

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

**PLANS FOR PROPOSED  
FEDERAL AID PROJECT**

**FAU 2845 (HARWOOD AVENUE)  
FROM 183rd STREET (FAU 1622) TO SOUTH VILLAGE BOUNDARY  
RESURFACING**

**VILLAGE OF HOMEWOOD  
SECTION : 10-00131-00-RS  
PROJECT NO.: ARA-9003(651)**

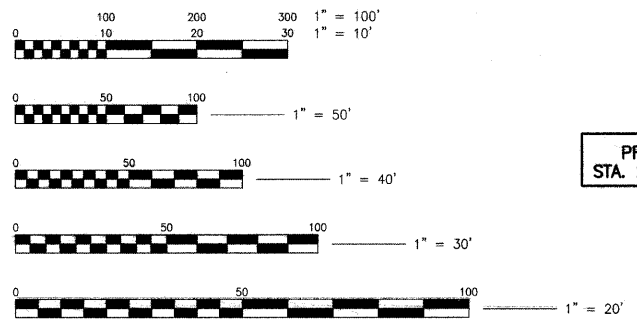
**COOK COUNTY  
JOB NO.: C-91-576-10**

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HIGHWAY STANDARDS

STANDARD NO.	TITLE
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-03	FRAMES AND LIDS, TYPE 1
606001-04	CONCRETE CURB TYPE-B AND COMBINATION CONCRETE CURB & GUTTER
701501-05	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED SIZE PLANS, THE ABOVE SCALES SHALL BE USED.

**HARWOOD AVENUE DESIGN DESIGNATION**  
POSTED SPEED: 25 MPH  
ADT: 6,500 (2009)  
MINOR ARTERIAL

**PROJECT IS LOCATED IN THE VILLAGE OF HOMEWOOD**

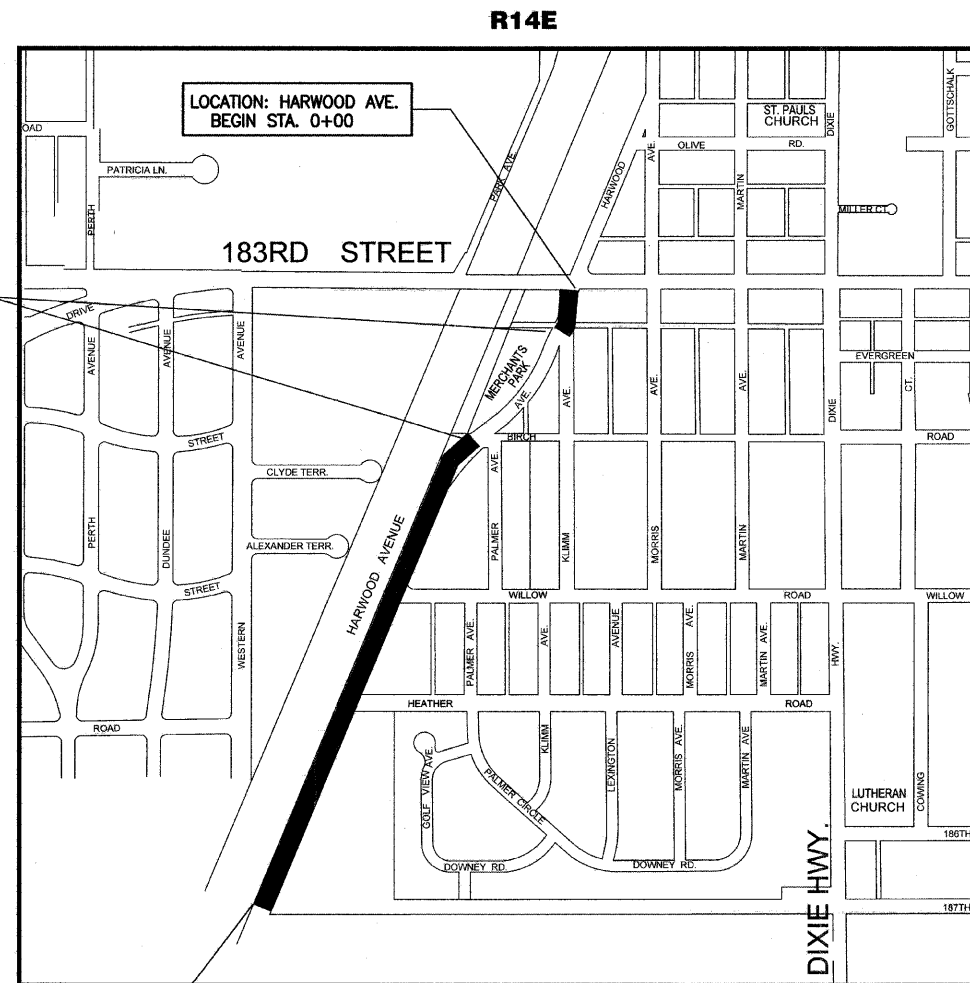


CALL JULIE (800) 892-0123  
WITH THE FOLLOWING INFORMATION:  
COUNTY = COOK  
48 HOURS BEFORE YOU DIG  
EXCLUDING SAT., SUN. & HOLIDAYS

CONTRACT NO.: 63480

PROJECT OMISSION  
STA. 2+41.93 TO 8+19.19

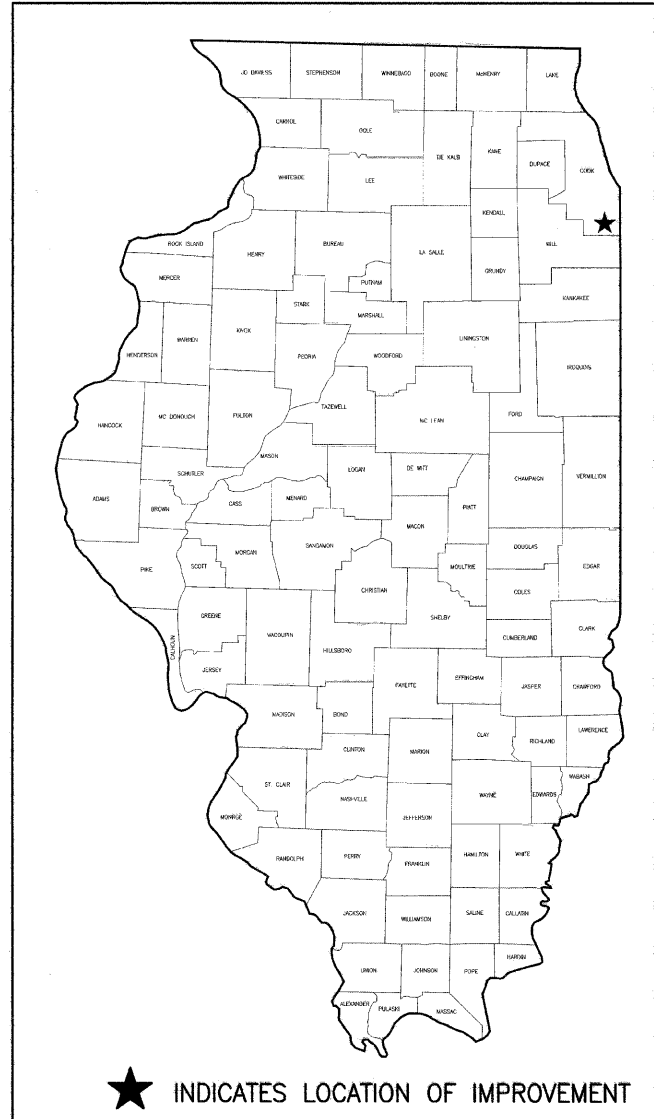
LOCATION: HARWOOD AVE.  
END STA. 30+89.16



LOCATION MAP  
NOT TO SCALE

**3RD PRINCIPAL MERIDIAN**

GROSS LENGTH OF PROJECT = 3,089 LINEAL FEET (0.585 MILES)  
NET LENGTH OF PROJECT = 2,512 FEET (0.476 MILES)



★ INDICATES LOCATION OF IMPROVEMENT



VILLAGE ENGINEER  
" LICENSE EXPIRES 11-30-11 " DATE: 5/4/10



VILLAGE OF HOMEWOOD  
Department of Public Works  
Engineering Division  
17755 S. Ashland Avenue  
Homewood, IL 60445  
(708) 206-3470

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
APPROVED	2010 <i>[Signature]</i> VILLAGE PRESIDENT
PASSED	MAY 27 2010 <i>[Signature]</i> DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	MAY 27, 2010 <i>[Signature]</i> DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

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THE STATE OF ILLINOIS

FIELD ENGINEER: MEL MANGOBA, (847) 705-4408

SUMMARY OF QUANTITIES		CONSTRUCTION TYPE CODE 1000	
CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	5
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	1
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	1
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	1
25200110	SODDING, SALT TOLERANT	SQ YD	5
25200200	SUPPLEMENTAL WATERING	UNIT	5
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	14
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	810
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	322
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	127
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	689
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	125
42400800	DETECTABLE WARNINGS	SQ FT	12
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	8,078
44000195	HOT-MIX ASPHALT DRIVEWAY PAVEMENT REMOVAL	SQ YD	29
44000600	SIDEWALK REMOVAL	SQ FT	125
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	69
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	3
67100100	MOBILIZATION	L SUM	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1

SUMMARY OF QUANTITIES		CONSTRUCTION TYPE CODE 1000	
CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	900
78001100	PAINT PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	37
78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	8,843
* 78001130	PAINT PAVEMENT MARKING - LINE 6"	FOOT	240
* 78001150	PAINT PAVEMENT MARKING - LINE 12"	FOOT	45
* 78001180	PAINT PAVEMENT MARKING - LINE 24"	FOOT	26
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	63
* 78300200	RAISED REFLECTIVE MARKER REMOVAL	EACH	63
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	190
X0321558	SANITARY MANHOLES TO BE ADJUSTED WITH NEW TYPE-1 FRAME, CLOSED LID	EACH	1
Z0004510	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	29

\* SPECIALTY ITEM



VILLAGE OF HOMEWOOD  
ENGINEERING  
DIVISION

USER NAME	DESIGNED	REVISION
---	DJA	5-21-10
	DRAWN	REVISION
	DJA	---
PLOT SCALE	CHECKED	REVISION
---	MM	---
PLOT DATE	DATE	REVISION
---	04-28-10	---

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 2 OF 12 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	10-00131-00-RS	COOK	12	2
CONTRACT NO.			63480	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

## GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION," ADOPTED JANUARY 1, 2007 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS), THE LATEST EDITION OF THE "ILLINOIS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" AND THE MANUAL OF TEST PROCEDURES FOR MATERIALS IN EFFECT ON THE DATE OF INVITATION FOR BID, THE LATEST EDITION OF THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" AND THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" INDICATED ON THE CHECK SHEET INCLUDED HEREIN AND IN CASE OF CONFLICT WITH ANY PART OR PARTS OF SAID SPECIFICATIONS, THE SAID SPECIAL PROVISIONS SHALL TAKE PRECEDENCE AND GOVERN.
2. ALL REFERENCES TO THE "VILLAGE" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE VILLAGE OF HOMEWOOD.
3. THE FINISHED HMA SURFACE SHALL BE CONSTRUCTED 1/4 - INCH ABOVE THE GUTTER FLAG.
4. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE VILLAGE AND THE ENGINEER DO NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING THE CONSTRUCTION OPERATION SO AS NOT TO DAMAGE THEM. IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 105.07 OF THE "STANDARD SPECIFICATIONS," THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE "STANDARD SPECIFICATIONS."
5. ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE PLANS AND THE IDOT STANDARDS FOR TRAFFIC CONTROL AND PROTECTION.
6. WHERE THE PROPOSED PAVEMENT ABUTS EXISTING PAVEMENT TO REMAIN IN PLACE (BEGIN, END AND LIMITS OF CONSTRUCTION), EXISTING DRIVEWAY PAVEMENT, SIDEWALK TO REMAIN IN PLACE. THE EXISTING PAVEMENT SHALL BE SAW CUT TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING SURFACES. THIS SAW CUT SHALL BE INCLUDED IN THE COST OF THE PAY ITEM "HMA SURFACE REMOVAL - BUTT JOINT".
7. CURB & GUTTER, DRIVEWAY AND SIDEWALK REMOVAL AND REPLACEMENT SHALL BE LIMITED TO WORKING ON ONE SIDE OF THE STREET AT ALL TIMES TO MINIMIZE CONGESTION. NO CURB & GUTTER SHALL BE REMOVED ON FRIDAYS. CONCRETE SHALL HAVE A MINIMUM OF 72 HOURS CURING TIME AND DRIVEWAYS SHALL BE ACCESSIBLE PRIOR TO REMOVING EXISTING CURB ON THE OTHER SIDE OF THE STREET.
8. THE CONTRACTOR SHALL ADJUST ALL VALVE BOXES WITHIN THE PAVEMENT AREA BY DIGGING THE TOP PIECE SUCH THAT IT MAY BE TURNED TO THE FINISHED PAVEMENT GRADE. CAST IRON INSERTS MAY BE USED ONLY IF THEY ARE ADJUSTABLE BY SCREWING INTO THE EXISTING PIECE OR RESTING FIRMLY ON THE EXISTING BOX. MORTAR OR TAR SHALL NOT BE USED TO HOLD AN INSERT IN PLACE.
9. ALL FRAME AND GRATES TO BE REPLACED SHALL REMAIN THE PROPERTY OF THE VILLAGE OF HOMEWOOD AND SHALL BE DELIVERED TO THE PUBLIC WORKS BUILDING BY THE CONTRACTOR. DELIVERY OF OLD FRAMES AND GRATES TO THE VILLAGE OF HOMEWOOD DEPARTMENT OF PUBLIC WORKS SHALL BE INCLUDED IN THE COST OF THE RESPECTIVE STRUCTURE ADJUSTMENTS.
10. THE CONTRACTOR(S) AND THEIR SUBCONTRACTOR(S) SHALL CONTACT J.U.L.I.E. (800) 892-0123 AND THE VILLAGE OF HOMEWOOD/DEPARTMENT OF PUBLIC WORKS (708) 206-3470 BEFORE DIGGING.
11. THE CONTRACTOR SHALL NOTIFY ENGINEER 24 HOURS PRIOR TO BEGINNING WORK SO THAT CARS CAN BE REMOVED FROM DRIVEWAYS. WORK MUST BE PLANNED TO MINIMIZE THE TIME DRIVEWAYS ARE OUT OF SERVICE. WORK SHALL BE SCHEDULED SO THAT DRIVES PULLED ON THE FIRST WORKING DAY ARE FORMED AND POURED BY THE THIRD WORKING DAY. BEYOND THIS 3 DAY TIME IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE AT HIS OWN EXPENSE TEMPORARY AGGREGATE FOR ACCESS TO THE DRIVEWAY.
12. DEBRIS REMOVAL - MATERIALS RESULTING FROM THE VARIOUS CONSTRUCTION OPERATIONS SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGEMENT OF THE ENGINEER, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE VILLAGE WILL REMOVE DEBRIS AND THE CONTRACTOR SHALL BE BILLED ACCORDINGLY.
13. WATER SUPPLY - THE CONTRACTOR CAN OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A WATERING BAN IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR. WATER IS AVAILABLE AT 1775 S. ASHLAND AVENUE, MUNICIPAL SERVICE CENTER FIRE HYDRANT. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER. THE SUPPLY OF WATER BY THE CONTRACTOR SHALL BE INCLUDED IN THE COST OF SUPPLEMENTAL WATERING.
14. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/HR) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/HR). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
15. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL PROVIDE TO THE VILLAGE A VIDEO TAPE COVERING FROM RIGHT-OF-WAY TO RIGHT-OF-WAY OF ALL ROADWAYS TO BE IMPROVED. THIS WORK SHALL BE INCLUDED IN THE COST OF MOBILIZATION.
16. WHEN REMOVING PAVEMENT, CURB AND GUTTER, SIDEWALK, DRIVEWAY PAVEMENT, PARKING LOT PAVEMENT, AND/OR OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKER WHICH MIGHT DISTURB UNDERGROUND PUBLIC UTILITIES WILL NOT BE PERMITTED.
17. PRIOR TO THE START OF CONSTRUCTION ACTIVITIES, THE ENGINEER AND CONTRACTOR, JOINTLY, SHALL PERFORM A VISUAL INSPECTION OF VILLAGE DRAINAGE AND UTILITY STRUCTURES TO DETERMINE THE AMOUNT OF EXISTING DEBRIS IN EACH STRUCTURE, THE CONTRACTOR WILL BE REQUIRED TO CLEAN THOSE STRUCTURES WITH DEBRIS THAT WERE CLEAN AT THE BEGINNING OF CONSTRUCTION AT NO ADDITIONAL COST TO THE CONTRACT.
18. MILLING OF PAVEMENT SHALL BE DONE SO AS TO NOT DAMAGE THE ADJACENT CURB OR STRUCTURES. REMOVAL ADJACENT TO THESE STRUCTURES SHALL BE ACCOMPLISHED TO THE SATISFACTION OF THE ENGINEER AND MAY REQUIRE HANDWORK.
19. THE CONTRACTOR SHALL PROVIDE TWO (2) SUITABLE TEMPORARY TOILET FACILITIES ALONG THE STREET, UNLESS OTHERWISE APPROVED BY THE ENGINEER, FOR USE OF ALL CONTRACTOR'S PERSONNEL EMPLOYED ON THE WORK, AND SHALL MAINTAIN SAME IN PROPER SANITARY CONDITION. AT COMPLETION, THE FACILITIES SHALL BE REMOVED AND THE PREMISES LEFT CLEAN. THE LOCATION OF THE TEMPORARY TOILETS SHALL BE APPROVED BY THE ENGINEER. COST FOR TOILET FACILITIES WILL NOT BE PAID FOR SEPARATELY BUT SHOULD BE INCLUDED IN THE COST OF MOBILIZATION.
20. THE CONTRACTOR SHALL REMOVE FROM THE PROJECT SITE ALL UNSUITABLE AND SURPLUS EXCAVATED MATERIAL NOT USED OR BACKFILLED. THE WASTE EXCAVATED MATERIAL SHALL NOT BE DEPOSITED IN PUBLIC OR PRIVATE PROPERTY UNLESS THE CONTRACTOR FIRST OBTAINS THE WRITTEN PERMISSION FROM THE PROPERTY OWNER OR THE AUTHORIZED REPRESENTATIVE OF THE APPROPRIATE PUBLIC AGENCY.
21. THE CONTRACTOR SHALL KEEP ONE (1) COPY OF ALL SPECIFICATIONS, DRAWINGS, ADDENDA, MODIFICATIONS, AND SHOP DRAWINGS AT THE SITE ON GOOD ORDER AND ANNOTATED TO SHOW ALL CHANGES MADE DURING THE CONSTRUCTION PROCESS. THE RECORD DRAWINGS SHALL BECOME THE PROPERTY OF THE VILLAGE.
22. THE CONTRACTOR SHALL MAINTAIN HANDICAP ACCESS TO ANY HOME DEEMED NECESSARY BY THE VILLAGE BY PROVIDING AND INSTALLING MATERIALS REQUIRED FOR REASONABLE INGRESS AND EGRESS AT ALL TIMES. THE COST OF THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN TO THE CONTRACT.
23. ADD THE FOLLOWING STATEMENT TO THE END OF SECTION 105.06: "THE CONTRACTOR SHALL NOT CHANGE HIS SUPERINTENDENT WITHOUT WRITTEN PERMISSION OF THE VILLAGE."
24. NO METAL RINGS SHALL BE USED FOR FRAME ADJUSTMENTS. THE RINGS AND FRAMES SHALL BE SET ON TWO CONCENTRIC RINGS OF CON-SEAL OR PRESS-SEAL "TAR ROPE". MOSTIC SHALL BE APPLIED WITH A TROWEL ON THE OUTSIDE OF THE RINGS, WHILE THE INSIDE SHALL BE PAINTED WITH A FINE CEMENT GROUT. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE STRUCTURE ADJUSTMENTS.



VILLAGE OF HOMEWOOD  
ENGINEERING  
DIVISION

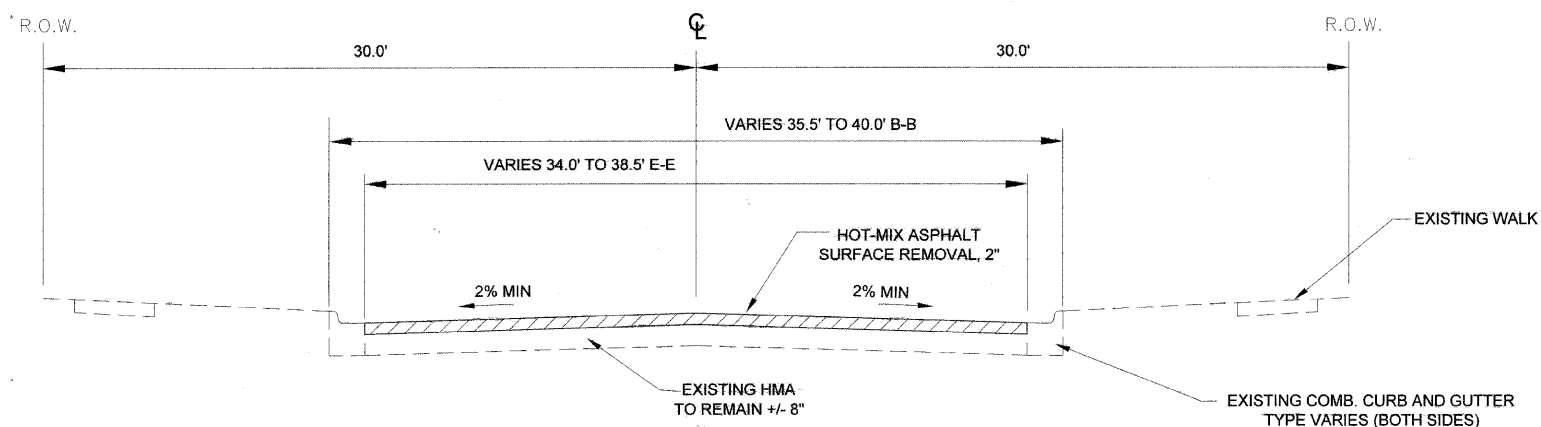
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		DRAWN	DJA	REVISED	--
PLOT SCALE	--	CHECKED	MM	REVISED	--
PLOT DATE	--	DATE	04-28-10	REVISED	--

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

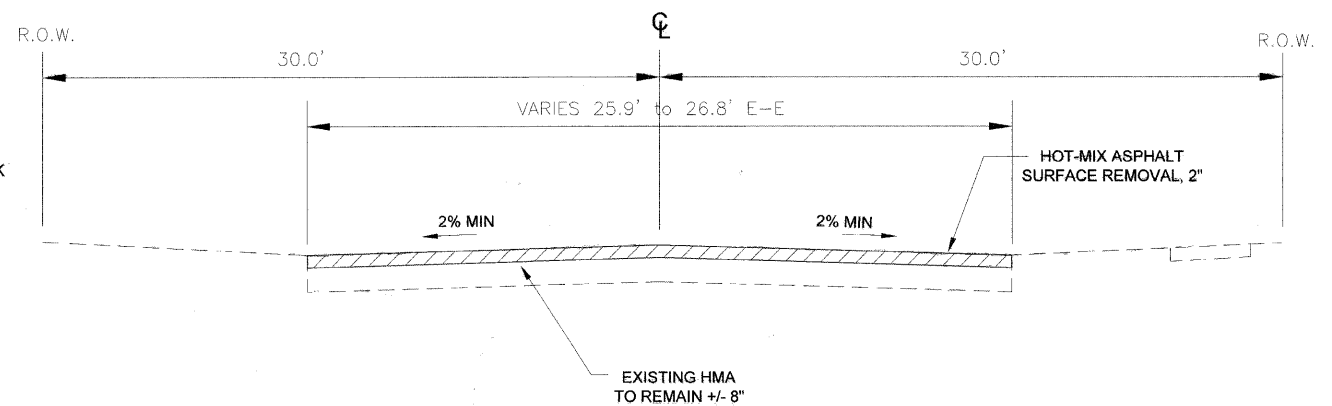
**GENERAL NOTES**

SCALE: NONE    SHEET NO. 3 OF 12 SHEETS    STA.    TO    STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	10-00131-00-RS	COOK	12	3
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 63480	
FED. AID PROJECT				



**EXISTING TYPICAL SECTION**  
 STA. 0+00 to STA. 2+41.93, HARWOOD AVENUE



**EXISTING TYPICAL SECTION**  
 STA. 8+19.19 to STA. 30+89.16, HARWOOD AVENUE



VILLAGE OF HOMEWOOD  
 ENGINEERING  
 DIVISION

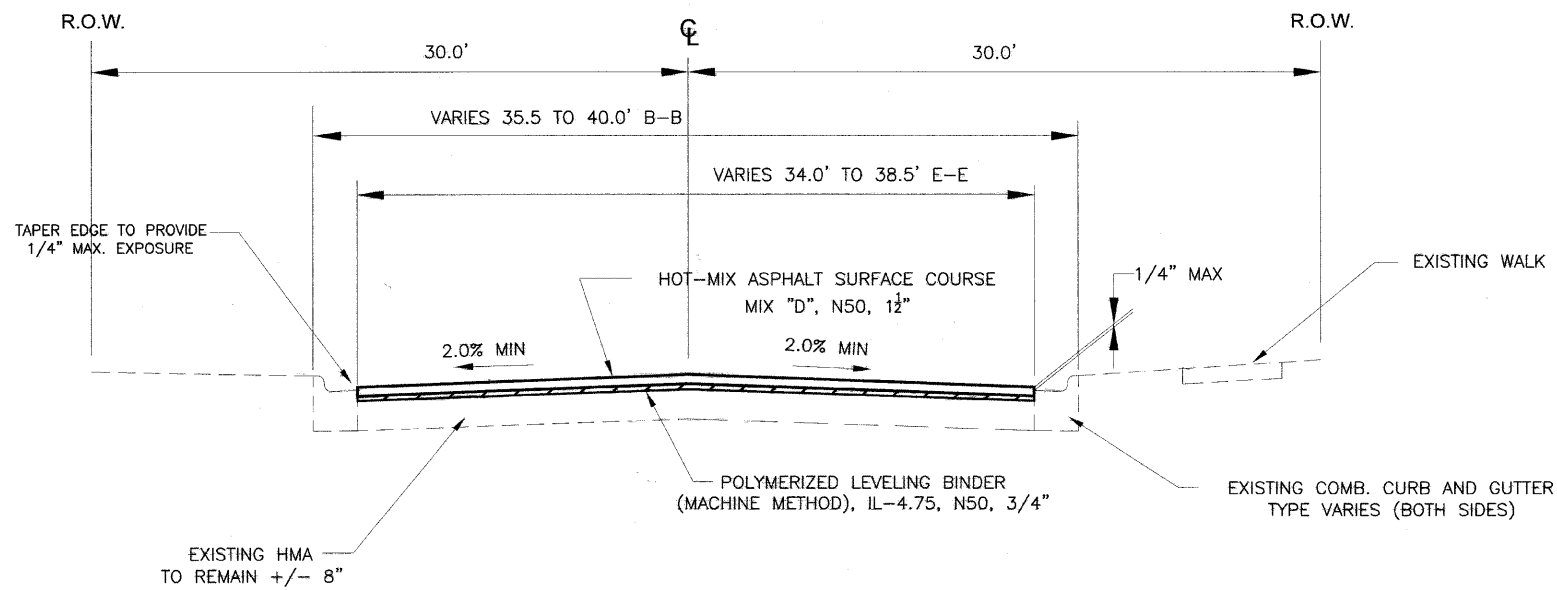
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		DATE	04-28-10	REVISED	--

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

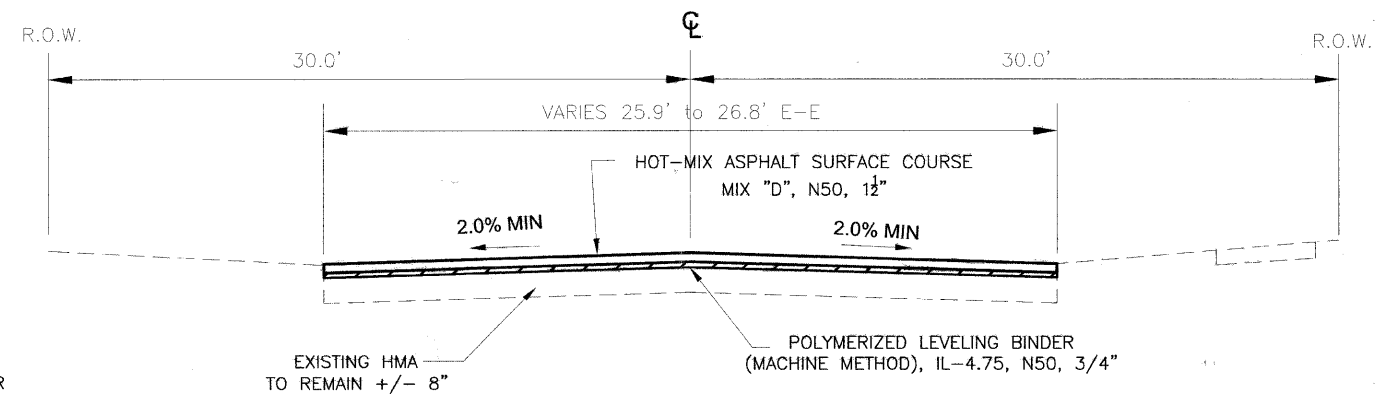
EXISTING TYPICAL SECTIONS

SCALE: NONE SHEET NO. 4 OF 12 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	10-00131-00-RS	COOK	12	4
CONTRACT NO.			63480	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**PROPOSED TYPICAL SECTION**  
 STA. 0+00 to STA. 2+41.93, HARWOOD AVENUE



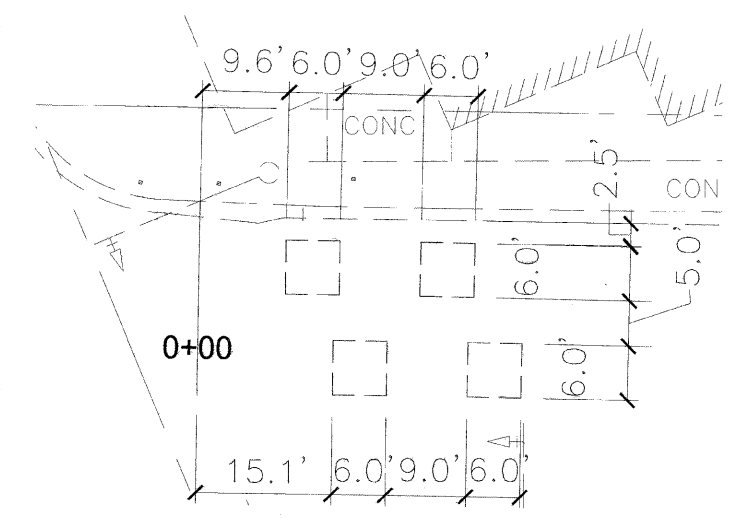
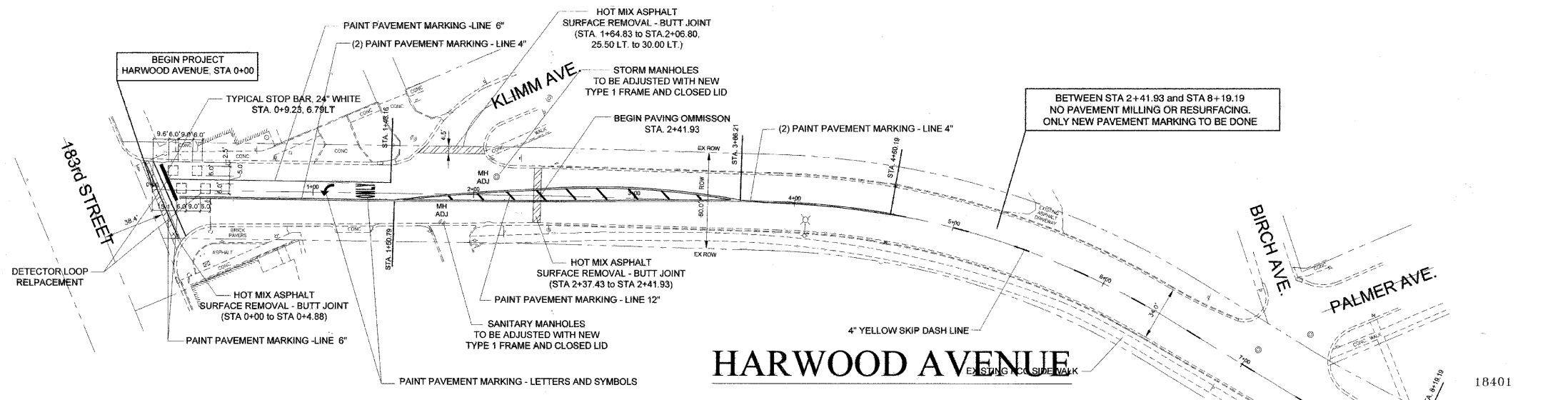
**PROPOSED TYPICAL SECTION**  
 STA. 8+19.19 to STA. 30+89.16, HARWOOD AVENUE

NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING.  
**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

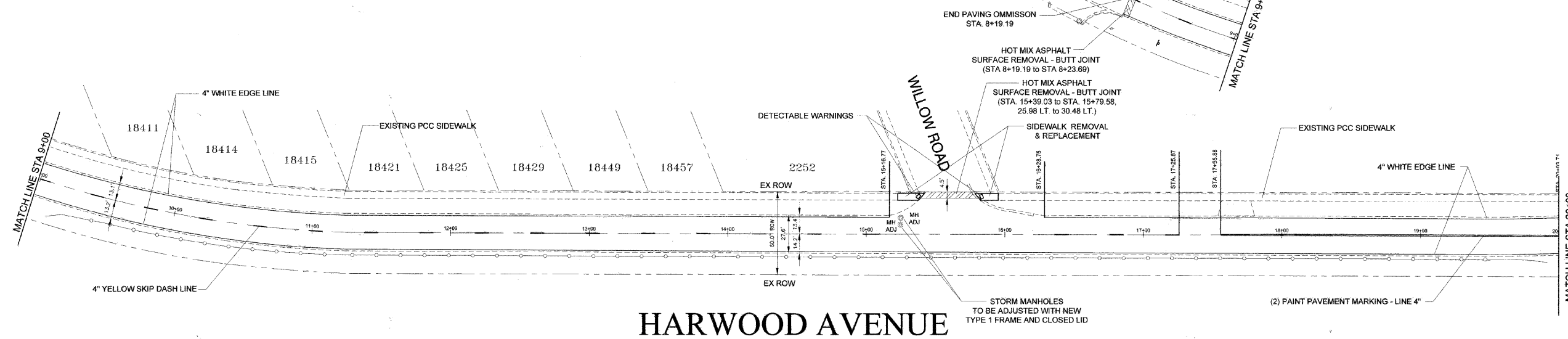
MIXTURE TYPE	VOIDS @ Ndes
<b>PAVEMENT RESURFACING</b>	
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	4% @ 50 GYR
HOT-MIX ASPHALT SURFACE COURSE MIX "D", N50 (IL-9.5mm)	4% @ 50 GYR
<b>DRIVEWAYS</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL-9.5mm), 3"	4% @ 50 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

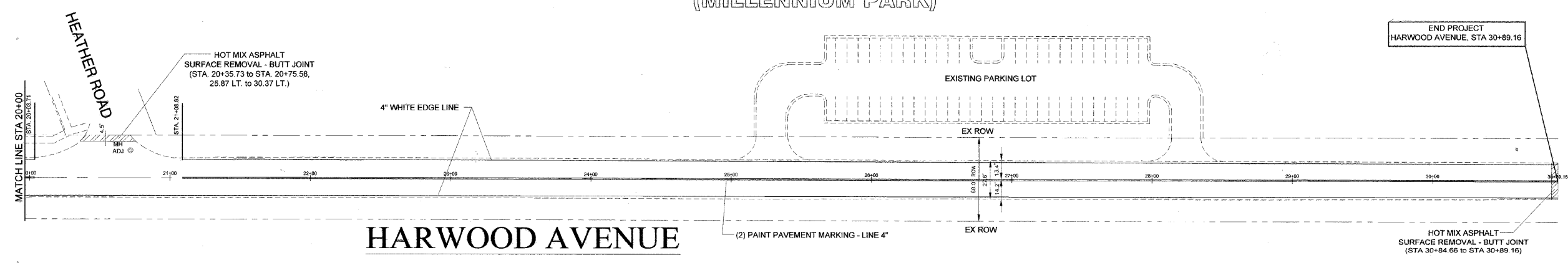


**DETECTOR LOOP LAYOUT**  
HARWOOD AVENUE & 183rd STREET



**HARWOOD AVENUE**

(MILLENNIUM PARK)



**HARWOOD AVENUE**

VILLAGE OF HOMEWOOD  
ENGINEERING  
DIVISION

USER NAME	--	DESIGNED	DJA	REVISED	--
DRAWN	DJA	CHECKED	MM	REVISED	--
DATE	04-28-10	REVISED	--		

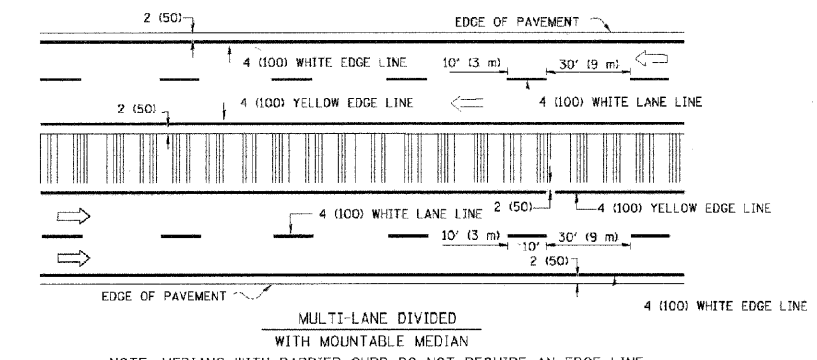
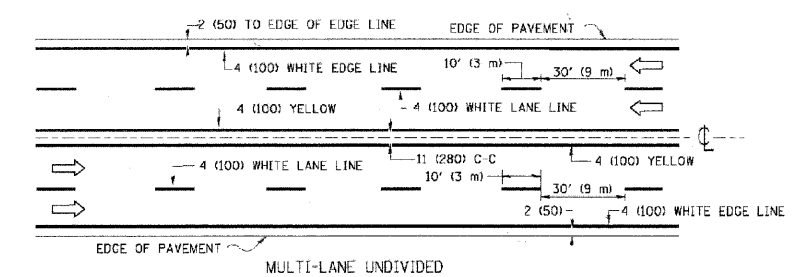
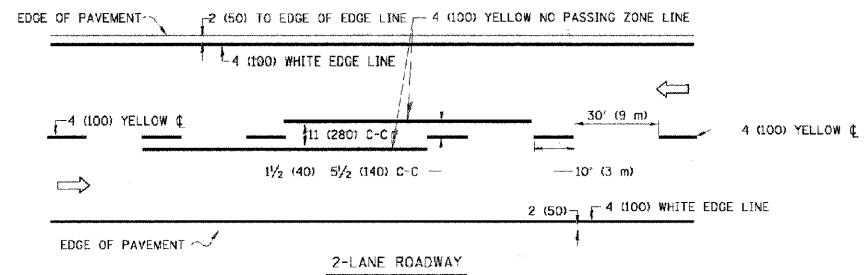
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**EXISTING & PROPOSED  
ROADWAY AND PAVEMENT MARKINGS  
STA. 0+00 TO STA. 30+89.16**

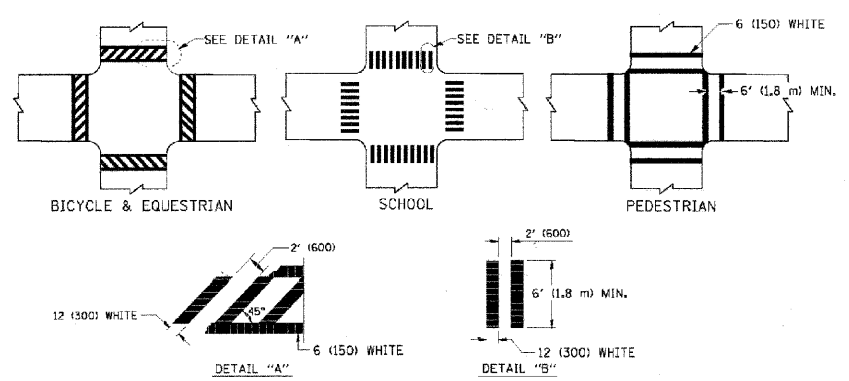
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	10-00131-00-RS	COOK	12	6
CONTRACT NO. 63480				

SCALE: 1"=40.0' SHEET NO. 6 OF 12 SHEETS STA. 0+00 TO STA. 30+89.16

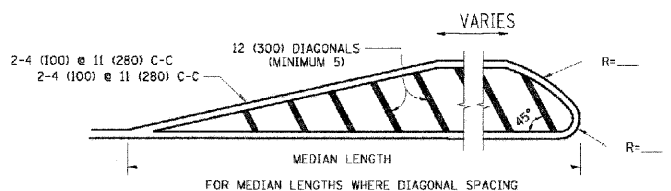
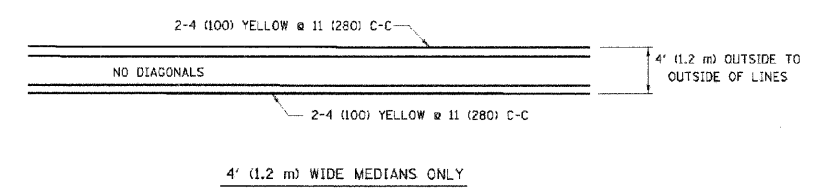
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



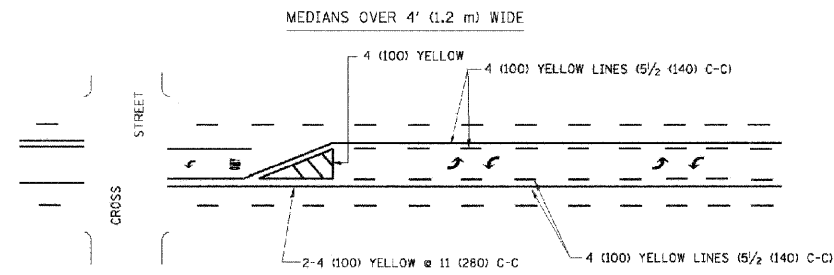
TYPICAL LANE AND EDGE LINE MARKING



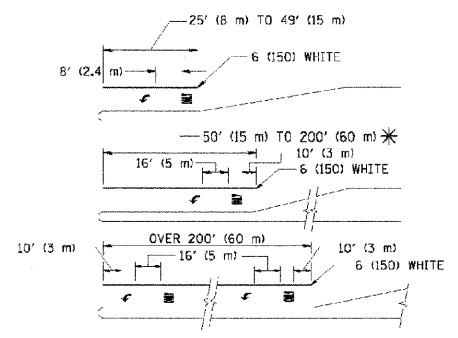
TYPICAL CROSSWALK MARKING



FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

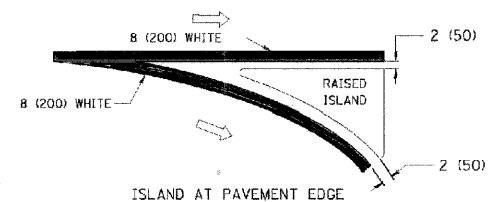
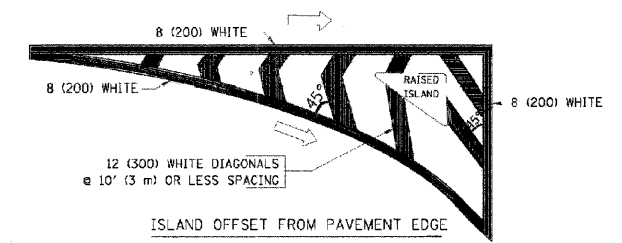


TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

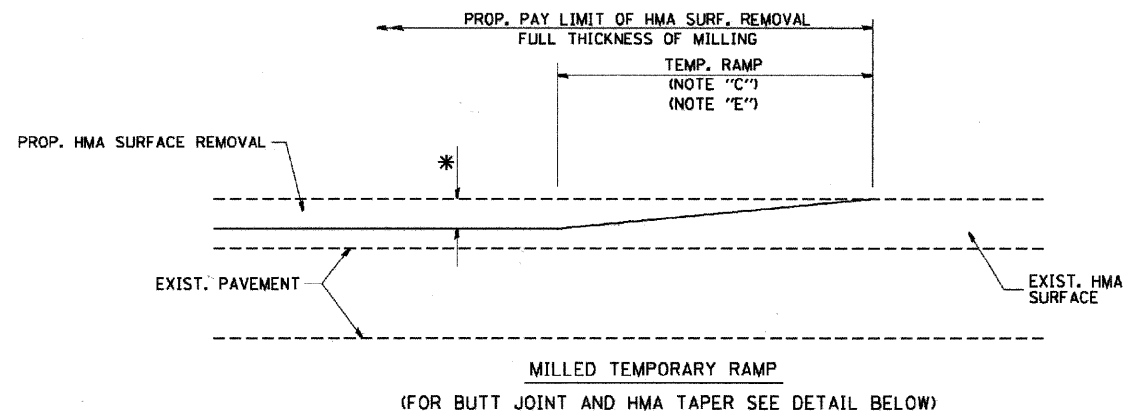
F.A.U. ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.

ILLINOIS PROJECT \*

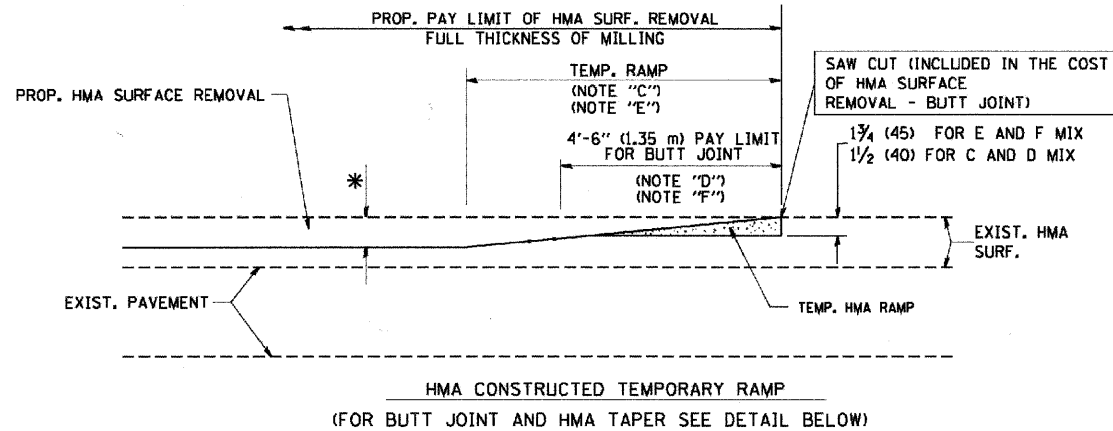
DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
DRAWN -	REVISED - C. JUCIUS 09-09-09
CHECKED -	REVISED -
DATE 03-19-90	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

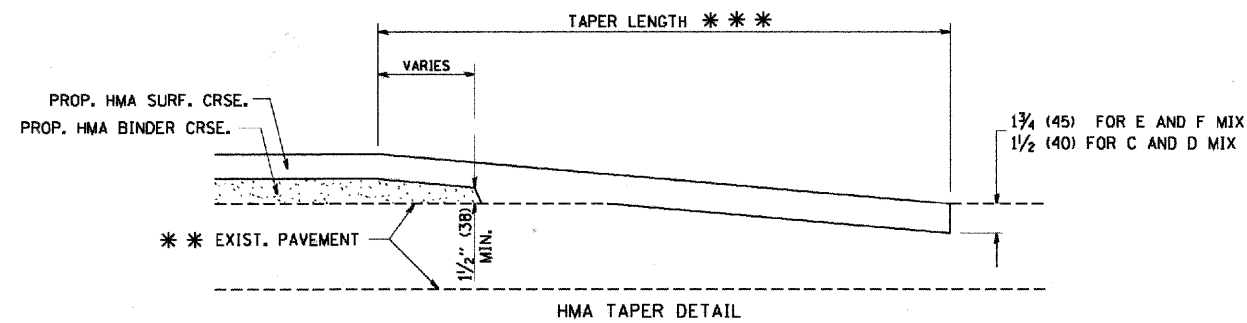
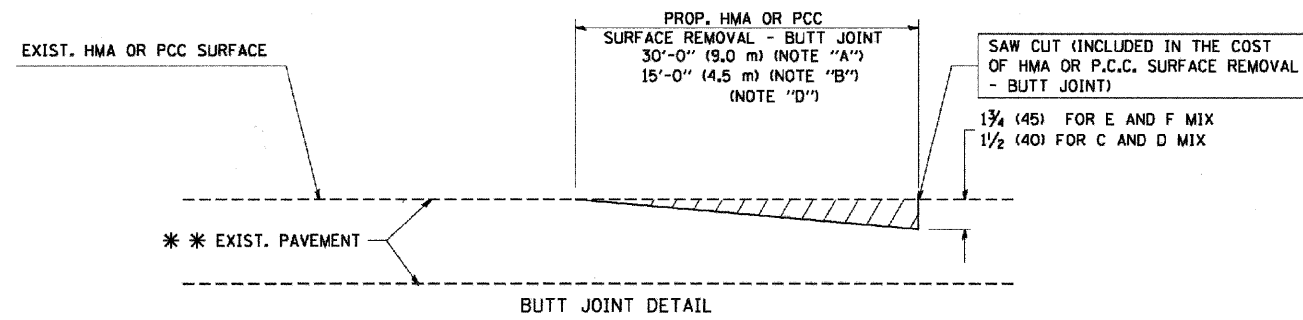
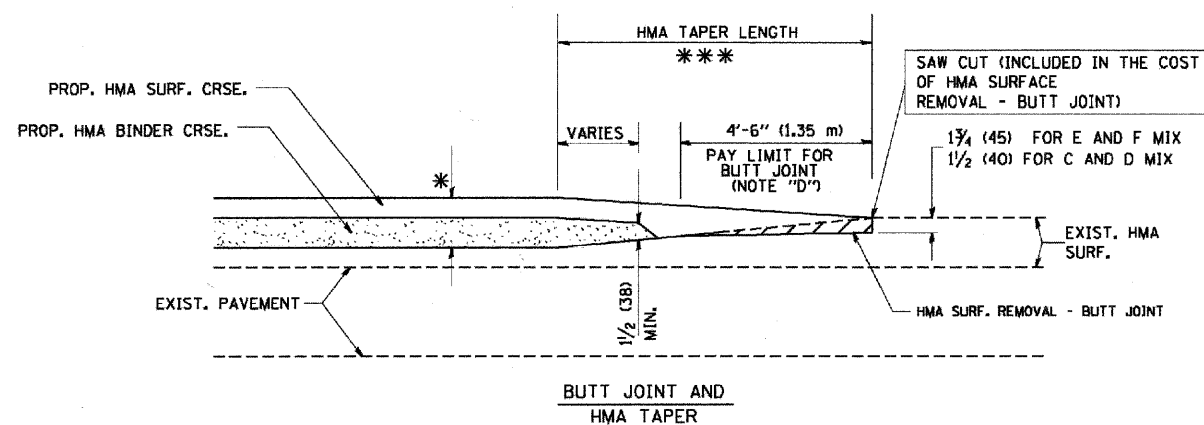
DISTRICT ONE TYPICAL PAVEMENT MARKINGS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		2845	10-00131-00-RS	COOK	12	7
		TC-13		CONTRACT NO. 63480		
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



OPTION 1



OPTION 2  
 TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. COMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: VERT. NONE  
 HORIZ. NONE  
 PLOT DATE: 1/18/2007

DRAWN BY  
 CHECKED BY

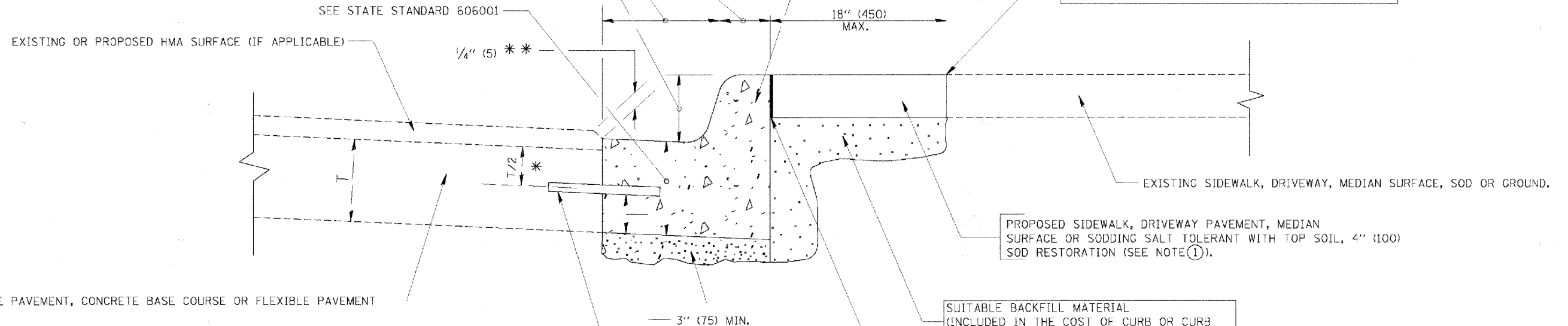
BD400-05 (V1-BD32)  
 REVISION DATE: 01/01/07



VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

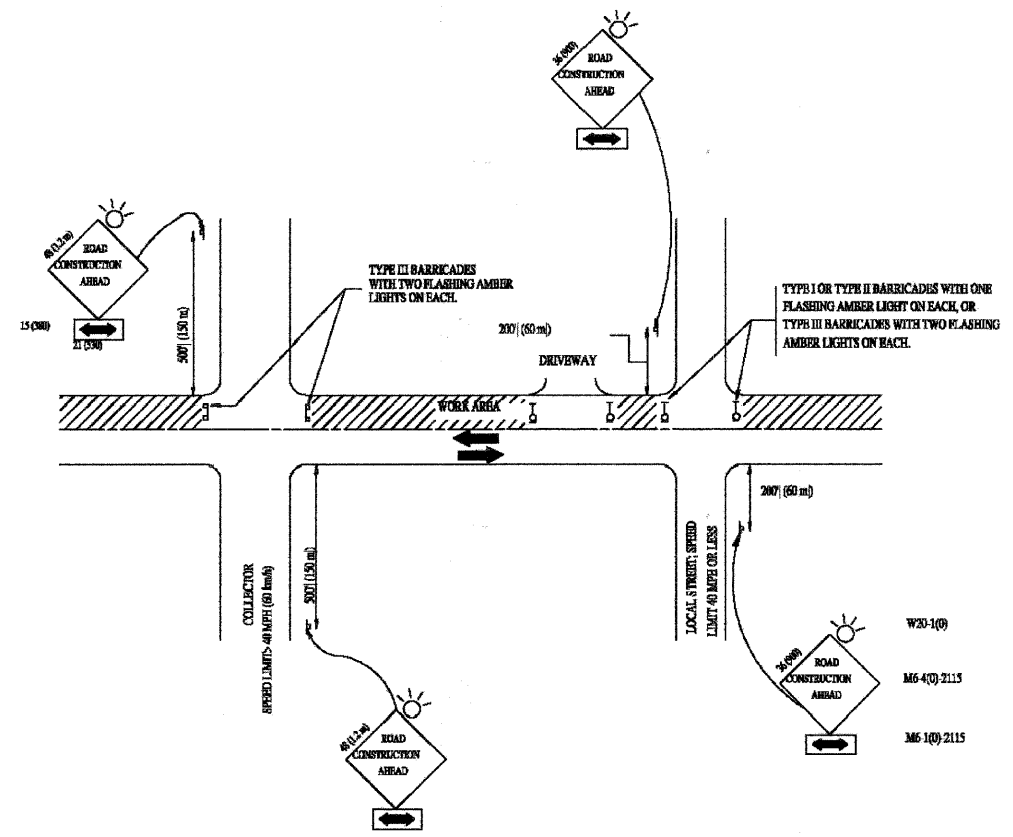
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME -	USER NAME -	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN	REVISED A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	2845	10-00131-00-RS	COOK	12	9
		CHECKED	REVISED M. GOMEZ 01-22-01				TO STA.		<b>BD600-06 (BD-24)</b>			
		DATE - 03-11-94	REVISED R. BORO 12-15-09					CONTRACT NO. 63480				
							FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT					



**TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

**NOTES:**

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS:
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (914x914) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1219 x 1219) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (152 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE-LEADED ARROW (M6-5) SHALL BE USED IN LIEU OF THE DOUBLE-LEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 7039), STD. 7036 OR THE APPROPRIATE STANDARD. THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY CLOSURE OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILENAME =	USERNAME =	DESIGNED =	LEA	REVISED =	1. 01/01/21 10:18:55
W:\s\2021\2101\2101.dwg	pgj/mst	DRAWN =	EMAWN	REVISED =	A. BOUSSE 05-06-16
		CHECKED =	C. CHURCH	REVISED =	A. FOLINER 10-15-16
		DATE =	10-20	REVISED =	T. BAUMACHER 01-06-10

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

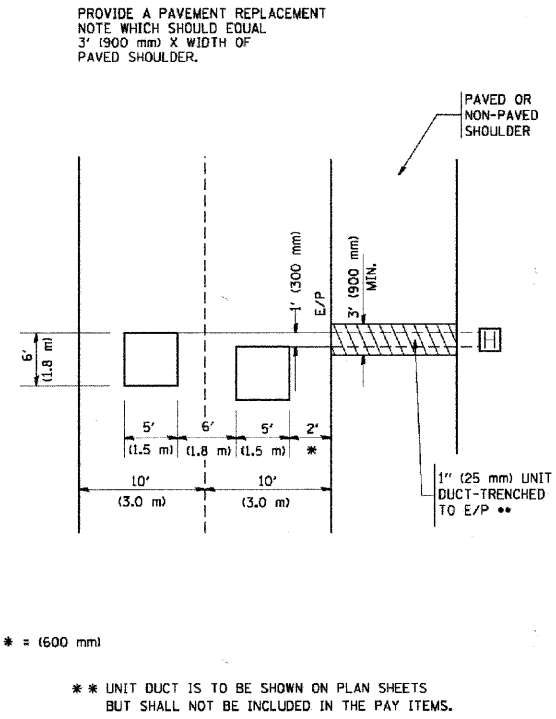
**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

STA. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	10-00127-00-RS	COOK	12	10
TC-10		CONTRACT NO. 63480		

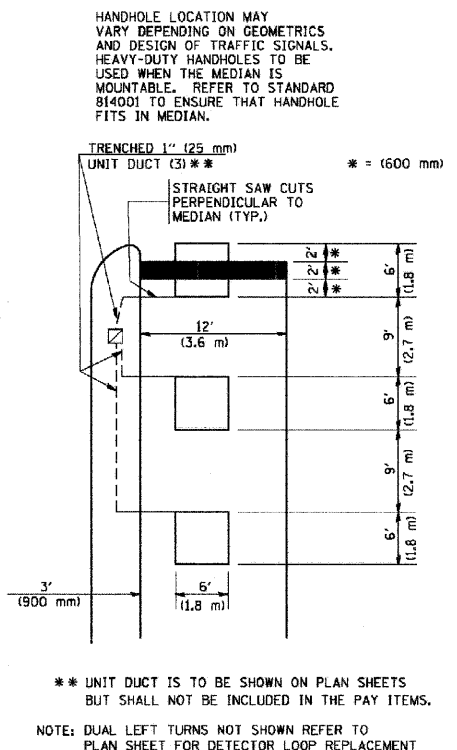
SCALE: NONE SHEET NO. 10 OF 12 SHEETS STA. TO STA. THESE REVISIONS SHALL BE SHOWN THE APPROVED

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	10-00131-00-RS	COOK	12	11
STA.		TO STA.		FED. AID PROJECT

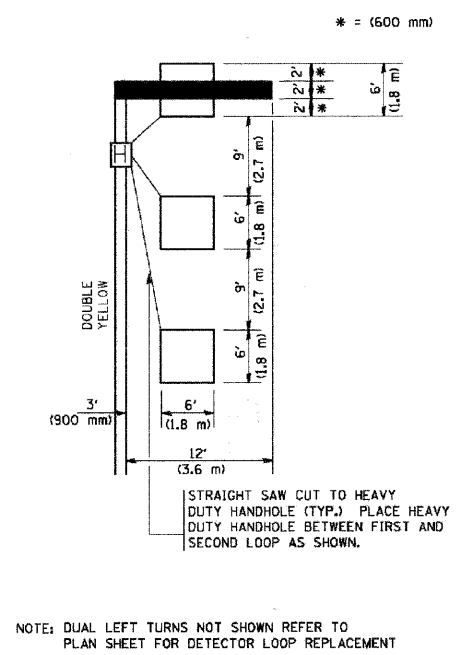
LOOPS NEXT TO SHOULDERS



LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)

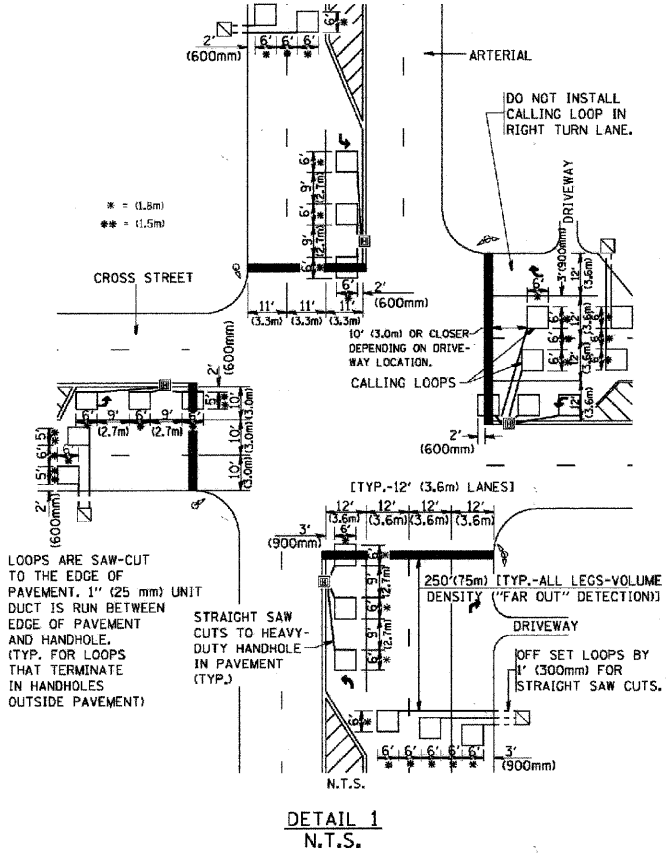


LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)

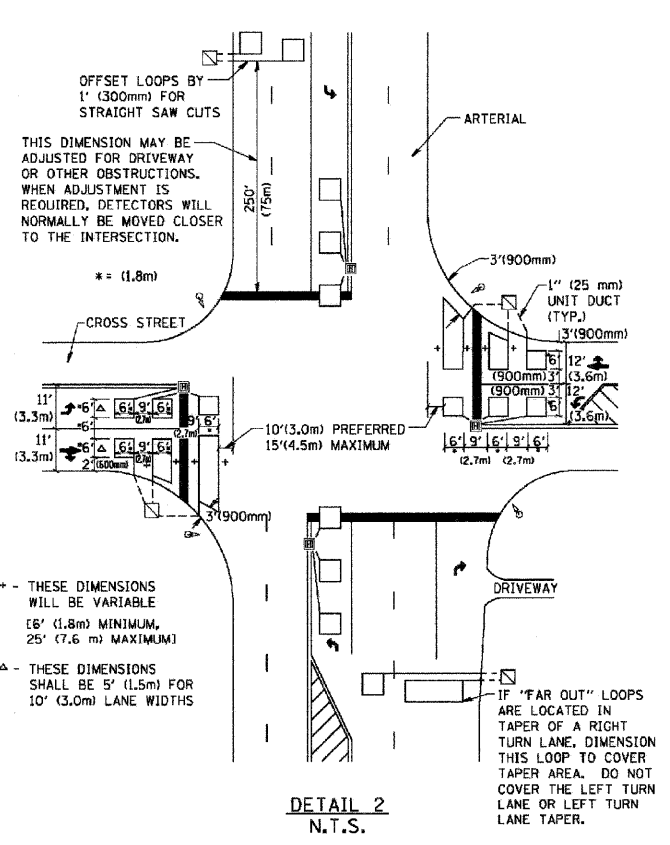


- NOTES:**
- VEHICLES LOOP DETECTORS**
- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
  - \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
  - \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
  - \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
  - \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
  - \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
  - \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



- PLACEMENT OF DETECTORS**
- THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.
- LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.
- "FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.
- NOTE:**
- ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995
- THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
DISTRICT 1  
DETECTOR LOOP  
INSTALLATION DETAILS  
FOR ROADWAY RESURFACING

DESIGNED BY  
DRAWN BY CADD  
CHECKED BY R.K.F.  
TS07  
REVISION DATE:

SCALE: NONE  
DATE: 2/15/2006

PLOT DATE = 2/15/2006  
FILE NAME = C:\WORK\2006\1007.dgn  
PLOT SCALE = 48.0000 / IN.  
USER NAME = gregjennett

