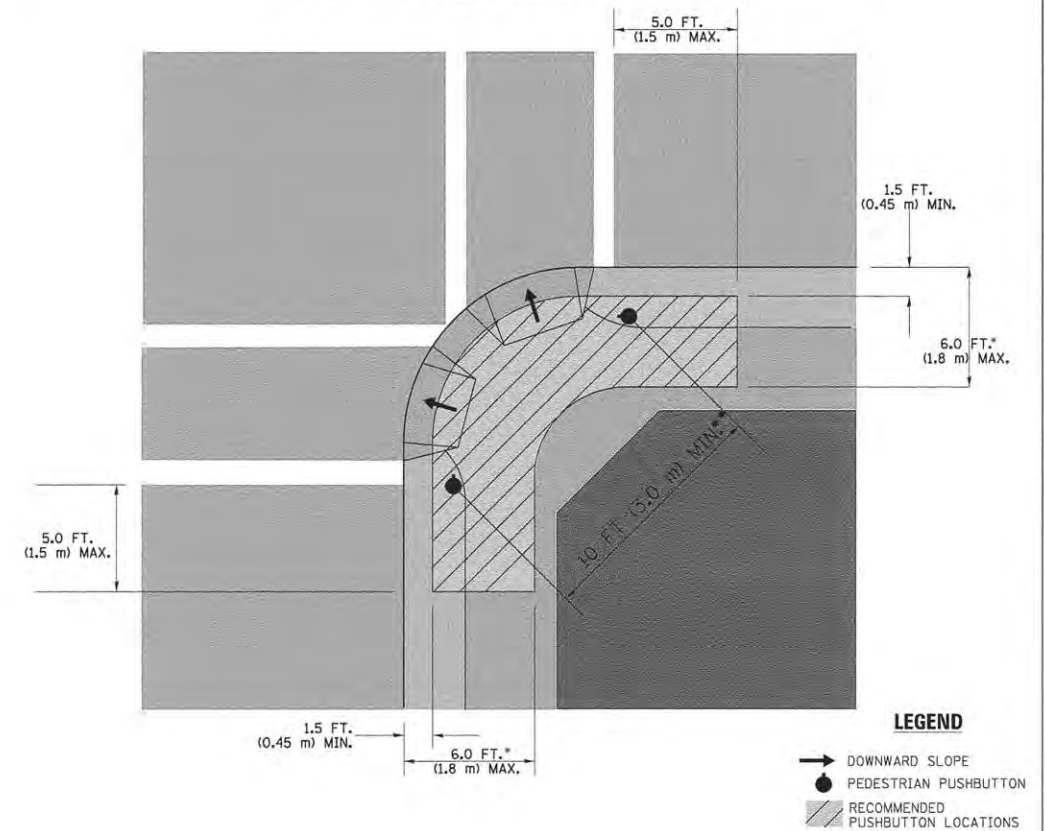
**PEDESTRIAN SIGNAL POST  
AND  
PEDESTRIAN PUSH BUTTON POST**



**NOTES:**

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

**RECOMMENDED PUSHBUTTON LOCATIONS**



**LEGEND**

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- RECOMMENDED PUSHBUTTON LOCATIONS

- \* WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- \*\* WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

**NOTES:**

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

**TRAFFIC SIGNAL EQUIPMENT OFFSET**

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

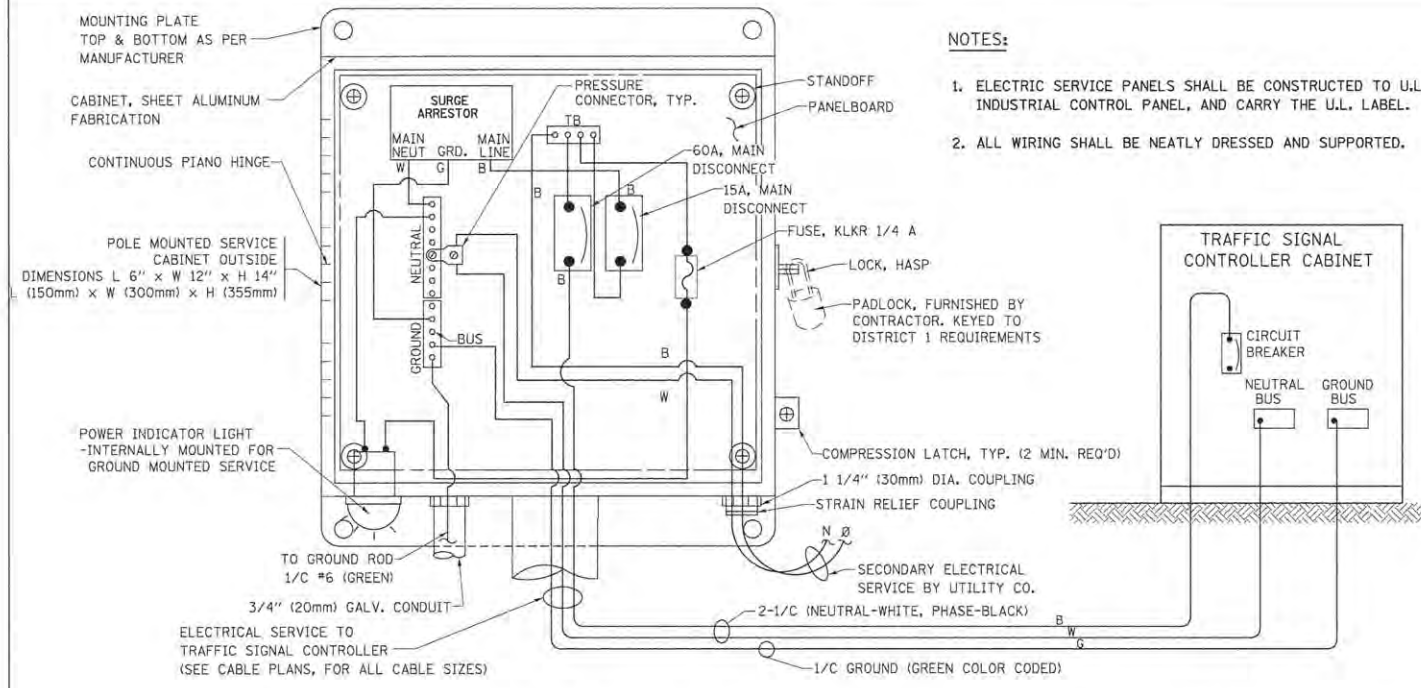
**NOTES:**

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD AFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

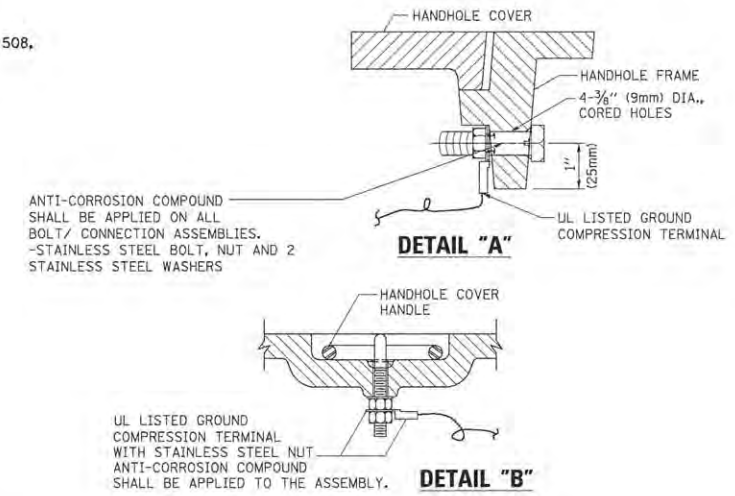
TS SHT NO. 3

FILE NAME =	USER NAME = footenj	DESIGNED - DAD	REVISED - DAG 1-1-14	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>		<b>DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>			F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\footenj\d0109315\ts05.dgn	DRAWN - BCK	REVISOR -	REVISOR -						1321	10-00059-01-SP	DUPAGE	77	43
PLOT SCALE = 5/8"=1'-0"	CHECKED - DAD	REVISOR -	REVISOR -	SCALE: NONE			SHEET NO. 3 OF 7 SHEETS	STA. TO STA.	<b>TS-05</b>		<b>CONTRACT NO. 61C82</b>		
PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISOR -	REVISOR -						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

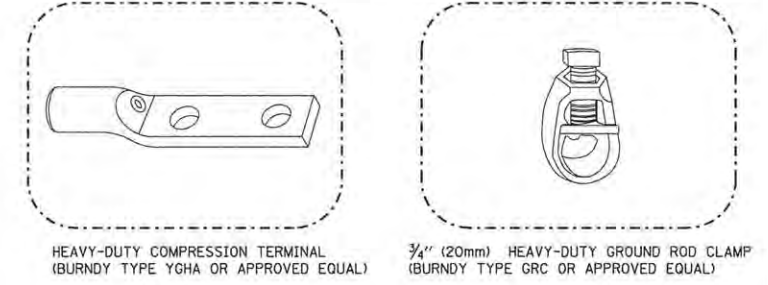
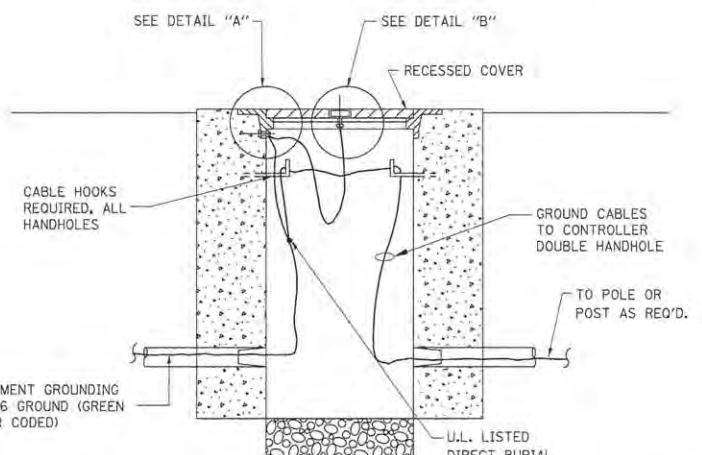




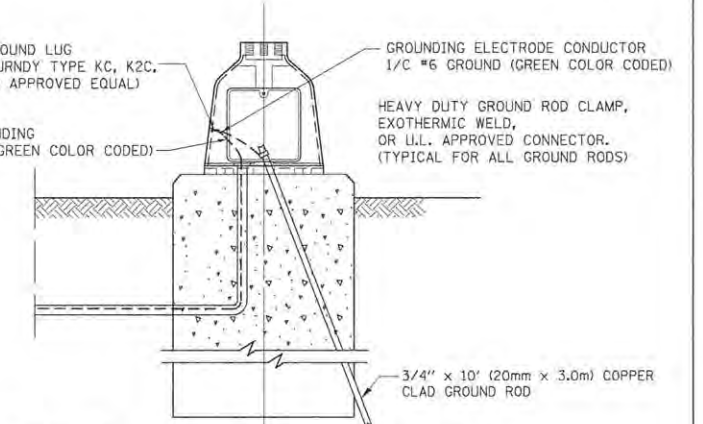
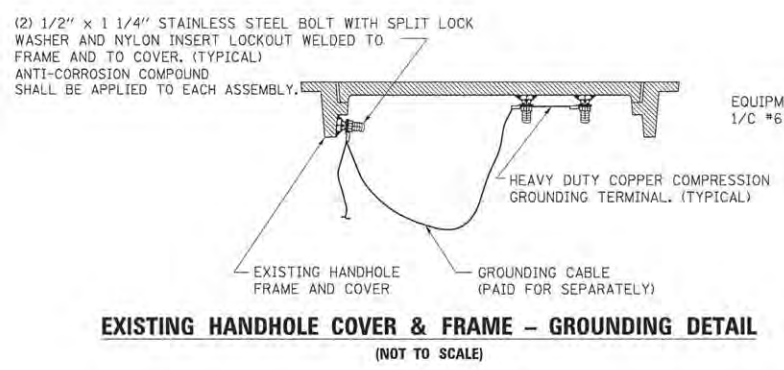
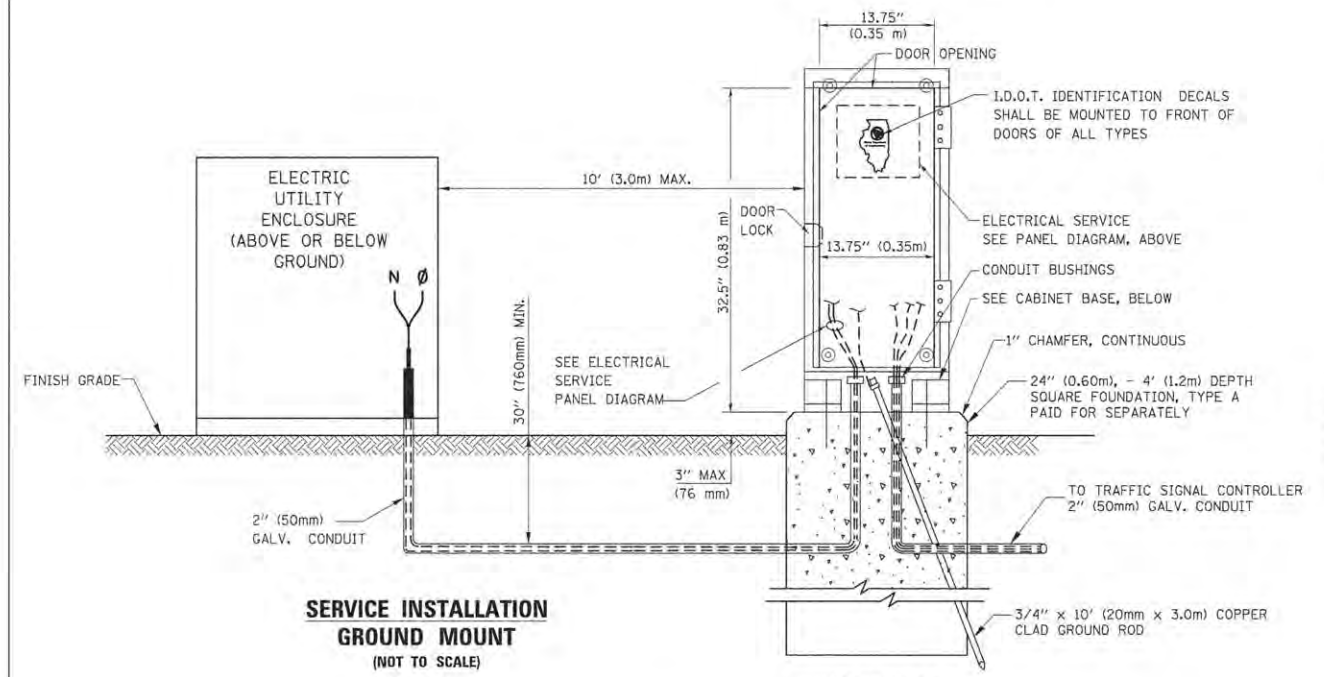
**ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)**  
**SERVICE INSTALLATION POLE MOUNT (SHOWN)**  
 (NOT TO SCALE)



- NOTES:**
- GROUNDING SYSTEM**
- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
  - THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
  - ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
  - THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

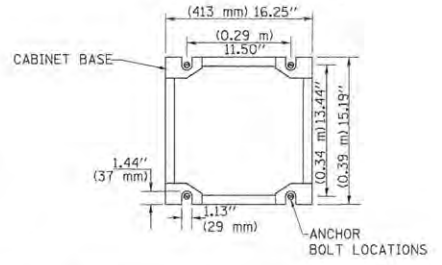


- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, U.L. APPROVED.
  - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
  - 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
  - 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
  - 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



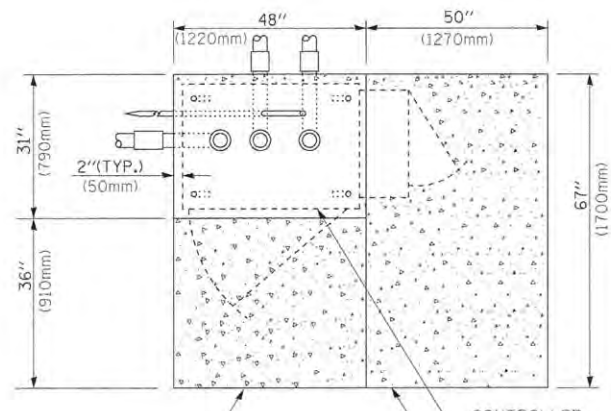
**SERVICE INSTALLATION GROUND MOUNT**  
 (NOT TO SCALE)

**CABINET – BASE BOLT PATTERN**  
 (NOT TO SCALE)

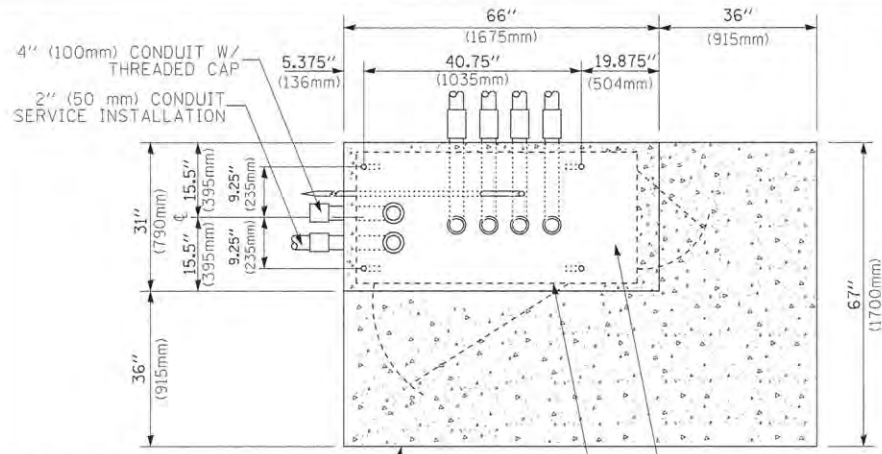


TS SHT NO. 4

FILE NAME =	USER NAME = foatemj	DESIGNED - DAD	REVISED - DAG 1-1-14	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>			F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 44	
PLOT SCALE = 50,0000 1/4 in.					CHECKED - DAD	REVISED -	<b>TS-05</b>			CONTRACT NO. 61C82			
PLOT DATE = 1/13/2014					DATE - 10-28-09	REVISED -	SCALE: NONE	SHEET NO. 4 OF 7 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

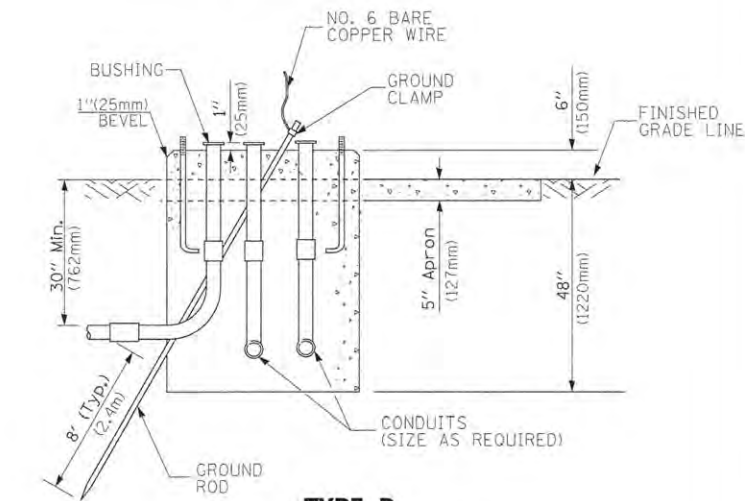


**TOP VIEW**

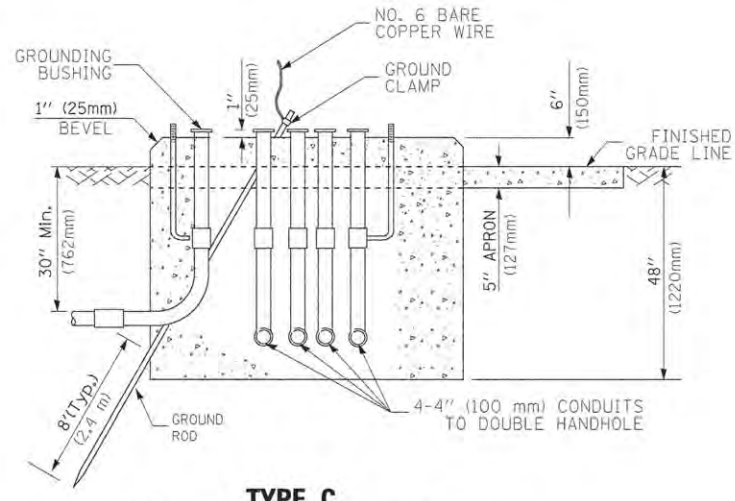


**TOP VIEW**

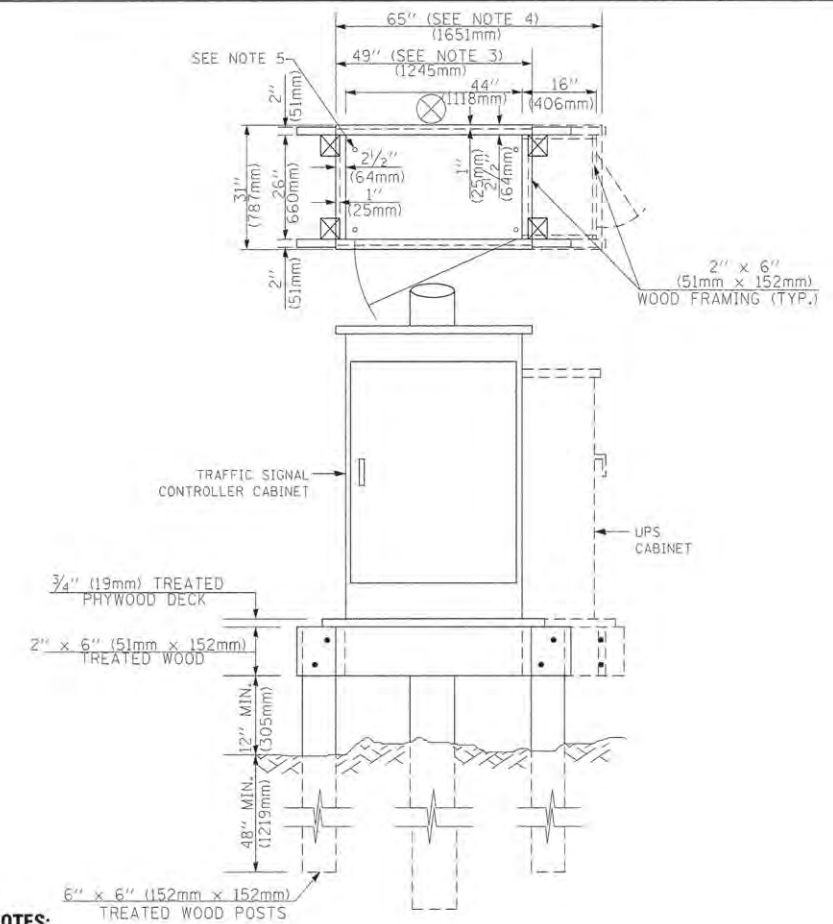
**NOTE:**  
TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE D  
FOR GROUND MOUNTED  
CONTROLLER CABINET  
AND UPS BATTERY CABINET**



**TYPE C  
FOR GROUND MOUNTED  
SUPER P (TYPE IV) AND SUPER R (TYPE V)  
CONTROLLER CABINETS**



**NOTES:**

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

**TEMPORARY SIGNAL CONTROLLER  
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

**CABLE SLACK**

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE ( MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

**VERTICAL CABLE LENGTH**

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

**DEPTH OF FOUNDATION**

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and up to 56' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

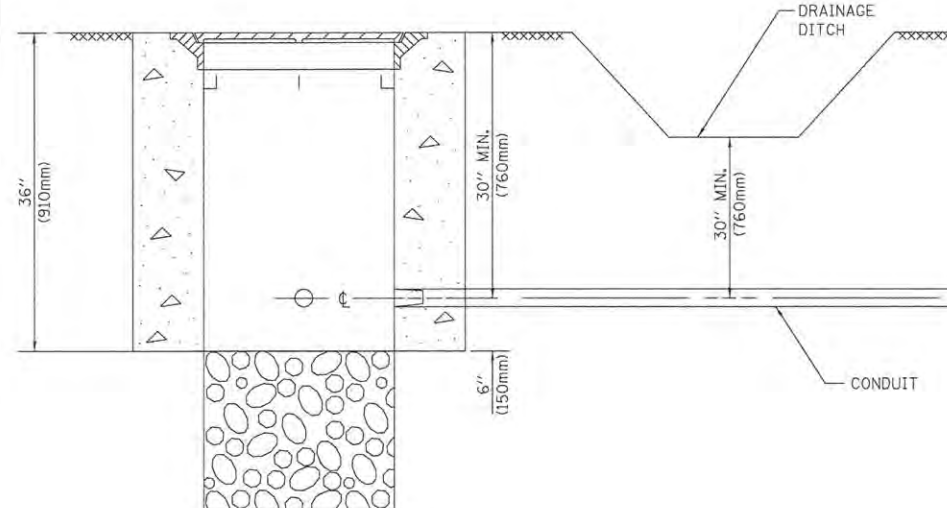
**NOTES:**

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001..

**DEPTH OF MAST ARM FOUNDATIONS, TYPE E**

**TS SHT NO. 5**

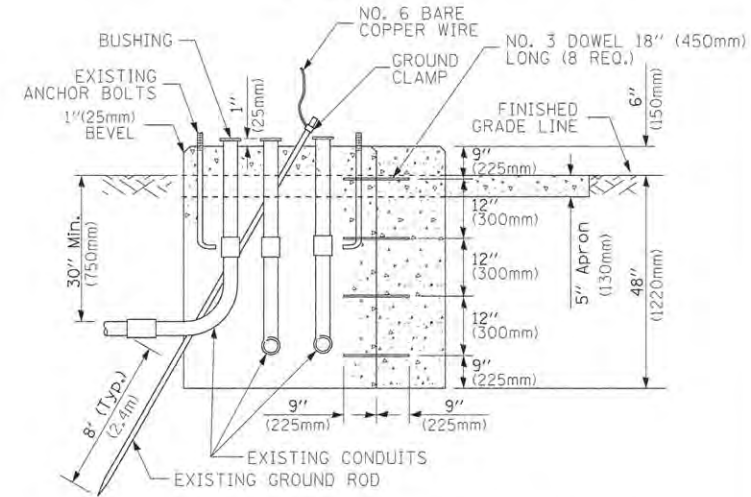
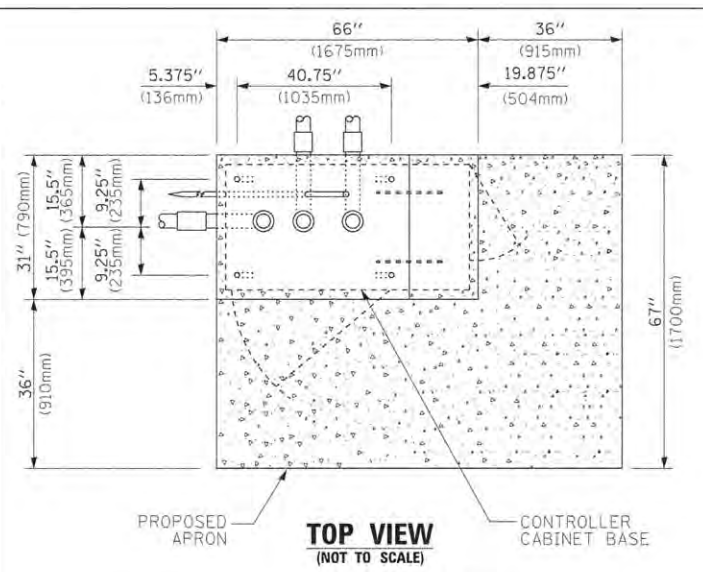




**NOTES:**

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

**HANDHOLE WITH MINIMUM CONDUIT DEPTH**  
(NOT TO SCALE)

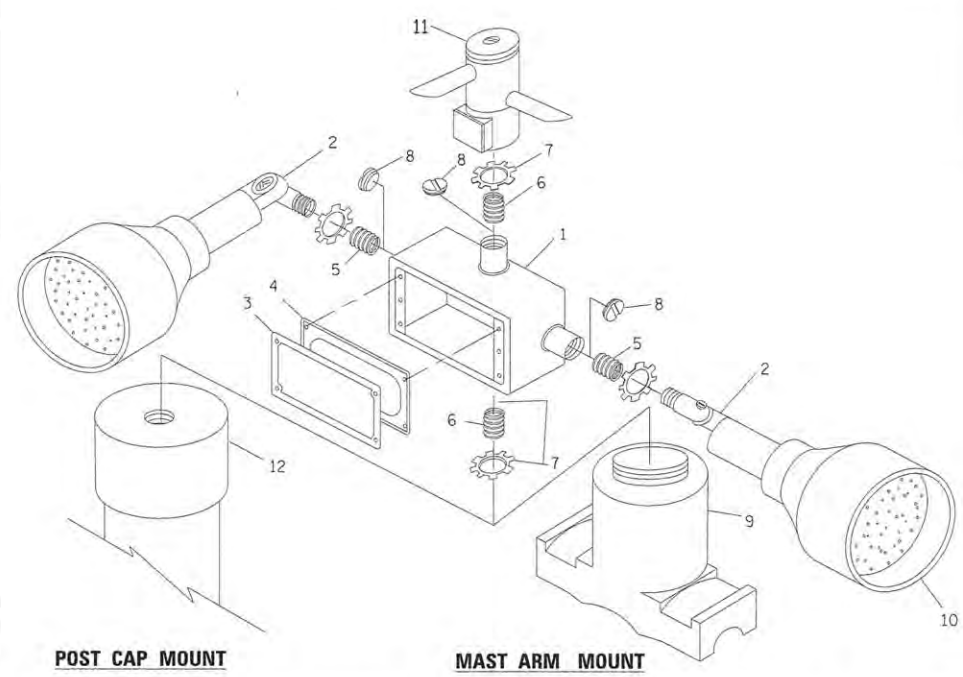


**MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION**  
(NOT TO SCALE)

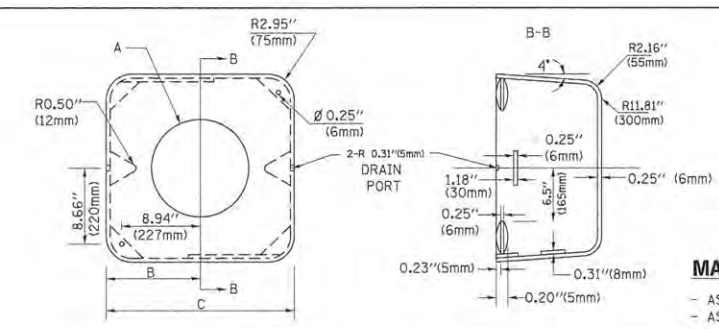
ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

**NOTES:**

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT  
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT  
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



**EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL**



A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

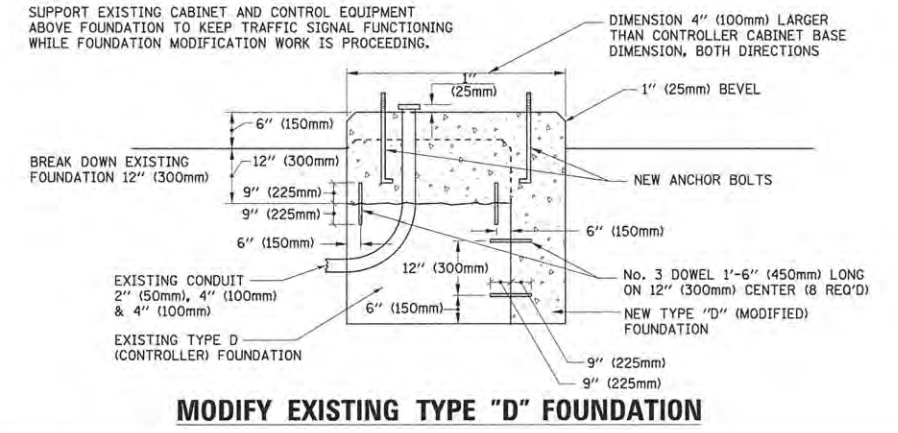
**SHROUD**

**NOTES:**

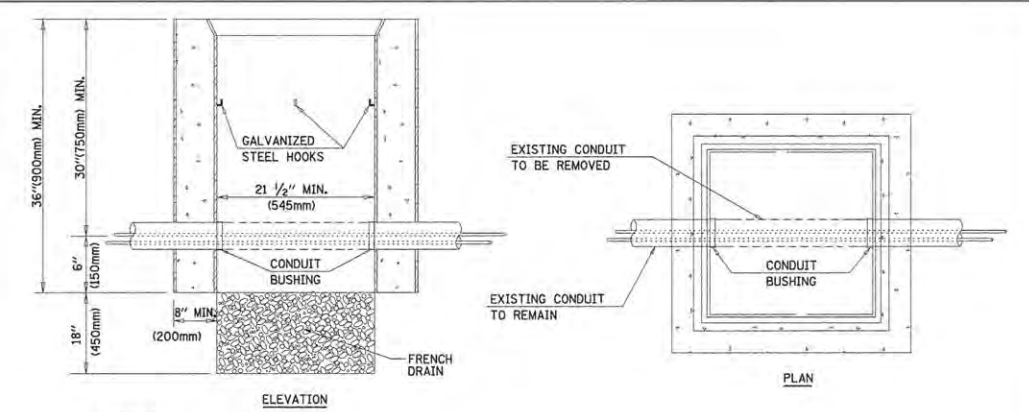
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS AND MAST ARM POLE BASE.

**NOTE:**

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



**MODIFY EXISTING TYPE "D" FOUNDATION**



**NOTES:**

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

**HANDHOLE TO INTERCEPT EXISTING CONDUIT**

TS SHT NO. 6

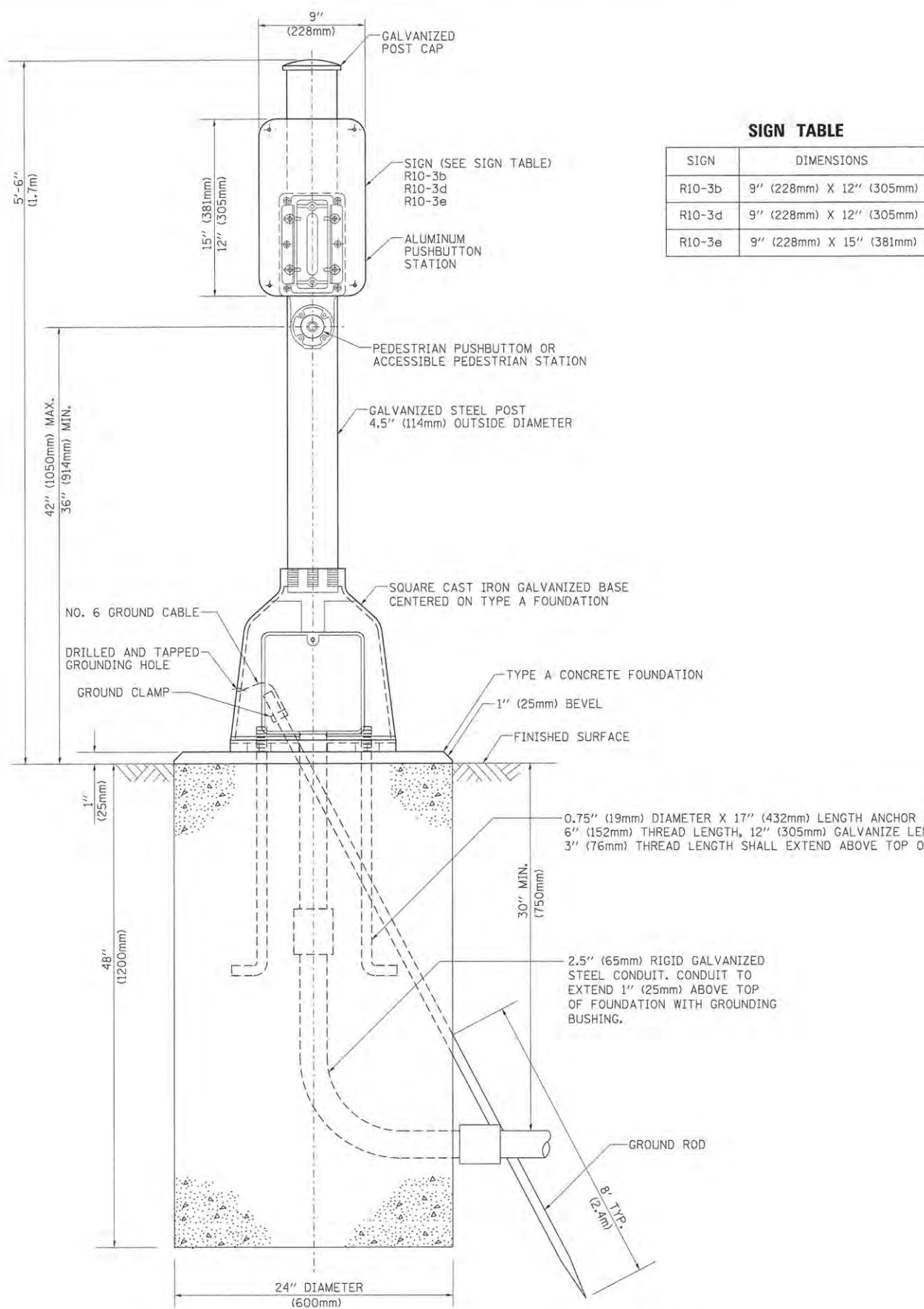
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PLOT SCALE = 50.0000 "/>		CHECKED - DAD	REVISED -
PLOT DATE = 1/13/2014		DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE**  
**STANDARD TRAFFIC SIGNAL DESIGN DETAILS**  
SCALE: NONE SHEET NO. 6 OF 7 SHEETS STA. TO STA.

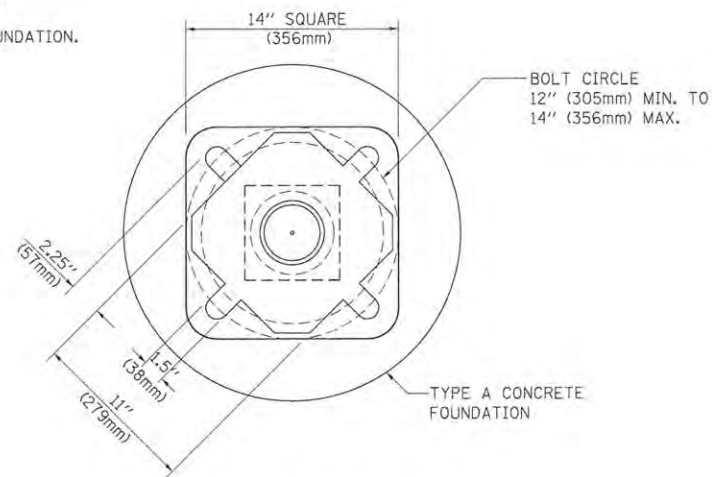
F.A.U. R.T.E. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 46
<b>TS-05</b>		<b>CONTRACT NO. 61C82</b>		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

TS SHT NO. 7



**SIGN TABLE**

SIGN	DIMENSIONS
R10-3b	9" (228mm) X 12" (305mm)
R10-3d	9" (228mm) X 12" (305mm)
R10-3e	9" (228mm) X 15" (381mm)



**BOLT PATTERN**

**PEDESTRIAN PUSH BUTTON POST, TYPE A**

FILE NAME =	USER NAME = footeamj	DESIGNED - DAG	REVISED - DAG 1-1-14	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cs:\pwwork\pwwork\footeamj\d0100315\ts05.dgn		DRAWN - GND	REVISED -		1321	10-00059-01-SP	DUPAGE	77	47			
PLOT SCALE = 50.0000' / 1"		CHECKED - DAD	REVISED -		<b>TS-05</b>			CONTRACT NO. <b>61C82</b>				
PLOT DATE = 1/13/2014		DATE - 10/1/2012	REVISED -		SCALE: NONE			SHEET NO. 7 OF 7 SHEETS				
					STA. TO STA.			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





**REMOVAL NOTES:**

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE SIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 4 EACH TEMPORARY WOOD POLE, 45 FT, CLASS 5
- 1 EACH SERVICE INSTALLATION, POLE MOUNTED
- 204 FOOT SPAN WIRE
- 269 FOOT TETHER WIRE
- 2352 FOOT ELECTRIC CABLE, AERIAL SUSPENDED
- 53 SOFT SIGN PANEL, TYPE 2

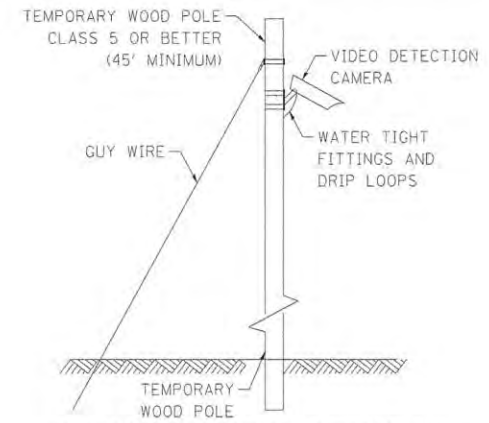
THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE, FREE FROM THEFT OR DAMAGE, AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

AGENCY: IDOT

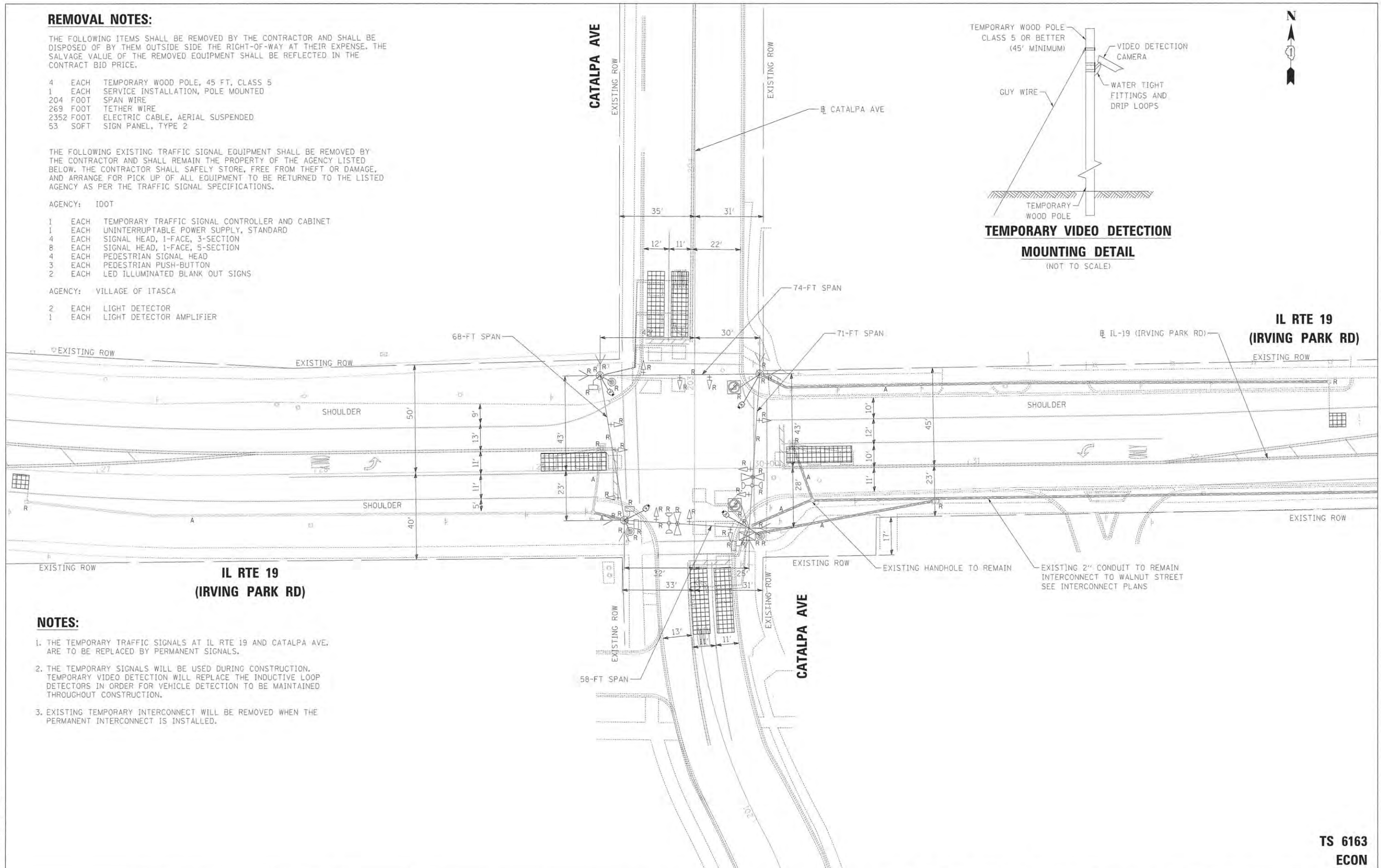
- 1 EACH TEMPORARY TRAFFIC SIGNAL CONTROLLER AND CABINET
- 1 EACH UNINTERRUPTABLE POWER SUPPLY, STANDARD
- 4 EACH SIGNAL HEAD, 1-FACE, 3-SECTION
- 8 EACH SIGNAL HEAD, 1-FACE, 5-SECTION
- 4 EACH PEDESTRIAN SIGNAL HEAD
- 3 EACH PEDESTRIAN PUSH-BUTTON
- 2 EACH LED ILLUMINATED BLANK OUT SIGNS

AGENCY: VILLAGE OF ITASCA

- 2 EACH LIGHT DETECTOR
- 1 EACH LIGHT DETECTOR AMPLIFIER



**TEMPORARY VIDEO DETECTION MOUNTING DETAIL**  
(NOT TO SCALE)



**NOTES:**

1. THE TEMPORARY TRAFFIC SIGNALS AT IL RTE 19 AND CATALPA AVE, ARE TO BE REPLACED BY PERMANENT SIGNALS.
2. THE TEMPORARY SIGNALS WILL BE USED DURING CONSTRUCTION. TEMPORARY VIDEO DETECTION WILL REPLACE THE INDUCTIVE LOOP DETECTORS IN ORDER FOR VEHICLE DETECTION TO BE MAINTAINED THROUGHOUT CONSTRUCTION.
3. EXISTING TEMPORARY INTERCONNECT WILL BE REMOVED WHEN THE PERMANENT INTERCONNECT IS INSTALLED.

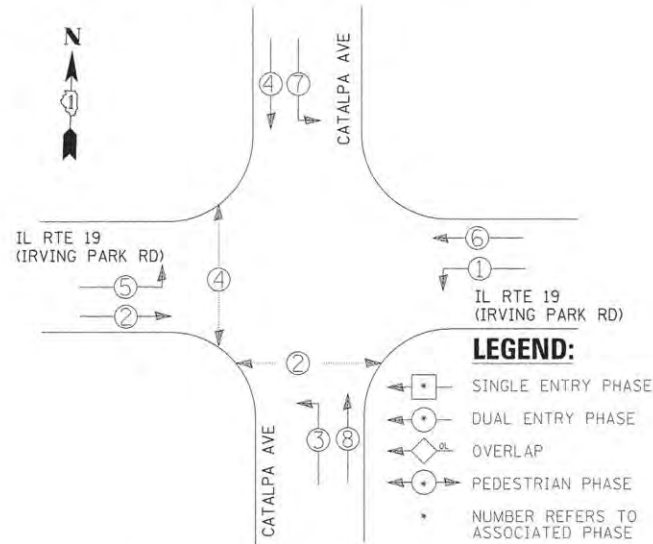
TS SHT NO. 9

**TS 6163  
ECON**

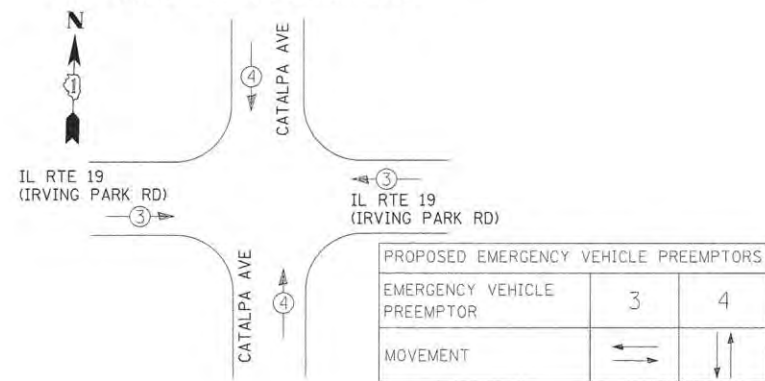
DESIGNED - MBR	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS</b>		F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 49
DRAWN - MBR	REVISED -		<b>TRAFFIC SIGNAL REMOVAL PLAN - IL-19 (IRVING PARK RD) AT CATALPA AVE</b>		SCALE: 1" = 20'		SHEET 9 OF 23 SHEETS STA. TO STA.		CONTRACT NO. 61C82
CHECKED - DAJ	REVISED -				ILLINOIS FED. AID PROJECT				
DATE - 5/20/2016	REVISED -								



**TEMPORARY CONTROLLER SEQUENCE**



**TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE**



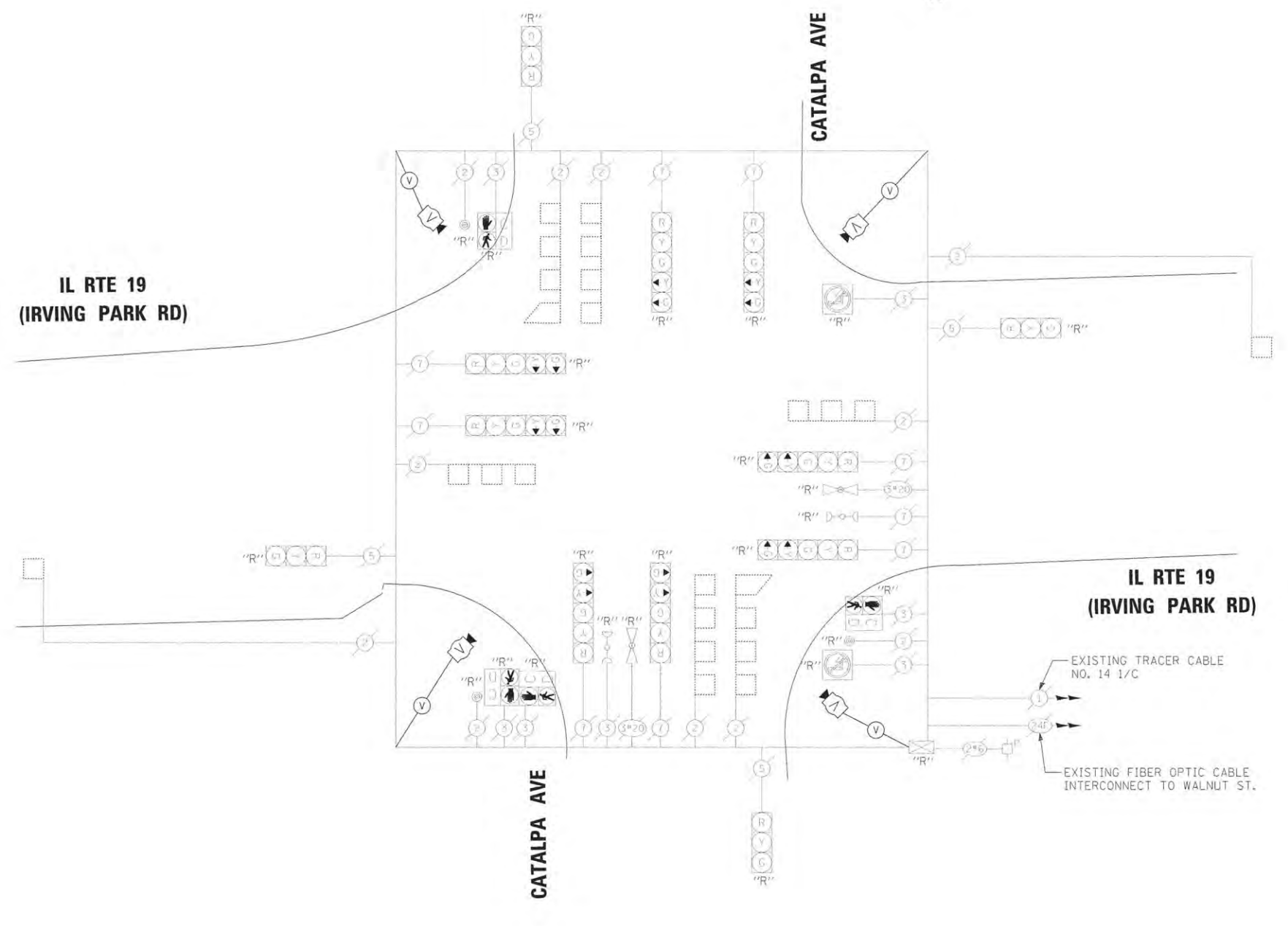
**TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS**

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	12	11	50	66.0
(YELLOW)	12	20	5	12.0
(GREEN)	12	12	45	64.8
PERMISSIVE ARROW	16	10	10	16.0
PED. SIGNAL	4	20	100	80.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	1	150	100	150.0
BLANK-OUT SIGN	2	25	5	2.5
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
<b>TOTAL =</b>				<b>516.3</b>

ENERGY COSTS TO:  
 ILLINOIS DEPT. OF TRANSPORTATION  
 201 WEST CENTER COURT  
 SCHAUMBURG, IL 60196

ENERGY SUPPLY: CONTACT: JOE STACHO  
 PHONE: (630) 424-5704  
 COMPANY: COMMONWEALTH EDISON  
 ACCOUNT NUMBER: ---

IL RTE 19 (IRVING PARK RD)



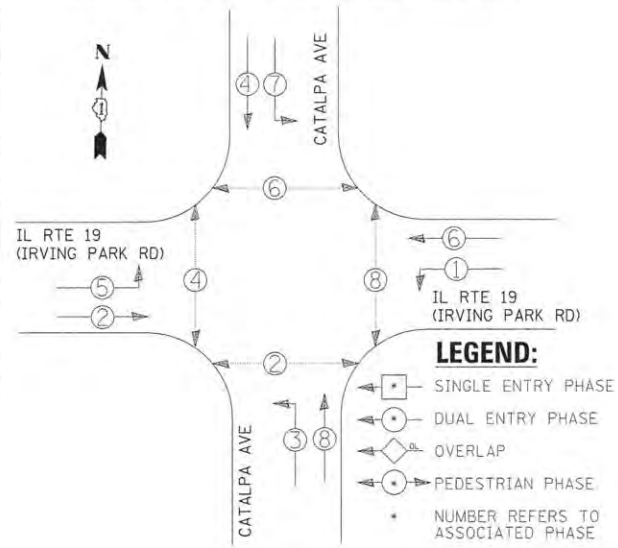
**EXISTING TEMPORARY CABLE PLAN**  
(NOT TO SCALE)

TS SHT NO. 10

TS 6163  
ECON



**PROPOSED CONTROLLER SEQUENCE**



**PROPOSED EMERGENCY VEHICLE  
PREEMPTION SEQUENCE**



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	← →	↑ ↓

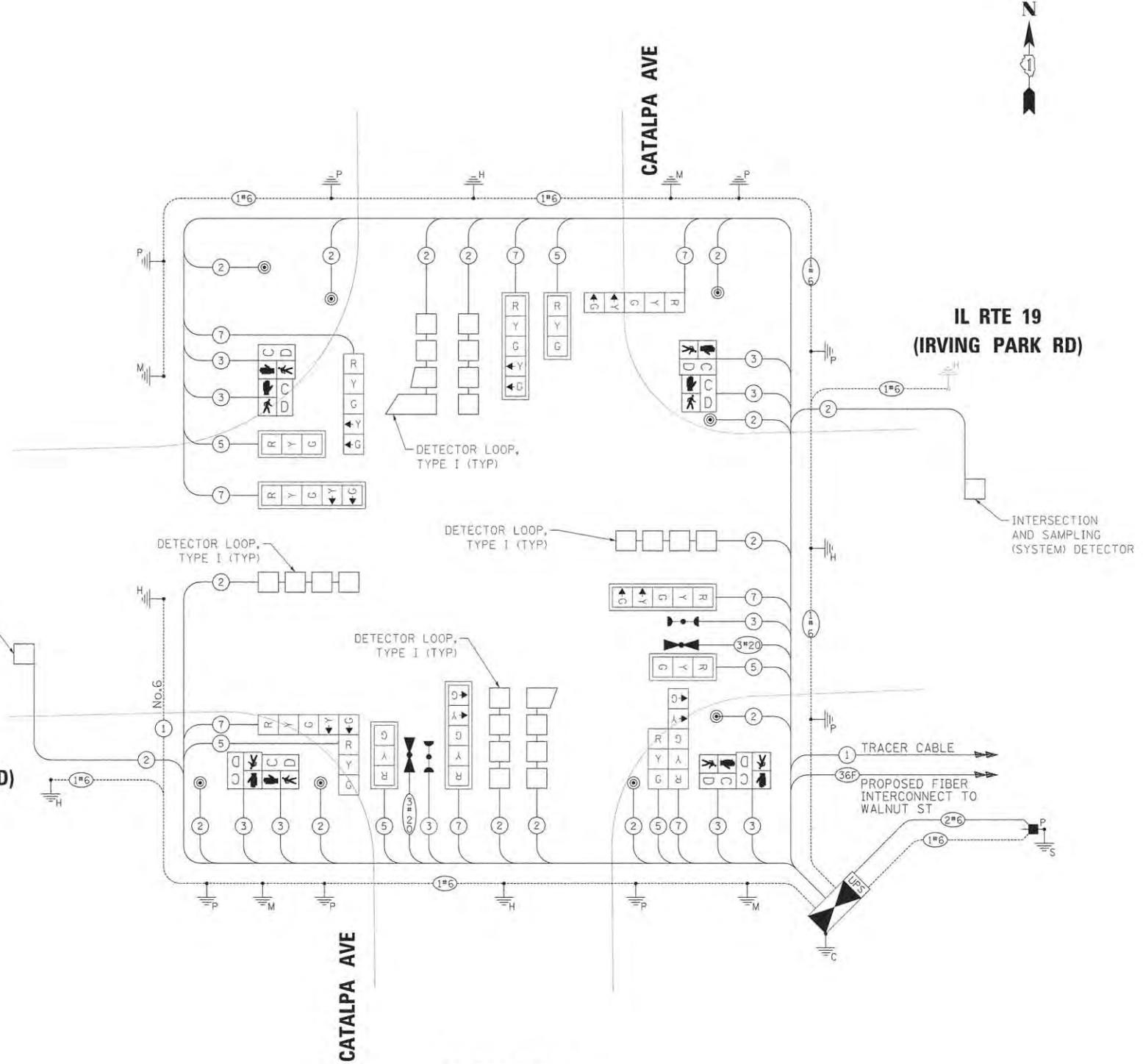
**TRAFFIC SIGNAL  
ELECTRICAL SERVICE REQUIREMENTS**

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	14	11	50	77.0
(YELLOW)	14	20	5	14.0
(GREEN)	14	12	45	75.6
PERMISSIVE ARROW	16	10	10	16.0
PED. SIGNAL	8	20	100	160.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				466.8

ENERGY COSTS TO:  
ILLINOIS DEPT. OF TRANSPORTATION  
201 WEST CENTER COURT  
SCHAUMBURG, IL 60196

ENERGY SUPPLY: CONTACT: JOE STACHO  
PHONE: (630) 424-5704  
COMPANY: COMMONWEALTH EDISON  
ACCOUNT NUMBER: ---

IL RTE 19  
(IRVING PARK RD)



**CABLE PLAN**  
(NOT TO SCALE)

TS SHT NO. 12

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
PROPOSED CABLE PLAN - IL-19 (IRVING PARK RD) AT CATALPA AVE

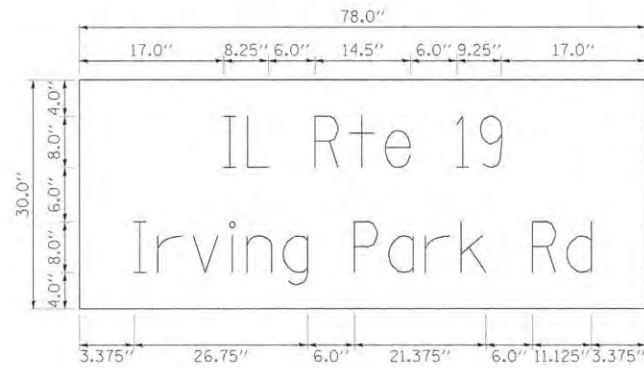
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	10-00059-01-SP	DUPAGE	77	52
CONTRACT NO. 61C82				
ILLINOIS FED. AID PROJECT				

TS 6163  
ECON

FILE NAME	USER NAME	DESIGNED	REVISED
01-17400K-0103-01-CATALPA-DATA.dgn	mbor	MBR	-
		DRAWN	MBR
		CHECKED	DAJ
		DATE	5/20/2016

SCALE: N.T.S. SHEET 12 OF 23 SHEETS STA. TO STA.

**MAST ARM MOUNTED STREET NAME SIGNS**



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
SERIES D	16.25	TYPE 2	ZZ	2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
SERIES D	8.25	TYPE 1	ZZ	2

NOTE: FOR ADDITIONAL DESIGN AND INSTALLATION INFORMATION, SEE DISTRICT ONE MAST ARM MOUNTED STREET NAME SIGNS DETAIL (TS-02).

**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY.
SIGN PANEL - TYPE 1	SQ FT	17
SIGN PANEL - TYPE 2	SQ FT	33
SERVICE INSTALLATION - POLE MOUNTED	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	745
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	92
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	54
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	296
HANDHOLE	EACH	3
HEAVY-DUTY HANDHOLE	EACH	5
DOUBLE HANDHOLE	EACH	2
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
PAINT NEW TRAFFIC SIGNAL POST	EACH	1
PAINT NEW MAST ARM AND POLE, UNDER 40 FOOT	EACH	4
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	967
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1260
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	787
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1184
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	2680
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	72
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	1489
TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 26 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 30 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	32
CONCRETE FOUNDATION, TYPE C	FOOT	4
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	47
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4
SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	2
PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	8
INDUCTIVE LOOP DETECTOR	EACH	8
DETECTOR LOOP, TYPE I	FOOT	953
LIGHT DETECTOR	EACH	4
LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	8
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	4
EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	244
PAINT NEW PEDESTRIAN PUSH-BUTTON POST	EACH	7
TEMPORARY VIDEO DETECTION	EACH	1
FULL-ACTUATED CONTROLLER AND TYPE SUPER P CABINET (SPECIAL)	EACH	1
UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1
PEDESTRIAN PUSH-BUTTON POST, TYPE A	EACH	7

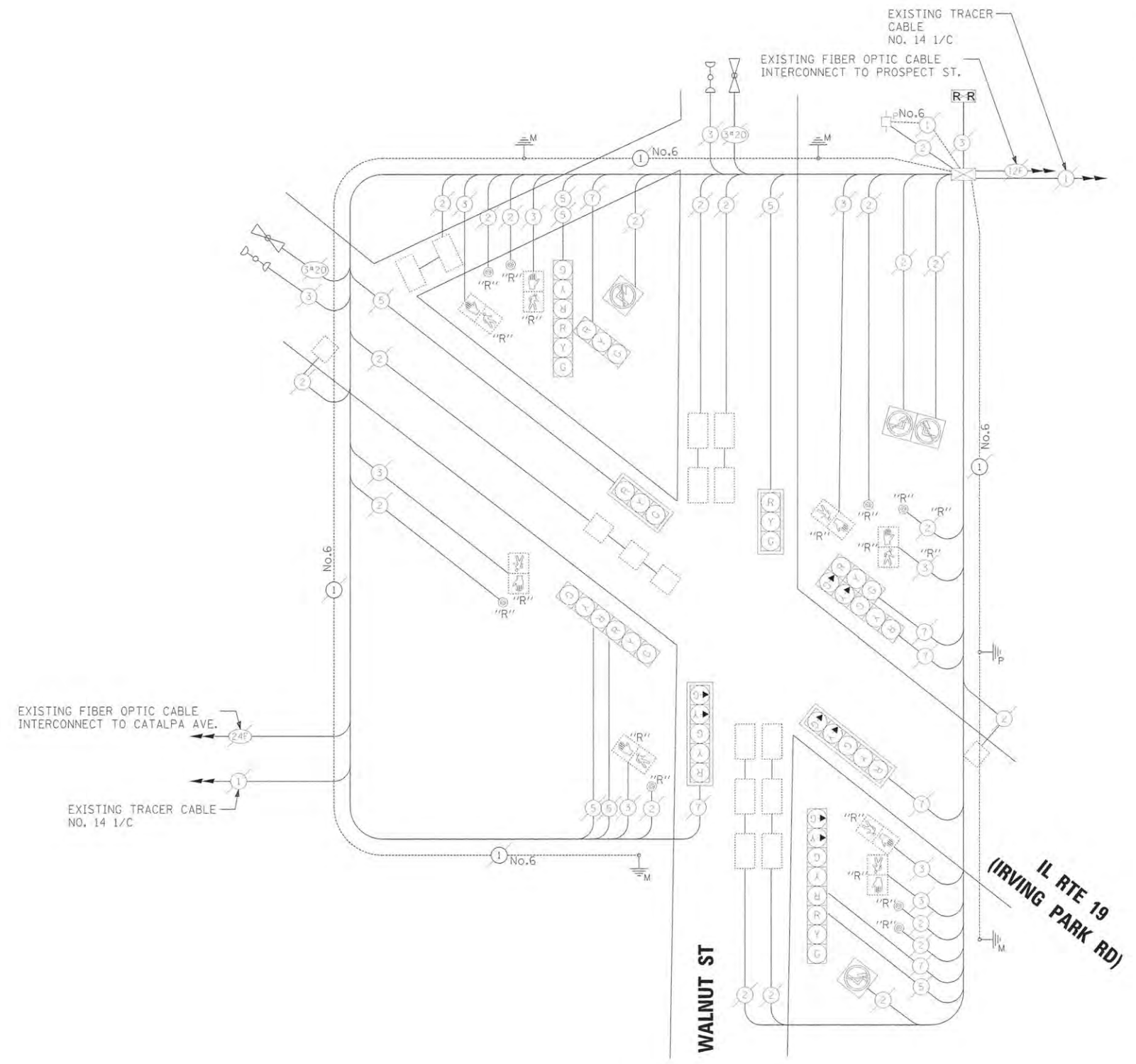
TS SHT NO. 13

TS 6163  
ECON

FILE NAME = D:\ITASCA\SIG\1-02_CATALPA_SHT05.dgn	USER NAME = mrichard	DESIGNED = MBR	REVISED =	<p align="center"><b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b></p> <p align="center"><b>VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS</b> <b>TRAFFIC SIGNAL SCHEDULE OF QUANTITIES - IL-19 (IRVING PARK RD) AT CATALPA AVE</b> <b>IL-19 (IRVING PARK RD) AT CATALPA AVE</b></p> <p>SCALE: N.T.S. SHEET 13 OF 23 SHEETS STA. TO STA.</p>	F.A.U. RTE. = 1321	SECTION = 10-00059-01-SP	COUNTY = DUPAGE	TOTAL SHEETS = 77	SHEET NO. = 53
PLT DATE = 5/19/2015	DATE = 5/20/2016	CHECKED = DAJ	REVISED =		CONTRACT NO. = 61C82				







**EXISTING CABLE PLAN**  
(NOT TO SCALE)

**TRAFFIC SIGNAL  
ELECTRICAL SERVICE REQUIREMENTS**

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	13	11	50	71.5
(YELLOW)	13	20	5	13.0
(GREEN)	13	12	45	70.2
PERMISSIVE ARROW	8	10	10	8.0
PED. SIGNAL	8	20	100	160.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	4	25	5	5.0
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
<b>TOTAL =</b>				<b>452.7</b>

ENERGY COSTS TO:  
ILLINOIS DEPT. OF TRANSPORTATION  
201 WEST CENTER COURT  
SCHAUMBURG, IL 60196

ENERGY SUPPLY: CONTACT: JOE STACHO  
PHONE: (630) 424-5704  
COMPANY: COMMONWEALTH EDISON  
ACCOUNT NUMBER: ---

TS SHT NO. 15

**EXISTING SEQUENCE OF OPERATIONS**

MOVEMENT	2+5		2+6				4+7				4+8				FLASH	
	1	2	3	4	5	6A	6B	7	8	9A	9B	10	11	12		13A
PHASE	2+5		2+6				4+7				4+8					
INTERVAL	1	2	3	4	5	6A	6B	7	8	9A	9B	10	11	12	13A	13B
CHANGE TO	θ	θ	2+6			4+7		θ	θ	2+5		4+8			2+5	
ILLINOIS ROUTE 19 (IRVING PARK ROAD) END MAST ARM AND FAR LEFT SIGNALS	SE/B	G ←G	G ←G	G ←Y	G	Y	R	R	R	R	R	R	R	R	R	R
ILLINOIS ROUTE 19 (IRVING PARK ROAD) NEAR RIGHT SIGNALS	SE/B	G	G	G	G	Y	R	R	R	R	R	R	R	R	R	R
ILLINOIS ROUTE 19 (IRVING PARK ROAD) ALL SIGNALS	NW/B	R	R	R	G	Y	R	R	R	R	R	R	R	R	R	R
WALNUT STREET ALL SIGNALS	N/B	R	R	R	R	R	R	R	R	R	R	R	G	G	Y	R
WALNUT STREET END MAST ARM AND FAR LEFT SIGNALS	S/B	R	R	R	R	R	R	G	G	Y	R	G	G	G	Y	R
WALNUT STREET NEAR RIGHT SIGNALS	S/B	R	R	R	R	R	R	G	G	Y	R	G	G	G	Y	R
PEDESTRIAN SIGNALS CROSSING WALNUT STREET ON SOUTH SIDE OF IL ROUTE 19		*P	**FH	H	*P	**FH	H	H	H	H	H	H	H	H	H	H
PEDESTRIAN SIGNALS CROSSING WALNUT STREET ON NORTH SIDE OF IL ROUTE 19		H	H	H	*P	**FH	H	H	H	H	H	H	H	H	H	H
PEDESTRIAN SIGNALS CROSSING ILLINOIS ROUTE 19 ON EAST SIDE OF WALNUT STREET		H	H	H	H	H	H	H	H	H	H	H	*P	**FH	H	H
PEDESTRIAN SIGNALS CROSSING ILLINOIS ROUTE 19 ON WEST SIDE OF WALNUT STREET		H	H	H	H	H	H	*P	**FH	H	H	H	*P	**FH	H	H

PHASE 2+6 SHALL BE PLACED ON RECALL

**EXISTING RAILROAD PREEMPTION SEQUENCE OF OPERATIONS**

CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1		4		7		11		PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 2						CLEAR TO NORMAL SEQUENCE
	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	2	3	4	5		
CHANGE FROM EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER									2	3							
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	2	3	4	5		
CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	2	2	1G	2	1J	2	1L	2	3	4	5			
ILLINOIS ROUTE 19 (IRVING PARK ROAD) END MAST ARM AND FAR LEFT SIGNALS	SE/B	Y	R	Y	R	R	R	Y	R	R	R	R	R	R	R	G	
ILLINOIS ROUTE 19 (IRVING PARK ROAD) NEAR RIGHT SIGNALS	SE/B	Y	R	Y	R	R	R	Y	R	R	R	R	R	R	R	G	
ILLINOIS ROUTE 19 (IRVING PARK ROAD) ALL SIGNALS	NW/B	R	R	Y	R	R	R	Y	R	R	R	R	R	R	R	G	
WALNUT STREET ALL SIGNALS	N/B	R	R	R	R	R	Y	R	R	Y	R	R	R	R	R	G	
WALNUT STREET END MAST ARM AND FAR LEFT SIGNALS	S/B	R	R	R	R	G	G	R	R	G	G	G	G	Y	R	R	
WALNUT STREET NEAR RIGHT SIGNALS	S/B	R	R	R	R	G	G	R	R	G	G	G	G	Y	R	R	
PEDESTRIAN SIGNALS CROSSING WALNUT STREET ON SOUTH SIDE OF IL ROUTE 19		FH	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	
PEDESTRIAN SIGNALS CROSSING WALNUT STREET ON NORTH SIDE OF IL ROUTE 19		H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	
PEDESTRIAN SIGNALS CROSSING ILLINOIS ROUTE 19 ON EAST SIDE OF WALNUT STREET		H	H	H	H	H	FH	H	H	H	H	H	H	H	H	H	
PEDESTRIAN SIGNALS CROSSING ILLINOIS ROUTE 19 ON WEST SIDE OF WALNUT STREET		H	H	H	H	FH	FH	H	H	H	H	H	H	H	H	H	
INTERNALLY ILLUMINATED 'NO RIGHT TURN' SIGNS	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	
INTERNALLY ILLUMINATED 'NO LEFT TURN' SIGNS	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	

△ RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.

HOLD

**EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS**

CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1		4		7		11		PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	CLEAR TO NORMAL SEQUENCE									
	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K		1L	1M	1N	1P	1Q	1R	1T	1U	
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1T	1U	2	3
CHANGE TO EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	1E	3	2	1H	1J	3	1L	1M	2	1P	3	1R	1T	2	3		
ILLINOIS ROUTE 19 (IRVING PARK ROAD) END MAST ARM AND FAR LEFT SIGNALS	SE/B	G ←G	G ←Y	G ←G	Y	R	G	G	Y	R	R	R	R	R	R	R	R	R	G	R
ILLINOIS ROUTE 19 (IRVING PARK ROAD) NEAR RIGHT SIGNALS	SE/B	G	G	G	Y	R	G	G	Y	R	R	R	R	R	R	R	R	R	G	R
ILLINOIS ROUTE 19 (IRVING PARK ROAD) ALL SIGNALS	NW/B	R	R	R	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	G	R
WALNUT STREET ALL SIGNALS	N/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	R
WALNUT STREET END MAST ARM AND FAR LEFT SIGNALS	S/B	R	R	R	R	R	R	R	R	G	Y	R	G	G	Y	R	G	R	G	R
WALNUT STREET NEAR RIGHT SIGNALS	S/B	R	R	R	R	R	R	R	R	G	Y	R	G	G	Y	R	G	R	G	R
PEDESTRIAN SIGNALS CROSSING WALNUT STREET ON SOUTH SIDE OF IL ROUTE 19		FH	H	FH	H	H	FH	FH	H	H	H	H	H	H	H	H	H	H	H	H
PEDESTRIAN SIGNALS CROSSING WALNUT STREET ON NORTH SIDE OF IL ROUTE 19		H	H	H	H	H	FH	FH	H	H	H	H	H	H	H	H	H	H	H	H
PEDESTRIAN SIGNALS CROSSING ILLINOIS ROUTE 19 ON EAST SIDE OF WALNUT STREET		H	H	H	H	H	H	H	H	H	H	H	H	H	H	FH	H	H	FH	H
PEDESTRIAN SIGNALS CROSSING ILLINOIS ROUTE 19 ON WEST SIDE OF WALNUT STREET		H	H	H	H	H	H	H	H	FH	H	H	FH	H	H	H	H	FH	H	H

◇ PROPER CLEARANCE TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY VEHICLE INTERVAL AFTER EMERGENCY VEHICLE INTERVAL 2 OR 3 IS TERMINATED.

- \* TO APPEAR ONLY UPON PUSHBUTTON ACTIVATION
- \*\* FLASHING "↓" IS TO TERMINATE AT THE COMPLETION OF THE PEDESTRIAN INTERVAL CLEARANCE.
- ⊖ THIS "↓" OR FLASHING "↓" INTERVAL MAY FINISH TIMING IN THE BIDIRECTIONAL STRAIGHT THROUGH MOVEMENT IF THE LEFT ARROW TIME IS NOT SUFFICIENT TO COMPLETE "↓" OR FLASHING "↓" INTERVALS. "↓" AND FLASHING "↓" TIMINGS TO BE SET ONLY ON PHASES WHERE "↓" AND FLASHING "↓" ARE INDICATED IN THE SEQUENCE OF OPERATION.
- P = ILLUMINATED PERSON = WALK
- FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
- H = ILLUMINATED SOLID HAND = DON'T WALK

PHASE 2+6 SHALL BE PLACED ON RECALL.

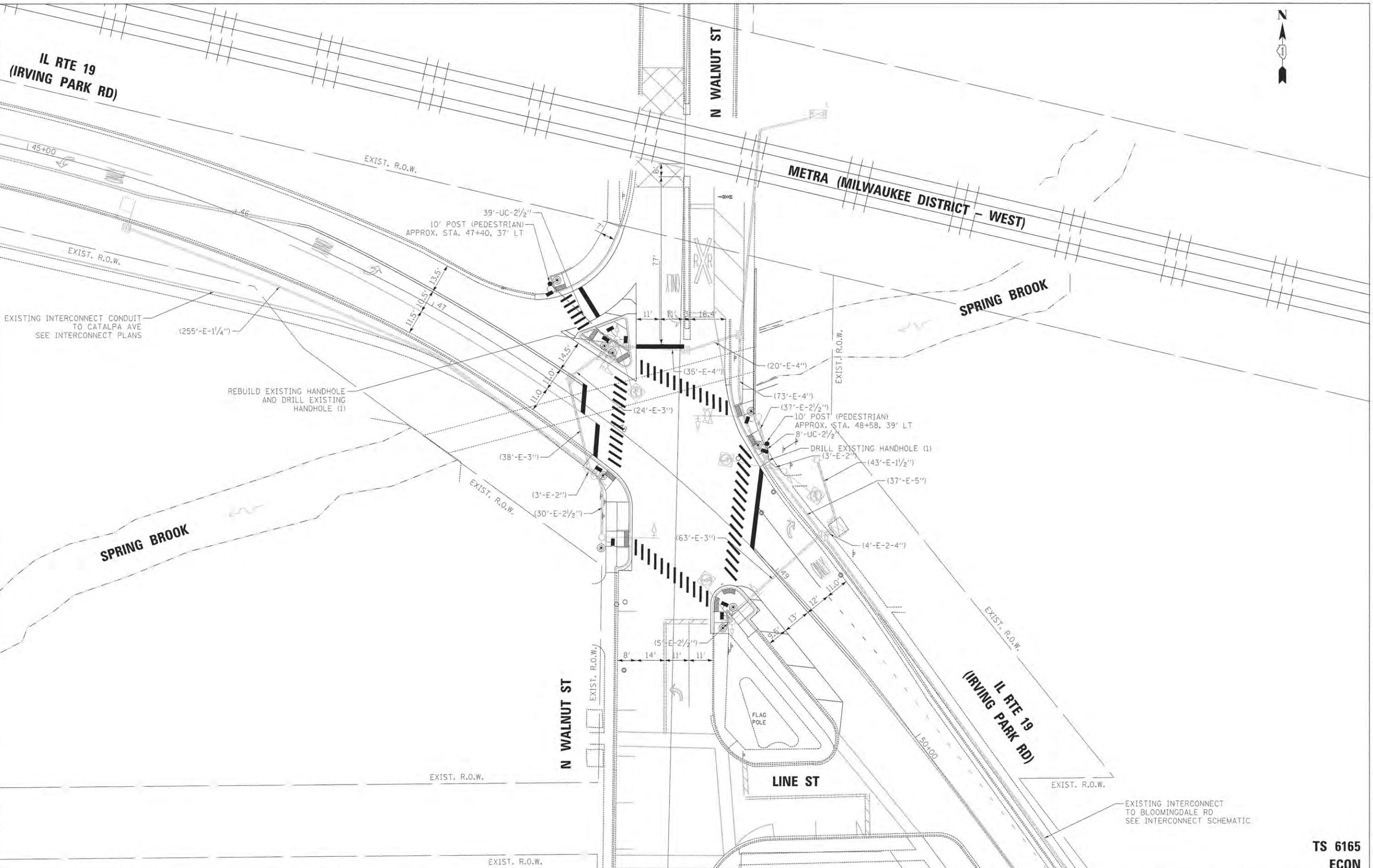
NLT = 'NO LEFT TURN' OR

NRT = 'NO RIGHT TURN' OR

TS SHT NO. 16

TS 6165  
ECON





TS SHT NO. 17

TS 6165  
ECON

FILE NAME 01 ITASCA SIGNALING WALNUT 2016.dwg BR/DAJ	USER NAME - mrichards	DESIGNED - MBR	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS</b> <b>PROPOSED TRAFFIC SIGNAL PLAN - IL-19 (IRVING PARK RD) AT WALNUT ST</b>			F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 57
	TITLE SCALE - 3/4"=1'-0"	CHECKED - DAJ	DATE - 3/14/2016		REVISED -	SCALE: 1" = 20' SHEET 17 OF 23 SHEETS STA. TO STA.	CONTRACT NO. 61C82 [ILLINOIS] FED. AID PROJECT					



**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY.
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	47
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	347
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	591
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	341
TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	2
CONCRETE FOUNDATION, TYPE A	FOOT	8
DRILL EXISTING HANDHOLE	EACH	2
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED	EACH	5
PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED	EACH	1
PEDESTRIAN SIGNAL HEAD, LED, 3-FACE, BRACKET MOUNTED	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	9
MODIFY EXISTING CONTROLLER	EACH	1
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REBUILD EXISTING HANDHOLE	EACH	1

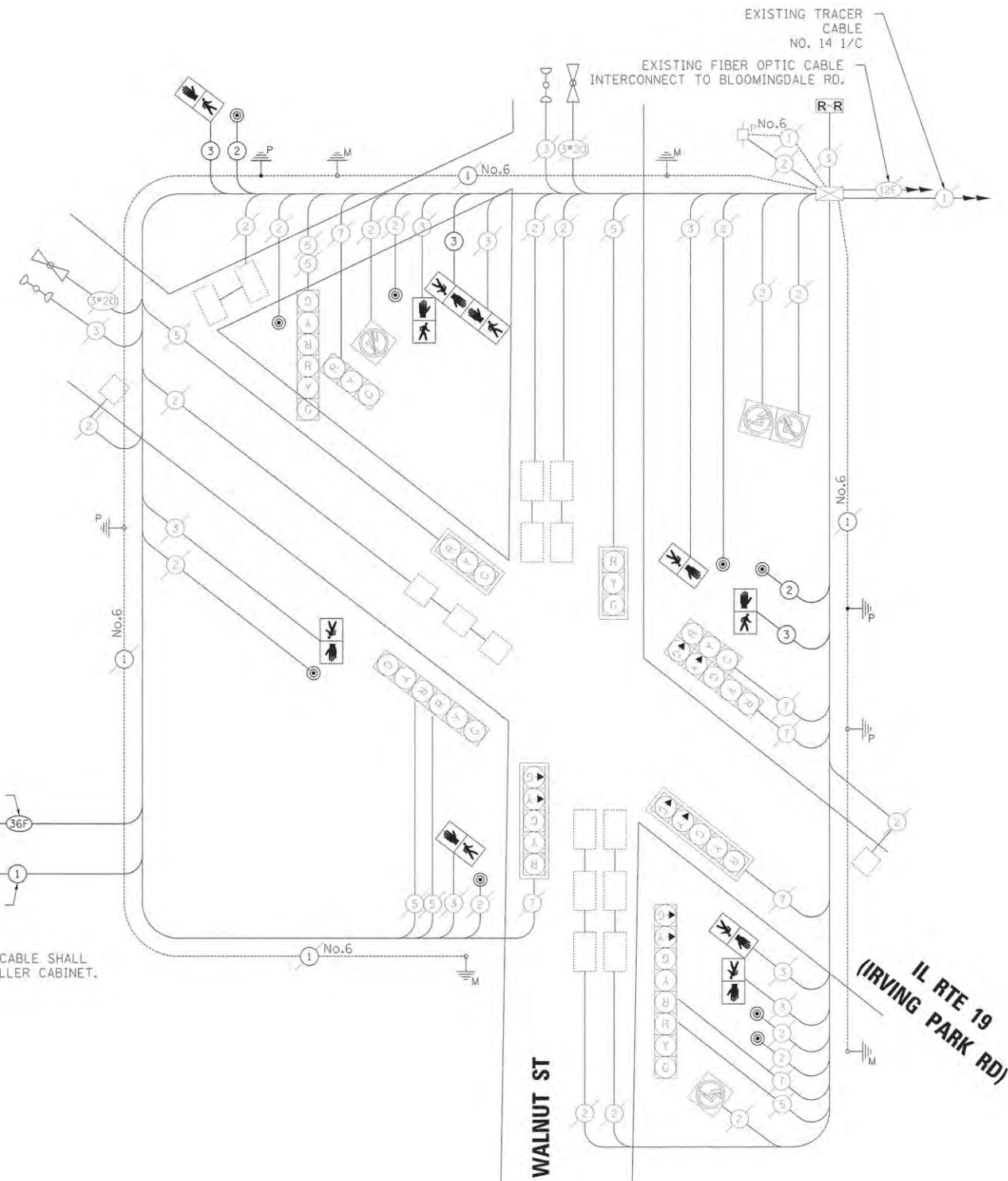
**TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS**

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	13	11	50	71.5
(YELLOW)	13	20	5	13.0
(GREEN)	13	12	45	70.2
PERMISSIVE ARROW	8	10	10	8.0
PED. SIGNAL	10	20	100	200.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	4	25	5	5.0
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				492.7

ENERGY COSTS TO:

ILLINOIS DEPT. OF TRANSPORTATION  
201 WEST CENTER COURT  
SCHAUMBURG, IL 60196

ENERGY SUPPLY: CONTACT: JOE STACHO  
PHONE: (630) 424-5704  
COMPANY: COMMONWEALTH EDISON  
ACCOUNT NUMBER: ---



PROPOSED FIBER OPTIC CABLE INTERCONNECT TO CATALPA AVE.

PROPOSED TRACER CABLE NO. 14 1/C

THE END OF THE TRACER CABLE SHALL EXTEND INTO THE CONTROLLER CABINET.

**CABLE PLAN**  
(NOT TO SCALE)

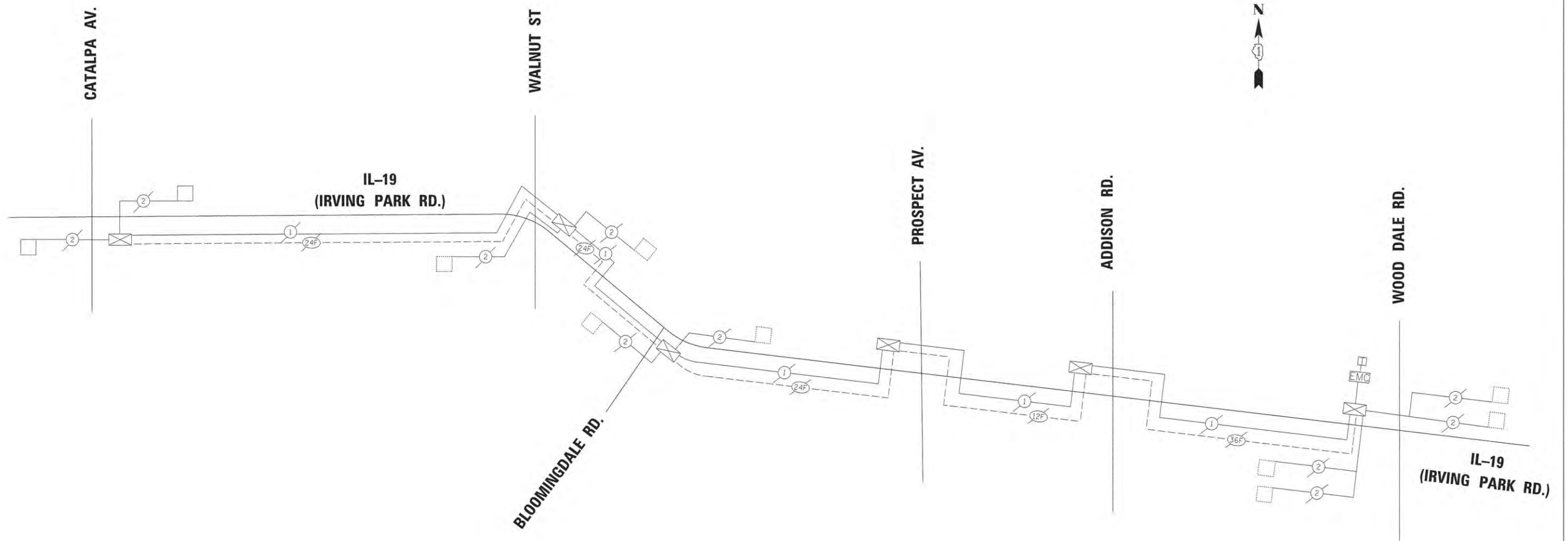
TS SHT NO. 18

FILE NAME = DJ-ITASCA.SIG.L-04-WALNUT_SHT05.dwg	USER NAME = mrichards	DESIGNED - MBR	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS PROPOSED CABLE PLAN - IL-19 (IRVING PARK RD) AT WALNUT ST</b>	F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 58	
Default	PLOT SCALE = 20,0000" = 1'-0"	CHECKED - DAJ	REVISED -			SCALE: N.T.S. SHEET 18 OF 23 SHEETS STA. TO STA.		CONTRACT NO. 61C82		ILLINOIS FED. AID PROJECT	
	PLOT DATE = 5/19/2016	DATE - 5/20/2016	REVISED -								

**TS 6165  
ECON**



TS SHT NO. 20



FILE NAME = 01-ITASCA SIS_EX_INT_SCHEMATIC.dgn	USER NAME = m-richards	DESIGNED = MBR	REVISED = -
		DRAWN = MBR	REVISED = -
		CHECKED = DAJ	REVISED = -
		DATE = 5/20/2016	REVISED = -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
EXISTING INTERCONNECT SCHEMATIC

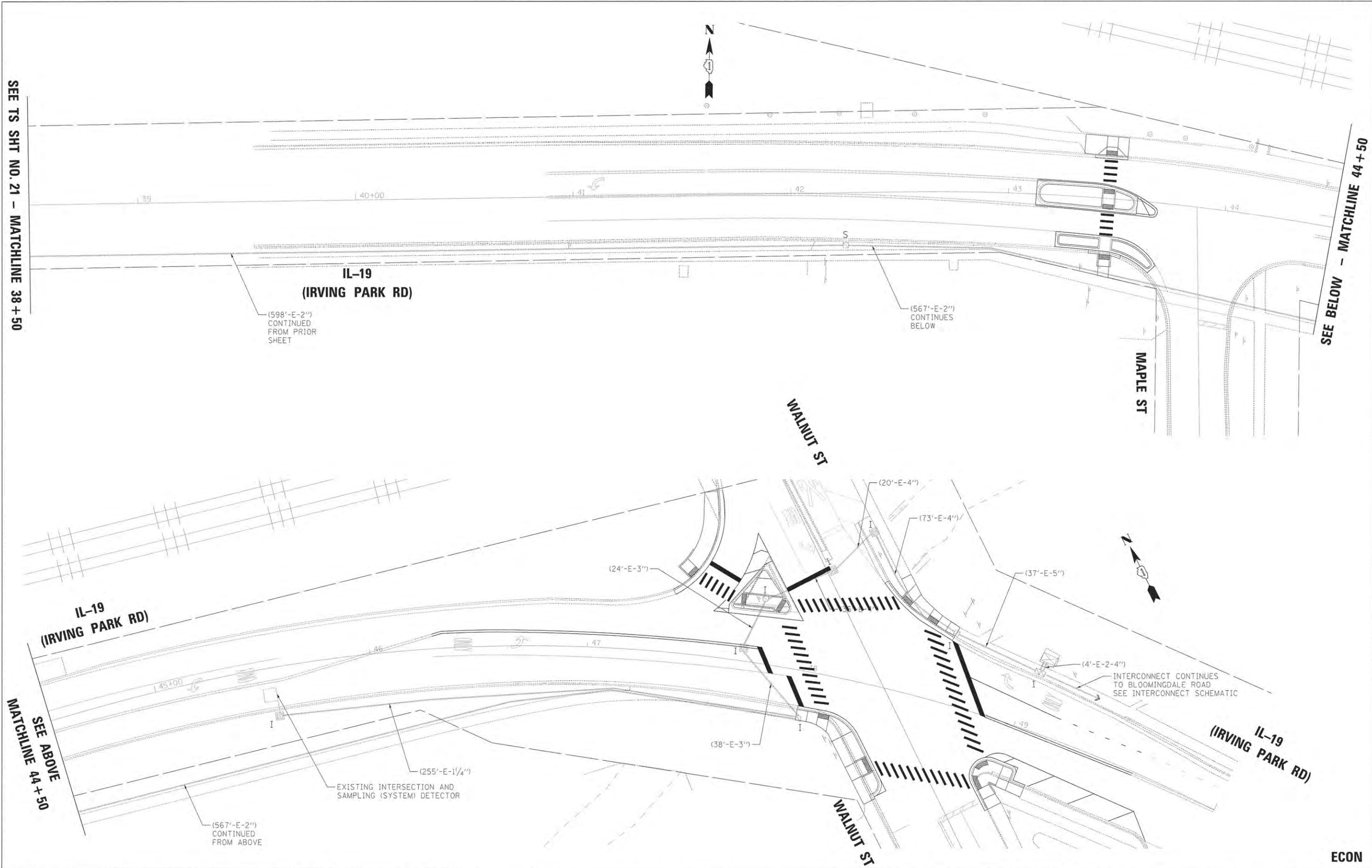
SCALE: N.T.S. SHEET 20 OF 23 SHEETS STA. TO STA.

F.A.U. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 60
CONTRACT NO. 61C82			ILLINOIS FED. AID PROJECT	

ECON







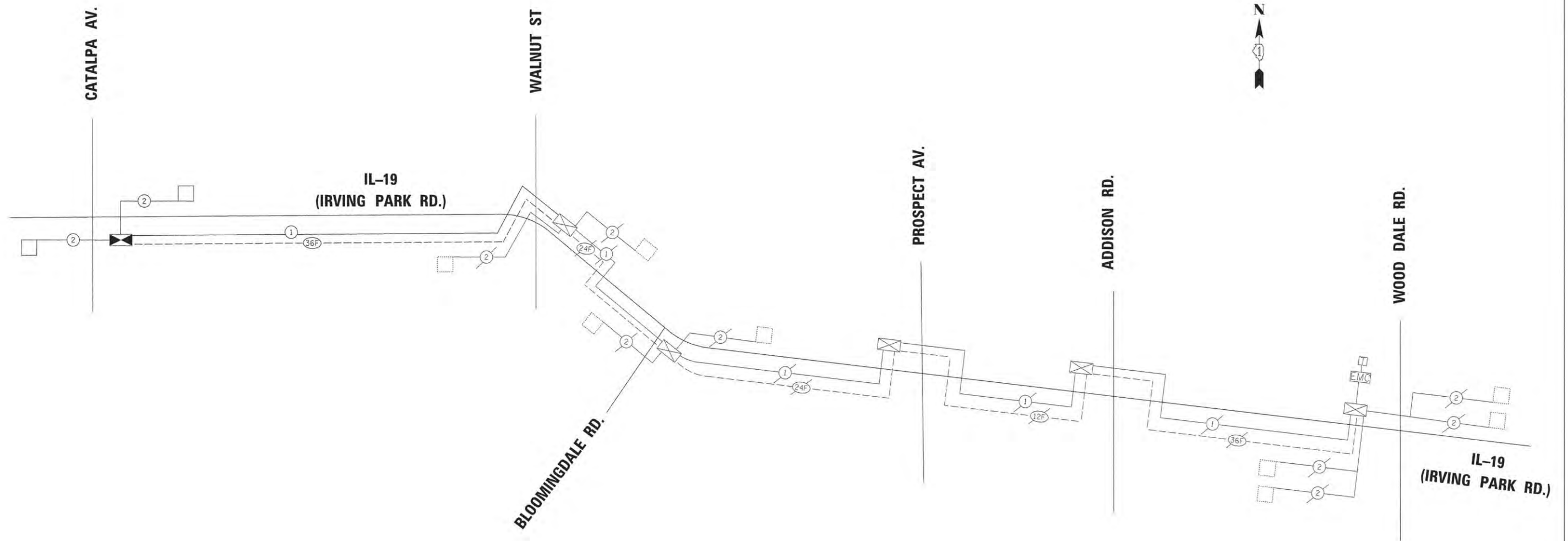
SEE TS SHT NO. 21 - MATCHLINE 38 + 50

SEE BELOW - MATCHLINE 44 + 50

TS SHT NO. 22  
MATCHLINE 44 + 50  
SEE ABOVE

ECON

FILE NAME = 01-ITASCA.SIG.PR.INT_SHT02.dgn	USER NAME = vortolands	DESIGNED - MBR	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS PROPOSED INTERCONNECT PLAN - FROM CATALPA AVE TO WALNUT ST			F.A.U. RTE. = 1321	SECTION = 10-00059-01-SP	COUNTY = DUPAGE	TOTAL SHEETS = 77	SHEET NO. = 62
	Default	PLOT SCALE = 20.0000' = 1" PL	CHECKED - DAJ		REVISED -	SCALE: 1" = 20'	SHEET 22 OF 23 SHEETS	STA. TO STA.	CONTRACT NO. 61C82		ILLINOIS FED. AID PROJECT	
	PLOT DATE = 5/19/2016	DATE = 5/20/2016	REVISED -									



**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY.
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	10
TRANSCEIVER - FIBER OPTIC	EACH	1
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	2091
DRILL EXISTING HANDHOLE	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	2083
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1
ROD AND CLEAN EXISTING CONDUIT	FOOT	500
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	2117
REMOVE FIBER OPTIC CABLE FROM CONDUIT	FOOT	2109

\* NOMINAL QUANTITY TO BE USED AS NEEDED AND AS APPROVED BY THE ENGINEER

TS SHT NO. 23

**ECON**

FILE NAME = 02-ITASCA.SIG.PRI.INT.SCHEMATIC.dgn	USER NAME = mrichards	DESIGNED - MBR	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS PROPOSED INTERCONNECT SCHEMATIC</b>		F.A.U. RTE. = 1321	SECTION = 10-00059-01-SP	COUNTY = DUPAGE	TOTAL SHEETS = 77	SHEET NO. = 63	
Default	PLOT SCALE = 1/8" = 1' 0"	CHECKED - DAJ	REVISED -		SCALE: N.T.S.	SHEET 23 OF 23 SHEETS	STA. TO STA.	CONTRACT NO. 61C82				
	PLOT DATE = 5/20/2016	DATE = 5/20/2016	REVISED -		ILLINOIS FED. AID PROJECT							



**GENERAL NOTES**

Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

The existing aluminum handrail that is to be removed shall be salvaged and delivered to the Illinois Department of Transportation Maintenance Yard at 1101 Biesterfeld Road, Elk Grove Village, IL 60007. Attention Shaker Asfour Phone 847-956-1501.

Protective Coat shall be applied to the top surface and entire traffic face of the proposed parapet.

**BILL OF MATERIAL**

Item	Unit	Total
Bridge Rail Removal	Foot	259
Concrete Superstructure	Cu. Yd.	34.4
Protective Coat	Sq. Yd.	123
Reinforcement Bars, Epoxy Coated	Pound	5580
Bridge Fence Railing	Foot	259
Parapet Railing	Foot	519
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq Ft.	45

\* Total includes quantity on approach roadway barrier. See Roadway Plans.

**DESIGN STRESSES  
FIELD UNITS**

$f'_c = 4,000$  psi  
 $f_y = 60,000$  psi (Reinforcement)

**DESIGN SPECIFICATIONS**

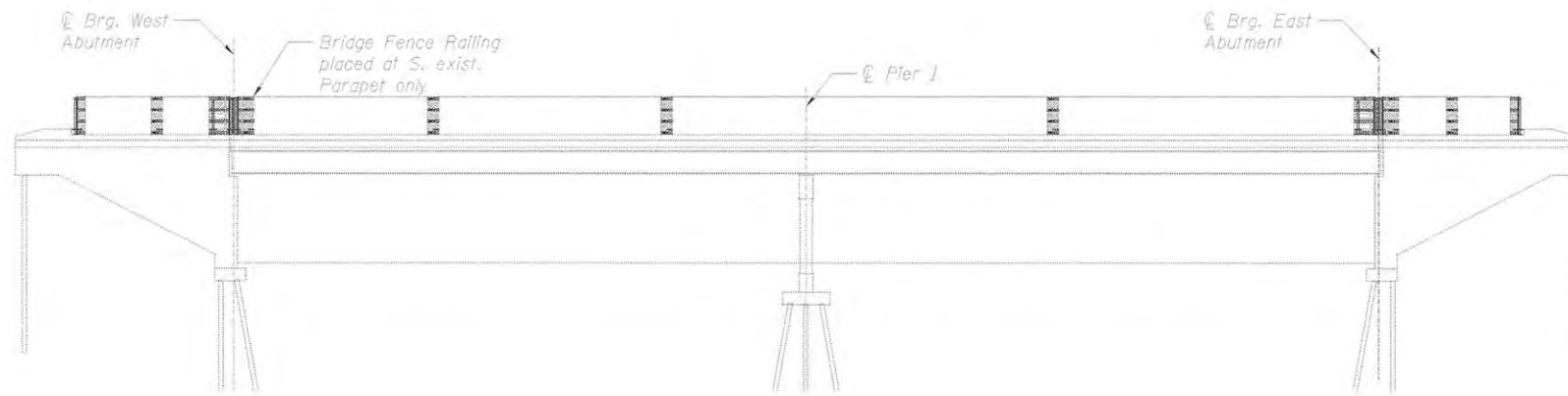
2002 AASHTO

**LOADING HS20-44**

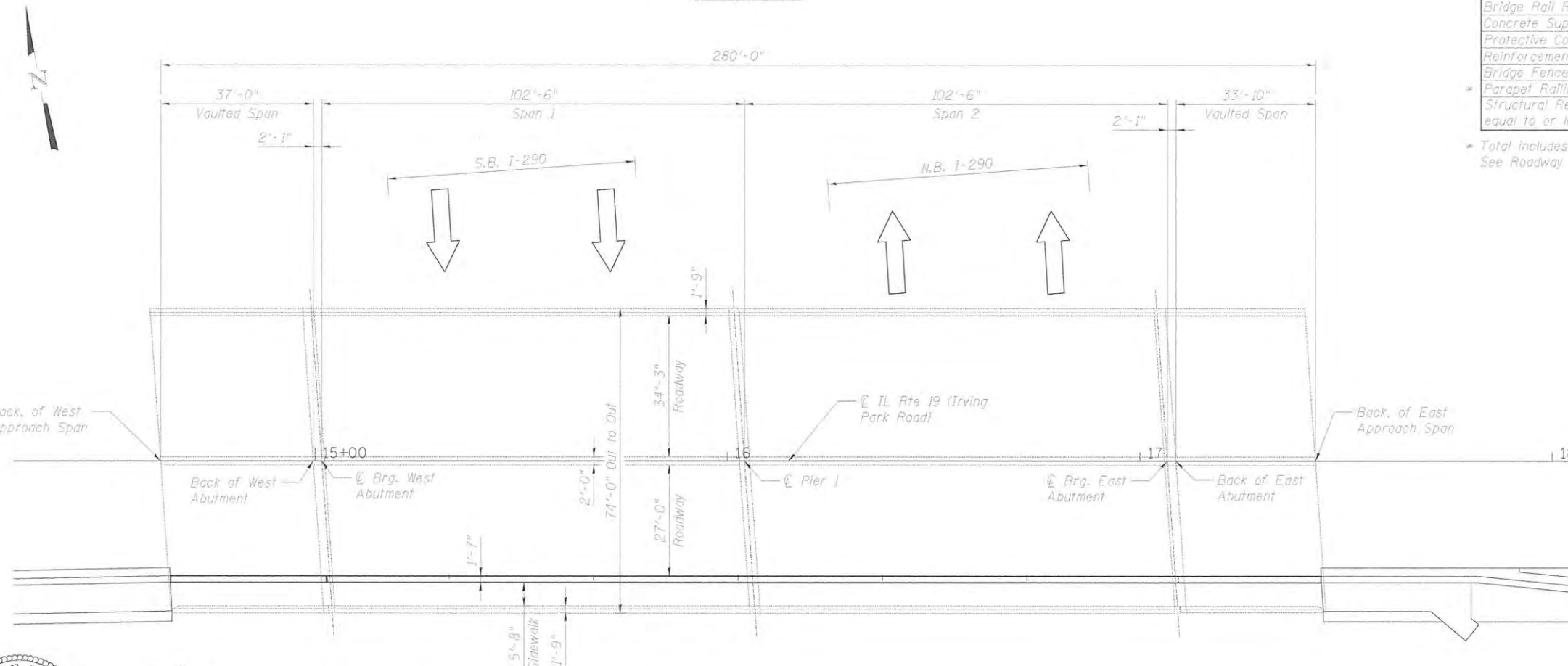
**SCOPE OF WORK**

Remove existing aluminum rail atop S. parapet and install Bridge Fence Railing on existing S. parapet.

Construct new parapet with parapet railing along south shoulder in EB lanes.



**ELEVATION**



**PLAN**



Signed *[Signature]*  
Spiros Pantazis, S.E. Il. Lic. No. 081-006448  
Expires 11-30-2016.  
Date 5/20/16

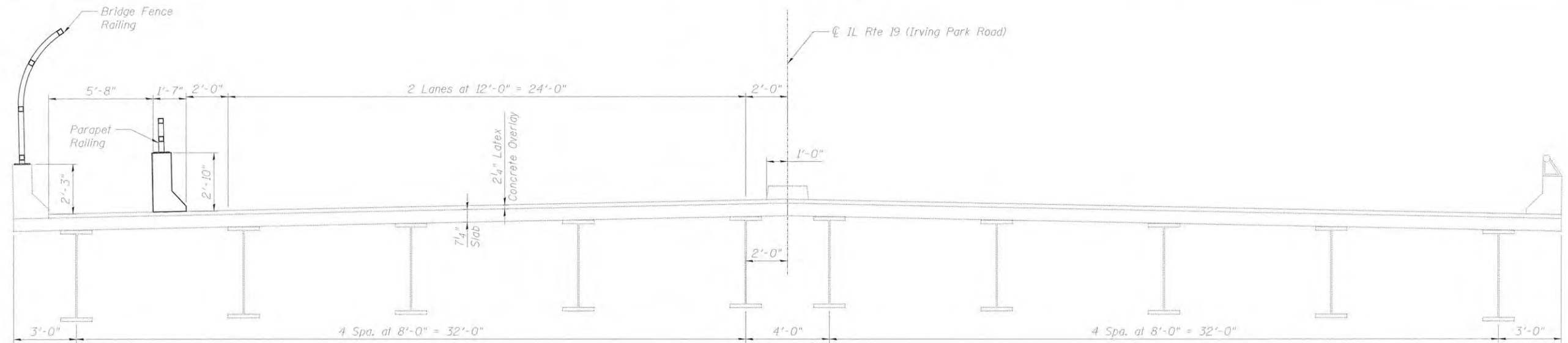
I certify that to the best of knowledge, information and belief, this bridge design is structurally adequate for the design loading as shown on the plans. The design is an economical one for the style of structure and complies with the requirements of the current AASHTO Standard Specifications for Highway Bridges.

**APPROVED**  
For Structural Adequacy Only  
*[Signature]*  
Engineer of Bridges & Structures

**GENERAL PLAN AND ELEVATION  
IL. ROUTE 19 (IRVING PARK ROAD)  
OVER I-290  
SECTION 10-00059-01-SP  
DuPAGE COUNTY  
STRUCTURE NO. 022-0091**

FILE NAME =	USER NAME =	DESIGNED - SP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
<b>TYLIN INTERNATIONAL</b>	CHECKED - PDF	REVISOR -	REVISED -		1321	10-00059-01-SP	DUPAGE	77	64
	DRAWN - SP	REVISOR -	REVISED -		CONTRACT NO. 61C82				
	CHECKED - PDF	REVISOR -	REVISED -		ILLINOIS FED. AID PROJECT				
					SHEET NO. OF SHEETS				

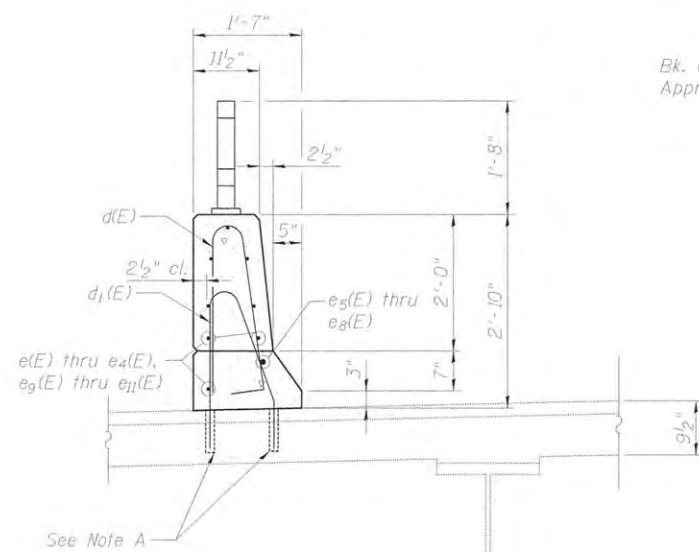
4/24/16 PM



**CROSS SECTION**  
(Looking West)

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape	
d(E)	305	#5	5'-7"	U	
d <sub>1</sub> (E)	305	#5	4'-7 1/2"	U	
e(E)	70	#4	16'-9"	—	
e <sub>1</sub> (E)	16	#4	17'-8"	—	
e <sub>2</sub> (E)	7	#4	14'-3"	—	
e <sub>3</sub> (E)	14	#4	19'-8"	—	
e <sub>4</sub> (E)	7	#4	17'-5"	—	
e <sub>5</sub> (E)	8	#8	25'-9"	—	
e <sub>6</sub> (E)	2	#8	17'-8"	—	
e <sub>7</sub> (E)	1	#8	34'-3"	—	
e <sub>8</sub> (E)	1	#8	37'-5"	—	
e <sub>9</sub> (E)	8	#4	23'-4"	—	
e <sub>10</sub> (E)	1	#4	34'-3"	—	
e <sub>11</sub> (E)	1	#4	37'-5"	—	
Concrete Superstructure				Cu. Yd.	34.4
Reinforcement Bars, Epoxy Coated				Pound	5580

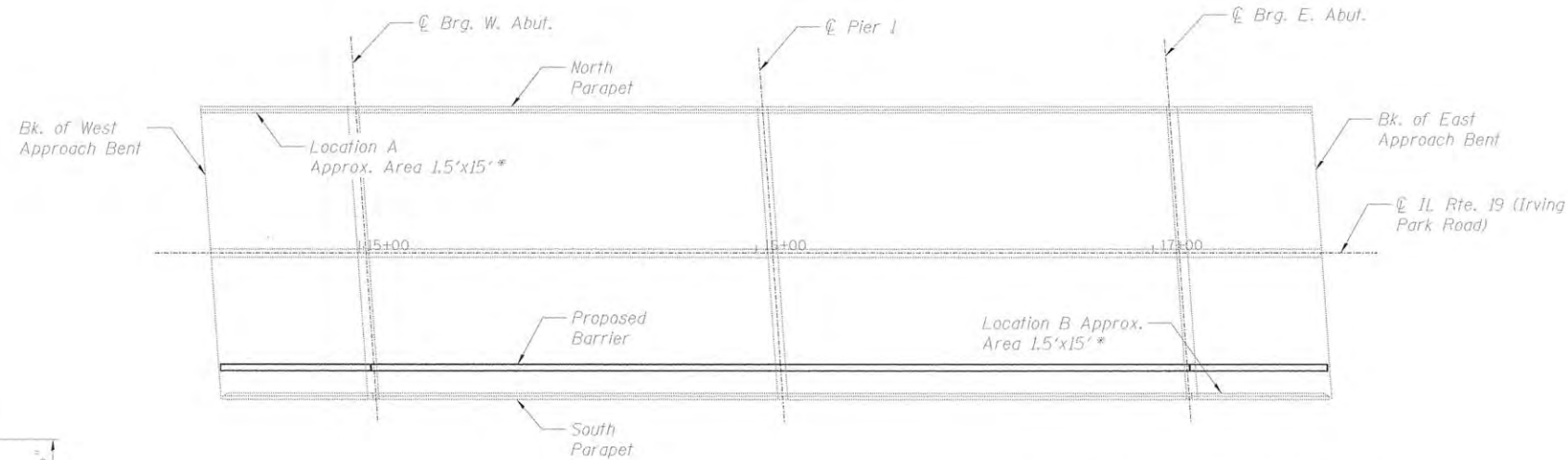


**SECTION THRU PARAPET**

(Steel main spans shown, detail similar in 36" PPC I-Beam Approach Spans)

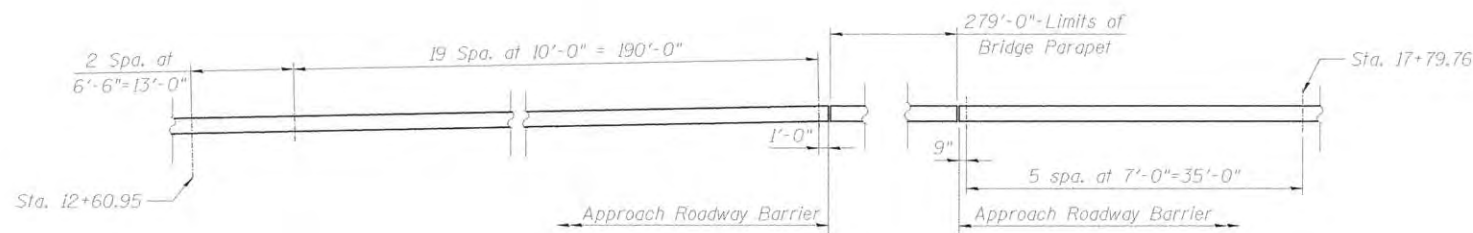
Note A:

Core and set #5 d<sub>1</sub>(E) bar according to Article 509.06 of the Standard Specifications. Cored holes shall be roughened or scored per manufacturer's recommendations. Maximum depth of hole shall not exceed 6". Cost included in Reinforcement Bars, Epoxy Coated.

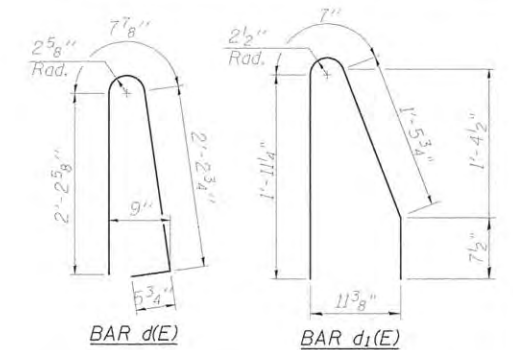


**PLAN - LOCATIONS OF EXISTING PARAPET REPAIRS**

\* Location of Structural Repair of Concrete



**PLAN - PARAPET RAILING SPACING ON APPROACH ROADWAY**



FILE NAME =  
**TYLIN INTERNATIONAL**

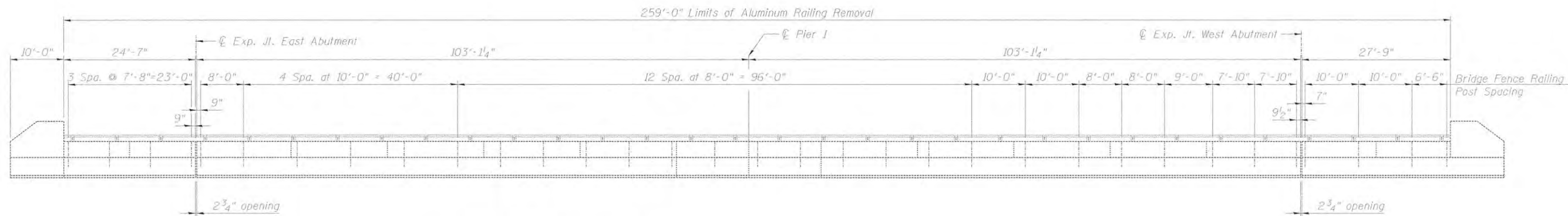
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PLOT SCALE =	DRAWN - SP	REVISED
PLOT DATE =	CHECKED - PDF	REVISED

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

SHEET NO. OF SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 61C82			ILLINOIS FED. AID PROJECT	

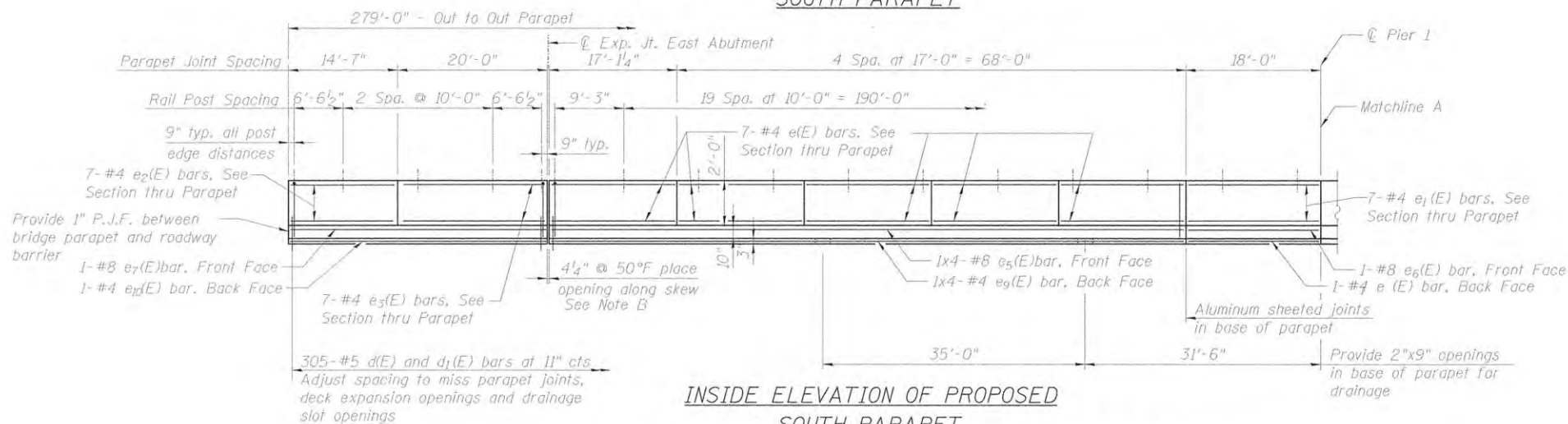
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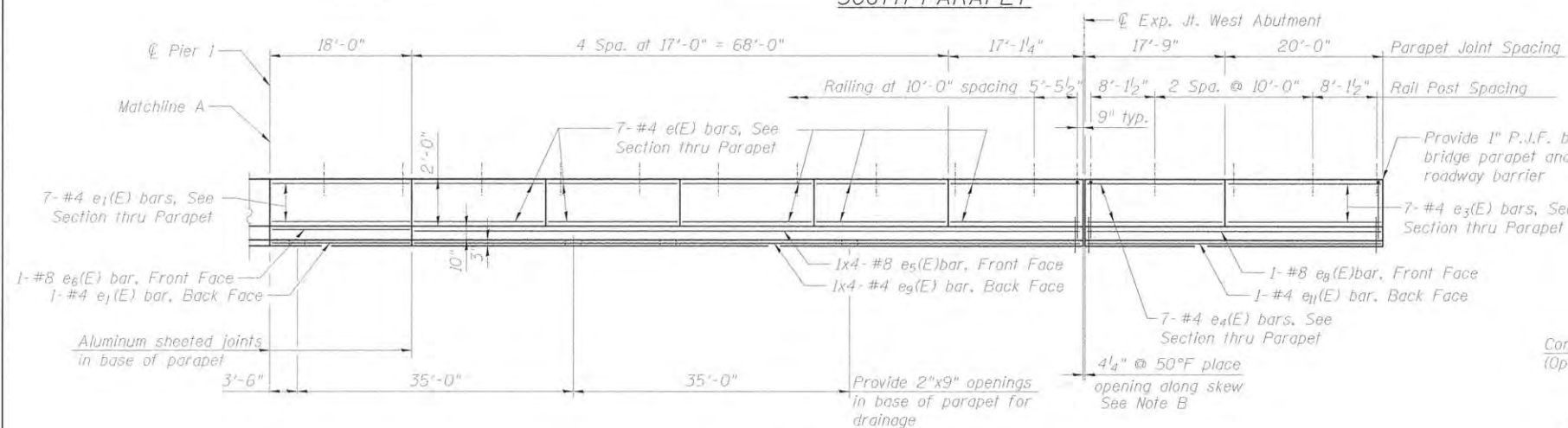
**INSIDE ELEVATION OF EXISTING SOUTH PARAPET**



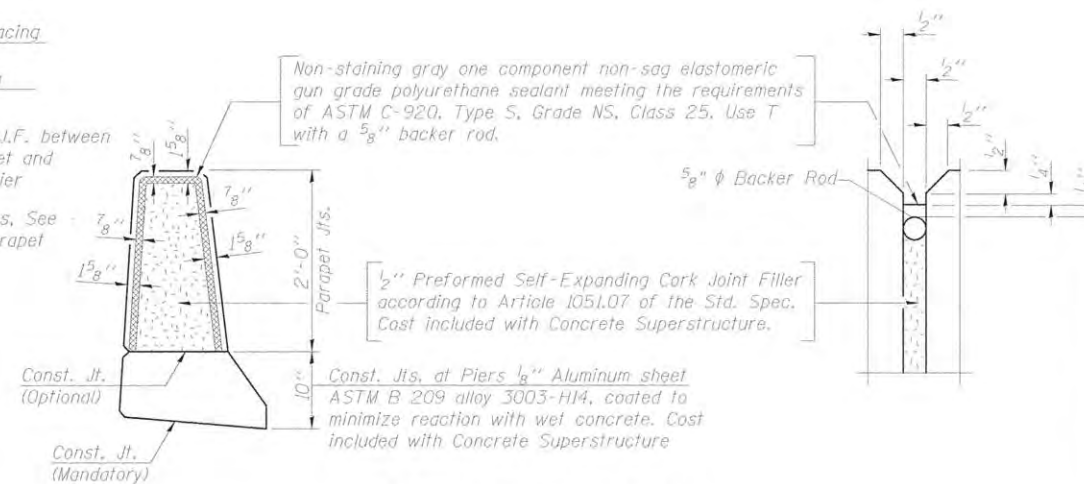
**REMOVAL DETAILS SECTION EXISTING SOUTH PARAPET**



**INSIDE ELEVATION OF PROPOSED SOUTH PARAPET**



**INSIDE ELEVATION OF PROPOSED SOUTH PARAPET**



**PARAPET JOINT DETAILS**

**MINIMUM BAR LAP**

(Parapet)  
 #4 bar = 2'-8"  
 #8 bar = 5'-11"

Note B:  
 Provide gap in parapet to clear expansion joint steel plates.

FILE NAME =  
**TYLIN INTERNATIONAL**

USER NAME =  
 DESIGNED - SP  
 CHECKED - PDF  
 PLOT SCALE =  
 DRAWN - SP  
 PLOT DATE =  
 CHECKED - PDF

REVISED  
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 REVISED

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF ITASCA PEDESTRIAN IMPROVEMENTS  
 STRUCTURE NO. 022-0091 - PARAPET ELEVATIONS**

SHEET NO. S-3 OF S-5 SHEETS

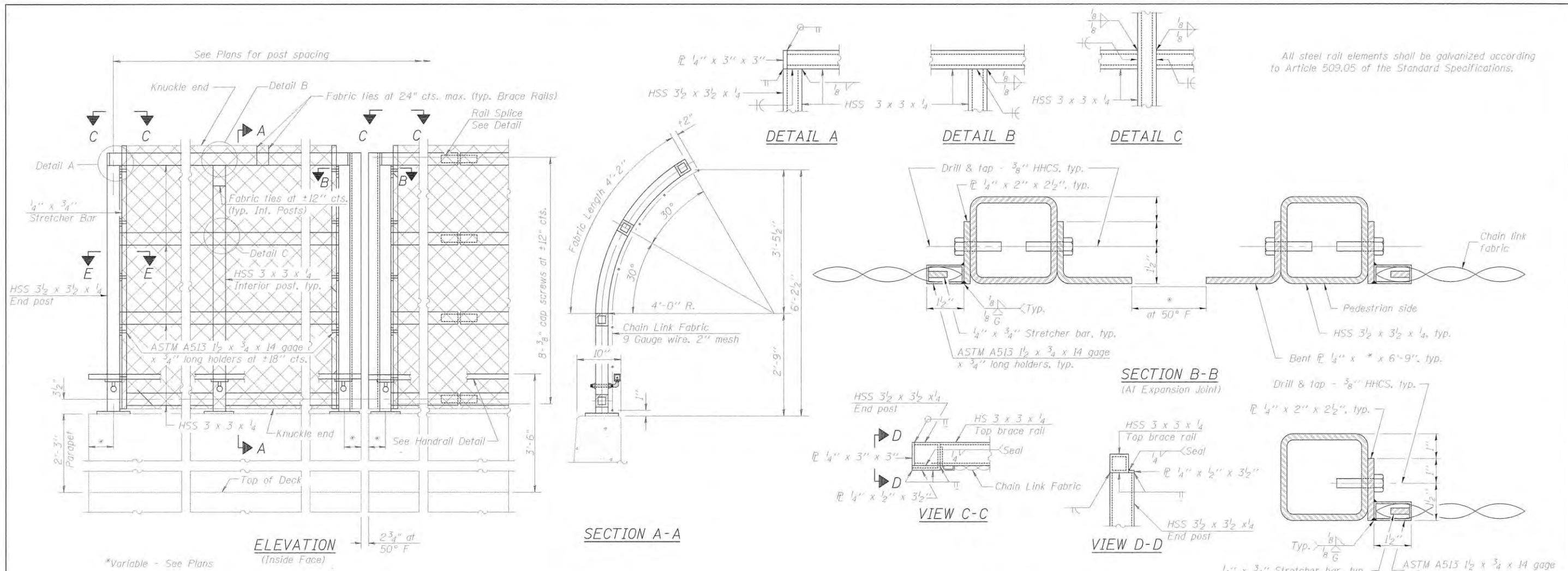
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1321	10-00059-01-SP	DUPAGE	77	66
			CONTRACT NO. 61C82	

ILLINOIS FED. AID PROJECT

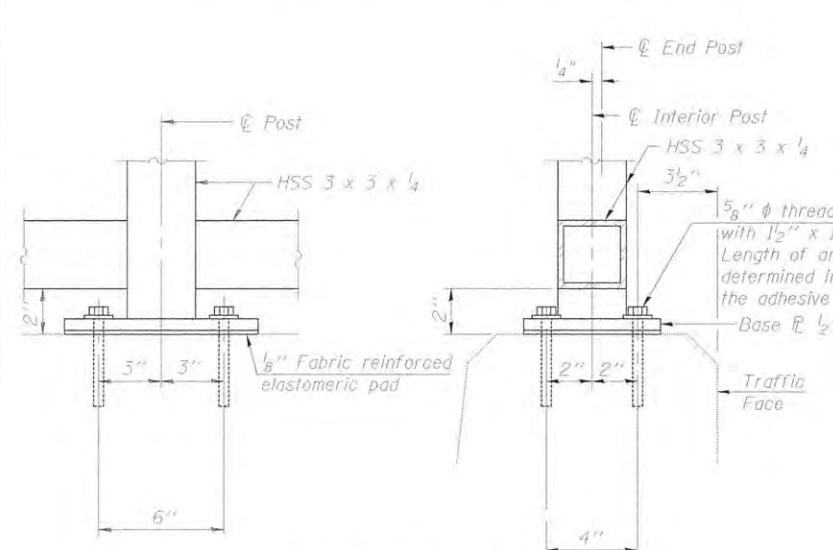
460459 PJM 5/19/2016



All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.

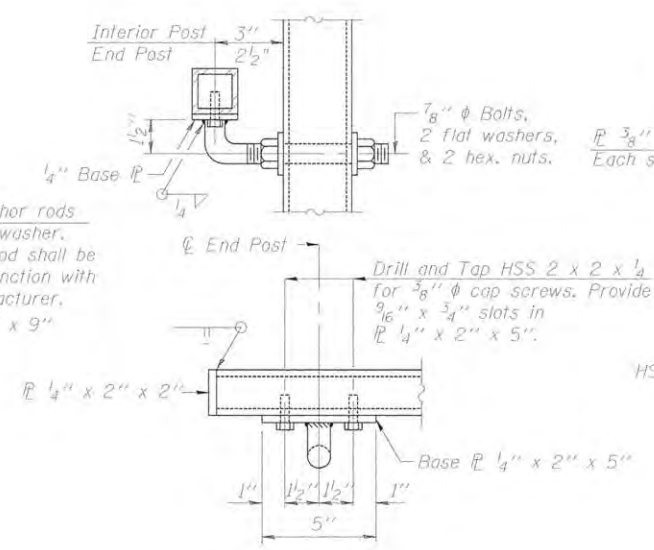


\*Variable - See Plans

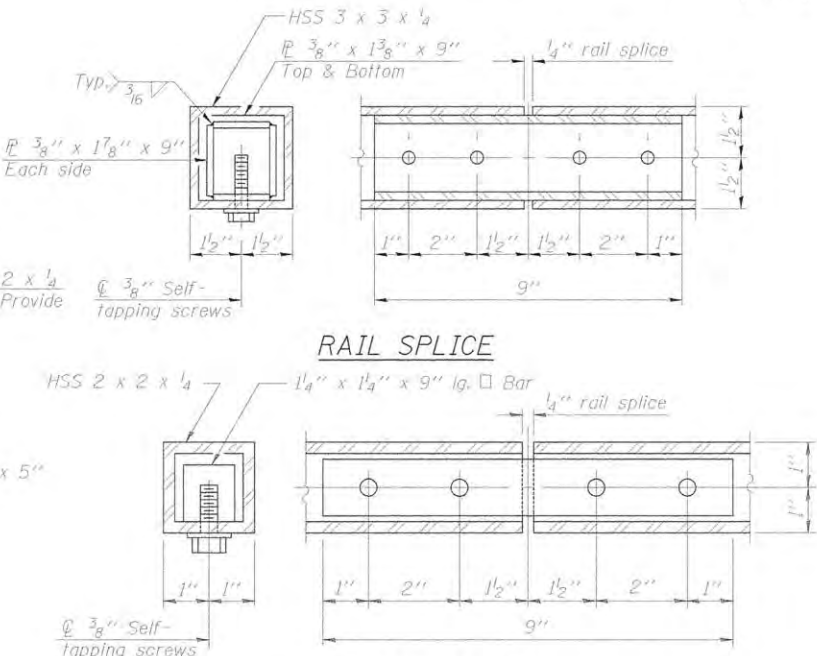


**ANCHOR BOLT DETAILS**

The Contractor shall drill and set 5/8"  $\phi$  anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.

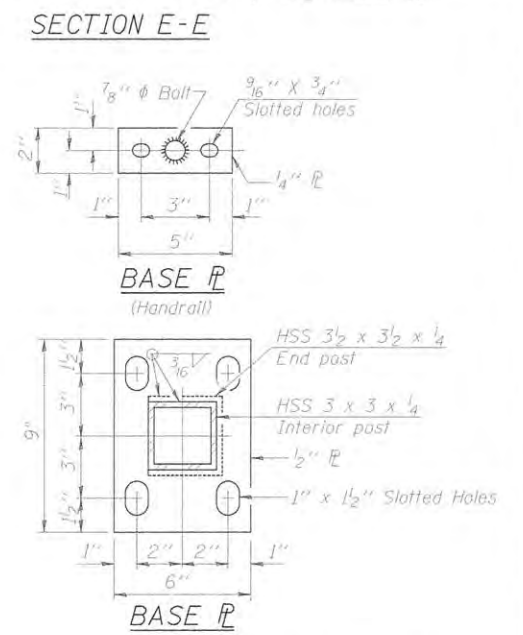


**HANDRAIL DETAIL**



**RAIL SPLICE**

**HANDRAIL SPLICE**



**BASE PL**  
(Handrail)

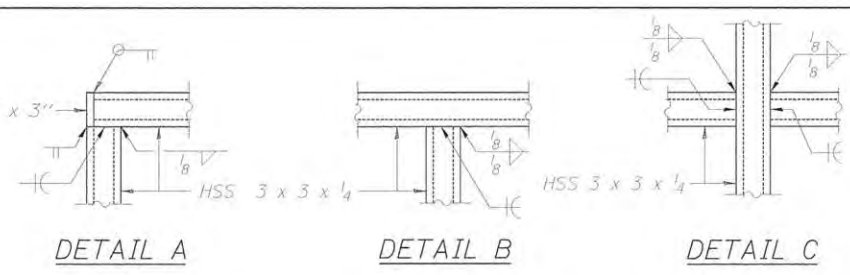
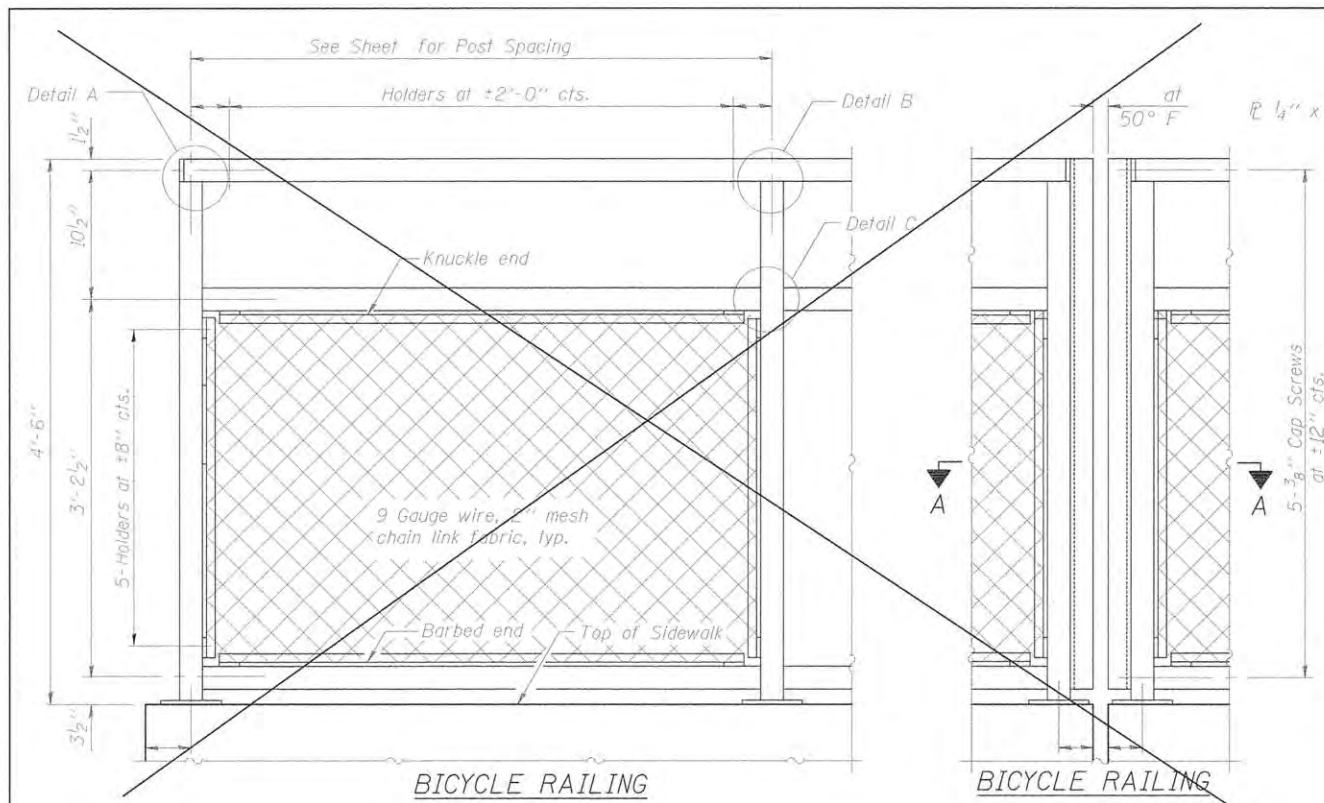
**BASE PL**

**BILL OF MATERIAL**

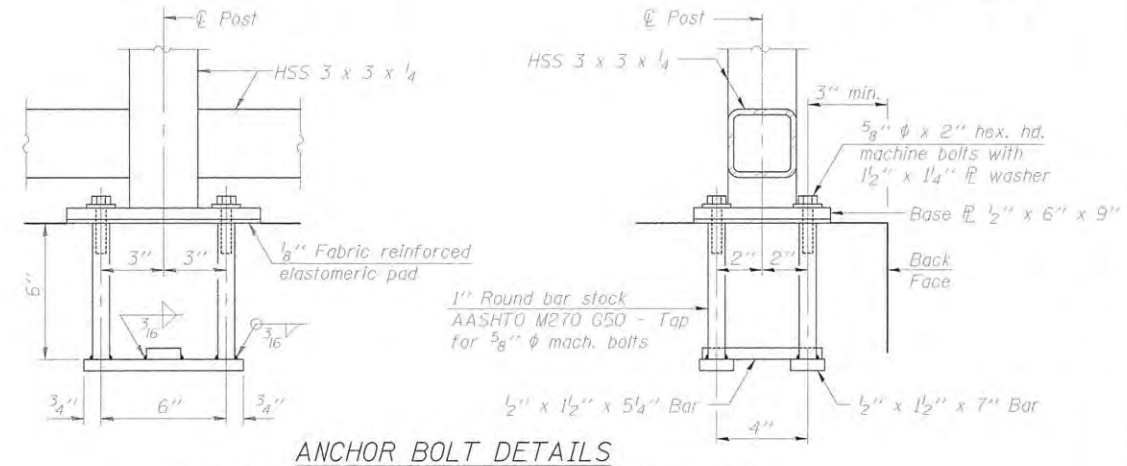
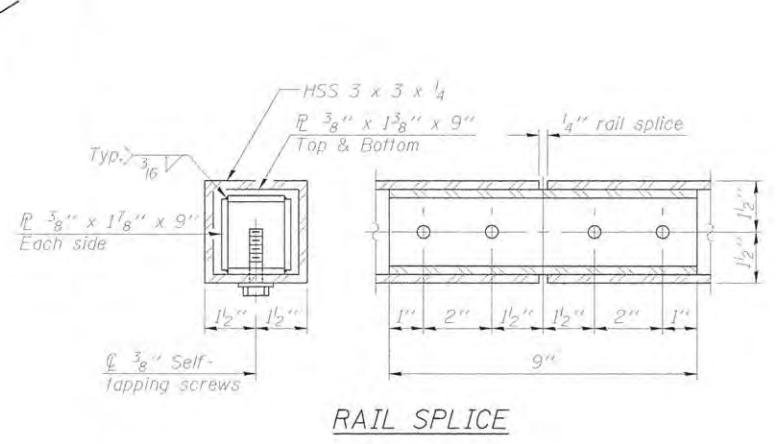
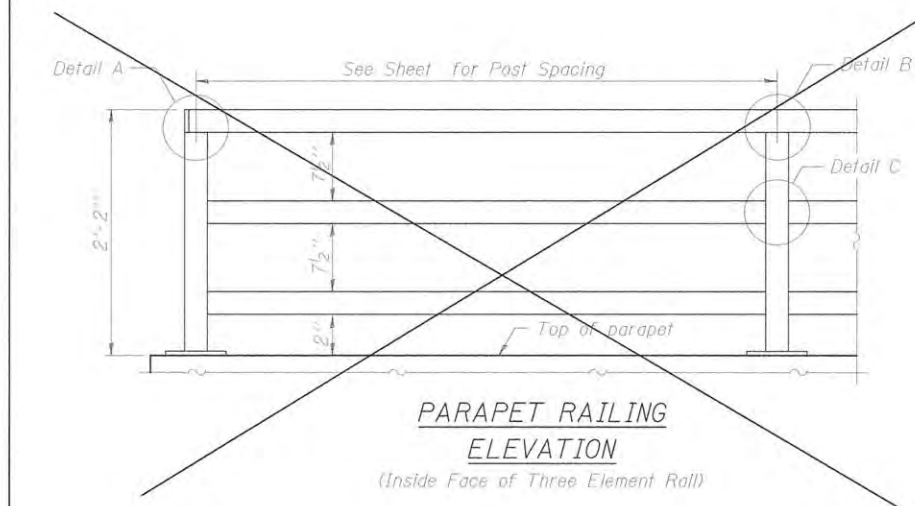
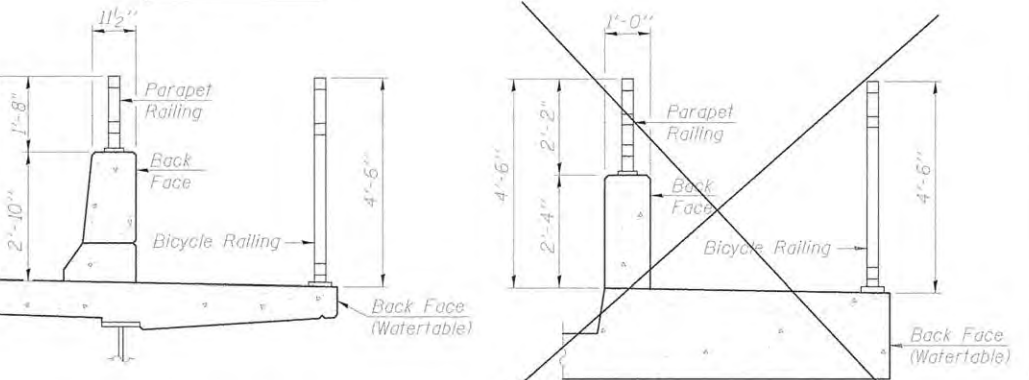
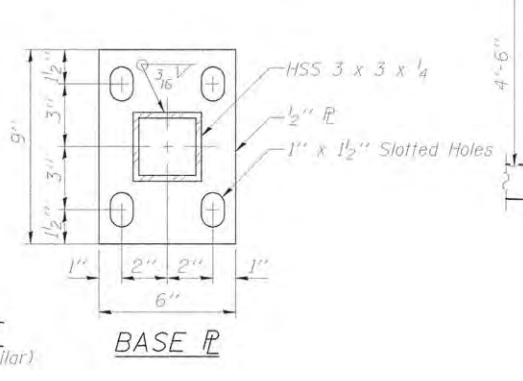
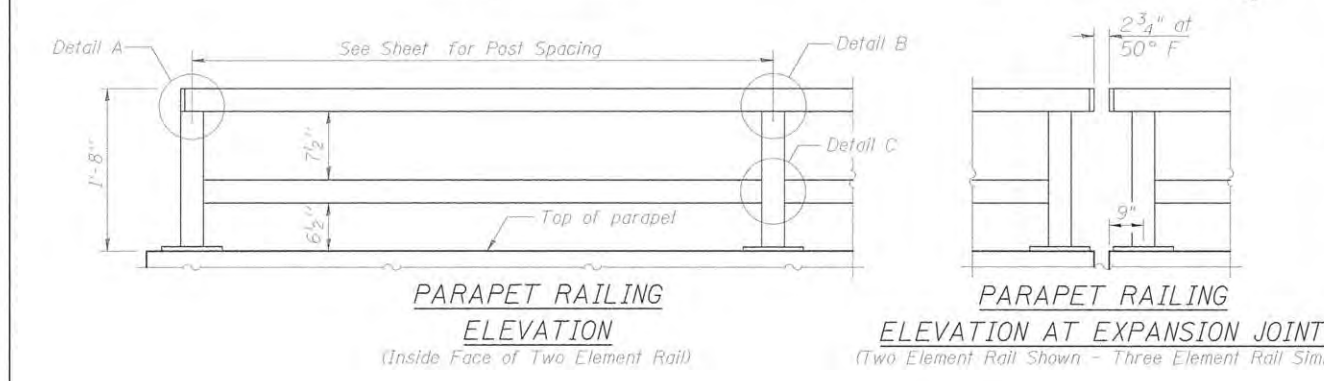
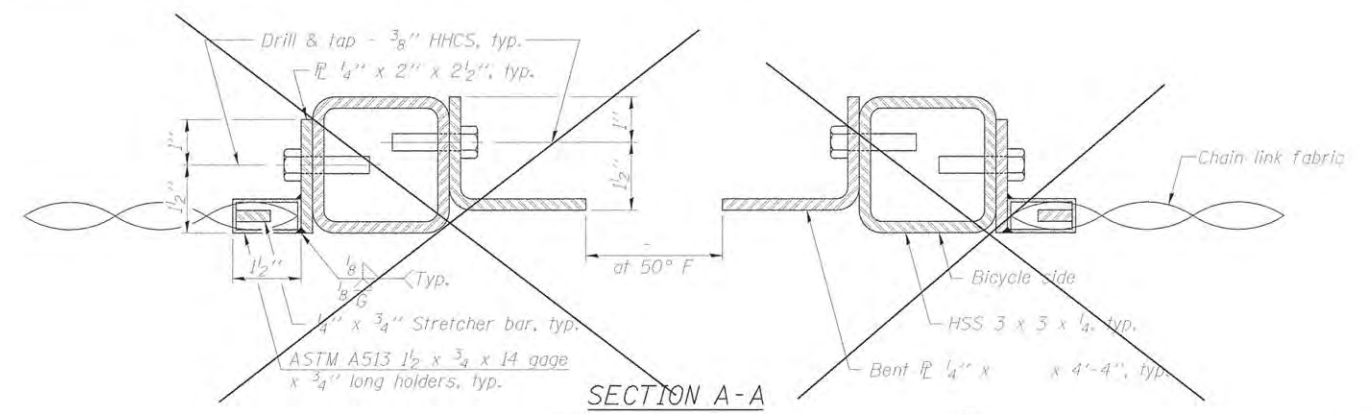
Item	Unit	Quantity
Bridge Fence Railing	Foot	259

R-32

1-12-15



All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.



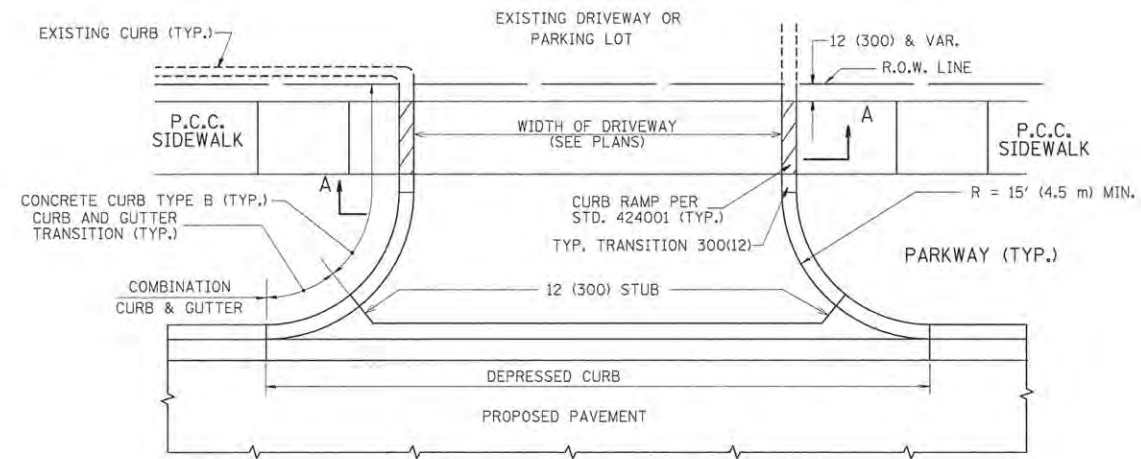
In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting 5/8" phi anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.

**BILL OF MATERIAL**

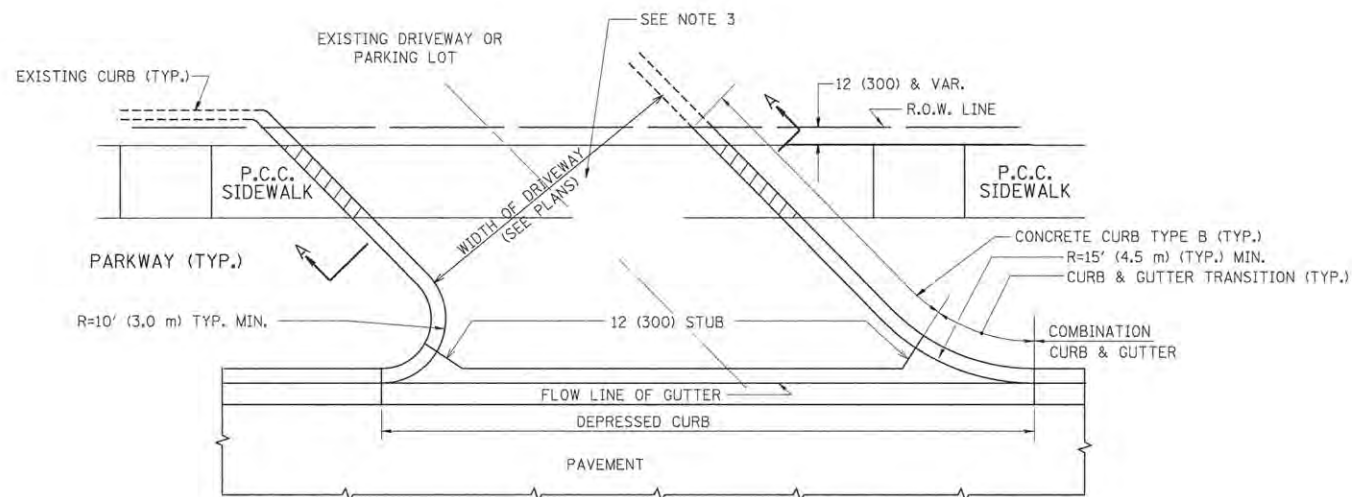
Item	Unit	Quantity
Parapet Railing	Foot	519

(Total includes quantity on approach roadway barrier, see Roadway Plans.)

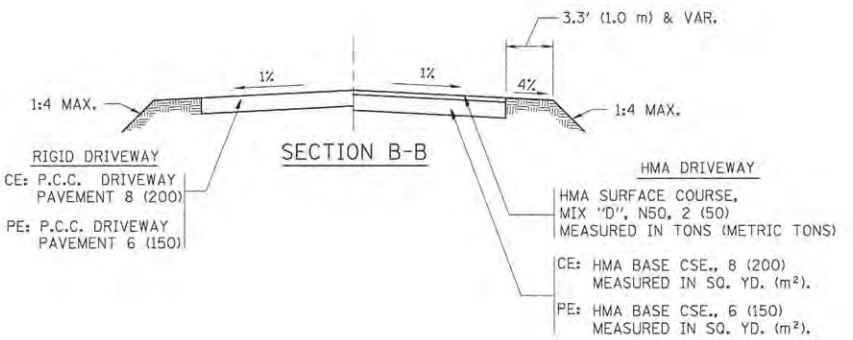
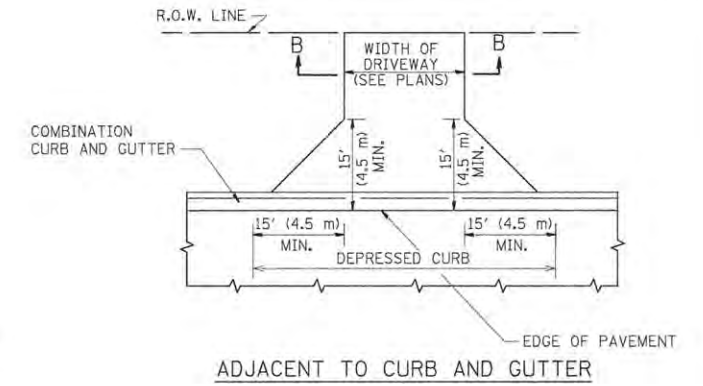
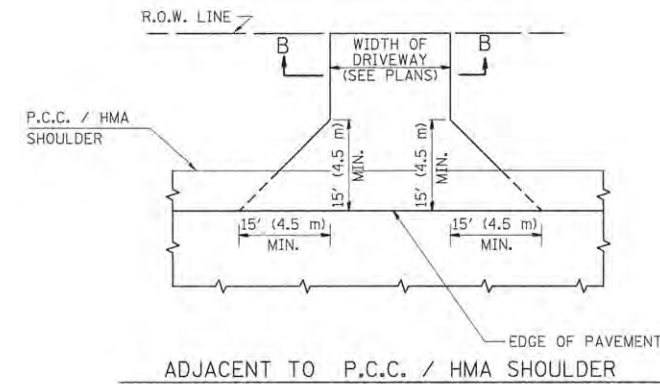
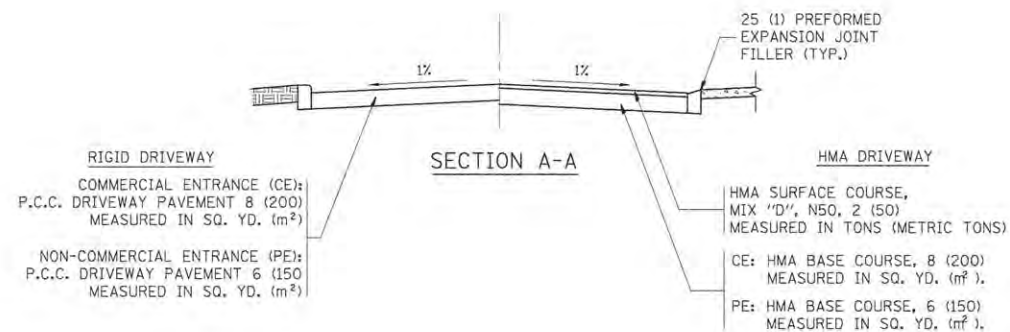
R-29 1-12-15 (10'-0" Maximum Post Spacing)



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE COURSE, TYPE B, 8 (200) MEASURED IN SQ. YD. (m<sup>2</sup>).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME =	USER NAME = lejso	DESIGNED - R. SHAH	REVISED - P. LOFLUER 04-15-03
c:\pwork\p\p\dot\lejso\0108315\bd01.dwg		DRAWN -	REVISED - R. BORO 01-01-07
		CHECKED -	REVISED - R. BORO 06-11-08
		DATE - 11-04-95	REVISED - R. BORO 09-06-11

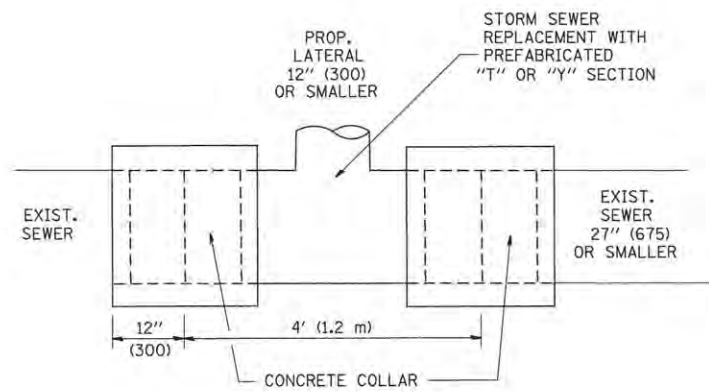
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.  
AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

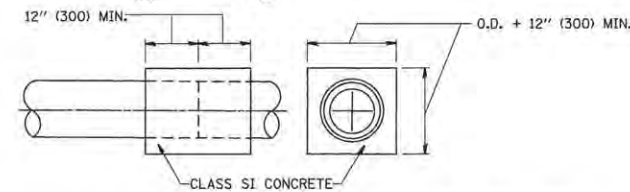
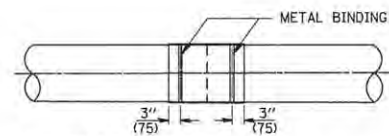
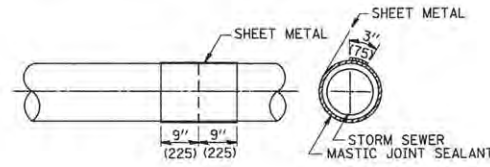
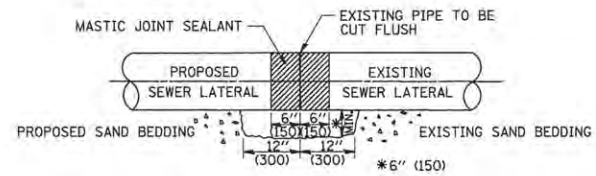
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	10-00059-01-SP	DUPAGE	77	69
BD0156-07 (BD-01)			CONTRACT NO. 61C82	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

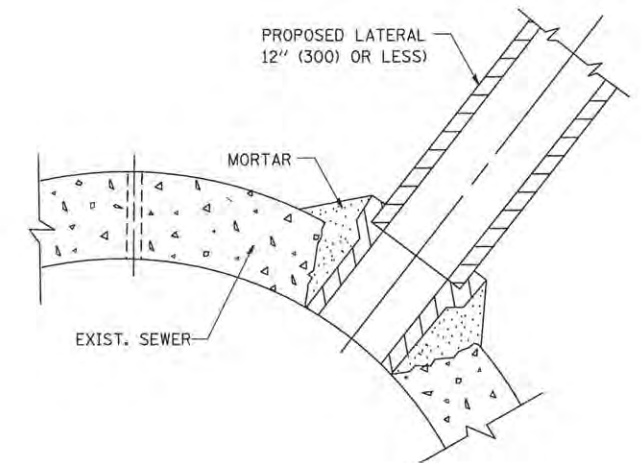


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT, BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6' (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
  - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
  - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

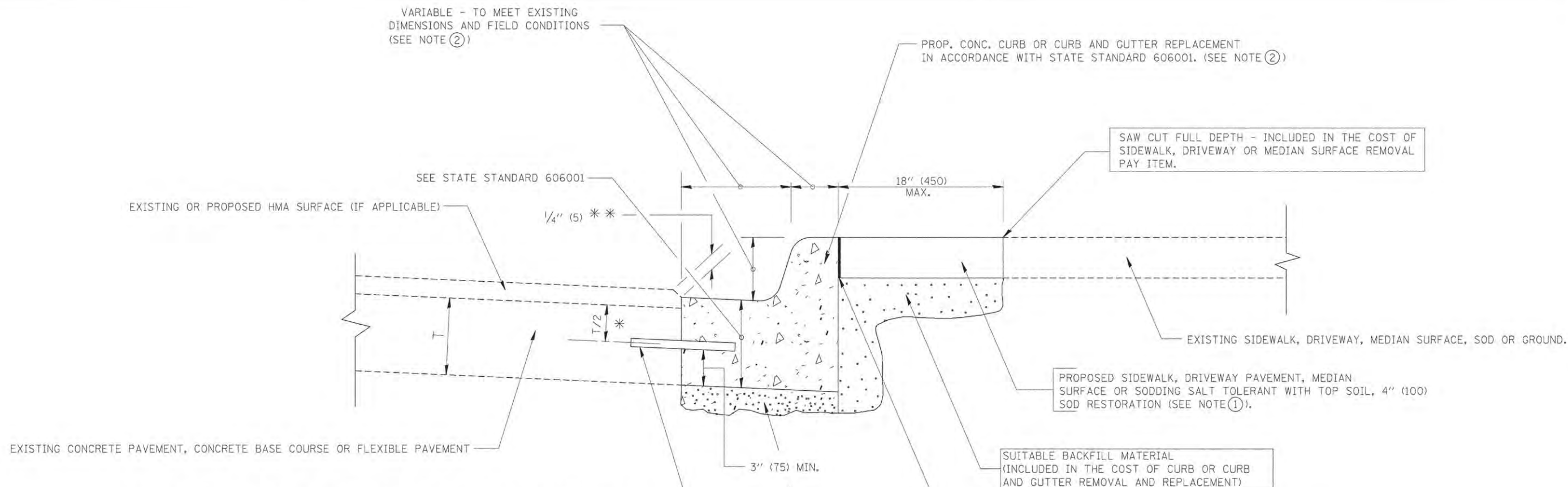
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	PLOT SCALE = 50.000 1 / IN.	CHECKED -	REVISED - R. SHAH 10-25-94
	PLOT DATE = 1/4/2008	DATE - 07-25-90	REVISED - R. SHAH 06-12-96

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DETAIL OF STORM SEWER  
CONNECTION TO EXISTING SEWER

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	10-00059-01-SP	DUPAGE	77	70
BD500-01 (BD-7)			CONTRACT NO. 61C82	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \* \* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.  
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

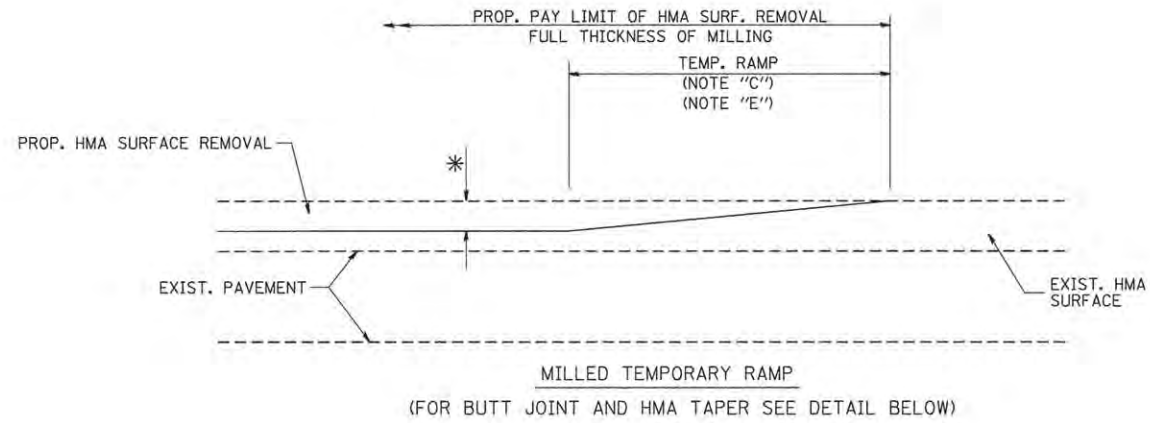
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

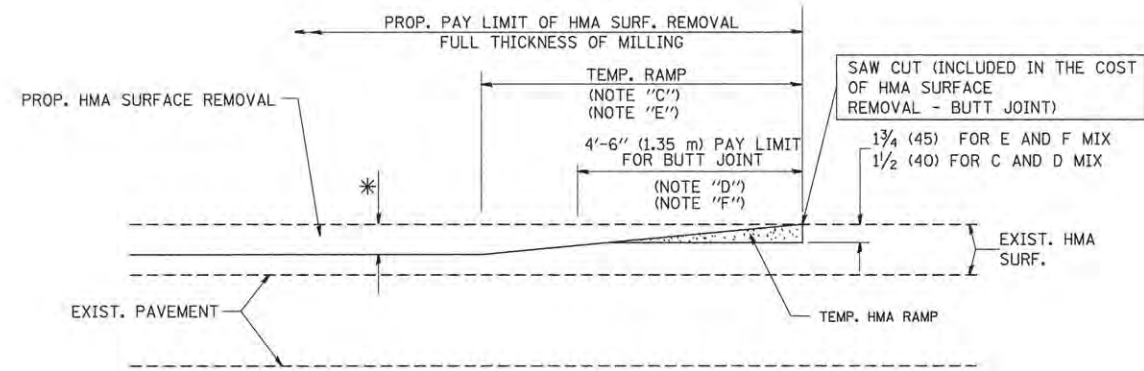
## CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0:\pwork\pvidot\drivakosgn\00108315\bd24.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97			1321	10-00059-01-SP	DUPAGE	77	71
PLOT SCALE = 50.000 = 1 IN.		CHECKED -	REVISED - M. GOMEZ 01-22-01				<b>BD600-06 (BD-24)</b>			<b>CONTRACT NO. 61C82</b>
PLOT DATE = 12/15/2009		DATE - 03-11-94	REVISED - R. BORO 12-15-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.			TO STA.
						FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

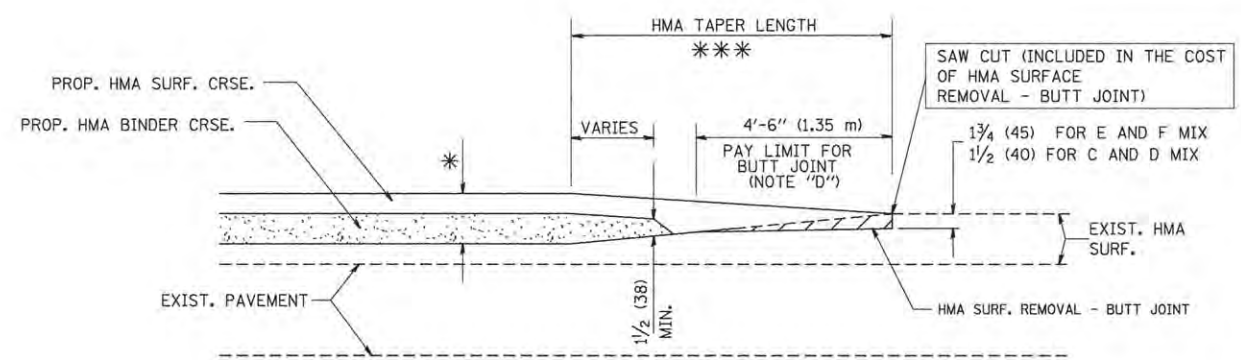


**OPTION 1**

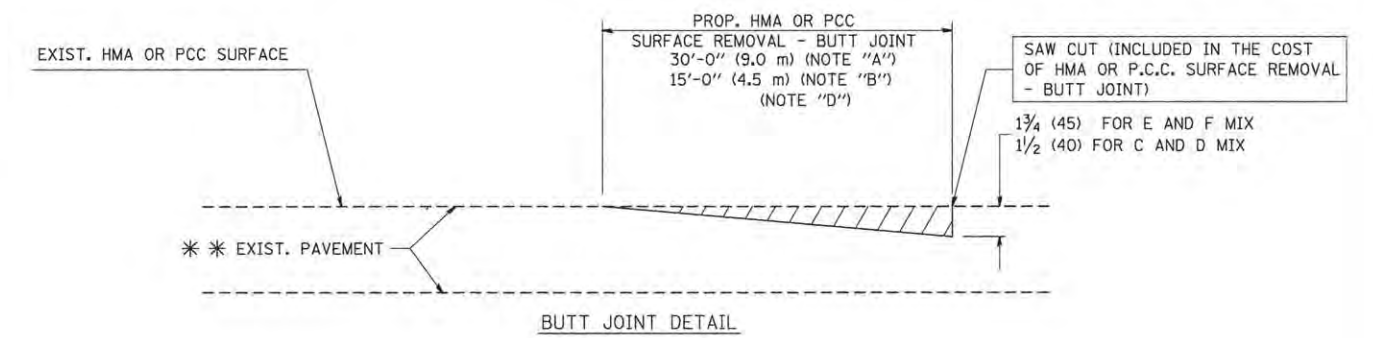


**OPTION 2**  
**HMA CONSTRUCTED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

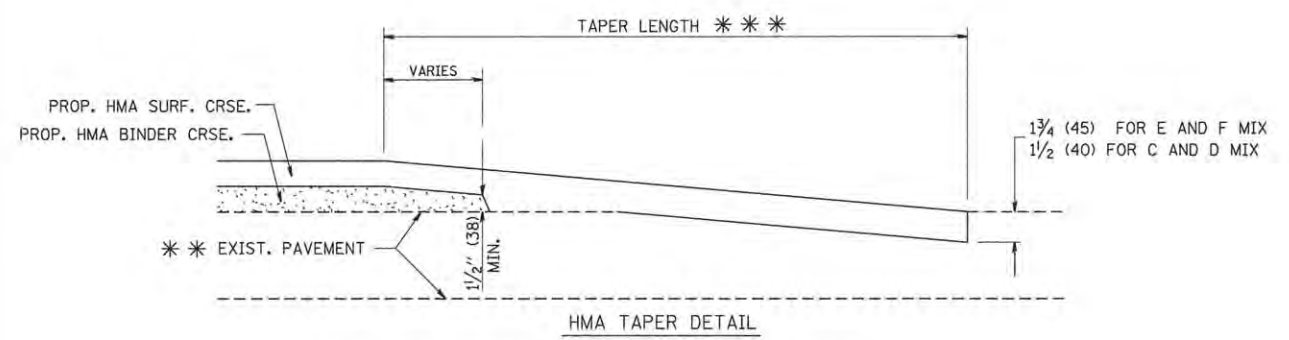
**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gajlonobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

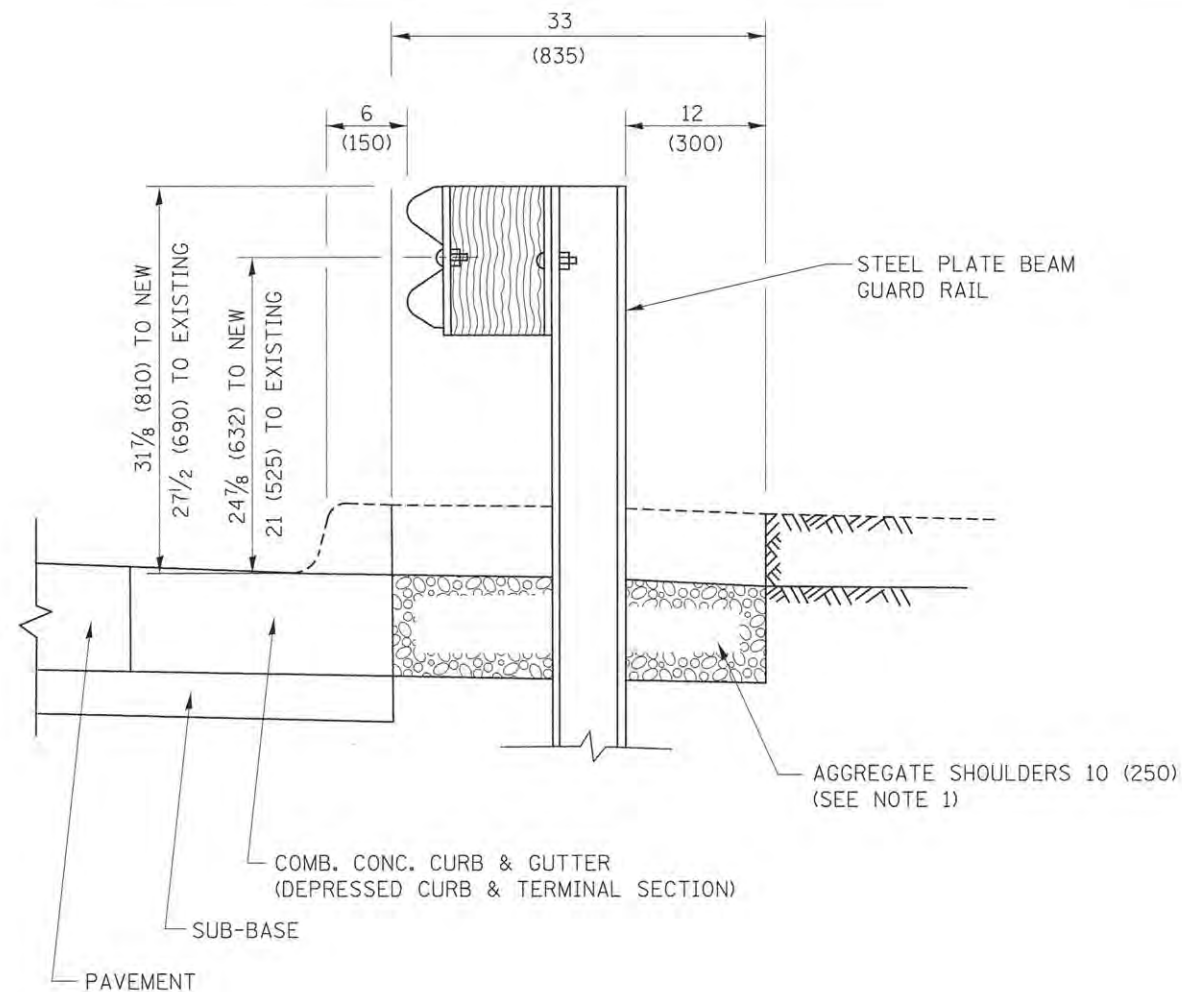
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 72
BD400-05 BD32			CONTRACT NO. 61C82	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

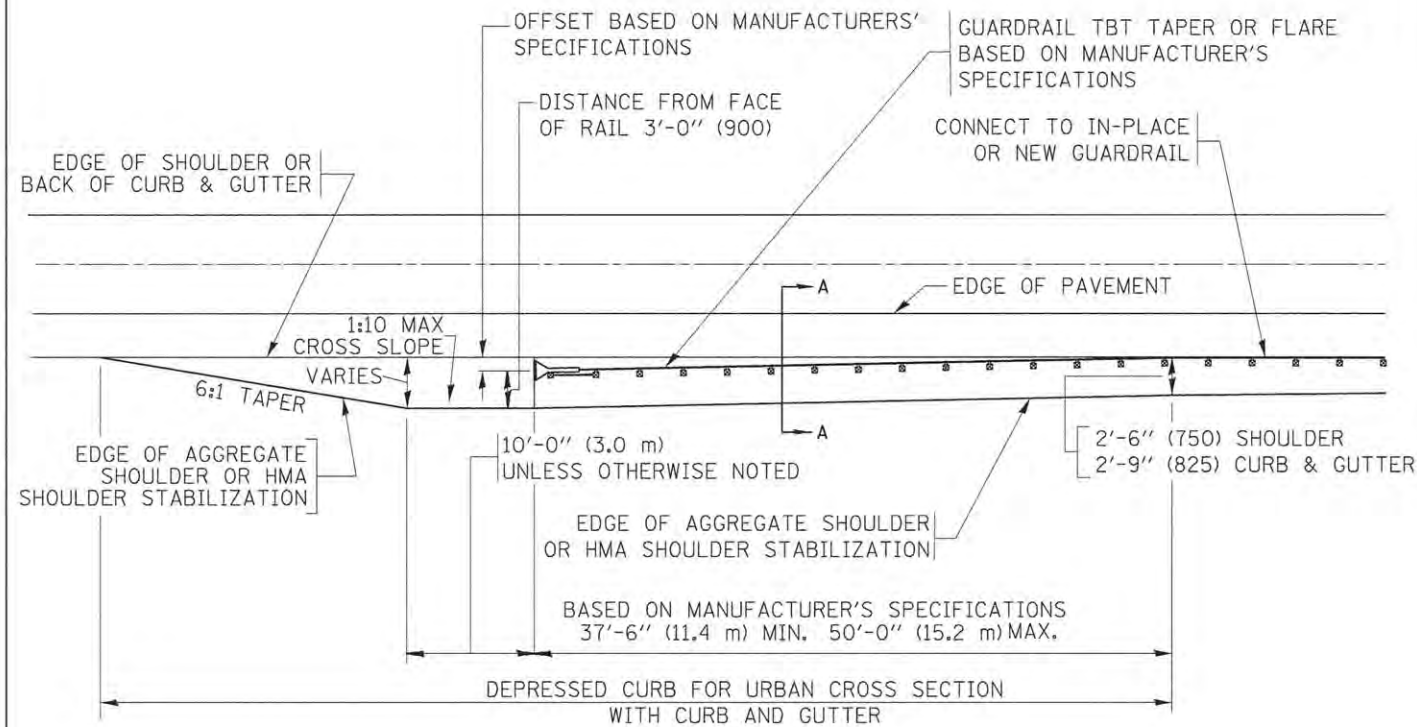




SECTION A-A

- NOTES:
1. THE AGGREGATE SHOULDER, 10 (250) OR HMA SHOULDER, 6 (150) (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
  2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
  3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

**DETAILS FOR STEEL PLATE BEAM  
GUARD RAIL ADJACENT TO CURB AND GUTTER  
[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]**



**DEPRESSED CURB AND GUTTER AND  
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

AGGREGATE SHOULDER, 10 (250) WILL BE PAID ACCORDING TO SECTION 481.

HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID ACCORDING TO SECTION 482.

COMB. CONC. C&G, STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL  
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)  
UNLESS OTHERWISE SHOWN.

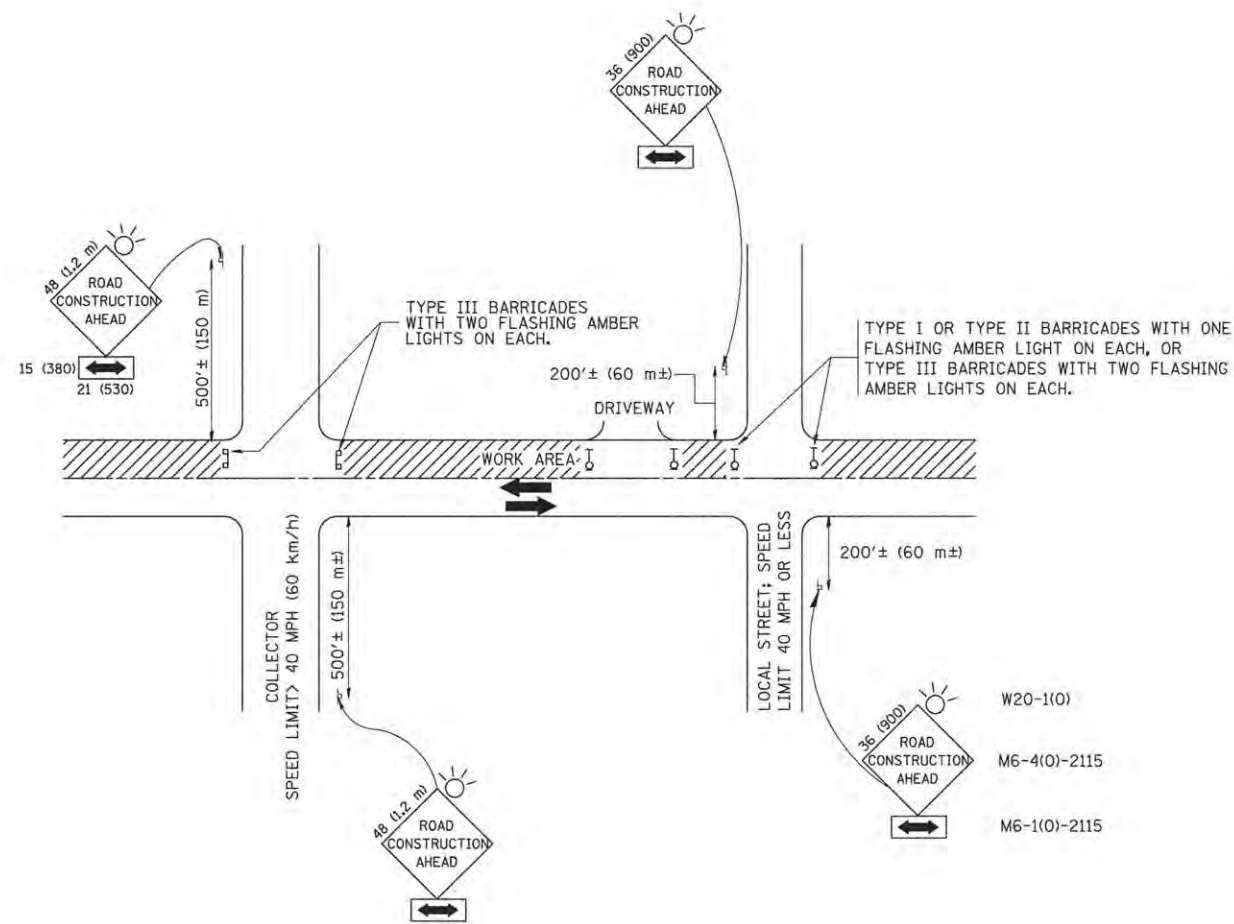
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Default	PLOT SCALE = 50.0000 ' / in.	DATE - 09-22-90	REVISED - R. BORO 08-06-2012
	PLOT DATE = 12/21/2015		REVISED - R. BORO 05-08-2015

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DETAILS FOR DEPRESSED CURB & GUTTER AND  
SHOULDER TREATMENT AT TBT TY. 1 SPL.

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	10-00059-01-SP	DUPAGE	77	73
BD600-10 (BD 34)			CONTRACT NO. 61C82	
ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

  - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
  - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (Inches) unless otherwise shown.

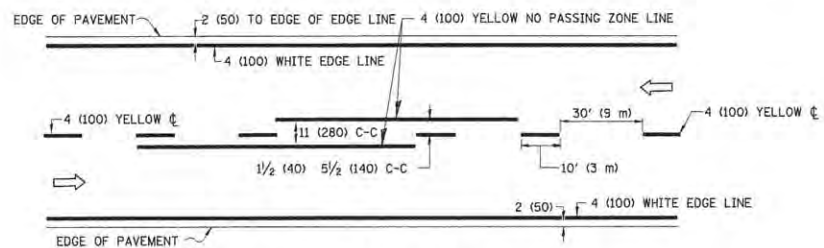
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		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

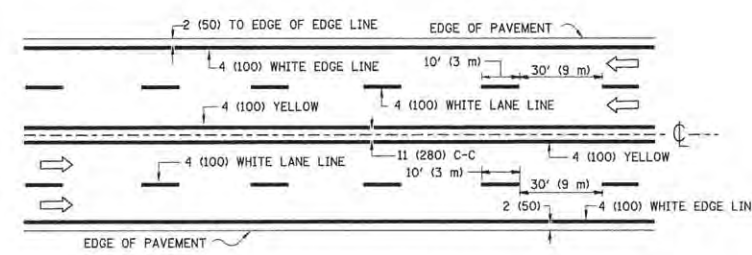
TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

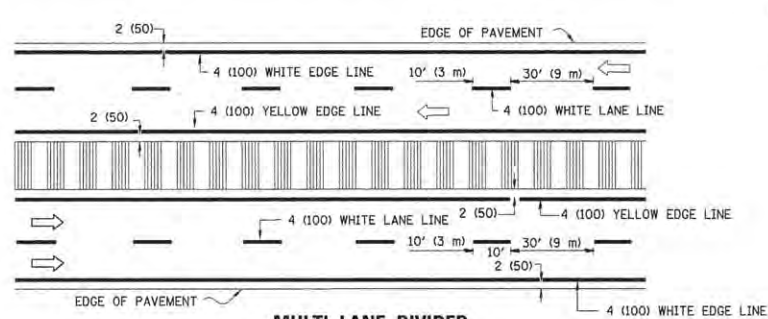
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1321	10-00059-01-SP	DUPAGE	77	74
TC-10			CONTRACT NO. 61C82	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**2-LANE ROADWAY**

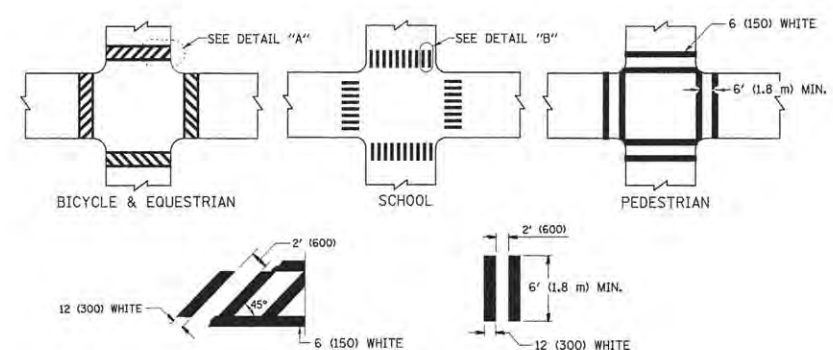


**MULTI-LANE UNDIVIDED**



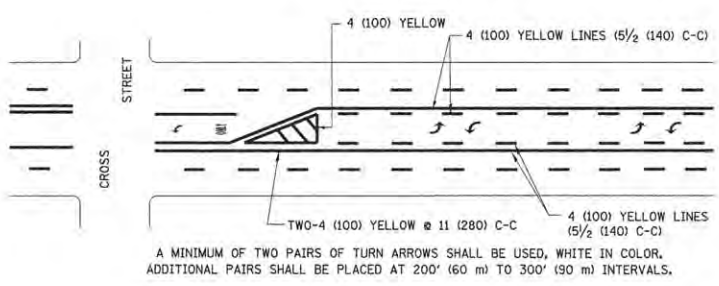
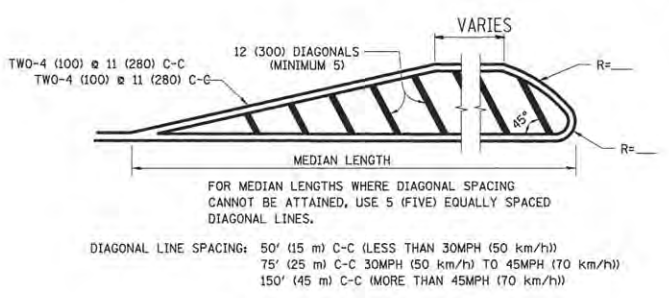
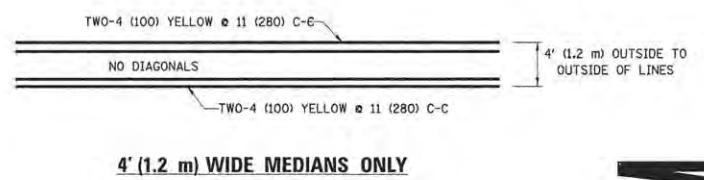
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

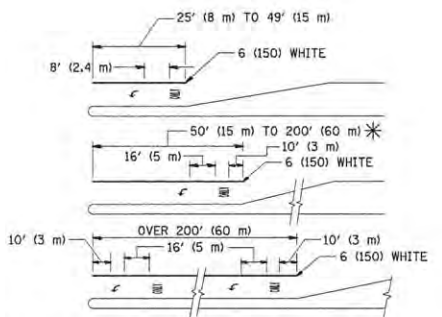


**DETAIL "A" TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



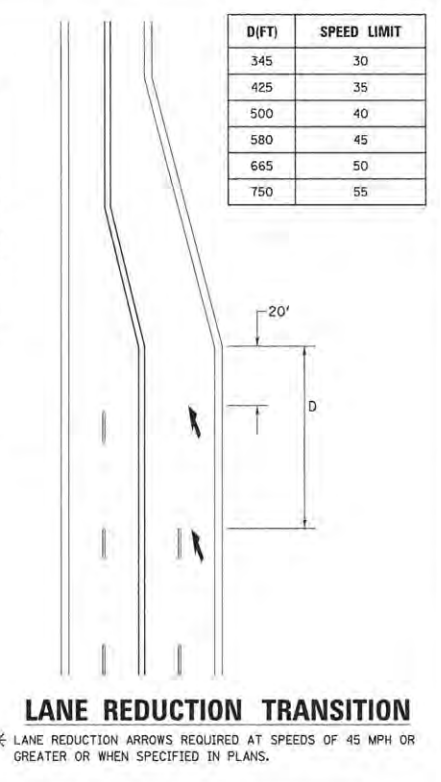
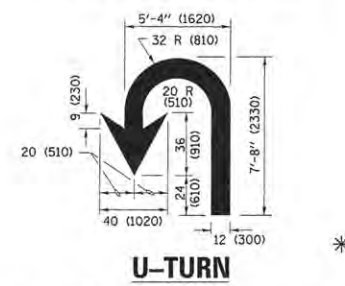
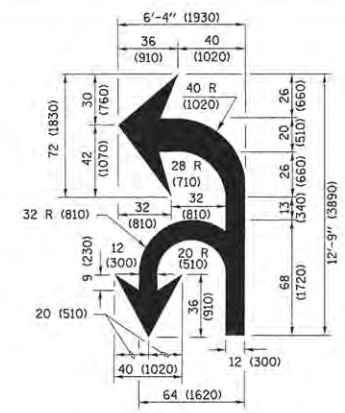
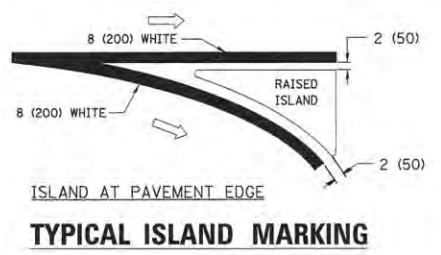
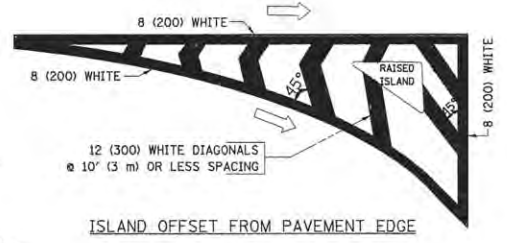
**MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING**



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING**



TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

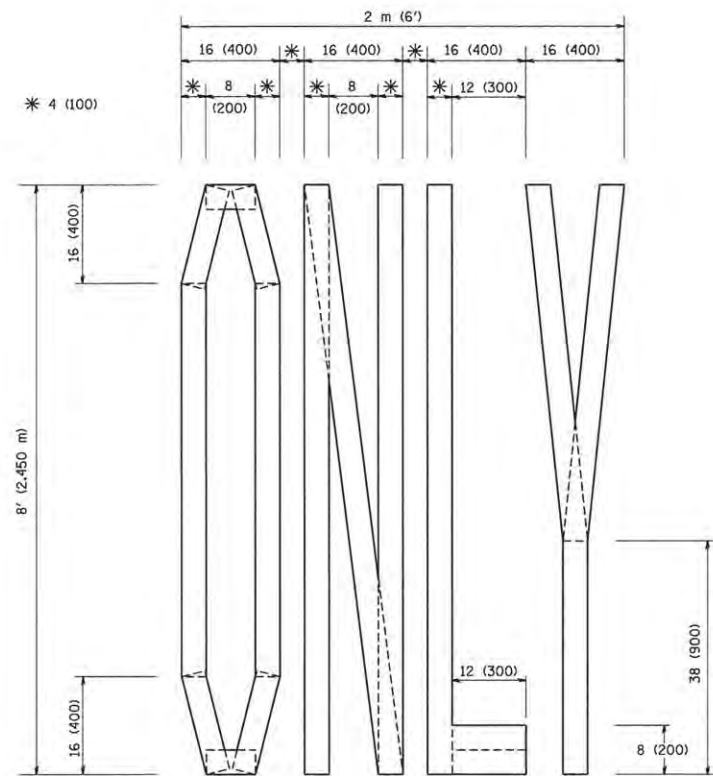
FILE NAME =	USER NAME = iszekr	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
Documents\IDOT Offices\District 1\Projects\District 1\CADData\CADsheets\1013.dgn	DRW	CHECKED -	REVISED - C. JUCIUS 09-09-09
Default	PLOT SCALE = 50.0000 / in.	DATE - 03-19-90	REVISED - C. JUCIUS 07-01-13
	PLOT DATE = 12/21/2015		REVISED - C. JUCIUS 12-21-15

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

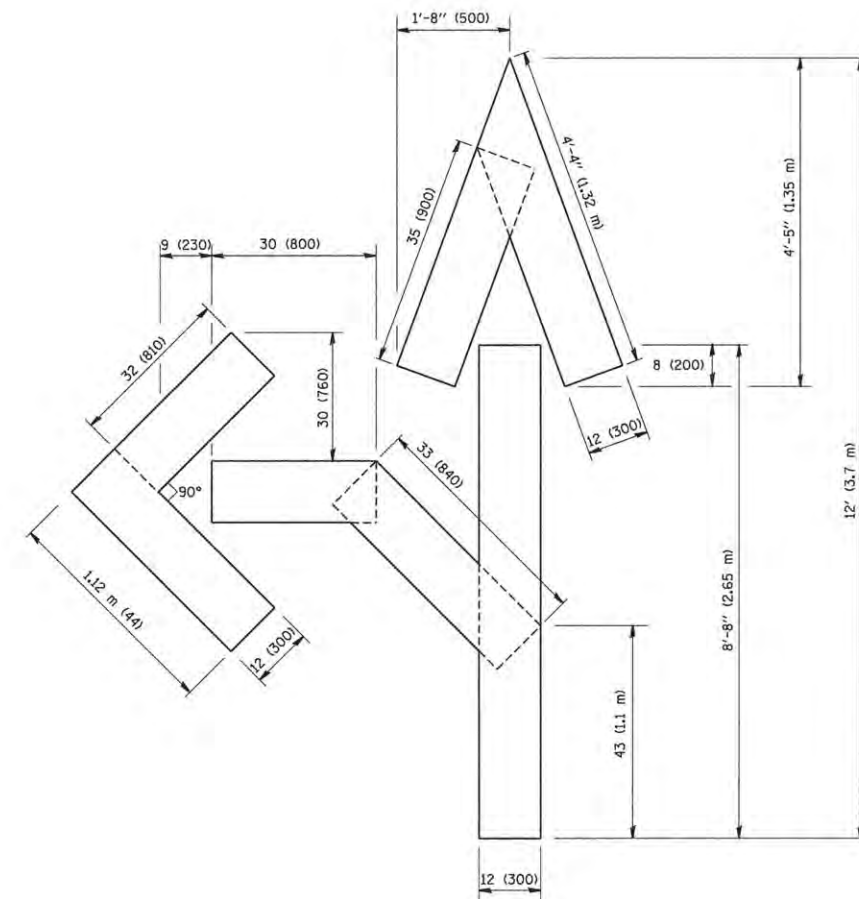
<b>DISTRICT ONE TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	10-00059-01-SP	DUPAGE	77	75
<b>TC-13</b>			<b>CONTRACT NO. 61C82</b>	
ILLINOIS FED. AID PROJECT				

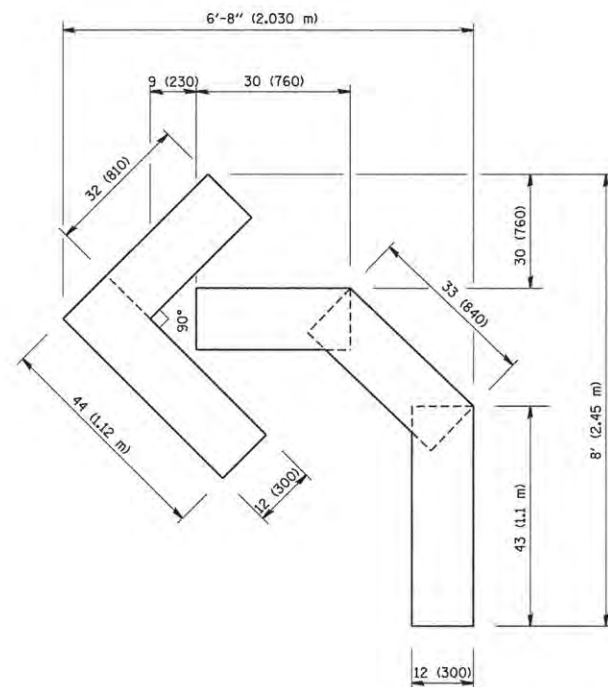




QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

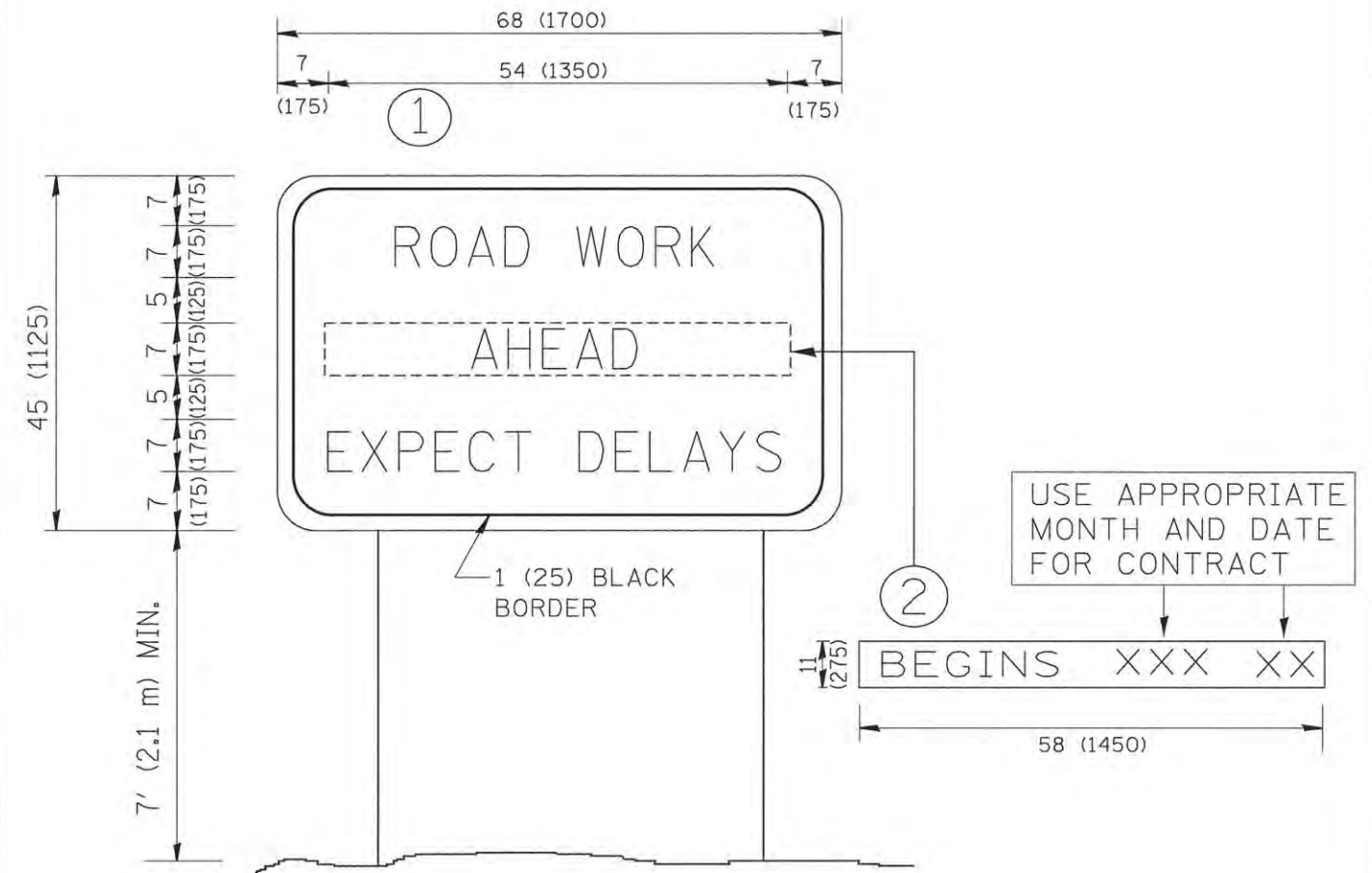
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		DRAWN -	REVISED -T. RAMMACHER 11-04-97
		CHECKED -	REVISED -T. RAMMACHER 03-02-98
		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 1321	SECTION 10-00059-01-SP	COUNTY DUPAGE	TOTAL SHEETS 77	SHEET NO. 76
TC-16			CONTRACT NO. 61C82	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dststd\22x34\to22.dgn	USER NAME = geglionbt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD  
INFORMATION SIGN

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	10-00059-01-SP	DUPAGE	77	77
TC-22			CONTRACT NO. 61C82	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				