

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

FAU 1599 / (WENTWORTH/ATLANTIC AVENUE)
142ND STREET (FAU 1599) TO 144TH STREET (FAU 1601)
LOCAL AGENCY PAVEMENT PRESERVATION
STREET RESURFACING
PROJECT: ARA-9003(606)
SECTION NO.: 10-00120-00-RS
JOB NO.: C-91-428-10
VILLAGE OF RIVERDALE
COOK COUNTY

F. A. D. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1599	10-00120-00-RS	COOK	12	1
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT ARA-9003(606)	

CONTRACT #63473

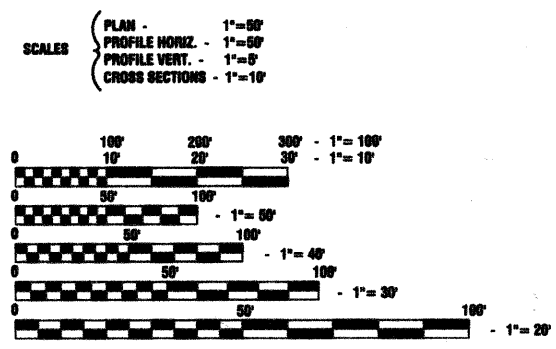
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- 000001-05 STANDARD STANDARDS, ABBREVIATIONS AND PATTERNS
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- 701501-05 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
- 701701-06 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701801-04 LANE CLOSURE, MULTILANE, 1W OR 2W, CROSSWALK OR SIDEWALK CLOSURE,
- 701901-01 TRAFFIC CONTROL DEVICES
- 780001-02 TYPICAL PAVEMENT MARKINGS

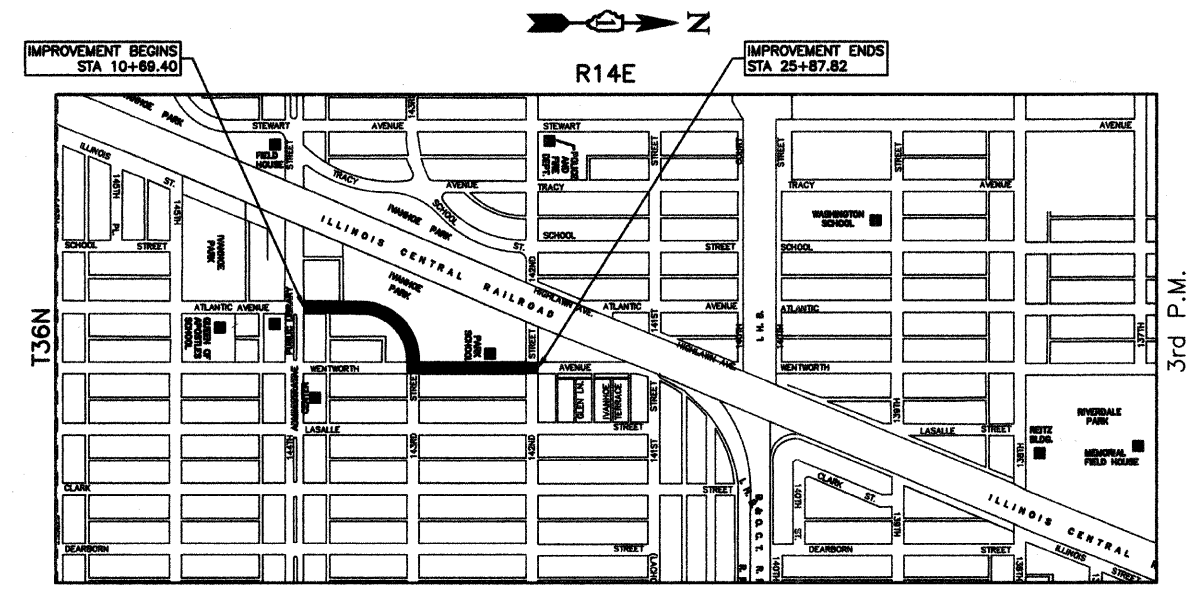
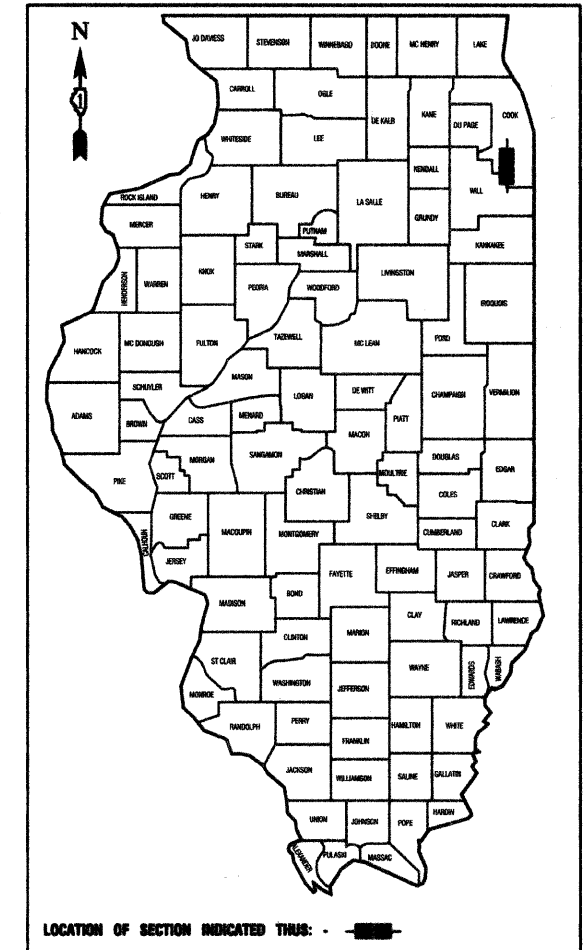
FAU 1599 - ATLANTIC AVENUE TO WENTWORTH AVENUE
DESIGN DESIGNATION - COLLECTOR
ADT=3,000 (2009)
POSTED SPEED LIMIT = 25 MPH
DESIGN SPEED = 25 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J. U. L. I. E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1 - 800 - 892 - 0123 or 811

CONTRACT NO. 63473



THORNTON TOWNSHIP

LOCATION MAP

- INDICATES PROPOSED IMPROVEMENT
GROSS LENGTH= 1,518.42 FEET = 0.288 MILES
NET LENGTH= 1,518.42 FEET = 0.288 MILES

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

Approved: 4/26/2010
Mayor, Village of Riverdale

Passed: MAY 27, 2010
District 1 Engineer of Local Roads & Streets

Released for Bid Based on Limited Review: MAY 27, 2010
Deputy Director of Highways, Region 1 Engineer

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PREPARED BY OR UNDER THE DIRECT SUPERVISION OF:
4/27/2010
REGISTERED PROFESSIONAL ENGINEER
LICENSE EXPIRES: 11/30/11

I.D.O.T. FIELD ENGINEER: MELCHOR MANGOBA 847-705-4408
CONSULTANTS: ROBINSON ENGINEERING, LTD. 708-331-6700
CONTACT ENGINEER: JEFF KOZA

SUMMARY OF QUANTITIES					PAVEMENT	MARKINGS
S.I.	CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE	
					1000	SFTY-1D
	20200100	EARTH EXCAVATION	CU YD	4	4	
	25200650	SODDING, SALT TOLERANT (SPECIAL)	SQ YD	354	354	
	31101200	SUB-BASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	6	6	
	40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	5	5	
	40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	546	546	
	40600300	AGGREGATE (PRIME COAT)	TON	12	12	
	40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	229	229	
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	96	96	
	40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	459	459	
	42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	20	20	
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1370	1370	
	42400800	DETECTABLE WARNINGS	SQ FT	136	136	
	44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	5465	5465	
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	358	358	
	44000235	HOT-MIX ASPHALT SIDEWALK REMOVAL	SQ FT	1293	1293	
	44000600	SIDEWALK REMOVAL	SQ FT	576	576	
	44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	300	300	
	44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	100	100	
	44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	150	150	
	44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	200	200	
	44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	750	750	
	56109210	WATER VALVES TO BE ADJUSTED	EACH	1	1	
	60250200	CATCH BASINS TO BE ADJUSTED	EACH	11	11	
	60255500	MANHOLES TO BE ADJUSTED	EACH	4	4	
	67100100	MOBILIZATION	L SUM	1	1	
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	402	402	
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	109	109	
*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	56	56	
	Z0004522	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6"	SQ YD	50	50	
	Z0004510	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	288	288	

* - INDICATES SPECIALTY ITEMS

GENERAL NOTES:

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED).
- UTILITIES INDICATED ON THE PLANS ARE PROVIDED FOR THE CONTRACTORS USE AND ARE BASED UPON INFORMATION AVAILABLE AT THE TIME OF THE ADVERTISEMENT FOR BIDS. THE OWNER AND ENGINEER DO NOT GUARANTEE THE ACCURACY OF UTILITIES INFORMATION.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- SCHEDULES INCLUDED IN THE PLANS HAVE BEEN PREPARED FROM FIELD NOTES. EXACT LOCATIONS FOR PATCHING SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER. THE CONTRACTOR SHALL MILL THE EXISTING PAVEMENT PRIOR TO COMMENCING PATCHING OPERATIONS.
- THE THICKNESS OF HOT-MIX ASPHALT MIXTURE STATED IN THE SPECIFICATIONS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HOT-MIX ASPHALT SURFACE IS PLACED.
- ITEMS OF WORK LISTED IN THE SUMMARY OF QUANTITIES WHICH ARE NOT SPECIFICALLY INDICATED IN THE PLANS SHALL BE PERFORMED AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- THE TOP OF ALL STRUCTURES SHALL BE FLUSH WITH THE ADJACENT SURFACE OR AT THE INDICATED ELEVATIONS SHOWN ON THE PLANS.
- WHEN, IN THE CONSTRUCTION OPERATION, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR OTHER DRAINAGE STRUCTURES SO THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH DAY BY THE CONTRACTOR AT HIS EXPENSE. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM ALL DIRT AND DEBRIS. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE APPROXIMATE LOCATION OF KNOWN PUBLIC UTILITIES ARE SHOWN ON THE PLANS. HOWEVER, THE DEPARTMENT DOES NOT GUARANTEE ITS ACCURACY. PRIOR TO COMMENCING OPERATIONS ON THE PROJECT WHICH MAY IN ANY WAY CREATE THE POSSIBILITY OF INVOLVEMENT WITH EXISTING UTILITIES, THE CONTRACTOR SHALL CONTACT THE FIRM (OR COMMUNITY) INVOLVED. ADJUSTMENT OF ALL PUBLIC UTILITIES WITHIN THE LIMITS OF THIS IMPROVEMENT WILL BE DONE BY THE RESPECTIVE OWNERS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED DUE TO DELAYS OR INCONVENIENCE CAUSED BY THESE ADJUSTMENTS IN ACCORDANCE WITH ARTICLES 105.07(b) AND 107.20. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY LOCATIONS OF UNDERGROUND INSTALLATION BEFORE STARTING CONSTRUCTION OPERATIONS.
- COMBINATION CONCRETE CURB AND GUTTER SHALL BE INSTALLED WITH A THICKNESS EQUAL TO NINE INCHES (9").
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON VILLAGE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
- BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH TYPE I OR TYPE II BARRICADE USED. (ONE (1) WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL).
- NO LONGITUDINAL PAVING JOINT OR SEAMS ARE ALLOWED WITHIN THE DRIVING LANES. ALL LONGITUDINAL PAVING JOINTS OR SEAMS WILL BE BETWEEN THE DRIVING LANES.
- ALL PAVEMENT, CURB AND SIDEWALK REMOVALS SHALL BE MADE BY MEANS OF STRAIGHT SAW CUT JOINT. THE COST FOR SAW CUTTING SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.
- CONTRACTOR SHALL BE RESPONSIBLE AT ALL TIMES FOR TRAFFIC CONTROL AND PROTECTION IN ACCORDANCE WITH THE LATEST IDOT STANDARD SPECIFICATIONS ADOPTED, THE LATEST EDITION OF THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, AND THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS.
- THE CONTRACTOR SHALL MAKE EVERY ATTEMPT NOT TO DAMAGE EXISTING TREES.
- THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS THROUGHOUT THE PROJECT LIMITS AT ALL TIMES. IF DRIVEWAY ACCESS MUST BE RESTRICTED, THE CONTRACTOR SHALL NOTIFY THE RESIDENT IN WRITING 24 HOURS IN ADVANCE.
- SAW CUT JOINTS - THE REMOVAL AND/OR REPLACEMENT OF ANY DRIVEWAYS, ALLEY ENTRANCES, PAVEMENT, CURB, SIDEWALK, HOT-MIX ASPHALT PARKWAYS, ETC. SHALL BE ACCOMPLISHED BY MEANS OF A SAW CUT JOINT AT THE DIRECTION OF THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE VARIOUS ITEMS.
- NEW COMBINATION CONCRETE CURB AND GUTTER SHALL BE PAINTED YELLOW AT LOCATIONS WHERE THE EXISTING CURB AND GUTTER THAT WAS REMOVED HAD BEEN PAINTED YELLOW. THE YELLOW PAINT SHALL BE MANUFACTURED BY DAVIES IMPERIAL COATINGS, INC. AND SHALL BE MODEL NUMBER "4902 YELLOW LATEX" OR APPROVED EQUAL. THE PAINT SHALL BE APPLIED PER THE MANUFACTURER'S SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE COMBINATION CONCRETE CURB AND GUTTER, REMOVAL AND REPLACEMENT PAY ITEM.
- DETECTABLE WARNINGS SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 424.09 OF THE STANDARD SPECIFICATIONS EXCEPT AS MODIFIED HEREIN. STAMPING OF THE CONCRETE WILL BE ALLOWED. THE STAMP USED SHALL CONFORM TO CURRENT ADA STANDARDS AND THE IDOT STANDARD 424001-05. THE DIMENSIONS OF THE STAMP SHALL BE FOUR FEET WIDE MINIMUM BY TWO FEET IN LENGTH (4' BY 2'). THE CONCRETE TO BE PLACED AS DETECTABLE WARNINGS SHALL BE RED IN COLOR AND ALREADY MIXED INTO THE CONCRETE. ONLY THE STAMPED PORTION OF THE SIDEWALK SHALL BE RED. THE USE OF POWDERS OR DYES ON SITE TO ACHIEVE A CONTRAST IN COLOR IS PROHIBITED. DETECTABLE WARNINGS SHALL BE MEASURED IN PLACE AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT (SQ FT).
- THE LOCATION OF THE WORK TO BE PERFORMED SHALL BE AS MARKED IN THE FIELD BY THE ENGINEER. THE EXACT LOCATIONS OF THE BUTT JOINTS, HOT-MIX ASPHALT SURFACE REMOVAL, CURB AND GUTTER REMOVAL, SIDEWALK REMOVAL SHALL BE PROVIDED DURING CONSTRUCTION BY THE RESIDENT ENGINEER.

FILE NAME = 0904-QJAN-01 - Q01

USER NAME =	DESIGNED -- MPN	REVISED -- 05-04-10
	CHECKED -- JAK	REVISED -- 05-18-10
PLOT SCALE =	DRAWN -- MJD	REVISED --
PLOT DATE = 04-26-10	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WENTWORTH/ATLANTIC AVENUE STREET RESURFACING SUMMARY OF QUANTITIES & GENERAL NOTES		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		1599	10-00120-00-RS	COOK	12	2
SCALE: NONE		SHEET NO. 2 OF 12 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(606)
						CONTRACT NO. 63473

LEGEND

- ① EXISTING CURB & GUTTER TO BE REMOVED AT LOCATIONS AS SHOWN ON PLANS OR DIRECTED BY ENGINEER
- ② HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/4"
- ③ EXISTING HOT-MIX ASPHALT BASE COURSE TO REMAIN, ±6"
- ④ EXISTING P.C. CONCRETE SIDEWALK TO BE REMOVED AT LOCATIONS AS SHOWN ON PLANS OR DIRECTED BY ENGINEER
- ⑤ HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50 1 1/2"
- ⑥ COMBINATION CONCRETE CURB & GUTTER AT LOCATIONS AS SHOWN ON PLANS OR DIRECTED BY ENGINEER
- ⑦ LEVELING BINDER (MACHINE METHOD), N50, 3/4"
- ⑧ P.C. CONCRETE SIDEWALK, 5" AT LOCATIONS AS SHOWN ON PLANS OR DIRECTED BY ENGINEER
- ⑨ STRIP REFLECTIVE CRACK CONTROL TREATMENT AS DIRECTED BY ENGINEER

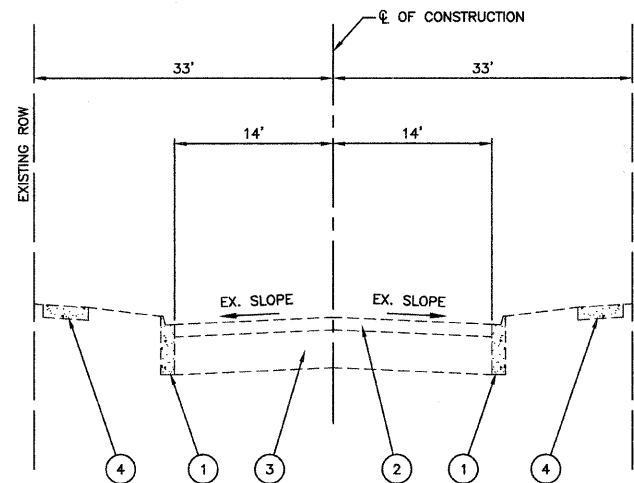
NOTE:
THE CONTRACTOR SHALL PERFORM MILLING OPERATIONS PRIOR TO PATCHING OPERATIONS.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

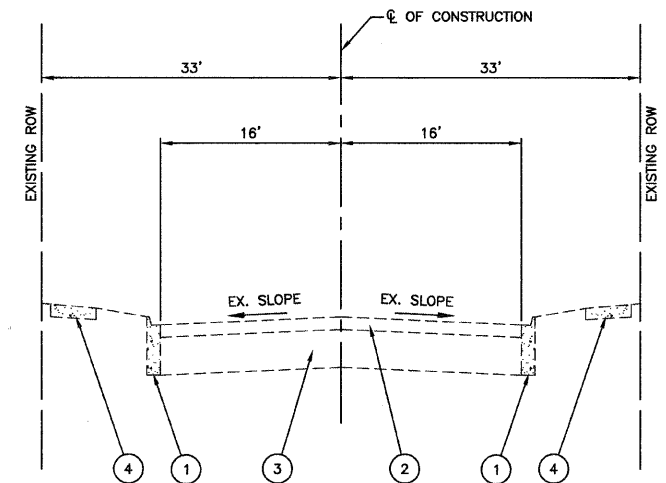
MIXTURE TYPE	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50, (IL 9.5 mm) 1 1/2"	4% @ 50 Gyr.
LEVELING BINDER (MACHINE METHOD), N50, (IL 9.5 mm) 3/4"	4% @ 50 Gyr.
HMA DRIVEWAY PAVEMENT, 6"	
HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50, (IL 9.5 mm) 2"	4% @ 50 Gyr.
HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N50, 4"	4% @ 50 Gyr.
HMA DRIVEWAY PAVEMENT, 3"	
HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50, (IL 9.5 mm) 3"	4% @ 50 Gyr.
PATCHING	
CLASS D PATCHES, TYPE II, III, IV 6" (HMA BINDER IL-19 mm)	4% @ 70 Gyr.

NOTE:
THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.

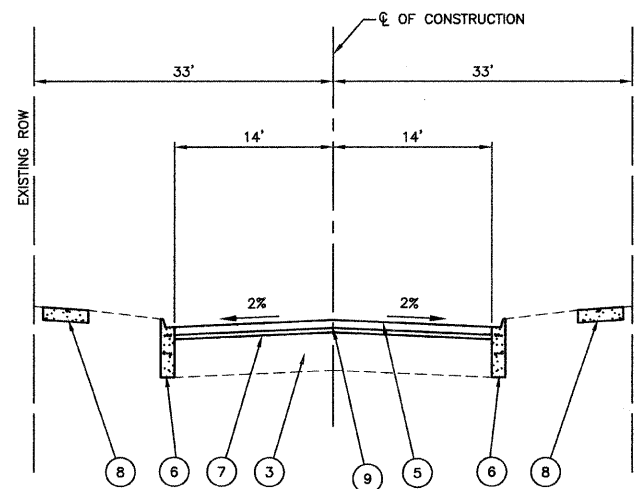
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.



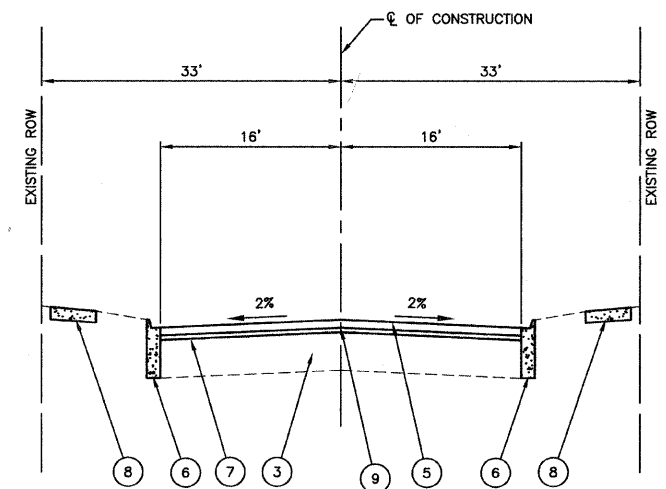
EXISTING TYPICAL CROSS SECTION
WENTWORTH AVENUE - 142ND STREET TO 143RD STREET
STATION 10+69.10 TO STATION 18+67.15



EXISTING TYPICAL CROSS SECTION
ATLANTIC AVENUE - 143RD STREET TO 144TH STREET
STATION 18+67.15 TO STATION 25+87.82



PROPOSED TYPICAL CROSS SECTION
WENTWORTH AVENUE - 142ND STREET TO 143RD STREET
STATION 10+69.10 TO STATION 18+67.15



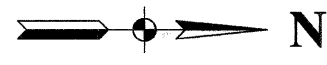
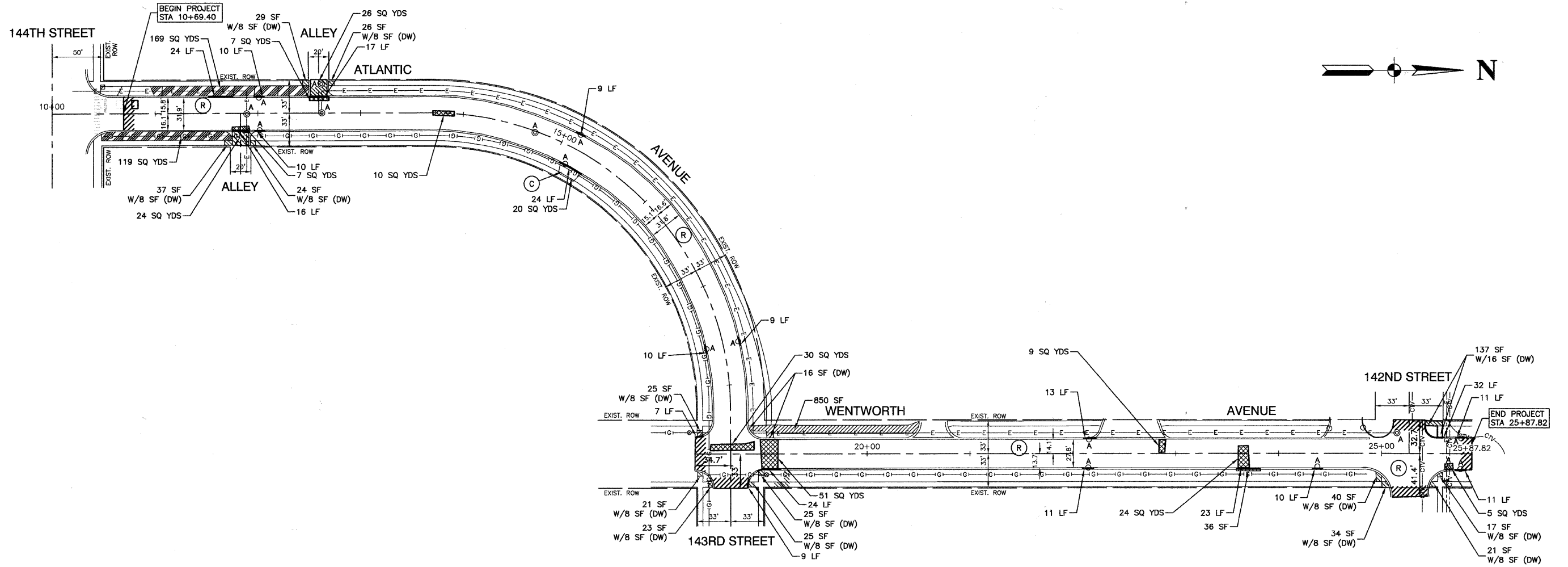
PROPOSED TYPICAL CROSS SECTION
ATLANTIC AVENUE - 143RD STREET TO 144TH STREET
STATION 18+67.15 TO STATION 25+87.82

FILE NAME = 06804-TYPX-01 - IDOT P01	USER NAME =	DESIGNED -- MPN	REVISED -- 05-04-10
		CHECKED -- JAK	REVISED --
	PLOT SCALE =	DRAWN -- MJD	REVISED --
	PLOT DATE = 04-26-10	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WENTWORTH/ATLANTIC AVENUE STREET RESURFACING TYPICAL CROSS SECTIONS		F.A.U. RTE. 1599	SECTION 10-00120-00-RS	COUNTY COOK	TOTAL SHEETS 12	SHEET NO. 3
SCALE: NONE	SHEET NO. 3 OF 12 SHEETS	STA.	TO STA.	CONTRACT NO. 63473		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(806)						

SECTION 4, TOWNSHIP 36, RANGE 14



LEGEND

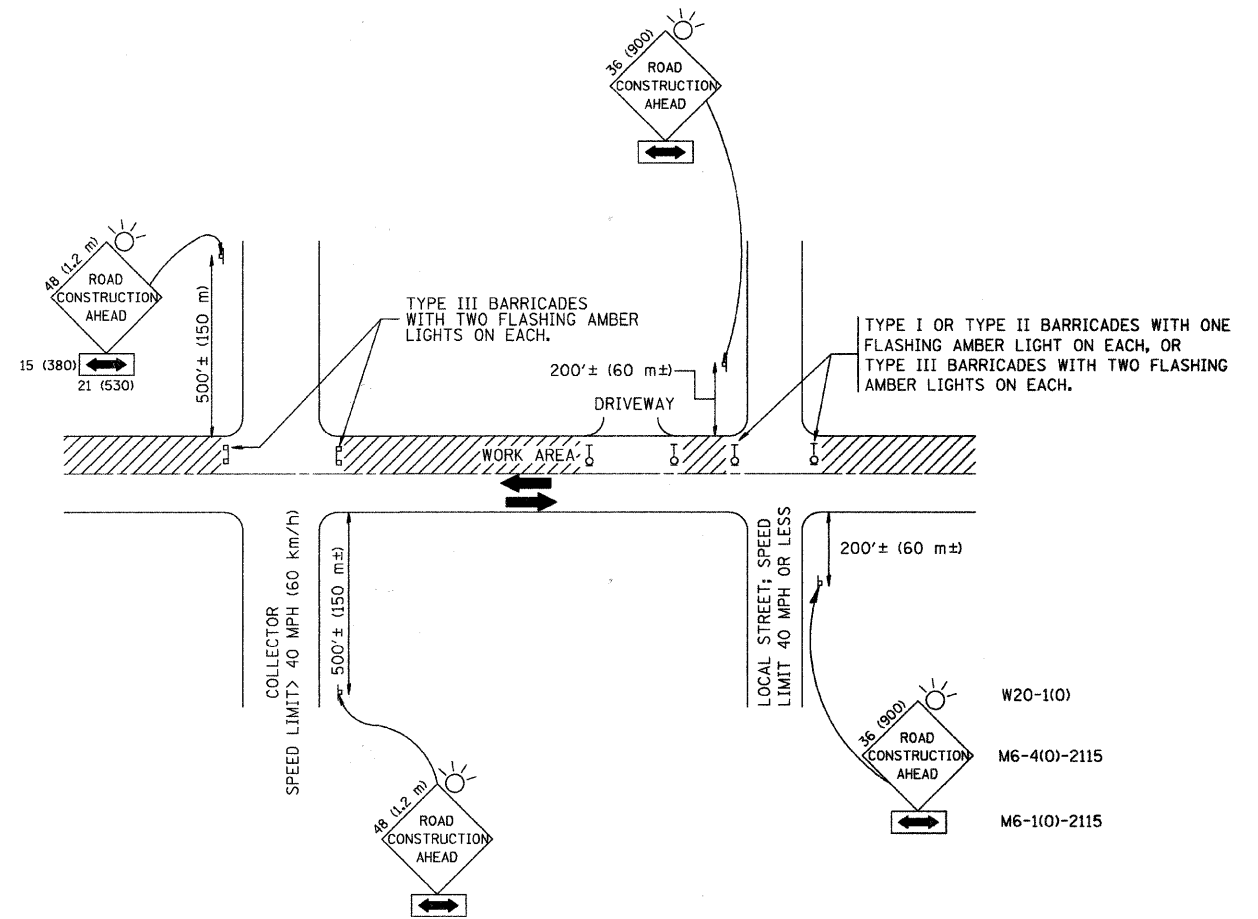
- CLASS D PATCH, 6"
- HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6"
- HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"
- PORTLAND CEMENT CONCRETE SIDEWALK, 5"
- CONCRETE SIDEWALK REMOVAL
- HOT-MIX ASPHALT SIDEWALK REMOVAL
- LIMITS OF RESURFACING / BUTT JOINT
- COMBINATION CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT
- A** STRUCTURE TO BE ADJUSTED
- (R)** HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 1 1/2"
LEVELING BINDER (MACHINE METHOD), N50 3/4"
- (C)** P.C.C. DRIVEWAY PAVEMENT, 6 INCH
- R** STRUCTURE TO BE RECONSTRUCTED
- (DW)** DETECTABLE WARNINGS

FILE NAME = 08604-PLAN-01 - IDOT P02	USER NAME =	DESIGNED — MPN	REVISED — 05-18-10
		CHECKED — JAK	REVISED —
	PLOT SCALE = 1"=50'	DRAWN — MJD	REVISED —
	PLOT DATE = 04-26-10	CHECKED — AG	REVISED —

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

WENTWORTH/ATLANTIC AVENUE STREET RESURFACING PAVEMENT PLAN			
SCALE: 1"=50'	SHEET NO. 4 OF 12 SHEETS	STA. 10+69.40 TO STA. 25+87.82	

F.A.U. RTE. 1999	SECTION 10-00120-00-RS	COUNTY COOK	TOTAL SHEETS 12	SHEET NO. 4
CONTRACT NO. 63473				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(606)				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

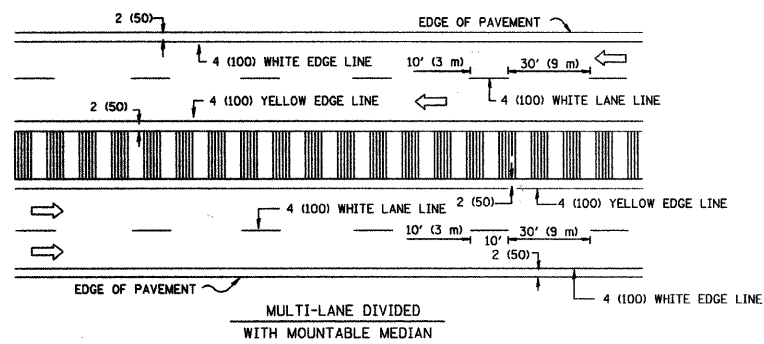
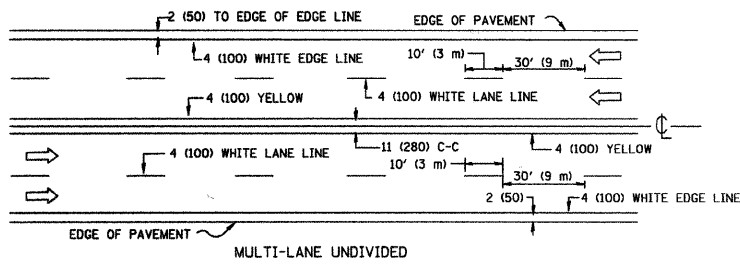
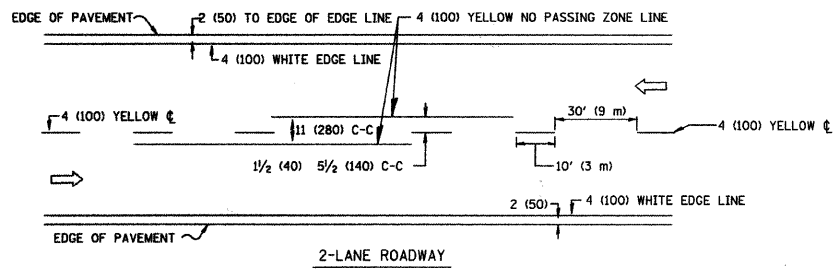
FILE NAME = 00904-DT13-01 - TC-10	USER NAME = geglianobt	DESIGNED -- LHA	REVISED -- J. OBERLE 10-18-95
		CHECKED --	REVISED -- A. HOUSEH 03-06-96
	PLOT SCALE = 50.000' / IN.	DRAWN --	REVISED -- A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	CHECKED -- 06-89	REVISED -- T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

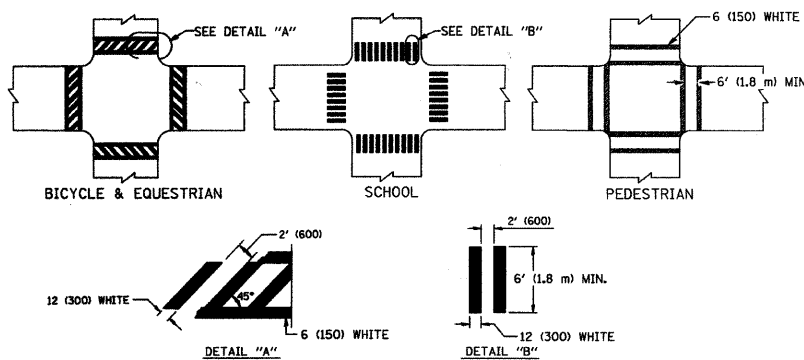
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: SHEET NO. 6 OF 12 SHEETS STA. TO STA.

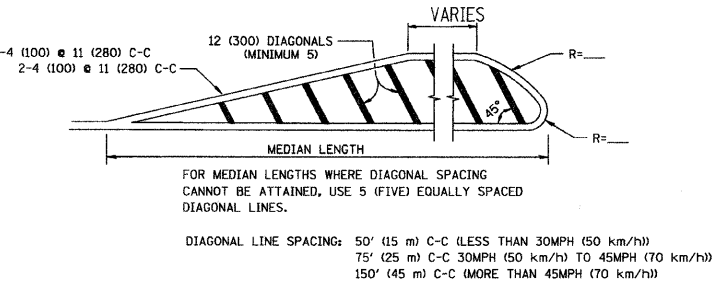
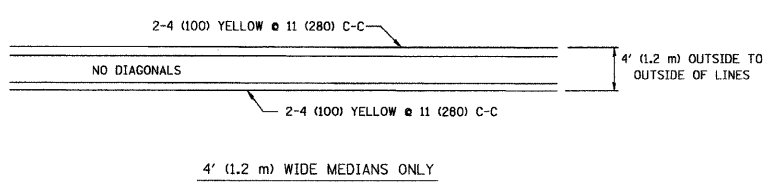
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1599	10-00120-00-RS	COOK	12	6
TC-10			CONTRACT NO. 63473	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(606)				



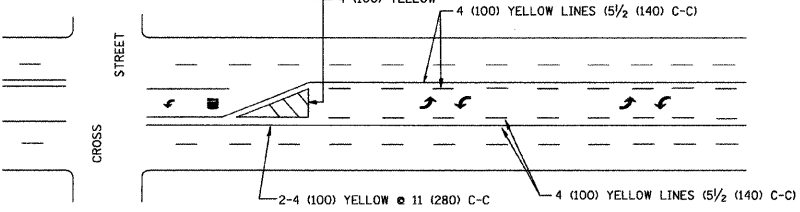
TYPICAL LANE AND EDGE LINE MARKING



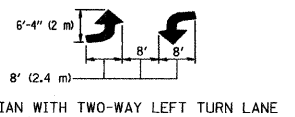
TYPICAL CROSSWALK MARKING



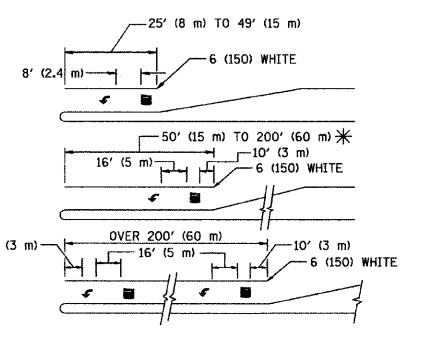
MEDIANS OVER 4' (1.2 m) WIDE



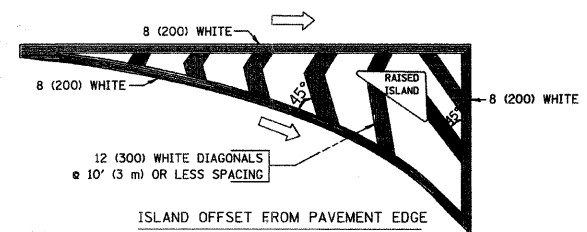
TYPICAL PAINTED MEDIAN MARKING



TYPICAL LEFT (OR RIGHT) TURN LANE



TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = 0804-DLS-02 - TC-13	USER NAME = drivakosgn	DESIGNED = EVERS	REVISED = T. RAMMACHER 10-27-94
		CHECKED =	REVISED = C. JUCIUS 09-09-09
PLOT SCALE = 58,800' / IN.	DRAWN =	REVISIONS =	
PLOT DATE = 9/9/2009	CHECKED = 03-19-90	REVISED =	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A.U. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		1599	10-00120-00-RS	COOK	12	7
SCALE:		SHEET NO. 7 OF 12 SHEETS		STA. N/A	TO STA. N/A	
		FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT ARA-6003(800)	

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

3" (75) MIN.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

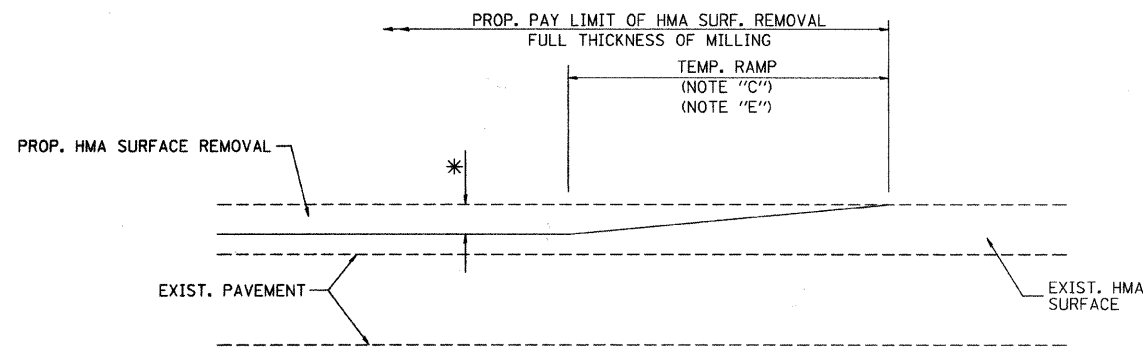
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

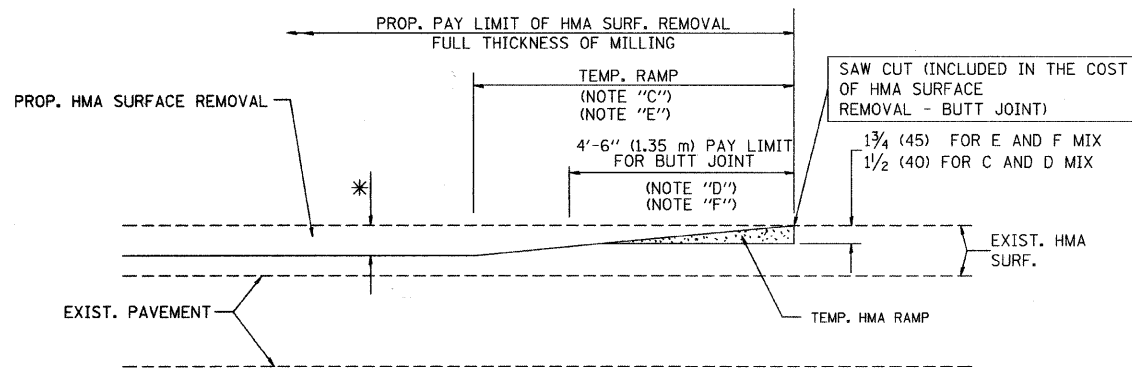
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = 00804-0718-05 - BD-24	USER NAME = dr-ivakosgn	DESIGNED = A. HOUSEH	REVISED = R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE. 1999	SECTION 10-00120-00-RS	COUNTY COOK	TOTAL SHEETS 12	SHEET NO. 8		
	PLOT SCALE = 5/8" = 1' IN.	CHECKED =	REVISED = A. ABBAS 03-21-97			SCALE:	SHEET NO. 8 OF 12 SHEETS	STA. TO STA.	BDS00-06 (BD-24) CONTRACT NO. 63473			
	PLOT DATE = 12/15/2009	DRAWN =	REVISED = M. GOMEZ 01-22-01			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(806)						
		CHECKED = 03-11-94	REVISED = R. BORO 12-15-09									



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

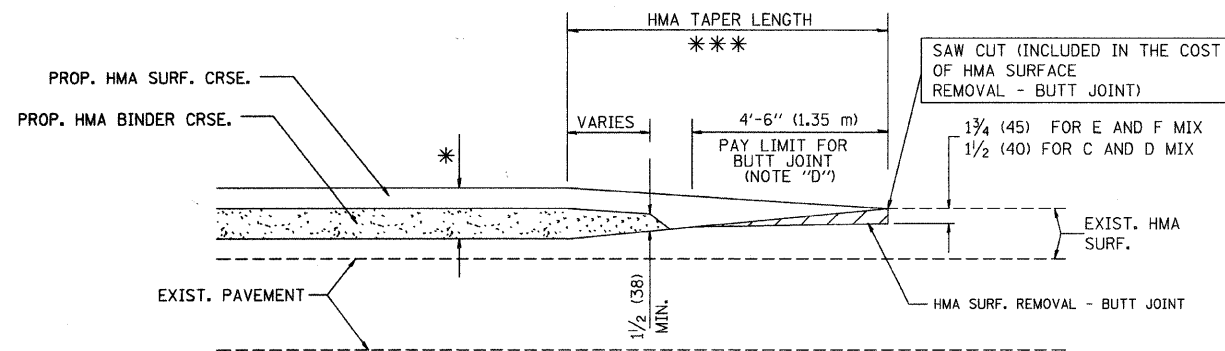
OPTION 1



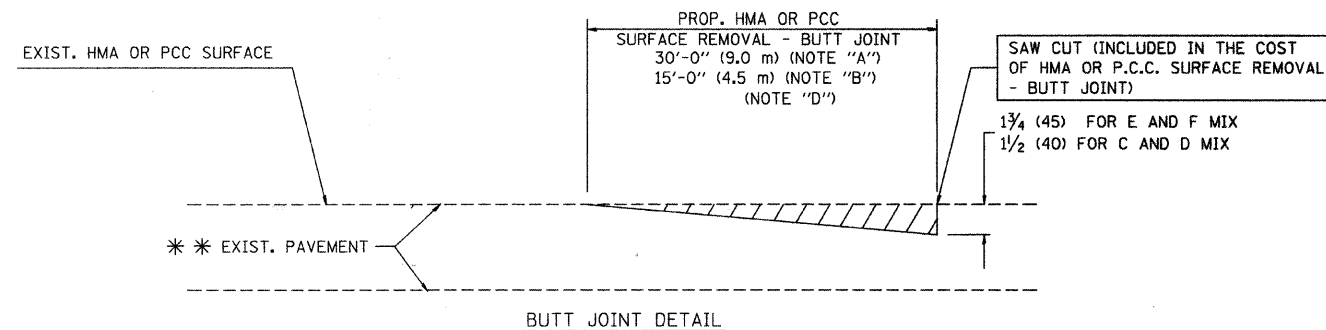
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

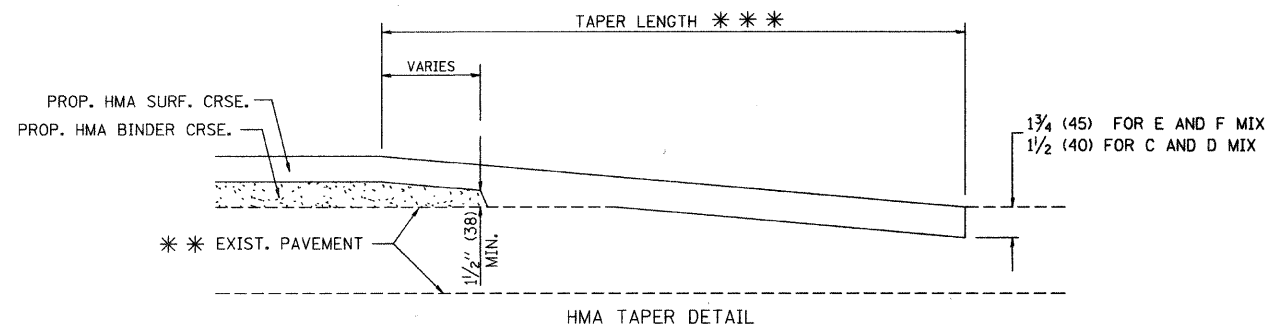
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = 08004-DTL8-08 - 8D-32

USER NAME = gegl1enobt	DESIGNED -- M. DE YONG	REVISED -- R. SHAH 10-25-94
	CHECKED --	REVISED -- A. ABBAS 03-21-97
PLOT SCALE = 58.8000' / IN.	DRAWN --	REVISED -- M. GOMEZ 04-06-01
PLOT DATE = 1/4/2008	CHECKED -- 06-13-90	REVISED -- R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

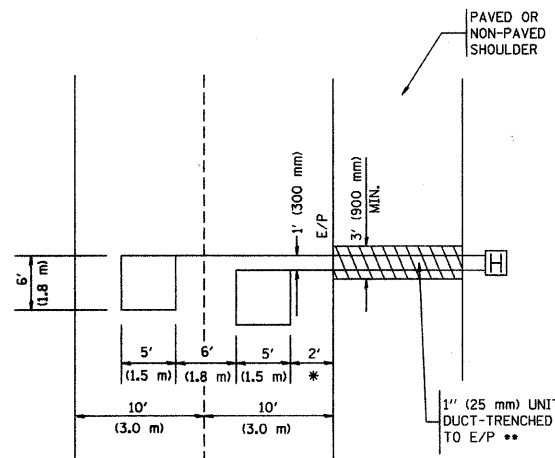
BUTT JOINT AND
HMA TAPER DETAILS

SCALE: SHEET NO. 9 OF 12 SHEETS STA. TO STA.

F.A.U. RTE. 1599	SECTION 10-00120-00-RS	COUNTY COOK	TOTAL SHEETS 12	SHEET NO. 9
BD400-05 BD32		CONTRACT NO. 63473		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(806)				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

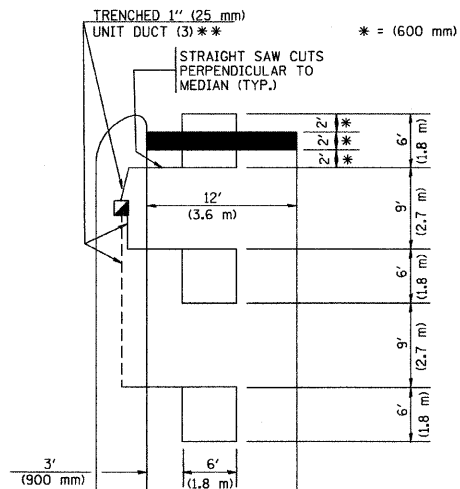


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.

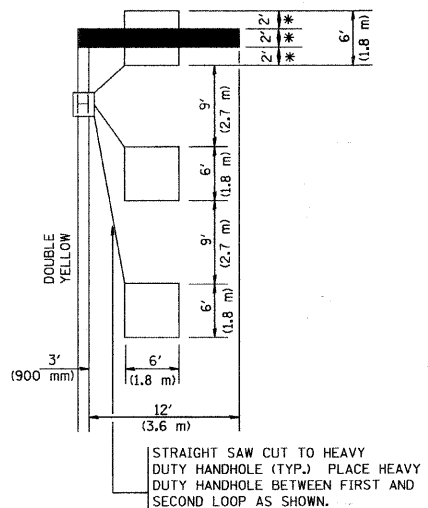


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

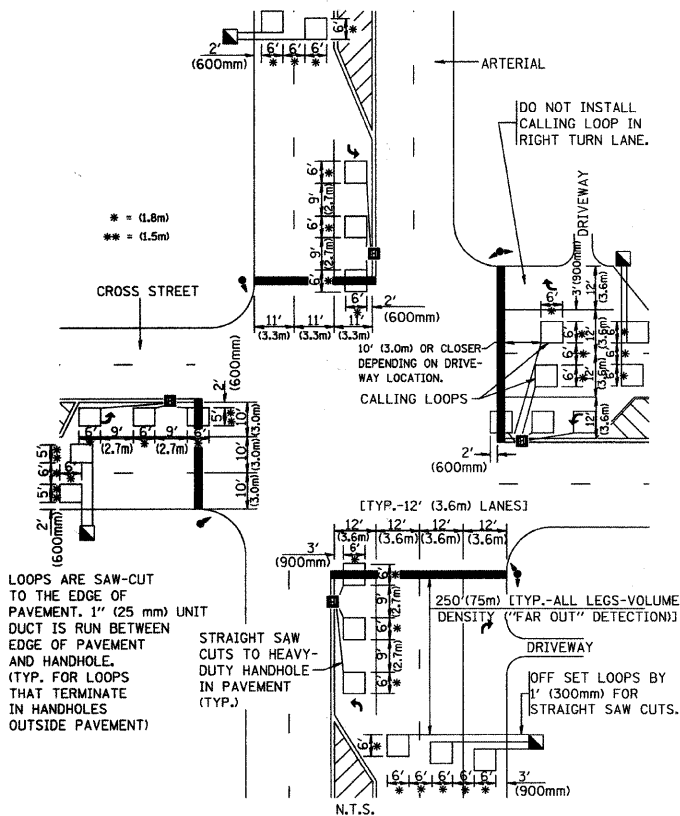
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

* = (600 mm)



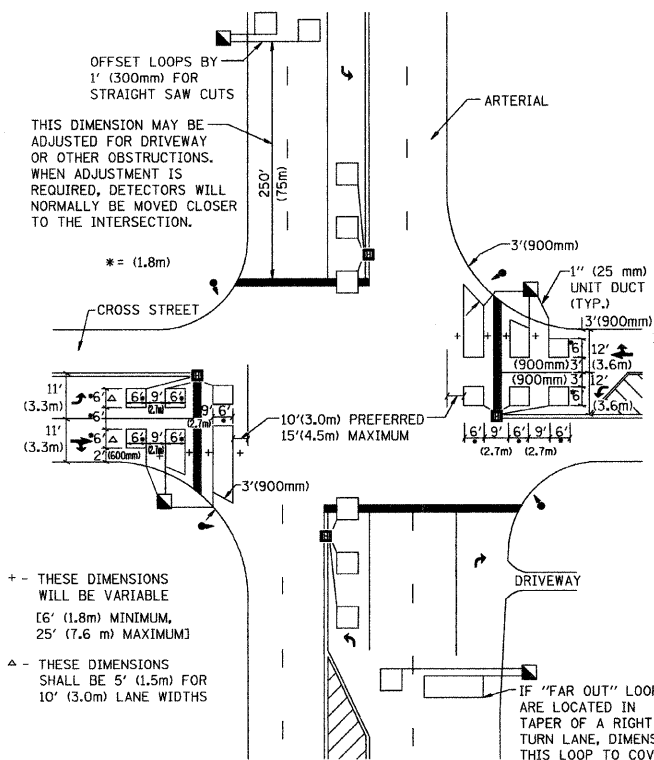
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = 06004-0718-07 - TS-07

USER NAME = geglianobt
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 1/4/2008

DESIGNED —
CHECKED —
DRAWN — R.K.F.
CHECKED —

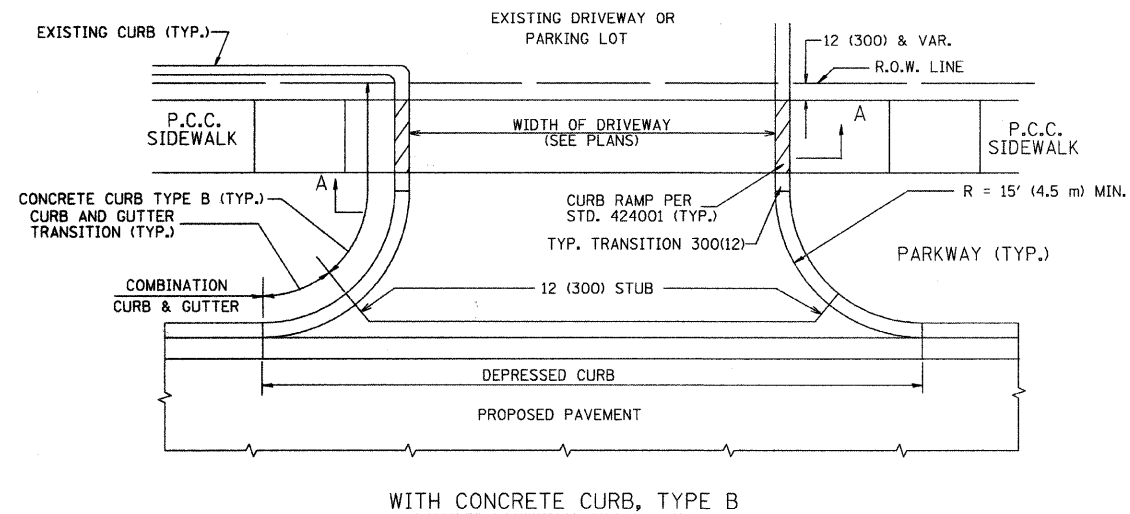
REVISED —
REVISED —
REVISED —
REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

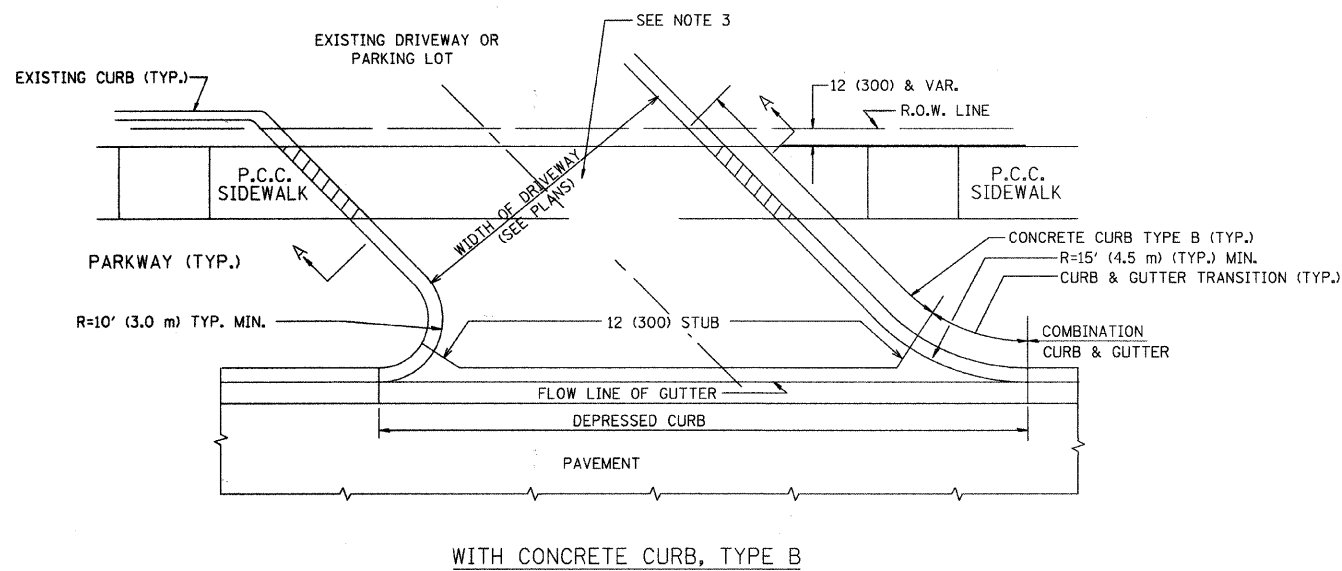
DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: SHEET NO. 10 OF 12 SHEETS STA. TO STA.

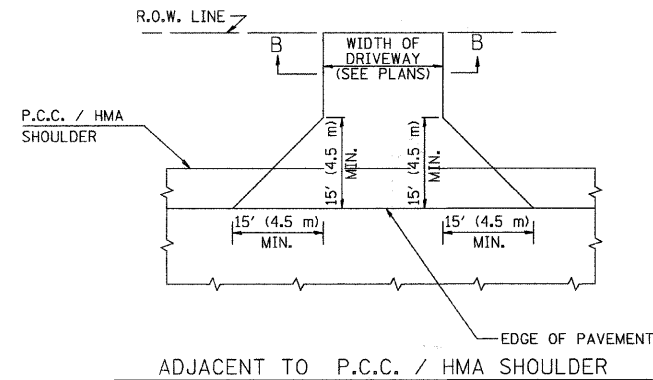
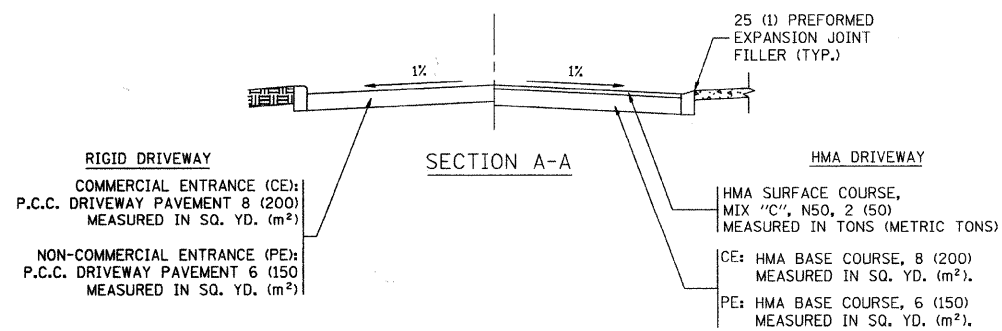
F.A.U. RTE. 1899	SECTION 10-00120-00-RS	COUNTY COOK	TOTAL SHEETS 12	SHEET NO. 10
TS-07			CONTRACT NO. 63473	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(806)				



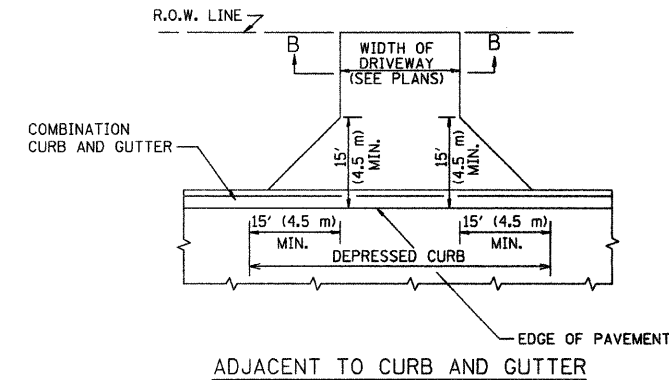
WITH CONCRETE CURB, TYPE B



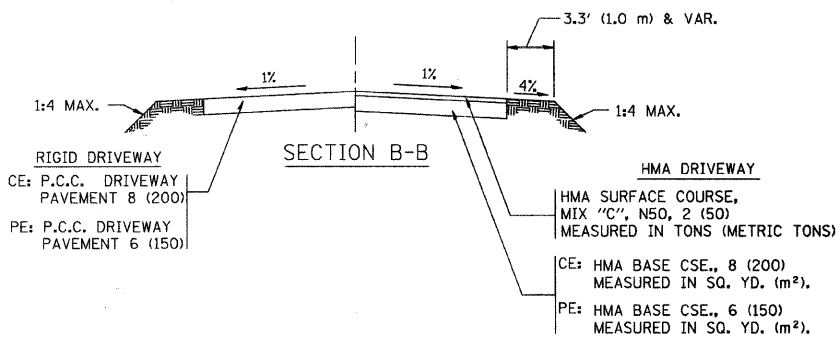
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE,
 MIX "C", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200)
 MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

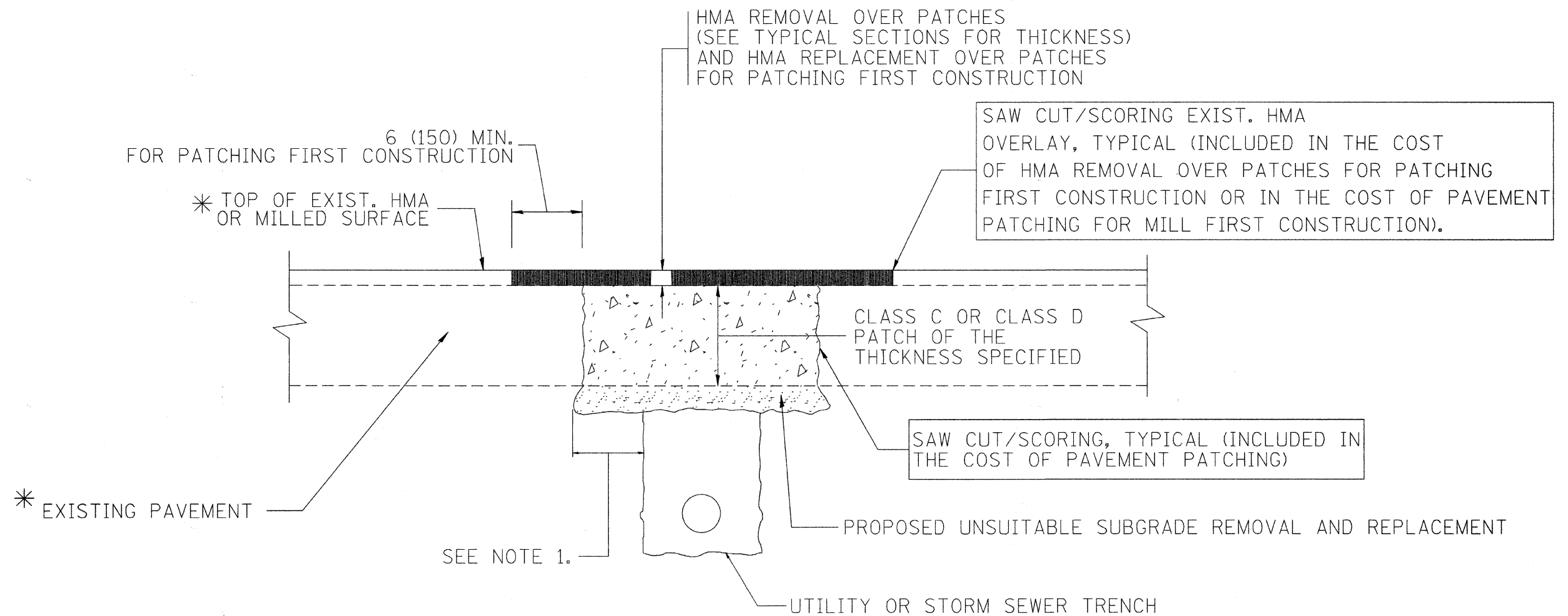
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME = 0904-DTLS-08 - 80-01	USER NAME = beuerdl	DESIGNED -- R. SHAH	REVISED -- M. GOMEZ 04-06-01
		CHECKED --	REVISED -- P. LoFLUER 04-15-03
	PLOT SCALE = 49.9999' / IN.	DRAWN --	REVISED -- R. BORO 01-01-07
	PLOT DATE = 6/12/2008	CHECKED -- 11-04-95	REVISED -- R. BORO 06-11-08

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)			
SCALE:	SHEET NO. 11 OF 12 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1599	10-00120-00-RS	COOK	12	11
BD0155-07 (80-01)			CONTRACT NO. 63473	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(608)				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = 08004-0713-04 - BD-22	USER NAME = bauerdl	DESIGNED -- R. SHAH	REVISED -- A. ABBAS 04-27-98
		CHECKED --	REVISED -- R. BORO 01-01-07
	PLOT SCALE = 50,000 / IN.	DRAWN --	REVISED -- R. BORO 09-04-07
	PLOT DATE = 10/27/2008	CHECKED -- 10-25-94	REVISED -- K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT

SCALE: SHEET NO. 12 OF 12 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1599	10-00120-00-RS	COOK	12	12
BD400-04 (BD-22)			CONTRACT NO. 63473	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(806)				