

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

# PLANS FOR PROPOSED FEDERAL AID PROJECT

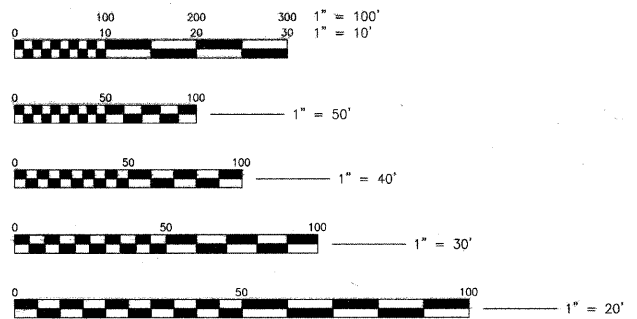
**FAU 2845 (HARWOOD AVENUE -MAPLE ROAD)  
FROM DIXIE HIGHWAY (FAU 2843)  
TO ASHLAND AVENUE (FAU 2860)  
RESURFACING  
SECTION : 10-00130-00-RS  
PROJECT NO.: ARA - 9003 (652)  
VILLAGE OF HOMEWOOD  
COOK COUNTY  
JOB NO.: C-91-577-10**

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HIGHWAY STANDARDS

STANDARD NO.	TITLE
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-03	FRAMES AND LIDS, TYPE 1
606001-04	CONCRETE CURB TYPE-B AND COMBINATION CONCRETE CURB & GUTTER
701501-05	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED SIZE PLANS, THE ABOVE SCALES SHALL BE USED.

**HARWOOD AVE. & MAPLE RD. DESIGN DESIGNATION**  
POSTED SPEED: 25 MPH  
ADT: 2,090 (2009)  
URBAN COLLECTOR

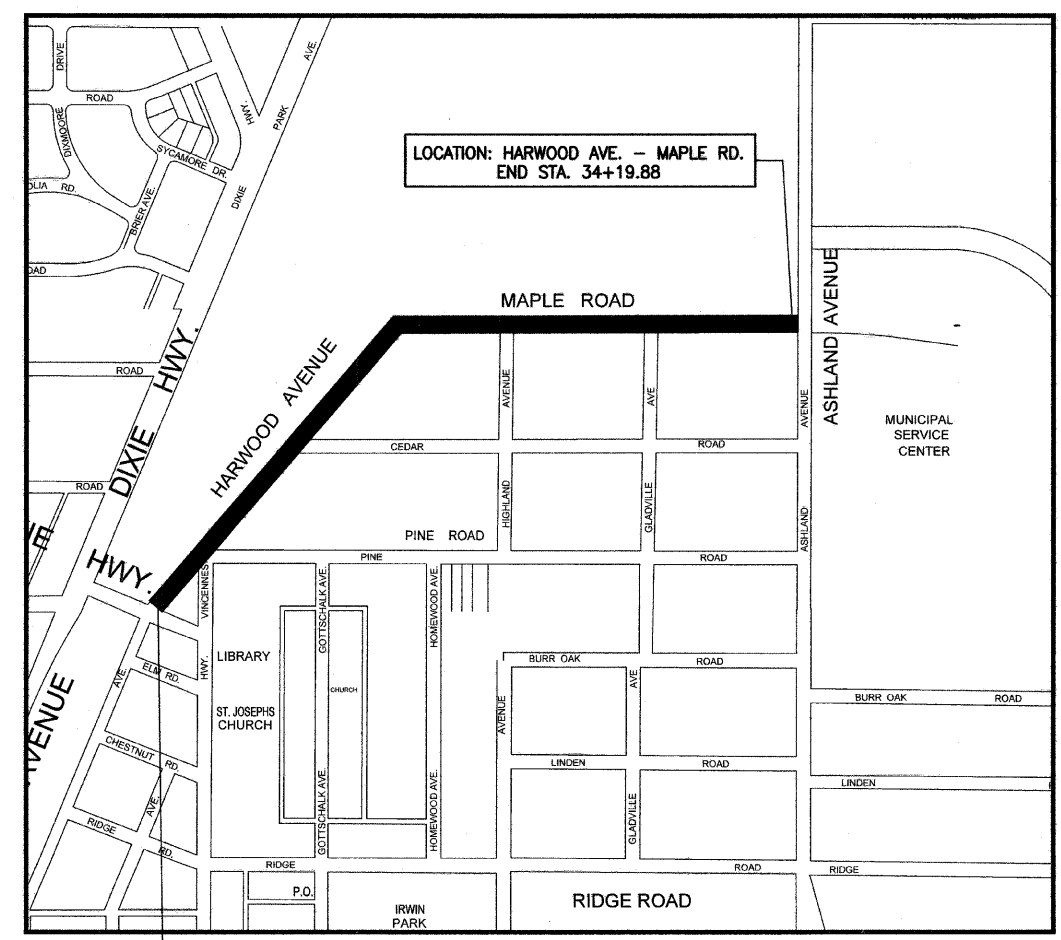
**PROJECT IS LOCATED IN THE VILLAGE OF HOMEWOOD**



CALL JULIE (800) 892-0123  
WITH THE FOLLOWING INFORMATION:  
COUNTY = COOK  
48 HOURS BEFORE YOU DIG  
EXCLUDING SAT., SUN. & HOLIDAYS

FIELD ENGINEER: MEL MANGOBA, (847) 705-4408

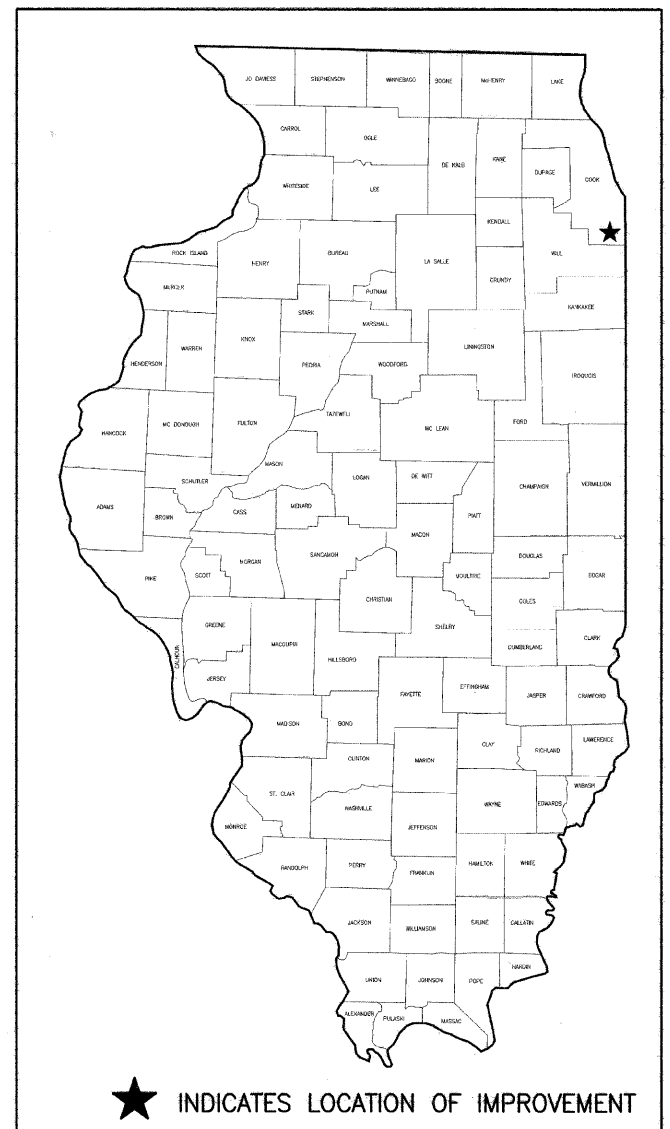
CONTRACT NO.: 63479



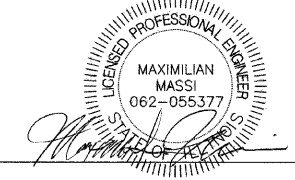
LOCATION: HARWOOD AVE. - MAPLE RD.  
BEGIN STA. 0+00

**3RD PRINCIPAL MERIDIAN**  
**LOCATION MAP**  
NOT TO SCALE

TOTAL NET & GROSS LENGTH OF PROJECT = 3,420 LINEAL FEET (0.648 MILES)



★ INDICATES LOCATION OF IMPROVEMENT



**VILLAGE ENGINEER**  
" LICENSE EXPIRES 11-30-11 " DATE: 5/4/10



VILLAGE OF HOMEWOOD  
Department of Public Works  
Engineering Division  
1775 S. Ashland Avenue  
Homewood, IL 60445  
(708) 206-3470

<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS</b>	
APPROVED <i>[Signature]</i> 2010	VILLAGE PRESIDENT
PASSED <i>[Signature]</i> May 27, 2010	DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	MAY 27, 2010 <i>[Signature]</i> DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

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THE STATE OF ILLINOIS

SUMMARY OF QUANTITIES		CONSTRUCTION TYPE CODE 1000	
CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	48
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	1
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	1
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	1
25200110	SODDING, SALT TOLERANT	SQ YD	48
25200200	SUPPLEMENTAL WATERING	UNIT	5
35101600	AGGREGATE BASE COURSE, TYPE B, 4"	SQ YD	156
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	1,270
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50	TON	516
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	115
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	1,077
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1,400
42400800	DETECTABLE WARNINGS	SQ FT	84
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	12,701
44000195	HOT-MIX ASPHALT DRIVEWAY PAVEMENT REMOVAL	SQ YD	57
44000600	SIDEWALK REMOVAL	SQ FT	1,400
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	65
44201709	CLASS D PATCHES, TYPE III, 5 INCH	SQ YD	22
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	9
60260300	INLETS TO BE ADJUSTED WITH NEW TYPE-1 FRAME, AND OPEN LID	EACH	3
60266600	VALVE BOXES TO BE ADJUSTED	EACH	12

SUMMARY OF QUANTITIES		CONSTRUCTION TYPE CODE 1000	
CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY
67100100	MOBILIZATION	L SUM	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	940
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	6,900
* 78001130	PAINT PAVEMENT MARKING - LINE 6"	FOOT	3,334
* 78001150	PAINT PAVEMENT MARKING - LINE 12"	FOOT	411
* 78001180	PAINT PAVEMENT MARKING - LINE 24"	FOOT	92
X0321558	SANITARY MANHOLES TO BE ADJUSTED WITH NEW TYPE-1 FRAME, CLOSED LID	EACH	19
Z0004510	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	57

\* SPECIALTY ITEM



VILLAGE OF HOMEWOOD  
ENGINEERING  
DIVISION

USER NAME	DESIGNED	REVISION
	DJA	--
	DJA	--
PLOT SCALE	CHECKED	REVISION
--	MM	--
PLOT DATE	DATE	REVISION
--	04-28-10	--

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

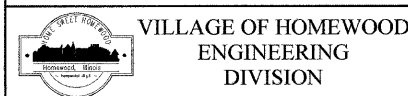
SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 2 OF 12 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	10-00130-00-RS	COOK	12	2
CONTRACT NO. 63479				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

## GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION," ADOPTED JANUARY 1, 2007 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS), THE LATEST EDITION OF THE "ILLINOIS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" AND THE MANUAL OF TEST PROCEDURES FOR MATERIALS IN EFFECT ON THE DATE OF INVITATION FOR BID.; THE LATEST EDITION OF THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" AND THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" INDICATED ON THE CHECK SHEET INCLUDED HEREIN AND IN CASE OF CONFLICT WITH ANY PART OR PARTS OF SAID SPECIFICATIONS, THE SAID SPECIAL PROVISIONS SHALL TAKE PRECEDENCE AND GOVERN.
2. ALL REFERENCES TO THE "VILLAGE" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE VILLAGE OF HOMEWOOD.
3. THE FINISHED HMA SURFACE SHALL BE CONSTRUCTED 1/4 - INCH ABOVE THE GUTTER FLAG.
4. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE VILLAGE AND THE ENGINEER DO NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING THE CONSTRUCTION OPERATION SO AS NOT TO DAMAGE THEM. IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 105.07 OF THE "STANDARD SPECIFICATIONS." THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE "STANDARD SPECIFICATIONS."
5. ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE PLANS AND THE IDOT STANDARDS FOR TRAFFIC CONTROL AND PROTECTION.
6. WHERE THE PROPOSED PAVEMENT ABUTS EXISTING PAVEMENT TO REMAIN IN PLACE (BEGIN, END AND LIMITS OF CONSTRUCTION), EXISTING DRIVEWAY PAVEMENT, SIDEWALK TO REMAIN IN PLACE. THE EXISTING PAVEMENT SHALL BE SAW CUT TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING SURFACES. THIS SAW CUT SHALL BE INCLUDED IN THE COST OF THE PAY ITEM "HMA SURFACE REMOVAL - BUTT JOINT".
7. CURB & GUTTER, DRIVEWAY AND SIDEWALK REMOVAL AND REPLACEMENT SHALL BE LIMITED TO WORKING ON ONE SIDE OF THE STREET AT ALL TIMES TO MINIMIZE CONGESTION. NO CURB & GUTTER SHALL BE REMOVED ON FRIDAYS. CONCRETE SHALL HAVE A MINIMUM OF 72 HOURS CURING TIME AND DRIVEWAYS SHALL BE ACCESSIBLE PRIOR TO REMOVING EXISTING CURB ON THE OTHER SIDE OF THE STREET.
8. THE CONTRACTOR SHALL ADJUST ALL VALVE BOXES WITHIN THE PAVEMENT AREA BY DIGGING THE TOP PIECE SUCH THAT IT MAY BE TURNED TO THE FINISHED PAVEMENT GRADE. CAST IRON INSERTS MAY BE USED ONLY IF THEY ARE ADJUSTABLE BY SCREWING INTO THE EXISTING PIECE OR RESTING FIRMLY ON THE EXISTING BOX. MORTAR OR TAR SHALL NOT BE USED TO HOLD AN INSERT IN PLACE.
9. ALL FRAME AND GRATES TO BE REPLACED SHALL REMAIN THE PROPERTY OF THE VILLAGE OF HOMEWOOD AND SHALL BE DELIVERED TO THE PUBLIC WORKS BUILDING BY THE CONTRACTOR. DELIVERY OF OLD FRAMES AND GRATES TO THE VILLAGE OF HOMEWOOD DEPARTMENT OF PUBLIC WORKS SHALL BE INCLUDED IN THE COST OF THE RESPECTIVE STRUCTURE ADJUSTMENTS.
10. THE CONTRACTOR(S) AND THEIR SUBCONTRACTOR(S) SHALL CONTACT J.U.L.I.E. (800) 892-0123 AND THE VILLAGE OF HOMEWOOD/DEPARTMENT OF PUBLIC WORKS (708) 206-3470 BEFORE DIGGING.
11. THE CONTRACTOR SHALL NOTIFY ENGINEER 24 HOURS PRIOR TO BEGINNING WORK SO THAT CARS CAN BE REMOVED FROM DRIVEWAYS. WORK MUST BE PLANNED TO MINIMIZE THE TIME DRIVEWAYS ARE OUT OF SERVICE. WORK SHALL BE SCHEDULED SO THAT DRIVES PULLED ON THE FIRST WORKING DAY ARE FORMED AND POURED BY THE THIRD WORKING DAY. BEYOND THIS 3 DAY TIME IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE AT HIS OWN EXPENSE TEMPORARY AGGREGATE FOR ACCESS TO THE DRIVEWAY.
12. DEBRIS REMOVAL - MATERIALS RESULTING FROM THE VARIOUS CONSTRUCTION OPERATIONS SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGEMENT OF THE ENGINEER, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE VILLAGE WILL REMOVE DEBRIS AND THE CONTRACTOR SHALL BE BILLED ACCORDINGLY.
13. WATER SUPPLY - THE CONTRACTOR CAN OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A WATERING BAN IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR. WATER IS AVAILABLE AT 17755 S. ASHLAND AVENUE, MUNICIPAL SERVICE CENTER FIRE HYDRANT. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER. THE SUPPLY OF WATER BY THE CONTRACTOR SHALL BE INCLUDED IN THE COST OF SUPPLEMENTAL WATERING.
14. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/HR) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/HR). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
15. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL PROVIDE TO THE VILLAGE A VIDEO TAPE COVERING FROM RIGHT-OF-WAY TO RIGHT-OF-WAY OF ALL ROADWAYS TO BE IMPROVED. THIS WORK SHALL BE INCLUDED IN THE COST OF MOBILIZATION.
16. WHEN REMOVING PAVEMENT, CURB AND GUTTER, SIDEWALK, DRIVEWAY PAVEMENT, PARKING LOT PAVEMENT, AND/OR OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKER WHICH MIGHT DISTURB UNDERGROUND PUBLIC UTILITIES WILL NOT BE PERMITTED.
17. PRIOR TO THE START OF CONSTRUCTION ACTIVITIES, THE ENGINEER AND CONTRACTOR, JOINTLY, SHALL PERFORM A VISUAL INSPECTION OF VILLAGE DRAINAGE AND UTILITY STRUCTURES TO DETERMINE THE AMOUNT OF EXISTING DEBRIS IN EACH STRUCTURE. THE CONTRACTOR WILL BE REQUIRED TO CLEAN THOSE STRUCTURES WITH DEBRIS THAT WERE CLEAN AT THE BEGINNING OF CONSTRUCTION AT NO ADDITIONAL COST TO THE CONTRACT.
18. MILLING OF PAVEMENT SHALL BE DONE SO AS TO NOT DAMAGE THE ADJACENT CURB OR STRUCTURES. REMOVAL ADJACENT TO THESE STRUCTURES SHALL BE ACCOMPLISHED TO THE SATISFACTION OF THE ENGINEER AND MAY REQUIRE HANDWORK.
19. THE CONTRACTOR SHALL PROVIDE TWO (2) SUITABLE TEMPORARY TOILET FACILITIES ALONG THE STREET, UNLESS OTHERWISE APPROVED BY THE ENGINEER, FOR USE OF ALL CONTRACTOR'S PERSONNEL EMPLOYED ON THE WORK, AND SHALL MAINTAIN SAME IN PROPER SANITARY CONDITION. AT COMPLETION, THE FACILITIES SHALL BE REMOVED AND THE PREMISES LEFT CLEAN. THE LOCATION OF THE TEMPORARY TOILETS SHALL BE APPROVED BY THE ENGINEER. COST FOR TOILET FACILITIES WILL NOT BE PAID FOR SEPARATELY BUT SHOULD BE INCLUDED IN THE COST OF MOBILIZATION.
20. THE CONTRACTOR SHALL REMOVE FROM THE PROJECT SITE ALL UNSUITABLE AND SURPLUS EXCAVATED MATERIAL NOT USED OR BACKFILLED. THE WASTE EXCAVATED MATERIAL SHALL NOT BE DEPOSITED IN PUBLIC OR PRIVATE PROPERTY UNLESS THE CONTRACTOR FIRST OBTAINS THE WRITTEN PERMISSION FORM THE PROPERTY OWNER OR THE AUTHORIZED REPRESENTATIVE OF THE APPROPRIATE PUBLIC AGENCY.
21. THE CONTRACTOR SHALL KEEP ONE (1) COPY OF ALL SPECIFICATIONS, DRAWINGS, ADDENDA, MODIFICATIONS, AND SHOP DRAWINGS AT THE SITE ON GOOD ORDER AND ANNOTATED TO SHOW ALL CHANGES MADE DURING THE CONSTRUCTION PROCESS. THE RECORD DRAWINGS SHALL BECOME THE PROPERTY OF THE VILLAGE.
22. THE CONTRACTOR SHALL MAINTAIN HANDICAP ACCESS TO ANY HOME DEEMED NECESSARY BY THE VILLAGE BY PROVIDING AND INSTALLING MATERIALS REQUIRED FOR REASONABLE INGRESS AND EGRESS AT ALL TIMES. THE COST OF THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN TO THE CONTRACT.
23. ADD THE FOLLOWING STATEMENT TO THE END OF SECTION 105.06: "THE CONTRACTOR SHALL NOT CHANGE HIS SUPERINTENDENT WITHOUT WRITTEN PERMISSION OF THE VILLAGE."
24. NO METAL RINGS SHALL BE USED FOR FRAME ADJUSTMENTS. THE RINGS AND FRAMES SHALL BE SET ON TWO CONCENTRIC RINGS OF CON-SEAL OR PRESS-SEAL "TAR ROPE". MOSTIC SHALL BE APPLIED WITH A TROWEL ON THE OUTSIDE OF THE RINGS, WHILE THE INSIDE SHALL BE PAINTED WITH A FINE CEMENT GROUT. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE STRUCTURE ADJUSTMENTS.



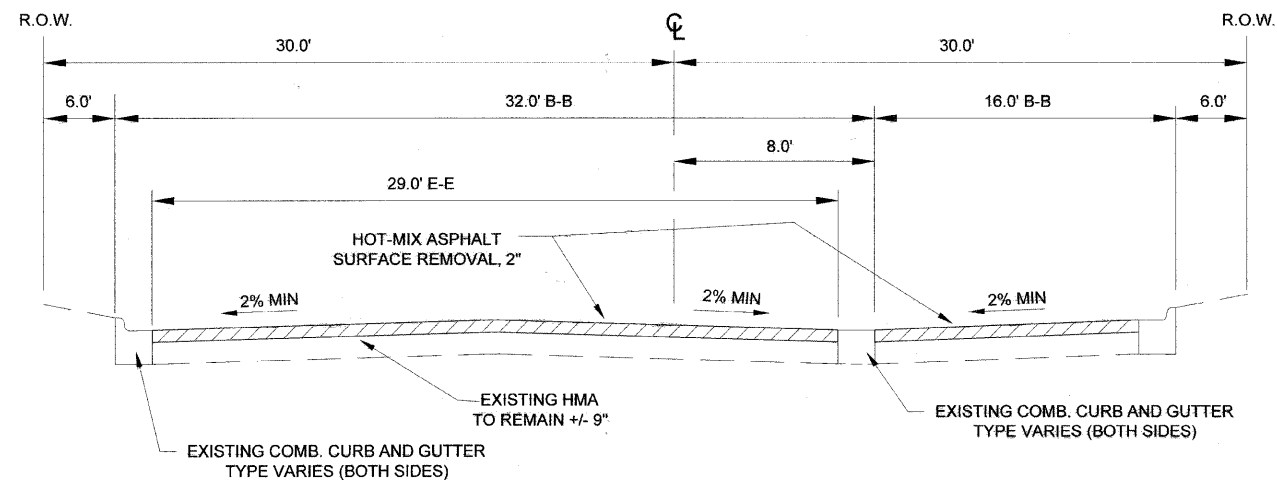
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		DRAWN	DJA	REVISED	--
PLOT SCALE	--	CHECKED	MM	REVISED	--
PLOT DATE	--	DATE	04-28-10	REVISED	--

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

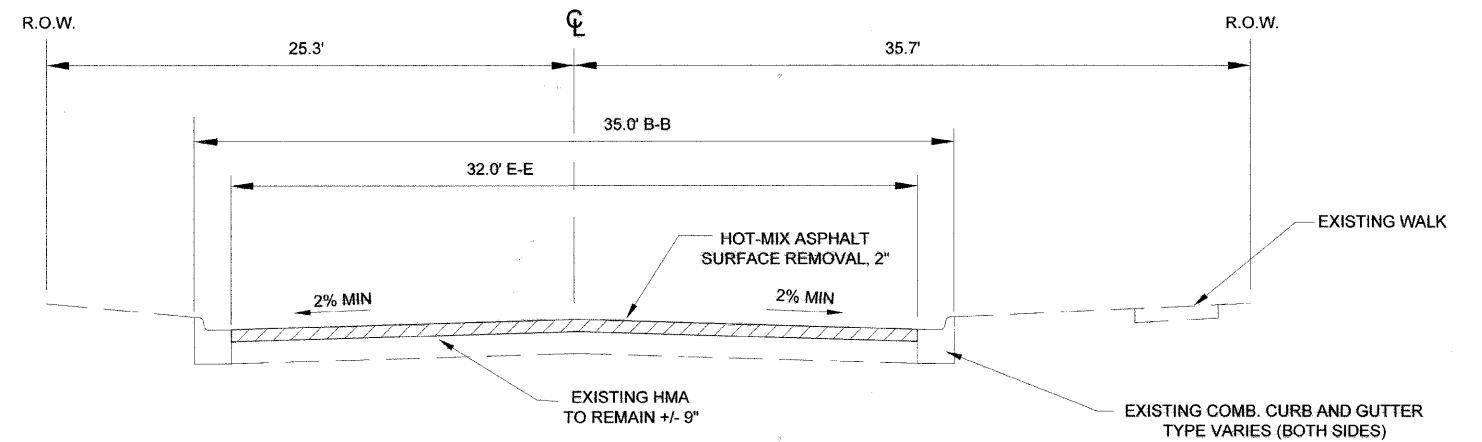
**GENERAL NOTES**

SCALE: NONE      SHEET NO. 3 OF 12 SHEETS      STA.      TO      STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	10-00130-00-RS	COOK	12	3
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			<b>CONTRACT NO. 63479</b>	



**EXISTING TYPICAL SECTION**  
 STA. 0+00 to STA. 2+50, HARWOOD AVENUE

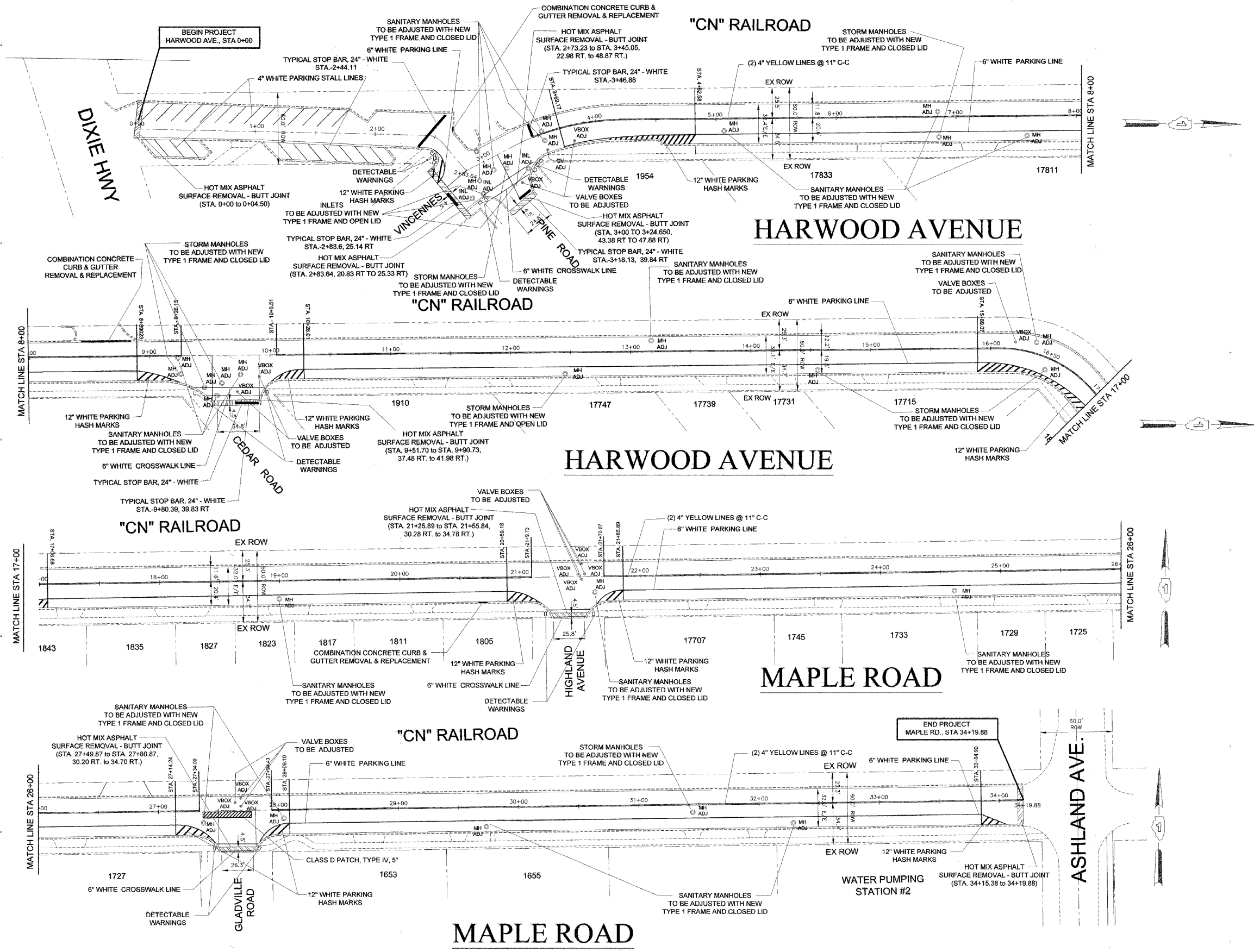


**EXISTING TYPICAL SECTION**  
 STA. 2+50 to STA. 34+19.88, HARWOOD AVENUE & MAPLE ROAD

USER NAME	DESIGNED	REVISIONS
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PLOT SCALE	DRAWN	REVISIONS
---	DJA	---
PLOT DATE	CHECKED	REVISIONS
---	MM	---
	DATE	REVISIONS
	04-28-10	---

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	10-00130-00-RS	COOK	12	4
CONTRACT NO. 63479				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





**VILLAGE OF HOMEWOOD**  
ENGINEERING  
DIVISION

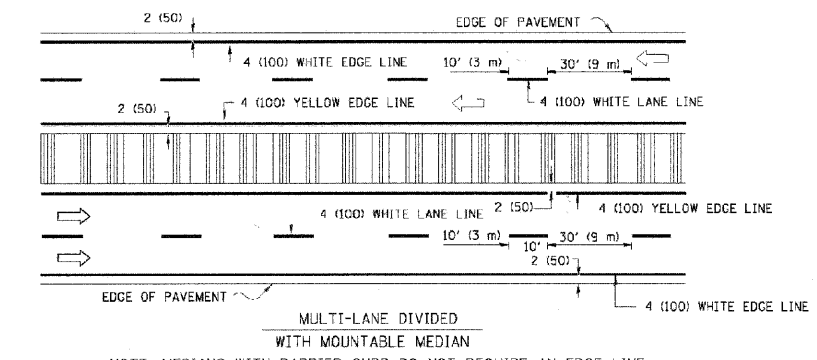
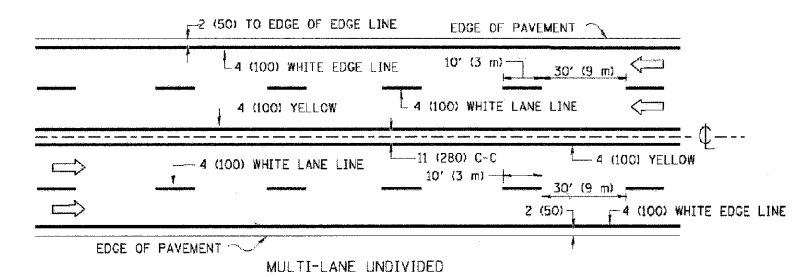
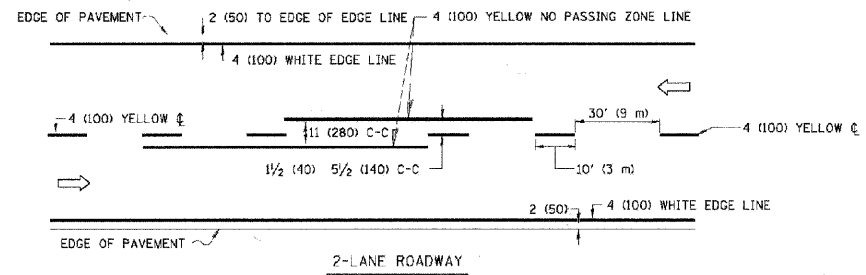
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	DATE	04-28-10	REVISED	

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

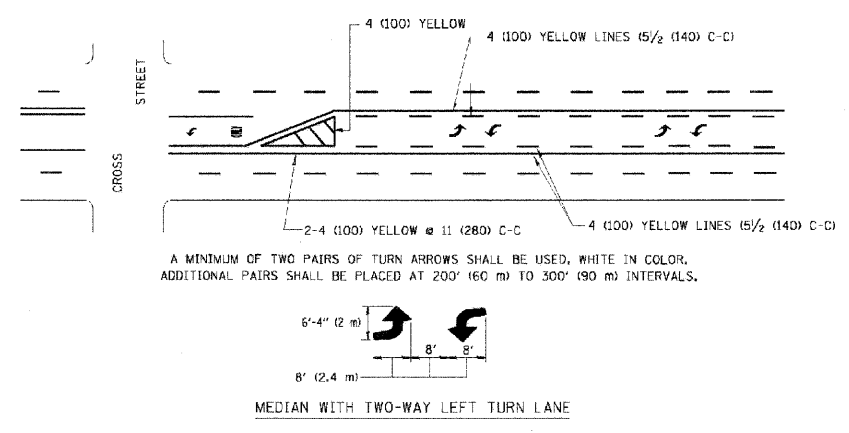
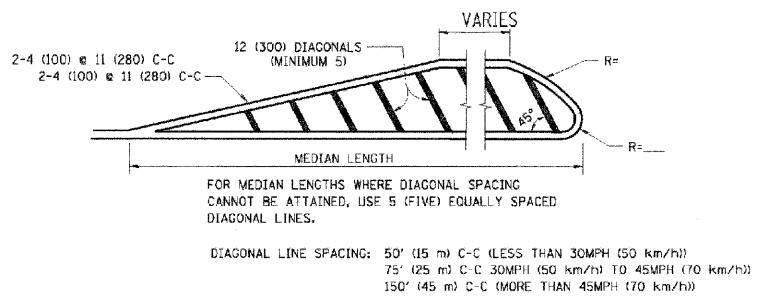
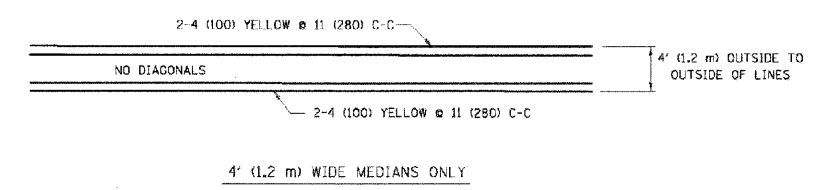
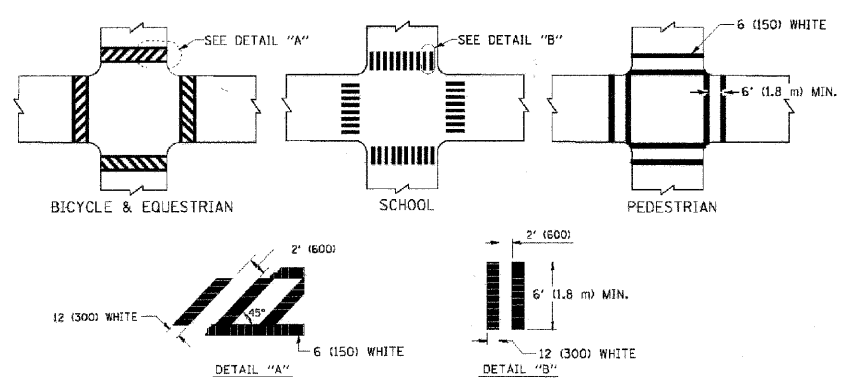
**EXISTING & PROPOSED ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: 1"=40.0'    SHEET NO. 6 OF 12 SHEETS    STA. TO STA.

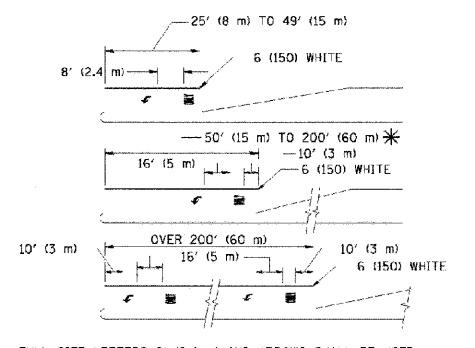
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	10-00130-00-RS	COOK	12	6
CONTRACT NO. 63479				
FED. ROAD DIST. NO. 1    ILLINOIS    FED. AID PROJECT				



TYPICAL LANE AND EDGE LINE MARKING



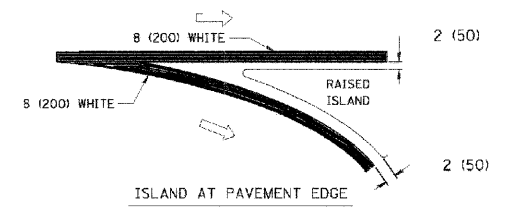
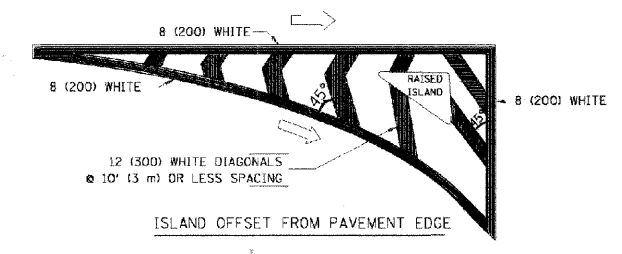
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
\* AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING



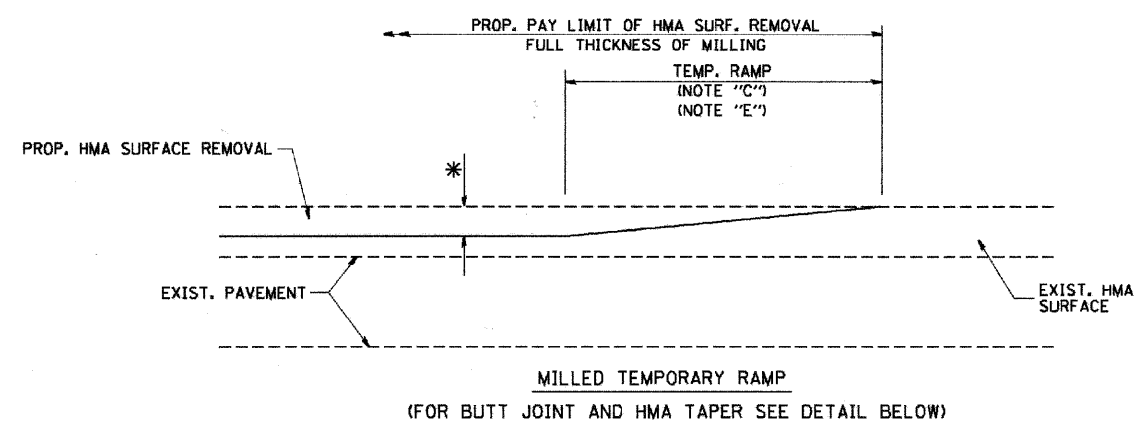
TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD T80001 AREA OF: "R"-3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"-54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

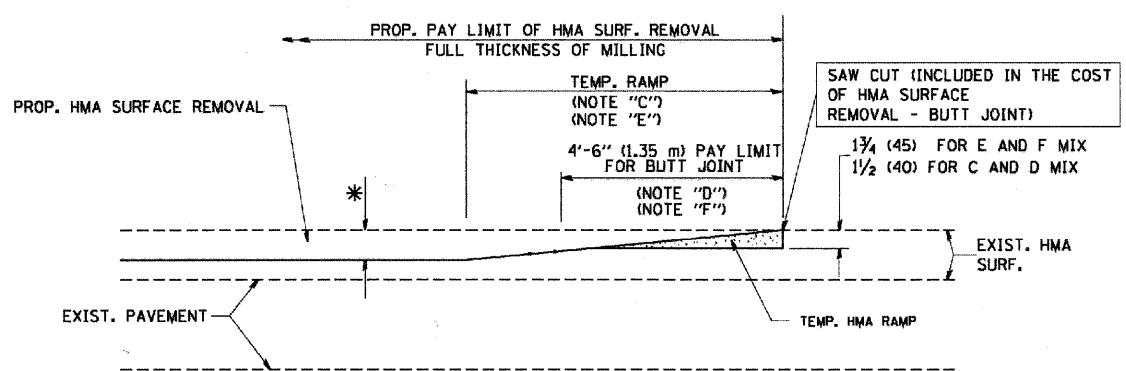
All dimensions are in inches (millimeters) unless otherwise shown.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	10-00130-00-RS	COOK	12	8
STA.		TO STA.		
FED. AID PROJECT				



MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

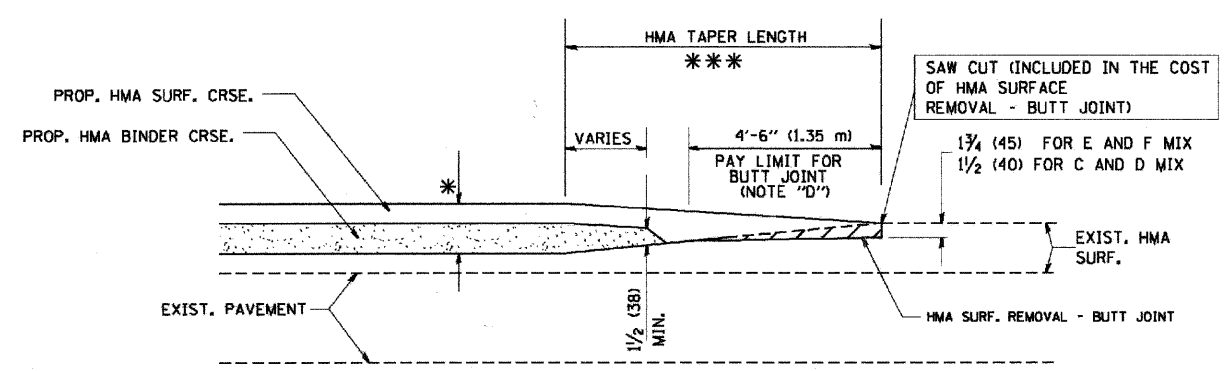
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

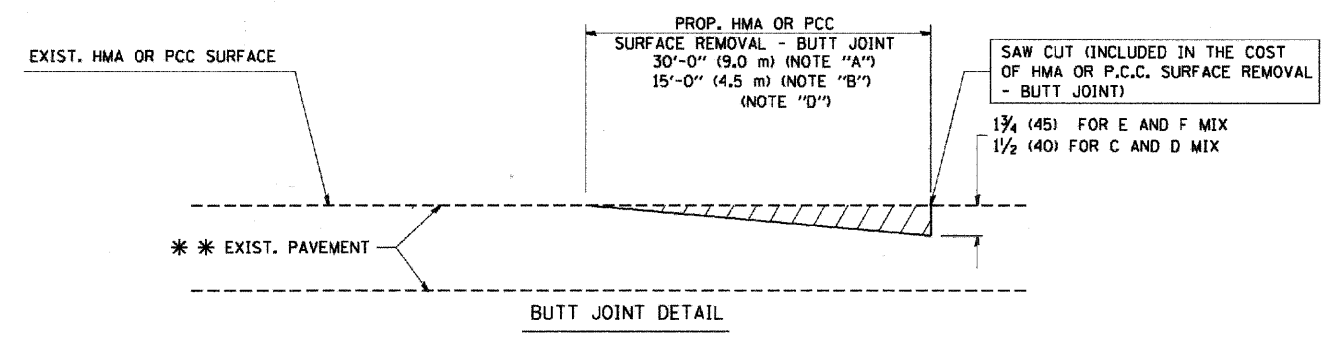
OPTION 2

TYPICAL TEMPORARY RAMP

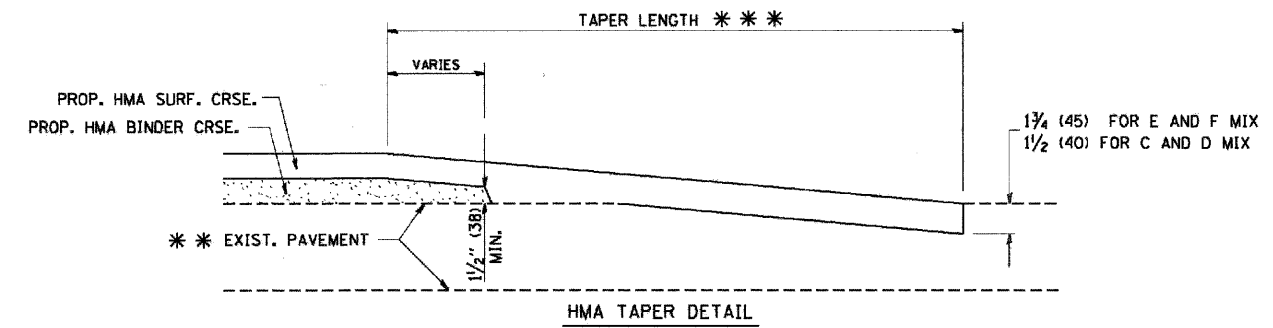


BUTT JOINT AND  
HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQ. METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER  
DETAILS

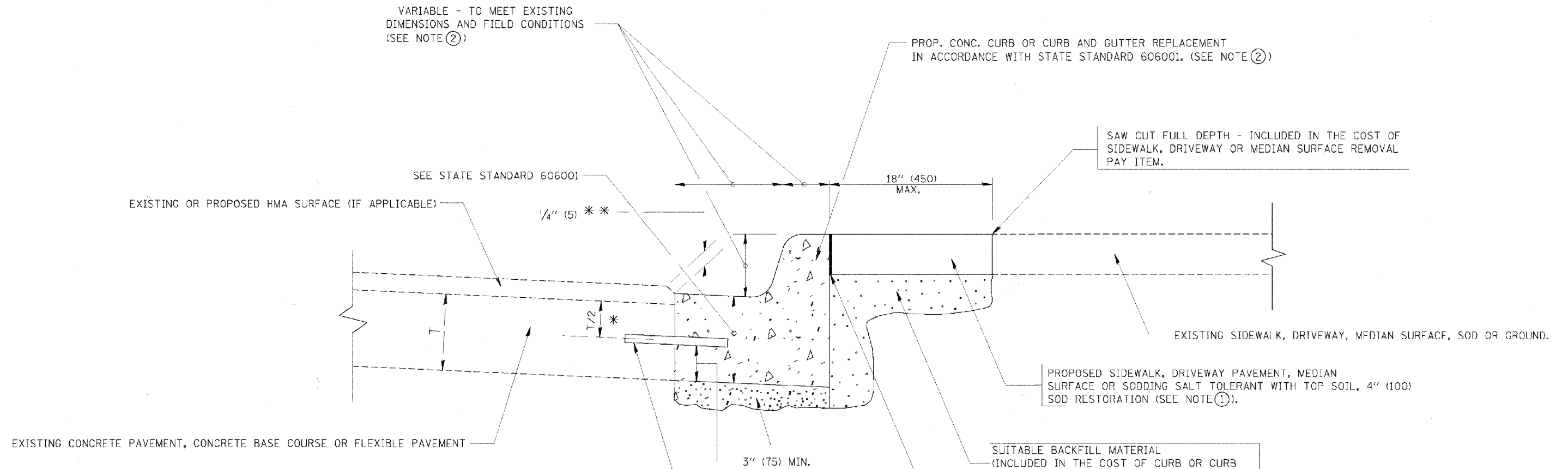
SCALE: VERT. NONE  
HORIZ. NONE  
PLOT DATE: 1/18/2007

DRAWN BY  
CHECKED BY

BD400-05 (V1=BD32)  
REVISION DATE: 01/01/07

PLT DATE = 1/18/2007  
FILE NAME = I:\182807\msh\drawings\bd32.dwg  
PLT SCALE = 1/8"=1'-0"  
USER NAME = dr-yaklongr





\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.  
 \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.  
 SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

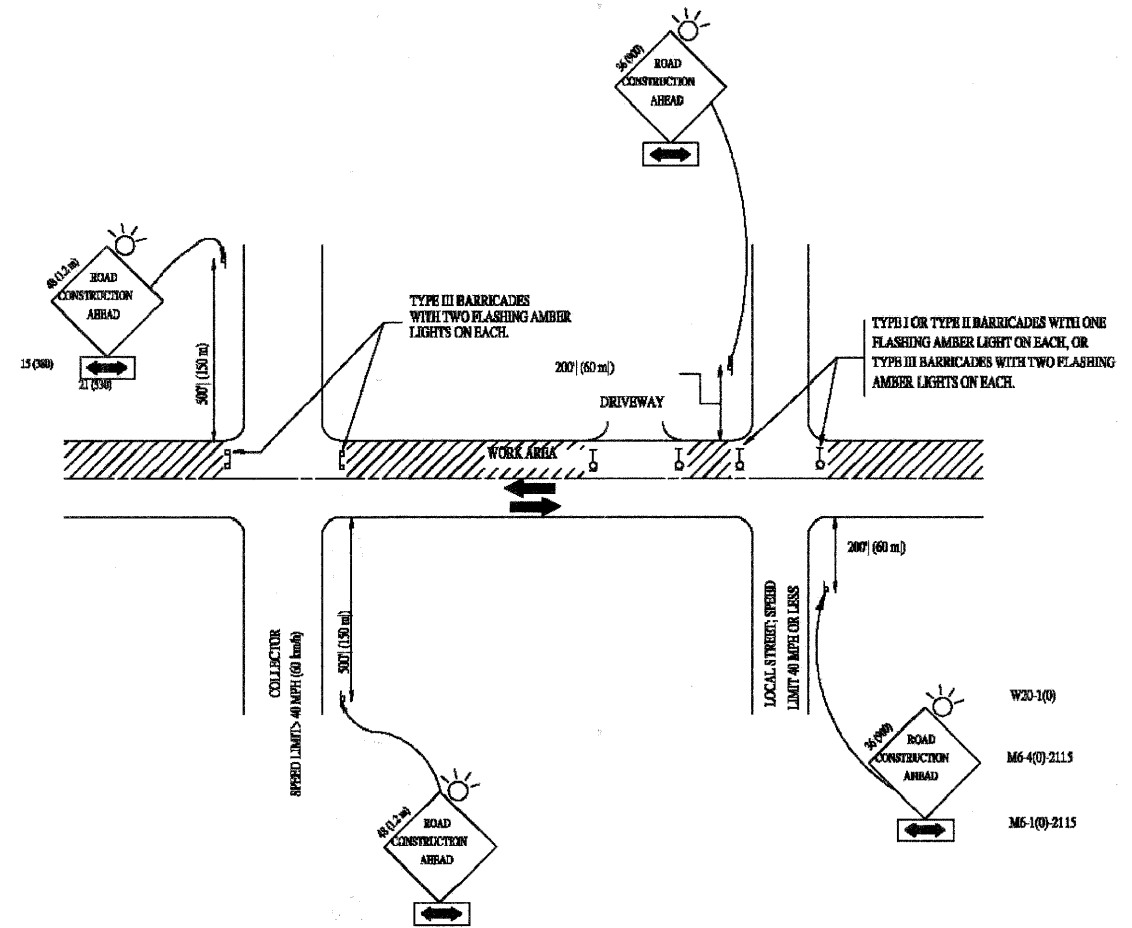
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME -	USER NAME - driva03gn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A. RTE. 2845	SECTION 10-00130-00-RS	COUNTY COOK	TOTAL SHEETS 12	SHEET NO. 9
PROJECT START - 04/03/94	CHECKED -	REVISED - M. GOMEZ 01-22-01	BD600-06 (BD-24)			CONTRACT NO. 63479				
PROJECT DATE - 12/15/2005	DATE - 03-11-94	REVISED - R. BORO 12-15-09	FED. ROAD DIST. NO. 1			ILLINOIS FED. AID PROJECT				
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.								



**TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

**NOTES:**

**A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 300' (90 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M5-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M4-4).

**B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701.90, STD. 701.96 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in feet unless (meters) unless otherwise shown.

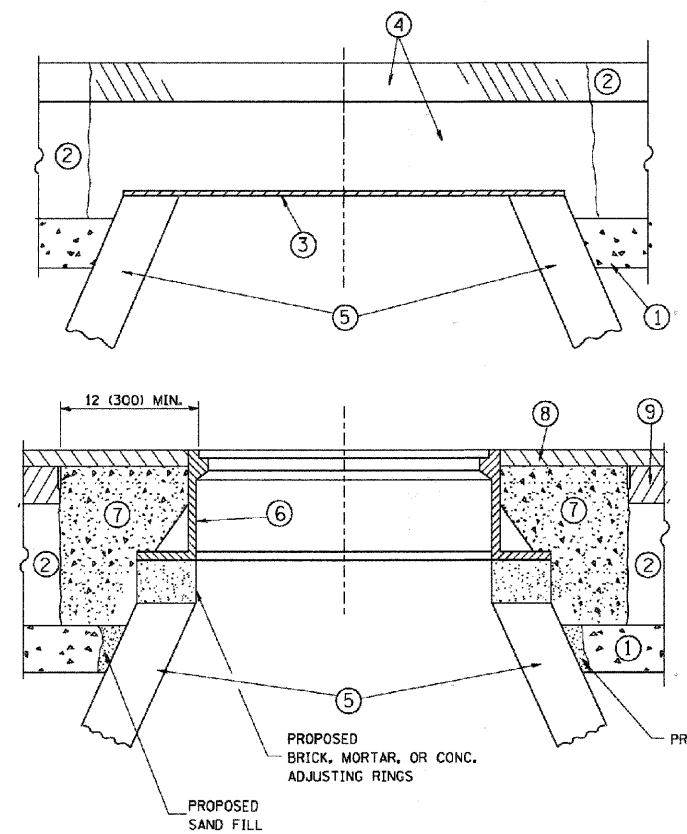
FILE NAME	DATE	BY	REVISION
W:\1000\1001\1001.dwg	10-18-95	J. CHENG	1. CHANGED TO 10-18-95
			REVISION
			2. A. BOOSE 03-06-96
			REVISION
			3. A. BOOSE 10-15-96
			REVISION
			4. T. KAMMACHER 01-08-00

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET NO. 10 OF 10 SHEETS STA. TO STA.

P.A.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	10-00130-00-RS	COOK	12	10
TC-10			CONTRACT NO. 63479	



**CONSTRUCTION PROCEDURES**

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
  - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
  - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
  - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS S1 CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**LOCATION OF STRUCTURES:**

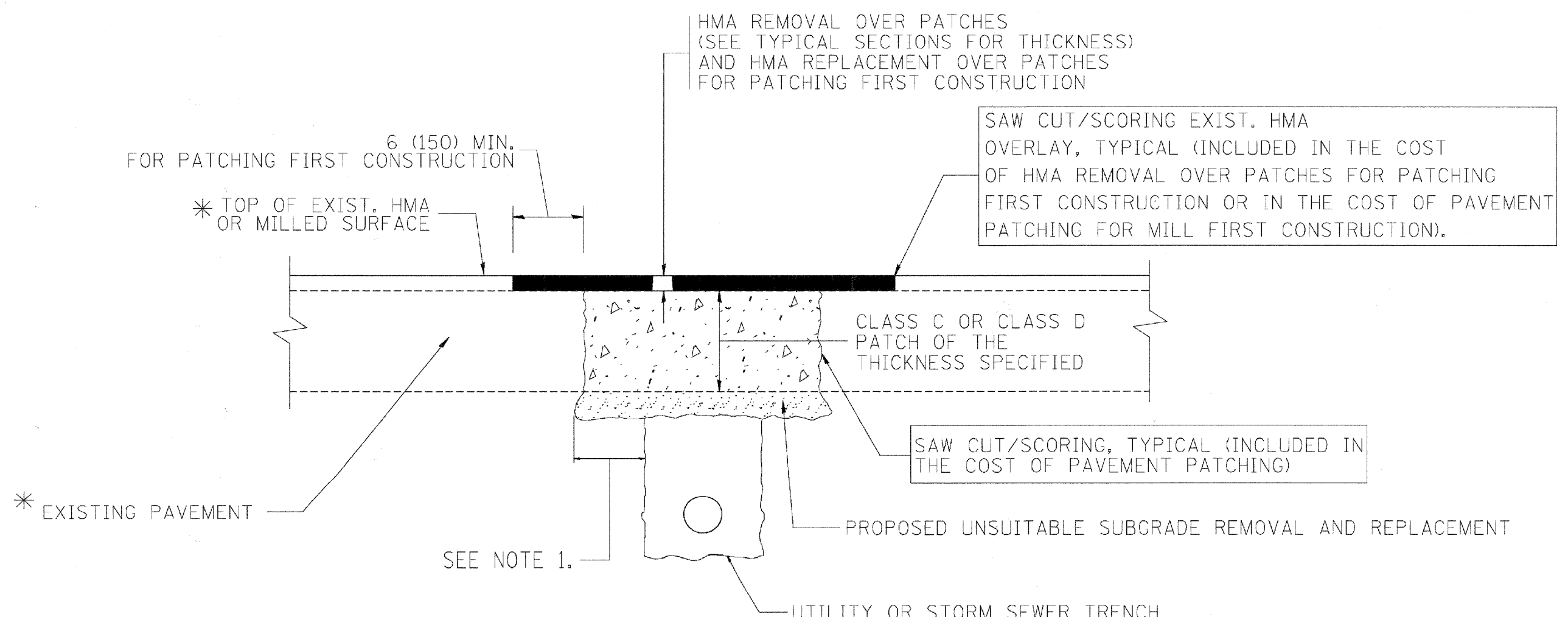
THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"  
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\d1stata\22x34\bd88.dgn	USER NAME = gegl:anob3	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			F.A. - RTE. 2845	SECTION 10-00130-00-RS	COUNTY COOK	TOTAL SHEETS 12	SHEET NO. 11	
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	<b>BD600-03 (BD-8)</b>		CONTRACT NO. 63479			
	PLOT DATE = 1/4/2009	DATE - 10-25-94	REVISED - R. WIEDEMAN 05-14-04			FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT							
			REVISED - R. BORO 01-01-07										



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION 'PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL'.

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\dststd22x34\bd22.dgn	USER NAME = bauerd1	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>	F.A. RTE. 2845	SECTION 10-00130-00-RS	COUNTY COOK	TOTAL SHEETS 12	SHEET NO. 12
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07			<b>BD400-04 (BD-22)</b>		CONTRACT NO. 63479		
PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08	SCALE: NONE		SHEET NO. 1 OF 1 SHEETS	STA. _____	TO STA. _____		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	