

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGE OF SOUTH ELGIN

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
F.A.U. ROUTE 1342 SPRING STREET
FROM WEST VILLAGE LIMITS TO McLEAN BOULEVARD (F.A.U. 2509)

LAPP RESURFACING
PROJECT No: ARA-9003 (608)
SECTION No: 10-00036-00-RS
JOB No: C-91-437-10
VILLAGE OF SOUTH ELGIN
KANE COUNTY

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1342	10-00036-00-RS	KANE	21	1
STA.	55+67	TO STA.	80+00	
FED. ROAD DIST. NO.	ILLINOIS	FED AID PROJECT		
C-91-437-10		ARA-9003 (608)		

CONTRACT No: 63469



LOCATION OF SECTION INDICATED THUS: [Symbol]

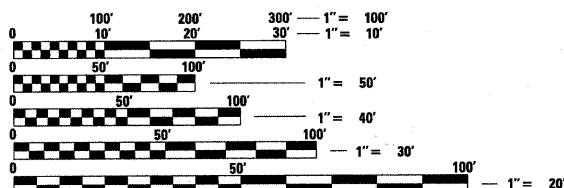
PROJECT NUMBER: ARA-9003(608)
F.A.U. ROUTE 1342
SPRING STREET
BEGIN RESURFACING
STA 55+67

PROJECT NUMBER: ARA-9003 (608)
F.A.U. ROUTE 1342
SPRING STREET
END RESURFACING /
BEGIN OMISSION
STA 80+00

PROJECT NUMBER: ARA-9003 (608)
F.A.U. ROUTE 1342
SPRING STREET
END OMISSION
STA 86+97

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STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
LICENSE NO. - 184-001121 - EXPIRES 4/30/2011

TRAFFIC DATA
SPRING STREET
POSTED & DESIGN SPEED LIMIT = 35-45 MPH
2010 ADT = 22,000 VPD
ROADWAY CLASSIFICATION: COLLECTOR



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

JULI.E. DESIGN STAGE REQUEST
DIG. No. A01805444

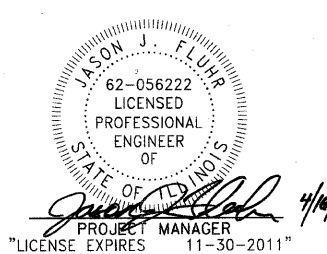


CONTACT JULIE AT 811 OR 800-892-0123
WITH THE FOLLOWING:

COUNTY = KANE
CITY-TWNSHP. = SOUTH ELGIN - ELGIN
SEC. & 1/4 SEC. NO. = SEC. 33 NE 1/4 AND SEC. 34 NW 1/4, T41N, R8E
48 HOURS (2 working days) BEFORE YOU DIG



ELGIN TOWNSHIP
GROSS LENGTH OF IMPROVEMENT = 3,130 LF OR 0.593 MILES
NET LENGTH OF IMPROVEMENT = 2,433 LF OR 0.461 MILES
LENGTH OF OMISSION = 697 LF OR 0.132 MILES



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 4-19-10
[Signature]
VILLAGE OF SOUTH ELGIN, REPRESENTATIVE

PASSED MAY 27, 2010
[Signature]
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW MAY 27, 2010
[Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

CONTRACT NO: 63469

Burlington, Wisconsin (Crystal Lake, Illinois) DeKalb, Illinois Itasca, Illinois Mokena, Illinois Grayslake, Illinois Plainfield, Illinois Madison, Wisconsin Chicago, Illinois
262.763.7834 815.459.1260 815.787.3111 630.773.1870 708.560.7070 847.223.5088 815.609.7425 608.347.1542 312.578.0050

B&W PROJECT NO.: 100084

DATE: 04-15-10

BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-001121
 EXPIRES 4/30/2011
 FIELD ENGINEER: MARILYN SOLOMON/PE /847-705-4407/ SCHAMBERG, IL

GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" AND THE "MANUAL OF TEST PROCEDURES FOR MATERIALS".
2. UTILITY LOCATIONS HAVE NOT BEEN SHOWN ON THESE PLANS. THE CONTRACTOR SHALL HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL THEIR FACILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL ALSO VERIFY THE DEPTHS OF THE EXISTING UTILITIES IF NECESSARY. ANY RELOCATION OR LOWERING OF UTILITIES SHALL BE COORDINATED BY THE CONTRACTOR
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER OR VILLAGE.
4. THE CONTRACTOR SHALL NOTIFY THE VILLAGE DIRECTOR OF PUBLIC WORKS AND THE ENGINEER AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE VILLAGE SUPERINTENDENT OF PUBLIC WORKS AND THE ENGINEER.
5. MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, DRIVEWAYS, CURB AND GUTTER, HOT-MIX ASPHALT SURFACES, SIDEWALKS AND EXCAVATION FOR NEW SIDEWALKS ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGMENT OF THE ENGINEER, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE ENGINEER WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR WILL BE BILLED (CHARGED) ACCORDINGLY.
6. THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING BAN" IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER IF DEEMED NECESSARY.
7. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE VILLAGE WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE VILLAGE TO RESIDENTS. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED. THE COST OF THIS WORK IS INCLUDED IN COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.
8. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNERS, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
9. THE CONTRACTOR SHALL NOTIFY IDOT BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS PRIOR TO THE PLACEMENT OF HMA OR CONCRETE.
10. ALL SIGNS AND MAILBOXES THAT ARE IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED AND REPLACED IN ACCORDANCE WITH VILLAGE STANDARDS AND INCLUDED IN THE PAY ITEM FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT. MAIL SERVICE SHALL BE MAINTAINED AT ALL TIMES.
11. EXISTING PAVEMENT, DRIVEWAY PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE COST OF THE APPROPRIATE REMOVAL PAY ITEM.
12. IN AREAS WHERE THE EXISTING DRIVEWAY (HOT-MIX ASPHALT OR GRAVEL), SIDEWALK, OR CURB AND GUTTER IS TO BE REMOVED AND REPLACED, THE REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL REQUIRED TO ESTABLISH THE PROPOSED DRIVEWAY, SIDEWALK, OR CURB AND GUTTER SUBGRADE ELEVATION SHALL BE INCLUDED IN THE PAY ITEMS, HMA SURFACE REMOVAL OF THE TYPE SPECIFIED, SIDEWALK REMOVAL OR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.
13. RESERVED.
14. THE PRIME COAT APPLICATION RATE SHALL BE 0.1 GAL/SY. THE MC-30 PRIME COAT APPLICATION RATE SHALL BE 0.3 GAL/SY.
15. THE LOCATIONS OF THE CLASS D PATCHES SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
16. ALL AGGREGATE USED ON THIS PROJECT SHALL BE CRUSHED MATERIAL.
17. THE QUANTITIES FOR PATCHING WILL NOT EXCEED THE AMOUNT LISTED IN THE SUMMARY OF QUANTITIES.
18. THE DAYS PAVING OPERATION SHALL RESULT IN A SINGLE TRANSVERSE JOINT. COLD LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY PAVING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE PAVING OF THE ADJACENT LANE IN THE SAME DAY.
19. FOR STEEL BARS CERTIFICATION, PLEASE CONTACT IDOT BUREAU OF MATERIALS AT (847) 705-4337.

20. ON STREETS TO BE FULL WIDTH MILLED (2" OR MORE), THE STRUCTURES IN THE PAVEMENT SHALL BE ADJUSTED IN ACCORDANCE WITH THE IDOT DETAIL "DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING". THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR FRAME AND LIDS TO BE ADJUSTED (SPECIAL). EXISTING FRAMES AND GRATES OR LIDS THAT ARE TO BE REPLACED AS PART OF STRUCTURE ADJUSTMENT SHALL BE DELIVERED TO THE VILLAGE OF SOUTH ELGIN PUBLIC WORKS.
21. THE CONTRACTOR SHALL UTILIZE A MECHANICAL SWEEPER TO CLEAN STREETS AFFECTED BY CONTRACTORS OPERATIONS, INCLUDING HAUL ROUTES, AT LEAST TWICE PER WEEK AND ADDITIONALLY AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF MOBILIZATION.
22. THE DISTRICT 1 DETAIL FOR DRIVEWAYS SHOWN IN THE PLANS SHALL BE MODIFIED TO PROVIDE HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 INSTEAD OF HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50.
23. CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
24. PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 6-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES PRIVATE DRIVEWAYS AND 8-INCHES WHERE THE SIDEWALK CROSSES COMMERCIAL DRIVEWAYS. 6" x 6" WELDED WIRE FABRIC SHALL BE INSTALLED IN THE SIDEWALK WHERE THE SIDEWALK CROSSES EITHER PRIVATE OR COMMERCIAL DRIVEWAYS. TRANSVERSE EXPANSION JOINTS 3/4-INCH SHALL BE PLACED EVERY 50 FEET OR AS DETERMINED BY THE ENGINEER. TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED EVERY 5-FEET. THIS WORK IS INCLUDED IN THE COST OF PCC SIDEWALK, 5".
25. THE DISTRICT 1 DETAIL FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT SHOWN IN THE PLANS SHALL BE MODIFIED TO INCLUDE THE FOLLOWING (THESE REQUIREMENTS SHALL APPLY TO REPLACED BARRIER CURB AS APPLICABLE). THE WORK SHALL INCLUDE SAW-CUTTING AND REMOVING THE EXISTING PAVEMENT A MINIMUM OF 6 INCHES MEASURED FROM THE EXISTING EDGE OF PAVEMENT, AND FILLING THE 6-INCH GAP WITH CLASS S1 CONCRETE TO AN ELEVATION 2 1/2 INCHES BELOW THE PROPOSED CURB AND GUTTER FLAG. IF THE CONCRETE IS PLACED HIGHER THAN 2-1/2 INCHES FROM THE GUTTER FLAG FOR STREETS TO BE RESURFACED, THE CONTRACTOR WILL BE REQUIRED TO GRIND ADDITIONAL CONCRETE TO THE REQUIRED 2-1/2 INCH DEPTH. INSTALL TWO (2) 10-FOOT NO.4 (1/2-INCH) REINFORCING BARS, CENTERED ON ALL UTILITY CROSSINGS. INSTALL TEMPORARY AGGREGATE, WHERE REQUIRED, TO MAINTAIN ACCESS. REMOVAL OF UNSUITABLE SUB-BASE MATERIAL AND REPLACEMENT WITH AGGREGATE BASE COURSE SHALL BE COMPLETED REGARDLESS OF DEPTH, AS DIRECTED BY THE ENGINEER. ALL OF THE ABOVE SHALL BE INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.
26. WORK AND MATERIALS REQUIRED TO INSTALL 1-INCH UNIT DUCT SHALL BE INCLUDED IN THE PAY ITEM DETECTOR LOOP REPLACEMENT.
27. THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.
28. DETECTABLE WARNINGS SHALL BE CONSTRUCTED WITH THE INSTALLATION OF A CAST-IN-PLACE 'ARMOR-TILE' 24" X 48" NOMINAL PANEL WIDTH AS MANUFACTURED BY 'ENGINEERED PLASTICS, INC.' (800) 682-2525 OF WILLIAMSVILLE, NY OR AN APPROVED EQUAL. THE PANEL SHALL BE A POLYMER COMPOSITE AND COMPLY WITH ADA REQUIREMENTS. THE DOMES LOCATED ON THE PANEL SHALL PARALLEL THE PAVEMENT CROSSWALK WITH THE CLOSEST EDGE LOCATED AT THE BACK OF CURB. THE PANEL COLOR SHALL BE RED OR AS SELECTED BY THE VILLAGE. INSTALLATION SHALL OCCUR IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATIONS.
29. THE CONTRACTOR WILL ONLY BE ALLOWED TO REMOVE AND REPLACE CURB AND GUTTER ON ONE SIDE OF THE ROAD AT A TIME TO MINIMIZE CONGESTION. REPLACEMENT MUST BE COMPLETE ON ONE SIDE OF THE ROAD BEFORE THE CONTRACTOR IS ALLOWED TO BEGIN REMOVING CURB AND GUTTER ON THE OTHER SIDE OF THE ROAD.
30. RESERVED
31. THE FINISHED HOT-MIX ASPHALT SURFACE SHALL BE CONSTRUCTED 0.25 INCHES ABOVE THE GUTTER FLAG.
32. A PORTABLE BATHROOM(S) SHALL BE PLACED ON THE JOB SITE(S) AND RELOCATED WHEN NECESSARY SO IT IS ACCESSIBLE TO WORKERS. IF WORK IS OCCURRING AT SEVERAL LOCATIONS, ONE PORTABLE BATHROOM SHALL BE PLACED AT EACH LOCATION WITHIN A REASONABLE DISTANCE FROM THE WORK AS DETERMINED BY THE ENGINEER. THIS SHALL BE INCLUDED IN THE PAY ITEM FOR MOBILIZATION.
33. ALL CRACKS AND JOINTS SHALL BE CLEANED PRIOR TO FILLING THEM. THIS WORK SHALL BE INCLUDED IN "MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS".
34. ALL NEW TYPE 1 OPEN LIDS SHALL BE BICYCLE SAFE.

INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	TYPICAL SECTIONS, HOT-MIX ASPHALT MIXTURE REQUIREMENTS AND SCHEDULE OF QUANTITIES
5 - 7	PLAN - SPRING STREET
8 - 10	PAVEMENT MARKING PLAN - SPRING STREET
11	DISTRICT 1 DETAIL - BD-01 DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND CURB OR EDGE GREATER THAN OF EQUAL TO 15' (4.5m)
12	DISTRICT 1 DETAIL - BD-08 DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
13	DISTRICT 1 DETAIL - BD-22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
14	DISTRICT 1 DETAIL - BD-24 CURB AND CURB AND GUTTER REMOVAL AND REPLACEMENT
15	DISTRICT 1 DETAIL - BD-32 BUTT JOINT AND HMA TAPER DETAILS
16	DISTRICT 1 DETAIL - TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
17	DISTRICT 1 DETAIL - TC-11 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKINGS (SNOW-PLOW RESISTANT)
18	DISTRICT 1 DETAIL - TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
19	DISTRICT 1 DETAIL - TC-16 DISTRICT ONE PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
20	DISTRICT 1 DETAIL - TC-18 SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS
21	DISTRICT 1 DETAIL - TC-22 ARTERIAL ROAD INFORMATION SIGN

HIGHWAY STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701502-03	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

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DESIGNED - MWP	REVISED - IDOT REVIEW 04-15-10
DRAWN - KAR	REVISED -
CHECKED - JJF	REVISED -
DATE - 03-11-10	FILE - 100084-GEN.NOTES.sht

**VILLAGE OF SOUTH ELGIN, ILLINOIS
 SPRING STREET ARRA (LAPP)
 IMPROVEMENTS**

**INDEX OF SHEETS, HIGHWAY STANDARDS
 AND GENERAL NOTES**

SCALE: NONE

STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1342	10-00036-00-RS	KANE	21	2
FED. ROAD DIST. NO. C-91-437-10				CONTRACT NO. 63469
ILLINOIS FED. AID PROJECT ARA-9003 (608)				

SUMMARY OF QUANTITIES

CODE NO.	PAY ITEM	UNIT	CONSTRUCTION CODE I000
20200100	EARTH EXCAVATION	CU YD	20
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	290
25200100	SODDING	SQ YD	290
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	62
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	5
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	1,183
40600300	AGGREGATE (PRIME COAT)	TON	26
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	5
40600895	CONSTRUCTING TEST STRIP	EACH	1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	471
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1,426
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2,940
42400800	DETECTABLE WARNINGS	SQ FT	200
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	11,825
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	5
44000600	SIDEWALK REMOVAL	SQ FT	2,530
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	588
44201725	CLASS D PATCHES, TYPE I, 7 INCH	SQ YD	71
44201729	CLASS D PATCHES, TYPE II, 7 INCH	SQ YD	213
44201733	CLASS D PATCHES, TYPE III, 7 INCH	SQ YD	426
44201735	CLASS D PATCHES, TYPE IV, 7 INCH	SQ YD	710
60250300	CATCH BASINS TO BE ADJUSTED (SPECIAL)	EACH	8
60260200	INLETS TO BE ADJUSTED (SPECIAL)	EACH	2
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	7
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	1
67100100	MOBILIZATION	L SUM	1
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	2,077
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	175

* INDICATES SPECIALTY ITEM

CODE NO.	PAY ITEM	UNIT	CONSTRUCTION CODE I000
72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	2
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	212
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	7,720
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,334
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1,240
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	92
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	130
* 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	130
X0321556	SANITARY MANHOLES TO BE ADJUSTED	EACH	3
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	156

* INDICATES SPECIALTY ITEM

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**VILLAGE OF SOUTH ELGIN, ILLINOIS
SPRING STREET ARRA (LAPP)
IMPROVEMENTS**

SUMMARY OF QUANTITIES

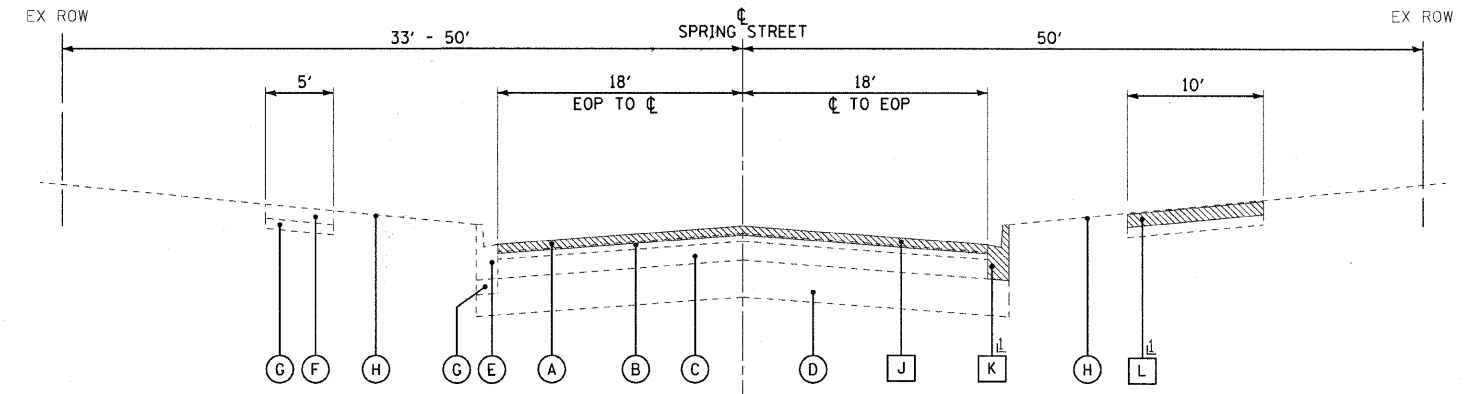
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STA. TO STA.

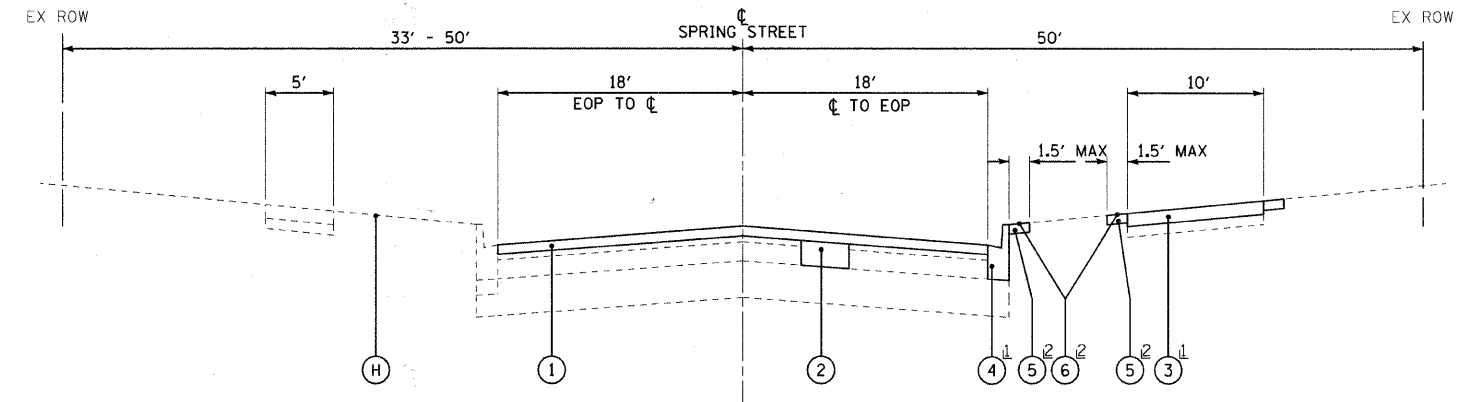
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FED. ROAD DIST. NO. C-91-437-10			CONTRACT NO. 63469	
ILLINOIS FED. AID PROJECT ARA-9003 (608)				

**SCHEDULE OF QUANTITIES
HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT**

LOCATION	AREA (SQ YD)
SPRING STREET (STA 55+67)	85
KINGSPORT DRIVE (STA 67+29 RT)	34
HOBART DRIVE (STA 67+34 LT)	63
KINGSPORT DRIVE (STA 67+58 RT)	34
HOBART DRIVE (STA 67+63 LT)	67
SANDHURST LANE (STA 74+31 LT)	74
IONE DRIVE (STA 76+97 RT)	63
SPRING STREET (STA 80+00)	85
TOTAL	505



**EXISTING TYPICAL SECTION
SPRING STREET
STA 55+67 TO STA 80+00**



**PROPOSED TYPICAL SECTION
SPRING STREET
STA 55+67 TO STA 80+00**

HOT-MIX ASPHALT MIXTURE REQUIREMENTS NOTES:

1. CONTRACTOR SHALL MILL BEFORE PATCHING.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm); 2"	4% @ 70 GYR.
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm); 2"	4% @ 70 GYR.
HMA BASE COURSE (HMA BINDER IL-19 mm); CE - 8"	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 mm); TYPE I - IV - 7-INCH	4% @ 70 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ. YD./IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

EXISTING LEGEND

- (A) HOT-MIX ASPHALT SURFACE COURSE, 1 1/2"
- (B) HOT-MIX ASPHALT BINDER COURSE, 1 1/2"
- (C) HOT-MIX ASPHALT BASE COURSE, 6"
- (D) AGGREGATE SUBGRADE, 12"
- (E) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (F) EXISTING SIDEWALK
- (G) AGGREGATE BASE COURSE
- (H) GROUND SURFACE
- (J) HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- (K) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- (L) SIDEWALK REMOVAL
- (/) ITEM TO BE REMOVED

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2"
- (2) CLASS D PATCHES, TYPE I - IV, 7-INCH
- (3) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (4) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- (5) TOPSOIL FURNISH AND PLACE, 4"
- (6) SODDING

LEGEND NOTES:

1. SIDEWALK REMOVAL AND REPLACEMENT TO BE DETERMINED BY THE ENGINEER, UNLESS OTHERWISE SHOWN.
2. CURB AND GUTTER REMOVAL AND REPLACEMENT TO BE DETERMINED BY THE ENGINEER, UNLESS OTHERWISE SHOWN.

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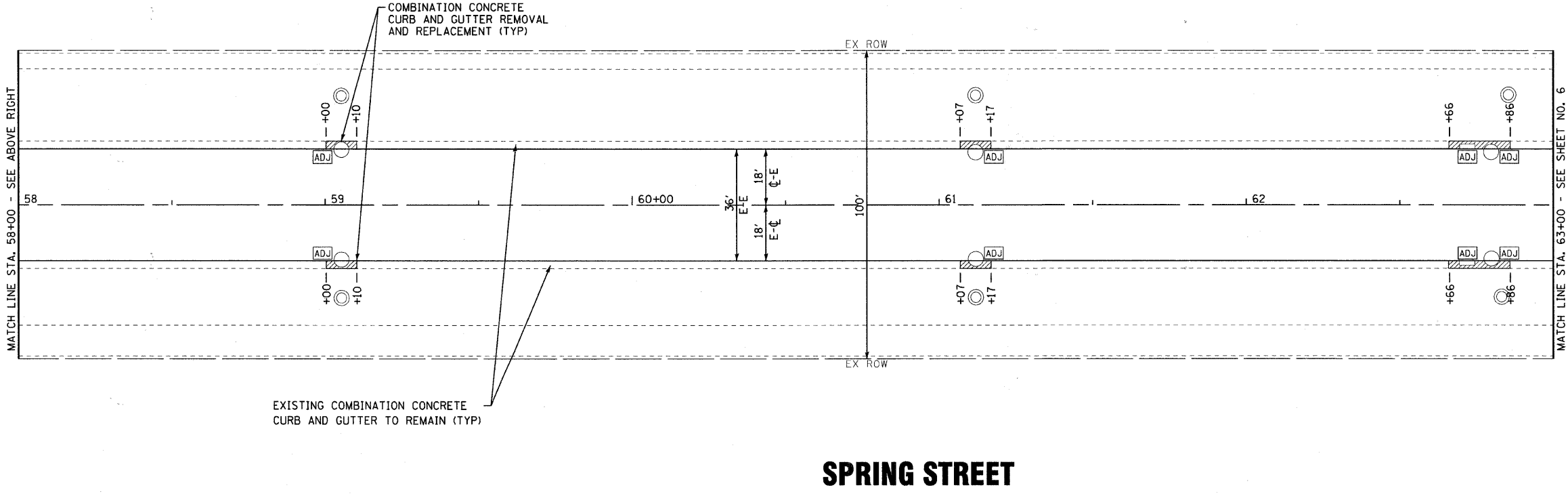
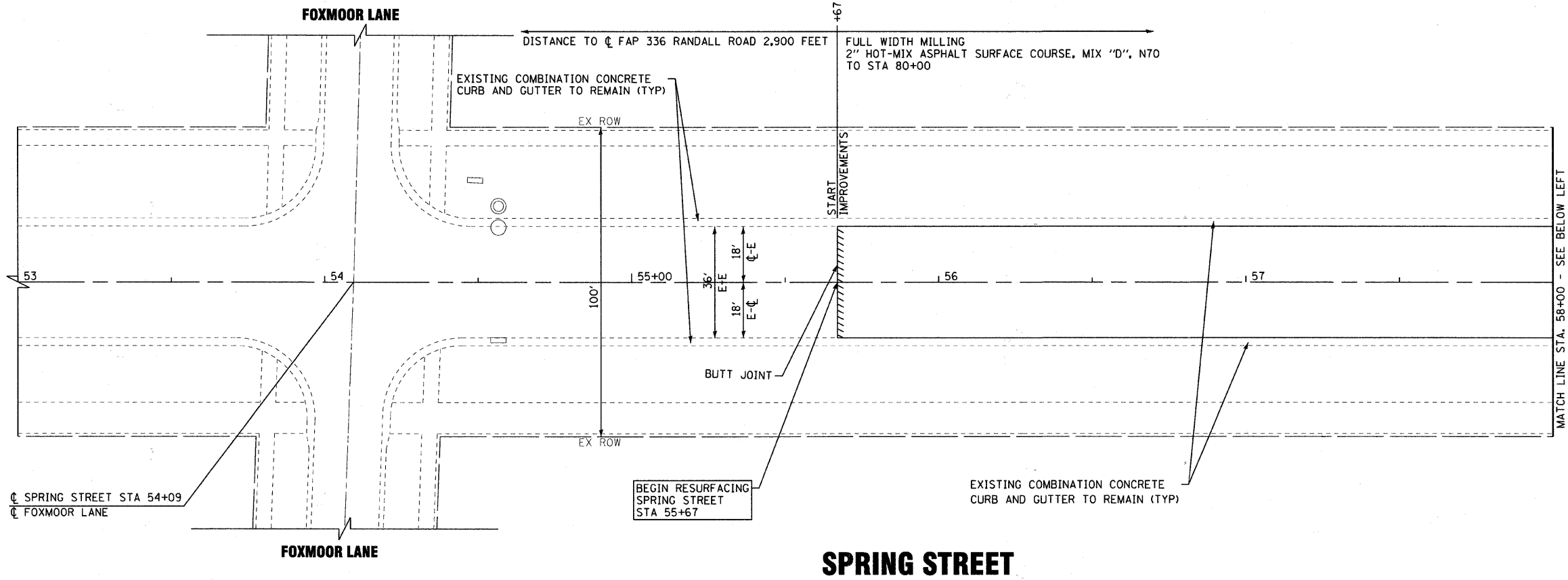
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**VILLAGE OF SOUTH ELGIN, ILLINOIS
SPRING STREET ARRA (LAPP)
IMPROVEMENTS**

**TYPICAL SECTIONS, HMA MIXTURE REQUIREMENTS
AND SCHEDULE OF QUANTITIES**

SCALE: NONE STA. 55+67 TO STA. 80+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1342	10-00036-00-RS	KANE	21	4
FED. ROAD DIST. NO. C-91-437-10			CONTRACT NO. 63469	
ILLINOIS FED. AID PROJECT ARA-9003 (608)				



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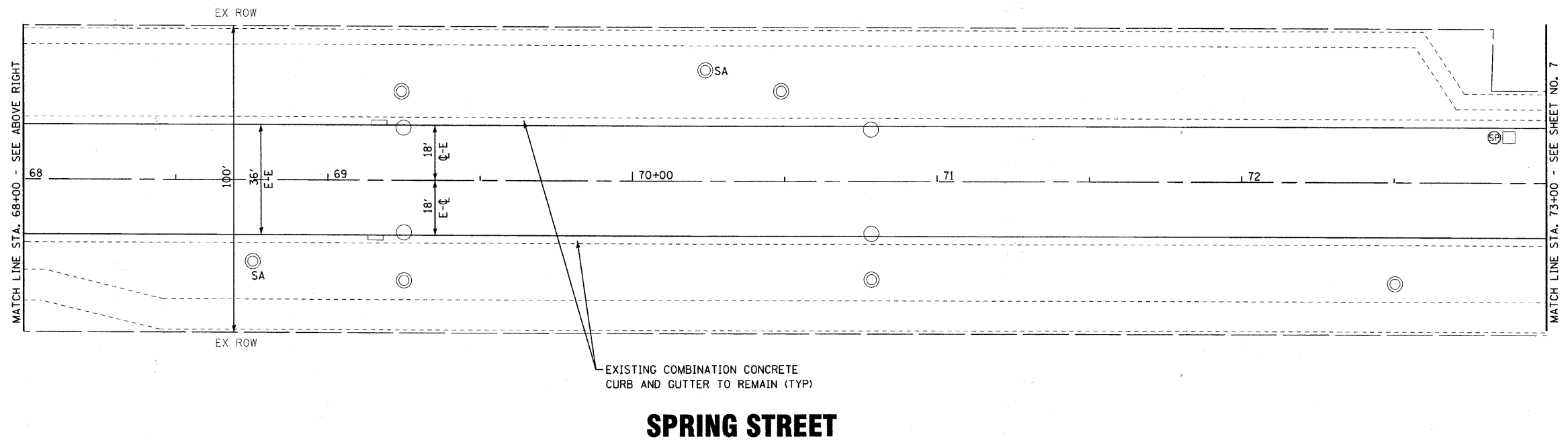
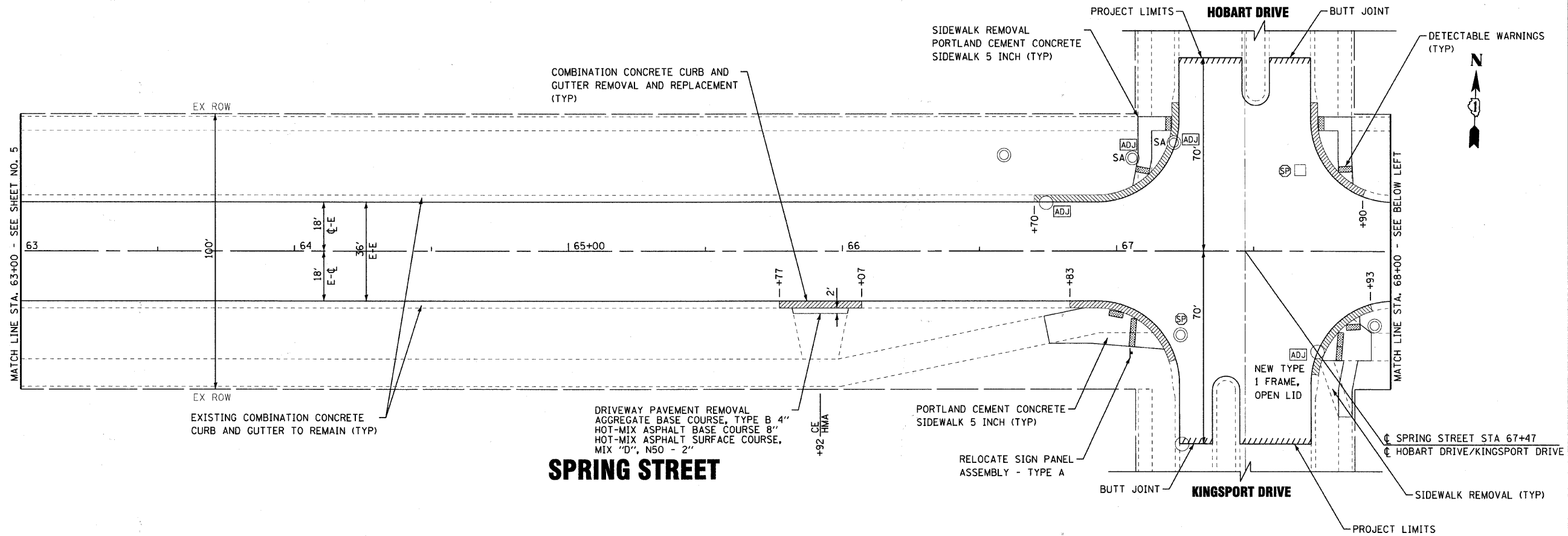
VILLAGE OF SOUTH ELGIN, ILLINOIS
SPRING STREET ARRA (LAPP)
IMPROVEMENTS

PLAN - SPRING STREET

SCALE: 1" = 20'

STA. 53+00 TO STA. 63+00

F.A.U. RTE. 1342	SECTION 10-00036-00-RS	COUNTY KANE	TOTAL SHEETS 21	SHEET NO. 5
FED. ROAD DIST. NO. C-91-437-10		CONTRACT NO. 63469		
ILLINOIS FED. AID PROJECT ARA-9003 (608)				



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 PROJECT: SPRING STREET LAPP
 DRAWING: SPRING STREET LAPP SURVEY DRAWINGS\DDNS\DDNS\00084_PP2.SHT
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VILLAGE OF SOUTH ELGIN, ILLINOIS
SPRING STREET ARRA (LAPP)
IMPROVEMENTS

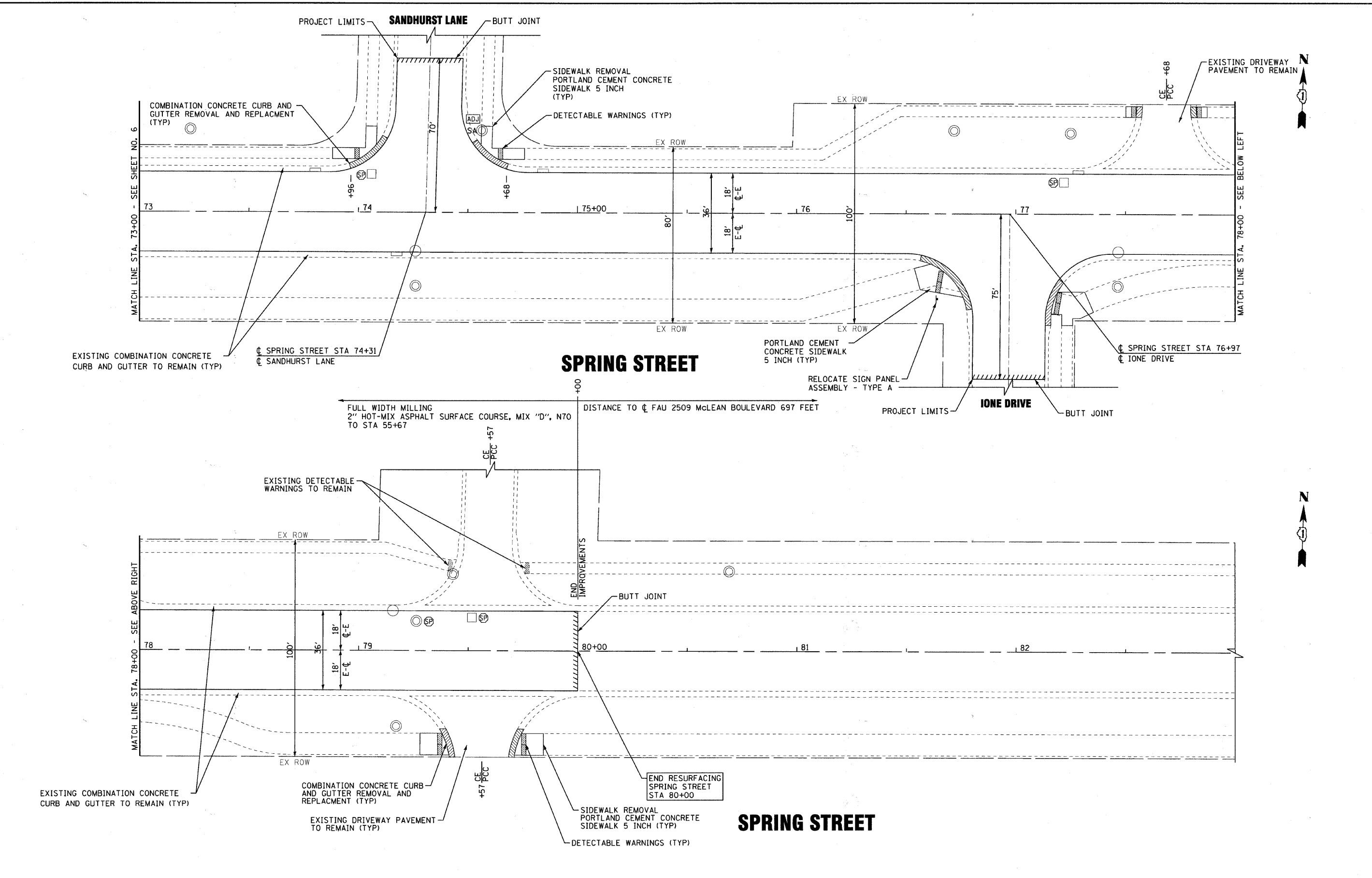
PLAN - SPRING STREET

SCALE: 1" = 20'

STA. 63+00 TO STA. 73+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1342	10-00036-00-RS	KANE	21	6
FED. ROAD DIST. NO. C-91-437-10			CONTRACT NO. 63469	
ILLINOIS FED. AID PROJECT ARA-9003 (608)				

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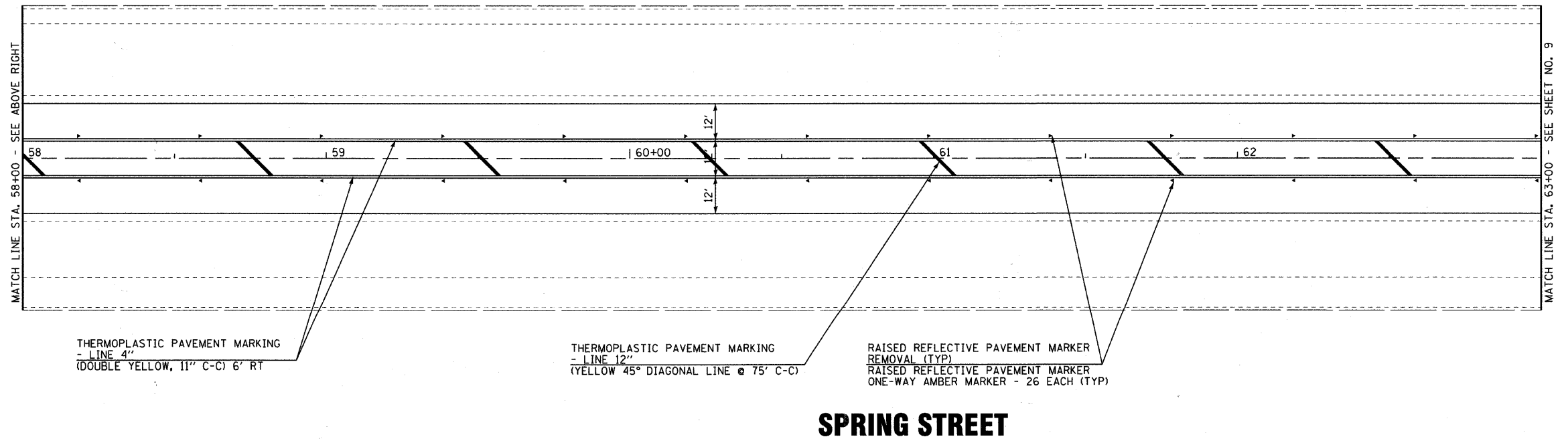
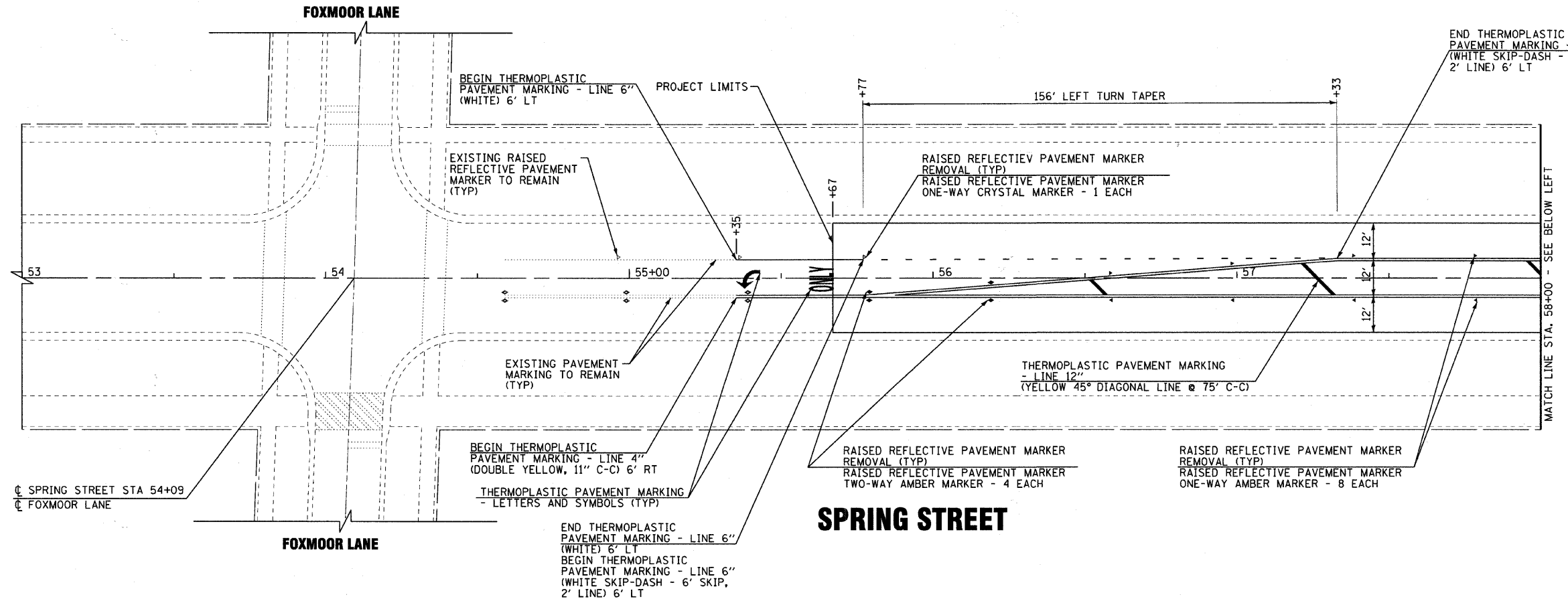


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DRAWN - KAR	REVISED -
CHECKED - JJF	REVISED -
DATE - 03-11-10	FILE - 100084-pp3.sht

VILLAGE OF SOUTH ELGIN, ILLINOIS
SPRING STREET ARRA (LAPP)
IMPROVEMENTS

PLAN - SPRING STREET	
SCALE: 1" = 20'	STA. 73+00 TO STA. 83+00

F.A.U. RTE. 1342	SECTION 10-00036-00-RS	COUNTY KANE	TOTAL SHEETS 21	SHEET NO. 7
FED. ROAD DIST. NO. C-91-437-10		CONTRACT NO. 63469		
ILLINOIS FED. AID PROJECT ARA-9003 (608)				



CONTRACT NO. 63469
 DRAWN BY: BAXTER & WOODMAN, INC.
 LICENSE NO. 184-0022 - EXPIRES 4/30/2011
 DATE: 4/19/2010
 PROJECT: VILLAGE OF SOUTH ELGIN, ILLINOIS
 DRAWING: SPRING STREET ARR (LAPP) IMPROVEMENTS
 FILE: 100084-pml.sht



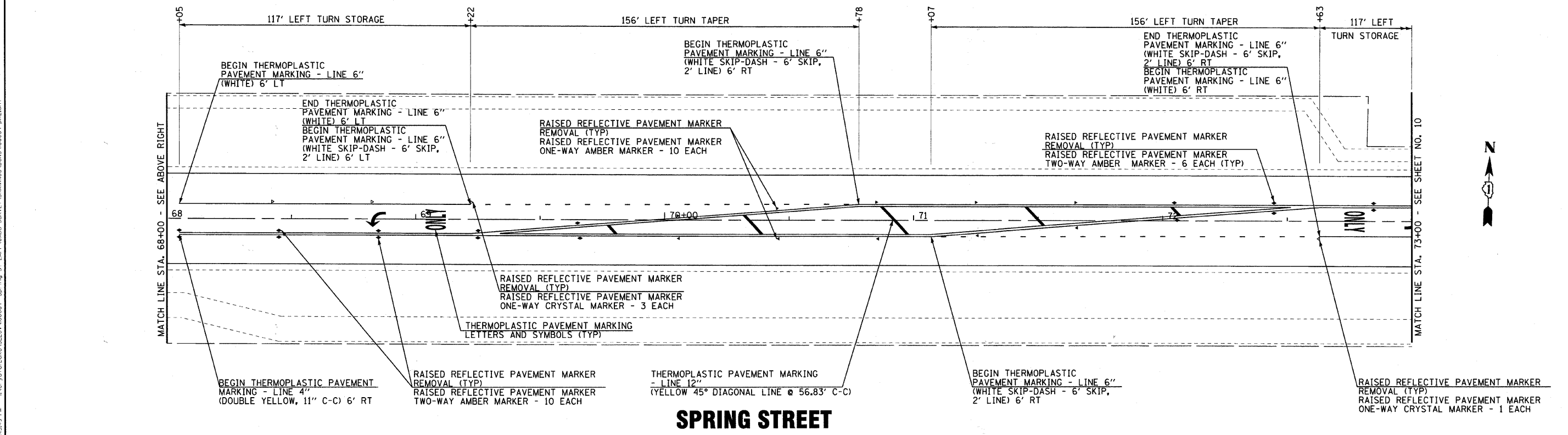
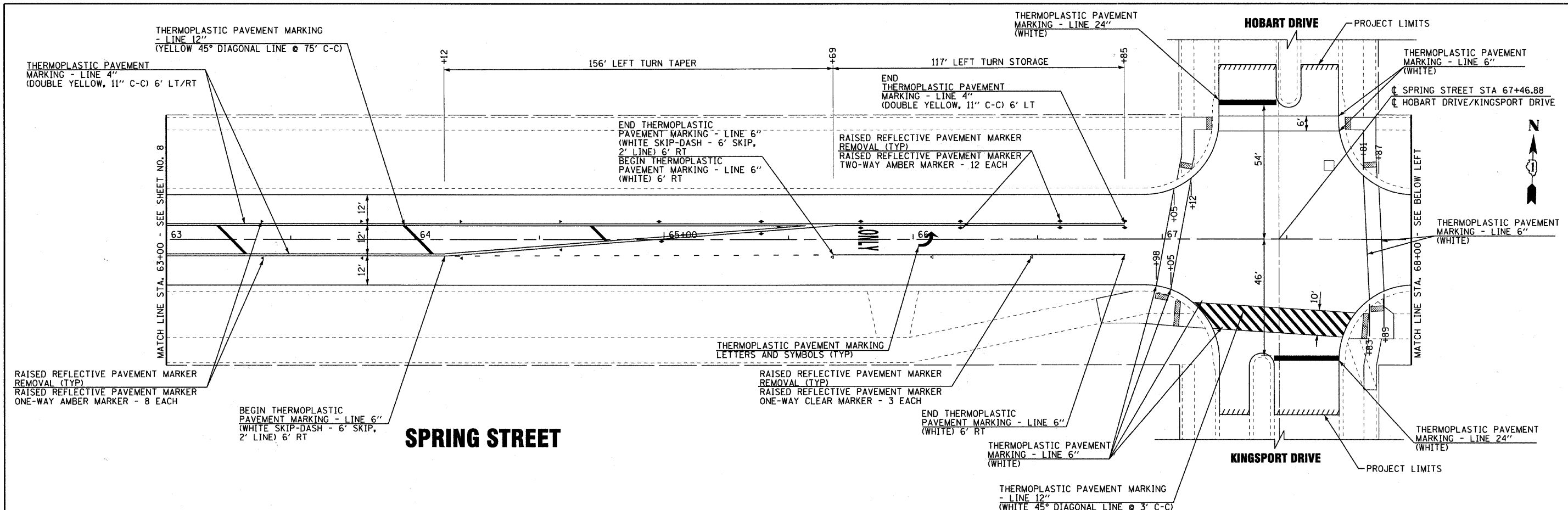
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DATE - 03-11-10	FILE - 100084-pml.sht

VILLAGE OF SOUTH ELGIN, ILLINOIS
SPRING STREET ARR (LAPP)
IMPROVEMENTS

PAVEMENT MARKING PLAN - SPRING STREET

SCALE: 1" = 20'
STA. 53+00 TO STA. 63+00

F.A.U. RTE. 1342	SECTION 10-00036-00-RS	COUNTY KANE	TOTAL SHEETS 21	SHEET NO. 8
FED. ROAD DIST. NO. C-91-437-10			CONTRACT NO. 63469	
ILLINOIS FED. AID PROJECT ARA-9003 (608)				



CONTRACT NO. 2005 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-000761 - EXPIRES 4/30/2011
 IN CHARGE: LARRY C. BAXTER, P.E. LICENSE NO. 000084 - EXPIRES 4/30/2011
 DATE: 4/19/2010



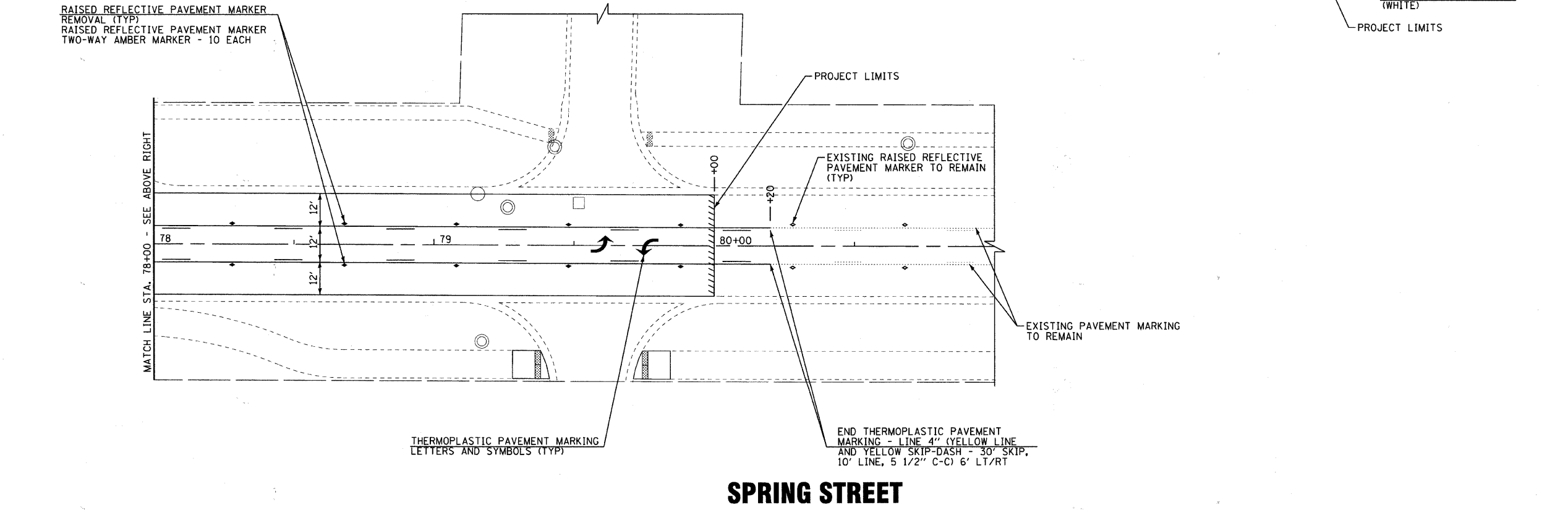
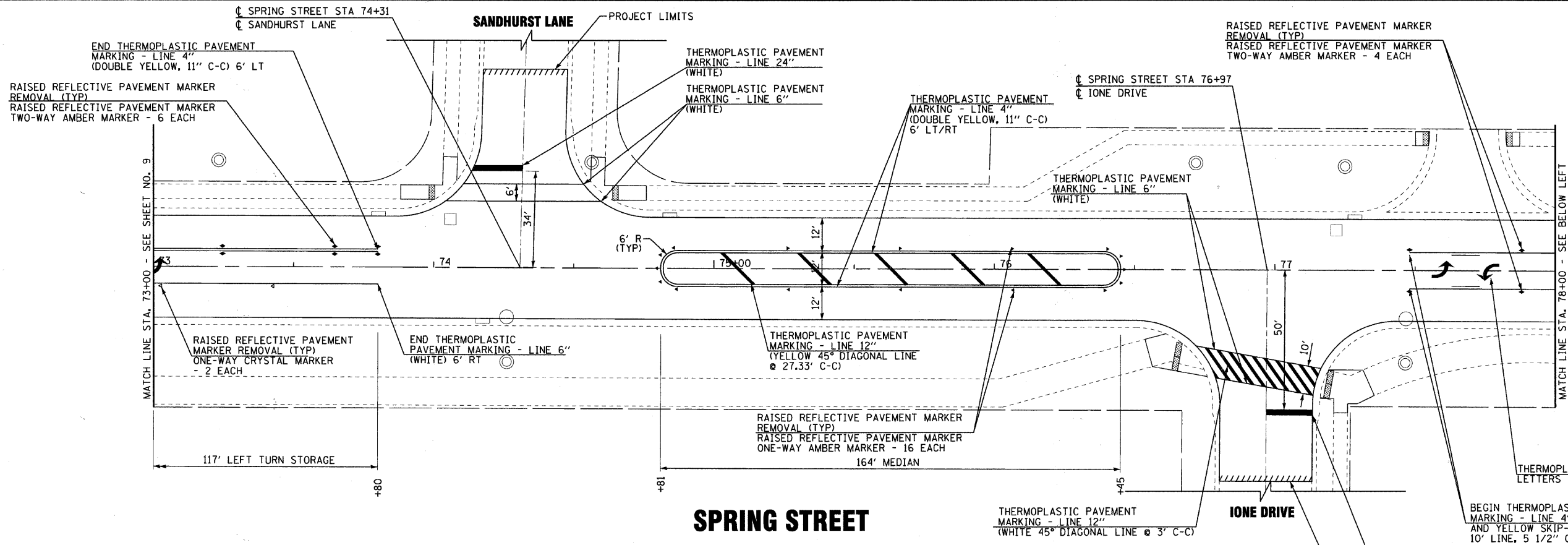
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VILLAGE OF SOUTH ELGIN, ILLINOIS
SPRING STREET ARRA (LAPP)
IMPROVEMENTS

PAVEMENT MARKING PLAN - SPRING STREET

SCALE: 1" = 20' STA. 63+00 TO STA. 73+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1342	10-00036-00-RS	KANE	21	9
FED. ROAD DIST. NO. C-91-437-10			CONTRACT NO. 63469	
ILLINOIS FED. AID PROJECT ARA-9003 (608)				



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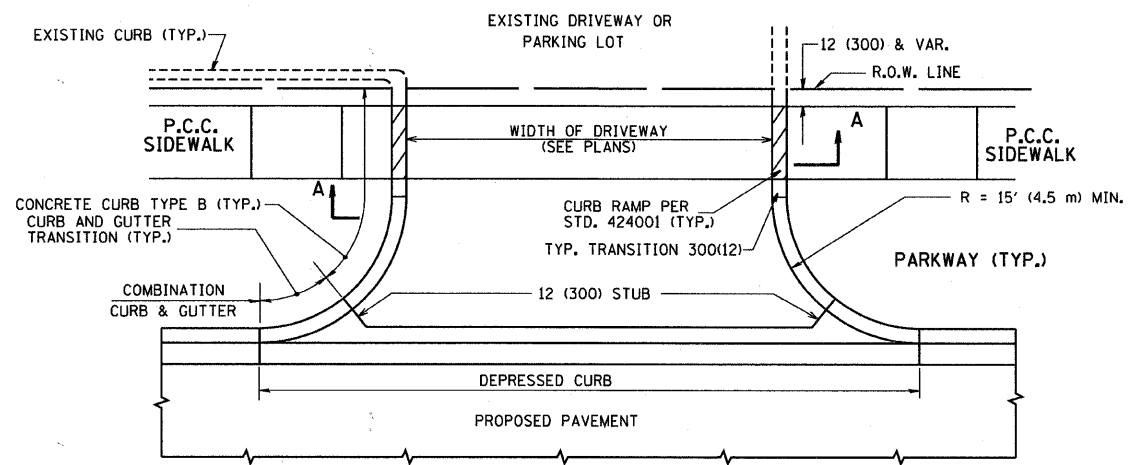
VILLAGE OF SOUTH ELGIN, ILLINOIS
SPRING STREET ARRA (LAPP)
IMPROVEMENTS

PAVEMENT MARKING PLAN - SPRING STREET

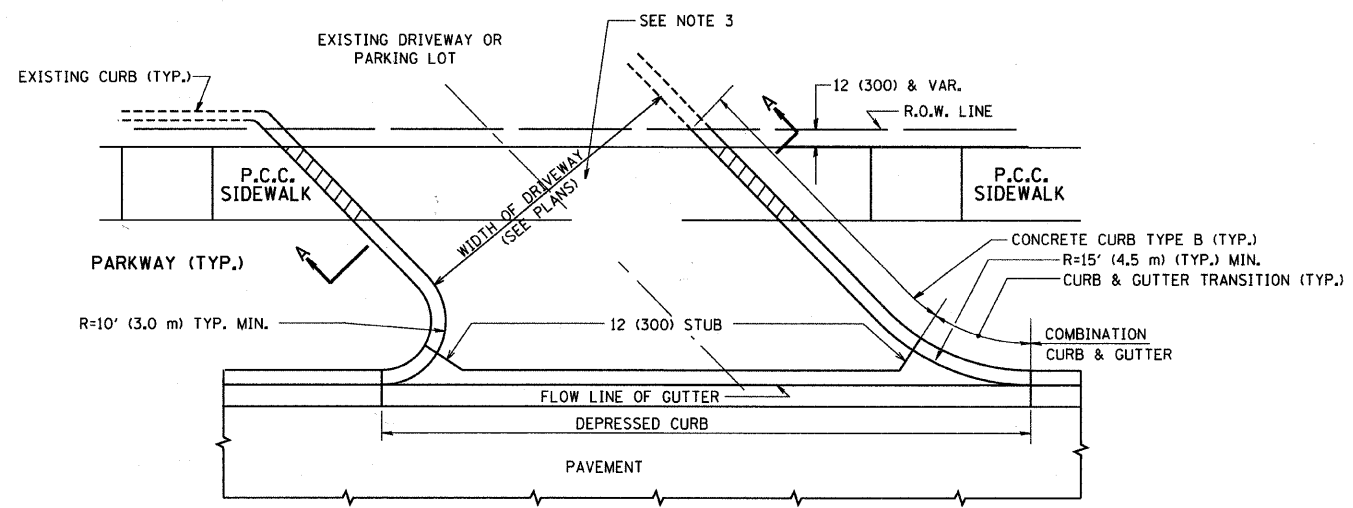
SCALE: 1" = 20'

STA. 73+00 TO STA. 81+00

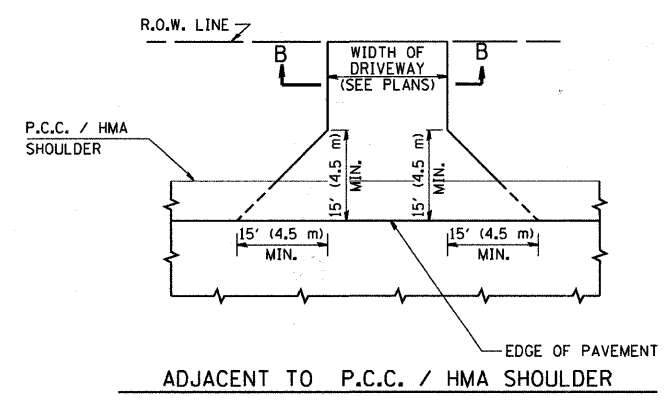
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1342	10-00036-00-RS	KANE	21	10
FED. ROAD DIST. NO. C-91-437-10			CONTRACT NO. 63469	
ILLINOIS FED. AID PROJECT ARA-9003 (608)				



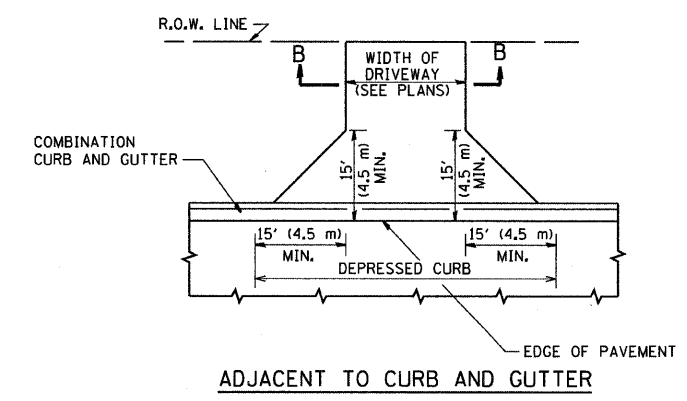
WITH CONCRETE CURB, TYPE B



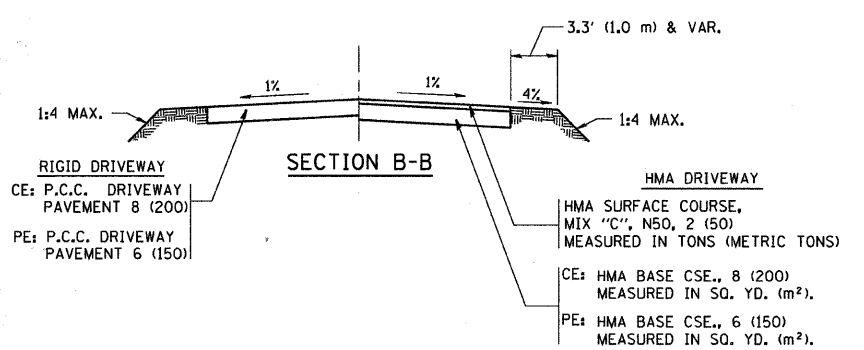
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)
 HMA SURFACE COURSE,
 MIX "C", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 CE: HMA BASE COURSE, 8 (200)
 MEASURED IN SQ. YD. (m²).
 PE: HMA BASE COURSE, 6 (150)
 MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

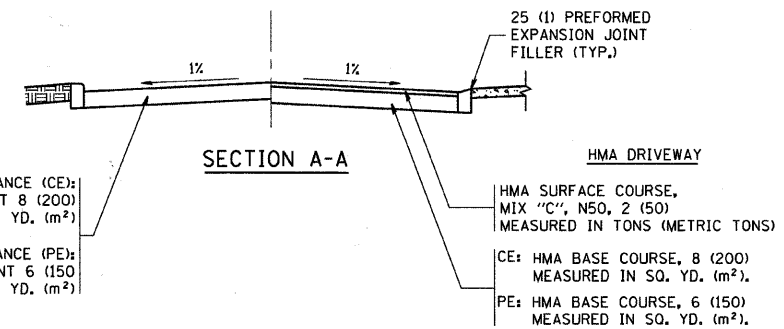
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

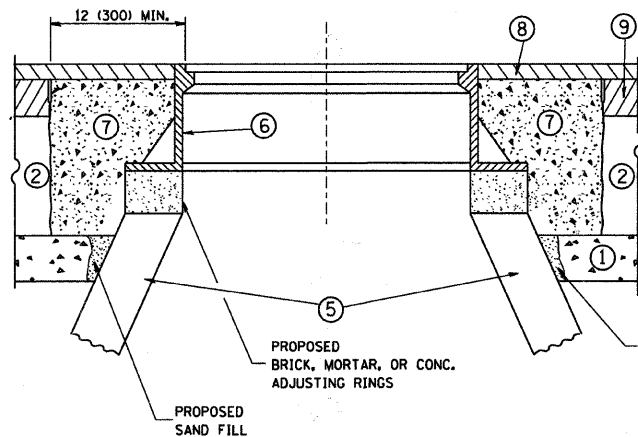
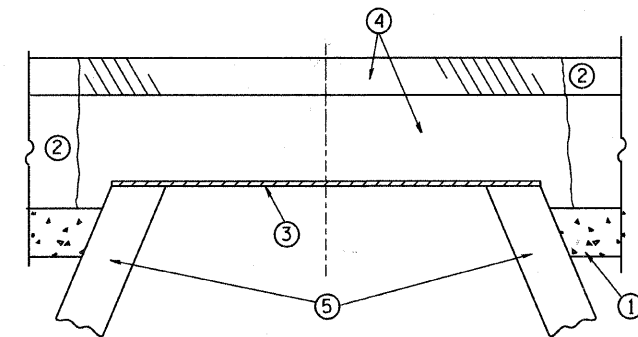
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



RIGID DRIVEWAY
 COMMERCIAL ENTRANCE (CE):
 P.C.C. DRIVEWAY PAVEMENT 8 (200)
 MEASURED IN SQ. YD. (m²)
 NON-COMMERCIAL ENTRANCE (PE):
 P.C.C. DRIVEWAY PAVEMENT 6 (150)
 MEASURED IN SQ. YD. (m²)

HMA DRIVEWAY
 HMA SURFACE COURSE,
 MIX "C", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 CE: HMA BASE COURSE, 8 (200)
 MEASURED IN SQ. YD. (m²).
 PE: HMA BASE COURSE, 6 (150)
 MEASURED IN SQ. YD. (m²).

FILE NAME = c:\projects\diststd22x34\bd01.dgn	USER NAME = bevardl	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)			F.A.U. RTE. 1342	SECTION 10-00036-00-RS	COUNTY KANE	TOTAL SHEETS 21	SHEET NO. 11
PLOT SCALE = 49,9999' / IN.	CHECKED -	DATE - 11-04-95	REVISED - P. LOFLUER 04-15-03		SCALE: NONE	SHEET NO. 1 OF 1	SHEETS	STA.	TO STA.	CONTRACT NO. 63469		
PLOT DATE = 6/12/2008	DATE -	REVISED - R. BORO 01-01-07	REVISED - R. BORO 06-11-08		FED. ROAD DIST. NO. 1			ILLINOIS FED. AID PROJECT ARA-9003 (608)			C-91-437-10	



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

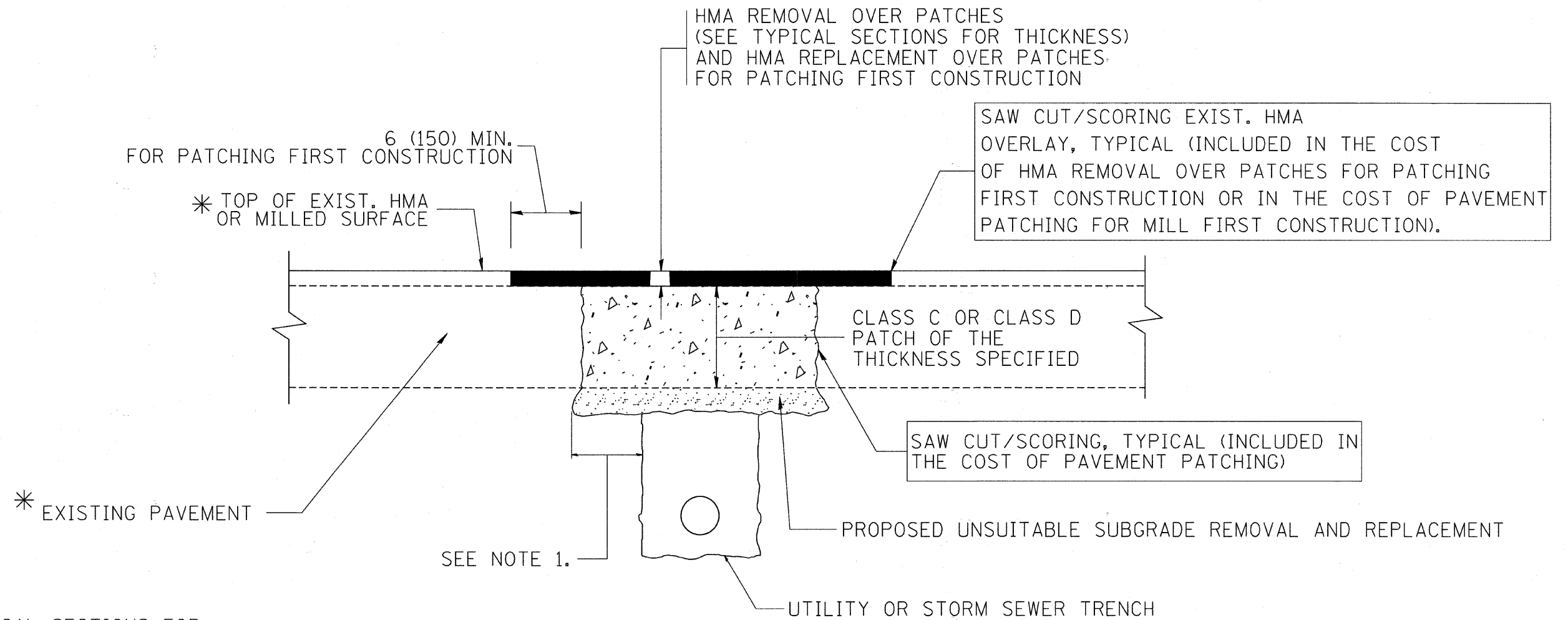
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\d1ststd\22x34\bd06.dgn	USER NAME = goglianobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	1342	10-00036-00-RS	KANE	21	12
		CHECKED -	REVISED - R. WIEDEMAN 05-14-04					BD600-03 (BD-8)			CONTRACT NO. 63489	
		DATE - 10-25-94	REVISED - R. BORO 01-01-07					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (608)				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

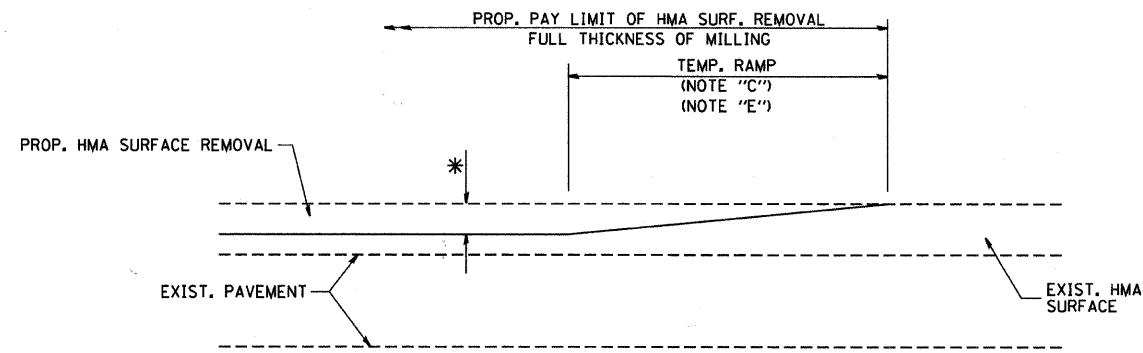
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

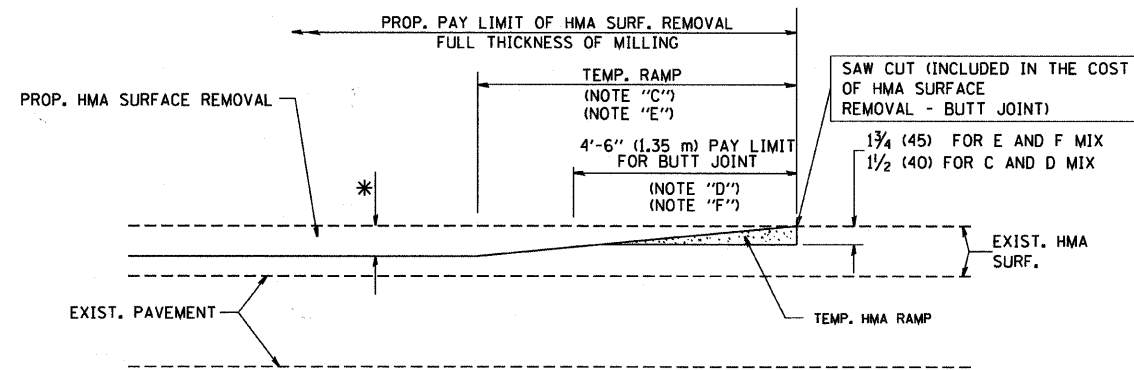
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.U. RTE. 1342	SECTION 10-00036-00-RS	COUNTY KANE	TOTAL SHEETS 21	SHEET NO. 13
PLOT SCALE = 50.000' / IN.	CHECKED -	REVISOR - R. BORO 01-01-07	REVISOR - R. BORO 09-04-07		SCALE: NONE	SHEET NO. 1 OF 1	SHEETS	STA.	TO STA.	BD400-04 (BD-22) CONTRACT NO. 63469		
PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISOR - K. ENG 10-27-08			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (608)							
					C-61-437-10							



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

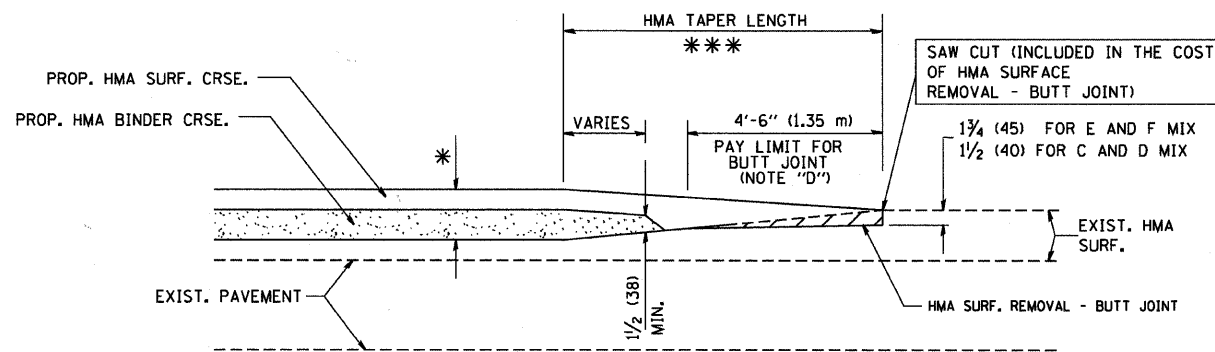
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

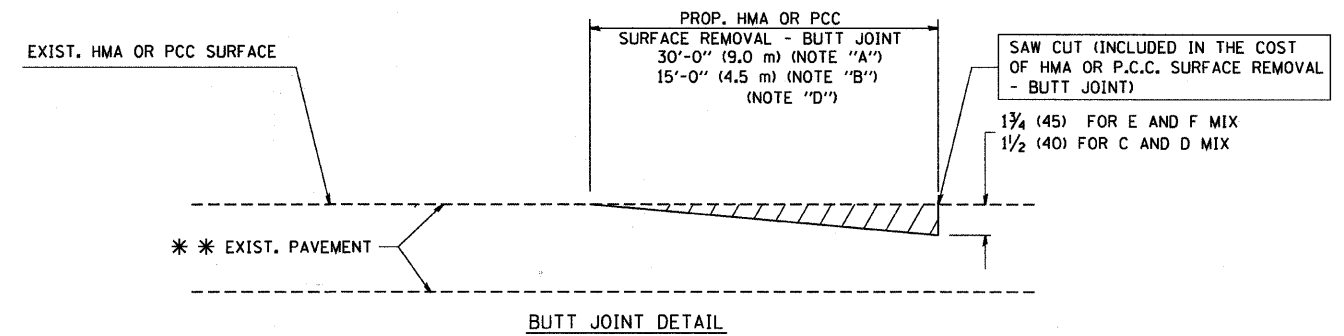
OPTION 2

TYPICAL TEMPORARY RAMP

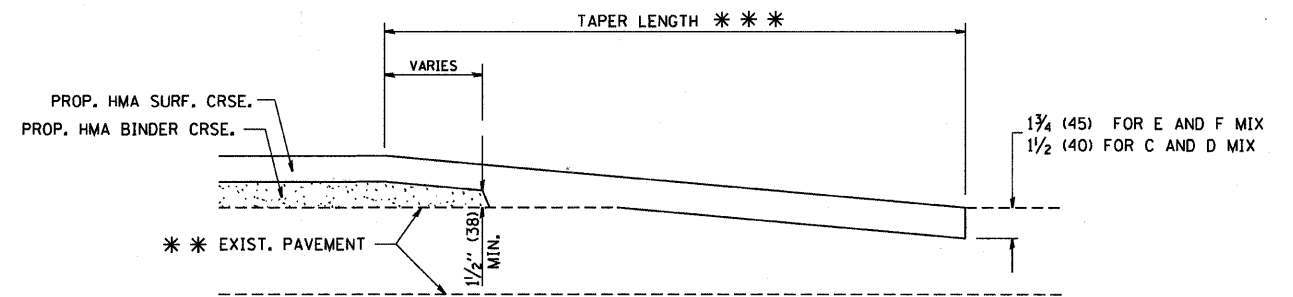


BUTT JOINT AND
HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

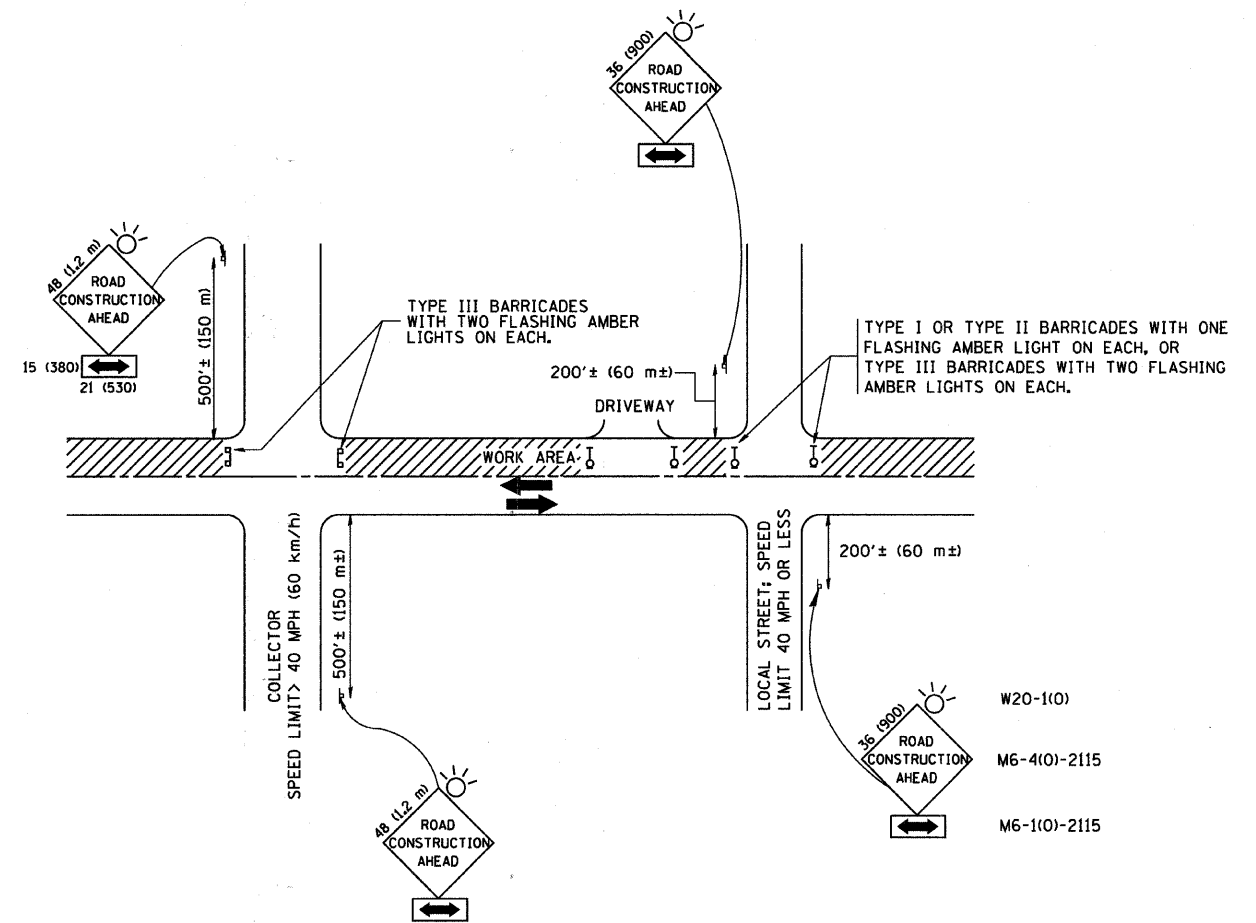
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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50,0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1342	10-00036-00-RS	KANE	21	15
BD400-05 BD32			CONTRACT NO. 63469	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (608)				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

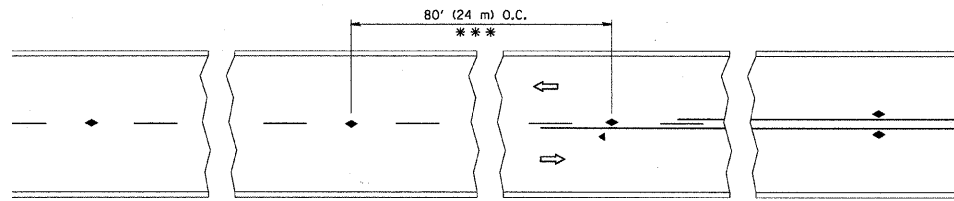
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		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

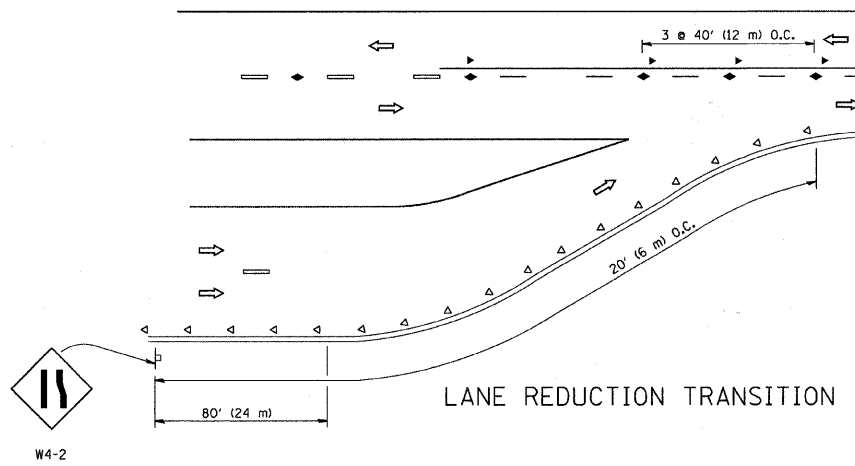
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 63469	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (608)				

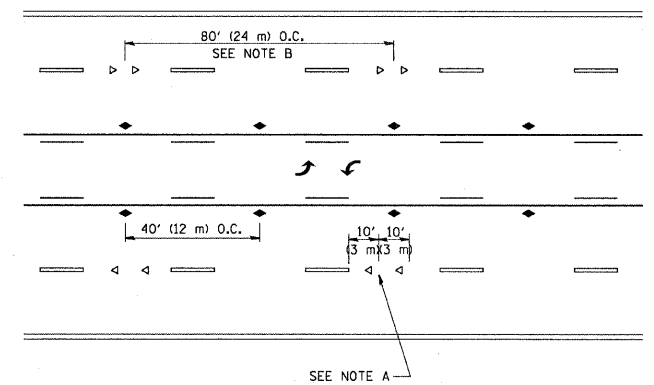


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

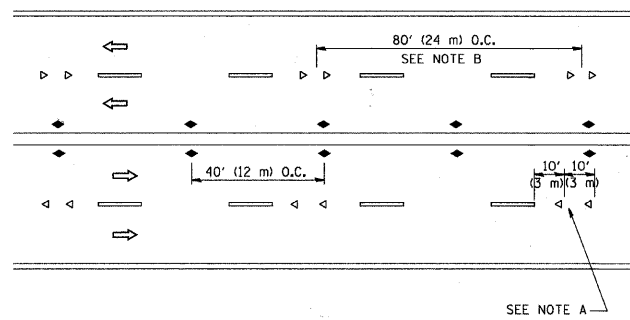
TWO-LANE/TWO-WAY



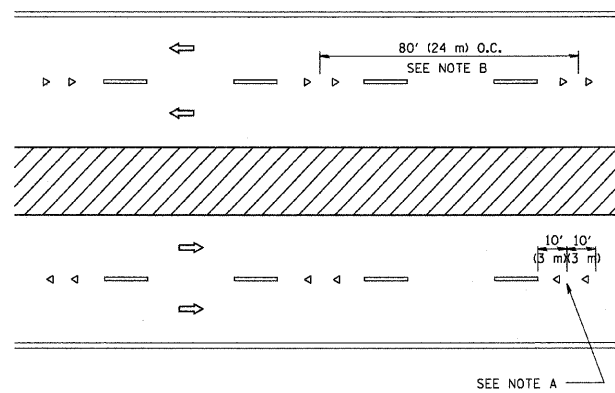
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

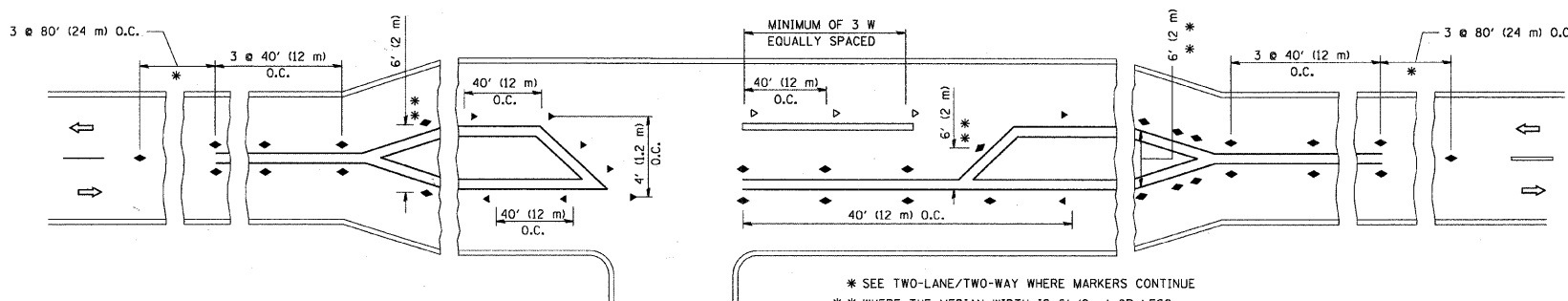
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

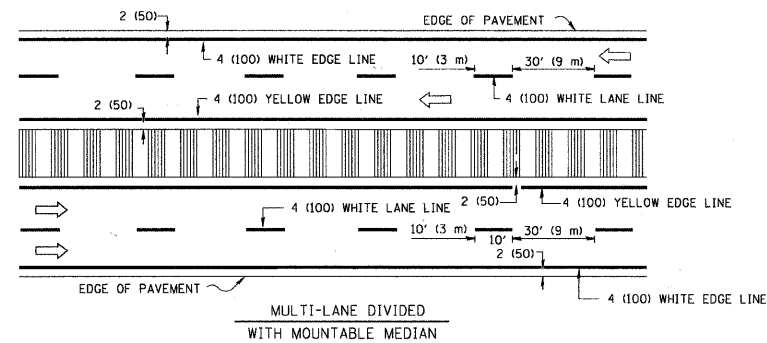
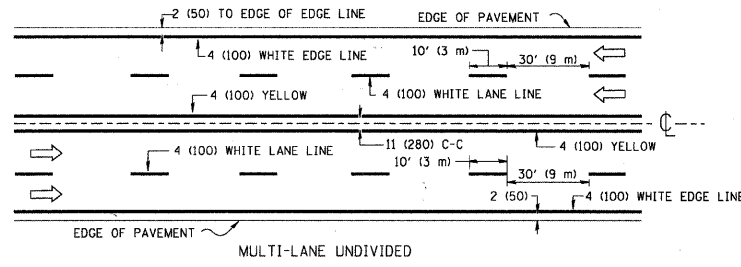
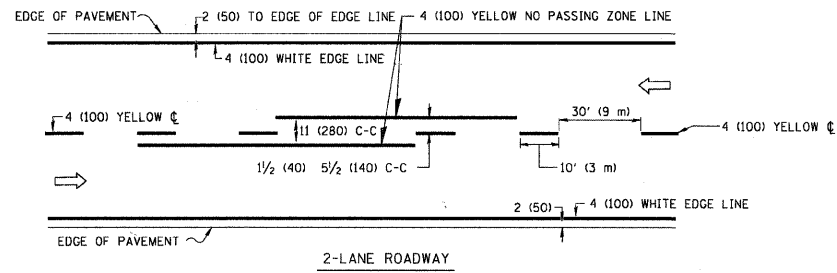


LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

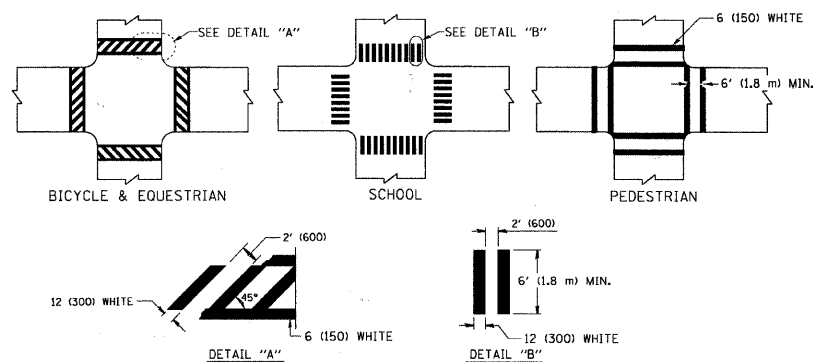
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drivakosgn	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		CHECKED -	REVISED - T. RAMMACHER 01-06-00		TC-11			CONTRACT NO. 63469				
		DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (608)				
								C-91-437-10				

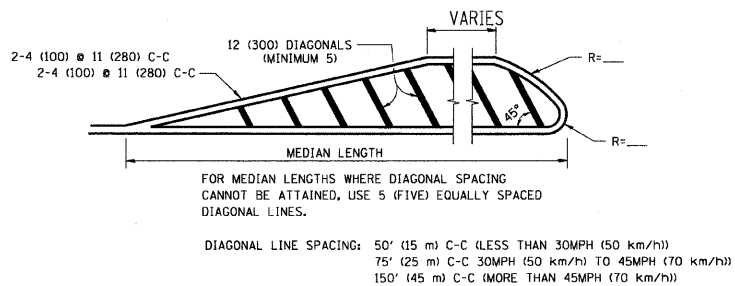
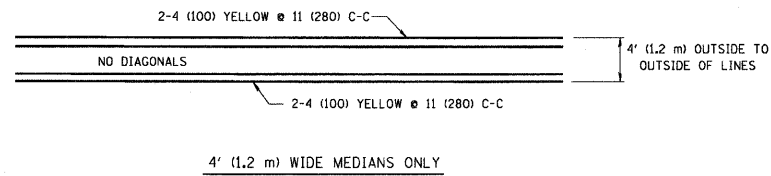


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

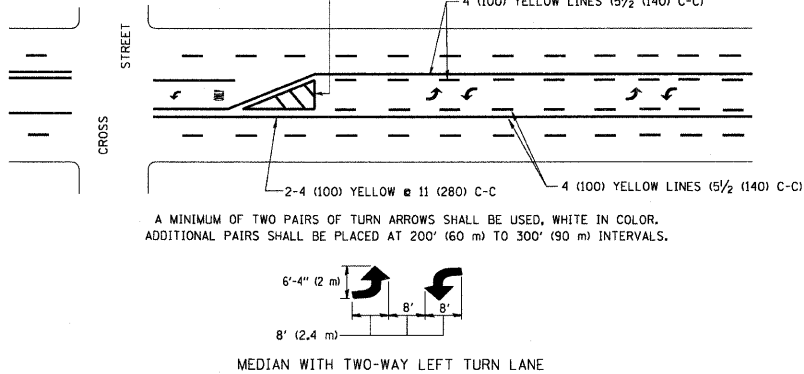
TYPICAL LANE AND EDGE LINE MARKING



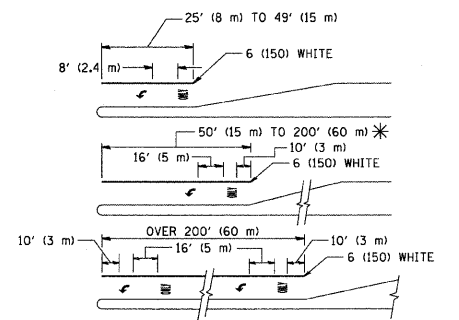
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING

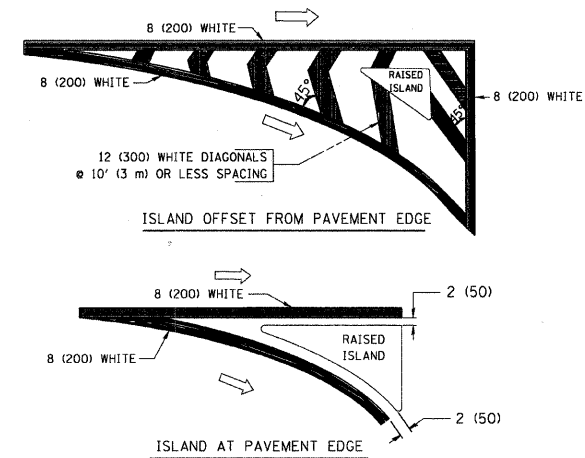


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

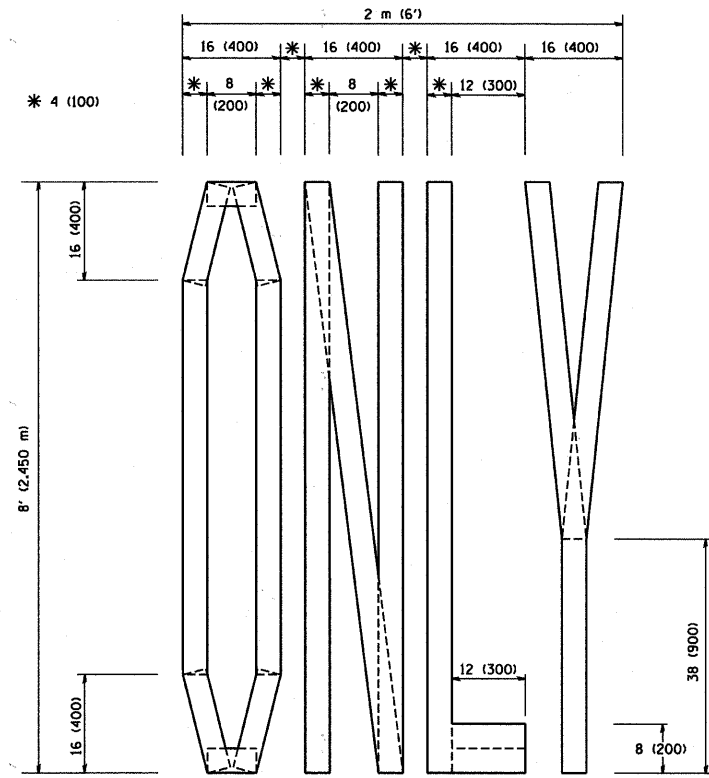


TYPICAL ISLAND MARKING

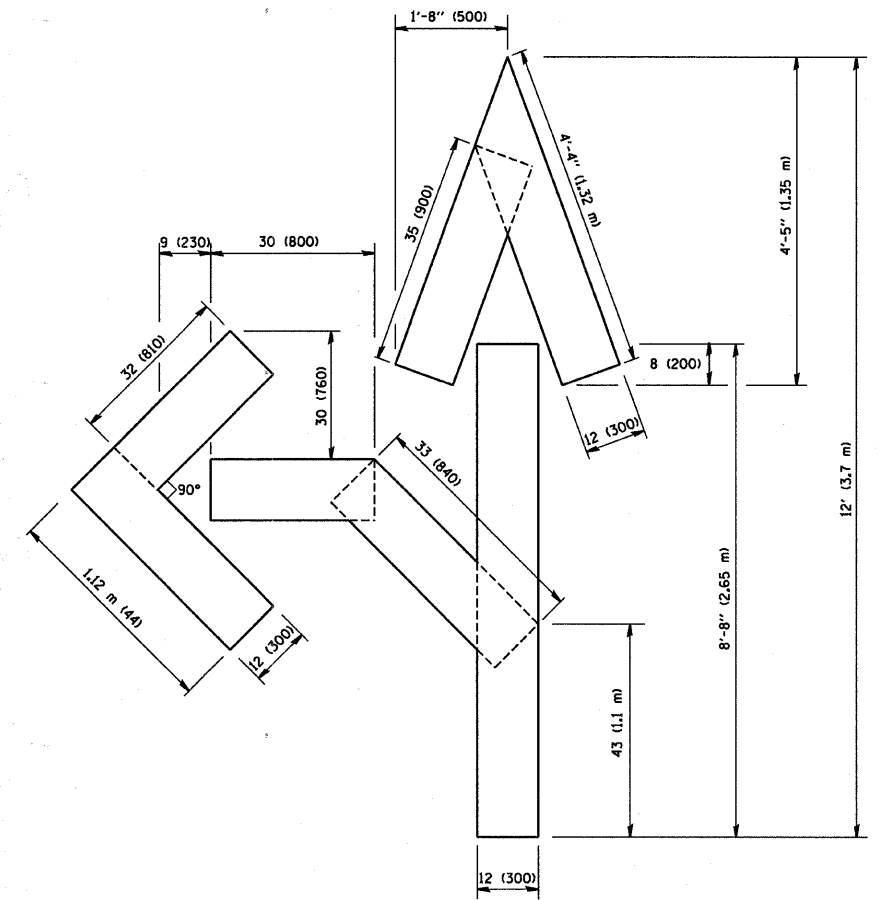
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

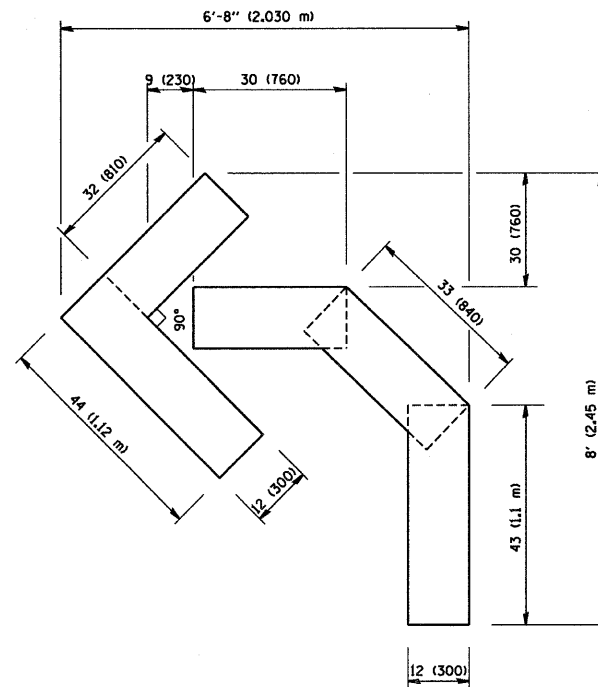
All dimensions are in inches (millimeters) unless otherwise shown.



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\col6.dgn	USER NAME = geglienobt	DESIGNED - DRAWN -	REVISED -T. RAMMACHER 06-05-96 REVISED -T. RAMMACHER 11-04-97
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98	
PLOT DATE = 1/4/2000	DATE = 09-18-94	REVISED -E. GOMEZ 08-28-00	

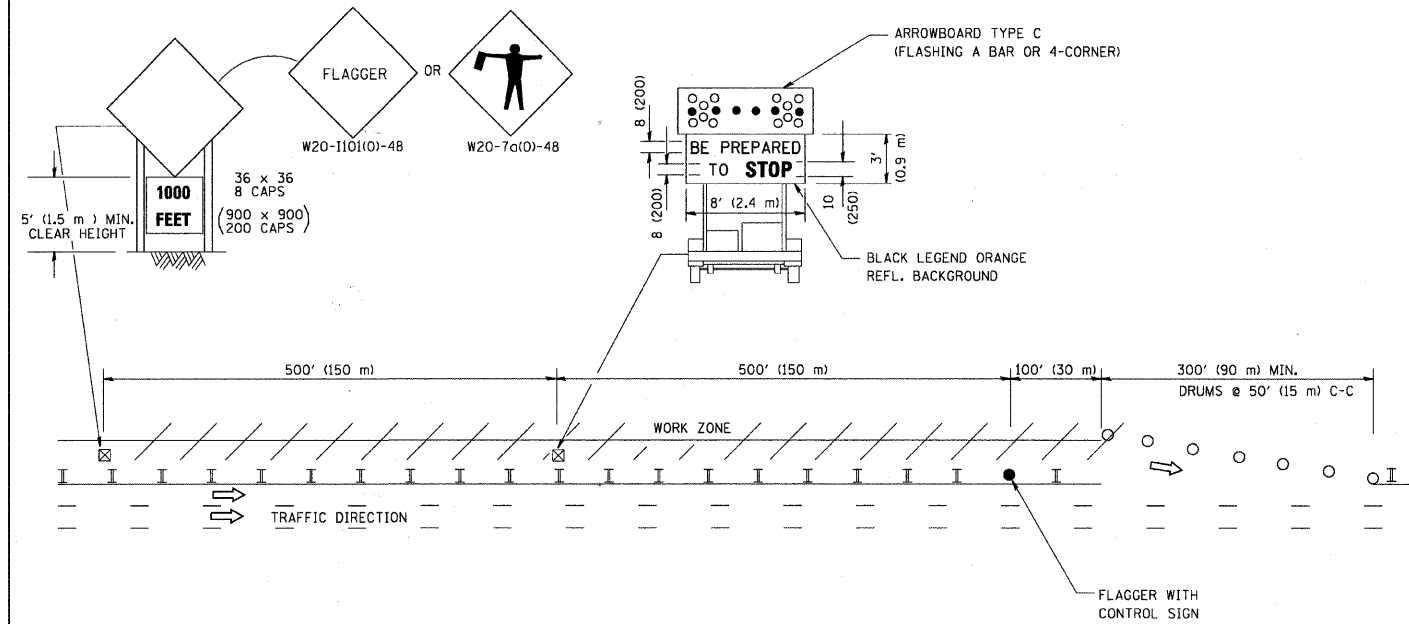
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			
SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA. TO STA.

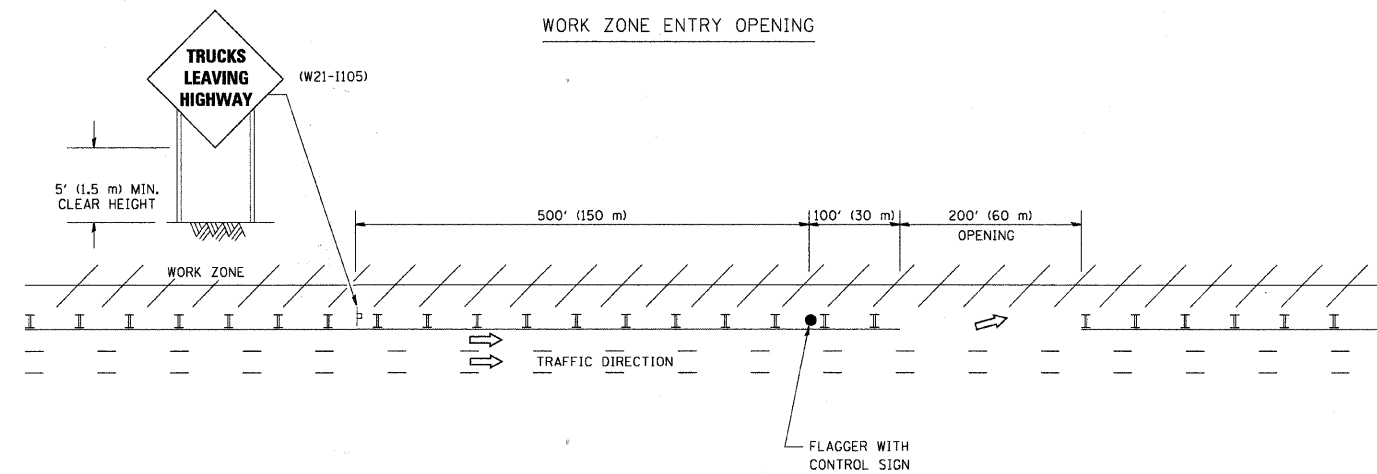
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1342	10-00036-00-RS	KANE	21	19
TC-16			CONTRACT NO. 63469	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (608)				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. THE ARROWBOARD, THE FLAGGER AHEAD SIGN AND THE TRUCKS LEAVING HIGHWAY SIGN SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE EXIT OPENINGS SHOULD BE A MINIMUM OF ONE HALF MILE APART.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

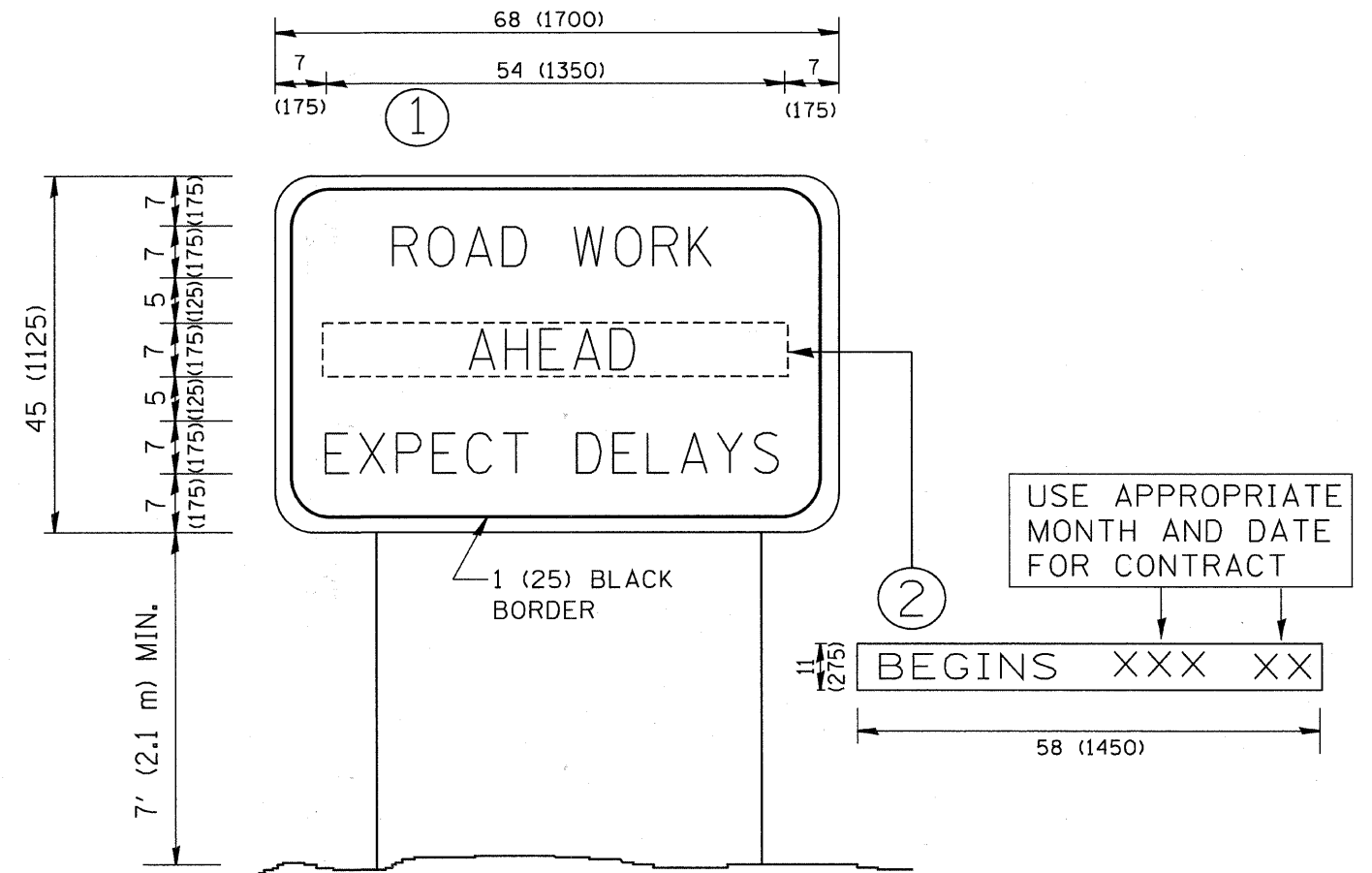
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		DRAWN -	REVISED - J.A.F. 02-06
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - S.P.B. 01-07
	PLOT DATE = 1/26/2010	DATE -	REVISED - S.P.B. 12-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 1342	SECTION 10-00036-00-RS	COUNTY KANE	TOTAL SHEETS 21	SHEET NO. 20
TC-18		CONTRACT NO. 63469		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (608) C-91-437-10				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dstato\22x34\to22.dgn	USER NAME = geglienobt	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN			F.A.U. RTE. 1342	SECTION 10-00036-00-RS	COUNTY KANE	TOTAL SHEETS 21	SHEET NO. 21
		DRAWN -	REVISED - R. MIRS 12-11-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-22		CONTRACT NO. 63469	
		PLOT SCALE = 60,000' / IN.	REVISED - T. RAMMACHER 02-02-99		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (608)							
		PLOT DATE = 1/4/2008	REVISED - C. JUCIUS 01-31-07		C-91-437-10							