

KANKAKEE VALLEY AIRPORT AUTHORITY

KANKAKEE, ILLINOIS

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CONSTRUCTION PLANS FOR GREATER KANKAKEE AIRPORT PERIMETER SAFETY/WILDLIFE FENCE PHASE 1

ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR150520	MOBILIZATION	LS	1	
AR151414	TREE TRIMMING	EACH	20	
AR151450	CLEARING AND GRUBBING	ACRE	4	
AR156530	TEMPORARY SEEDING	ACRE	4	
AR156531	EROSION CONTROL BLANKET	SY	6,750	
AR161900	REMOVE CLASS C FENCE	LF	7,200	
AR161910	REMOVE CLASS C GATE	EACH	3	
AR162216	CLASS E MANUAL SLIDE GATE - 16'	EACH	4	
AR162224	CLASS E MANUAL SLIDE GATE - 24'	EACH	5	
AR162524	DRIVEWAY GATE, CL E 24	EACH	1	
AR162900	REMOVE CLASS E FENCE	LF	4,600	
AR162910	REMOVE CLASS E GATE	EACH	4	
AR163000	TEMPORARY CONSTRUCTION FENCE	LF	1,000	
AR800033	CL & GRUB. TREES 0.5'-2.5' BUTT	EACH	20	
AR800034	CL & GRUB. TREES 2.5'-5' BUTT DI	EACH	10	
AR800153	CONCRETE WASHOUT	LS	1	
AR800157	CLASS E FENCE 10' W/ 2' BURY	LF	17,300	
AR800173	WILDLIFE DETERRENT BARRIER - PAD	SF	680	
AR901510	SEEDING	ACRE	5.5	
AR908514	LIGHT-DUTY HYDRAULIC MULCH	ACRE	4	

ILLINOIS PROJECT: IKK- 4401
S.B.G. PROJECT: 3-17-SBGP-XX



JUNE 3, 2016

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J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS
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THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

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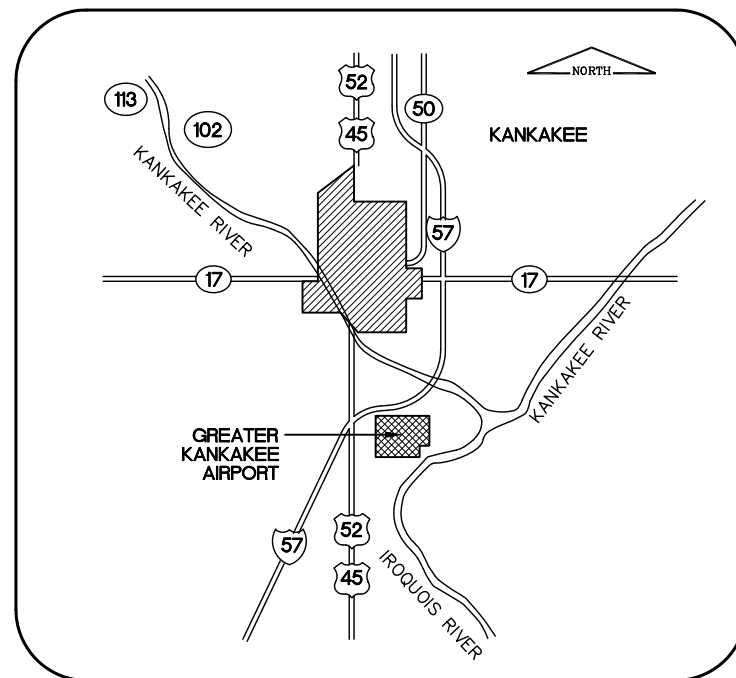
SUBMITTED BY *Antonio R. Marin*
ANTONIO R. MARIN, P.E.

DATE *June 3, 2016*

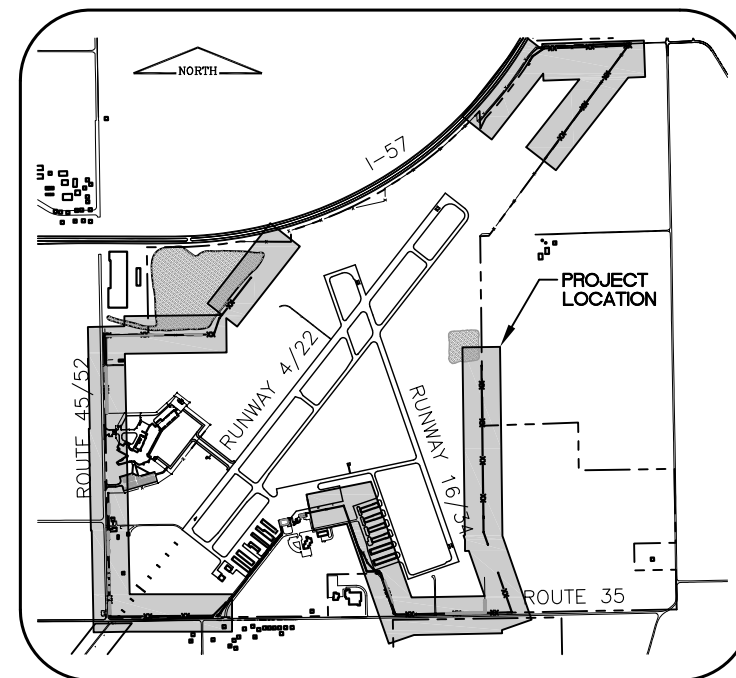
KANKAKEE VALLEY AIRPORT AUTHORITY

APPROVED *Victoria Ruble*
VICTORIA RUBLE AIRPORT MANAGER

DATE *May 31* 2016



SITE PLAN



LOCATION MAP

DESIGN INFORMATION

DESIGN AIRCRAFT APPROACH CATEGORY D
DESIGN AIRCRAFT GROUP II

KANKAKEE VALLEY AIRPORT AUTHORITY
GREATER KANKAKEE AIRPORT

SECTION: 21 COUNTY: KANKAKEE
RANGE: R 12 E TOWNSHIP: OTTO
TOWNSHIP: T 30 N

UNICOM RADIO FREQUENCY - 123.0

REF: DWG: IMAGE FILES: KAAV logo from web.qif LAYOUT: Layout1 FILE: K:\Kankakee\14075-05_Fencing\PH1\Draw\Sheets\02_Site_Planning

IL CONTRACT: **KA046**
 IL LETTING ITEM: **11A**
 IL PROJECT: **IKK-4401**
 S.B.G. PROJECT: **3-17-SPGP-XX**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**KANKAKEE VALLEY AIRPORT AUTHORITY
 KANKAKEE, ILLINOIS
 PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1
 SITE PLAN**

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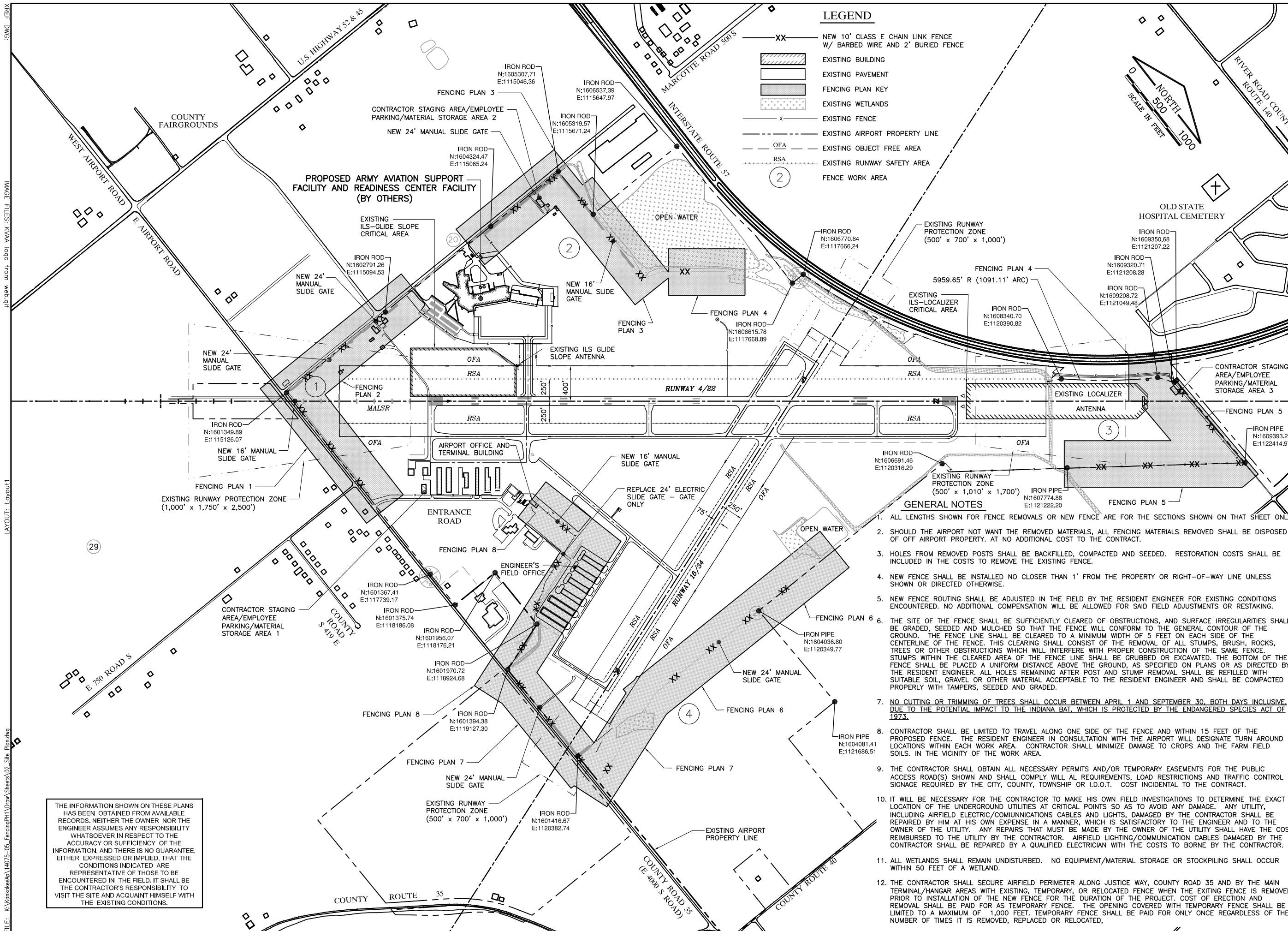
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DESIGN BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	ARM
DATE:	6/3/2016
JOB No:	14075-05

FINAL
 SHEET 2 OF 18 SHEETS

LEGEND

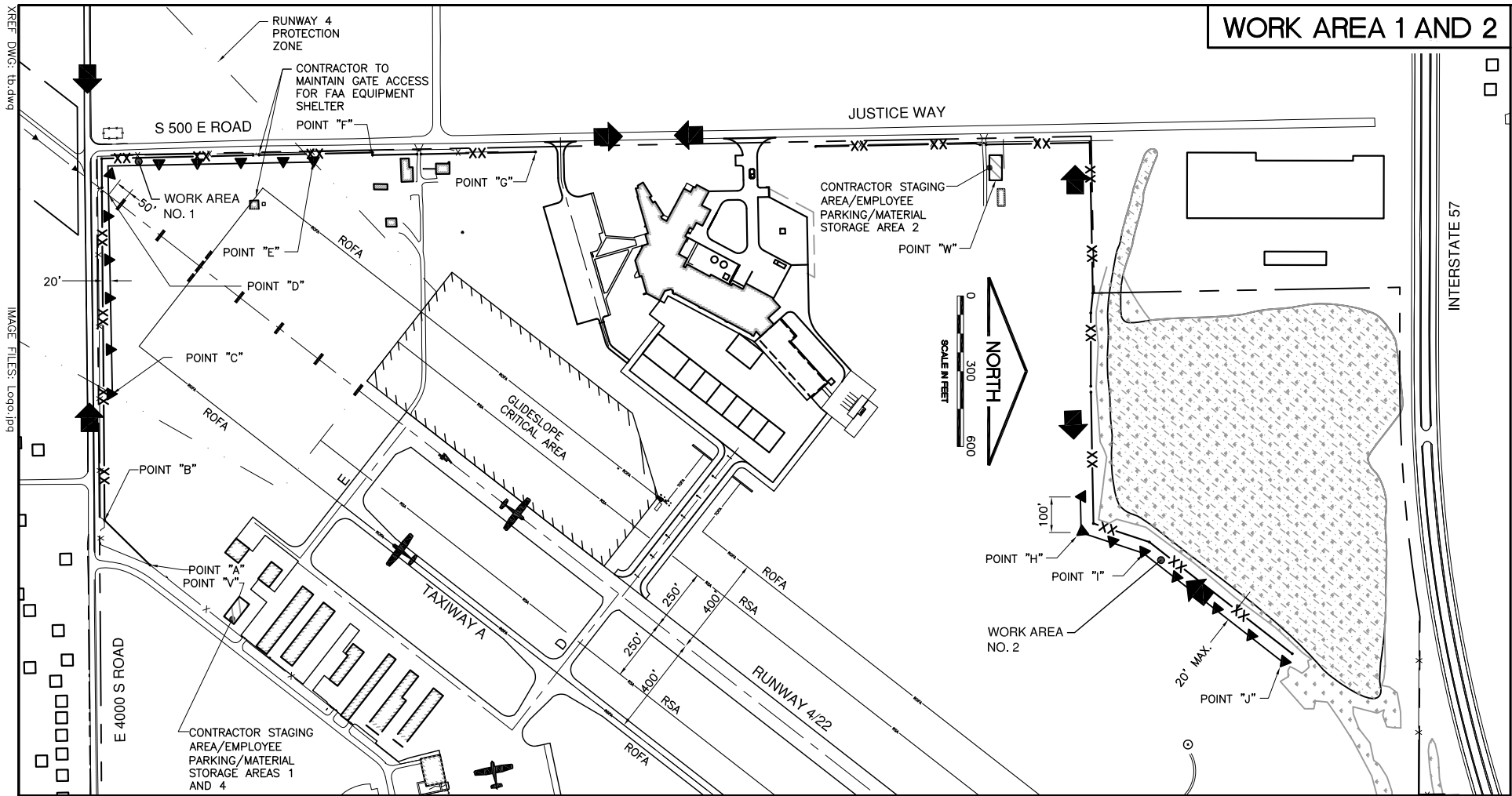
- XX NEW 10' CLASS E CHAIN LINK FENCE W/ BARBED WIRE AND 2' BURIED FENCE
- [Hatched Box] EXISTING BUILDING
- [White Box] EXISTING PAVEMENT
- [Dotted Box] FENCING PLAN KEY
- [Stippled Box] EXISTING WETLANDS
- x EXISTING FENCE
- - - EXISTING AIRPORT PROPERTY LINE
- - - OFA EXISTING OBJECT FREE AREA
- - - RSA EXISTING RUNWAY SAFETY AREA
- (2) FENCE WORK AREA



GENERAL NOTES

1. ALL LENGTHS SHOWN FOR FENCE REMOVALS OR NEW FENCE ARE FOR THE SECTIONS SHOWN ON THAT SHEET ONLY.
2. SHOULD THE AIRPORT NOT WANT THE REMOVED MATERIALS, ALL FENCING MATERIALS REMOVED SHALL BE DISPOSED OF OFF AIRPORT PROPERTY. AT NO ADDITIONAL COST TO THE CONTRACT.
3. HOLES FROM REMOVED POSTS SHALL BE BACKFILLED, COMPACTED AND SEEDED. RESTORATION COSTS SHALL BE INCLUDED IN THE COSTS TO REMOVE THE EXISTING FENCE.
4. NEW FENCE SHALL BE INSTALLED NO CLOSER THAN 1' FROM THE PROPERTY OR RIGHT-OF-WAY LINE UNLESS SHOWN OR DIRECTED OTHERWISE.
5. NEW FENCE ROUTING SHALL BE ADJUSTED IN THE FIELD BY THE RESIDENT ENGINEER FOR EXISTING CONDITIONS ENCOUNTERED. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR SAID FIELD ADJUSTMENTS OR RESTAKING.
6. THE SITE OF THE FENCE SHALL BE SUFFICIENTLY CLEARED OF OBSTRUCTIONS, AND SURFACE IRREGULARITIES SHALL BE GRADED, SEEDED AND MULCHED SO THAT THE FENCE WILL CONFORM TO THE GENERAL CONTOUR OF THE GROUND. THE FENCE LINE SHALL BE CLEARED TO A MINIMUM WIDTH OF 5 FEET ON EACH SIDE OF THE CENTERLINE OF THE FENCE. THIS CLEARING SHALL CONSIST OF THE REMOVAL OF ALL STUMPS, BRUSH, ROCKS, TREES OR OTHER OBSTRUCTIONS WHICH WILL INTERFERE WITH PROPER CONSTRUCTION OF THE SAME FENCE. STUMPS WITHIN THE CLEARED AREA OF THE FENCE LINE SHALL BE GRUBBED OR EXCAVATED. THE BOTTOM OF THE FENCE SHALL BE PLACED A UNIFORM DISTANCE ABOVE THE GROUND, AS SPECIFIED ON PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. ALL HOLES REMAINING AFTER POST AND STUMP REMOVAL SHALL BE REFILLED WITH SUITABLE SOIL, GRAVEL OR OTHER MATERIAL ACCEPTABLE TO THE RESIDENT ENGINEER AND SHALL BE COMPACTED PROPERLY WITH TAMPERS, SEEDED AND GRADED.
7. NO CUTTING OR TRIMMING OF TREES SHALL OCCUR BETWEEN APRIL 1 AND SEPTEMBER 30, BOTH DAYS INCLUSIVE, DUE TO THE POTENTIAL IMPACT TO THE INDIANA BAT, WHICH IS PROTECTED BY THE ENDANGERED SPECIES ACT OF 1973.
8. CONTRACTOR SHALL BE LIMITED TO TRAVEL ALONG ONE SIDE OF THE FENCE AND WITHIN 15 FEET OF THE PROPOSED FENCE. THE RESIDENT ENGINEER IN CONSULTATION WITH THE AIRPORT WILL DESIGNATE TURN AROUND LOCATIONS WITHIN EACH WORK AREA. CONTRACTOR SHALL MINIMIZE DAMAGE TO CROPS AND THE FARM FIELD SOILS. IN THE VICINITY OF THE WORK AREA.
9. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND/OR TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS AND TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP OR I.D.O.T. COST INCIDENTAL TO THE CONTRACT.
10. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATIONS TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRIC/COMMUNICATIONS CABLES AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER, WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE CONTRACTOR REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING/COMMUNICATION CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BORNE BY THE CONTRACTOR.
11. ALL WETLANDS SHALL REMAIN UNDISTURBED. NO EQUIPMENT/MATERIAL STORAGE OR STOCKPILING SHALL OCCUR WITHIN 50 FEET OF A WETLAND.
12. THE CONTRACTOR SHALL SECURE AIRFIELD PERIMETER ALONG JUSTICE WAY, COUNTY ROAD 35 AND BY THE MAIN TERMINAL/HANGAR AREAS WITH EXISTING, TEMPORARY, OR RELOCATED FENCE WHEN THE EXISTING FENCE IS REMOVED PRIOR TO INSTALLATION OF THE NEW FENCE FOR THE DURATION OF THE PROJECT. COST OF ERECTION AND REMOVAL SHALL BE PAID FOR AS TEMPORARY FENCE. THE OPENING COVERED WITH TEMPORARY FENCE SHALL BE LIMITED TO A MAXIMUM OF 1,000 FEET. TEMPORARY FENCE SHALL BE PAID FOR ONLY ONCE REGARDLESS OF THE NUMBER OF TIMES IT IS REMOVED, REPLACED OR RELOCATED.

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION, AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

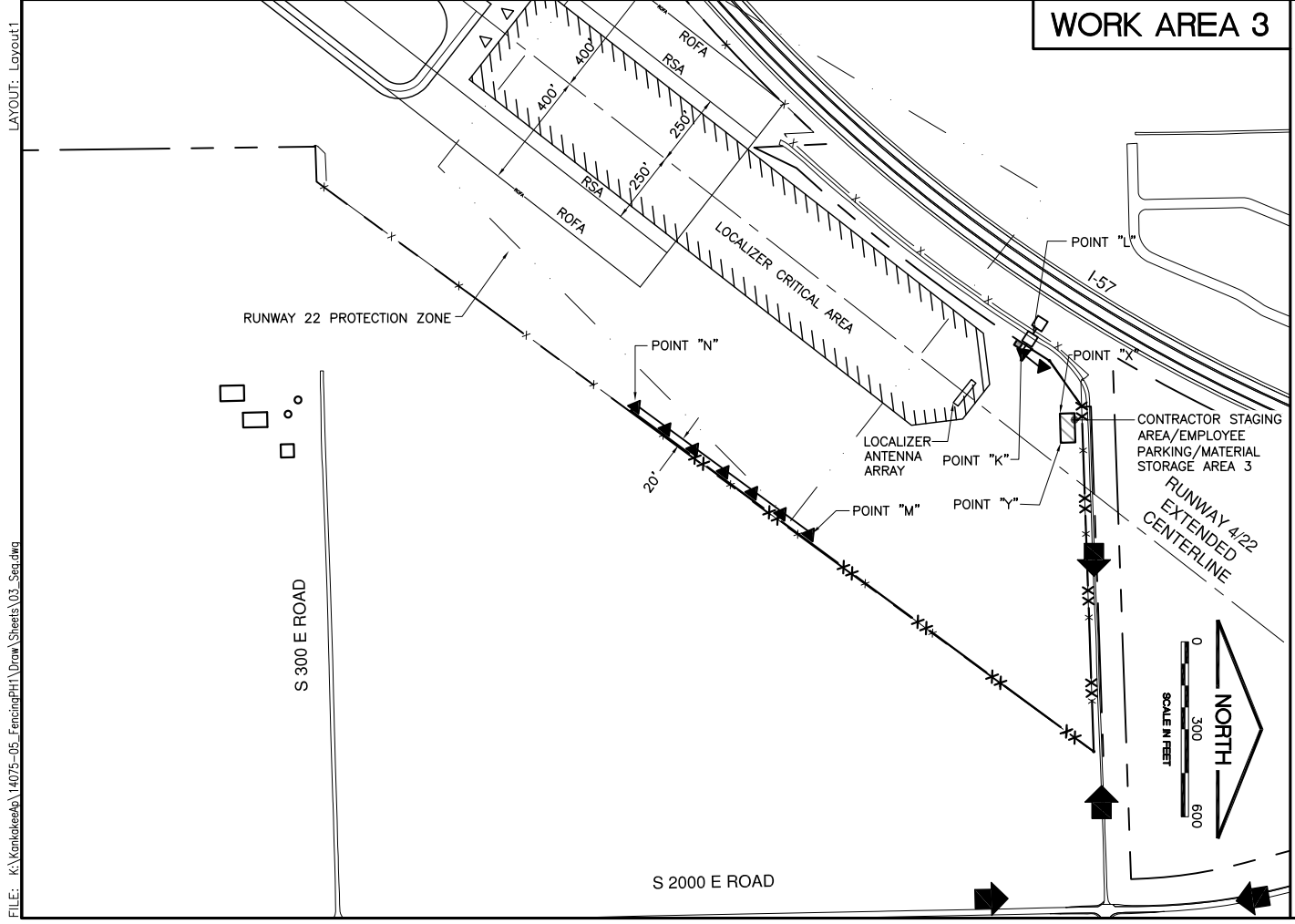


LEGEND

- XX — NEW 10' CLASS E FENCE WITH BARBED WIRE AND 2' BURIED FABRIC
- ➔ — CONTRACTOR'S HAUL ROUTE
- ▲ — TEMPORARY STAKE LATHE WITH "CAUTION" WARNING TAPE
- — IDOT TYPE 2 BARRICADE OR DRUM W/ RED LIGHT
- — IDOT TYPE 2 BARRICADE OR DRUM W/ RED LIGHT (PHASE 4B ONLY)
- RSA — RUNWAY SAFETY AREA (RSA)
ACTIVE RUNWAY 4/22 500' FROM CENTERLINE
ACTIVE RUNWAY 16/34 150' FROM CENTERLINE
- ROFA — RUNWAY OBJECT FREE AREA (ROFA)
ACTIVE RUNWAY 4/22 400' FROM CENTERLINE
ACTIVE RUNWAY 16/34 250' FROM CENTERLINE
- ROFZ — RUNWAY OBSTACLE FREE ZONE (ROFZ)
ACTIVE RUNWAY 4/22 400' FROM CENTERLINE
ACTIVE RUNWAY 16/34 200' FROM CENTERLINE
- ✈ — AIRCRAFT MOVEMENT AREA
- X — CLOSED RUNWAY MARKER (PHASE 4B ONLY)

SUGGESTED SEQUENCE OF CONSTRUCTION

- NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER 14 DAYS PRIOR TO START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
- WORK AREAS 1 THRU 4.
 - PLACE BARRICADES, LATHE AND TAPE AS SHOWN.
 - LOCATE ALL UTILITIES.
 - LOCATE APPROPRIATE PROPERTY CORNERS AND STAKE PROPOSED FENCE LAYOUT.
 - CLEAR FENCE LINE AS NOTED ON PLANS OR AS DIRECTED BY THE ENGINEER.
 - AUGER POST HOLES AND INSTALL POSTS.
 - INSTALL TOP RAIL AND TENSION WIRE.
 - PLACE FABRIC AND SIGNS.
 - INSTALL VEHICLE GATE AND WILDLIFE DETERRENT PAD.
 - SEED AND MULCH AREAS SHOWN ON THE PLANS.
 - REMOVE MISCELLANEOUS DEBRIS, LATHE, TAPE AND BARRICADES FROM WORK AREA.
- RESTORE STAGING AREAS.

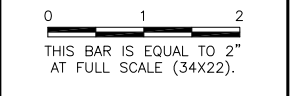


WORK AREA	ALLOWABLE WORK PERIODS	OPERATIONAL STATUS/RESTRICTION
AREA 1	NO RESTRICTIONS	ALL RUNWAYS/TAXIWAYS OPEN
AREA 2	NO RESTRICTIONS	ALL RUNWAYS/TAXIWAYS OPEN
AREA 3	NO RESTRICTIONS	ALL RUNWAYS/TAXIWAYS OPEN
AREA 4A	NO RESTRICTIONS	ALL RUNWAYS/TAXIWAYS OPEN
AREA 4B	CONTRACTOR ALLOWED TO CLOSE RUNWAY 16/34 FOR ONE (1) DAY. ALL WORK WITHIN THE DESIGNATED AREA SHALL BE COMPLETED. BARRICADES AND RUNWAY CLOSURE MARKERS SHALL BE PLACED AS NOTED ON WORK AREA PLAN VIEW OR AS DIRECTED BY THE ENGINEER AND/OR AIRPORT.	RUNWAY 4/22 OPEN RUNWAY 16/34 CLOSED TAXIWAY B1 CLOSED TAXIWAY H AND B PARTIALLY CLOSED ALL OTHER TAXIWAYS OPEN
	CONTRACTOR ALLOWED TO USE AREA 4B AS HAUL ROUTE BETWEEN WORK AREA 4A SECTIONS, WHEN IN RADIO CONTACT WITH ACTIVE AIRFIELD. SEE NOTE 32 ON SEQUENCE OF CONSTRUCTION NOTES SHEET. NO AIRFIELD BARRICADES/RUNWAY CLOSED MARKERS ARE REQUIRED TO BE PLACED.	ALL RUNWAYS/TAXIWAYS OPEN

IL CONTRACT: KA046
 IL LETTING ITEM: 11A
 IL PROJECT: IKK-4401
 S.B.G. PROJECT: 3-17-SPGP-XX

SURVEY BOOK # BOOK #

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**KANKAKEE VALLEY AIRPORT AUTHORITY
 KANKAKEE, ILLINOIS
 PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1**

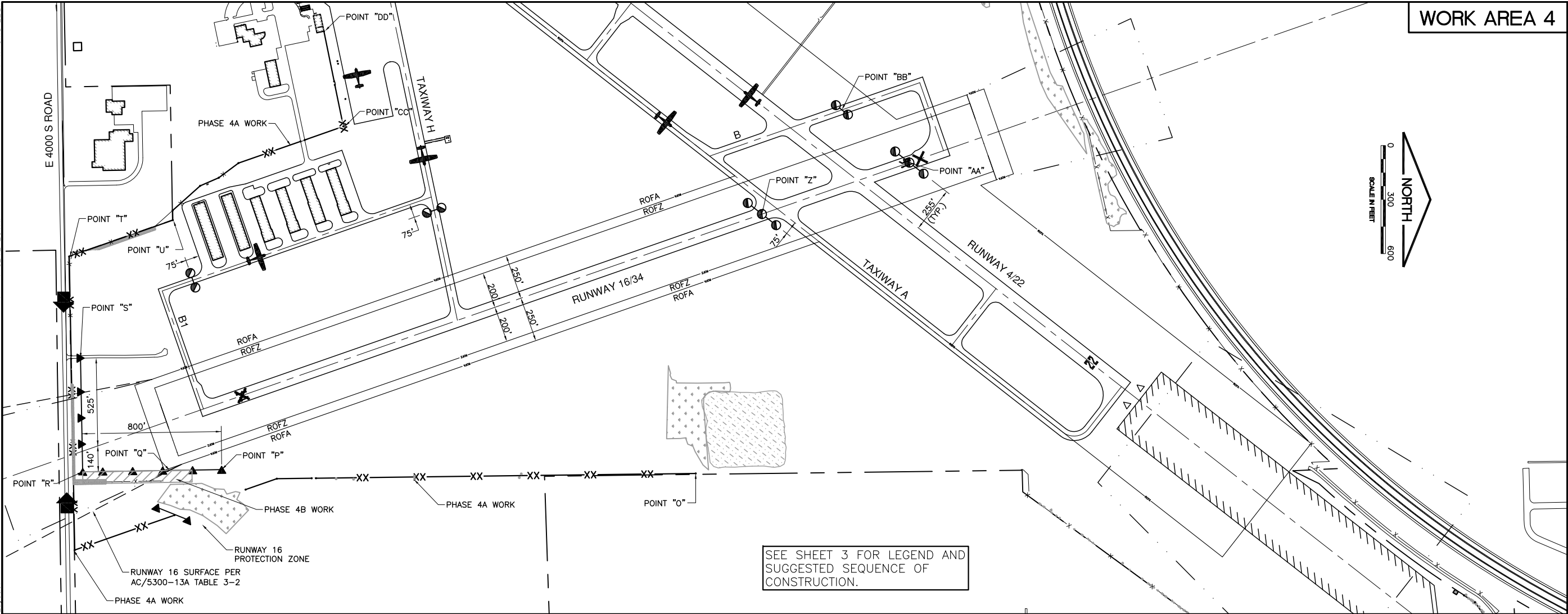
SEQUENCE OF CONSTRUCTION - 1

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SEE SHEET 3 FOR LEGEND AND SUGGESTED SEQUENCE OF CONSTRUCTION.



WORK AREA 4

IL CONTRACT: KA046
 IL LETTING ITEM: 11A
 IL PROJECT: IKK-4401
 S.B.G. PROJECT: 3-17-SPGP-XX

SURVEY BOOK # BOOK #

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NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**KANKAKEE VALLEY AIRPORT AUTHORITY
 KANKAKEE, ILLINOIS
 PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1**

SEQUENCE OF CONSTRUCTION - 2

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GENERAL NOTES

- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2F (LATEST EDITION) **OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION**.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
- THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
- PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES WITH ONE ORANGE FLAG (20" x 20") ON EACH BARRICADE SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A STEADY BURN OR FLASHING RED LIGHT. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL SUPPLY AND USE AS DIRECTED BY THE AIRPORT, REFLECTIVE LOW PROFILE TYPE BARRICADES. ALL BARRICADES SHALL BE PLACED OUTSIDE OF ACTIVE SAFETY AREAS.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER FOURTEEN (14) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.
- IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE A READY MIX CONCRETE TRUCK, WHICH HAS A MAXIMUM HEIGHT OF 15 FEET.
- IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.
- THE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
- MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.

BARRICADE NOTES

- FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OR SOLAR POWER OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
- FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
- BARRICADES TO BE PLACED WITH A MAXIMUM OF 10' SPACING CENTER TO CENTER BETWEEN RED LIGHTS ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
- FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.

DESIGN AIRCRAFT APPROACH CATEGORY: B AND D

DESIGN AIRPORT GROUP: II
 RUNWAY 4/22 SAFETY AREA WIDTH: 500'
 RUNWAY 16/34 SAFETY AREA WIDTH: 150'
 TAXIWAY CENTERLINE TO OBJECT SEPARATION: 65.5'
 TAXILANE CENTERLINE TO OBJECT SEPARATION: 57.5'

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS.

MAXIMUM ANTICIPATED HEIGHT OF EQUIPMENT - 15'

CLOSEST CONSTRUCTION POINT TABLE					
POINT	NEAREST ACTIVE RUNWAY	LATITUDE	LONGITUDE	SITE ELEVATION	
A	RUNWAY 4/22	N41°03'47.04"	W87°51'10.20"	619.2	
B	RUNWAY 4/22	N41°03'45.25"	W87°51'12.54"	619.6	
C	RUNWAY 4/22	N41°03'45.44"	W87°51'19.22"	620.9	
D	RUNWAY 4/22	N41°03'45.42"	W87°51'29.52"	622.0	
E	RUNWAY 4/22	N41°03'53.43"	W87°51'31.27"	621.9	
F	RUNWAY 4/22	N41°03'55.88"	W87°51'31.33"	621.6	
G	RUNWAY 4/22	N41°04'02.29"	W87°51'31.51"	619.0	
H	RUNWAY 4/22	N41°04'23.97"	W87°51'11.90"	618.4	
I	RUNWAY 4/22	N41°04'26.26"	W87°51'10.37"	618.4	
J	RUNWAY 4/22	N41°04'31.83"	W87°51'05.05"	614.7	
K	RUNWAY 4/22	N41°05'01.40"	W87°50'13.92"	618.5	
L	RUNWAY 4/22	N41°05'01.81"	W87°50'14.75"	618.5	
M	RUNWAY 4/22	N41°04'54.20"	W87°50'05.82"	612.5	
N	RUNWAY 4/22	N41°04'48.31"	W87°50'11.63"	609.1	
O	RUNWAY 16/34	N41°04'19.61"	W87°50'23.49"	610.0	
P	RUNWAY 16/34	N41°03'53.58"	W87°50'23.17"	612.0	
Q	RUNWAY 16/34	N41°03'49.97"	W87°50'23.14"	611.6	
R	RUNWAY 16/34	N41°03'45.63"	W87°50'23.10"	611.8	
S	RUNWAY 16/34	N41°03'45.54"	W87°50'31.82"	616.0	
T	RUNWAY 16/34	N41°03'45.27"	W87°50'39.22"	619.0	
U	RUNWAY 16/34	N41°03'50.97"	W87°50'41.81"	616.1	
V	RUNWAY 4/22	N41°03'50.86"	W87°51'08.40"	619.4	
W	RUNWAY 4/22	N41°04'20.39"	W87°51'29.97"	617.1	
X	RUNWAY 4/22	N41°05'02.70"	W87°50'11.18"	617.5	
Y	RUNWAY 4/22	N41°05'02.73"	W87°50'09.88"	617.5	
Z	RUNWAY 4/22	N41°04'23.32"	W87°50'42.01"	621.0	
AA	RUNWAY 4/22	N41°04'31.43"	W87°50'45.69"	622.0	
BB	RUNWAY 4/22	N41°04'27.74"	W87°50'49.54"	618.0	
CC	RUNWAY 16/34	N41°04'00.46"	W87°50'48.56"	621.0	
DD	RUNWAY 4/22	N41°03'59.38"	W87°50'55.47"	620.0	

CONTRACTOR CROSSING RUNWAY/TAXIWAY/TAXILANE/APRON AIR OPERATIONS AREA (A.O.A.)

- LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO NEXT EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.
- COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT AT A MINIMUM COORDINATION MEETINGS EVERY TWO WEEKS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE RESIDENT ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.
- CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS / TAXILANES, THE RUNWAY SAFETY AREA (RSA) OF ACTIVE RUNWAYS AND APPROACH SURFACES AS SHOWN ON THE PLANS.
- CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 IMAGINARY SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL. UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER, ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE. ANY NECESSARY TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAVE BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTORS WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER. ANY DEFICIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.
- THE CONTRACTOR IS REQUIRED TO PROVIDE A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD), WHICH DETAILS HOW THEY WILL COMPLY WITH THE SEQUENCE OF CONSTRUCTION PLANS, NOTES AND DETAILS. IT WILL ALSO PROVIDE SUPPLEMENTAL INFORMATION THAT COULD NOT BE INCLUDED IN THE SEQUENCE OF CONSTRUCTION PRIOR TO THE CONTRACT AWARD. THE SPCD SHALL BE IN CONFORMANCE WITH THE CURRENT EDITION OF FAA ADVISORY CIRCULAR 150/5370-2. THE SPCD SHOULD BE SUBMITTED FOR REVIEW NO LATER THAN THE PRE-CONSTRUCTION MEETING. THE NOTICE TO PROCEED WILL NOT BE ISSUED UNTIL THE SPCD IS RECEIVED FOR REVIEW.**
- ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS OR RUNWAY APPROACH FOR ACCESS TO AND FROM THE WORK ZONE, A CROSSING GUARD OR ESCORT IN RADIO CONTACT WITH AIR TRAFFIC SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURRENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.
- ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE MEN AND EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

IL. CONTRACT: **KA046**
 IL. LETTING ITEM: **11A**
 IL. PROJECT: **IKK-4401**
 S.B.G. PROJECT: **3-17-SPGP-XX**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

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 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**KANKAKEE VALLEY AIRPORT AUTHORITY
 KANKAKEE, ILLINOIS
 PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1**

SEQUENCE OF CONSTRUCTION NOTES

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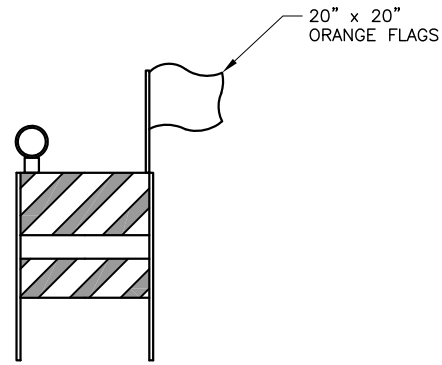
KANKAKEE VALLEY AIRPORT AUTHORITY

DESIGN BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	ARM
DATE:	6/3/2016
JOB No:	14075-05

FINAL

SHEET 5 OF 18 SHEETS

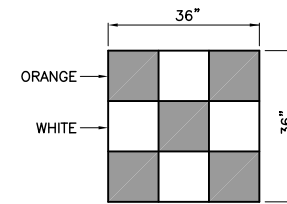
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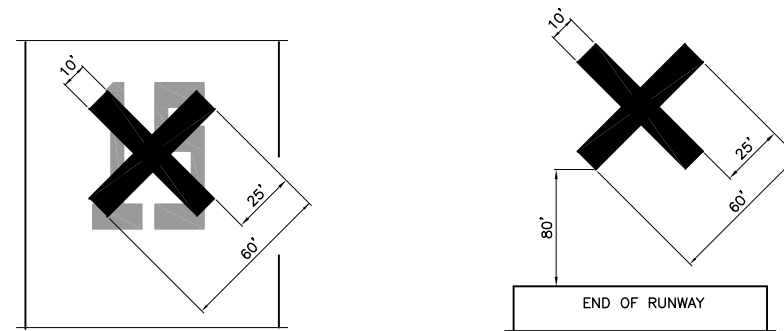
FLASHER BARRICADE DETAIL
NOT TO SCALE

NOTES

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES. ALTERNATE FLASHER LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT TEN (10') INTERVALS.
5. ONE 20"x20" ORANGE FLAG TO BE INSTALLED ON EACH BARRICADE.
6. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING BARRICADES AT ALL TIMES TO THE SATISFACTION OF THE AIRPORT MANAGER.



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
NOT TO SCALE

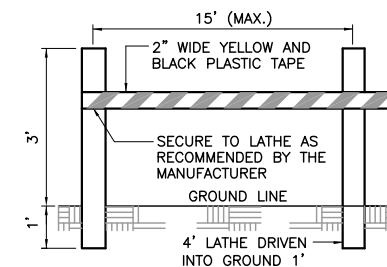


ON PAVEMENT TEMPORARY CLOSED RUNWAY MARKER DETAIL
NOT TO SCALE

OFF PAVEMENT TEMPORARY CLOSED RUNWAY MARKER DETAIL
NOT TO SCALE

CLOSED RUNWAY MARKER DETAIL NOTES

1. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
2. MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
4. MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
5. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.



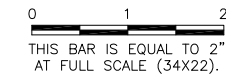
LATHING AND WARNING TAPE DETAIL
NOT TO SCALE

MATERIALS ARE TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION. COST OF MATERIALS, INSTALLATION, RELOCATION AND MAINTENANCE OF LATHING AND WARNING TAPE SHALL BE INCIDENTAL TO THE CONTRACT.

IL. CONTRACT: **KA046**
 IL. LETTING ITEM: **11A**
 IL. PROJECT: **IKK-4401**
 S.B.G. PROJECT: **3-17-SPGP-XX**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1

SEQUENCE OF CONSTRUCTION
DETAILS

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KANKAKEE VALLEY AIRPORT AUTHORITY

DESIGN BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	ARM
DATE:	6/3/2016
JOB No:	14075-05

FINAL

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STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF CONSTRUCTING A NEW SECURITY FENCE WITH BURIED WILDLIFE FENCING AT KANKAKEE AIRPORT.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

1. INSTALL AND MAINTAIN TEMPORARY EROSION CONTROL MEASURES.
2. REMOVE EXISTING FENCE.
3. INSTALL THE NEW FENCE POSTS AND DETERRENT FENCE EXCAVATION.
4. INSTALL NEW FENCE IMPROVEMENTS.
5. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
6. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND BLANKET.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 9.8 ACRES OF WHICH 5.5 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE UNNAMED TRIBUTARIES THAT OUTLET INTO THE KANKAKEE RIVER.

SEDIMENTATION AND EROSION CONTROL NOTES

THE SOIL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE INSPECTED WEEKLY AND AFTER 1/2 INCH OF RAIN OR MORE BY THE RESIDENT ENGINEER.

ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL, UNLESS OTHERWISE STATED.

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS POSSIBLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
 - A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
 - B. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

5. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.
6. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

CONTRACTORS

1. THE STORM WATER POLLUTION PREVENTION PLAN MUST CLEARLY IDENTIFY FOR EACH MEASURE IDENTIFIED IN THE PLAN, THE CONTRACTOR(S) OR SUBCONTRACTOR(S) THAT WILL IMPLEMENT THE MEASURE. ALL CONTRACTORS AND SUBCONTRACTORS IDENTIFIED IN THE PLAN MUST SIGN A COPY OF THE CERTIFICATION STATEMENT IN PARAGRAPH 2 BELOW IN ACCORDANCE WITH PART VI.G (SIGNATORY REQUIREMENTS) OF THIS PERMIT. ALL CERTIFICATIONS MUST BE INCLUDED IN THE STORM WATER POLLUTION PREVENTION PLAN EXCEPT FOR OWNERS THAT ARE ACTING AS CONTRACTOR.
2. CERTIFICATION STATEMENT. ALL CONTRACTORS AND SUBCONTRACTORS IDENTIFIED IN A STORM WATER POLLUTION PREVENTION PLAN IN ACCORDANCE WITH PARAGRAPH 1 ABOVE SHALL SIGN A COPY OF THE FOLLOWING CERTIFICATION STATEMENT BEFORE CONDUCTING ANY PROFESSIONAL SERVICE AT THE SITE IDENTIFIED IN THE STORM WATER POLLUTION PREVENTION PLAN:

"I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION."

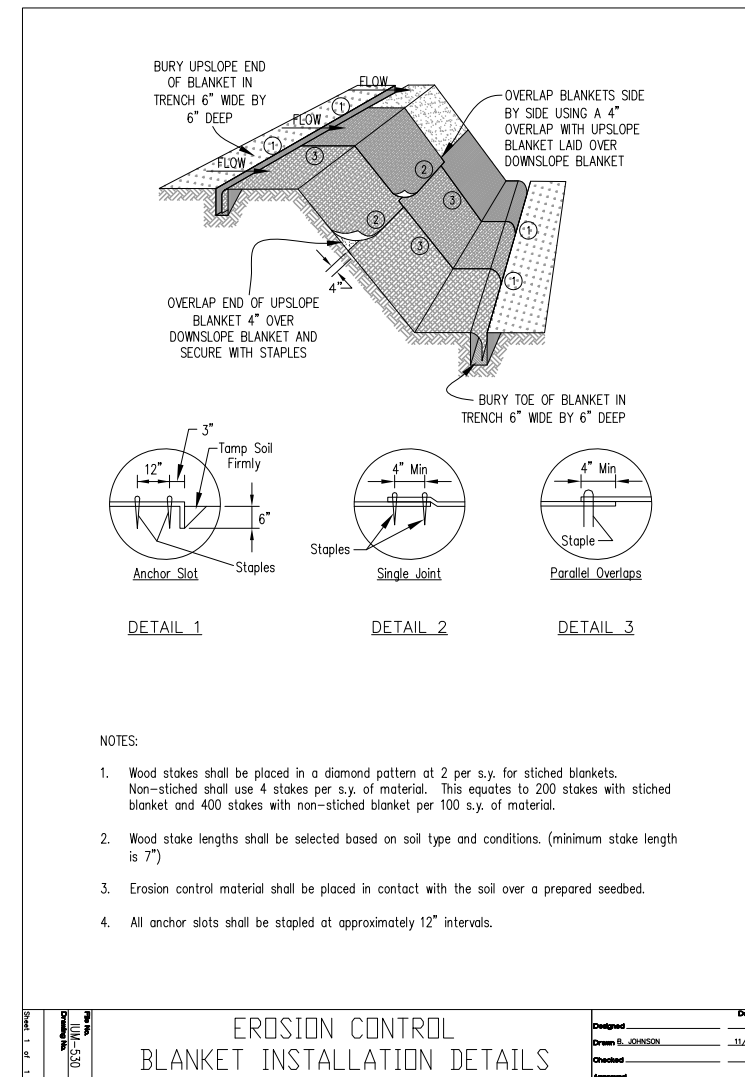
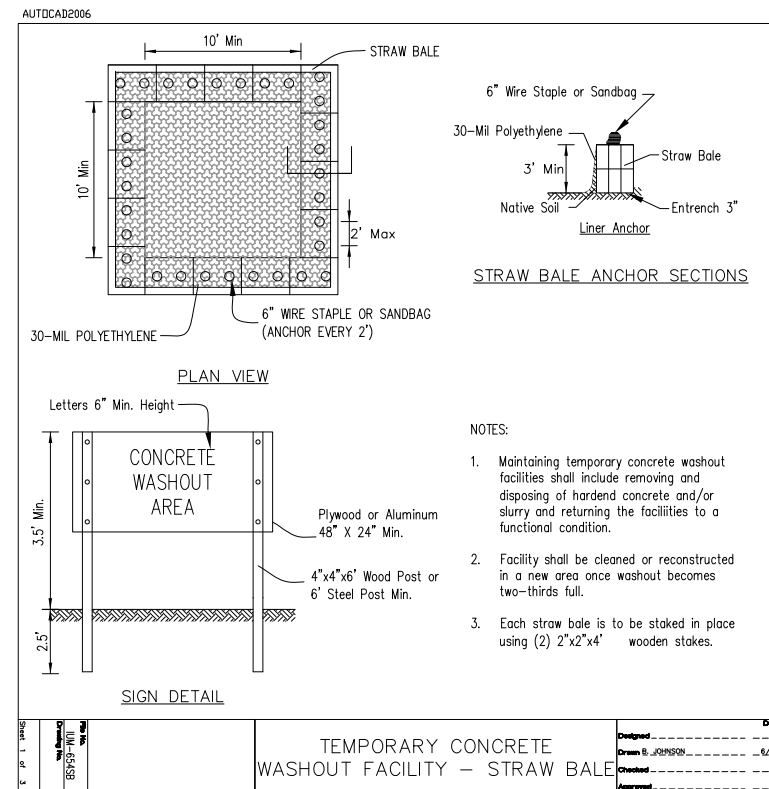
THE CERTIFICATION MUST INCLUDE THE NAME AND TITLE OF THE PERSON PROVIDING THE SIGNATURE IN ACCORDANCE WITH PART VI.G OF THIS PERMIT: THE NAME, ADDRESS AND TELEPHONE NUMBER OF THE CONTRACTING FIRM; THE ADDRESS (OR OTHER IDENTIFYING DESCRIPTION) OF THE SITE; AND THE DATE THE CERTIFICATION IS MADE.

CONTRACTOR CERTIFICATION

"I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION."

GENERAL CONTRACTOR

SIGNATURE _____ TITLE _____ DATE _____
 COMPANY _____



IL. CONTRACT: **KA046**
 IL. LETTING ITEM: **11A**
 IL. PROJECT: **IKK-4401**
 S.B.G. PROJECT: **3-17-SPGP-XX**

SURVEY BOOK # BOOK #

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

KANKAKEE VALLEY AIRPORT AUTHORITY KANKAKEE, ILLINOIS PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1 STORMWATER POLLUTION PREVENTION PLAN NOTES AND DETAILS

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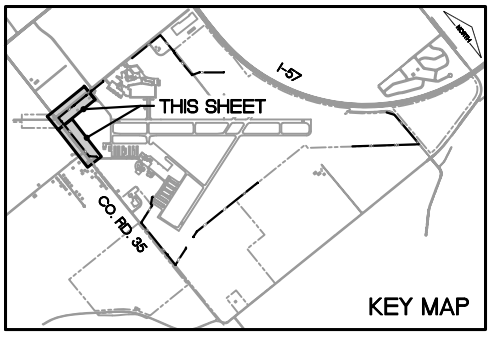
KANKAKEE VALLEY AIRPORT AUTHORITY

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CHECKED BY:	DKP
APPROVED BY:	ARM
DATE:	6/3/2016
JOB No:	14075-05

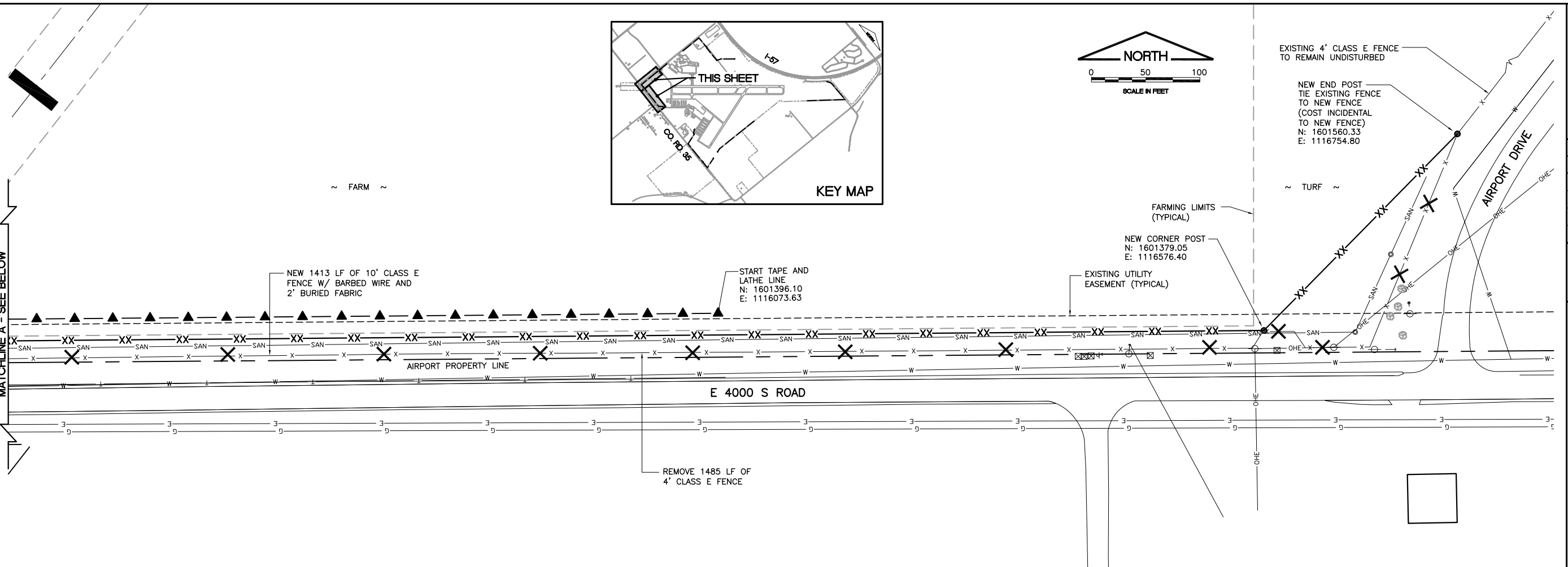
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SHEET 7 OF 18 SHEETS

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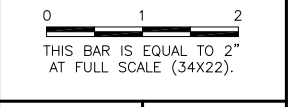
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 NEW END POST TIE EXISTING FENCE TO NEW FENCE (COST INCIDENTAL TO NEW FENCE)
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IL. CONTRACT: KA046
 IL. LETTING ITEM: 11A
 IL. PROJECT: IKK-4401
 S.B.G. PROJECT: 3-17-SPGP-XX

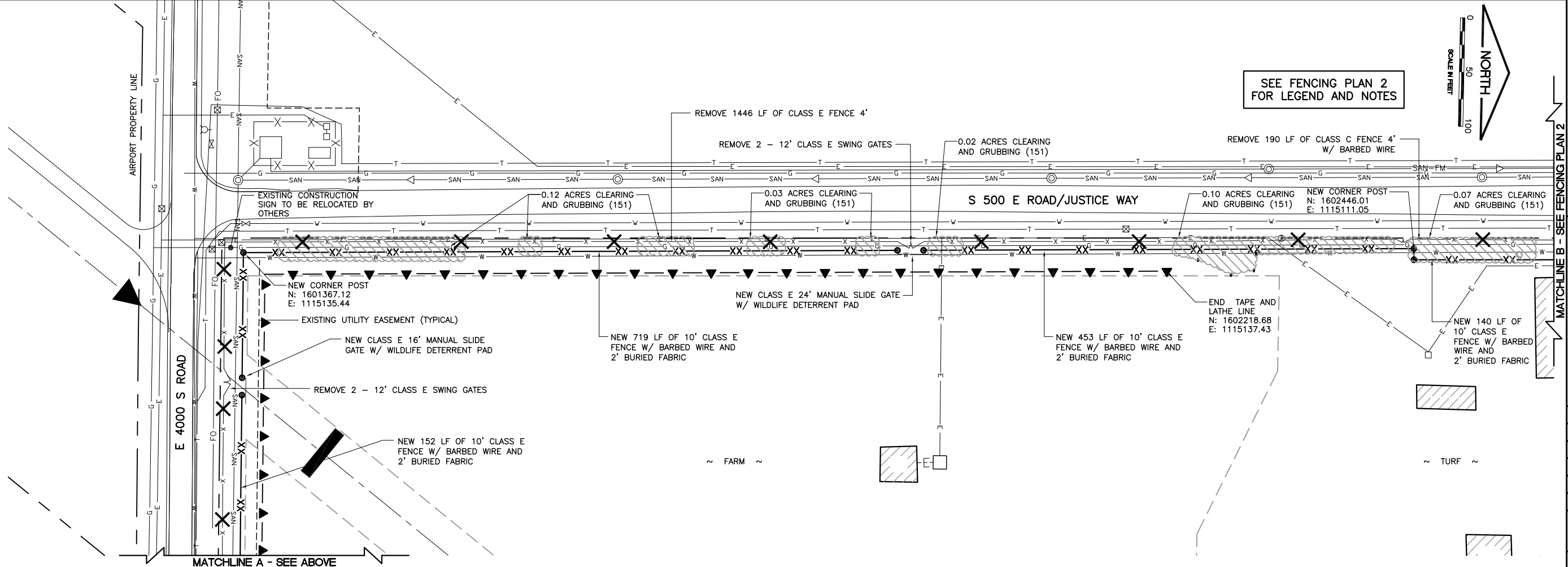
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**KANKAKEE VALLEY AIRPORT AUTHORITY
 KANKAKEE, ILLINOIS
 PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1**

FENCING PLAN 1



SEE FENCING PLAN 2 FOR LEGEND AND NOTES

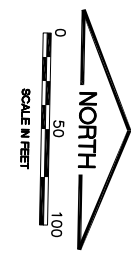
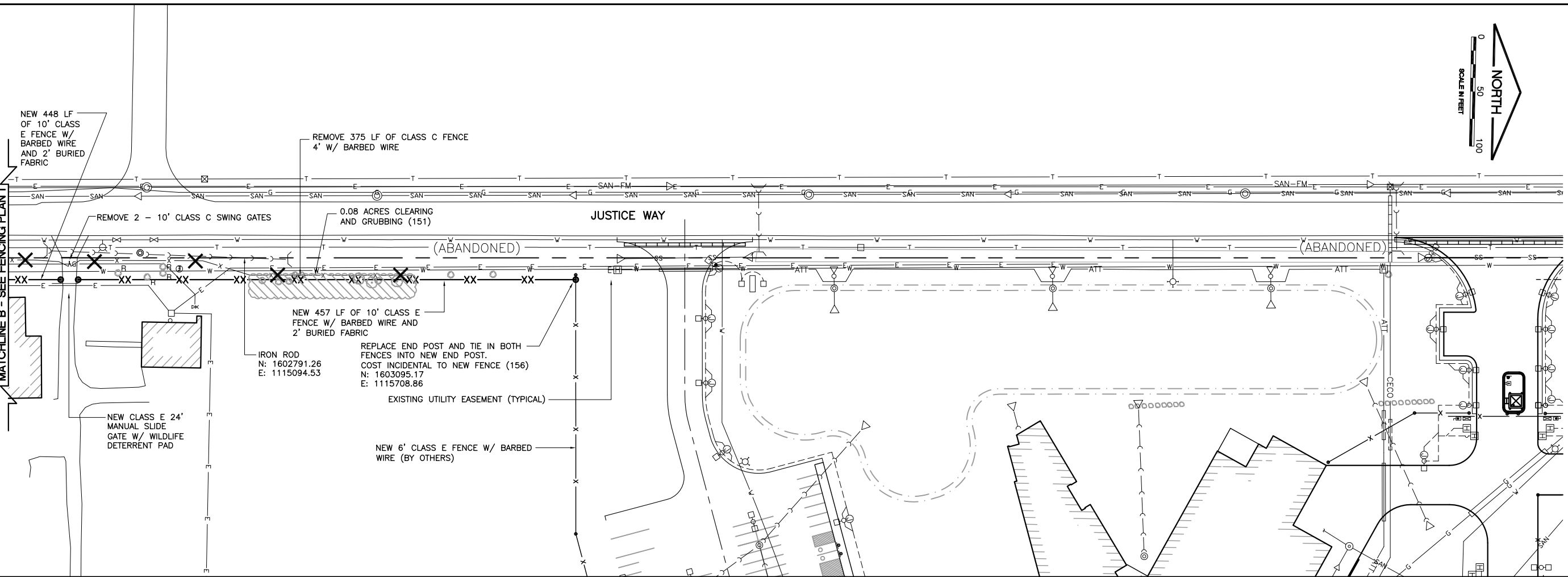
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SHEET	8 OF 18 SHEETS

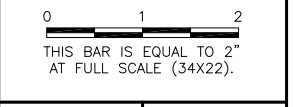
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IL. CONTRACT: **KA046**
 IL. LETTING ITEM: **11A**
 IL. PROJECT: **IKK-4401**
 S.B.G. PROJECT: **3-17-SPGP-XX**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1
FENCING PLAN 2

LEGEND

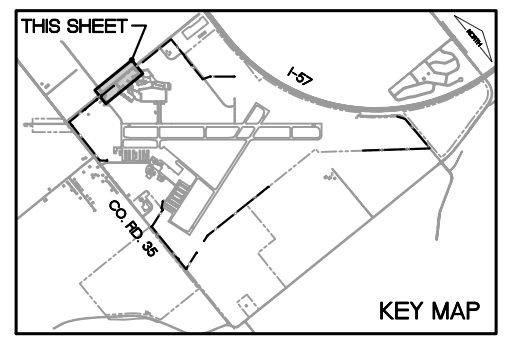
- XX — NEW CLASS E FENCE
- ▲ —▲ —▲ NEW "CAUTION" TAPE AND LATHE (SEE GENERAL NOTE 6)
- x — EXISTING FENCE
- - - - EXISTING AIRPORT PROPERTY LINE
- SS- EXISTING STORM SEWER
- ○ △ EXISTING STORM SEWER STRUCTURE
- SAN- EXISTING SANITARY SEWER
- E- EXISTING ELECTRIC
- W- EXISTING WATER MAIN
- ⊙ ⊖ EXISTING FIRE HYDRANT
- ⊗ EXISTING WATER VALVE
- G- EXISTING GAS MAIN
- T- EXISTING TELEPHONE LINE
- OHE- EXISTING OVERHEAD ELECTRIC
- ⊕ EXISTING UTILITY POLE
- FO- EXISTING FIBER OPTIC CABLE
- ⊠ EXISTING HANDHOLE
- - - - EXISTING EASEMENT
- X/R ITEM TO BE REMOVED
- + + + + + EXISTING WATERS OF THE UNITED STATES
- ↓ ↓ ↓ ↓ EXISTING WETLAND
- ▭ EXISTING TREE/BRUSH LINE
- ▨ EXISTING TREES AND BRUSH TO BE CLEARED AND GRUBBED (151)
- UTILITY POLE GUY TIE POINT
- ▭ EXISTING BUILDING

EROSION CONTROL NOTES

1. THE CONTRACTOR SHALL BE REQUIRED TO COMPLY WITH THE REQUIREMENTS OF THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) STORM WATER PERMIT FOR CONSTRUCTION SITE ACTIVITIES.
2. ALL PROJECT AREAS, INCLUDING STOCKPILES, ABANDONED HAUL ROADS AND STAGING AREAS, AS SHOWN ON THE PLANS, SHALL HAVE 4 INCHES OF TOPSOIL PLACED AND BE SEEDED AND MULCHED IN ACCORDANCE WITH THE SPECIFICATIONS. AREAS DISTURBED OUTSIDE THE PROJECT LIMITS WILL BE SEEDED AND MULCHED BY THE CONTRACTOR AT HIS COST AND RESTORED TO ORIGINAL CONDITIONS.
3. FOR DETAILS, SEE STORMWATER POLLUTION PREVENTION NOTES AND DETAILS SHEET.
4. FOR DETAILS, SEE STORMWATER POLLUTION PREVENTION NOTES AND DETAILS SHEET.
5. FOR EXACT LOCATIONS OF INLETS, SEE DRAINAGE PLAN SHEETS.
6. SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EACH 1/2" RAIN EVENT.
7. THE CONTRACTOR SHALL HAVE A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN ON THE SITE AT ALL TIMES.
8. ANY FIELD/DRAIN TILES THAT ARE DAMAGED IN ANY WAY SHALL BE IMMEDIATELY REPAIRED ACCORDING TO THE PLANS/SPECIFICATIONS AND SHALL BE PROTECTED FROM SEDIMENT-LADEN WATER.
9. PRIOR TO COMMENCING LAND DISTURBING ACTIVITIES IN AREAS OTHER THAN INDICATED ON THESE PLANS (INCLUDING BUT NOT LIMITED TO, ADDITIONAL PHASES OF DEVELOPMENT AND OFF-SITE BORROW OR WASTE AREAS) A SUPPLEMENTARY EROSION CONTROL PLAN SHALL BE SUBMITTED TO THE OWNER FOR REVIEW.

GENERAL NOTES

1. ALL LENGTHS SHOWN FOR FENCE REMOVALS OR NEW FENCE ARE FOR THE SECTIONS SHOWN ON THAT SHEET ONLY.
2. SHOULD THE AIRPORT NOT WANT THE REMOVED MATERIALS, ALL FENCING MATERIALS REMOVED SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
3. HOLES FROM REMOVED POSTS SHALL BE BACKFILLED, COMPACTED AND SEEDED. RESTORATION COSTS SHALL BE INCLUDED IN THE COSTS TO REMOVE THE EXISTING FENCE.
4. NEW FENCE SHALL BE INSTALLED NO CLOSER THAN 1' FROM THE PROPERTY LINE UNLESS OTHERWISE SHOWN OR DIRECTED BY THE RESIDENT ENGINEER AND AIRPORT.
5. NEW FENCE ROUTING MAY BE ADJUSTED IN THE FIELD BY RESIDENT ENGINEER FOR EXISTING CONDITIONS ENCOUNTERED. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR SAID FIELD ADJUSTMENTS.
6. CONTRACTOR SHALL INSTALL "CAUTION DO NOT ENTER" TAPE AND LATHE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. COST INCIDENTAL TO THE CONTRACT.
7. AREAS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER TO BE CLEARED AND GRUBBED SHALL BE SMOOTHLY GRADED TO DRAIN MEETING THE SATISFACTION OF THE ENGINEER AND AIRPORT. GRADING SHALL BE INCIDENTAL TO THE SEEDING PAY ITEM. THE AREAS DISTURBED BY CLEARING AND GRUBBING OPERATIONS SHALL BE SEEDED (901) AND MULCHED (908).
8. GATE LOCATIONS TO BE DETERMINED BY THE AIRPORT.
9. UPON REQUEST BY THE CONTRACTOR, A COPY OF THE AIRPORT'S BOUNDARY SURVEY IS AVAILABLE FOR USE BY THE CONTRACTOR.
10. WHEN THE NEW FENCE IS LOCATED WITHIN OR ADJACENT TO A FARM FIELD, THE CONTRACTOR SHALL SEED (901) AND BLANKET (156) THE FARM FIELD WITHIN 5' OF THE FENCE AS DIRECTED BY THE ENGINEER.
10. DURING DEWATERING OPERATION, WATER SHALL BE PUMPED INTO SEDIMENT BASINS OR SILT TRAPS. COSTS OF DEWATERING, SEDIMENT BASINS AND SILT TRAPS ARE INCIDENTAL TO THE CONTRACT. DEWATERING DIRECTLY INTO FIELD TILES OR STORMWATER STRUCTURES IS PROHIBITED.
11. UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE CONSTRUCTED ACCORDING TO MINIMUM STANDARDS AND SPECIFICATIONS IN THE ILLINOIS URBAN MANUAL REVISED FEBRUARY 2012.
12. WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER FOR PROPER EROSION AND SEDIMENT CONTROL.
13. IN AREAS WHERE WORK IS COMPLETE, PERMANENT STABILIZATION SHALL OCCUR WITHIN 7 DAYS OF COMPLETION, AND IN AREAS WORK HAS TEMPORARILY CEASED, TEMPORARY STABILIZATION SHALL OCCUR BY THE 14TH DAY AFTER WORK HAS CEASED.
14. SEE GENERAL NOTES 7 AND 10.
15. TEMPORARY SEEDING (156) SHALL BE PAID FOR ONLY WHEN NEEDED FOR WINTERIZATION OF THE SITE AT THE LOCATIONS NOTED BY THE ENGINEER. ALL OTHER APPLICATIONS OF TEMPORARY SEED NEEDED FOR COMPLIANCE FOR THE NPDES PERMIT SHALL BE INCIDENTAL TO THE CONTRACT.



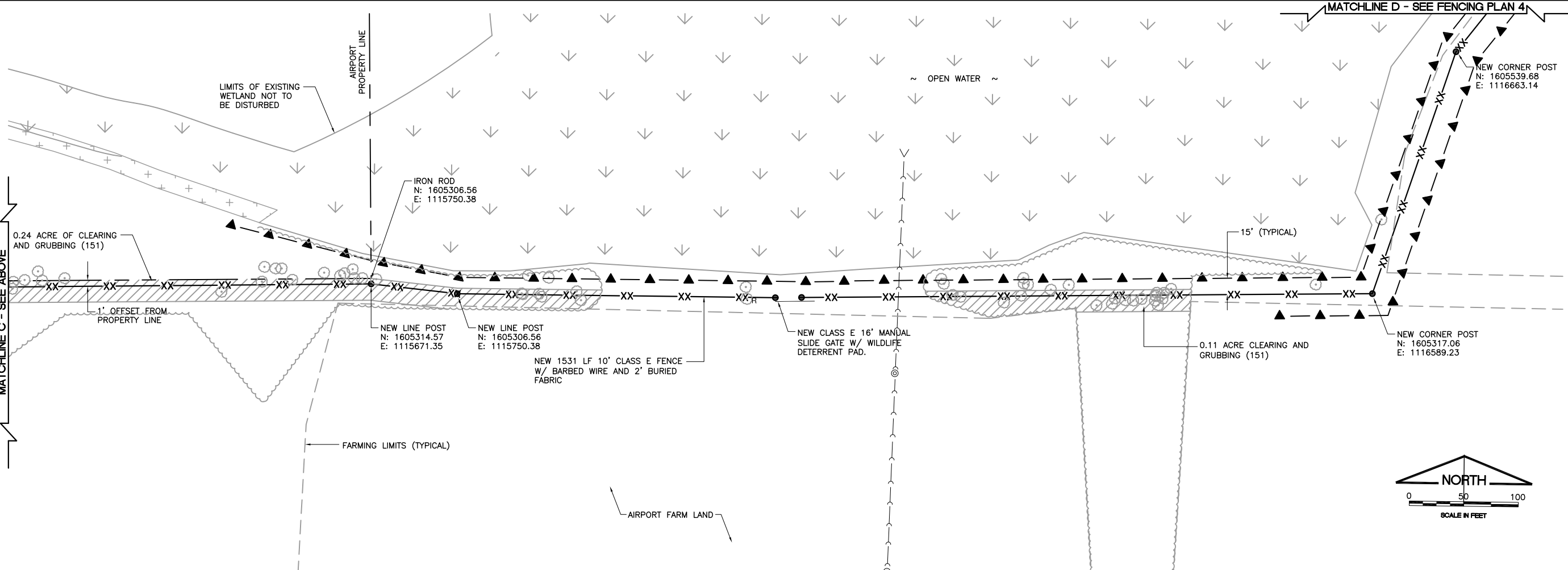
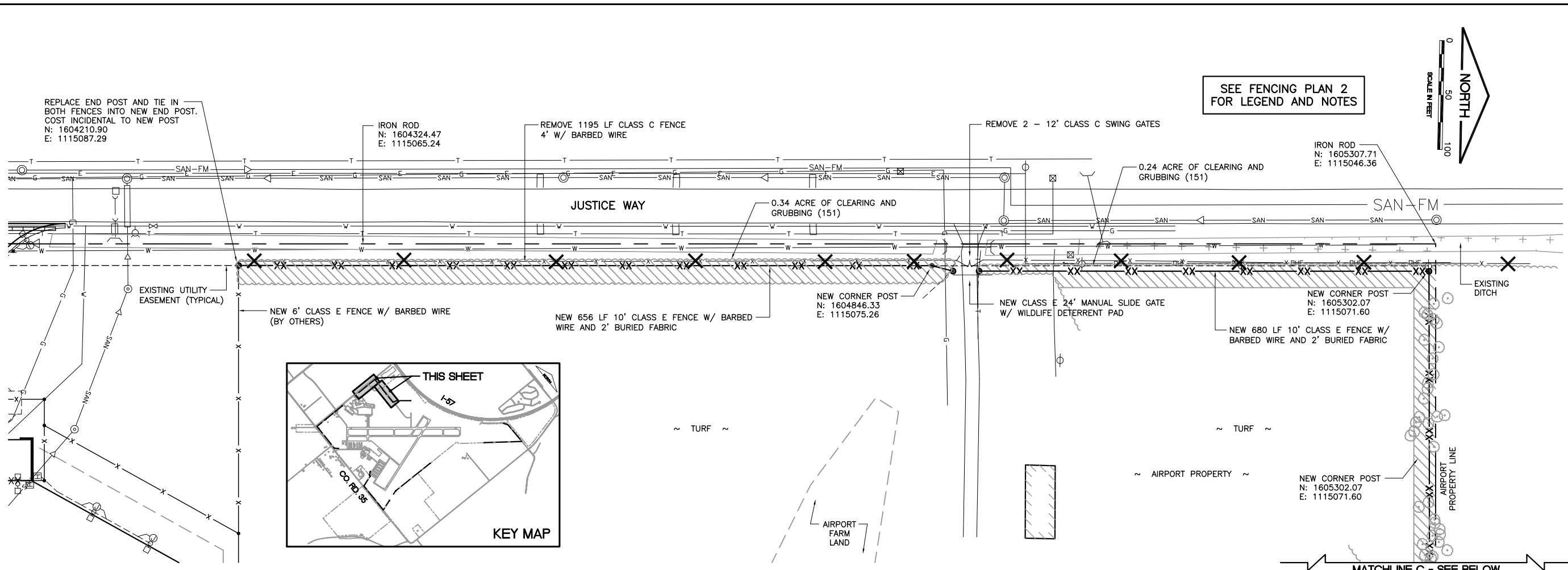
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JOB No:	14075-05

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SHEET 9 OF 18 SHEETS

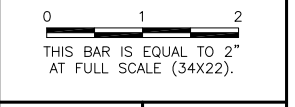
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IL. CONTRACT: **KA046**
 IL. LETTING ITEM: **11A**
 IL. PROJECT: **IKK-4401**
 S.B.G. PROJECT: **3-17-SPGP-XX**

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KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1
FENCING PLAN 3

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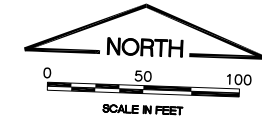
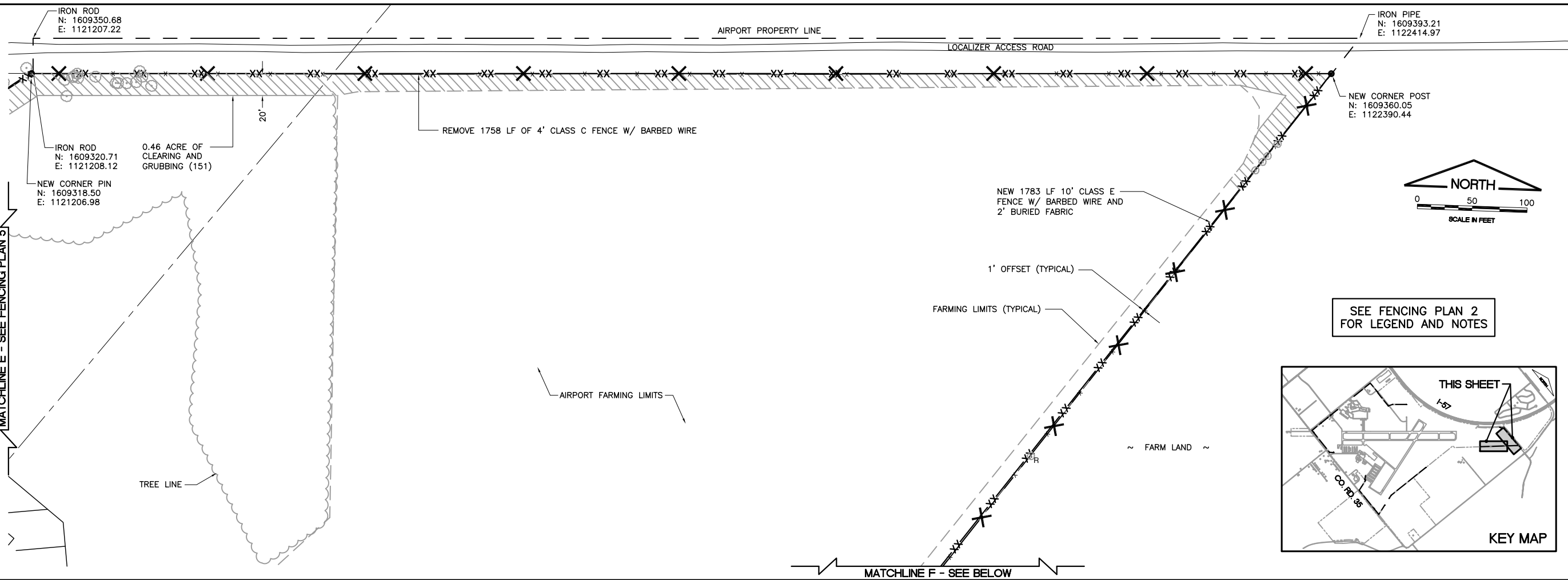
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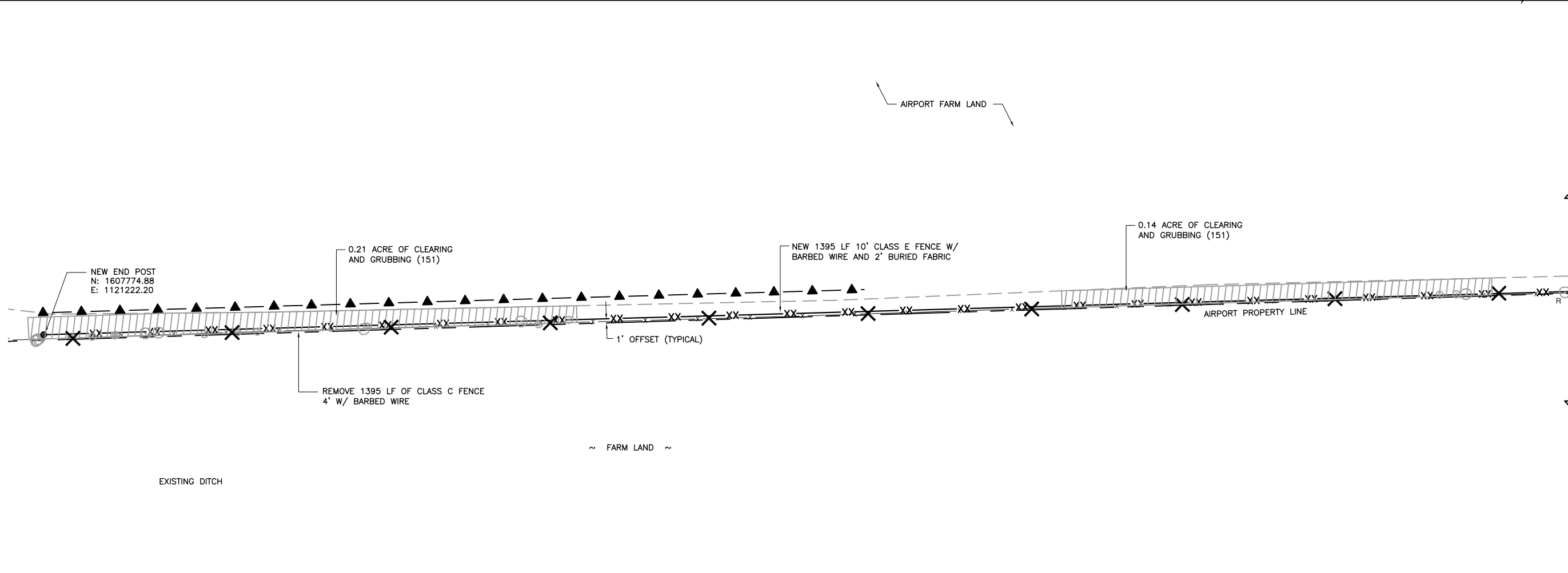
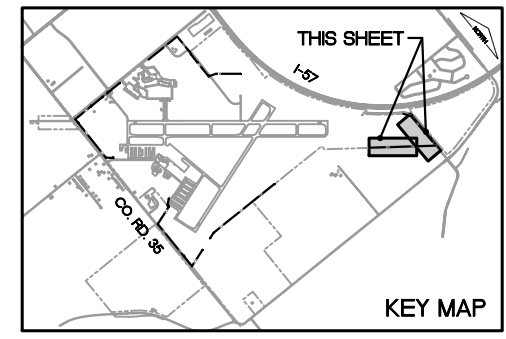
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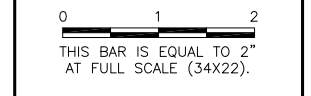
SEE FENCING PLAN 2 FOR LEGEND AND NOTES



IL. CONTRACT: **KA046**
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 IL. PROJECT: **IKK-4401**
 S.B.G. PROJECT: **3-17-SPGP-XX**

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KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1
FENCING PLAN 5

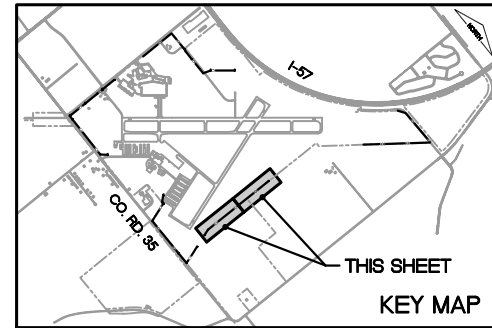
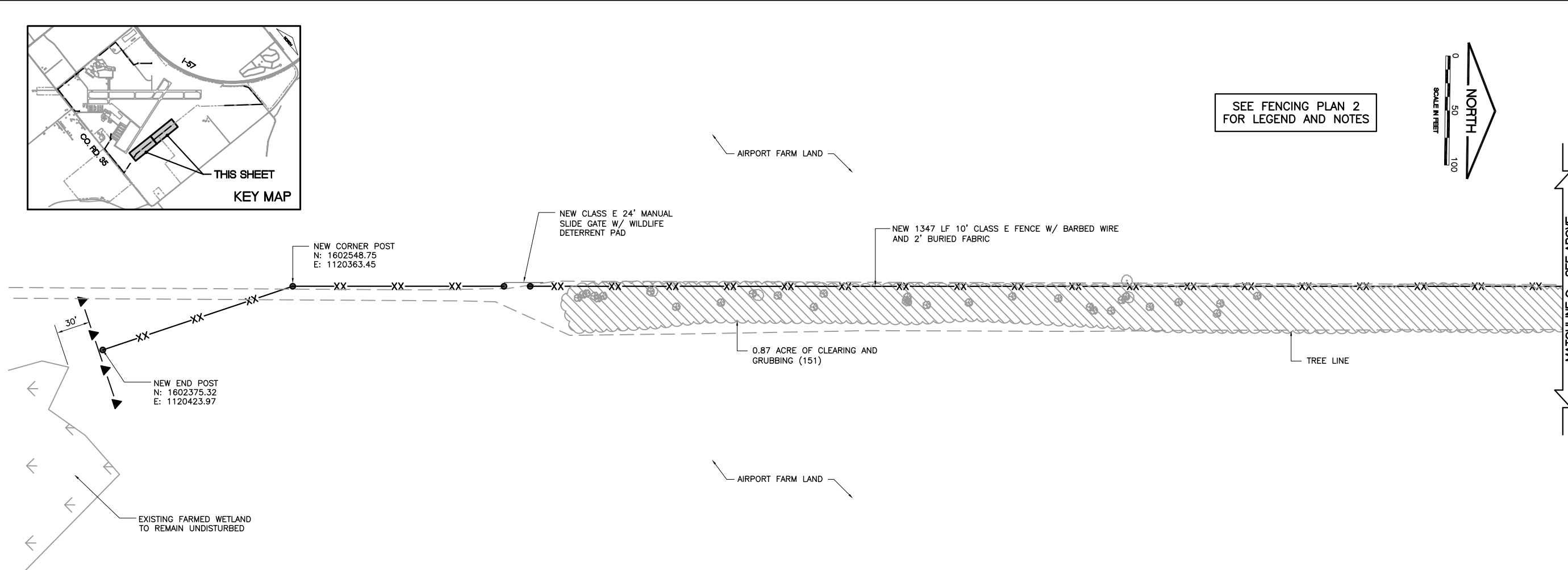
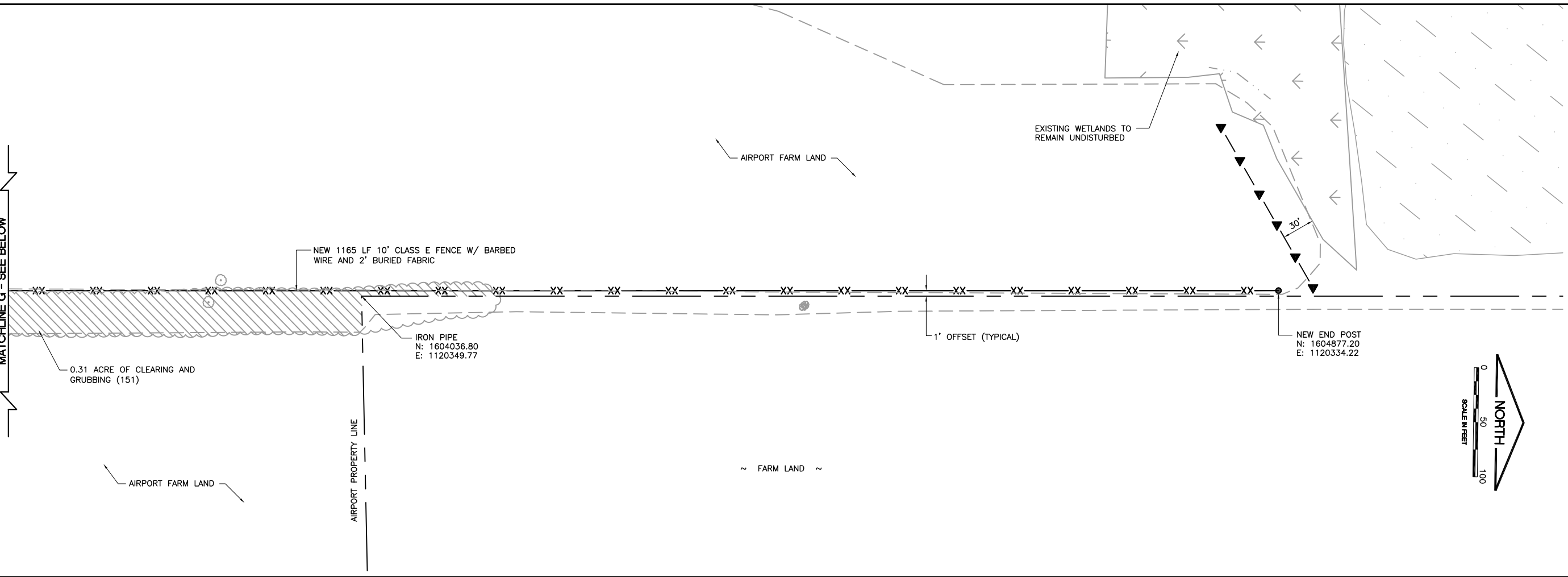
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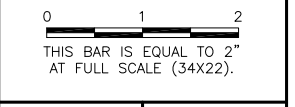


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 IL. PROJECT: **IKK-4401**
 S.B.G. PROJECT: **3-17-SPGP-XX**

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KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1

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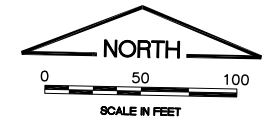
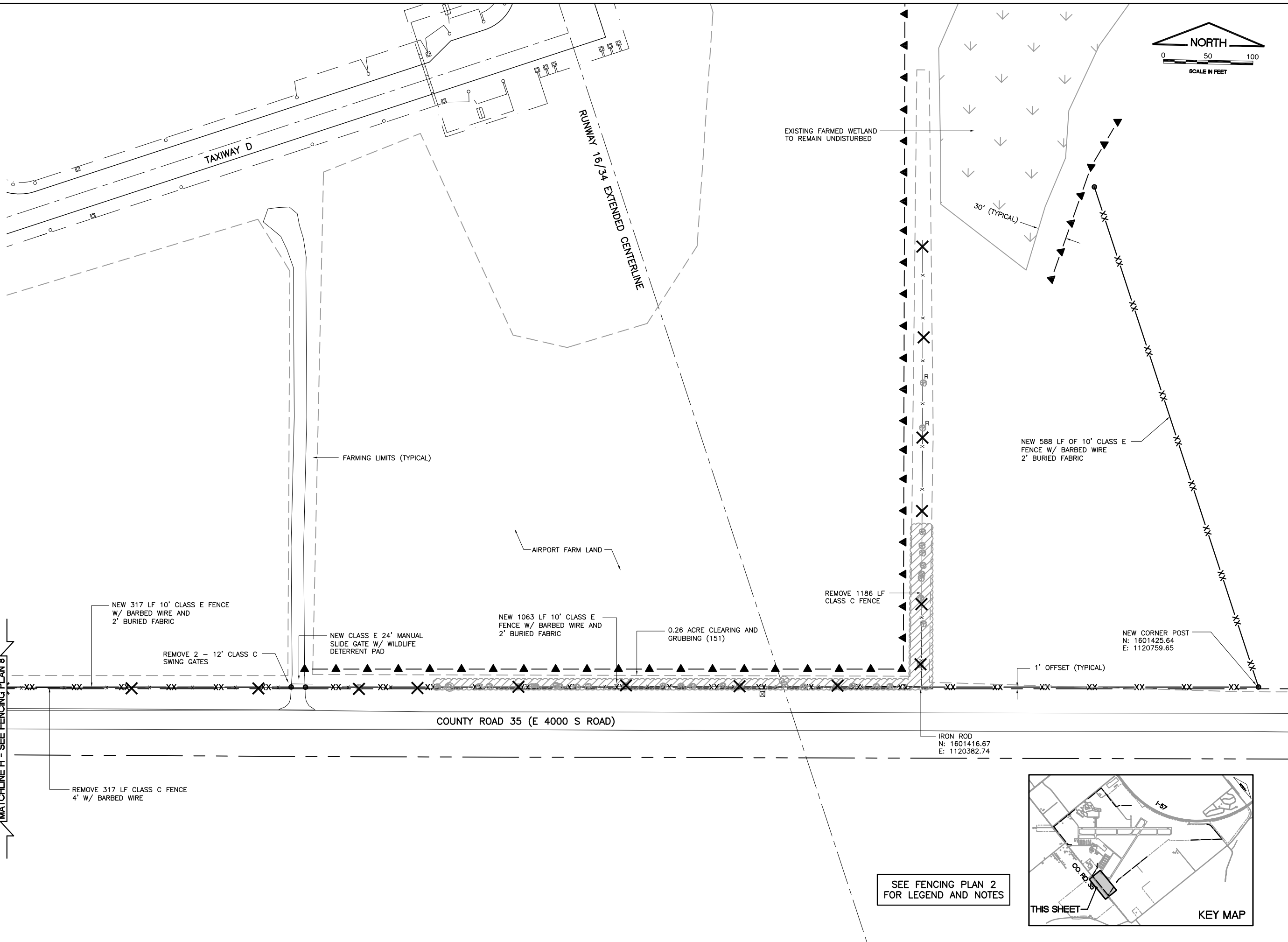
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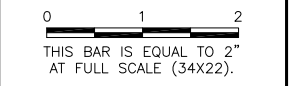
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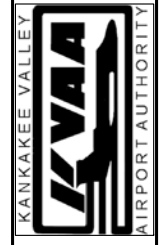
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KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1
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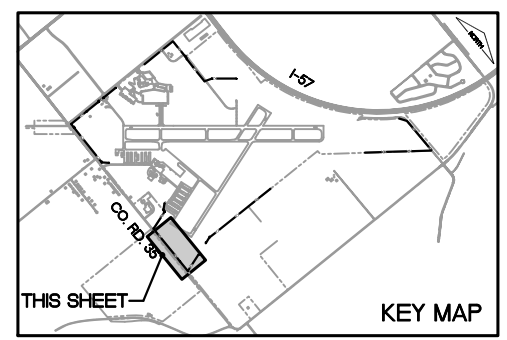
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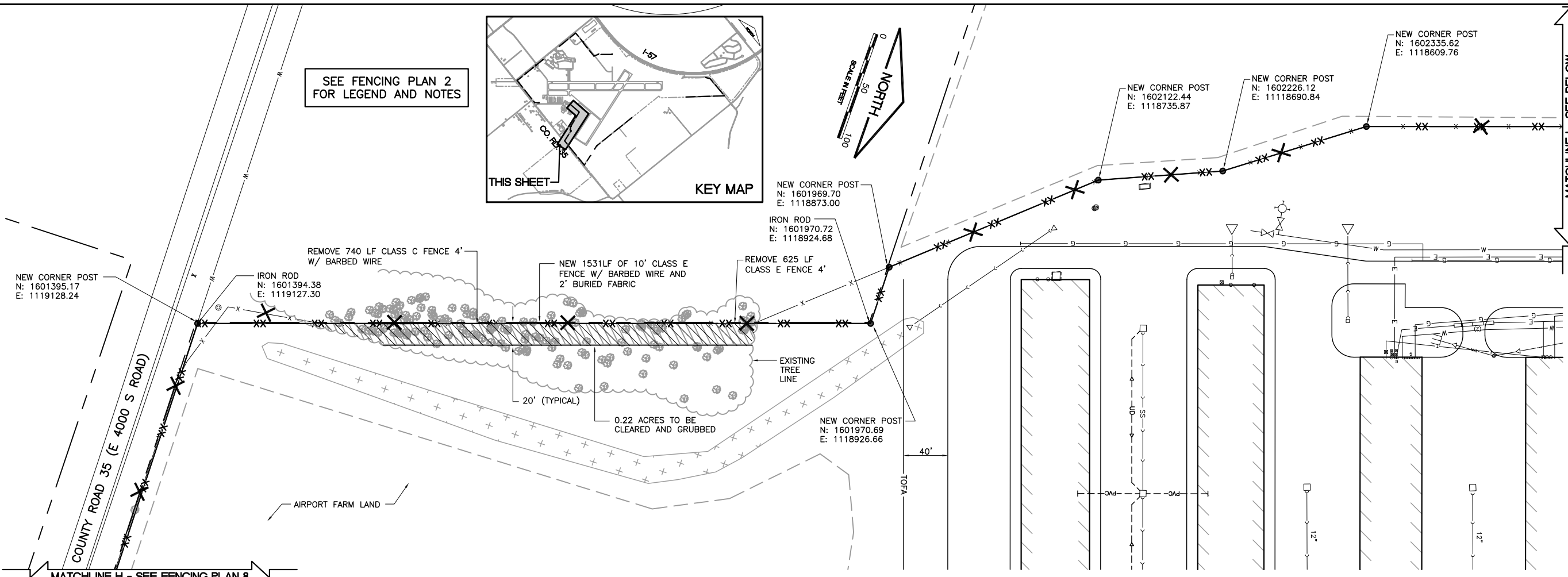
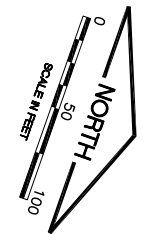
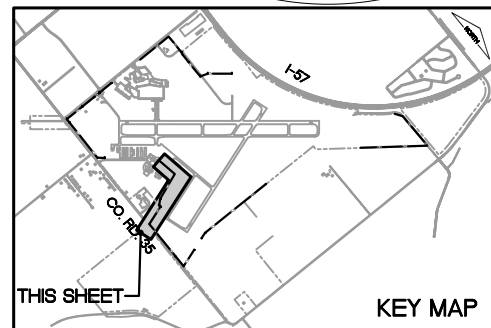
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 FOR LEGEND AND NOTES



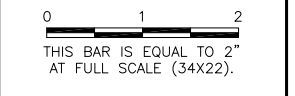
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 S.B.G. PROJECT: **3-17-SPGP-XX**

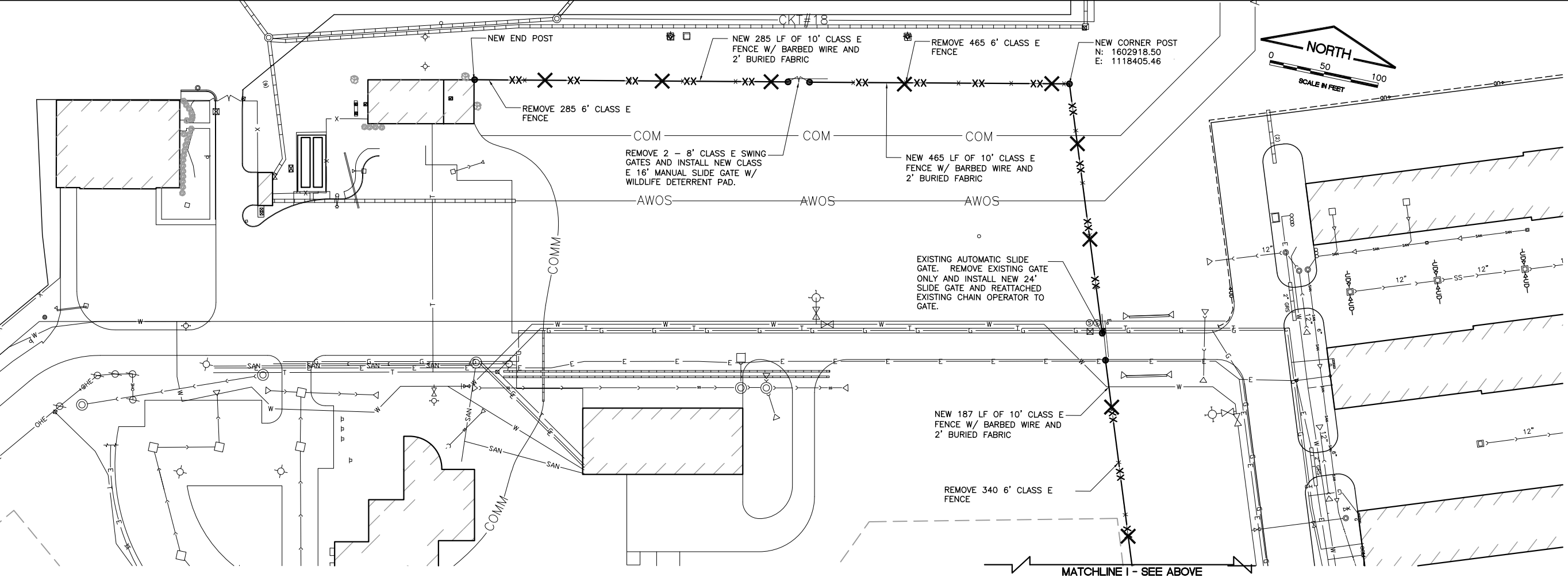
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**KANKAKEE VALLEY AIRPORT AUTHORITY
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 PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1**

FENCING PLAN 8



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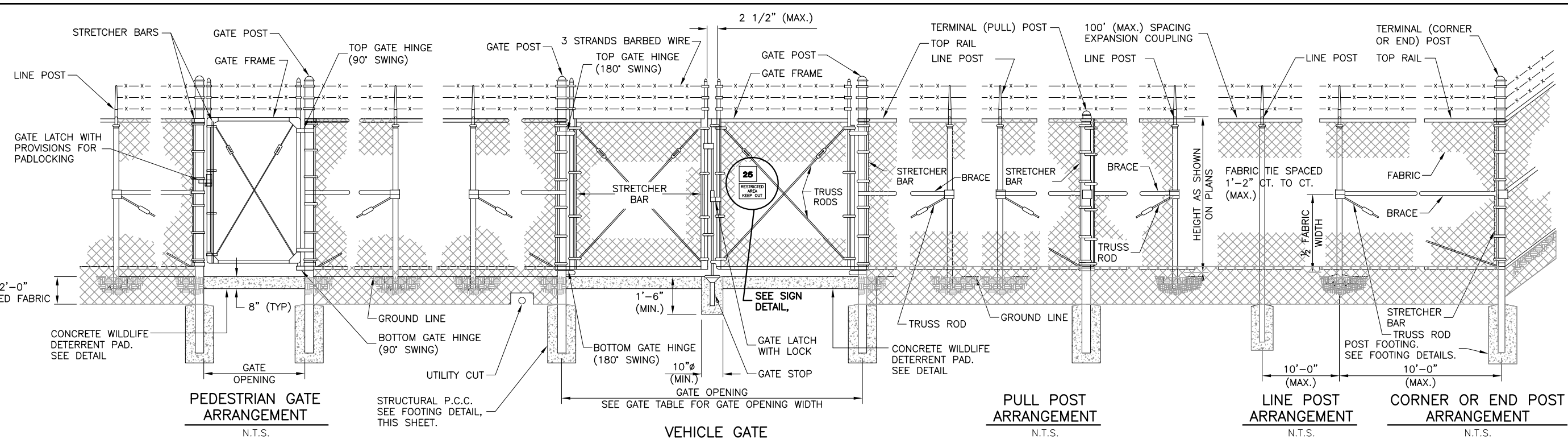
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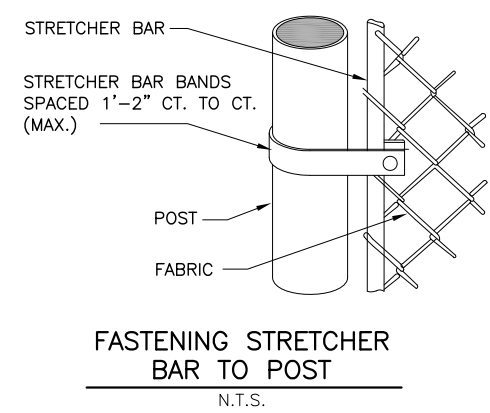
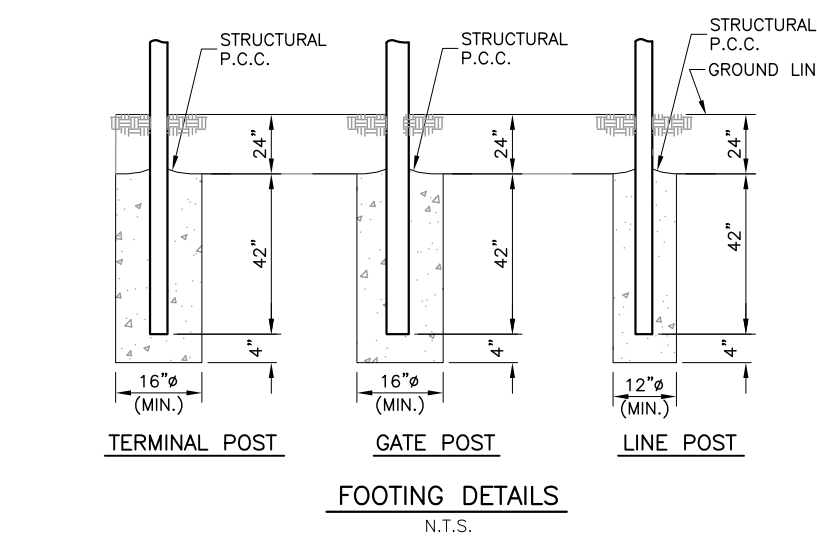
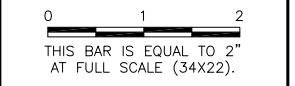
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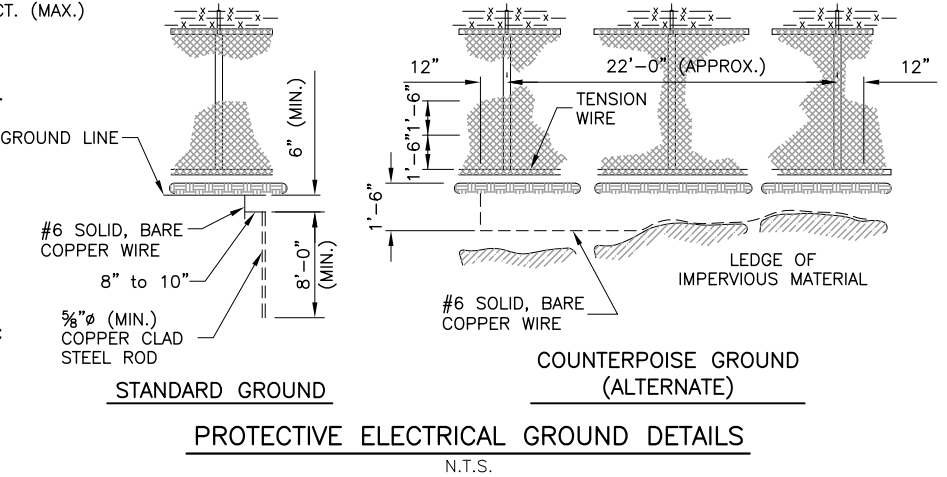
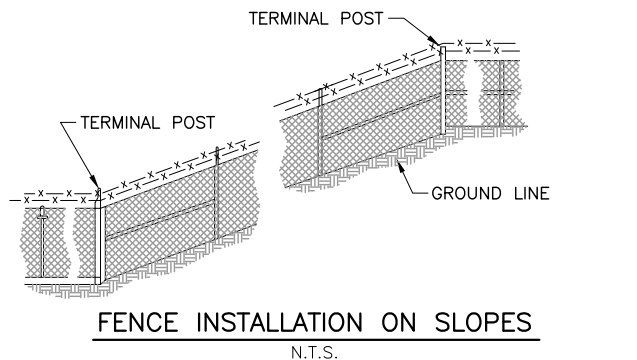
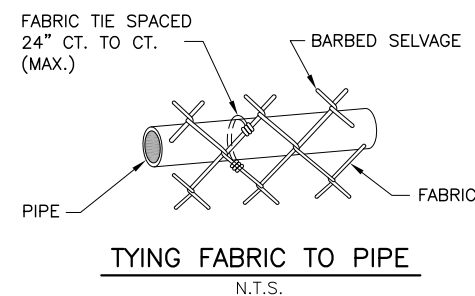
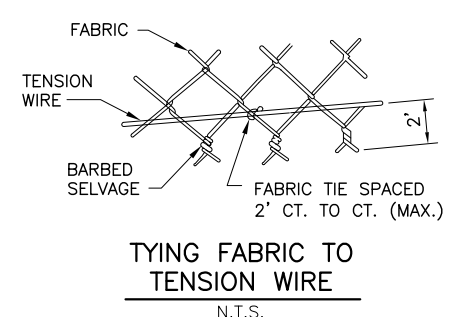
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SURVEY BOOK # BOOK #

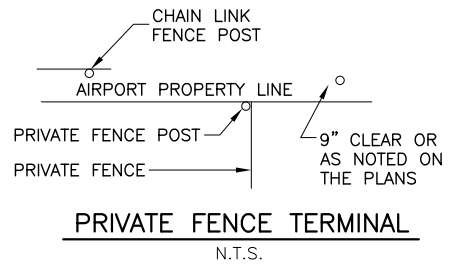
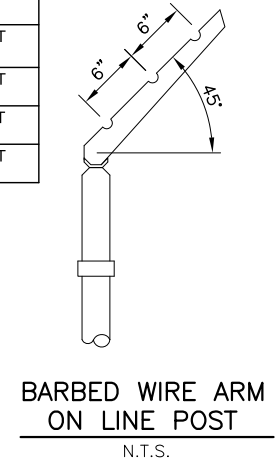
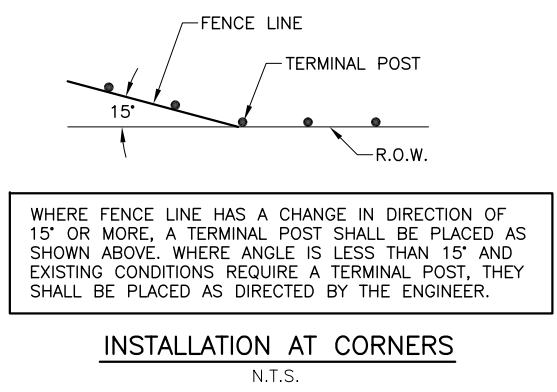
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NUMBER	BY	DATE



PULL POSTS SHALL BE PLACED AT LOCATIONS DETERMINED BY THE ENGINEER. THEY SHALL BE PLACED AT 500 FT. INTERVALS BETWEEN POSTS TO WHICH THE ENDS OF THE FABRIC ARE CLAMPED OR MIDWAY BETWEEN SUCH POSTS WHEN THE DISTANCE IS LESS THAN 1000 FT. AND GREATER THAN 500 FT.



- NOTES**
- CONTINUOUS FENCE SHALL BE GROUNDED AT INTERVALS NOT EXCEEDING 500 FT. EXCEPT THERE SHALL BE A GROUND NOT EXCEEDING 100 FT. FROM GATE IN EACH SECTION OF THE FENCE ADJACENT TO THE GATE.
 - FENCE UNDER POWER LINES SHALL BE GROUNDED BY 3 GROUNDS, ONE DIRECTLY UNDER THE CROSSING AND ONE ON EACH SIDE 25 TO 50 FT. AWAY. A SINGLE GROUND SHALL BE LOCATED DIRECTLY UNDER EACH TELEPHONE WIRE OR CABLE CROSSINGS.
 - THE COUNTERPOISE SHALL BE USED ONLY WHERE IT IS IMPOSSIBLE TO DRIVE A GROUND ROD BECAUSE OF IMPERVIOUS EARTH STRUCTURES.
 - THE GROUND WIRE SHALL BE CONNECTED TO THE FABRIC AND THE GROUND ROD BY A MECHANICAL CLAMP OF CAST BRONZE BODY AND BRONZE OR STAINLESS STEEL BOLTS AND WASHERS. WHEN A TENSION WIRE IS REQUIRED, THE BOTTOM CONNECTION OF THE GROUND WIRE SHALL BE MADE TO THE TENSION WIRE.



FENCE POST TABLE

POST TYPE	ASTM F 1083		ASTM F 1043	
	DIAMETER	WEIGHT	DIAMETER	WEIGHT
CORNER, END, PULL	3.5" O.D.	7.58 LB/FT	3.5" O.D.	5.71 LB/FT
LINE POST	2.875" O.D.	5.80 LB/FT	2.875" O.D.	4.64 LB/FT
TOP RAIL	1.66" O.D.	2.27 LB/FT	1.66" O.D.	1.84 LB/FT
GATE POST	4" O.D.	9.12 LB/FT	4" O.D.	6.56 LB/FT

KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1
FENCING DETAILS 1

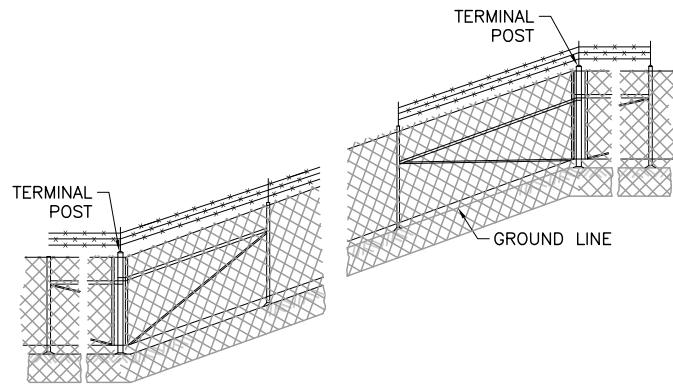
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DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	ARM
DATE:	6/3/2016
JOB No:	14075-05

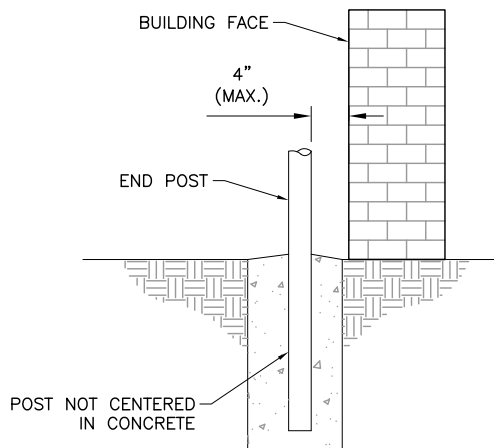
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SHEET 16 OF 18 SHEETS

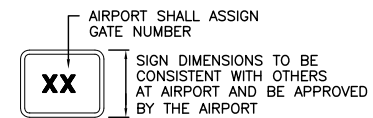
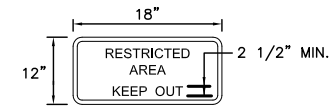
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INSTALLATION ON SLOPES
N.T.S.



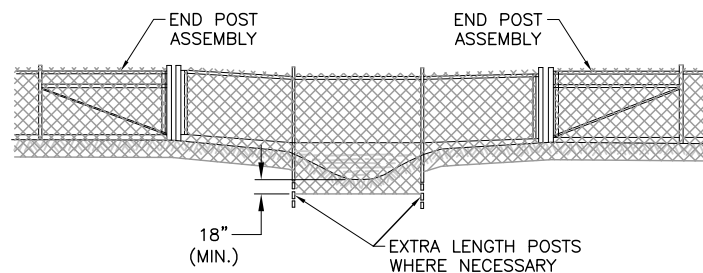
FENCE INSTALLATION AT BUILDING FACE
N.T.S.



SIGN DETAILS
N.T.S.

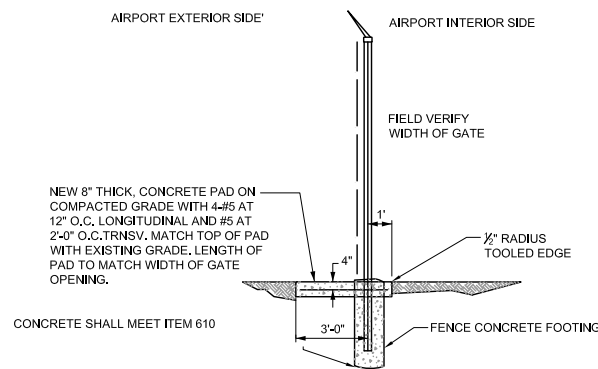
NOTES:

1. EACH PEDESTRIAN OR VEHICLE SWING GATE SHALL REQUIRE ONE "RESTRICTED" SIGN AND ONE "NUMBER" SIGN. AIRPORT SHALL PROVIDE GATE NUMBER. EVERY 100' OF FENCE SHALL REQUIRE ONE "RESTRICTED" SIGN. COST INCIDENTAL TO FENCE.
2. 0.08 GA ALUMINUM ALLOY SHEET. LETTERING SHALL BE RED ON A WHITE BACKGROUND.
3. CANTILEVERED SLIDE GATES SHALL INCLUDE TWO ADDITIONAL SIGNS AS DETAILED ON THE CANTILEVER GATE DETAIL SHEETS.



THE CHAIN LINK FABRIC SHALL BE EXTENDED TO MAINTAIN A MINIMUM BURIED DEPTH OF 18". ADDITIONAL FABRIC MAY BE ATTACHED TO EXTEND TO THE DEPTH REQUIRED. (COST INCIDENTAL TO NEW FENCE)

ELEVATION
FENCE INSTALLATION OVER STREAM OR SWALES



DETAIL
TYPICAL NOT TO SCALE
TYPICAL WILDLIFE DETERRENT CONCRETE PAD AT GATES
NOT TO SCALE

SIGNS					
CONTRACTOR SHALL FURNISH AND INSTALL SIGNS AS INDICATED IN TABLE BELOW					
SIGN	DIMENSIONS WIDTH x HEIGHT	BACKGROUND COLOR	LETTERING		TEXT
			COLOR	HEIGHT	
1	GATE NUMBER - SEE DETAIL ON FENCING DETAILS SHEET 2				
2	24" x 18"	WHITE	RED	1.5"	FAA REGULATIONS REQUIRE THAT AFTER ENTERING OR EXITING THROUGH THIS SECURITY GATE YOU ARE REQUIRED TO WAIT UNTIL THE GATE CLOSES BEFORE PROCEEDING. FINES OR PENALTIES WILL BE IMPOSED.
3	18" x 12"	WHITE	RED	2-1/2"	RESTRICTED AREA KEEP OUT
4	WARNING SIGN - SEE DETAIL ON CANTILEVER GATE DETAILS SHEET				

NOTES:

1. SIGNS #1, #2 AND #4 SHALL BE INSTALLED ON BOTH SIDES OF GATE. SIGN #3 SHALL BE INSTALLED ON "LAND" SIDE OF GATE.
2. ALL CANTILEVERED SLIDE GATES, BOTH MANUAL AND ELECTRIC OPERATION, SHALL HAVE ALL FOUR SIGNS INSTALLED, INCIDENTAL TO GATE INSTALLATION.
3. ALL SWING GATES SHALL HAVE SIGNS #1 AND #3 INSTALLED, INCIDENTAL TO GATE INSTALLATION.

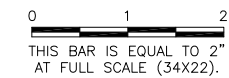
GENERAL NOTES - GATE SIGNS

1. SIGNS OTHER THAN THOSE SHOWN ABOVE THAT ARE AFFIXED TO EXISTING GATES SHALL BE REMOVED AND TRANSFERRED TO THE NEW GATE AT THE COMPLETION OF THE GATE INSTALLATION. COST INCIDENTAL TO NEW GATE.

IL. CONTRACT: **KA046**
 IL. LETTING ITEM: **11A**
 IL. PROJECT: **IKK-4401**
 S.B.G. PROJECT: **3-17-SPGP-XX**

SURVEY BOOK # BOOK #

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KANKAKEE, ILLINOIS
PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1

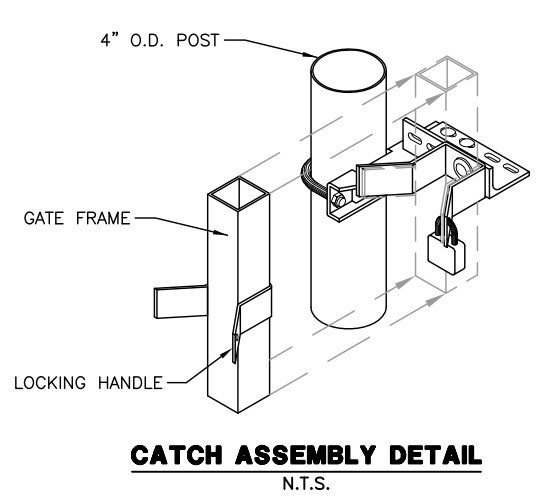
FENCING DETAILS 2

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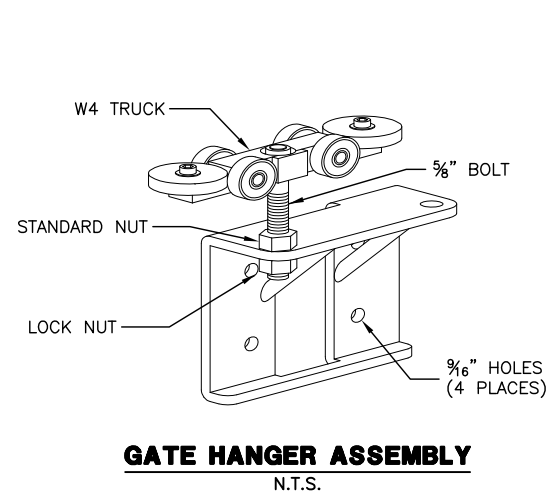
DESIGN BY: ARM
 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: ARM
 DATE: 6/3/2016
 JOB No: 14075-05

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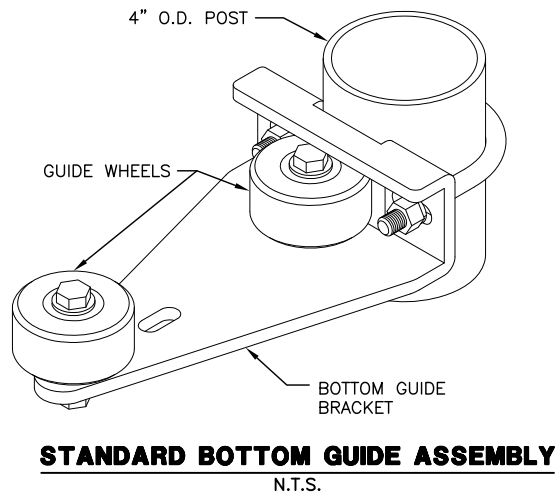
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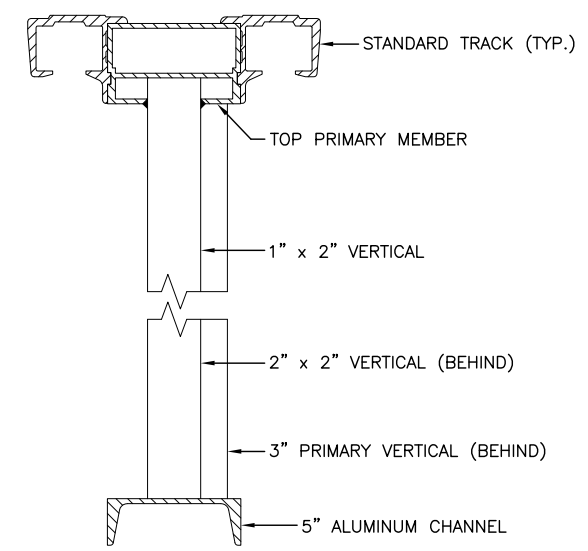
CATCH ASSEMBLY DETAIL
N.T.S.



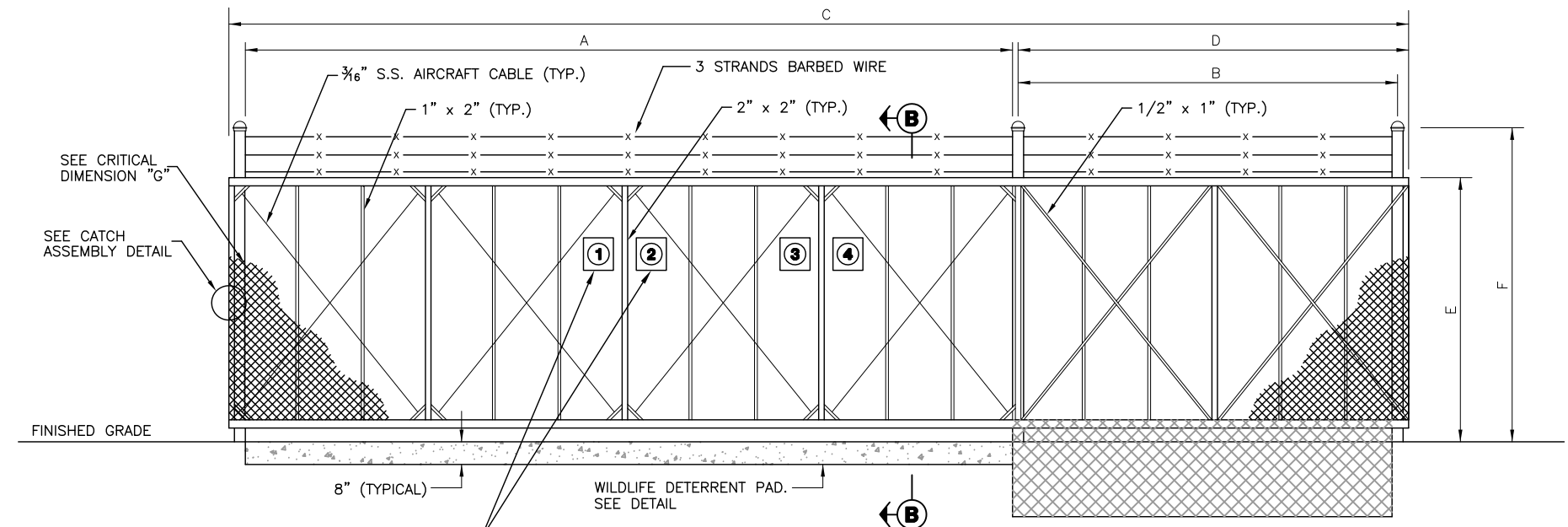
GATE HANGER ASSEMBLY
N.T.S.



STANDARD BOTTOM GUIDE ASSEMBLY
N.T.S.



GATE FRAME SECTION
N.T.S.



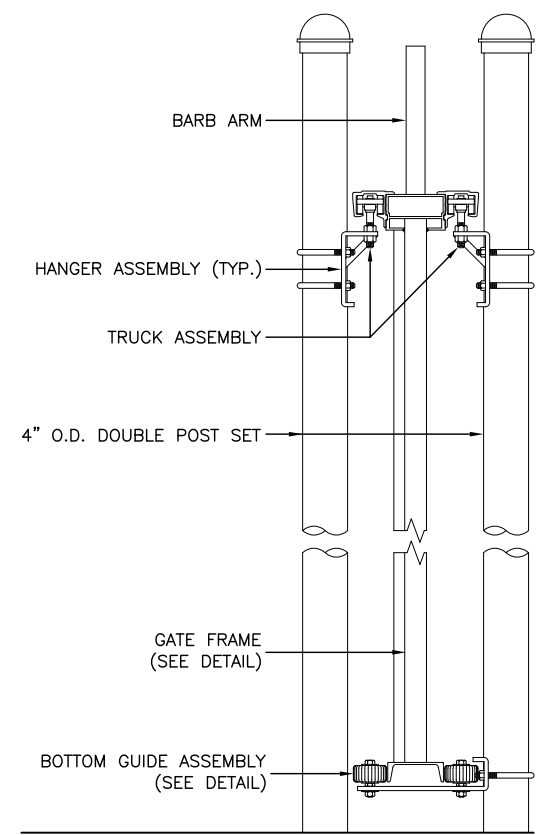
ELEVATION
CANTILEVER SLIDE GATE
N.T.S.

CRITICAL DIMENSIONS			
A	CLEAR OPENING	16'-0"	24'-0"
B	COUNTER BALANCE POST SPACING, CENTER TO CENTER	7'-1"	11'-1"
C	OVERALL GATE LENGTH	24'-0"	36'-0"
D	COUNTERBALANCE LENGTH	8'-0"	12'-0"
E	NOMINAL GATE HEIGHT	10'-0"	10'-0"
F	POST HEIGHT	11'-6"	11'-6"
G	FABRIC HEIGHT	9'-0"	9'-0"

- UL 235 COMPLIANCE NOTES**
- GATE INSTALLATION SHALL COMPLY WITH ALL REQUIREMENTS OF UL 235, INCLUDING, BUT NOT LIMITED TO:
- ALL OPENINGS OF THE SLIDE GATE ARE GUARDED OR SCREENED FROM THE BOTTOM OF THE GATE TO A MINIMUM OF 4 FEET ABOVE GROUND TO PREVENT A 2-1/4" DIAMETER SPHERE FROM PASSING THROUGH THE OPENINGS ANYWHERE IN THE GATE, AND IN THAT PORTION OF THE ADJACENT FENCE THAT THE GATE COVERS IN THE OPEN POSITION.
 - ALL EXPOSED PINCH POINTS ARE ELIMINATED OR GUARDED AND GUARDING IS SUPPLIED FOR ALL EXPOSED ROLLERS.
 - A WARNING SIGN (SEE DETAIL, THIS SHEET) MUST BE AFFIXED TO BOTH SIDES OF THE SLIDE GATE, BOTH MANUAL AND ELECTRIC.



WARNING SIGN DETAIL
N.T.S.



NOTE:
EXTRUSIONS SHALL BE OVERSIZED FOR EXTRA RIGIDITY OVER "STANDARD" GATE DESIGN.

SECTION B-B
(TYPICAL ALL GATES)

LOCATIONS, DETAILS AND CHARACTER OF EQUIPMENT SHOWN ON THIS SHEET ARE GENERIC. EQUIPMENT LOCATIONS SHALL BE AS RECOMMENDED BY THE EQUIPMENT MANUFACTURER.

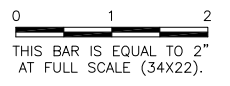
MANUAL SLIDE GATE NOTES:

- CANTILEVERED GATE SHALL BE SUFFICIENTLY RIGID TO WITHSTAND FLEXING OR BENDING DURING WINDY CONDITIONS. CONTRACTOR SHALL PROVIDE STIFFENERS, STRUCTURAL SHAPES IN EXCESS OF THE MINIMUM SPECIFIED DIMENSIONS OR ADDITIONAL ROLLERS AND POSTS SUFFICIENT TO PREVENT DISPLACEMENT OF THE GATE BY WIND OR BY UNAUTHORIZED PERSONNEL.
- CONTRACTOR SHALL PROVIDE AND INSTALL GATE AS A COMPLETE WORKING UNIT. THE GATE WORK SHALL INCLUDE, BUT NOT BE LIMITED TO: GATE, POSTS, AND ALL CONNECTIONS, SIGNS, LABOR AND MATERIALS NECESSARY FOR COMPLETE OPERATION.
- THE FABRIC TYPE & FINISH OF THE GATE SHALL MATCH THE PROPOSED FENCE OR BE AS DIRECTED BY THE ENGINEER.
- ALL SLIDING GATES SHALL HAVE ALL ROLLERS ENCLOSED IN STEEL OR PLASTIC SHROUDS TO PREVENT ACCIDENTAL INJURY.
- 3 STRANDS OF BARBED WIRE ON TOP OF GATE NOT SHOWN TO IMPROVE CLARITY.
- SEE FENCING DETAILS SHEET 2 FOR SIGN DETAILS.

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KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PERIMETER SAFETY/WILDLIFE FENCE - PHASE 1
CANTILEVER GATE DETAILS

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SHEET 18 OF 18 SHEETS