

GREATER ROCKFORD AIRPORT AUTHORITY ROCKFORD, ILLINOIS

CONSTRUCTION PLANS FOR CHICAGO ROCKFORD INTERNATIONAL AIRPORT

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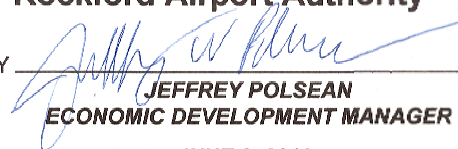
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REHABILITATE THE INTERSECTION OF TAXIWAY F AND A; REHAB. TAXIWAY F AND G

**ILLINOIS PROJECT: RFD-4541
A.I.P.PROJECT: 3-17-0088-XX**

JUNE 03, 2016



Greater Rockford Airport Authority
APPROVED BY 
JEFFREY POLSEAN
ECONOMIC DEVELOPMENT MANAGER
DATE **JUNE 3, 2016**

DESIGN INFORMATION

TOWNSHIP: 43 NORTH COUNTY: WINNEBAGO
RANGE: 1 EAST ROCKFORD TOWNSHIP


MAXIMUM EQUIPMENT HEIGHT = 25'

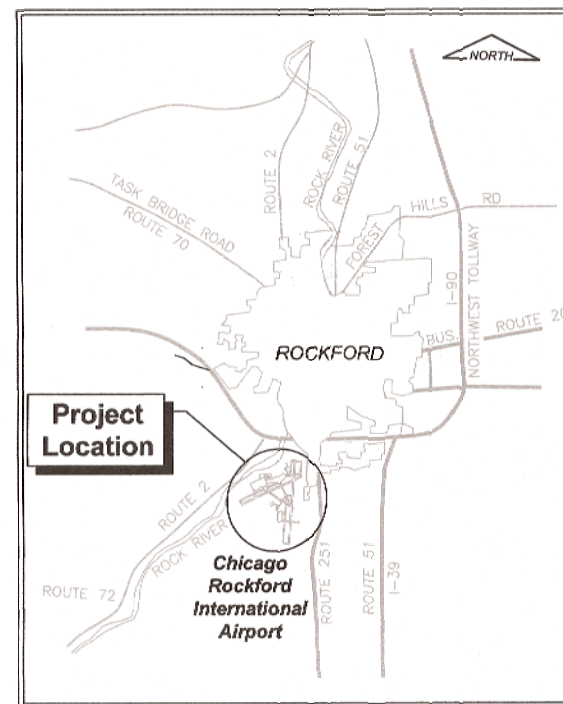


Illinois Professional Engineering Practice Act of 1989

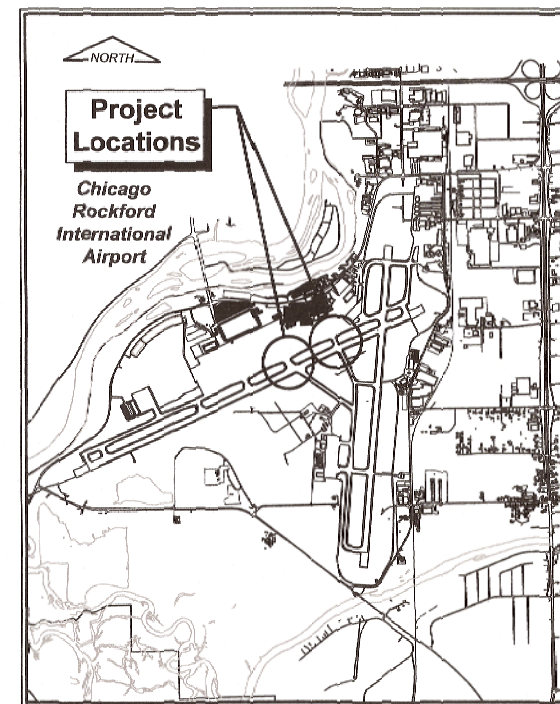
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CONSULTING ENGINEERS
License No. 184-000613



SUBMITTED BY 
Daniel L. Pape, P.E.
DATE 6/3/16
license expires 11-30-2017



LOCATION MAP



SITE PLAN



MICHAEL P. DUNN
DIRECTOR

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CHICAGO ROCKFORD
INTERNATIONAL AIRPORT
ROCKFORD, ILLINOIS

GREATER ROCKFORD
AIRPORT AUTHORITY

IL. CONTRACT: R0027
IL. LETTING ITEM: 22A
IL. PROJECT: RFD-4541
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN
BY:

REV | DATE | DESCRIPTION

PROJECT NAME:

REHABILITATE THE INTERSECTION OF
TAXIWAY F AND A;
REHAB. TAXIWAY F AND G

SHEET TITLE:

SUMMARY OF QUANTITIES

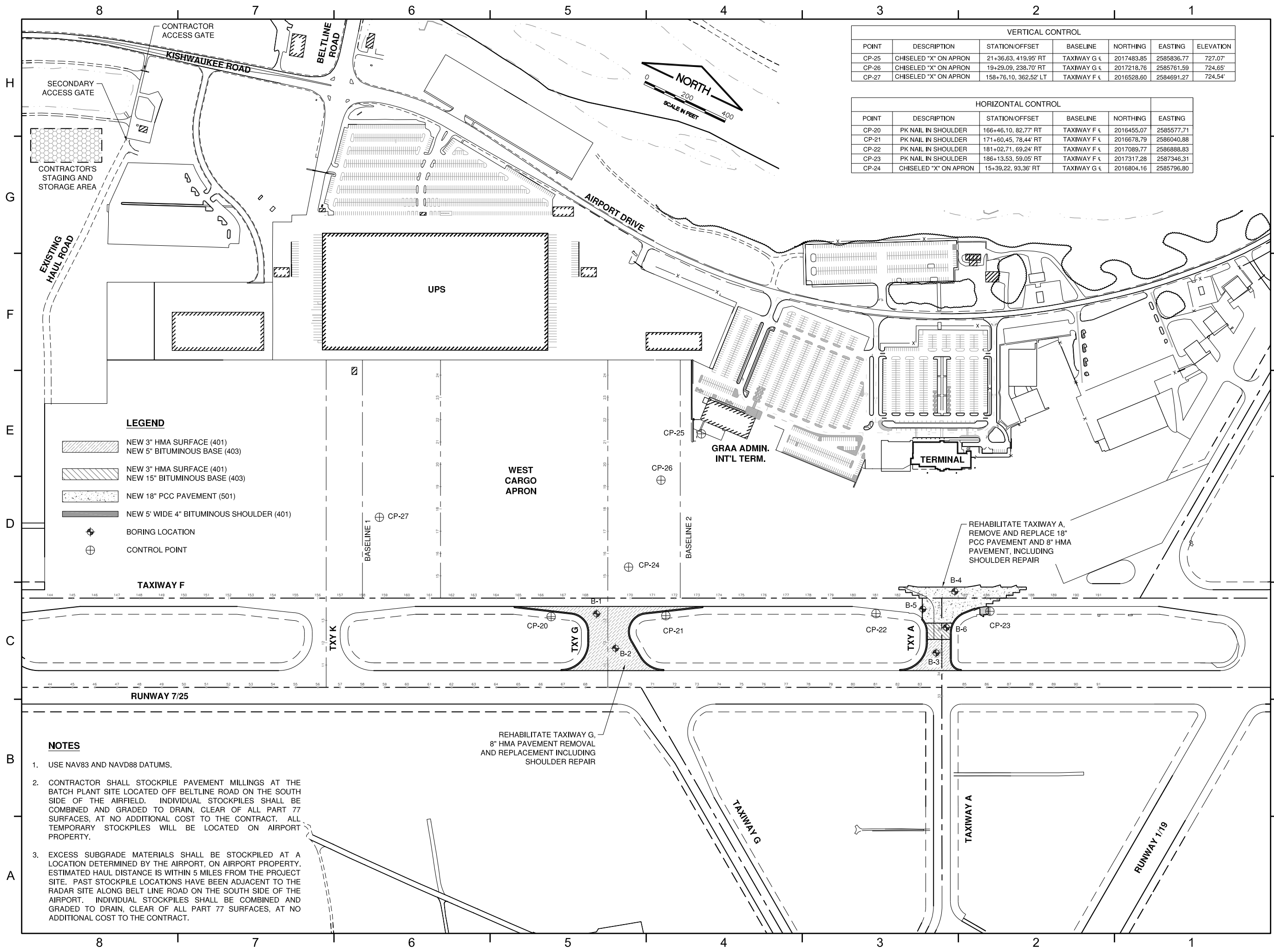
DESIGNED: **ABM** DRAWN: **JRO** CHECKED: **DLP**

PROJECT NO.: 15258-05
DATE: 06/03/2016

SHEET NO. **2 OF 26** REVISION

SUMMARY OF QUANTITIES

REHABILITATE TAXIWAY F INTERSECTIONS WITH TAXIWAYS A AND G				
ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR125110	SEMIFLUSH RETROREFLECTIVE MARKER	EACH	8	
AR150520	MOBILIZATION	LS	1	
AR152410	UNCLASSIFIED EXCAVATION	CY	50	
AR208515	POROUS GRANULAR EMBANKMENT	CY	50	
AR209650	AGGREGATE BASE PREPARATION	SY	7,619	
AR401610	BITUMINOUS SURFACE COURSE	TON	2,115	
AR403610	BITUMINOUS BASE COURSE	TON	3,925	
AR403630	BITUMINOUS BASE TEST SECTION	EACH	1	
AR501518	18" PCC PAVEMENT	SY	5,530	
AR501900	REMOVE PCC PAVEMENT	SY	6,400	
AR602510	BITUMINOUS PRIME COAT	GAL	1,050	
AR603510	BITUMINOUS TACK COAT	GAL	3,725	
AR605510	JOINT SEALING FILLER	LF	8,075	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	13,700	
AR620525	PAVEMENT MARKING - BLACK BORDER	SF	12,680	
AR620595	TEMPORARY MARKING & REMOVAL	SF	18,775	
AR620900	PAVEMENT MARKING REMOVAL	SF	3,380	
AR751920	REPLACE INLET	EACH	1	
AR800005	RUNWAY GUARD LIGHT BAR	LS	1	
AR800007	REMOVE LIGHT BAR AND CONDUIT	LS	1	
AR800116	PAVEMENT MARKING - PREFORMED THERMOPLASTIC	SF	1,180	
AR800143	4" BITUMINOUS PAVEMENT MILLING	SY	1,235	
AR800144	8" BITUMINOUS PAVEMENT MILLING	SY	10,560	



VERTICAL CONTROL						
POINT	DESCRIPTION	STATION/OFFSET	BASELINE	NORTHING	EASTING	ELEVATION
CP-25	CHISELED "X" ON APRON	21+36.63, 419.95' RT	TAXIWAY G €	2017483.85	2585836.77	727.07'
CP-26	CHISELED "X" ON APRON	19+29.09, 238.70' RT	TAXIWAY G €	2017218.76	2585761.59	724.65'
CP-27	CHISELED "X" ON APRON	158+76.10, 362.52' LT	TAXIWAY F €	2016528.60	2584691.27	724.54'

HORIZONTAL CONTROL						
POINT	DESCRIPTION	STATION/OFFSET	BASELINE	NORTHING	EASTING	
CP-20	PK NAIL IN SHOULDER	166+46.10, 82.77' RT	TAXIWAY F €	2016455.07	2585577.71	
CP-21	PK NAIL IN SHOULDER	171+60.45, 78.44' RT	TAXIWAY F €	2016678.79	2586040.88	
CP-22	PK NAIL IN SHOULDER	181+02.71, 69.24' RT	TAXIWAY F €	2017089.77	2586888.83	
CP-23	PK NAIL IN SHOULDER	186+13.53, 59.05' RT	TAXIWAY F €	2017317.28	2587346.31	
CP-24	CHISELED "X" ON APRON	15+39.22, 93.36' RT	TAXIWAY G €	2016804.16	2585796.80	



MICHAEL P. DUNN
DIRECTOR

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INTERNATIONAL AIRPORT
ROCKFORD, ILLINOIS

GREATER ROCKFORD
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APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE THE INTERSECTION OF
TAXIWAY F AND A;
REHAB. TAXIWAY F AND G

SHEET TITLE:
**SITE PLAN
AND PROJECT CONTROL**

DESIGNED:	DRAWN:	CHECKED:
ABM	JRO	DLP
PROJECT NO.:	15258-05	
DATE:	06/03/2016	
SHEET NO.	3 OF 26	REVISION



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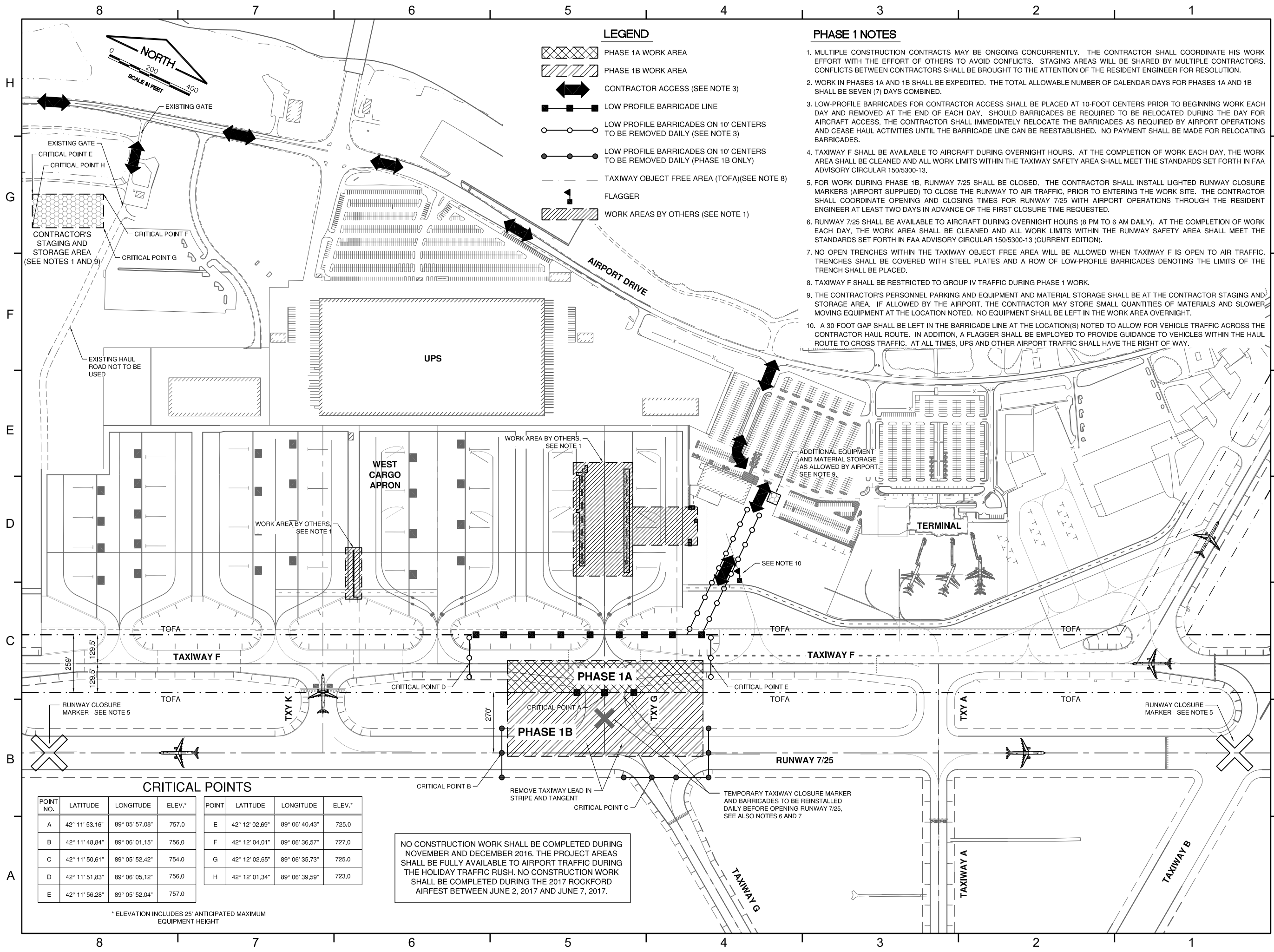
SHEET TITLE:

**SEQUENCE OF CONSTRUCTION
PHASE 1**

DESIGNED: **SMS** DRAWN: **JRO** CHECKED: **DLP**

PROJECT NO.: 15258-05
DATE: 06/03/2016

SHEET NO. **4** OF 26 REVISION



LEGEND

- PHASE 1A WORK AREA
- PHASE 1B WORK AREA
- CONTRACTOR ACCESS (SEE NOTE 3)
- LOW PROFILE BARRICADE LINE
- LOW PROFILE BARRICADES ON 10' CENTERS TO BE REMOVED DAILY (SEE NOTE 3)
- LOW PROFILE BARRICADES ON 10' CENTERS TO BE REMOVED DAILY (PHASE 1B ONLY)
- TAXIWAY OBJECT FREE AREA (TOFA)(SEE NOTE 8)
- FLAGGER
- WORK AREAS BY OTHERS (SEE NOTE 1)

PHASE 1 NOTES

1. MULTIPLE CONSTRUCTION CONTRACTS MAY BE ONGOING CONCURRENTLY. THE CONTRACTOR SHALL COORDINATE HIS WORK EFFORT WITH THE EFFORT OF OTHERS TO AVOID CONFLICTS. STAGING AREAS WILL BE SHARED BY MULTIPLE CONTRACTORS. CONFLICTS BETWEEN CONTRACTORS SHALL BE BROUGHT TO THE ATTENTION OF THE RESIDENT ENGINEER FOR RESOLUTION.
2. WORK IN PHASES 1A AND 1B SHALL BE EXPEDITED. THE TOTAL ALLOWABLE NUMBER OF CALENDAR DAYS FOR PHASES 1A AND 1B SHALL BE SEVEN (7) DAYS COMBINED.
3. LOW-PROFILE BARRICADES FOR CONTRACTOR ACCESS SHALL BE PLACED AT 10-FOOT CENTERS PRIOR TO BEGINNING WORK EACH DAY AND REMOVED AT THE END OF EACH DAY. SHOULD BARRICADES BE REQUIRED TO BE RELOCATED DURING THE DAY FOR AIRCRAFT ACCESS, THE CONTRACTOR SHALL IMMEDIATELY RELOCATE THE BARRICADES AS REQUIRED BY AIRPORT OPERATIONS AND CEASE HAUL ACTIVITIES UNTIL THE BARRICADE LINE CAN BE REESTABLISHED. NO PAYMENT SHALL BE MADE FOR RELOCATING BARRICADES.
4. TAXIWAY F SHALL BE AVAILABLE TO AIRCRAFT DURING OVERNIGHT HOURS. AT THE COMPLETION OF WORK EACH DAY, THE WORK AREA SHALL BE CLEANED AND ALL WORK LIMITS WITHIN THE TAXIWAY SAFETY AREA SHALL MEET THE STANDARDS SET FORTH IN FAA ADVISORY CIRCULAR 150/5300-13.
5. FOR WORK DURING PHASE 1B, RUNWAY 7/25 SHALL BE CLOSED. THE CONTRACTOR SHALL INSTALL LIGHTED RUNWAY CLOSURE MARKERS (AIRPORT SUPPLIED) TO CLOSE THE RUNWAY TO AIR TRAFFIC, PRIOR TO ENTERING THE WORK SITE. THE CONTRACTOR SHALL COORDINATE OPENING AND CLOSING TIMES FOR RUNWAY 7/25 WITH AIRPORT OPERATIONS THROUGH THE RESIDENT ENGINEER AT LEAST TWO DAYS IN ADVANCE OF THE FIRST CLOSURE TIME REQUESTED.
6. RUNWAY 7/25 SHALL BE AVAILABLE TO AIRCRAFT DURING OVERNIGHT HOURS (8 PM TO 6 AM DAILY). AT THE COMPLETION OF WORK EACH DAY, THE WORK AREA SHALL BE CLEANED AND ALL WORK LIMITS WITHIN THE RUNWAY SAFETY AREA SHALL MEET THE STANDARDS SET FORTH IN FAA ADVISORY CIRCULAR 150/5300-13 (CURRENT EDITION).
7. NO OPEN TRENCHES WITHIN THE TAXIWAY OBJECT FREE AREA WILL BE ALLOWED WHEN TAXIWAY F IS OPEN TO AIR TRAFFIC. TRENCHES SHALL BE COVERED WITH STEEL PLATES AND A ROW OF LOW-PROFILE BARRICADES DENOTING THE LIMITS OF THE TRENCH SHALL BE PLACED.
8. TAXIWAY F SHALL BE RESTRICTED TO GROUP IV TRAFFIC DURING PHASE 1 WORK.
9. THE CONTRACTOR'S PERSONNEL PARKING AND EQUIPMENT AND MATERIAL STORAGE SHALL BE AT THE CONTRACTOR STAGING AND STORAGE AREA. IF ALLOWED BY THE AIRPORT, THE CONTRACTOR MAY STORE SMALL QUANTITIES OF MATERIALS AND SLOWER MOVING EQUIPMENT AT THE LOCATION NOTED. NO EQUIPMENT SHALL BE LEFT IN THE WORK AREA OVERNIGHT.
10. A 30-FOOT GAP SHALL BE LEFT IN THE BARRICADE LINE AT THE LOCATION(S) NOTED TO ALLOW FOR VEHICLE TRAFFIC ACROSS THE CONTRACTOR HAUL ROUTE. IN ADDITION, A FLAGGER SHALL BE EMPLOYED TO PROVIDE GUIDANCE TO VEHICLES WITHIN THE HAUL ROUTE TO CROSS TRAFFIC. AT ALL TIMES, UPS AND OTHER AIRPORT TRAFFIC SHALL HAVE THE RIGHT-OF-WAY.

CRITICAL POINTS

POINT NO.	LATITUDE	LONGITUDE	ELEV.*	POINT	LATITUDE	LONGITUDE	ELEV.*
A	42° 11' 53.16"	89° 05' 57.08"	757.0	E	42° 12' 02.69"	89° 06' 40.43"	725.0
B	42° 11' 48.84"	89° 06' 01.15"	756.0	F	42° 12' 04.01"	89° 06' 36.57"	727.0
C	42° 11' 50.61"	89° 05' 52.42"	754.0	G	42° 12' 02.65"	89° 06' 35.73"	725.0
D	42° 11' 51.83"	89° 06' 05.12"	756.0	H	42° 12' 01.34"	89° 06' 39.59"	723.0
E	42° 11' 56.28"	89° 05' 52.04"	757.0				

* ELEVATION INCLUDES 25' ANTICIPATED MAXIMUM EQUIPMENT HEIGHT

NO CONSTRUCTION WORK SHALL BE COMPLETED DURING NOVEMBER AND DECEMBER 2016. THE PROJECT AREAS SHALL BE FULLY AVAILABLE TO AIRPORT TRAFFIC DURING THE HOLIDAY TRAFFIC RUSH. NO CONSTRUCTION WORK SHALL BE COMPLETED DURING THE 2017 ROCKFORD AIRFEST BETWEEN JUNE 2, 2017 AND JUNE 7, 2017.



MICHAEL P. DUNN
DIRECTOR



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INTERNATIONAL AIRPORT
ROCKFORD, ILLINOIS

GREATER ROCKFORD
AIRPORT AUTHORITY

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IL. LETTING ITEM: 22A
IL. PROJECT: RFD-4541
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE THE INTERSECTION OF
TAXIWAY F AND A;
REHAB. TAXIWAY F AND G

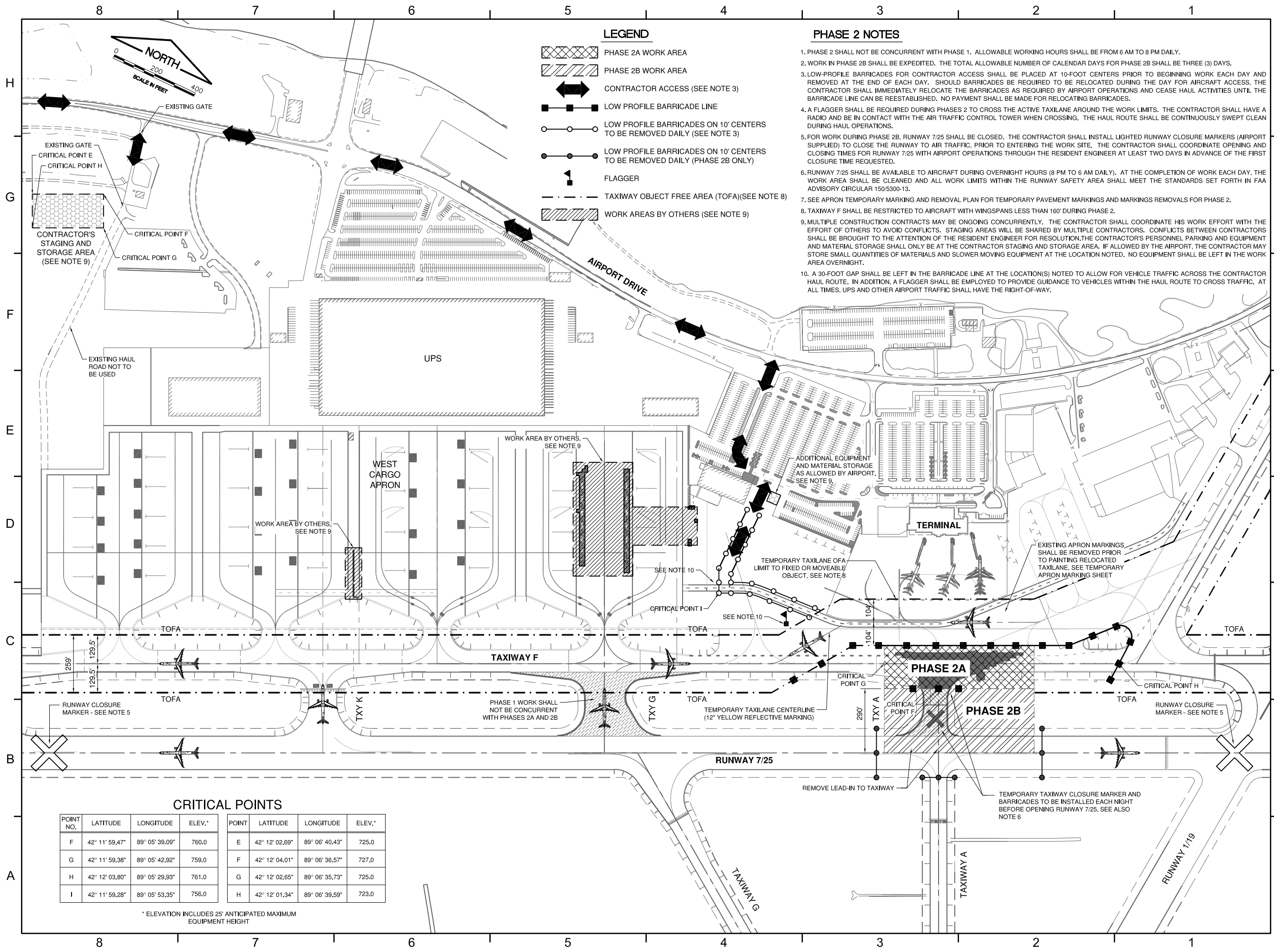
SHEET TITLE:

**SEQUENCE OF CONSTRUCTION
PHASE 2**

DESIGNED: **SMS** DRAWN: **JRO** CHECKED: **DLP**

PROJECT NO.: 15258-05
DATE: 06/03/2016

SHEET NO. **5** OF 26 REVISION



LEGEND

- PHASE 2A WORK AREA
- PHASE 2B WORK AREA
- CONTRACTOR ACCESS (SEE NOTE 3)
- LOW PROFILE BARRICADE LINE
- LOW PROFILE BARRICADES ON 10' CENTERS TO BE REMOVED DAILY (SEE NOTE 3)
- LOW PROFILE BARRICADES ON 10' CENTERS TO BE REMOVED DAILY (PHASE 2B ONLY)
- FLAGGER
- TAXIWAY OBJECT FREE AREA (TOFA)(SEE NOTE 8)
- WORK AREAS BY OTHERS (SEE NOTE 9)

PHASE 2 NOTES

1. PHASE 2 SHALL NOT BE CONCURRENT WITH PHASE 1. ALLOWABLE WORKING HOURS SHALL BE FROM 6 AM TO 8 PM DAILY.
2. WORK IN PHASE 2B SHALL BE EXPEDITED. THE TOTAL ALLOWABLE NUMBER OF CALENDAR DAYS FOR PHASE 2B SHALL BE THREE (3) DAYS.
3. LOW-PROFILE BARRICADES FOR CONTRACTOR ACCESS SHALL BE PLACED AT 10-FOOT CENTERS PRIOR TO BEGINNING WORK EACH DAY AND REMOVED AT THE END OF EACH DAY. SHOULD BARRICADES BE REQUIRED TO BE RELOCATED DURING THE DAY FOR AIRCRAFT ACCESS, THE CONTRACTOR SHALL IMMEDIATELY RELOCATE THE BARRICADES AS REQUIRED BY AIRPORT OPERATIONS AND CEASE HAUL ACTIVITIES UNTIL THE BARRICADE LINE CAN BE REESTABLISHED. NO PAYMENT SHALL BE MADE FOR RELOCATING BARRICADES.
4. A FLAGGER SHALL BE REQUIRED DURING PHASES 2 TO CROSS THE ACTIVE TAXILANE AROUND THE WORK LIMITS. THE CONTRACTOR SHALL HAVE A RADIO AND BE IN CONTACT WITH THE AIR TRAFFIC CONTROL TOWER WHEN CROSSING. THE HAUL ROUTE SHALL BE CONTINUOUSLY SWEEP CLEAN DURING HAUL OPERATIONS.
5. FOR WORK DURING PHASE 2B, RUNWAY 7/25 SHALL BE CLOSED. THE CONTRACTOR SHALL INSTALL LIGHTED RUNWAY CLOSURE MARKERS (AIRPORT SUPPLIED) TO CLOSE THE RUNWAY TO AIR TRAFFIC, PRIOR TO ENTERING THE WORK SITE. THE CONTRACTOR SHALL COORDINATE OPENING AND CLOSING TIMES FOR RUNWAY 7/25 WITH AIRPORT OPERATIONS THROUGH THE RESIDENT ENGINEER AT LEAST TWO DAYS IN ADVANCE OF THE FIRST CLOSURE TIME REQUESTED.
6. RUNWAY 7/25 SHALL BE AVAILABLE TO AIRCRAFT DURING OVERNIGHT HOURS (8 PM TO 6 AM DAILY). AT THE COMPLETION OF WORK EACH DAY, THE WORK AREA SHALL BE CLEANED AND ALL WORK LIMITS WITHIN THE RUNWAY SAFETY AREA SHALL MEET THE STANDARDS SET FORTH IN FAA ADVISORY CIRCULAR 150/5300-13.
7. SEE APRON TEMPORARY MARKING AND REMOVAL PLAN FOR TEMPORARY PAVEMENT MARKINGS AND MARKINGS REMOVALS FOR PHASE 2.
8. TAXIWAY F SHALL BE RESTRICTED TO AIRCRAFT WITH WINGSPANS LESS THAN 160' DURING PHASE 2.
9. MULTIPLE CONSTRUCTION CONTRACTS MAY BE ONGOING CONCURRENTLY. THE CONTRACTOR SHALL COORDINATE HIS WORK EFFORT WITH THE EFFORT OF OTHERS TO AVOID CONFLICTS. STAGING AREAS WILL BE SHARED BY MULTIPLE CONTRACTORS. CONFLICTS BETWEEN CONTRACTORS SHALL BE BROUGHT TO THE ATTENTION OF THE RESIDENT ENGINEER FOR RESOLUTION. THE CONTRACTOR'S PERSONNEL PARKING AND EQUIPMENT AND MATERIAL STORAGE SHALL ONLY BE AT THE CONTRACTOR STAGING AND STORAGE AREA. IF ALLOWED BY THE AIRPORT, THE CONTRACTOR MAY STORE SMALL QUANTITIES OF MATERIALS AND SLOWER MOVING EQUIPMENT AT THE LOCATION NOTED. NO EQUIPMENT SHALL BE LEFT IN THE WORK AREA OVERNIGHT.
10. A 30-FOOT GAP SHALL BE LEFT IN THE BARRICADE LINE AT THE LOCATION(S) NOTED TO ALLOW FOR VEHICLE TRAFFIC ACROSS THE CONTRACTOR HAUL ROUTE. IN ADDITION, A FLAGGER SHALL BE EMPLOYED TO PROVIDE GUIDANCE TO VEHICLES WITHIN THE HAUL ROUTE TO CROSS TRAFFIC. AT ALL TIMES, UPS AND OTHER AIRPORT TRAFFIC SHALL HAVE THE RIGHT-OF-WAY.

CRITICAL POINTS

POINT NO.	LATITUDE	LONGITUDE	ELEV.*	POINT	LATITUDE	LONGITUDE	ELEV.*
F	42° 11' 59.47"	89° 05' 39.09"	760.0	E	42° 12' 02.69"	89° 06' 40.43"	725.0
G	42° 11' 59.38"	89° 05' 42.92"	759.0	F	42° 12' 04.01"	89° 06' 36.57"	727.0
H	42° 12' 03.80"	89° 05' 29.93"	761.0	G	42° 12' 02.65"	89° 06' 35.73"	725.0
I	42° 11' 59.28"	89° 05' 53.35"	756.0	H	42° 12' 01.34"	89° 06' 39.59"	723.0

* ELEVATION INCLUDES 25' ANTICIPATED MAXIMUM EQUIPMENT HEIGHT



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REV	DATE	DESCRIPTION

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REHABILITATE THE INTERSECTION OF
TAXIWAY F AND A;
REHAB. TAXIWAY F AND G

SHEET TITLE:
**SEQUENCE OF CONSTRUCTION
TEMPORARY APRON
MARKING PLAN**

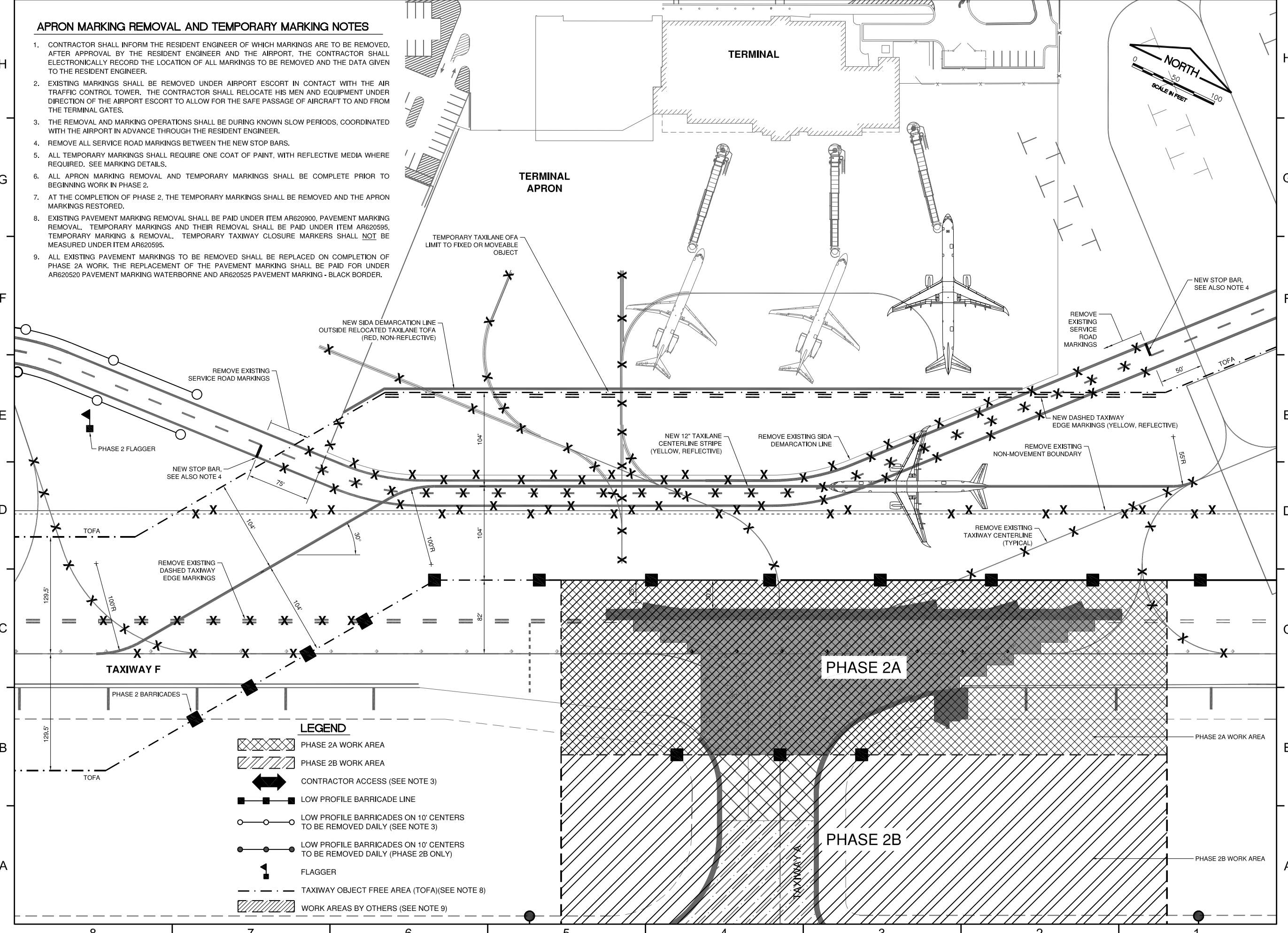
DESIGNED: **SMS** DRAWN: **JRO** CHECKED: **DLP**

PROJECT NO.: 15258-05
DATE: 06/03/2016

SHEET NO. 6 OF 26 REVISION

APRON MARKING REMOVAL AND TEMPORARY MARKING NOTES

1. CONTRACTOR SHALL INFORM THE RESIDENT ENGINEER OF WHICH MARKINGS ARE TO BE REMOVED. AFTER APPROVAL BY THE RESIDENT ENGINEER AND THE AIRPORT, THE CONTRACTOR SHALL ELECTRONICALLY RECORD THE LOCATION OF ALL MARKINGS TO BE REMOVED AND THE DATA GIVEN TO THE RESIDENT ENGINEER.
2. EXISTING MARKINGS SHALL BE REMOVED UNDER AIRPORT ESCORT IN CONTACT WITH THE AIR TRAFFIC CONTROL TOWER. THE CONTRACTOR SHALL RELOCATE HIS MEN AND EQUIPMENT UNDER DIRECTION OF THE AIRPORT ESCORT TO ALLOW FOR THE SAFE PASSAGE OF AIRCRAFT TO AND FROM THE TERMINAL GATES.
3. THE REMOVAL AND MARKING OPERATIONS SHALL BE DURING KNOWN SLOW PERIODS, COORDINATED WITH THE AIRPORT IN ADVANCE THROUGH THE RESIDENT ENGINEER.
4. REMOVE ALL SERVICE ROAD MARKINGS BETWEEN THE NEW STOP BARS.
5. ALL TEMPORARY MARKINGS SHALL REQUIRE ONE COAT OF PAINT, WITH REFLECTIVE MEDIA WHERE REQUIRED. SEE MARKING DETAILS.
6. ALL APRON MARKING REMOVAL AND TEMPORARY MARKINGS SHALL BE COMPLETE PRIOR TO BEGINNING WORK IN PHASE 2.
7. AT THE COMPLETION OF PHASE 2, THE TEMPORARY MARKINGS SHALL BE REMOVED AND THE APRON MARKINGS RESTORED.
8. EXISTING PAVEMENT MARKING REMOVAL SHALL BE PAID UNDER ITEM AR620900, PAVEMENT MARKING REMOVAL. TEMPORARY MARKINGS AND THEIR REMOVAL SHALL BE PAID UNDER ITEM AR620595, TEMPORARY MARKING & REMOVAL. TEMPORARY TAXIWAY CLOSURE MARKERS SHALL NOT BE MEASURED UNDER ITEM AR620595.
9. ALL EXISTING PAVEMENT MARKINGS TO BE REMOVED SHALL BE REPLACED ON COMPLETION OF PHASE 2A WORK. THE REPLACEMENT OF THE PAVEMENT MARKING SHALL BE PAID FOR UNDER AR620520 PAVEMENT MARKING WATERBORNE AND AR620525 PAVEMENT MARKING - BLACK BORDER.



LEGEND

- PHASE 2A WORK AREA
- PHASE 2B WORK AREA
- CONTRACTOR ACCESS (SEE NOTE 3)
- LOW PROFILE BARRICADE LINE
- LOW PROFILE BARRICADES ON 10' CENTERS TO BE REMOVED DAILY (SEE NOTE 3)
- LOW PROFILE BARRICADES ON 10' CENTERS TO BE REMOVED DAILY (PHASE 2B ONLY)
- FLAGGER
- TAXIWAY OBJECT FREE AREA (TOFA)(SEE NOTE 8)
- WORK AREAS BY OTHERS (SEE NOTE 9)

PHASING PLAN PROJECT NOTES:

1. APPROVED PROGRESS SCHEDULE:

PRIOR TO THE START OF CONSTRUCTION, AN APPROVED PROGRESS SCHEDULE SHALL BE EXECUTED BY THE CONTRACTOR AND MONITORED BY THE RESIDENT ENGINEER. THIS SCHEDULE SHALL SHOW START/ STOP DATES OF ALL PHASES, INCLUDING ALL AOA WORK REQUIRING DAYTIME ONLY CLOSURES OF TAXIWAYS AND RUNWAYS. THE APPROVED PROGRESS SCHEDULE SHALL BE DISTRIBUTED TO ALL PARTIES AT LEAST 3 WORKING DAYS PRIOR TO START OF CONSTRUCTION. THE CONTRACTOR MAY BE REQUIRED TO SUBMIT A REVISED PROGRESS SCHEDULE TO ACCOMODATE AIRPORT EVENTS (I.E. AIRSHOW). SHOULD A REVISED SCHEDULE BE REQUIRED, THE REVISION SHALL BE COMPLETED AT NO ADDITIONAL COST TO THE CONTRACT.

2. NOTAM (NOTICE TO AIRMEN) COORDINATION:

THE RESIDENT ENGINEER SHALL COORDINATE NOTAM AND FAA FACILITY COORDINATION WITH AIRPORT / FAA PERSONNEL.

3. CONSTRUCTION SITE ACCESS AND STAGING AREA:

THE CONTRACTOR ACCESS ROAD AND STAGING AREAS SHALL BE AS SHOWN ON THE REFERENCED PLAN. THE CONTRACTOR SHALL MAINTAIN AND REPAIR THE CONSTRUCTION ACCESS ROAD AND STAGING AREA TO ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT. ALTERNATE STAGING AREAS AND ACCESS FOR THIS AREA WILL NOT BE ALLOWED. THIS WORK AREA SHALL BE RESTRICTED FROM ACTIVE AOA AREAS WITH THE BARRICADE/FENCING PERIMETERS SHOWN.

THE ENTRANCES SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLE ACCESS AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO THE CITY OF ROCKFORD AND IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.

4. CONSTRUCTION EQUIPMENT FLAGGING / BEACON REQUIREMENTS:

THE CONTRACTOR WILL BE REQUIRED TO MOUNT AIRPORT FLAGS AND HAVE BEACON (FLASHING YELLOW) LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION, EXCEPT HAUL VEHICLES.

5. GATE SECURITY:

THE GATE SHALL BE MAINTAINED, CLOSED AND LOCKED AS DIRECTED BY THE DEPUTY DIRECTOR OF OPERATIONS. SHOULD THE CONTRACTOR'S OPERATIONS REQUIRE THE GATE TO REMAIN OPEN TO PROVIDE ACCESS TO HAULING OPERATIONS, A COMPETENT GATE GUARD EMPLOYED BY THE CONTRACTOR SHALL BE REQUIRED TO CONTROL ACCESS TO THE AIRFIELD. A \$1,000 FINE SHALL BE ASSESSED FOR ANY OCCURRENCE OF AN UNSECURE GATE THAT IS THE CONTRACTOR'S RESPONSIBILITY. AN UNSECURED GATE SHALL BE DEFINED AS ANY GATE THAT IS NOT WITHIN THE SIGHT AND PHYSICAL CONTROL OF THE CONTRACTOR'S GUARD. IN THE EVENT THAT THE GATE MAY NOT BE SECURED, THE CONTRACTOR WILL BE CHARGED FOR AIRPORT PERSONNEL TO REMAIN AT THE GATE UNTIL SECURED.

6. UNAUTHORIZED ACCESS TO AIRFIELD:

THE CONTRACTOR SHALL RESTRICT ALL CONSTRUCTION ACTIVITIES TO THE CONSTRUCTION AREA DETAILED IN THE PHASING PLAN. ANY UNAUTHORIZED MOVEMENTS, PEDESTRIAN OR VEHICULAR, BEYOND THE CONSTRUCTION LIMITS SHOWN SHALL BE CONSIDERED AN AIRFIELD INCURSION. AIRFIELD INCURSIONS, AT THE DISCRETION OF THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS, MAY BE FINED \$10,000.00 PER INCIDENT. INCURSION FINES WILL BE ASSESSED IMMEDIATELY AND DEDUCTED FROM ANY MONIES DUE OR THAT MAY BECOME DUE TO THE CONTRACTOR ON THE NEXT CONSTRUCTION PAYMENT.

7. VEHICLE SIGNAGE

THE CONTRACTOR'S VEHICLES SHALL HAVE MAGNETIC SIGNS INDICATING THE COMPANY NAME AND VEHICLE NUMBER FOR LONG DISTANCE IDENTIFICATION BY ATCT PERSONNEL. SIGNAGE AND NUMBERING SHALL BE COORDINATED PRIOR TO CONSTRUCTION WITH AIRPORT PERSONNEL. ALL COSTS ASSOCIATED WITH SIGNING OF VEHICLES SHALL BE INCIDENTAL TO THE CONTRACT.

PHASING PLAN GENERAL NOTES:

1. SUGGESTED SEQUENCE OF CONSTRUCTION:

THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE RESIDENT ENGINEER. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS.

2. STAGING AREA RESTORATION:

ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A STAGING AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE PAVEMENTS / AREAS SHALL BE INCIDENTAL TO THE CONTRACT.

3. HAUL ROUTE / HAUL ROUTE RESTORATION:

THE CONTRACTOR SHALL USE THE HAUL ROUTE(S) AS SHOWN IN THESE PLANS. THE COST OF MAINTAINING THE HAUL ROUTE(S) INCLUDING EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE HAUL ROUTE(S) SHALL BE INCIDENTAL TO CONTRACT.

4. AIRPORT APPROVAL OF PHASING:

THE RESIDENT ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT OPERATIONS. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. VEHICULAR TRAFFIC AND CONTRACTOR ACTIVITIES SHALL YIELD TO AIRCRAFT OPERATIONS. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT OR BARRICADES AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AND AT NO EXTRA COST TO THE OWNER.

5. AIRFIELD PAVEMENT / SITE DEBRIS REMOVAL:

THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.

6. PROJECT LIGHTING OUTSIDE OF DAYLIGHT HOURS:

WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.

7. EXISTING UTILITY COORDINATION:

COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATIONS.

8. TRAFFIC CONTROL PAYMENT:

PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, AIR OPERATIONS AREA (A.O.A.) LATH AND RIBBON, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES USED ON THE AIRFIELD MUST BE INTERLOCKING LOW PROFILE BARRICADES. INTERLOCKING LOW PROFILE BARRICADES WITH STEADY BURN RED LIGHTS SHALL BE INTERLOCKED WITH NO GAPS BETWEEN BARRICADES. BARRICADES SHALL BE PLACED AS SHOWN ON THIS PLAN AND AS DIRECTED BY THE RESIDENT ENGINEER. WHEN NOT IN USE, THESE BARRICADES SHALL BE STORED AT THE CONTRACTOR'S STAGING AREA OR OFF SITE. ACCESS TO THE ACTIVE RUNWAY AND TAXIWAY PAVEMENTS (TOWER CONTROLLED AREAS) SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON THE CLOSEST BARRICADES (2 EACH, RIGHT AND LEFT) AT THE ENTRANCE. IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "TOWER CONTROL AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE." ALL NON-AIRFIELD LOCATIONS REQUIRING BARRICADES SHALL BE TYPE II BARRICADES AND SHALL CONFORM TO IDOT STANDARD 702001. FOR AIRSIDE BARRICADE PLACEMENT, SEE PHASING PLAN SUMMARY SHEETS.

(NOTES CONTINUED ON PHASING PLAN - NOTES AND DETAILS - 2)



MICHAEL P. DUNN
DIRECTOR



CHICAGO ROCKFORD
INTERNATIONAL AIRPORT
ROCKFORD, ILLINOIS

GREATER ROCKFORD
AIRPORT AUTHORITY

IL. CONTRACT: R0027
IL. LETTING ITEM: 22A
IL. PROJECT: RFD-4541
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE THE INTERSECTION OF
TAXIWAY F AND A;
REHAB. TAXIWAY F AND G

SHEET TITLE:

**PHASING PLAN -
NOTES AND DETAILS - 1**

DESIGNED:	DRAWN:	CHECKED:
SMS	JRO	DLP

PROJECT NO.:	15258-05
DATE:	06/03/2016

SHEET NO.	7 OF 26	REVISION
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PHASING PLAN GENERAL NOTES (CONT'D):

(NOTES CONTINUED FROM PHASING PLAN - NOTES AND DETAILS - 1)

9. DRIVERS TRAINING AND BADGING:

DRIVER'S TRAINING AND BADGING SHALL BE REQUIRED FOR THE CONTRACTOR'S SUPERVISORY PERSONNEL, OTHER CONSTRUCTION PERSONNEL CAN BE WITHIN THE AIRFIELD LIMITS PROVIDED THAT THEY ARE UNDER ESCORT AND IN THE PRESENCE OF AN AUTHORIZED SUPERVISOR. THE DRIVER'S TRAINING AND BADGING OF THE INITIAL SUPERVISORY PERSONNEL MUST BE COMPLETED PRIOR TO THE START OF CONSTRUCTION.

10. DUST CONTROL REQUIREMENTS:

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ON SITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE DEPUTY DIRECTOR OF OPERATIONS. PAYMENT FOR DUST CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.

11. OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION (AC 150/5370-2F):

ALL WORK SHALL CONFORM TO AC 150/5370-2F OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION. THIS AC IS AVAILABLE AT www.faa.gov/arp/pdf/5370-2f.pdf.

12. STAGING AREA:

THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT REFUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFERS SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.

13. AIRFIELD LIGHTING COORDINATION:

THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD LIGHTING CIRCUITS AT THE START AND END OF EACH CONSTRUCTION DAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE DONE SO AT NO EXTRA COST TO THE CONTRACT.

14. WEEKLY COORDINATION MEETINGS:

WEEKLY COORDINATION MEETINGS SHALL BE REQUIRED TO DISCUSS PROJECT PROGRESS. AS A MINIMUM, PROJECT SCHEDULE AND GATE VISITOR LOGS SHALL BE DISCUSSED. REPRESENTATION BY THE PRIME CONTRACTOR IS MANDATORY.

15. TEMPORARY CABLES:

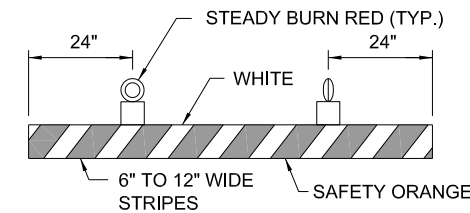
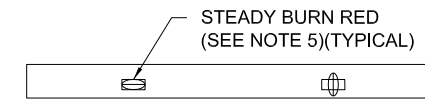
ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

16. SAFETY PLAN COMPLIANCE DOCUMENT:

THE CONTRACTOR SHALL PREPARE A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD). THE SPCD SHALL DETAIL HOW THE CONTRACTOR WILL COMPLY WITH THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). DURING THE DEVELOPMENT OF THE CSPP, IT IS NOT POSSIBLE TO DETERMINE ALL SAFETY PLAN DETAILS, SUCH AS SPECIFIC EQUIPMENT HAZARDS AND LIGHTING, CONTRACTOR'S POINTS OF CONTACT, CONSTRUCTION EQUIPMENT HEIGHT, ETC. THE SUCCESSFUL CONTRACTOR MUST DEFINE SUCH DETAILS BY PREPARING AN SPCD THAT IS REVIEWED BY THE AIRPORT OPERATOR FOR APPROVAL PRIOR TO ISSUANCE OF THE NOTICE TO PROCEED. THE SPCD IS A SUBSET OF THE CSPP AND WILL BE ADDRESSED IN THE SAME FASHION AS A SHOP DRAWING REVIEW FOR TECHNICAL COMPLIANCE WITH THE CSPP.

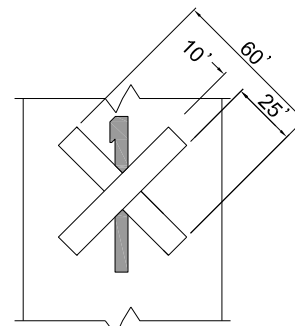
LIGHTED RUNWAY CLOSURE MARKERS

1. THE AIRPORT SHALL FURNISH TWO LIGHTED RUNWAY CLOSURE MARKERS FOR THE DURATION OF THE PROJECT. IF LIGHTED RUNWAY CLOSURE MARKERS ARE NOT AVAILABLE THE CONTRACTOR SHALL REFER TO THE "CLOSED RUNWAY MARKER DETAIL" ON THIS SHEET.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRANSPORTING, INSTALLING, MAINTAINING, REFUELING, REPOSITIONING AND REMOVING THE LIGHTED RUNWAY CLOSURE MARKERS AS SHOWN IN THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
3. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS.
4. ALL COST ASSOCIATED WITH THE LIGHTED RUNWAY CLOSURE MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
5. DURING CONSTRUCTION, IT WILL BE NECESSARY TO CLOSE RUNWAY 7/25 TO AIR TRAFFIC DURING A PORTION OF THE PROJECT. THE CONTRACTOR SHALL MARK RUNWAY 7/25 CLOSED BY PLACING LIGHTED RUNWAY CLOSURE MARKER AT THE LOCATION DETAILED ON THE PLANS OR AS DIRECTED BY THE ENGINEER AND AIRPORT. THE LIGHTED RUNWAY CLOSURE MARKERS ARE REQUIRED TO BE IN OPERATION ANYTIME THE RUNWAY IS CLOSED.



INTERLOCKING LOW PROFILE BARRICADES - ON PAVEMENT
NOT TO SCALE

1. LOW PROFILE BARRICADES SHALL BE PLACED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. THE BARRICADES SHALL BE INTERLOCKED WITH NO GAPS BETWEEN BARRICADES. BARRICADES SHALL BE WEIGHTED WITH A MINIMUM OF 6 SAND BAGS TO PREVENT THEM FROM BEING BLOWN OVER OR DISPLACED.
2. THE BARRICADE LINE SHALL EXTEND ONE BARRICADE PAST THE EDGE OF PAVEMENT INTO THE TURF.
3. LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
4. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
5. ALTERNATE LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
6. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT'S COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR AS LOW POSSIBLE, BUT NOT TO EXCEED 3 INCHES ABOVE THE GROUND.
7. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES, INCLUDING RELOCATING AS NEEDED FOR THE OCCASIONAL PASSAGE OF AIRCRAFT, SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

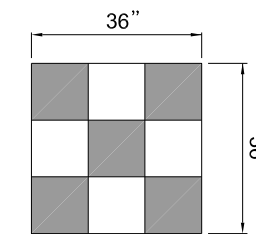


CLOSED RUNWAY MARKER DETAIL
ON PAVEMENT - NO SCALE

1. THE CONTRACTOR SHALL USE THE LIGHTED RUNWAY CLOSURE MARKERS PROVIDED BY THE AIRPORT. IF THE LIGHTED RUNWAY CLOSURE MARKERS ARE NOT AVAILABLE THE CONTRACTOR WILL BE REQUIRED TO INSTALL RUNWAY CLOSURE MARKERS AS DETAILED ABOVE.
2. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
3. MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER AND THE AIRPORT.
4. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
5. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
6. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
5. DURING CONSTRUCTION, IT WILL BE NECESSARY TO CLOSE RUNWAY 7/25 TO AIR TRAFFIC DURING A PORTION OF THE PROJECT. THE CONTRACTOR SHALL MARK RUNWAY 7/25 CLOSED BY PLACING LIGHTED RUNWAY CLOSURE MARKER AT THE LOCATION DETAILED ON THE PLANS OR AS DIRECTED BY THE ENGINEER AND AIRPORT. THE LIGHTED RUNWAY CLOSURE MARKERS ARE REQUIRED TO BE IN OPERATION ANYTIME THE RUNWAY IS CLOSED.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT:

- REHABILITATE WEST CARGO APRON
- MIDFIELD AUTO PARKING LOT AND UTILITY IMPROVEMENTS
- TERMINAL EXPANSION



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
NOT TO SCALE



MICHAEL P. DUNN
DIRECTOR



CHICAGO ROCKFORD INTERNATIONAL AIRPORT
ROCKFORD, ILLINOIS

GREATER ROCKFORD AIRPORT AUTHORITY

IL. CONTRACT: R0027
IL. LETTING ITEM: 22A
IL. PROJECT: RFD-4541
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE THE INTERSECTION OF TAXIWAY F AND A; REHAB. TAXIWAY F AND G

SHEET TITLE:

PHASING PLAN - NOTES AND DETAILS - 2

DESIGNED: SMS DRAWN: JRO CHECKED: DLP

PROJECT NO.: 15258-05
DATE: 06/03/2016

SHEET NO. 8 OF 26 REVISION



MICHAEL P. DUNN
DIRECTOR

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

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INTERNATIONAL AIRPORT
ROCKFORD, ILLINOIS

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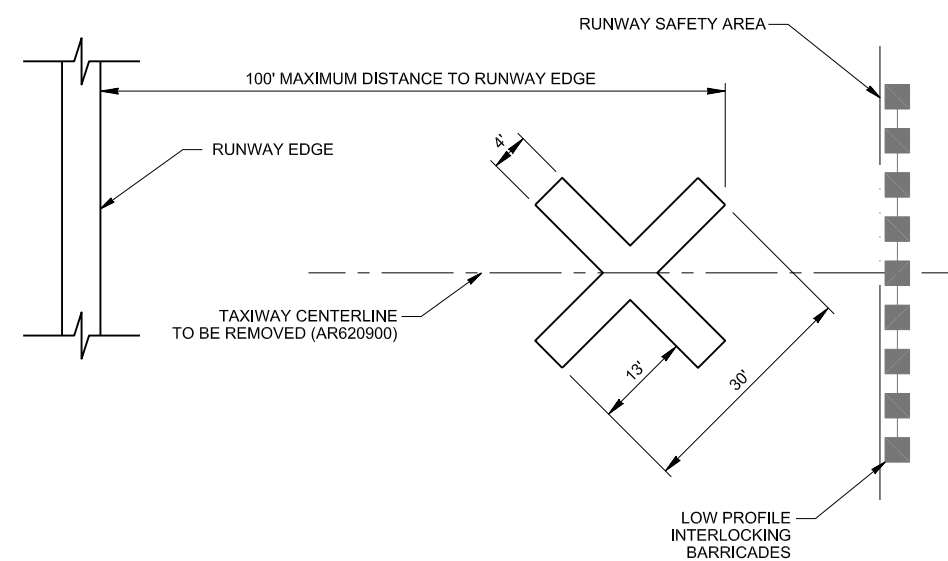
SHEET TITLE:

**PHASING PLAN -
NOTES AND DETAILS - 3**

DESIGNED: **SMS** DRAWN: **JRO** CHECKED: **DLP**

PROJECT NO.: 15258-05
DATE: 06/03/2016

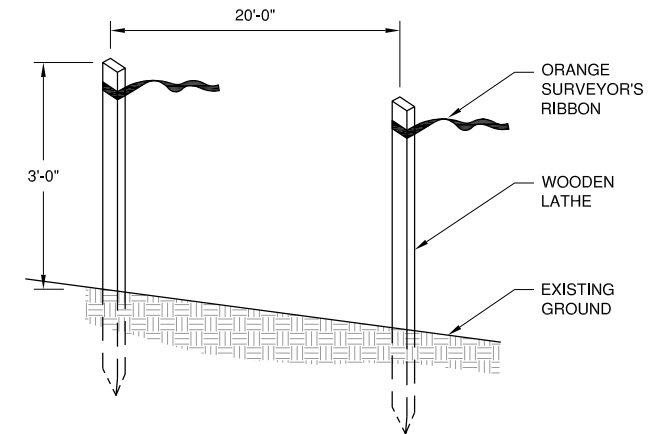
SHEET NO. **9** OF **26** REVISION



TYPICAL TAXIWAY CLOSURE MARKER DETAIL
NOT TO SCALE

TAXIWAY CLOSURE MARKER NOTES

1. THE TAXIWAY CLOSURE MARKER CAN BE PAINTED WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING, OR CAN BE ANOTHER MATERIAL THAT DOES NOT VIOLATE THE OFA CRITERIA AND IS APPROVED BY THE ENGINEER AND THE AIRPORT.
2. THE TAXIWAY CLOSURE MARKER SHALL BE YELLOW AND ADEQUATELY SECURED TO WITHSTAND JET BLAST OF 100 MPH.
3. THE MARKER SHALL BE PLACED OVER THE TAXIWAY CENTERLINE.
4. THE TAXIWAY LEAD-IN LINES AND CENTERLINE WITHIN THE RUNWAY SAFETY AREA (R.S.A.) SHALL BE REMOVED. THE REMOVAL OF THESE MARKINGS WILL BE PAID FOR UNDER PAY ITEM "AR620900 PAVEMENT MARKING REMOVAL."
5. THE INSTALLATION AND REMOVAL OF THE TAXIWAY CLOSURE MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



CONSTRUCTION SETBACK LINE DETAIL
NOT TO SCALE

CONSTRUCTION SETBACK NOTES

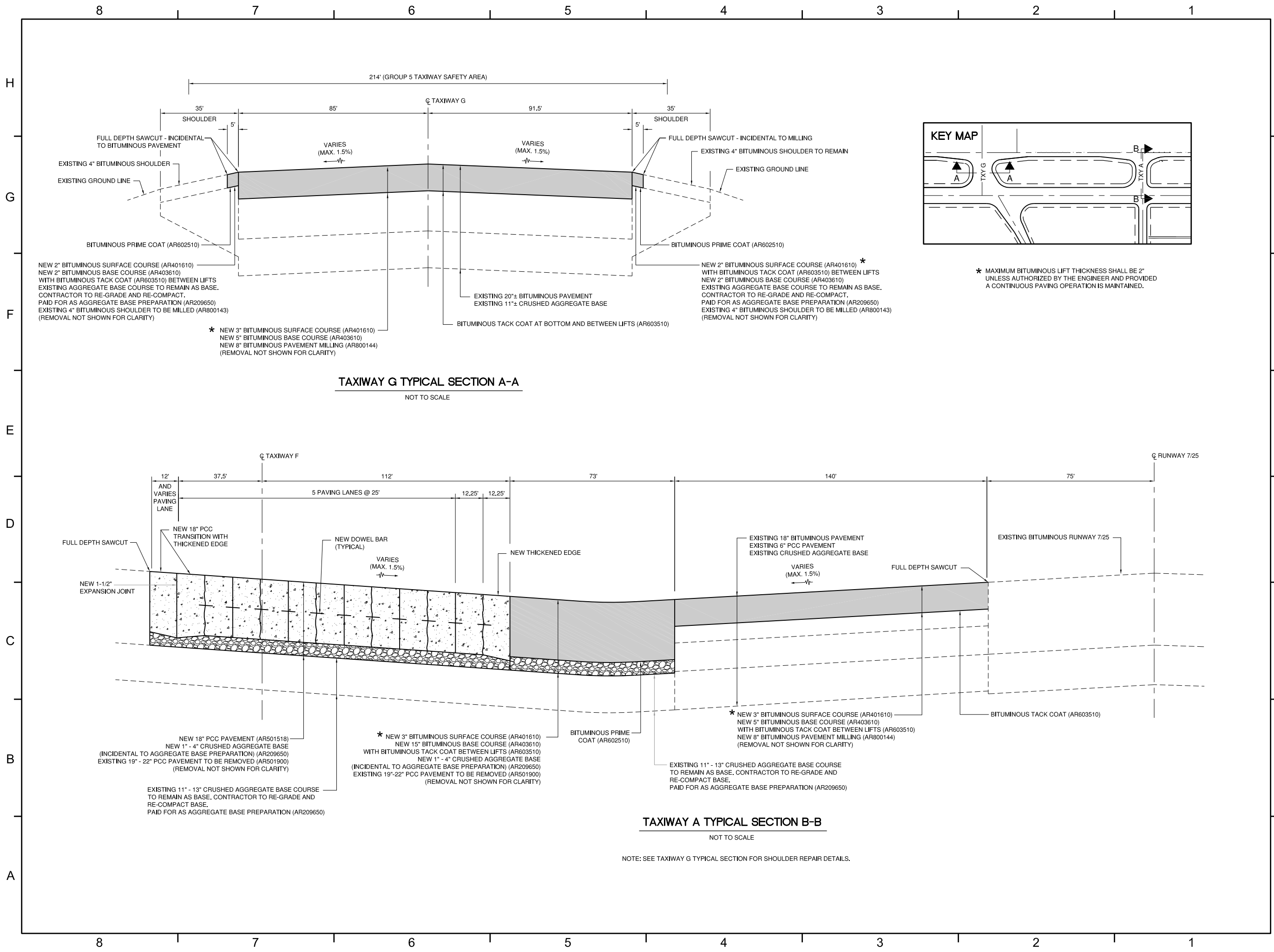
1. CONTRACTOR SHALL MARKER THE RUNWAY SAFETY AREA PER THE CONSTRUCTION SETBACK DETAIL AS DIRECTED BY THE RESIDENT ENGINEER.
2. ALL COST ASSOCIATED WITH THE CONSTRUCTION SETBACK LINE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

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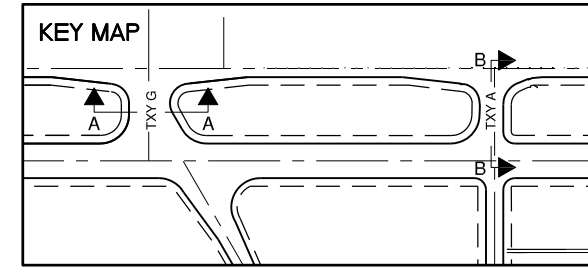
TAXIWAY G TYPICAL SECTION A-A

NOT TO SCALE

TAXIWAY A TYPICAL SECTION B-B

NOT TO SCALE

NOTE: SEE TAXIWAY G TYPICAL SECTION FOR SHOULDER REPAIR DETAILS.



* MAXIMUM BITUMINOUS LIFT THICKNESS SHALL BE 2" UNLESS AUTHORIZED BY THE ENGINEER AND PROVIDED A CONTINUOUS PAVING OPERATION IS MAINTAINED.



MICHAEL P. DUNN
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REHABILITATE THE INTERSECTION OF
TAXIWAY F AND A;
REHAB. TAXIWAY F AND G

SHEET TITLE:

TYPICAL SECTIONS

DESIGNED:	DRAWN:	CHECKED:
ABM	JRO	DLP

PROJECT NO.: 15258-05
DATE: 06/03/2016

SHEET NO.	REVISION
10 OF 26	



MICHAEL P. DUNN
DIRECTOR

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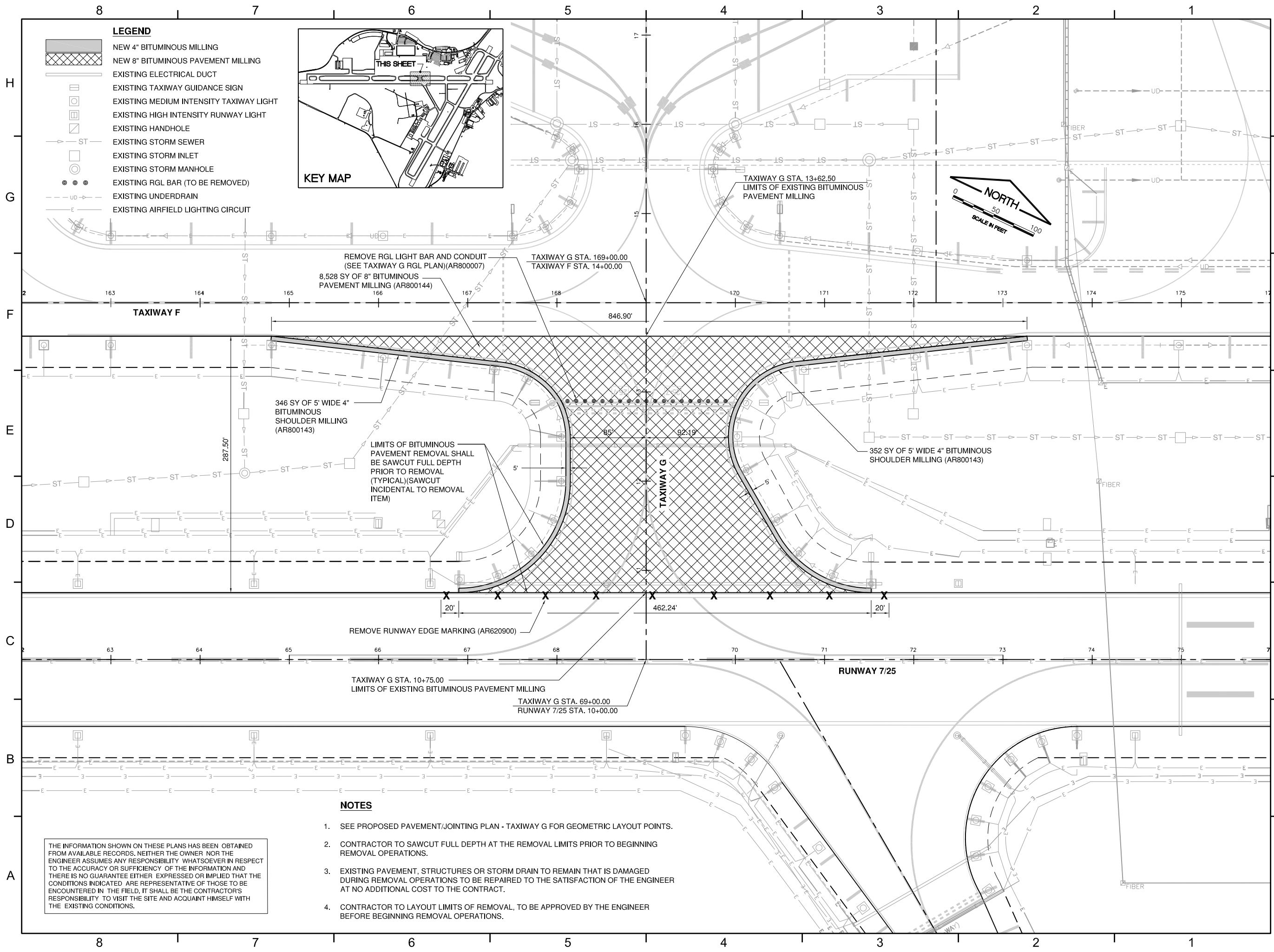
SHEET TITLE:

**EXISTING CONDITIONS/
PROPOSED REMOVALS -
TAXIWAY G**

DESIGNED: DRAWN: CHECKED:
ABM JRO DLP

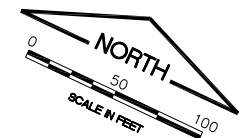
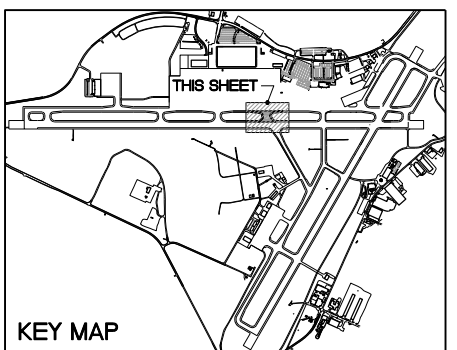
PROJECT NO.: 15258-05
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LEGEND

- NEW 4" BITUMINOUS MILLING
- NEW 8" BITUMINOUS PAVEMENT MILLING
- EXISTING ELECTRICAL DUCT
- EXISTING TAXIWAY GUIDANCE SIGN
- EXISTING MEDIUM INTENSITY TAXIWAY LIGHT
- EXISTING HIGH INTENSITY RUNWAY LIGHT
- EXISTING HANDHOLE
- EXISTING STORM SEWER
- EXISTING STORM INLET
- EXISTING STORM MANHOLE
- EXISTING RGL BAR (TO BE REMOVED)
- EXISTING UNDERDRAIN
- EXISTING AIRFIELD LIGHTING CIRCUIT



REMOVE RGL LIGHT BAR AND CONDUIT
(SEE TAXIWAY G RGL PLAN)(AR800007)
8,528 SY OF 8" BITUMINOUS
PAVEMENT MILLING (AR800144)

TAXIWAY G STA. 169+00.00
TAXIWAY F STA. 14+00.00

TAXIWAY G STA. 13+62.50
LIMITS OF EXISTING BITUMINOUS
PAVEMENT MILLING

346 SY OF 5' WIDE 4"
BITUMINOUS
SHOULDER MILLING
(AR800143)

LIMITS OF BITUMINOUS
PAVEMENT REMOVAL SHALL
BE SAWCUT FULL DEPTH
PRIOR TO REMOVAL
(TYPICAL)(SAWCUT
INCIDENTAL TO REMOVAL
ITEM)

352 SY OF 5' WIDE 4"
BITUMINOUS
SHOULDER MILLING
(AR800143)

REMOVE RUNWAY EDGE MARKING (AR620900)

TAXIWAY G STA. 10+75.00
LIMITS OF EXISTING BITUMINOUS PAVEMENT MILLING
TAXIWAY G STA. 69+00.00
RUNWAY 7/25 STA. 10+00.00

NOTES

1. SEE PROPOSED PAVEMENT/JOINTING PLAN - TAXIWAY G FOR GEOMETRIC LAYOUT POINTS.
2. CONTRACTOR TO SAWCUT FULL DEPTH AT THE REMOVAL LIMITS PRIOR TO BEGINNING REMOVAL OPERATIONS.
3. EXISTING PAVEMENT, STRUCTURES OR STORM DRAIN TO REMAIN THAT IS DAMAGED DURING REMOVAL OPERATIONS TO BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE CONTRACT.
4. CONTRACTOR TO LAYOUT LIMITS OF REMOVAL, TO BE APPROVED BY THE ENGINEER BEFORE BEGINNING REMOVAL OPERATIONS.

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.



MICHAEL P. DUNN
DIRECTOR

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License No. 184-000613

CHICAGO ROCKFORD
INTERNATIONAL AIRPORT
ROCKFORD, ILLINOIS

GREATER ROCKFORD
AIRPORT AUTHORITY

IL. CONTRACT: R0027
IL. LETTING ITEM: 22A
IL. PROJECT: RFD-4541
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN
BY:

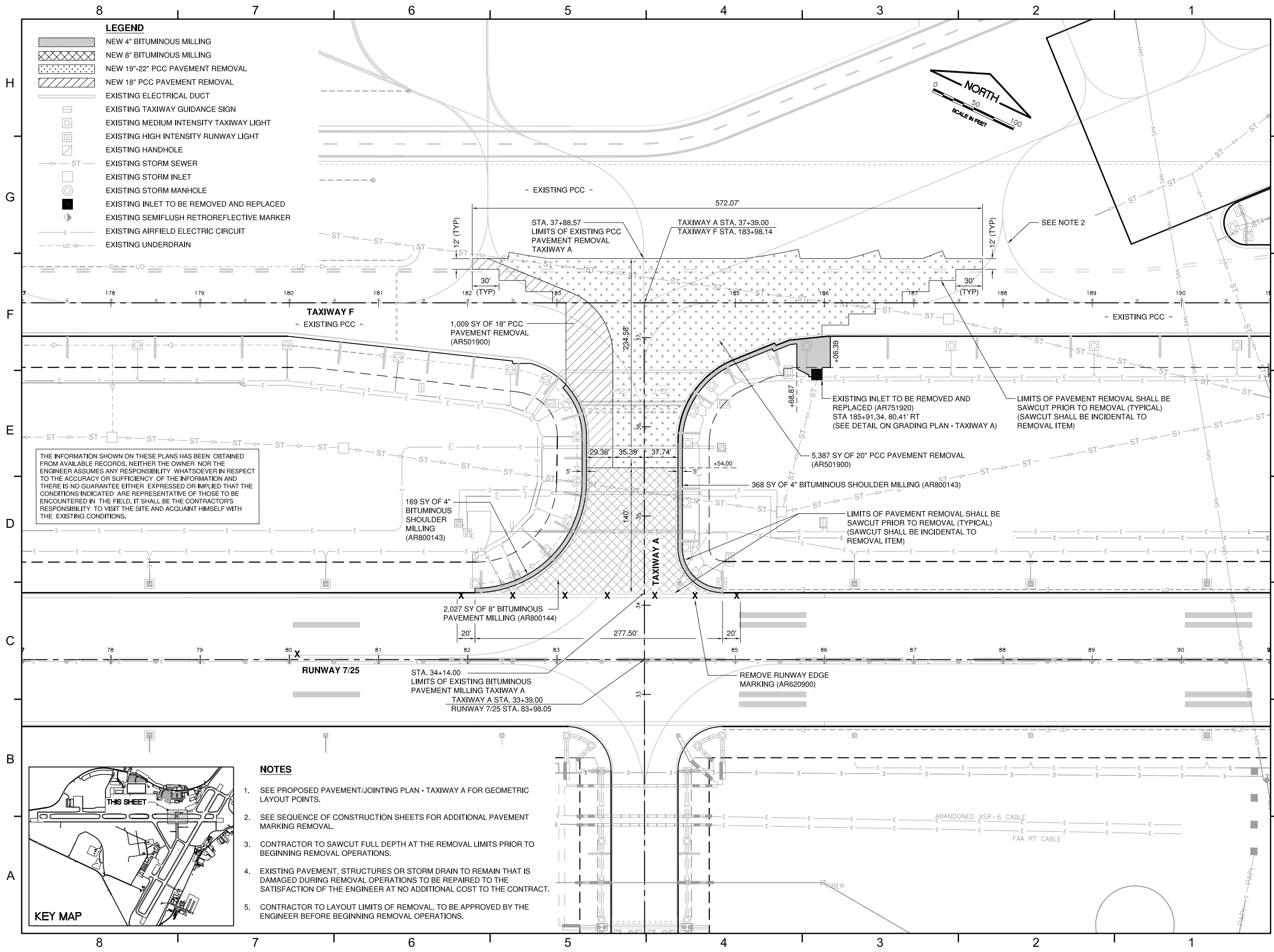
REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE THE INTERSECTION OF
TAXIWAY F AND A;
REHAB. TAXIWAY F AND G

SHEET TITLE:
**EXISTING CONDITIONS/
PROPOSED REMOVALS -
TAXIWAY A**

DESIGNED:	DRAWN:	CHECKED:
ABM	JRO	DLP
PROJECT NO.:	15258-05	
DATE:	06/03/2016	
SHEET NO.	12 OF 26	REVISION



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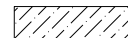

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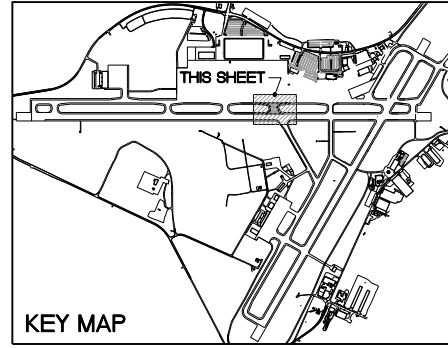
8 7 6 5 4 3 2 1

8 7 6 5 4 3 2 1

GEOMETRIC POINTS				
POINT #	STATION/OFFSET	BASELINE	NORTHING	EASTING
1	13+62.50, 420.00' LT	TXY G €	2016425.02	2585408.19
2	13+62.50, 426.90' RT	TXY G €	2016786.93	2586173.87
3	10+75.00, 210.00' LT	TXY G €	2016254.83	2585720.91
4	10+75.00, 252.24' RT	TXY G €	2016452.37	2586138.82

LEGEND

-  NEW 3" BITUMINOUS SURFACE COURSE
NEW 5" BITUMINOUS BASE COURSE
-  NEW 5' WIDE 4" BITUMINOUS SHOULDER



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REV	DATE	DESCRIPTION

PROJECT NAME:

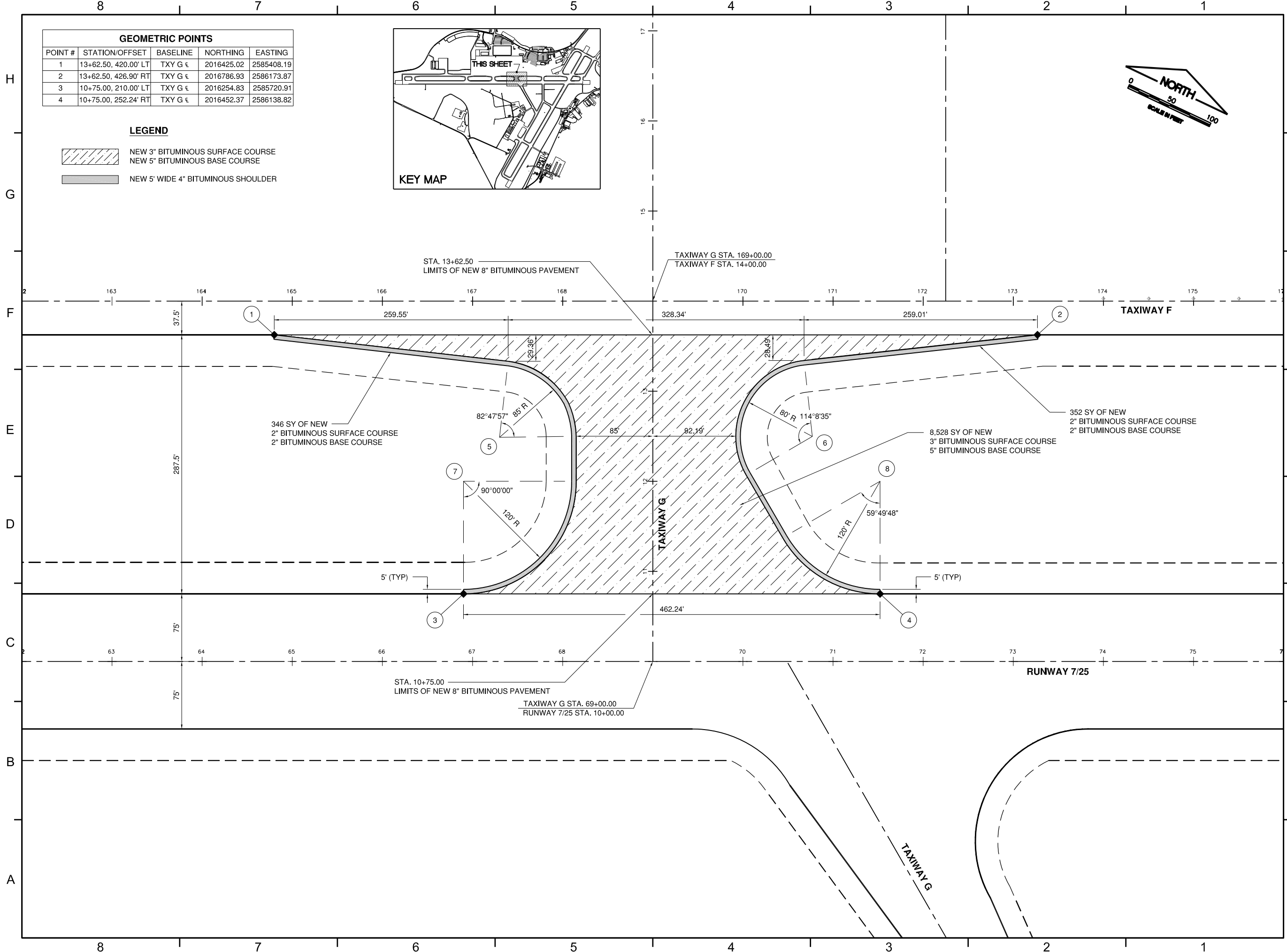
REHABILITATE THE INTERSECTION OF TAXIWAY F AND A;
REHAB. TAXIWAY F AND G

SHEET TITLE:
PROPOSED PAVEMENT PLAN - TAXIWAY G

DESIGNED:	DRAWN:	CHECKED:
ABM	JRO	DLP

PROJECT NO.: 15258-05
DATE: 06/03/2016

SHEET NO.	REVISION
13 OF 26	





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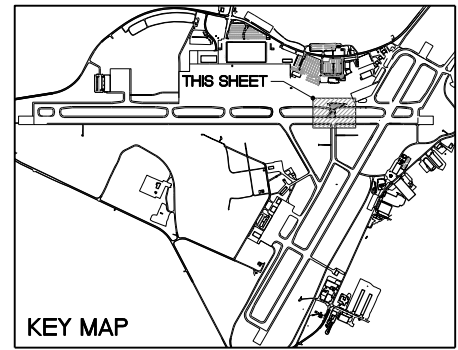
SHEET TITLE:
**PROPOSED PAVEMENT/
JOINTING PLAN -
TAXIWAY A**

DESIGNED: ABM DRAWN: JRO CHECKED: DLP

PROJECT NO.: 15258-05
DATE: 06/03/2016

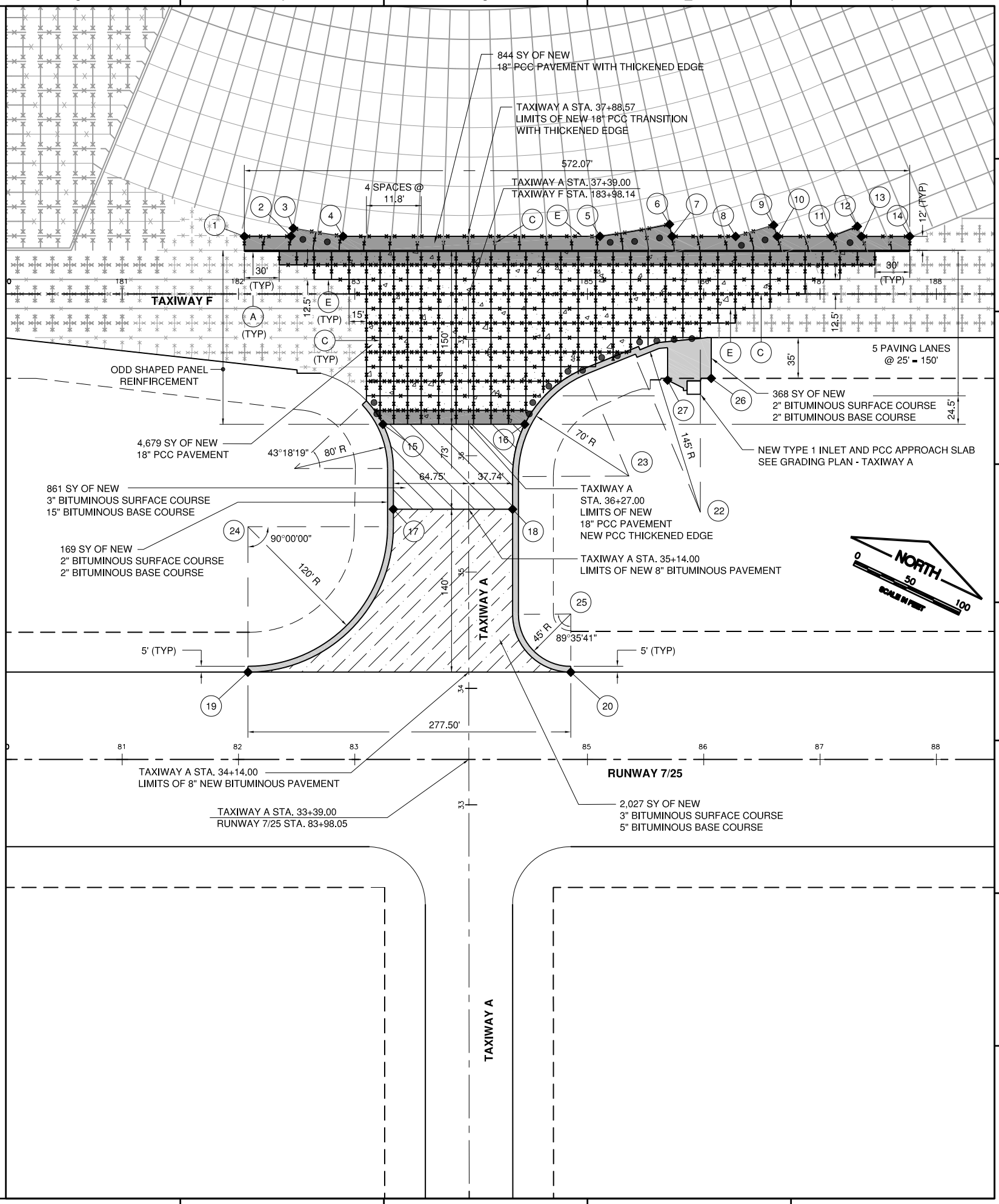
SHEET NO. 14 OF 26 REVISION

GEOMETRIC POINTS				
POINT #	STATION/OFFSET	BASELINE	NORTHING	EASTING
1	182+05.00, 49.50' LT	TXY F €	2017240.84	2586930.57
2	182+45.14, 49.50' LT	TXY F €	2017258.08	2586966.81
3	182+46.48, 56.66' LT	TXY F €	2017265.03	2586965.02
4	182+89.91, 49.50' LT	TXY F €	2017277.21	2587007.30
5	185+08.18, 49.50' LT	TXY F €	2017371.52	2587206.94
6	185+70.56, 59.89' LT	TXY F €	2017406.44	2587256.63
7	185+73.13, 49.50' LT	TXY F €	2017397.98	2587262.94
8	186+27.37, 49.50' LT	TXY F €	2017421.36	2587312.42
9	186+60.13, 59.24' LT	TXY F €	2017444.14	2587337.89
10	186+63.17, 49.50' LT	TXY F €	2017436.65	2587344.79
11	187+09.88, 49.50' LT	TXY F €	2017456.60	2587387.03
12	187+32.03, 58.12' LT	TXY F €	2017473.85	2587403.37
13	187+35.51, 49.50' LT	TXY F €	2017467.55	2587410.19
14	187+77.07, 49.50' LT	TXY F €	2017485.30	2587447.78
15	36+27.05, 73.67' LT	TXY A €	2017145.75	2587107.33
16	36+27.05, 49.91' RT	TXY A €	2017197.85	2587217.54
17	35+54.04, 64.75' LT	TXY A €	2017083.59	2587146.64
18	35+54.04, 37.75' RT	TXY A €	2017127.39	2587239.30
19	34+14.00, 189.76' LT	TXY A €	2016903.60	2587093.45
20	34+14.00, 87.64' RT	TXY A €	2017022.18	2587344.33
21	35+89.11, 149.73' LT	TXY A €	2017078.75	2587054.74
22	185+97.02, 187.50' RT	TXY F €	2017194.09	2587386.28
23	35+82.42, 137.76' RT	TXY A €	2017195.90	2587317.53
24	35+39.14, 189.76' LT	TXY A €	2017016.61	2587040.03
25	34+63.94, 87.68' RT	TXY A €	2017067.39	2587322.97
26	186+06.39, 72.50' RT	TXY F €	2017302.07	2587345.60
27	185+68.87, 74.00' RT	TXY F €	2017286.03	2587311.68



LEGEND

- NEW 18" PCC PAVEMENT
NEW 1-4" CRUSHED AGGREGATE BASE
- NEW 18" PCC TRANSITION WITH THICKENED EDGE
NEW 1-4" CRUSHED AGGREGATE BASE
- NEW 3" BITUMINOUS SURFACE COURSE
NEW 15" BITUMINOUS BASE COURSE
NEW 1-4" CRUSHED AGGREGATE BASE
- NEW 3" BITUMINOUS SURFACE COURSE
NEW 5" BITUMINOUS BASE COURSE
- NEW 5' WIDE 4" BITUMINOUS SHOULDER FILL
- EXISTING TYPE E DOWELED CONSTRUCTION JOINT
- EXISTING TYPE C DOWELED CONSTRUCTION JOINT
- NEW TYPE A EXPANSION JOINT
- NEW TYPE E DOWELED CONSTRUCTION JOINT
INSTALL DOWEL BARS IN EXISTING PAVEMENT (COST INCIDENTAL)
- NEW TYPE C DOWELED CONSTRUCTION JOINT
- NEW ODD SHAPED PANEL REINFORCEMENT





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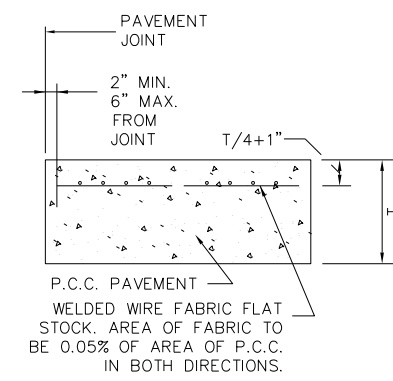
SHEET TITLE:

JOINTING DETAILS

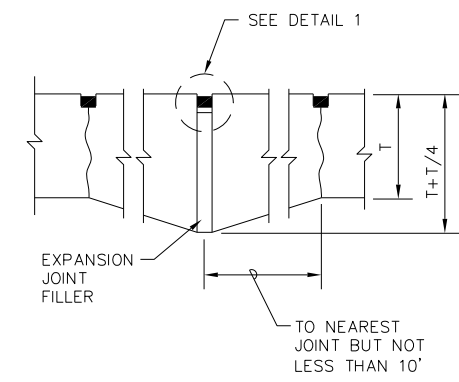
DESIGNED: **ABM** DRAWN: **JRO** CHECKED: **DLP**

PROJECT NO.: 15258-05
DATE: 06/03/2016

SHEET NO. 15 OF 26 REVISION



**ODD SHAPED
PANEL REINFORCEMENT**
• R



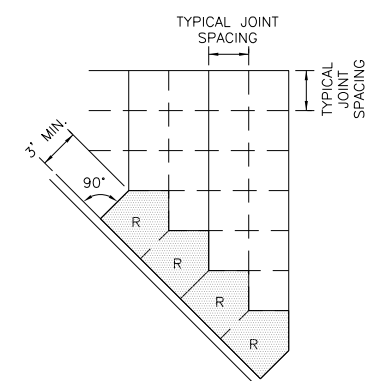
**THICKENED EDGE -
TYPE A EXPANSION JOINT**
SYMBOL = A =

TABLE 2

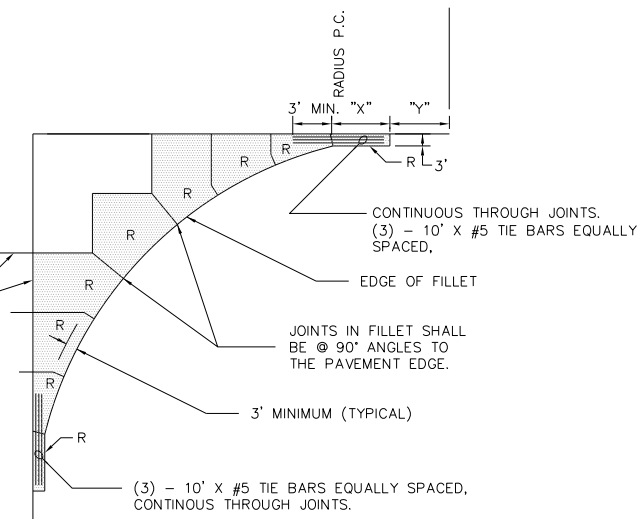
PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"
19	1 - 1/2"	20"	18"	#5	30"	30"
20	1 - 1/2"	20"	18"	#5	30"	30"

TABLE 1

THICKNESS T - INCHES	INITIAL SAW CUT I, INCHES I=(T/3) ±1/4"
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"
19	6.33"
20	6.67"



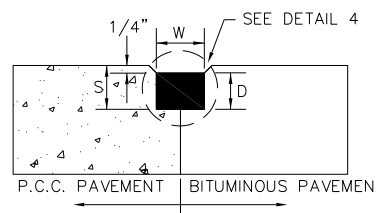
**JOINTING AT SKEWED EDGE
N.T.S.**



FILLET RADIUS	"X" (IN FEET)	"Y" (IN FEET)
20	4.30	6.24
25	4.88	7.00
30	5.40	7.68
50	7.11	9.95
75	8.79	12.21
85	9.38	13.00
100	10.21	14.11
125	11.44	15.78
150	12.56	17.29
175	13.58	18.68
200	14.53	19.98

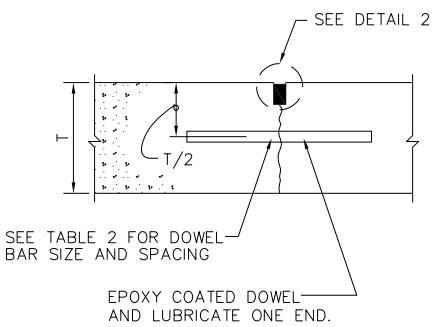
**FILLET DETAIL AND FILLET REINFORCING LAYOUT
N.T.S.**

MISCELLANEOUS JOINT



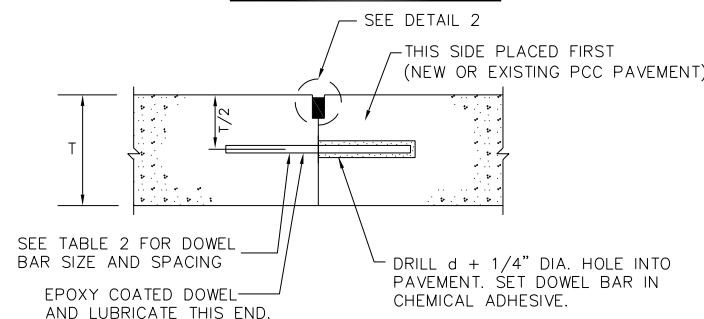
**CONCRETE / BITUMINOUS INTERFACE
JOINT SEALING**

CONTRACTION JOINTS

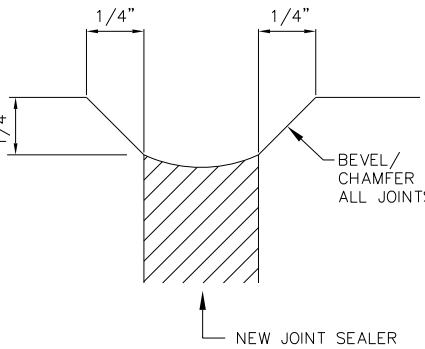


TYPE C DOWELED
SYMBOL = C =

CONSTRUCTION JOINTS

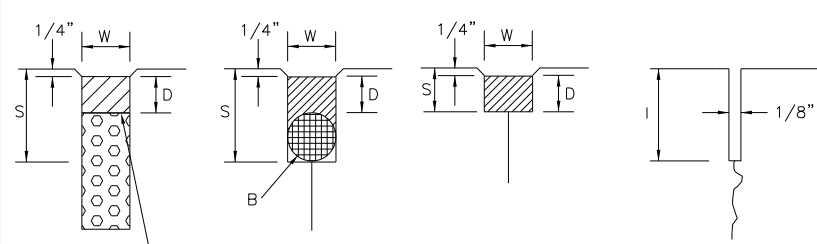


TYPE E DOWELED
SYMBOL = E =



CHAMFER/BEVEL JOINT DETAIL

JOINT SEALING DETAILS



	DETAIL 1	DETAIL 2	DETAIL 4
	SILICONE	SILICONE	SILICONE
W=WIDTH OF SEALANT RESERVOIR (IN.)	1-1/2	1/2	3/8
D=DEPTH OF SEALANT RESERVOIR (IN.)	1-1/2	1/2	1/4
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	N/A
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-3/8	1/2

NOTE: JOINT SEALING SHALL BE PER SECTION 605 OF SPECIAL PROVISIONS. DIMENSIONS MAY VARY WITH MANUFACTURER'S RECOMMENDATIONS.

JOINTING NOTES

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL ENSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO INSTALLATION. ALTERNATE METHODS OF PLACEMENT OF DOWEL BARS MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. TRANSVERSE DOWEL BAR IMPLANTING WILL NOT BE ALLOWED.
- ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- COST OF ALL JOINT SAWING, CLEANING AND SEALING TO BE PAID UNDER AR605510.
- SHOULD THE POURING OPERATION REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
- EPOXY-COATED DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE RESIDENT ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.
- CONCRETE / BITUMINOUS INTERFACE SHALL BE SEALED PER JOINT DETAIL ABOVE.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
- CONTRACTOR SHALL CONSTRUCT A 1/4" CHAMFER ON ALL CONCRETE JOINTS AT NO ADDITIONAL COST.
- JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.



MICHAEL P. DUNN
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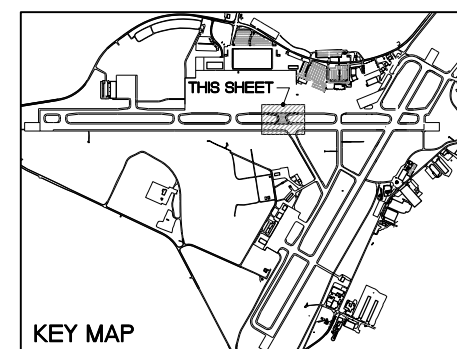
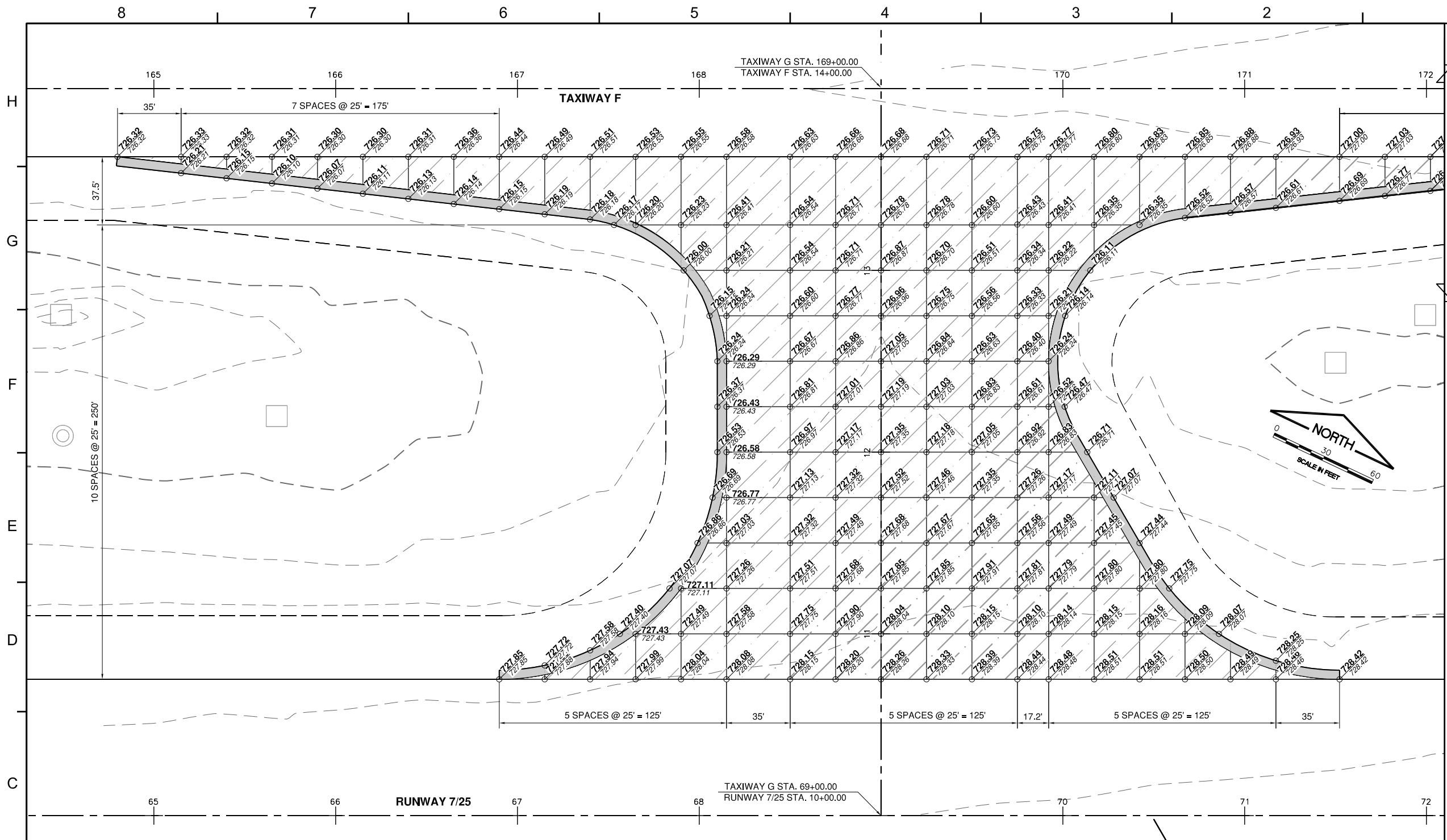
SHEET TITLE:

**GRADING PLAN -
TAXIWAY G**

DESIGNED: **SMS** DRAWN: **JRO** CHECKED: **DLP**

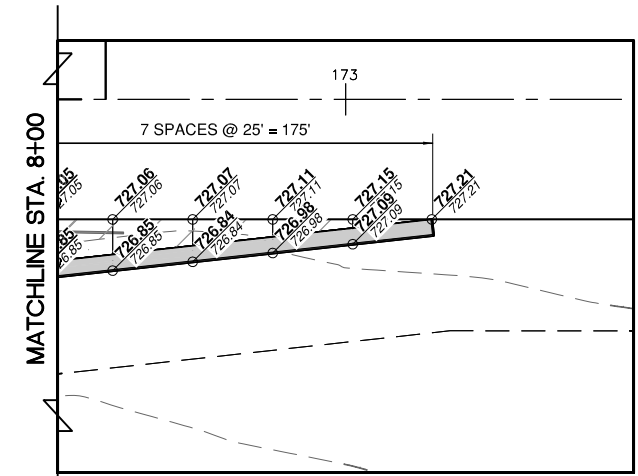
PROJECT NO.: 15258-05
DATE: 06/03/2016

SHEET NO. 16 OF 26 REVISION

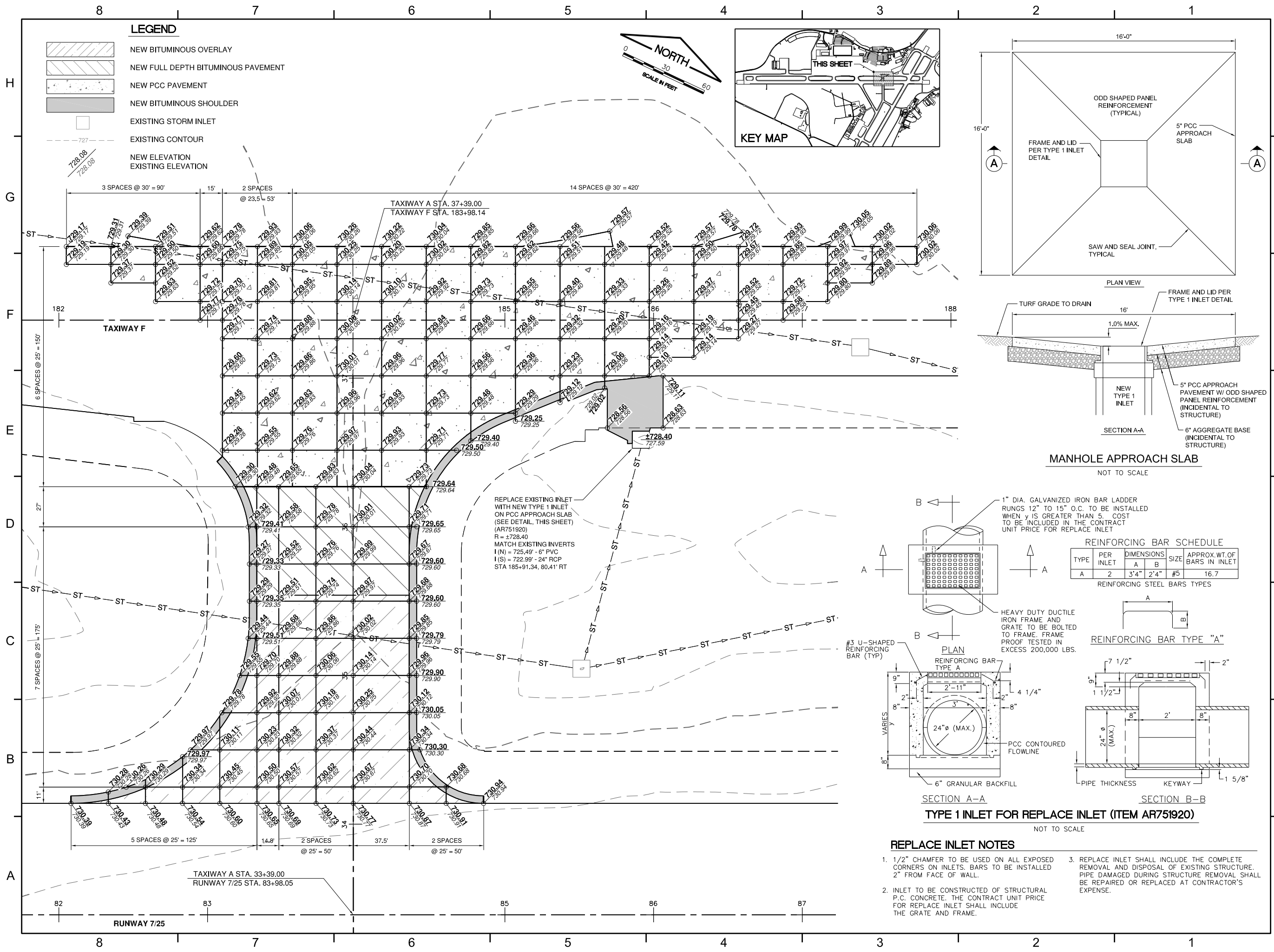


LEGEND

- NEW BITUMINOUS OVERLAY
- NEW BITUMINOUS SHOULDER
- EXISTING STORM INLET
- EXISTING CONTOUR
- NEW ELEVATION
- EXISTING ELEVATION



8 7 6 5 4 3 2 1



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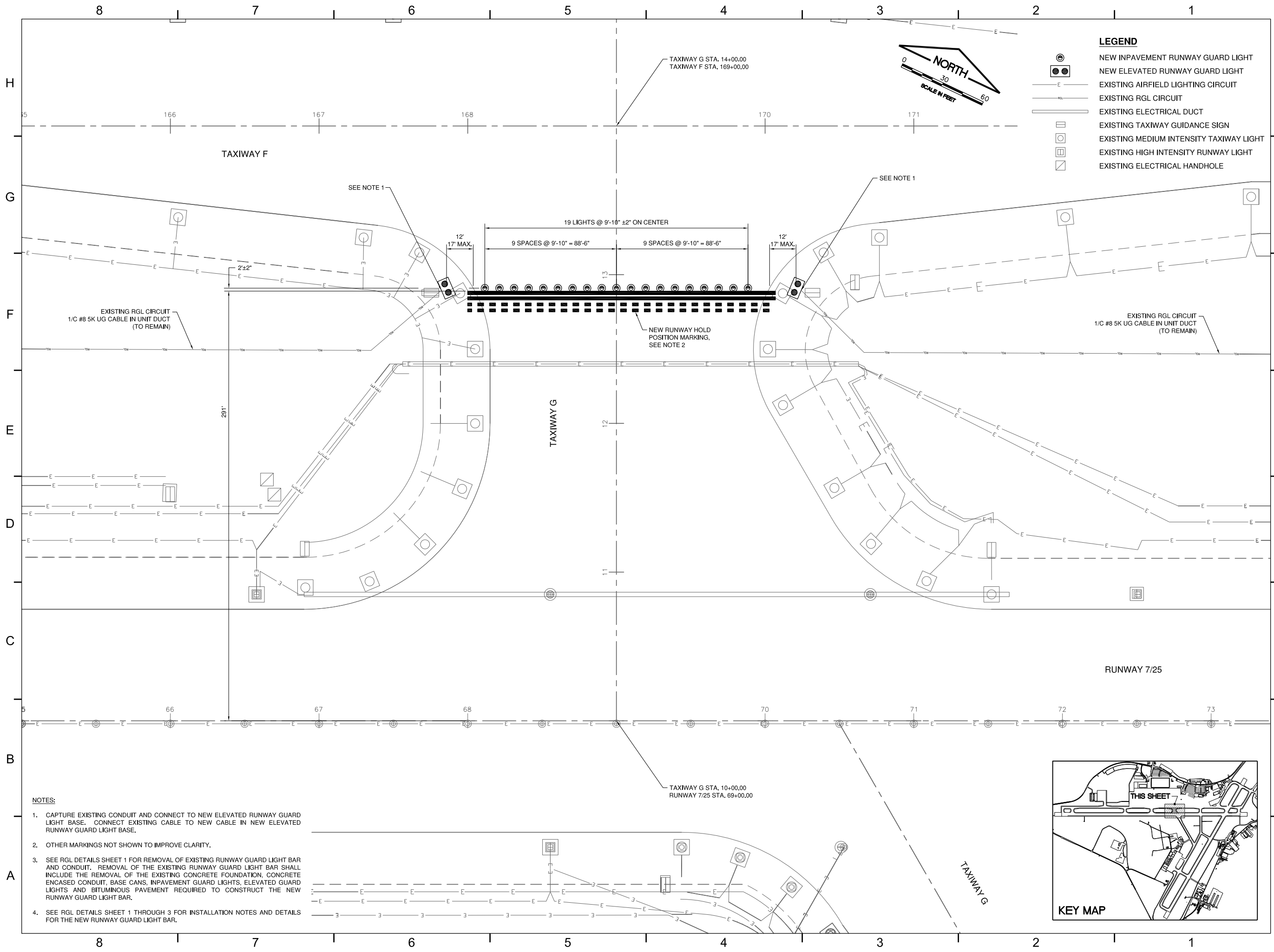
SHEET TITLE:

GRADING PLAN - TAXIWAY A

DESIGNED: **SMS** DRAWN: **JRO** CHECKED: **DLP**

PROJECT NO.: 15258-05
DATE: 06/03/2016

SHEET NO. 17 OF 26 REVISION



- LEGEND**
- NEW INPAVEMENT RUNWAY GUARD LIGHT
 - NEW ELEVATED RUNWAY GUARD LIGHT
 - EXISTING AIRFIELD LIGHTING CIRCUIT
 - EXISTING RGL CIRCUIT
 - EXISTING ELECTRICAL DUCT
 - EXISTING TAXIWAY GUIDANCE SIGN
 - EXISTING MEDIUM INTENSITY TAXIWAY LIGHT
 - EXISTING HIGH INTENSITY RUNWAY LIGHT
 - EXISTING ELECTRICAL HANDHOLE



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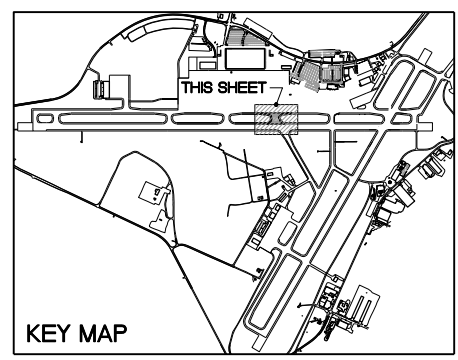
SHEET TITLE:
TAXIWAY G RGL PLAN

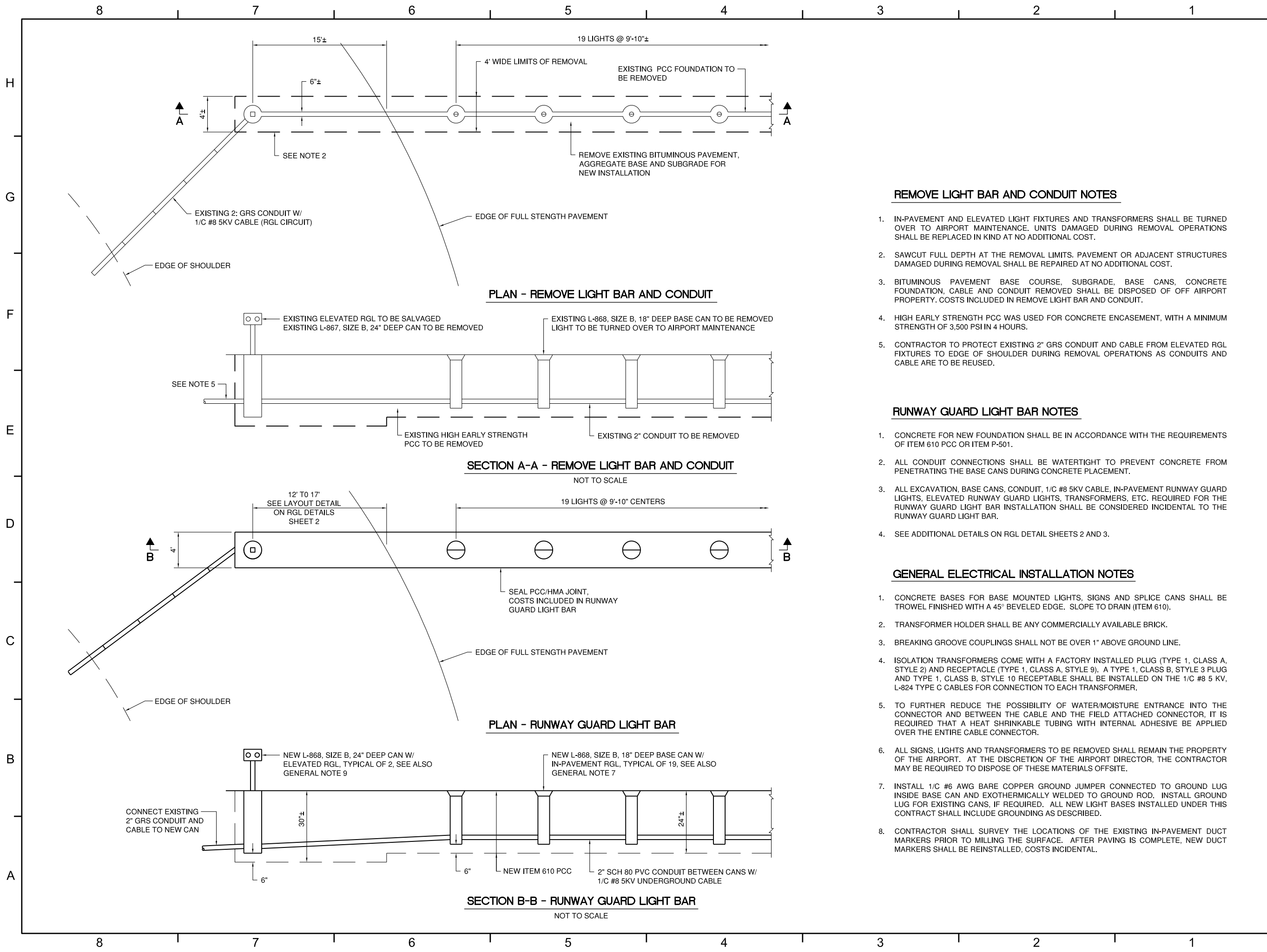
DESIGNED: **SMS** DRAWN: **JRO** CHECKED: **DLP**

PROJECT NO.: 15258-05
DATE: 06/03/2016

SHEET NO. 18 OF 26 REVISION

- NOTES:**
1. CAPTURE EXISTING CONDUIT AND CONNECT TO NEW ELEVATED RUNWAY GUARD LIGHT BASE. CONNECT EXISTING CABLE TO NEW CABLE IN NEW ELEVATED RUNWAY GUARD LIGHT BASE.
 2. OTHER MARKINGS NOT SHOWN TO IMPROVE CLARITY.
 3. SEE RGL DETAILS SHEET 1 FOR REMOVAL OF EXISTING RUNWAY GUARD LIGHT BAR AND CONDUIT. REMOVAL OF THE EXISTING RUNWAY GUARD LIGHT BAR SHALL INCLUDE THE REMOVAL OF THE EXISTING CONCRETE FOUNDATION, CONCRETE ENCASED CONDUIT, BASE CANS, INPAVEMENT GUARD LIGHTS, ELEVATED GUARD LIGHTS AND BITUMINOUS PAVEMENT REQUIRED TO CONSTRUCT THE NEW RUNWAY GUARD LIGHT BAR.
 4. SEE RGL DETAILS SHEET 1 THROUGH 3 FOR INSTALLATION NOTES AND DETAILS FOR THE NEW RUNWAY GUARD LIGHT BAR.





REMOVE LIGHT BAR AND CONDUIT NOTES

1. IN-PAVEMENT AND ELEVATED LIGHT FIXTURES AND TRANSFORMERS SHALL BE TURNED OVER TO AIRPORT MAINTENANCE. UNITS DAMAGED DURING REMOVAL OPERATIONS SHALL BE REPLACED IN KIND AT NO ADDITIONAL COST.
2. SAWCUT FULL DEPTH AT THE REMOVAL LIMITS. PAVEMENT OR ADJACENT STRUCTURES DAMAGED DURING REMOVAL SHALL BE REPAIRED AT NO ADDITIONAL COST.
3. BITUMINOUS PAVEMENT BASE COURSE, SUBGRADE, BASE CANS, CONCRETE FOUNDATION, CABLE AND CONDUIT REMOVED SHALL BE DISPOSED OF OFF AIRPORT PROPERTY. COSTS INCLUDED IN REMOVE LIGHT BAR AND CONDUIT.
4. HIGH EARLY STRENGTH PCC WAS USED FOR CONCRETE ENCASMENT, WITH A MINIMUM STRENGTH OF 3,500 PSI IN 4 HOURS.
5. CONTRACTOR TO PROTECT EXISTING 2" GRS CONDUIT AND CABLE FROM ELEVATED RGL FIXTURES TO EDGE OF SHOULDER DURING REMOVAL OPERATIONS AS CONDUITS AND CABLE ARE TO BE REUSED.

RUNWAY GUARD LIGHT BAR NOTES

1. CONCRETE FOR NEW FOUNDATION SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF ITEM 610 PCC OR ITEM P-501.
2. ALL CONDUIT CONNECTIONS SHALL BE WATERTIGHT TO PREVENT CONCRETE FROM PENETRATING THE BASE CANS DURING CONCRETE PLACEMENT.
3. ALL EXCAVATION, BASE CANS, CONDUIT, 1/2 #8 5KV CABLE, IN-PAVEMENT RUNWAY GUARD LIGHTS, ELEVATED RUNWAY GUARD LIGHTS, TRANSFORMERS, ETC. REQUIRED FOR THE RUNWAY GUARD LIGHT BAR INSTALLATION SHALL BE CONSIDERED INCIDENTAL TO THE RUNWAY GUARD LIGHT BAR.
4. SEE ADDITIONAL DETAILS ON RGL DETAIL SHEETS 2 AND 3.

GENERAL ELECTRICAL INSTALLATION NOTES

1. CONCRETE BASES FOR BASE MOUNTED LIGHTS, SIGNS AND SPLICE CANS SHALL BE TROWEL FINISHED WITH A 45° BEVELED EDGE. SLOPE TO DRAIN (ITEM 610).
2. TRANSFORMER HOLDER SHALL BE ANY COMMERCIALY AVAILABLE BRICK.
3. BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
4. ISOLATION TRANSFORMERS COME WITH A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/2 #8 5 KV, L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
5. TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR AND BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
6. ALL SIGNS, LIGHTS AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT DIRECTOR, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE.
7. INSTALL 1/2 #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN AND EXOTHERMICALLY WELDED TO GROUND ROD. INSTALL GROUND LUG FOR EXISTING CANS, IF REQUIRED. ALL NEW LIGHT BASES INSTALLED UNDER THIS CONTRACT SHALL INCLUDE GROUNDING AS DESCRIBED.
8. CONTRACTOR SHALL SURVEY THE LOCATIONS OF THE EXISTING IN-PAVEMENT DUCT MARKERS PRIOR TO MILLING THE SURFACE. AFTER PAVING IS COMPLETE, NEW DUCT MARKERS SHALL BE REINSTALLED, COSTS INCIDENTAL.



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INTERNATIONAL AIRPORT
ROCKFORD, ILLINOIS

GREATER ROCKFORD
AIRPORT AUTHORITY

IL. CONTRACT: R0027
IL. LETTING ITEM: 22A
IL. PROJECT: RFD-4541
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE THE INTERSECTION OF
TAXIWAY F AND A;
REHAB. TAXIWAY F AND G

SHEET TITLE:

RGL DETAILS 1

DESIGNED: **SMS** DRAWN: **JRO** CHECKED: **DLP**

PROJECT NO.: 15258-05
DATE: 06/03/2016

SHEET NO. **19 OF 26** REVISION



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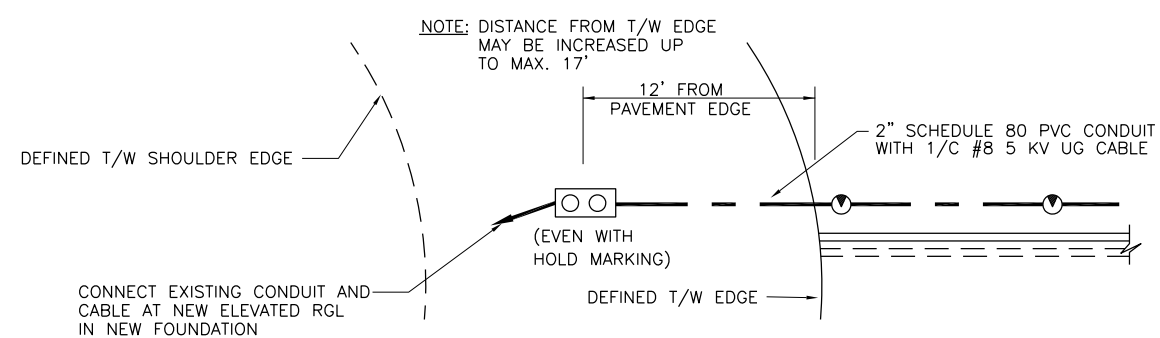
PROJECT NAME:

REHABILITATE THE INTERSECTION OF
TAXIWAY F AND A;
REHAB. TAXIWAY F AND G

SHEET TITLE:

RGL DETAILS 2

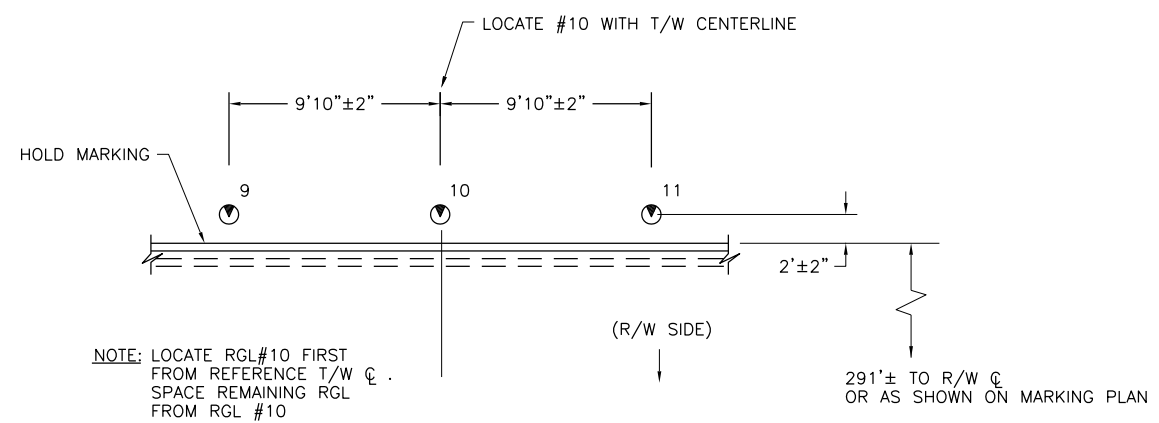
DESIGNED:	DRAWN:	CHECKED:
SMS	JRO	DLP
PROJECT NO.:	15258-05	
DATE:	06/03/2016	
SHEET NO.	20 OF 26	REVISION



ELEVATED RGL LAYOUT DETAIL (TYP)

NTS

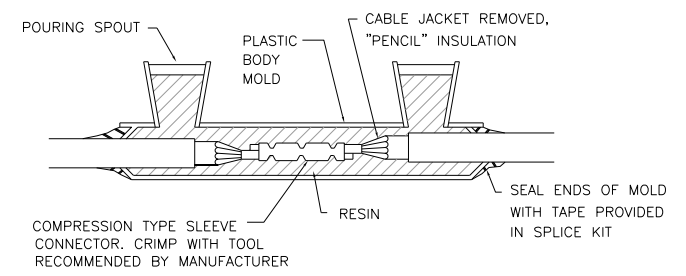
AIMING: TO A POINT 150' ALONG TAXIWAY CENTERLINE AWAY FROM RUNWAY.



INPAVEMENT RGL LAYOUT DETAIL

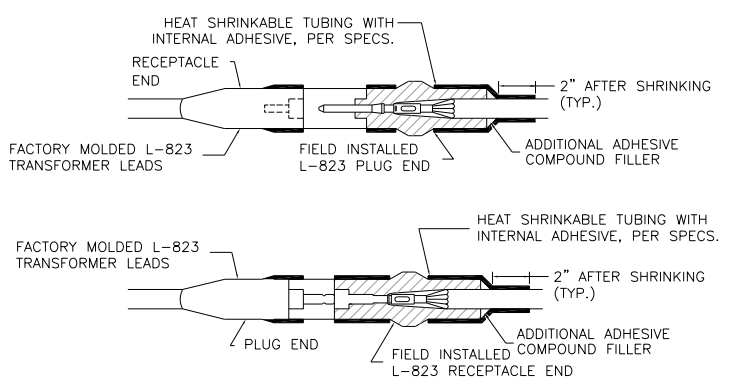
NTS

AIMING: PERPENDICULAR TO HOLD MARKING AWAY FROM RUNWAY.



TYPE A - CABLE SPLICE

FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY
NOT TO SCALE

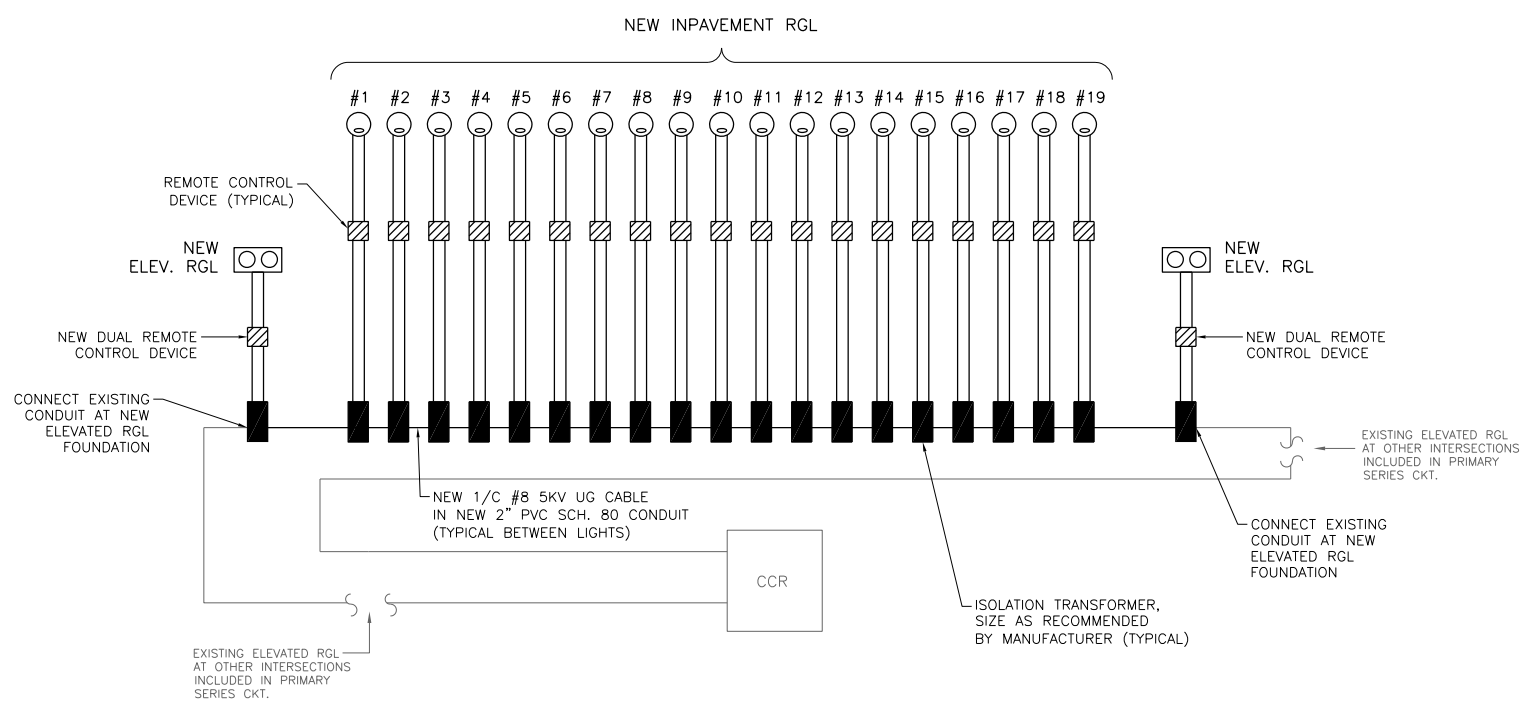


TYPE C AND D - CABLE SPLICE

FOR SPLICES AT RUNWAY/TAXIWAY LIGHTS AND SIGNS
NOT TO SCALE

SPLICE NOTES

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE RGL INSTALLATION.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF FIVE (5) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.
- THE CONTRACTOR MAY ELECT TO USE AN FAA APPROVED PRIMARY CONNECTOR KIT IN LIEU OF HEAT SHRINK KIT AT NO ADDITIONAL COST.



RGL SCHEMATIC DIAGRAM
(TYP. FOR INTERSECTION WITH INPAVEMENT RGL)

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8 7 6 5 4 3 2 1

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BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE THE INTERSECTION OF
TAXIWAY F AND A;
REHAB. TAXIWAY F AND G

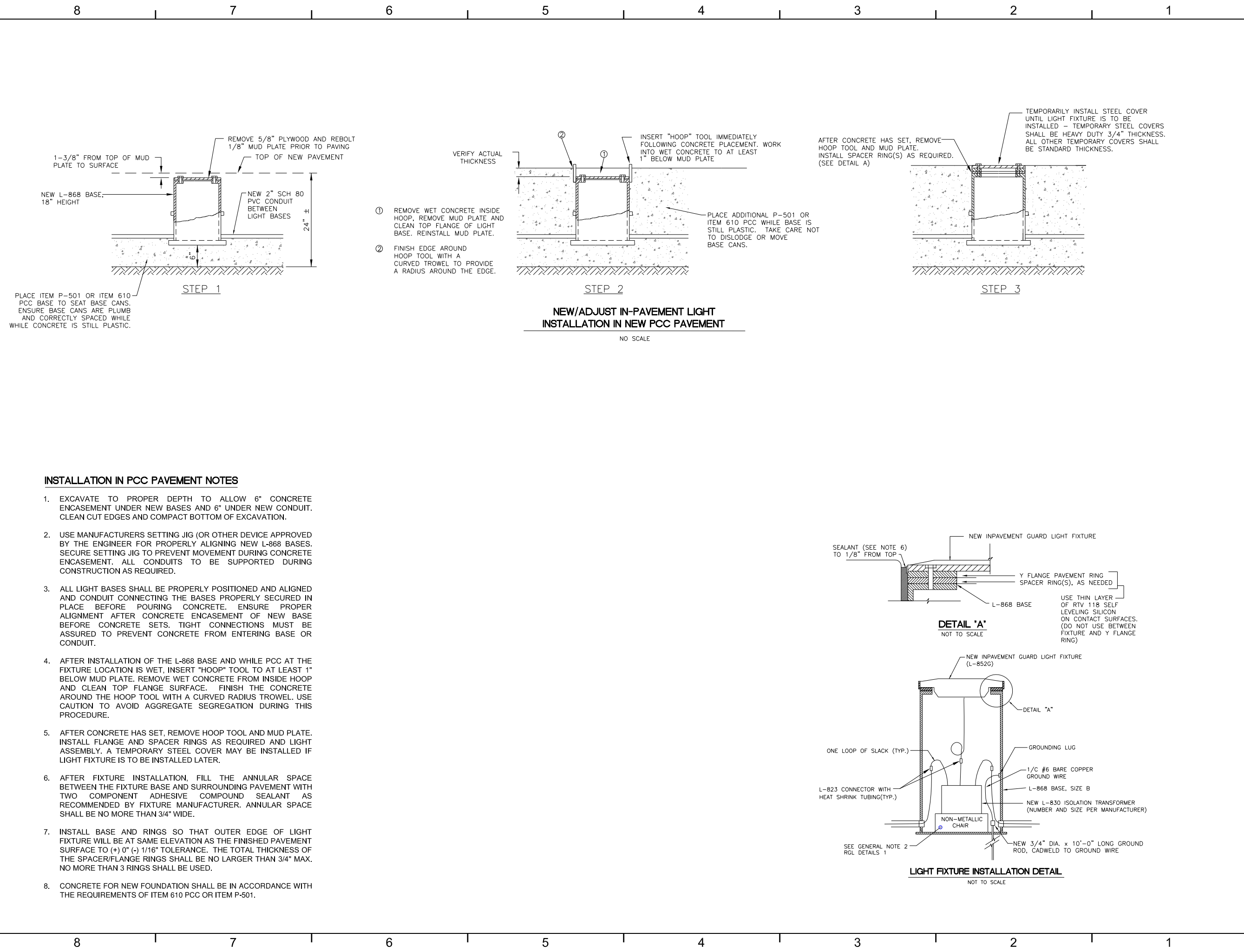
SHEET TITLE:

RGL DETAILS 3

DESIGNED: **SMS** DRAWN: **JRO** CHECKED: **DLP**

PROJECT NO.: 15258-05
DATE: 06/03/2016

SHEET NO. **21 OF 26** REVISION





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REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE THE INTERSECTION OF
TAXIWAY F AND A;
REHAB. TAXIWAY F AND G

SHEET TITLE:

**PAVEMENT MARKING PLAN -
TAXIWAY G**

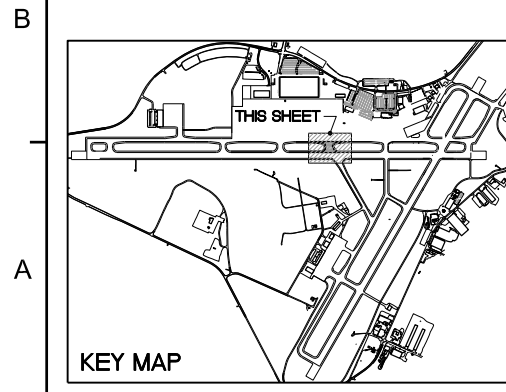
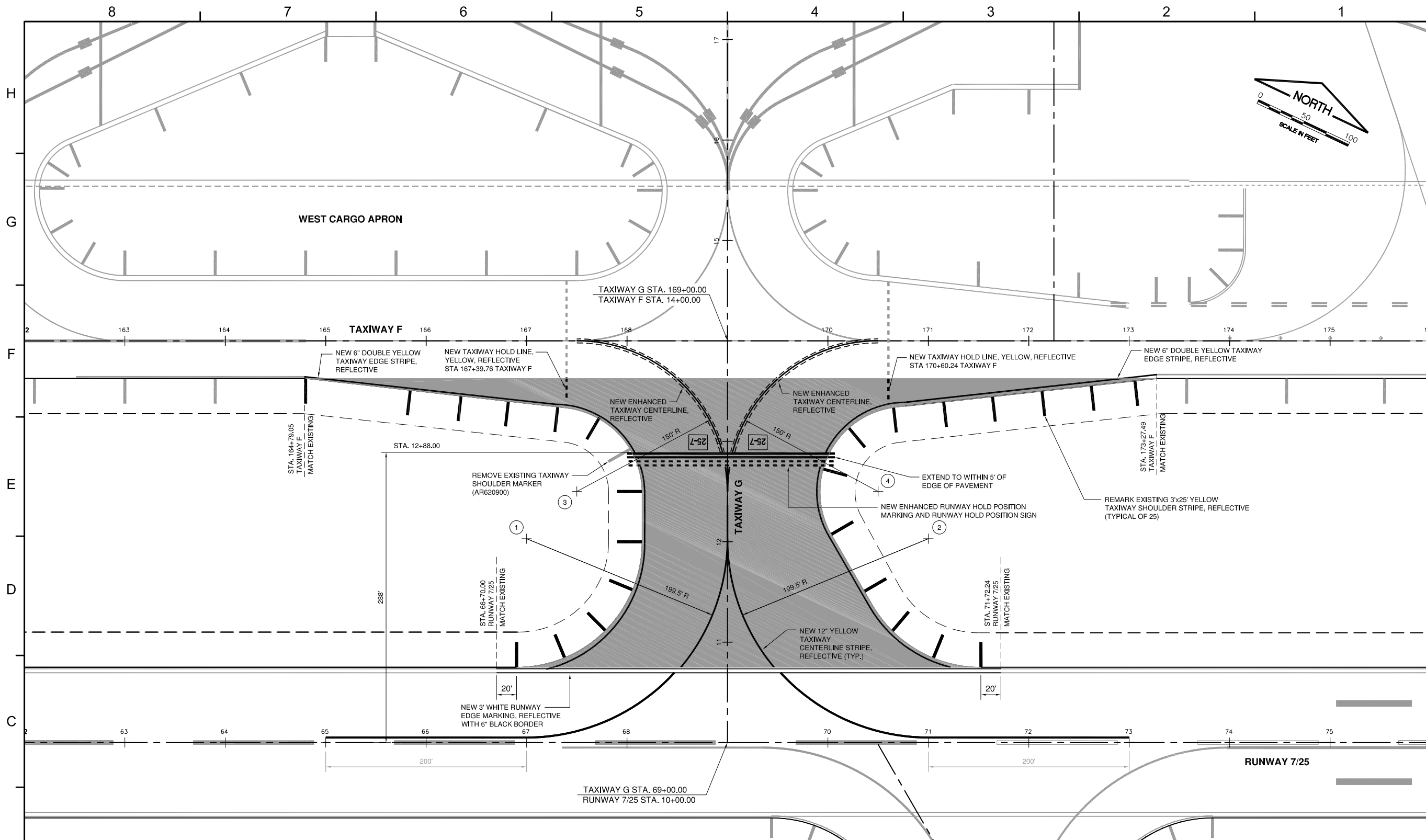
DESIGNED: DRAWN: CHECKED:
ABM JRO DLP

PROJECT NO.: 15258-05

DATE: 06/03/2016

SHEET NO. REVISION

22 OF 26



LEGEND

- NEW PAVEMENT MARKING
- NEW RUNWAY HOLD POSITION SIGN
- EXISTING PAVEMENT MARKING
- NEW BITUMINOUS PAVEMENT

NOTES

1. ALL PAVEMENT MARKINGS WITHIN LIMITS OF PAVEMENT WORK SHALL BE REPLACED.
2. MARKINGS SHALL BE STOPPED SHORT OF LIGHTING FIXTURES AND CONTINUED ON THE OTHER SIDE.
3. WHEN YELLOW TAXIWAY STRIPING CROSSES ANY WHITE RUNWAY STRIPING, THE TAXIWAY STRIPE SHALL BE GAPPED 6" EACH SIDE OF RUNWAY STRIPE EXCEPT RUNWAY/RUNWAY HOLDLINES.
4. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
5. ALL LETTERS, NUMBERS AND SYMBOLS SHALL CONFORM TO FAA ADVISORY CIRCULAR 150/5340-1L (LATEST EDITION).
6. SHOULDER MARKINGS IN CONFLICT WITH THE RUNWAY HOLD LINE SHALL BE OMITTED.
7. RUNWAY HOLD POSITION SIGNS SHALL BE PRE-FORMED THERMOPLASTIC MARKINGS (AR800116). ALL OTHER MARKINGS SHALL BE WATERBORNE (AR620520).
8. ALL TAXIWAY MARKINGS ARE REFLECTIVE YELLOW WITH A 6" BLACK BORDER (AR620525) UNLESS NOTED.

POINT TABLE

1	N:	2016375.17	TAXIWAY F STA 167+00.00
	E:	2585675.09	TAXIWAY G STA 12+03.38
2	N:	2016546.11	TAXIWAY F STA 171+00.00
	E:	2586036.73	TAXIWAY G STA 12+03.38
3	N:	2016439.03	TAXIWAY F STA 167+50.00
	E:	2585700.21	TAXIWAY G STA 12+50.38
4	N:	2016567.23	TAXIWAY F STA 170+50.00
	E:	2585971.44	TAXIWAY G STA 12+50.38



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GREATER ROCKFORD
AIRPORT AUTHORITY

IL. CONTRACT: R0027
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IL. PROJECT: RFD-4541
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN
BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE THE INTERSECTION OF
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REHAB. TAXIWAY F AND G

SHEET TITLE:

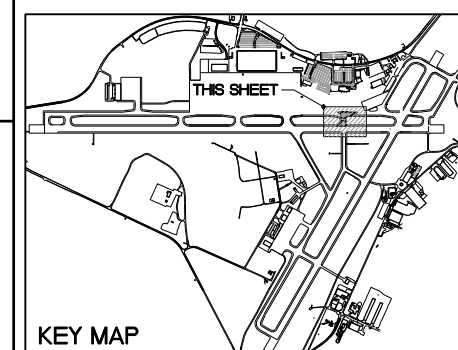
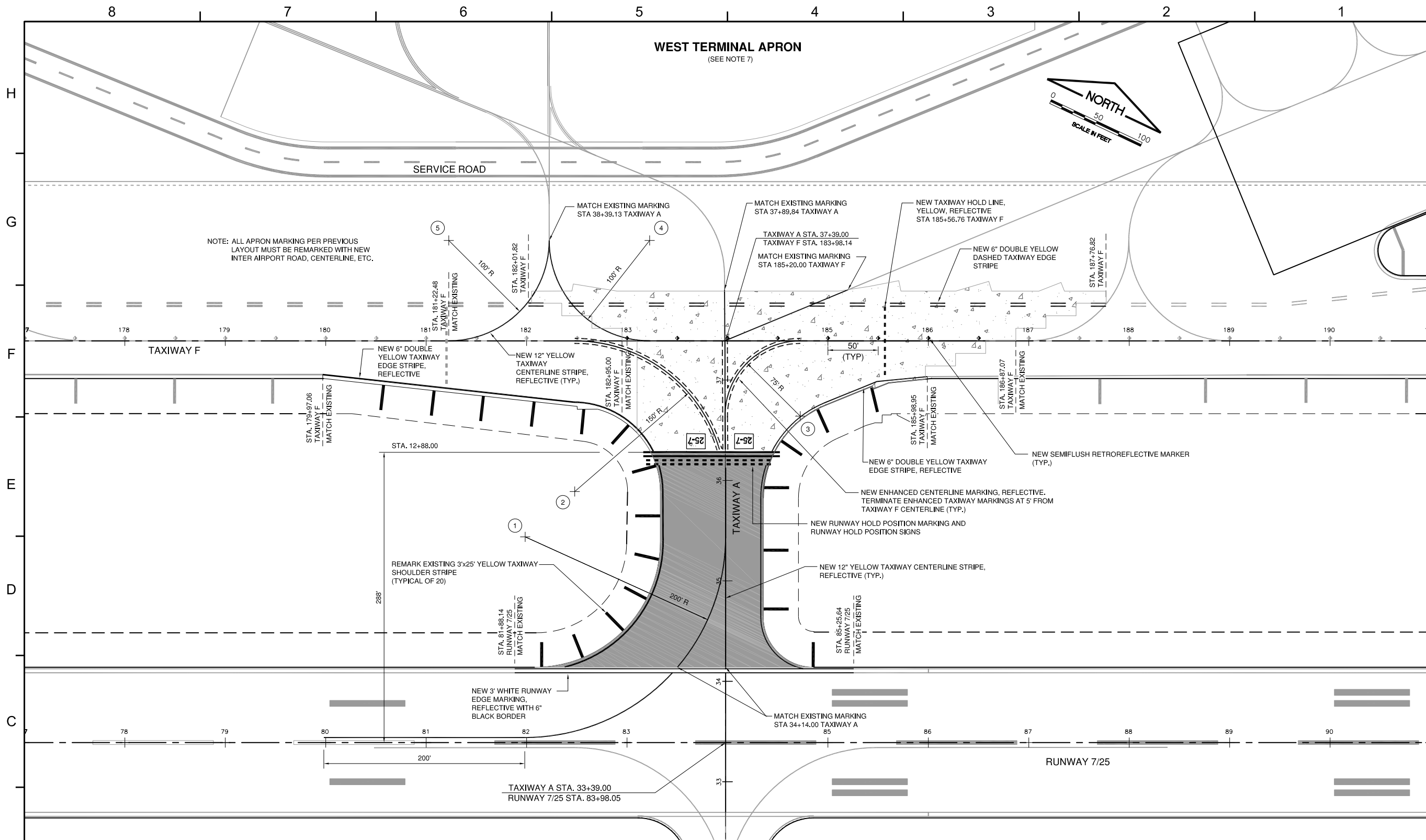
**PAVEMENT MARKING PLAN -
TAXIWAY A**

DESIGNED: DRAWN: CHECKED:
ABM JRO DLP

PROJECT NO.: 15258-05
DATE: 06/03/2016

SHEET NO. REVISION

23 OF 26



LEGEND

	NEW PAVEMENT MARKING
	NEW RUNWAY HOLD POSITION SIGN
	EXISTING PAVEMENT MARKING
	NEW BITUMINOUS PAVEMENT
	NEW PCC PAVEMENT
	NEW SEMIFLUSH RETROREFLECTIVE MARKER
	EXISTING SEMIFLUSH RETROREFLECTIVE MARKER

- NOTES**
- ALL PAVEMENT MARKINGS WITHIN LIMITS OF PAVEMENT WORK SHALL BE REPLACED.
 - MARKINGS SHALL BE STOPPED SHORT OF LIGHTING FIXTURES AND CONTINUED ON THE OTHER SIDE.
 - WHEN YELLOW TAXIWAY STRIPING CROSSES ANY WHITE RUNWAY STRIPING, THE TAXIWAY STRIPE SHALL BE GAPPED 6" EACH SIDE OF RUNWAY STRIPE EXCEPT RUNWAY/RUNWAY HOLDLINES.
 - THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
 - ALL LETTERS, NUMBERS AND SYMBOLS SHALL CONFORM TO FAA ADVISORY CIRCULAR 150/5340-1L (LATEST EDITION).
 - SHOULDER MARKINGS IN CONFLICT WITH THE RUNWAY HOLD LINE SHALL BE OMITTED.
 - RUNWAY HOLD POSITION SIGNS SHALL BE PRE-FORMED THERMOPLASTIC MARKINGS (AR800116). ALL OTHER MARKINGS SHALL BE WATERBORNE (AR620520).
 - ALL TAXIWAY MARKINGS ARE REFLECTIVE YELLOW WITH A 6" BLACK BORDER (AR620525) UNLESS NOTED.

POINT TABLE

	N:	E:	DESCRIPTION
1	2017016.87	2587029.38	TAXIWAY F STA 181+98.62
			TAXIWAY A STA 35+43.94
2	2017078.84	2587054.68	TAXIWAY F STA 182+47.98
			TAXIWAY A STA 35+89.11
3	2017242.59	2587225.62	TAXIWAY F STA 184+72.51
			TAXIWAY A STA 36+63.95
4	2017336.72	2587015.22	TAXIWAY F STA 183+22.51
			TAXIWAY A STA 38+39.06
5	2017251.25	2586834.41	TAXIWAY F STA 181+22.51
			TAXIWAY A STA 38+39.21



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IL. PROJECT: RFD-4541
A.I.P. PROJECT: 3-17-0088-XX

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BY:

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PROJECT NAME:

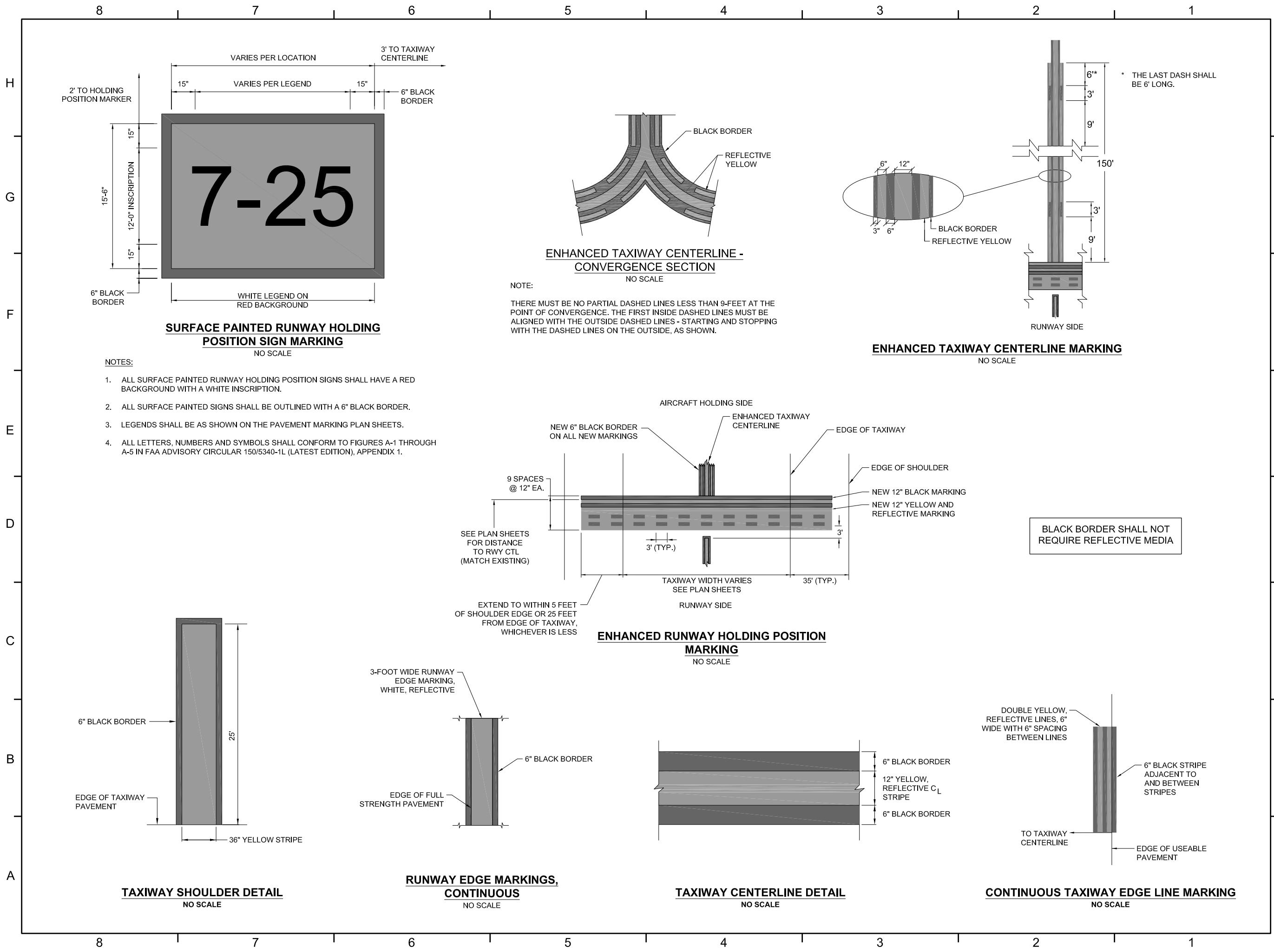
REHABILITATE THE INTERSECTION OF
TAXIWAY F AND A;
REHAB. TAXIWAY F AND G

SHEET TITLE:
**PAVEMENT MARKING
DETAILS - 1**

DESIGNED: **ABM** DRAWN: **JRO** CHECKED: **DLP**

PROJECT NO.: 15258-05
DATE: 06/03/2016

SHEET NO. **24** OF **26** REVISION



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BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE THE INTERSECTION OF
TAXIWAY F AND A;
REHAB. TAXIWAY F AND G

SHEET TITLE:

**PAVEMENT MARKING
DETAILS - 2**

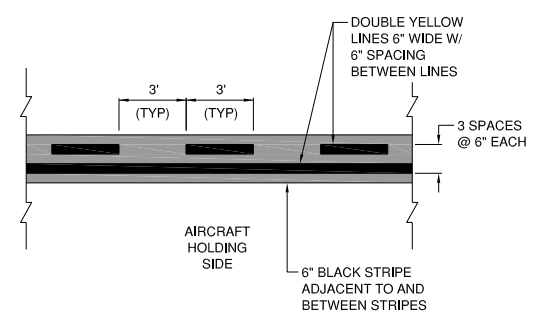
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PROJECT NO.: 15258-05
DATE: 06/03/2016

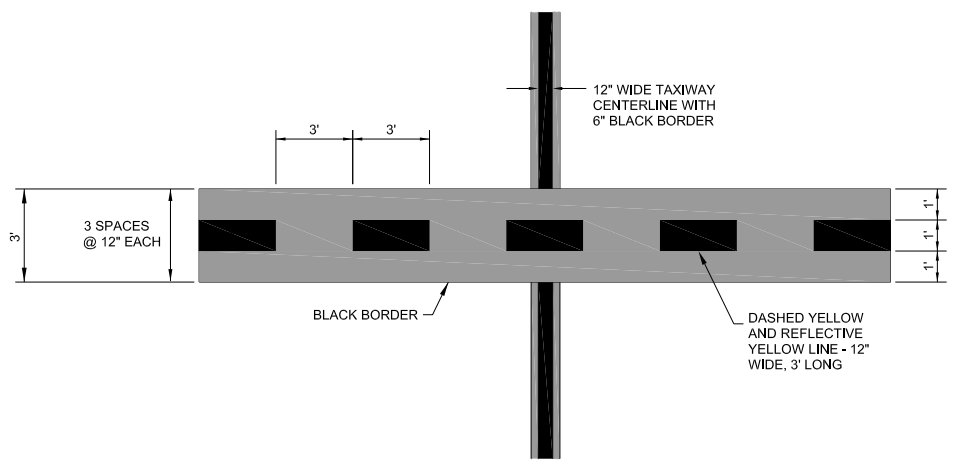
SHEET NO. 25 OF 26 REVISION

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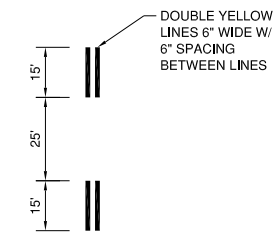
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NON-MOVEMENT AREA MARKING DETAIL
NOT TO SCALE



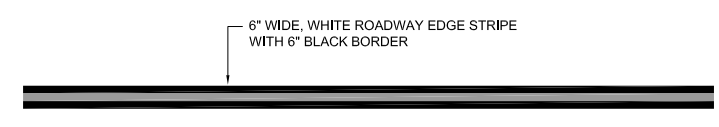
TAXIWAY HOLD POSITION MARKING
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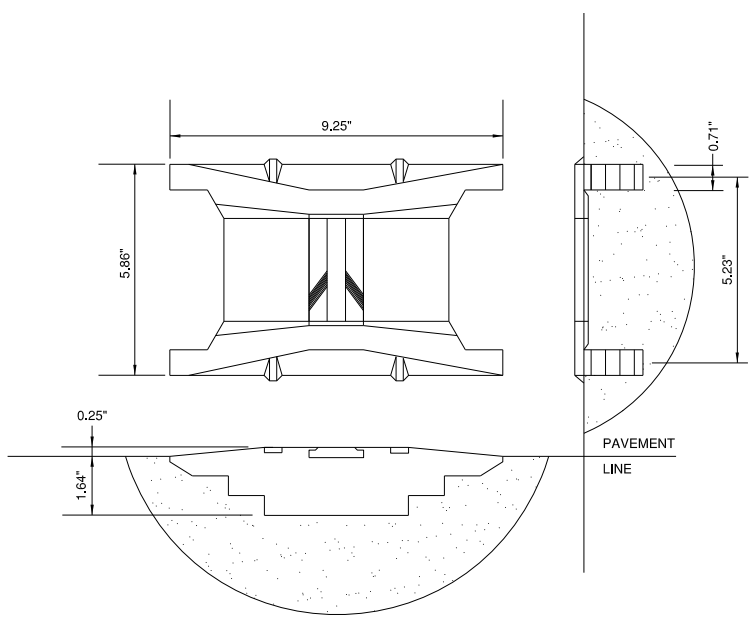
TAXIWAY EDGE MARKINGS DASHED
NOT TO SCALE

ALL TAXIWAY DOUBLE EDGE STRIPES SHALL HAVE A 6" WIDE BLACK STRIPE ADJACENT TO AND BETWEEN STRIPES.

BLACK BORDER SHALL NOT REQUIRE REFLECTIVE MEDIA

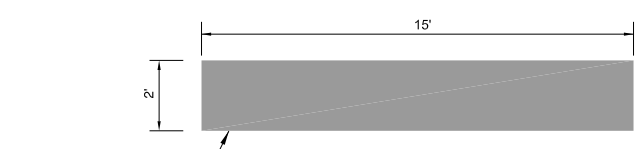


AIRSIDE SERVICE ROAD MARKINGS
NOT TO SCALE



SEMIFLUSH RETROREFLECTIVE MARKER
NOT TO SCALE

MARKER SHALL BE STIMSONITE MODEL 96LP TWO-WAY GREEN OR APPROVED EQUAL.



SERVICE ROAD STOP BAR
NOT TO SCALE



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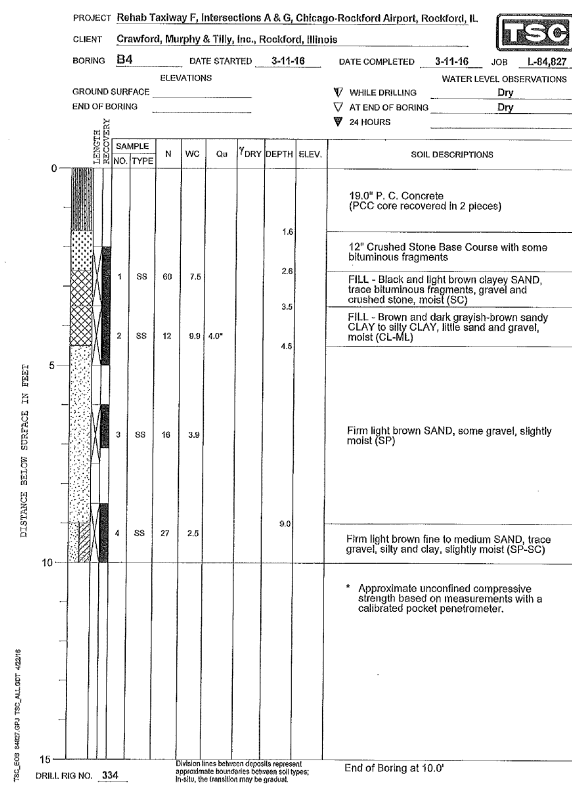
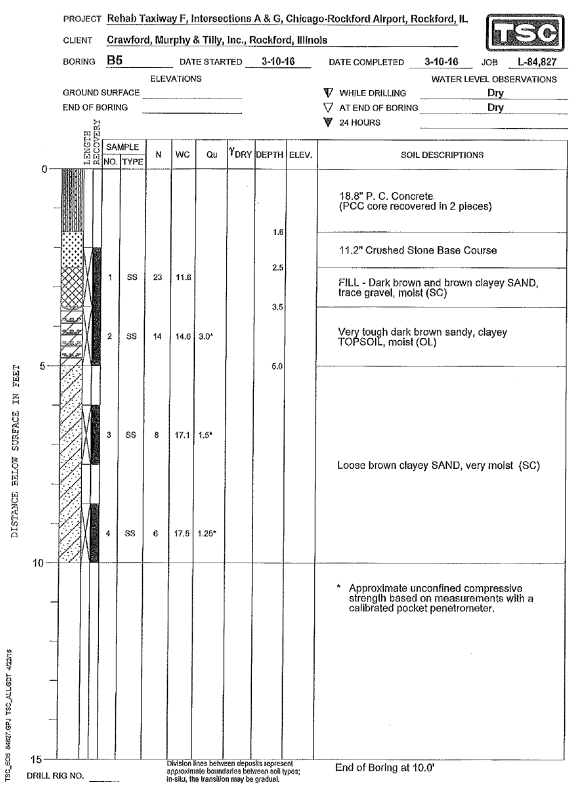
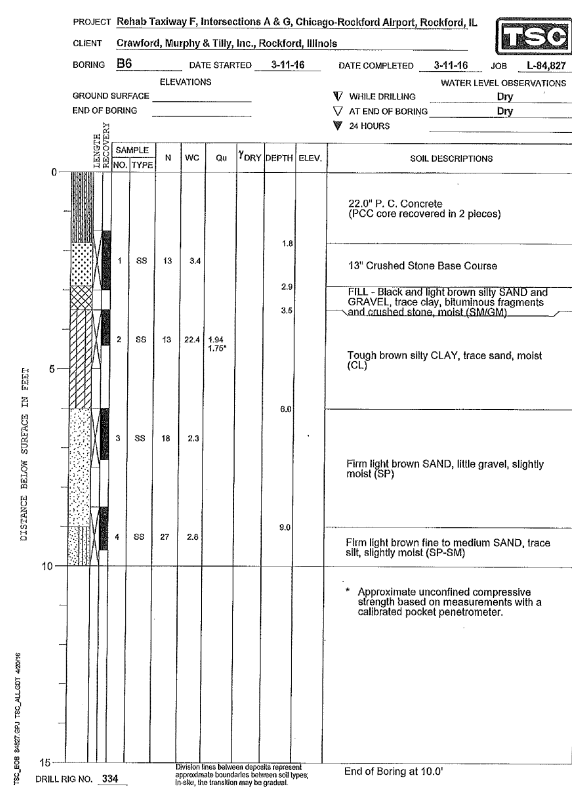
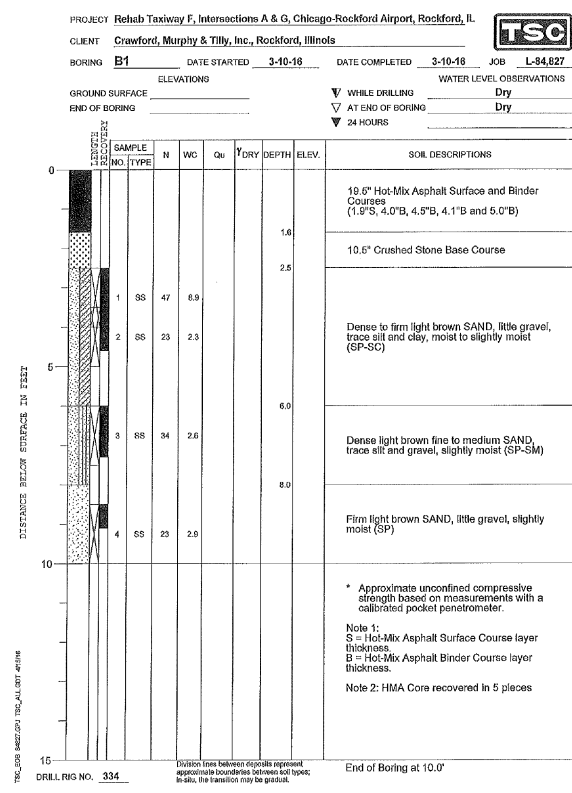
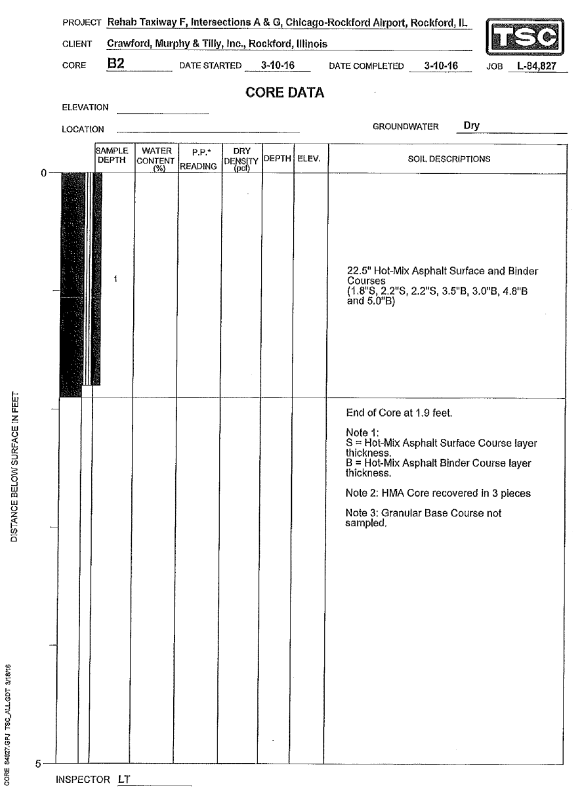
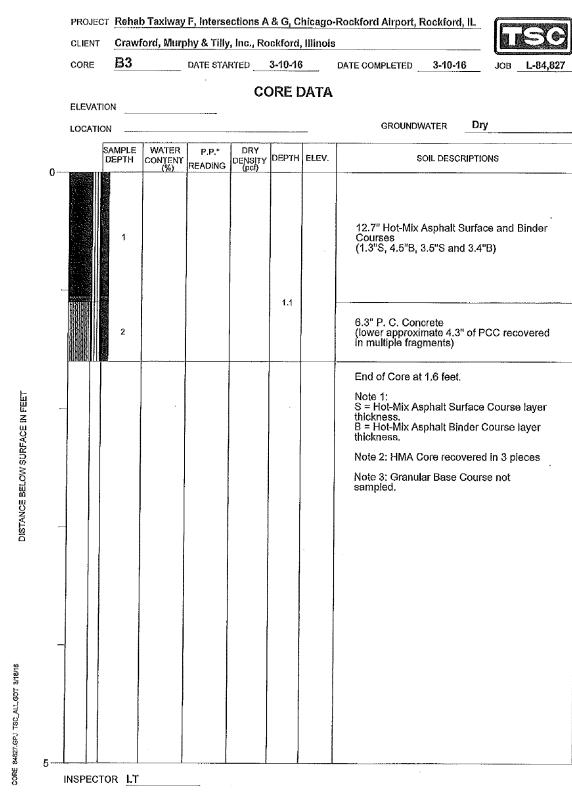
REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE THE INTERSECTION OF
TAXIWAY F AND A;
REHAB. TAXIWAY F AND G

SHEET TITLE:
ENGINEERING INFORMATION

DESIGNED:	DRAWN:	CHECKED:
ABM	JRO	DLP
PROJECT NO.:	15258-05	
DATE:	06/03/2016	
SHEET NO.	REVISION	
26	OF 26	



SEE SITE PLAN FOR BORING LOCATIONS