

LETTING ITEM NO. 1A  
 LETTING DATE: JULY 29, 2016

TOTAL SHEETS: 32  
 PE001

# CONSTRUCTION PLANS FOR PEORIA INTERNATIONAL AIRPORT

GENERAL WAYNE A. DOWNING  
 PEORIA, ILLINOIS

100% SUBMITTAL

IL. PROJ. NO. PIA-4542 CONTRACT NO. PE001  
 AIP PROJ. NO. 3-17-0080-XX

## RECONSTRUCT TERMINAL APRON PHASE 2

JUNE 3, 2016

**811** Know what's below.  
 Call before you dig.  
 COMMON GROUND ALLIANCE  
 www.call811.com or  
 Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES, PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

**DESIGN INFORMATION**

GEOMETRIC CRITERIA

AIRPLANE DESIGN GROUP IV  
 TAXIWAY DESIGN GROUP IV

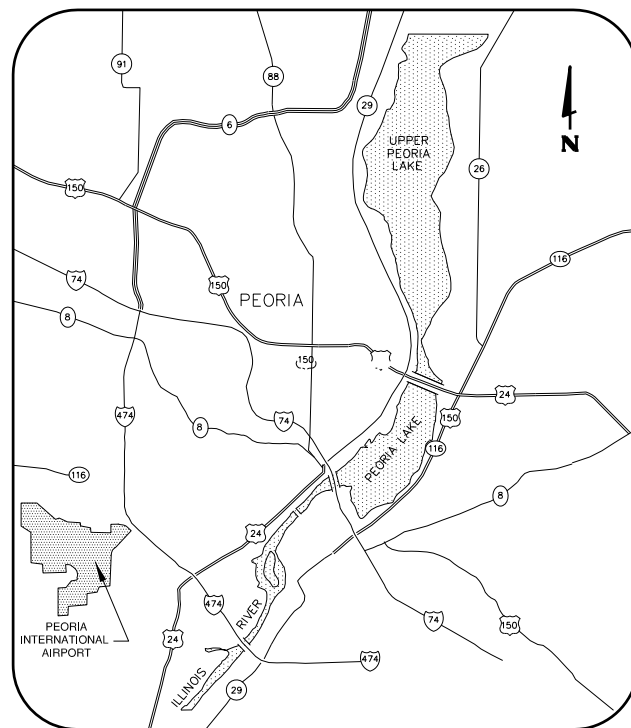
PAVEMENT DESIGN CRITERIA

AIRCRAFT DUAL WHEEL GEAR  
 DEPARTURE WEIGHT = 161,000 LBS  
 32 EQUIV. ANNUAL DEPARTURES

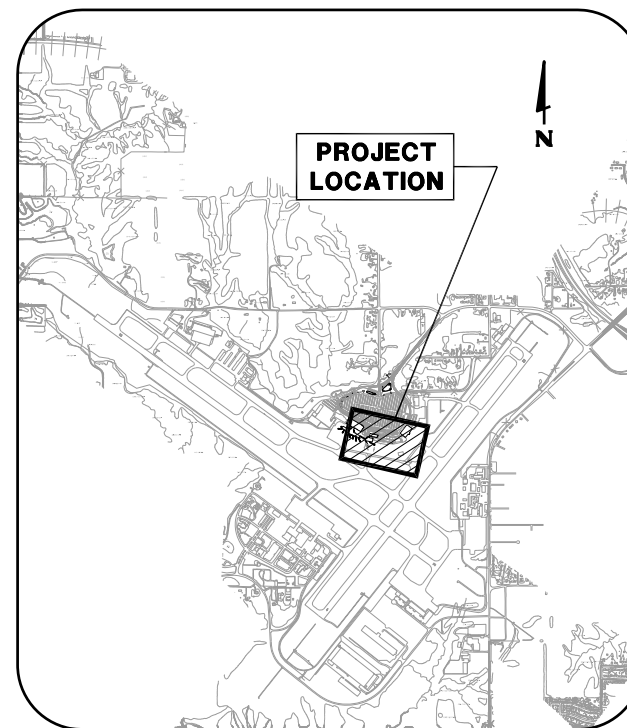
MAXIMUM EQUIPMENT HEIGHT = 25'  
 GROUND FREQUENCY 121.85

GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT

TOWNSHIP: 8 NORTH  
 RANGE: 7 EAST OF THE 4TH P.M.  
 SECTION: 22  
 COUNTY: PEORIA  
 CIVIL TOWNSHIP: LIMESTONE



LOCATION MAP



SITE PLAN

EXP. 11-30-2017  
 JEREMY M. WASILEWSKI  
 062.067850  
 LICENSED PROFESSIONAL ENGINEER OF ILLINOIS  
 6-16-2016

GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT



APPROVED *[Signature]*

DATE June 16, 2016



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SUBMITTED BY *[Signature]*

DATE June 16, 2016

CMT JOB NUMBER: 15061-05

K:\Peoria\15061-05\_TermApronRehab\Draw\Sheets  
 FILE: 15061-05\_G1001.dwg  
 UPDATE BY: Jeremy Wasilewski  
 PLOT DATE: 6/16/2016 3:23 PM

100% SUBMITTAL  
 JUNE 3, 2016

 RECONSTRUCT TERMINAL  
 APRON PHASE 2

OWNER

 GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL  
 AIRPORT  
 PEORIA, ILLINOIS

| MARK | DATE | DESCRIPTION |
|------|------|-------------|
|      |      |             |
|      |      |             |
|      |      |             |
|      |      |             |

AIP PROJ. NO. 3-17-0080-XX

IL PROJ. NO. PIA-4542 CONTRACT NO. PE001

CMT PROJECT NO: 15061-05

CAD DWG FILE: 15061-05\_GI002.DWG

DESIGNED BY: JMW

DRAWN BY: CMT

CHECKED BY: APP

APPROVED BY: CHK

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SHEET TITLE

 INDEX TO SHEETS &  
 SUMMARY OF  
 QUANTITIES

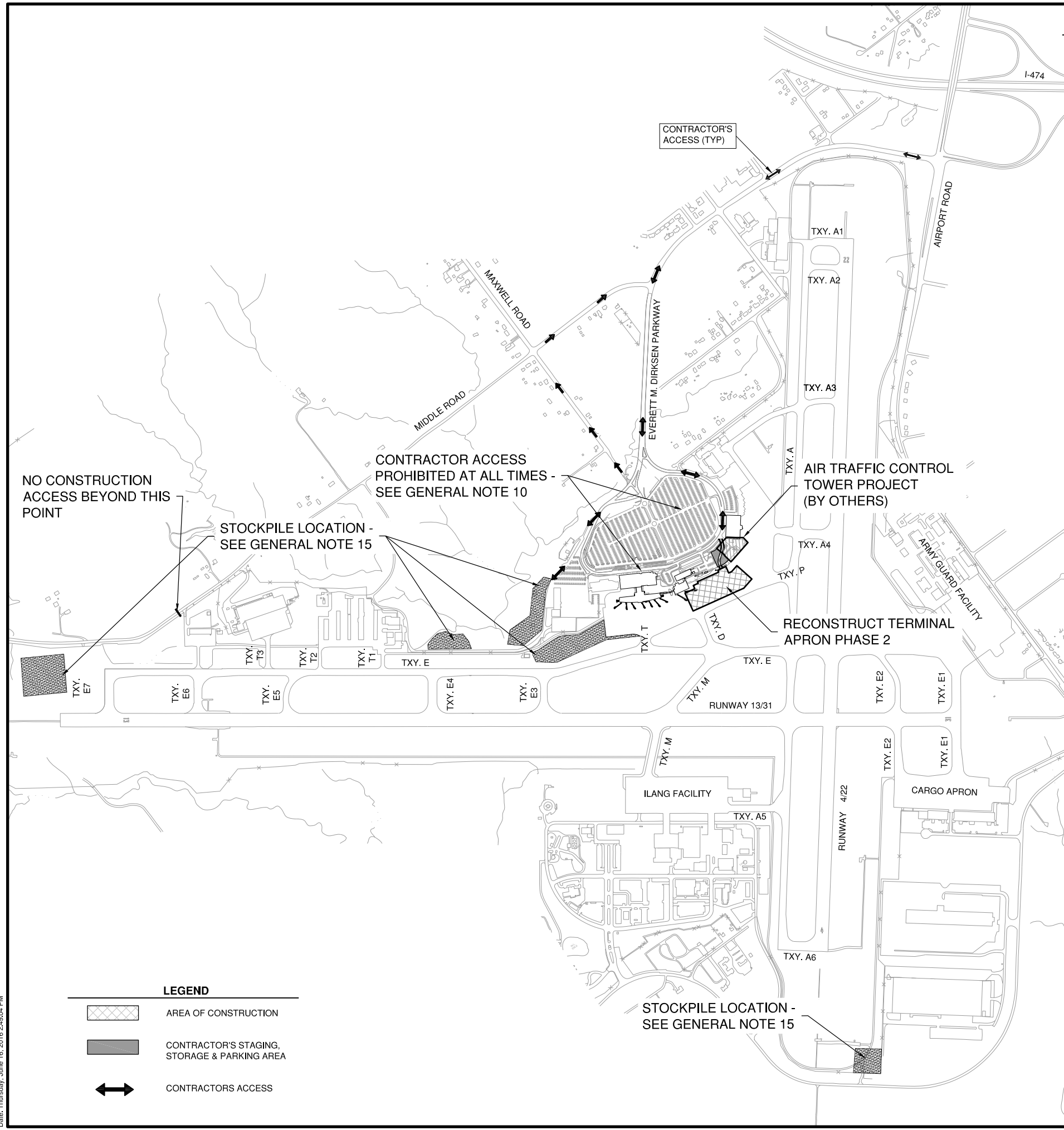
 SHEET **2** OF **32**
**SUMMARY OF QUANTITIES**

| BASE BID             |                                    |      |          |
|----------------------|------------------------------------|------|----------|
| ITEM NO.             | ITEM DESCRIPTION                   | UNIT | QUANTITY |
| AR150520             | MOBILIZATION                       | LS   | 1        |
| AR152410             | UNCLASSIFIED EXCAVATION            | CY   | 5,300    |
| AR156513             | SEPARATION FABRIC                  | SY   | 16,225   |
| AR208540             | OVERSIZE AGGREGATE                 | TON  | 12,330   |
| AR209604             | CRUSHED AGG. BASE COURSE - 4"      | SY   | 16,225   |
| AR209612             | CRUSHED AGG. BASE COURSE - 12"     | SY   | 580      |
| AR501509             | 9" PCC PAVEMENT                    | SY   | 580      |
| AR501518             | 18" PCC PAVEMENT                   | SY   | 16,225   |
| AR501900             | REMOVE PCC PAVEMENT                | SY   | 16,225   |
| AR605510             | JOINT SEALING FILLER               | LF   | 16,200   |
| AR620510             | PAVEMENT MARKING                   | SF   | 3,000    |
| AR620595             | TEMPORARY MARKING & REMOVAL        | SF   | 815      |
| AR620900             | PAVEMENT MARKING REMOVAL           | SF   | 815      |
| AR701900             | REMOVE PIPE                        | LF   | 45       |
| AR705524             | 4" PERFORATED UNDERDRAIN W/SOCK    | LF   | 1,380    |
| AR751900             | REMOVE INLET                       | EA   | 1        |
| AR800204             | MOVE AIRLINE WORKSTATION           | EA   | 2        |
|                      |                                    |      |          |
| ADDITIVE ALTERNATE 1 |                                    |      |          |
| ITEM NO.             | ITEM DESCRIPTION                   | UNIT | QUANTITY |
| AS152410             | UNCLASSIFIED EXCAVATION            | CY   | 315      |
| AS156513             | SEPARATION FABRIC                  | SY   | 1,015    |
| AS208540             | OVERSIZE AGGREGATE                 | TON  | 775      |
| AS209604             | CRUSHED AGG. BASE COURSE - 4"      | SY   | 1,015    |
| AS501518             | 18" PCC PAVEMENT                   | SY   | 1,015    |
| AS501900             | REMOVE PCC PAVEMENT                | SY   | 1,015    |
| AS605510             | JOINT SEALING FILLER               | LF   | 1,300    |
|                      |                                    |      |          |
| ADDITIVE ALTERNATE 2 |                                    |      |          |
| ITEM NO.             | ITEM DESCRIPTION                   | UNIT | QUANTITY |
| AT152410             | UNCLASSIFIED EXCAVATION            | CY   | 415      |
| AT156513             | SEPARATION FABRIC                  | SY   | 1,345    |
| AT208540             | OVERSIZE AGGREGATE                 | TON  | 1,025    |
| AT209604             | CRUSHED AGGREGATE BASE COURSE - 4" | SY   | 1,345    |
| AT501518             | 18" PCC PAVEMENT                   | SY   | 1,345    |
| AT501900             | REMOVE PCC PAVEMENT                | SY   | 1,345    |
| AT605510             | JOINT SEALING FILLER               | LF   | 1,750    |
|                      |                                    |      |          |
| ADDITIVE ALTERNATE 3 |                                    |      |          |
| ITEM NO.             | ITEM DESCRIPTION                   | UNIT | QUANTITY |
| AU152410             | UNCLASSIFIED EXCAVATION            | CY   | 455      |
| AU156513             | SEPARATION FABRIC                  | SY   | 1,480    |
| AU208540             | OVERSIZE AGGREGATE                 | TON  | 1,125    |
| AU209604             | CRUSHED AGGREGATE BASE COURSE - 4" | SY   | 1,480    |
| AU501518             | 18" PCC PAVEMENT                   | SY   | 1,480    |
| AU501900             | REMOVE PCC PAVEMENT                | SY   | 1,480    |
| AU605510             | JOINT SEALING FILLER               | LF   | 1,425    |

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**LEGEND**

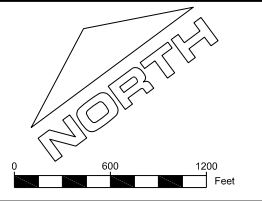
- AREA OF CONSTRUCTION
- CONTRACTOR'S STAGING, STORAGE & PARKING AREA
- CONTRACTORS ACCESS

**GENERAL NOTES**

1. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATION SHOWN ON THE CONSTRUCTION ACTIVITY PLAN OVERVIEW. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS, AND STOCKPILES SHALL BE 25 FEET ABOVE GROUND ELEVATION AND SHALL BE STORED A MINIMUM OF 10 FEET FROM THE AIRPORT FENCE.
2. ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF AN ESCORT OR A FLAGMAN IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED MAAP/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
3. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT DIRECTOR OF OPERATIONS.
4. EXCESS EARTH, BROKEN ASPHALT, AND CONCRETE SHALL BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
5. ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
6. ALL DISTURBED AREAS WITHIN THE PROJECT LIMITS SHALL BE SEEDED AND STABILIZED. ANY AREAS DISTURBED BY THE CONTRACTOR OUTSIDE THE LIMITS SHOWN SHALL BE SEEDED & STABILIZED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
7. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC.
8. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE RESIDENT ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
9. SEE CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS SHEET AND CONSTRUCTION ACCESS SHEET FOR NOTES REGARDING THE CONTRACTOR'S ACCESS AND CONSTRUCTION ACTIVITY PLANS FOR DETAILED PHASING REQUIREMENTS.
10. CONTRACTOR SHALL BE EXPLICITLY RESTRICTED FROM USING THE RING ROAD DIRECTLY IN FRONT OF THE MAIN TERMINAL BUILDING AND THE MAIN TERMINAL PARKING LOT FOR CONSTRUCTION ACCESS OF ANY KIND UNLESS OTHERWISE NOTED IN THE PLANS. THE CONTRACTOR SHALL BE REQUIRED TO STAGE CONSTRUCTION AROUND THE USE OF THESE AREAS AT ALL TIMES. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR'S VEHICLES OR EQUIPMENT BE ALLOWED TO USE THESE AREAS.
11. CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE LIGHTED, MARKED, AND FLAGGED PER SECTION 70-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25 FEET.
12. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT PUBLIC AND PRIVATE PROPERTY INCLUDING UTILITIES AND STRUCTURES TO REMAIN. IF AT ANY TIME, THE CONTRACTOR DAMAGES OR DESTROYS ANY OTHER PUBLIC OR PRIVATE PROPERTY, THE CONTRACTOR SHALL, AT HIS OWN EXPENSE, RESTORE SUCH PROPERTY TO A CONDITION EQUAL TO THAT OF EXISTING BEFORE SUCH DAMAGE. PUBLIC ROADWAYS, PRIVATE ENTRANCES, AND OTHER FACILITIES SHALL REMAIN ACCESSIBLE AND FREE OF CONSTRUCTION DEBRIS. ANY MATERIAL TO BE STAGED OR STOCKPILED SHALL BE COORDINATED WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER.
13. IF EXISTING ROADWAY SIGNS INTERFERE WITH THE INSTALLATION OF THE WORK FOR THIS PROJECT, SUCH SIGNS AND POSTS SHALL BE REMOVED AND REPLACED AFTER CONSTRUCTION. THE COST OF REMOVING AND STORING EXISTING SIGNS WILL NOT BE PAID FOR DIRECTLY. THE COST SHALL BE INCLUDED IN OTHER PAY ITEMS.
14. ACCESS TO ALL ENTRANCES SHALL BE MAINTAINED AT ALL TIMES UNLESS NOTED OTHERWISE ON THE PLANS.
15. EXCESS EARTH MATERIAL FROM THE PROJECT SITE SHALL BE TESTED TO ENSURE THAT IT IS CLEAN FROM CONTAMINATES PRIOR TO PLACING IN THE STOCKPILE LOCATION. THE EXCESS EARTHWORK SHALL BE PLACED IN ACCORDANCE WITH ITEM 152 OF THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS PER THIS PROJECT.



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JUNE 3, 2016

**RECONSTRUCT TERMINAL APRON PHASE 2**



**GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL  
AIRPORT  
PEORIA, ILLINOIS**

| MARK   | DATE | DESCRIPTION |
|--|------|-------------|
| AIP PROJ. NO. 3-17-0080-XX                     |      |             |
| IL PROJ. NO. PIA-4542 CONTRACT NO. PE001       |      |             |
| CMT PROJECT NO: 15061-05                       |      |             |
| CAD DWG FILE: 15061-05_GI101.DWG               |      |             |
| DESIGNED BY: JMW                               |      |             |
| DRAWN BY: CMT                                  |      |             |
| CHECKED BY: APP                                |      |             |
| APPROVED BY: CHK                               |      |             |
| COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |      |             |

**AIRPORT SITE PLAN**



100% SUBMITTAL  
JUNE 3, 2016

RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER

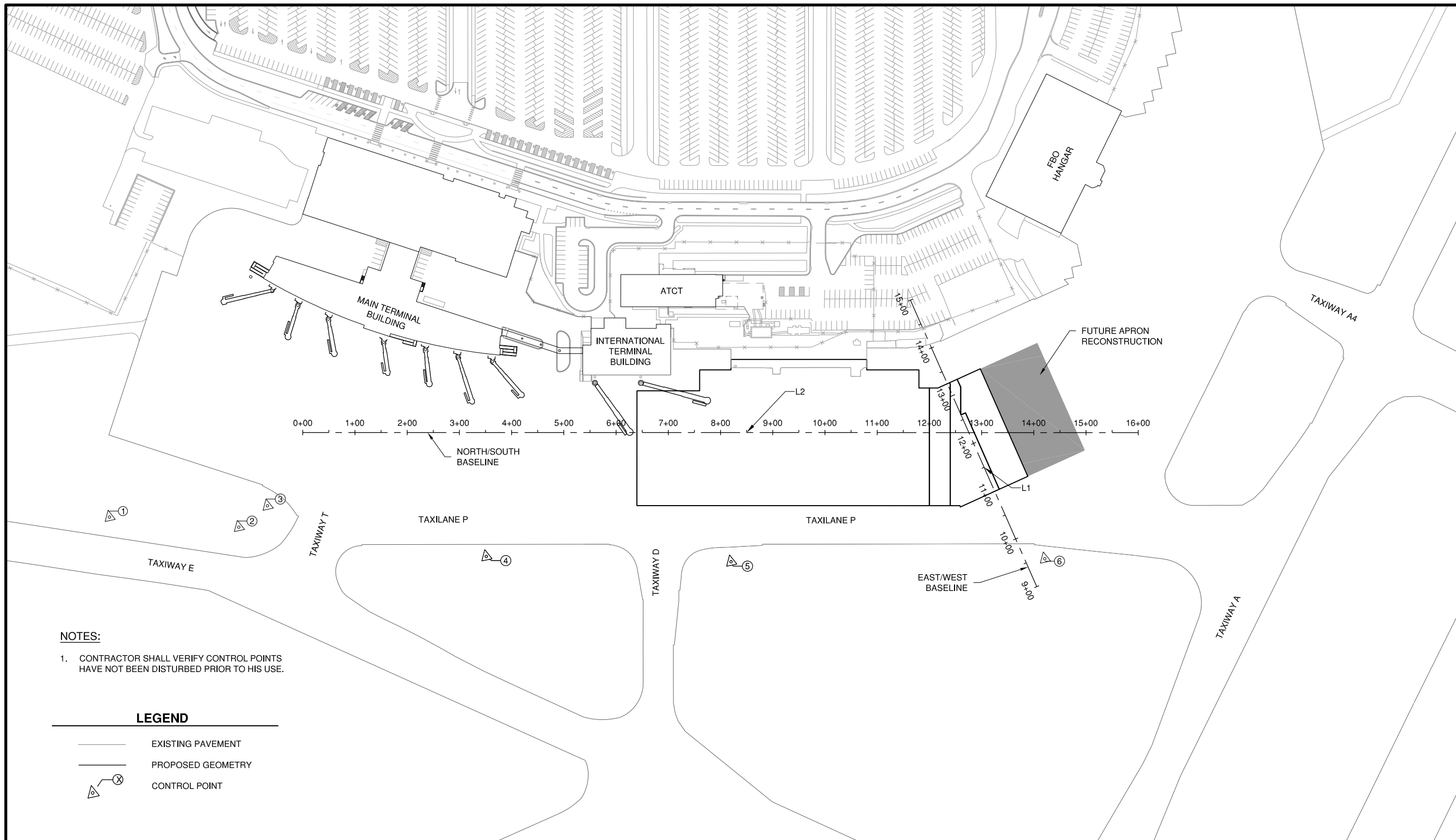


GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL  
AIRPORT  
PEORIA, ILLINOIS

| MARK | DATE | DESCRIPTION |
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| AIP PROJ. NO. 3-17-0080-XX                     |
| IL PROJ. NO. PIA-4542 CONTRACT NO. PE001       |
| CMT PROJECT NO: 15061-05                       |
| CAD DWG FILE: 15061-05_G1102.DWG               |
| DESIGNED BY: JMW                               |
| DRAWN BY: CMT                                  |
| CHECKED BY: APP                                |
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SHEET TITLE  
**CONTROL POINTS**



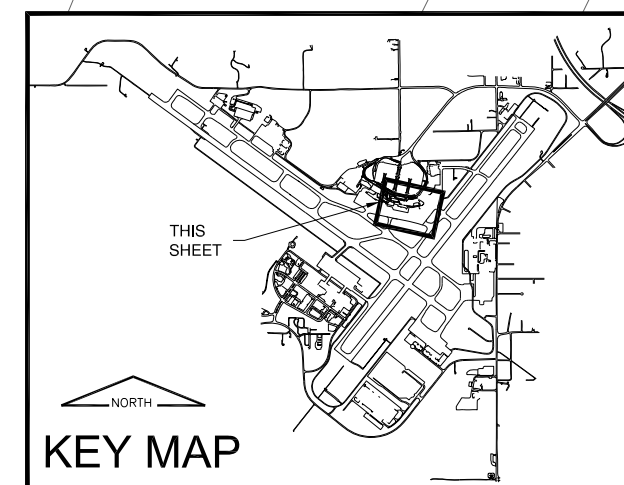
- NOTES:**
- CONTRACTOR SHALL VERIFY CONTROL POINTS HAVE NOT BEEN DISTURBED PRIOR TO HIS USE.

**LEGEND**

- EXISTING PAVEMENT
- PROPOSED GEOMETRY
- CONTROL POINT

| ALIGNMENT DATA    |                      |                           |                                  |                           |                                    |
|-------------------|----------------------|---------------------------|----------------------------------|---------------------------|------------------------------------|
| ALIGNMENT SEGMENT | ALIGNMENT            | BEGIN STATION/ PI STATION | NORTHING/ EASTING                | END STATION/ CURVE RADIUS | NORTHING/ EASTING                  |
| L2                | EAST/WEST BASELINE   | STA. 0+00.00              | N:1456674.2044<br>E:2428030.5281 | STA. 16+00.00             | N: 1456342.8320<br>E: 2429595.8371 |
| L1                | NORTH/SOUTH BASELINE | STA. 9+00.00              | N:1456094.2056<br>E:2429344.6098 | STA. 15+00.00             | N: 1456681.4605<br>E: 2429221.5990 |

| CONTROL POINT TABLE |             |             |           |             |
|---------------------|-------------|-------------|-----------|-------------|
| POINT               | NORTHING    | EASTING     | ELEVATION | DESCRIPTION |
| 1                   | 1456592.350 | 2427634.884 | 642.08    | IRON PIN    |
| 2                   | 1456522.208 | 2427873.088 | 643.62    | IRON PIN    |
| 3                   | 1456550.859 | 2427935.323 | 645.44    | 60D NAIL    |
| 4                   | 1456369.185 | 2428325.158 | 647.58    | IRON PIN    |
| 5                   | 1456262.221 | 2428781.719 | 649.45    | IRON PIN    |
| 6                   | 1456143.511 | 2429370.691 | 651.37    | IRON PIN    |



**KEY MAP**



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JUNE 3, 2016

RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER



GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL  
AIRPORT  
PEORIA, ILLINOIS

MARK | DATE | DESCRIPTION

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SHEET TITLE

CONSTRUCTION  
SAFETY PHASING  
PLAN NOTES

GC001  
SHEET 5 OF 32

**12. PENALTIES**

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

**13. SPECIAL CONDITIONS**

- ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

**14. RUNWAY AND TAXIWAY VISUAL AIDS**

- NO RUNWAY CLOSURES WILL BE REQUIRED FOR THIS PROJECT. HOWEVER, A TAXIWAY CLOSURE IS REQUIRED FOR THIS PROJECT. WHEN THE TAXIWAY CLOSURE IS COORDINATED AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2F (OR LATEST VERSION).
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

**15. HAZARD MARKING AND LIGHTING**

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F (OR LATEST VERSION) AND 150/5210-5C (OR LATEST VERSION) AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

**16. PROTECTION**

- ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 129.5' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED.
- ALL WORK REQUIRED ON AN ACTIVE TAXILANE OR INSIDE AN ACTIVE TAXILANE OBJECT FREE AREA, WHICH EXTENDS 112.5' FROM THE TAXILANE CENTERLINE, WILL REQUIRE THE TAXILANE TO BE CLOSED.
- TAXIWAY/TAXILANE OBJECT FREE AREA DISTANCES MAY BE LESS THAN STATED IN NOTES 1 AND 2 FOR CERTAIN PHASES AND NOTED IN THIS PLAN SET. THOSE OBJECT FREE AREA DISTANCES HAVE BEEN DETERMINED BASED ON SPECIFIC AIRCRAFT THAT TAXI IN THOSE PHASE LOCATIONS.
- THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

**17. OTHER LIMITATIONS ON CONSTRUCTION**

- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

**6. WILDLIFE MANAGEMENT**

- THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL ALWAYS BE MONITORED WITH A GATE GUARD WHEN IN USE.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED AND LOCKED WHEN THE CONTRACTOR IS NOT WORKING.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

**7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT**

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

**8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT**

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

**9. NOTIFICATION OF CONSTRUCTION ACTIVITIES**

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
- CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.

DIRECTOR OF OPERATIONS  
DOUG PALMER 309-303-0994

AIRPORT MAINTENANCE  
GREG HUSER 309-303-1005

AIRPORT OPERATIONS  
OPS CELL 309-303-1001

ENGINEER  
CHUCK TAYLOR, P.E. - PROJECT ENGINEER 217-787-8050  
CMT - RESIDENT ENGINEER 217-787-8050

**10. INSPECTION REQUIREMENTS**

- THE CONTRACTOR SHALL INSPECT THE JOB SITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F (OR LATEST VERSION) MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL REQUEST AN OPERATIONAL INSPECTION WITH THE AIRPORT OPERATIONS THROUGH THE RESIDENT ENGINEER OF EACH PHASE WORK AREA PRIOR TO THE AREA BEING REOPENED FOR AIRCRAFT USE. AIRPORT OPERATIONS SHALL MAKE THE FINAL DETERMINATION OF THE ACCEPTANCE TO REOPEN THIS WORK AREA. THIS WILL NOT BE CONSIDERED AS FINAL OR PARTIAL ACCEPTANCE OF THE WORK IN THIS PHASE.
- THE CONTRACTOR SHALL SUBMIT TO THE RESIDENT ENGINEER AN OFFICIAL 'SUBSTANTIALLY COMPLETE' LETTER WHEN HE FEELS THE PROJECT IS COMPLETE AND AT THE POINT OF HAVING A FINAL ACCEPTANCE. UPON THE RESIDENT ENGINEERS RECOMMENDATION TO IDA, A FINAL ACCEPTANCE OF THE PROJECT WILL THEN BE SCHEDULED.

**11. UNDERGROUND UTILITIES**

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

**5. CONTRACTOR ACCESS**

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN.
- CONTRACTOR EMPLOYEES SHALL OBTAIN AN AIRPORT IDENTIFICATION BADGE AS REQUIRED BY THE AIRPORT. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
- ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATIONS AREA (AOA) SHALL ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT. ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
- DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA, THE TRUCK DRIVERS MUST BE ESCORTED BY THE CONTRACTOR.
- CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES WHEN DRIVING/WORKING IN MOVEMENT AREAS OF THE AIRFIELD. THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS.
- FLAGMEN TO BE USED DURING THE CONSTRUCTION OF THE PROJECT IN THE LOCATIONS CALLED OUT IN THE PLAN SET SHALL BE FIELD ENDORSED BY THE AIRPORT.
- THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF NORMAL EMERGENCY ACCESS TO THE AIRPORT. CHANGES SHALL BE MADE TO MAINTAIN EMERGENCY ACCESS TO THE AIRPORT AT ALL TIMES WITH THE APPROVAL OF THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY.

**GENERAL**

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F (OR LATEST VERSION), AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F (OR LATEST VERSION). NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP IS SPECIALIZED TO AIRPORT OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEANS AND METHODS THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

**1. COORDINATION**

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

**2. PHASING**

- TOTAL CONTRACT TIME SHALL BE 125 CALENDAR DAYS IF ONLY THE BASE BID IS AWARDED. HOWEVER, IF ALL THREE ADDITIVE ALTERNATES ARE AWARDED IN CONJUNCTION WITH THE BASE BID, THE CONTRACT TIME SHALL BE 150 CALENDAR DAYS. ADDITIVE ALTERNATE #1 FOR 7 CALENDAR DAYS, ADDITIVE ALTERNATE #2 FOR 9 CALENDAR DAYS, AND ADDITIVE ALTERNATE #3 FOR 9 CALENDAR DAYS.
- PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.

**3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY**

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

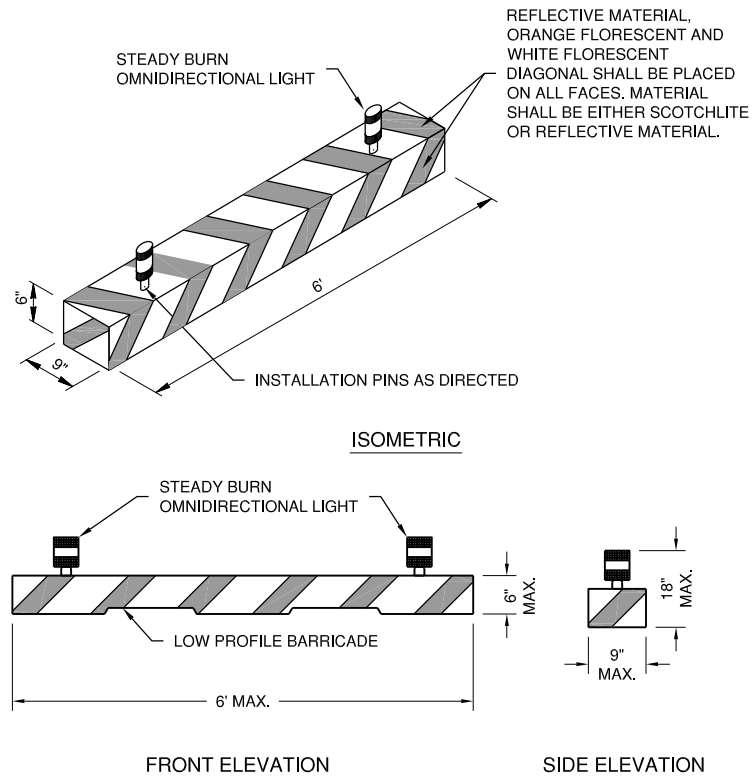
**4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)**

- THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

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**BEAM BARRICADE NOTES**

- BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
- BARRICADE SHALL BE EASILY COLLAPSIBLE UPON CONTACT WITH AIRCRAFT.
- PLACE BARRICADES END TO END AROUND CONSTRUCTION PHASE LIMITS UNLESS NOTED IN THE PLANS TO ALLOW ACCESS FOR ARFF VEHICLES AND GROUND CREW OPERATIONS.
- BARRICADES CLOSING TAXIWAYS AND CONSTRUCTION ACCESS ROUTES SHALL BE PLACED AT 15 FOOT INTERVALS.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.



**LOW PROFILE - BEAM BARRICADE DETAILS**

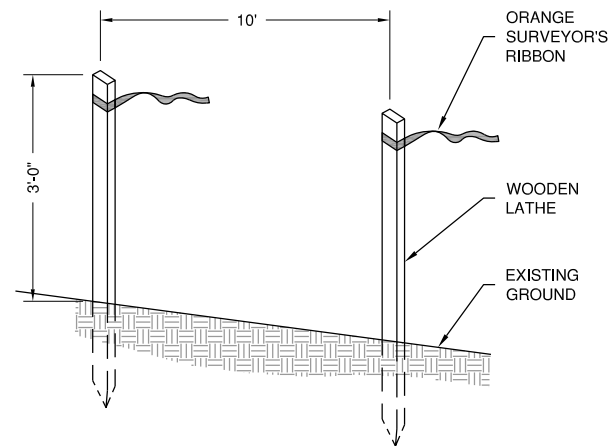
N.T.S.

**SECURITY NOTES**

- MAINTAINING THE SECURITY REQUIREMENTS OF THE AIRPORT SHALL BE A PRIMARY CONCERN FOR THE CONTRACTOR.
- AT NO TIME ARE UNSUPERVISED GATE OPENINGS PERMITTED AT THE AIRPORT. IF THE PROPOSED WORK REQUIRES GATE OPENING, THE CONTRACTOR SHALL PROVIDE A GUARD TO PREVENT UNAUTHORIZED ACCESS. THE CONTRACTOR SHALL SECURE THE GATE AT THE COMPLETION OF THE WORK DAY.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING AIRPORT SECURITY BY SUPERVISING OPENINGS OR MAINTAINING THE AIRPORT PERIMETER FENCE LINE AT ALL TIMES DURING THE COURSE OF THE WORK.
- FINES CAN BE LEVIED AGAINST THE CONTRACTOR BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) FOR NEGLIGENCE IF THE AIRPORT SECURITY IS COMPROMISED AND THE AIRPORT PERIMETER FENCE LINE IS NOT MAINTAINED AS SPECIFIED ABOVE. FINES CAN ALSO BE LEVIED AGAINST THE CONTRACTOR FOR FAILURE TO COOPERATE WITH THE AIRPORT MANAGEMENT AS REQUIRED TO MAINTAIN AIRPORT SECURITY.

**MOVE AIRLINE WORKSTATION NOTES:**

- PHASE 2 AND 3 WILL REQUIRE MOVING OF AIRLINE COMPUTER WORKSTATIONS IN ORDER TO FACILITATE THE LIMITS OF WORK NEEDED TO CONSTRUCT EACH PHASE OF THIS PROJECT.
- IN THE FOLLOWING CONSTRUCTION ACTIVITY PLAN SHEETS, IT HAS BEEN OUTLINED THAT GATE 14 WILL TEMPORARILY USE GATE 12 DURING PHASE 2. AT THE END OF PHASE 2, THE AIRCRAFT WILL MOVE BACK TO GATE 14.
- THE FOLLOWING STEPS WILL BE INVOLVED IN THE MOVING OF AN AIRLINE:
  - WHEN AIRCRAFT ARRIVES FROM LAST FLIGHT IT SHALL BE PARKED AT NEW TEMPORARY GATE LOCATION PRIOR TO STARTING WORK IN NEW PHASE WORK AREA PER THAT PHASE NOTES (SEE CONSTRUCTION ACTIVITY PLAN 2) .
  - PRIOR TO ANY DISCONNECTIONS, THE RESIDENT ENGINEER SHALL BE PRESENT AND DOCUMENT WITH PICTURES AND SKETCHES OF ALL CONNECTIONS AT THE WORKSTATION LOCATIONS, INTERMEDIATE DISTRIBUTION FRAME (IDF) ROOM(S) AND MAIN DISTRIBUTION FRAME (MDF) ROOM (LAN ROOM LOCATIONS INFORMATION WILL BE OBTAINED FROM THE AIRPORT).
  - DISCONNECT WORKSTATION (COMPUTER, KEYBOARD, MOUSE, MONITOR, SCANNER, PRINTER) FROM TERMINAL NETWORK AT EXISTING GATE.
  - MOVE WORKSTATION TO NEW TEMPORARY GATE LOCATION.
  - CONNECT WORKSTATION TO TERMINAL NETWORK AT THE PODIUM.
  - MAKE NECESSARY CHANGES TO THE PATCH PANELS AND SWITCHES IN THE IDF ROOM(S) AND MDF ROOM FOR THE FOLLOWING SYSTEMS: INTERNET/AIRLINE NETWORK CONNECTION, PAGING SYSTEM, DOOR ACCESS CONTROL, AND MUFIDS.
  - TEST SYSTEM WITH AN AIRLINE REPRESENTATIVE ONCE THE MOVING OF THE WORKSTATION AND THE CONNECTIONS IN THE IDF AND MDF ROOMS HAVE BEEN FINISHED TO ENSURE PROPER OPERATION PRIOR TO USING THE SYSTEM IN THE MORNING FOR THE FIRST DEPARTING FLIGHT AT THE NEW TEMPORARY GATE LOCATION.
- IF THE SYSTEM DOES NOT WORK AFTER BEING MOVED THEN CONSTRUCTION WORK ON THE APRON SHALL NOT INITIATE UNTIL THE AIRLINE WORKSTATION ISSUE HAS BEEN RESOLVED.



**CONSTRUCTION SETBACK LINE DETAIL**

N.T.S.

**CONSTRUCTION SETBACK LINE NOTES**

- CONSTRUCTION SETBACK LINE SHALL BE INSTALLED WHERE SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

**CONSTRUCTION ACTIVITY PLAN GENERAL NOTES**

- THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK SO AS TO NOT INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS.
- THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS.
- IT SHALL BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECT IS THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS.
- WHEN CONFLICTS ARISE, RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT THROUGH THE RESIDENT ENGINEER IN THE BEST INTEREST OF THE AIRPORT.
- DELAYS, CHANGES IN SCHEDULING OR THE EXPEDITION OF WORK UNDER THIS CONTRACT TO PROVIDE FOR THE TIMELY PROSECUTION OF THE PROJECT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ALL PERIMETER ROADS INSIDE THE FENCE SHALL REMAIN ACCESSIBLE TO THE AIRPORT.
- THE CONTRACTOR SHALL BE AWARE THAT DAILY OPERATION OF THE AIRPORT WILL CONTINUE THROUGHOUT THIS PROJECT. AS A RESULT, DAILY COORDINATION WILL BE NECESSARY TO LIMIT DISRUPTION TO AIRPORT/TENTANT AND CONTRACTOR OPERATIONS.
- CONTRACTOR'S STAGING, STORAGE, AND PARKING WILL BE AS SHOWN ON THE AIRPORT SITE PLAN.
- THE CONTRACTOR SHALL PLACE ALL BARRICADES, CONSTRUCTION SETBACK LINES, AND EROSION CONTROL ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- CONSTRUCTION PHASING IS OF CRITICAL IMPORTANCE TO THE AIRPORT FOR THIS PROJECT. LIQUIDATED DAMAGES MAY BE SOUGHT FOR EXCEEDING THE CALENDAR DAYS ALLOWED FOR EACH PHASE.
- CONSTRUCTION RELATED ITEMS REQUIRING THE CLOSURE OF RUNWAYS AND/OR TAXIWAYS SHALL REQUIRE A MINIMUM OF 72 HOURS NOTICE TO COORDINATION WITH THE AIRPORT AND FAA. NO EXTENSION TO CONTRACT TIME WILL BE GIVEN FOR DELAYS CAUSED BY LACK OF ADEQUATE COORDINATION.
- CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
  - THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
  - THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE AIRPORT OPERATIONS BY THE CONTRACTOR.
  - THE CONTRACTOR SHALL USE AN EXISTING GATE(S), OR NEW GATES AS CALLED OUT IN THE PLANS, FOR ACCESS TO THE AIRFIELD.
  - CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
  - DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED AND LOCKED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
  - THE CONTRACTOR SHALL CLOSE AND SECURE THE ACCESS GATE(S) UPON LEAVING THE SITE.
  - ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
  - THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE USED BY HIS VEHICLES AND CONSTRUCTION EQUIPMENT ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE OR CONSTRUCTION EQUIPMENT.
  - THE CONTRACTOR SHALL EVALUATE AND PRESERVE THE CONDITION OF THE EXISTING AIRPORT PERIMETER ROAD AND ALL OTHER ACCESS ROUTES UTILIZED BY THE CONTRACTOR FOR ACCESS TO THE SITE. THE CONTRACTOR SHALL DOCUMENT THE CONDITION OF THE EXISTING PAVEMENT IN THE PRESENCE OF THE AIRPORT AND RESIDENT ENGINEER PRIOR TO THE INITIATION OF CONSTRUCTION. AT THE COMPLETION OF CONSTRUCTION, ANY PAVEMENT USED BY THE CONTRACTOR THAT IS SAID TO BE DAMAGED BY HIS OPERATIONS SHALL BE REPAIRED TO ITS ORIGINAL CONDITION AND/OR TO THE SATISFACTION OF THE AIRPORT AT THE CONTRACTOR'S EXPENSE.



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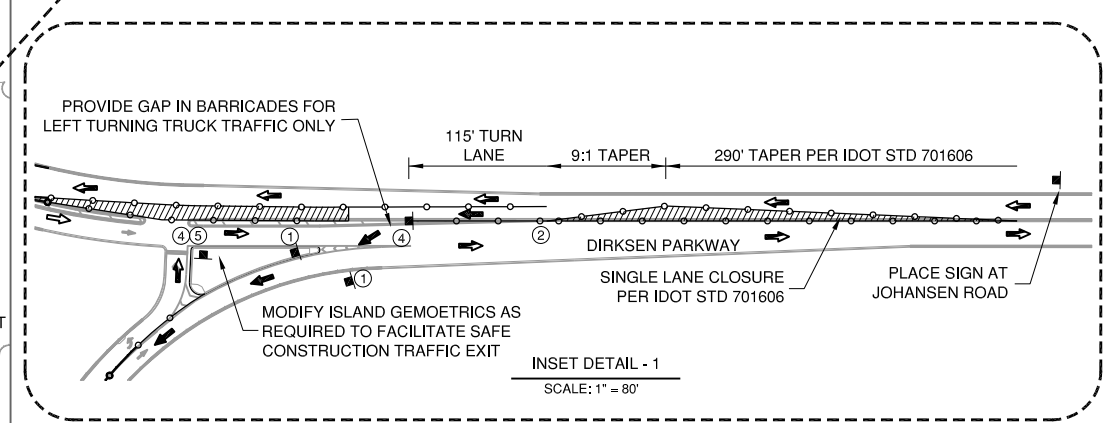
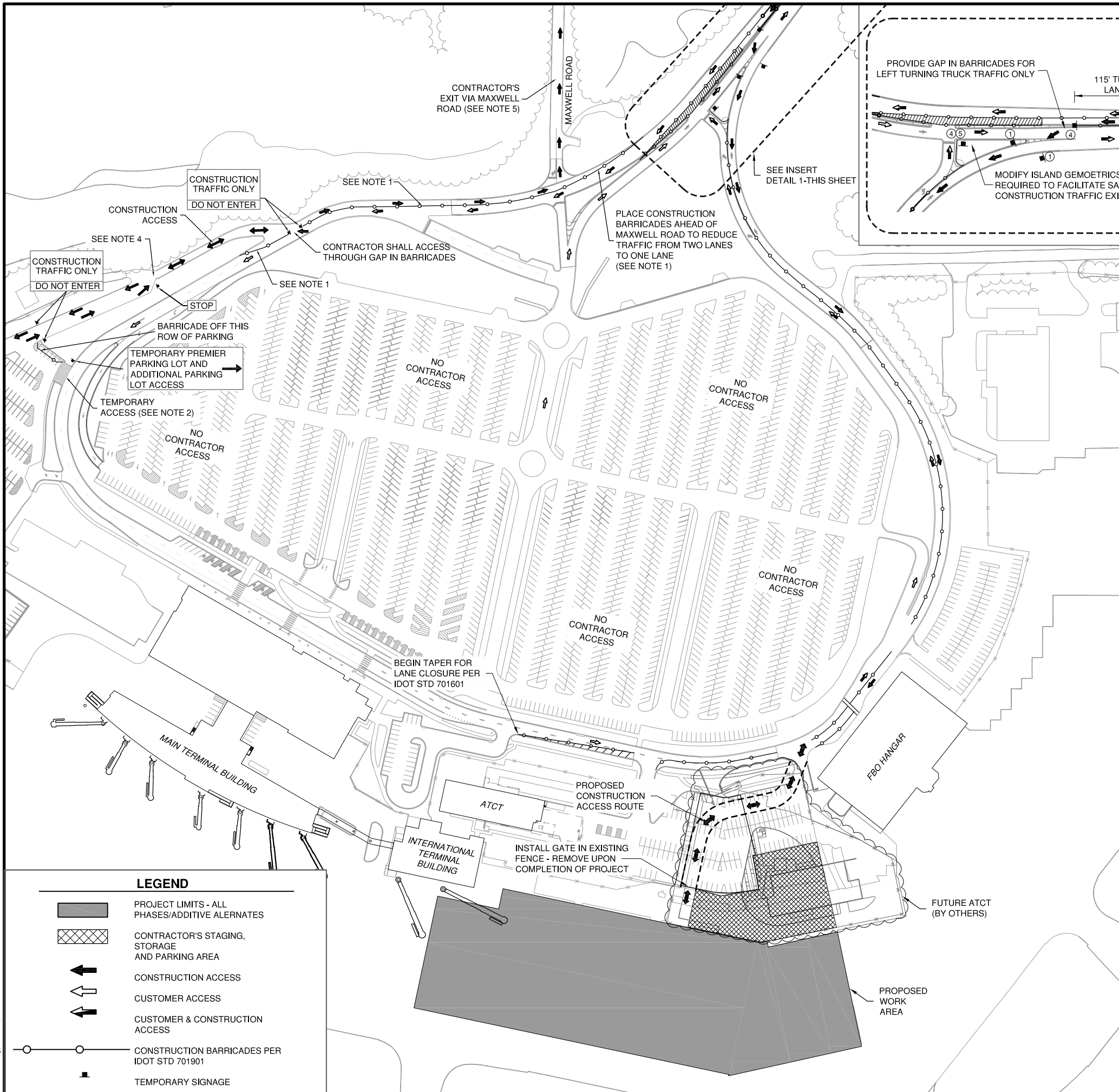
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| AIP PROJ. NO. 3-17-0080-XX                     |
| IL PROJ. NO. PIA-4542 CONTRACT NO. PE001       |
| CMT PROJECT NO: 15061-05                       |
| CAD DWG FILE: 15061-05_GC002.DWG               |
| DESIGNED BY: JMW                               |
| DRAWN BY: CMT                                  |
| CHECKED BY: APP                                |
| APPROVED BY: CHK                               |
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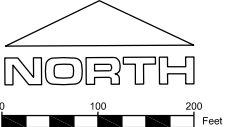
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CONSTRUCTION  
ACTIVITY PLAN  
NOTES AND DETAILS



**CONSTRUCTION STAGING / ACCESS NOTES**

1. PLACEMENT OF BARRICADES AND TRAFFIC SIGNAGE NECESSARY FOR THE REDUCTION OF TRAFFIC FROM TWO LANES TO ONE LANE SHALL MEET IDOT STANDARD 701606 REQUIREMENTS FOR LONG TERM LANE CLOSURES. CONTRACTOR SHALL MAINTAIN MINIMUM 11 FOOT WIDE LANES AT ALL TIMES.
2. CONTRACTOR SHALL USE EXISTING TEMPORARY ACCESS ROAD IN PLACE. CONTRACTOR SHALL MAINTAIN THIS TEMPORARY ROAD THROUGHOUT THE DURATION OF THIS PROJECT. THIS TEMPORARY ROAD IS TO BE USED BY PUBLIC/NON-CONSTRUCTION TRAFFIC ONLY.
3. THE SIGNAGE SHOWN ON THIS SHEET SHALL BE IN ADDITION TO ALL TEMPORARY CONSTRUCTION TRAFFIC SIGNAGE REQUIRED BY IDOT STANDARDS FOR LANE CLOSURES WHICH ARE NOT SHOWN.
4. CONTRACTOR SHALL PLACE A TEMPORARY STOP SIGN DIRECTING ALL CONSTRUCTION TRAFFIC EXITING THE SITE TO STOP AND ALLOW ALL ONCOMING CONSTRUCTION TRAFFIC TO PROCEED PRIOR TO PROCEEDING THROUGH THE 2-WAY 1-LANE ZONE. THE CONTRACTOR SHALL USE EXTREME CAUTION IN THIS AREA AND SHALL REQUIRE COORDINATED COMMUNICATION WITH ALL INDIVIDUALS USING THIS ACCESS.
5. CONTRACTOR SHALL PROVIDE SIGNAGE TO TEMPORARILY CLOSE AIRPORT ACCESS FOR THE GENERAL PUBLIC FROM MAXWELL ROAD. MAXWELL ROAD SHALL BE CLOSED TO THRU TRAFFIC IN ACCORDANCE WITH IDOT STANDARD 701901.
6. MAINTENANCE OF TRAFFIC CONTROL ITEMS AND TEMPORARY ROAD MILLINGS SHALL BE MAINTAINED AT ALL TIMES THROUGHOUT THE PROJECT DURATION AND SHALL BE PAID FOR UNDER ITEM AR150520 CONSTRUCTION ACCESS.
7. THE CONTRACTOR'S ACCESS TO THE PROJECT LIMITS SHALL BE IN ACCORDANCE WITH DETAILS ON THE CONSTRUCTION ACTIVITY PLAN OVERVIEW SHEET (GC100). CONSTRUCTION TRAFFIC SHALL BE PROHIBITED FROM UTILIZING THE DRIVE IN FRONT OF THE MAIN TERMINAL AND THE MAIN PARKING LOT AT ALL TIMES.



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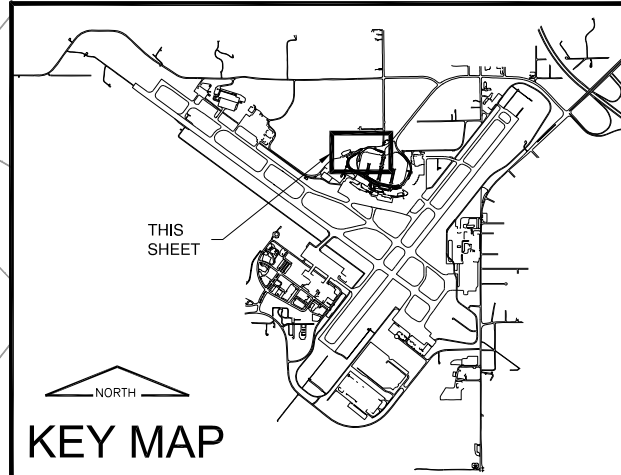
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| IL PROJ. NO. PIA-4542                          |      | CONTRACT NO. PE001 |
| CMT PROJECT NO:                                |      | 15061-05           |
| CAD DWG FILE:                                  |      | 15061-05_GC003.DWG |
| DESIGNED BY:                                   |      | JMW                |
| DRAWN BY:                                      |      | CMT                |
| CHECKED BY:                                    |      | APP                |
| APPROVED BY:                                   |      | CHK                |
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**CONSTRUCTION  
ACCESS**

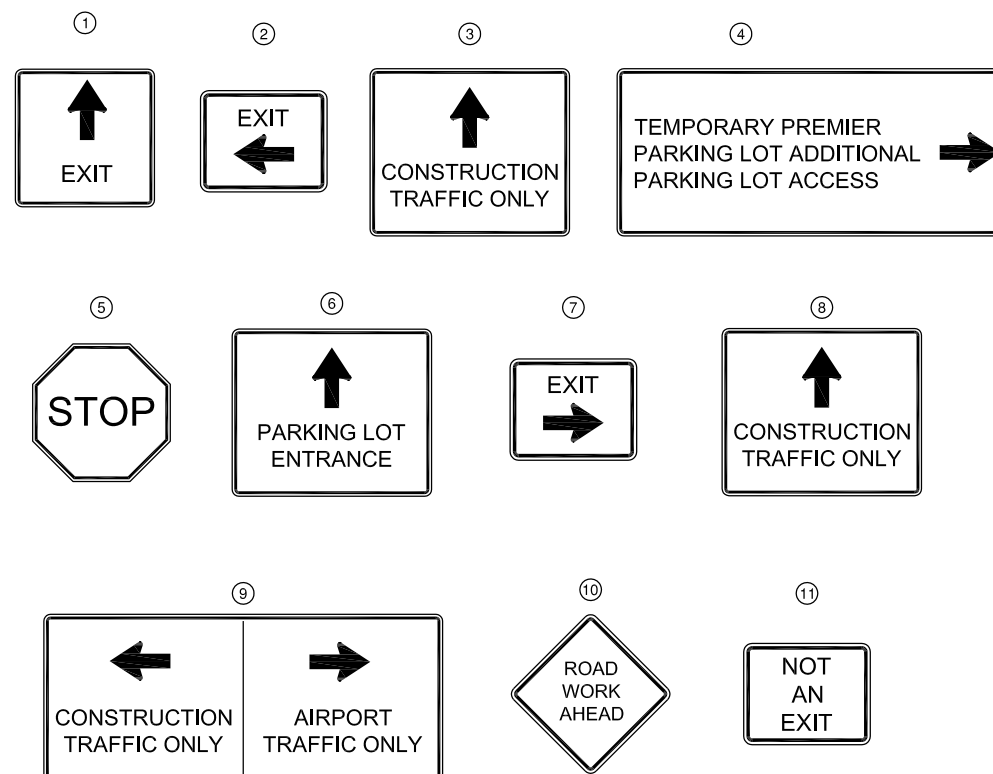


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**TEMPORARY CONSTRUCTION SIGNAGE NOTES**

1. ALL SIGNAGE SHALL BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED APRIL 1, 2016, THE DETAILS IN THESE PLANS AND THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
2. ALL SIGNS SHALL BE PROVIDED, PLACED AND MAINTAINED BY THE CONTRACTOR.
3. THE TRAFFIC CONTROL STANDARDS PROVIDED IN THESE PLANS MAY REQUIRE MODIFICATION AT TIMES. THE AIRPORT SHALL APPROVE ANY MODIFICATIONS PROPOSED BY THE CONTRACTOR.
4. ALL EXISTING SIGNAGE THAT IS NOT APPLICABLE TO THE TRAFFIC CONTROL IN PLACE SHALL BE REMOVED OR COMPLETELY COVERED.
5. ALL WORK ZONE SIGNS ARE REQUIRED TO MEET, AT A MINIMUM, TYPE B REFLECTIVITY REQUIREMENTS OF TABLE 1091-2 IN ARTICLE 1091.02 OF THE IDOT STANDARD SPECIFICATIONS.

NOTE: REFER TO CONSTRUCTION ACTIVITY PLANS FOR NUMBERED LOCATIONS



**TEMPORARY CONSTRUCTION SIGNAGE**

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SHEET TITLE  
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ACCESS DETAILS**





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APRON PHASE 2

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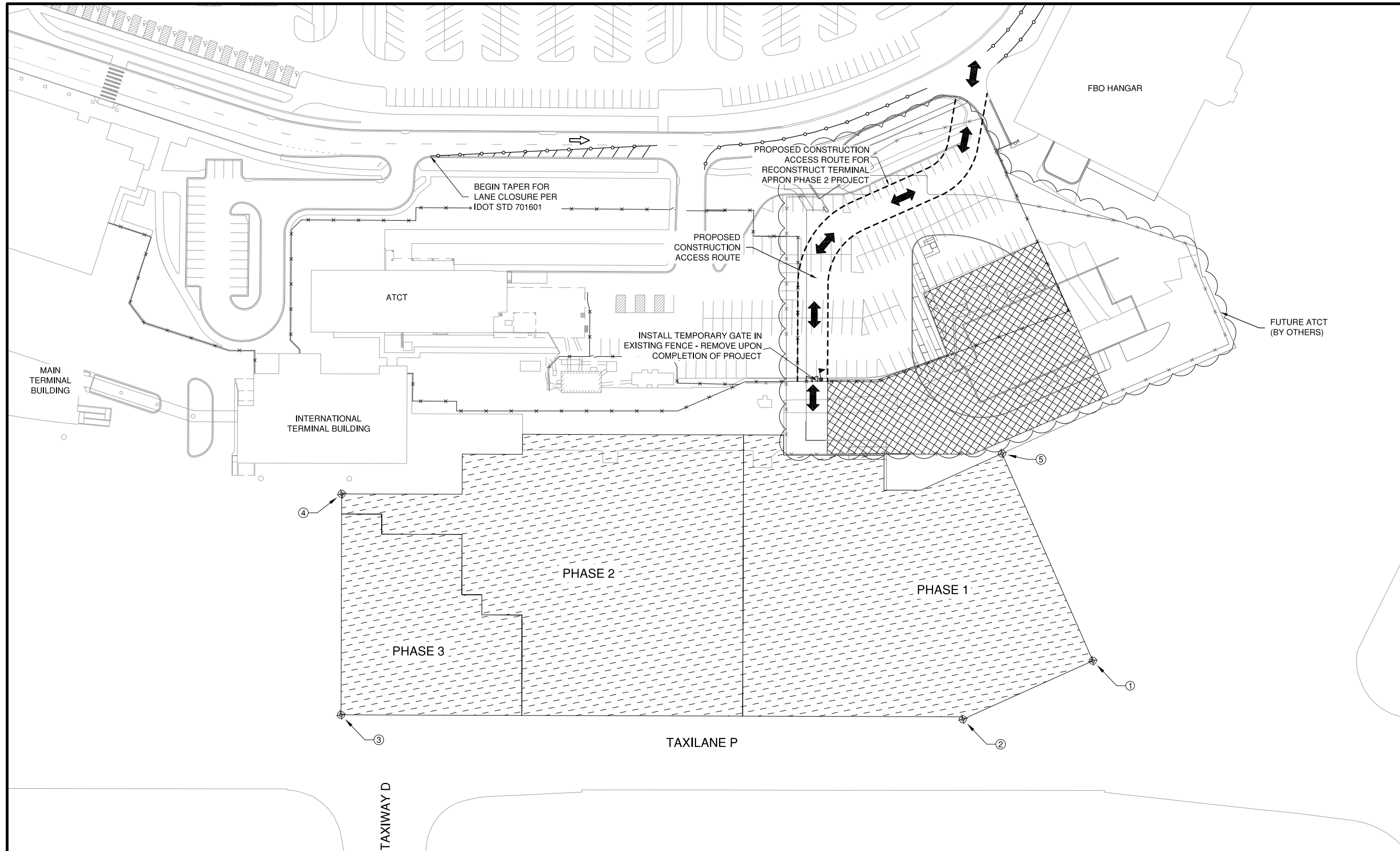


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| IL PROJ. NO. PIA-4542 CONTRACT NO. PE001       |
| CMT PROJECT NO: 15061-05                       |
| CAD DWG FILE: 15061-05_GC100.DWG               |
| DESIGNED BY: JMW                               |
| DRAWN BY: CMT                                  |
| CHECKED BY: APP                                |
| APPROVED BY: CHK                               |
| COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |

SHEET TITLE  
**CONSTRUCTION  
ACTIVITY PLAN  
OVERVIEW**



CRITICAL POINT TABLE

| POINT | LATITUDE        | LONGITUDE       | ELEVATION | HEIGHT ABOVE | TOP ELEVATION |
|-------|-----------------|-----------------|-----------|--------------|---------------|
| 1     | N40° 39' 52.27" | W89° 41' 16.80" | 652.51    | 25'          | 677.512       |
| 2     | N40° 39' 51.97" | W89° 41' 18.60" | 652.15    | 25'          | 677.149       |
| 3     | N40° 39' 53.30" | W89° 41' 26.45" | 651.33    | 25'          | 676.329       |
| 4     | N40° 39' 55.42" | W89° 41' 25.84" | 653.47    | 25'          | 678.472       |
| 5     | N40° 39' 54.45" | W89° 41' 17.38" | 650.07    | 25'          | 675.068       |

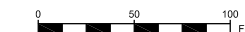
NOTES

- CONSTRUCTION ACCESS ROAD SHALL BE CONSTRUCTED OF MILLINGS OR SUITABLE EQUIVALENT MATERIAL AS SHOWN IN THE DETAIL ON TYPICAL SECTIONS 3.
- THE PAVEMENT STRUCTURE SHALL BE CAPABLE OF HANDLING HEAVY TRUCK TRAFFIC FOR THE DURATION OF THE PROJECT WITHOUT TRACKING FOD ONTO THE APRON. SHOULD FOD ACCUMULATE ON THE PAVEMENT, THE CONTRACTOR SHALL BROOM OFF AND MAKE CORRECTIVE ACTION TO PREVENT IT FROM REOCCURRING. SEE TYPICAL SECTIONS 3, SHEET 26.
- A MINIMUM OF 75 FEET OF THE CONSTRUCTION ACCESS ROAD ADJACENT TO THE THE APRON PAVEMENT EDGE SHALL BE PAVED BITUMINOUS SURFACE COURSE MATERIAL WITH A MINIMUM OF 4" IN THICKNESS.
- STOP SIGN SHALL BE MOUNTED ON A FRANGIBLE BASE AND APPROVED FOR USE ON AIRFIELDS. MAXIMUM HEIGHT FROM BASE OF SIGN TO TOP OF SIGN SHALL BE 24". SIGN SHALL BE PLACED 45FT FROM EDGE OF APRON PAVEMENT ALONG CONSTRUCTION ACCESS ROAD.

LEGEND

- WORK AREA
- CONTRACTOR'S STAGING AND STORAGE
- CONTRACTOR'S ACCESS
- CRITICAL POINT
- GATE GUARD (FLAGGER)
- CUSTOMER'S ACCESS

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Date: Thursday, June 16, 2016 2:51:58 PM



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JUNE 3, 2016

RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER

GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL AIRPORT

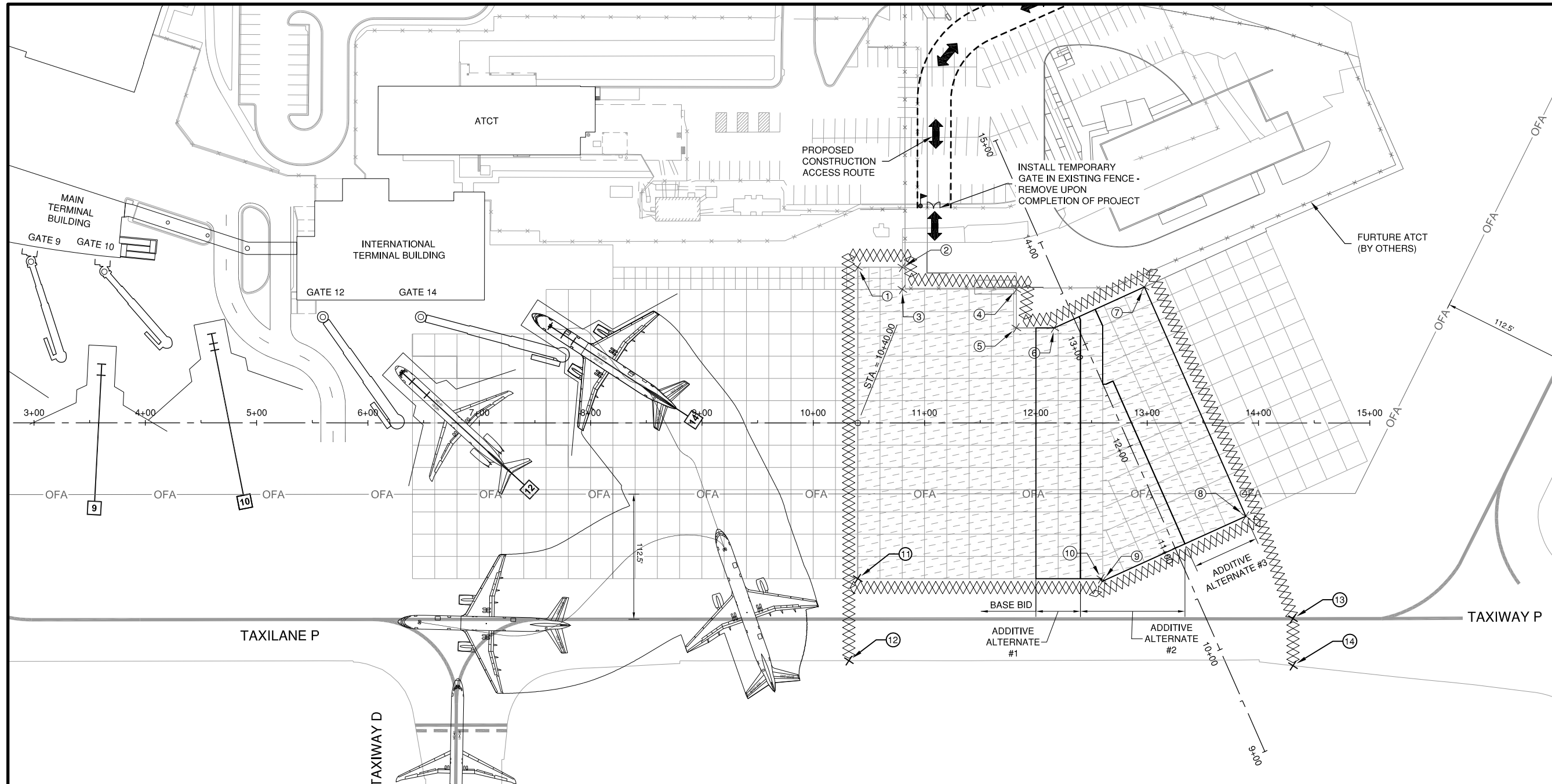
GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL  
AIRPORT  
PEORIA, ILLINOIS

| MARK | DATE | DESCRIPTION |
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| AIP PROJ. NO. 3-17-0080-XX                     |
| IL PROJ. NO. PIA-4542 CONTRACT NO. PE001       |
| CMT PROJECT NO: 15061-05                       |
| CAD DWG FILE: 15061-05_GC101.DWG               |
| DESIGNED BY: JMW                               |
| DRAWN BY: CMT                                  |
| CHECKED BY: APP                                |
| APPROVED BY: CHK                               |
| COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |

SHEET TITLE  
**CONSTRUCTION  
ACTIVITY PLAN 1**

GC101  
SHEET 10 OF 32

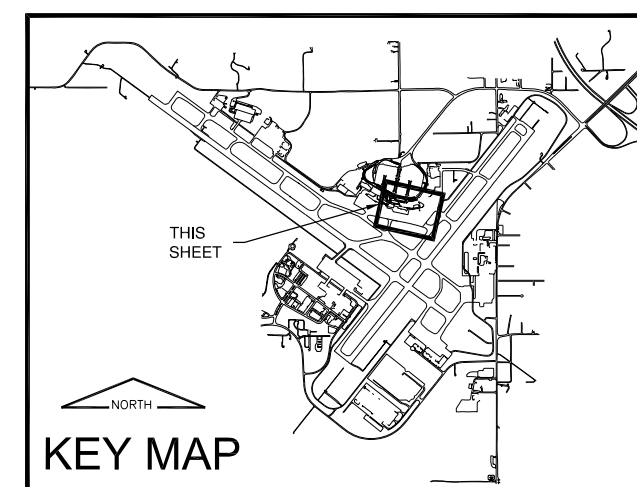


| PHASE LIMITS POINT TABLE |             |             |
|--------------------------|-------------|-------------|
| POINT                    | NORTHING    | EASTING     |
| 1                        | 1456595.777 | 2429076.974 |
| 2                        | 1456587.493 | 2429116.107 |
| 3                        | 1456567.926 | 2429111.965 |
| 4                        | 1456546.710 | 2429212.185 |
| 5                        | 1456512.868 | 2429205.056 |
| 6                        | 1456505.826 | 2429238.322 |
| 7                        | 1456525.113 | 2429325.203 |
| 8                        | 1456304.614 | 2429371.628 |
| 9                        | 1456274.153 | 2429233.763 |
| 10                       | 1456276.534 | 2429233.035 |
| 11                       | 1456321.848 | 2429018.984 |
| 12                       | 1456251.421 | 2428995.943 |
| 13                       | 1456205.731 | 2429394.071 |
| 14                       | 1456164.421 | 2429385.332 |

- PHASE 1 NOTES**
1. PRIOR TO STARTING WORK ON THE TERMINAL APRON, THE CONTRACTOR SHALL HAVE TRAFFIC CONTROL AND ACCESS ROUTES IN PLACE.
  2. THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR 7 DAYS PRIOR TO THE INITIATION OF PHASE 1.
  3. ANY AIRCRAFT USING GATE 12 OR 14 DURING THIS PHASE SHALL HAVE UNRESTRICTED ACCESS TO THESE GATES. THE CONSTRUCTION LIMITS FOR THIS PHASE MAY HAVE TO BE ALTERED TO ACCOMMODATE LARGER AIRCRAFT.
  4. ALL BARRICADES SHALL BE SET UP PRIOR TO THE INITIATION OF THIS PHASE AND SHALL REMAIN IN PLACE UNTIL COMPLETE AND THE PCC IS CURED.
  5. ALL WORK IN PHASE 1 SHALL BE COMPLETED PRIOR TO BEGINNING PHASE 2 WORK. HOWEVER, PHASE 2 DEMOLITION MAY BE CONCURRENT WITH THE PCC CURE TIME DURING PHASE 1.
  6. IF ONLY THE BASE BID IS AWARDED THEN THIS PHASE SHALL CONSIST OF 31 CALENDAR DAYS. IF ANY ONE OF THE ADDITIVE ALTERNATES ARE AWARDED THEN THIS PHASE SHALL CONSIST OF THE BASE BID CALENDAR DAYS PLUS 7 CAL DAYS FOR AA#1, 9 CAL DAYS FOR AA#2, AND 9 CAL DAYS FOR AA#3.
  7. WORK TO BE COMPLETED IN THIS PHASE SHALL INCLUDE: PCC REMOVAL AND PCC PAVEMENT STRUCTURE CONSTRUCTION.

**PHASE 1  
LEGEND**

- PHASE 1 WORK AREA
- CONTRACTOR'S ACCESS
- LOW PROFILE BARRICADES
- OFA/SETBACK LINE
- GATE GUARD (FLAGGER)
- AIRCRAFT ACCESS
- PHASE LIMITS



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JUNE 3, 2016

RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER

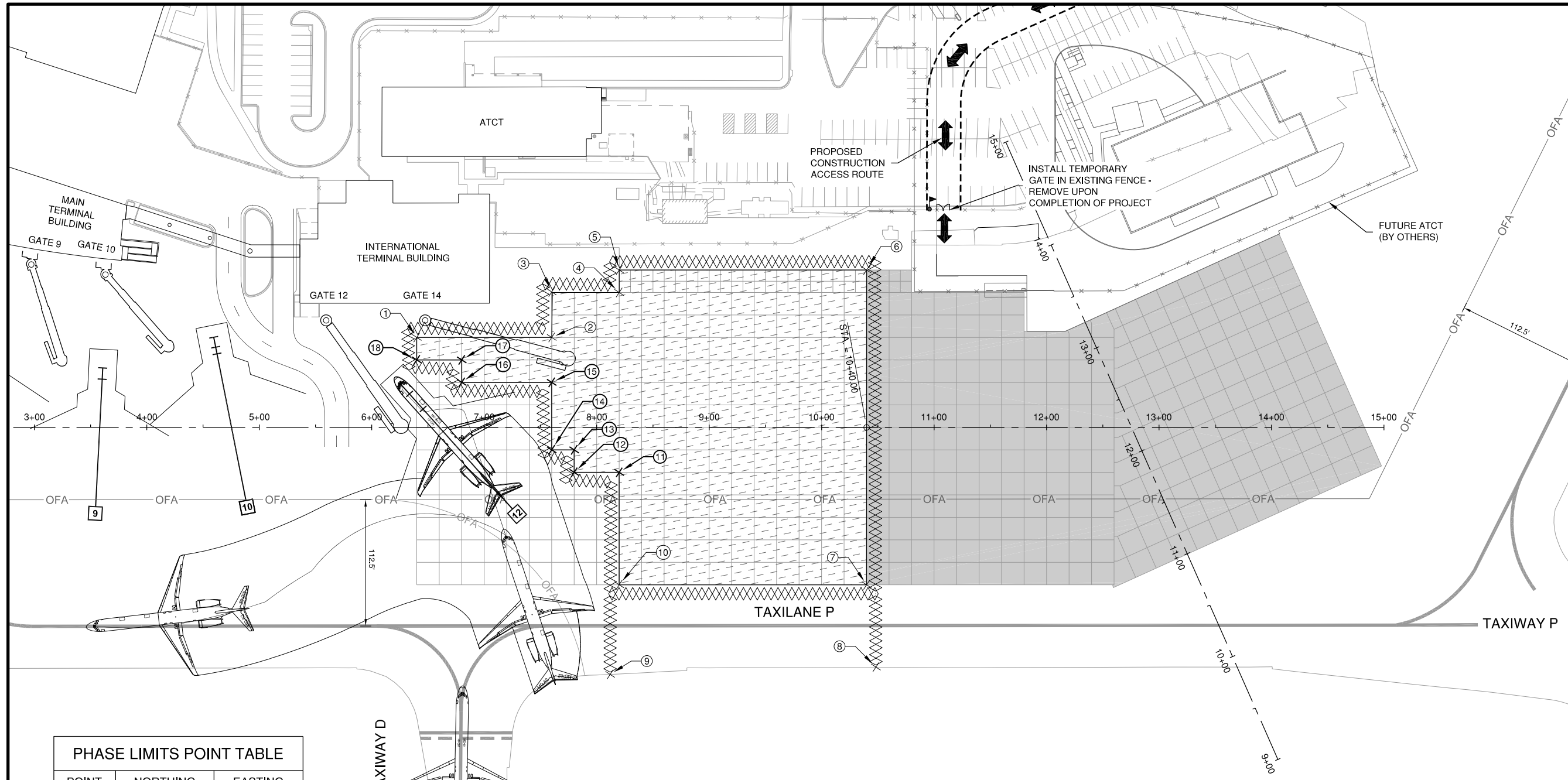


GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL  
AIRPORT  
PEORIA, ILLINOIS

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| AIP PROJ. NO.   | 3-17-0080-XX                        |
| IL PROJ. NO.    | PIA-4542 CONTRACT NO. PE001         |
| CMT PROJECT NO. | 15061-05                            |
| CAD DWG FILE:   | 15061-05_GC102.DWG                  |
| DESIGNED BY:    | JMW                                 |
| DRAWN BY:       | CMT                                 |
| CHECKED BY:     | APP                                 |
| APPROVED BY:    | CHK                                 |
| COPYRIGHT:      | CRAWFORD, MURPHY & TILLY, INC. 2015 |

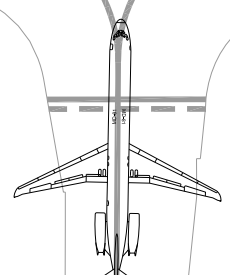
SHEET TITLE  
**CONSTRUCTION  
ACTIVITY PLAN 2**



PHASE LIMITS POINT TABLE

| POINT | NORTHING    | EASTING     |
|-------|-------------|-------------|
| 1     | 1456619.921 | 2428673.220 |
| 2     | 1456595.068 | 2428790.618 |
| 3     | 1456634.660 | 2428799.000 |
| 4     | 1456621.774 | 2428857.602 |
| 5     | 1456641.341 | 2428861.744 |
| 6     | 1456595.777 | 2429076.974 |
| 7     | 1456321.848 | 2429018.984 |
| 8     | 1456248.579 | 2429011.966 |
| 9     | 1456291.071 | 2428779.398 |
| 10    | 1456367.420 | 2428803.756 |
| 11    | 1456465.243 | 2428824.465 |
| 12    | 1456473.528 | 2428785.332 |
| 13    | 1456493.094 | 2428789.474 |
| 14    | 1456497.236 | 2428769.908 |
| 15    | 1456555.935 | 2428782.334 |
| 16    | 1456572.504 | 2428704.069 |
| 17    | 1456592.070 | 2428708.211 |
| 18    | 1456600.355 | 2428669.078 |

TAXIWAY D



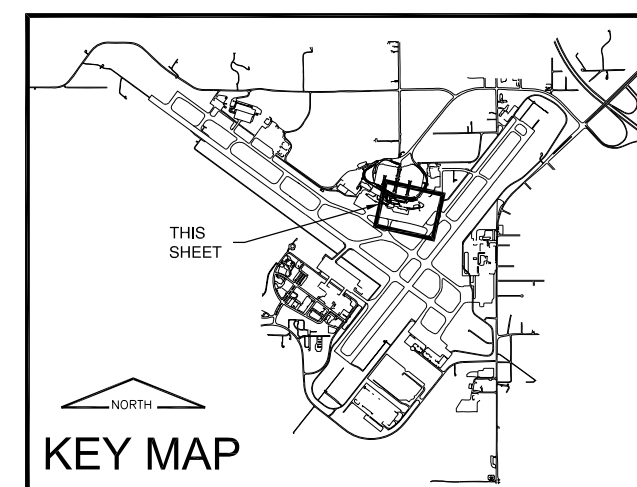
PHASE 2 NOTES

1. THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR 7 DAYS PRIOR TO THE INITIATION OF PHASE 2.
2. GATE 12 MARKING SHALL BE REMOVED AND TEMPORARY GATE 12 MARKED, PRIOR TO THE START OF PHASE 2.
3. ANY AIRCRAFT USING GATE 12 DURING THIS PHASE SHALL HAVE UNRESTRICTED ACCESS TO THESE GATES.
4. ALL BARRICADES SHALL BE SET UP PRIOR TO THE INITIATION OF THIS PHASE AND SHALL REMAIN IN PLACE UNTIL COMPLETE AND THE PCC IS CURED.
5. WORK DURING THIS PHASE SHALL BE PLANNED TO ENSURE THAT THE JET BRIDGE AT GATE 14 ALWAYS HAS A SOLID PCC SURFACE ON WHICH TO REST.
6. PHASE 2 SHALL BE COMPLETED IN 56 CALENDAR DAYS.
7. WORK TO BE COMPLETED IN THIS PHASE SHALL INCLUDE: PAVEMENT MARKING AND REMOVAL, PCC REMOVAL AND PCC PAVEMENT STRUCTURE CONSTRUCTION.

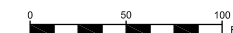
PHASE 2

LEGEND

- PHASE 2 WORK AREA
- COMPLETED WORK
- CONTRACTOR'S ACCESS
- LOW PROFILE BARRICADES
- OFA/SETBACK LINE
- GATE GUARD (FLAGGER)
- AIRCRAFT ACCESS
- PHASE LIMITS
- TAXIWAY CLOSURE "X"



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RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER

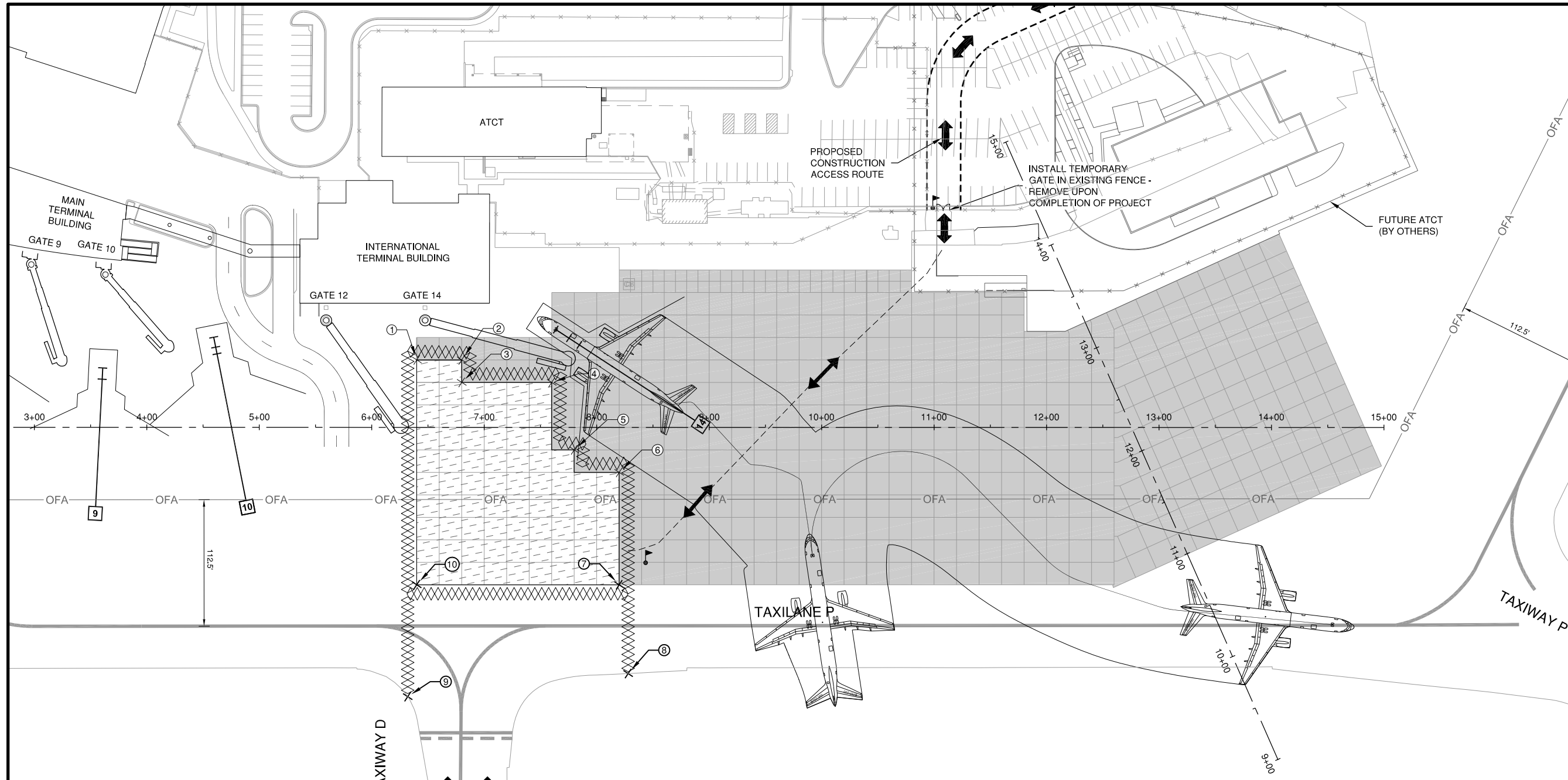


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PEORIA INTERNATIONAL  
AIRPORT  
PEORIA, ILLINOIS

| MARK | DATE | DESCRIPTION |
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AIP PROJ. NO. 3-17-0080-XX  
IL PROJ. NO. PIA-4542 CONTRACT NO. PE001  
CMT PROJECT NO: 15061-05  
CAD DWG FILE: 15061-05\_GC103.DWG  
DESIGNED BY: JMW  
DRAWN BY: CMT  
CHECKED BY: APP  
APPROVED BY: CHK  
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SHEET TITLE  
**CONSTRUCTION  
ACTIVITY PLAN 3**



**PHASE 3 NOTES**

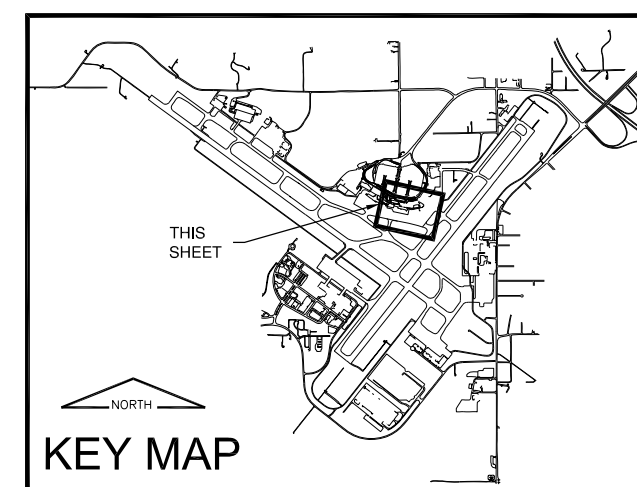
1. THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR 7 DAYS PRIOR TO THE INITIATION OF PHASE 3.
2. ALL WORK IN PHASE 2 SHALL BE COMPLETED PRIOR TO BEGINNING PHASE 3 WORK.
3. GATE 12 TEMPORARY MARKING SHALL BE REMOVED AND GATE 14 MARKED PRIOR TO THE START OF PHASE 3.
4. ANY AIRCRAFT USING GATE 14 DURING THIS PHASE SHALL HAVE UNRESTRICTED ACCESS TO THIS GATE.
5. ALL BARRICADES SHALL BE SET UP PRIOR TO THE INITIATION OF THIS PHASE AND SHALL REMAIN IN PLACE UNTIL COMPLETE AND THE PCC IS CURED.
6. THIS PHASE SHALL BE COMPLETED IN 38 CALENDAR DAYS.
7. WORK TO BE COMPLETED IN THIS PHASE SHALL INCLUDE: PAVEMENT MARKING AND REMOVAL, PCC REMOVAL AND PCC PAVEMENT STRUCTURE CONSTRUCTION.

**PHASE 3  
LEGEND**

- PHASE 3 WORK AREA
- COMPLETED WORK
- CONTRACTOR'S ACCESS
- LOW PROFILE BARRICADES
- OFA/SETBACK LINE
- GATE GUARD (FLAGGER)
- AIRCRAFT ACCESS
- PHASE LIMITS
- TAXIWAY CLOSURE "X"

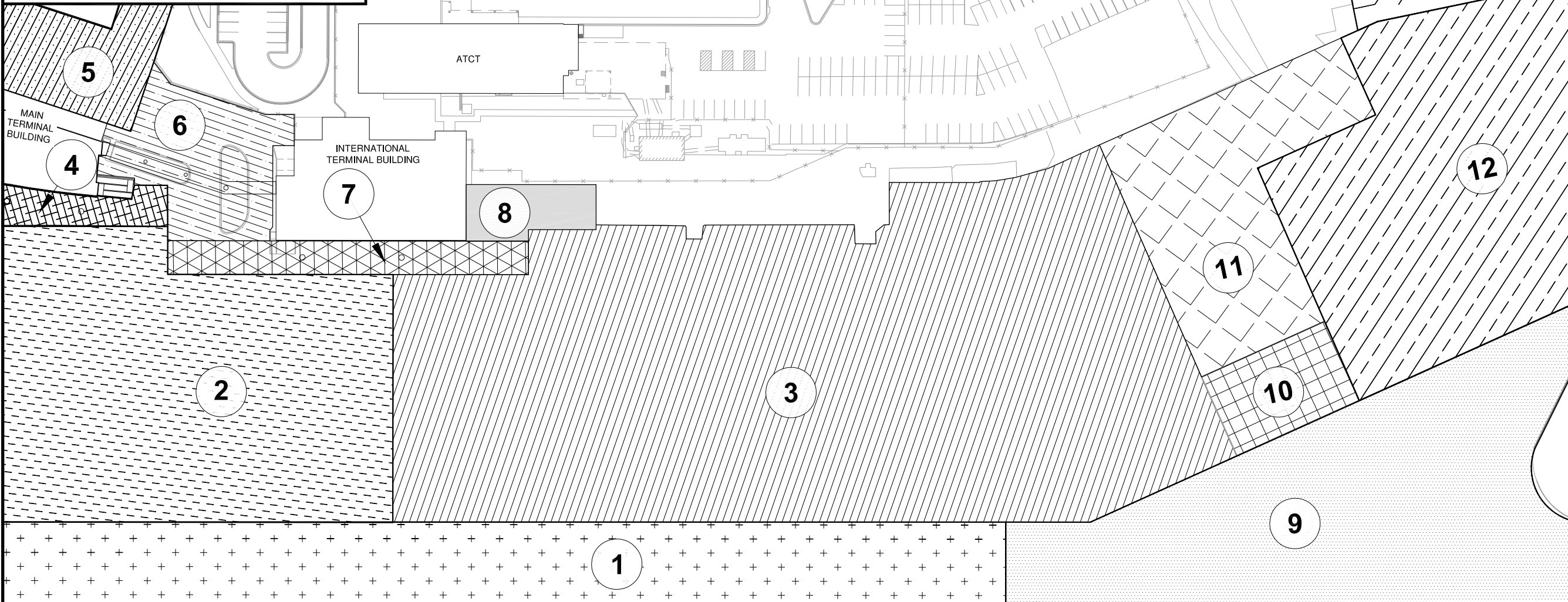
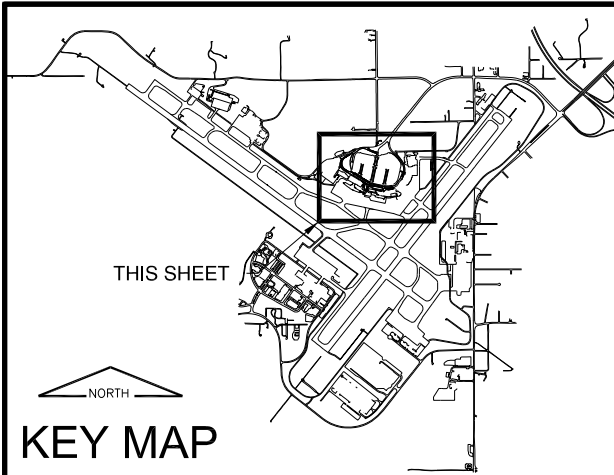
**PHASE LIMITS POINT TABLE**

| POINT | NORTHING    | EASTING     |
|-------|-------------|-------------|
| 1     | 1456600.354 | 2428669.078 |
| 2     | 1456592.070 | 2428708.211 |
| 3     | 1456572.504 | 2428704.069 |
| 4     | 1456555.935 | 2428782.334 |
| 5     | 1456493.094 | 2428789.474 |
| 6     | 1456465.243 | 2428824.465 |
| 7     | 1456367.412 | 2428803.754 |
| 8     | 1456288.526 | 2428795.231 |
| 9     | 1456309.025 | 2428599.227 |
| 10    | 1456404.691 | 2428627.657 |



**KEY MAP**

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Date: Thursday, June 16, 2016 2:54:15 PM



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RECONSTRUCT TERMINAL  
APRON PHASE 2



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AIRPORT  
PEORIA, ILLINOIS

| MARK | DATE | DESCRIPTION |
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| AIP PROJ. NO.           | 3-17-0080-XX                        |
| IL PROJ. NO. / PIA-4542 | CONTRACT NO. PE001                  |
| CMT PROJECT NO.         | 15061-05                            |
| CAD DWG FILE:           | 15061-05_CD100.DWG                  |
| DESIGNED BY:            | JMW                                 |
| DRAWN BY:               | CMT                                 |
| CHECKED BY:             | APP                                 |
| APPROVED BY:            | CHK                                 |
| COPYRIGHT:              | CRAWFORD, MURPHY & TILLY, INC. 2015 |

SHEET TITLE  
**EXISTING PAVEMENT  
STRUCTURES**

CD100  
SHEET 13 OF 32

PAVEMENT STRUCTURE LEGEND

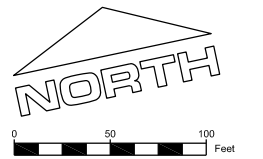
|  |   |  |   |  |  |  |   |
|--|---|--|---|--|--|--|---|
|  | <b>1</b> 18" PCC PAVEMENT (501)<br>8" CRUSHED AGGREGATE BASE COURSE (209)                                 |  | <b>4</b> 10" PCC PAVEMENT (501)<br>6" CRUSHED AGGREGATE BASE COURSE (209)       |  | <b>7</b> 14" PCC PAVEMENT (501)<br>12" CRUSHED AGGREGATE BASE COURSE (209)<br>12" OVERSIZE AGGREGATE (208) |  | <b>11</b> 14" PCC PAVEMENT (501)<br>7" BITUMINOUS BASE COURSE (201)<br>6" TREATED LIME SUBGRADE (155) |
|  | <b>2</b> 18" PCC PAVEMENT (510)<br>4" CRUSHED AGGREGATE BASE COURSE (209)<br>16" OVERSIZE AGGREGATE (208) |  | <b>5</b> 8" PCC PAVEMENT (501)<br>4" CRUSHED AGGREGATE BASE COURSE (209)        |  | <b>8</b> 9" PCC PAVEMENT (501)<br>12" CRUSHED AGGREGATE BASE COURSE (209)                                  |  | <b>12</b> 15" PCC PAVEMENT (501)<br>12" CRUSHED AGGREGATE BASE COURSE (209)                           |
|  | <b>3</b> 9" PCC PAVEMENT (501)<br>10" PCC PAVEMENT (501)<br>8" GRANULAR SUBBASE (154)                     |  | <b>6</b> 8" PCC PAVEMENT (501)<br>6" CRUSHED AGGREGATE BASE COURSE (209)        |  | <b>9</b> 18" PCC PAVEMENT (501)<br>8" CRUSHED AGGREGATE BASE COURSE (209)                                  |  | <b>13</b> 9" PCC PAVEMENT (501)<br>12" CRUSHED AGGREGATE BASE COURSE (209)                            |
|  |   |  | <b>10</b> 4" AND VAR. BITUMINOUS SURFACE COURSE (401)<br>10" PCC PAVEMENT (501) |  |  |  |   |

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES, PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

**CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.**



License No. 184-000613  
CONSULTANTS

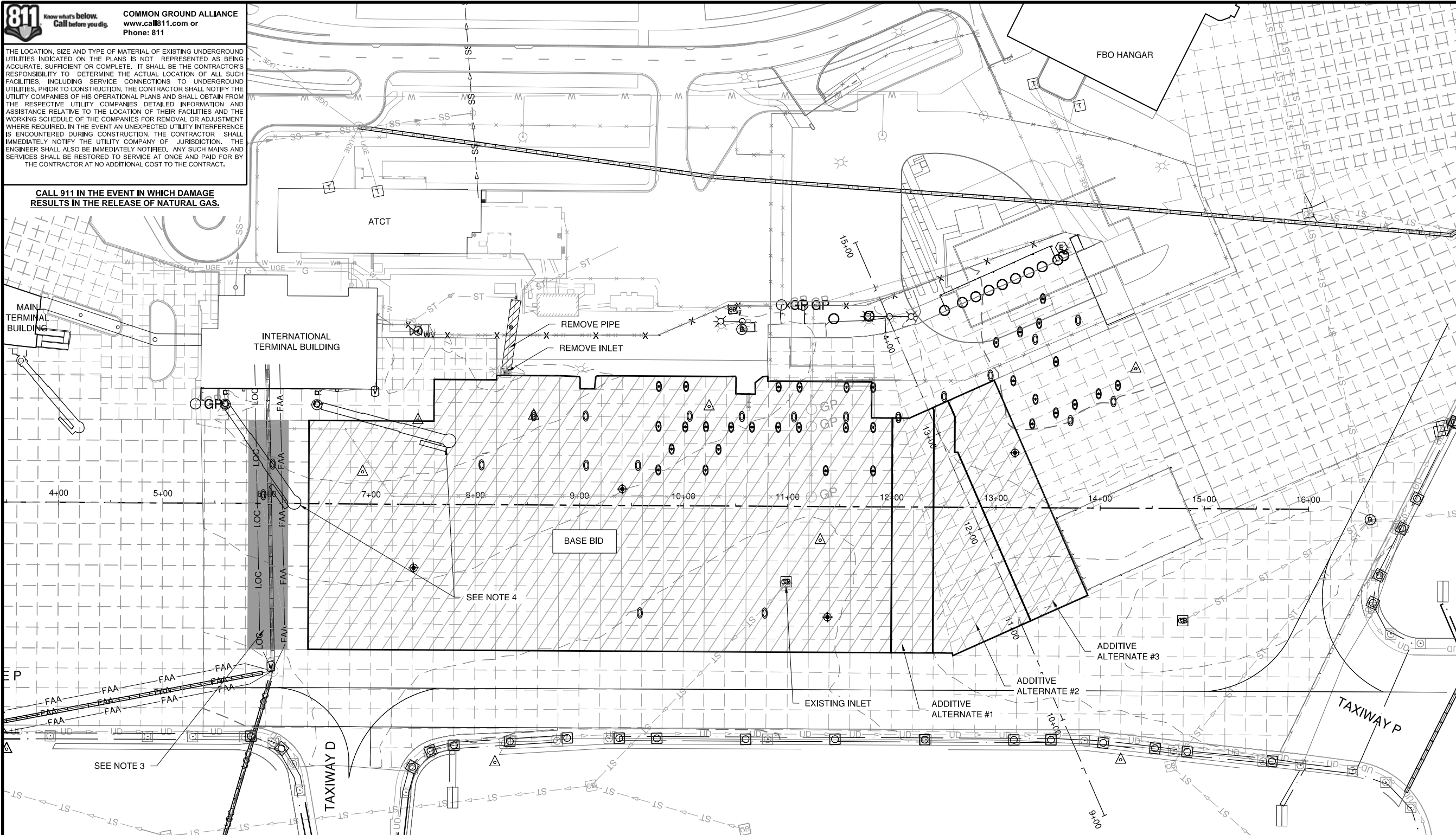


100% SUBMITTAL  
JUNE 3, 2016

**RECONSTRUCT TERMINAL APRON PHASE 2**



OWNER  
**GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL AIRPORT  
PEORIA, ILLINOIS**

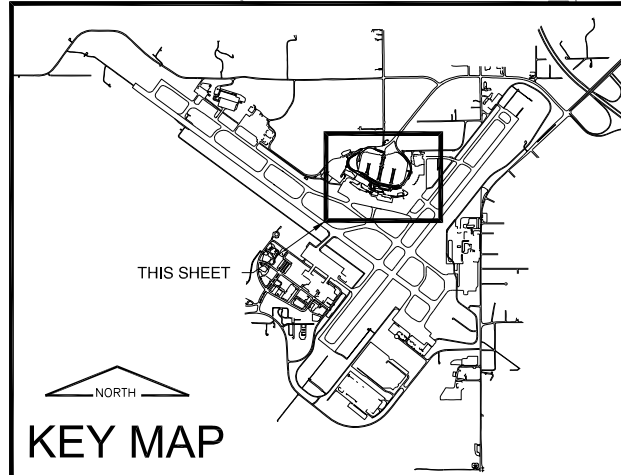


**LEGEND**

|  |                                     |  |                               |
|--|-------------------------------------|--|-------------------------------|
|  | EXISTING PAVEMENT                   |  | EXISTING ELECTRICAL DUCT      |
|  | EXISTING BASE MOUNTED TAXIWAY LIGHT |  | EXISTING STORM SEWER          |
|  | EXISTING ELECTRICAL MANHOLE         |  | EXISTING PAVEMENT JOINTS      |
|  | EXISTING TRANSFORMER                |  | EXISTING LOCALIZER CABLE      |
|  | EXISTING JET BRIDGE                 |  | EXISTING FAA CABLE            |
|  | EXISTING INLET                      |  | EXISTING EDGE LIGHT CIRCUIT 1 |
|  | EXISTING TAXIWAY GUIDANCE SIGN      |  | EXISTING EDGE LIGHT CIRCUIT 2 |
|  | EXISTING LIGHT POLE                 |  | EXISTING JOINT                |
|  | PAVEMENT REMOVAL LIMITS             |  | EXISTING WATER LINE           |
|  |                                     |  | EXISTING UNDERDRAIN           |

**NOTES**

1. DEMOLITION AND REMOVALS SHALL BE COMPLETED ONLY WITHIN THE LIMITS OF EACH PHASE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS.
2. THE CONTRACTOR SHALL COORDINATE WITH AIRPORT OPERATIONS SEVEN DAYS PRIOR TO PAVEMENT REMOVAL.
3. RECORD DRAWINGS INDICATE AN EXISTING FAA DUCT BENEATH THE EXISTING PAVEMENT THAT RUNS FROM AN EXISTING ELECTRICAL MANHOLE TO THE NORTH. THE DEPTH OF THE DUCT SHALL BE VERIFIED AND PAVEMENT REMOVAL OPERATIONS SHALL TAKE NECESSARY PRECAUTIONS TO PROTECT THE DUCT DURING CONSTRUCTION. HAND REMOVAL OF MATERIAL OVER EXISTING FAA DUCT SHALL BE PERFORMED IN THE SHADED REGION AS SHOWN ON THIS SHEET. THE DUCT HOUSES CONTROL CABLES FOR THE FAA RADAR AND LOCALIZER CONTROL. DAMAGE TO THIS DUCT AND CABLES MAY RESTRICT THE AIRPORTS ABILITY TO LAND AIRCRAFT. ANY DAMAGE TO THESE ITEMS SHALL BE REPAIRED BY THE CONTRACTOR WITH ALL COSTS TO BE BORNE BY THE CONTRACTOR.
4. SEE CONSTRUCTION ACTIVITY PLAN SHEETS FOR MORE INFORMATION ON CONSTRUCTION PHASING AROUND JET BRIDGES.



| MARK | DATE | DESCRIPTION                                    |
|------|------|--|
|      |      | AIP PROJ. NO. 3-17-0080-XX                     |
|      |      | IL PROJ. NO. PIA-4542 CONTRACT NO. PE001       |
|      |      | CMT PROJECT NO: 15061-05                       |
|      |      | CAD DWG FILE: 15061-05_CD101.DWG               |
|      |      | DESIGNED BY: JMW                               |
|      |      | DRAWN BY: CMT                                  |
|      |      | CHECKED BY: APP                                |
|      |      | APPROVED BY: CHK                               |
|      |      | COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |

**EXISTING CONDITIONS AND REMOVALS**

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JUNE 3, 2016

RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER

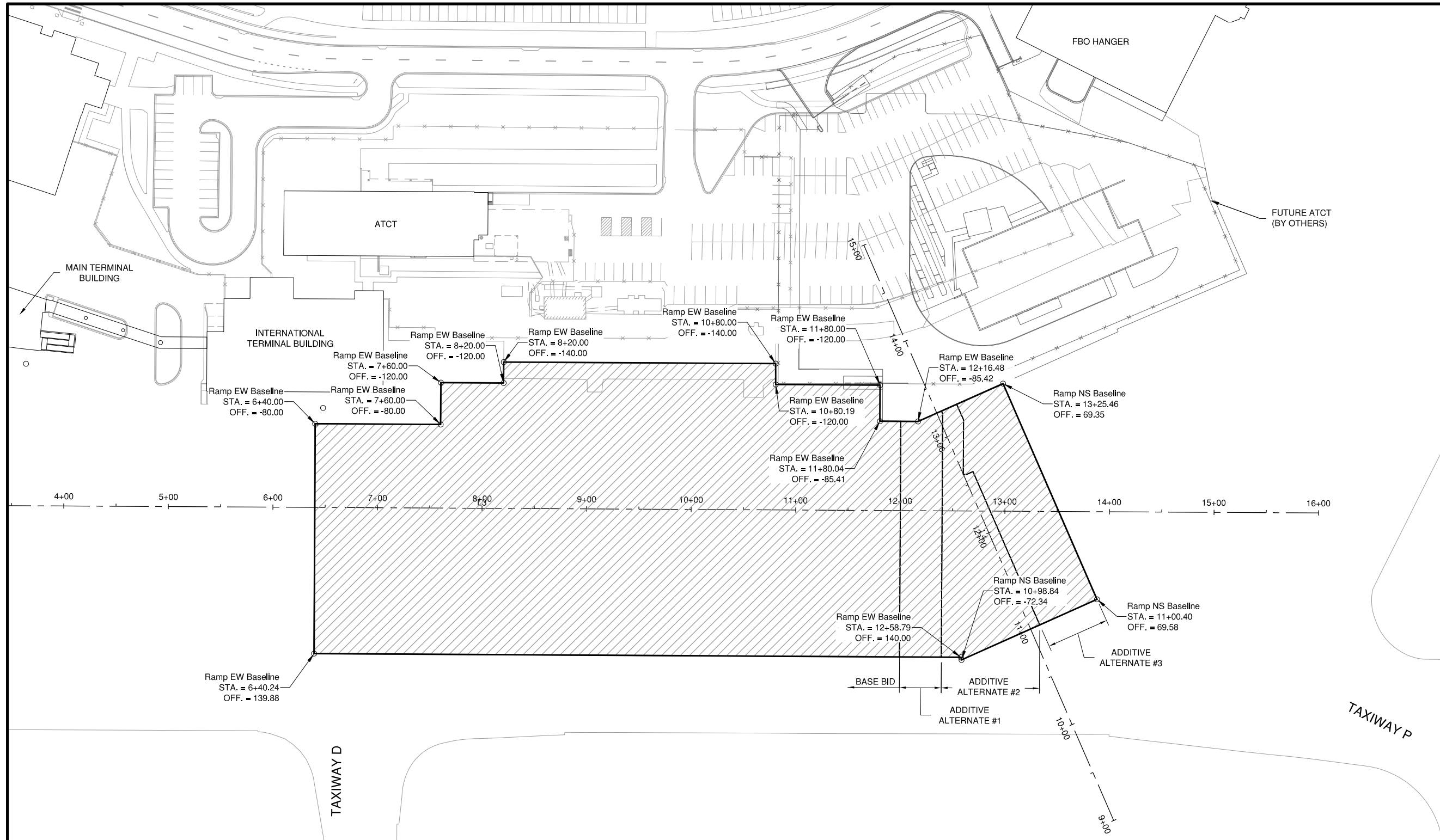
GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL AIRPORT

GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL AIRPORT  
PEORIA, ILLINOIS

| MARK | DATE | DESCRIPTION |
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| AIP PROJ. NO. 3-17-0080-XX                     |
| IL PROJ. NO. PIA-4542 CONTRACT NO. PE001       |
| CMT PROJECT NO: 15061-05                       |
| CAD DWG FILE: 15061-05_CD103.DWG               |
| DESIGNED BY: JMW                               |
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| CHECKED BY: APP                                |
| APPROVED BY: CHK                               |
| COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |

SHEET TITLE  
**DEMOLITION PLAN**



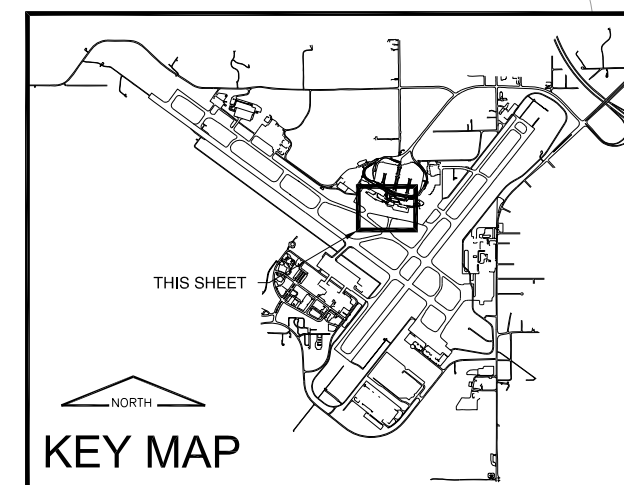
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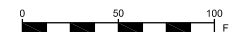
- ALL PROPOSED PAVEMENT REMOVAL ADJACENT TO EXISTING PAVEMENT TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH ALONG THE LIMITS OF THE REMOVAL PRIOR TO THE PAVEMENT DEMOLITION.
- USE CAUTION WHEN PERFORMING DEMOLITION OPERATIONS NEAR BUILDINGS AND EXISTING FAA FACILITY. DAMAGE, OR NOISE AND VIBRATIONAL DISTURBANCE WILL REQUIRE THE CONTRACTOR TO MODIFY HIS REMOVAL METHODS.
- WHERE SAW CUTTING IS SHOWN ADJACENT TO EXISTING PAVEMENT TO REMAIN IN PLACE, A SECOND SAWCUT LINE SHALL BE MADE ON THE PAVEMENT TO BE REMOVED TO PREVENT DAMAGE TO THE EXISTING DURING BREAKING OPERATIONS.
- PROJECT LIMITS SHALL BE SAWCUT TO ISOLATE PANELS TO BE REMOVED FROM PANELS THAT ARE TO REMAIN. A MINIMUM OF 2' OFFSET SAWCUTS WILL BE REQUIRED AT THE PERIMETER OF THE REMOVAL.

**LEGEND**

- EXISTING PAVEMENT
- PROPOSED FULL DEPTH SAWCUTS
- PROPOSED APRON REMOVAL LIMITS

| ALIGNMENT DATA    |                      |                           |                                  |                           |                                   |
|-------------------|----------------------|---------------------------|----------------------------------|---------------------------|-----------------------------------|
| ALIGNMENT SEGMENT | ALIGNMENT            | BEGIN STATION/ PI STATION | NORTHING/ EASTING                | END STATION/ CURVE RADIUS | NORTHING/ EASTING                 |
| L3                | EAST/WEST BASELINE   | STA. 0+00.00              | N:1456674.2044<br>E:2428030.5281 | STA. 16+00.00             | N: 1456342.8320<br>E:2429595.8371 |
| L4                | NORTH/SOUTH BASELINE | STA. 9+00.00              | N:1456094.2056<br>E:2429344.6098 | STA. 15+00.00             | N: 1456681.4605<br>E:2429221.5990 |





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JUNE 3, 2016

RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER

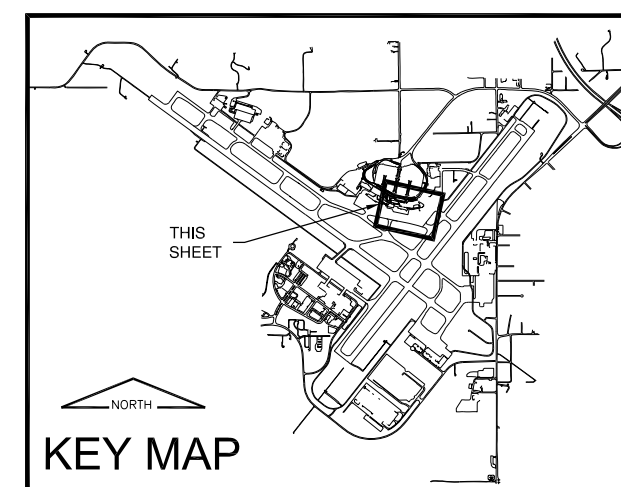
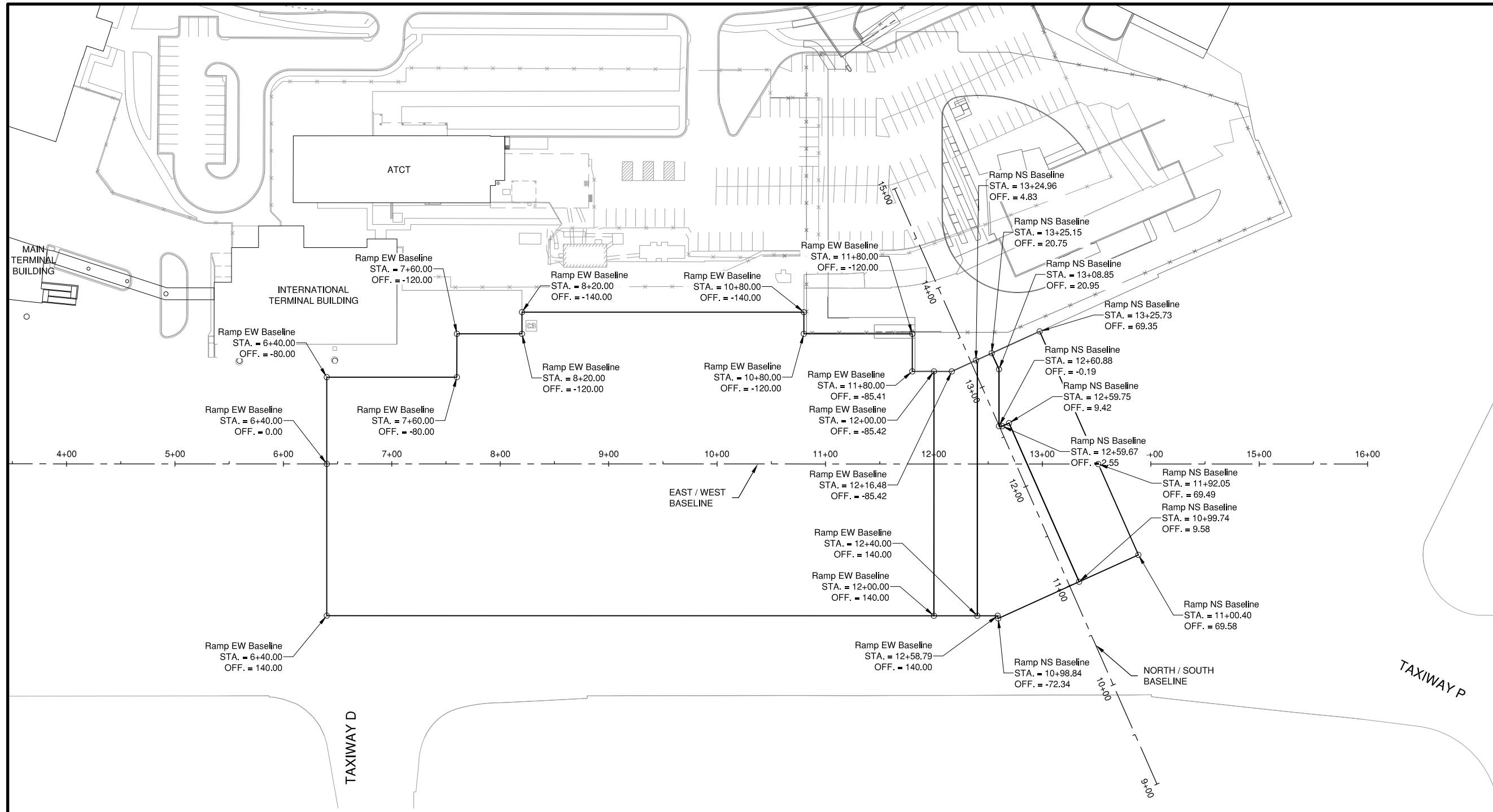


GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL  
AIRPORT  
PEORIA, ILLINOIS

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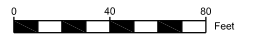
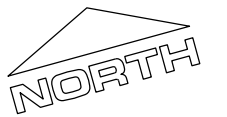
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| AIP PROJ. NO. 3-17-0080-XX                     |
| IL PROJ. NO. PIA-4542 CONTRACT NO. PE001       |
| CMT PROJECT NO: 15061-05                       |
| CAD DWG FILE: 15061-05_CP101.DWG               |
| DESIGNED BY: JMW                               |
| DRAWN BY: CMT                                  |
| CHECKED BY: APP                                |
| APPROVED BY: CHK                               |
| COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |

SHEET TITLE  
**PROPOSED  
GEOMETRY**



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RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER

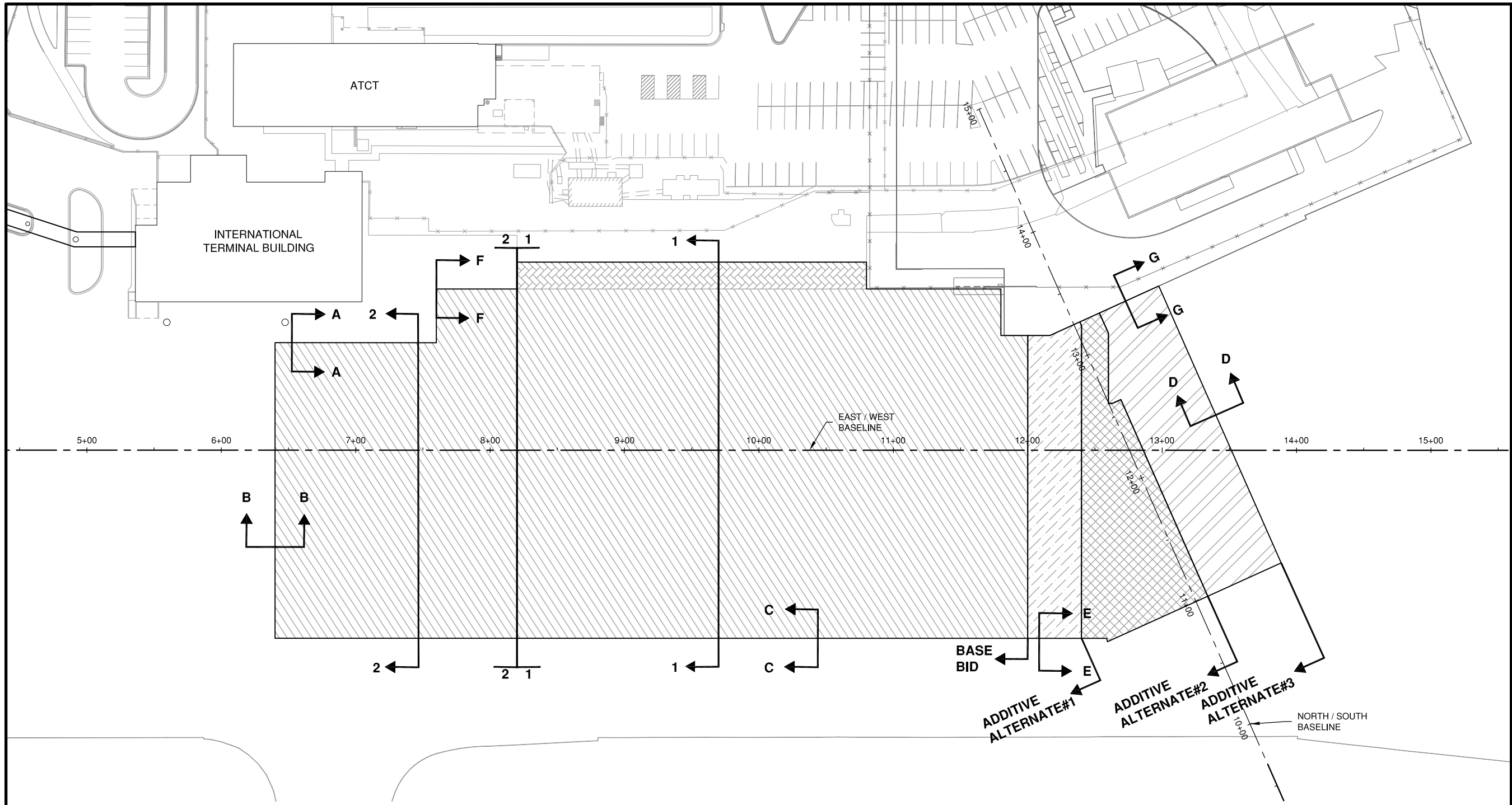


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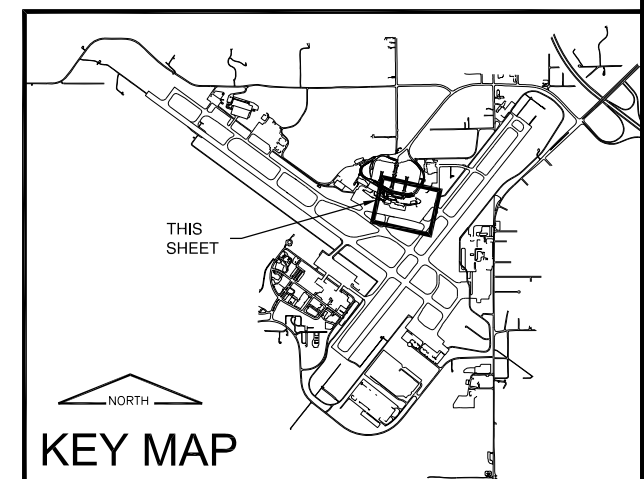
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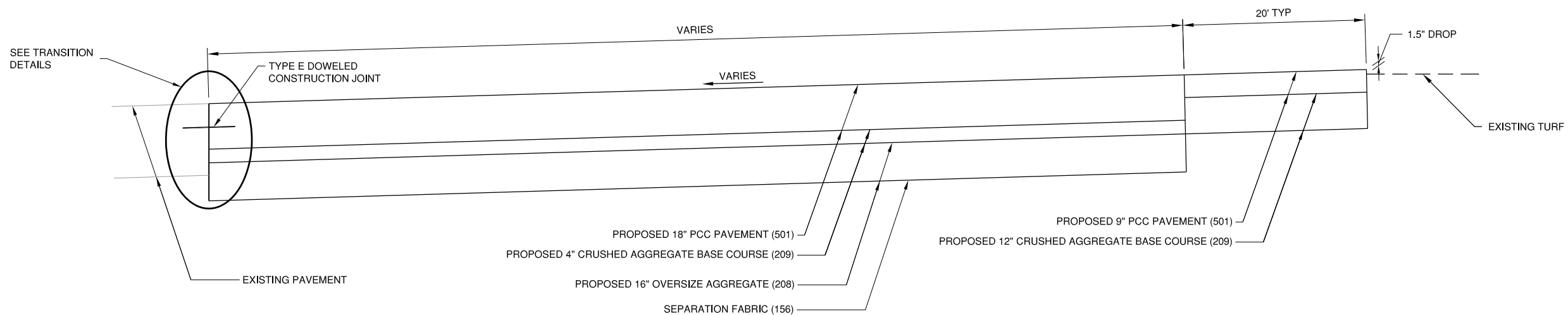
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**PROPOSED  
IMPROVEMENTS**



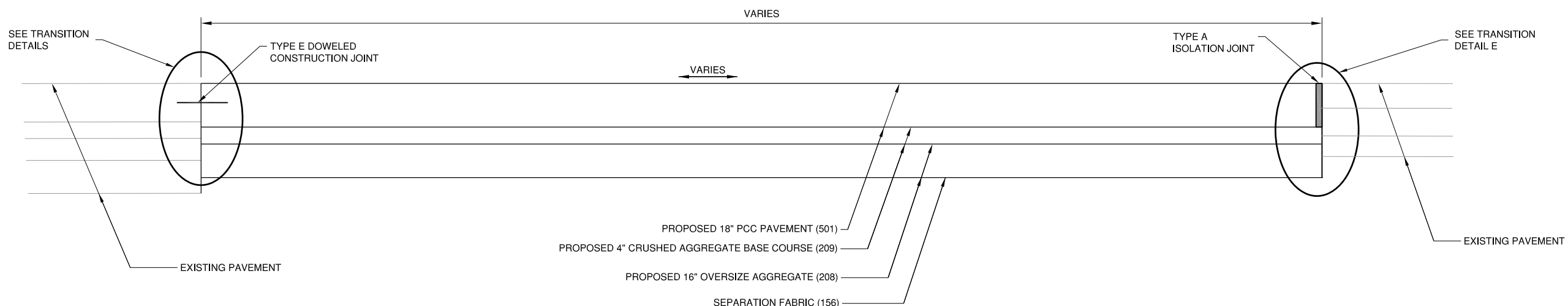
**LEGEND**

|  |  |  |  |  |  |
|--|--|--|--|--|--|
|  | BASE BID<br>NEW 18" PCC PAVEMENT (501)<br>NEW 4" CRUSHED AGGREGATE (209)<br>NEW 16" OVERSIZED AGGREGATE (208)<br>NEW SEPARATION FABRIC (156) |  | ADDITIVE ALTERNATE 1<br>NEW 18" PCC PAVEMENT (501)<br>NEW 4" CRUSHED AGGREGATE (209)<br>NEW 16" OVERSIZED AGGREGATE (208)<br>NEW SEPARATION FABRIC (156) |  | ADDITIVE ALTERNATE 3<br>NEW 18" PCC PAVEMENT (501)<br>NEW 4" CRUSHED AGGREGATE (209)<br>NEW 16" OVERSIZED AGGREGATE (208)<br>NEW SEPARATION FABRIC (156) |
|  | BASE BID<br>NEW 9" PCC PAVEMENT (501)<br>NEW 12" CRUSHED AGGREGATE (209)   |  | ADDITIVE ALTERNATE 2<br>NEW 18" PCC PAVEMENT (501)<br>NEW 4" CRUSHED AGGREGATE (209)<br>NEW 16" OVERSIZED AGGREGATE (208)<br>NEW SEPARATION FABRIC (156) |  |  |

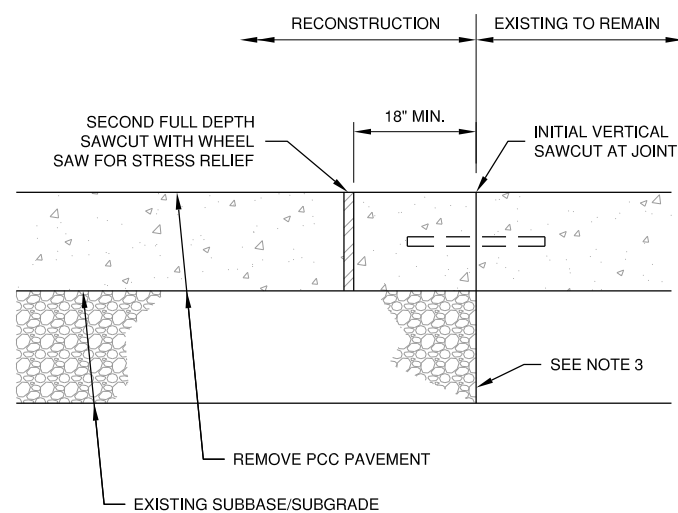




① PROPOSED 18" APRON PAVEMENT ①  
N.T.S.  
STA.8+20 to STA. 12+60



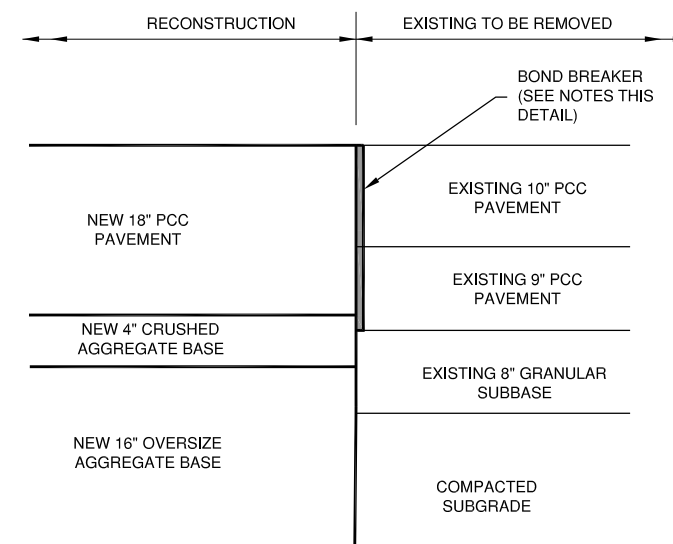
② PROPOSED 18" APRON PAVEMENT ②  
N.T.S.  
STA.6+40 to STA. 8+20



PCC PAVEMENT REMOVAL DETAIL  
N.T.S.

**NOTES:**

1. SEE JOINTING PLAN FOR JOINT SPACING AND JOINT TYPES.
2. TWO VERTICAL SAWCUTS SHALL BE MADE AS INDICATED PRIOR TO STARTING ANY PAVEMENT REMOVAL ADJACENT TO EXISTING PAVEMENTS TO REMAIN IN PLACE.
3. CONTRACTOR SHALL TAKE ACTION AS NEEDED TO PREVENT LOSS OF AGGREGATE FROM UNDERNEATH PAVEMENT TO REMAIN IN PLACE.
4. SAW CUT AT EACH PHASE LIMIT. INCIDENTAL TO PCC REMOVAL.



- NOTES:**
1. LIQUID BOND BREAKER SHALL NOT BE USED.
  2. THICKNESS OF BOND BREAKER SHALL NOT CHANGE PROPOSED JOINT SPACING OR PANEL DIMENSIONS.

PHASE BOND BREAKER DETAIL  
N.T.S.

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JUNE 3, 2016

RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER



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AIRPORT  
PEORIA, ILLINOIS

| MARK | DATE | DESCRIPTION |
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AIP PROJ. NO. 3-17-0080-XX  
IL PROJ. NO. PIA-4542 CONTRACT NO. PE001  
CMT PROJECT NO: 15061-05  
CAD DWG FILE: 15061-05\_CP301.DWG  
DESIGNED BY: JMW  
DRAWN BY: CMT  
CHECKED BY: APP  
APPROVED BY: CHK  
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SHEET TITLE  
TYPICAL SECTIONS 1

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RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER

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PEORIA INTERNATIONAL AIRPORT

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PEORIA INTERNATIONAL AIRPORT  
PEORIA, ILLINOIS

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AIP PROJ. NO. 3-17-0080-XX

IL PROJ. NO. PIA-4542 CONTRACT NO. PE001

CMT PROJECT NO: 15061-05

CAD DWG FILE: 15061-05\_CP302.DWG

DESIGNED BY: JMW

DRAWN BY: CMT

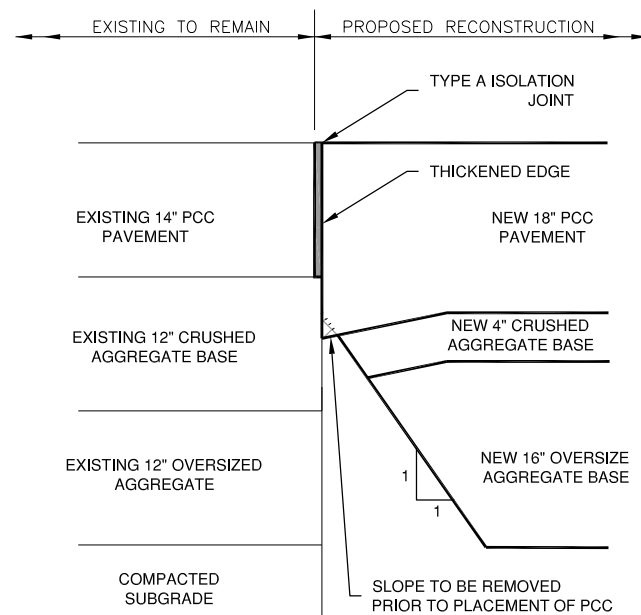
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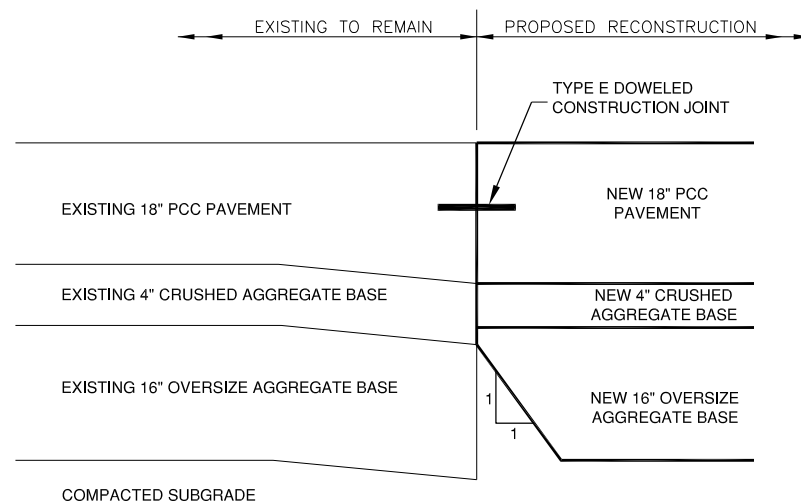
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SHEET TITLE  
**TYPICAL SECTIONS 2**

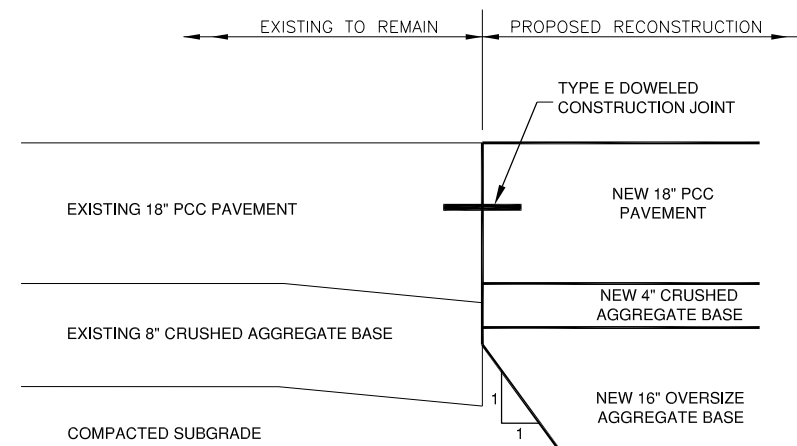
SHEET **19** OF **32**  
CP302



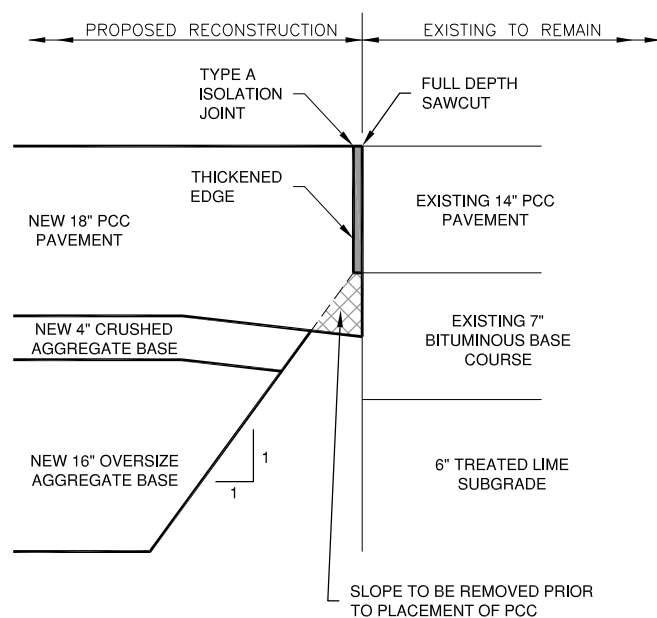
**(A) ISOLATION TRANSITION DETAIL (A)**  
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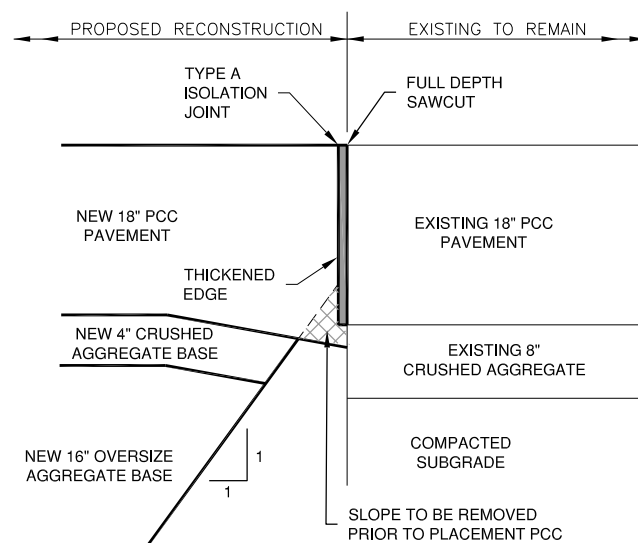
**(B) DOWELED TRANSITION DETAIL (B)**  
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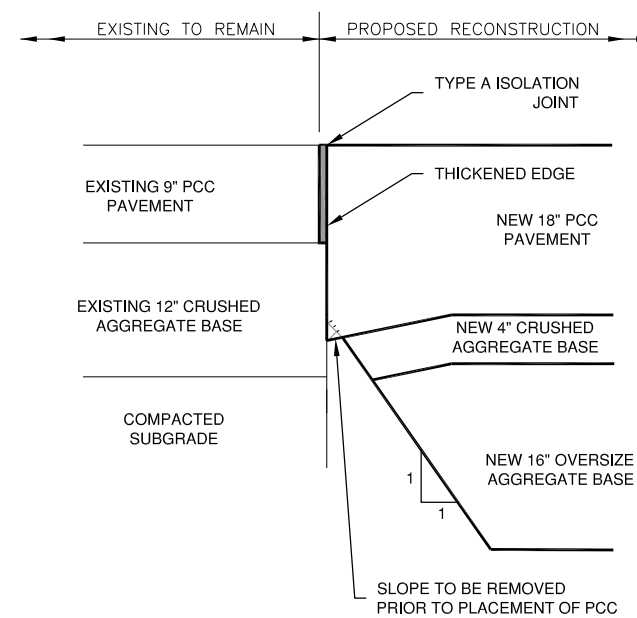
**(C) DOWELED TRANSITION DETAIL (C)**  
N.T.S.



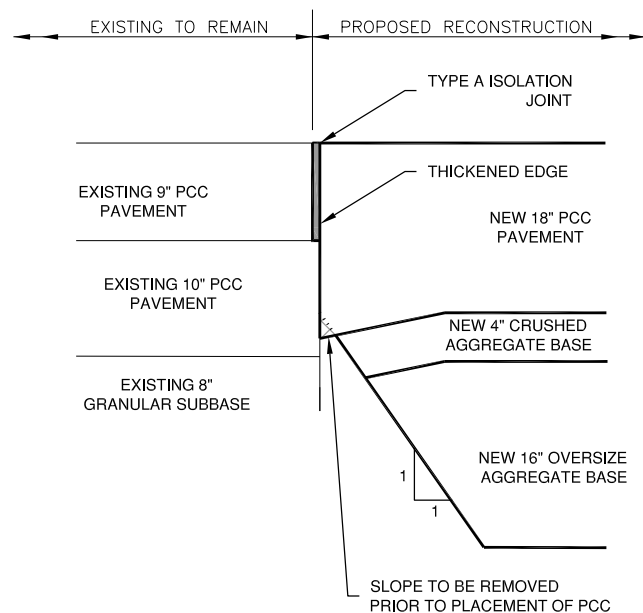
**(D) ISOLATION TRANSITION DETAIL (D)**  
N.T.S.



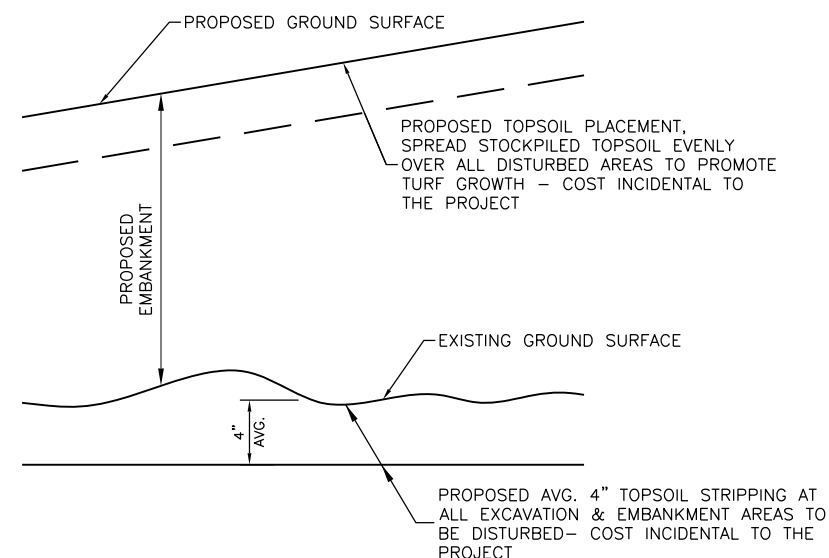
**(E) ISOLATION TRANSITION DETAIL (E)**  
N.T.S.



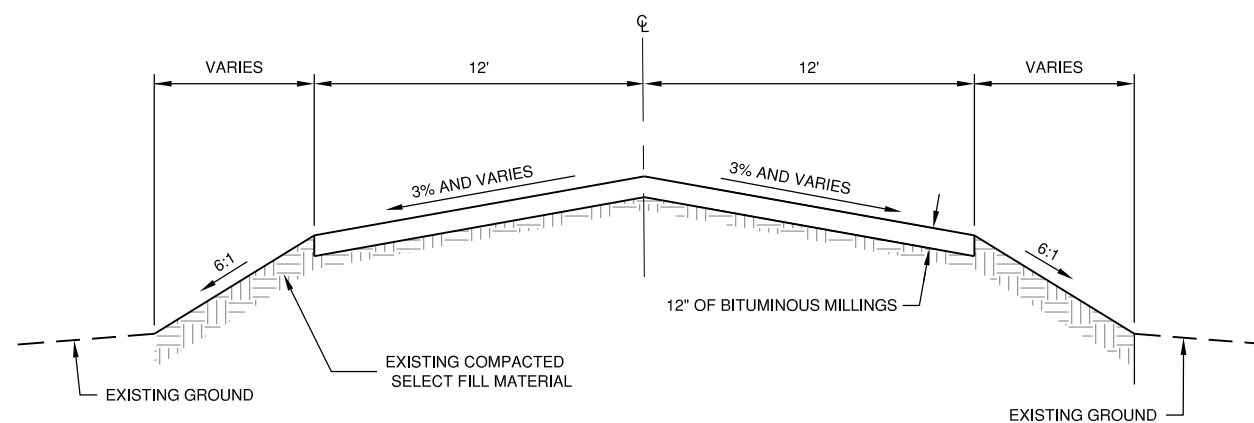
**(F) ISOLATION TRANSITION DETAIL (F)**  
N.T.S.



**ISOLATION TRANSITION DETAIL**  
N.T.S.



**TOPSOIL STRIPPING DETAIL**  
N.T.S.



**EXISTING TERMINAL APRON HAUL ROUTE SECTION**  
N.T.S.

**NOTE:**  
1. CONSTRUCTION ACCESS HAUL ROUTE ROAD SHALL BE SLOPED TO BE THE SAME AS THE EXISTING TURF GRADING OFF OF AIRFIELD PAVEMENT.

100% SUBMITTAL  
JUNE 3, 2016

RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER

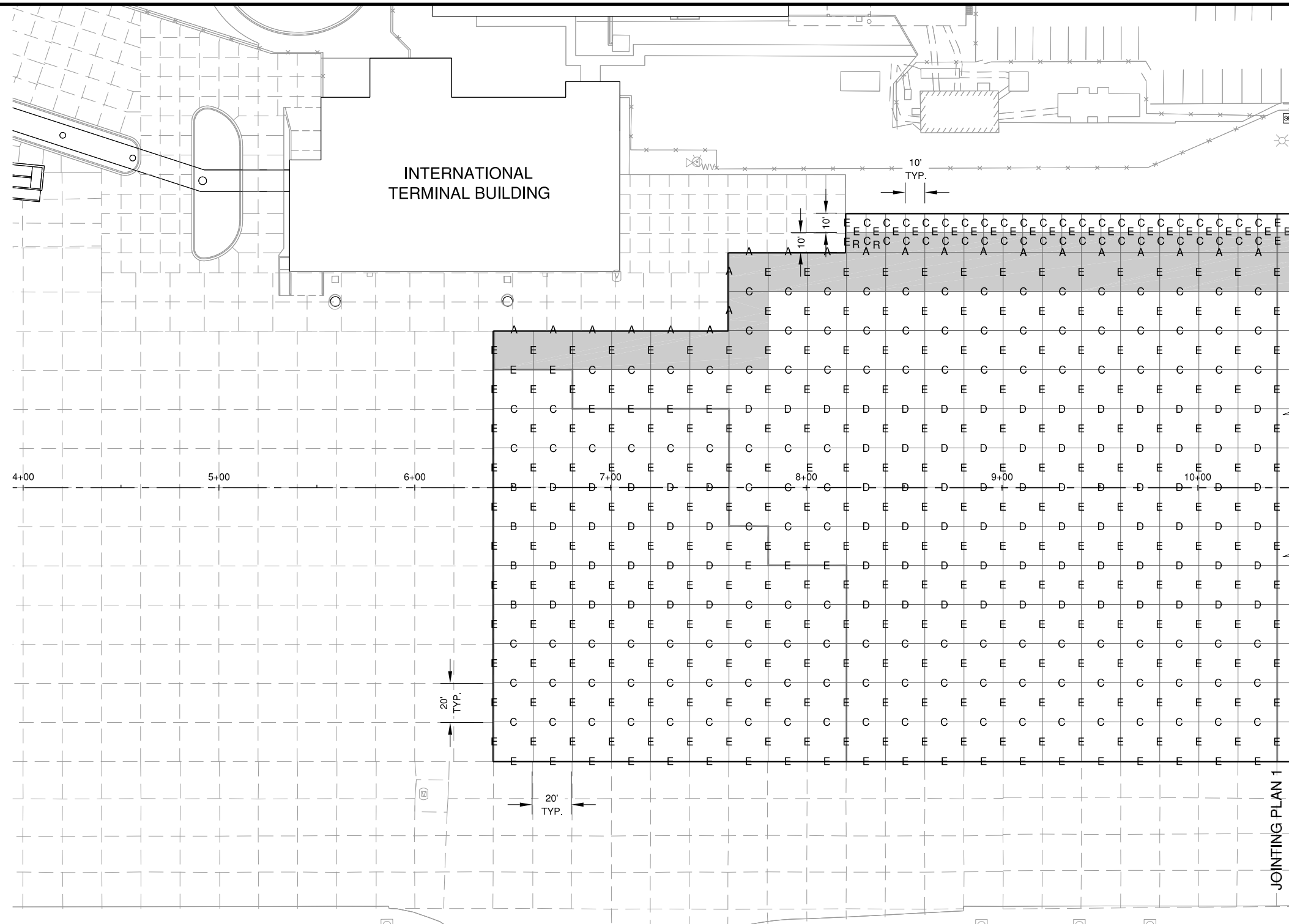


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AIRPORT  
PEORIA, ILLINOIS

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| AIP PROJ. NO. 3-17-0080-XX                     |
| IL PROJ. NO. PIA-4542 CONTRACT NO. PE001       |
| CMT PROJECT NO: 15061-05                       |
| CAD DWG FILE: 15061-05_CP303.DWG               |
| DESIGNED BY: JMW                               |
| DRAWN BY: CMT                                  |
| CHECKED BY: APP                                |
| APPROVED BY: CHK                               |
| COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |

SHEET TITLE  
**TYPICAL SECTIONS 3**



INTERNATIONAL  
TERMINAL BUILDING

10'  
TYP.

20'  
TYP.

20'  
TYP.

MATCHLINE STA. 10 + 40

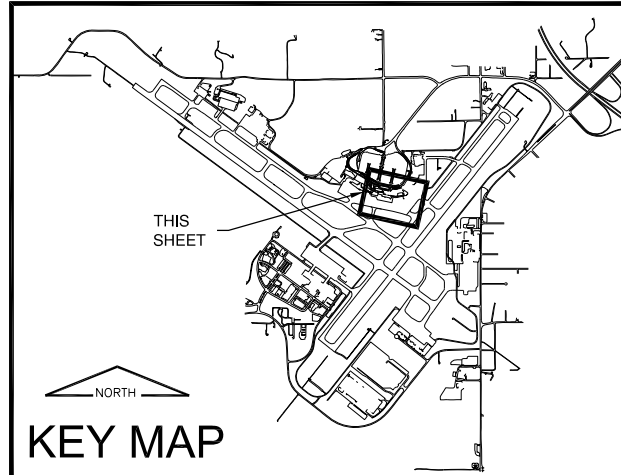
JOINTING PLAN 1  
JOINTING PLAN 2

**NOTES:**

1. THE CONTRACTOR MAY SUBMIT AN ALTERNATE JOINTING/PAVING PLAN FOR REVIEW AND CONSIDERATION PRIOR TO INITIATING THE PAVING OPERATIONS. THE PAVING PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO, THE CONTRACTOR'S PROPOSED CHANGES TO PAVING DIRECTION, PROPOSED CHANGES TO PAVING LANE WIDTHS, PROPOSED CHANGES TO THE JOINTING TYPE, CONTRACTOR'S PLAN FOR "BOXING OUT" AROUND INLETS, LIGHTS, HAND-HOLES, OR OTHER PAVEMENT PENETRATIONS, PLANS TO PREVENT PREMATURE CRACKING IN "BOXED OUT" PANELS, CONTRACTOR'S PLAN FOR PAVING AGAINST ADJACENT STRUCTURES, AND ANY OTHER REVISION OR ALTERATION THAT THE CONTRACTOR SHOULD PROPOSE THAT DIFFERS FROM THE PLAN SHEETS.

**LEGEND**

- A — TYPE A ISOLATION JOINT (3/4")
- B — TYPE B HINGED (TIED) CONTRACTION JOINT
- C — TYPE C DOWELED CONTRACTION JOINT
- D — TYPE D DUMMY CONTRACTION JOINT
- E — TYPE E DOWELED CONSTRUCTION JOINT
- R — REINFORCED PANEL
- THICKENED EDGE



KEY MAP



License No. 184-000613  
CONSULTANTS



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JUNE 3, 2016

RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER

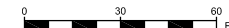


GENERAL WAYNE A. DOWNING  
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AIRPORT  
PEORIA, ILLINOIS

| MARK | DATE | DESCRIPTION                                    |
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|      |      | AIP PROJ. NO. 3-17-0080-XX                     |
|      |      | IL PROJ. NO. PIA-4542 CONTRACT NO. PE001       |
|      |      | CMT PROJECT NO: 15061-05                       |
|      |      | CAD DWG FILE: 15061-05_CJ101.DWG               |
|      |      | DESIGNED BY: JMW                               |
|      |      | DRAWN BY: CMT                                  |
|      |      | CHECKED BY: APP                                |
|      |      | APPROVED BY: CHK                               |
|      |      | COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |

SHEET TITLE  
**JOINTING PLAN 1**

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RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER



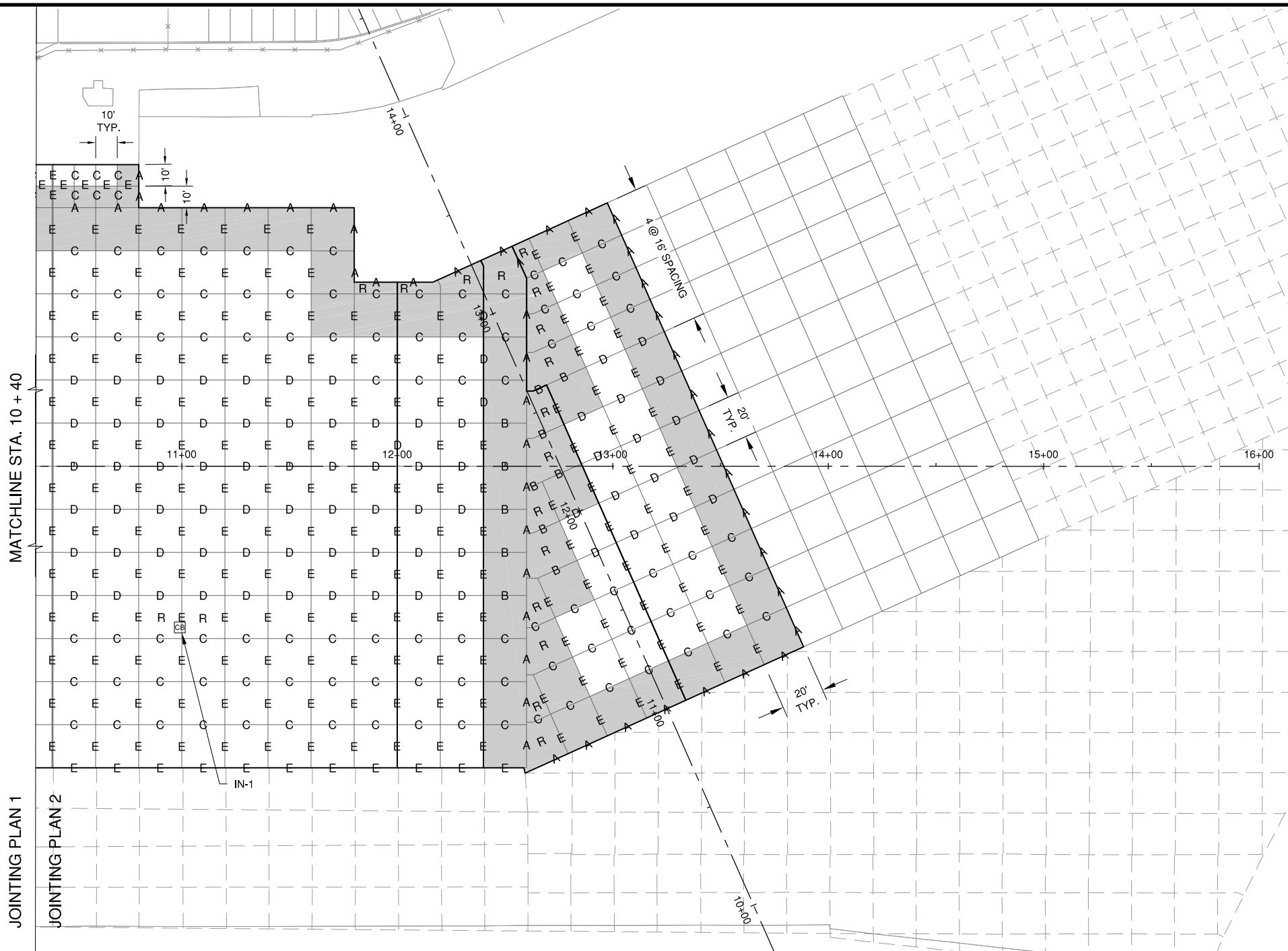
GENERAL WAYNE A. DOWNING  
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| AIP PROJ. NO.           | 3-17-0080-XX                        |
| IL PROJ. NO. / PIA-4542 | CONTRACT NO. PE001                  |
| CMT PROJECT NO:         | 15061-05                            |
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| DESIGNED BY:            | JMW                                 |
| DRAWN BY:               | CMT                                 |
| CHECKED BY:             | APP                                 |
| APPROVED BY:            | CHK                                 |
| COPYRIGHT:              | CRAWFORD, MURPHY & TILLY, INC. 2015 |

SHEET TITLE  
**JOINTING PLAN 2**

SHEET **22** OF **32**

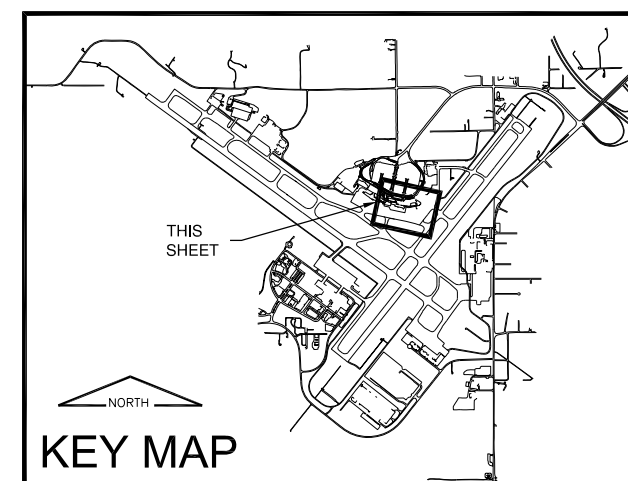


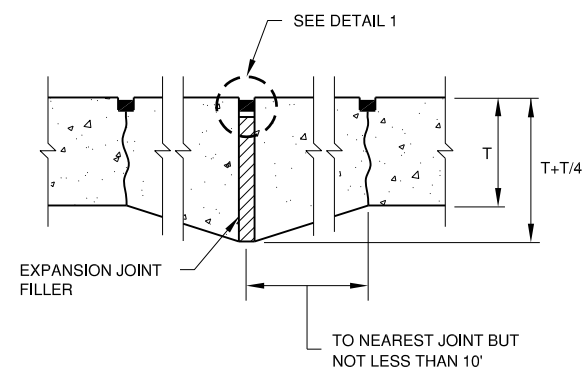
**NOTES:**

1. THE CONTRACTOR MAY SUBMIT AN ALTERNATE JOINTING/PAVING PLAN FOR REVIEW AND CONSIDERATION PRIOR TO INITIATING THE PAVING OPERATIONS. THE PAVING PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO, THE CONTRACTOR'S PROPOSED CHANGES TO PAVING DIRECTION, PROPOSED CHANGES TO PAVING LANE WIDTHS, PROPOSED CHANGES TO THE JOINTING TYPE, CONTRACTOR'S PLAN FOR "BOXING OUT" AROUND INLETS, LIGHTS, HAND-HOLES, OR OTHER PAVEMENT PENETRATIONS, PLANS TO PREVENT PREMATURE CRACKING IN "BOXED OUT" PANELS, CONTRACTOR'S PLAN FOR PAVING AGAINST ADJACENT STRUCTURES, AND ANY OTHER REVISION OR ALTERATION THAT THE CONTRACTOR SHOULD PROPOSE THAT DIFFERS FROM THE PLAN SHEETS.

**LEGEND**

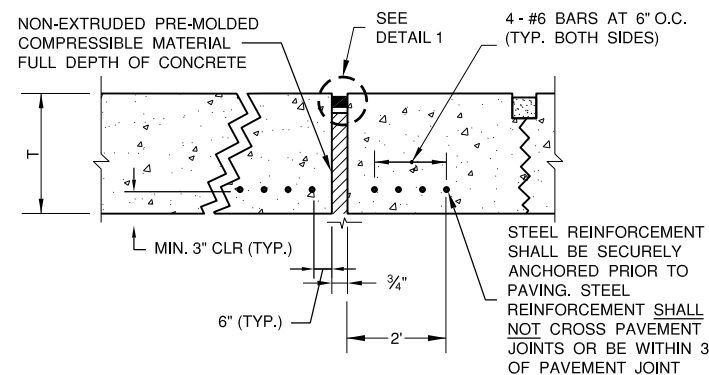
- A — TYPE A ISOLATION JOINT (3/4")
- B — TYPE B HINGED (TIED) CONTRACTION JOINT
- C — TYPE C DOWELED CONTRACTION JOINT
- D — TYPE D DUMMY CONTRACTION JOINT
- E — TYPE E DOWELED CONSTRUCTION JOINT
- R — REINFORCED PANEL
- THICKENED EDGE





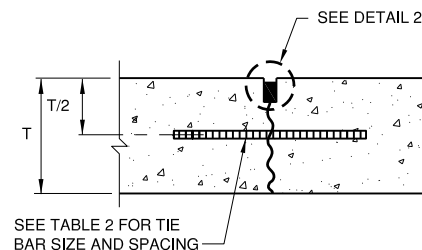
**TYPE A THICKENED ISOLATION**

SYMBOL — A —



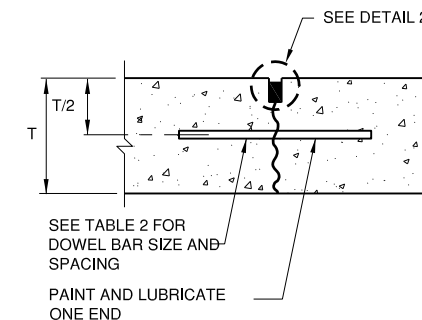
**TYPE A1 REINFORCED ISOLATION**

SYMBOL — A1 —



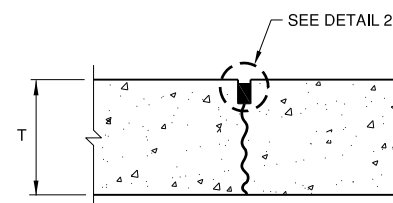
**TYPE B HINGED (TIED) CONTRACTION**

SYMBOL — B —



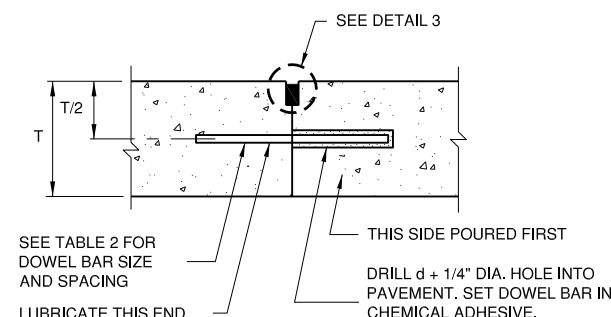
**TYPE C DOWELED CONTRACTION**

SYMBOL — C —



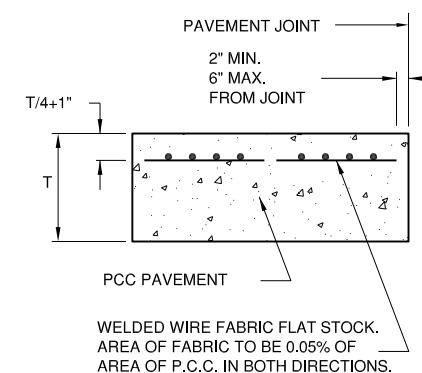
**TYPE D DUMMY CONTRACTION**

SYMBOL — D —



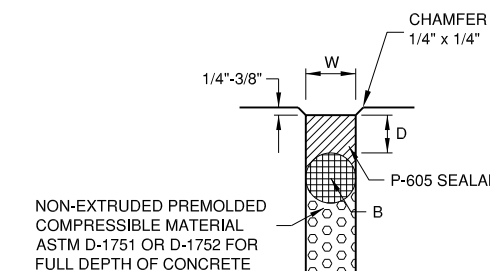
**TYPE E DOWELED CONSTRUCTION**

SYMBOL — E —

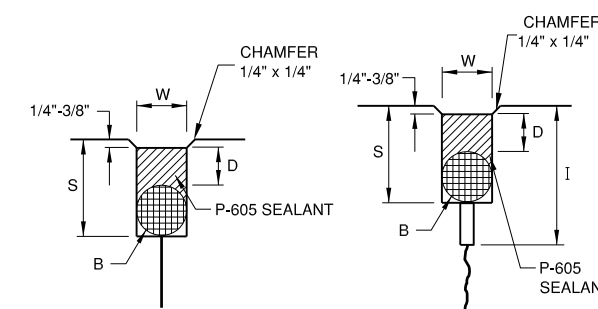


**ODD SHAPED PANEL REINFORCEMENT**

SYMBOL — R —



**DETAIL 1**



**DETAIL 3**

**DETAIL 2**

**JOINT SEALING DETAILS**

**JOINT NOTES**

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A CHAMFER OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A CHAMFER > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.

100% SUBMITTAL  
JUNE 3, 2016

RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER  
  
GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL AIRPORT

GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL AIRPORT  
PEORIA, ILLINOIS

MARK | DATE | DESCRIPTION

AIP PROJ. NO. 3-17-0080-XX  
IL PROJ. NO. PIA-4542 CONTRACT NO. PE001  
CMT PROJECT NO: 15061-05  
CAD DWG FILE: 15061-05\_CP501.DWG  
DESIGNED BY: JMW  
DRAWN BY: CMT  
CHECKED BY: APP  
APPROVED BY: CHK  
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**JOINTING DETAILS**

| PAVEMENT THICKNESS T - INCHES | DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES I=(T/3) ±1/4" |
|-------------------------------|--|
| 5                             | 1.67"  |
| 6                             | 2.00"  |
| 7                             | 2.33"  |
| 8                             | 2.67"  |
| 9                             | 3.00"  |
| 10                            | 3.33"  |
| 11                            | 3.67"  |
| 12                            | 4.00"  |
| 13                            | 4.33"  |
| 14                            | 4.67"  |
| 15                            | 5.00"  |
| 16                            | 5.33"  |
| 17                            | 5.67"  |
| 18                            | 6.00"  |

| PAVEMENT THICKNESS T - INCHES | DOWEL BAR DETAILS |        |         | TIE BAR DETAILS |        |         |
|-------------------------------|-------------------|--------|---------|-----------------|--------|---------|
|                               | DIA.              | LENGTH | SPACING | BAR SIZE        | LENGTH | SPACING |
| 5                             | 5/8"              | 12"    | 12"     | #4              | 24"    | 30"     |
| 6                             | 3/4"              | 18"    | 12"     | #5              | 30"    | 30"     |
| 7                             | 3/4"              | 18"    | 12"     | #5              | 30"    | 30"     |
| 8                             | 1"                | 19"    | 12"     | #5              | 30"    | 30"     |
| 9                             | 1"                | 19"    | 12"     | #5              | 30"    | 30"     |
| 10                            | 1"                | 19"    | 12"     | #5              | 30"    | 30"     |
| 11                            | 1"                | 19"    | 12"     | #5              | 30"    | 30"     |
| 12                            | 1"                | 19"    | 12"     | #5              | 30"    | 30"     |
| 13                            | 1 - 1/4"          | 20"    | 15"     | #5              | 30"    | 30"     |
| 14                            | 1 - 1/4"          | 20"    | 15"     | #5              | 30"    | 30"     |
| 15                            | 1 - 1/4"          | 20"    | 15"     | #5              | 30"    | 30"     |
| 16                            | 1 - 1/4"          | 20"    | 15"     | #5              | 30"    | 30"     |
| 17                            | 1 - 1/2"          | 20"    | 18"     | #5              | 30"    | 30"     |
| 18                            | 1 - 1/2"          | 20"    | 18"     | #5              | 30"    | 30"     |

|                                     | DETAIL 1   | DETAIL 2 | DETAIL 3 |
|-------------------------------------|------------|----------|----------|
| W=WIDTH OF SEALANT RESERVOIR (IN.)  | 3/4 +/-1/8 | 1/2      | 1/2      |
| D=DEPTH OF SEALANT RESERVOIR (IN.)  | 3/4 +/-1/8 | 1/4      | 1/4      |
| B=BACKER ROD DIAMETER (IN.)         | 7/8        | 5/8      | 5/8      |
| S=SECOND SAWCUT DEPTH (IN.) MINIMUM | N/A        | 1-3/8    | 1-3/8    |

100% SUBMITTAL  
JUNE 3, 2016

RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER



GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL  
AIRPORT  
PEORIA, ILLINOIS

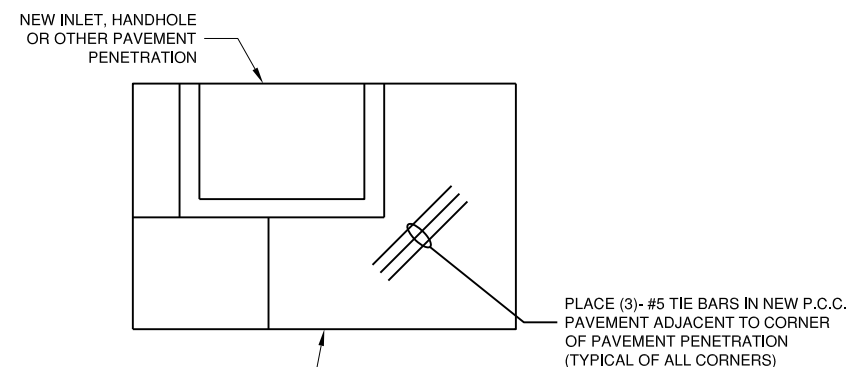
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AIP PROJ. NO. 3-17-0080-XX  
IL PROJ. NO. PIA-4542 CONTRACT NO. PE001  
CMT PROJECT NO: 15061-05  
CAD DWG FILE: 15061-05\_CP502.DWG  
DESIGNED BY: JMW  
DRAWN BY: CMT  
CHECKED BY: APP  
APPROVED BY: CHK  
COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015

SHEET TITLE  
**PAVING AND  
MISCELLANEOUS  
DETAILS**

SHEET **24** OF **32**

CP502

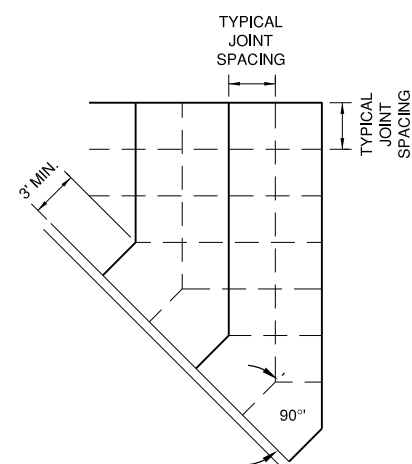


**PAVEMENT PENETRATION DETAIL**

N.T.S.

PLACE (3) - #5 TIE BARS IN NEW P.C.C. PAVEMENT ADJACENT TO CORNER OF PAVEMENT PENETRATION (TYPICAL OF ALL CORNERS)

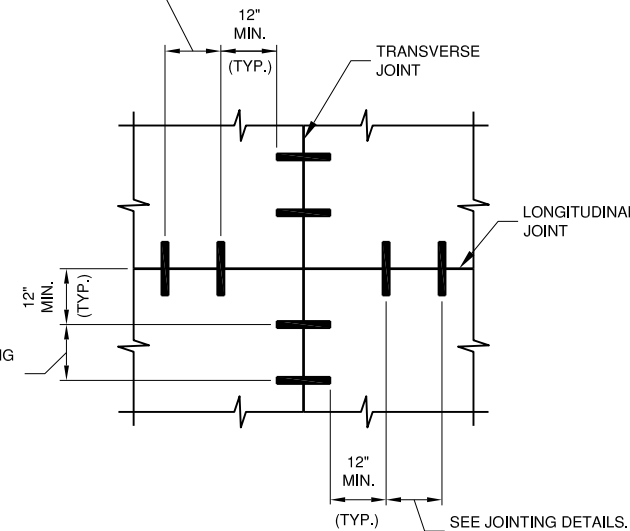
NEW P.C.C. PAVEMENT



**JOINTING AT SKEWED EDGE**

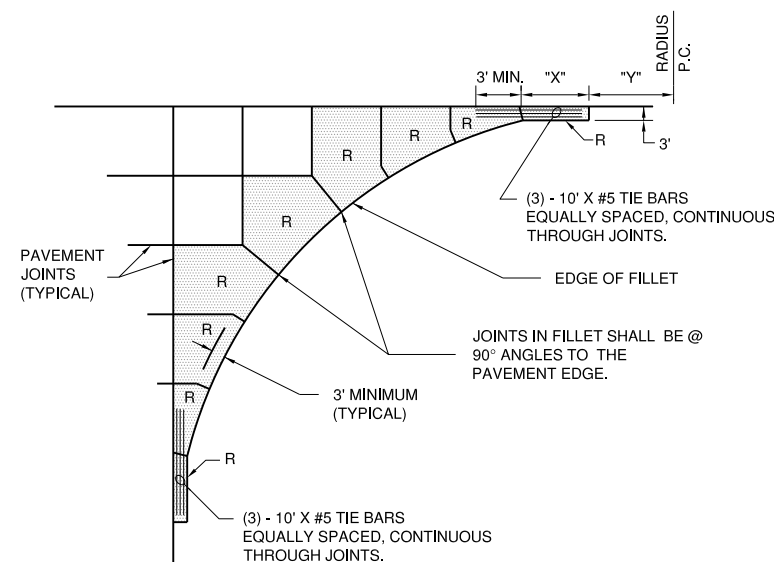
N.T.S.

SEE JOINTING DETAILS. TABLE 2



**DOWEL PLACEMENT DETAIL**

N.T.S.

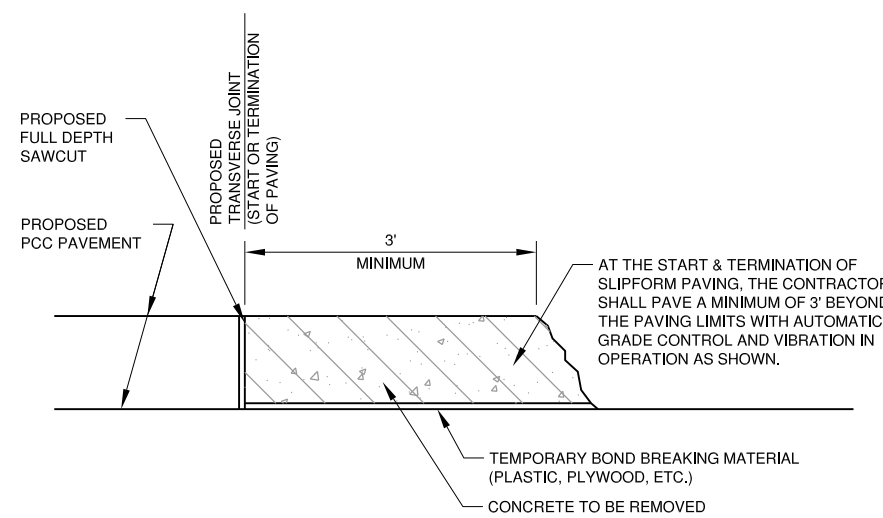


**FILLET DETAIL & FILLET REINFORCING LAYOUT**

N.T.S.

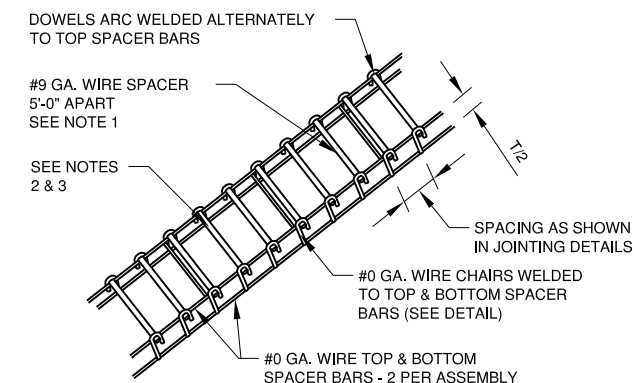
Ⓡ DENOTES ODD SHAPED REINFORCED PANELS TO BE REINFORCED WITH WELDED WIRE FABRIC AS SHOWN ON JOINTING DETAILS SHEET. ALL NON RECTANGULAR SHAPED PANELS SHALL BE REINFORCED. (REINFORCEMENT NOT SHOWN)

| FILLET RADIUS | "X" (IN FEET) | "Y" (IN FEET) |
|---------------|---------------|---------------|
| 20            | 4.30          | 6.24          |
| 25            | 4.88          | 7.00          |
| 30            | 5.40          | 7.68          |
| 50            | 7.11          | 9.95          |
| 75            | 8.79          | 12.21         |
| 85            | 9.38          | 13.00         |
| 100           | 10.21         | 14.11         |
| 125           | 11.44         | 15.78         |
| 150           | 12.56         | 17.29         |
| 175           | 13.58         | 18.68         |
| 200           | 14.53         | 19.98         |



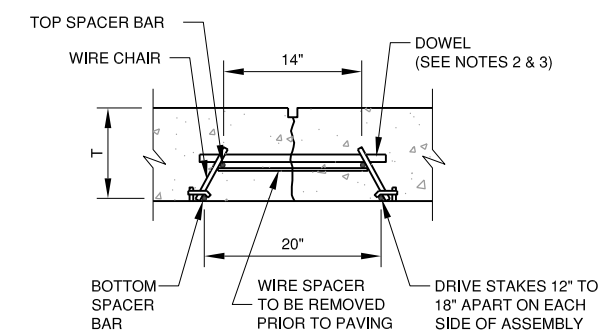
**REQUIREMENTS AT START & TERMINATION OF SLIPFORM PAVING**

N.T.S.



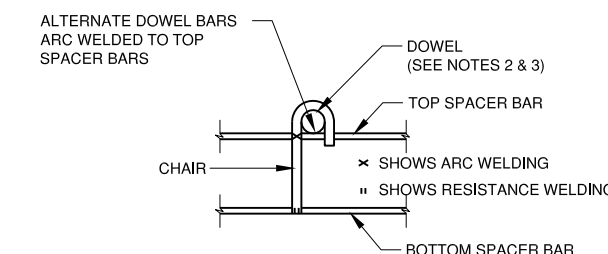
**DOWEL BASKET ASSEMBLY DETAIL**

N.T.S.



**DOWEL BAR INSTALLATION DETAIL**

N.T.S.



**TYPICAL DOWEL BASKET ELEVATION  
DETAIL SHOWING CHAIR**

N.T.S.

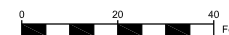
**DOWEL BASKET DETAILS**

N.T.S.

**DOWEL BASKET NOTES**

- #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.
- DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN TABLE 2.
- DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.




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 JUNE 3, 2016

 RECONSTRUCT TERMINAL  
 APRON PHASE 2

OWNER

 GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL AIRPORT

 GENERAL WAYNE A. DOWNING  
 PEORIA INTERNATIONAL  
 AIRPORT  
 PEORIA, ILLINOIS

| MARK | DATE | DESCRIPTION |
|------|------|-------------|
|      |      |             |
|      |      |             |

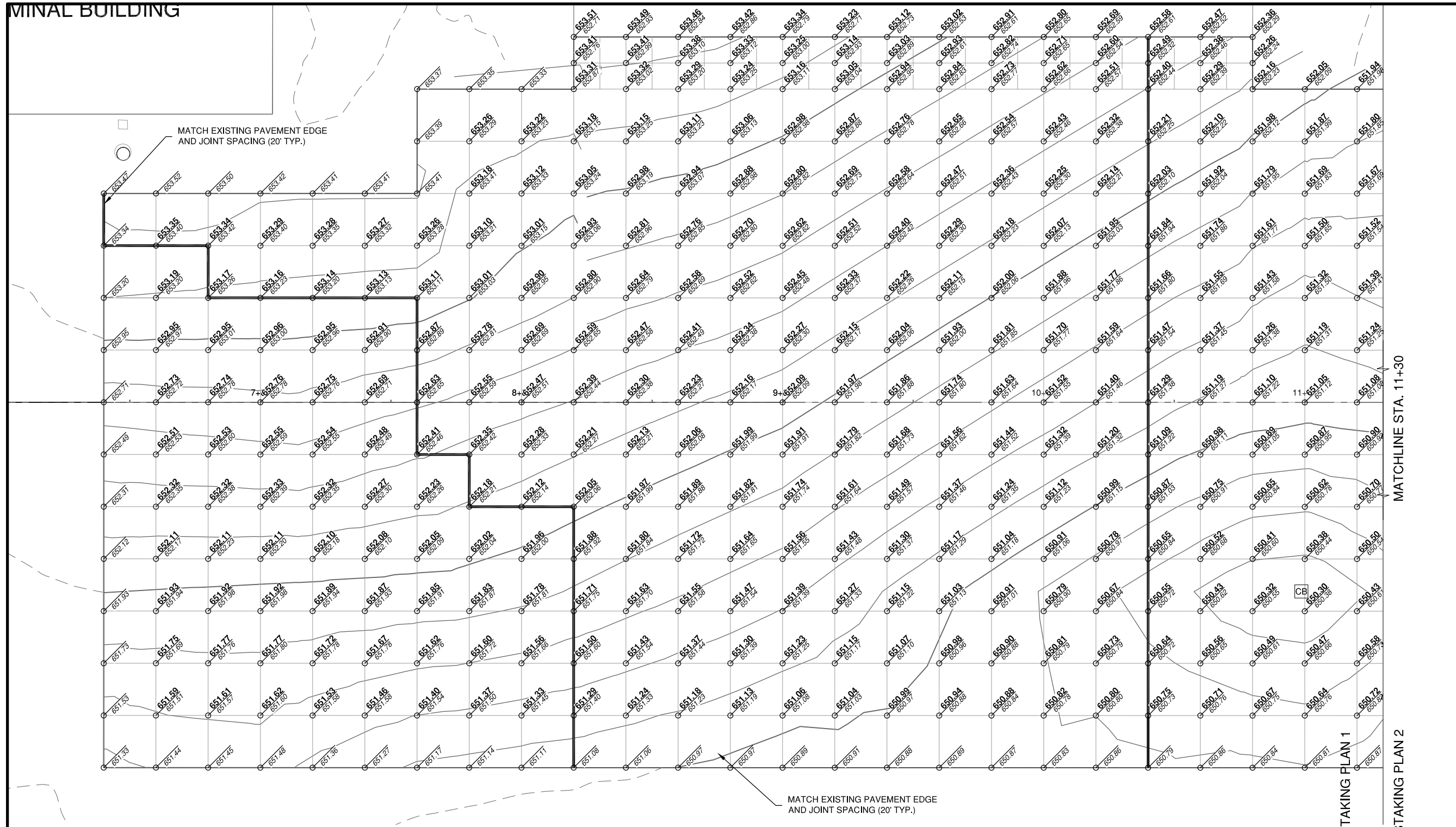
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| AIP PROJ. NO. 3-17-0080-XX                     |
| IL PROJ. NO. PIA-4542 CONTRACT NO. PE001       |
| CMT PROJECT NO: 15061-05                       |
| CAD DWG FILE: 15061-05_CP103.DWG               |
| DESIGNED BY: JMW                               |
| DRAWN BY: CMT                                  |
| CHECKED BY: APP                                |
| APPROVED BY: CHK                               |
| COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |

SHEET TITLE

**STAKING PLAN 1**

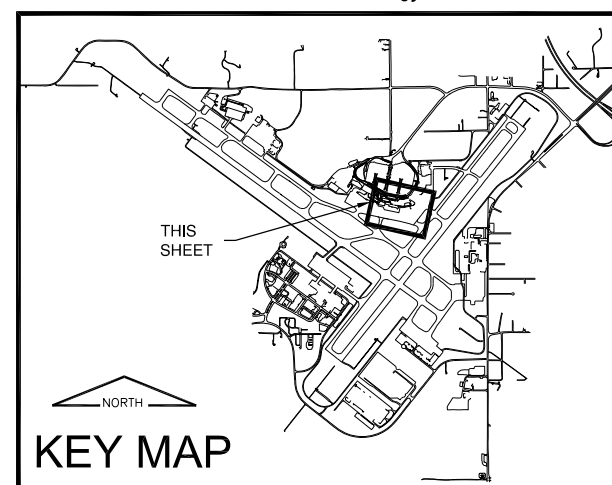
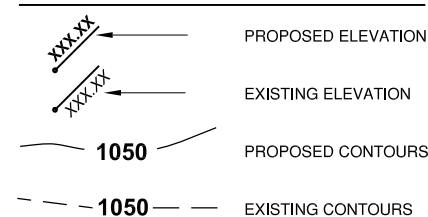
 SHEET **25** OF **32**

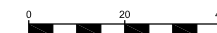
MINIAL BUILDING


 MATCH EXISTING PAVEMENT EDGE  
 AND JOINT SPACING (20' TYP.)

**NOTES:**

- SEE JOINTING PLANS FOR MORE INFORMATION ON JOINT TYPES.

**LEGEND**




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JUNE 3, 2016

RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER

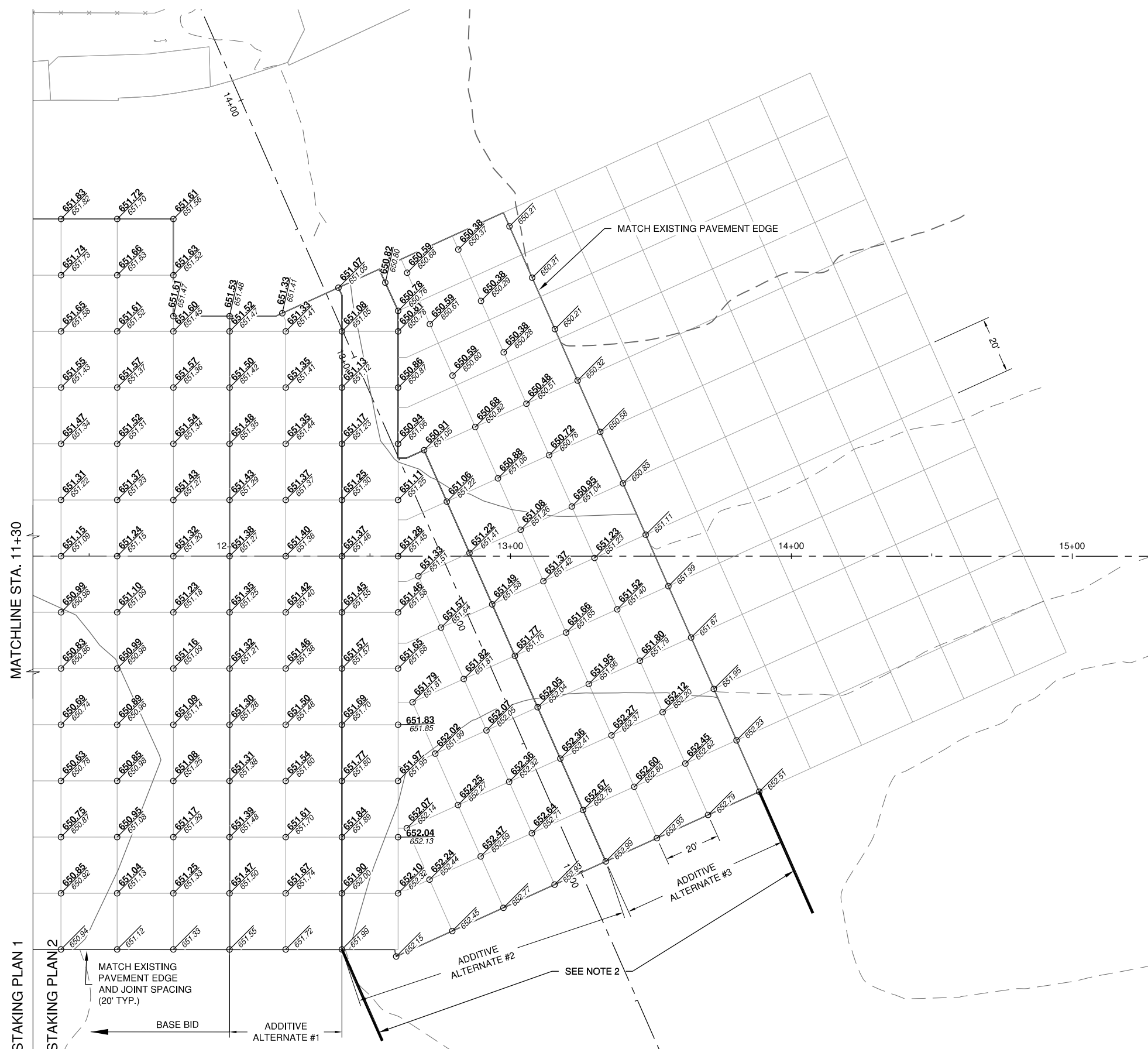


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AIRPORT  
PEORIA, ILLINOIS

| MARK | DATE | DESCRIPTION |
|------|------|-------------|
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| AIP PROJ. NO. 3-17-0080-XX                     |
| IL PROJ. NO. PIA-4542 CONTRACT NO. PE001       |
| CMT PROJECT NO: 15061-05                       |
| CAD DWG FILE: 15061-05_CP104.DWG               |
| DESIGNED BY: JMW                               |
| DRAWN BY: CMT                                  |
| CHECKED BY: APP                                |
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| COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |

SHEET TITLE  
**STAKING PLAN 2**

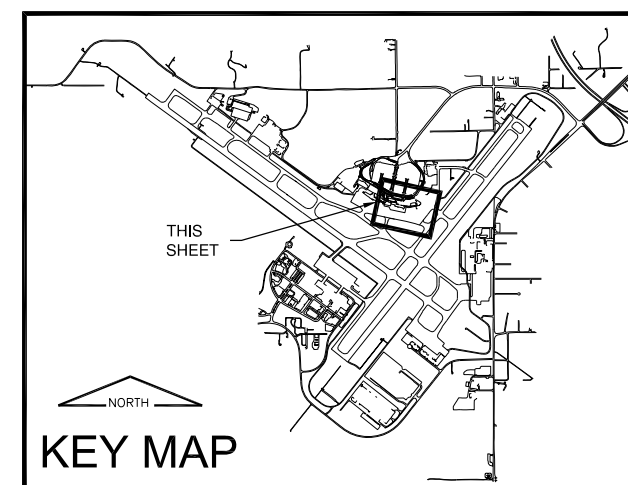


**NOTES:**

1. SEE JOINTING PLANS FOR MORE INFORMATION ON JOINT TYPES.
2. GRADING AT THE ADDITIVE ALTERNATES WILL BE REVISED TO MATCH EXISTING PAVEMENT IF ADDITIVE ADDITIVE ALTERNATES ARE NOT AWARDED.
3. NORTH LIMITS OF PAVING ADJACENT TO PAVEMENT TO BE CONSTRUCTED BY OTHERS. GRADES IN THIS AREA MAY BE REVISED TO MEET PROPOSED PAVEMENT (BY OTHERS).

**LEGEND**

- PROPOSED ELEVATION
- EXISTING ELEVATION
- 1050 PROPOSED CONTOURS
- 1050 EXISTING CONTOURS



**KEY MAP**

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JUNE 3, 2016

RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER



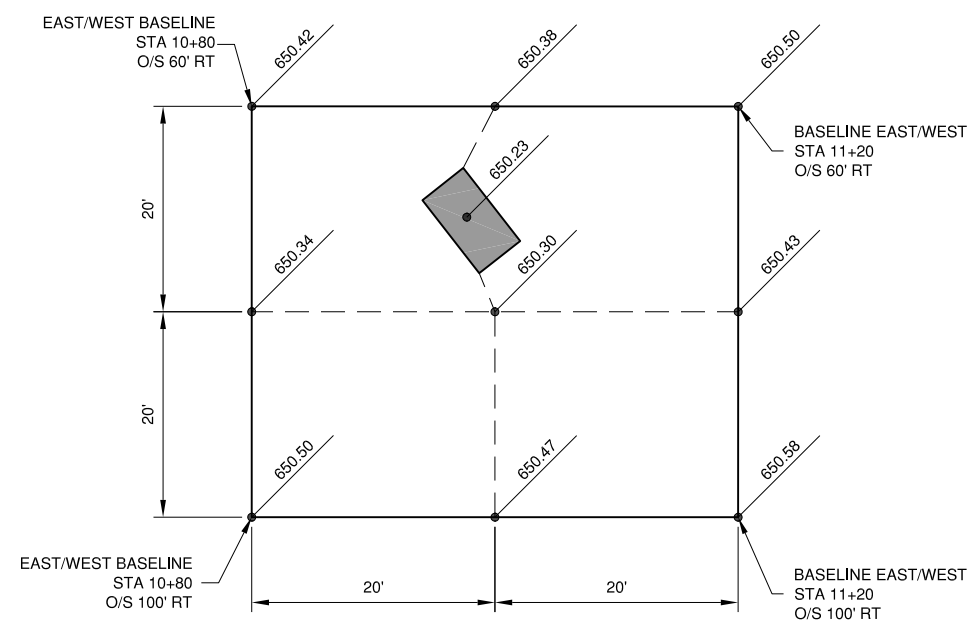
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AIRPORT  
PEORIA, ILLINOIS

| MARK | DATE | DESCRIPTION |
|------|------|-------------|
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|--|
| AIP PROJ. NO. 3-17-0080-XX                     |
| IL PROJ. NO. PIA-4542 CONTRACT NO. PE001       |
| CMT PROJECT NO: 15061-05                       |
| CAD DWG FILE: 15061-05_CP105.DWG               |
| DESIGNED BY: JMW                               |
| DRAWN BY: CMT                                  |
| CHECKED BY: APP                                |
| APPROVED BY: CHK                               |
| COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |

SHEET TITLE  
**INLET STAKE PLAN**

SHEET **27** OF **32** CP105



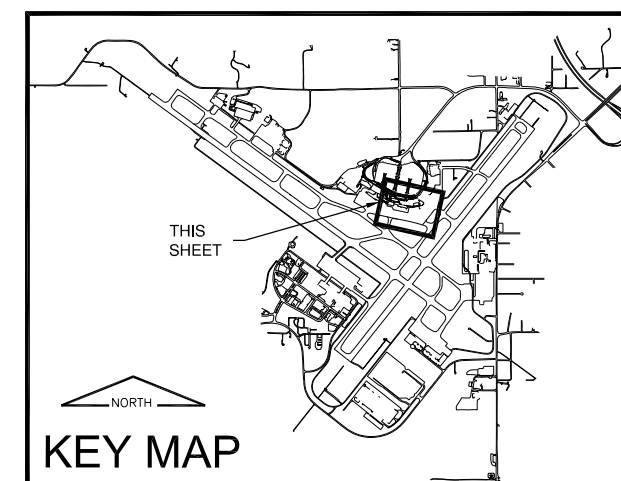
(IN-1)

**INLET STAKING PLAN**

N.T.S.

**NOTES:**

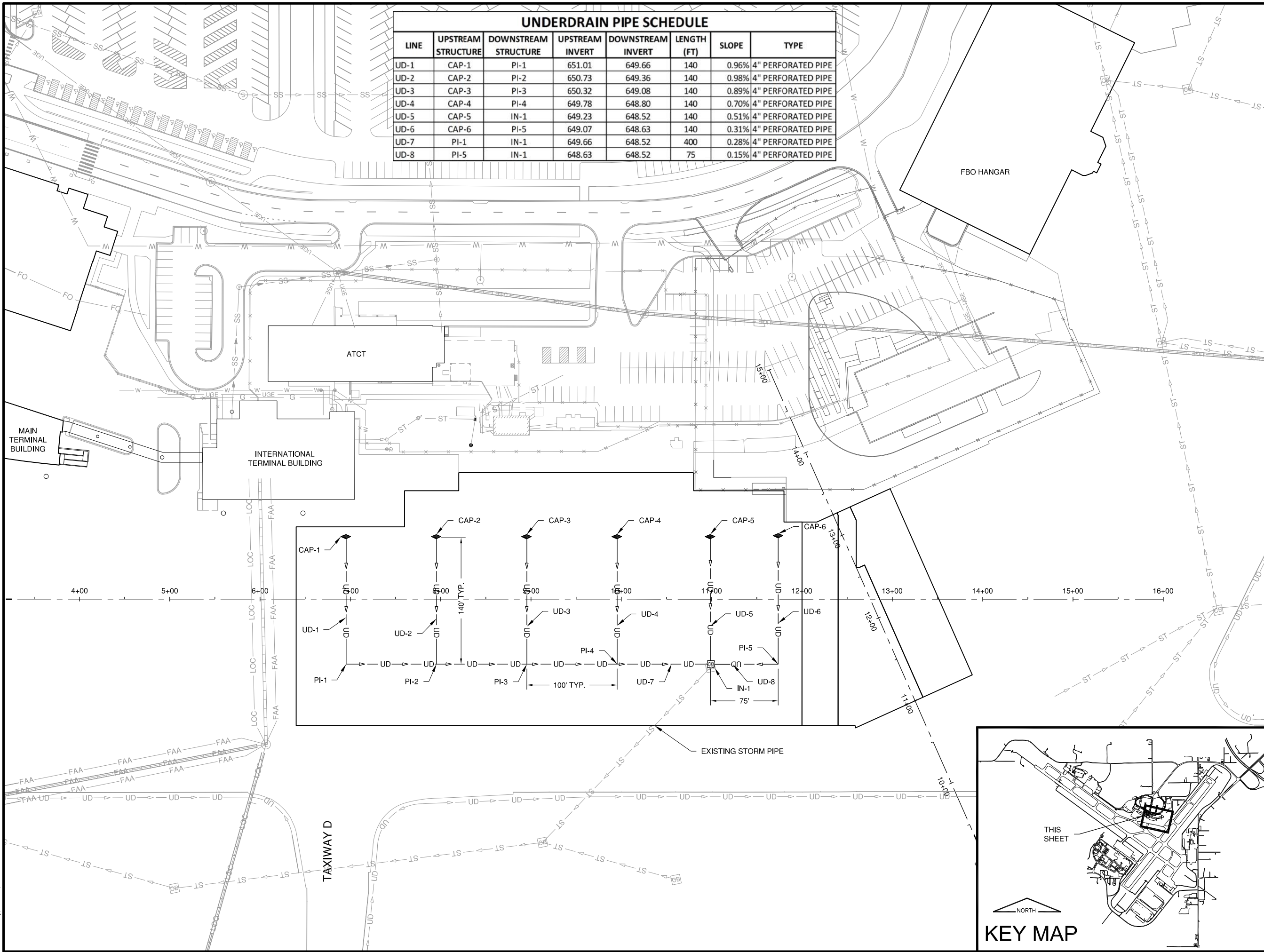
1. IN ADDITION TO THE MESH REINFORCING IN THE PANELS AROUND PAVEMENT PENETRATIONS, THE CONTRACTOR SHALL PLACE 3 - #5 BARS OFF THE CORNERS OF ALL INLETS OR OTHER PAVEMENT PENETRATIONS AS SHOWN IN THE "PAVEMENT PENETRATION DETAIL" ON THE PAVING AND MISCELLANEOUS DETAILS SHEET.
2. THE CONTRACTOR SHALL CONSTRUCT AND SEAL AN EXPANSION JOINT AROUND THE PROPOSED AND EXISTING INLETS TO REDUCE THE POSSIBILITY OF SYMPATHETIC CRACKING.



**KEY MAP**

### UNDERDRAIN PIPE SCHEDULE

| LINE | UPSTREAM STRUCTURE | DOWNSTREAM STRUCTURE | UPSTREAM INVERT | DOWNSTREAM INVERT | LENGTH (FT) | SLOPE | TYPE               |
|------|--------------------|----------------------|-----------------|-------------------|-------------|-------|--------------------|
| UD-1 | CAP-1              | PI-1                 | 651.01          | 649.66            | 140         | 0.96% | 4" PERFORATED PIPE |
| UD-2 | CAP-2              | PI-2                 | 650.73          | 649.36            | 140         | 0.98% | 4" PERFORATED PIPE |
| UD-3 | CAP-3              | PI-3                 | 650.32          | 649.08            | 140         | 0.89% | 4" PERFORATED PIPE |
| UD-4 | CAP-4              | PI-4                 | 649.78          | 648.80            | 140         | 0.70% | 4" PERFORATED PIPE |
| UD-5 | CAP-5              | IN-1                 | 649.23          | 648.52            | 140         | 0.51% | 4" PERFORATED PIPE |
| UD-6 | CAP-6              | PI-5                 | 649.07          | 648.63            | 140         | 0.31% | 4" PERFORATED PIPE |
| UD-7 | PI-1               | IN-1                 | 649.66          | 648.52            | 400         | 0.28% | 4" PERFORATED PIPE |
| UD-8 | PI-5               | IN-1                 | 648.63          | 648.52            | 75          | 0.15% | 4" PERFORATED PIPE |



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CONSULTANTS



0 50 100 Feet

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RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER

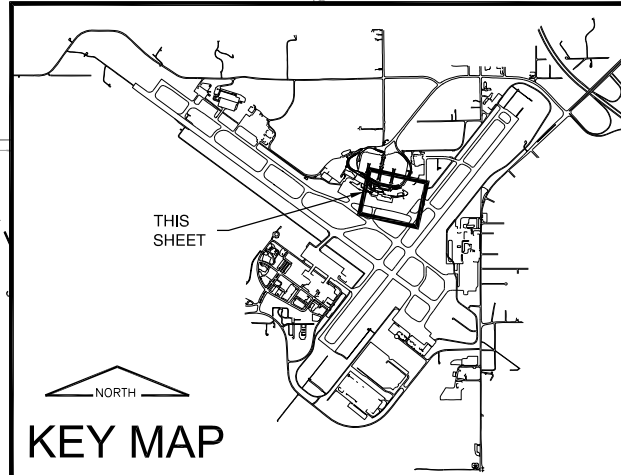


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AIRPORT  
PEORIA, ILLINOIS

MARK | DATE | DESCRIPTION

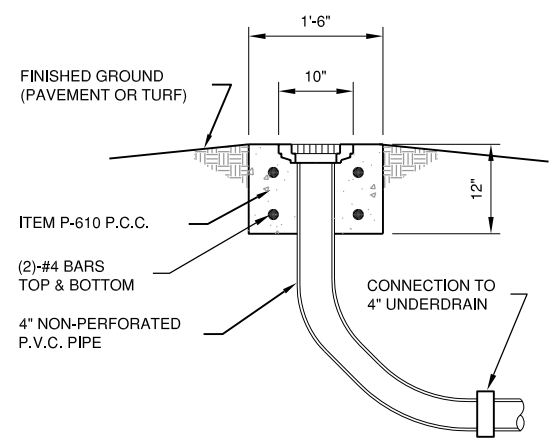
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| IL PROJ. NO.    | PIA-4542 CONTRACT NO. PE001         |
| CMT PROJECT NO. | 15061-05                            |
| CAD DWG FILE:   | 15061-05_CP106.DWG                  |
| DESIGNED BY:    | JMW                                 |
| DRAWN BY:       | CMT                                 |
| CHECKED BY:     | APP                                 |
| APPROVED BY:    | CHK                                 |
| COPYRIGHT:      | CRAWFORD, MURPHY & TILLY, INC. 2015 |

SHEET TITLE  
**UNDERDRAIN PLAN**

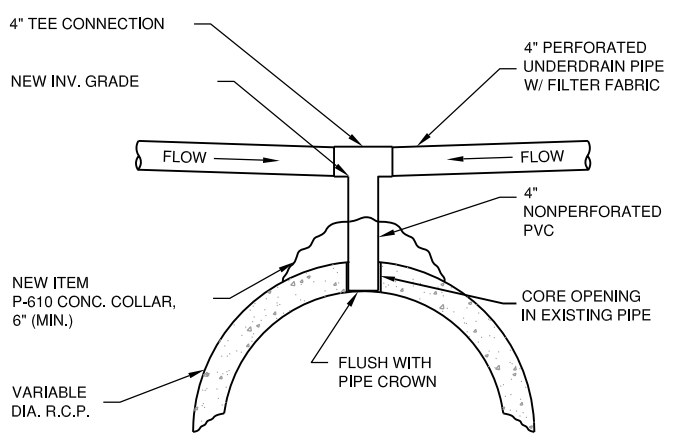


KEY MAP

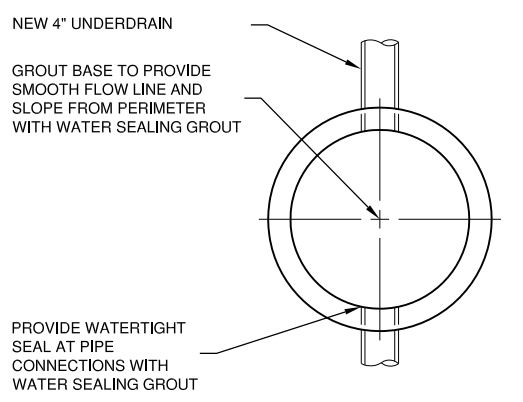
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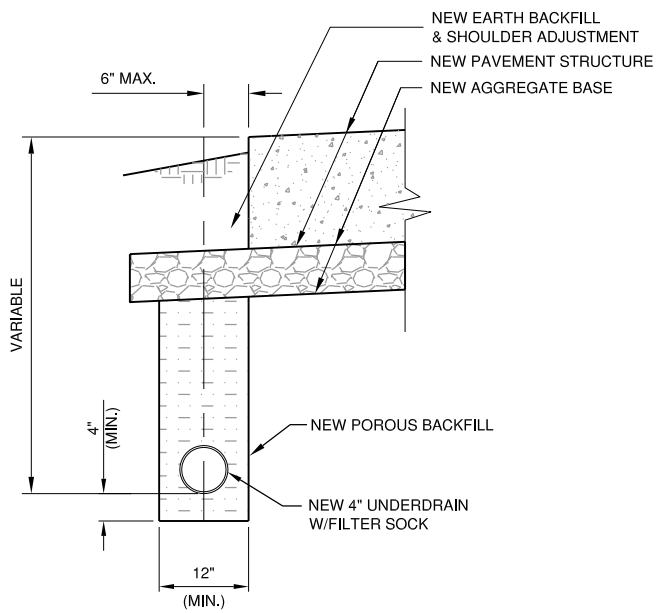
**SIDE VIEW**  
**UNDERDRAIN CLEAN-OUT DETAIL (TYPE 1)**  
N.T.S.



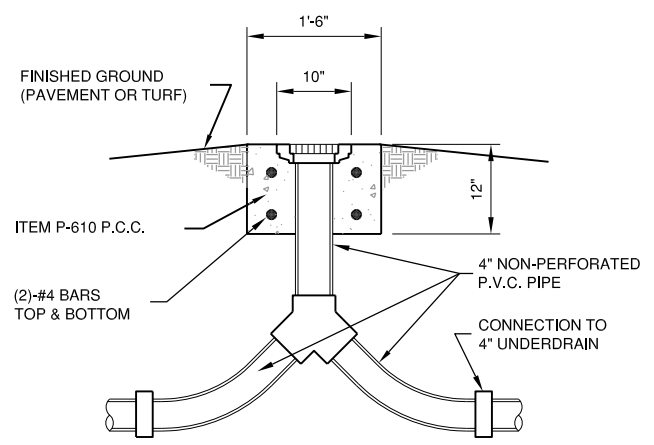
**UNDERDRAIN DIRECT TOP CONNECTION DETAIL**  
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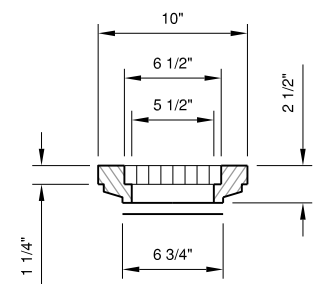
**UNDERDRAIN CLEAN-OUT DETAIL (PLAN)**  
N.T.S.



**TYPICAL UNDERDRAIN DETAIL PAVEMENT EDGE**  
N.T.S.

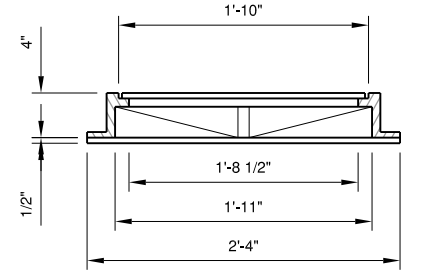


**UNDERDRAIN CLEAN-OUT DETAIL (TYPE 2)**  
N.T.S.



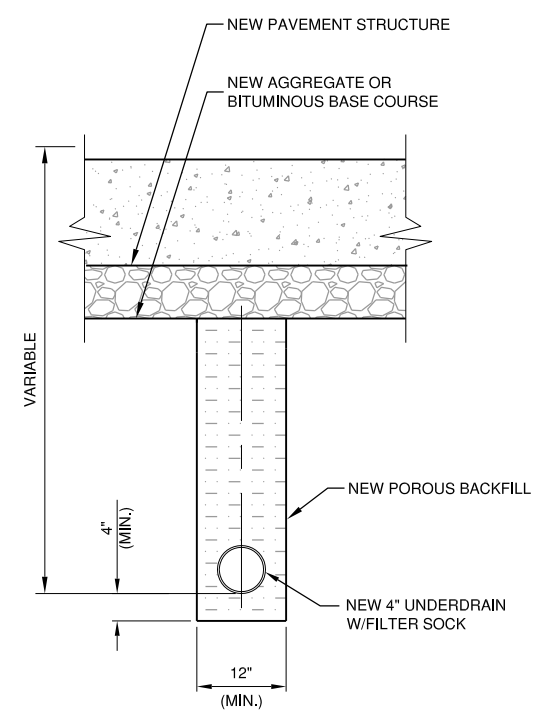
CAST IRON FRAME & COVER (SIMILAR TO NEENAH R-6013 OR APPROVED EQUAL)

**FRAME & COVER**

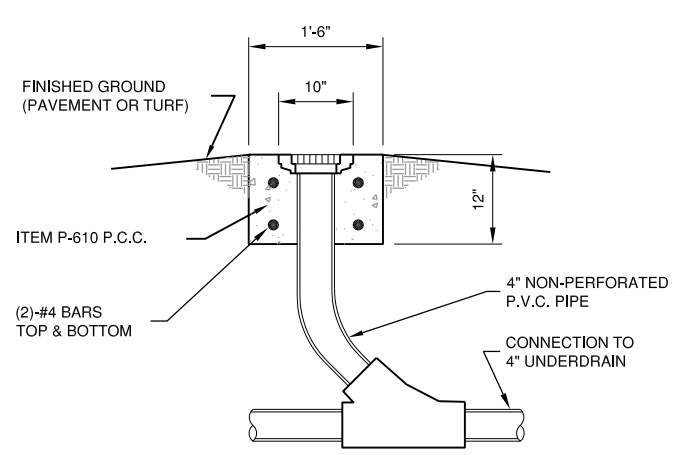


CAST IRON FRAME & LID (SIMILAR TO NEENAH R-1690-A HEAVY DUTY OR APPROVED EQUAL)

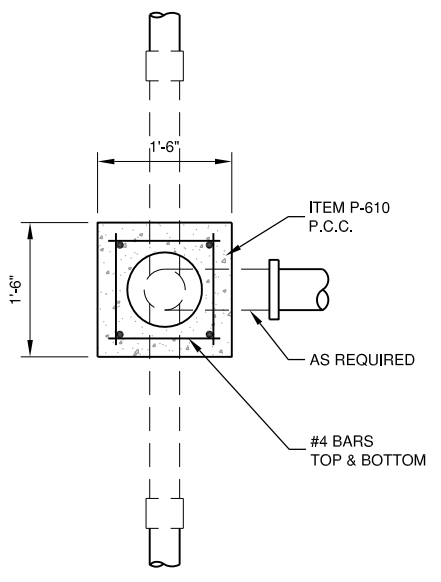
**FRAME & LID**



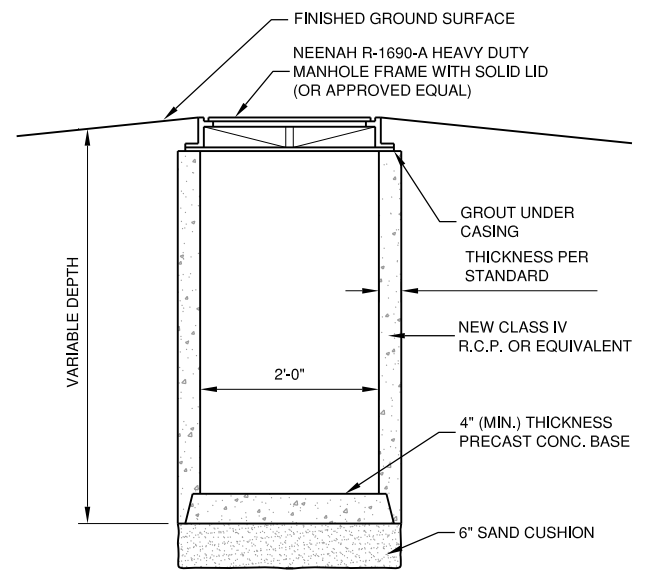
**TYPICAL UNDERDRAIN DETAIL BELOW PAVEMENT**  
N.T.S.



**UNDERDRAIN CLEAN-OUT DETAIL (TYPE 3)**  
N.T.S.



**UNDERDRAIN CLEAN-OUT DETAIL (PLAN)**  
N.T.S.



**UNDERDRAIN COLLECTION STRUCTURE DETAIL**  
N.T.S.

**UNDERDRAIN NOTE**  
DURING UNDERDRAIN CONSTRUCTION, CARE SHALL BE TAKEN TO ENSURE GOOD DRAINAGE BETWEEN THE SUB-BASE AND THE POROUS BACKFILL MATERIAL WHICH WILL AFFECT DRAINAGE TO THE UNDERDRAIN SHALL BE REMOVED PRIOR TO THE PLACEMENT OF PROPOSED POROUS BACKFILL. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL.

100% SUBMITTAL  
JUNE 3, 2016

**RECONSTRUCT TERMINAL APRON PHASE 2**



GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL AIRPORT  
PEORIA, ILLINOIS

| MARK   | DATE | DESCRIPTION        |
|--|------|--------------------|
| AIP PROJ. NO. 3-17-0080-XX                     |      |                    |
| IL PROJ. NO. PIA-4542                          |      | CONTRACT NO. PE001 |
| CMT PROJECT NO:                                |      | 15061-05           |
| CAD DWG FILE:                                  |      | 15061-05_CP503.DWG |
| DESIGNED BY:                                   |      | JMW                |
| DRAWN BY:                                      |      | CMT                |
| CHECKED BY:                                    |      | APP                |
| APPROVED BY:                                   |      | CHK                |
| COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |      |                    |

**UNDERDRAIN DETAILS**

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Date: Thursday, June 16, 2016 3:03:45 PM



100% SUBMITTAL  
JUNE 3, 2016

RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER



GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL  
AIRPORT  
PEORIA, ILLINOIS

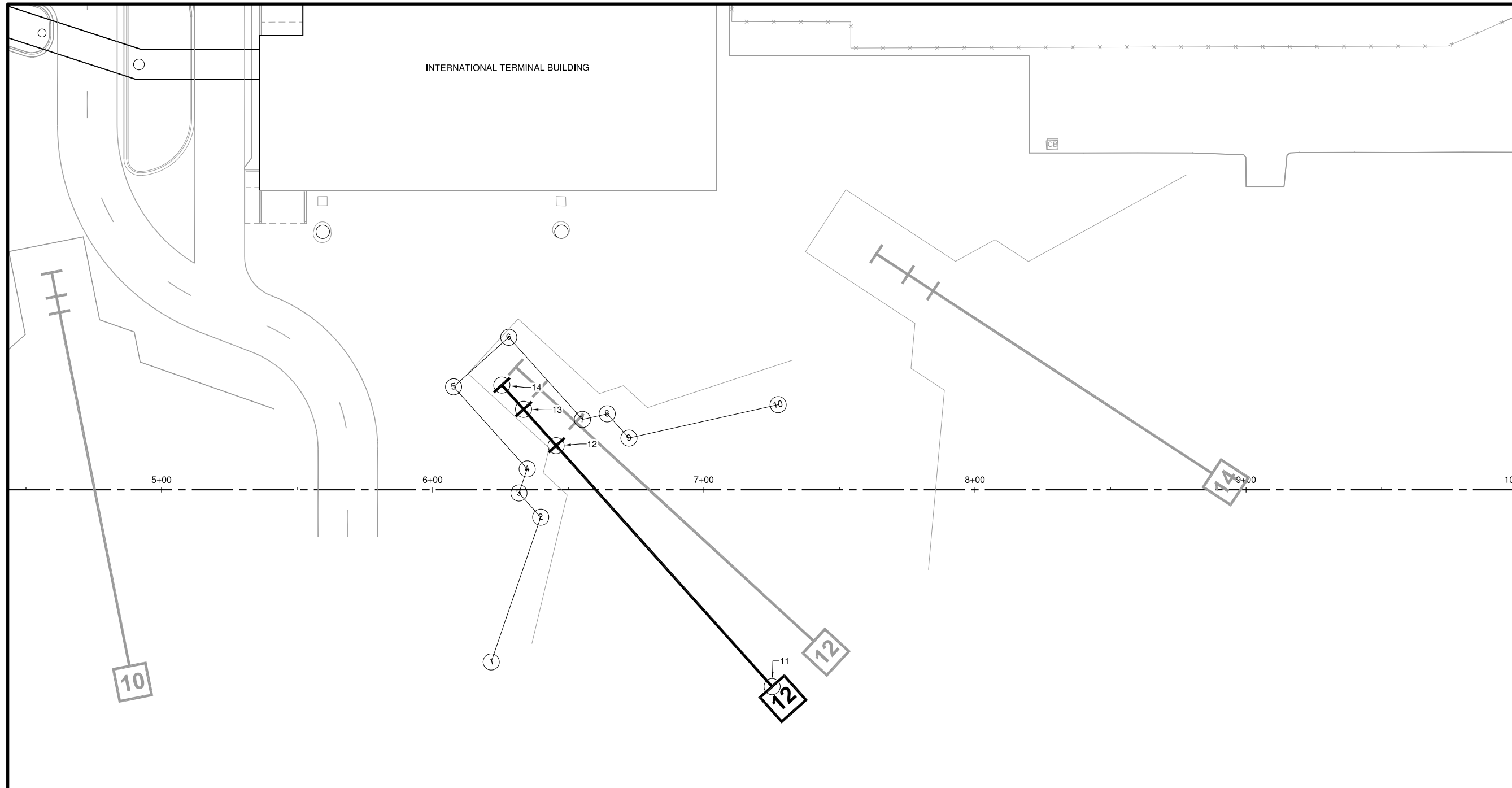
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|                         |                                     |
|-------------------------|-------------------------------------|
| AIP PROJ. NO.           | 3-17-0080-XX                        |
| IL PROJ. NO. / PIA-4542 | CONTRACT NO. PE001                  |
| CMT PROJECT NO.         | 15061-05                            |
| CAD DWG FILE:           | 15061-05_CM101.DWG                  |
| DESIGNED BY:            | JMW                                 |
| DRAWN BY:               | CMT                                 |
| CHECKED BY:             | APP                                 |
| APPROVED BY:            | CHK                                 |
| COPYRIGHT:              | CRAWFORD, MURPHY & TILLY, INC. 2015 |

SHEET TITLE

MARKING PLAN 1

INTERNATIONAL TERMINAL BUILDING



| POINT | NORTHING     | EASTING      |
|-------|--------------|--------------|
| 1     | 1456483.3575 | 2428625.5014 |
| 2     | 1456531.7956 | 2428654.3968 |
| 3     | 1456542.1914 | 2428648.4130 |
| 4     | 1456550.2512 | 2428653.2216 |
| 5     | 1456585.5133 | 2428632.9235 |
| 6     | 1456599.1505 | 2428656.6151 |
| 7     | 1456563.8876 | 2428676.9122 |
| 8     | 1456563.9973 | 2428686.2973 |
| 9     | 1456553.6025 | 2428692.2811 |
| 10    | 1456554.2604 | 2428748.6789 |
| 11    | 1456453.0009 | 2428724.9712 |
| 12    | 1456556.4341 | 2428665.4319 |
| 13    | 1456572.0353 | 2428656.4526 |
| 14    | 1456582.4359 | 2428650.4665 |

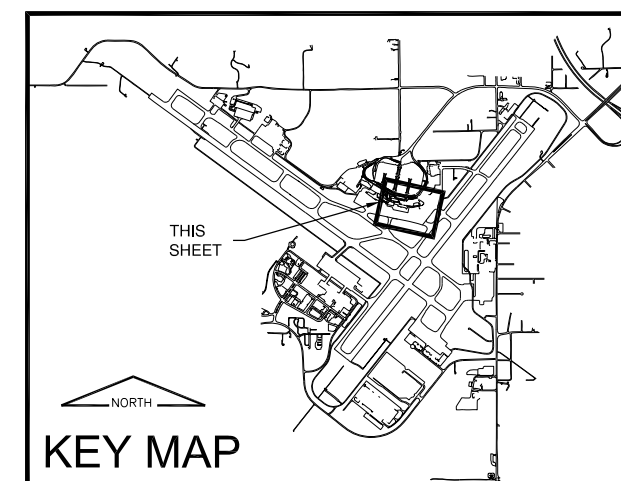
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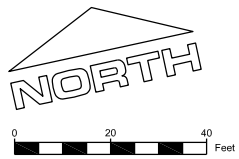
- THE TEMPORARY MARKING LOCATION FOR GATE 12 WILL BE MARKED PRIOR TO THE START OF PHASE 2 AND REMOVED PRIOR TO THE START OF PHASE 3.

TEMPORARY GATE MARKING

LEGEND

- EXISTING MARKING
- PROPOSED TEMPORARY MARKING
- MARKING LAYOUT POINT





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JUNE 3, 2016

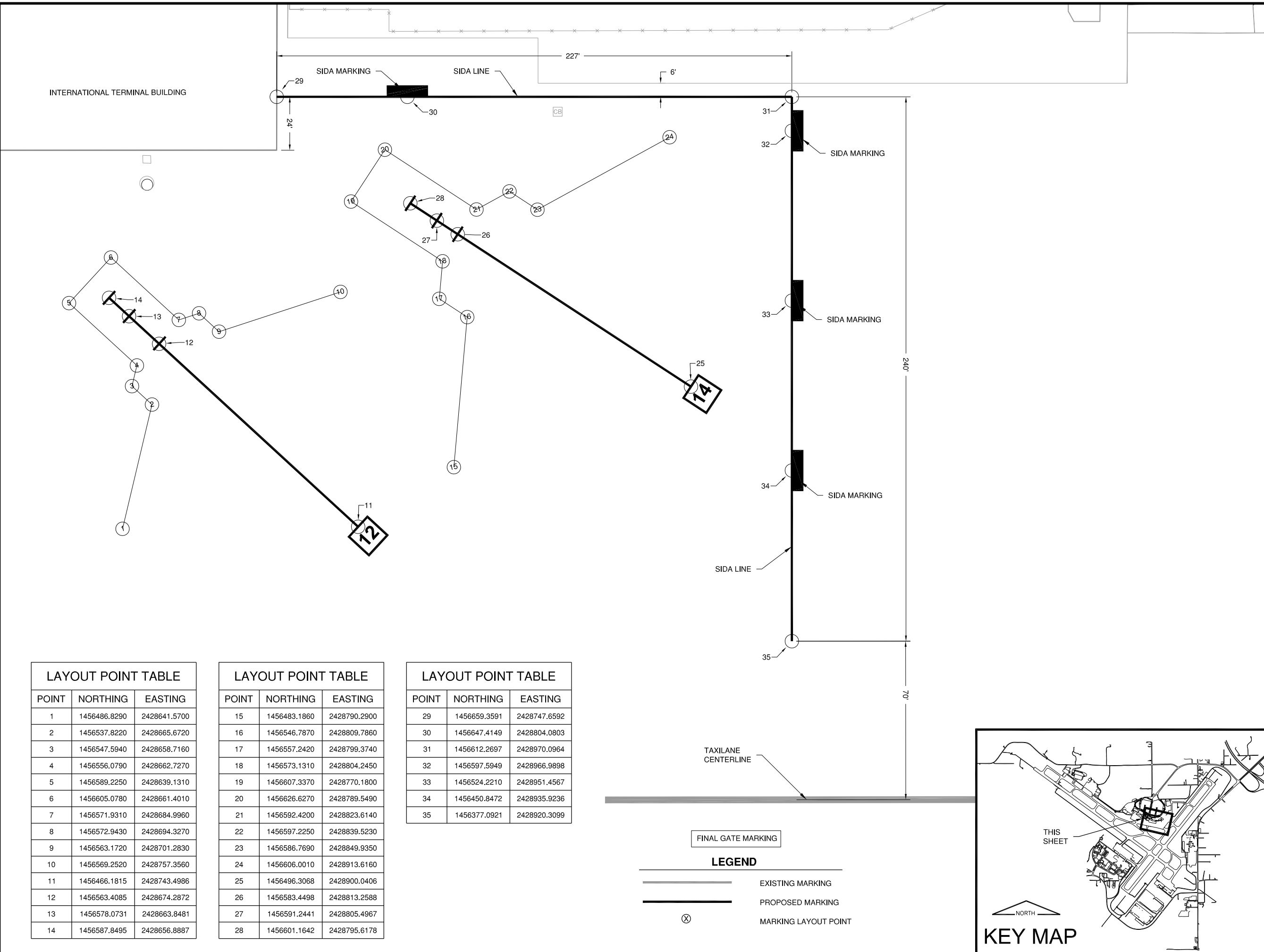
RECONSTRUCT TERMINAL  
APRON PHASE 2



GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL  
AIRPORT  
PEORIA, ILLINOIS

| MARK   | DATE | DESCRIPTION        |
|--|------|--------------------|
| AIP PROJ. NO. 3-17-0080-XX                     |      |                    |
| IL PROJ. NO. PIA-4542                          |      | CONTRACT NO. PE001 |
| CMT PROJECT NO: 15061-05                       |      |                    |
| CAD DWG FILE: 15061-05_CM102.DWG               |      |                    |
| DESIGNED BY: JMW                               |      |                    |
| DRAWN BY: CMT                                  |      |                    |
| CHECKED BY: APP                                |      |                    |
| APPROVED BY: CHK                               |      |                    |
| COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |      |                    |

SHEET TITLE  
**MARKING PLAN 2**



LAYOUT POINT TABLE

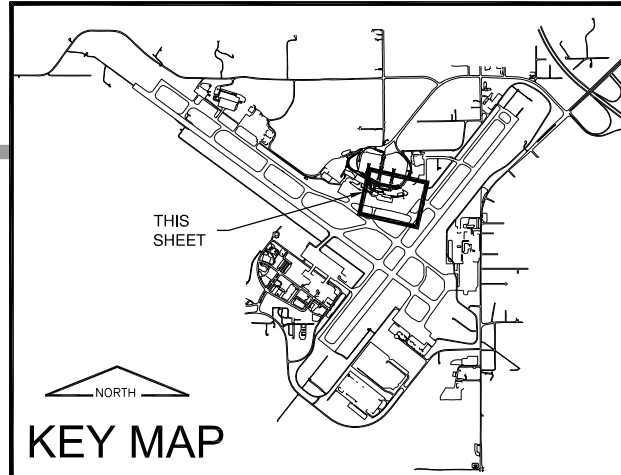
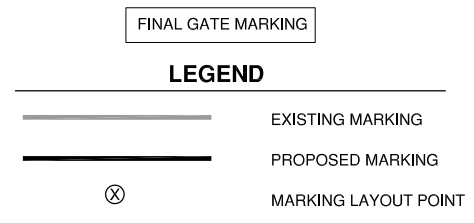
| POINT | NORTHING     | EASTING      |
|-------|--------------|--------------|
| 1     | 1456486.8290 | 2428641.5700 |
| 2     | 1456537.8220 | 2428665.6720 |
| 3     | 1456547.5940 | 2428658.7160 |
| 4     | 1456556.0790 | 2428662.7270 |
| 5     | 1456589.2250 | 2428639.1310 |
| 6     | 1456605.0780 | 2428661.4010 |
| 7     | 1456571.9310 | 2428684.9960 |
| 8     | 1456572.9430 | 2428694.3270 |
| 9     | 1456563.1720 | 2428701.2830 |
| 10    | 1456569.2520 | 2428757.3560 |
| 11    | 1456466.1815 | 2428743.4986 |
| 12    | 1456563.4085 | 2428674.2872 |
| 13    | 1456578.0731 | 2428663.8481 |
| 14    | 1456587.8495 | 2428656.8887 |

LAYOUT POINT TABLE

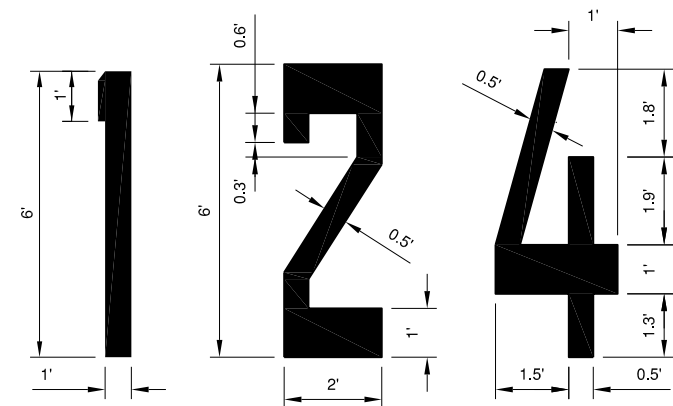
| POINT | NORTHING     | EASTING      |
|-------|--------------|--------------|
| 15    | 1456483.1860 | 2428790.2900 |
| 16    | 1456546.7870 | 2428809.7860 |
| 17    | 1456557.2420 | 2428799.3740 |
| 18    | 1456573.1310 | 2428804.2450 |
| 19    | 1456607.3370 | 2428770.1800 |
| 20    | 1456626.6270 | 2428789.5490 |
| 21    | 1456592.4200 | 2428823.6140 |
| 22    | 1456597.2250 | 2428839.5230 |
| 23    | 1456586.7690 | 2428849.9350 |
| 24    | 1456606.0010 | 2428913.6160 |
| 25    | 1456496.3068 | 2428900.0406 |
| 26    | 1456583.4498 | 2428813.2588 |
| 27    | 1456591.2441 | 2428805.4967 |
| 28    | 1456601.1642 | 2428795.6178 |

LAYOUT POINT TABLE

| POINT | NORTHING     | EASTING      |
|-------|--------------|--------------|
| 29    | 1456659.3591 | 2428747.6592 |
| 30    | 1456647.4149 | 2428804.0803 |
| 31    | 1456612.2697 | 2428970.0964 |
| 32    | 1456597.5949 | 2428966.9898 |
| 33    | 1456524.2210 | 2428951.4567 |
| 34    | 1456450.8472 | 2428935.9236 |
| 35    | 1456377.0921 | 2428920.3099 |



Path: K:\Peoria\AIP\15061-05\_TerminalApronRehab\PH2\Drawn\Sheets\15061-05\_CM102.dwg  
 Date: Thursday, June 16, 2016 3:04:52 PM

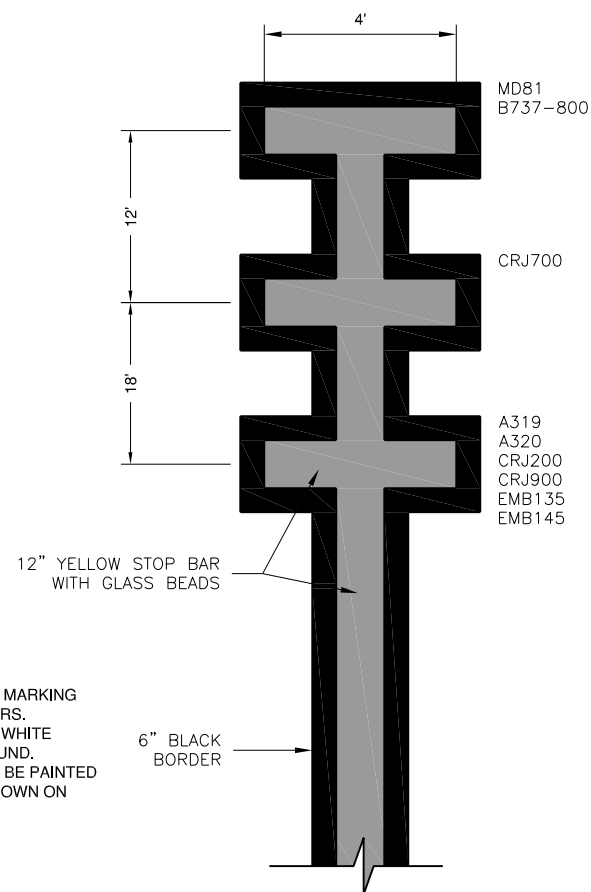


J-LINE NUMERALS (TYP.)

N.T.S.

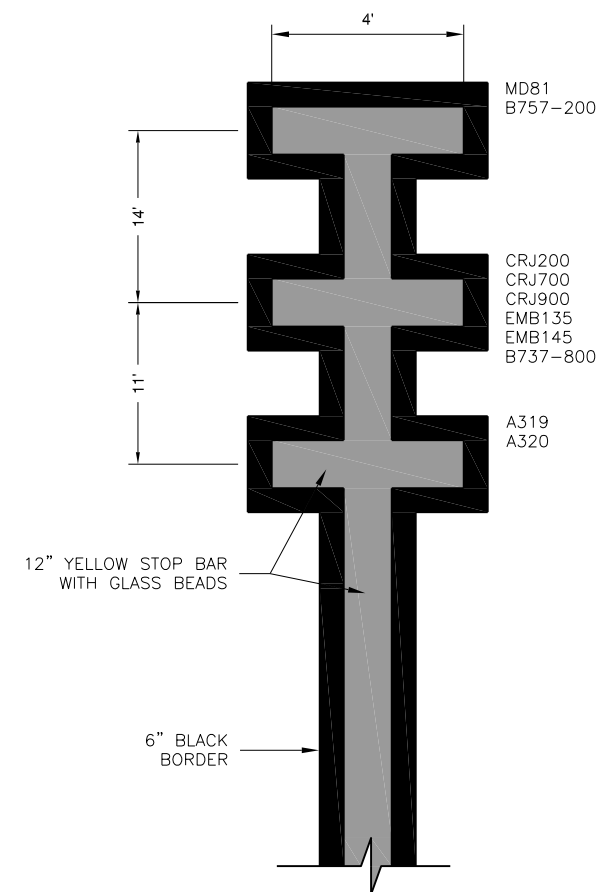
**NOTE:**

- ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE MEDIA & 6" BLACK BORDER.
- BLACK BORDER SHALL NOT REQUIRE REFLECTIVE MEDIA.



STOP BAR DETAIL - GATE 12

N.T.S.

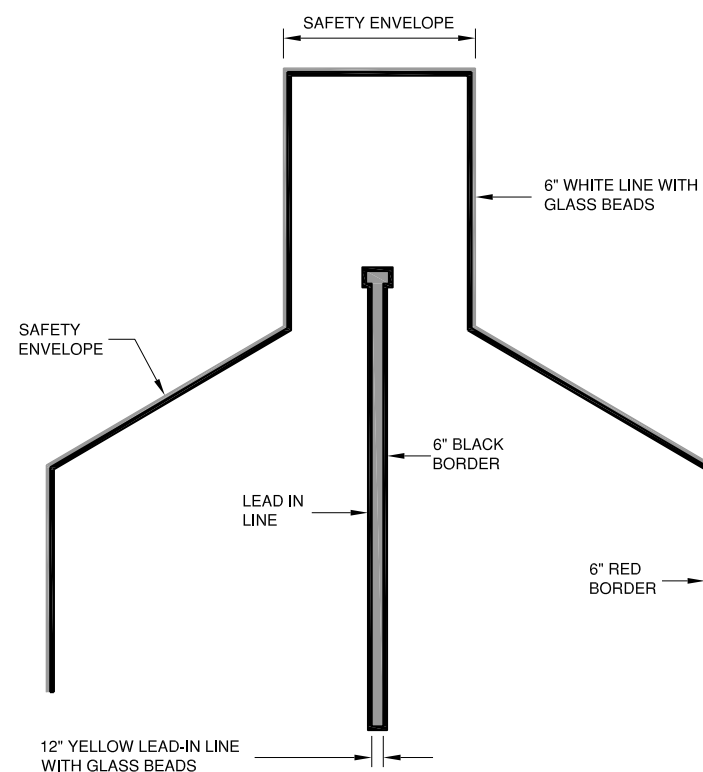


STOP BAR DETAIL - GATE 14

N.T.S.

**NOTE:**

- VERIFY WITH AIRPORT PRIOR TO MARKING THE AIRCRAFT TYPE AT STOP BARS.
- AIRCRAFT TYPE MARKING TO BE WHITE LETTERS WITH BLACK BACKGROUND.
- AIRCRAFT TYPE MARKING SHALL BE PAINTED 180° ROTATED FROM WHAT IS SHOWN ON THIS SHEET.

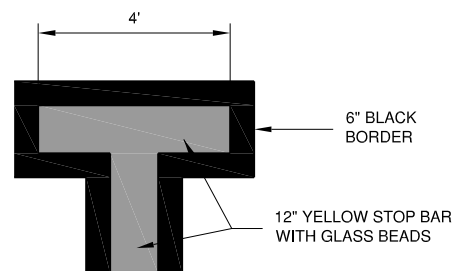


TYPICAL LEAD-IN AND SAFETY ENVELOPE FOR CONCRETE SURFACE

N.T.S.

**NOTE:**

- SEE MARKING PLANS FOR EXACT DIMENSIONS OF SAFETY ENVELOPES



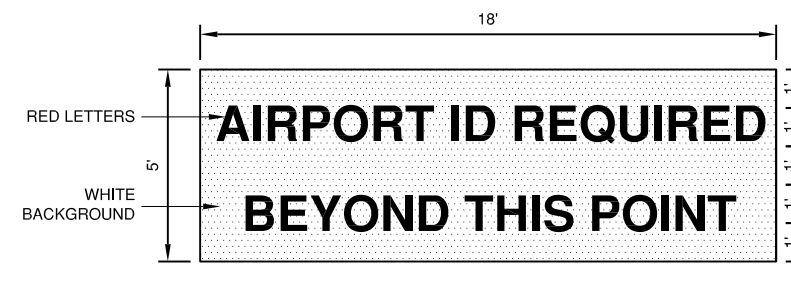
TYPICAL STOP BAR FOR CONCRETE SURFACE

N.T.S.



SIDA LINE

N.T.S.



SIDA MARKING

N.T.S.

100% SUBMITTAL  
JUNE 3, 2016

RECONSTRUCT TERMINAL  
APRON PHASE 2

OWNER



GENERAL WAYNE A. DOWNING  
PEORIA INTERNATIONAL  
AIRPORT  
PEORIA, ILLINOIS

| MARK | DATE | DESCRIPTION |
|------|------|-------------|
|      |      |             |
|      |      |             |
|      |      |             |
|      |      |             |

|  |
|--|
| AIP PROJ. NO. 3-17-0080-XX                     |
| IL PROJ. NO. PIA-4542 CONTRACT NO. PE001       |
| CMT PROJECT NO: 15061-05                       |
| CAD DWG FILE: 15061-05_CM501.DWG               |
| DESIGNED BY: JMW                               |
| DRAWN BY: CMT                                  |
| CHECKED BY: APP                                |
| APPROVED BY: CHK                               |
| COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015 |

SHEET TITLE  
**MARKING DETAILS 1**