

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3824	09-00173-00-RS	DUPAGE	17	1

CONTRACT NO. 63377

**INDEX OF SHEETS**

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
**PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY PROJECT**

**FAU ROUTE 3824 (VALLETTE STREET)  
FAU 2672 (SPRING ROAD) TO (POPLAR AVENUE) FAU 3823  
RESURFACING  
SECTION 09-00173-00-RS  
PROJECT ARA-9003(411)  
DUPAGE COUNTY  
JOB NO. C-91-796-09**

**TRAFFIC DATA**

ADT:  
4,200 VPD (2008)      5,000 VPD (2030)

**POSTED SPEED**

25 MPH (EXISTING)      30 MPH (EXISTING)  
25 MPH (PROPOSED)      30 MPH (PROPOSED)

DESIGN DESIGNATION: 4200 (2010) COLLECTOR 0.12 (RS)

PROJECT IS LOCATED IN THE CITY OF ELMHURST

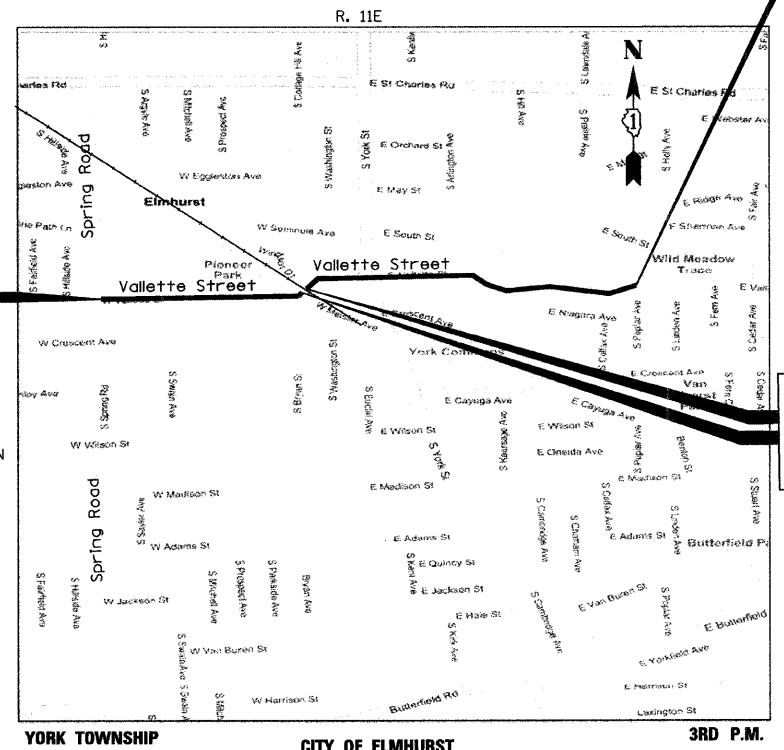
**DESCRIPTION OF IMPROVEMENT**

THIS IMPROVEMENT CONSISTS OF A MILL AND OVERLAY, REMOVING AND REPLACING DETERIORATED PORTIONS OF CURB AND GUTTER AND SIDEWALKS, PAVEMENT PATCHING, STRUCTURE ADJUSTMENTS OR RECONSTRUCTION, STRIPING, AND ALL INCIDENTAL AND COLLATERAL WORK AS NECESSARY TO COMPLETE THE IMPROVEMENT SHOWN HEREIN AND AS DESCRIBED IN THE SPECIFICATIONS.

PROJECT ARA-9003(411)  
PROJECT BEGINS VALLETTE STREET STATION 500+36

PROJECT ARA-9003(411)  
PROJECT ENDS VALLETTE STREET STATION 555+09

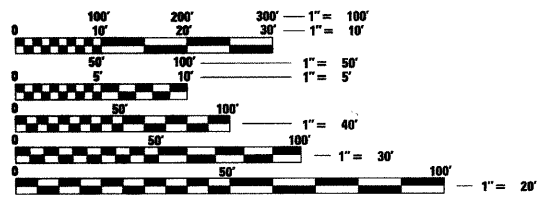
PROJECT OMISSION VALLETTE STREET STATION 521+10 TO STATION 521+30



**LOCATION MAP**

NOT TO SCALE

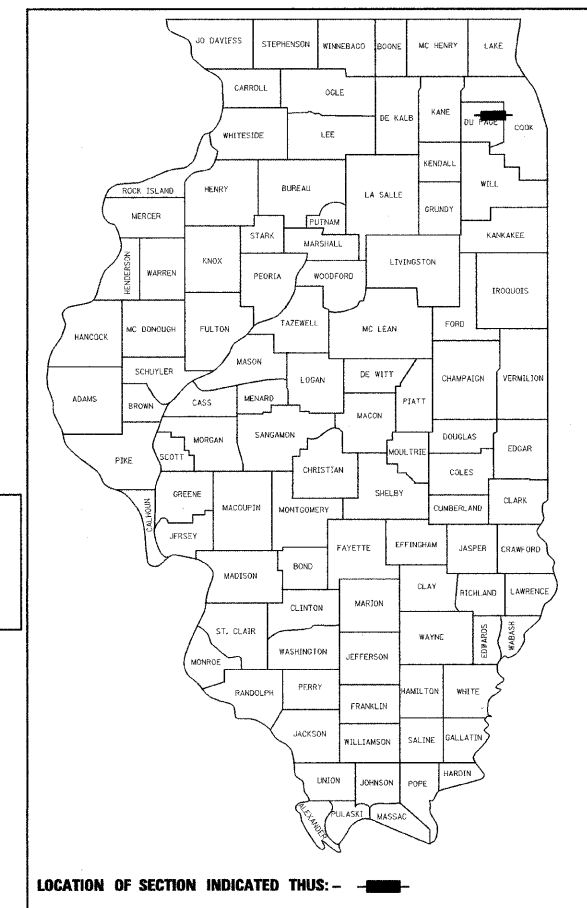
PROJECT GROSS LENGTH  
VALLETTE STREET 5,473 FT (1.04 MILES) (GROSS)  
OMISSION = 20 FT (0.004 MI)  
PROJECT NET LENGTH = 5,453 FT (1.03 MILES) (NET)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
DR 811

CONTRACT NO. 63377

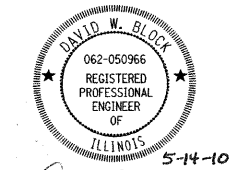


STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED 5-10-10  
*Cow Tibeni*  
CITY OF ELMHURST, CITY ENGINEER

PASSED MAY 27, 2010  
*Cherise H. Butler*  
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW MAY 27, 2010  
*Diane M. O'Keefe*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER



DAVID W. BLOCK, P.E.  
NO. 062-050966  
EXP. DATE 11/30/11

**PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS**

IDOT FIELD ENGINEER MARILYN SOLOMON 847/705-4407

1051 PERIMETER DRIVE, SUITE 1025 SCHAMBOURG, ILLINOIS 60173 (847) 605-9600



**GENERAL NOTES**

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2007.
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
- BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1 AND THE CITY OF ELMHURST AT 630-530-3777 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR CITY PROPERTY OR ROW WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT OR CITY.
- SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
- HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL VERIFY FINAL PATCH LOCATIONS IN THE FIELD, PRIOR TO REMOVAL.

11. THE CONTRACTOR SHALL COORDINATE PAVING OPERATIONS FOR BOTH HMA LEVEL BINDER AND SURFACE COURSES SO THAT THE LONGITUDINAL JOINTS ARE CLOSED AND COMPACTED AT THE END OF EACH DAY. PAVING OPERATIONS SHALL BE SCHEDULED SO THAT ADJACENT LANES ARE OPENED IN THE SAME DIRECTION AS THE INITIAL LANE MINIMIZING THE TIME THE EDGE OF A PAVEMENT MAT IS ALLOWED TO COOL. IF THE CONTRACTOR ELECTS TO INSTALL A ONE-WAY DAILY DETOUR ROUTE FOR CONVENIENCE TO MINIMIZE TRAFFIC IN THE CONSTRUCTION ZONE AND ACROSS THE PAVEMENT MAT EDGE, THE ONE-WAY DAILY DETOUR ROUTE SHALL BE COORDINATED WITH AND APPROVED BY THE ENGINEER PRIOR TO INSTALLATION. THE COST TO INSTALL AND MAINTAIN THE DAILY ONE-WAY DETOUR ROUTE SHALL BE AT THE CONTRACTOR'S OWN EXPENSE.

**STORM SEWERS, WATER MAINS, AND UTILITIES**

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
- THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN IF NOT SHOWN ON THE PLANS. ALL UTILITY PROPERTY DAMAGE DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.
- ALL UTILITY OWNERS SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
- THE CONTRACTOR SHALL ENSURE ALL WATER SYSTEM VALVES, VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.

7. ALL LOOSE MATERIAL DEPOSITED IN THE FLOWLINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT IN ACCORDANCE WITH ARTICLE 107.15.

8. CATCH BASINS, MANHOLES AND INLETS CONSTRUCTED IN A LOCATION WHERE AN EXISTING STRUCTURE WAS REMOVED SHALL INCLUDE UP TO FIVE FEET OF PIPE TO CONNECT EACH EXISTING PIPE. THE NECESSARY PIPE BEYOND FIVE FEET WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "STORM" SEWER" AND OF THE TYPE AND SIZE REQUIRED.

THE CONTRACTOR SHALL CONFIRM ALL EXISTING STORM SEWER PIPE SIZES AND INVERTS PRIOR TO ORDERING STRUCTURES. ANY MODIFICATION OF STRUCTURES DUE TO THE FAILURE OF THE CONTRACTOR TO PERFORM THIS TASK SHALL BE AT THE CONTRACTOR'S EXPENSE AND MAY LEAD TO THE REJECTION OF THE STRUCTURE IN THE FIELD.

**SIGNING AND STRIPING**

- SEE IDOT STANDARD DETAIL 780001, DISTRICT ONE DETAIL TC-13 AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.
- THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL SIGNS THAT INTERFERE WITH CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING AND MUST BE REMOVED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT IN ACCORDANCE WITH ARTICLE 107.25.

**TRAFFIC CONTROL**

- SEE TRAFFIC CONTROL HIGHWAY STANDARDS CONCERNING TRAFFIC CONTROL AND PROTECTION.
- THE CONTRACTOR SHALL SCHEDULE CONSTRUCTION ACTIVITIES SO THAT THERE ARE ALWAYS TWO LANES OF TRAFFIC OPEN AT THE END OF EACH DAY. IF THE CONTRACTOR ELECTS TO INSTALL A DETOUR FOR CONVENIENCE, IT WILL ONLY BE ALLOWED UPON SUBMITTAL OF A DETOUR PLAN AND APPROVAL BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE AT THE CONTRACTOR'S OWN EXPENSE.

**MISCELLANEOUS**

- MATERIALS RESULTING FROM THE REMOVAL OF CONCRETE SURFACES, UTILITY STRUCTURE ADJUSTMENT, RESTORATION WORK, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGMENT OF THE ENGINEER, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE ENGINEER WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR SHALL BE BILLED (CHARGED) ACCORDINGLY.
- THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS, OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS/HER YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO USE OF THE WATER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. WITHIN 24 HOURS OF THE LAYING OF CURB OR SIDEWALK, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS AND ALL LOOSE MATERIAL.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE CITY WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO SIDEWALK REPLACEMENT AND/OR CURB AND GUTTER REPLACEMENT. AT LOCATIONS WHERE THE SIDEWALK OR CURB AND GUTTER IS SCHEDULED TO BE REMOVED, THE CONTRACTOR SHALL CONTACT THE BUSINESS/HOMEOWNER 24 HOURS PRIOR TO REMOVING THE CURB OR SIDEWALK. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES. THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE A DRIVEWAY FOR MORE THAN 48 HOURS UNDER ANY CIRCUMSTANCE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE BARRICADES TO PREVENT TRAFFIC FROM USING THE DRIVEWAYS DURING THIS PERIOD.
- IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO COORDINATE WITH THE CANADIAN NATIONAL RAILROAD WHENEVER CONSTRUCTION ACTIVITY IS WITHIN 25 FEET OF THE RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY THE CANADIAN NATIONAL RAILROAD TO MONITOR ON-COMING TRAIN TRAFFIC, AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD RIGHT OF WAY MAY PROCEED. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 109.05.
- WHEN REMOVING PAVEMENT, CURB AND GUTTER, SHOULDER, AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES AND BUILDING FOUNDATIONS WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.

STD. NO.
000001-05
424001-05
442201-03
602001-01
602011-01
602301-02
604001-03
604036-02
604056-03
606001-04
701301-03
701311-03
701501-05
701701-06
701801-04
701901-01
780001-02
886001-01
886006-01

**HIGHWAY STANDARDS**

DESCRIPTION  
 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS  
 CURB RAMPS FOR SIDEWALKS  
 CLASS C AND D PATCHES  
 CATCH BASIN TYPE A  
 CATCH BASIN TYPE C  
 INLET - TYPE A  
 FRAME AND LIDS TYPE 1  
 GRATE TYPE B  
 FRAME AND GRATE TYPE 11V  
 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER  
 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS  
 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY  
 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED  
 URBAN LANE CLOSURE, MULTILANE INTERSECTION  
 LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE  
 TRAFFIC CONTROL DEVICES  
 TYPICAL PAVEMENT MARKINGS  
 DETECTOR LOOP INSTALLATIONS  
 TYPICAL LAYOUTS FOR DETECTION LOOPS

**SUMMARY OF QUANTITIES**

#	CODE NO	DESCRIPTION	PAY ITEM	UNIT	QUANTITY	1000-2A ROADWAY 100% ARRA	1000-2A ROADWAY 100% LA
	20101400	NITROGEN FERTILIZER NUTRIENT		POUND	14	14	
	20101500	PHOSPHORUS FERTILIZER NUTRIENT		POUND	14	14	
	20101600	POTASSIUM FERTILIZER NUTRIENT		POUND	14	14	
	20101700	SUPPLEMENTAL WATERING		UNIT	17	17	
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL		CY YD	322	322	
	20700420	POROUS GRANULAR EMBANKMENT, SUBGRADE		CY YD	319	319	
	20800150	TRENCH BACKFILL		CY YD	5	5	
	21101625	TOPSOIL FURNISH AND PLACE, 6"		SQ YD	1,475	1,475	
	25200110	SODDING, SALT TOLERANT		SQ YD	1,475	1,475	
	31101200	SUB-BASE GRANULAR MATERIAL, TYPE B 4"		SQ YD	2,862	2,862	
	35300400	PORTLAND CEMENT CONCRETE BASE COURSE 9"		SQ YD	482	482	
	40600100	BITUMINOUS MATERIALS (PRIME COAT)		GALLON	3,549	3,549	
	40600300	AGGREGATE (PRIME COAT)		TON	71	71	
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANSEWAYS		TON	5	5	
	40600635	LEVELING BINDER (MACHINE METHOD, N70)		TON	1,094	1,094	
	40600885	CONSTRUCTING TEST STRIP		EACH	2	2	
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT		SQ YD	300	300	
	40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT		SQ YD	52	52	
	40600990	TEMPORARY RAMP		SQ YD	300	300	
	40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "0", N70		TON	1,987	1,987	
	42001300	PROTECTIVE COAT		SQ YD	2,978	2,978	
	42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH		SQ YD	262	262	
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH		SQ FT	11,877	11,877	
	42400800	DETECTABLE WARNINGS		SQ FT	972	972	
	44001000	PAVEMENT REMOVAL		SQ YD	482	482	
	44001061	HOT-MIX ASPHALT SURFACE REMOVAL, 3"		SQ YD	11,976	11,976	
	44001098	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH		SQ YD	645	645	
	44002000	DRIVEWAY PAVEMENT REMOVAL		SQ YD	306	306	
	44002050	COMBINATION CURB AND GUTTER REMOVAL		FOOT	4,305	4,305	
	44002060	SIDEWALK REMOVAL		SQ FT	12,849	12,849	
	44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT		FOOT	1,043	1,043	
	44201749	CLASS D PATCHES, TYPE I, 9 INCH		SQ YD	392	392	
	44201753	CLASS D PATCHES, TYPE II, 9 INCH		SQ YD	392	392	
	44201757	CLASS D PATCHES, TYPE III, 9 INCH		SQ YD	392	392	
	44201759	CLASS D PATCHES, TYPE IV, 9 INCH		SQ YD	392	392	
	44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT		FOOT	6,373	6,373	
	55080020	STORM SEWERS, CLASS B, TYPE I 6"		FOOT	15	15	
	60201110	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 11V FRAME AND GRATE		EACH	2	2	
	60236200	INLETS, TYPE A, TYPE B GRATE		EACH	1	1	
	60266600	VALVE BOXES TO BE ADJUSTED		EACH	11	11	
	60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)		EACH	69	69	
	60404805	FRAMES AND GRATES, TYPE 11V		EACH	2	2	
	60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID		EACH	8	8	
	60500050	REMOVING CATCH BASINS		EACH	1	1	
	60500060	REMOVING INLETS		EACH	2	2	
	60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-612		FOOT	4,305	4,305	
	67100100	MOBILIZATION		L SUM	1	1	
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501		L SUM	1	1	
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701		L SUM	1	1	
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801		L SUM	1	1	
	70300100	SHORT-TERM PAVEMENT MARKING		FOOT	188	188	
	70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS		SQ FT	391	391	
	70300220	TEMPORARY PAVEMENT MARKING - LINE 4"		FOOT	4,184	4,184	
	70300240	TEMPORARY PAVEMENT MARKING - LINE 6"		FOOT	2,704	2,704	
	70300260	TEMPORARY PAVEMENT MARKING - LINE 12"		FOOT	1,188	1,188	
	70300280	TEMPORARY PAVEMENT MARKING - LINE 24"		FOOT	546	546	
	70301000	WORK ZONE PAVEMENT MARKING REMOVAL		SQ FT	186	186	
	78005100	EPOXY PAVEMENT MARKING - LETTERS AND SYMBOLS		SQ FT	196	196	
	78005110	EPOXY PAVEMENT MARKING - LINE 4"		FOOT	1,046	1,046	
	78005130	EPOXY PAVEMENT MARKING - LINE 6"		FOOT	1,172	1,172	
	78005150	EPOXY PAVEMENT MARKING - LINE 12"		FOOT	198	198	
	78005180	EPOXY PAVEMENT MARKING - LINE 24"		FOOT	273	273	
	78300100	PAVEMENT MARKING REMOVAL		SQ FT	256	256	
	88600600	DETECTOR LOOP REPLACEMENT		FOOT	322	322	
	X0322256	TEMPORARY INFORMATION SIGNING		SQ FT	228	228	
	X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)		SQ YD	3,057	3,057	
	X7030048	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS		SQ FT	196	196	
	X7030070	GROOVING FOR RECESSED PAVEMENT MARKING 3"		FOOT	1,046	1,046	
	X7030074	GROOVING FOR RECESSED PAVEMENT MARKING 7"		FOOT	1,172	1,172	
	X7030078	GROOVING FOR RECESSED PAVEMENT MARKING 13"		FOOT	198	198	
	X7030080	GROOVING FOR RECESSED PAVEMENT MARKING 25"		FOOT	273	273	
	Z0004822	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6"		SQ YD	44	44	
	Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED		EACH	29	29	
	Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE		L SUM	1	1	
	Z0076600	TRAINERS		HR	500	500	

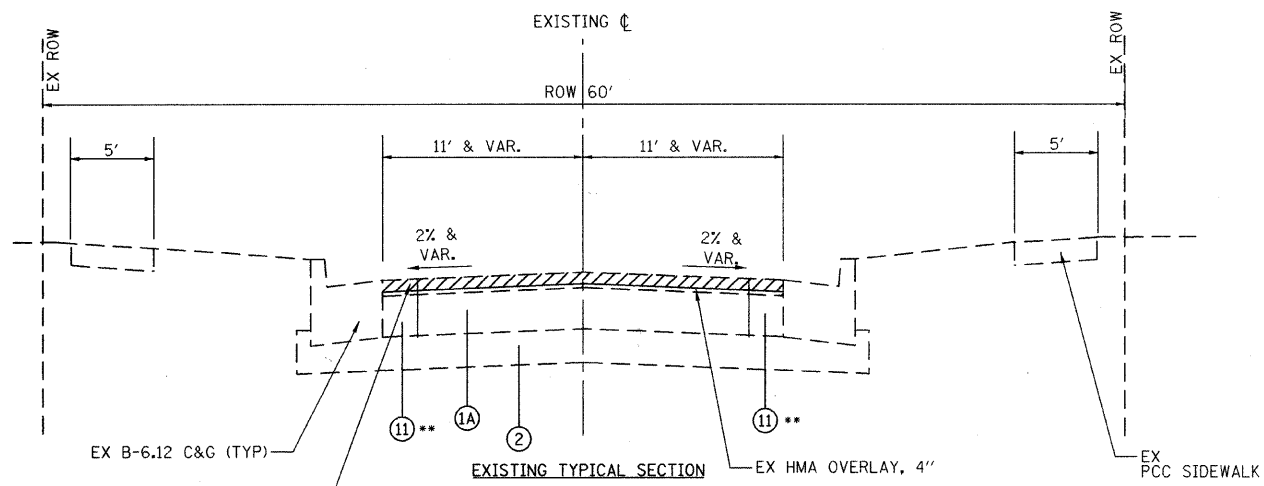
\* SPECIALTY ITEM  
 Δ Y080

FILE NAME =	USER NAME = USER	DESIGNED - CEC	REVISED -
g:\CH09\00822\Road\Sheets\G-102-GenNotes.sht		DRAWN - JKM	REVISED -
	PLOT SCALE = 63,9996 ft / IN.	CHECKED - DWB	REVISED -
	PLOT DATE = 5/20/2010	DATE - 05/20/2010	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

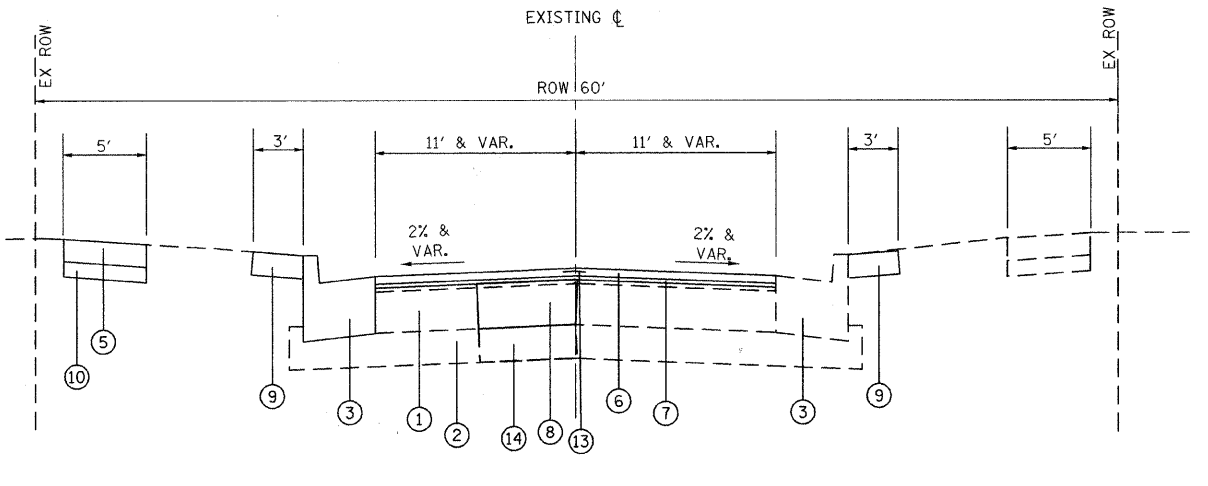
**VALLETTE STREET  
 GENERAL NOTES, HIGHWAY STANDARDS,  
 AND SUMMARY OF QUANTITIES**

NOT TO SCALE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	F.A.U. RTE. 3824	SECTION 09-00173-00-RS	COUNTY DUPAGE	TOTAL SHEETS 17	SHEET NO. 2
				CONTRACT NO. 63377				
				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

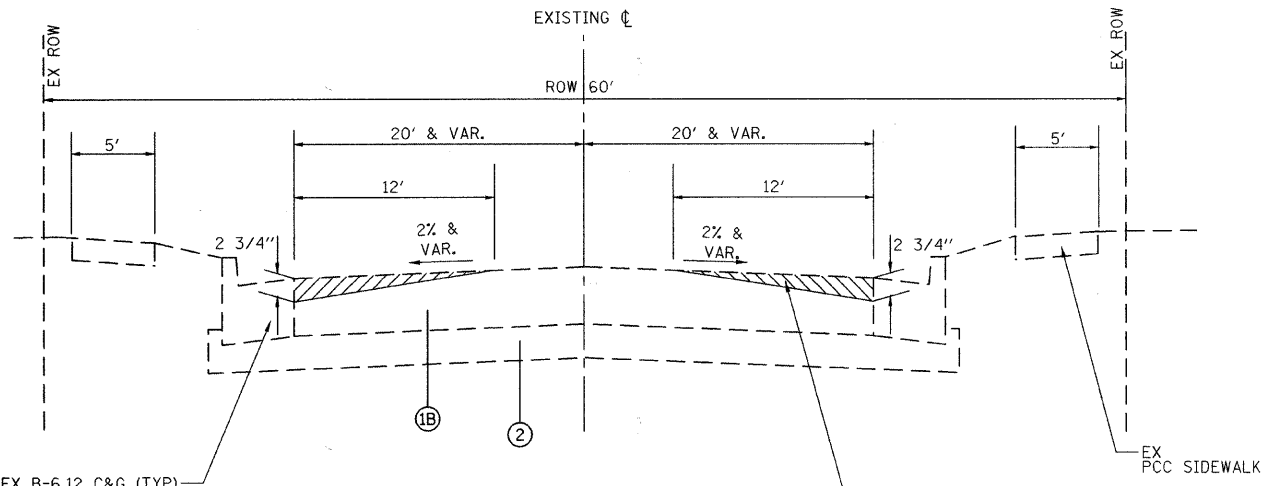


\* NOTE: HMA SURFACE REMOVAL, VARIABLE DEPTH (3"-5") FROM STA. 537+45 TO 537+76 RT, 539+12 TO 539+22 LT, 540+23 TO 541+44 LT, 542+32 TO 544+14 LT, 543+00 TO 544+43 RT, 549+96 TO 550+46 RT (FOR PROFILE CORRECTION)

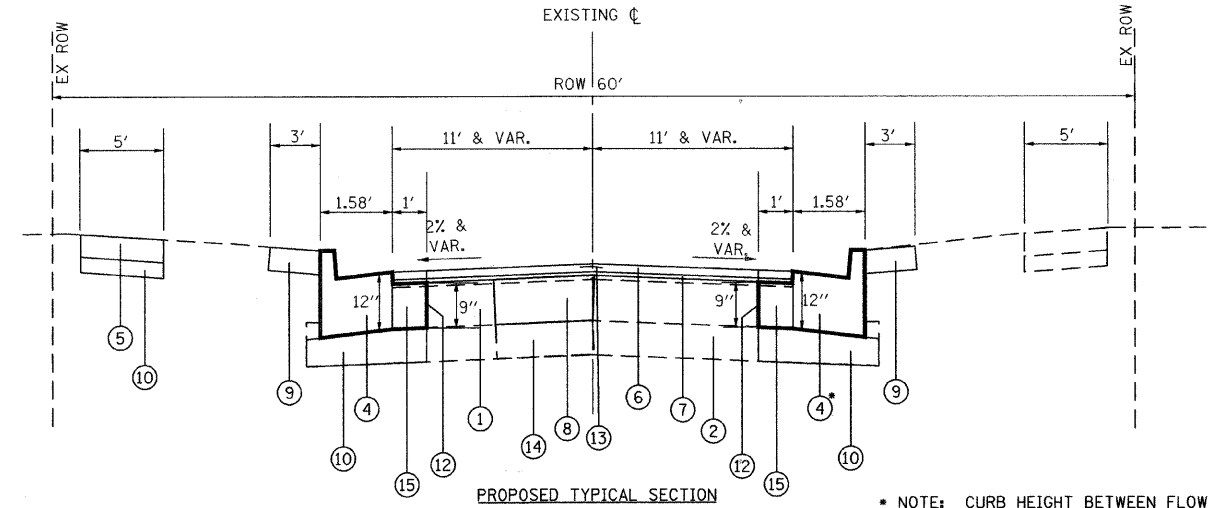
\*\* NOTE: PAVEMENT REMOVAL IS FROM STATION 535+38 TO STATION 555+09



PROPOSED TYPICAL SECTION VALLETTE STREET STATION 500+36 TO STATION 535+38 PROJECT OMISSION ON VALLETTE STREET FROM STATION 521+10 TO STATION 521+30



EXISTING TYPICAL SECTION VALLETTE STREET STATION 522+41 TO STATION 532+75



PROPOSED TYPICAL SECTION VALLETTE STREET STATION 535+38 TO STATION 555+09

\* NOTE: CURB HEIGHT BETWEEN FLOW LINE AND TOP OF CURB SHALL BE 4" STATION 542+05 TO STATION 542+54, RT TO PREVENT PARKWAY FROM HOLDING WATER (PAID FOR AS COMB CONC C&G, TYPE B-6.12)

**LEGEND**

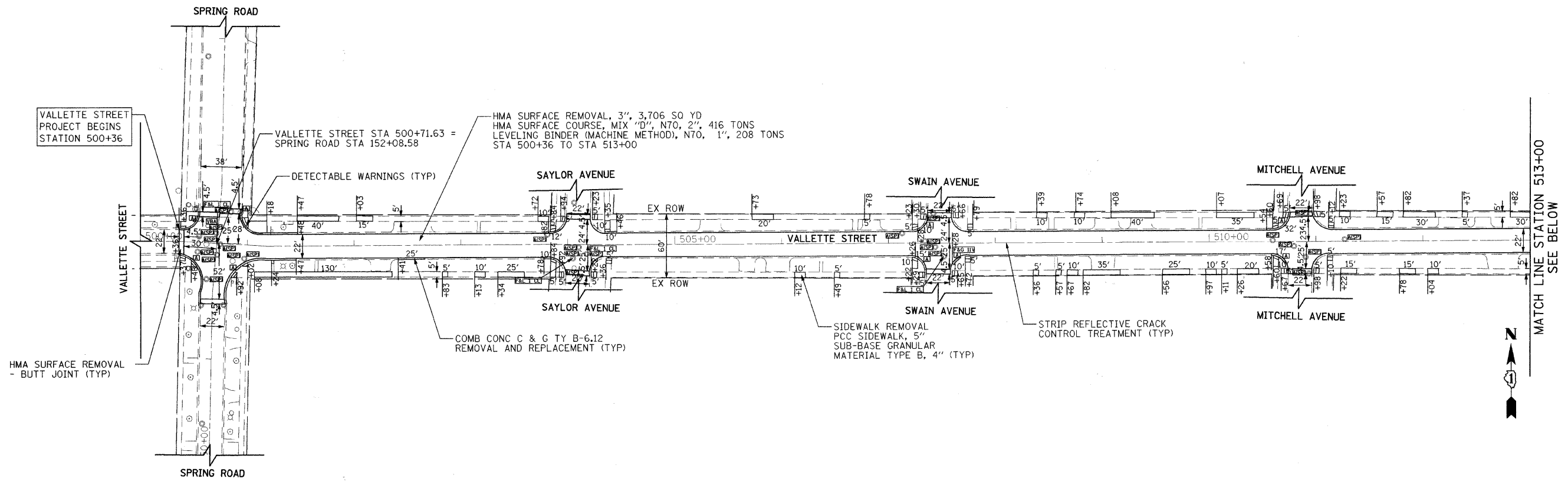
- (1A) EXISTING HMA PAVEMENT, 12"
- (1B) EXISTING PCC PAVEMENT, 8"
- (2) EXISTING SUBBASE GRAN. MAT'L., CA-6
- (3) COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12 REMOVAL AND REPLACEMENT, REMOVE AND REPLACE AS DIRECTED BY ENGINEER
- (4) COMBINATION CURB & GUTTER REMOVAL (REMOVAL AND DISPOSAL OF THE MATERIAL UNDER THE PROPOSED CURB AND GUTTER LOCATION IN ORDER TO INSTALL THE PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B 4" UNDER THE PROPOSED CURB AND GUTTER SHALL BE INCLUDED IN THIS ITEM) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12 (SEE \* NOTE)
- (5) SIDEWALK REMOVAL AND PCC SIDEWALK 5", REMOVE AND REPLACE AS DIRECTED BY ENGINEER
- (6) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (7) LEVELING BINDER (MACHINE METHOD), N70, 1"
- (8) CLASS D PATCHES, 9", AS DIRECTED BY THE ENGINEER
- (9) SODDING, SALT TOLERANT TOPSOIL FURNISH AND PLACE, 6"
- (10) SUB-BASE GRANULAR MATERIAL, TYPE B 4"
- (11) PAVEMENT REMOVAL
- (12) SAWCUT FULL DEPTH OF EXISTING PAVEMENT (INCLUDED IN THE COST OF PAVEMENT REMOVAL)
- (13) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (14) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SUBGRADE AS DIRECTED BY THE ENGINEER
- (15) PORTLAND CEMENT CONCRETE BASE COURSE 9" (POURED MONOLITHICALLY WITH THE COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12)

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

THE CONTRACTOR SHALL MILL BEFORE PATCHING.

MIXTURE TYPE	AIR VOIDS @ Ndes
HMA SURFACE COURSE, MIX "D", N70 (IL 9.5mm), 2"	4% @ 70 GYRATIONS
LEVELING BINDER (MACHINE METHOD), N70, 1"	4% @ 70 GYRATIONS
CLASS D PATCHES (HMA BINDER IL-19 MM)	4% @ 70 GYRATIONS
<b>STABILIZED DRIVEWAYS 6"</b>	
HMA SURFACE COURSE, MIX "C", N50 (IL 9.5mm) 2"	4% @ 50 GYRATIONS
HMA BASE COURSE (HMA BINDER IL - 19mm) 4"	4% @ 50 GYRATIONS

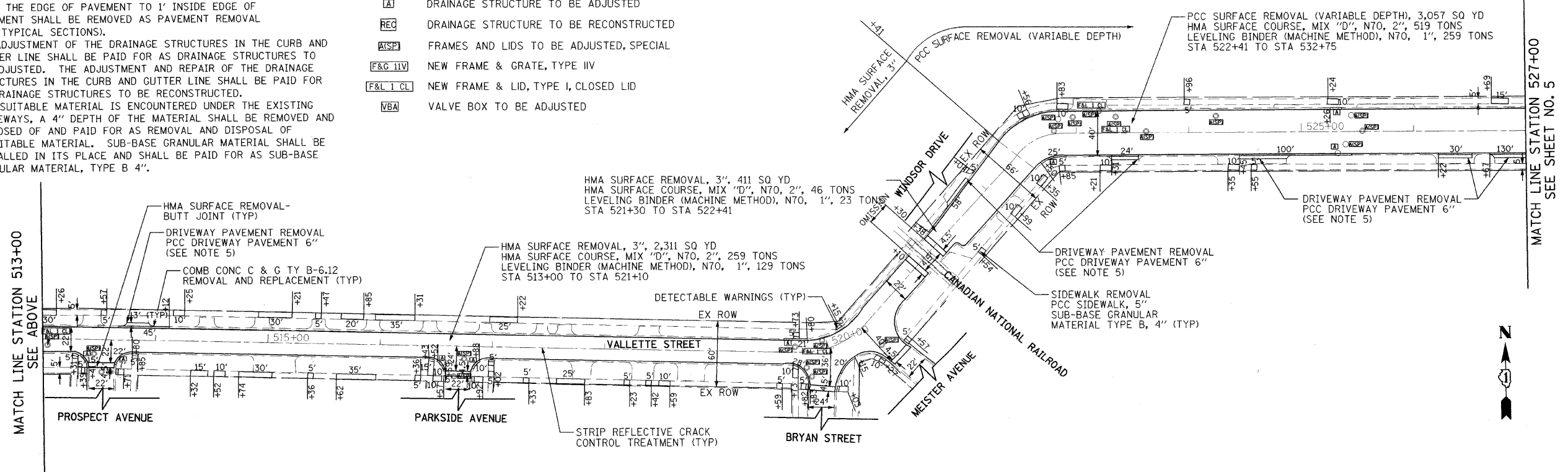
NOTES: 1) THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.  
2) THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.



- NOTES:
- 1) CLASS D PATCHES, 9" AS DIRECTED BY ENGINEER
  - 2) INSTALL STRIP REFLECTIVE CRACK CONTROL TREATMENT ALONG EXISTING LONGITUDINAL PAVEMENT JOINTS
  - 3) NW, NE, SW, AND SE CORNERS OF SPRING ROAD AND VALLETTE STREET, FROM THE EDGE OF PAVEMENT TO 1' INSIDE EDGE OF PAVEMENT SHALL BE REMOVED AS PAVEMENT REMOVAL (SEE TYPICAL SECTIONS).
  - 4) THE ADJUSTMENT OF THE DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE ADJUSTED. THE ADJUSTMENT AND REPAIR OF THE DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE RECONSTRUCTED.
  - 5) IF UNSUITABLE MATERIAL IS ENCOUNTERED UNDER THE EXISTING DRIVEWAYS, A 4" DEPTH OF THE MATERIAL SHALL BE REMOVED AND DISPOSED OF AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL. SUB-BASE GRANULAR MATERIAL SHALL BE INSTALLED IN ITS PLACE AND SHALL BE PAID FOR AS SUB-BASE GRANULAR MATERIAL, TYPE B 4".

LEGEND

[A]	DRAINAGE STRUCTURE TO BE ADJUSTED
[REC]	DRAINAGE STRUCTURE TO BE RECONSTRUCTED
[ASFP]	FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
[F&G IIV]	NEW FRAME & GRATE, TYPE IIV
[F&L I CL]	NEW FRAME & LID, TYPE I, CLOSED LID
[VBA]	VALVE BOX TO BE ADJUSTED



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PLOT DATE = 5/20/2010	

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DRAWN - JKM	REVISED -
CHECKED - DWB	REVISED -
DATE - 05/20/2010	REVISED -

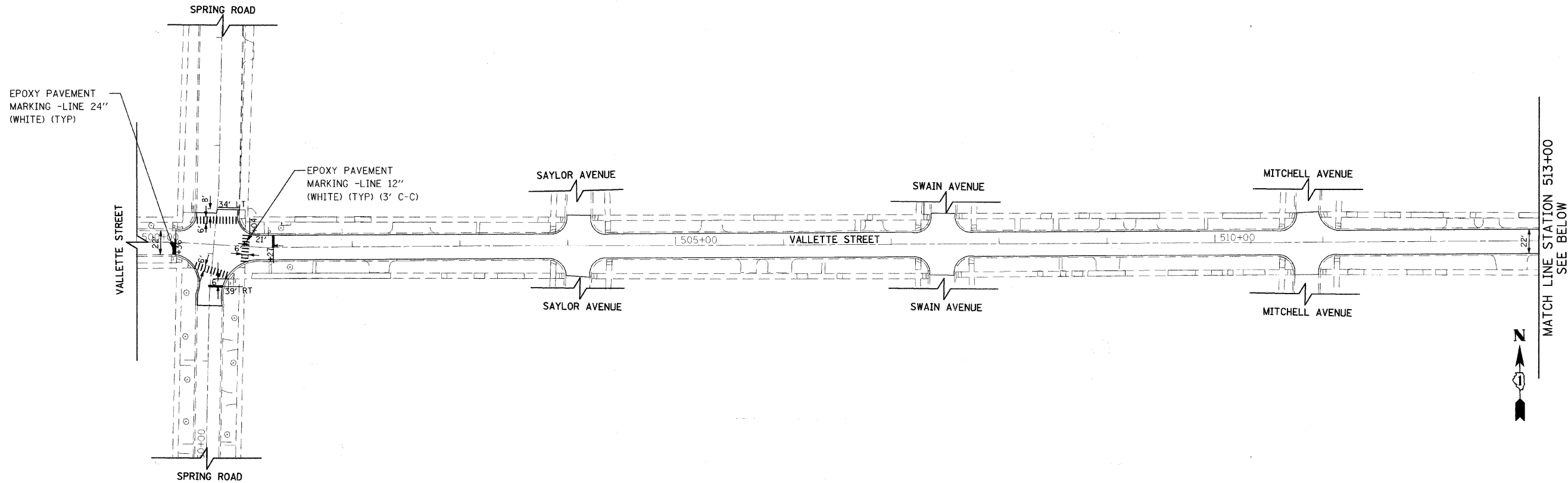
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**VALLETTE STREET  
IMPROVEMENT PLAN**

SCALE: SHEET NO. 1 OF 2 SHEETS STA. 500+36.00 TO STA. 527+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3824	09-00173-00-RS	DUPAGE	17	4
CONTRACT NO. 63377				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

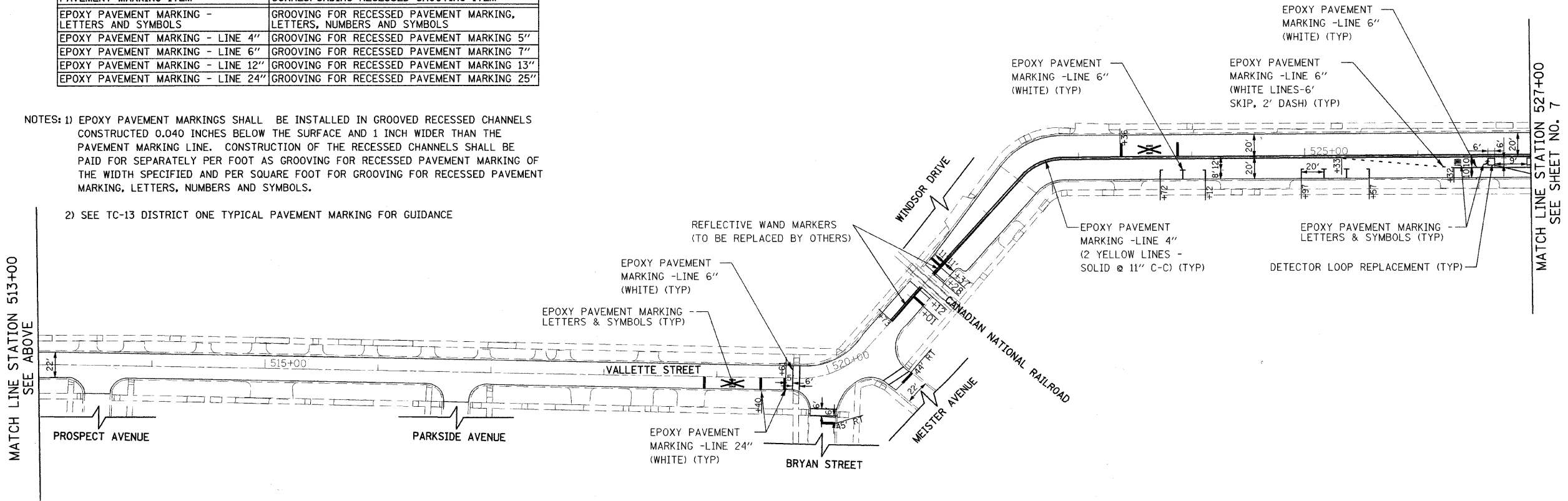




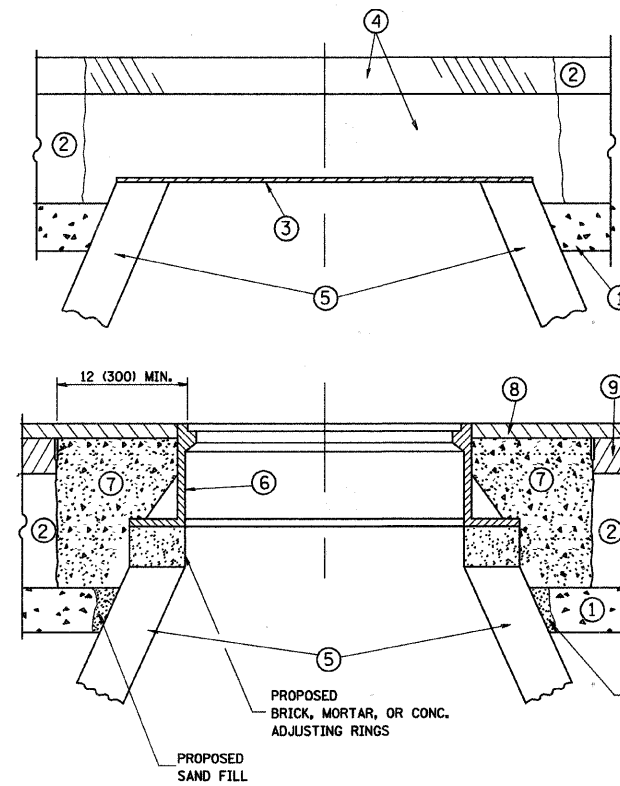
PAVEMENT MARKING ITEM	CORRESPONDING RECESSED GROOVING ITEM
EPOXY PAVEMENT MARKING - LETTERS AND SYMBOLS	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS
EPOXY PAVEMENT MARKING - LINE 4"	GROOVING FOR RECESSED PAVEMENT MARKING 5"
EPOXY PAVEMENT MARKING - LINE 6"	GROOVING FOR RECESSED PAVEMENT MARKING 7"
EPOXY PAVEMENT MARKING - LINE 12"	GROOVING FOR RECESSED PAVEMENT MARKING 13"
EPOXY PAVEMENT MARKING - LINE 24"	GROOVING FOR RECESSED PAVEMENT MARKING 25"

NOTES: 1) EPOXY PAVEMENT MARKINGS SHALL BE INSTALLED IN GROOVED RECESSED CHANNELS CONSTRUCTED 0.040 INCHES BELOW THE SURFACE AND 1 INCH WIDER THAN THE PAVEMENT MARKING LINE. CONSTRUCTION OF THE RECESSED CHANNELS SHALL BE PAID FOR SEPARATELY PER FOOT AS GROOVING FOR RECESSED PAVEMENT MARKING OF THE WIDTH SPECIFIED AND PER SQUARE FOOT FOR GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS.

2) SEE TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKING FOR GUIDANCE







**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"  
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

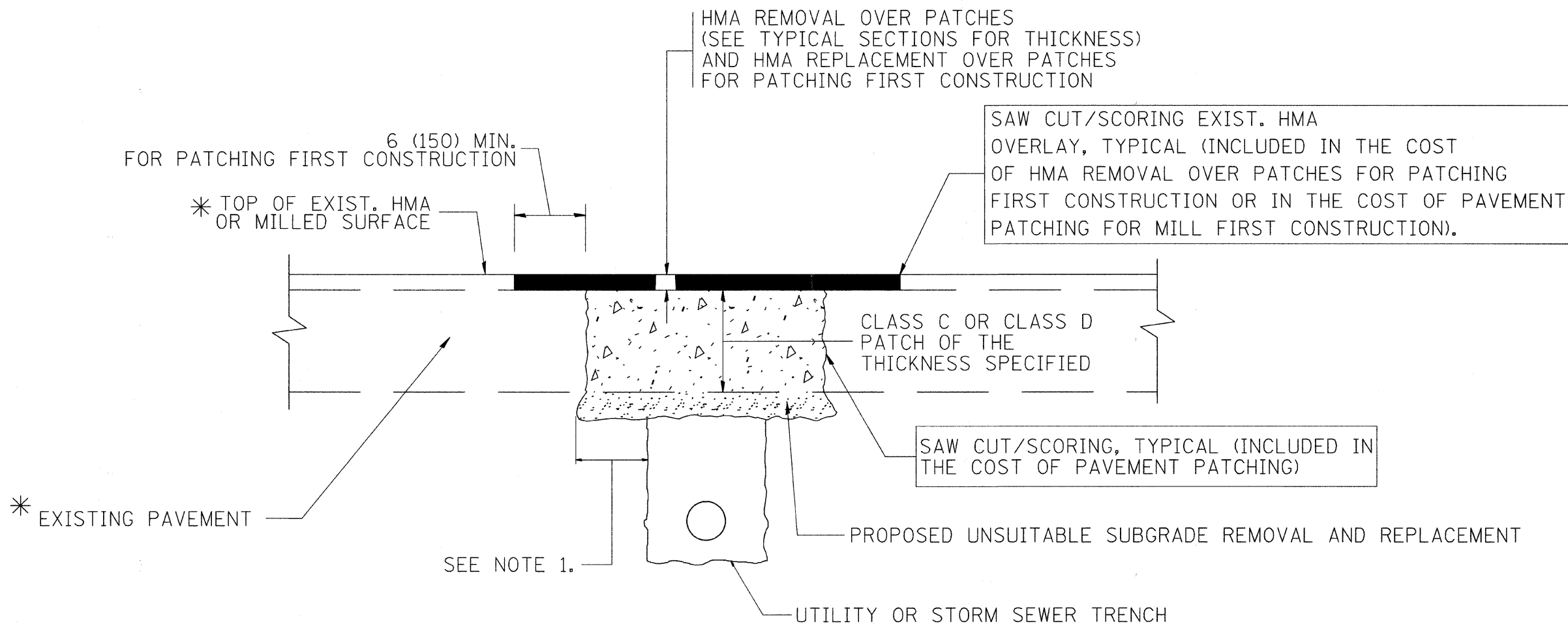
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	PLOT DATE = 1/4/2008	CHECKED -	REVISED - R. WIEDEMAN 05-14-04
		DATE - 10-25-94	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3824	09-00173-00-RS	DUPAGE	17	8
BD600-03 (BD-8)			CONTRACT NO. 63377	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				





\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

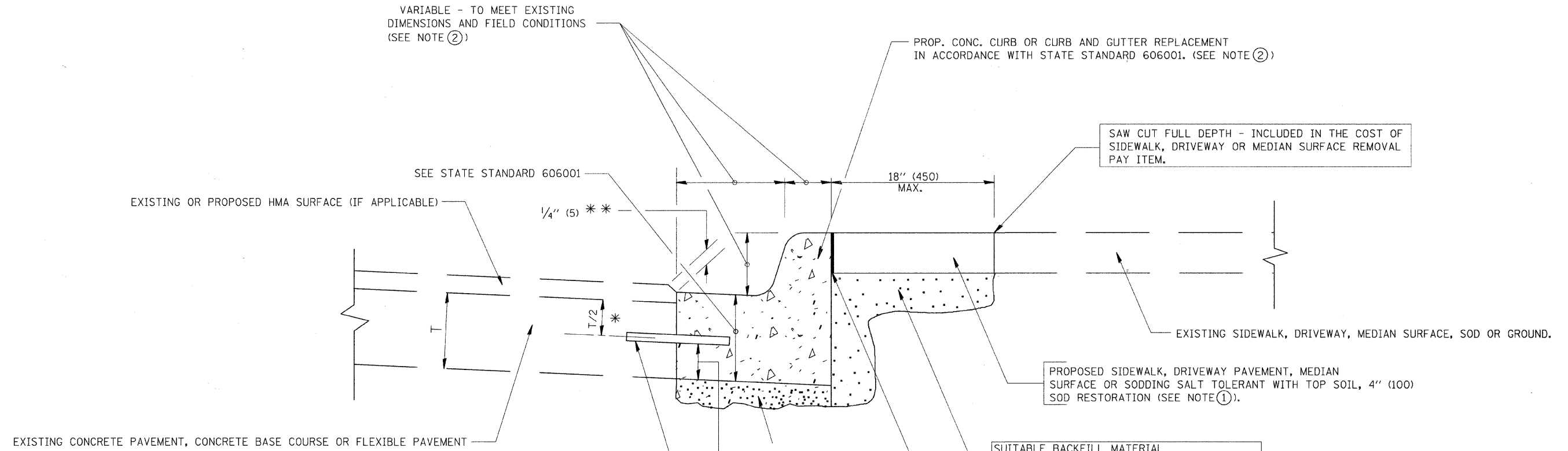
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\dietstd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A. RTE. 3824	SECTION 09-00173-00-RS	COUNTY DUPAGE	TOTAL SHEETS 17	SHEET NO. 9
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	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - R. BORO 09-04-07					<b>BD400-04 (BD-22)</b>			CONTRACT NO. 63377	
			REVISED - K. ENG 10-27-08									



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
  - \* \* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
  - ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
  - ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
  - ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
  - ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

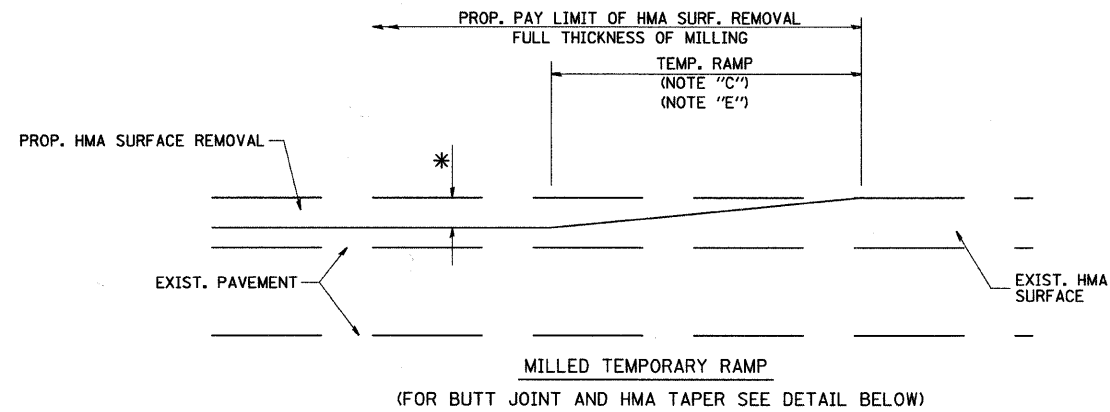
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

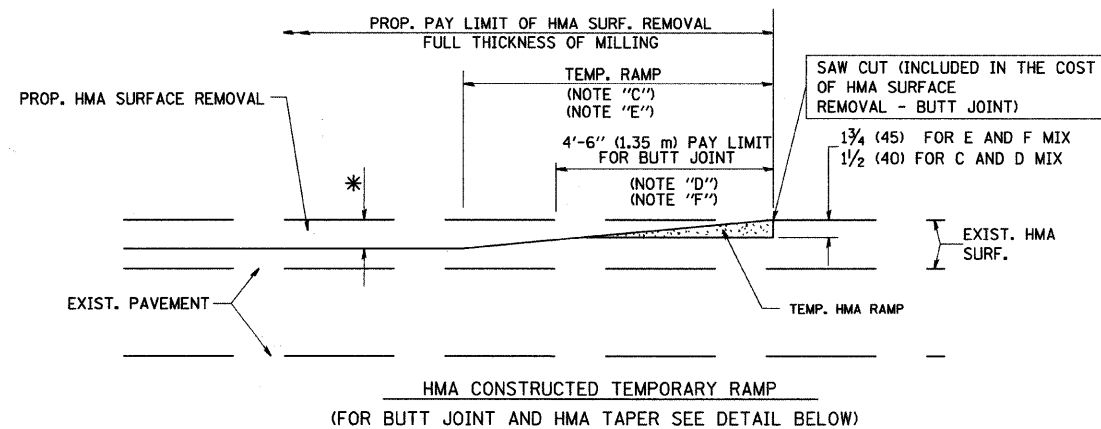
## CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drvakoagn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cd:\pw_work\pwidat\drvakoagn\0108315\bd24.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97			3824	09-00173-00-RS	DUPAGE	17	10
PLOT SCALE = 50,000 / IN.		CHECKED -	REVISED - M. GOMEZ 01-22-01			<b>BD600-06 (BD-24)</b>		CONTRACT NO. 63377		
PLOT DATE = 12/15/2009		DATE - 03-11-94	REVISED - R. BORO 12-15-09	SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

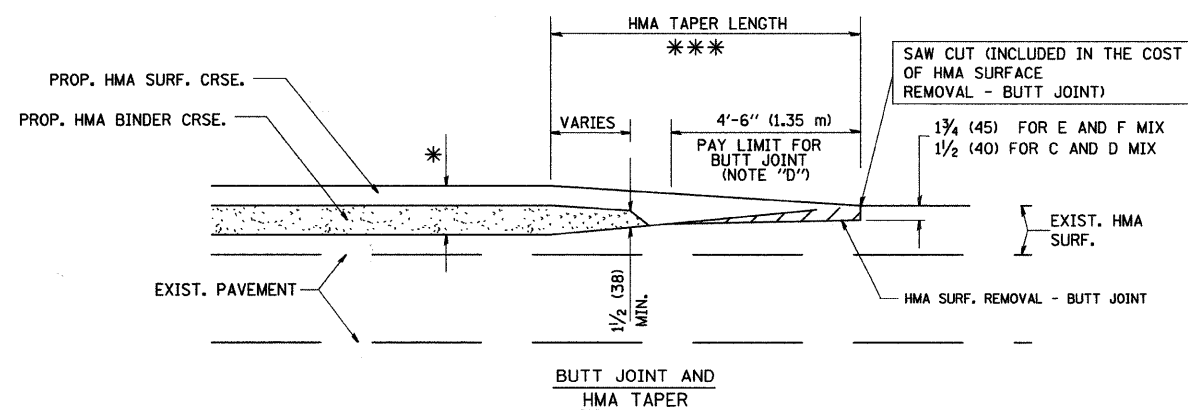


OPTION 1

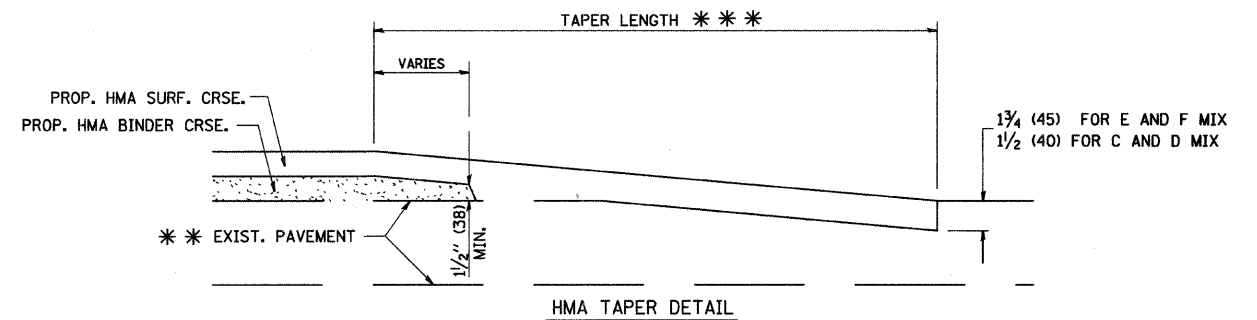
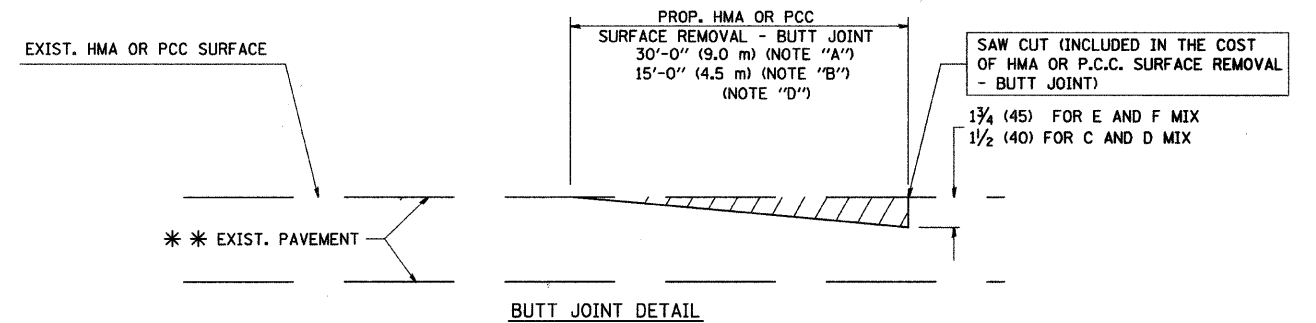


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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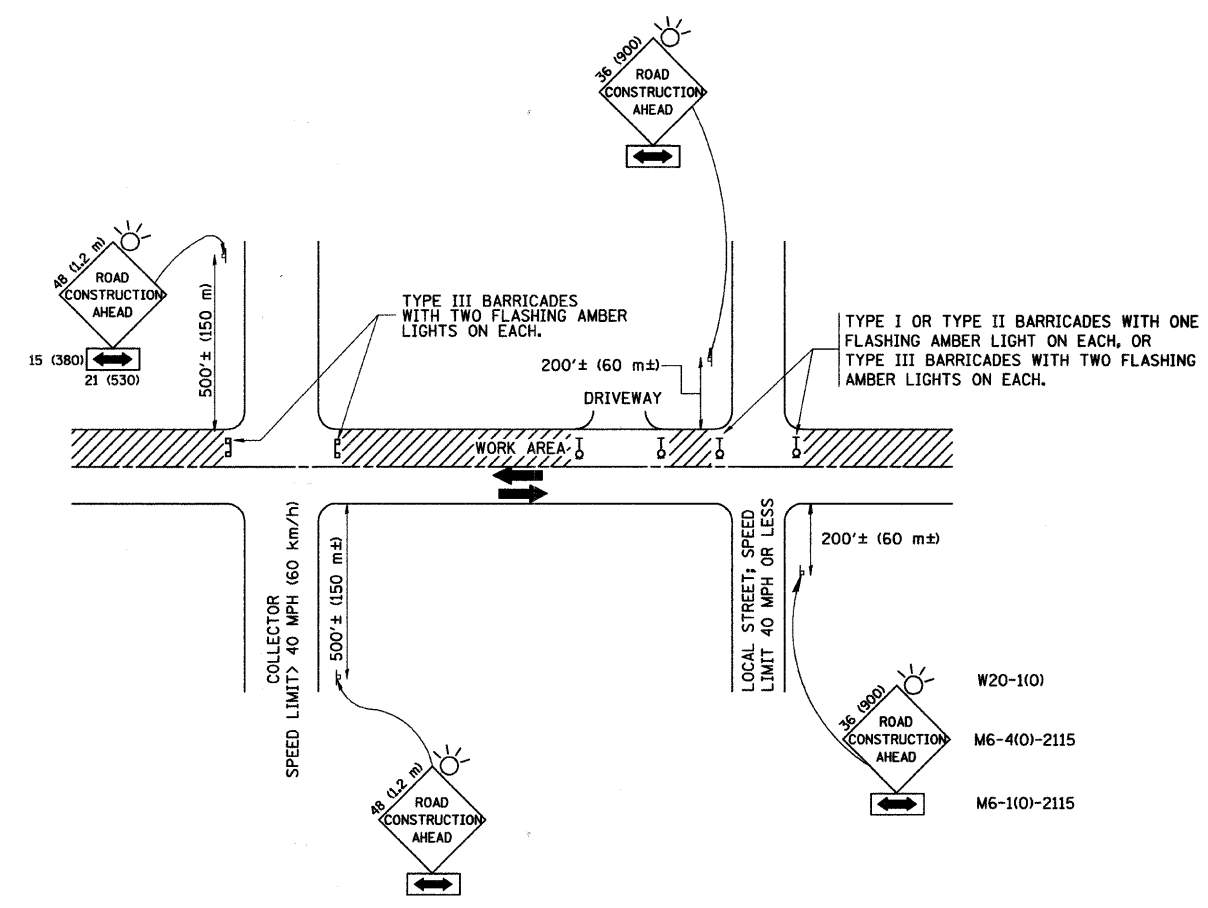
DESIGNED - M. DE YONG  
DRAWN -  
CHECKED -  
DATE - 06-13-90

REVISED - R. SHAH 10-25-94  
REVISED - A. ABBAS 03-21-97  
REVISED - M. GOMEZ 04-06-01  
REVISED - R. BORO 01-01-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER DETAILS  
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3824	09-00173-00-RS	DUPAGE	17	11
BD400-05 BD32			CONTRACT NO. 63377	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

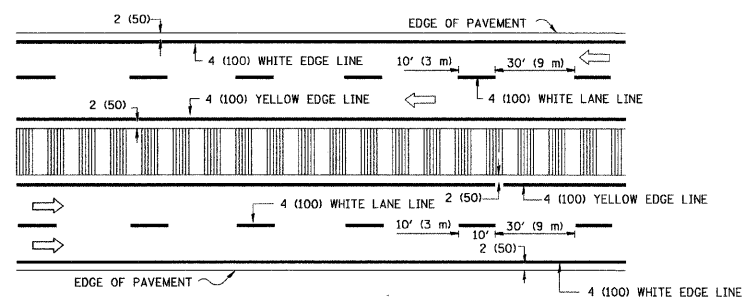
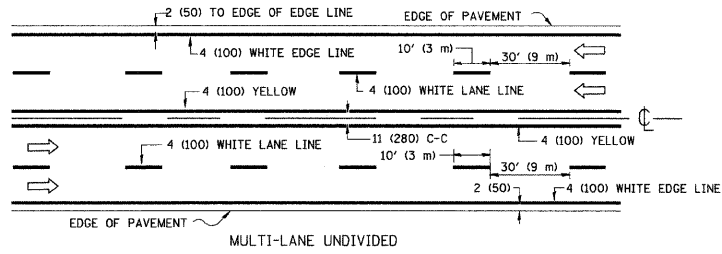
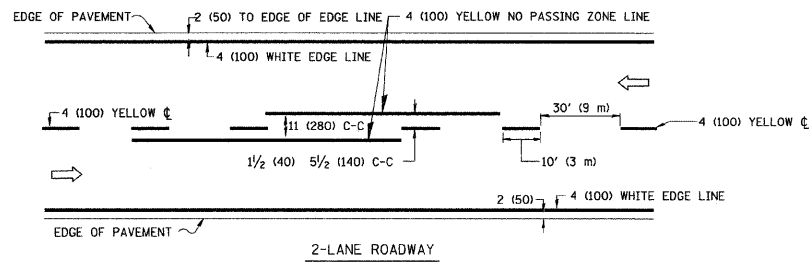
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\distatd\22x34\col18.dgn	USER NAME = gaglianobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50,000 / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

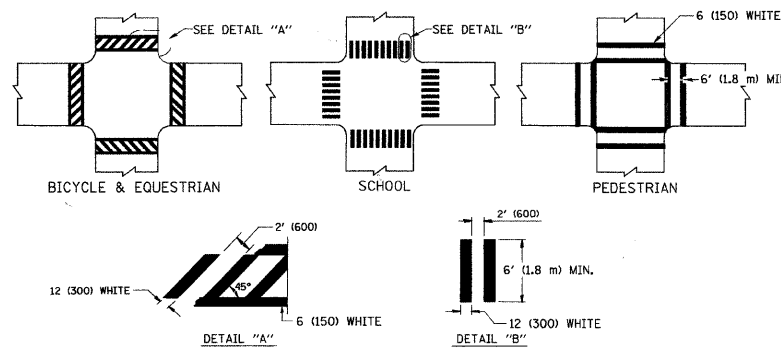
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE. 3824	SECTION 09-00173-00-RS	COUNTY DUPAGE	TOTAL SHEETS 17	SHEET NO. 12
TC-10			CONTRACT NO. 63377	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

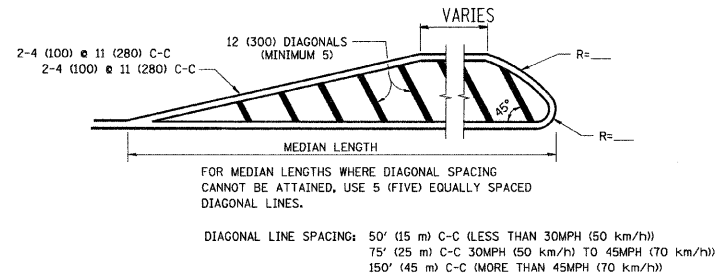
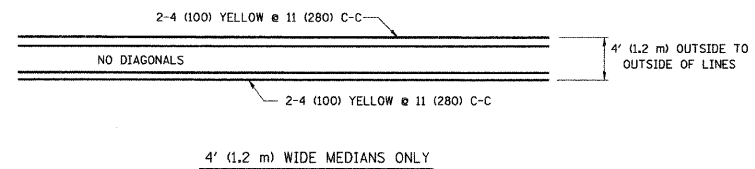


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

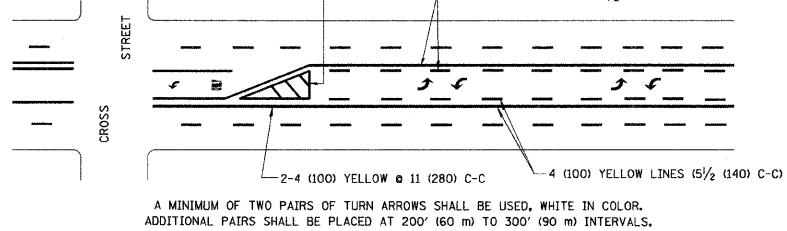
TYPICAL LANE AND EDGE LINE MARKING



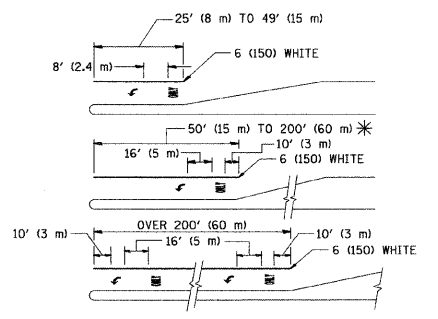
TYPICAL CROSSWALK MARKING



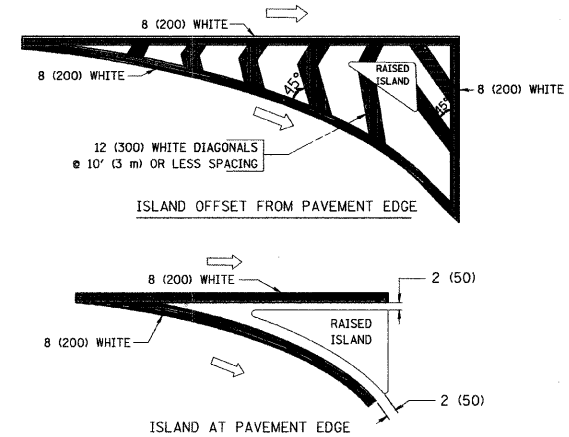
MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING



TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS 18" (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK; IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LINE FOR "X" LETTERS: 16 (400)	SOLID	WHITE	SEE STATE STANDARD 78001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

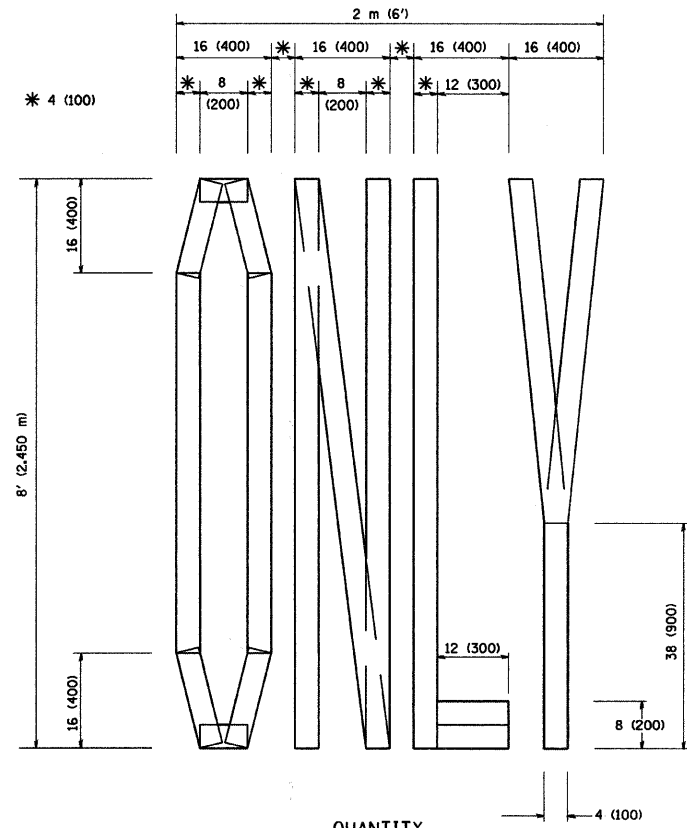
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

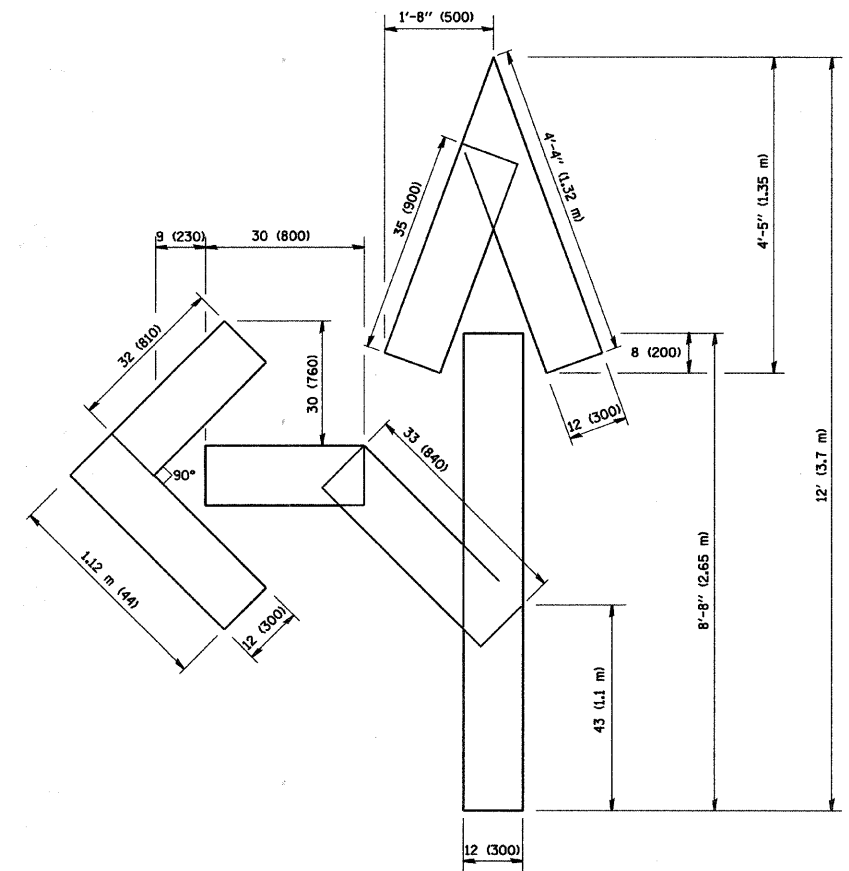
FILE NAME =	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
ca\p\work\p\dot\drivakosgn\d0108315\co	3.dgn	CHECKED -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50,000' / IN.	DATE - 03-19-90	REVISED -
	PLOT DATE = 9/9/2009		

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

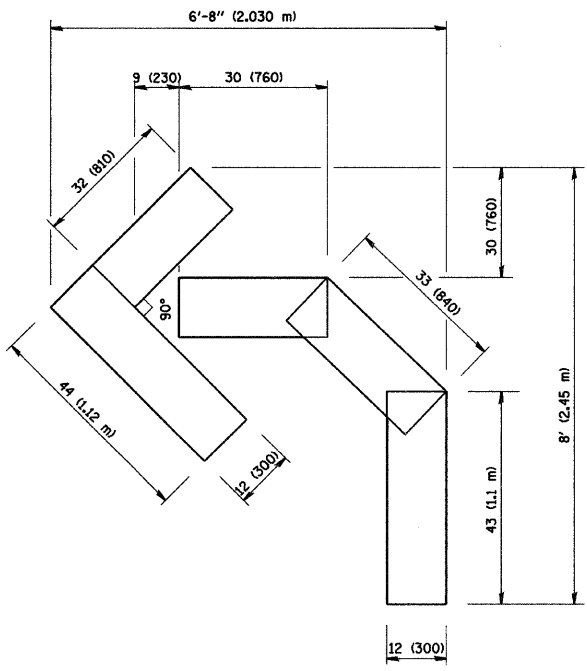
DISTRICT ONE		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		3824	09-00173-00-RS	DUPAGE	17	13
SCALE: NONE		TC-13		CONTRACT NO. 63377		
SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT		



QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



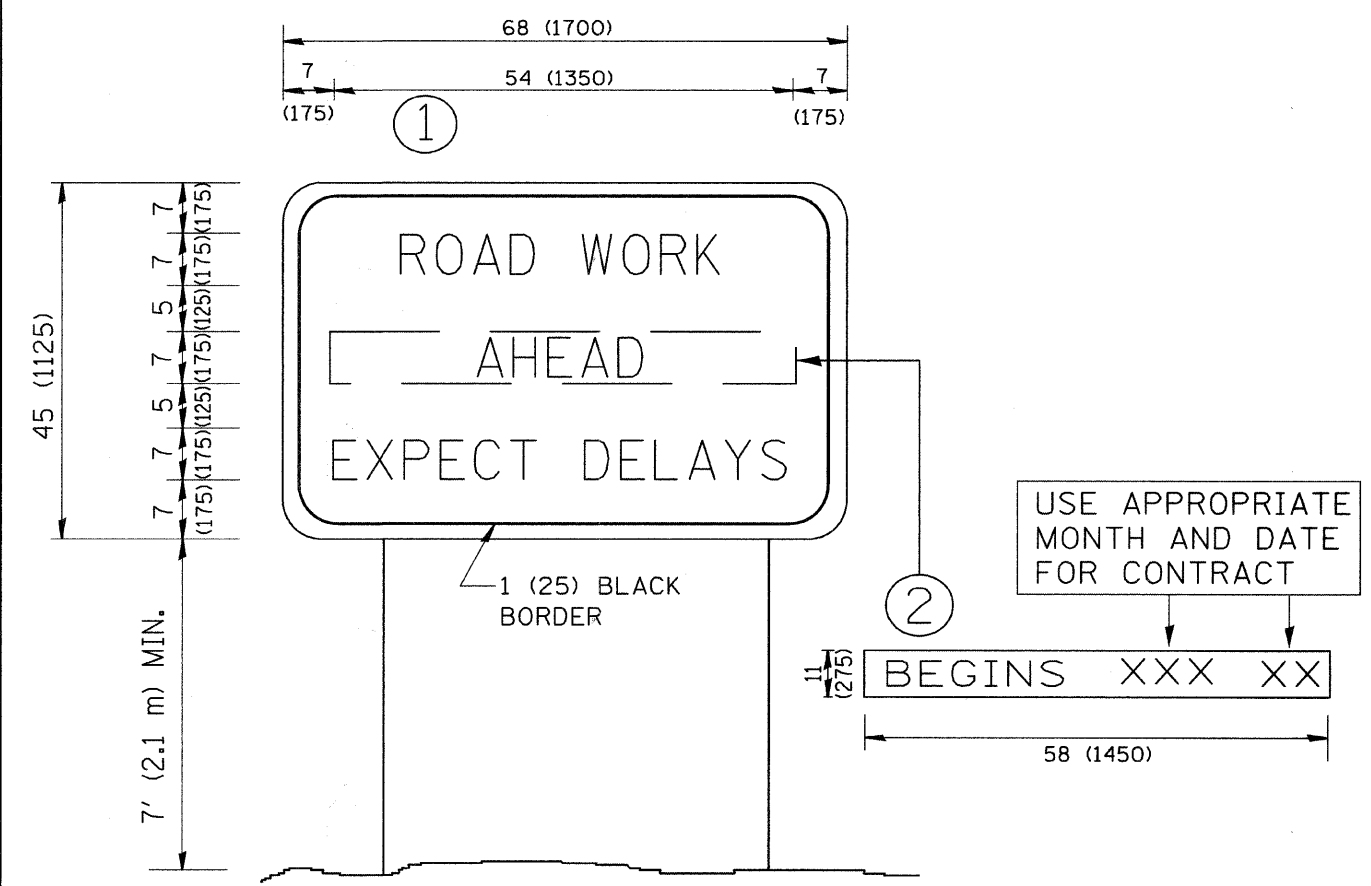
QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\distatd\22x34\1616.dgn	USER NAME = gaglianobt	DESIGNED - -	REVISED - -T. RAMMACHER 06-05-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
	PLOT SCALE = 50,0000' / IN.	CHECKED - -	REVISED - -T. RAMMACHER 11-04-97		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	3824	09-00173-00-RS	DUPAGE	17	14
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED - -T. RAMMACHER 03-02-98												
			REVISED - -E. GOMEZ 08-28-00												
											TC-16		CONTRACT NO. 63377		
											FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

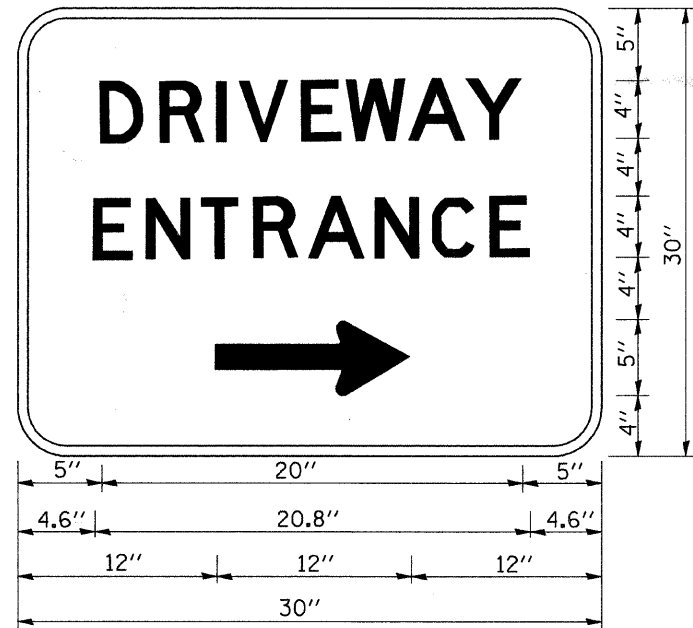


**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\to22.dgn	USER NAME = gegltenobt	DESIGNED -	REVISED - R. MIRS 09-15-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50,000' / IN.	DRAWN -	REVISED - R. MIRS 12-11-97			3824	09-00173-00-RS	DUPAGE	17	15
	PLOT DATE = 1/4/2000	CHECKED -	REVISED - T. RAMMACHER 02-02-99			<b>TC-22</b>		<b>CONTRACT NO. 63377</b>		
		DATE -	REVISED - C. JUCIUS 01-31-07	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT		



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

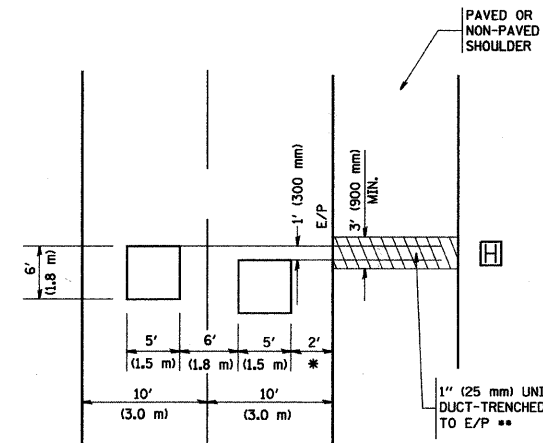
1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = W:\dstatd\22x34\to26.dgn	USER NAME = geglennobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DRIVEWAY ENTRANCE SIGNING</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50,000' / IN.	DRAWN -	REVISED -					1824	09-00173-00-RS	DUPAGE	17	16
	PLOT DATE = 1/4/2008	CHECKED -	REVISED -					<b>TC-26</b>		CONTRACT NO. 63377		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.					



LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



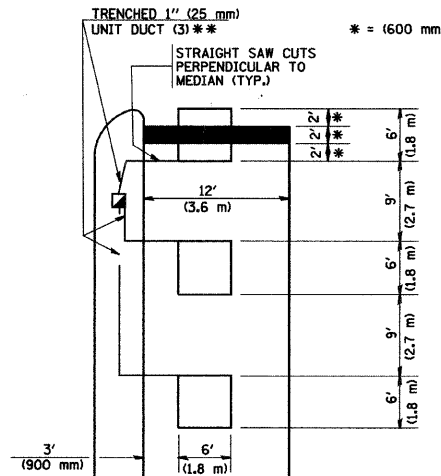
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



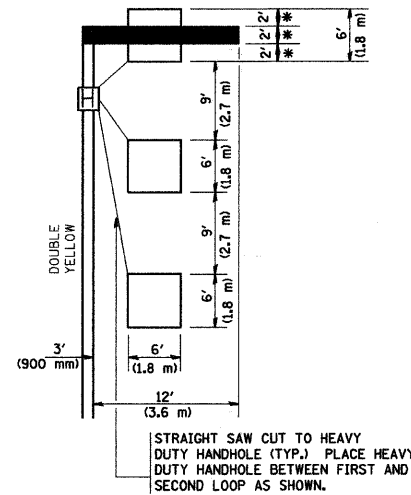
\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH

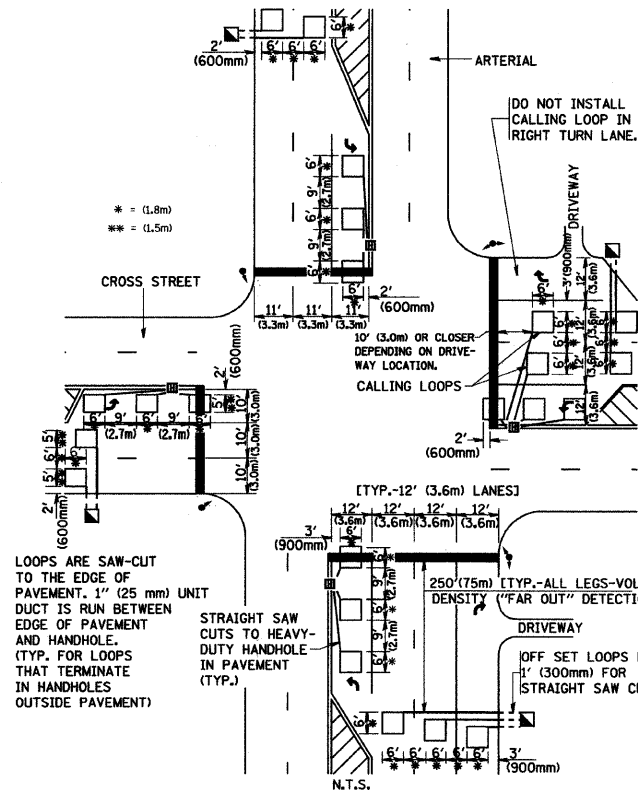
(PROTECTED / PERMITTED LEFT TURN PHASING)

\* = (600 mm)



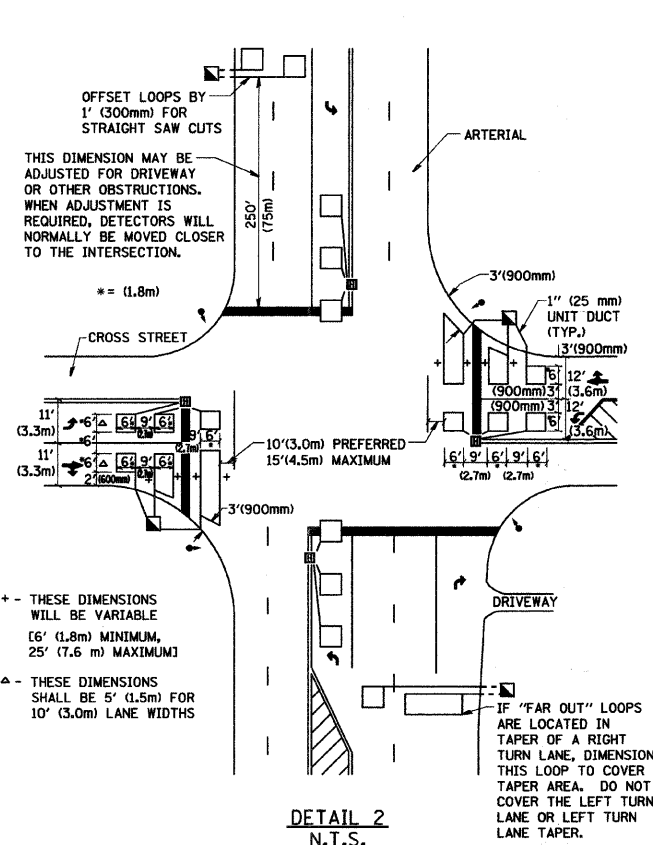
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1  
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2  
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\distatd\22x34\ts97.dgn

USER NAME = gaglianobt  
PLOT SCALE = 50,0000' / IN.  
PLOT DATE = 1/4/2008

DESIGNED -  
DRAWN -  
CHECKED - R.K.F.  
DATE -

REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION  
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3824	09-00173-00-RS	DUPAGE	17	17
TS-07			CONTRACT NO.	63377
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				