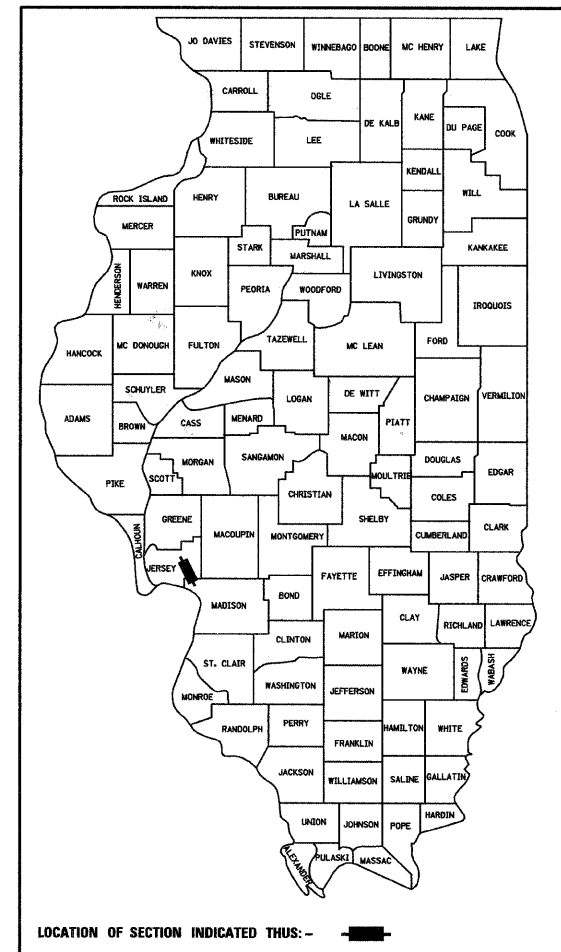


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
HIGHWAY IMPROVEMENT

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B, 42-1&1B	MADISON AND JERSEY	481	1
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	310
D-98-034-00		CONTRACT NO. 76318		



FAP ROUTE 310 (US 67)
SECTION 60-16-1&1B, 42-1&1B
MADISON CO. LINE NORTH OF
GODFREY TO DELHI BYPASS
FOUR-LANE EXPRESSWAY
MADISON AND JERSEY COUNTY

PROJ. ACNHF-0310(141)

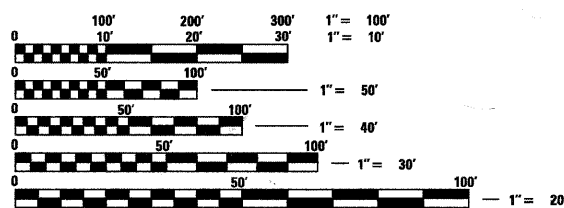
PROJECT ENGINEER: PATTI J. LEBEAU
SQUAD LEADER: CHERYL KEPLAR

PHONE NO.: 1-618-346-3179

PHONE NO.: 1-618-346-3186

DESIGN DESIGNATION
FAP 310 - 2021 ADT = 20,000 EXPRESSWAY - 6.41 (FD-20)
2001 ADT = 11,500
2015 ADT = 17,450

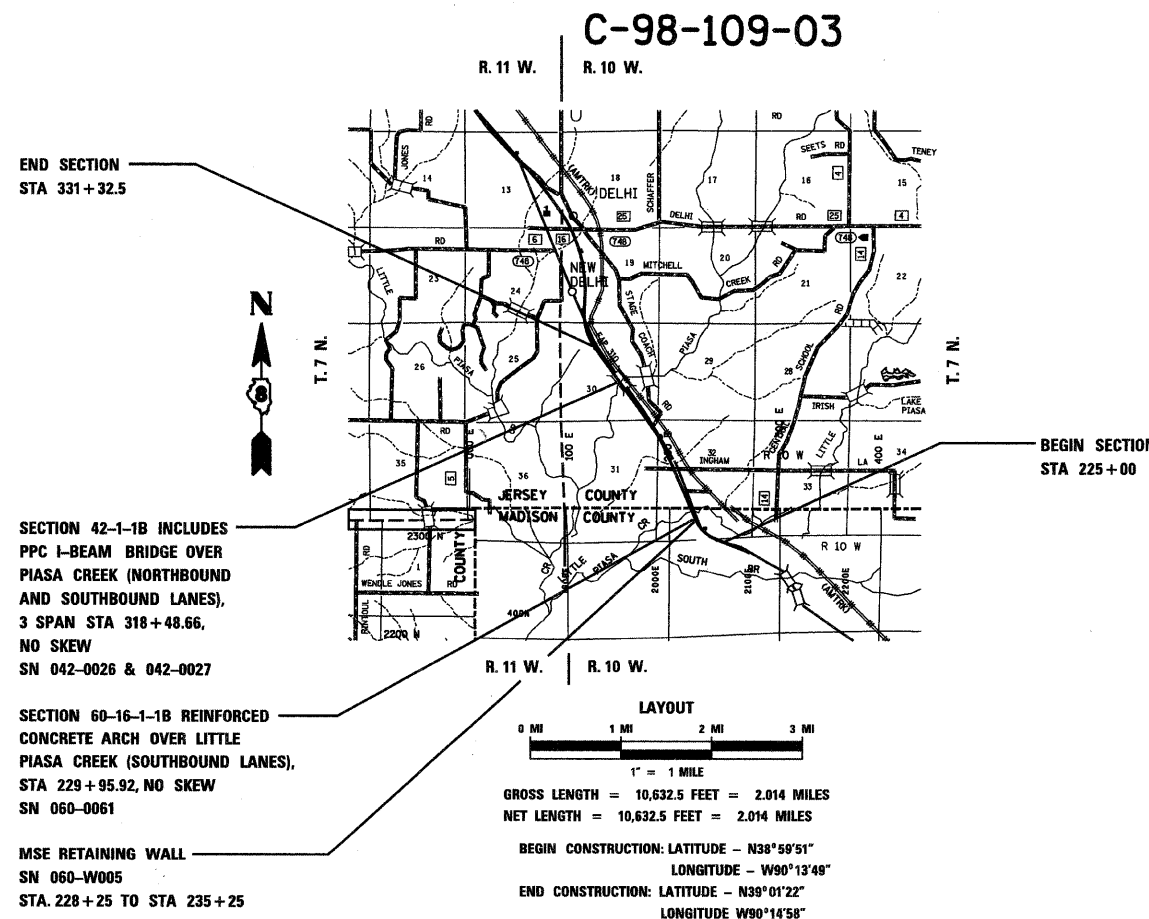
MICROFILMED _____
REEL NUMBER _____
AWARDED _____
RESIDENT ENGINEER _____
AS BUILT CHANGES WERE MADE
ON THE FOLLOWING SHEETS



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

CONTRACT NO. 76318



SECTION 42-1-1B INCLUDES
PPC I-BEAM BRIDGE OVER
PIASSA CREEK (NORTHBOUND
AND SOUTHBOUND LANES),
3 SPAN STA 318+48.66,
NO SKEW
SN 042-0026 & 042-0027

SECTION 60-16-1-1B REINFORCED
CONCRETE ARCH OVER LITTLE
PIASSA CREEK (SOUTHBOUND LANES),
STA 229+95.92, NO SKEW
SN 060-0061

MSE RETAINING WALL
SN 060-W005
STA. 228+25 TO STA 235+25

LAYOUT
0 MI 1 MI 2 MI 3 MI
1" = 1 MILE
GROSS LENGTH = 10,632.5 FEET = 2.014 MILES
NET LENGTH = 10,632.5 FEET = 2.014 MILES
BEGIN CONSTRUCTION: LATITUDE - N38°59'51"
LONGITUDE - W90°13'49"
END CONSTRUCTION: LATITUDE - N39°01'22"
LONGITUDE W90°14'58"



EXPIRES Nov 30, 2011
RESPONSIBLE FOR SHEETS W/
HOMER L CHASTAIN LOGO
AND ALL SHEETS WITHOUT A LOGO



EXPIRES Nov 30, 2011
RESPONSIBLE FOR SHEETS W/
HUTCHISON ENGINEERING LOGO



EXPIRES Nov 30, 2011
RESPONSIBLE FOR SHEETS W/
HURST-ROSCH ENGINEERING LOGO

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED March 18 20 10
Mary C. James
DEPUTY DIRECTOR OF HIGHWAYS REGION FIVE ENGINEER

PASSED May 7 20 10
Scott E. Stitt, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

APPROVED May 7 20 10
Christine M. Roadler
DIRECTOR, DIVISION OF HIGHWAYS

HOMER L.
CHASTAIN
& ASSOCIATES, LLP
CONSULTING ENGINEERS
DECATUR (317) 422-8544
CHICAGO (773) 714-0050
ROCKFORD (815) 489-0050
184-001397

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET NO	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STANDARDS, AND GENERAL NOTES
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406-411	CROSS SECTIONS - CONNECTOR
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426-442	CROSS SECTIONS - NORTH INGHAM
443-449	CROSS SECTIONS - INGHAM RUNAROUND
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477-481	CROSS SECTIONS - 335+00 PE

GENERAL NOTES

- (1) ILLINOIS STATE LAW REQUIRES A 48 HOUR NOTICE BE GIVEN TO ALL UTILITIES BEFORE DIGGING. FIELD MARKING OF FACILITIES MAY BE OBTAINED BY CONTACTING J. U. L. I. E. OR FOR NON-MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:
 - AT&T (TELEPHONE)
 - AMEREN CIPS (ELECTRIC)
 - CHARTER COMMUNICATIONS
 - FRONTIER COMMUNICATIONS, INC. (TELEPHONE)
 - NUSTAR PIPELINE, LLC (PIPELINE)
 - JERSEY COUNTY RURAL WATER CO., INC. (WATER)
 - M. J. M. ELECTRIC CO-OP, INC. (ELECTRIC)
 - AT&T CORPORATION (FIBER OPTIC)
- (2) MEMBERS OF JULIE (800-892-0123) ARE INDICATED BY AN ASTERIK * NON - J. U. L. I. E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.
- (2) THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.
- (3) THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- (4) BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH TYPE I OR TYPE II BARRICADE USED - ONE (1) WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL.
- (5) ALL DIMENSIONS SHOWN ARE IN FEET UNLESS OTHERWISE NOTED. PAVEMENT THICKNESS, ROUND CULVERT SIZE, CONDUIT SIZE AND PAVEMENT MARKING WIDTHS ARE SHOWN IN INCHES UNLESS NOTED.
- (6) SEEDING SHALL BE PLACED ON ALL AREAS THAT ARE DISTURBED BY CONSTRUCTION OPERATIONS. NUTRIENTS AND MULCH, IF REQUIRED, SHALL BE APPLIED TO ALL SEEDING AREAS. THE SEEDING SHALL BE PERFORMED ACCORDING TO ARTICLES 250 AND 251 OF THE STANDARD SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER. THE SEEDING DATES FOR BARE EARTH SEEDING OF MIXTURE CLASS 2 SHALL BE FROM APRIL 1 TO JUNE 1, AND FROM AUGUST 15 TO SEPTEMBER 30.
- (7) THE LENGTH OF ALL END SECTIONS TO BE REMOVED SHALL BE INCLUDED IN THE LENGTH OF THE PIPE CULVERT REMOVAL OR STORM SEWER REMOVAL.
- (8) ALL STATIONS AND OFFSETS FOR DRAINAGE STRUCTURES ARE GIVEN TO THE CENTER OF THE STRUCTURE.
- (9) ALL STATIONS AND OFFSETS FOR END SECTIONS ARE AT THE POINT OF CONNECTION OF THE PIPE.
- (10) ALL STATION AND OFFSETS SHOWN FOR PROPOSED PAVEMENT ARE GIVEN AT THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- (11) THE THICKNESS OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
- (12) THE HOT-MIX ASPHALT SURFACE OF ALL PRIVATE ENTRANCES, COMMERCIAL ENTRANCES AND SIDE ROADS SHALL BE MADE NEATLY AND IN A WORKMANLIKE MANNER, AND SHALL ACCURATELY CONFORM TO THE SHAPES AND DIMENSIONS SHOWN IN THE PLAN DETAILS. (IF REQUIRED BY THE ENGINEER, THE CONTRACTOR WILL BE REQUIRED TO SAW CUT THE BITUMINOUS SURFACE TO CONFORM TO THE SHAPES AND DIMENSIONS SHOWN IN THE PLAN DETAILS.)
- (13) FOR ALL TEMPORARY PAVEMENT MARKINGS ON FINAL SURFACES (I.E. MAINLINE PAVEMENT), ONLY TEMPORARY PAVEMENT MARKING TAPE SHALL BE USED. ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC. SHORT TERM PAVEMENT MARKINGS SHALL BE PLACED AND REMOVED IN ACCORDANCE WITH SECTION 703 OF THE IDOT STANDARD SPECIFICATIONS.
- (14) WHERE SECTION OR SUB-SECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL MONUMENTS OR OTHERWISE REFERENCE THEIR LOCATION. THE CONTRACTOR WILL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR RE-ESTABLISH ANY SECTION OR SUB-SECTION MONUMENTS DESTROYED BY HIS OPERATIONS.
- (15) ONLY THOSE TREES DESIGNATED BY THE ENGINEER SHALL BE REMOVED. THE CONTRACTOR SHALL PROTECT ALL REMAINING TREES FROM DAMAGE DUE TO HIS OPERATIONS.
- (16) ALL SAW CUTTING OF EXISTING PAVEMENT SHALL BE CONSIDERED AS INCLUDED IN THE VARIOUS ASSOCIATED PAY ITEMS PROVIDED IN THE CONTRACT. THE MINIMUM SAW CUT DEPTH SHALL BE FULL DEPTH UNLESS OTHERWISE SPECIFIED IN THE PLANS, STANDARD SPECIFICATIONS, SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.
- (17) UNLESS OTHERWISE DIRECTED BY THE ENGINEER, RIGHT OF WAY MARKERS SHALL BE INSTALLED AS A FIRST ITEM OF CONSTRUCTION ON THIS CONTRACT.
- (18) ALL ELEVATIONS REFER TO U.S.G.S. MEAN SEA LEVEL DATUM.
- (19) ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT OF WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.
- (20) ALL BUILDING DEMOLITION AND ASSOCIATED SITE CLEARING WILL BE PERFORMED BY OTHERS UNDER A SEPARATE CONTRACT.
- (21) EXCAVATION NECESSARY FOR INSTALLATION OF RIPRAP SHALL BE CONSIDERED AS INCLUDED IN THE UNIT COST OF THE RIPRAP.
- (22) THE THICKNESS OF LIME MODIFIED SOIL SHALL BE DETERMINED BASED ON FIELD DCP MEASUREMENTS AND EXISTING SOIL BORING INFORMATION. THE THICKNESS SHOWN IN THE PLANS HAS BEEN ESTIMATED.
- (23) RIGHT OF WAY MARKERS SHALL BE SET SO THE BACK OF THE POST IS 12 INCHES INSIDE THE RIGHT OF WAY BOUNDARY. RIGHT OF WAY CORNERS ARE MARKED BY A 5/8 INCH IRON ROD WITH AN IDOT ALUMINUM CAP AND SHALL NOT BE REMOVED, DAMAGED OR OTHERWISE DISTURBED WHEN SETTING THE RIGHT OF WAY MARKERS.
- (24) IF THERE IS A DISCREPANCY BETWEEN THE RIGHT OF WAY SHOWN IN THE PLANS AND SHOWN ON THE PLAT OF HIGHWAYS, THE PLAT OF HIGHWAYS SHALL TAKE PRECEDENCE.

COMMITMENTS:

(1) ACCESS PURCHASE AGREEMENTS ALONG DOMINO LANE SERVICE DRIVE SHALL CLEARLY STATE THE ENTRANCES ARE FOR PRIVATE RESIDENTIAL OR FARM ACCESS ONLY.

GENERAL NOTES CONTINUED

- (25) ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUB-NUMBER LISTED IN THE INDEX OF SHEETS OR THE COPY OF STANDARD INCLUDED IN THESE PLANS.
- (26) REMOVAL OF EXISTING HOT-MIX ASPHALT SURFACE (OIL AND CHIP), AGGREGATE BASE COURSE, AGGREGATE SURFACE COURSE AND AGGREGATE SHOULDERS SHALL BE CONSIDERED INCLUDED IN EARTH EXCAVATION.
- (27) SIDEROAD DESIGN DESIGNATION IS AS FOLLOWS:
 - QUARRY ROAD: ADT = 100(2001) 100(2021) SU(70%) MU(20%)
 - DOMINO LANE SERVICE ROAD: ADT = 150(2001) 250(2021) SU(0.6%) MU(0.5%)
 - NORTH INGHAM: ADT = 150(2001) 250(2021) SU(0.6%) MU(0.5%)
 - INGHAM ACCESS DRIVE: ADT = 50(2001) 50(2021) SU(10%) MU(1%)
 - STAGECOACH ROAD: ADT = 150(2001) 250(2021) SU(0.6%) MU(0.5%)
 - D&R ACCESS ROAD: ADT = 50(2001) 50(2021) SU(10%) MU(1%)

LIST OF STATE STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
202001-01	EARTH MEDIAN DITCH CHECK
280001-05	TEMPORARY EROSION CONTROL SYSTEMS
420401-08	BRIDGE APPROACH PAVEMENT CONNECTOR
482001-02	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
515001-03	NAME PLATE FOR BRIDGES
542101-02	REINFORCED CONCRETE END SECTIONS FOR PIPE CULVERTS, 375MM (15") THRU 900MM (36") DIAMETER AT RIGHT ANGLES WITH ROADWAY
542111-02	REINFORCED CONCRETE END SECTIONS FOR PIPE CULVERTS, 1650MM (66") THRU 2100MM (84") DIAMETER AT RIGHT ANGLES WITH ROADWAY
542301-02	PRECAST REINFORCED CONCRETE FLARED END SECTION
542311-01	GRATING FOR CONCRETE FLARED END SECTION (FOR 600MM (24") THRU 1350MM (54") PIPE)
542501-02	INLET BOX TYPE 600 (24) A
542511-02	INLET BOX TYPE 600 (24) C
542521-02	INLET BOX TYPE 600 (24) E
542531-03	INLET BOX TYPE G, 600 MM (24")
542606-01	REINFORCED CONCRETE PIPE TEE
601001-03	SUB-SURFACE DRAINS
601101-01	CONCRETE HEADWALL FOR PIPE DRAIN
602101-02	DRAINAGE STRUCTURES, TYPES 1, 2 AND 3
602401-02	MANHOLE, TYPE A
604071-04	FRAME AND GRATE, TYPE 20
604101-01	MEDIAN INLET FOR 600MM (24") REINFORCED CONCRETE PIPE
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
630001-08	STEEL PLATE BEAM GUARDRAIL
630201-06	PCC/HOT-MIX ASPHALT STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
631011-06	TRAFFIC BARRIER TERMINAL, TYPE 2
631026-05	TRAFFIC BARRIER TERMINAL, TYPE 5
631031-08	TRAFFIC BARRIER TERMINAL, TYPE 6
635001-01	DELINEATORS
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
642001-01	SHOULDER RUMBLE STRIPS
666001-01	RIGHT-OF-WAY MARKERS
667001-01	DRAINAGE MARKERS
667101-01	PERMANENT SURVEY MARKERS
701001-02	OFF-ROAD OPERATION 2L, 2W, MORE THAN 4.5M (15') AWAY
701006-03	OFF-ROAD OPERATION 2L, 2W, 4.5M (15') TO 600 MM (24") FROM PAVEMENT EDGE
701011-02	OFF-ROAD MOVING OPERATIONS 2L, 2W DAY ONLY
701101-02	OFF-ROAD OPERATIONS, MULTILANE, 4.5M (15') TO 600MM (24") FROM PAVEMENT EDGE
701106-02	OFF-ROAD OPERATIONS, MULTILANE MORE THAN 4.5M (15') AWAY
701201-03	LANE CLOSURE, 2L, 2W DAY ONLY FOR SPEEDS > OR = 45 MPH
701301-03	LANE CLOSURE 2L, 2W SHORT TIME OPERATIONS
701326-03	LANE CLOSURE 2L, 2W PAVEMENT WIDENING FOR SPEEDS > OR = 45 MPH
701331-03	LANE CLOSURE 2L, 2W WITH RUN-AROUND FOR SPEEDS > OR = 45 MPH
701400-04	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701416-06	LANE CLOSURE, FREEWAY/EXPRESSWAY, WITH CROSSOVER AND BARRIER
701901-01	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-02	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS
729001-01	APPLICATION OF TYPE A AND B METAL POSTS (FOR SIGNS AND MARKERS)
780001-02	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
BLR 22-6	TYP. APPL. OF T.C.D. FOR RURAL LOC. HWY.(2-LANE 2 WAY RURAL TRAF.)(RD. CLOSED TO THRU TRAF.)

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	2
STA		TO STA		
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**INDEX OF SHEETS, STANDARDS,
 AND GENERAL NOTES**
 FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTY
 DRAWN BY: EMR
 CHECKED BY: GLF

SUMMARY OF QUANTITIES

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
310	60-16-1A1B,42-1A1B	MADISON AND JERSEY	481	3
STA		TO STA		
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
				JERSEY COUNTY				MADISON COUNTY
				80% FEDERAL 20% STATE ROADWAY	80% FEDERAL 20% STATE BRIDGE (ARCH)	80% FEDERAL 20% STATE BRIDGE (BEAM)	80% FEDERAL 20% STATE RETAINING WALL	80% FEDERAL 20% STATE ROADWAY
CODE NUMBER	ITEM	UNITS	TOTAL	I000-2A	X024-2A	X081-2A	Y007	I000-2A
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	252	252				
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	4,065	4,065				
20100500	TREE REMOVAL, ACRES	ACRE	20.40	16.00				4.40
20200100	EARTH EXCAVATION	CU YD	318,833	318,389				444
20300100	CHANNEL EXCAVATION	CU YD	3,593	3,593				
20400800	FURNISHED EXCAVATION	CU YD	207,793	205,293				2,500
20700400	POROUS GRANULAR EMBANKMENT, SPECIAL	CU YD	430	430				
20800150	TRENCH BACKFILL	CU YD	2500	2400				100
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	61,790	61,790				
25000200	SEEDING, CLASS 2	ACRE	38.25	37.25				1.00
25000300	SEEDING, CLASS 3	ACRE	8.00	8.00				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	4,166	4,076				90
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	4,166	4,076				90
25000600	POTASSIUM FERTILIZER NUTRIENT	PCUND	4,166	4,076				90
25000700	AGRICULTURAL GROUND LIMESTONE	TON	16.0	15.5				0.5
25100115	MULCH, METHOD 2	ACRE	36.00	35.00				1.00
25100630	EROSION CONTROL BLANKET	SQ YD	70,155	70,155				

Rev.

SUMMARY OF QUANTITIES

F&P ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1A1B,42-1A1B	MADISON AND JERSEY	481	4
STA		TO STA		
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
				JERSEY COUNTY				MADISON COUNTY
				80% FEDERAL 20% STATE ROADWAY	80% FEDERAL 20% STATE BRIDGE (ARCH)	80% FEDERAL 20% STATE BRIDGE (BEAM)	80% FEDERAL 20% STATE RETAINING WALL	80% FEDERAL 20% STATE ROADWAY
CODE NUMBER	ITEM	UNITS	TOTAL	I000-2A	X024-2A	X081-2A	Y007	I000-2A
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	11,920	11,660				260
<i>28000305</i>	<i>TEMPORARY DITCH CHECKS</i>	<i>FOOT</i>	<i>2,500</i>	<i>2,400</i>				<i>100</i>
28000400	PERIMETER EROSION BARRIER	FOOT	36,595	35,751				844
28000500	INLET AND PIPE PROTECTION	EACH	36	35				1
28100103	STONE RIPRAP, CLASS A2	SQ YD	934	934				
28100107	STONE RIPRAP, CLASS A4	SQ YD	5,635	5,562				73
28100109	STONE RIPRAP, CLASS A5	SQ YD	4,570	4,452				118
28100111	STONE RIPRAP, CLASS A6	SQ YD	522	522				
28100113	STONE RIPRAP, CLASS A7	SQ YD	991	991				
28200200	FILTER FABRIC	SQ YD	11,718	11,718				
30200650	PROCESSING MODIFIED SOIL 12"	SQ YD	63,818	56,280				7,538
30201500	LIME	TON	1,723	1,520				203
31100300	SUB-BASE GRANULAR MATERIAL, TYPE A 4"	SQ YD	607					607
31101000	SUB-BASE GRANULAR MATERIAL, TYPE B	TON	15,685	15,685				
31102300	SUB-BASE GRANULAR MATERIAL, TYPE C 6"	SQ YD	31,500	30,266				1,234
35100100	AGGREGATE BASE COURSE, TYPE A	TON	7,040	7,040				
35.01400	AGGREGATE BASE COURSE, TYPE B	TON	5,201	5,201				

Rev.

SUMMARY OF QUANTITIES

F&P ROUTE	SECTION	C	TOTAL SHEETS
310	60-16-1A1B,42-1A1B	MADISON, ILLINOIS	48
STA	TO STA	EXISTING CONDITIONS:	
		CONTRACT NO.	

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
				JERSEY COUNTY				MADISON COUNTY
				80% FEDERAL 20% STATE ROADWAY	80% FEDERAL 20% STATE BRIDGE (ARCH)	80% FEDERAL 20% STATE BRIDGE (BEAM)	80% FEDERAL 20% STATE RETAINING WALL	80% FEDERAL 20% STATE ROADWAY
CODE NUMBER	ITEM	UNITS	TOTAL	I000-2A	X024-2A	X081-2A	Y007	I000-2A
40200100	AGGREGATE SURFACE COURSE, TYPE A	TON	2,122	2,122				
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	1,000	1,000				
40300200	BITUMINOUS MATERIALS (PRIME COAT)	TON	19	19				
40300400	BITUMINOUS MATERIALS (COVER AND SEAL COATS)	TON	37	37				
40300500	COVER COAT AGGREGATE	TON	110	110				
40300600	SEAL COAT AGGREGATE	TON	110	110				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	33	31				2
40600300	AGGREGATE (PRIME COAT)	TON	1.1	1.1				
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	599	599				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	80	80				
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	494	169				325
40603090	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90	TON	8,734	8,734				
40603315	HOT-MIX ASPHALT, SURFACE COURSE, MIX "C", N70	TON	731	731				
40603345	HOT-MIX ASPHALT, SURFACE COURSE, MIX "D", N90	TON	1,939	1,939				
40701961	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 14"	SQ YD	44,420	40,986				3,434
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	107	107				
42001165	BRIDGE APPROACH PAVEMENT	SQ YD	600		600			

Rev.

SUMMARY OF QUANTITIES

F&P ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	6
STA		TO STA		
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
				JERSEY COUNTY				MADISON COUNTY
				80% FEDERAL 20% STATE ROADWAY	80% FEDERAL 20% STATE BRIDGE (ARCH)	80% FEDERAL 20% STATE BRIDGE (BEAM)	80% FEDERAL 20% STATE RETAINING WALL	80% FEDERAL 20% STATE ROADWAY
CODE NUMBER	ITEM	UNITS	TOTAL	I000-2A	X024-2A	X081-2A	Y007	I000-2A
44000100	PAVEMENT REMOVAL	SQ YD	7,495	4,325				3,170
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	2,122	2,122				
44000400	GUTTER REMOVAL	FOOT	5,250	5,250				
44004250	PAVED SHOULDER REMOVAL	SQ YD	6,244	4,836				1,408
44201815	CLASS D PATCHES, TYPE II, 14 INCH	SQ YD	14					14
44201819	CLASS D PATCHES, TYPE III, 14 INCH	SQ YD	41	41				
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	4,107	4,107				
48101200	AGGREGATE SHOULDER, TYPE B	TON	13,851	12,676				1,175
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	31,500	30,266				1,234
50100300	REMOVAL OF EXISTING STRUCTURES N1	EACH	1	1				
50100400	REMOVAL OF EXISTING STRUCTURES N2	EACH	1			1		
50102400	CONCRETE REMOVAL	CU YD	110		110			
50104400	CONCRETE HEADWALL REMOVAL	EACH	4	2				2
50104650	SLOPE WALL REMOVAL	SQ YD	150	150				
50105220	PIPE CULVERT REMOVAL	FOOT	1,611	1046				565
50200100	STRUCTURE EXCAVATION	CU YD	3,138		2,194	944		

SUMMARY OF QUANTITIES

F&P ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-181B,42-181B	MADISON AND JERSEY	481	7
STA		TO STA		
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
				JERSEY COUNTY				MADISON COUNTY
				80% FEDERAL 20% STATE ROADWAY	80% FEDERAL 20% STATE BRIDGE (ARCH)	80% FEDERAL 20% STATE BRIDGE (BEAM)	80% FEDERAL 20% STATE RETAINING WALL	80% FEDERAL 20% STATE ROADWAY
CODE NUMBER	ITEM	UNITS	TOTAL	I000-2A	X024-2A	X081-2A	Y007	I000-2A
* 50200400	ROCK EXCAVATION FOR STRUCTURES	CU YD	1,162		1,162			
50300100	FLOOR DRAINS	EACH	44			44		
50300225	CONCRETE STRUCTURES	CU YD	958.0		453.0	505.0		
50300255	CONCRETE SUPERSTRUCTURES	CU YD	1,418.0		619.0	799.0		
50300260	BRIDGE DECK GROOVING	SQ YD	2,170			2,170		
50300280	CONCRETE ENCASEMENT	CU YD	29			29		
50300300	PROTECTIVE COAT	SQ YD	4,108		1,478	2,630		
50401005	FURNISHING AND ERECTING PRECAST PRESTRESSED CONCRETE I-BEAMS, 48 IN.	FOOT	3,127			3,127		
50800105	REINFORCEMENT BARS	POUND	57,920		57,920			
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	396,510		197,270	199,240		
50800515	BAR SPLICERS	EACH	180			180		
51201600	FURNISHING STEEL PILES HP12X53	FOOT	929			929		
51201900	FURNISHING STEEL PILES HP14X89	FOOT	1,495			1,495		
51202305	DRIVING PILES	FOOT	2,424			2,424		
51203600	TEST PILE STEEL HP12X53	EACH	3			3		
51203900	TEST PILE STEEL HP14X89	EACH	2			2		

* Specialty Items

SUMMARY OF QUANTITIES

F&P ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1A1B,42-1A1B	MADISON AND JERSEY	481	8
STA		TO STA		
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE				
					JERSEY COUNTY				MADISON COUNTY
					80% FEDERAL 20% STATE ROADWAY	80% FEDERAL 20% STATE BRIDGE (ARCH)	80% FEDERAL 20% STATE BRIDGE (BEAM)	80% FEDERAL 20% STATE RETAINING WALL	80% FEDERAL 20% STATE ROADWAY
CODE NUMBER	ITEM	UNITS	TOTAL	I000-2A	X024-2A	X081-2A	Y007	I000-2A	
51204650	PILE SHOES	EACH	62			62			
51500100	NAME PLATES	EACH	3		1	2			
54001001	BOX CULVERT END SECTION, CULVERT NO. 1	EACH	2	2					
54001002	BOX CULVERT END SECTION, CULVERT NO. 2	EACH	2	2					
54001003	BOX CULVERT END SECTION, CULVERT NO. 3	EACH	2	2					
54010302	PRECAST CONCRETE BOX CULVERT 3' X 2'	FOOT	172	172					
54003000	CONCRETE BOX CULVERTS	CU YD	225.5	225.5					
54213450	END SECTIONS 15"	EACH	2	2					
54213453	END SECTIONS 18"	EACH	2	2					
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTION 12"	EACH	1					1	
54213669	PRECAST REINFORCED CONCRETE FLARED END SECTION 24"	EACH	20	20					
54213693	PRECAST REINFORCED CONCRETE FLARED END SECTION 48"	EACH	1	1					
54215424	CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 24"	EACH	6	6					
54215472	CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 72"	EACH	2	2					
54215559	METAL END SECTIONS 24"	EACH	2	2					
54217680	REINFORCED CONCRETE PIPE TEE, 24" PIPE WITH 24" RISER	EACH	1	1					

SUMMARY OF QUANTITIES

FWP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
310	60-16-181B,42-181B	MADISON AND JERSEY	481	9
STA		TO STA		
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
				JERSEY COUNTY				MADISON COUNTY
				80% FEDERAL 20% STATE ROADWAY	80% FEDERAL 20% STATE BRIDGE (ARCH)	80% FEDERAL 20% STATE BRIDGE (BEAM)	80% FEDERAL 20% STATE RETAINING WALL	80% FEDERAL 20% STATE ROADWAY
CODE NUMBER	ITEM	UNITS	TOTAL	I000-2A	X024-2A	X081-2A	Y007	I000-2A
5421D012	PIPE CULVERTS, CLASS D, TYPE 1 12" (TEMPORARY)	FOOT	478	478				
5421A024	PIPE CULVERTS, CLASS A, TYPE 1 24" (TEMPORARY)	FOOT	140					140
54244805	INLET BOX, STANDARD 542501	EACH	3	2				1
54245005	INLET BOX, STANDARD 542521	EACH	1	1				
54245205	INLET BOX, STANDARD 542511	EACH	5	5				
54246205	INLET BOX TYPE 542526	EACH	2	2				
54248510	CONCRETE COLLAR	CU YD	24.8	24.8				
542A0229	PIPE CULVERTS, CLASS A, TYPE 1 24"	FOOT	529	485				44
542A1069	PIPE CULVERTS, CLASS A, TYPE 2 24"	FOOT	1,085	1,085				
542A1093	PIPE CULVERTS, CLASS A, TYPE 2 48"	FOOT	78	78				
542A1909	PIPE CULVERTS, CLASS A, TYPE 3 24"	FOOT	235	194				41
542A2797	PIPE CULVERTS, CLASS A, TYPE 4 72"	FOOT	207	207				
542A3379	PIPE CULVERTS, CLASS A, TYPE 5 24"	FOOT	75	75				
542A4009	PIPE CULVERTS, CLASS A, TYPE 6 24"	FOOT	305	259				46
542D0220	PIPE CULVERTS, CLASS D, TYPE 1 15"	FOOT	432	432				
542D0223	PIPE CULVERTS, CLASS D, TYPE 1 18"	FOOT	164	164				

SUMMARY OF QUANTITIES

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	10
STA		TO STA		
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
				JERSEY COUNTY				MADISON COUNTY
				80% FEDERAL 20% STATE ROADWAY	80% FEDERAL 20% STATE BRIDGE (ARCH)	80% FEDERAL 20% STATE BRIDGE (BEAM)	80% FEDERAL 20% STATE RETAINING WALL	80% FEDERAL 20% STATE ROADWAY
CODE NUMBER	ITEM	UNITS	TOTAL	I000-2A	X024-2A	X081-2A	Y007	I000-2A
542D0229	PIPE CULVERTS, CLASS D, TYPE 1 24"	FOOT	275	275				
542D1060	PIPE CULVERTS, CLASS D, TYPE 2 15"	FOOT	117	117				
542D1063	PIPE CULVERTS, CLASS D, TYPE 2 18"	FOOT	62	62				
542D1069	PIPE CULVERTS, CLASS D, TYPE 2 24"	FOOT	46	46				
542D1117	PIPE CULVERTS, CLASS D, TYPE 2 72"	FOOT	108	108				
542JA024	PIPE CULVERTS, CLASS A, 24" (JACKED)	FOOT	246	246				
542JA072	PIPE CULVERTS, CLASS A, 72" (JACKED)	FOOT	144	144				
550A0040	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	19	19				
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	251					251
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	192			192		
60100060	CONCRETE HEADWALL FOR PIPE DRAINS	EACH	92	89				3
60100985	PIPE DRAINS 24"	FOOT	151	151				
60107600	PIPE UNDERDRAIN 4"	FOOT	41,836	40,394				1,442
60108100	PIPE UNDERDRAIN 4" (SPECIAL)	FOOT	1,680	1,632				48
60220005	MANHOLES, TYPE A, 4'-DIAMETER, WITH MEDIAN INLET (604101)	EACH	2					2
60221000	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	1				

SUMMARY OF QUANTITIES

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	11
STA		TO STA		
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
				JERSEY COUNTY				MADISON COUNTY
				80% FEDERAL 20% STATE ROADWAY	80% FEDERAL 20% STATE BRIDGE (ARCH)	80% FEDERAL 20% STATE BRIDGE (BEAM)	80% FEDERAL 20% STATE RETAINING WALL	80% FEDERAL 20% STATE ROADWAY
CODE NUMBER	ITEM	UNITS	TOTAL	I000-2A	X024-2A	X081-2A	Y007	I000-2A
60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	6	6				
60222705	MANHOLES, TYPE A, 5'-DIAMETER, WITH MEDIAN INLET (604101)	EACH	4	4				
60224066	MANHOLES, TYPE A, 6'-DIAMETER, WITH MEDIAN INLET (604101)	EACH	1	1				
60236400	INLETS, TYPE A, TYPE 8 GRATE (24" DEPTH)	EACH	1	1				
60246605	MEDIAN INLET (604101)	EACH	2	2				
60247132	DRAINAGE STRUCTURES, TYPE 1A WITH ONE TYPE 20 FRAME AND GRATE	EACH	1					1
60500060	REMOVING INLETS	EACH	6	3				3
60600095	CLASS SI CONCRETE (OUTLET)	CU YD	8.1	8.1				
60602800	CONCRETE GUTTER, TYPE B	FOOT	174	174				
60900515	CONCRETE THRUST BLOCKS	EACH	2	2				
* 63000001	STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS	FOOT	2,300	1,963				337
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	4	4				
* 63100070	TRAFFIC BARRIER TERMINAL, TYPE 5	EACH	1	1				
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	6	5				1
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	9	8				1
63200310	GUARDRAIL REMOVAL	FOOT	8,085	7,087				998

*Specialty Items

SUMMARY OF QUANTITIES

MAP SHEET NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1A1B,42-1A1B	MADISON AND JERSEY	481	12
STA		TO STA		
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE				
					JERSEY COUNTY				MADISON COUNTY
					80% FEDERAL 20% STATE ROADWAY	80% FEDERAL 20% STATE BRIDGE (ARCH)	80% FEDERAL 20% STATE BRIDGE (BEAM)	80% FEDERAL 20% STATE RETAINING WALL	80% FEDERAL 20% STATE ROADWAY
CODE NUMBER	ITEM	UNITS	TOTAL	I000-2A	X024-2A	X081-2A	Y007	I000-2A	
63500105	DELINEATORS	EACH	85	85					
64200105	SHOULDER RUMBLE STRIP	FOOT	39,624	38,036				1,588	
66600105	FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS	EACH	158	157				1	
66700205	PERMANENT SURVEY MARKERS, TYPE I	EACH	3	3					
66700305	PERMANENT SURVEY MARKERS, TYPE II	EACH	19	19					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	24	24					
67000600	ENGINEER'S FIELD LABORATORY	CAL MO	24	24					
67100100	MOBILIZATION	L SUM	1	1					
67201000	SEALING ABANDONED WATER WELLS	EACH	2	2					
70101800	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1	1					
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	500	500					
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	1,214	745				469	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	4,234	3,934				300	
* 72000100	SIGN PANEL - TYPE 1	SQ FT	431	423				8	
* 72000200	SIGN PANEL - TYPE 2	SQ FT	804	804					
* 72500100	OBJECT MARKER - TYPE 1	EACH	8	8					
* 73000100	WOOD SIGN SUPPORT	FOOT	1,879	1,850				29	

*Specialty Items

Rev.

SUMMARY OF QUANTITIES

F&P ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
310	60-16-1&18,42-1&18	MADISON AND JERSEY	481	13
STA		TO STA		
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE				
					JERSEY COUNTY				MADISON COUNTY
					80% FEDERAL 20% STATE ROADWAY	80% FEDERAL 20% STATE BRIDGE (ARCH)	80% FEDERAL 20% STATE BRIDGE (BEAM)	80% FEDERAL 20% STATE RETAINING WALL	80% FEDERAL 20% STATE ROADWAY
CODE NUMBER	ITEM	UNITS	TOTAL	1000-2A	X024-2A	X081-2A	Y007	1000-2A	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	406	406					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	63,041	60,291					2,750
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	292	292					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	191	191					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	205	205					
* 78008210	POLYUREA PAVEMENT MARKING - LINE 4" <i>TYPE I</i>	FOOT	1,619	1,619					
* 78008230	POLYUREA PAVEMENT MARKING - LINE 6" <i>TYPE I</i>	FOOT	16	16					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	434	434					
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	45	45					
* 78200530	BARRIER WALL MARKERS, TYPE C	EACH	30	30					
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	9	8					1
X0323080	DRAINAGE SCUPPERS, DS-12	EACH	12				12		
X0323586	PIPE DRAIN REMOVAL	FOOT	155	84					71
X0323988	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	1,893	1,893					
X0325445	<i>RIGHT-OF-WAY AND</i> PROPERTY CORNERS	EACH	19	18					1
X4020500	AGGREGATE SURFACE COURSE, TYPE B 6"	SQ YD	2,974	2,974					

*Specialty Items

SUMMARY OF QUANTITIES

F&P NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-181B,42-181B	MADISON AND JERSEY	481	14
STA	TO STA			
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

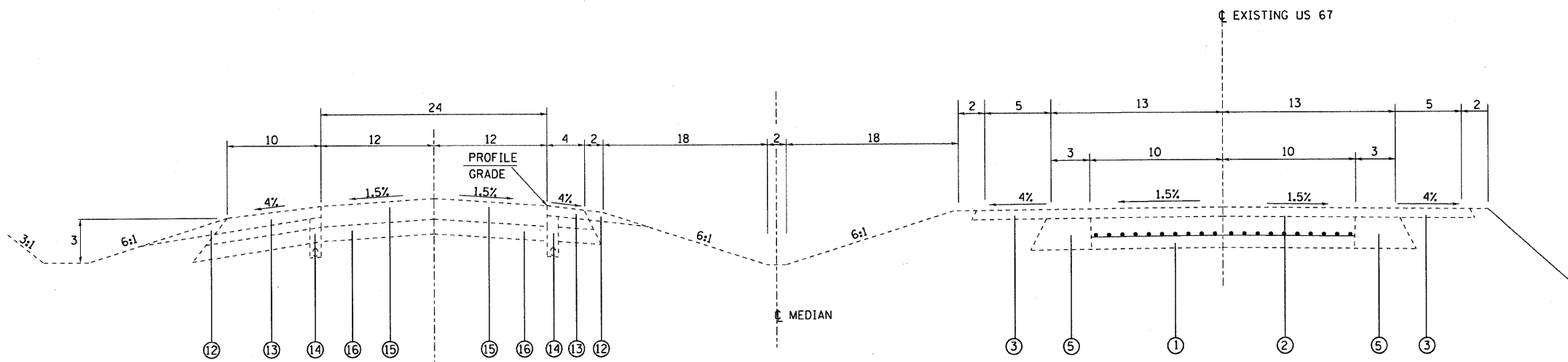
SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
				JERSEY COUNTY				MADISON COUNTY
				80% FEDERAL 20% STATE ROADWAY	80% FEDERAL 20% STATE BRIDGE (ARCH)	80% FEDERAL 20% STATE BRIDGE (BEAM)	80% FEDERAL 20% STATE RETAINING WALL	80% FEDERAL 20% STATE ROADWAY
CODE NUMBER	ITEM	UNITS	TOTAL	1000-2A	X024-2A	X081-2A	Y007	1000-2A
X5020501	UNDERWATER STRUCTURE EXCAVATION PROTECTION - LOCATION 1	EACH	2			2		
X5020502	UNDERWATER STRUCTURE EXCAVATION PROTECTION - LOCATION 2	EACH	2			2		
X5020503	UNDERWATER STRUCTURE EXCAVATION PROTECTION - LOCATION 3	EACH	1			1		
X5020504	UNDERWATER STRUCTURE EXCAVATION PROTECTION - LOCATION 4	EACH	1			1		
Z0007601	BUILDING REMOVAL NO. 1	L SUM	1	1				
Z0007602	BUILDING REMOVAL NO. 2	L SUM	1	1				
Z0034210	MECHANICALLY STABILIZED EARTH RETAINING WALL	SO FT	16,420				16,420	
Z0049801	REMOVAL AND DISPOSAL OF FRIABLE ASBESTOS, BUILDING NO. 1	L SUM	1	1				
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1				
Z0049802	REMOVAL AND DISPOSAL OF FRIABLE ASBESTOS, BUILDING NO. 2	L SUM	1	1				
Z0023500	FILLING EXISTING CULVERTS	CU YD	298.5	298.5				
Z0049901	REMOVAL AND DISPOSAL OF NON-FRIABLE ASBESTOS, BUILDING NO. 1	L SUM	1	1				
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1				
Z0049902	REMOVAL AND DISPOSAL OF NON-FRIABLE ASBESTOS, BUILDING NO. 2	L SUM	1	1				
Z0064505	SECTION CORNER MARKERS	EACH	3	3				
⊙ Z0076600	TRAINEES	HOUR	6,000	6,000				
⚠ Z00761921	HOT MIX ASPHALT PAVEMENT FULL DEPTH 1.12"	58 YD	5,415	5,415				
X0322461	DRUM AND CHEVRON SIGN	EACH	19	19				

⊙ Y080

⚠ Rev. 5-26-10

Rev.

DATE	
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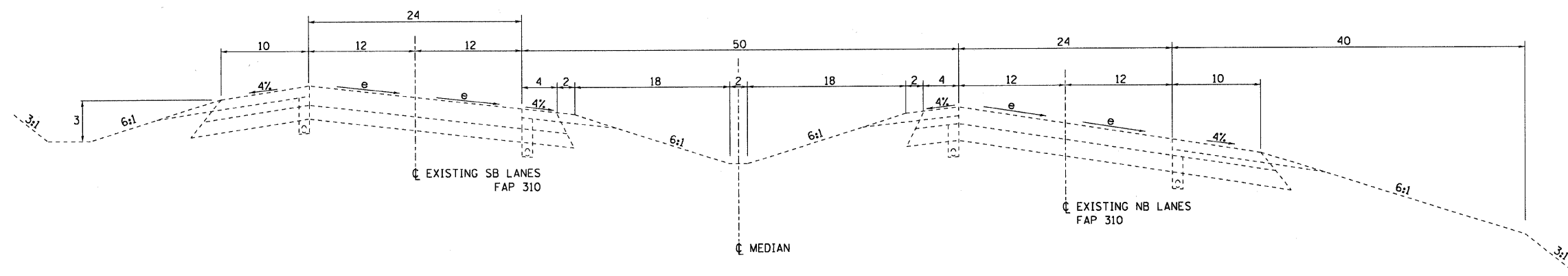
EXISTING SB LANES
FAP 310

EXISTING TYPICAL US 67

STA 225+00 TO STA 228+25

ALL UNITS ARE IN FEET
UNLESS OTHERWISE SHOWN

- LEGEND**
- ① EXISTING PCC PAVEMENT 10" WITH REINFORCEMENT
 - ② EXISTING HMA RESURFACING (THICKNESS VARIES)
 - ③ EXISTING HMA SHOULDER 6"
 - ④ EXISTING AGGREGATE SHOULDER 6"
 - ⑤ EXISTING PCC WIDENING 9" (POSSIBLE TIE BARS)
 - ⑥ EXISTING OIL AND CHIP BUILD-UP ON AGGREGATE BASE
 - ⑦ EXISTING EARTH SHOULDER (TYP)
 - ⑧ EXISTING HMA CURB 6"
 - ⑨ EXISTING GUARDRAIL, TYPE A
 - ⑩ EXISTING CONCRETE GUTTER, TYPE A
 - ⑪ EXISTING PAVEMENT MARKING
 - ⑫ EXISTING AGGREGATE SHOULDERS, TYPE B
 - ⑬ EXISTING HMA SHOULDERS
 - ⑭ EXISTING PIPE UNDERDRAIN 4"
 - ⑮ EXISTING HMA CONCRETE PAVEMENT (FULL DEPTH)
 - ⑯ EXISTING LIME MODIFIED SOIL 12"



EXISTING SB LANES
FAP 310

EXISTING TYPICAL US 67

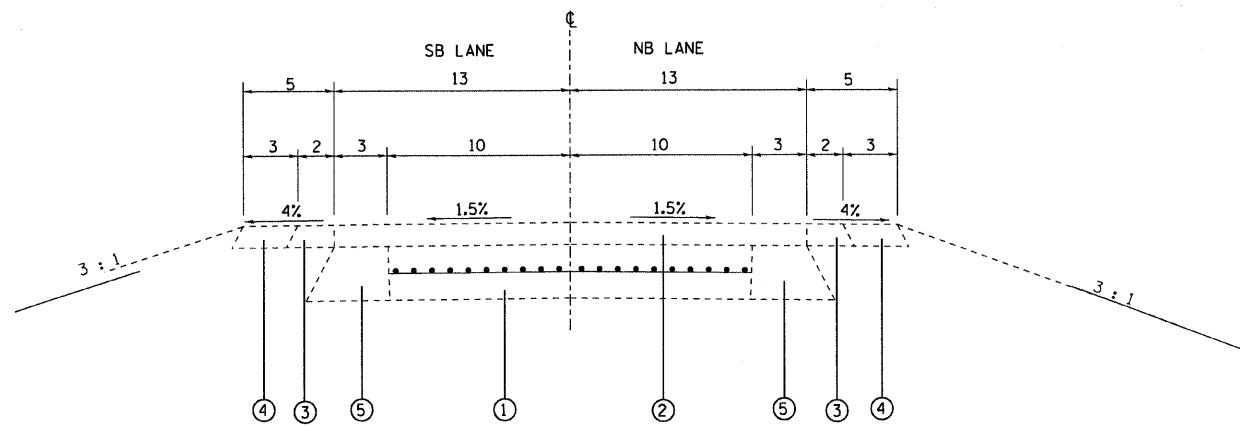
STA 220+00 TO STA 225+00

REVISIONS	
NAME	DATE

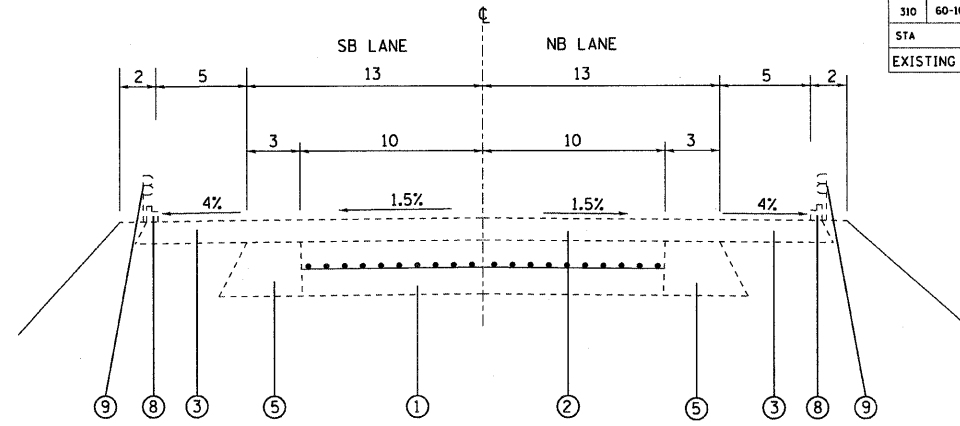
ILLINOIS DEPARTMENT OF TRANSPORTATION
EXISTING TYPICAL SECTIONS

FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON AND JERSEY COUNTIES

DRAWN BY: DLB CHECKED BY: GLF

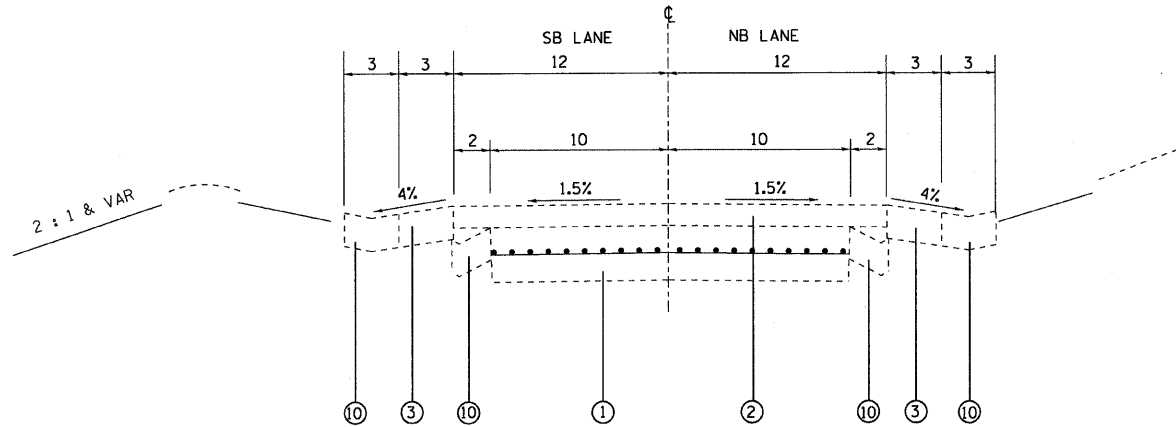


EXISTING TYPICAL SECTION US 67
 STA 233+03 TO STA 234+38 STA 242+33 TO STA 261+53
 STA 279+50 TO STA 290+78
 STA 299+03 TO STA 299+83
 STA 310+43 TO STA 322+47
 STA 326+06 TO STA 326+53



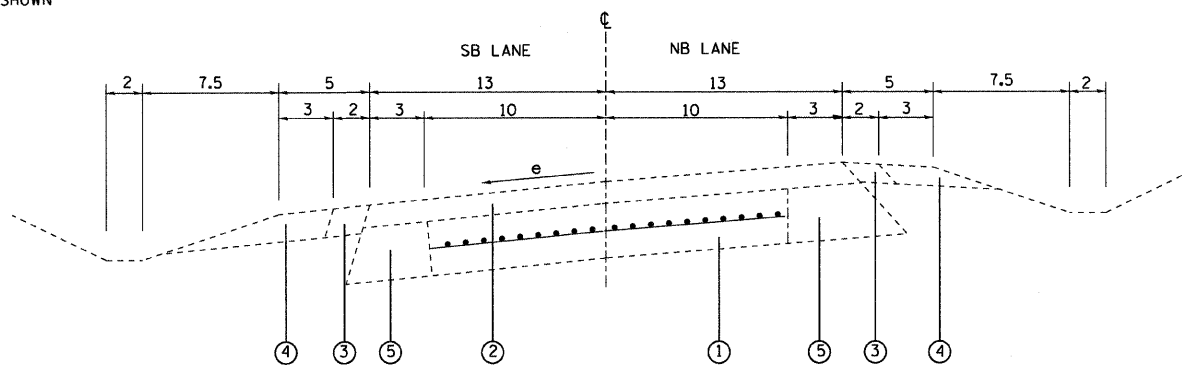
EXISTING TYPICAL SECTION US 67
 STA 228+25 TO STA 233+03 STA 261+53 TO STA 270+53
 STA 290+78 TO STA 299+03
 STA 326+53 TO STA 333+32.5

PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
	ALIGNED	
	CHECKED	
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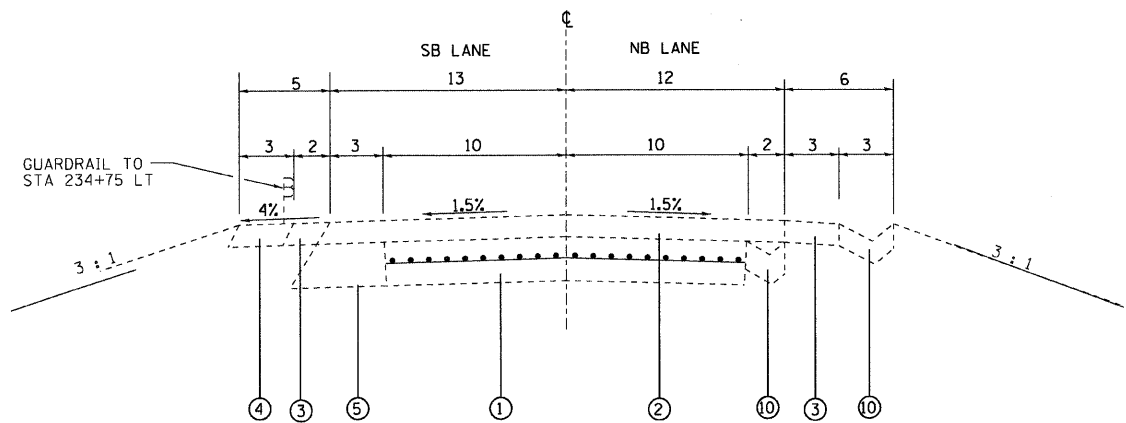


EXISTING TYPICAL SECTION US 67
 STA 235+07 TO STA 242+33
 STA 299+83 TO STA 309+38

ALL UNITS ARE IN FEET
 UNLESS OTHERWISE SHOWN



EXISTING TYPICAL SECTION US 67
 STA 270+53 TO STA 279+50



EXISTING TYPICAL SECTION US 67
 STA 234+38 TO STA 235+07 STA 309+38 TO STA 310+43
 STA 322+47 TO STA 326+06

LEGEND

- ① EXISTING PCC PAVEMENT 10" WITH REINFORCEMENT
- ② EXISTING HMA RESURFACING (THICKNESS VARIES)
- ③ EXISTING HMA SHOULDER 6"
- ④ EXISTING AGGREGATE SHOULDER 6"
- ⑤ EXISTING PCC WIDENING 9" (POSSIBLE TIE BARS)
- ⑥ EXISTING OIL AND CHIP BUILD-UP ON AGGREGATE BASE
- ⑦ EXISTING EARTH SHOULDER (TYP)
- ⑧ EXISTING HMA CURB 6"
- ⑨ EXISTING GUARDRAIL, TYPE A
- ⑩ EXISTING CONCRETE GUTTER, TYPE A
- ⑪ EXISTING PAVEMENT MARKING
- ⑫ EXISTING AGGREGATE SHOULDERS, TYPE B
- ⑬ EXISTING HMA SHOULDERS 14"
- ⑭ EXISTING PIPE UNDERDRAIN 4"
- ⑮ EXISTING HMA CONCRETE PAVEMENT (FULL DEPTH), TYPE 1 14"
- ⑯ EXISTING LIME MODIFIED SOIL 12"

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
EXISTING TYPICAL SECTIONS

FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTIES

DRAWN BY: DLB

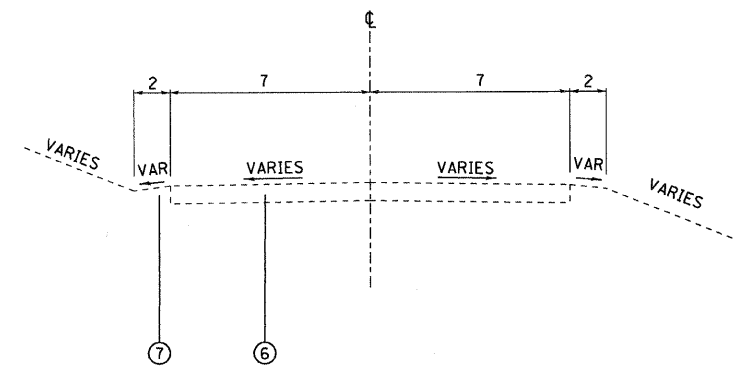
CHECKED BY: GLF

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	17
STA		TO STA		

EXISTING CONDITIONS:
CONTRACT NO. 76318

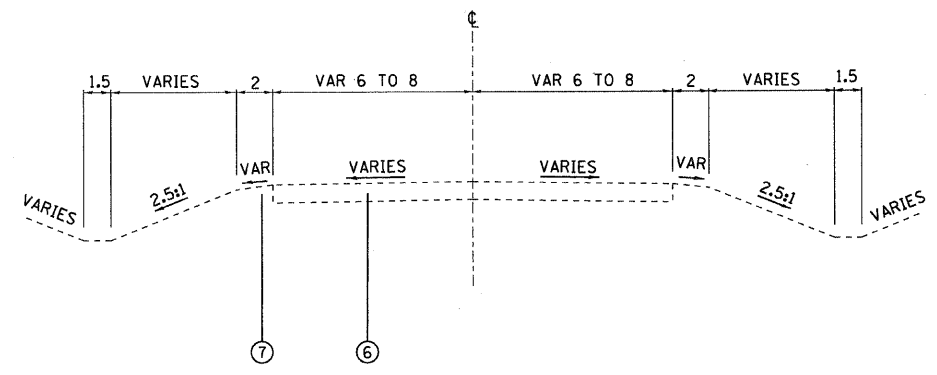
DATE	BY
PLAN	NO.
NO.	

SURVEYED _____
 ALIGNED _____
 CHECKED _____
 DRAWN _____
 DATE _____
 PLAN NO. _____
 REF. _____



STAGECOACH ROAD
 STA 194+25 TO STA 200+25.6
 STA 200+50 TO STA 205+80.43

ALL UNITS ARE IN FEET
UNLESS OTHERWISE SHOWN



NORTH INGHAM
 STA 145+70 TO STA 150+26.5
 STA 150+55.5 TO STA 157+25

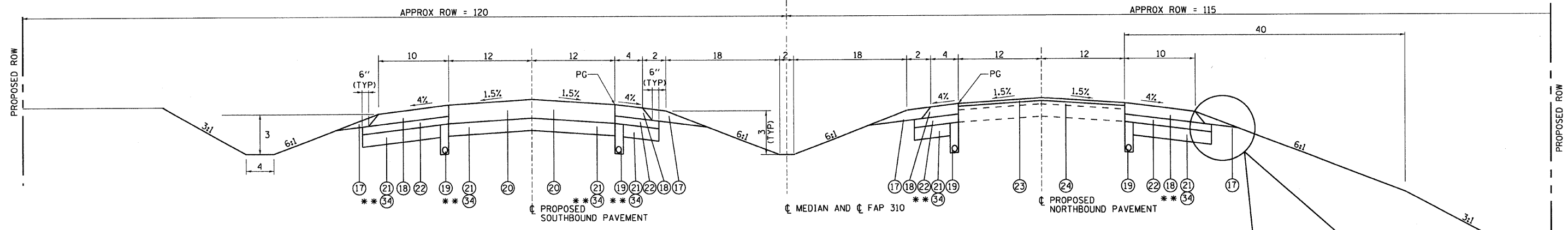
LEGEND

- ① EXISTING PCC PAVEMENT 10" WITH REINFORCEMENT
- ② EXISTING HMA RESURFACING (THICKNESS VARIES)
- ③ EXISTING HMA SHOULDER 6"
- ④ EXISTING AGGREGATE SHOULDER 6"
- ⑤ EXISTING PCC WIDENING 9" (POSSIBLE TIE BARS)
- ⑥ EXISTING OIL AND CHIP BUILD-UP ON AGGREGATE BASE
- ⑦ EXISTING EARTH SHOULDER (TYP)
- ⑧ EXISTING HMA CURB 6"
- ⑨ EXISTING GUARDRAIL, TYPE A
- ⑩ EXISTING CONCRETE GUTTER, TYPE A
- ⑪ EXISTING PAVEMENT MARKING
- ⑫ EXISTING AGGREGATE SHOULDERS, TYPE B
- ⑬ EXISTING HMA SHOULDERS 14"
- ⑭ EXISTING PIPE UNDERDRAIN 4"
- ⑮ EXISTING HMA CONCRETE PAVEMENT (FULL DEPTH), TYPE 1 14"
- ⑯ EXISTING LIME MODIFIED SOIL 12"

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
EXISTING TYPICAL SECTIONS
 FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTIES
 DRAWN BY: DLB CHECKED BY: GLF

DATE	
BY	
PLAN	
NO.	
NO. BOOK	
NO. SHEET	
NO. CHECKED	
NO. APPROVED	
NO. FILE NAME	



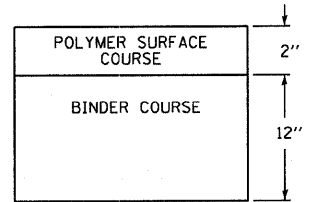
TYPICAL SECTION - FAP 310

STA 233+90 TO STA 243+00.49
 STA 253+60.49 TO STA 256+58.52
 ** STA 280+56.94 TO STA 282+42.53
 ** STA 293+57.47 TO STA 300+10

MATERIAL LEGEND:

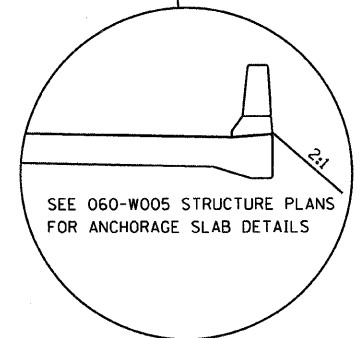
- | | |
|--|--|
| ⑰ AGGREGATE SHOULDERS, TYPE B | ⑳ AGGREGATE BASE COURSE, TYPE B, 8" |
| ⑱ HOT-MIX ASPHALT SHOULDERS, 8" | ㉑ BITUMINOUS SURFACE TREATMENT, CLASS A-2 |
| ㉒ PIPE UNDERDRAIN 4" | ㉒ AGGREGATE BASE COURSE, TYPE A, 10 1/2" |
| ㉓ HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 14" | ㉓ AGGREGATE BASE COURSE, TYPE A, 10 3/4" |
| ㉔ LIME MODIFIED SOILS 12" | ㉔ AGGREGATE BASE COURSE, TYPE A, 17" |
| ㉕ SUBBASE GRANULAR MATERIAL, TYPE C 6" | ㉕ AGGREGATE SURFACE COURSE, TYPE A, 11" |
| ㉖ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90, 1 1/2" | ㉖ AGGREGATE SURFACE COURSE, TYPE A, 11 1/2" |
| ㉗ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 3 1/2" AND VARIES IN SUPERELEVATED SECTIONS | ㉗ SUBBASE GRANULAR MATERIAL TYPE B 12" |
| ㉘ HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70, 1 1/2" | ㉘ STRIP REFLECTIVE CRACK CONTROL TREATMENT |
| ㉙ LEVELING BINDER (MACHINE METHOD), N70, 1 1/2" | ㉙ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 12" |
| | ㉚ AGGREGATE SURFACE COURSE, TYPE B, 6" |
| | ㉛ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12" |

ALL UNITS ARE IN FEET
UNLESS OTHERWISE SHOWN



FULL DEPTH PAVEMENT DETAIL

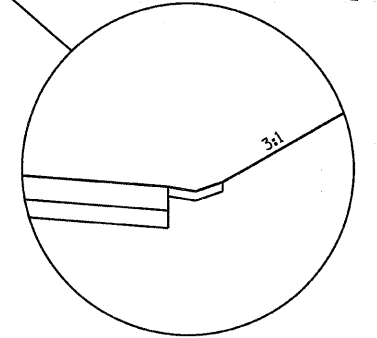
NOTE:
SAW CUT AND REMOVE HMA WEDGE PRIOR TO INSTALLATION OF PIPE UNDERDRAINS, 4"



DETAIL OF ANCHORAGE SLAB

(NOT TO SCALE)

STA 228+68.75 TO STA 231+60 RT



DETAIL OF TYPE B GUTTER

(NOT TO SCALE)

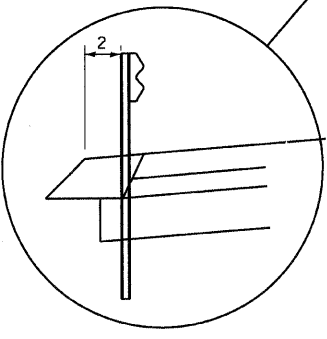
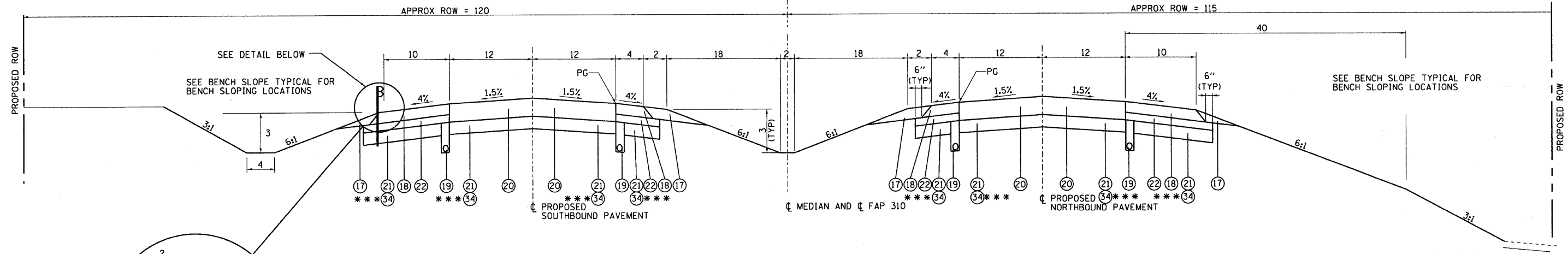
STA 271+44 TO STA 273+18 RT

STRUCTURAL DESIGN TRAFFIC:	Year <u>2014</u>
Pv= <u>14,811</u>	SU= <u>1.022</u> MU= <u>1.192</u>
ROAD/STREET CLASSIFICATION:	Class: <u>I</u>
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:	P= <u>32%</u> S= <u>45%</u> M= <u>45%</u>
TRAFFIC FACTOR:	Actual TF= <u>6.41</u> AC Type= <u>20</u>
	Minimum TF= <u>4.27</u>
PG GRADE: Binder= _____	Surface= _____
SUBGRADE SUPPORT RATING:	
SSR= <u>POOR</u> (Sta. _____ to _____)	
SSR= _____ (Sta. _____ to _____)	

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
PROPOSED TYPICAL SECTIONS
 FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTIES
 DRAWN BY: DLB
 CHECKED BY: GLF

PLAN	DATE
SURVEYED	BY
PLOTTED	
CHECKED	
NO. _____	
NO. _____	
NO. _____	
NO. _____	



DETAIL OF GUARDRAIL

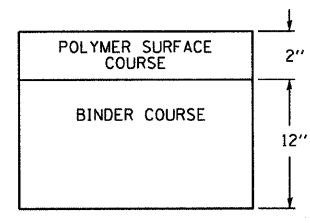
SEE GUARDRAIL SCHEDULE FOR LOCATIONS

TYPICAL SECTION - FAP 310

- STA 225+00 TO STA 228+25 RT SIDE ONLY
- STA 228+25.00 TO STA 233+90
- ** STA 300+10 TO STA 331+32.50 (NB)
- ** STA 300+10 TO STA 337+00 (SB)
- *** STA 300+10 TO STA 310+69
- ** PAVEMENT OMISSION
- STA 317+05.74 TO STA 319+91.58

MATERIAL LEGEND:

- | | |
|---|--|
| <ul style="list-style-type: none"> 17 AGGREGATE SHOULDERS, TYPE B 18 HOT-MIX ASPHALT SHOULDERS, 8" 19 PIPE UNDERDRAIN 4" 20 HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 14" 21 LIME MODIFIED SOILS 12" 22 SUBBASE GRANULAR MATERIAL, TYPE C 6" 23 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90, 1 1/2" 24 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 3 1/2" AND VARIES IN SUPERELEVATED SECTIONS 25 HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70, 1 1/2" 26 LEVELING BINDER (MACHINE METHOD), N70, 1 1/2" | <ul style="list-style-type: none"> 27 AGGREGATE BASE COURSE, TYPE B, 8" 28 BITUMINOUS SURFACE TREATMENT, CLASS A-2 29 AGGREGATE BASE COURSE, TYPE A, 10 1/2" 30 AGGREGATE BASE COURSE, TYPE A, 10 3/4" 31 AGGREGATE BASE COURSE, TYPE A, 17" 32 AGGREGATE SURFACE COURSE, TYPE A, 11" 33 AGGREGATE SURFACE COURSE, TYPE A, 11 1/2" 34 SUBBASE GRANULAR MATERIAL TYPE B 12" 35 STRIP REFLECTIVE CRACK CONTROL TREATMENT 36 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 12" 37 AGGREGATE SURFACE COURSE, TYPE B, 6" 38 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12" |
|---|--|



FULL DEPTH PAVEMENT DETAIL

ALL UNITS ARE IN FEET
UNLESS OTHERWISE SHOWN

NOTE:
SAW CUT AND REMOVE HMA WEDGE PRIOR
TO INSTALLATION OF PIPE UNDERDRAINS, 4"

*TO BE FILLED IN AFTER MIXTURE
REQUIREMENT CHART IS DETERMINED.

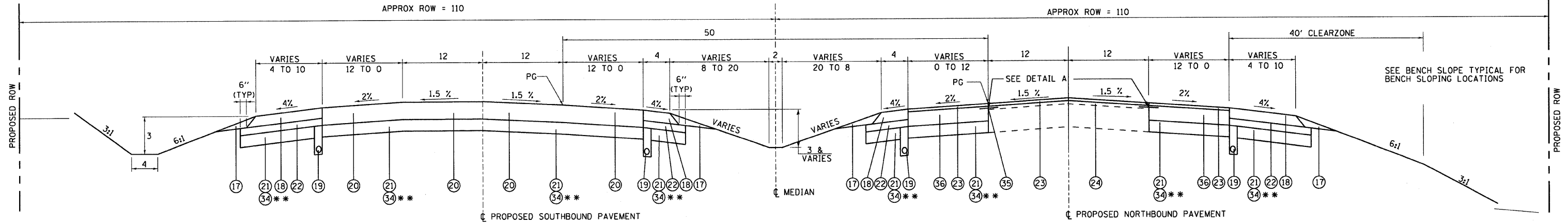
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
PROPOSED TYPICAL SECTIONS

FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON AND JERSEY COUNTIES

DRAWN BY: DLB
CHECKED BY: GLF

DATE: _____
 BY: _____
 SURVEYED: _____
 PLOTTED: _____
 CHECKED: _____
 NOTE BOOK NO. _____
 CADD FILE NAME _____

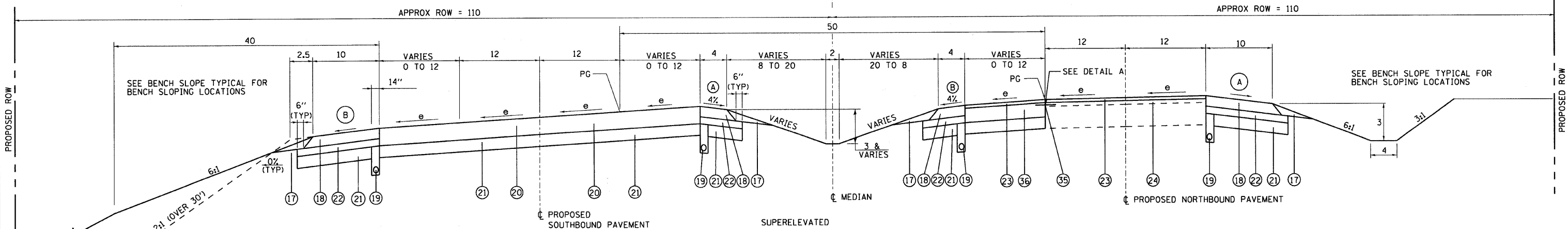
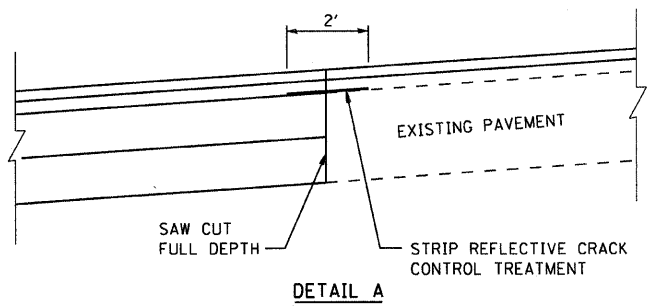


TYPICAL SECTION - FAP 310

WITH LEFT AND RIGHT TURN LANES
 STA 243+00.49 TO STA 253+60.49
 STA 256+58.52 TO STA 262+93.48
 STA 282+42.53 TO STA 293+57.47
 ** STA 282+42.53 TO STA 284+25.00 (NB LT)
 ** STA 283+04.90 TO STA 284+25.00 (NB RT)
 ** STA 293+50.00 TO STA 293+57.47 (SB RT)

MATERIAL LEGEND:

- 17 AGGREGATE SHOULDERS, TYPE B
- 18 HOT-MIX ASPHALT SHOULDERS, 8"
- 19 PIPE UNDERDRAIN 4"
- 20 HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 14"
- 21 LIME MODIFIED SOILS 12"
- 22 SUBBASE GRANULAR MATERIAL, TYPE C 6"
- 23 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90, 1 1/2"
- 24 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 3 1/2" AND VARIES IN SUPERELEVATED SECTIONS
- 25 HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70, 1 1/2"
- 26 LEVELING BINDER (MACHINE METHOD), N70, 1 1/2"
- 27 AGGREGATE BASE COURSE, TYPE B, 8"
- 28 BITUMINOUS SURFACE TREATMENT, CLASS A-2
- 29 AGGREGATE BASE COURSE, TYPE A, 10 1/2"
- 30 AGGREGATE BASE COURSE, TYPE A, 10 3/4"
- 31 AGGREGATE BASE COURSE, TYPE A, 17"
- 32 AGGREGATE SURFACE COURSE, TYPE A, 11"
- 33 AGGREGATE SURFACE COURSE, TYPE A, 11 1/2"
- 34 SUBBASE GRANULAR MATERIAL TYPE B 12"
- 35 STRIP REFLECTIVE CRACK CONTROL TREATMENT
- 36 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 12"
- 37 AGGREGATE SURFACE COURSE, TYPE B, 6"
- 38 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12"



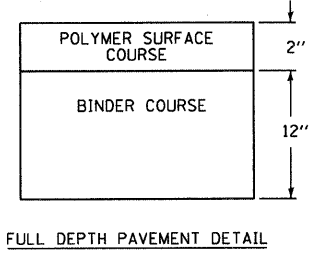
TYPICAL SECTION - FAP 310

WITH LEFT AND RIGHT TURN LANES

STA 262+93.48 TO STA 265+06.74 (LOOKING NORTH) e = 0.030 TRANSITION IN
 STA 265+06.74 TO STA 267+91.94 (LOOKING NORTH) e = 0.030

NOTE:
 SAW CUT AND REMOVE HMA WEDGE PRIOR
 TO INSTALLATION OF PIPE UNDERDRAINS, 4"

ALL UNITS ARE IN FEET
 UNLESS OTHERWISE SHOWN



- (A) SLOPE IS LESSER OF 4% OR 8% MINUS THE SE RATE
- (B) SLOPE IS THE GREATER OF 4% OR THE SE RATE

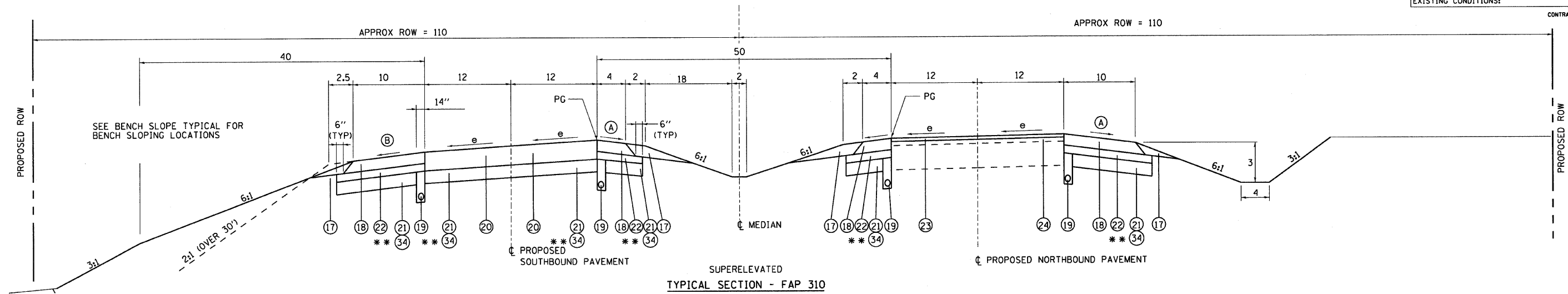
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
PROPOSED TYPICAL SECTIONS

FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTIES

DRAWN BY: DLB CHECKED BY: GLF

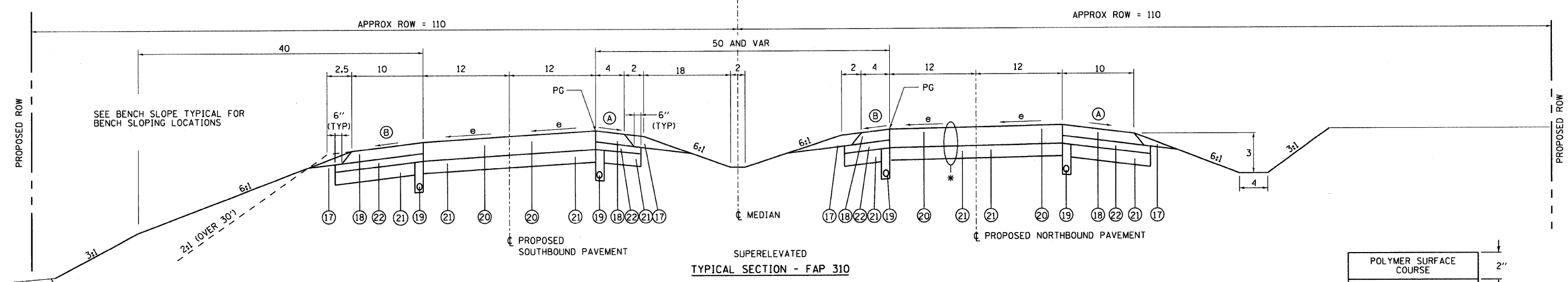
DATE _____ BY _____
 REVISIONS
 PLOTTED
 ALIGNED
 CHECKED
 DRAWN
 NO. _____



**SUPERELEVATED
TYPICAL SECTION - FAP 310**

STA 267+88.88 TO STA 278+43.76 (LOOKING NORTH) e= 0.030
 STA 278+43.76 TO STA 280+56.94 (LOOKING NORTH) e= 0.030 TRANSITION OUT
 ** STA 270+70.00 TO STA 280+56.94

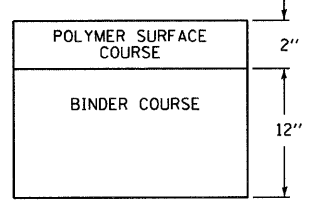
ALL UNITS ARE IN FEET
UNLESS OTHERWISE SHOWN



**SUPERELEVATED
TYPICAL SECTION - FAP 310**

STA 331+77.50 TO STA 333+90.75 (LOOKING SOUTH) e= 0.050 TRANSITION IN
 STA 333+90.75 TO STA 337+00 (LOOKING SOUTH) e= 0.050
 (END NB LANES STA 331+32.50)

NOTE:
SAW CUT AND REMOVE HMA WEDGE PRIOR
TO INSTALLATION OF PIPE UNDERDRAINS, 4"



FULL DEPTH PAVEMENT DETAIL

MATERIAL LEGEND:

- | | |
|---|---|
| 17 AGGREGATE SHOULDERS, TYPE B | 27 AGGREGATE BASE COURSE, TYPE B, 8" |
| 18 HOT-MIX ASPHALT SHOULDERS, 8" | 28 BITUMINOUS SURFACE TREATMENT, CLASS A-2 |
| 19 PIPE UNDERDRAIN 4" | 29 AGGREGATE BASE COURSE, TYPE A, 10 1/2" |
| 20 HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 14" | 30 AGGREGATE BASE COURSE, TYPE A, 10 3/4" |
| 21 LIME MODIFIED SOILS 12" | 31 AGGREGATE BASE COURSE, TYPE A, 17" |
| 22 SUBBASE GRANULAR MATERIAL, TYPE C 6" | 32 AGGREGATE SURFACE COURSE, TYPE A, 11" |
| 23 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90, 1 1/2" | 33 AGGREGATE SURFACE COURSE, TYPE A, 11 1/2" |
| 24 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 3 1/2" AND VARIES IN SUPERELEVATED SECTIONS | 34 SUBBASE GRANULAR MATERIAL TYPE B 12" |
| 25 HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70, 1 1/2" | 35 STRIP REFLECTIVE CRACK CONTROL TREATMENT |
| 26 LEVELING BINDER (MACHINE METHOD), N70, 1 1/2" | 36 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 12" |
| | 37 AGGREGATE SURFACE COURSE, TYPE B, 6" |
| | 38 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12" |

- (A) SLOPE IS LESSER OF 4% OR 8% MINUS THE SE RATE
 (B) SLOPE IS THE GREATER OF 4% OR THE SE RATE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

PROPOSED TYPICAL SECTIONS

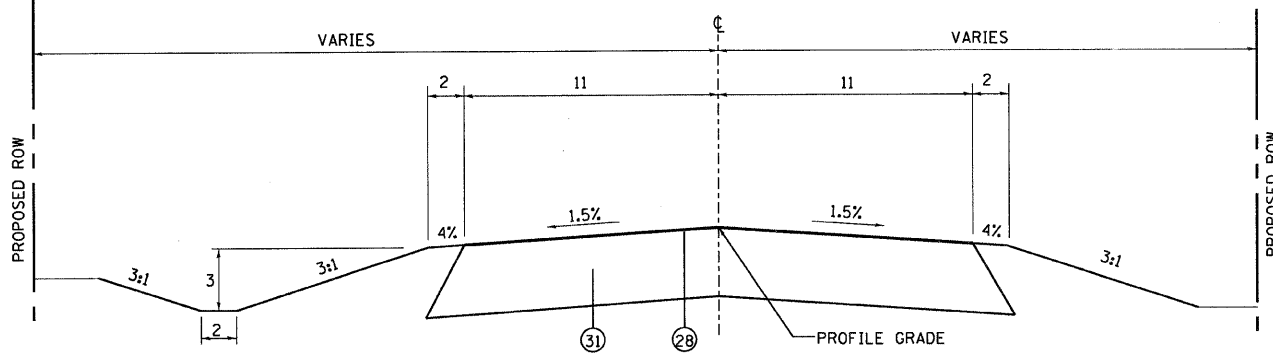
FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTIES

DRAWN BY: DLB CHECKED BY: GLF

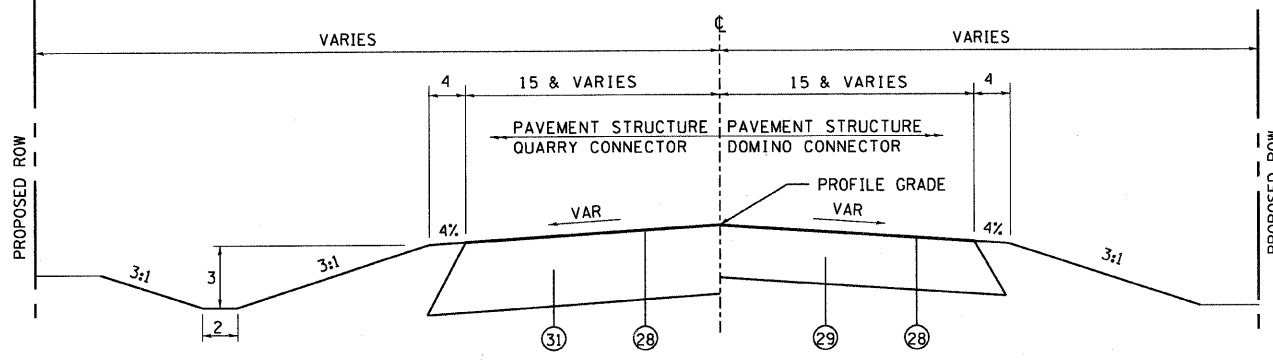
APP. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	23
STA TO STA		EXISTING CONDITIONS:		

CONTRACT NO. 76318

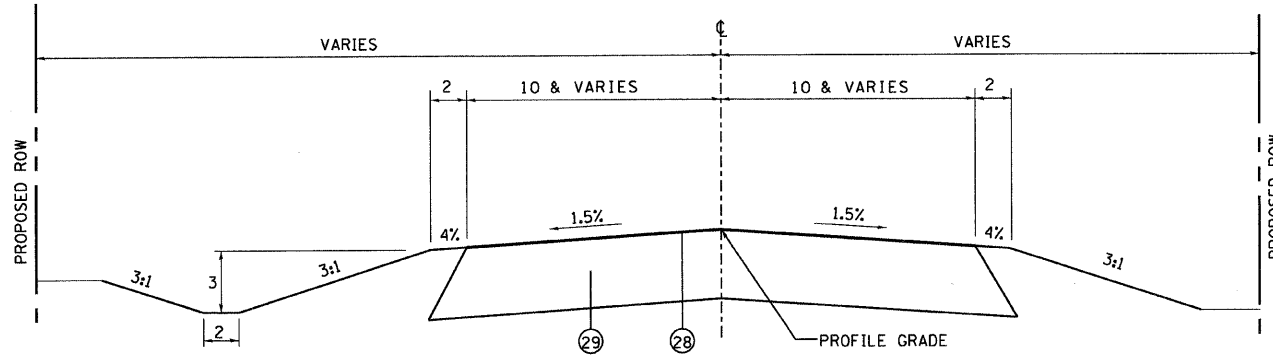
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NOTE BOOK	PLOTTED	
NO.	BY	
	DATE	
	NO. OF SHEETS	
	TOTAL SHEETS	
	FILE NAME	



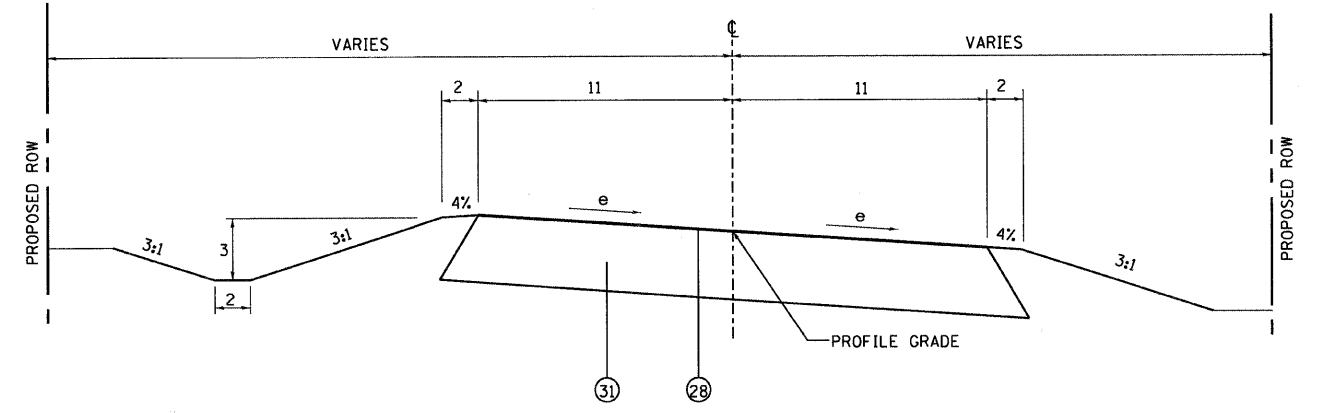
QUARRY ROAD
 STA 54+50.00 TO STA 55+23.18
 STA 62+97.87 TO STA 71+60.19



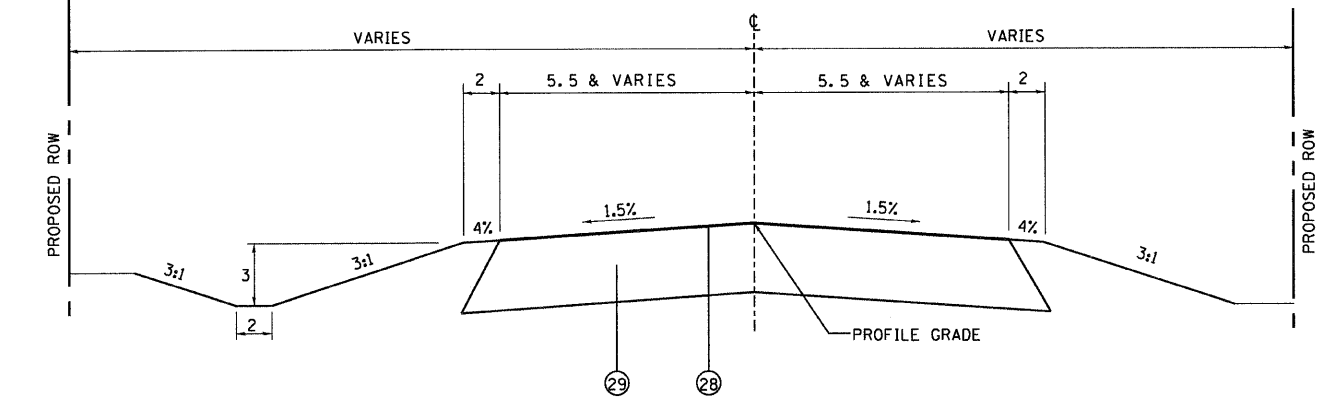
QUARRY & DOMINO CONNECTOR ROAD
 STA 50+11.00 TO STA 51+19.00 (QUARRY CONNECTOR)
 STA 52+41.00 TO STA 53+50.00 (DOMINO CONNECTOR)



DOMINO LANE SERVICE ROAD
 STA 99+00.00 TO STA 114+32.43



QUARRY ROAD
 STA 55+23.18 TO STA 55+84.18 (LOOKING NORTH) TRANSITION IN
 STA 55+84.18 TO STA 57+42.09 (LOOKING NORTH) $e=0.04\%$
 STA 57+42.09 TO STA 57+94.13 (LOOKING NORTH) TRANSITION OUT
 STA 57+94.13 TO STA 58+46.17 (LOOKING SOUTH) TRANSITION IN
 STA 58+46.17 TO STA 62+36.87 (LOOKING SOUTH) $e=0.04\%$
 STA 62+36.87 TO STA 62+97.87 (LOOKING SOUTH) TRANSITION OUT



DOMINO LANE
 STA 10+10.00 TO STA 10+75.00

QUARRY ROAD

STRUCTURAL DESIGN TRAFFIC:	Year 2014
PV= 10	SU= 70 MU= 20
ROAD/STREET CLASSIFICATION:	Class: IV
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:	P= 50% S= 50% M= 50%
TRAFFIC FACTOR:	Actual TF= 0.023 AC Type= 20 Minimum TF= DOES NOT APPLY
PG GRADE:	Binder= Surface=
SUBGRADE SUPPORT RATING:	IBR= 3.0
	SSR= (Sta. to)
	SSR= (Sta. to)

DOMINO LANE SERVICE ROAD

STRUCTURAL DESIGN TRAFFIC:	Year 2014
PV= 213	SU= 1 MU= 1
ROAD/STREET CLASSIFICATION:	Class: IV
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:	P= 50% S= 50% M= 50%
TRAFFIC FACTOR:	Actual TF= .0012 AC Type= 20 Minimum TF= DOES NOT APPLY
PG GRADE:	Binder= Surface=
SUBGRADE SUPPORT RATING:	IBR= 3.0
	SSR= (Sta. to)
	SSR= (Sta. to)

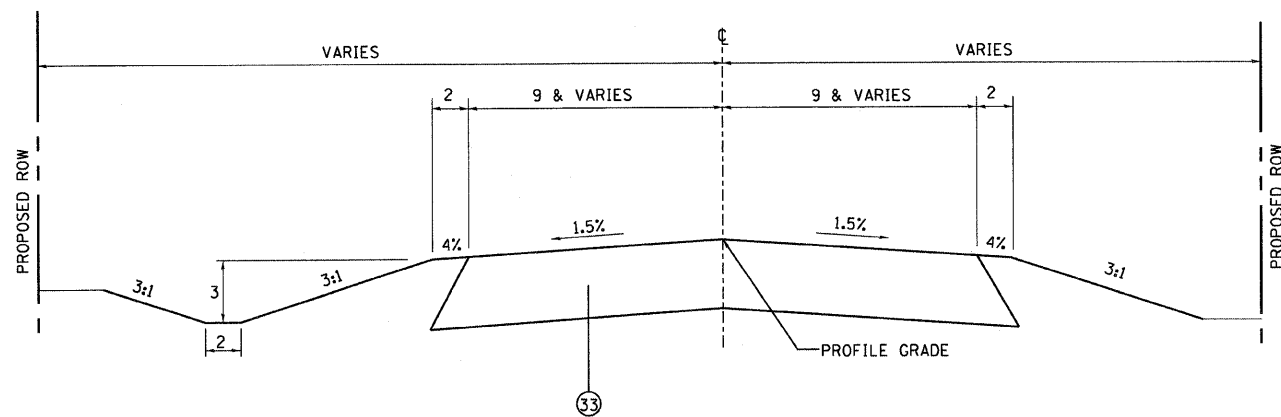
- MATERIAL LEGEND:**
- | | |
|---|---|
| ①7 AGGREGATE SHOULDERS, TYPE B | ②7 AGGREGATE BASE COURSE, TYPE B, 8" |
| ①8 HOT-MIX ASPHALT SHOULDERS, 8" | ②8 BITUMINOUS SURFACE TREATMENT, CLASS A-2 |
| ①9 PIPE UNDERDRAIN 4" | ②9 AGGREGATE BASE COURSE, TYPE A, 10 1/2" |
| ②0 HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 14" | ③0 AGGREGATE BASE COURSE, TYPE A, 10 3/4" |
| ②1 LIME MODIFIED SOILS 12" | ③1 AGGREGATE BASE COURSE, TYPE A, 17" |
| ②2 SUBBASE GRANULAR MATERIAL, TYPE C 6" | ③2 AGGREGATE SURFACE COURSE, TYPE A, 11" |
| ②3 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90, 1 1/2" | ③3 AGGREGATE SURFACE COURSE, TYPE A, 11 1/2" |
| ②4 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 3 1/2" AND VARIES IN SUPERELEVATED SECTIONS | ③4 SUBBASE GRANULAR MATERIAL TYPE B 12" |
| ②5 HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70, 1 1/2" | ③5 STRIP REFLECTIVE CRACK CONTROL TREATMENT |
| ②6 LEVELING BINDER (MACHINE METHOD), N70, 1 1/2" | ③6 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 12" |
| | ③7 AGGREGATE SURFACE COURSE, TYPE B, 6" |
| | ③8 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12" |

ALL UNITS ARE IN FEET
 UNLESS OTHERWISE SHOWN

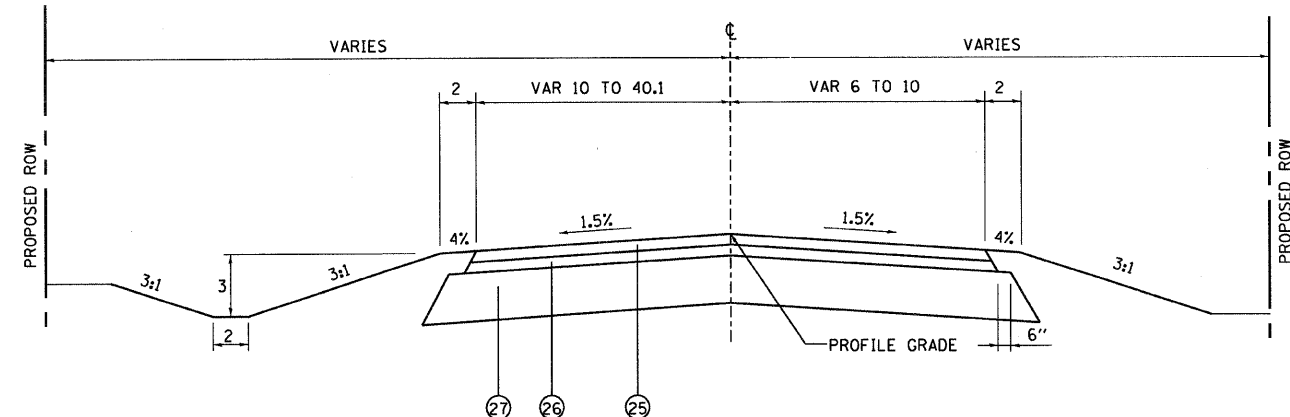
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
PROPOSED TYPICAL SECTIONS
SIDEROADS
 F&P ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTIES
 DRAWN BY: DLB CHECKED BY: GLF

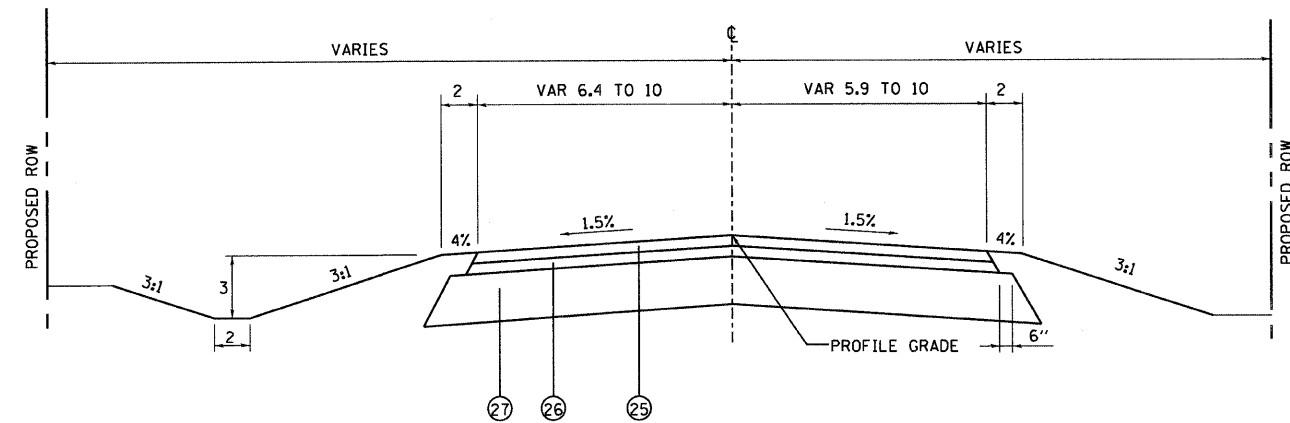
DATE	
BY	
DESIGNED	
CHECKED	
PLANNED	
NOTED	
NO.	



INGHAM ACCESS DRIVE
STA 170+10.00 TO STA 175+40.00



JUN ACCESS ROAD
STA 20+11.00 TO STA 25+88.0



NORTH INGHAM
STA 145+70.00 TO STA 149+32.69
STA 150+66.62 TO STA 157+25.00

MATERIAL LEGEND:

- | | |
|---|---|
| ①7 AGGREGATE SHOULDERS, TYPE B | ②7 AGGREGATE BASE COURSE, TYPE B, 8" |
| ①8 HOT-MIX ASPHALT SHOULDERS, 8" | ②8 BITUMINOUS SURFACE TREATMENT, CLASS A-2 |
| ①9 PIPE UNDERDRAIN 4" | ②9 AGGREGATE BASE COURSE, TYPE A, 10 1/2" |
| ②0 HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 14" | ③0 AGGREGATE BASE COURSE, TYPE A, 10 3/4" |
| ②1 LIME MODIFIED SOILS 12" | ③1 AGGREGATE BASE COURSE, TYPE A, 17" |
| ②2 SUBBASE GRANULAR MATERIAL, TYPE C 6" | ③2 AGGREGATE SURFACE COURSE, TYPE A, 11" |
| ②3 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90, 1 1/2" | ③3 AGGREGATE SURFACE COURSE, TYPE A, 11 1/2" |
| ②4 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 3 1/2" AND VARIES IN SUPERELEVATED SECTIONS | ③4 SUBBASE GRANULAR MATERIAL TYPE B 12" |
| ②5 HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70, 1 1/2" | ③5 STRIP REFLECTIVE CRACK CONTROL TREATMENT |
| ②6 LEVELING BINDER (MACHINE METHOD), N70, 1 1/2" | ③6 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 12" |
| | ③7 AGGREGATE SURFACE COURSE, TYPE B, 6" |
| | ③8 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12" |

NORTH INGHAM ACCESS DRIVE

STRUCTURAL DESIGN TRAFFIC:	Year <u>2014</u>
PV= <u>44</u>	SU= <u>5</u> MU= <u>1</u>
ROAD/STREET CLASSIFICATION:	Class: <u>IV</u>
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:	
P= <u>50%</u>	S= <u>50%</u> M= <u>50%</u>
TRAFFIC FACTOR:	Actual TF= <u>0.0013</u> AC Type= <u>20</u>
	Minimum TF= <u>DOES NOT APPLY</u>
PG GRADE: Binder= _____	Surface= _____
SUBGRADE SUPPORT RATING:	IBR= <u>3.0</u>
SSR= _____ (Sta. _____ to _____)	
SSR= _____ (Sta. _____ to _____)	

NORTH INGHAM

STRUCTURAL DESIGN TRAFFIC:	Year <u>2014</u>
PV= <u>212</u>	SU= <u>2</u> MU= <u>1</u>
ROAD/STREET CLASSIFICATION:	Class: <u>IV</u>
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:	
P= <u>50%</u>	S= <u>50%</u> M= <u>50%</u>
TRAFFIC FACTOR:	Actual TF= <u>0.001</u> AC Type= <u>20</u>
	Minimum TF= <u>DOES NOT APPLY</u>
PG GRADE: Binder= _____	Surface= _____
SUBGRADE SUPPORT RATING:	IBR= <u>3.0</u>
SSR= _____ (Sta. _____ to _____)	
SSR= _____ (Sta. _____ to _____)	

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
PROPOSED TYPICAL SECTIONS

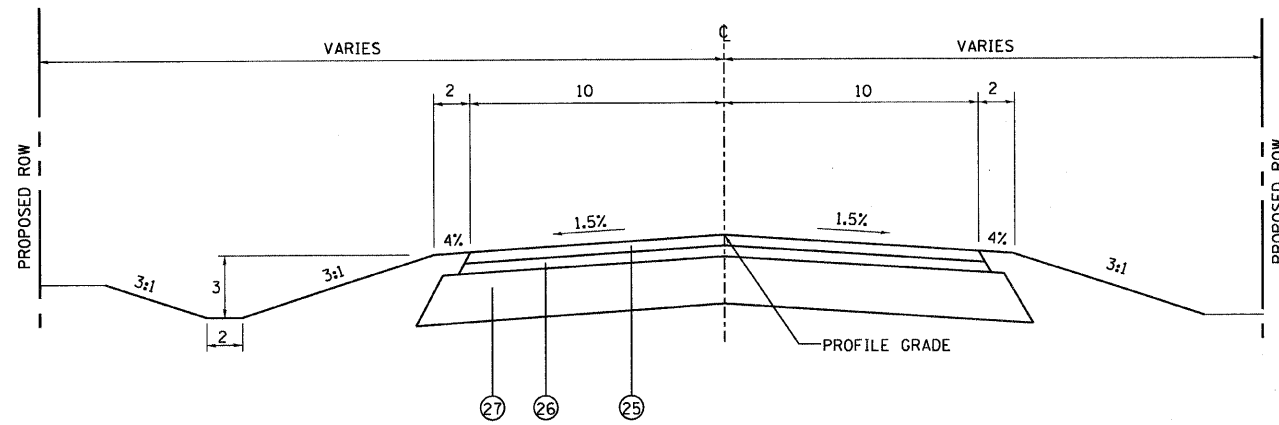
SIDERoadS
FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON AND JERSEY COUNTIES

DRAWN BY: DLB CHECKED BY: GLF

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	25
STA		TO STA		
EXISTING CONDITIONS:				

CONTRACT NO. 76318

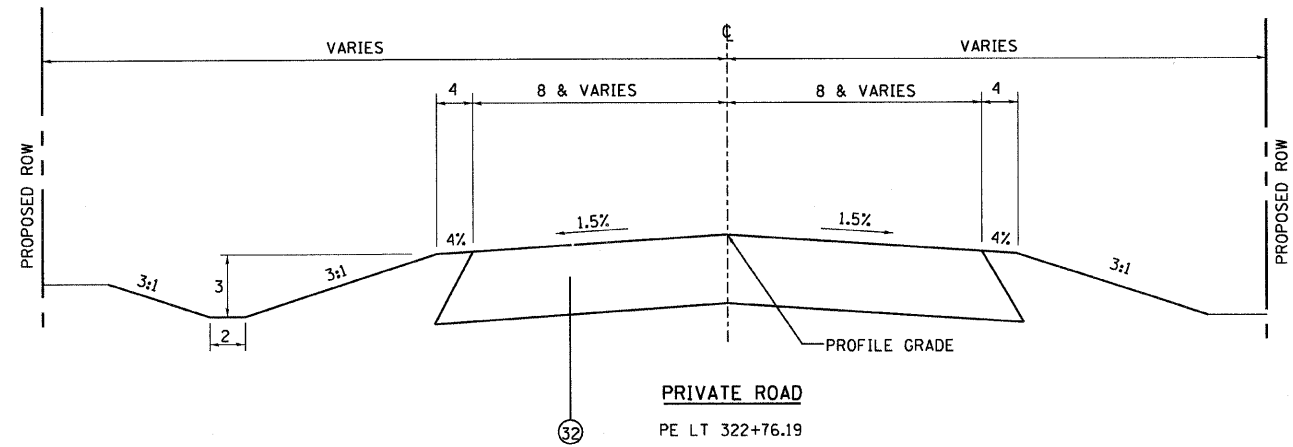
PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	REVISIONS	
	NOT CHECKED	
	ADDY FILE NAME	



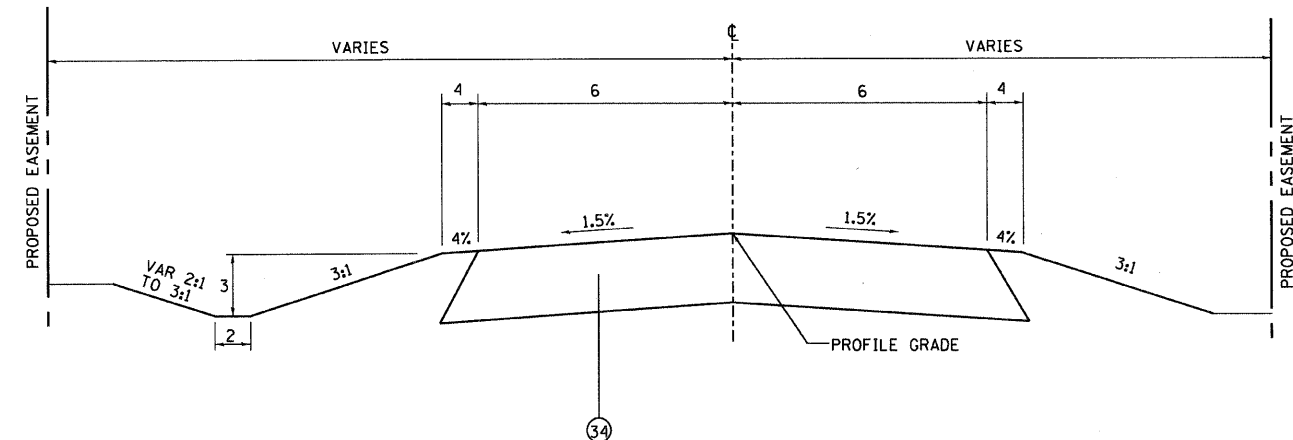
BUNSE WAY/STAGECOACH ROAD
 STA 194+65.71 TO STA 199+36.52
 STA 200+63.48 TO STA 205+80.43

STAGECOACH ROAD

STRUCTURAL DESIGN TRAFFIC:	Year 2014
PV= 212	SU= 2 MU= 1
ROAD/STREET CLASSIFICATION:	Class: IV
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:	
P= 50%	S= 50% M= 50%
TRAFFIC FACTOR:	Actual TF= .001 AC Type= 20
	Minimum TF= DOES NOT APPLY
PG GRADE: Binder=	Surface=
SUBGRADE SUPPORT RATING:	IBR= 3.0
SSR=	(Sta. to)
SSR=	(Sta. to)



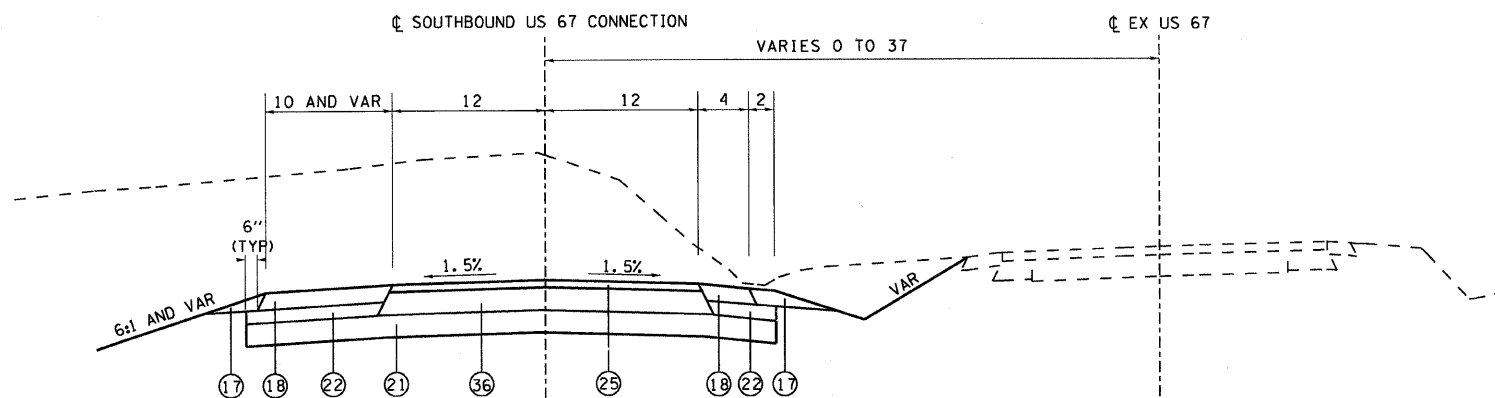
PRIVATE ROAD
 PE LT 322+76.19



PRIVATE ROAD
 PE LT 335+00.00

MATERIAL LEGEND:

- ⑰ AGGREGATE SHOULDERS, TYPE B
- ⑱ HOT-MIX ASPHALT SHOULDERS, 8"
- ⑲ PIPE UNDERDRAIN 4"
- ⑳ HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 14"
- ㉑ LIME MODIFIED SOILS 12"
- ㉒ SUBBASE GRANULAR MATERIAL, TYPE C 6"
- ㉓ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90, 1 1/2"
- ㉔ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 3/2" AND VARIES IN SUPERELEVATED SECTIONS
- ㉕ HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70, 1 1/2"
- ㉖ LEVELING BINDER (MACHINE METHOD), N70, 1 1/2"
- ㉗ AGGREGATE BASE COURSE, TYPE B, 8"
- ㉘ BITUMINOUS SURFACE TREATMENT, CLASS A-2
- ㉙ AGGREGATE BASE COURSE, TYPE A, 10 1/2"
- ㉚ AGGREGATE BASE COURSE, TYPE A, 10 3/4"
- ㉛ AGGREGATE BASE COURSE, TYPE A, 17"
- ㉜ AGGREGATE SURFACE COURSE, TYPE A, 11"
- ㉝ AGGREGATE SURFACE COURSE, TYPE A, 11 1/2"
- ㉞ SUBBASE GRANULAR MATERIAL TYPE B 12"
- ㉟ STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ㊱ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 12"
- ㊲ AGGREGATE SURFACE COURSE, TYPE B, 6"
- ㊳ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12"



TYPICAL SECTION - CROSSOVER C SOUTHBOUND US 67 CONNECTION
 NOT TO SCALE
 STA 337+00.00 TO STA 342+57.86

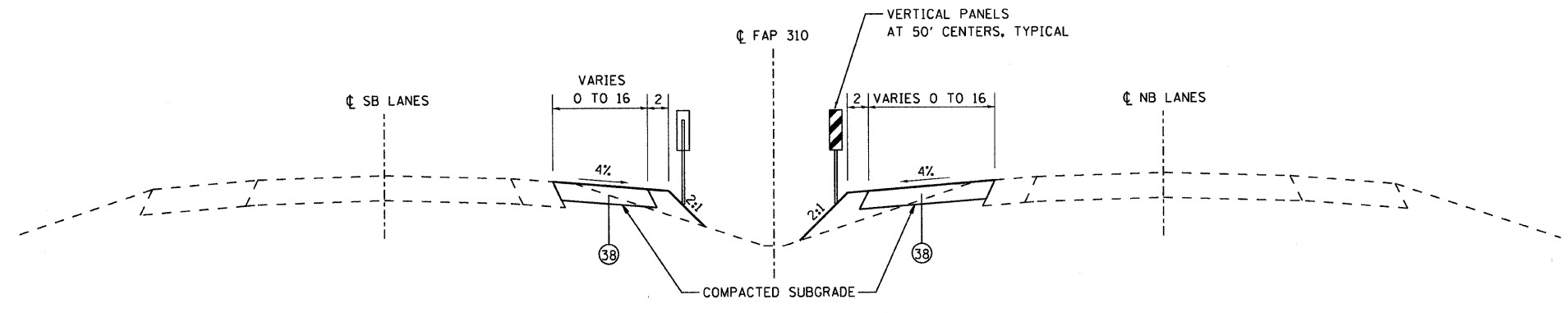
ALL UNITS ARE IN FEET
 UNLESS OTHERWISE SHOWN

REVISIONS	
NAME	DATE

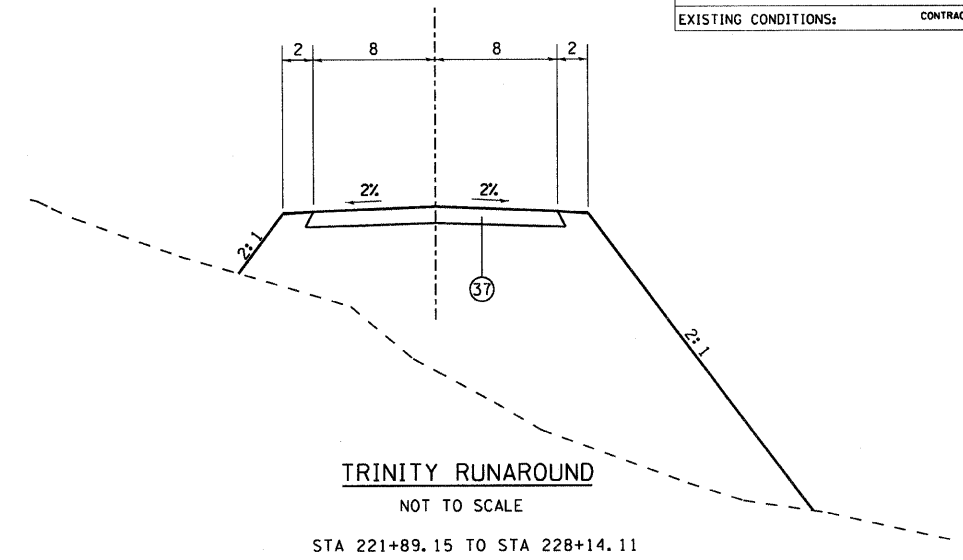
ILLINOIS DEPARTMENT OF TRANSPORTATION
PROPOSED TYPICAL SECTIONS
 SIDEROADS
 FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTIES

DRAWN BY: DLB CHECKED BY: GLF

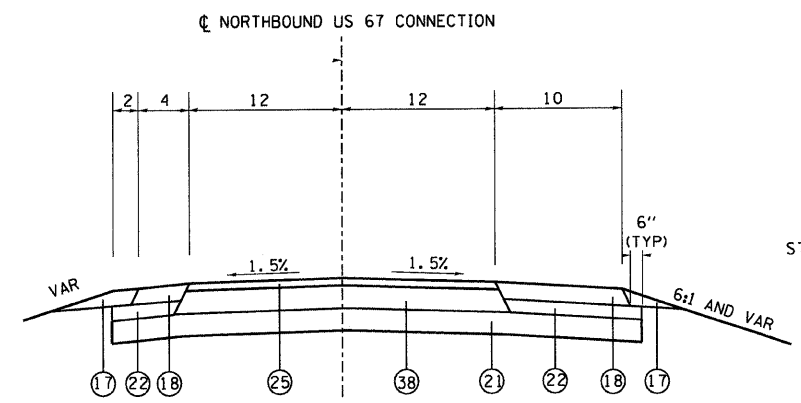
DATE _____
 BY _____
 SURVEYED _____
 PLOTTED _____
 PLAN CHECKED _____
 NOTE BOOK NO. _____
 CADD FILE NAME _____



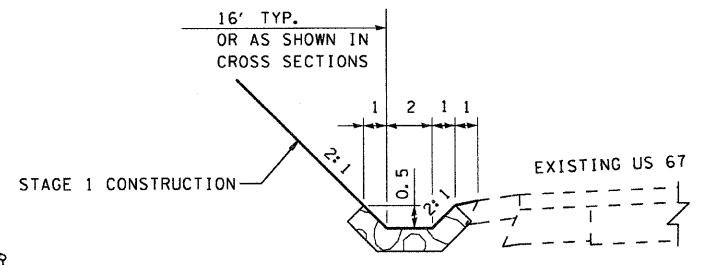
TYPICAL SECTION - CROSSOVER B
 NOT TO SCALE
 STA 221+89.15 TO STA 228+14.11



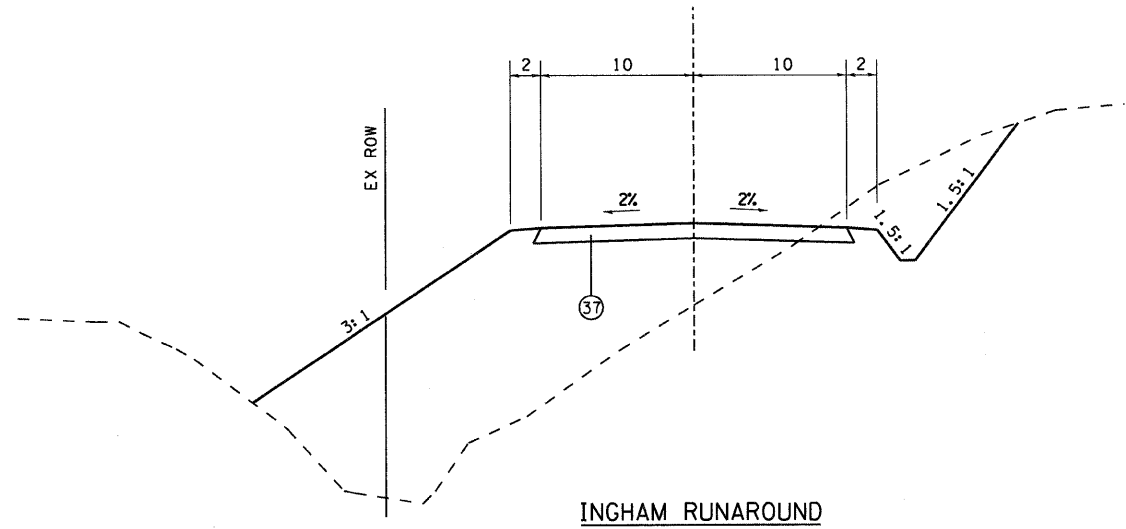
TRINITY RUNAROUND
 NOT TO SCALE
 STA 221+89.15 TO STA 228+14.11



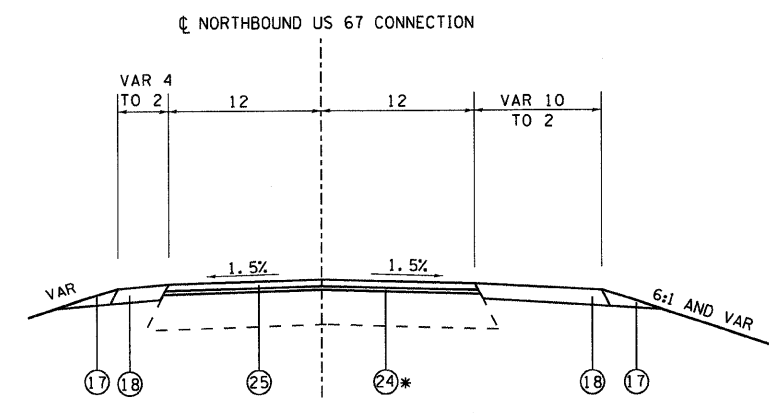
TYPICAL SECTION - TRANSITION TAPER
 NOT TO SCALE
 STA 331+32.50 TO STA 332+25



TEMPORARY RIPRAP DETAIL
 NOT TO SCALE
 FAP 310 STAGE 1 CONSTRUCTION
 STA 311+00.00 TO STA 331+32.5



INGHAM RUNAROUND
 NOT TO SCALE
 STA 337+00.00 TO STA 342+57.86



TYPICAL SECTION - TRANSITION TAPER
 NOT TO SCALE
 STA 332+25 TO STA 333+32.50
 *THICKNESS VARIES FROM 12" TO 0"
 USE LEVELING BINDER (MACHINE METHOD)
 WHEN THICKNESS IS 2.25" OR LESS

MATERIAL LEGEND:

- 17 AGGREGATE SHOULDERS, TYPE B
- 18 HOT-MIX ASPHALT SHOULDERS, 8"
- 19 PIPE UNDERDRAIN 4"
- 20 HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 14"
- 21 LIME MODIFIED SOILS 12"
- 22 SUBBASE GRANULAR MATERIAL, TYPE C 6"
- 23 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90, 1 1/2"
- 24 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 3 1/2" AND VARIES IN SUPERELEVATED SECTIONS
- 25 HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70, 1 1/2"
- 26 LEVELING BINDER (MACHINE METHOD), N70, 1 1/2"
- 27 AGGREGATE BASE COURSE, TYPE B, 8"
- 28 BITUMINOUS SURFACE TREATMENT, CLASS A-2
- 29 AGGREGATE BASE COURSE, TYPE A, 10 1/2"
- 30 AGGREGATE BASE COURSE, TYPE A, 10 3/4"
- 31 AGGREGATE BASE COURSE, TYPE A, 17"
- 32 AGGREGATE SURFACE COURSE, TYPE A, 11"
- 33 AGGREGATE SURFACE COURSE, TYPE A, 11 1/2"
- 34 SUBBASE GRANULAR MATERIAL TYPE B 12"
- 35 STRIP REFLECTIVE CRACK CONTROL TREATMENT
- 36 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 12"
- 37 AGGREGATE SURFACE COURSE, TYPE B, 6"
- 38 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12"

ALL UNITS ARE IN FEET
 UNLESS OTHERWISE SHOWN

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
MOT TYPICAL SECTIONS
CROSSOVERS AND DETOURS
 FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTIES
 DRAWN BY: DLB CHECKED BY: WTS

FAP 310 MAINLINE RESURFACING (NB LANES)

20 YR. ESAL'S:

MIXTURE USE	POLYMERIZED SURFACE	BINDER	SHOULDER	TOP LIFT SHOULDER	PATCHING
AC/PG	SBS PG 76-22	PG 64-22	PG 64-22	PG 64-22	PG 64-22
RAP%(MAX)	10%	10%	30%	30%	10%
DESIGN AIR VOIDS	4%Ndes=90	4%Ndes=90	2%Ndes=30	1.5%Ndes=30	4%Ndes=90
MIX COMPOSITION (GRADIATION MIXTURE)		IL 19.0			IL 19.0
FRICITION AGG	MIXTURE D	MIXTURE B	BAM	BAM	MIXTURE B

FAP 310 MAINLINE FULL DEPTH PAVEMENT (SB LANES)

20 YR. ESAL'S:

MIXTURE USE	POLYMERIZED SURFACE	BINDER	SHOULDER	TOP LIFT SHOULDER
AC/PG	SBS PG 76-22	PG 64-22	PG 64-22	PG 64-22
RAP%(MAX)	10%	10%	30%	30%
DESIGN AIR VOIDS	4%Ndes=90	4%Ndes=90	2%Ndes=30	1.5%Ndes=30
MIX COMPOSITION (GRADIATION MIXTURE)		IL 19.0		
FRICITION AGG	MIXTURE D	MIXTURE B	BAM	BAM

SEE TYPICAL SECTION FOR FULL DEPTH DETAILS

NORTH INGHAM LANE, JUN ACCESS ROAD, BUNSE WAY,
STAGECOACH ROAD, CROSSOVER C, CROSSOVER B*

20 YR. ESAL'S:

MIXTURE USE	SURFACE	BINDER
AC/PG	PG 64-22	PG 64-22
RAP%(MAX)	10%	10%
DESIGN AIR VOIDS	4%Ndes=70	4%Ndes=70
MIX COMPOSITION (GRADIATION MIXTURE)		IL 19.0
FRICITION AGG	MIXTURE C	MIXTURE B

* CROSSOVER IS BINDER ONLY

ENTRANCES

20 YR. ESAL'S:

MIXTURE USE	INCIDENTAL
AC/PG	PG 64-22
RAP%(MAX)	10%
DESIGN AIR VOIDS	4%Ndes=70
MIX COMPOSITION (GRADIATION MIXTURE)	
FRICITION AGG	MIXTURE C

DATE	
BY	
REVIEWED	
PLANNED	
ALIGNED	
CHECKED	
FILE NAME	
NO.	

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

MIXTURE REQUIREMENTS

FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON AND JERSEY COUNTIES

DRAWN BY: DLB

CHECKED BY: GLF

TREE REMOVAL, ACRES

O/S	LOCATION STATION TO STATION	TREE REMOVAL (ACRES)	TREE COUNT (ESTIMATED)
U. S. ROUTE 67			
RT	227+90 234+18	1.1	140
LT	227+52 238+35	3.3	420
LT	240+40 243+15	0.2	50
LT	262+70 270+00	2.6	250
RT	264+50 270+20	1.1	140
LT	279+60 287+79	2.4	310
RT	283+00 286+53	0.3	80
LT	289+92 295+56	0.8	130
RT	290+61 297+48	0.6	150
RT	298+10 299+87	0.1	40
LT	298+39 303+03	0.6	150
RT	301+74 304+92	0.4	90
LT	306+88 310+24	0.7	75
RT	310+94 318+36	1.2	200
LT	314+83 317+92	0.4	120
LT	318+65 322+36	0.8	100
RT	319+36 322+20	0.3	50
LT	325+23 333+33	2.0	200
RT	327+12 333+78	0.6	120
LT	335+61 336+90	0.1	10
QUARRY ROAD			
LT&RT	56+42 61+10	0.8	90
PROJECT TOTAL		20.40	2915

TREE REMOVAL (6 TO 15 UNITS)

STATION	OFFSET	LT/RT	2001	2010 GROWTH *
247+42.95	45.9	LT	10	13
252+05.85	43.0	LT	10	13
253+57.97	97.7	RT	9	12
253+86.37	97.1	RT	8	10
254+05.18	14.1	LT	11	14
254+26.45	13.6	LT	8	10
254+50.10	14.1	LT	7	9
254+61.56	119.6	RT	6	8
254+61.59	105.2	RT	9	12
255+46.43	109.4	RT	10	13
255+56.06	108.7	RT	6	8
255+65.75	108.1	RT	10	13
255+77.44	118.5	RT	8	10
257+90.28	11.1	LT	11	14
257+96.72	11.9	LT	6	8
288+78.40	60.0	LT	6	8
288+93.53	92.5	LT	10	13
288+95.31	92.8	LT	10	13
305+70.31	146.8	RT	10	13
305+81.43	145.3	RT	9	12
306+00.69	148.8	RT	10	13
306+04.37	150.2	RT	10	13
TOTAL				252

TREE REMOVAL (OVER 15 UNITS)

STATION	OFFSET	LT/RT	2001	DESC	2010 GROWTH *
235+40.89	78.3	RT	43		56
242+56.02	5.3	RT	26		34
243+21.49	0.7	LT	26		34
243+54.88	7.6	RT	27		35
245+67.14	36.1	LT	15		20
245+73.85	62.2	LT	15		20
245+77.37	80.0	LT	21		27
245+81.48	30.4	LT	19		25
245+88.81	62.6	LT	18		23
246+06.21	128.0	LT	33		43
246+09.45	40.7	LT	22		29
246+36.51	36.3	LT	24		31
246+76.37	28.8	LT	24		31
247+05.85	110.1	LT	18		23
247+38.49	98.7	LT	25		33
247+75.28	28.5	LT	15		20
247+94.34	4.4	LT	22		29
247+95.06	3.8	LT	23		30
248+05.00	90.5	LT	20		26
248+72.26	137.7	RT	25		33
248+74.25	4.1	RT	36		47
248+81.32	100.6	RT	23		30
249+03.16	103.1	RT	18		23
249+24.27	69.4	RT	18		23
249+33.68	101.9	RT	26		34
249+38.26	3.5	LT	23		30
249+43.39	37.4	LT	25		33
249+51.10	72.4	RT	18		23
249+79.52	28.4	LT	25		33
249+84.33	136.3	RT	14		18
249+88.50	115.4	RT	16		21
249+89.94	34.0	LT	18		23
249+93.16	43.1	LT	22		29
249+98.57	57.4	LT	46		60
250+00.20	13.1	LT	21		27
250+16.35	88.0	RT	20		26
250+42.95	84.2	RT	22		29
250+51.04	85.5	RT	16	STUMP	21
250+54.46	85.4	RT	16	STUMP	21
250+63.51	86.2	RT	16	STUMP	21
250+65.39	86.8	RT	16	STUMP	21
250+71.49	87.1	RT	16	STUMP	21
250+78.00	87.1	RT	16	STUMP	21
250+79.75	17.5	LT	12		16
250+83.23	87.0	RT	16	STUMP	21
250+91.50	87.1	RT	16	STUMP	21
250+93.35	87.0	RT	16	STUMP	21
250+95.44	87.1	RT	16	STUMP	21
250+98.45	87.2	RT	16	STUMP	21
251+02.42	87.1	RT	16	STUMP	21
251+03.66	87.3	RT	16	STUMP	21
SUBTOTAL 1:					1400

2001 SIZE X 1.305

STATION	OFFSET	LT/RT	2001	DESC	2010 GROWTH *
251+04.88	20.0	LT	20		26
251+05.60	87.3	RT	16	STUMP	21
251+11.74	87.0	RT	16	STUMP	21
251+21.91	90.3	RT	15		20
251+37.36	68.1	LT	14		18
251+42.62	86.1	RT	15		20
251+56.89	63.8	LT	51		67
251+77.94	52.7	LT	15		20
251+79.96	0.6	LT	24		31
252+10.90	93.6	RT	16		21
252+23.04	94.5	RT	19		25
252+35.44	93.3	RT	17		22
252+43.03	23.3	LT	16	STUMP	21
252+46.18	77.1	RT	16		21
252+46.96	93.9	RT	18		23
252+59.66	21.2	LT	22		29
252+66.14	95.4	RT	15		20
252+85.61	130.6	RT	23		30
252+89.29	115.3	RT	19		25
253+15.35	116.4	RT	22		29
253+73.03	43.8	LT	38		50
253+96.64	86.7	LT	32		42
253+97.26	124.5	RT	20		26
254+10.87	53.7	LT	15		20
254+12.39	15.0	LT	13		17
254+17.15	13.8	LT	13		17
254+20.38	69.6	RT	19		25
254+23.19	141.6	RT	20		26
254+32.77	122.4	RT	21		27
254+56.48	154.0	RT	17		22
254+57.49	146.1	RT	13		17
254+59.71	135.0	RT	15		20
254+65.94	96.9	RT	16		21
254+69.45	91.5	RT	16	STUMP	21
254+81.69	87.0	RT	16	STUMP	21
254+85.40	131.8	RT	17		22
254+95.17	88.3	RT	16	STUMP	21
255+07.43	89.8	RT	16	STUMP	21
255+10.11	88.1	RT	16	STUMP	21
255+12.81	14.4	LT	24		31
255+18.74	89.8	RT	16	STUMP	21
255+27.36	100.5	RT	26		34
255+35.06	104.0	RT	24		31
255+44.27	97.0	RT	16	STUMP	21
255+51.09	133.3	RT	19		25
255+51.20	12.1	LT	12		16
255+56.06	108.7	RT	6		8
255+58.73	103.8	RT	16	STUMP	21
255+59.22	77.0	RT	26		34
255+60.95	106.0	RT	16	STUMP	21
255+72.83	122.5	RT	25		33
SUBTOTAL 2:					1263

2001 SIZE X 1.305

STATION	OFFSET	LT/RT	2001	DESC	2010 GROWTH *
255+80.74	71.4	RT	19		25
256+04.67	77.2	RT	14		18
256+14.27	159.6	RT	21		27
256+18.28	87.9	RT	16	STUMP	21
256+44.54	87.3	RT	16	STUMP	21
256+63.47	88.4	RT	16	STUMP	21
256+69.41	87.7	RT	16	STUMP	21
256+77.30	85.0	RT	16	STUMP	21
256+79.87	84.9	RT	16	STUMP	21
256+89.03	87.2	RT	16	STUMP	21
256+94.80	86.1	RT	16	STUMP	21
257+01.25	86.5	RT	16	STUMP	21
257+21.15	89.5	RT	16	STUMP	21
257+45.21	89.6	RT	16	STUMP	21
257+67.75	10.1	LT	30		39
257+77.48	10.9	LT	14		18
257+77.95	79.4	RT	16	STUMP	21
257+86.79	9.2	LT	17		22
258+23.15	9.6	LT	22		29
258+32.28	10.7	LT	14		18
258+86.86	11.7	LT	14		18
259+36.34	11.6	LT	17		22
259+81.59	46.8	LT	15		20
260+84.82	34.6	LT	16		21
260+85.10	32.5	LT	13		17
260+90.93	5.9	LT	13		17
261+06.04	79.7	RT	42		55
261+74.48	35.6	LT	20		26
262+24.88	90.2	RT	34		44
262+27.69	98.9	RT	12		16
270+47.13	87.6	LT	12		16
270+80.27	117.0	LT	16		21
270+85.55	110.4	LT	14		18
271+31.91	97.0	LT	24		31
274+44.29	43.8	LT	20		26
274+64.47	38.4	LT	21		27
274+87.97	39.3	LT	13		17
275+05.52	42.8	LT	12		16
279+68.99	3.7	LT	30		39
287+92.84	10.2	LT	12		16
288+00.89	1.9	LT	36		47
288+24.90	16.8	LT	12		16
288+51.39	97.3	LT	28		37
288+78.31	116.1	LT	14		18
288+79.78	112.6	LT	20		26
289+08.44	16.9	LT	18		23
289+71.05	8.1	LT	32		42
289+71.40	4.3	LT	30		39
301+35.99	72.7	RT	20		26
301+77.78	14.7	LT	20		26
305+62.73	143.8	RT	13		17
305+75.87	145.1	RT	12		16
305+87.24	145.6	RT	16		21
306+10.41	147.0	RT	15		20
306+14.76	149.5	RT	12		16
306+24.43	148.1	RT	12		16
306+27.88	148.0	RT	14		18
310+04.18	89.0	RT	36		47
SUBTOTAL 3:					1402
SUBTOTAL 2:					1263
SUBTOTAL 1:					1400
PROJECT TOTAL:					4065

2001 SIZE X 1.305

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON AND JERSEY COUNTIES

DRAWN BY: RLK

CHECKED BY: CAS

REVISIONS	
NAME	DATE

MISCELLANEOUS REMOVAL

STATION	OFFSET	STATION	OFFSET	PAVEMENT REMOVAL (SQ YD)	PAVED SHOULDER REMOVAL (SQ YD)	DRIVEWAY PAVEMENT REMOVAL (SQ YD)	GUTTER REMOVAL (FT)	CONCRETE HEADWALL REMOVAL (EACH)	REMOVAL OF EXISTING STRUCTURES (EACH)	SLOPE WALL REMOVAL (SQ YD)	REMOVING INLETS (EACH)
EXISTING CROSSOVER											
222+50	CL										1
222+15	LT	222+50	RT	919							
CROSSOVER "B"											
222+32	RT	223+93	CL	133							
224+26	CL	228+14	LT	351							
224+50	RT										1
US 67											
223+00	LT	226+87	LT		172						
225+00	RT	228+11	RT				311				
225+00	CL	228+25	CL	939							
225+00	LT	233+03	LT		446						
225+00	RT	233+03	RT		446						
228+25	CL	233+90	CL	1,632							
230+96	RT										1
231+12	RT										1
232+14	LT							1			
232+73	RT							1			
233+03	LT	235+07	LT		45						
233+03	RT	234+38	RT		30						
233+67	RT	242+46	RT				879				
234+38	RT	242+33	RT		265						
234+61	LT	242+53	LT				792				
235+07	LT	242+33	LT		242						
236+04	LT					158					
242+33	LT	261+53	LT		427						
242+33	RT	261+53			427						
242+99	RT							1			
259+65	RT					117					
261+53	LT	270+53	LT		500						
261+53	RT	270+53	RT		500						
266+28	RT	267+71	RT						150		
270+53	LT	290+78	LT		450						
270+53	RT	290+78	RT		450						
270+67	RT					358					
280+15	RT							1			
280+23	LT-RT					260					
280+50	RT					129					
281+42	LT							1			
282+18	RT					254					
290+78	LT	299+03	LT		458						
290+78	RT	299+03	RT		458						
295+07	RT										2
299+03	CL	299+83	CL	231							
299+03	LT	299+83	LT		18						
299+03	RT	299+83	RT		18						
299+71	LT	309+28	LT				957				
299+74	RT	310+90	RT				1,116				
299+83	CL	310+00	CL	2,712							
299+83	LT	309+38	LT		318						
299+83	RT	309+40	RT		319						
300+12	RT					166					
308+93	LT-RT					223					
309+38	CL	310+43	CL								
310+43	CL	311+00	CL								
322+53	RT	326+60	RT					407			
322+58	RT						161				
329+40	RT	332+20	RT								
330+00	CL	333+20	CL	578							
332+20	LT	333+33	LT		63						
332+20	RT	333+33	RT		63						
334+70		342+58						788			
338+70	LT	342+58	LT		129						
DOMINO LANE											
10+00	LT					72					
NORTH INGHAM											
152+47	RT					85					
STAGECOACH ROAD											
202+11	LT-RT					139					
FROM PIASA BRIDGE											
TOTAL				7,495	6,244	2,122	5,250	4	2	150	6

GUARDRAIL REMOVAL

GUARDRAIL REMOVAL		
STATION TO	STATION	FOOT
228+07	234+79	672
227+83	233+22	539
264+60	271+46	686
264+55	270+00	545
282+93	287+08	415
282+61	286+63	402
289+86	299+74	988
290+09	300+09	1,000
309+10	317+35	825
309+34	316+06	672
319+52	321+66	214
319+52	320+03	51
325+40	328+03	263
326+32	333+19	687
333+01	334+27	126
PROJECT TOTAL		8,085

FILLING EXISTING CULVERTS

STATION	OFFSET	FILLING EXISTING CULVERTS (CU YD)
US 67		
262+20	RT	15.6
266+80	AR	20.6
292+83	AR	17.3
295+31	AR	19.6
299+25	AR	144.2
327+13	AR	81.2
TOTAL		298.5

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTIES

DRAWN BY: RLK

CHECKED BY: CAS

REMOVE EXISTING CULVERTS

STATION	OFFSET	STATION	OFFSET	REMOVE EXISTING CULVERTS (FT)	PIPE DRAIN REMOVAL (FT)	COMMENTS
US 67						
222+00	CL	227+40	CL	546		24" RCP
224+50	LT			21		24" RCP
230+96	AR				39	12" CMP W/ INLET
231+12	RT				32	12" CMP W/ INLET
243+00	AR			67		24" CMPW/HDWL & END SEC
246+75	RT			30		15" CMP
247+44	RT			71		15" CMP
248+30	RT			31		12" CMP
249+22	RT			21		12" CMP
249+62	RT			32		12" CMP
250+27	RT			22		12" CMP
251+32	RT			23		12" CMP
251+47	RT			32		12" CMP
251+71	RT			25		12" CMP
253+40	RT			35		12" CMP
254+67	RT			29		12" CMP
259+65	RT			24		12" CMP
259+75	RT			20		12" CMP
268+15	RT			66		12" CMP
280+24	LT			31		30" CMP
280+35	AR			73		36" CMP W/DROP BOX & HDWL
281+26	LT			42		24" CMP W/HDWL
286+75	RT			61		12" CMP
289+47	RT			26		12" CMP
295+09	RT				35	12" CMP W/ INLET & END SEC
295+09	LT				49	12" CMP W/ INLET & END SEC
322+62	RT			69		12" CMP
328+43	RT			53		12" CMP
QUARRY ROAD						
57+71	AR			50		12" CIP
NORTH INGHAM						
149+11	AR			34		15" CMP
154+35	LT			19		10" PVC
154+92	AR			39		30" CMP
156+00	LT			19		12" CMP
TOTAL				1,611	155	

TEMPORARY ACCESS

STATION	AREA (SQ YD)	THICKNESS (FEET)	AGGREGATE SURFACE COURSE, TYPE B (TONS)	COMMENTS
STAGE 1				
MAINLINE				
RT 251+70	111	0.5	44	
LT 255+35	133	0.5	52	
RT 262+35	69	0.5	27	
LT 278+83	187	0.5	73	
LT 281+11	147	0.5	57	
RT 288+30	111	0.5	44	
LT 300+46	220	0.5	86	
LT 309+33	133	0.5	52	
LT 322+10	200	0.5	78	
LT 322+50	33	0.5	13	
LT 329+00	139	0.5	54	
LT 331+10	147	0.5	58	
LT 335+00	67	0.5	26	
NORTH INGHAM				
LT 146+70	40	0.5	16	
STAGECOACH ROAD				
LT 196+50	67	0.5	26	
LT 198+20	133	0.5	52	
STAGE 2				
MAINLINE				
RT 422+20	40	0.5	16	(AND/OR DOM. SERV.)
RT 246+50	40	0.5	16	(AND/OR DOM. SERV.)
RT 248+20	53	0.5	21	(AND/OR DOM. SERV.)
RT 249+60	40	0.5	16	(AND/OR DOM. SERV.)
RT 251+70	120	0.5	47	(AND/OR DOM. SERV.)
RT 254+70	40	0.5	16	(AND/OR DOM. SERV.)
RT 326+00	52	0.5	20	
NORTH INGHAM				
LT 151+65	53	0.5	21	
LT 152+75	53	0.5	21	
STAGECOACH ROAD				
RT 202+50	40	0.5	16	
RT 204+40	40	0.5	16	
RT 204+80	40	0.5	16	
SCHEDULE TOTAL (1)			999	
USE			1,000	

TEMPORARY EROSION CONTROL SEEDING

STATION TO STATION	STAGE 1 (LBS)	STAGE 2 (LBS)	
MAINLINE			
221+00	228+25	60	100
225+00	240+00		420
228+25	240+00	340	200
240+00	255+00	460	460
255+00	270+00	620	720
270+00	285+00	700	640
285+00	300+00	940	640
300+00	315+00	780	800
315+00	330+00	780	760
330+00	342+00	520	180
QUARRY ROAD			
54+50	60+00	140	
60+00	71+60	420	
DOMINO LANE SERVICE ROAD			
109+00	112+50		460
N. INGHAM			
144+90	149+00	120	
151+00	157+25		180
ACCESS ROAD			
170+00	175+40		140
STAGECOACH ROAD			
194+50	199+00	120	
201+00	205+80		120
JUN ACCESS ROAD			
20+11	25+88		100
STAGE TOTAL		6,000	5,920
PROJECT TOTAL			11,920

GEOTECHNICAL FABRIC FOR GROUND STABILIZATION

LT SIDE	STATION TO STATION	LENGTH (FT)	AVE. HGT. (FT)	DEPTH (FT)	AREA (SQ. YD.)	
	267+50	269+50	200	20	9	4,000
	283+50	285+50	200	28	9	5,600
	315+25	317+35	210	20	9	4,200
	319+50	322+50	300	20	9	6,000
	325+85	328+90	305	22	9	6,710
RT SIDE						
	228+50	229+50	100	35	9	3,500
	283+50	285+50	200	23	9	4,600
	312+25	317+35	510	18	9	9,180
	319+50	321+50	200	15	9	3,000
	327+25	332+25	500	30	9	15,000
			PROJECT	TOTAL		61,790

SHORT TERM PAVEMENT MARKING

STATION TO STATION	SHORT-TERM PAVEMENT MARKING (FEET)	
MAINLINE		
LT 228+25	342+00	1,034
NORTH INGHAM		
CL 145+70	149+15	31
CL 150+75	157+25	59
STAGECOACH ROAD		
CL 194+50	199+30	44
CL 200+70	205+80	46
PROJECT TOTAL		1,214

GUARDRAIL

STATION TO STATION	OFFSET	SPBG TYPE A (FT)	TRAF BAR TERM T2 (EACH)	TRAF BAR TERM T5 (EACH)	TRAF BAR TERM T6 (EACH)	TRAF BAR SPL TAN (EACH)	GUARDRAIL MARKERS, TYPE A (EACH)	BARRIER WALL MARKERS, TYPE C (EACH)	TERMINAL MARKER - DIRECT APPLIED (EACH)
225+31.25	228+68.75	RT	337.5		1	1	5		1
228+25.00	235+25.00	59' LT						10	
229+25.00	231+65.00	59' RT						4	
235+56.25	238+93.75	LT	337.5		1	1	5		1
281+52.50	285+27.50	RT	375.0	1		1	5		1
283+70.77	286+45.77	LT	275.0	1		1	4		1
315+22.49	317+22.49	LT	200.0	1	1		4		
316+54.49		RT				1			1
	317+35.74	RT			1				
316+54.49		RT				1	4		1
	317+35.74	RT			1				
317+35.00	319+69.00	59' RT						4	
317+35.00	319+69.00	9' RT						4	
317+35.00	319+69.00	19' LT						4	
317+35.00	319+69.00	59' LT						4	
319+61.58	319+74.83	RT					4		
319+61.58		LT			1				
	320+42.83	LT				1	4		1
319+92.83	321+92.83	LT	200.0		1	1	4		1
327+01.00	332+76.00	RT	575.0	1		1	6		1
TOTAL			2,300	4	1	6	45	30	9

WORK ZONE PAVEMENT MARKING REMOVAL

STATION TO STATION	WORK ZONE PAVEMENT MARKING REMOVAL (SQ FT)			
STAGE 2				
CROSSOVER "B"	220+00	223+00	LT&RT	200
CROSSOVER "C"	340+00	342+00	LT	67
END STAGE 2				
CROSSOVER "B"	220+00	223+00	RT	100
US 67	226+00	342+00	LT	3,867
TOTAL				4,234

CONCRETE GUTTER

STATION TO STATION	CONCRETE GUTTER, TYPE B (FOOT)	CLASS SI CONCRETE (OUTLET) (CU YD)
271+32	271+44	0.9
271+44	273+18	174
273+18	274+00	7.2
PROJECT TOTAL		174
		8.1

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON AND JERSEY COUNTIES

REVISIONS	NAME	DATE

DRAWN BY: RLK

CHECKED BY: CAS

PAVEMENT

STATION	STATION	OFFSET	HMA PAVEMENT (FULL DEPTH) 14" (SQ YD)	AGGREGATE SHOULDERS, TYPE B (TON)	SUB-BASE GRANULAR MATERIAL, TYPE B (TON)	SUB-BASE GRANULAR MATERIAL, TYPE C 6" (SQ YD)	SUB-BASE GRANULAR MATERIAL, TYPE A 4" (SQ YD)	HMA BINDER COURSE N90, 12" (SQ YD)	AGGREGATE BASE COURSE, TYPE A (TON)	AGGREGATE BASE COURSE, TYPE B (TON)	AGGREGATE SURFACE COURSE, TYPE A (TON)	AGGREGATE SURFACE COURSE, TYPE B (TON)	HMA SHOULDERS 8" (SQ YD)	SHOULDER RUMBLE STRIP (FOOT)	HMA SURFACE REMOVAL - BUTT JOINT (SQ YD)	AGGREGATE SURFACE COURSE, TYPE B 6" (SQ YD)	HMA SURF CSE MIX C, N70 (TON)	HMA SURF CSE MIX D, N90 (TON)	HMA BIND CSE IL-19, N90 (TON)	HMA BIND CSE IL-19, N70 (TON)	LEVELING BINDER MACHINE (MACHINE METHOD), N70 (TON)	BITUMINOUS MATERIALS (PRIME COAT) (TON)	BITUMINOUS MATERIALS (COVER AND SEAL COATS) (TON)	COVER COAT AGGREGATE (TON)	SEAL COAT AGGREGATE (TON)	BITUMINOUS MATERIALS (PRIME COAT) (TON)	INCIDENTAL HOT-MIX ASPHALT SURFACING (TON)	BRIDGE APPROACH PAVEMENT (SQ YD)
US 67				13,442																								
225+00.00	337+00.00																											
225+00.00	333+19.67	NB																										
225+00.00	333+32.60	49' RT																										
225+00.00	333+32.50	25' RT											10,745	10,059														
225+00.00	233+90.00	NB	2,373										4,547	9,734														
233+90.00	300+10.00	NB																										
243+00.49	249+25.50	LT TURN																										
243+30.49	248+95.50	RT TURN																										
257+11.43	260+60.72	LT TURN																										
256+58.52	262+93.52	RT TURN																										
270+70.00	284+25.00	NB				1,846																						
282+42.53	288+78.70	LT TURN																										
283+04.90	289+04.23	RT TURN																										
293+50.00	300+10.00	NB																										
310+00.00	310+69.00	NB																										
300+10.00	317+05.70	NB	4,522																									
315+75.70	317+05.70	LT TURN																										
317+05.74	317+35.74	NB																										
319+61.58	319+91.58	NB																										
319+91.60	322+76.19	NB	759																									
319+91.60	323+71.19	LT TURN	661																									
322+76.19	331+32.50	NB	2,283																									
333+02.50	333+32.50	NB																										
225+52.27	226+87.27	LT TURN																										
228+25.00	248+30.49	SB	5,348																									
228+25.00	337+00.00	SB																										
228+25.00	342+57.08	49' LT																										
228+25.00	339+64.00	25' LT											11,370	10,041														
247+35.49	253+60.49	LT TURN	794										4,838	9,790														
247+65.49	253+30.49	RT TURN	570																									
248+30.49	262+23.15	SB	3,714																									
260+56.94	267+39.00	LT TURN	877																									
261+79.17	267+94.61	RT TURN	624																									
262+23.15	288+00.00	SB	6,872																									
270+70.00	284+25.00	SB				4,252																						
287+21.30	293+57.48	LT TURN	894																									
286+95.77	292+86.49	RT TURN	603																									
288+00.00	317+05.74	SB	7,749																									
293+50.00	310+69.00	SB				5,394																						
317+05.74	317+35.74	SB																										
319+61.58	319+91.58	SB																										
319+91.58	322+76.19	SB	759																									
321+81.19	328+06.19	LT TURN	794																									
322+76.19	337+00.00	SB	3,797																									
CROSSOVER "B"							607																					
CROSSOVER "C"								1,032									87	87										
TRANSITION TAPER																												
331+33.00	332+25.00	NB																										
332+25.00	333+33.00	NB																										
QUARRY ROAD																												
JUN ACCESS ROAD																												
CONNECTOR ROAD		X-OVER																										
CONNECTOR ROAD																												
DOMINO LANE SERVICE ROAD																												
DOMINO LANE																												
NORTH INGHAM DETOUR																												
NORTH INGHAM		X-OVER																										
145+70.00	149+32.69																											
150+66.62	157+25.00																											
INGHAM ACCESS DRIVE																												
STAGECOACH ROAD		X-OVER																										
194+50.00	199+37.00																											
200+63.00	205+80.00																											
PRIVATE ROAD 327+79.19 LT		X-OVER																										
PRIVATE ROAD 327+79.19 LT																												
SUB-TOTAL			44,420	13,851	15,684	31,500	607	5,415	7,040																			

ENTRANCE PAVEMENT

STATION	OFFSET	LENGTH (FT)	WIDTH (FT)	INCIDENTAL HOT-MIX ASPHALT SURFACING (TON)	AGGREGATE BASE COURSE, TYPE B (TON) (1)	AGGREGATE SURFACE COURSE, TYPE B 6" (SQ YD)
U. S. ROUTE 67						
255+35	PE LT	61	12			81
272+27	FE LT	111	20			247
278+43	PE LT	19	12		12	
278+83	PE LT	105	12	24	72	240
281+11	PE LT	105	12			140
300+46	FE LT	162	20			360
300+46	PE RT	111	12	25	76	
309+33	PE LT	61	20	23	69	
325+97	FE LT	81	20			180
331+35	PE LT	56	20			124
335+00	FE LT	198	12			264
DOMINO LANE SERVICE ROAD						
99+13	FE RT	50	10			56
103+42	FE RT	22	12			82
105+29	PE RT	67	12			100
106+60	PE RT	30	12			51
110+45	PE RT	30	12			51
111+64	PE RT	30	12			51
113+93	CE LT	61	20	15	41	
NORTH INGHAM						
146+50	FE RT	45	20			111
146+68	PE LT	VAR	71			221
151+64	PE LT	40	14			73
152+75	PE LT	96	20			224
STAGECOACH ROAD						
198+20	PE LT	67	12			100
202+50	PE RT	67	12			100
203+50	PE LT	VAR	VAR	20	54	
204+38	PE RT	32	12			54
204+77	PE RT	40	12			64
SCHEDULE TOTAL (1)				107	324	2,974

(1) NOT PROJECT TOTAL

PERIMETER EROSION BARRIER

LOCATION	STATION TO	STATION	PERIMETER EROSION BARRIER (FT)
US 67			
LT	228+25	229+90	165
RT	225+00	229+30	430
LT	230+45	238+50	3,155
RT	229+70	242+00	3,130
LT	249+25	262+00	3,775
RT	256+50	261+00	3,125
LT	263+50	287+00	2,350
RT	262+25	287+75	2,550
LT	288+50	318+00	2,950
RT	288+75	318+40	2,965
LT	318+75	343+50	2,475
RT	319+00	334+00	1,500
QUARRY ROAD			
LT	54+75	66+85	1,210
LT	68+00	71+60	360
RT	55+00	61+00	600
DOMINO LANE SERVICE DRIVE			
RT	99+00	108+25	925
RT	108+75	113+65	
DOMINO LANE			
LT	10+15	10+80	65
RT	10+50	10+80	30
NORTH INGHAM			
LT	144+90	148+30	340
RT	144+90	148+90	400
LT	151+00	155+25	425
RT	151+50	157+25	575
LT	156+00	157+25	125
INGHAM ACCESS DR			
LT	170+50	174+75	425
RT	170+40	174+80	440
STAGECOACH ROAD			
LT	194+50	199+00	450
RT	194+50	198+50	400
LT	201+50	205+80	430
RT	201+00	203+20	220
RT	203+80	205+80	200
JUN ACCESS			
LT	20+25	22+90	265
LT	23+25	24+65	140
TOTAL			36,595

INLET AND PIPE PROTECTION

STATION	EACH	STATION	EACH
MAINLINE			
CL 232+26	1	LT 57+72	1
CL 232+98	1	RT 63+00	1
RT 243+00	1	CONNECTOR ROAD	
CL 243+23	1	LT 50+89	1
CL 249+30	1	LT 52+71	1
CL 261+00	1	DOMINO LN SERVICE RD	
RT 267+51	1	RT 102+00	1
CL 272+00	1	DOMINO LANE	
CL 280+00	1	LT 10+30	1
RT 284+79	1	N. INGHAM	
CL 286+50	1	RT 149+09	1
CL 293+00	1	RT 151+40	1
RT 293+00	1	CL 307+25	1
RT 296+48	1	CL 317+00	1
CL 299+25	1	CL 320+00	1
RT 299+25	1	CL 323+40	1
CL 307+25	1	LT 326+00	1
CL 317+00	1	RT 327+80	1
CL 320+00	1	CL 333+46	1
CL 323+40	1	STAGECOACH ROAD	
LT 326+00	1	LT 194+66	1
RT 327+80	1	JUN ACCESS ROAD	
CL 333+46	1	RT 21+69.4	1
		LT 23+50	1
PROJECT TOTAL			36

PERMANENT DITCH CHECKS (FOR INFORMATION ONLY)

STATION	(EACH)
CL 243+00	1
CL 272+23	1
CL 280+23	1
CL 299+45	1
CL 307+48	1
CL 333+23	1
TOTAL	6

NOTE: PERMANENT DITCH CHECKS ARE INCLUDED FOR PAYMENT AS EARTHWORK.

STRIP REFLECTIVE CRACK CONTROL TREATMENT

STA	TO	STA	OFFSET	STRIP REFLECTIVE CRACK CONTROL TREATMENT (FT)
243+00.50		249+25.50	25' RT	625
243+30.50		248+95.50	49' RT	565
257+11.40		263+60.70	25' RT	649
256+58.50		262+71.30	49' RT	613
282+42.50		288+78.70	25' RT	636
283+04.90		289+04.20	49' RT	599
319+51.90		323+71.20	25' RT	419
PROJECT TOTAL				4,107

PROCESSING MODIFIED SOILS

LOCATION	PROCESSING MODIFIED SOIL 12" (SQ YD)	LIME (TON)
MAINLINE & SHOULDERS	63,818	1,723
TOTAL	63,818	1,723

PAVEMENT PATCHING

STATION	PAVEMENT PATCHING CLASS D, 14" TY 2 (SQ YD)	PAVEMENT PATCHING CLASS D, 14" TY 3 (SQ YD)
232+00	14.0	
235+50		20.2
243+00		20.7
TOTAL	14	41

AN ESTIMATED QUANTITY FOR TEMPORARY DITCH CHECKS - 25.00 FT. IS INCLUDED TO BE USED AS DIRECTED BY THE ENGINEER.

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON AND JERSEY COUNTIES

REVISIONS	
NAME	DATE

DRAWN BY: RLK

CHECKED BY: CAS

EROSION CONTROL BLANKET

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	33
STA.		TO STA.		
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

LOCATION	FINAL STATION	FINAL STATION	USE LINING WIDTH (FT)	FLOW (CFS)	LOCATION	EROSION CONTROL BLANKET (SQ YD)	
US RTE 67							
LEFT	236+00	240+00	8.0	2.4	DITCH LINING	356	
	240+70	240+73	6.0	19.7	DITCH LINING	2	
	240+73	243+00	12.0	19.7	DITCH LINING	303	
	243+00	247+98	8.0	13.4	DITCH LINING	443	
	248+64	252+00	8.0	6.2	DITCH LINING	299	
	256+00	260+00	8.0	2.2	DITCH LINING	356	
	260+00	262+06	8.0	6.9	DITCH LINING	184	
	263+70	270+00			SIDESLOPE	2806	
	263+38	266+00	12.0	15.0	DITCH LINING	350	
	278+00	280+00	8.0	3.3	DITCH LINING	178	
	280+00	281+50	8.0	3.7	DITCH LINING	134	
	282+00	286+00			SIDESLOPE	3189	
	284+59	287+00	8.0	2.9	DITCH LINING	215	
	301+00	306+00	8.0	3.8	DITCH LINING	445	
	306+00	307+25	8.0	4.2	DITCH LINING	112	
	307+25	309+00	12.0	9.4	DITCH LINING	234	
	310+00	3120+00			SIDESLOPE	1216	
	315+00	317+50			SIDESLOPE	1479	
	318+49	323+00			SIDESLOPE	2109	
	325+75	329+00			SIDESLOPE	2029	
	326+50	328+00	12.0	2.2	DITCH LINING	200	
	330+20	333+80	12.0	14.7	DITCH LINING	480	
	331+00	335+00			SIDESLOPE	1985	
	333+80	337+00	8.0	6.0	DITCH LINING	285	
	336+00	342+00			SIDESLOPE	4847	
	337+00	342+00	8.0	3.7	DITCH LINING	445	
	RIGHT	225+00	228+00	12.0	9.4	DITCH LINING	400
		228+25	229+61			SIDESLOPE	1003
		230+30	232+00			SIDESLOPE	440
		232+00	232+98	8.0	0.8	DITCH LINING	88
		236+00	242+50	8.0	5.2	DITCH LINING	578
		243+00	246+00	12.0	15.9	DITCH LINING	400
		246+00	247+94	8.0	5.7	DITCH LINING	173
248+61		250+00	8.0	3.0	DITCH LINING	124	
250+00		252+00	8.0	2.2	DITCH LINING	178	
255+00		261+19	8.0	3.0	DITCH LINING	551	
262+13		265+00	8.0	4.8	DITCH LINING	256	
262+50		266+00			SIDESLOPE	1280	
267+50		270+00	8.0	4.8	DITCH LINING	223	
270+00		271+32	12.0	2.5	DITCH LINING	176	
274+00		281+00	12.0	4.9	DITCH LINING	934	
281+00		283+00	8.0	6.1	DITCH LINING	178	
283+00		286+00			SIDESLOPE	1568	
284+85		286+35	8.0	3.0	DITCH LINING	134	
290+00		291+00	8.0	2.8	DITCH LINING	89	
291+00		292+98	12.0	4.8	DITCH LINING	264	
292+98		293+02	6.0	4.8	DITCH LINING	3	
293+02		294+00	8.0	3.3	DITCH LINING	88	
294+00		296+34	8.0	2.2	DITCH LINING	208	
296+34		296+38	6.0	2.2	DITCH LINING	3	
296+38		298+50	8.0	1.5	DITCH LINING	189	
298+50		299+20	8.0	1.0	DITCH LINING	63	
299+20		299+28	8.0	1.0	DITCH LINING	8	
299+60		310+00	12.0	7.7	DITCH LINING	1387	
301+00		306+00			SIDESLOPE	4251	
312+00		317+69			SIDESLOPE	2828	
316+00		318+31	8.0	12.6	DITCH LINING	206	
318+90		321+00	8.0	5.5	DITCH LINING	187	
319+38		322+00			SIDESLOPE	1225	
323+00	325+75	8.0	2.7	DITCH LINING	245		
323+00	325+40			SIDESLOPE	1120		
327+00	333+00			SIDESLOPE	4820		
MEDIAN	225+00	230+89	12.0	9.1	DITCH LINING	786	
	232+26	242+74	12.0	8.3	DITCH LINING	1398	
	243+20	247+30	8.0	4.4	DITCH LINING	365	
	249+30	254+44	8.0	4.4	DITCH LINING	457	
	254+44	261+00	8.0	6.2	DITCH LINING	584	
	263+60	272+00	8.0	7.5	DITCH LINING	747	
	272+50	280+00	8.0	5.5	DITCH LINING	667	
	280+50	286+50	8.0	4.8	DITCH LINING	534	
	289+20	293+00	8.0	4.3	DITCH LINING	338	
	293+00	294+15	8.0	1.2	DITCH LINING	103	
	294+15	295+00	8.0	0.8	DITCH LINING	76	
	295+00	299+25	8.0	4.1	DITCH LINING	378	
	299+75	307+25	12.0	6.6	DITCH LINING	1000	
	307+75	317+00	8.0	7.4	DITCH LINING	823	
	320+00	321+90	8.0	2.7	DITCH LINING	169	
	323+70	333+00	8.0	7.6	DITCH LINING	827	
	333+46	337+50	8.0	2.5	DITCH LINING	360	

LOCATION	FINAL STATION	FINAL STATION	USE LINING WIDTH (FT)	FLOW (CFS)	LOCATION	EROSION CONTROL BLANKET (SQ YD)	
QUARRY ROAD							
LEFT	55+00	57+72	4.0	0.9	DITCH LINING	121	
	57+72	61+00	4.0	1.3	DITCH LINING	146	
	55+00	61+00			SIDESLOPE	1408	
	63+65	64+25	8.0	8.1	DITCH LINING	54	
	64+25	64+60	8.0	8.0	DITCH LINING	32	
	67+50	70+00	4.0	1.7	DITCH LINING	112	
	70+00	71+60	4.0	1.2	DITCH LINING	72	
	57+72	59+00	4.0	0.9	DITCH LINING	57	
	59+00	62+00	4.0	1.0	DITCH LINING	134	
	59+00	62+00			SIDESLOPE	629	
66+00	70+00	4.0	1.2	DITCH LINING	178		
71+06	71+60	4.0	0.9	DITCH LINING	24		
CONNECTOR							
LEFT	50+30	50+86	4.0	1.1	DITCH LINING	25	
	52+71	53+25	4.0	1.0	DITCH LINING	24	
RIGHT	50+36	50+89	4.0	0.3	DITCH LINING	24	
DOMINO LANE SERVICE ROAD							
LEFT	102+00	104+00	4.0	0.9	DITCH LINING	89	
	105+65	108+00	4.0	0.9	DITCH LINING	105	
RIGHT	100+00	102+00	8.0	2.1	DITCH LINING	178	
	102+00	106+00	8.0	6.2	DITCH LINING	356	
	106+00	108+28	8.0	2.1	DITCH LINING	203	
	108+56	110+00	4.0	0.6	DITCH LINING	64	
	110+00	112+25	4.0	0.4	DITCH LINING	100	
DOMINO LANE							
RIGHT	10+30	10+50	4.0	0.8	DITCH LINING	9	
NORTH INGHAM							
LEFT	146+80	148+50			SIDESLOPE	428	
147+00	147+50	4.0	0.6	DITCH LINING	23		
147+50	148+59	4.0	1.3	DITCH LINING	49		
151+00	151+50	4.0	0.6	DITCH LINING	23		
151+80	154+00	4.0	1.2	DITCH LINING	98		
152+00	154+00			SIDESLOPE	539		
154+00	154+88	4.0	1.4	DITCH LINING	40		
154+88	154+93	6.0	1.4	DITCH LINING	4		
156+00	157+20	4.0	0.7	DITCH LINING	54		
156+50	157+30			SIDESLOPE	105		
RIGHT	147+00	148+00	4.0	0.6	DITCH LINING	45	
	148+00	148+50	4.0	0.6	DITCH LINING	23	
	148+50	149+10	4.0	1.1	DITCH LINING	27	
	151+64	154+50			SIDESLOPE	756	
	152+00	154+00	4.0	1.1	DITCH LINING	89	
	154+00	154+88	4.0	1.4	DITCH LINING	40	
	154+88	154+92	6.0	1.4	DITCH LINING	3	
	154+92	156+00	8.0	2.6	DITCH LINING	96	
	156+00	157+20	4.0	2.2	DITCH LINING	54	
	ACCESS ROAD						
	LEFT	172+25	174+00	4.0	0.4	DITCH LINING	78
	RIGHT	170+25	172+00	8.0	2.9	DITCH LINING	156
		172+00	172+50	8.0	3.0	DITCH LINING	45
172+50	174+00	8.0	3.4	DITCH LINING	134		
STAGECOACH ROAD							
LEFT	194+50	199+00			SIDESLOPE	1247	
	194+50	194+63	4.0	0.3	DITCH LINING	6	
	194+63	194+68	6.0	0.5	DITCH LINING	4	
	194+68	195+00	4.0	0.5	DITCH LINING	15	
	195+00	196+15	4.0	0.5	DITCH LINING	52	
	201+00	201+50	4.0	2.3	DITCH LINING	23	
	201+00	203+40			SIDESLOPE	814	
	RIGHT	198+00	198+75	4.0	0.9	DITCH LINING	34
		200+73	202+40			SIDESLOPE	374
		200+85	201+50	4.0	0.7	DITCH LINING	29
203+75		205+80	4.0	0.8	DITCH LINING	92	
JUN ACCESS							
RIGHT	20+20	21+00	4.0		DITCH LINING	36	
	21+00	21+69	4.0		DITCH LINING	31	
	21+69	22+50	8.0		DITCH LINING	72	
	22+50	24+50	4.0		DITCH LINING	89	
	24+50	25+00	4.0		DITCH LINING	23	
SCHEDULE TOTAL						70,155	

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON AND JERSEY COUNTIES

DRAWN BY: RLK
CHECKED BY: CAS

*ADDITIONAL
 *CONTRACT
 *CON-SPEC
 *REF.

PIPE UNDERDRAINS

STATION (USFL)	STATION (DSFL)	PIPE UNDERDRAINS 4" (FT)	PIPE UNDERDRAINS 4" (SPECIAL) (FT)	PIPE DRAIN 24" (FT)	CONCRETE THRUST BLOCK (EACH)	CONCRETE HEADWALL FOR PIPE DRAINS (EACH)
NORTHBOUND LANE (OUTSIDE)						
225+00.00	230+89.00	589	12			1
254+44.00	250+00.00	444	22			1
250+00.00	245+00.00	500	15			1
245+00.00	240+00.00	500	22			1
240+00.00	235+00.00	500	22			1
235+00.00	230+89.00	411	12			1
254+44.00	260+00.00	556	15			1
260+00.00	265+00.00	500	22			1
265+00.00	270+00.00	500	22			1
270+00.00	275+00.00	500	22			1
275+00.00	280+00.00	500	22			1
280+00.00	285+00.00	500	9			1
285+00.00	290+00.00	500	22			1
286+50.00				43		
290+00.00	295+00.00	500	22			1
293+00.00				108		
295+00.00	300+00.00	500	22			1
300+00.00	305+00.00	500	22			1
305+00.00	310+00.00	500	22			1
310+00.00	315+00.00	500	22			1
315+00.00	317+00.00	200	22			1
333+20.00	328+20.00	500	15			1
328+20.00	324+00.00	420	22			1
324+00.00	320+00.00	400	22			1
NORTHBOUND LANE (MEDIAN)						
225+00.00	230+89.00	589	18			1
254+44.00	250+00.00	444	13			1
250+00.00	245+00.00	500	14			1
245+00.00	240+00.00	500	18			1
240+00.00	235+00.00	500	18			1
235+00.00	230+89.00	411	18			1
254+44.00	260+00.00	556	13			1
260+00.00	265+00.00	500	12			1
265+00.00	270+00.00	500	18			1
270+00.00	275+00.00	500	18			1
275+00.00	280+00.00	500	18			1
280+00.00	285+00.00	500	13			1
285+00.00	290+00.00	500	12			1
290+00.00	295+00.00	500	18			1
295+00.00	300+00.00	500	18			1
300+00.00	305+00.00	500	18			1
305+00.00	310+00.00	500	18			1
310+00.00	315+00.00	500	18			1
315+00.00	317+00.00	200	21			1
333+20.00	328+20.00	500	18			1
328+20.00	324+00.00	420	12			1
324+00.00	320+00.00	400	13			1
SOUTHBOUND LANE (OUTSIDE)						
254+44.00	250+00.00	444	15			1
250+00.00	245+00.00	500	22			1
245+00.00	240+00.00	500	22			1
240+00.00	235+40.00	460	22			1
235+40.00	230+89.00	451	45			1

PIPE UNDERDRAINS

STATION (USFL)	STATION (DSFL)	PIPE UNDERDRAINS 4" (FT)	PIPE UNDERDRAINS 4" (SPECIAL) (FT)	PIPE DRAIN 24" (FT)	CONCRETE THRUST BLOCK (EACH)	CONCRETE HEADWALL FOR PIPE DRAINS (EACH)
254+44.00	260+00.00	556	22			1
260+00.00	265+00.00	500	15			1
265+00.00	270+00.00	500	22			1
270+00.00	275+00.00	500	22			1
275+00.00	279+95.00	495	22			1
280+05.00	285+00.00	495	15			1
285+00.00	290+00.00	500	15			1
286+50.00					1	
290+00.00	295+00.00	500	22			1
293+00.00					1	
295+00.00	300+00.00	500	22			1
300+00.00	305+00.00	500	22			1
305+00.00	310+00.00	500	22			1
310+00.00	315+00.00	500	22			1
315+00.00	316+95.00	195	15			1
337+00.00	334+50.00	250	22			1
334+50.00	330+00.00	450	22			1
330+00.00	325+00.00	500	22			1
325+00.00	320+05.00	495	15			1
SOUTHBOUND LANE (MEDIAN)						
228+25.00	230+89.00	264	18			1
254+44.00	249+34.00	510	13			1
249+25.00	245+00.00	425	18			1
245+00.00	240+00.00	500	18			1
240+00.00	235+40.00	460	18			1
235+40.00	230+89.00	451	18			1
254+44.00	257+44.00	300	16			1
257+44.00	260+94.00	350	12			1
261+05.00	265+00.00	395	13			1
265+00.00	267+50.00	250	18			1
267+50.00	271+95.00	445	18			1
272+05.00	277+00.00	495	18			1
277+00.00	279+95.00	295	18			1
280+05.00	285+00.00	495	12			1
285+00.00	290+00.00	500	13			1
290+00.00	295+00.00	500	18			1
295+00.00	300+00.00	500	18			1
300+00.00	305+00.00	500	18			1
305+00.00	307+20.00	220	18			1
307+30.00	310+00.00	270	18			1
310+00.00	315+00.00	500	18			1
315+00.00	316+95.00	195	18			1
337+00.00	333+50.00	350	18			1
333+40.00	330+00.00	340	18			1
330+00.00	325+00.00	500	13			1
325+00.00	320+05.00	495	12			1
PROJECT TOTAL		41,836	1,680	151	2	92

DATE-TIME
 CON-SPEC
 REF
 REF

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON AND JERSEY COUNTIES

DRAWN BY: RLK CHECKED BY: CAS

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	35

PROTECTIVE LINING

EXISTING CONDITIONS:
CONTRACT NO. 76318

LOCATION	STATION TO	STATION	LENGTH (FT)	Sideslope Left (X:1)	Sideslope Right (X:1)	Bottom Width (FT)	Depth (FT)	WIDTH (FT)	RIPRAP THICKNESS (FT)	STONE RIPRAP CLASS					FILTER FABRIC (SQ YD)
										A2 (SQ YD)	A4 (SQ YD)	A5 (SQ YD)	A6 (SQ YD)	A7 (SQ YD)	
US RTE 67 LEFT	228+25	229+28	103	3	3	4	1.0	10.3	1.9			118			118
	229+28	229+92	64	3	3	4	1.0	10.3	1.4		73				73
	235+21	235+94	73						1.4		150				150
	243+00								1.4		29				29
	260+00	262+06	206	3	6	4	1.0	13.2	1.4		302				302
	266+00	267+00	100	2.5	4	4	1.0	10.8	2.2				120		120
	267+00	269+49	249	2	2.25	4	1.0	8.7	1.4			241			241
	272+00								1.4		19				19
	281+50	284+55	305	2.4	3	4	1.0	9.8	1.4		332				332
	284+55	284+59	4						1.9			29			29
	284+59	287+00	241	3	2.5	4	1.0	9.9	1.4		265				265
	306+00	307+25	125	3	6	4	1.0	13.2	1.4		183				183
	309+11								1.4		17				17
	309+62	310+00	38	3	3	4	1.0	10.3	1.4		43				43
	310+00	311+26	126	2.5	2.5	4	1.0	9.4	1.4		132				132
	318+65	319+38	73	2.5	2	4	2.5	16.3	1.4		132				132
	328+00	329+50	150	2	2	2	2.0	10.9	1.9			182			182
	329+50	330+21	71	3	6	4	1.0	13.2	2.5					104	104
	330+83	331+09	26	3	6	4	1.0	13.2	1.4		38				38
	333+46	334+00	54	2	6	4	1.0	12.3	1.4		74				74
334+55	334+82	27	2	6	4	1.0	12.3	1.4		37				37	
RIGHT	228+00	229+47	147	2.5	3	4	1.0	9.9	2.5					162	162
	231+95	232+95	100	3	3	4	1.0	10.3	1.4		114				114
	232+95	233+03							1.4		19				19
	233+03	236+00	297	3	3	4	1.0	10.3	1.4		340				340
	265+00	266+00	100	4.5	2.5	5.5	1.0	12.8	1.9			142			142
	266+00	267+43	143	3	2.5	10.5	6.0	45.6	2.5					725	725
	267+50	270+00	250	3	3	4	1.0	10.3	1.4		286				286
	283+00	284+75	175	3.4	3	4	1.0	10.7	1.4		208				208
	284+75	284+85	10						1.4		27				27
	284+85	286+35	150	2.5	2.5	4	1.0	9.4	1.4		157				157
	291+00	292+98	198	3.8	3	4	1.0	11.1	1.4		244				244
	293+02	299+20	618	3	3	4	1.0	10.3	1.4		707				707
	310+00	312+00	200	3.8	3	4	1.0	11.1	1.4		247				247
	312+00	313+00	100	2.5	3	4	1.0	9.9			110				110
	321+00	323+00	200	3.5	3	4	1.0	10.8	1.4		240				240
	329+80	330+00	20	2	3	4	1.0	9.4	1.9			21			21
	330+00	332+00	200	2.5	3	4	1.0	9.9	1.4		220				220
	332+00	333+46	146	2.75	3	4	1.0	10.1	1.4		164				164
Stage 1 RIGHT	311+00	318+25	725	2	2	2	0.5	4.2	0.5	338					
RIGHT	318+55	331+33	1278	2	2	2	0.5	4.2	0.5	596					
QUARRY ROAD															
	LEFT	62+78	63+65	87					1.4		58				58
RIGHT		65+24						1.4		29				29	
INGHAM ACCESS DRIVE															
	LEFT	174+50	174+83	33	3	3	2	1.0	8.3	1.4	30				30
RIGHT	174+50	174+83	33	3	3	2	1.0	8.3	1.4	30				30	
JUN ACCESS															
	LEFT	21+31							1.4		29				29
SUBTOTAL										934	5326	492	120	991	6929
FROM RIPRAP ENERGY DISSIPATOR											226	164	157		547
FROM RIPRAP LINED ENERGY DISSIPATING CHANNEL											83		245		328
FROM PIASA BRIDGE												3,914			3,914
SCHEDULE TOTAL										934	5,635	4,570	522	991	11,718

- * DEPTH MEASURED AT RIGHT ANGLE FROM CENTERLINE OF FINISH DITCH TO UPPER LIMIT OF SIDE SLOPE PROTECTION
- ** WIDTH MEASURED ALONG SIDE SLOPES AND BOTTOM OF DITCH
- *** SIDESLOPES ARE AVERAGED ALONG DITCH LENGTH. REFERENCE CROSS SECTIONS FOR SIDESLOPES AND STATIONS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SCHEDULE OF QUANTITIES
FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON AND JERSEY COUNTIES

DRAWN BY: RLK CHECKED BY: CAS

#DATE##
#DOK#SPEC
#REF-
#REF-

EARTHWORK

LOCATION	1	2	3	4
	EARTH EXCAVATION (CU YD)	*EARTH EXCAVATION (ADJUSTED FOR 25% SHRINKAGE) (CU YD)	*EMBANKMENT (FILL) (CU YD)	EARTHWORK BALANCE WASTE (+) OR FURNISHED EXCAVATION (-) (COL 2 - COL 3) (CU YD)
STAGE 1A				
US 67	184,999	138,749	341,493	-202,744
CONNECTOR	1,951	1,463	0	1,463
STAGECOACH	10,179	7,634	33	7,601
335 PRIVATE ENTRANCE	3,313	2,485	490	1,995
INGHAM RUNAROUND	745	559	1,699	-1,140
TRINTY RUNAROUND	17	13	823	-810
SUBTOTAL STAGE 1A	201,204	150,903	344,538	-193,635
STAGE 1B				
QUARRY ROAD	11,607	8,705	7,020	1,685
INGHAM	5,101	3,826	2,922	904
JUN	960	720	440	280
SUBTOTAL STAGE 1B	17,668	13,251	10,382	2,869
STAGE 2				
US 67	78,863	59,147	88,596	-29,449
CONNECTOR	815	611	88	523
DOMINO SERVICE LANE	3,435	2,576	1,324	1,252
DOMINO LANE	93	70	1	69
INGHAM	6,532	4,899	605	4,294
INGHAM ACCESS DRIVE	782	587	1,311	-724
STAGECOACH	9,441	7,081	1	7,080
SUBTOTAL STAGE 2	99,961	74,971	91,926	-16,955
DITCH CHECKS	0	0	72	-72
TOTAL	318,833	239,125	446,846	-207,793
*TOTAL EXCESS EXCAVATION				2,869

EXCESS EXCAVATION IN STAGE 1B SHALL BE USED TO REDUCE FURNISHED EXCAVATION IN STAGE 2
ITEMS MARKED WITH AN ASTERISK(*) ARE FOR INFORMATION ONLY.

SEEDING

LOCATION	SEEDING CLASS 2 (ACRE)	SEEDING CLASS 3 (ACRE)	MULCH METHOD 2 (ACRE)	NITROGEN FERTILIZER NUTRIENT (LB)	PHOSPHORUS FERTILIZER NUTRIENT (LB)	POTASSIUM FERTILIZER NUTRIENT (LB)	AGRICULTURAL GROUND LIMESTONE (TON) (CLASS 3)
US 67							
STAGE 1	18.25	3.00	18.25	1,913	1,913	1,913	6.0
STAGE 2	12.50	3.00	12.50	1,395	1,395	1,395	6.0
QUARRY ROAD							
STAGE 1	1.75	0.50	1.75	203	203	203	1.0
CONNECTOR							
STAGE 1	0.25	0.00	0.25	23	23	23	
DOMINO LANE SERVICE ROAD							
STAGE 2	1.25	0.00	1.25	113	113	113	
DOMINO LANE							
STAGE 2	0.25	0.00	0.25	23	23	23	
NORTH INGHAM							
STAGE 1	0.75	0.25	0.75	90	90	90	0.5
STAGE 2	0.50	0.25	0.50	68	68	68	0.5
INGHAM ACCESS DR							
STAGE 2	0.50	0.25	0.50	68	68	68	0.5
STAGE COACH ROAD							
STAGE 1	0.75	0.25	0.75	90	90	90	0.5
STAGE 2	0.75	0.25	0.75	90	90	90	0.5
JUN ACCESS ROAD							
STAGE 2	0.50	0.0	0.50	45	45	45	
335 PRIVATE ROAD							
STAGE 1	0.25	0.3	0.25	45	45	45	0.5
EROSION CONTROL BLANKET DEDUCT							
TOTAL	38.25	8.00	36.00	4,166	4,166	4,166	16.0

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTIES

DRAWN BY: RLK

CHECKED BY: CAS

FURNISHING AND ERECTING RIGHT OF WAY MARKERS

ROAD	STATION	OFFSET	EACH	ROAD	STATION	OFFSET	EACH	
QUARRY RD	54+86.48	40.00 LT	1		175+31.63	97.26 RT	1	
	55+16.89	40.00 RT	1		175+43.49	39.31 LT	1	
	55+64.05	40.00 LT	1		175+56.08	33.36 RT	1	
	55+64.05	60.00 LT	1	STAGECOACH RD	194+40.00	50.00 RT	1	
	55+64.05	40.00 RT	1		194+40.00	14.29 RT	1	
	57+62.22	60.00 LT	1		194+40.00	50.00 LT	1	
	57+62.22	70.00 LT	1		194+65.71	50.00 RT	1	
	57+62.22	50.00 RT	1		194+65.71	50.00 LT	1	
	57+62.22	40.00 RT	1		195+63.78	50.00 LT	1	
	58+26.04	70.00 LT	1		196+00.57	50.00 RT	1	
	58+26.04	50.00 RT	1		196+30.52	185.30 LT	1	
	60+00.00	70.00 LT	1		197+08.83	50.00 RT	1	
	60+00.00	60.00 LT	1		203+00.00	50.00 LT	1	
	62+57.00	60.00 LT	1		203+60.43	40.18 RT	1	
	65+55.70	60.00 LT	1		204+10.06	50.00 LT	1	
	66+07.94	60.00 LT	1		205+64.18	18.51 LT	1	
	66+28.35	60.00 LT	1		205+80.43	50.00 LT	1	
	67+29.53	216.69 LT	1		205+91.69	50.00 LT	1	
	70+37.92	40.00 LT	1		205+91.83	46.10 LT	1	
	70+91.41	40.00 LT	1	FAP 310	232+28.77	150.00 RT	1	
DOMINO LANE SR	100+97.03	50.00 RT	1		233+55.75	140.00 LT	1	
	101+95.70	50.00 RT	1		237+85.00	140.00 LT	1	
	102+34.34	50.00 RT	1		237+85.08	190.00 LT	1	
	103+28.52	50.00 RT	1		241+60.00	150.00 RT	1	
	103+60.45	50.00 RT	1		241+60.00	190.00 RT	1	
	105+30.77	50.00 RT	1		242+88.48	190.00 RT	1	
	105+32.37	40.00 RT	1		249+30.26	165.00 LT	1	
	106+82.26	40.00 RT	1		250+31.71	120.00 LT	1	
	107+28.52	40.00 RT	1		254+28.98	120.00 LT	1	
	108+14.47	40.00 RT	1		256+57.51	158.04 RT	1	
	108+14.61	63.73 RT	1		256+64.42	130.00 RT	1	
	108+35.46	74.53 RT	1		257+56.78	130.00 RT	1	
	109+57.03	127.68 RT	1		259+73.39	120.00 LT	1	
	109+97.63	30.00 RT	1		260+50.00	120.00 LT	1	
	111+40.89	30.00 RT	1		260+60.00	130.00 RT	1	
	111+51.38	30.00 RT	1		262+39.46	156.48 LT	1	
	112+66.14	30.00 RT	1		262+42.06	130.00 RT	1	
	113+73.92	30.00 RT	1		263+69.50	153.00 LT	1	
	114+42.43	30.00 LT	1		264+35.65	130.00 RT	1	
	114+42.43	30.00 RT	1		265+00.00	130.00 RT	1	
INGHAM	145+07.13	32.76 LT	1		265+20.00	120.00 LT	1	
	145+24.94	46.67 RT	1		265+70.38	225.00 RT	1	
	147+47.58	63.35 LT	1		266+22.17	120.00 LT	1	
	147+53.46	56.51 RT	1		266+46.93	170.00 LT	1	
	152+10.43	56.26 RT	1		268+00.00	225.00 RT	1	
	153+74.24	57.73 RT	1		268+33.02	521.23 LT	1	
	155+12.35	66.04 LT	1		269+00.00	156.60 RT	1	
	157+32.45	15.93 RT	1		269+39.54	111.58 RT	1	
	157+44.45	61.04 RT	1		269+39.57	156.58 RT	1	
INGHAM ACCESS	170+24.29	148.18 RT	1		272+45.15	164.00 LT	1	
	172+41.01	53.35 RT	1		272+47.00	308.46 LT	1	
	172+52.25	135.51 RT	1		273+00.00	111.53 RT	1	
	175+21.38	38.11 RT	1		274+00.00	145.00 RT	1	
PROJECT TOTAL					158			

PERMANENT SURVEY MARKERS

STATION	TYPE I (EACH)	TYPE II (EACH)
FAP 310		
POT 248+30.49	CL	1
POT 254+28.78	CL	1
POT 262+23.15	CL	1
PC 264+35.65	CL	1
PI 271+78.01		1
PT 279+14.84	CL	1
POT 288+00.00	CL	1
POT 300+00.00	CL	1
POT 316+00.00	CL	1
PC 333+19.67	CL	1
INGHAM ROAD		
POT 145+70.00	CL	1
POT 146+26.65	CL	1
PC 147+98.95	CL	1
PT 149+01.05	CL	1
POT 158+00.00	CL	1
STAGECOACH ROAD		
PC 194+63.78	CL	1
PT 196+00.57	CL	1
PC 197+08.83	CL	1
PT 198+41.48	CL	1
PC 204+10.06	CL	1
PRC 205+80.43	CL	1
PT 205+80.43	CL	1
PROJECT TOTAL		
	3	19

ROW AND PROPERTY CORNERS

LOCATION	STATION	OFFSET	EACH
DOMINO LANE	103+28.52	50.00' RT	1
DOMINO LANE	112+66.14	30.00' RT	1
FAP 310	232+28.77	150.00' RT	1
FAP 310	237+85.00	140.00' LT	1
FAP 310	237+85.08	190.00' LT	1
FAP 310	241+60.00	150.00' RT	1
FAP 310	256+57.51	157.04' RT	1
FAP 310	256+64.42	130.00' RT	1
FAP 310	279+00.69	163.65' LT	1
FAP 310	300+75.00	123.31' RT	1
FAP 310	302+00.00	153.00' RT	1
FAP 310	309+50.00	120.00' LT	1
FAP 310	310+02.73	138.78' RT	1
FAP 310	312+35.19	170.00' LT	1
FAP 310	312+35.19	170.00' LT	1
FAP 310	322+96.49	259.61' LT	1
FAP 310	322+96.49	259.61' LT	1
INGHAM ACCESS ROAD	174+92.09	42.19' LT	1
QUARRY ROAD	68+10.54	40.00' LT	1
QUARRY ROAD	70+91.41	40.00' LT	1
QUARRY ROAD	71+60.19	15.00' RT	1
TOTAL			
			21

SECTION CORNERS

STATION	OFFSET	EACH
NW Corner NE 1/4, SW 1/4 Sec. 32-7-10	7.20' LT INGHAM LANE	1
SE Corner NW 1/4, SW 1/4 Sec. 32-7-10	19.42' LT QUARRY ROAD	1
SE Corner NE 1/4, NE 1/4 Sec. 31-7-10	163.65' LT FAP 310	1
TOTAL		
		3

DATE: 03/09/2010
 TIME: 10:00 AM
 DRAWN BY: RLK
 CHECKED BY: CAS

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTIES

DRAWN BY: RLK

CHECKED BY: CAS

REVISED 03/09/2010 (BLA)

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	40
STA.		TO STA.		

EXISTING CONDITIONS:
CONTRACT NO. 76318

STORM SEWERS

STATION	STATION	OFFSET	PRECAST REINFORCED CONCRETE FLARED END SECTION (EACH)	STORM SEWERS CLASS A (FEET)		MAN TA 4D M IN STD 604101 (EACH)	MAN TA 5' DIA T1F OL (EACH)	DR STR T1A W/1 T20F&G (EACH)	INLET BOX STD 542521 (EACH)
				TYPE 1	TYPE 2				
230+89		LT	12"	18"	12"			1	
230+89		CL				1			
230+89	232+00	CL			107				
232+00		RT	1		88	1			
243+02	243+21	LT		19					1
325+47	325+98	LT					1		
326+00	328+00	LT							
SCHEDULE TOTAL			1	19	251	2	1	1	1

TRENCH BACKFILL

STATION	TRENCH BACKFILL CU YD
MAINLINE	
230+89	7.8
232+00	36.6
232+26	17.6
235+50	12.4
249+30	14.8
261+00	14.2
272+00	14.2
278+83 LT	8.0
280+00	9.9
286+50	12.4
295+65 SB	81.3
299+25 SB	33.0
307+25	11.1
309+33 LT	9.2
323+00	42.9
326+66	136.0
333+46	8.0
QUARRY ROAD	
57+72	22.9
63+00	36.8
DOMINO SERVICE	
102+00	3.9
CONNECTOR	
52+71	7.6
50+89	17.3
JUN ACCESS	
21+50	5.4
23+50	2.9
INGHAM	
145+70	1.4
148+85	83.7
151+15	15.1
154+90	32.3
STAGECOACH	
203+50 LT	5.6
PROJECT TOTAL	704.0

CHANNEL EXCAVATION

LOCATION	1	2	3	4
	CHANNEL EXCAVATION (CU YD)	*EXCAVATION TO BE USED IN EMBANKMENT (ADJUSTED FOR 25% SHRINKAGE) (CU YD)	*EMBANKMENT (FILL) (CU YD)	EARTHWORK BALANCE WASTE (+) OR FURNISHED EXCAVATION (-) (COL 2 - COL 3) (CU YD)
STAGE 1A				
LITTLE PIASA	194	194	0	194
PIASA	2,002	1,502	206	1,296
SUBTOTAL STAGE 1	2,196	1,696	206	1,490
STAGE 2				
PIASA	1,397	1,048	5	1,043
SUBTOTAL STAGE 2	1,397	1,048	5	1,043
PROJECT TOTAL	3,593	2,744	211	2,533

ITEMS MARKED WITH AN ASTERISK (*) ARE FOR INFORMATION ONLY.
EXCESS EXCAVATION SHALL BE DISPOSED OF OFF R.O.W. IN ACCORDANCE WITH ARTICLE 202.03

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON AND JERSEY COUNTIES

REVISIONS	
NAME	DATE

DRAWN BY: RLK

CHECKED BY: CAS

CONTRACT NO. 76318

STATION TO STATION		LANE	THERMOPLASTIC PAVEMENT MARKING								POLYUREA PAVEMENT MARKING TYPE I				
			4" WHITE - LINE		4" YELLOW - LINE		8" WHITE - LINE	12" WHITE - LINE	12" YELLOW - LINE	24" WHITE - LINE	4" WHITE - LINE		6" WHITE -LINE		
			SOLID	SKIP-DASH	SOLID	SKIP-DASH					SOLID	SKIP-DASH	SOLID	SKIP-DASH	SOLID
FEET											SQ FT	FEET			
FAP 310 (US 67) MAINLINE															
213+40.0	243+00.5	NB	2938.2	735.8	2948.4										
243+00.5	248+30.5	NB	1028.0	132.5	484.1						62.4				
248+30.5	256+58.6	NB	855.2	207.0	801.0										
256+58.6	262+23.2	NB	996.0	141.2	509.9	141.9	70.0				62.4				
262+23.2	282+42.5	NB	2086.7	504.8	1957.1										
282+42.5	288+00.0	NB	1051.8	139.4	503.5						62.4				
288+00.0	317+05.7	NB	2905.7	726.4	2852.8										
317+05.7	317+46.2	NB										40.5	10.1	40.5	
317+46.2	319+91.6	NB										245.4	61.4	245.7	15.4
319+91.6	322+76.2	NB	525.6	71.2	238.4						31.2				
322+76.2	327+60.0	NB	483.8	121.0	456.7										
327+60.0	331+32.5	NB	372.5		372.5										
331+32.5	342+48.0	NB	1115.5		1126.2										
217+50.0	248+30.5	SB	2959.9	770.2	2905.7							147.7	36.4	147.7	
248+30.5	253+60.4	SB	1078.0	132.5	484.1						62.4				
253+60.4	262+23.2	SB	919.9	215.7	797.9										
262+23.2	267+87.0	SB	986.1	141.0	507.0	150.1	73.8				62.4				
267+87.0	288+00.0	SB	2012.0	503.3	1950.3										
288+00.0	293+57.5	SB	1038.7	139.4	503.5						62.4				
293+57.5	322+76.2	SB	2645.5	658.2	2605.7							285.9	71.5	285.9	
322+76.2	337+00.0	SB	1692.8	356.0	1658.8										
337+00.0	339+65.0	SB	265.0		265.0	66.3									
339+65.0	342+48.0	SB	290.2		292.0										
CHEVRONS		NB & SB							47.1						
EXISTING US 67															
DOUBLE YELLOW CENTERLINE		CL													
CONNECTOR ROAD															
50+69	51+20	-			102.0										
	51+21.0	-								33.3					
	52+39.0	-								33.3					
52+38	52+91	-			106.0										
NORTH INGHAM ROAD															
145+70.0	149+31.4	-	607.3		200.0	70.0									
	149+32.4	-								18.2					
	150+66.5	-								19.2					
150+67.5	157+25.0	-	1009.1		200.0	140.0									
STAGECOACH ROAD															
194+66.1	199+33.0	-	851.8		200.0	90.0									
	199+34.0	-								50.3					
	200+66.1	-								50.3					
200+67.1	205+80.5	-	934.7		200.0	100.0									
TOTALS			31650.0	5695.6	25228.6	466.3	292.0	143.8	47.1	204.6	405.6	719.5	179.4	719.8	15.4
USE				63041			292	191		205	406		1619		16

DELINEATORS				
STATION	SIDE*	SINGLE WHITE	DOUBLE YELLOW EACH	TRIPLE YELLOW
214+50	RT	1		
216+00	RT	1		
217+50	RT	1		
219+00	RT	1		
220+50	RT	1		
222+00	RT	1		
224+00	RT	1		
226+00	RT	1		
229+00	RT	1		
233+00	RT	1		
237+00	RT	1		
241+00	RT	1		
245+00	RT	1		
249+00	RT	1		
253+00	RT	1		
257+00	RT	1		
260+00	RT	1		
265+00	RT	1		
269+00	RT	1		
273+00	RT	1		
277+00	RT	1		
281+00	RT	1		
285+00	RT	1		
289+00	RT	1		
293+00	RT	1		
297+00	RT	1		
301+00	RT	1		
305+00	RT	1		
309+00	RT	1		

DELINEATORS				
STATION	SIDE*	SINGLE WHITE	DOUBLE YELLOW EACH	TRIPLE YELLOW
313+00	RT	1		
316+00	RT	1		
322+00	RT	1		
326+00	RT	1		
330+00	RT	1		
SOUTHBOUND FAP 310 MAINLINE				
214+50	RT	1		
216+00	RT	1		
217+50	RT	1		
219+00	RT	1		
220+50	RT	1		
222+00	RT	1		
224+00	RT	1		
226+00	RT	1		
229+00	RT	1		
233+00	RT	1		
237+00	RT	1		
241+00	RT	1		
245+00	RT	1		
249+00	RT	1		
253+00	RT	1		
257+00	RT	1		
260+00	RT	1		
265+00	RT	1		
269+00	RT	1		
273+00	RT	1		
277+00	RT	1		
281+00	RT	1		
285+00	RT	1		

DELINEATORS				
STATION	SIDE*	SINGLE WHITE	DOUBLE YELLOW EACH	TRIPLE YELLOW
289+00	RT	1		
293+00	RT	1		
297+00	RT	1		
301+00	RT	1		
305+00	RT	1		
309+00	RT	1		
313+00	RT	1		
316+00	RT	1		
322+00	RT	1		
326+00	RT	1		
330+00	RT	1		
334+00	RT	1		
CONNECTOR ROAD INTERSECTION				
NORTHBOUND LEFT TURN LANE				
239+35	LT			1
247+35	LT		1	
SOUTHBOUND LEFT TURN LANE				
249+26	LT		1	
257+26	LT			1
NORTH INGHAM ROAD INTERSECTION				
NORTHBOUND LEFT TURN LANE				
253+21	LT			1
261+21	LT		1	
SOUTHBOUND LEFT TURN LANE				
263+16	LT		1	
271+16	LT			1
STAGECOACH ROAD INTERSECTION				
NORTHBOUND LEFT TURN LANE				
278+84	LT			1

DELINEATORS				
STATION	SIDE*	SINGLE WHITE	DOUBLE YELLOW EACH	TRIPLE YELLOW
NORTHBOUND LEFT TURN LANE (CONT.)				
286+84	LT		1	
SOUTHBOUND LEFT TURN LANE				
289+22	LT		1	
297+22	LT			1
TRINITY LANE INTERSECTION				
NORTHBOUND LEFT TURN LANE				
313+81	LT			1
321+81	LT		1	
SOUTHBOUND LEFT TURN LANE				
323+71	LT		1	
331+71	LT			1
TOTALS		69	8	8
USE			85	

* REFERS TO SIDE RELATIVE TO THE DIRECTION OF TRAVEL, NOT RELATIVE TO THE BASELINE STATIONING

Hutchison Engineering Inc.
 Since 1945
 Jacksonville * Shorewood

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULES OF QUANTITIES
 FAP ROUTE 310
 SECTION 60-16-1&1B, 42-1&1B
 MADISON AND JERSEY COUNTIES

SCALE: VERT. HORIZ.
 DATE: FEBRUARY 25, 2010
 DRAWN BY TJD
 CHECKED BY BMB

CONTRACT NO. 76318

RAISED REFLECTIVE PAVEMENT MARKERS

STATION TO STATION	LOCATION	ONE-WAY MARKERS	
		CRYSTAL	AMBER
EACH			
FAP 310 (US 67) NORTHBOUND			
213+40.0	327+60.0	CENTERLINE	143
245+60.5	247+86.8	RIGHT TURN LANE	6
245+30.5	247+86.8	LEFT TURN LANE	7
258+88.3	261+45.4	RIGHT TURN LANE	7
259+40.4	261+54.3	LEFT TURN LANE	6
285+34.9	287+43.1	RIGHT TURN LANE	6
284+71.9	287+42.5	LEFT TURN LANE	7
319+76.2	322+32.5	LEFT TURN LANE	6
330+54.4	339+73.3	CROSSOVER	47
FAP 310 (US 67) SOUTHBOUND			
217+50.0	339+65.0	CENTERLINE	153
248+74.1	251+00.4	RIGHT TURN LANE	6
248+74.1	251+30.4	LEFT TURN LANE	7
263+06.1	265+59.3	RIGHT TURN LANE	7
262+92.3	265+03.0	LEFT TURN LANE	6
288+67.2	290+69.0	RIGHT TURN LANE	6
288+67.2	291+38.0	LEFT TURN LANE	7
323+20.7	325+77.0	LEFT TURN LANE	7
TOTALS			384
USE			434

DRUM AND CHEVRON SIGNS

LOCATION	DRUM AND CHEVRON SIGNS
	EACH
NB LANE TAPER	19
TOTAL	19
USE	19

SIGNING SCHEDULE

STATION	OFFSET FEET	SIDE	LANE	FACING	TYPE OF SIGN	SIGN CODE	SIGN SIZE INCH	SIGN PANEL - TYPE 1	SIGN PANEL - TYPE 2	OBJECT MARKER - TY 1 EACH	WOOD SIGN SUPPORT		
								SQ FT	SQ FT		SIZE	NUMBER	FT
FAP 310 (US 67) MAINLINE													
229+50	20.0	RT	NB	SOUTH	LITTLE PIASA CREEK	SIGN 001	48X12	4.0			4X4	1	14.5
23040	20.0	RT	SB	NORTH	LITTLE PIASA CREEK	SIGN 001	48X12	4.0			4X6	1	14.5
241+01	20.0	RT	NB	SOUTH	CROSSROAD	W2-1	48X48		16.0		4X6	1	20.0
244+82	18.0	RT	SB	NORTH	SPEED LIMIT 55 MPH	R2-1	36X48		12.0		4X6	1	18.0
245+30	20.0	LT	NB	SOUTH	LEFT TURN LANE	R3-I100L	24X24	4.0			4X6	1	15.5
				NORTH	WRONG WAY	R5-1a	36X24	6.0					
245+60	20.0	RT	NB	SOUTH	RIGHT TURN LANE	R3-I100R	24X24	4.0			4X6	1	15.5
				NORTH	WRONG WAY	R5-1a	36X24	6.0					
247+36	20.0	RT	NB	NORTH	DO NOT ENTER	R5-1	48X48		16.0		4X6	1	17.5
				NORTH	DO NOT ENTER	R5-1	48X48		16.0				
247+63	8.0	LT	CL	NORTH	KEEP RIGHT SYMBOL	R4-7	36X48		12.0		4X6	1	19.0
				NORTH	ONE WAY	R6-1R	36X12	3.0					
247+73	5.0	LT	CL	WEST	ONE WAY	R6-1R	36X12	3.0			4X4	1	16.5
248+88	5.0	RT	CL	EAST	ONE WAY	R6-1R	36X12	3.0			4X4	1	16.5
248+99	8.0	RT	CL	SOUTH	KEEP RIGHT SYMBOL	R4-7	36X48		12.0		4X6	1	19.0
				SOUTH	DO NOT ENTER	R5-1	48X48		16.0				
249+25	20.0	RT	SB	SOUTH	DO NOT ENTER	R5-1	48X48		16.0		4X6	1	17.5
				SOUTH	DO NOT ENTER	R5-1	48X48		16.0				
251+01	20.0	RT	NB	SOUTH	RIGHT TURN LANE	R3-I100R	24X24	4.0			4X6	1	15.5
				NORTH	WRONG WAY	R5-1a	36X24	6.0					
251+31	20.0	LT	NB	SOUTH	LEFT TURN LANE	R3-I100L	24X24	4.0			4X6	1	15.5
				NORTH	WRONG WAY	R5-1a	36X24	6.0					
251+79	18.0	RT	NB	SOUTH	SPEED LIMIT 55 MPH	R2-1	36X48		12.0		4X6	1	18.0
				SOUTH	CROSSROAD	W2-1	48X48		16.0				
254+99	20.0	RT	NB	SOUTH	ROAD NAME	W17-I100	48X12	4.0			4X6	1	21.5
				SOUTH	CROSSROAD	W2-1	48X48		16.0				
256+00	20.0	RT	SB	NORTH	CROSSROAD	W2-1	48X48		16.0		4X6	1	20.0
				NORTH	SPEED LIMIT 55 MPH	R2-1	36X48		12.0				
258+75	18.0	RT	SB	SOUTH	RIGHT TURN LANE	R3-I100R	24X24	4.0			4X6	1	15.5
				SOUTH	WRONG WAY	R5-1a	36X24	6.0					
258+89	20.0	RT	NB	SOUTH	LEFT TURN LANE	R3-I100L	24X24	4.0			4X6	1	15.5
				NORTH	WRONG WAY	R5-1a	36X24	6.0					
259+41	20.0	LT	NB	NORTH	ROUTE	M1-4	48X48		16.0		4X6	1	19.5
				NORTH	DIRECTION	M3-3	30X15	3.2					
260+10	18.0	RT	SB	NORTH	DO NOT ENTER	R5-1	48X48		16.0		4X6	1	17.5
				NORTH	DO NOT ENTER	R5-1	48X48		16.0				
260+89	20.0	RT	NB	NORTH	KEEP RIGHT SYMBOL	R4-7	36X48		12.0		4X6	1	19.0
				NORTH	ONE WAY	R6-1R	36X12	3.0					
261+31	5.0	LT	CL	NORTH	ONE WAY	R6-1R	36X12	3.0			4X4	1	16.5
261+42	4.0	RT	CL	WEST	ONE WAY	R6-1R	36X12	3.0			4X4	1	16.5
263+07	4.0	LT	CL	EAST	ONE WAY	R6-1R	36X12	3.0			4X4	1	16.5
263+18	5.0	RT	CL	SOUTH	KEEP RIGHT SYMBOL	R4-7	36X48		12.0		4X6	1	19.0
263+58	20.0	RT	SB	SOUTH	DO NOT ENTER	R5-1	48X48		16.0		4X6	1	17.5
				SOUTH	DO NOT ENTER	R5-1	48X48		16.0				
264+38	18.0	RT	NB	SOUTH	ROUTE	M1-4	48X48		16.0		4X6	1	19.5
				SOUTH	DIRECTION	M3-3	30X15	3.2					
265+05	20.0	RT	NB	SOUTH	LEFT TURN LANE	R3-I100L	24X24	4.0			4X6	1	15.5
				NORTH	WRONG WAY	R5-1a	36X24	6.0					
265+59	20.0	LT	NB	SOUTH	RIGHT TURN LANE	R3-I100R	24X24	4.0			4X6	1	15.5
				NORTH	WRONG WAY	R5-1a	36X24	6.0					
265+72	18.0	RT	NB	SOUTH	SPEED LIMIT 55 MPH	R2-1	36X48		12.0		4X6	1	18.0
				SOUTH	CROSSROAD	W2-1	48X48		16.0				
270+00	20.0	RT	SB	NORTH	ROAD NAME	W17-I100	48X12	4.0			4X6	1	21.5
				NORTH	CROSSROAD	W2-1	48X48		16.0				
280+49	20.0	RT	NB	SOUTH	ROAD NAME	W17-I100	48X12	4.0			4X6	1	21.5
				SOUTH	CROSSROAD	W2-1	48X48		16.0				
284+51	18.0	RT	SB	NORTH	SPEED LIMIT 55 MPH	R2-1	36X48		12.0		4X6	1	18.0
				SOUTH	LEFT TURN LANE	R3-I100L	24X24	4.0					
284+73	20.0	LT	NB	NORTH	WRONG WAY	R5-1a	36X24	6.0			4X6	1	15.5
				SOUTH	RIGHT TURN LANE	R3-I100R	24X24	4.0					
285+35	20.0	RT	NB	SOUTH	WRONG WAY	R5-1a	36X24	6.0			4X6	1	15.5
				NORTH	DO NOT ENTER	R5-1	48X48		16.0				
286+76	20.0	RT	NB	NORTH	DO NOT ENTER	R5-1	48X48		16.0		4X6	1	17.5
				NORTH	DO NOT ENTER	R5-1	48X48		16.0				
287+15	18.0	LT	CL	NORTH	KEEP RIGHT SYMBOL	R4-7	36X48		12.0		4X6	1	19.0
				NORTH	ONE WAY	R6-1R	36X12	3.0					
287+25	10.0	LT	CL	WEST	ONE WAY	R6-1R	36X12	3.0			4X4	1	16.5
288+75	10.0	RT	CL	EAST	ONE WAY	R6-1R	36X12	3.0			4X4	1	16.5
288+85	18.0	RT	CL	SOUTH	KEEP RIGHT SYMBOL	R4-7	36X48		12.0		4X6	1	19.0
				SOUTH	DO NOT ENTER	R5-1	48X48		16.0				
289+22	18.0	RT	SB	SOUTH	DO NOT ENTER	R5-1	48X48		16.0		4X6	1	17.5
				SOUTH	DO NOT ENTER	R5-1	48X48		16.0				
290+59	20.0	RT	NB	SOUTH	RIGHT TURN LANE	R3-I100R	24X24	4.0			4X6	1	15.5
				NORTH	WRONG WAY	R5-1a	36X24	6.0					
291+50	20.0	LT	NB	SOUTH	LEFT TURN LANE	R3-I100L	24X24	4.0			4X6	1	15.5
				NORTH	WRONG WAY	R5-1a	36X24	6.0					
291+50	18.0	RT	NB	SOUTH	SPEED LIMIT 55 MPH	R2-1	36X48		12.0		4X6	1	18.0
				SOUTH	CROSSROAD	W2-1	48X48		16.0				
294+99	20.0	RT	SB	NORTH	ROAD NAME	W17-I100	48X12	4.0			4X6	1	21.5
				NORTH	CROSSROAD	W2-1	48X48		16.0				

(CONTINUED ON NEXT PAGE)

NOTE: OFFSETS ARE FROM THE EDGE OF PAVEMENT OF THE MAINLINE FOR THE THROUGH LANES IN THE DIRECTION OF TRAVEL.
 OFFSETS FOR SIGNS WITHIN THE MEDIAN NOSE AND SIDE ROAD SIGNS ARE FROM THE CENTERLINE WHEN LOOKING UP STATION.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULES OF QUANTITIES
 FAP ROUTE 310
 SECTION 60-16-1&1B, 42-1&1B
 MADISON AND JERSEY COUNTIES
 SCALE: VERT. DRAWN BY TJD
 HORIZ. CHECKED BY BMB
 DATE: FEBRUARY 25, 2010

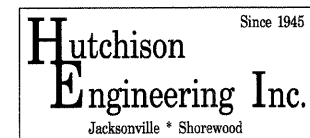
CONTRACT NO. 76318

SIGNING SCHEDULE (CONT.)

STATION	OFFSET FEET	SIDE	LANE	FACING	TYPE OF SIGN	SIGN CODE	SIGN PANEL - TYPE 1		SIGN PANEL - TYPE 2	OBJECT MARKER - TY 1	WOOD SIGN SUPPORT			
							SIGN SIZE INCH	SQ FT			SIZE	NUMBER	FT	
FAP 310 (US 67) MAINLINE (CONT.)														
314+50	20.0	RT	NB	SOUTH	SIDE ROAD SYMBOL	W2-2	48X48		16.0		4X6	1	21.5	
					ROAD NAME	W17-I100	48X12	4.0						
316+00	18.0	RT	SB	NORTH	SPEED LIMIT 55 MPH	R2-1	36X48		12.0		4X6	1	18.0	
316+22	18.0	RT	NB	SOUTH	EXPRESSWAY ENDS 1500 FT	W8-I103	48X48		16.0		4X6	1	21.0	
317+30	20.0	RT	NB	SOUTH	PIASA CREEK	SIGN 002	48X12	4.0			4X4	1	14.5	
319+65	20.0	RT	SB	NORTH	PIASA CREEK	SIGN 002	48X12	4.0			4X6	1	14.5	
319+78	20.0	RT	NB	NORTH	WRONG WAY	R5-1a	36X24	6.0			4X6	1	18.0	
				SOUTH	SPEED LIMIT 55 MPH	R2-1	36X48		12.0					
		LT	NB	NORTH	WRONG WAY	R5-1a	36X24	6.0				4X6	1	18.0
				SOUTH	LEFT TURN LANE	R3-1100L	24X24	4.0						
321+77	20.0	RT	NB	NORTH	DO NOT ENTER	R5-1	48X48		16.0		4X6	1	18.0	
				SOUTH	LANE ENDS MERGE RIGHT	W9-2(R)	48X48		16.0					
		LT	NB	NORTH	DO NOT ENTER	R5-1	48X48		16.0			4X6	1	18.0
				SOUTH	LANE ENDS MERGE RIGHT	W9-2(R)	48X48		16.0					
322+05	12.0	LT	CL	NORTH	KEEP RIGHT SYMBOL	R4-7	36X48		12.0		4X6	1	19.0	
322+15	4.0	LT	CL	WEST	ONE WAY	R6-1R	36X12	3.0			4X4	1	16.5	
323+44	10.0	RT	CL	SOUTH	KEEP RIGHT SYMBOL	R4-7	36X48		12.0		4X6	1	19.0	
323+71	20.0	RT	SB	SOUTH	DO NOT ENTER	R5-1	48X48		16.0		4X6	1	17.5	
				SOUTH	DO NOT ENTER	R5-1	48X48		16.0					
325+74	20.0	RT	SB	SOUTH	WRONG WAY	R5-1a	36X24	6.0			4X6	1	15.5	
				SOUTH	WRONG WAY	R5-1a	36X24	6.0						
326+33	18.0	RT	NB	SOUTH	PAVEMENT WIDTH TRANSITION	W4-2	48X48		16.0		4X6	1	20.0	
				LT	NB	SOUTH	PAVEMENT WIDTH TRANSITION	W4-2	48X48		16.0		4X6	1
330+00	18.0	RT	SB	NORTH	SIDE ROAD SYMBOL	W2-2	48X48		16.0		4X6	1	21.5	
				NORTH	ROAD NAME	W17-I100	48X12	4.0						
336+00	18.0	RT	NB	SOUTH	DIVIDED HIGHWAY ENDS	W6-2	48X48		16.0		4X6	1	20.0	
340+30	2.0	LT	NB	NORTH	KEEP RIGHT	R4-7b	36X48		12.0		4X6	1	19.0	
EXISTING US 67														
342+48	16.0	RT	NB	SOUTH	TWO WAY TRAFFIC	W6-3	48X48		16.0		4X6	1	20.0	
500' BEYOND TAPER	16.0	RT	SB	NORTH	REVERSE CURVE	W1-4R	48X48		16.0		4X6	1	20.0	
1000' BEYOND TAPER	16.0	RT	SB	NORTH	DIVIDED HIGHWAY BEGINS	W6-1	48X48		16.0		4X6	1	20.0	
PRIVATE ROAD - WEST OF INTERSECTION OF CONNECTOR ROAD AND QUARRY ROAD														
OFF END OF ROAD				WEST	ROAD CLOSED	R11-2	48X30		10.0		4X6	1	11.5	
OFF END OF ROAD				WEST	OBJECT MARKER - TYPE 1		18X18			1	4X4	1	11.5	
OFF END OF ROAD				WEST	OBJECT MARKER - TYPE 1		18X18			1	4X4	1	11.5	
500 FT WEST OF CLOSURE				WEST	ROAD ENDS 500 FEET	W8-I108	36X36	9.0			4X6	1	19.0	
1500 FT WEST OF CLOSURE				WEST	DEAD END	W14-1	36X36	9.0			4X6	1	18.5	
CONNECTOR ROAD														
51+06	40.0	RT	CL	WEST	STOP	R1-1	36X36	9.0			4X6	1	19.0	
					CROSS TRAFFIC DOES NOT STOP	R1-I100	30X18	3.8						
52+54	40.0	RT	CL	EAST	STOP	R1-1	36X36	9.0			4X6	1	19.0	
					CROSS TRAFFIC DOES NOT STOP	R1-I100	30X18	3.8						
QUARRY ROAD														
66+92	16.0	RT		SOUTH	STOP AHEAD	W3-1A	36X36	9.0			4X6	1	18.5	
70+15	16.0	RT		SOUTH	STOP	R1-1	36X36	9.0			4X6	1	17.5	
71+60		CL		SOUTH	OBJECT MARKER - TYPE 1		18X18			1	4X4	1	11.5	
				SOUTH	ROAD CLOSED	R11-2	48X30		10.0			4X6	1	11.5
				SOUTH	OBJECT MARKER - TYPE 1		18X18			1	4X4	1	11.5	
DOMINO LANE SERVICE ROAD														
99+00		CL		NORTH	OBJECT MARKER - TYPE 1		18X18			1	4X4	1	11.5	
				NORTH	ROAD CLOSED	R11-2	48X30		10.0			4X6	1	11.5
				NORTH	OBJECT MARKER - TYPE 1		18X18			1	4X4	1	11.5	
104+63	16.0	LT		NORTH	DEAD END	W14-1	36X36	9.0			4X6	1	18.5	
104+83	16.0	RT		SOUTH	STOP	R1-1	36X36	9.0			4X6	1	17.5	
105+74	16.0	RT		NORTH	STOP	R1-1	36X36	9.0			4X6	1	17.5	
106+00	16.0	RT		SOUTH	DEAD END	W14-1	36X36	9.0			4X6	1	18.5	
109+32	16.0	RT		SOUTH	ROAD ENDS 500 FEET	W8-I108	36X36	9.0			4X4	1	19.0	
114+32		CL		SOUTH	OBJECT MARKER - TYPE 1		18X18			1	4X4	1	11.5	
				SOUTH	ROAD CLOSED	R11-2	48X30		10.0			4X6	1	11.5
				SOUTH	OBJECT MARKER - TYPE 1		18X18			1	4X4	1	11.5	
NORTH INGHAM ROAD														
146+27	12.0	RT	CL	WEST	STOP AHEAD SYMBOL	W3-1A	36X36	9.0			4X6	1	18.5	
149+35	37.5	RT	CL	WEST	STOP	R1-1	36X36	9.0			4X6	1	19.0	
					CROSS TRAFFIC DOES NOT STOP	R1-I100	30X18	3.8						
150+64	37.5	RT	CL	EAST	STOP	R1-1	36X36	9.0			4X6	1	19.0	
					CROSS TRAFFIC DOES NOT STOP	R1-I100	30X18	3.8						
153+72	12.0	RT	CL	EAST	STOP AHEAD SYMBOL	W3-1A	36X36	9.0			4X6	1	18.5	
NORTH INGHAM ACCESS ROAD														
170+25	30.0	RT	CL	NORTH	STOP	R1-1	36X36	9.0			4X6	1	16.0	
STAGECOACH ROAD														
196+28	12.0	RT	CL	WEST	STOP AHEAD SYMBOL	W3-1A	36X36	9.0			4X6	1	18.5	
199+08	50.5	RT	CL	WEST	STOP	R1-1	36X36	9.0			4X6	1	19.0	
					CROSS TRAFFIC DOES NOT STOP	R1-I100	30X18	3.8						
200+92	50.5	RT	CL	EAST	STOP	R1-1	36X36	9.0			4X6	1	19.0	
					CROSS TRAFFIC DOES NOT STOP	R1-I100	30X18	3.8						
203+77	12.0	RT	CL	EAST	STOP AHEAD SYMBOL	W3-1A	36X36	9.0			4X6	1	18.5	
TRINITY LANE														
322+54 (MAINLINE)	72.0	LT	CL	WEST	STOP	R1-1	36X36	9.0			4X6	1	18.5	
					CROSS TRAFFIC DOES NOT STOP	R1-I100	30X18	3.8						
TOTAL							431.0	804.0	8		1878.5			
USE							431	804	8		1879			

NOTE: OFFSETS ARE FROM THE EDGE OF PAVEMENT OF THE MAINLINE FOR THE THROUGH LANES IN THE DIRECTION OF TRAVEL.
 OFFSETS FOR SIGNS WITHIN THE MEDIAN NOSE AND SIDE ROAD SIGNS ARE FROM THE CENTERLINE WHEN LOOKING UP STATION.

REVISIONS	
NAME	DATE



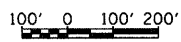
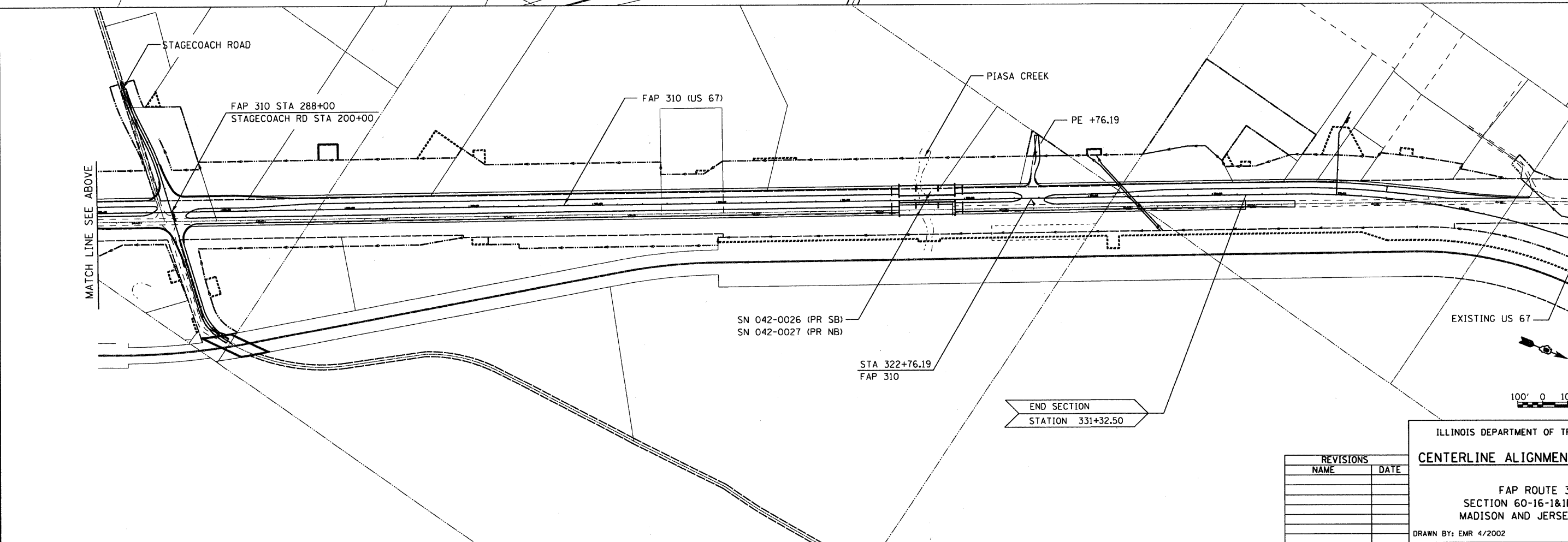
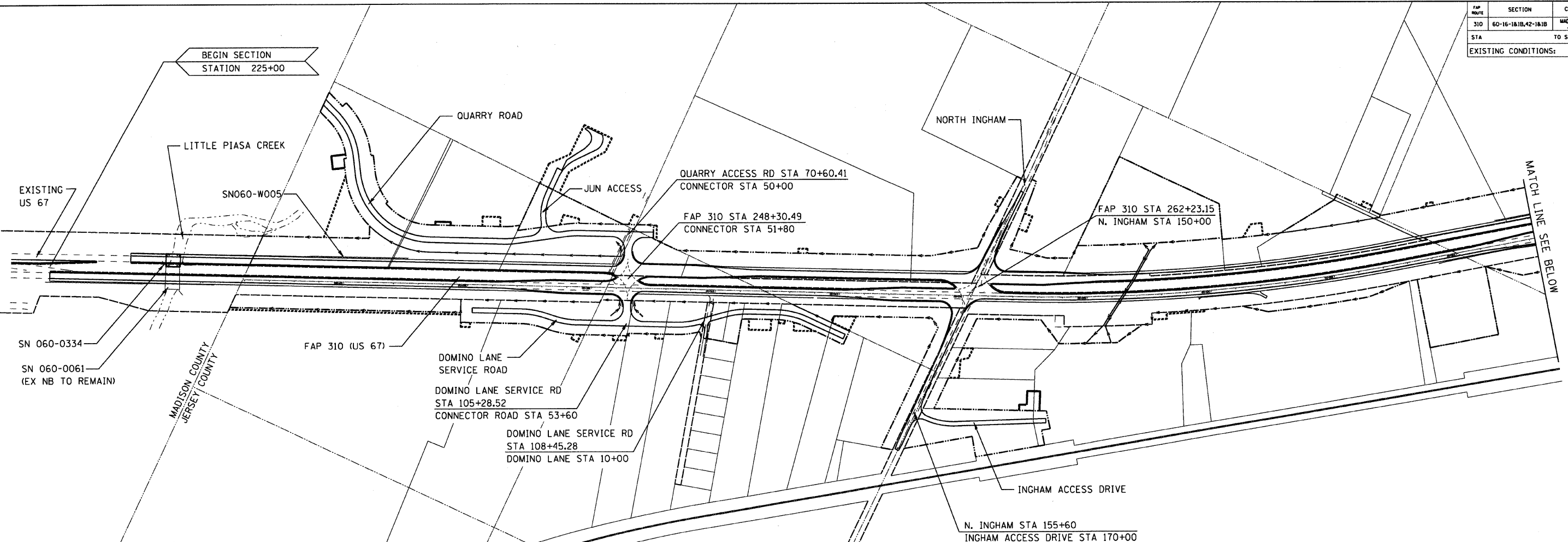
ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULES OF QUANTITIES
 FAP ROUTE 310
 SECTION 60-16-1&1B, 42-1&1B
 MADISON AND JERSEY COUNTIES
 SCALE: VERT. DRAWN BY TJD
 HORIZ. CHECKED BY BMB
 DATE FEBRUARY 25, 2010

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	44
STA	TO STA			

EXISTING CONDITIONS:

CONTRACT NO. 76318

PLAN	DATE	BY
DESIGNED		
PLOTTED		
ALIGNED		
CHECKED		
NO. _____		
NOTE BOOK		
NO. _____		
FILE NAME		

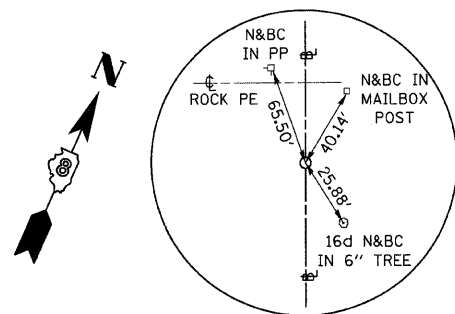


REVISIONS	
NAME	DATE

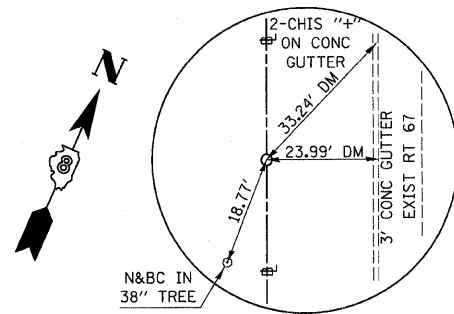
ILLINOIS DEPARTMENT OF TRANSPORTATION
CENTERLINE ALIGNMENT SCHEMATIC
 FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTY
 DRAWN BY: EMR 4/2002
 CHECKED BY: GLF

CONTRACT NO. 76318

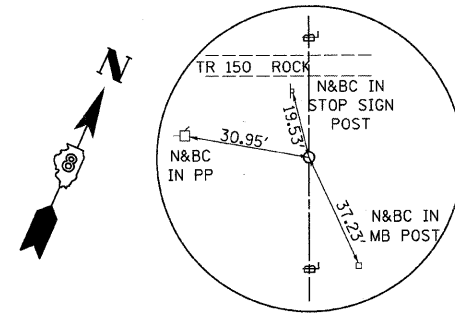
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NOTE BOOK NO.	PLotted	
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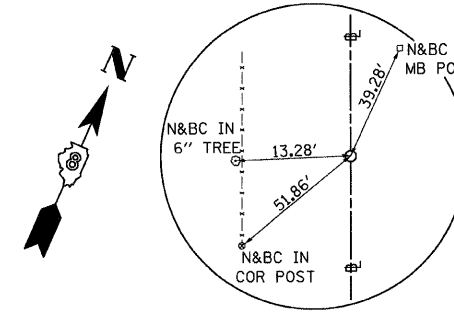
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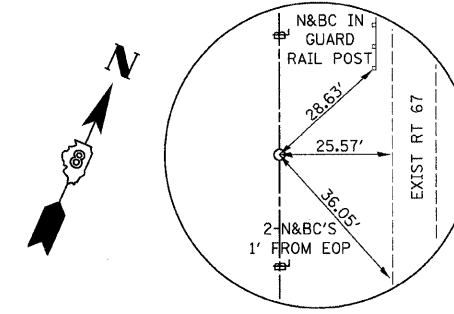
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 5/8" I. ROD W/CAP



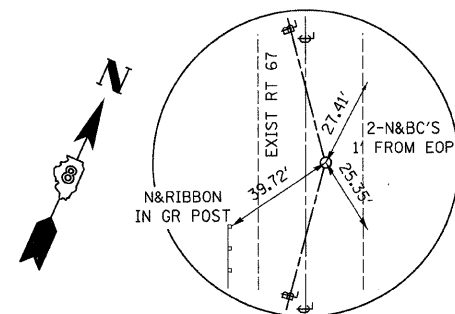
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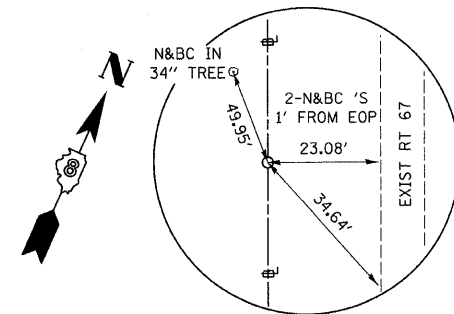
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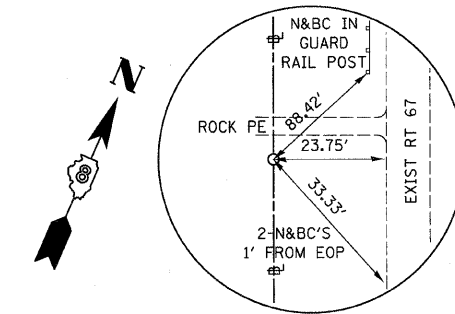
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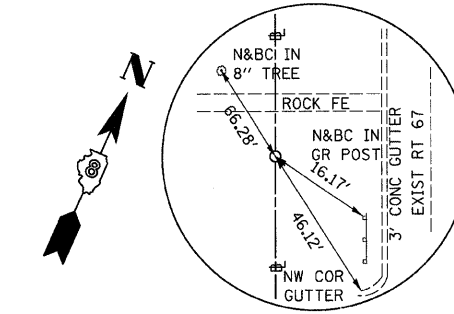
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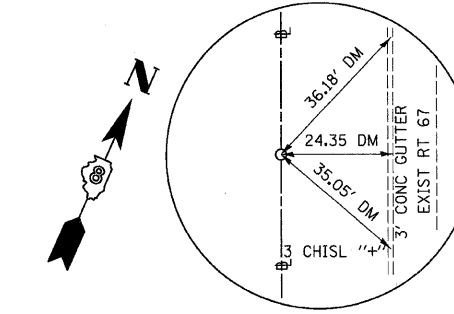
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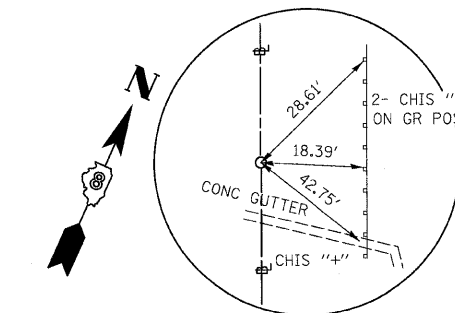
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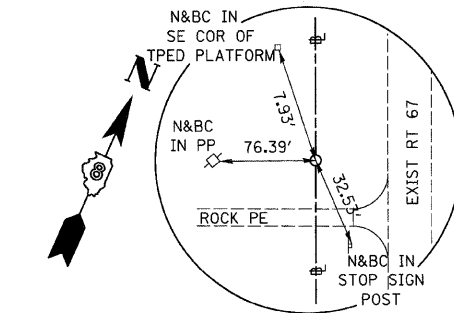
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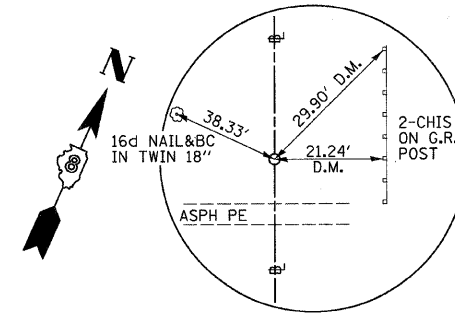
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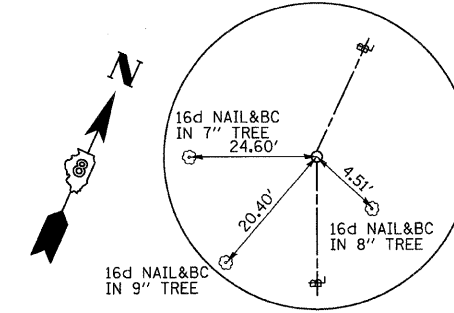
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 5/8" I. ROD W/CAP



PC 103 STA 333+19.67
 N - 858025.21 E - 2273053.11
 5/8" I. ROD W/CAP



PI 103 STA 344+84.06
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• BOUNTY
 • CON-SPEC
 • REF
 • REF

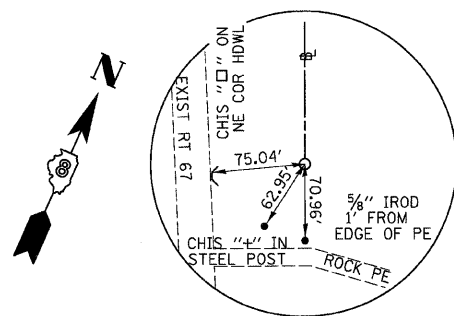
21041p10010

Hutchison Engineering Inc.
 Since 1945
 Jacksonville * Shorewood

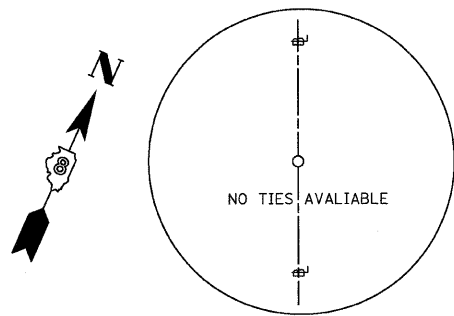
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 MAINLINE TIE POINTS
 FAP ROUTE 310
 SECTION 60-16-1&1B, 42-1&1B
 MADISON AND JERSEY COUNTIES
 SCALE: VERT. DRAWN BY JST
 HORIZ. CHECKED BY AWM
 DATE FEBRUARY 25, 2010
 PLOT DATE: *DATE-TIME*

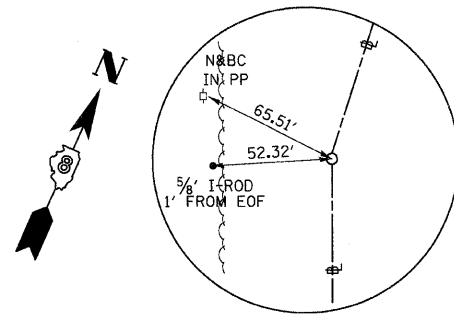
CONTRACT NO. 76318



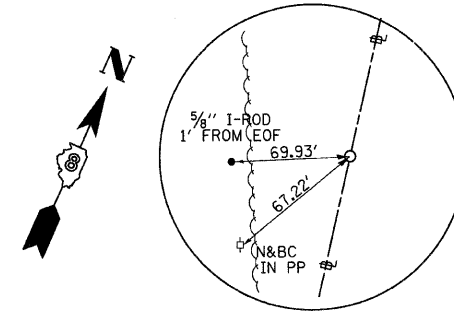
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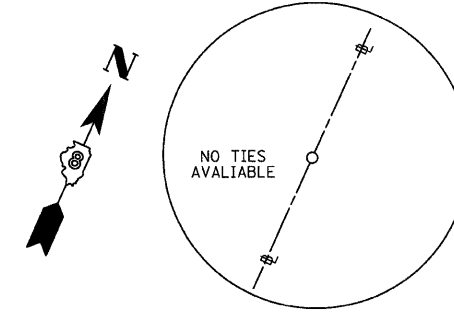
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PC STA 100+97.03
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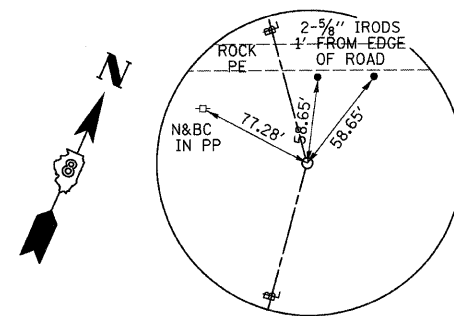
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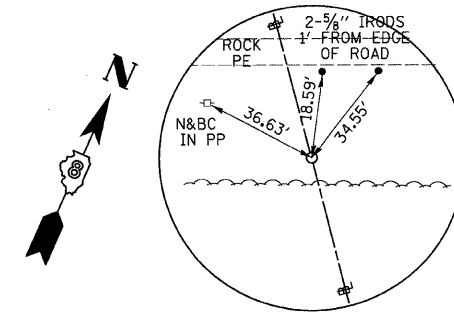
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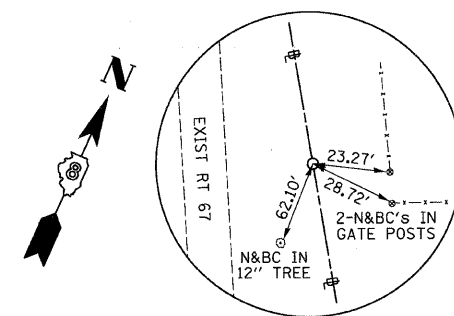
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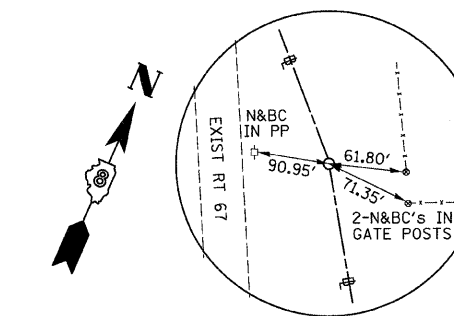
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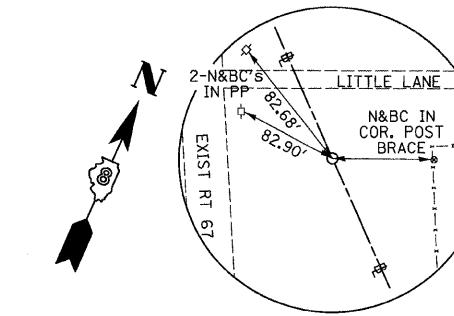
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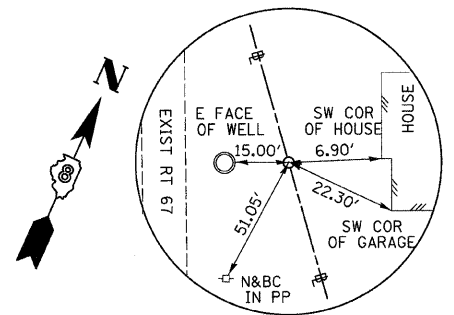
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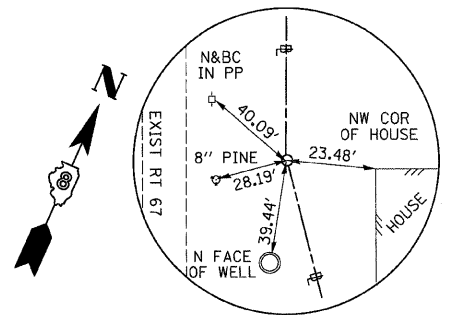
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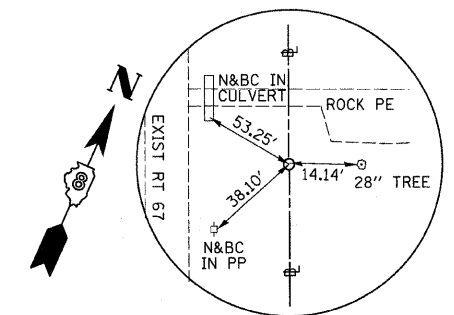
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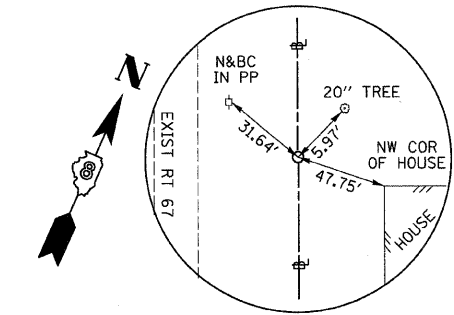
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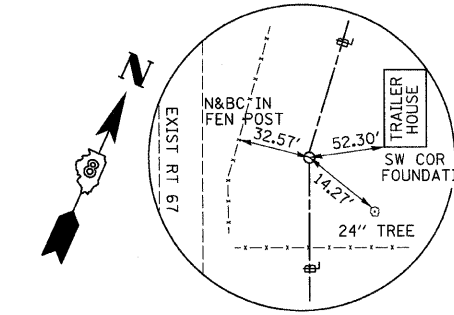
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PI STA 109+78.95
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5/8" I. ROD W/CAP



DOMINO LANE SERVICE ROAD
PT STA 110+27.91
N - 851383.19 E - 2277497.09
5/8" I. ROD W/CAP



DOMINO LANE SERVICE ROAD
PC STA 111+40.89
N - 851486.76 E - 2277426.59
5/8" I. ROD W/CAP



DOMINO LANE SERVICE ROAD
PI STA 112+04.44
N - 851545.02 E - 2277426.59
5/8" I. ROD W/CAP

DATE	
BY	
PLAN	
SURVEYED	
NOTE BOOK	
ALIGNED	
CHECKED	
FILED	
NO. 1	
NO. 2	
NO. 3	
NO. 4	
NO. 5	
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NO. 10	

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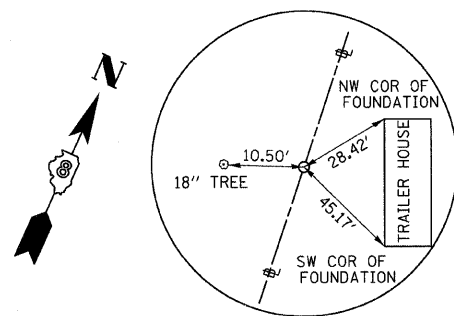
21041b1002a

Hutchison Since 1945
Engineering Inc.
Jacksonville * Shorewood

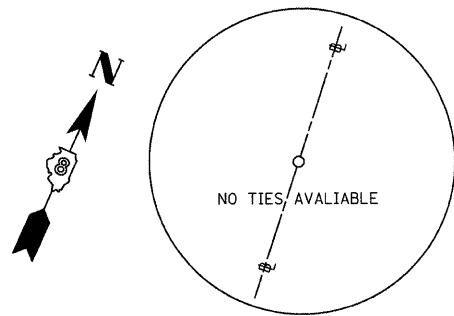
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SIDEROAD TIE POINTS
FAP ROUTE 310
SECTION 60-16-1&1B, 42-1&1B
MADISON AND JERSEY COUNTIES
SCALE: VERT. DRAWN BY JST
 HORIZ. CHECKED BY ANW
DATE FEBRUARY 25, 2010
PLOT DATE: DATE-TIME

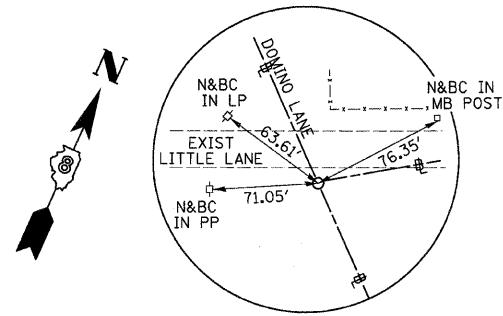
CONTRACT NO. 76318



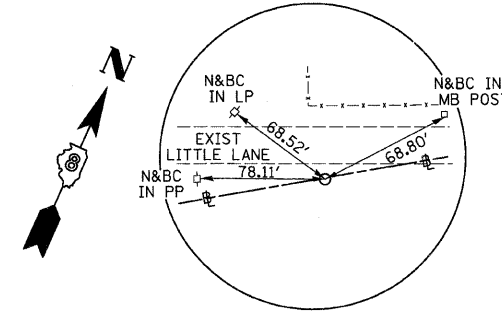
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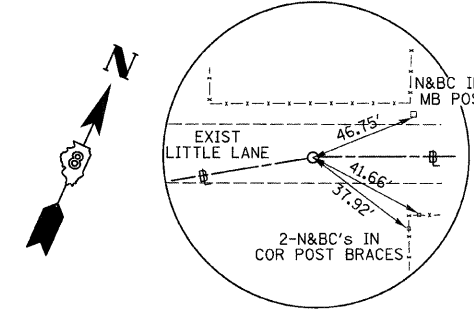
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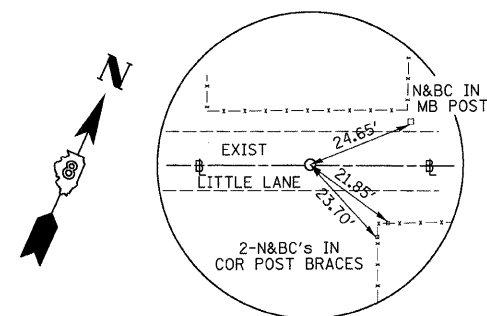
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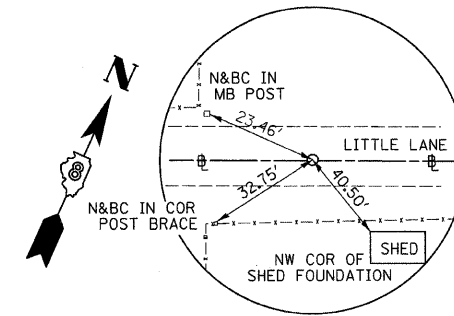
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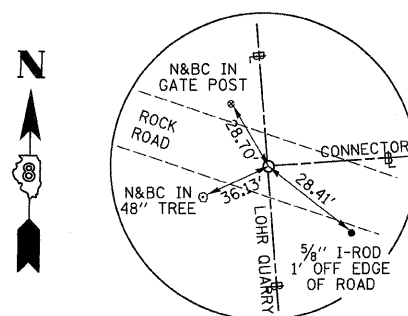
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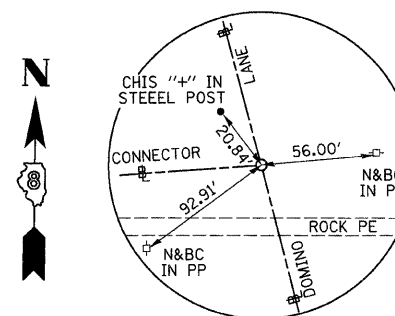
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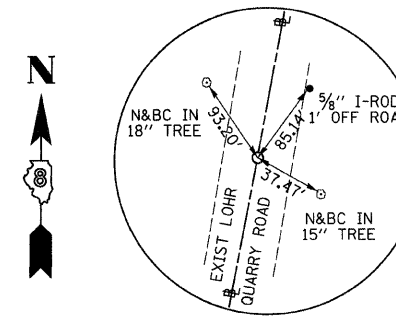
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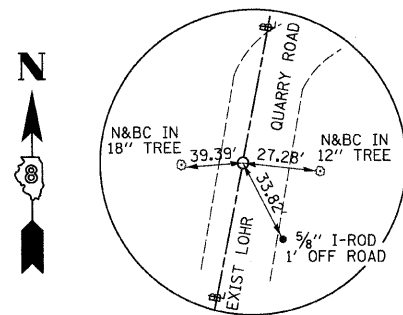
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BURIED 4"



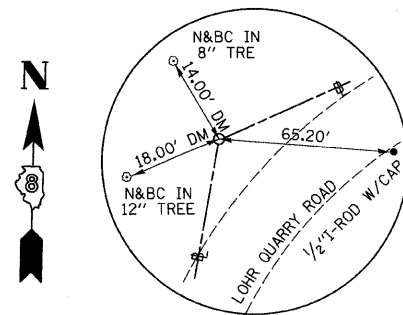
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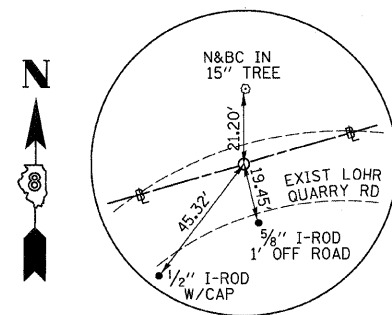
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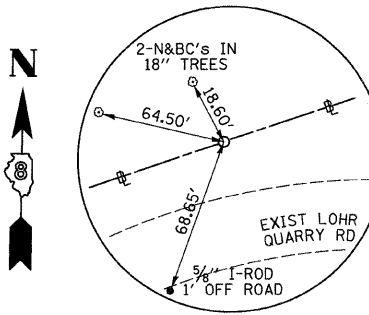
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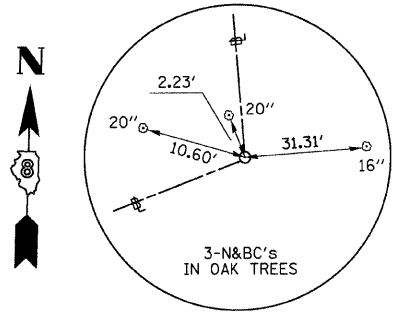
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5/8" I. ROD W/CAP



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N - 849685.20 E - 2277600.77
RR SPIKE W/PUNCH HOLE
BURIED 3"



QUARRY ROAD
PC STA 58+26.04
N - 849718.29 E - 2277655.33
5/8" I. ROD W/CAP



QUARRY ROAD
PI STA 60+88.24
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5/8" I. ROD W/CAP

PLAN	SURVEYED	DATE
NOTE BOOK	BY	
NO. OF WAYS CHECKED		
ROAD FILE NAME		

DATE-TIME
DATE-TIME
DATE-TIME
DATE-TIME

21041d1002c

Hutchison Since 1945
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Jacksonville * Shorewood

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SIDEROAD TIE POINTS
FAP ROUTE 310
SECTION 60-16-1&1B, 42-1&1B
MADISON AND JERSEY COUNTIES

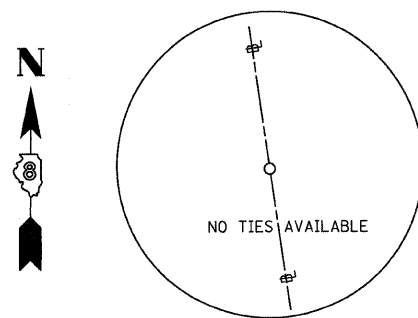
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HORIZ. CHECKED BY AWM
DATE FEBRUARY 25, 2010

PLOT DATE: *DATE-TIME*

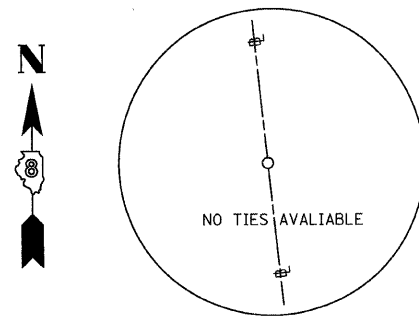
FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
310	60-16-1&1B, 42-1&1B	MADISON & JERSEY	481	48

CONTRACT NO. 76318

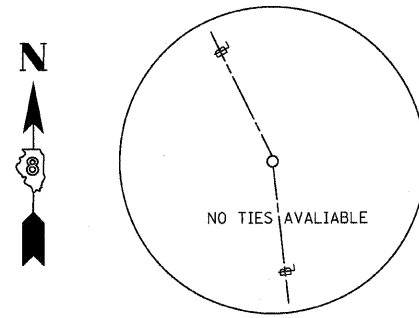
PLAN	SURVEYED	DATE
NOTE BOOK	BY	
NO.		



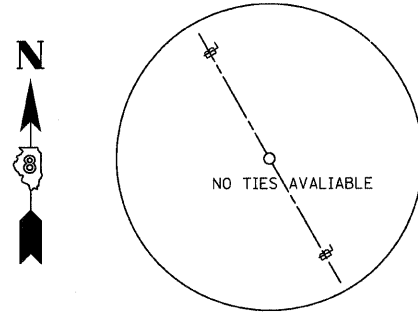
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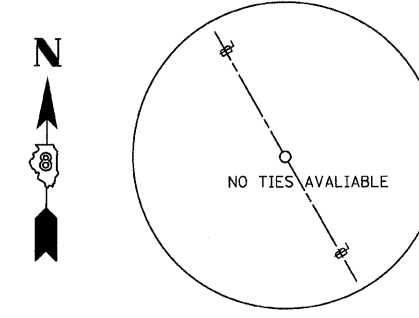
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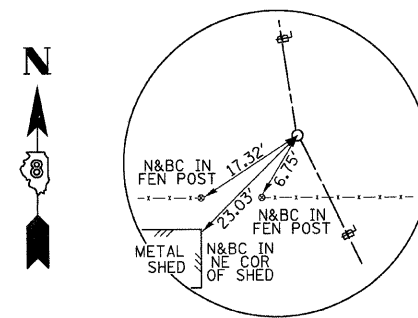
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PI STA 65+81.87
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5/8" I. ROD W/CAP



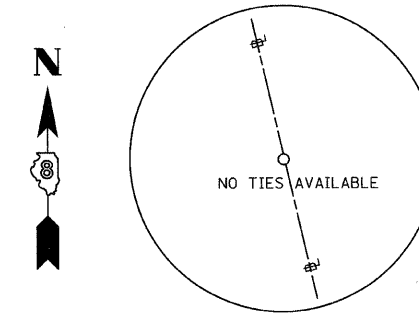
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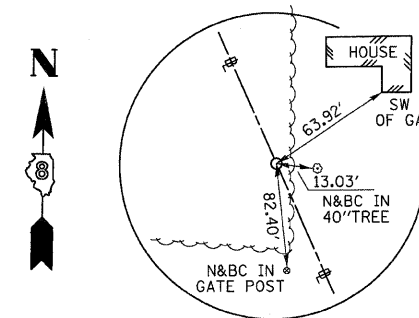
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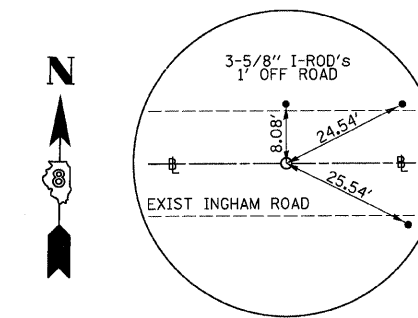
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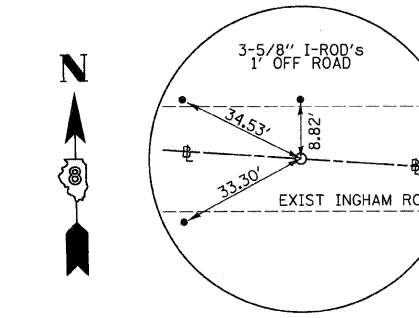
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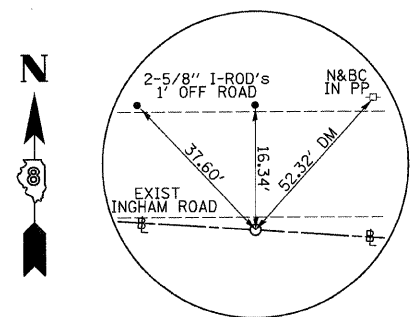
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POT STA 71+60.19
N - 850903.97 E - 2277378.62
5/8" I-ROD W/CAP"



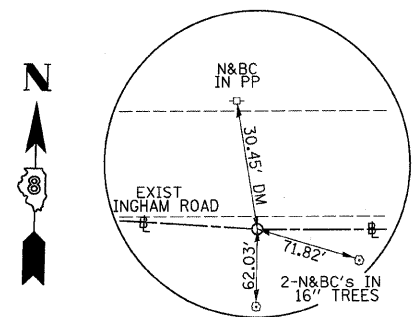
N INGHAM ROAD
POT STA 145+70.00
N - 852180.95 E - 2276596.69
5/8" I. ROD W/CAP



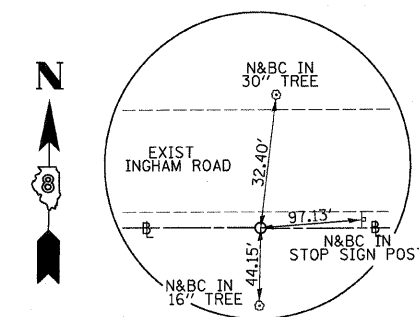
N INGHAM ROAD
POT STA 146+26.65
N - 852178.36 E - 2276653.28
5/8" I-ROD W/CAP



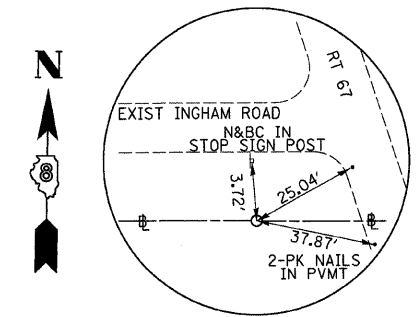
N INGHAM ROAD
PC STA 147+98.95
N - 852167.48 E - 2276825.24
5/8" I-ROD W/CAP



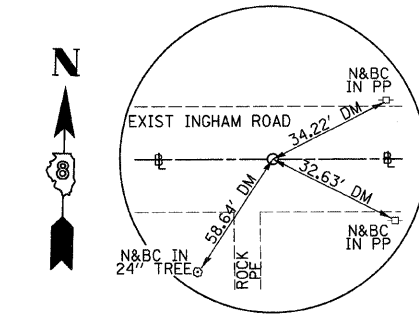
N INGHAM ROAD
PI STA 148+50.01
N - 852164.25 E - 2276876.19
5/8" I. ROD W/CAP



N INGHAM ROAD
PT STA 149+01.05
N - 852163.03 E - 2276927.24
5/8" I-ROD W/CAP



N INGHAM ROAD
POT STA 150+00.00
N - 852160.67 E - 2277026.15
5/8" I. ROD W/CAP



N INGHAM ROAD
POT STA 158+00.00
N - 852159.25 E - 2277826.15
5/8" I. ROD W/CAP

DATE: 1/25/10
DRAWN BY: JST
CHECKED BY: AWM

21041tp1002g

Hutchison Since 1945
Engineering Inc.
Jacksonville * Shorewood

REVISIONS	
NAME	DATE

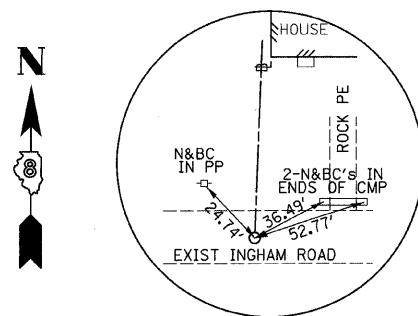
ILLINOIS DEPARTMENT OF TRANSPORTATION

SIDEROAD TIE POINTS
FAP ROUTE 310
SECTION 60-16-1&1B, 42-1&1B
MADISON AND JERSEY COUNTIES

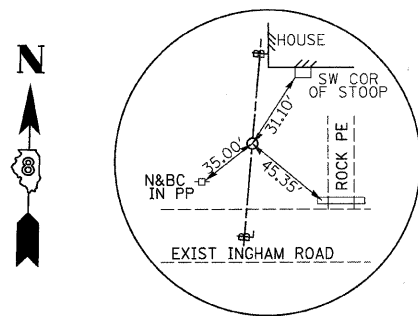
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HORIZ. 1"=40'
DATE: FEBRUARY 25, 2010
DRAWN BY: JST
CHECKED BY: AWM
PLOT DATE: *DATE-TIME*

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B, 42-1&1B	MADISON & JERSEY	481	49

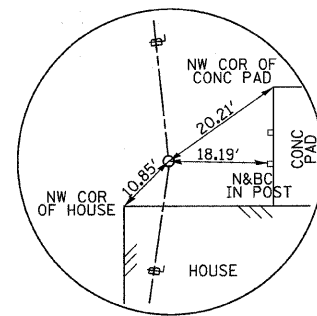
CONTRACT NO. 76318



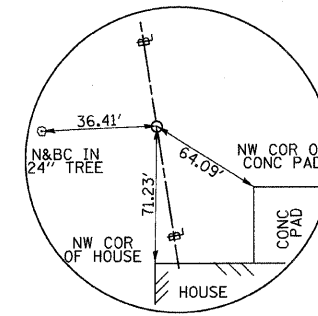
**N INGHAM ACCESS DRIVE
POT STA 170+00.00**
N - 852159.67 E - 2277586.15
5/8" I. ROD W/CAP



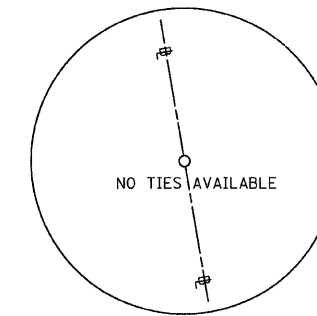
**N INGHAM ACCESS DRIVE
PC STA 170+52.86**
N - 852212.53 E - 2277586.24
5/8" I. ROD W/CAP



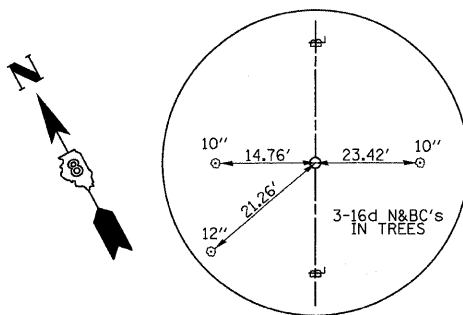
**N INGHAM ACCESS DRIVE
PI STA 171+15.00**
N - 852274.67 E - 2277586.35
5/8" I. ROD W/CAP



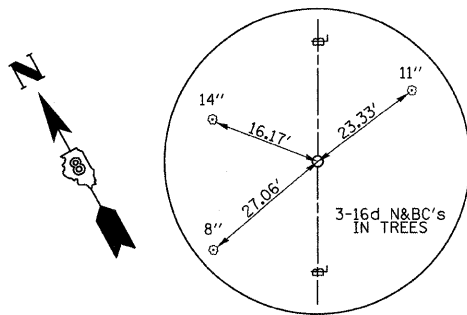
**N INGHAM ACCESS DRIVE
PT STA 171+74.85**
N - 852330.14 E - 2277586.35
5/8" I. ROD W/CAP



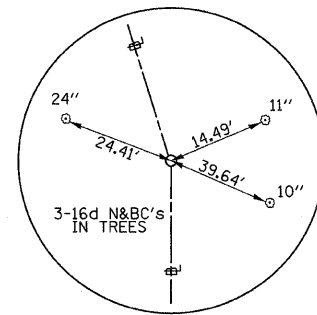
**N INGHAM ACCESS DRIVE
POT STA 175+50**
N - 852665.06 E - 2277389.32
5/8" I. ROD W/CAP



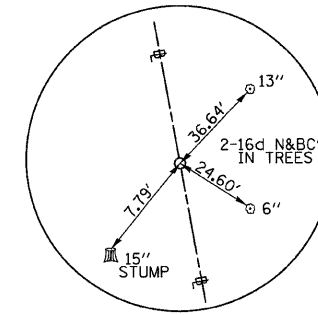
**STAGECOACH ROAD
POT STA 194+24.76**
N - 853882.05 E - 2275370.00
5/8" I. ROD W/CAP



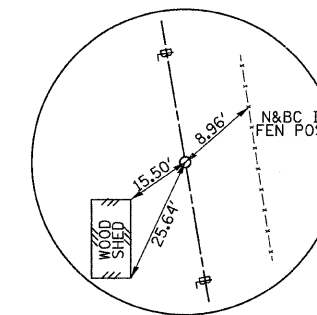
**STAGECOACH ROAD
PC STA 194+65.71**
N - 853914.16 E - 2275395.41
5/8" I. ROD W/CAP



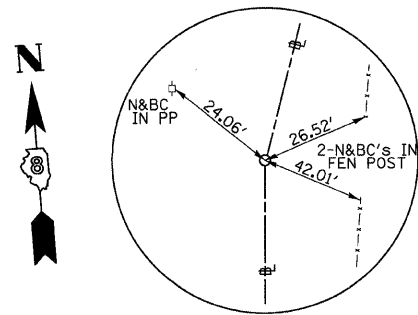
**STAGECOACH ROAD
PI STA 195+33.30**
N - 853967.17 E - 2275437.36
5/8" I. ROD W/CAP



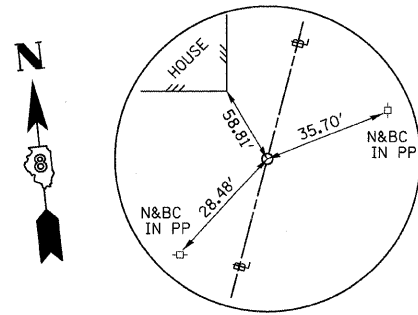
**STAGECOACH ROAD
PT STA 196+00.57**
N - 854026.45 E - 2275469.81
5/8" I. ROD W/CAP



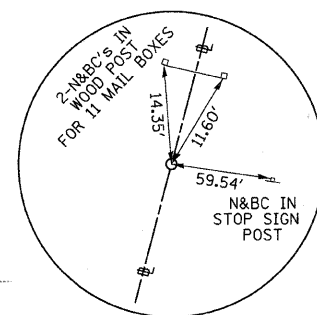
**STAGECOACH ROAD
PC STA 197+08.83**
N - 854121.41 E - 2275521.80
5/8" I. ROD W/CAP



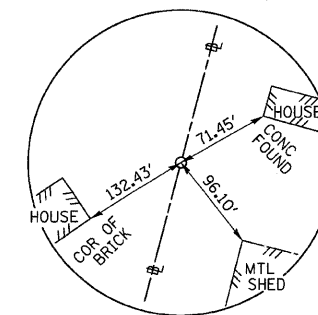
**STAGECOACH ROAD
PI STA 197+75.31**
N - 854179.72 E - 2275553.72
5/8" I-ROD W/CAP



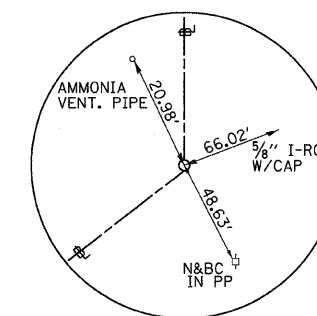
**STAGECOACH ROAD
PT STA 198+41.48**
N - 854231.96 E - 2275594.83
5/8" I-ROD W/CAP



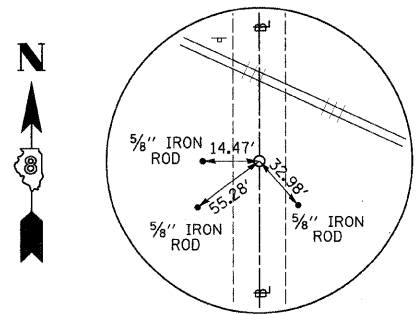
**STAGECOACH ROAD
POT STA 200+00.00**
N - 854356.54 E - 2275692.86
5/8" I-ROD W/CAP



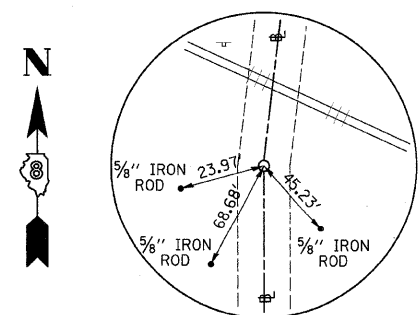
**STAGECOACH ROAD
PC STA 204+10.06**
N - 854678.79 E - 2275946.44
5/8" I. ROD W/CAP



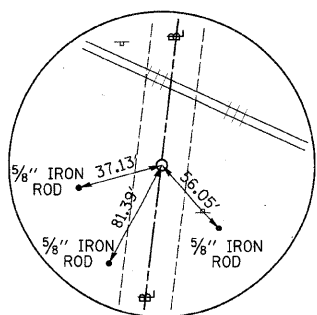
**STAGECOACH ROAD
PI STA 205+00.00**
N - 854749.47 E - 2276002.07
5/8" I. ROD W/CAP



**STAGECOACH ROAD
PRC STA 205+80.43**
N - 854838.70 E - 2275990.79
RR SPIKE W/ PUNCH HOLE



**STAGECOACH ROAD
PI STA 205+93.79**
N - 854851.96 E - 2275989.11
RR SPIKE W/ PUNCH HOLE



**STAGECOACH ROAD
PT STA 206+06.85**
N - 854864.92 E - 2275992.40
RR SPIKE W/ PUNCH HOLE

PLAN	SURVEYED	DATE
NOTE BOOK	ALIGNED	
NO.	BY	
	CHKD	
	DATE	

DATE-TIME
DATE-TIME
DATE-TIME
DATE-TIME

21041pt002a

Hutchison Engineering Inc.
Since 1945
Jacksonville * Shorewood

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

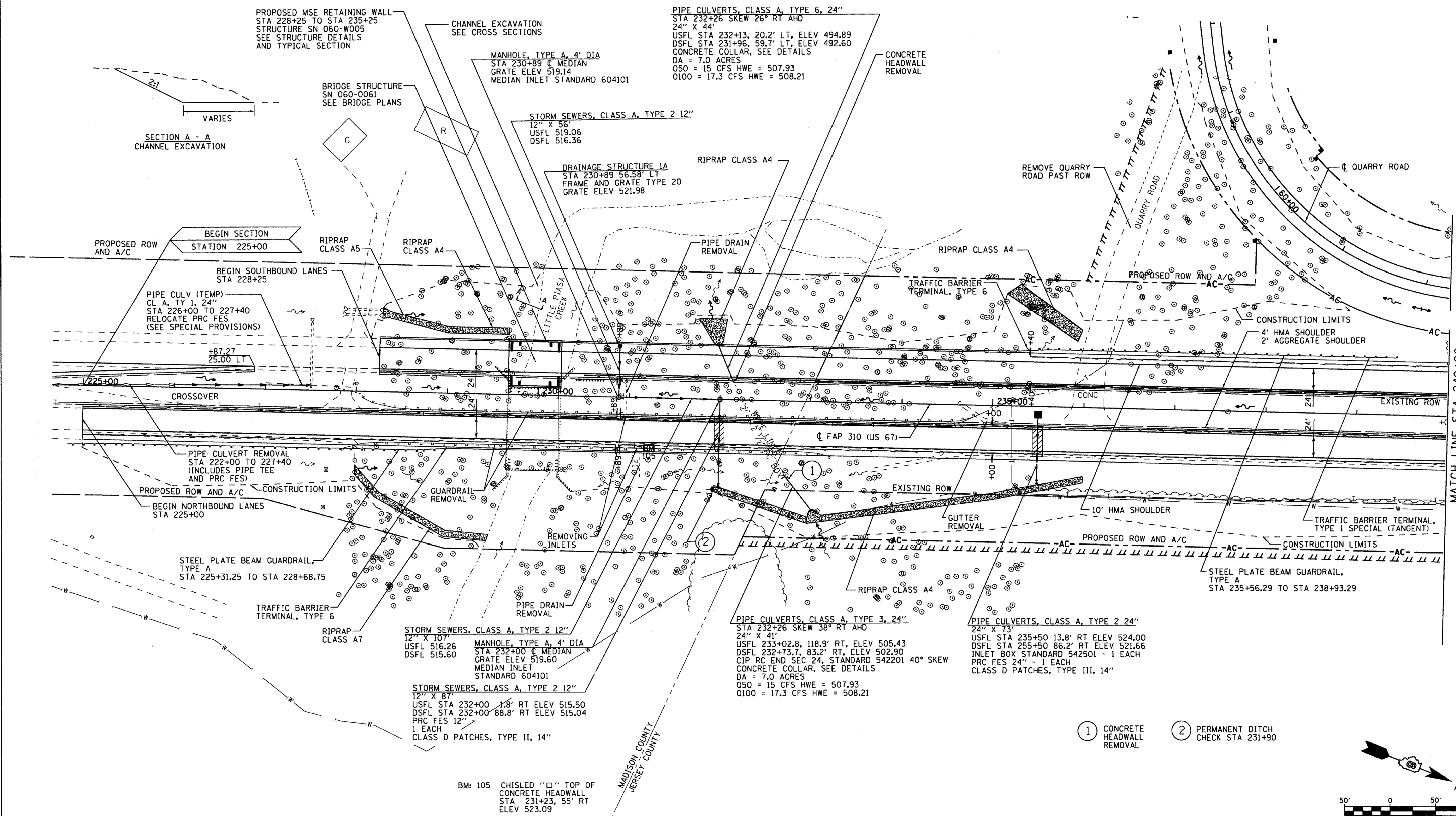
SIDEROAD TIE POINTS
FAP ROUTE 310
SECTION 60-16-1&1B, 42-1&1B
MADISON AND JERSEY COUNTIES

SCALE: VERT.
HORIZ.
DATE FEBRUARY 25, 2010
DRAWN BY JST
CHECKED BY AWM
PLOT DATE: *DATE-TIME*

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON/JERSEY	481	51
STA.		TO STA.		
EXISTING CONDITIONS:				

CONTRACT NO. 76318

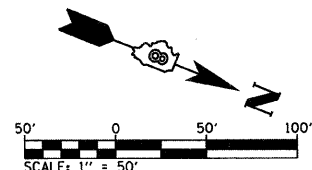
DATE	BY	SURVEYED	ALIGNED	CHECKED	DESIGNED
PLAN		NOTE BOOK			
		NO. _____			
		CADD FILE NAME			



BM: 105 CHISLED "□" TOP OF CONCRETE HEADWALL STA 231+23, 55' RT ELEV 523.09

MADISON COUNTY
JERSEY COUNTY

- ① CONCRETE HEADWALL REMOVAL
- ② PERMANENT DITCH CHECK STA 231+90

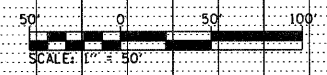


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
ROADWAY PLAN STA 225+00 TO 240+00
FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON/JERSEY COUNTY
DRAWN BY: EMR
CHECKED BY: GLF

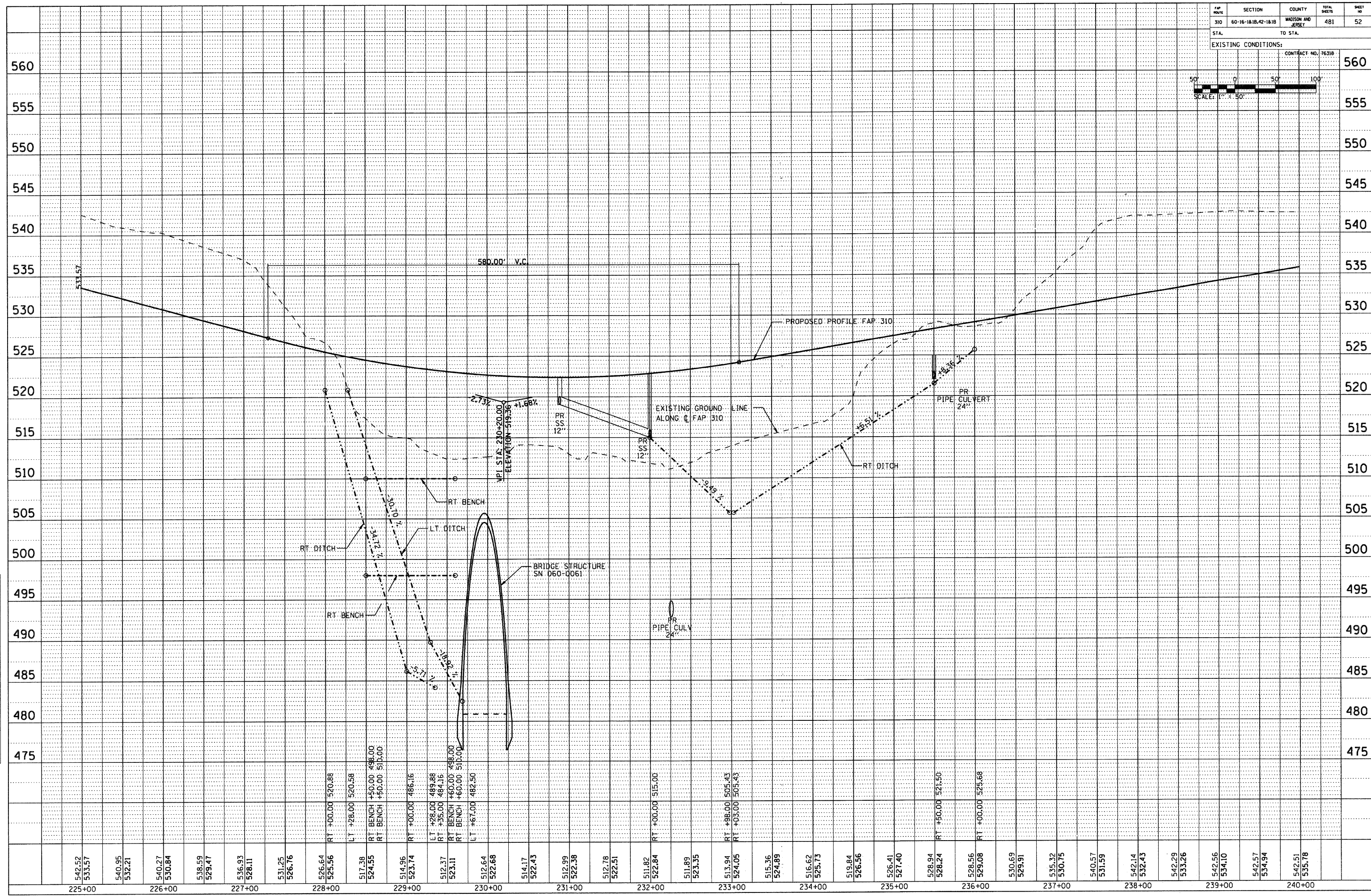
FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-181B,42-181B	MADISON AND JERSEY	481	52

EXISTING CONDITIONS:



PLAN	DATE
NO.	
BY	
DATE	
NO.	
BY	
DATE	

PROFILE	DATE
NO.	
BY	
DATE	
NO.	
BY	
DATE	

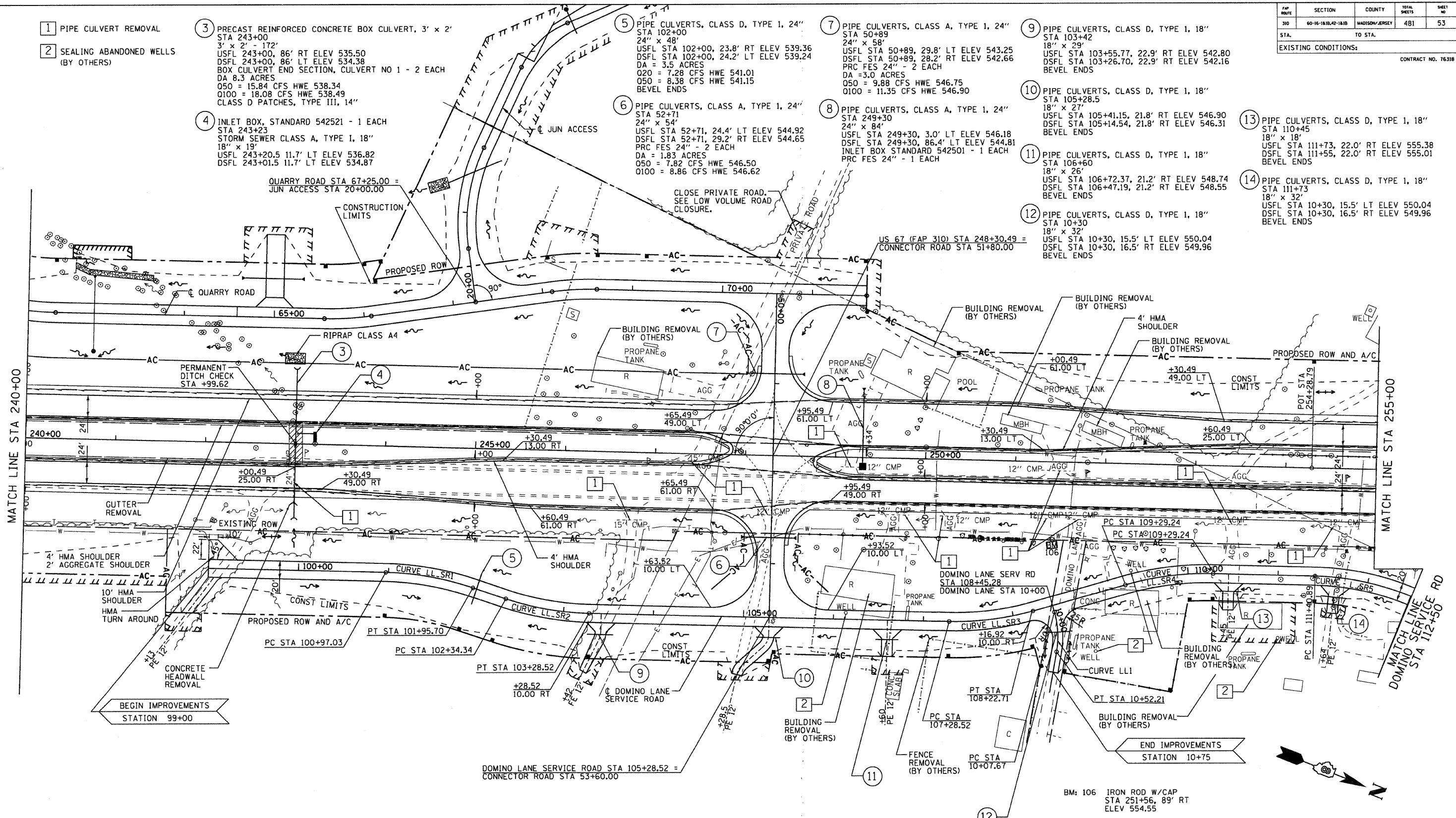


STA 225+00 TO STA 240+00

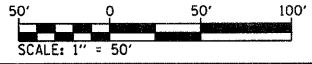
FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
60-16-1&1B,42-1&1B		MADISON/JERSEY	481	53
STA.	TO STA.			
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

- 1 PIPE CULVERT REMOVAL
- 2 SEALING ABANDONED WELLS (BY OTHERS)
- 3 PRECAST REINFORCED CONCRETE BOX CULVERT, 3' x 2' 3" x 2' - 172'
USFL 243+00, 86' RT ELEV 535.50
DSFL 243+00, 86' LT ELEV 534.38
BOX CULVERT END SECTION, CULVERT NO 1 - 2 EACH
DA 8.3 ACRES
O50 = 15.84 CFS HWE 538.34
O100 = 18.08 CFS HWE 538.49
CLASS D PATCHES, TYPE III, 14"
- 4 INLET BOX, STANDARD 542521 - 1 EACH
STA 243+23
STORM SEWER CLASS A, TYPE I, 18" x 19"
USFL 243+20.5 11.7' LT ELEV 536.82
DSFL 243+01.5 11.7' LT ELEV 534.87
- 5 PIPE CULVERTS, CLASS D, TYPE 1, 24" x 48"
STA 102+00
USFL STA 102+00, 23.8' RT ELEV 539.36
DSFL STA 102+00, 24.2' LT ELEV 539.24
DA = 3.5 ACRES
O20 = 7.28 CFS HWE 541.01
O50 = 8.38 CFS HWE 541.15
BEVEL ENDS
- 6 PIPE CULVERTS, CLASS A, TYPE 1, 24" x 54"
STA 52+71
USFL STA 52+71, 24.4' LT ELEV 544.92
DSFL STA 52+71, 29.2' RT ELEV 544.65
PRC FES 24" - 2 EACH
DA = 1.83 ACRES
O50 = 7.82 CFS HWE 546.50
O100 = 8.86 CFS HWE 546.62
- 7 PIPE CULVERTS, CLASS A, TYPE 1, 24" x 58"
STA 50+89
USFL STA 50+89, 29.8' LT ELEV 543.25
DSFL STA 50+89, 28.2' RT ELEV 542.66
PRC FES 24" - 2 EACH
DA = 3.0 ACRES
O50 = 9.88 CFS HWE 546.75
O100 = 11.35 CFS HWE 546.90
- 8 PIPE CULVERTS, CLASS A, TYPE 1, 24" x 84"
STA 249+30
USFL STA 249+30, 3.0' LT ELEV 546.18
DSFL STA 249+30, 86.4' LT ELEV 544.81
INLET BOX STANDARD 542501 - 1 EACH
PRC FES 24" - 1 EACH
- 9 PIPE CULVERTS, CLASS D, TYPE 1, 18" x 29"
STA 103+42
USFL STA 103+55.77, 22.9' RT ELEV 542.80
DSFL STA 103+26.70, 22.9' RT ELEV 542.16
BEVEL ENDS
- 10 PIPE CULVERTS, CLASS D, TYPE 1, 18" x 27"
STA 105+28.5
USFL STA 105+41.15, 21.8' RT ELEV 546.90
DSFL STA 105+14.54, 21.8' RT ELEV 546.31
BEVEL ENDS
- 11 PIPE CULVERTS, CLASS D, TYPE 1, 18" x 26"
STA 106+60
USFL STA 106+72.37, 21.2' RT ELEV 548.74
DSFL STA 106+47.19, 21.2' RT ELEV 548.55
BEVEL ENDS
- 12 PIPE CULVERTS, CLASS D, TYPE 1, 18" x 32"
STA 10+30
USFL STA 10+30, 15.5' LT ELEV 550.04
DSFL STA 10+30, 16.5' RT ELEV 549.96
BEVEL ENDS
- 13 PIPE CULVERTS, CLASS D, TYPE 1, 18" x 18"
STA 110+45
USFL STA 111+73, 22.0' RT ELEV 555.38
DSFL STA 111+55, 22.0' RT ELEV 555.01
BEVEL ENDS
- 14 PIPE CULVERTS, CLASS D, TYPE 1, 18" x 32"
STA 111+73
USFL STA 10+30, 15.5' LT ELEV 550.04
DSFL STA 10+30, 16.5' RT ELEV 549.96
BEVEL ENDS

DATE	
BY	
REVISIONS	
NO.	
DATE	
BY	
REVISIONS	
NO.	
DATE	
BY	
REVISIONS	
NO.	



PR CURVE LL.SR1 PI STA 101+46.74 Δ = 17° 07' 55" (RT) D = 17° 21' 44" T = 49.71' R = 330.00' L = 98.67' E = 3.72' PC STA 100+97.03 PT STA 101+95.70	PR CURVE LL.SR2 PI STA 102+81.78 Δ = 17° 07' 55" (LT) D = 18° 11' 21" T = 47.45' R = 315.00' L = 94.19' E = 3.55' PC STA 102+34.34 PT STA 103+28.52	PR CURVE LL1 PI STA 10+30.31 Δ = 25° 31' 08" (RT) D = 57° 17' 45" T = 22.65' R = 100.00' L = 44.54' E = 2.53' PC STA 10+07.67 PT STA 10+52.21	PR CURVE LL.SR3 PI STA 107+75.97 Δ = 17° 07' 54" (LT) D = 18° 11' 21" T = 47.45' R = 315.00' L = 94.19' E = 3.55' PC STA 107+28.52 PT STA 108+22.71	PR CURVE LL.SR4 PI STA 109+78.95 Δ = 17° 07' 55" (RT) D = 17° 21' 44" T = 49.71' R = 330.00' L = 125.24' E = 3.72' PC STA 109+29.24 PT STA 110+27.91	PR CURVE LL.SR5 PI STA 112+04.44 Δ = 23° 55' 11" (RT) D = 19° 05' 55" T = 63.55' R = 300.00' L = 125.24' E = 6.66' e = 0.04 TR = 23 SE RUN IN STA 110+74.56 TO 111+73.56 SE RUN OUT STA 112+33.47 TO 113+32.47 PC STA 111+40.89 PT STA 112+66.14
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ILLINOIS DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN STA 240+00 TO 255+00

FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON/JERSEY COUNTY

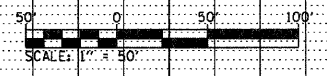
DRAWN BY: EMR CHECKED BY: GLF

REVISIONS	
NAME	DATE

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-181B,42-181B	MADISON AND JERSEY	481	54

STA. TO STA.

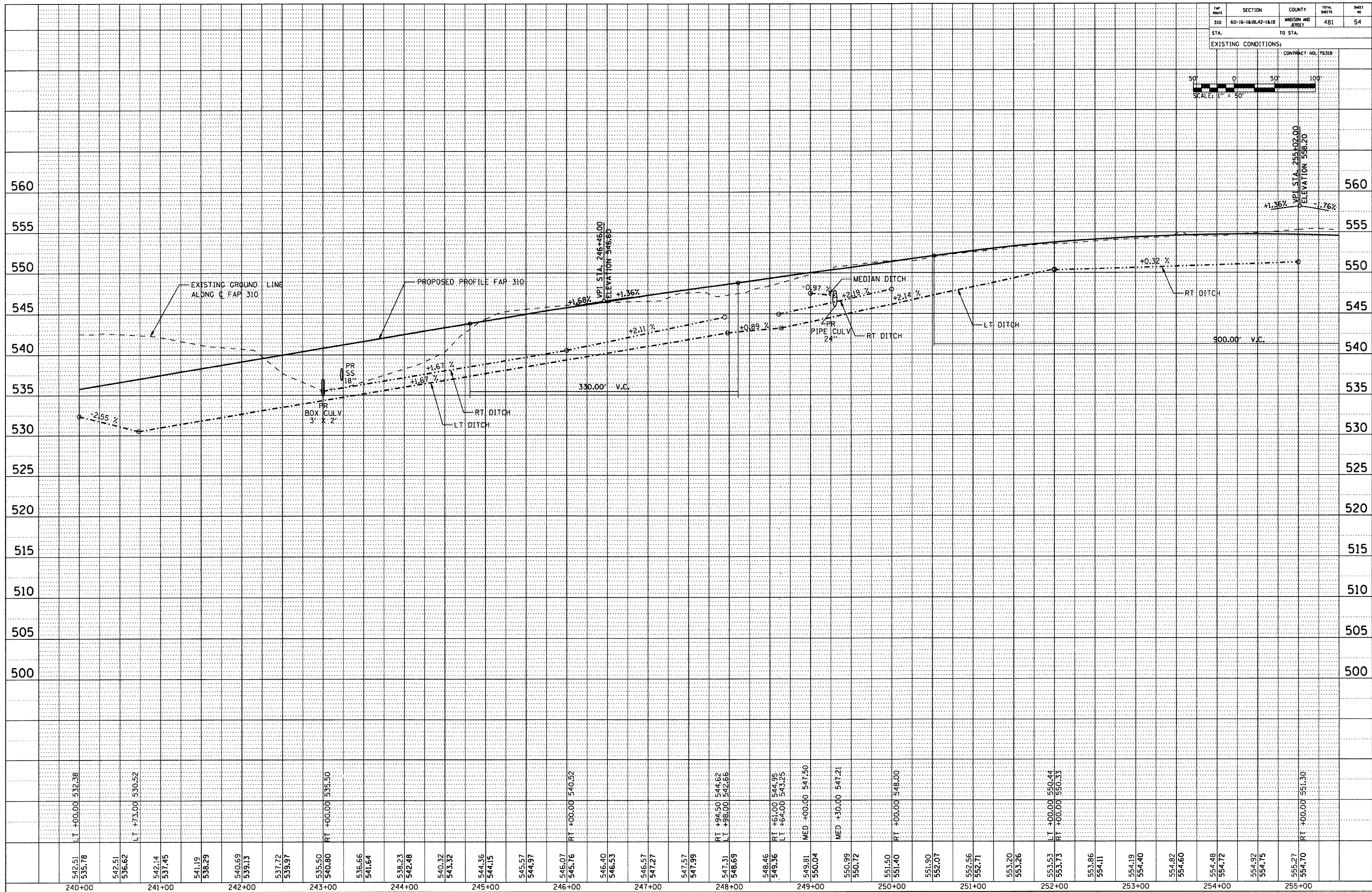
EXISTING CONDITIONS:



CONTRACT NO. 1631B

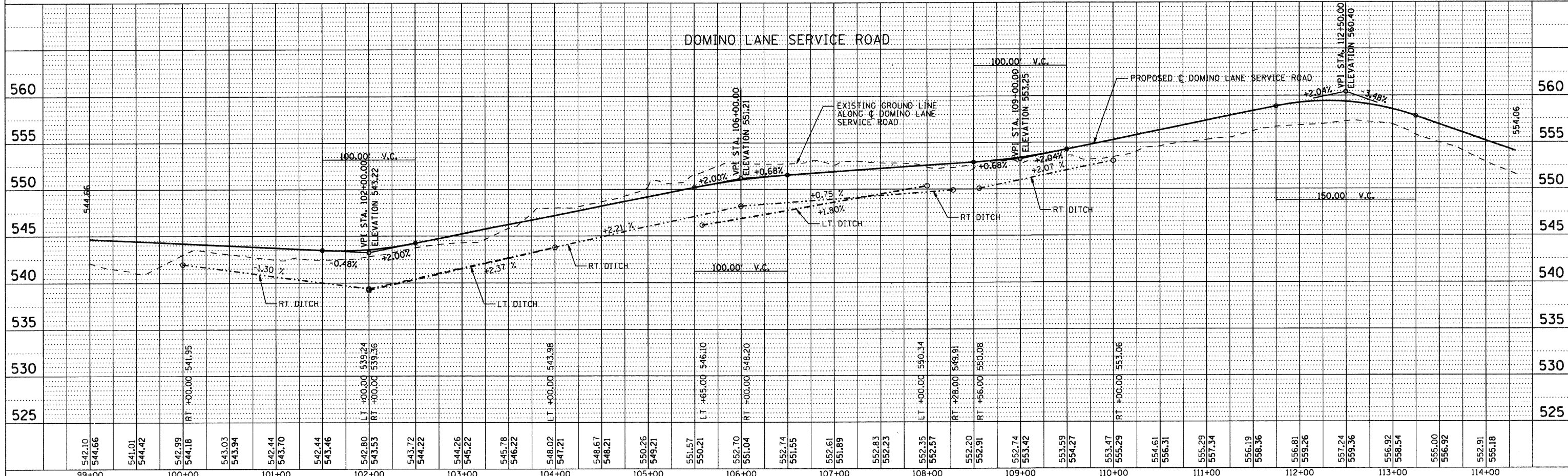
PLAN	REVIEWED	DATE
NO.	NO.	

PROFILE	REVIEWED	DATE
NO.	NO.	

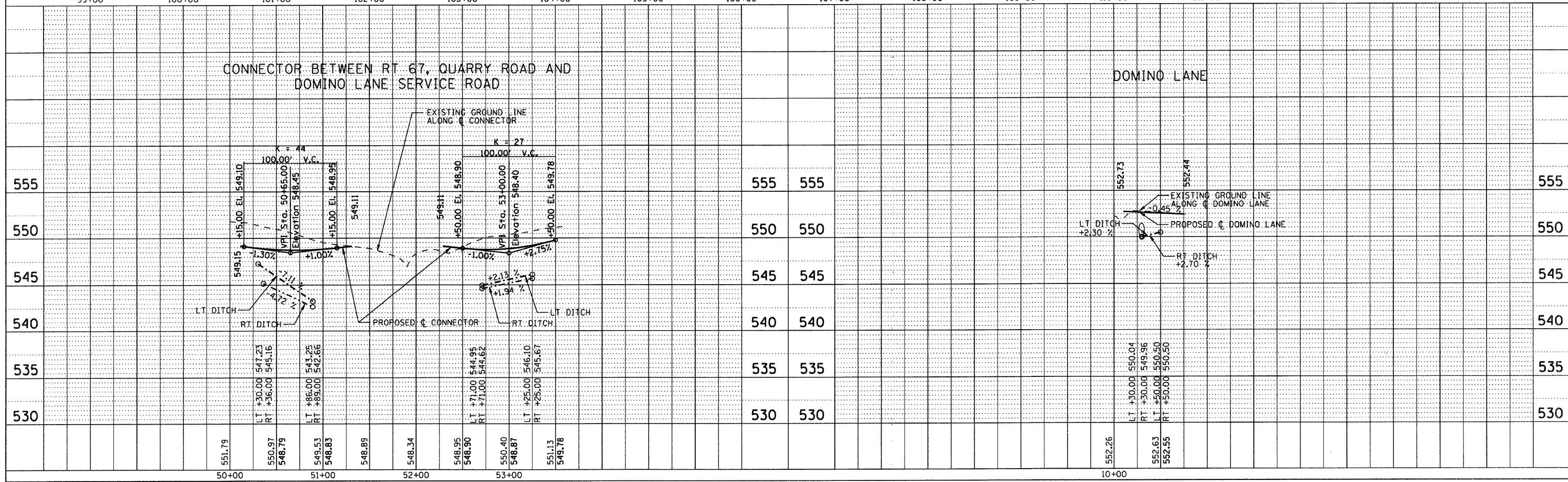


STA 240+00 TO STA 255+00

PLAN	SURVEYED	BY	DATE
NO.	NO.		
NOTE BOOK NO.	DATE CHECKED		
RT. OF WAY CHECKED			
NO.	DATE FILE NAME		



PROFILE	SURVEYED	BY	DATE
NO.	NO.		
NOTE BOOK NO.	DATE CHECKED		
RT. OF WAY CHECKED			
NO.	DATE FILE NAME		



FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON/JERSEY	481	56
STA.	TO STA.			
EXISTING CONDITIONS:				

CONTRACT NO. 76318

DATE	
BY	
PLAN	
NO. _____	
REVISIONS	
NO. _____	
DATE	
BY	
PLAN	
NO. _____	

CIP CONCRETE BOX CULVERT 6' X 7'
 STA 268+54 SKEW 32° LT AHD
 6' X 7' X 136'
 USFL STA 268+70.35, 22.8' LT, ELEV 499.90
 DSFL STA 269+50.20, 133.4' LT, ELEV 498.68
 CONCRETE COLLARS, SEE DETAILS
 BOX CULVERT END SECTIONS CULVERT NO. 2 - 1 EACH
 DA = 162.1 ACRES
 O50 = 239.3 CFS, HWE = 508.16
 O100 = 274.5 CFS, HWE = 508.77

PR CURVE P1.102
 PI STA 271+78.01
 $\Delta = 12^\circ 06' 26''$ (LT)
 $D = 0^\circ 49' 07''$
 $T = 742.36'$
 $R = 7,000.00'$
 $L = 1,479.19'$
 $E = 39.25'$
 $e = .03$
 $TR\ IN = 45'$
 $SE\ RUN\ IN = 262+93.48\ TO\ 265+06.74$
 $SE\ RUN\ OUT = 278+43.76\ TO\ 281+57.01$
 $TR\ OUT = 45'$
 $PC\ STA\ 264+35.65$
 $PT\ STA\ 279+14.84$

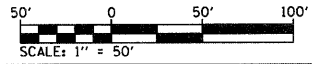
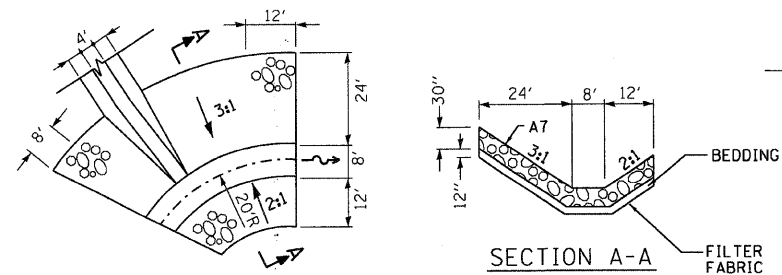
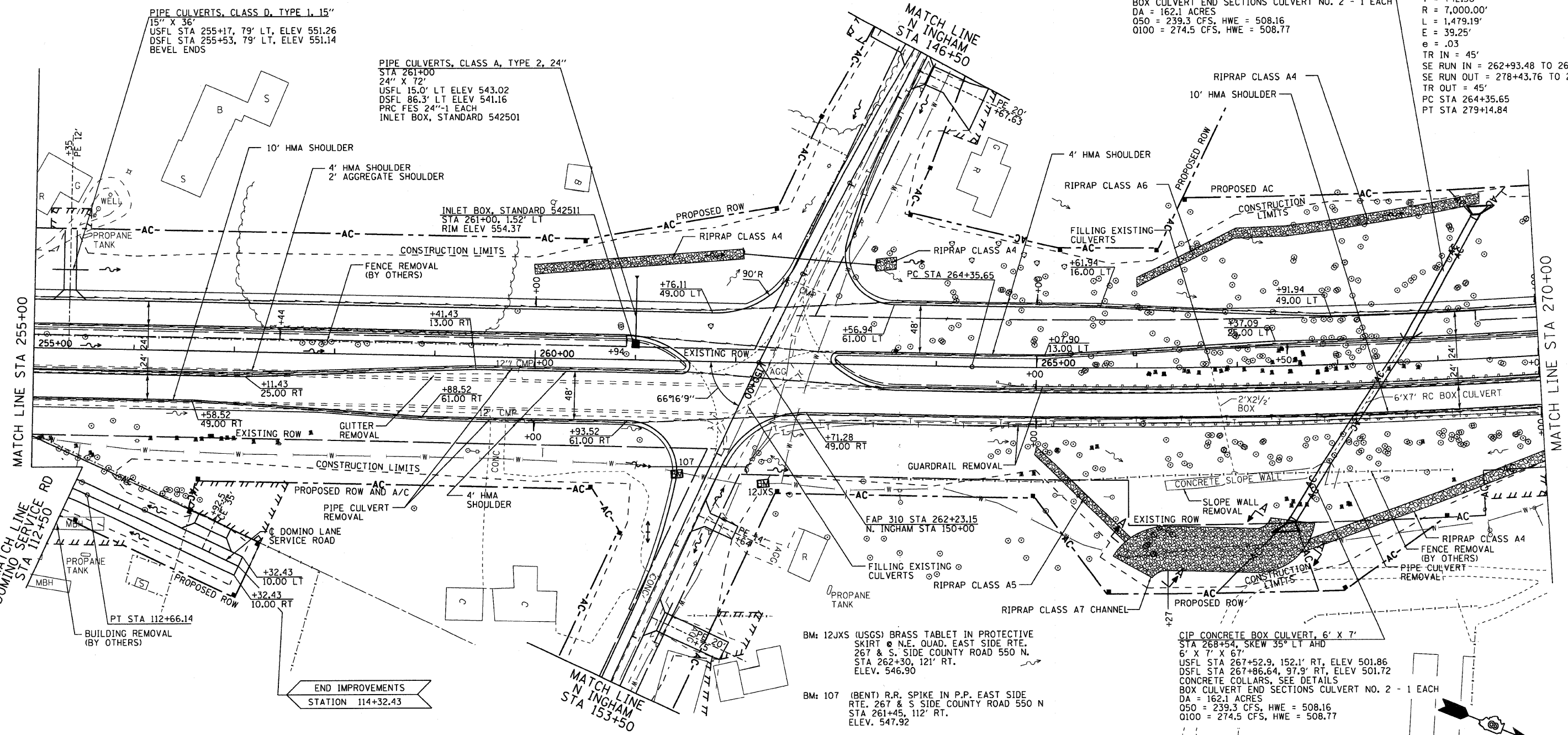
PIPE CULVERTS, CLASS D, TYPE 1, 15"
 15" X 36"
 USFL STA 255+17, 79' LT, ELEV 551.26
 DSFL STA 255+53, 79' LT, ELEV 551.14
 BEVEL ENDS

PIPE CULVERTS, CLASS A, TYPE 2, 24"
 STA 261+00
 24" X 72"
 USFL 15.0' LT ELEV 543.02
 DSFL 86.3' LT ELEV 541.16
 PRC FES 24"-1 EACH
 INLET BOX, STANDARD 542501

INLET BOX, STANDARD 542511
 STA 261+00, 1.52' LT
 RIM ELEV 554.37

BM: 12JXS (USGS) BRASS TABLET IN PROTECTIVE SKIRT @ N.E. QUAD. EAST SIDE RTE. 267 & S. SIDE COUNTY ROAD 550 N. STA 262+30, 121' RT. ELEV. 546.90
 BM: 107 (BENT) R.R. SPIKE IN P.P. EAST SIDE RTE. 267 & S. SIDE COUNTY ROAD 550 N. STA 261+45, 112' RT. ELEV. 547.92

CIP CONCRETE BOX CULVERT, 6' X 7'
 STA 268+54, SKEW 35° LT AHD
 6' X 7' X 67'
 USFL STA 267+52.9, 152.1' RT, ELEV 501.86
 DSFL STA 267+86.64, 97.9' RT, ELEV 501.72
 CONCRETE COLLARS, SEE DETAILS
 BOX CULVERT END SECTIONS CULVERT NO. 2 - 1 EACH
 DA = 162.1 ACRES
 O50 = 239.3 CFS, HWE = 508.16
 O100 = 274.5 CFS, HWE = 508.77



ILLINOIS DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN STA 255+00 TO 270+00

FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON/JERSEY COUNTY

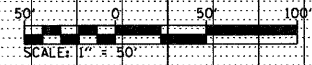
DRAWN BY: EMR CHECKED BY: GLF

REVISIONS	
NAME	DATE

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-181B,42-181B	MADISON AND JERSEY	481	57

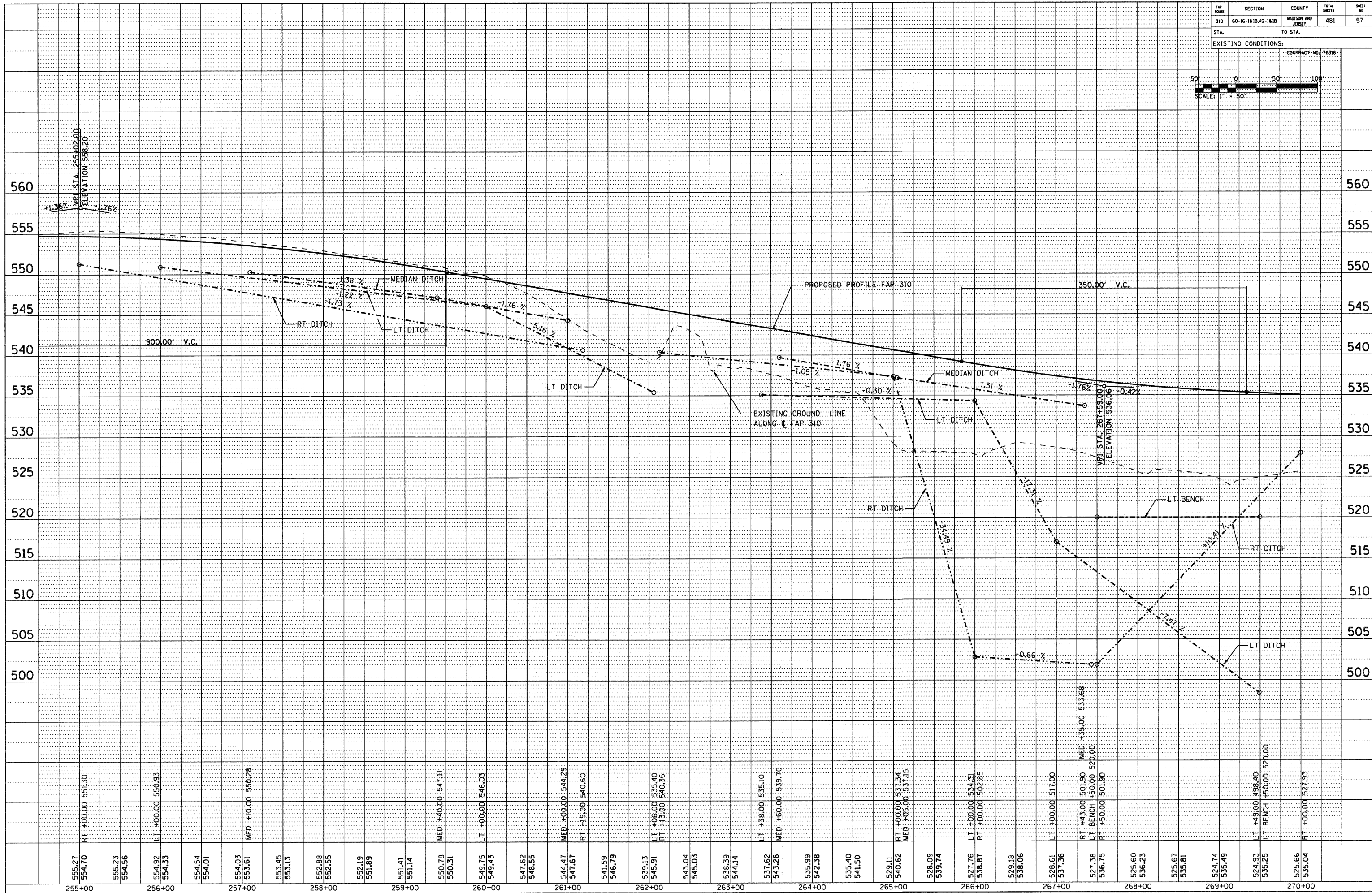
STA. TO STA.

EXISTING CONDITIONS:
CONTRACT NO. 76318



PLAN	DESIGNED	DATE
	BY	
	PLANNED	
	NOTED	
	NO. _____	

PROFILE	DESIGNED	DATE
	BY	
	PLANNED	
	NOTED	
	NO. _____	



STA 255+00 TO STA 270+00

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON/JERSEY	481	58
STA.	TO STA.			
EXISTING CONDITIONS:				

CONTRACT NO. 76318

DATE	BY

PR CURVE P1.102
 PI STA 271+78.01
 $\Delta = 12^{\circ} 06' 26''$ (LT)
 $D = 0^{\circ} 49' 07''$
 $T = 742.36'$
 $R = 7,000.00'$
 $L = 1,479.19'$
 $E = 39.25'$
 $e = 0.03$
 $TR IN = 45'$
 SE RUN IN = 262+93.48 TO 265+06.74
 SE RUN OUT = 278+43.76 TO 280+56.94
 $TR OUT = 45'$
 PC STA 264+35.65
 PT STA 279+14.84

PIPE CULVERTS, CLASS D, TYPE 2, 15"
 15" X 60"
 USFL STA 280+80, 97' LT, ELEV 524.15
 DSFL STA 281+40, 97' LT, ELEV 523.91
 BEVEL ENDS

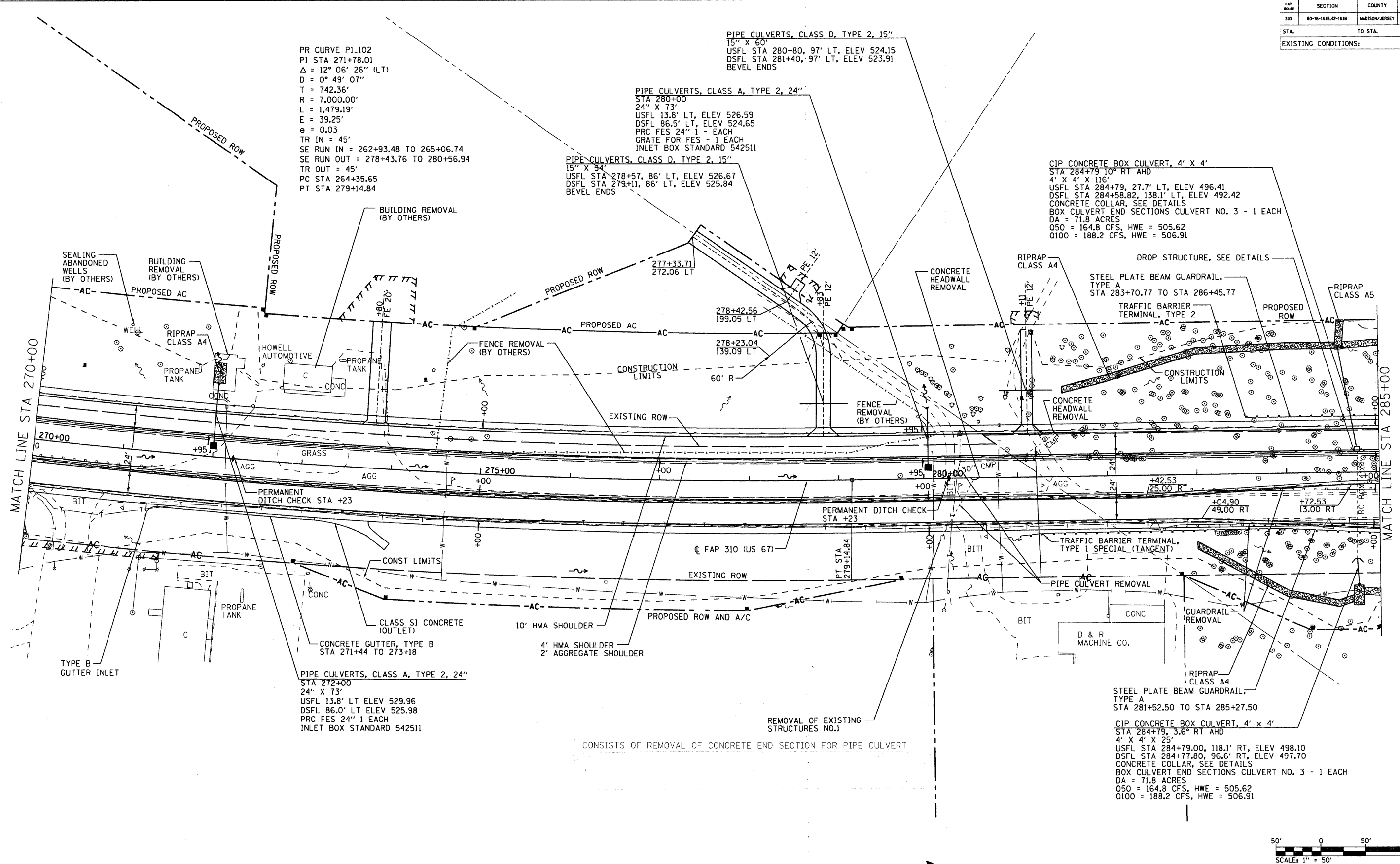
PIPE CULVERTS, CLASS A, TYPE 2, 24"
 24" X 73'
 USFL 13.8' LT, ELEV 526.59
 DSFL 86.5' LT, ELEV 524.65
 PRC FES 24" 1 - EACH
 GRATE FOR FES - 1 EACH
 INLET BOX STANDARD 542511

PIPE CULVERTS, CLASS D, TYPE 2, 15"
 15" X 54"
 USFL STA 278+57, 86' LT, ELEV 526.67
 DSFL STA 279+11, 86' LT, ELEV 525.84
 BEVEL ENDS

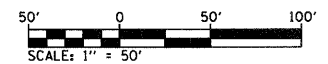
CIP CONCRETE BOX CULVERT, 4' X 4'
 STA 284+79 TO RT AHD
 4' X 4' X 116'
 USFL STA 284+79, 27.7' LT, ELEV 496.41
 DSFL STA 284+58.82, 138.1' LT, ELEV 492.42
 CONCRETE COLLAR, SEE DETAILS
 BOX CULVERT END SECTIONS CULVERT NO. 3 - 1 EACH
 DA = 71.8 ACRES
 O50 = 164.8 CFS, HWE = 505.62
 O100 = 188.2 CFS, HWE = 506.91

PIPE CULVERTS, CLASS A, TYPE 2, 24"
 STA 272+00
 24" X 73'
 USFL 13.8' LT ELEV 529.96
 DSFL 86.0' LT ELEV 525.98
 PRC FES 24" 1 EACH
 INLET BOX STANDARD 542511

STEEL PLATE BEAM GUARDRAIL, TYPE A
 STA 281+52.50 TO STA 285+27.50
 CIP CONCRETE BOX CULVERT, 4' x 4'
 STA 284+79, 3.6° RT AHD
 4' x 4' x 25'
 USFL STA 284+79.00, 118.1' RT, ELEV 498.10
 DSFL STA 284+77.80, 96.6' RT, ELEV 497.70
 CONCRETE COLLAR, SEE DETAILS
 BOX CULVERT END SECTIONS CULVERT NO. 3 - 1 EACH
 DA = 71.8 ACRES
 O50 = 164.8 CFS, HWE = 505.62
 O100 = 188.2 CFS, HWE = 506.91



CONSISTS OF REMOVAL OF CONCRETE END SECTION FOR PIPE CULVERT



ILLINOIS DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN STA 270+00 TO 285+00

FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON/JERSEY COUNTY

DRAWN BY: EMR CHECKED BY: GLF

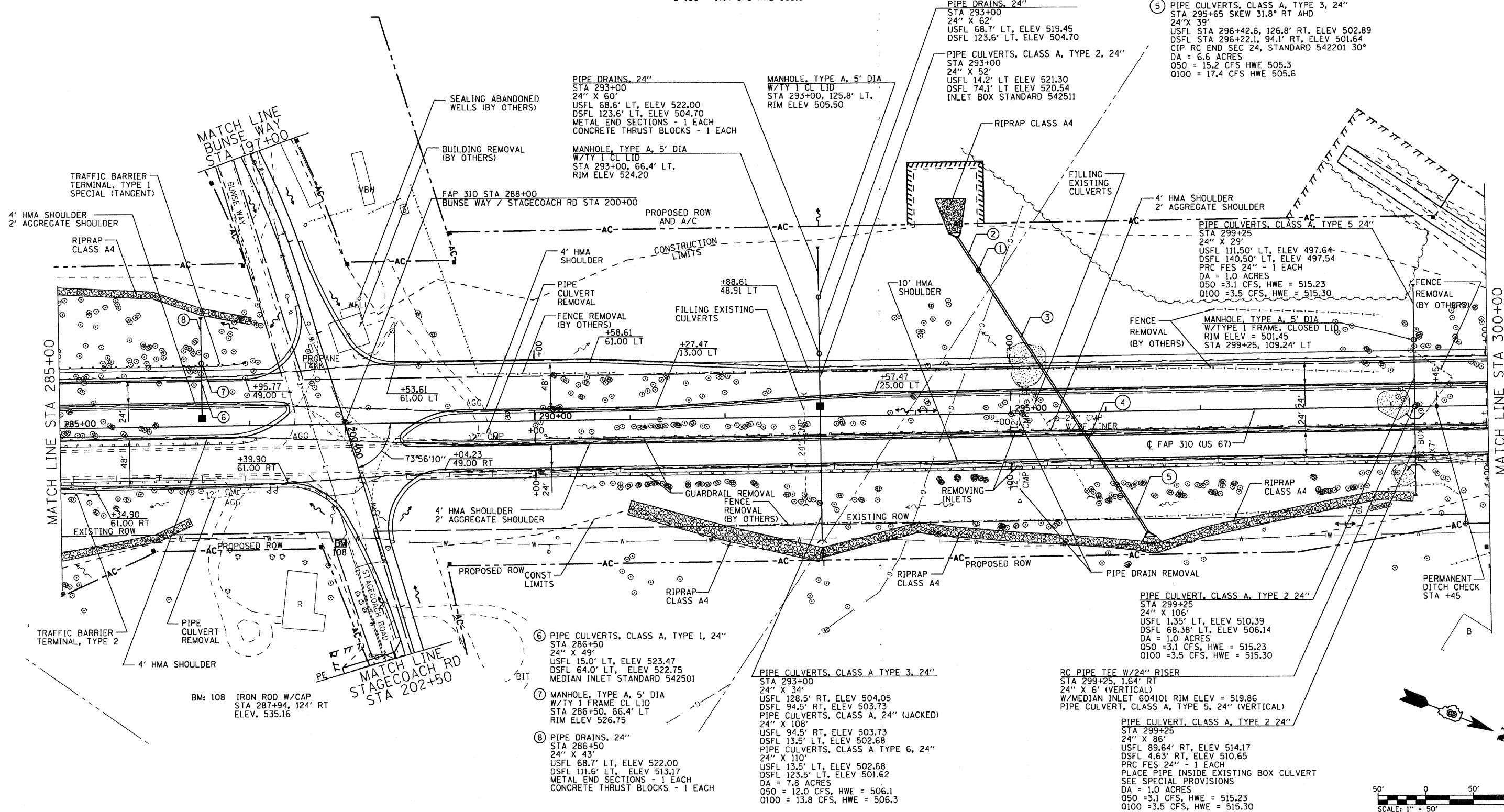
REVISIONS	
NAME	DATE

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
60-16-1&1B, 42-1&1B	MADISON/JERSEY	481	60	
STA.	TO STA.			
EXISTING CONDITIONS:				

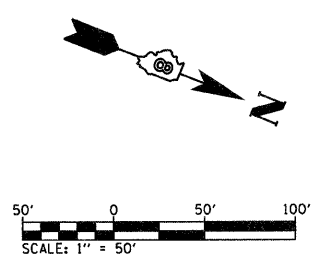
CONTRACT NO. 76318

DATE	
BY	
REVISIONS	
NOTED	
ALTERNATE CHECKED	
ADD FILE NAME	
PLAN	
NO.	

- ① MANHOLE, TYPE A, 5' DIA W/TY 1 CL LID STA 294+68.6, 151.0' LT RIM ELEV 498.85
- ② PIPE CULVERTS, CLASS A, TYPE 5, 24" STA 295+65 SKEW 31.8° RT AHD 24" X 40' USFL STA 294+67.9, 152.0' LT ELEV 484.5 DSFL STA 294+46.8, 185.7' LT ELEV 484.01 CIP RC END SEC 24, STANDARD 542201 30° DA = 6.6 ACRES O50 = 15.2 CFS HWE 505.3 O100 = 17.4 CFS HWE 505.6
- ③ PIPE CULVERTS, CLASS A, TYPE 6, 24" STA 295+65 SKEW 31.8° RT AHD 24" X 149' USFL STA 295+49.3, 22.0' LT, ELEV 497.84 DSFL STA 294+70.2, 148.3' LT, ELEV 493.65 DA = 6.6 ACRES O50 = 15.2 CFS HWE 505.3 O100 = 17.4 CFS HWE 505.6
- ④ PIPE CULVERTS, CLASS A, 24" (JACKED) STA 295+65 SKEW 31.8° RT AHD 24" X 138' USFL STA 296+22.1, 94.1' RT, ELEV 501.85 DSFL STA 295+49.3, 22.0' LT, ELEV 497.95 DA = 6.6 ACRES O50 = 15.2 CFS HWE 505.3 O100 = 17.4 CFS HWE 505.6
- ⑤ PIPE CULVERTS, CLASS A, TYPE 3, 24" STA 295+65 SKEW 31.8° RT AHD 24" X 39' USFL STA 296+42.6, 126.8' RT, ELEV 502.89 DSFL STA 296+22.1, 94.1' RT, ELEV 501.64 CIP RC END SEC 24, STANDARD 542201 30° DA = 6.6 ACRES O50 = 15.2 CFS HWE 505.3 O100 = 17.4 CFS HWE 505.6



- ⑥ PIPE CULVERTS, CLASS A, TYPE 1, 24" STA 286+50 24" X 49' USFL 15.0' LT, ELEV 523.47 DSFL 64.0' LT, ELEV 522.75 MEDIAN INLET STANDARD 542501
- ⑦ MANHOLE, TYPE A, 5' DIA W/TY 1 FRAME CL LID STA 286+50, 66.4' LT RIM ELEV 526.75
- ⑧ PIPE DRAINS, 24" STA 286+50 24" X 43' USFL 68.7' LT, ELEV 522.00 DSFL 111.6' LT, ELEV 513.17 METAL END SECTIONS - 1 EACH CONCRETE THRUST BLOCKS - 1 EACH
- PIPE CULVERTS, CLASS A TYPE 3, 24" STA 293+00 24" X 34' USFL 128.5' RT, ELEV 504.05 DSFL 94.5' RT, ELEV 503.73 PIPE CULVERTS, CLASS A, 24" (JACKED) 24" X 108' USFL 94.5' RT, ELEV 503.73 DSFL 13.5' LT, ELEV 502.68 PIPE CULVERTS, CLASS A TYPE 6, 24" 24" X 110' USFL 13.5' LT, ELEV 502.68 DSFL 123.5' LT, ELEV 501.62 DA = 7.8 ACRES O50 = 12.0 CFS, HWE = 506.1 O100 = 13.8 CFS, HWE = 506.3
- PIPE CULVERT, CLASS A, TYPE 2 24" STA 299+25 24" X 106' USFL 1.35' LT, ELEV 510.39 DSFL 68.38' LT, ELEV 506.14 DA = 1.0 ACRES O50 = 3.1 CFS, HWE = 515.23 O100 = 3.5 CFS, HWE = 515.30
- RC PIPE TEE W/24" RISER STA 299+25, 1.64' RT 24" X 6" (VERTICAL) W/MEDIAN INLET 604101 RIM ELEV = 519.86 PIPE CULVERT, CLASS A, TYPE 5, 24" (VERTICAL) STA 299+25 24" X 86' USFL 89.64' RT, ELEV 514.17 DSFL 4.63' RT, ELEV 510.65 PRC FES 24" - 1 EACH PLACE PIPE INSIDE EXISTING BOX CULVERT SEE SPECIAL PROVISIONS DA = 1.0 ACRES O50 = 3.1 CFS, HWE = 515.23 O100 = 3.5 CFS, HWE = 515.30



ILLINOIS DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN STA 285+00 TO 300+00

FAP ROUTE 310
SECTION 60-16-1&1B, 42-1&1B
MADISON/JERSEY COUNTY

DRAWN BY: EMR CHECKED BY: GLF

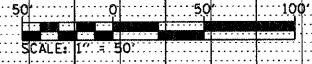
REVISIONS	
NAME	DATE

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MAJISON AND ESSEX	481	61

STA. TO STA.

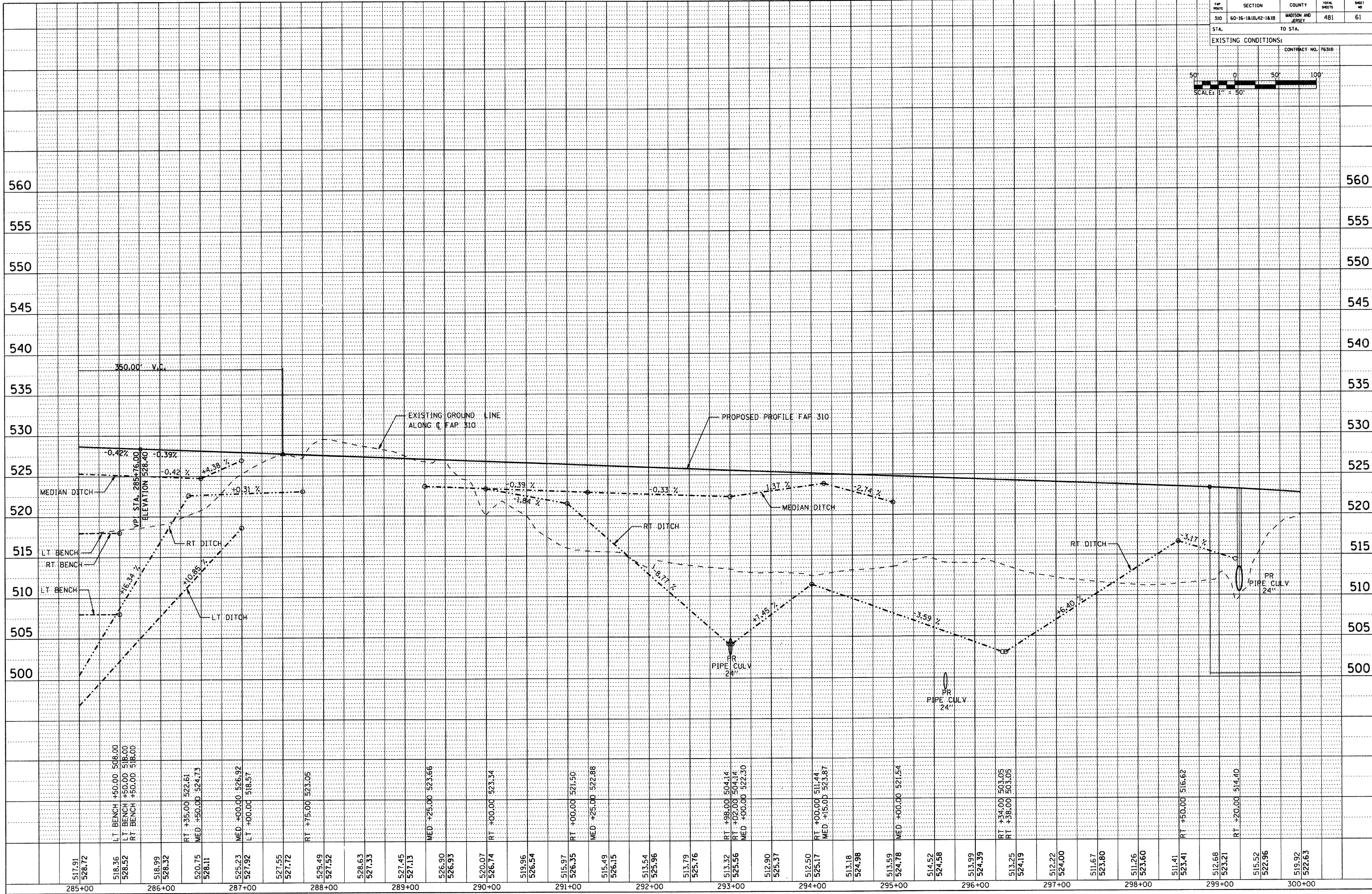
EXISTING CONDITIONS:

CONTRACT NO. 7631B



PLAN	SURVEYED	BY	DATE
	PLOTTED		
	REVISIONS		
	NOTE BOOK		
	NO.		

PROFILE	DRAWN	BY	DATE
	REVISIONS		
	NOTE BOOK		
	NO.		

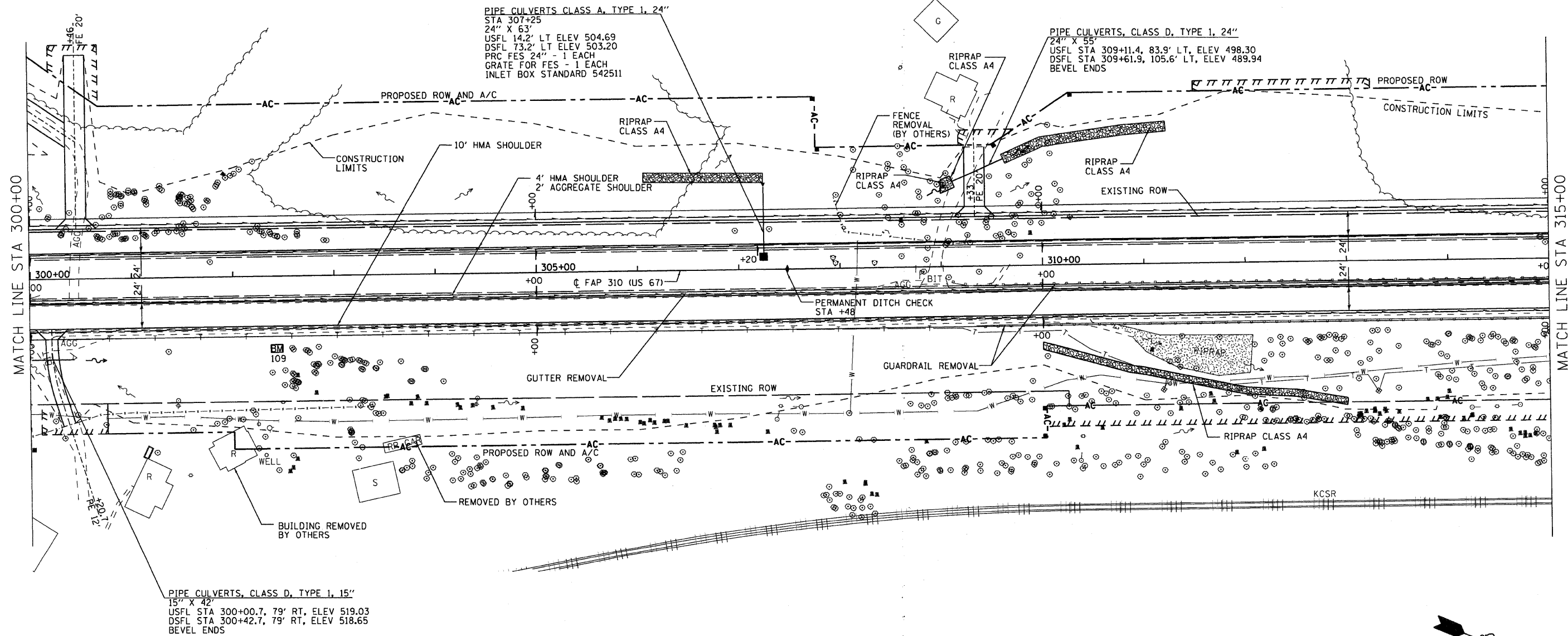


STA 285+00 TO STA 300+00

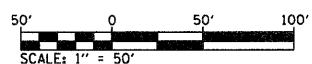
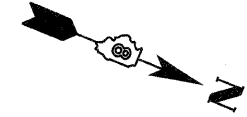
FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
310	60-16-181B,42-181B	MADISON/JERSEY	481	62
STA.		TO STA.		
EXISTING CONDITIONS:				

CONTRACT NO. 76318

PLAN	DATE
SUBMITTED	
REVISED	
NOTED	
BY	
NO.	



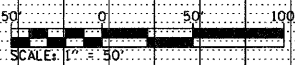
BM 109
 1/2" LAG BOLT IN BASE & ON WEST SIDE OF
 16" OAK ON THE EAST SIDE OF ILL 267 AT
 STA 525+71, 34.5' RT (302+50, 75' RT)
 ELEV. 526.43



REVISIONS	
NAME	DATE

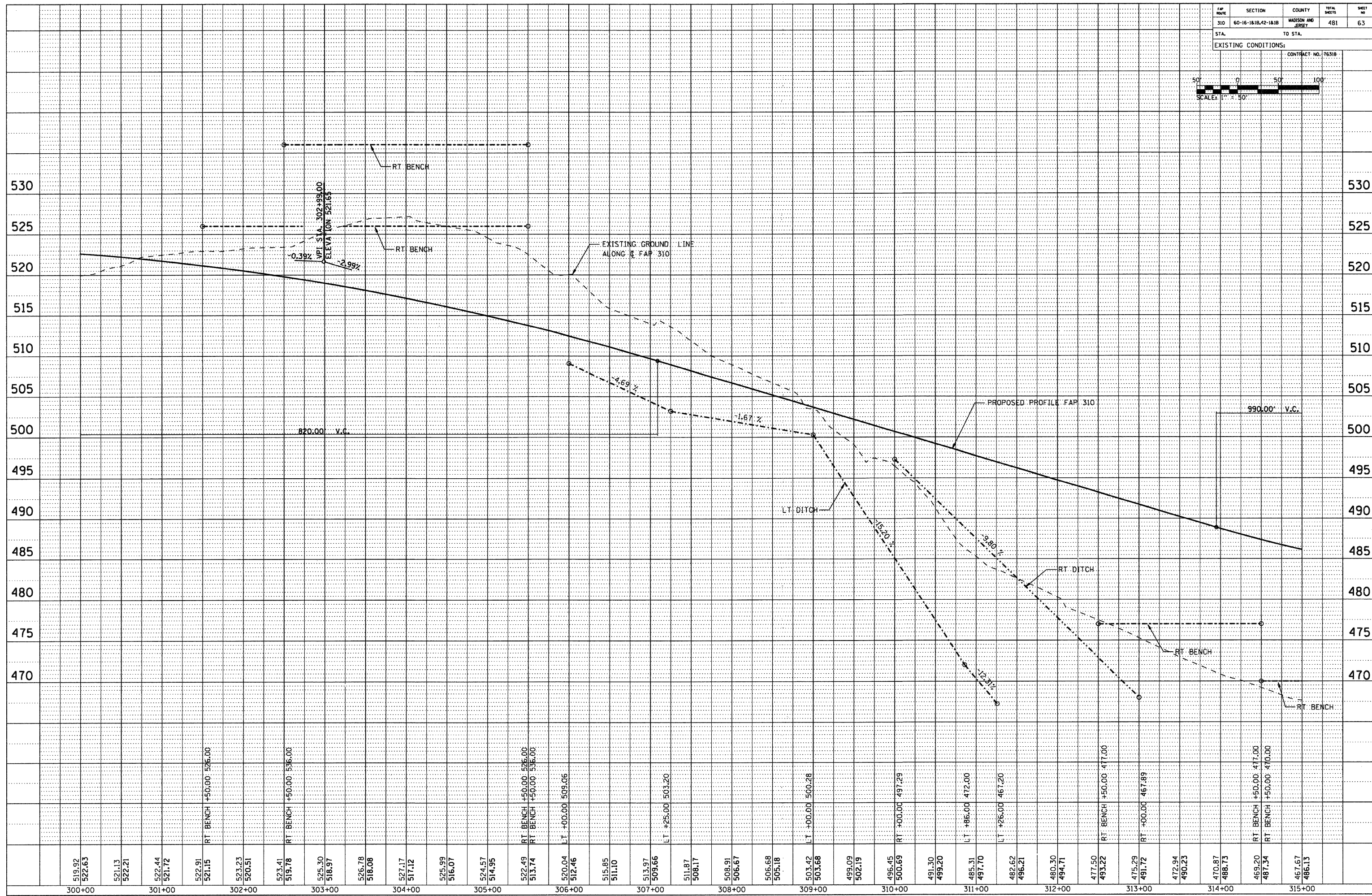
ILLINOIS DEPARTMENT OF TRANSPORTATION
ROADWAY PLAN STA 300+00 TO 315+00
 FAP ROUTE 310
 SECTION 60-16-181B,42-181B
 MADISON/JERSEY COUNTY
 DRAWN BY: EMR CHECKED BY: GLF

F.P. ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-181B/42-181B	MADISON AND JERSEY	481	63
STA. TO STA.		CONTRACT NO. 7631B		



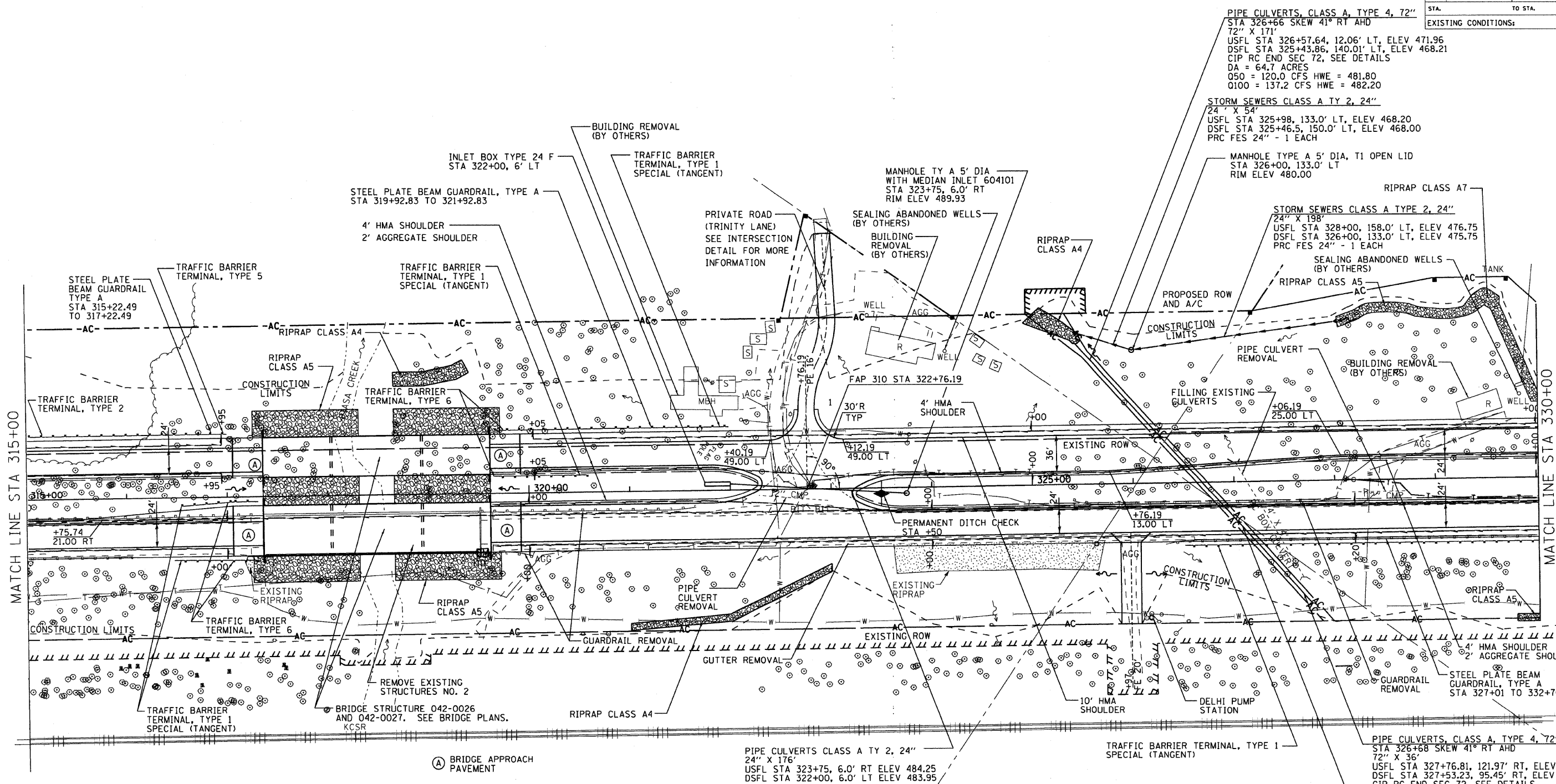
PLAN	REVIEWED	BY	DATE
NOTE BOOK NO.	GRAPHS CHECKED		
	ALIGNED		
	PLOTTED		
	CAD FILE NAME		

PROFILE	REVIEWED	BY	DATE
NOTE BOOK NO.	GRAPHS CHECKED		
	STRUCTURE		
	NOTATIONS CHD		



STA 300+00 TO STA 315+00

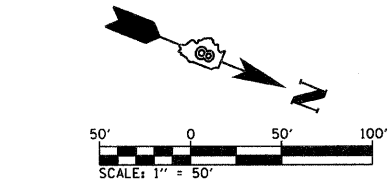
DATE	
BY	
REVISIONS	
PLANNED	
CHECKED	
IN CHARGE	
NO.	



BM: 110 CHISLED "□" NE CORNER WINGWALL EAST SIDE RTE. 267 BRIDGE OVER PIASA CREEK STA 319+53, 59' RT ELEV. 475.07

PIPE CULVERTS CLASS A TY 2, 24" 24" X 176' USFL STA 323+75, 6.0' RT ELEV 484.25 DSFL STA 322+00, 6.0' LT ELEV 483.95

PIPE CULVERTS, CLASS A, 72" (JACKED) STA 326+66 SKEW 41° RT AHD 72" X 144' USFL STA 327+53.23, 95.45' RT, ELEV 475.11 DSFL STA 326+57.64, 12.06' LT, ELEV 471.96 DA = 64.7 ACRES Q50 = 120.0 CFS HWE = 481.80 Q100 = 137.2 CFS HWE = 482.20



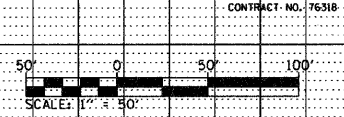
ILLINOIS DEPARTMENT OF TRANSPORTATION
ROADWAY PLAN STA 315+00 TO 330+00

FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON/JERSEY COUNTY

DRAWN BY: EMR CHECKED BY: GLF

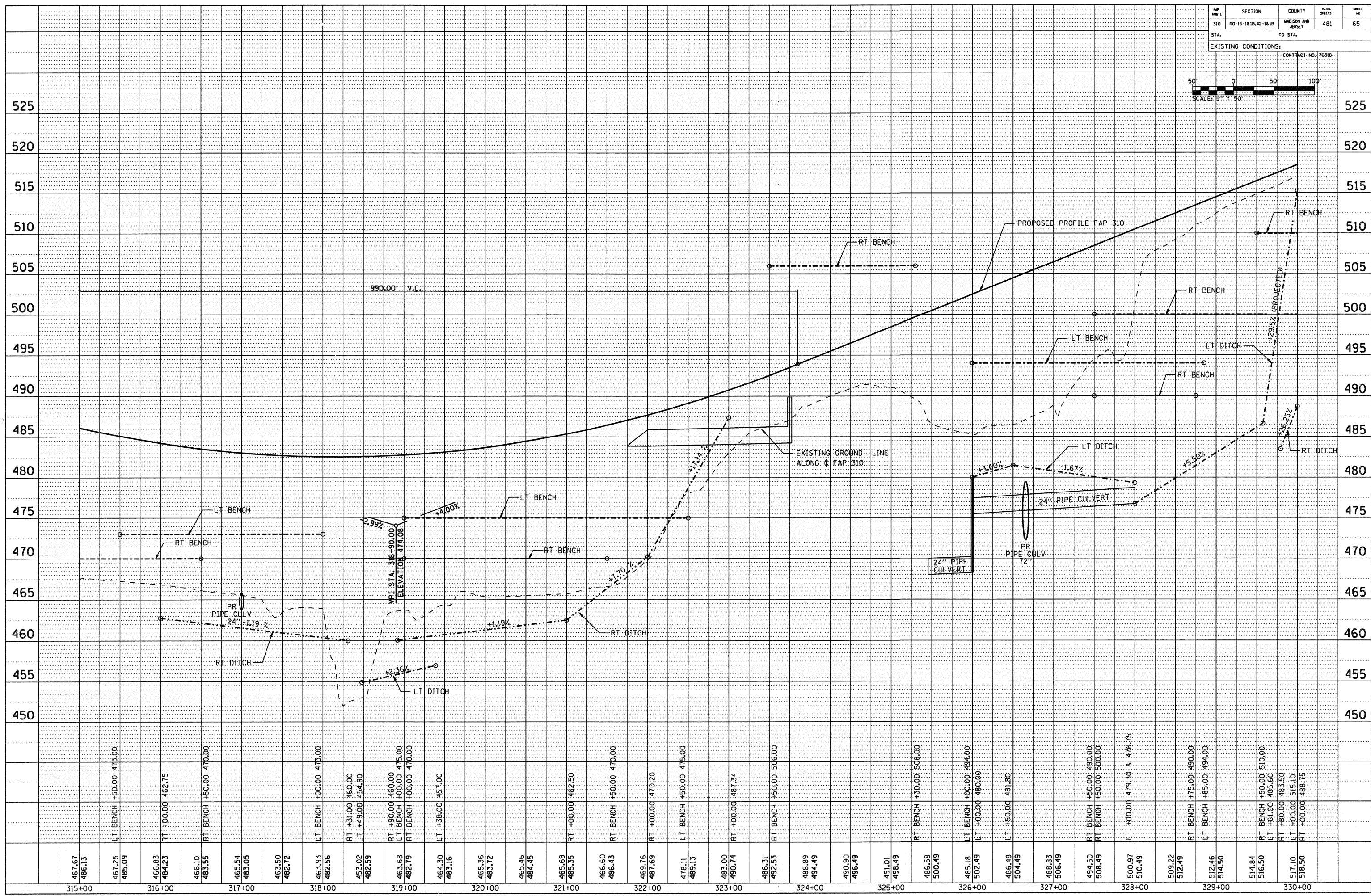
REVISIONS	
NAME	DATE

EXISTING CONDITIONS:

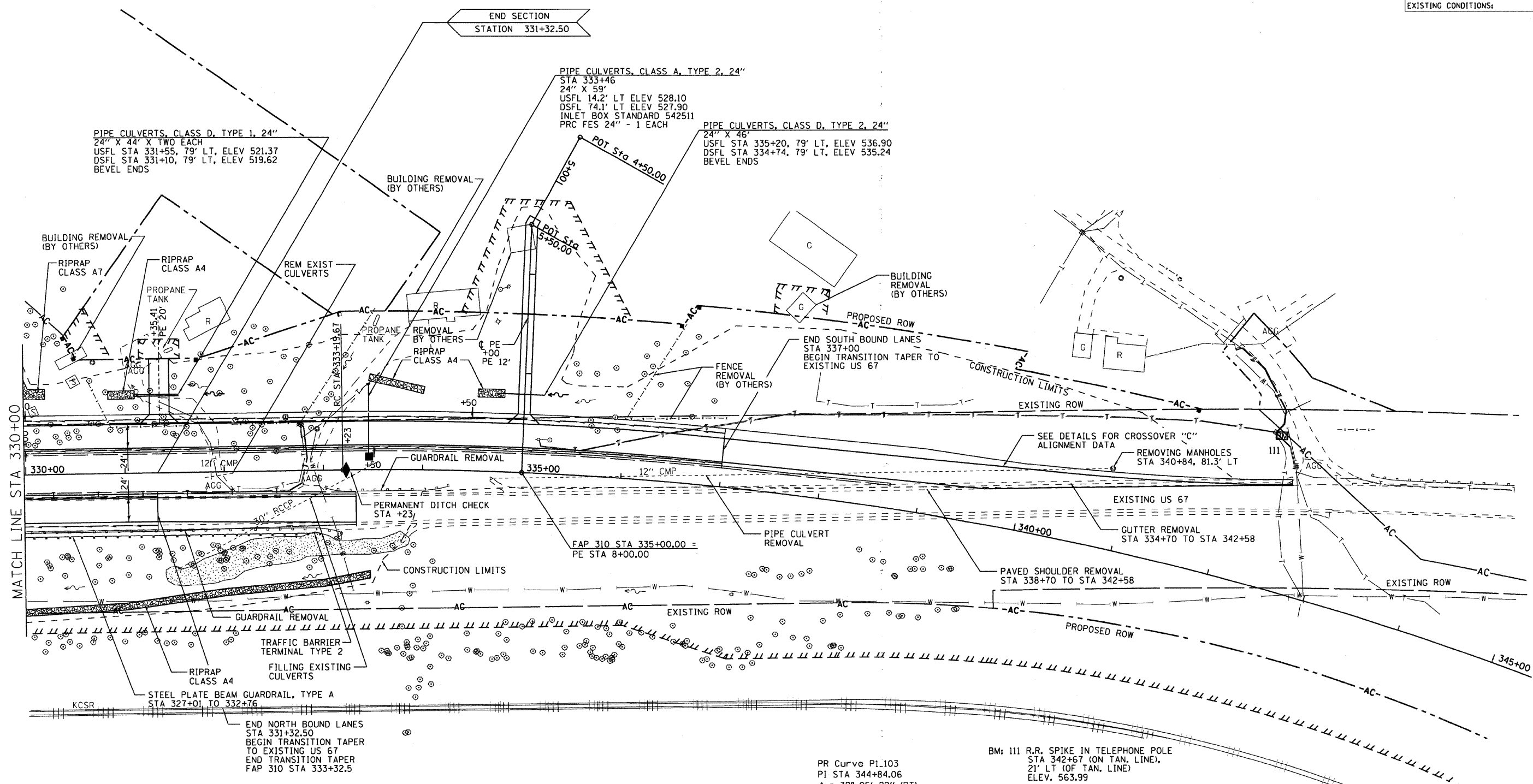


PLAN	REVIEWED	DATE
NO.	BY	
	CHECKED	
	DATE	

PROFILE	REVIEWED	DATE
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	DATE	

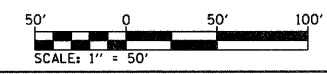


DATE	
BY	
REVISIONS	
NO.	
DESCRIPTION	
DATE	
BY	
REVISIONS	
NO.	
DESCRIPTION	
DATE	
BY	



PR Curve P1.103
 PI STA 344+84.06
 $\Delta = 39^\circ 05' 22''$ (RT)
 $D = 1^\circ 44' 49''$
 $T = 1,164.39'$
 $R = 3,280.00'$
 $L = 2,237.74'$
 $E = 200.55'$
 $e = 0.05$
 TR IN = 45 FT
 SE RUN IN = 331+77.50 TO 333+90.75
 SE RUN OUT = 354+86.33 TO 356+99.58
 TR OUT = 45 FT
 PC STA 333+19.67
 PT STA 355+57.41

BM: 111 R.R. SPIKE IN TELEPHONE POLE
 STA 342+67 (ON TAN. LINE),
 21' LT (OF TAN. LINE)
 ELEV. 563.99



ILLINOIS DEPARTMENT OF TRANSPORTATION

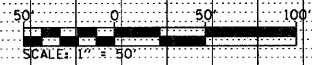
ROADWAY PLAN STA 330+00 TO 337+00

FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON/JERSEY COUNTY

DRAWN BY: EMR CHECKED BY: GLF

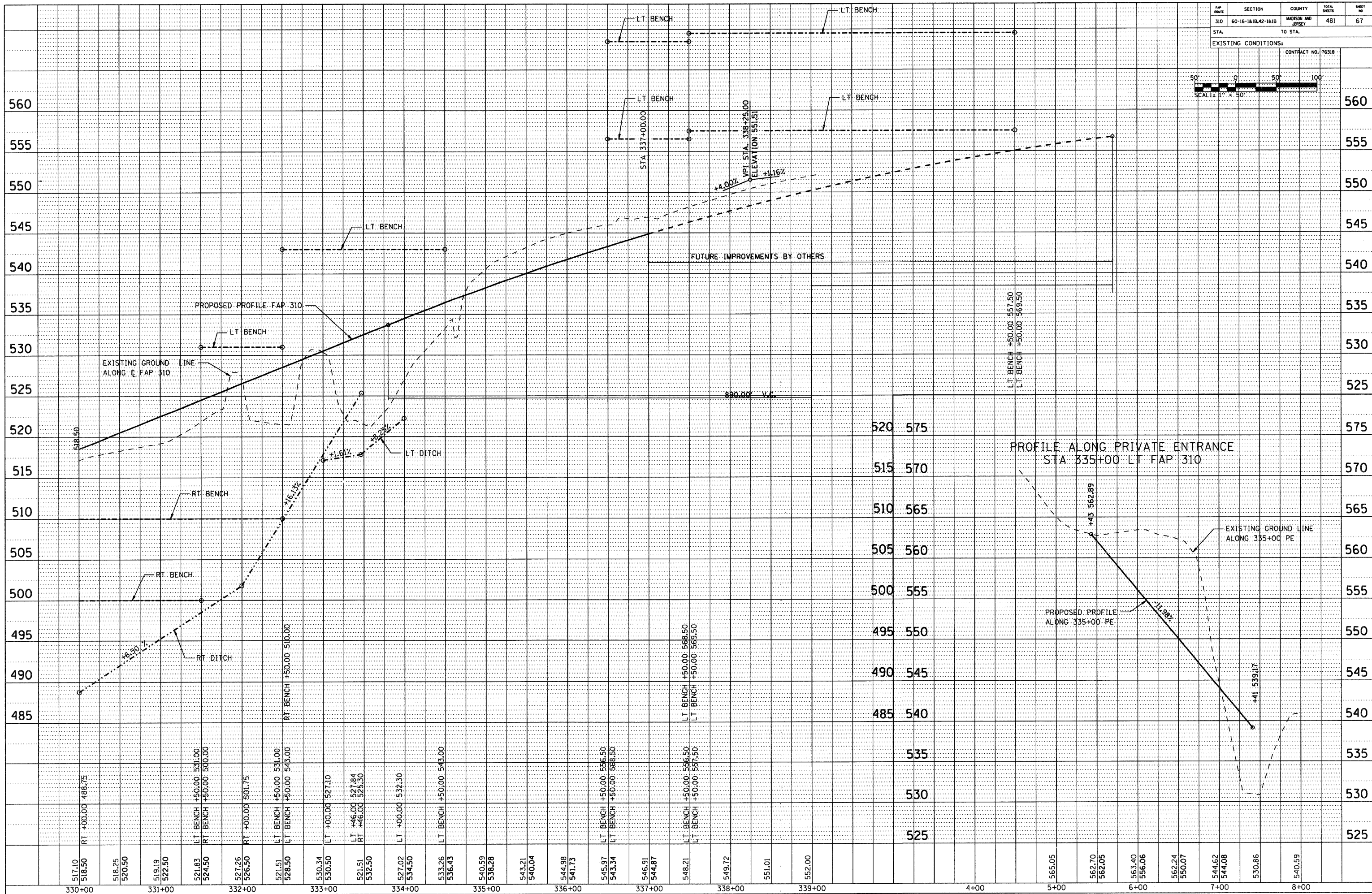
REVISIONS	
NAME	DATE

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	67
STA. _____ TO STA. _____		EXISTING CONDITIONS:		
CONTRACT NO. 76318				



PLAN	SURVEYED	BY	DATE
	PLOTTED		
	NOTED		
	NO. _____		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	NOTED		
	NO. _____		



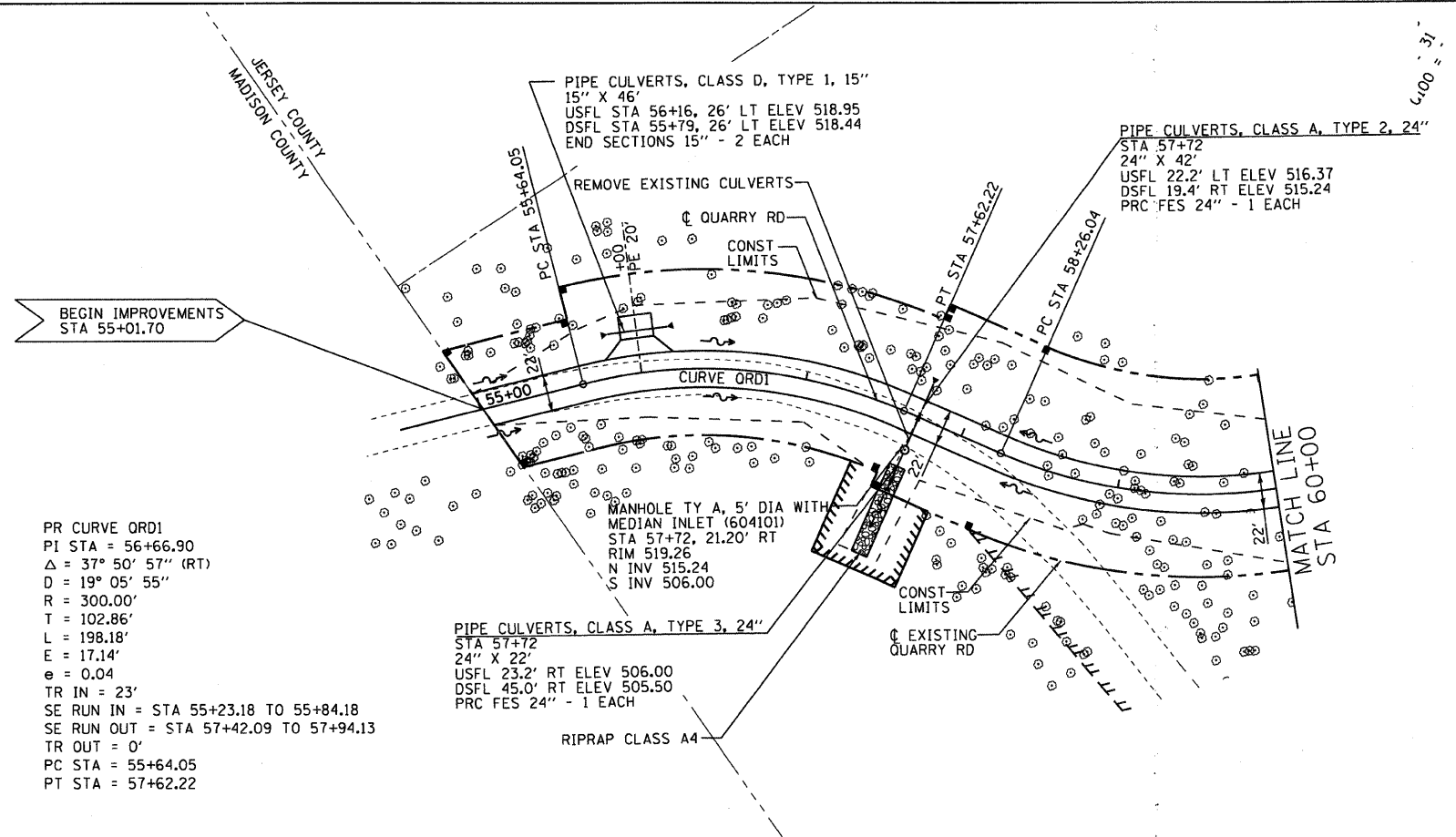
STA 330+00 TO STA 335+00

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-181B,42-181B	MADISON AND JERSEY	481	68
STA		TO STA		
EXISTING CONDITIONS:				

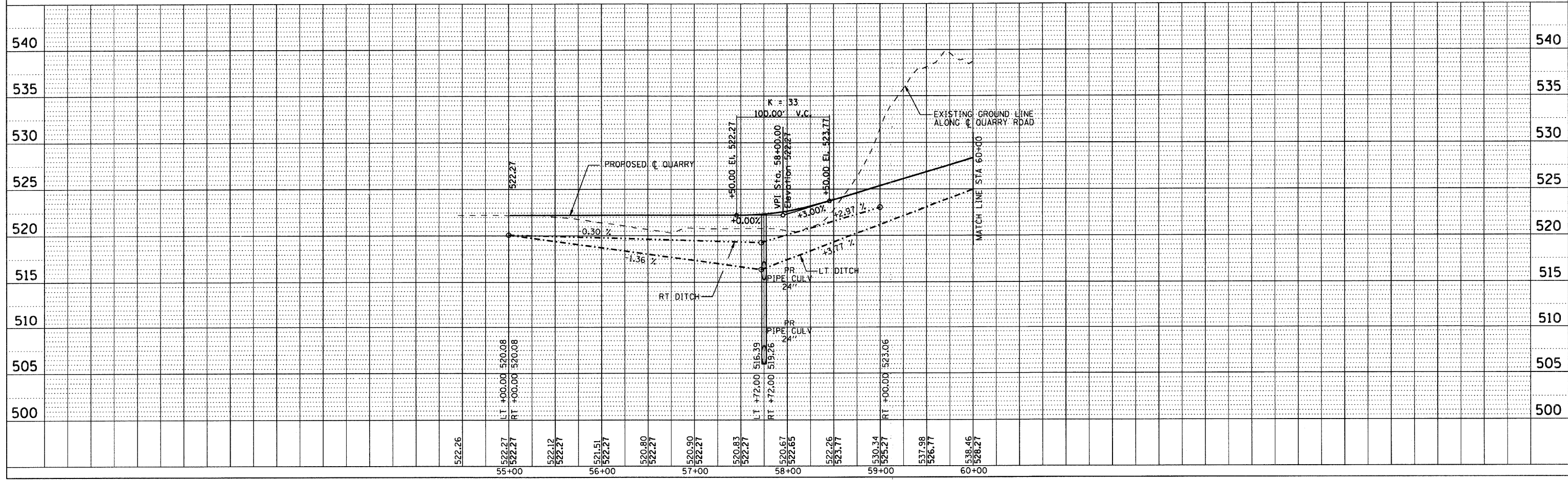
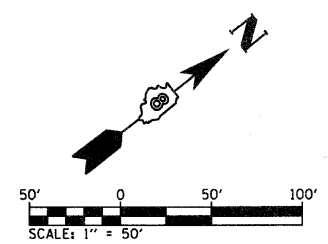
CONTRACT NO. 76318

PLAN	SURVEYED	DATE
NO.	BY	
	CHECKED	
	DATE	

PROFILE	SURVEYED	DATE
NO.	BY	
	CHECKED	
	DATE	



PR CURVE ORD1
 PI STA = 56+66.90
 $\Delta = 37^\circ 50' 57''$ (RT)
 $D = 19^\circ 05' 55''$
 $R = 300.00'$
 $L = 102.86'$
 $E = 17.14'$
 $e = 0.04$
 $TR\ IN = 23'$
 $SE\ RUN\ IN = STA\ 55+23.18\ TO\ 55+84.18$
 $SE\ RUN\ OUT = STA\ 57+42.09\ TO\ 57+94.13$
 $TR\ OUT = 0'$
 $PC\ STA = 55+64.05$
 $PT\ STA = 57+62.22$



QUARRY ROAD

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-181B,42-181B	MADISON AND JERSEY	481	69
STA	TO STA			
EXISTING CONDITIONS:				

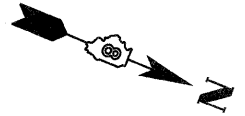
CONTRACT NO. 76318

- ① PIPE CULVERTS, CLASS A, TYPE 2, 48" STA 63+00 48" X 78' (EXCLUDES PIPE TEE)
USFL STA 63+00, 39.3' RT ELEV 523.50
DSFL STA 63+00, 38.7' LT ELEV 523.27
PRC FES - 1 EACH
MANHOLE TY A, 6' DIA WITH MEDIAN INLET STANDARD 604101
RIM ELEV 530.52
DA = 14.6 ACRES
050 = 31.06 CFS HWE 526.00
0100 = 35.48 CFS HWE 526.20
- ② PIPE CULVERTS, CLASS D, TYPE 2, 18" 18" X 62'
USFL STA 65+29.0, 43' LT ELEV 533.67
DSFL STA 64+67.0, 43' LT ELEV 532.57
END SECTIONS 18" - 2 EACH

PR CURVE ORD2
PI STA = 60+88.24
 $\Delta = 82^\circ 18' 30''$ (LT)
D = 19° 05' 55"
R = 300.00'
T = 262.21'
L = 430.96'
E = 98.44'
e = 0.04
TR IN = 0
SE RUN IN = STA 57+94.13 TO 58+46.17
SE RUN OUT = STA 62+36.87 TO 62+97.87
PC STA = 58+26.04
PT STA = 62+57.00

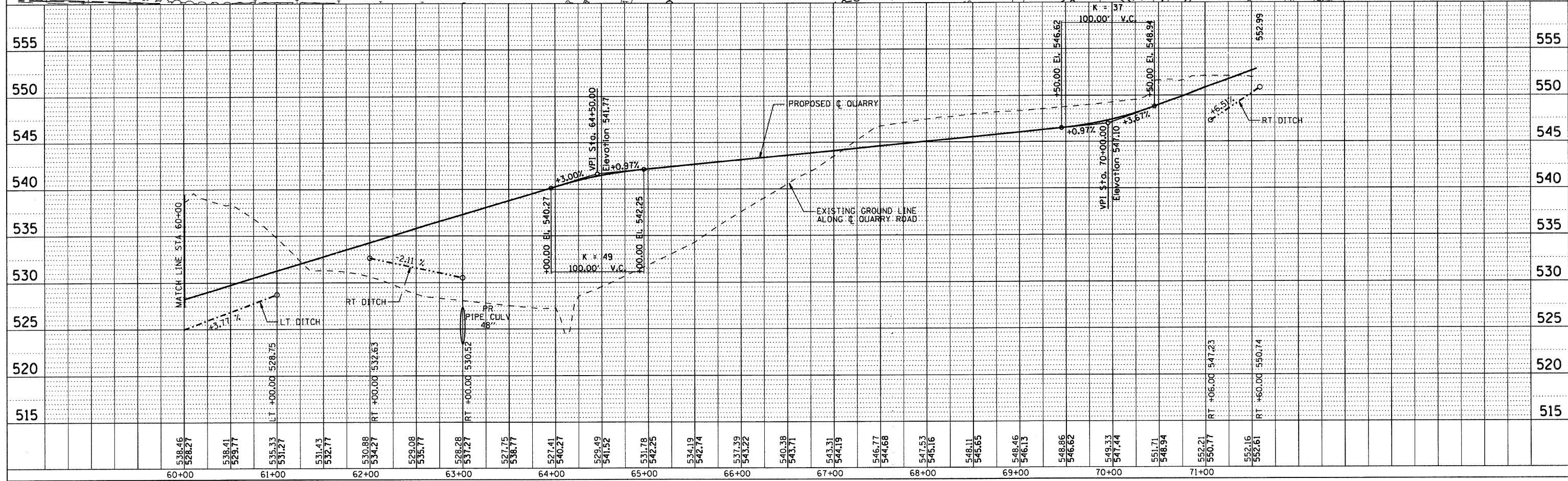
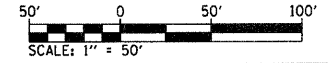
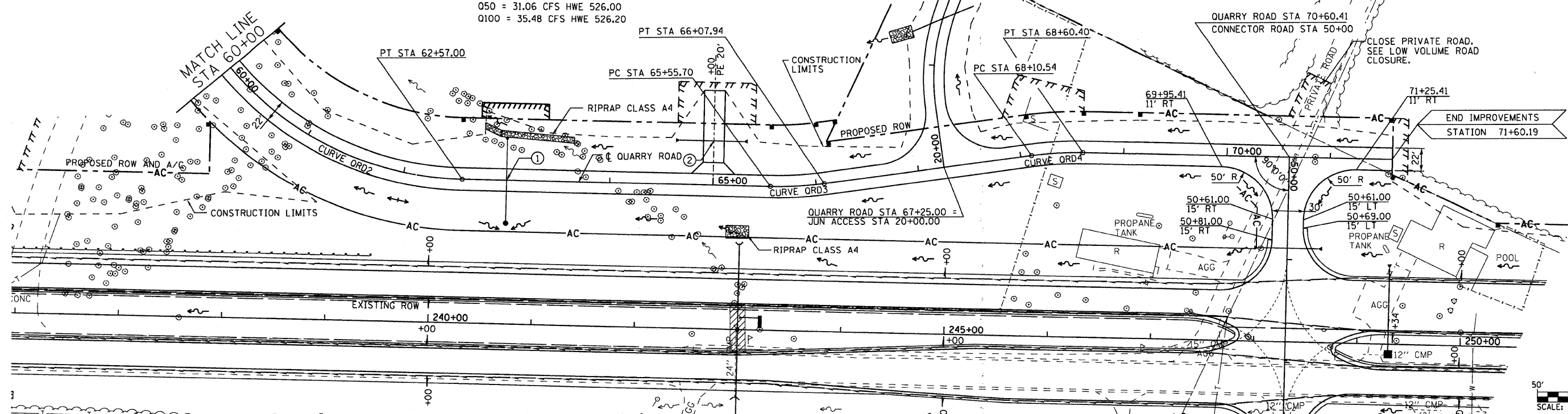
PR CURVE ORD3
PI STA = 65+81.87
 $\Delta = 9^\circ 04' 10''$ (LT)
D = 17° 21' 44"
R = 330.00'
T = 26.17'
L = 52.24'
E = 1.04'
e = N/A
TR IN = N/A
SE RUN = N/A
PC STA = 65+55.70
PT STA = 66+07.94

PR CURVE ORD4
PI STA = 68+35.52
 $\Delta = 9^\circ 04' 10''$ (RT)
D = 18° 11' 21"
R = 315.00'
T = 24.98'
L = 49.86'
E = 0.99'
e = N/A
TR IN = N/A
SE RUN = N/A
PC STA = 68+10.54
PT STA = 68+60.40



DATE	
BY	
PLAN	
SURVEYED	
PLOTTED	
GRADES CHECKED	
STRUCTURE NOTATIONS CHECKED	
NOTE BOOK NO.	
FILE NAME	

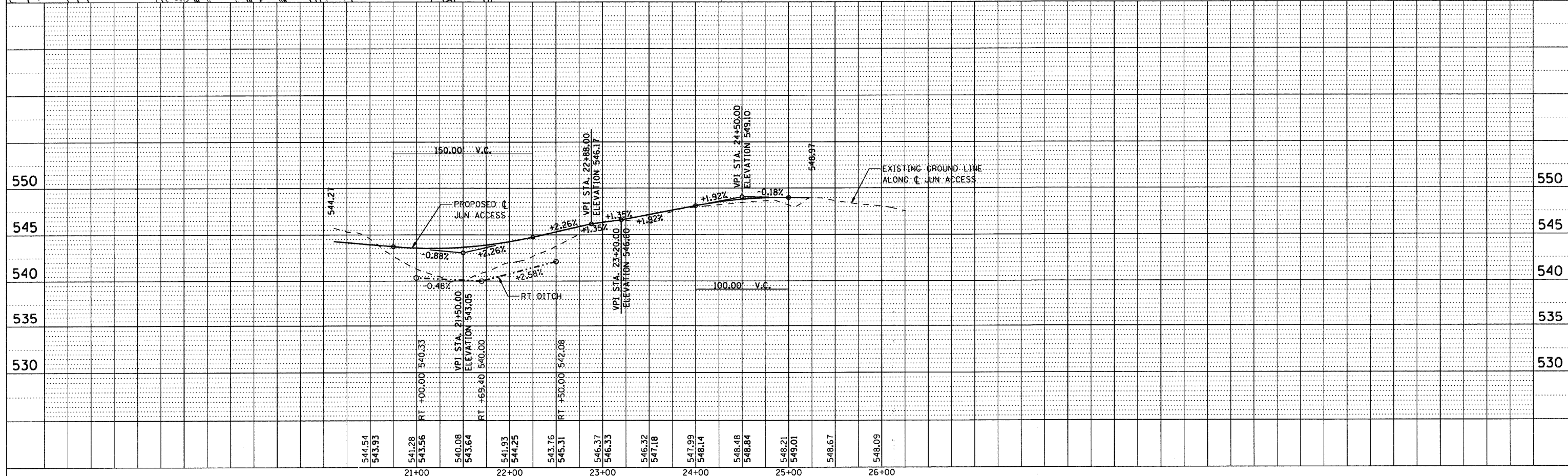
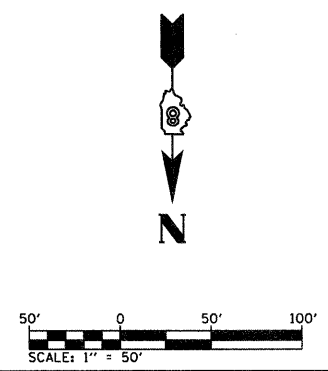
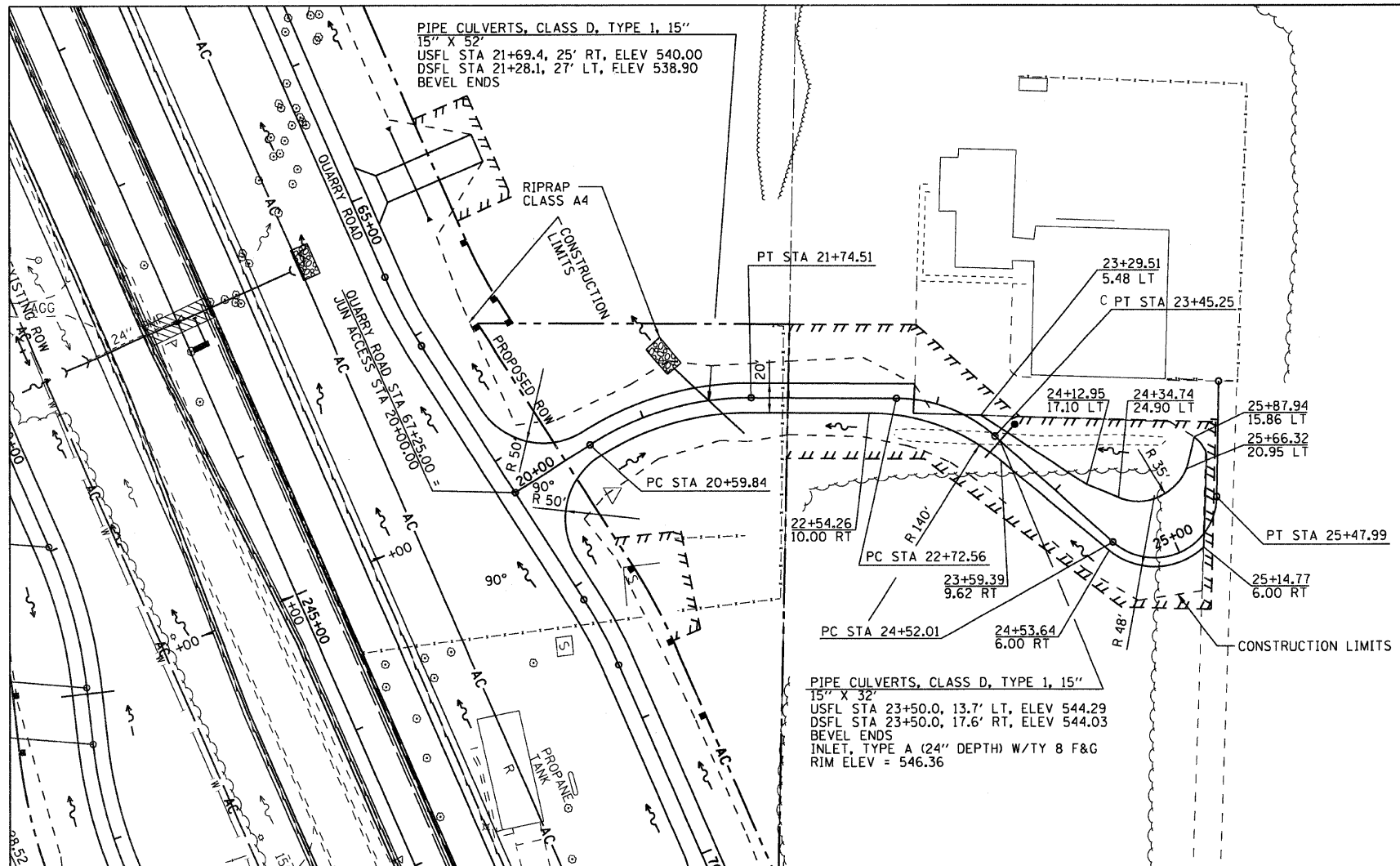
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STRUCTURE NOTATIONS CHECKED	
NOTE BOOK NO.	
FILE NAME	



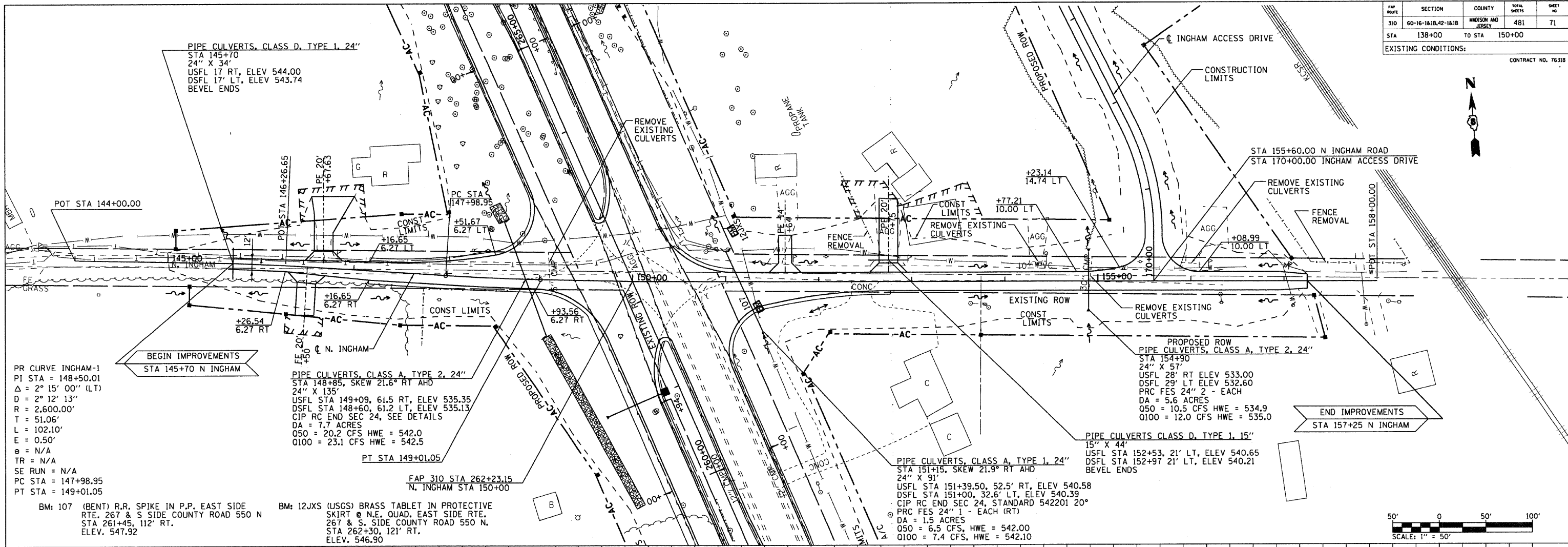
F&P ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-181B, 42-181B	MADISON AND JERSEY	481	70
STA		TO STA		
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

PLAN	DATE
REVIEWED	
PLOTTED	
NOTE BOOK	
NO.	

PROFILE	DATE
REVIEWED	
PLOTTED	
NOTE BOOK	
NO.	



FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	71
STA 138+00		TO STA 150+00		
EXISTING CONDITIONS:				
CONTRACT NO. 76318				



PR CURVE INGHAM-1
 PI STA = 148+50.01
 $\Delta = 2^\circ 15' 00''$ (LT)
 $D = 2^\circ 12' 13''$
 $R = 2,600.00'$
 $T = 51.06'$
 $L = 102.10'$
 $E = 0.50'$
 $e = N/A$
 $TR = N/A$
 $SE RUN = N/A$
 $PC STA = 147+98.95$
 $PT STA = 149+01.05$

BEGIN IMPROVEMENTS
 STA 145+70 N INGHAM

PIPE CULVERTS, CLASS A, TYPE 2, 24"
 STA 148+85, SKEW 21.6° RT AHD
 24" X 135'
 USFL STA 149+09, 61.5 RT, ELEV 535.35
 DSFL STA 148+60, 61.2 LT, ELEV 535.13
 CIP RC END SEC 24, SEE DETAILS
 DA = 7.7 ACRES
 050 = 20.2 CFS HWE = 542.0
 0100 = 23.1 CFS HWE = 542.5

PT STA 149+01.05

FAP 310 STA 262+23.15
 N. INGHAM STA 150+00

PIPE CULVERTS, CLASS A, TYPE 1, 24"
 STA 151+15, SKEW 21.9° RT AHD
 24" X 91'
 USFL STA 151+39.50, 52.5' RT, ELEV 540.58
 DSFL STA 151+00, 32.6' LT, ELEV 540.39
 CIP RC END SEC 24, STANDARD 542201 20°
 PRC FES 24" 1 - EACH (RT)
 DA = 1.5 ACRES
 050 = 6.5 CFS, HWE = 542.00
 0100 = 7.4 CFS, HWE = 542.10

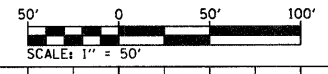
PIPE CULVERTS CLASS D, TYPE 1, 15"
 15" X 44'
 USFL STA 152+53, 21' LT, ELEV 540.65
 DSFL STA 152+97 21' LT, ELEV 540.21
 BEVEL ENDS

PROPOSED ROW
 PIPE CULVERTS, CLASS A, TYPE 2, 24"
 STA 154+90
 24" X 57'
 USFL 28' RT ELEV 533.00
 DSFL 29' LT ELEV 532.60
 PRC FES 24" 2 - EACH
 DA = 5.6 ACRES
 050 = 10.5 CFS HWE = 534.9
 0100 = 12.0 CFS HWE = 535.0

END IMPROVEMENTS
 STA 157+25 N INGHAM

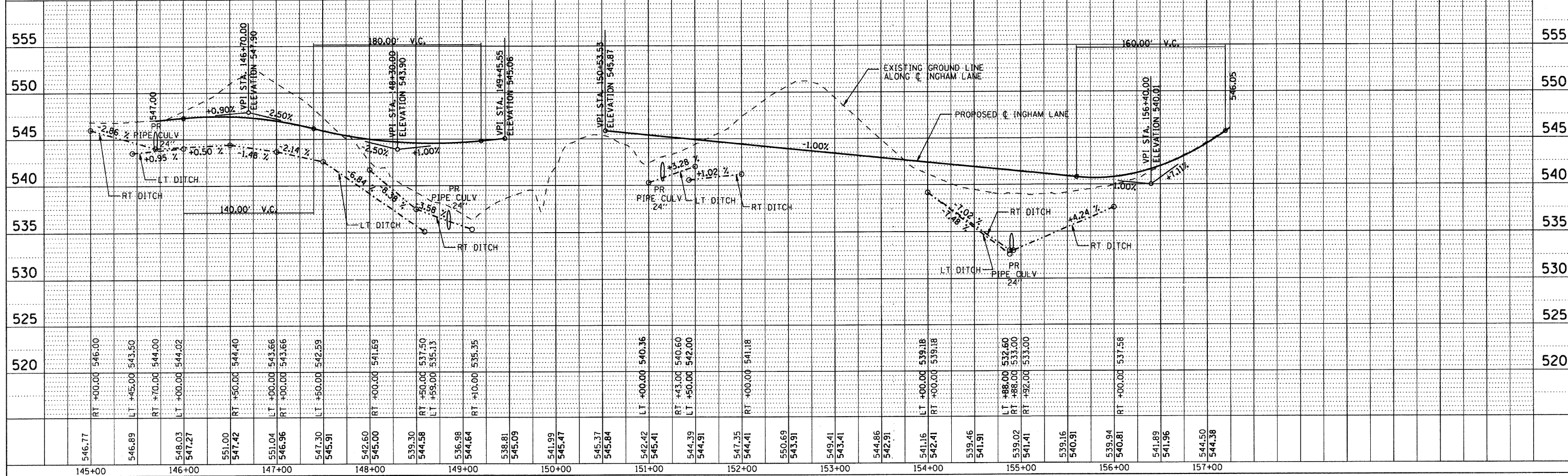
BM: 107 (BENT) R.R. SPIKE IN P.P. EAST SIDE
 RTE. 267 & S SIDE COUNTY ROAD 550 N
 STA 261+45, 112' RT.
 ELEV. 547.92

BM: 12JXS (USGS) BRASS TABLET IN PROTECTIVE
 SKIRT @ N.E. QUAD, EAST SIDE RTE.
 267 & S. SIDE COUNTY ROAD 550 N.
 STA 262+30, 121' RT.
 ELEV. 546.90



DATE	BY	REVIEWED	PLOTTED	CHECKED	NO. OF WAY CHECKED	PAID FILE NAME

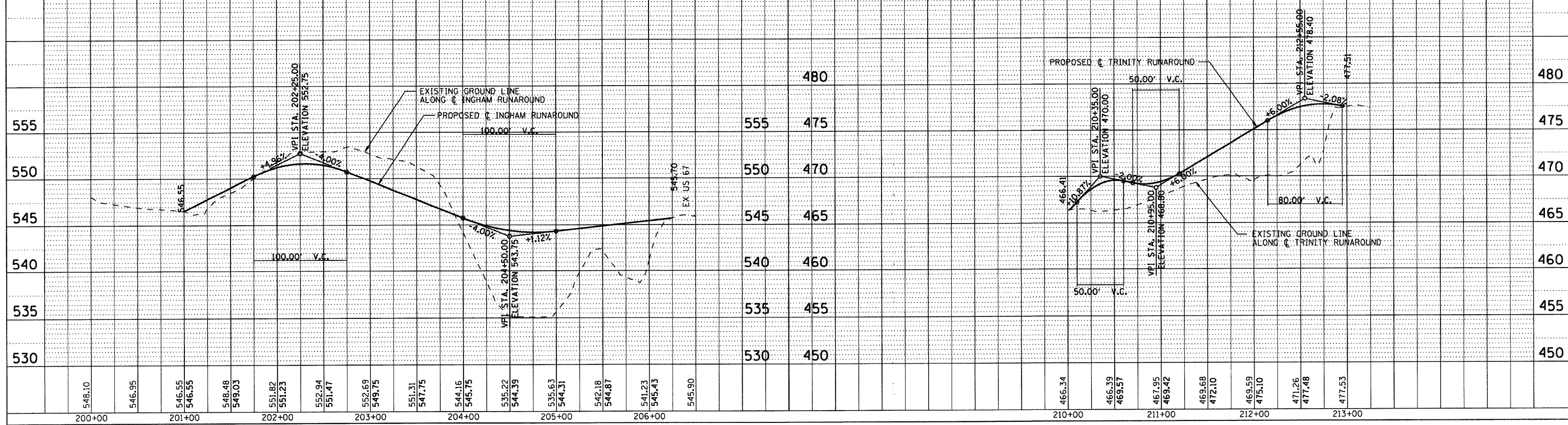
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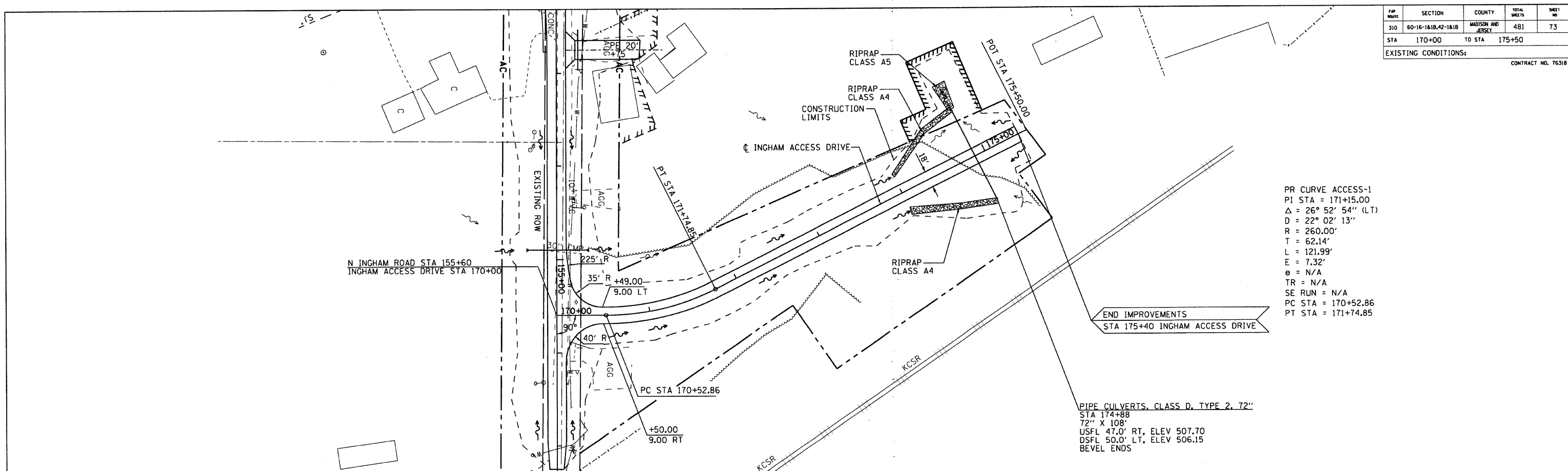
DATE	BY	DATE

PROFILE	REVISED	DATE

PLAN	REVISED	DATE



NORTH INGHAM RUNAROUND/TRINITY RUNAROUND



FWP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
60-16-18B, 42-181B	MADISON AND JERSEY	481	73	
STA 170+00	TO STA 175+50	EXISTING CONDITIONS:		

PR CURVE ACCESS-1
 PI STA = 171+15.00
 $\Delta = 26^\circ 52' 54''$ (LT)
 $D = 22^\circ 02' 13''$
 $R = 260.00'$
 $T = 62.14'$
 $L = 121.99'$
 $E = 7.32'$
 $e = N/A$
 $TR = N/A$
 $SE RUN = N/A$
 $PC STA = 170+52.86$
 $PT STA = 171+74.85$

END IMPROVEMENTS
 STA 175+40 INGHAM ACCESS DRIVE

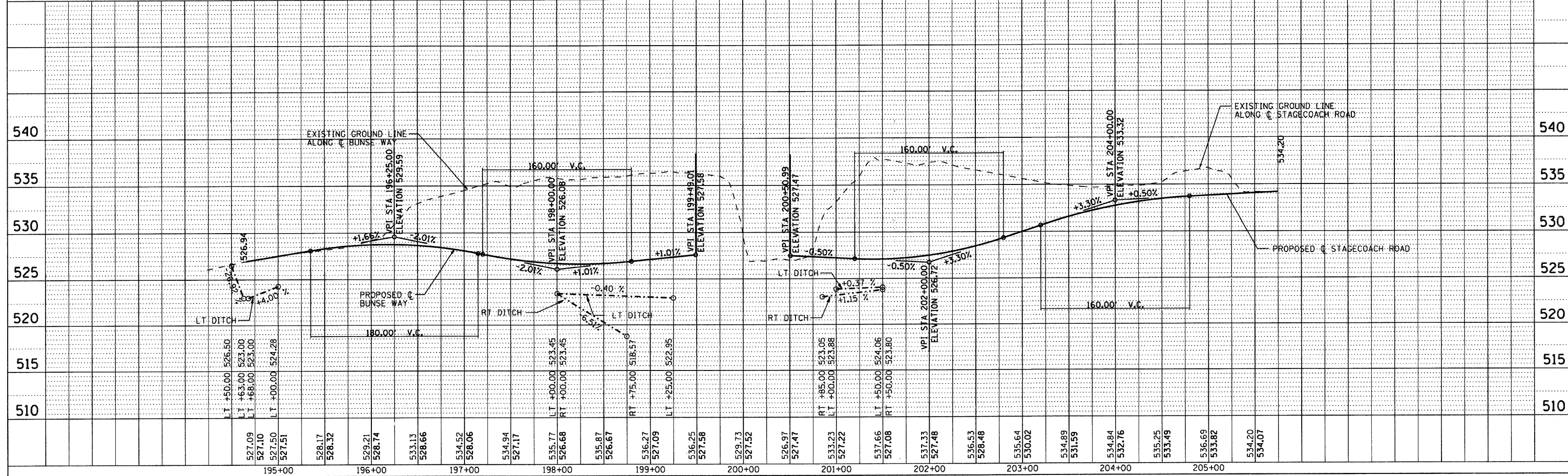
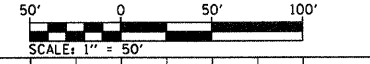
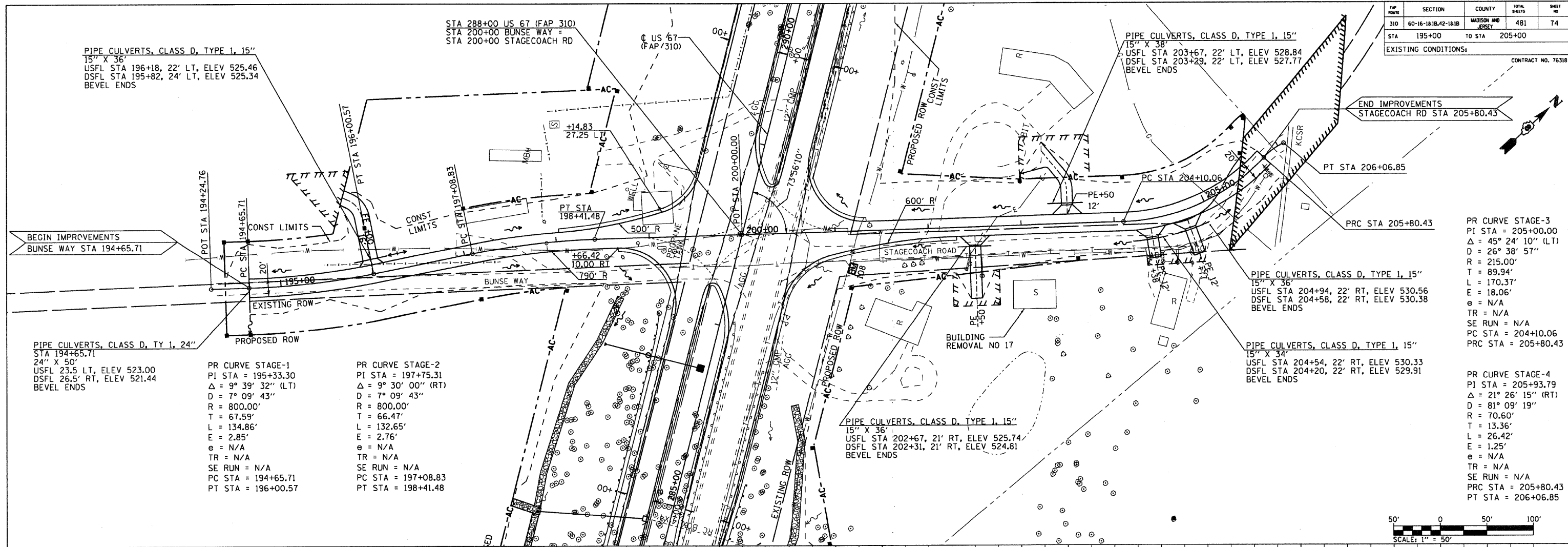
PIPE CULVERTS, CLASS D, TYPE 2, 72"
 STA 174+88
 72" X 108'
 USFL 47.0' RT, ELEV 507.70
 DSFL 50.0' LT, ELEV 506.15
 BEVEL ENDS

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1A1B,42-1A1B	MADISON AND JERSEY	481	74
STA 195+00		TO STA 205+00		

EXISTING CONDITIONS: CONTRACT NO. 76318

DATE	BY	REVISIONS
		DESIGNED
		PLOTTED
		ALIGNED
		CHECKED
		NO. _____

DATE	BY	REVISIONS
		DESIGNED
		PLOTTED
		GRADES CHECKED
		STRUCTURE NOTATIONS CHECKED
		NO. _____



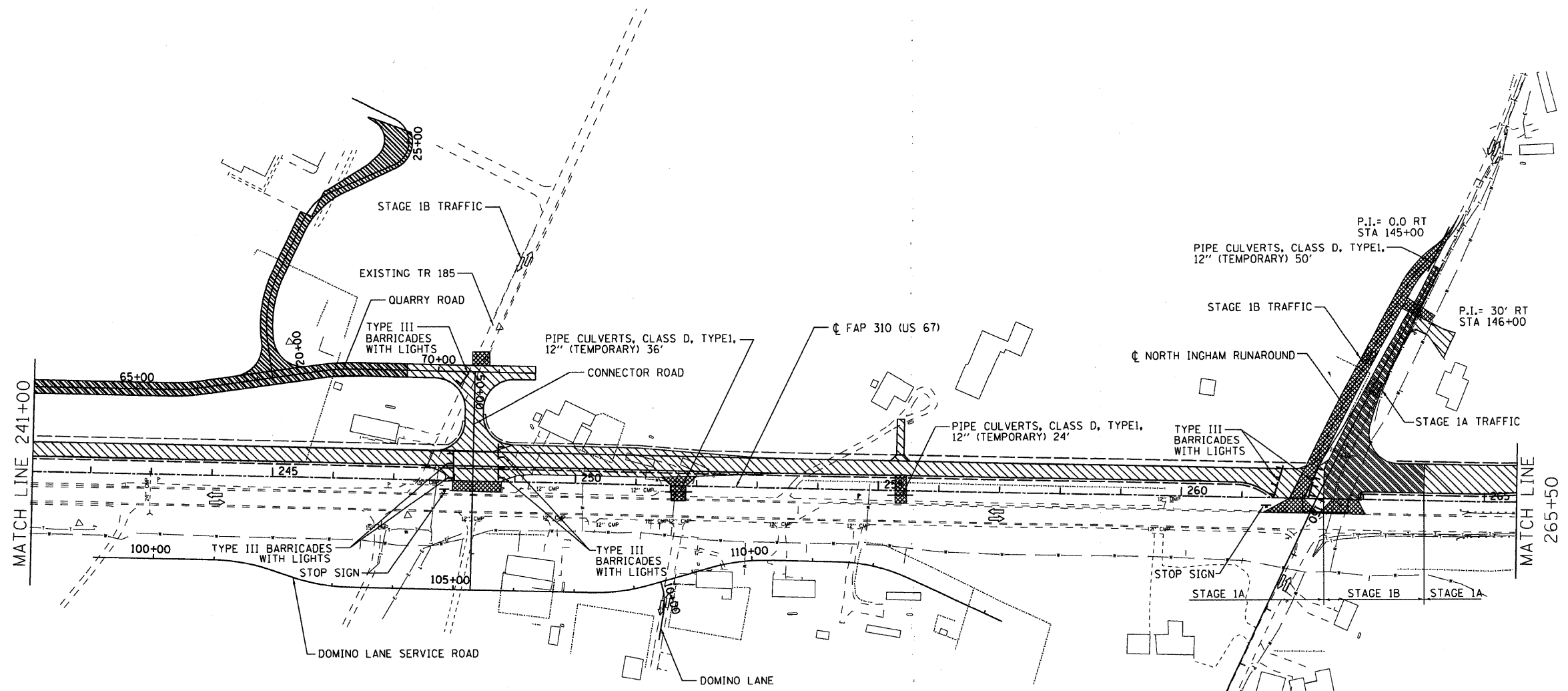
BUNSE WAY / STAGECOACH ROAD

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	76
STA	TO STA			

CONTRACT NO. 76318

EXISTING CONDITIONS:

DATE	
BY	
REVISIONS	
PLANNED	
DESIGNED	
CHECKED	
NO. OF WAY CHECKED	
CADD FILE NAME	
PLAN NO.	



CONSTRUCTION STAGING AND MAINTENANCE OF TRAFFIC

STAGE 1A

MAINTAIN TRAFFIC ON THE EXISTING LANES OF US 67, THE NORTH INGHAM RUNAROUND. CONSTRUCT THE PROPOSED FAP 310 SOUTHBOUND LANES, QUARRY ROAD, THE WESTERN PORTION OF THE CONNECTOR ROAD AND THE WESTERN PORTION OF NORTH INGHAM AS SHOWN.

1. THE PROPOSED SOUTHBOUND LANES OF FAP 310 PAVEMENT SHALL BE GAPPED AT EXISTING QUARRY ROAD UNTIL TRAFFIC HAS BEEN MOVED TO THE PROPOSED CONNECTOR ROAD AND THE CONNECTION TO TR 185 LEFT OF STATION 248+30.
2. USE TEMPORARY GRAVEL SURFACE TO CONNECT THE NEW FAP 310 SOUTHBOUND LANES TO DOMINO LANE. DURING CONSTRUCTION OF THE TEMPORARY GRAVEL SURFACE ADJACENT TO US 67, TRAFFIC SHALL BE MAINTAINED IN ACCORDANCE WITH TRAFFIC CONTROL AND PROTECTION STANDARD 701011.
3. THE PROPOSED FAP 310 PAVEMENT SHALL BE GAPPED AT NORTH INGHAM UNTIL TRAFFIC HAS BEEN MOVED TO THE NORTH INGHAM RUNAROUND. DURING CONSTRUCTION OF THE TEMPORARY GRAVEL SURFACE ADJACENT TO US 67, TRAFFIC SHALL BE MAINTAINED IN ACCORDANCE WITH TRAFFIC CONTROL AND PROTECTION STANDARD 701011.



R1-1-3636

STAGE 1B

MAINTAIN TRAFFIC ON THE EXISTING LANES OF US 67. CONSTRUCT THE REMAINING PORTIONS OF THE PROPOSED FAP 310 SOUTHBOUND LANES AND THE WEST LEG OF NORTH INGHAM AS SHOWN.

1. DURING THE CONSTRUCTION OF NORTH INGHAM, TRAFFIC SHALL BE MAINTAINED ON THE RUNAROUND AS SHOWN IN THE PLANS IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF TRAFFIC CONTROL AND PROTECTION STANDARD 701331. TRAFFIC SHALL BE MAINTAINED ACROSS THE PROPOSED SOUTHBOUND LANES OF FAP 310 UTILIZING TYPE III BARRICADES AS SHOWN.
2. DURING THE CONSTRUCTION OF QUARRY ROAD, QUARRY TRAFFIC SHALL BE MAINTAINED ON EXISTING TR 185. TRAFFIC SHALL BE MAINTAINED ACROSS THE PROPOSED SOUTHBOUND LANES OF FAP 310 UTILIZING TYPE III BARRICADES AS SHOWN. UPON THE COMPLETION OF QUARRY ROAD CONSTRUCTION, OPEN IT TO TRAFFIC AND REMOVE THE CONNECTION TO TR 185.
3. UPON THE COMPLETION OF NORTH INGHAM CONSTRUCTION, OPEN IT TO TRAFFIC AND REMOVE THE RUNAROUND. MAINTAIN TRAFFIC IN ACCORDANCE WITH TRAFFIC CONTROL AND PROTECTION STANDARD 701301.

LEGEND

- TEMPORARY PAVEMENT
- WORK ZONE STAGE 1A
- WORK ZONE STAGE 1B
- DIRECTION OF TRAFFIC
- TYPE III BARRICADE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC
STAGE 1
 FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTY
 DESIGNED BY: WTS
 CHECKED BY: GLF
 DRAWN BY: EMR
 CHECKED BY: WTS

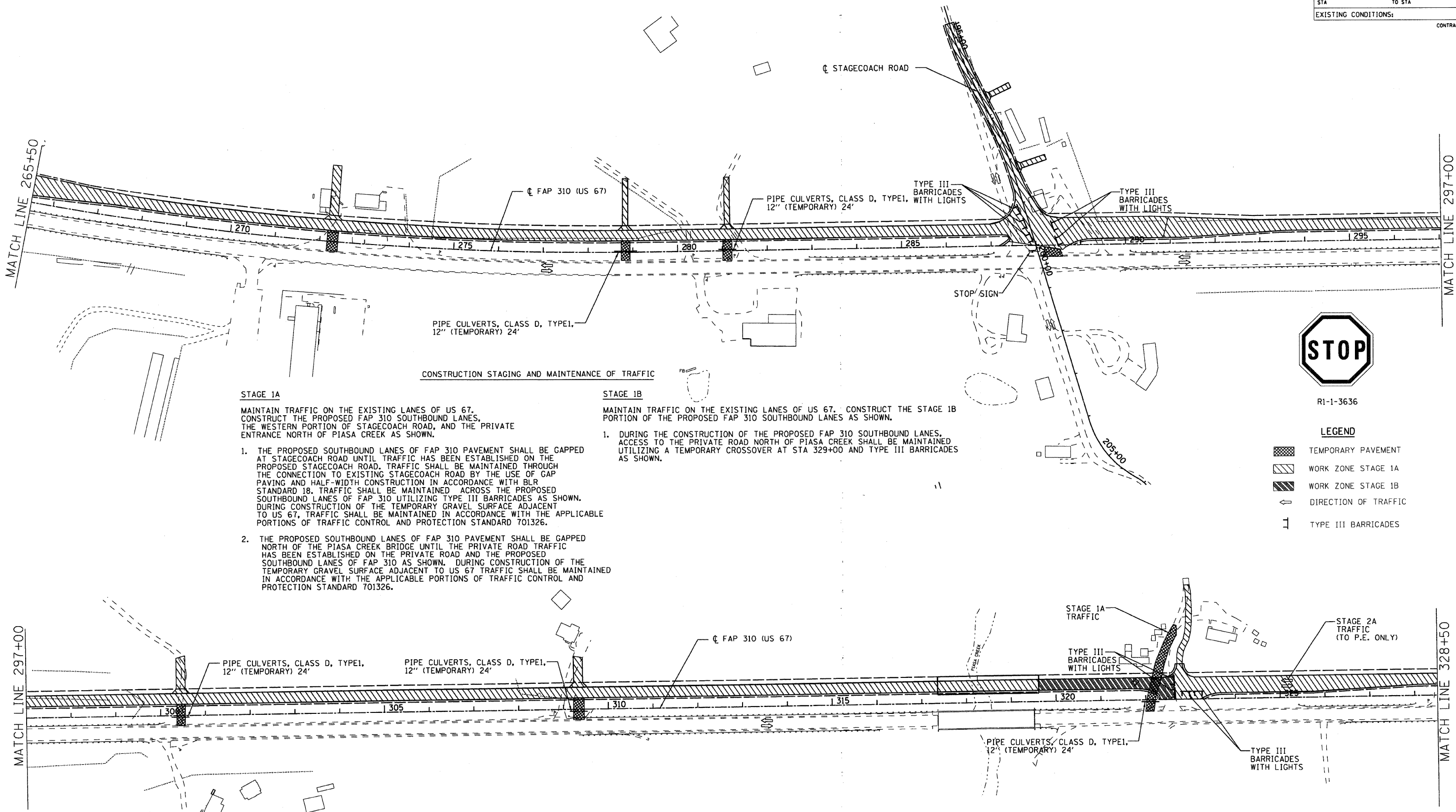
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FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	461	77
STA		TO STA		
EXISTING CONDITIONS:				

CONTRACT NO. 76318

PLAN	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NOTE BOOK	
	NO.	
	PAID FILE NAME	



STAGE 1A

MAINTAIN TRAFFIC ON THE EXISTING LANES OF US 67. CONSTRUCT THE PROPOSED FAP 310 SOUTHBOUND LANES, THE WESTERN PORTION OF STAGECOACH ROAD, AND THE PRIVATE ENTRANCE NORTH OF PIASA CREEK AS SHOWN.

1. THE PROPOSED SOUTHBOUND LANES OF FAP 310 PAVEMENT SHALL BE GAPPED AT STAGECOACH ROAD UNTIL TRAFFIC HAS BEEN ESTABLISHED ON THE PROPOSED STAGECOACH ROAD. TRAFFIC SHALL BE MAINTAINED THROUGH THE CONNECTION TO EXISTING STAGECOACH ROAD BY THE USE OF GAP PAVING AND HALF-WIDTH CONSTRUCTION IN ACCORDANCE WITH BLR STANDARD 18. TRAFFIC SHALL BE MAINTAINED ACROSS THE PROPOSED SOUTHBOUND LANES OF FAP 310 UTILIZING TYPE III BARRICADES AS SHOWN. DURING CONSTRUCTION OF THE TEMPORARY GRAVEL SURFACE ADJACENT TO US 67, TRAFFIC SHALL BE MAINTAINED IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF TRAFFIC CONTROL AND PROTECTION STANDARD 701326.
2. THE PROPOSED SOUTHBOUND LANES OF FAP 310 PAVEMENT SHALL BE GAPPED NORTH OF THE PIASA CREEK BRIDGE UNTIL THE PRIVATE ROAD TRAFFIC HAS BEEN ESTABLISHED ON THE PRIVATE ROAD AND THE PROPOSED SOUTHBOUND LANES OF FAP 310 AS SHOWN. DURING CONSTRUCTION OF THE TEMPORARY GRAVEL SURFACE ADJACENT TO US 67 TRAFFIC SHALL BE MAINTAINED IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF TRAFFIC CONTROL AND PROTECTION STANDARD 701326.

STAGE 1B

MAINTAIN TRAFFIC ON THE EXISTING LANES OF US 67. CONSTRUCT THE STAGE 1B PORTION OF THE PROPOSED FAP 310 SOUTHBOUND LANES AS SHOWN.

1. DURING THE CONSTRUCTION OF THE PROPOSED FAP 310 SOUTHBOUND LANES, ACCESS TO THE PRIVATE ROAD NORTH OF PIASA CREEK SHALL BE MAINTAINED UTILIZING A TEMPORARY CROSSOVER AT STA 329+00 AND TYPE III BARRICADES AS SHOWN.



R1-1-3636

LEGEND

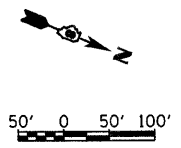
- TEMPORARY PAVEMENT
- WORK ZONE STAGE 1A
- WORK ZONE STAGE 1B
- DIRECTION OF TRAFFIC
- TYPE III BARRICADES

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC

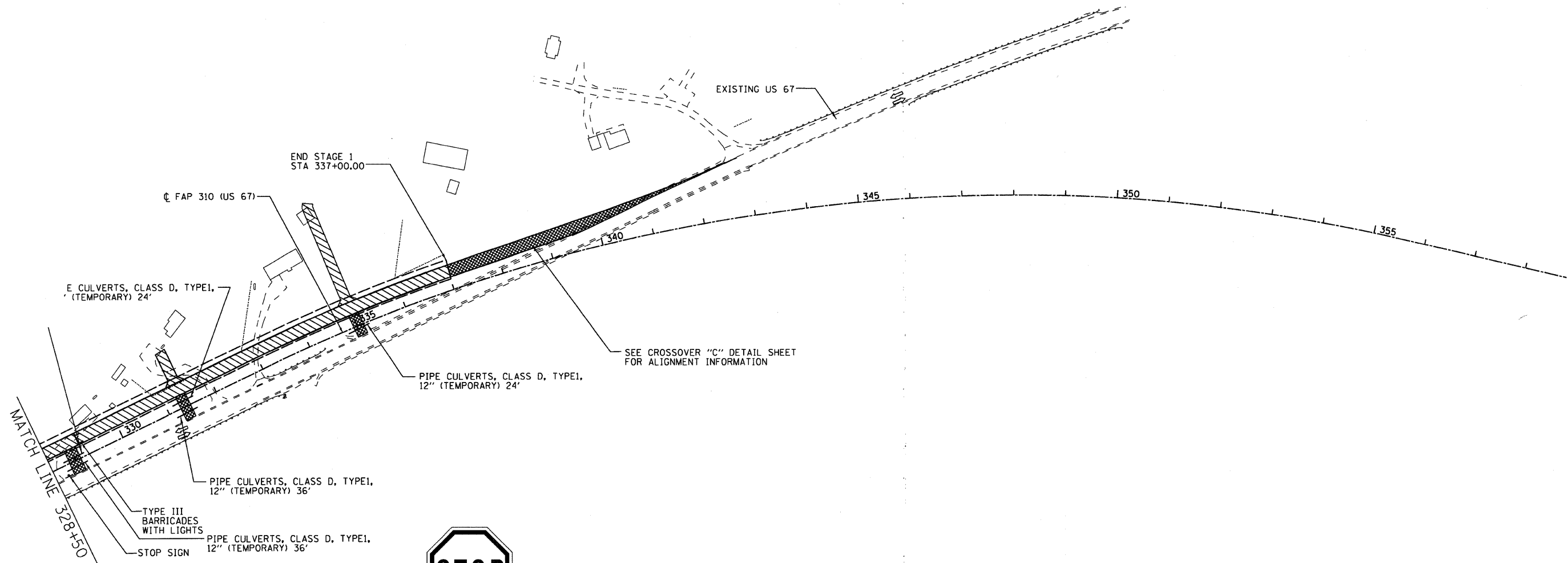
STAGE 1
FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON AND JERSEY COUNTY

DESIGNED BY: WTS
CHECKED BY: GLF
DRAWN BY: EMR
CHECKED BY: WTS



FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	78
STA		TO STA		
EXISTING CONDITIONS:				
CONTRACT NO. 76318				

PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	
	CHECKED	
	BY	
	FILE NAME	



R1-1-3636

CONSTRUCTION STAGING AND MAINTENANCE OF TRAFFIC

STAGE 1

MAINTAIN TRAFFIC ON THE EXISTING LANES OF US 67. CONSTRUCT THE PROPOSED FAP 310 SOUTHBOUND LANES AND CROSSOVER "C" AS SHOWN.

1. CONSTRUCT THE TEMPORARY CONNECTION BETWEEN THE PROPOSED SOUTHBOUND LANES OF FAP 310 AND EXISTING US 67. DURING CONSTRUCTION OF THE TEMPORARY CONNECTION ADJACENT TO US 67, TRAFFIC SHALL BE MAINTAINED IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF TRAFFIC CONTROL AND PROTECTION STANDARD 701326.

LEGEND

- TYPE III BARRICADES
- TEMPORARY PAVEMENT
- WORK ZONE STAGE 1A
- DIRECTION OF TRAFFIC

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC

STAGE 1

FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON AND JERSEY COUNTY

DESIGNED BY: WTS
CHECKED BY: GLF

DRAWN BY: EMR
CHECKED BY: WTS



50' 0 50' 100'

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	79
STA	TO STA			

EXISTING CONDITIONS:
CONTRACT NO. 16318

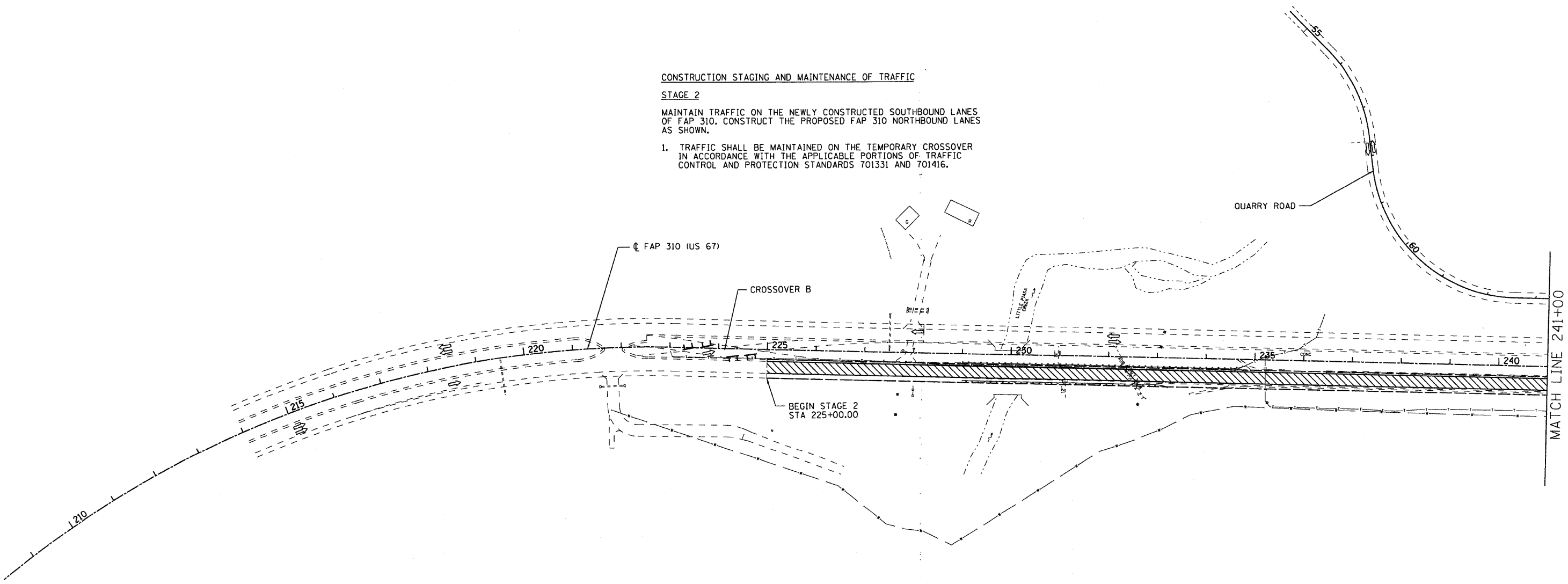
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NO.	PLOTTED	
	ALIGNED	
	CHECKED	
	ADD FILE NAME	

CONSTRUCTION STAGING AND MAINTENANCE OF TRAFFIC





STAGE 2

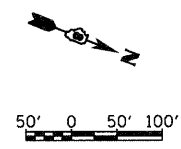
MAINTAIN TRAFFIC ON THE NEWLY CONSTRUCTED SOUTHBOUND LANES OF FAP 310. CONSTRUCT THE PROPOSED FAP 310 NORTHBOUND LANES AS SHOWN.

1. TRAFFIC SHALL BE MAINTAINED ON THE TEMPORARY CROSSOVER IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF TRAFFIC CONTROL AND PROTECTION STANDARDS 701331 AND 701416.



LEGEND

-  TEMPORARY PAVEMENT
-  WORK ZONE STAGE 2A
-  DIRECTION OF TRAFFIC
-  TYPE III BARRICADES



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC
STAGE 2
 FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTY

DESIGNED BY: WTS
 CHECKED BY: GLF

DRAWN BY: EMR
 CHECKED BY: WTS

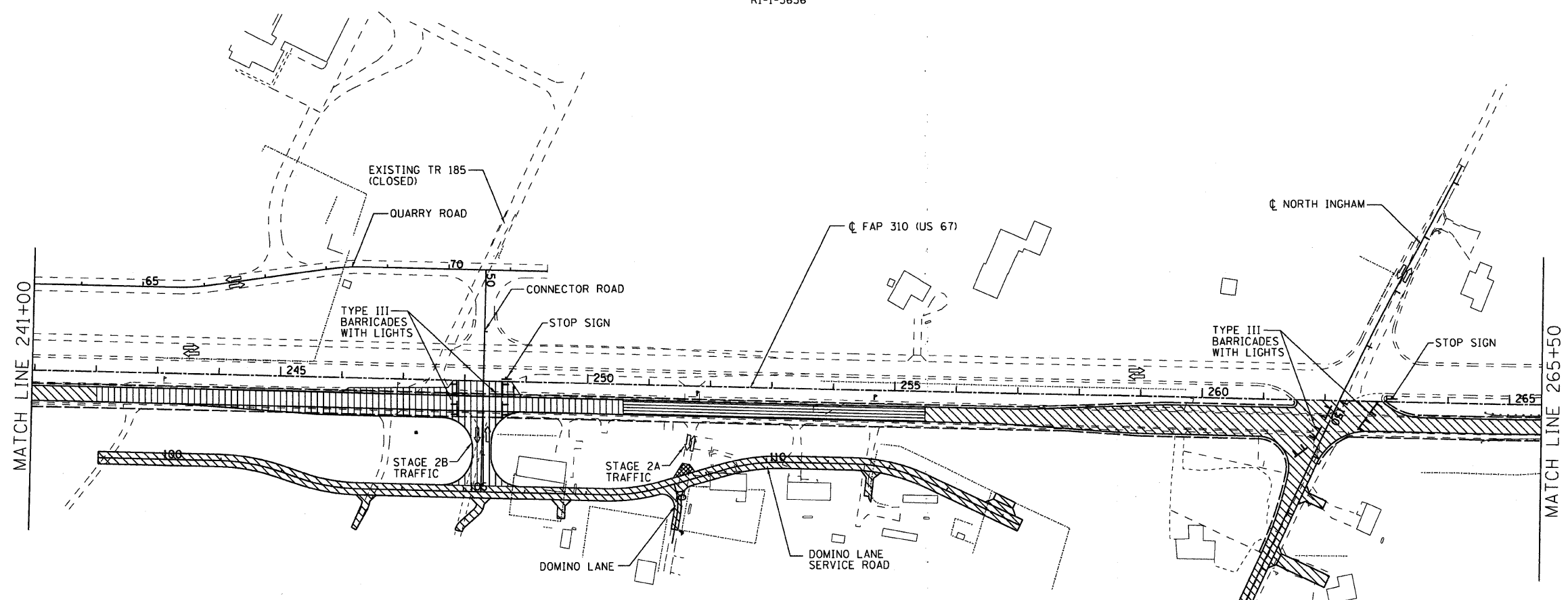
FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	80
STA	TO STA			
EXISTING CONDITIONS:				

CONTRACT NO. 76318



R1-1-3636

PLAN	SUBMITTED	DATE
NOTE BOOK	PLOTTED	
NO.	ALIGNED	
	CHECKED	
	BY	
	DATE	
	CADD FILE NAME	



CONSTRUCTION STAGING AND MAINTENANCE OF TRAFFIC

STAGE 2A

MAINTAIN TRAFFIC ON THE NEWLY CONSTRUCTED SOUTHBOUND LANES OF FAP 310. CONSTRUCT THE PROPOSED FAP 310 NORTHBOUND LANES, EASTERN PORTION OF THE CONNECTOR ROAD, EASTERN PORTION OF NORTH INGHAM, DOMINO LANE SERVICE ROAD, DOMINO LANE, AND INGHAM ACCESS DRIVE AS SHOWN.

1. THE PROPOSED NORTHBOUND LANES OF FAP 310 PAVEMENT SHALL BE GAPPED FROM APPROXIMATELY STATION 242+00 TO STATION 255+50 UNTIL TRAFFIC HAS BEEN MOVED TO THE DOMINO LANE SERVICE ROAD. THE CONSTRUCTION OF THE INTERSECTION OF DOMINO LANE SERVICE ROAD WITH DOMINO LANE SHALL BE ACCOMPLISHED BY MEANS OF GAP PAVING AND STAGE CONSTRUCTION ONE LANE AT A TIME. DOMINO LANE SERVICE ROAD SHALL BE CONSTRUCTED IN SEQUENTIAL PHASES BEGINNING NORTH AND SOUTH FROM DOMINO LANE. DURING THE CONSTRUCTION OF DOMINO LANE SERVICE ROAD, ACCESS TO THE AFFECTED RESIDENCES SHALL BE MAINTAINED WITH TEMPORARY GRAVEL SURFACES CONNECTING THE DRIVEWAYS TO THE SEVERED ROADWAY ACROSS DOMINO LANE SERVICE ROAD. ACCESS FROM DOMINO LANE TO FAP 310 SHALL BE MAINTAINED UTILIZING TYPE III BARRICADES AS SHOWN.
2. NORTH INGHAM EAST OF FAP 310 SHALL BE CONSTRUCTED IN SEQUENTIAL PHASES TO MAXIMIZE ACCESS TO THE LOCAL RESIDENCES. ACCESS TO THE EAST AND TO THE WEST OF THE CONSTRUCTION PHASES SHALL BE ALLOWED. ACCESS THROUGH THE AREA BEING CONSTRUCTED SHALL NOT BE ALLOWED. DURING THIS TIME ACCESS TO FAP 310 FROM EAST OF THE CONSTRUCTION WILL BE VIA ALTERNATE ROADS TO THE NORTH AND SOUTH. DURING CONSTRUCTION OF NORTH INGHAM, TRAFFIC SHALL BE MAINTAINED ACROSS THE PROPOSED NORTHBOUND LANES OF FAP 310 UTILIZING TYPE III BARRICADES AS SHOWN. TRAFFIC SHALL BE MAINTAINED ON THE EASTERN PORTION OF NORTH INGHAM IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF LOCAL ROAD STANDARD BLR-22.

STAGE 2B

MAINTAIN TRAFFIC ON THE NEWLY CONSTRUCTED SOUTHBOUND LANES OF FAP 310. MAINTAIN LOCAL TRAFFIC ON THE NEWLY CONSTRUCTED DOMINO LANE SERVICE ROAD. CONSTRUCT THE STAGE 2B PORTION OF THE PROPOSED FAP 310 NORTHBOUND LANES AND THE DOMINO LANE SERVICE ROAD CONNECTION TO FAP 310 AS SHOWN.

1. THE PROPOSED NORTHBOUND LANES OF FAP 310 PAVEMENT SHALL BE GAPPED FROM APPROXIMATELY STATION 250+50 TO STATION 255+50 UNTIL TRAFFIC HAS BEEN ESTABLISHED ON THE PROPOSED CONNECTOR ROAD. DURING CONSTRUCTION OF THE PROPOSED FAP 310 NORTHBOUND LANES, TRAFFIC SHALL BE MAINTAINED ACROSS THE PROPOSED NORTHBOUND LANES OF FAP 310 UTILIZING TYPE III BARRICADES AS SHOWN.

STAGE 2C

MAINTAIN TRAFFIC ON THE NEWLY CONSTRUCTED SOUTHBOUND LANES OF FAP 310. MAINTAIN TRAFFIC ACROSS THE PROPOSED NORTHBOUND LANES OF FAP 310 TO THE CONNECTOR ROAD UTILIZING TYPE III BARRICADES. CONSTRUCT THE REMAINING SEGMENT OF FAP 310 AS SHOWN.

LEGEND

- TEMPORARY PAVEMENT
- WORK ZONE STAGE 2A
- WORK ZONE STAGE 2B
- WORK ZONE STAGE 2C
- DIRECTION OF TRAFFIC
- TYPE III BARRICADES

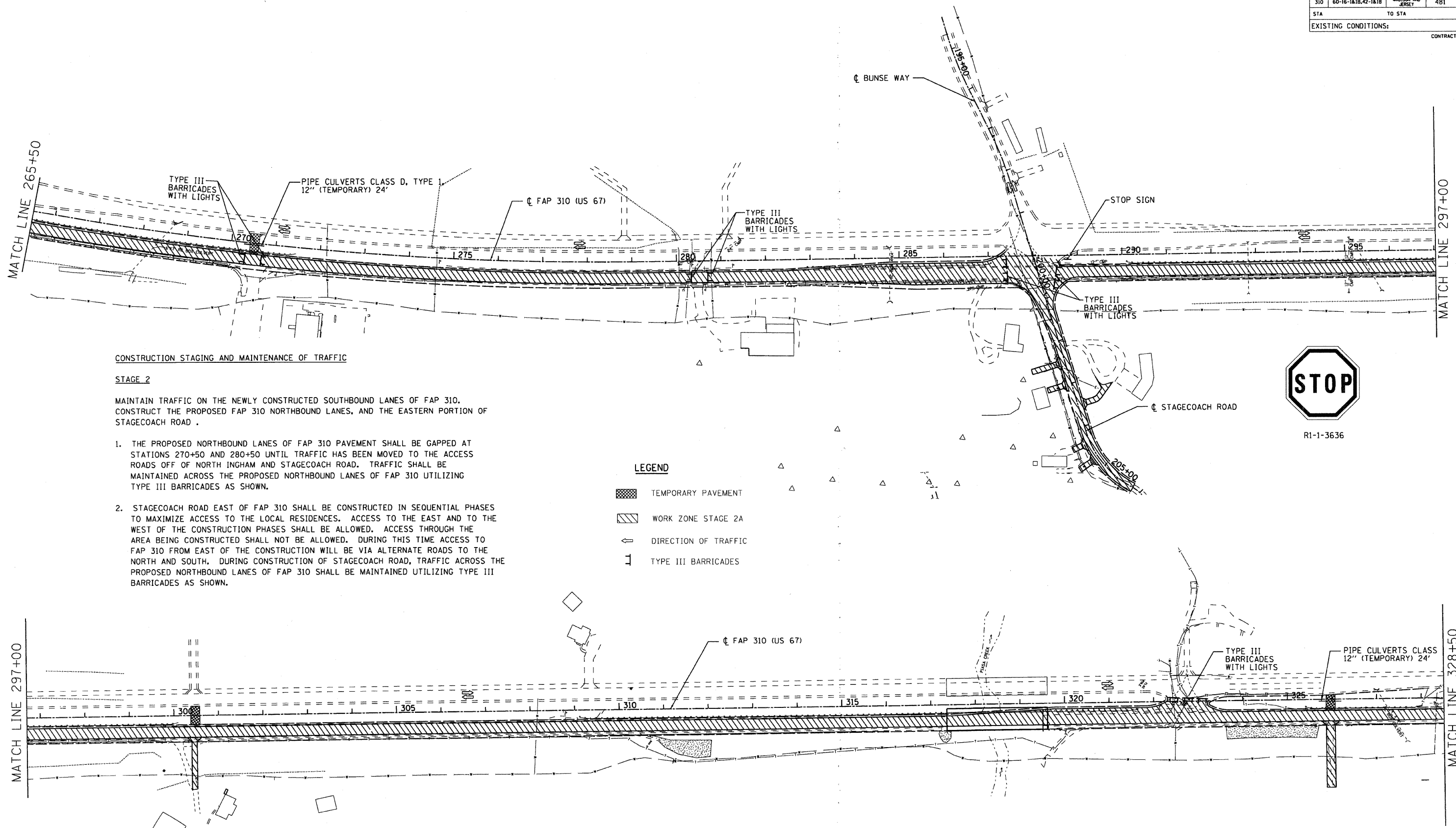
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC
STAGE 2
 FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTY
 DESIGNED BY: WTS
 CHECKED BY: GLF
 DRAWN BY: EMR
 CHECKED BY: WTS

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	81
STA		TO STA		
EXISTING CONDITIONS:				

CONTRACT NO. 76318

DATE	
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REVISIONS	
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NO.	
PLAN	
NOTE BOOK	
NO.	
DATE	







CONSTRUCTION STAGING AND MAINTENANCE OF TRAFFIC

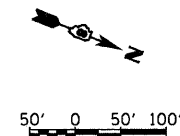
STAGE 2

MAINTAIN TRAFFIC ON THE NEWLY CONSTRUCTED SOUTHBOUND LANES OF FAP 310. CONSTRUCT THE PROPOSED FAP 310 NORTHBOUND LANES, AND THE EASTERN PORTION OF STAGECOACH ROAD .

1. THE PROPOSED NORTHBOUND LANES OF FAP 310 PAVEMENT SHALL BE GAPPED AT STATIONS 270+50 AND 280+50 UNTIL TRAFFIC HAS BEEN MOVED TO THE ACCESS ROADS OFF OF NORTH INGHAM AND STAGECOACH ROAD. TRAFFIC SHALL BE MAINTAINED ACROSS THE PROPOSED NORTHBOUND LANES OF FAP 310 UTILIZING TYPE III BARRICADES AS SHOWN.
2. STAGECOACH ROAD EAST OF FAP 310 SHALL BE CONSTRUCTED IN SEQUENTIAL PHASES TO MAXIMIZE ACCESS TO THE LOCAL RESIDENCES. ACCESS TO THE EAST AND TO THE WEST OF THE CONSTRUCTION PHASES SHALL BE ALLOWED. ACCESS THROUGH THE AREA BEING CONSTRUCTED SHALL NOT BE ALLOWED. DURING THIS TIME ACCESS TO FAP 310 FROM EAST OF THE CONSTRUCTION WILL BE VIA ALTERNATE ROADS TO THE NORTH AND SOUTH. DURING CONSTRUCTION OF STAGECOACH ROAD, TRAFFIC ACROSS THE PROPOSED NORTHBOUND LANES OF FAP 310 SHALL BE MAINTAINED UTILIZING TYPE III BARRICADES AS SHOWN.

LEGEND

-  TEMPORARY PAVEMENT
-  WORK ZONE STAGE 2A
-  DIRECTION OF TRAFFIC
-  TYPE III BARRICADES



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC

STAGE 2

FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON AND JERSEY COUNTY

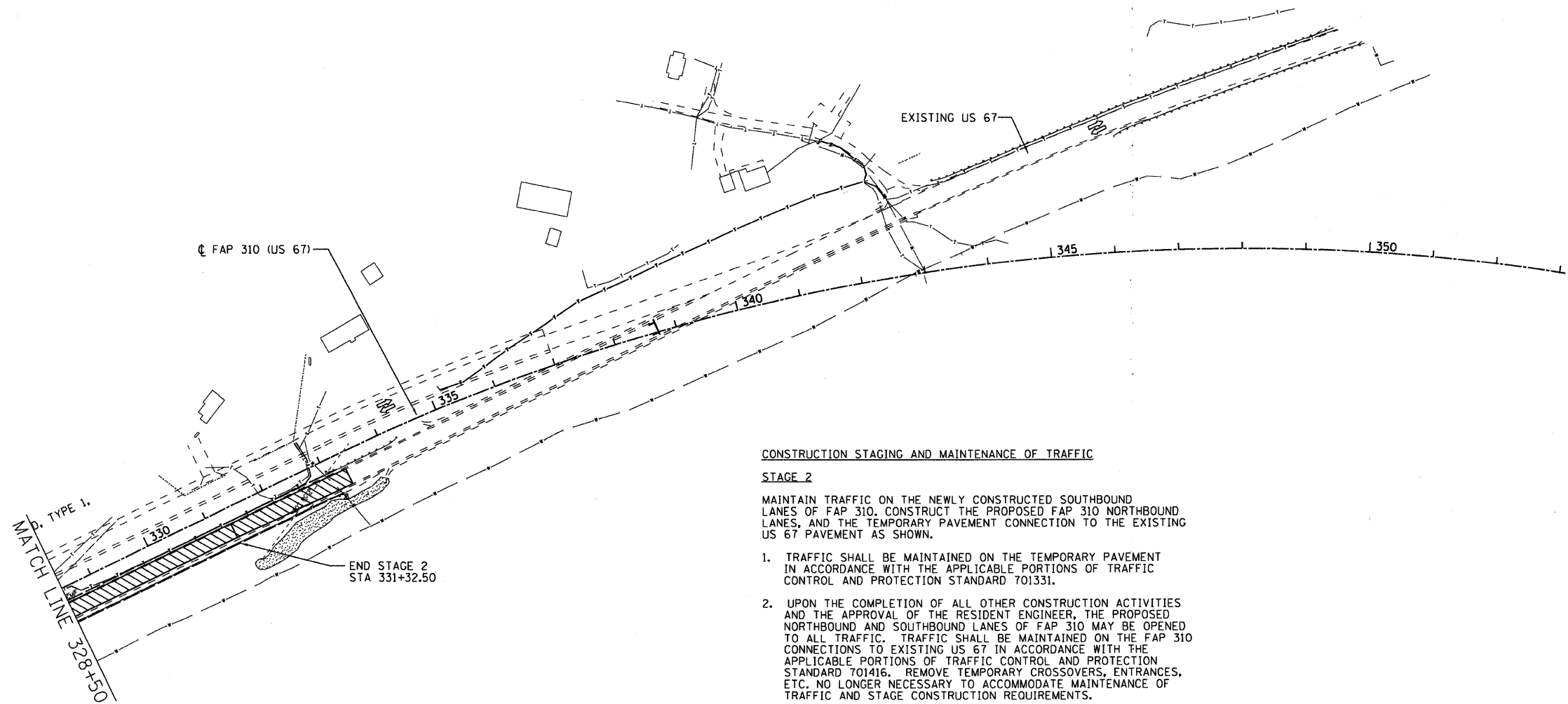
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FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	82
STA		TO STA		

EXISTING CONDITIONS:
CONTRACT NO. 76318

PLAN	DATE
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

CONSTRUCTION STAGING AND MAINTENANCE OF TRAFFIC

STAGE 2

MAINTAIN TRAFFIC ON THE NEWLY CONSTRUCTED SOUTHBOUND LANES OF FAP 310. CONSTRUCT THE PROPOSED FAP 310 NORTHBOUND LANES, AND THE TEMPORARY PAVEMENT CONNECTION TO THE EXISTING US 67 PAVEMENT AS SHOWN.

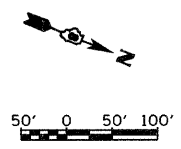
1. TRAFFIC SHALL BE MAINTAINED ON THE TEMPORARY PAVEMENT IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF TRAFFIC CONTROL AND PROTECTION STANDARD 701331.
2. UPON THE COMPLETION OF ALL OTHER CONSTRUCTION ACTIVITIES AND THE APPROVAL OF THE RESIDENT ENGINEER, THE PROPOSED NORTHBOUND AND SOUTHBOUND LANES OF FAP 310 MAY BE OPENED TO ALL TRAFFIC. TRAFFIC SHALL BE MAINTAINED ON THE FAP 310 CONNECTIONS TO EXISTING US 67 IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF TRAFFIC CONTROL AND PROTECTION STANDARD 701416. REMOVE TEMPORARY CROSSOVERS, ENTRANCES, ETC. NO LONGER NECESSARY TO ACCOMMODATE MAINTENANCE OF TRAFFIC AND STAGE CONSTRUCTION REQUIREMENTS.

LEGEND

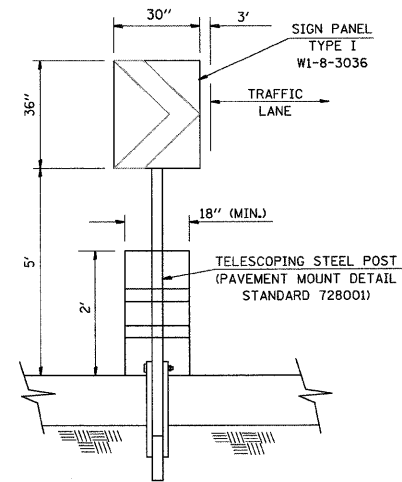
-  TEMPORARY PAVEMENT
-  WORK ZONE STAGE 2A
-  DIRECTION OF TRAFFIC
-  TYPE III BARRICADES

REVISIONS	
NAME	DATE

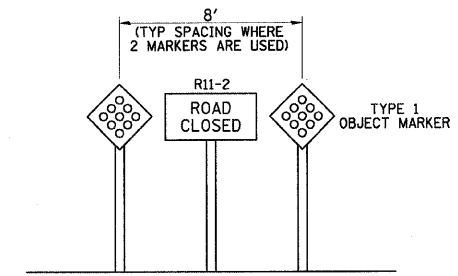
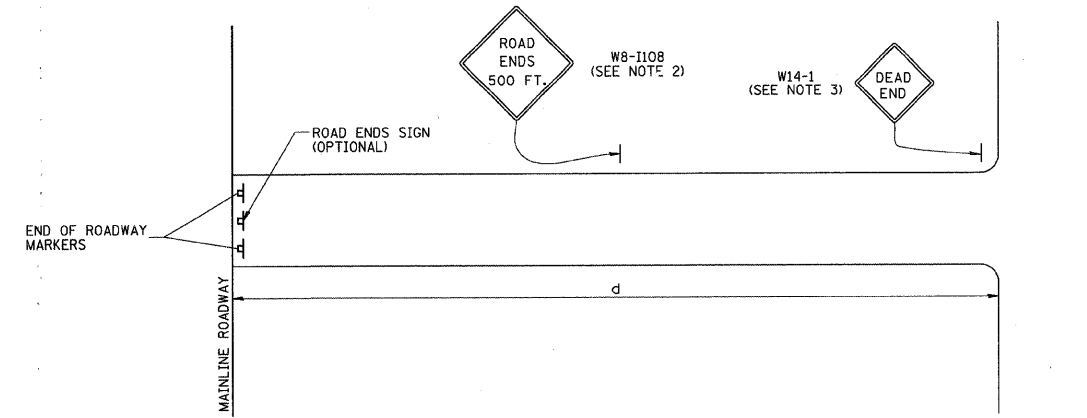
ILLINOIS DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC
STAGE 2
 FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON AND JERSEY COUNTY
 DESIGNED BY: WTS
 CHECKED BY: GLF
 DRAWN BY: EMR
 CHECKED BY: WTS



CONTRACT NO. 76318



DRUM AND CHEVRON SIGN DETAIL



LOW-VOLUME ROAD CLOSURE

NOTES:

1. THE MARKERS FOR THE END OF THE ROADWAY SHALL CONFORM WITH SECTION 3C-4 OF THE MUTCD. THE MARKERS AND SIGN SHOULD BE PLACED AT A MOUNTING HEIGHT OF 4 FT MIN.
2. THE "ROAD ENDS 500 FT" SIGN (W8-I108) SHOULD BE USED WHERE "d" EXCEEDS 1500 FT OR WHERE SIGHT DISTANCE TO THE CLOSURE IS LESS THAN 500 FT.
3. THE "DEAD END" SIGN (W14-1) SHOULD BE USED IN ALL CASES EXCEPT WHERE THE CLOSURE POINT IS VISIBLE FROM THE CROSSROAD.

Hutchison Engineering Inc.
 Since 1945
 Jacksonville * Shorewood

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL DETAILS
 FAP ROUTE 310
 SECTION 60-16-1&1B, 42-1&1B
 MADISON AND JERSEY COUNTIES

SCALE: VERT. DRAWN BY TJD
 HORIZ. CHECKED BY
 DATE FEBRUARY 25, 2010

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	84
STA. TO STA.			EXISTING CONDITIONS:	

CONTRACT NO. 76318

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

I CERTIFY UNDER PENALTY OF LAW THAT THIS DOCUMENT AND ALL ATTACHMENTS WERE PREPARED UNDER MY DIRECTION OR SUPERVISION IN ACCORDANCE WITH A SYSTEM DESIGNED TO ASSURE THAT QUALIFIED PERSONNEL PROPERLY GATHERED AND EVALUATED THE INFORMATION SUBMITTED. BASED ON MY INQUIRY OF THE PERSON OR PERSONS WHO MANAGE THE SYSTEM, OR THOSE PERSONS DIRECTLY RESPONSIBLE FOR GATHERING THE INFORMATION, THE INFORMATION SUBMITTED IS, TO THE BEST OF MY KNOWLEDGE AND BELIEF, TRUE, ACCURATE AND COMPLETE. I AM AWARE THAT THERE ARE SIGNIFICANT PENALTIES FOR SUBMITTING FALSE INFORMATION, INCLUDING THE POSSIBILITY OF FINE AND IMPRISONMENT FOR KNOWING VIOLATIONS.

MARY C. LAMIE
 PRINT NAME
 DEPUTY DIRECTOR OF HIGHWAYS
 REGION FIVE ENGINEER
 TITLE
 IL DEPT. OF TRANSPORTATION
 AGENCY

Mary C. Lamie
 SIGNATURE
 3-18-10
 DATE

DATE	
BY	
PLAN	
NOTE BOOK	
NO.	
SURVEYED	
PLOTTED	
REVISIONS	
BY	
DATE	
FILED	
BY	
DATE	

I. SITE DESCRIPTION:

A. THE FOLLOWING IS A DESCRIPTION OF THE PROJECT LOCATION:

THE PROJECT CONSISTS OF THE PROPOSED IMPROVEMENTS OF 0.28 MILES OF IL ROUTE 59, SPECIFICALLY IL ROUTE 59 BRIDGE OVER THE DUPAGE RIVER.

B. THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

CONSTRUCTION WILL INCLUDE THE REMOVAL AND THE REPLACEMENT OF THE IL ROUTE 59 BRIDGE OVER THE DUPAGE RIVER, ROADWAY CONSTRUCTION, STORM SEWER AND DRAINAGE STRUCTURES, AGGREGATE SHOULDERS, COMBINATION CONCRETE CURB AND GUTTER, PAVEMENT MARKING, LANDSCAPING AND ALL INCIDENTAL AND COLLATERAL WORK NECESSARY TO COMPLETE THE PROJECT AS SHOWN ON THE PLANS.

C. THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

PRE-STAGE 1: CONSTRUCTION OF TEMPORARY PAVEMENT ADJACENT TO EXISTING PAVEMENT AND INSTALLATION OF THE PIPE CULVERT UNDER ROLF ROAD.

STAGE 1: BEGIN REMOVAL AND CONSTRUCTION OF THE IL ROUTE 59 BRIDGE OVER DUPAGE RIVER. BEGIN CONSTRUCTION OF THE WEST HALF OF THE PROPOSED PAVEMENT AND TEMPORARY PAVEMENT. CONSTRUCTION OF THE PROPOSED STORM SEWER AND DRAINAGE STRUCTURES ON THE WEST SIDE.

STAGE 2: CONTINUE CONSTRUCTION OF THE PROPOSED BRIDGE AND COMPLETE CONSTRUCTION OF THE WEST HALF OF THE PROPOSED PAVEMENT.

STAGE 3: COMPLETE THE CONSTRUCTION OF THE PROPOSED BRIDGE. BEGIN CONSTRUCTION OF THE EAST HALF OF THE PROPOSED PAVEMENT. CONSTRUCTION OF PROPOSED AGGREGATE SHOULDERS, COMBINATION CONCRETE CURB AND GUTTER, STORM SEWER AND DRAINAGE STRUCTURES ON THE EAST SIDE.

STAGE 4: COMPLETE CONSTRUCTION OF THE WEST HALF OF THE PROPOSED PAVEMENT. BEGIN CONSTRUCTION OF PROPOSED AGGREGATE SHOULDERS AND COMBINATION CONCRETE CURB AND GUTTER.

STAGE 5: COMPLETE CONSTRUCTION OF PROPOSED AGGREGATE SHOULDERS ON THE WEST SIDE.

D. THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 100 ACRES.

THE TOTAL AREA OF THE SITE THAT IS ESTIMATED WILL BE DISTURBED BY EXCAVATION, GRADING OR OTHER ACTIVITIES IS 77 ACRES.

E. THE FOLLOWING IS A WEIGHTED AVERAGE OF THE RUNOFF COEFFICIENT FOR THIS PROJECT AFTER CONSTRUCTION ACTIVITIES ARE COMPLETED: 0.48

F. THE FOLLOWING IS A DESCRIPTION OF THE SOIL TYPES FOUND AT THE PROJECT SITE FOLLOWED BY INFORMATION REGARDING THEIR EROSIVITY:

TWO SOIL TYPES ARE LOCATED WITHIN THE PROJECT AREA OF THE US 67 RECONSTRUCTION PROJECT. THESE ARE:

BOTTOMLAND AND TERRACE SOIL ASSOCIATION: NEARLY LEVEL TO GENTLY SLOPING, POORLY DRAINED TO WELL DRAINED BOTTOMLAND SOILS AND NEARLY LEVEL TO STEEP, IMPERFECTLY DRAINED TO WELL DRAINED TERRACE SOILS.

CLINTON-KEOMAH ASSOCIATION: NEARLY LEVEL TO VERY STEEP, POORLY DRAINED TO MODERATELY WELL-DRAINED, LIGHT-COLORED AND MODERATELY DARK-COLORED UPLAND SOILS DEVELOPED FROM MODERATELY THICK LOESS.

G. THE FOLLOWING IS A DESCRIPTION OF POTENTIALLY EROSIIVE AREAS ASSOCIATED WITH THIS PROJECT:

THERE ARE NO KNOWN CRITICAL EROSIIVE AREAS.

H. THE FOLLOWING IS A DESCRIPTION OF SOIL DISTURBING ACTIVITIES, THEIR LOCATIONS, AND THEIR EROSIIVE FACTORS (E.G. STEEPNESS OF SLOPES, LENGTH OF SLOPES, ETC):

THE NATURE AND PURPOSE OF LAND DISTURBING ACTIVITIES ON THIS PROJECT ARE TO RECONSTRUCT US 67 FROM A TWO-LANE TO A FOUR-LANE DIVIDED RURAL EXPRESSWAY. PROPOSED RIGHT-OF-WAY WILL BE REQUIRED TO ACCOMMODATE THE RECONSTRUCTION OF US 67. THERE ARE NO SCHEDULED NEIGHBORING ACTIVITIES THAT WILL AFFECT THE SOIL EROSION AND SEDIMENT CONTROL PLANS AND NO OFF-SITE LAND DISTURBING ACTIVITIES.

I. SEE THE EROSION CONTROL PLANS AND/OR DRAINAGE PLANS FOR THIS CONTRACT FOR INFORMATION REGARDING DRAINAGE PATTERNS, APPROXIMATE SLOPES ANTICIPATED BEFORE AND AFTER MAJOR GRADING ACTIVITIES, LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE AND CONTROLS TO PREVENT OFF SITE SEDIMENT TRACKING (TO BE ADDED AFTER CONTRACTOR IDENTIFIES LOCATIONS), AREAS OF SOIL DISTURBANCE, THE LOCATION OF MAJOR STRUCTURAL AND NON-STRUCTURAL CONTROLS IDENTIFIED IN THE PLAN, THE LOCATION OF AREAS WHERE STABILIZATION PRACTICES ARE EXPECTED TO OCCUR, SURFACE WATERS (INCLUDING WETLANDS) AND LOCATIONS WHERE STORM WATER IS DISCHARGED TO SURFACE WATER INCLUDING WETLANDS.

J. THE FOLLOWING IS A LIST OF RECEIVING WATER(S) AND THE ULTIMATE RECEIVING WATER(S), AND AERIAL EXTENT OF WETLAND ACREAGE AT THE SITE. THE LOCATION OF THE RECEIVING WATERS CAN BE FOUND ON THE EROSION AND SEDIMENT CONTROL PLANS:

LITTLE PIASA CREEK, PIASA CREEK, AND MULTIPLE UNNAMED CREEKS AND TRIBUTARIES

K. THE FOLLOWING POLLUTANTS OF CONCERN WILL BE ASSOCIATED WITH THIS CONSTRUCTION PROJECT: (CHECK ALL THAT APPLY)

- | | |
|---|--|
| <input checked="" type="checkbox"/> SOIL SEDIMENT | <input checked="" type="checkbox"/> PETROLEUM (GAS, DIESEL, OIL, KEROSENE, HYDRAULIC OIL/FLUIDS) |
| <input checked="" type="checkbox"/> CONCRETE | <input checked="" type="checkbox"/> ANTIFREEZE / COOLANTS |
| <input checked="" type="checkbox"/> CONCRETE TRUCK WASTE | <input checked="" type="checkbox"/> WASTE WATER FROM CLEANING CONSTRUCTION EQUIPMENT |
| <input checked="" type="checkbox"/> CONCRETE CURING COMPOUNDS | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input checked="" type="checkbox"/> SOLID WASTE DEBRIS | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> PAINTS | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> SOLVENTS | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> FERTILIZERS / PESTICIDES | <input type="checkbox"/> OTHER (SPECIFY)..... |

II. CONTROLS

THIS SECTION OF THE PLAN ADDRESSES THE CONTROLS THAT WILL BE IMPLEMENTED FOR EACH OF THE MAJOR CONSTRUCTION ACTIVITIES DESCRIBED IN I.C. ABOVE AND FOR ALL USE AREAS, BORROW SITES, AND WASTE SITES. FOR EACH MEASURE DISCUSSED, THE CONTRACTOR WILL BE RESPONSIBLE FOR ITS IMPLEMENTATION AS INDICATED. THE CONTRACTOR SHALL PROVIDE TO THE RESIDENT ENGINEER A PLAN FOR THE IMPLEMENTATION OF THE MEASURES INDICATED. THE CONTRACTOR, AND SUBCONTRACTORS, WILL NOTIFY THE RESIDENT ENGINEER OF ANY PROPOSED CHANGES, MAINTENANCE, OR MODIFICATIONS TO KEEP CONSTRUCTION ACTIVITIES COMPLIANT WITH THE PERMIT. EACH SUCH CONTRACTOR HAS SIGNED THE REQUIRED CERTIFICATION ON FORMS WHICH ARE ATTACHED TO, AND ARE A PART OF THIS PLAN:

A. EROSION AND SEDIMENT CONTROL

1. STABILIZED PRACTICES: PROVIDED BELOW IS A DESCRIPTION OF INTERIM AND PERMANENT STABILIZATION PRACTICES, INCLUDING SITE SPECIFIC SCHEDULING OF THE IMPLEMENTATION OF THE PRACTICES. SITE PLANS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES MAY INCLUDE BUT ARE NOT LIMITED TO: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, GEOTEXTILES, SODDING, VEGETATIVE BUFFER STRIPS, PROTECTION OF TREES, PRESERVATION OF MATURE VEGETATION, AND OTHER APPROPRIATE MEASURES. EXCEPT AS PROVIDED BELOW IN II(A)(1)(a) AND II(A)(3), STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED ON ALL DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION WILL NOT OCCUR FOR A PERIOD OF 14 OR MORE CALENDAR DAYS.

a. WHERE THE INITIATION OF STABILIZATION MEASURES BY THE 7TH DAY AFTER CONSTRUCTION ACTIVITY TEMPORARILY OR PERMANENTLY CEASES IS PRECLUDED BY SNOW COVER, STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE THEREAFTER.

THE FOLLOWING STABILIZATION PRACTICES WILL BE USED FOR THIS PROJECT: (CHECK ALL THAT APPLY)

- | | |
|---|--|
| <input type="checkbox"/> PRESERVATION OF MATURE VEGETATION | <input checked="" type="checkbox"/> EROSION CONTROL BLANKET / MULCHING |
| <input type="checkbox"/> VEGETATED BUFFER STRIPS | <input type="checkbox"/> SODDING |
| <input type="checkbox"/> PROTECTION OF TREES | <input type="checkbox"/> GEOTEXTILES |
| <input checked="" type="checkbox"/> TEMPORARY EROSION CONTROL SEEDING | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> TEMPORARY TURF (SEEDING, CLASS 7) | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> TEMPORARY MULCHING | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input checked="" type="checkbox"/> PERMANENT SEEDING | <input type="checkbox"/> OTHER (SPECIFY)..... |

DESCRIBE HOW THE STABILIZATION PRACTICES LISTED ABOVE WILL BE UTILIZED:

1. TEMPORARY EROSION CONTROL SEEDING - THIS ITEM WILL BE APPLIED TO ALL BARE AREAS EVERY SEVEN DAYS TO MINIMIZE THE AMOUNT OF EXPOSED SURFACE AREAS.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED IF THEY ARE TO REMAIN UNUSED FOR MORE THAN 14 DAYS.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

BARE AND SPARSELY VEGETATED GROUND IN HIGHLY ERODIBLE AREAS AS DETERMINED BY THE ENGINEER SHALL BE TEMPORARILY SEEDED AT THE BEGINNING OF CONSTRUCTION WHERE NO CONSTRUCTION ACTIVITIES ARE EXPECTED WITHIN 7 DAYS.

2. PERMANENT SEEDING - SEEDING, CLASS 2 AND CLASS 3 WILL BE INSTALLED PER IDOT SPECIFICATIONS.

3. EROSION CONTROL BLANKETS/MULCHING - EROSION CONTROL BLANKETS WILL BE INSTALLED OVER FILL SLOPES AND IN HIGH VELOCITY AREAS (I.E. DITCHES) THAT HAVE BEEN BROUGHT TO FINAL GRADE AND SEEDED TO PROTECT SLOPES FROM EROSION AND ALLOW SEEDS TO GERMINATE. MULCH, METHOD 2 WILL BE APPLIED IN RELATIVELY FLAT AREAS TO PROTECT THE DISTURBED AREAS AND PREVENT FURTHER EROSION.

MULCH AS APPLIED TO TEMPORARY EROSION CONTROL SEEDING SHALL BE BY THE METHOD SPECIFIED IN THE CONTRACT AND AT THE DIRECTION OF THE ENGINEER. MULCH WILL BE PAID SEPARATELY AND SHALL CONFORM TO SECTION 251 OF THE STANDARD SPECIFICATIONS.

4. PERMANENT STABILIZATION - ALL AREAS DISTURBED BY CONSTRUCTION WILL BE STABILIZED WITH PERMANENT SEEDING IMMEDIATELY FOLLOWING THE FINISHED GRADING. EROSION CONTROL BLANKETS WILL BE INSTALLED OVER FILL SLOPES WHICH HAVE BEEN BROUGHT TO FINAL GRADE AND HAVE BEEN SEEDED TO PROTECT THE SLOPES FROM RILL AND GULLY EROSION AND ALLOW SEED TO GERMINATE PROPERLY. MULCH, METHOD 2 WILL BE USED ON RELATIVELY FLAT AREAS.

2. STRUCTURAL PRACTICES: PROVIDED BELOW IS A DESCRIPTION OF STRUCTURAL PRACTICES THAT WILL BE IMPLEMENTED, TO THE DEGREE ATTAINABLE, TO DIVERT FLOWS FROM EXPOSED SOILS, STORE FLOWS OR OTHERWISE LIMIT RUNOFF AND THE DISCHARGE OF POLLUTANTS FROM EXPOSED AREAS OF THE SITE. SUCH PRACTICES MAY INCLUDE BUT ARE NOT LIMITED TO: PERIMETER EROSION BARRIER, EARTH DIKES, DRAINAGE SWALES, SEDIMENT TRAPS, DITCH CHECKS, SUBSURFACE DRAINS, PIPE SLOPE DRAINS, LEVEL SPREADERS, STORM DRAIN INLET PROTECTION, ROCK OUTLET PROTECTION, REINFORCED SOIL RETAINING SYSTEMS, GABIONS, AND TEMPORARY OR PERMANENT SEDIMENT BASINS. THE INSTALLATION OF THESE DEVICES MAY BE SUBJECT TO SECTION 404 OF THE CLEAN WATER ACT.

THE FOLLOWING STRUCTURAL PRACTICES WILL BE USED FOR THIS PROJECT:

- | | |
|--|--|
| <input checked="" type="checkbox"/> PERIMETER EROSION BARRIER | <input checked="" type="checkbox"/> ROCK OUTLET PROTECTION |
| <input type="checkbox"/> TEMPORARY DITCH CHECK | <input checked="" type="checkbox"/> RIPRAP |
| <input checked="" type="checkbox"/> STORM DRAIN INLET PROTECTION | <input type="checkbox"/> GABIONS |
| <input type="checkbox"/> SEDIMENT TRAP | <input type="checkbox"/> SLOPE MATTRESS |
| <input type="checkbox"/> TEMPORARY PIPE SLOPE DRAIN | <input type="checkbox"/> RETAINING WALLS |
| <input type="checkbox"/> TEMPORARY SEDIMENT BASIN | <input type="checkbox"/> SLOPE WALLS |
| <input type="checkbox"/> TEMPORARY STREAM CROSSING | <input type="checkbox"/> CONCRETE REVETMENT MATS |
| <input type="checkbox"/> STABILIZED CONSTRUCTION EXITS | <input type="checkbox"/> LEVEL SPREADERS |
| <input type="checkbox"/> TURF REINFORCEMENT MATS | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> PERMANENT CHECK DAMS | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> PERMANENT SEDIMENT BASIN | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> AGGREGATE DITCH | <input type="checkbox"/> OTHER (SPECIFY)..... |
| <input type="checkbox"/> PAVED DITCH | <input type="checkbox"/> OTHER (SPECIFY)..... |

DESCRIBE HOW THE STRUCTURAL PRACTICES LISTED ABOVE WILL BE UTILIZED:

1. PERIMETER EROSION BARRIER - SILT FENCES WILL BE PLACED ALONG THE BANKS OF THE DUPAGE RIVER IN AN EFFORT TO CONTAIN SILT AND RUNOFF FROM LEAVING THE SITE.

CONSTRUCT AT BEGINNING OF CONSTRUCTION. REMOVE AT END OF CONSTRUCTION.

2. STORM DRAIN INLET PROTECTION - INLET AND PIPE PROTECTION WILL BE PROVIDED FOR STORM SEWERS AND CULVERTS. SEDIMENT FILTERS WILL BE PLACED IN ALL INLETS, CATCH BASINS AND MANHOLES DURING CONSTRUCTION AND WILL BE CLEANED ON A REGULAR BASIS.

3. TEMPORARY DITCH CHECKS - DITCH CHECKS WILL BE PLACED IN SWALES WHERE RUNOFF VELOCITY IS HIGH. ALL STRUCTURAL PRACTICES ARE SHOWN IN DETAIL ON THE EROSION CONTROL PLANS.

TEMPORARY DITCH CHECKS SHALL BE LOCATED AT EVERY ... FT. FALL/RISE IN DITCH GRADE.

TEMPORARY DITCH CHECKS, AGGREGATE USES GRADING NO. 3- REMOVE AT END OF CONSTRUCTION.

STRAW BALES, HAY BALES, PERIMETER EROSION BARRIER AND SILT FENCE WILL NOT BE PERMITTED FOR TEMPORARY OR PERMANENT DITCH CHECKS. DITCH CHECKS SHALL BE COMPOSED OF AGGREGATE (IF SPECIFIED), ENVIROBERM, TRIANGULAR SILT DIKES, GEORIDGE AND ROLLED EXCELSIOR.

4. RIPRAP - STONE RIPRAP WITH FILTER FABRIC WILL BE USED AS PROTECTION AT THE DISCHARGE END OF ALL CULVERT END SECTIONS AND AS INLET/OUTLET PROTECTION TO PREVENT SCOURING AT THE END OF PIPES AND PREVENT DOWNSTREAM EROSION.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, TEMPORARY DITCH CHECKS, INLET AND PIPE PROTECTION, AND PERIMETER EROSION BARRIER SHALL BE INSTALLED AS CALLED OUT IN THIS PLAN AND DIRECTED BY THE ENGINEER.

ALL EROSION CONTROL PRODUCTS FURNISHED SHALL BE SPECIFICALLY RECOMMENDED BY THE MANUFACTURER FOR THE USE SPECIFIED IN THE EROSION CONTROL PLAN. PRIOR TO THE APPROVAL AND USE OF THE PRODUCT, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A NOTARIZED CERTIFICATION BY THE PRODUCER STATING THE INTENDED USE OF THE PRODUCT AND THAT THE PHYSICAL PROPERTIES REQUIRED FOR THIS APPLICATION ARE MET OR EXCEEDED. THE CONTRACTOR SHALL PROVIDE MANUFACTURER INSTALLATION PROCEDURES TO FACILITATE THE ENGINEER IN CONSTRUCTION INSPECTION.

REVISIONS	
NAME	DATE

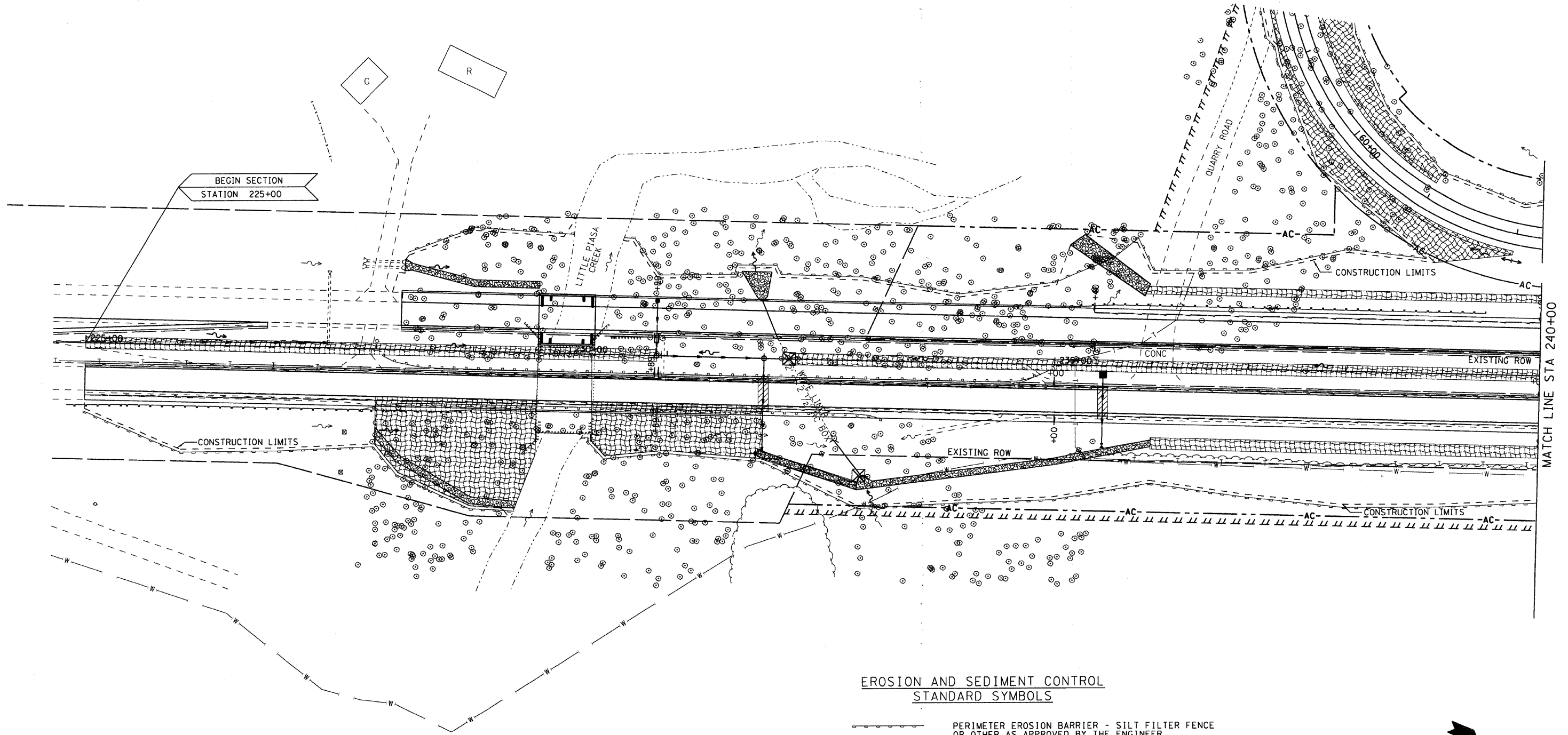
ILLINOIS DEPARTMENT OF TRANSPORTATION
 STORM WATER POLLUTION
 PREVENTION PLAN

FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON/JERSEY COUNTY

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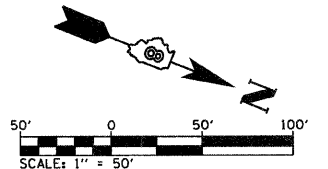
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DATE	



**EROSION AND SEDIMENT CONTROL
STANDARD SYMBOLS**

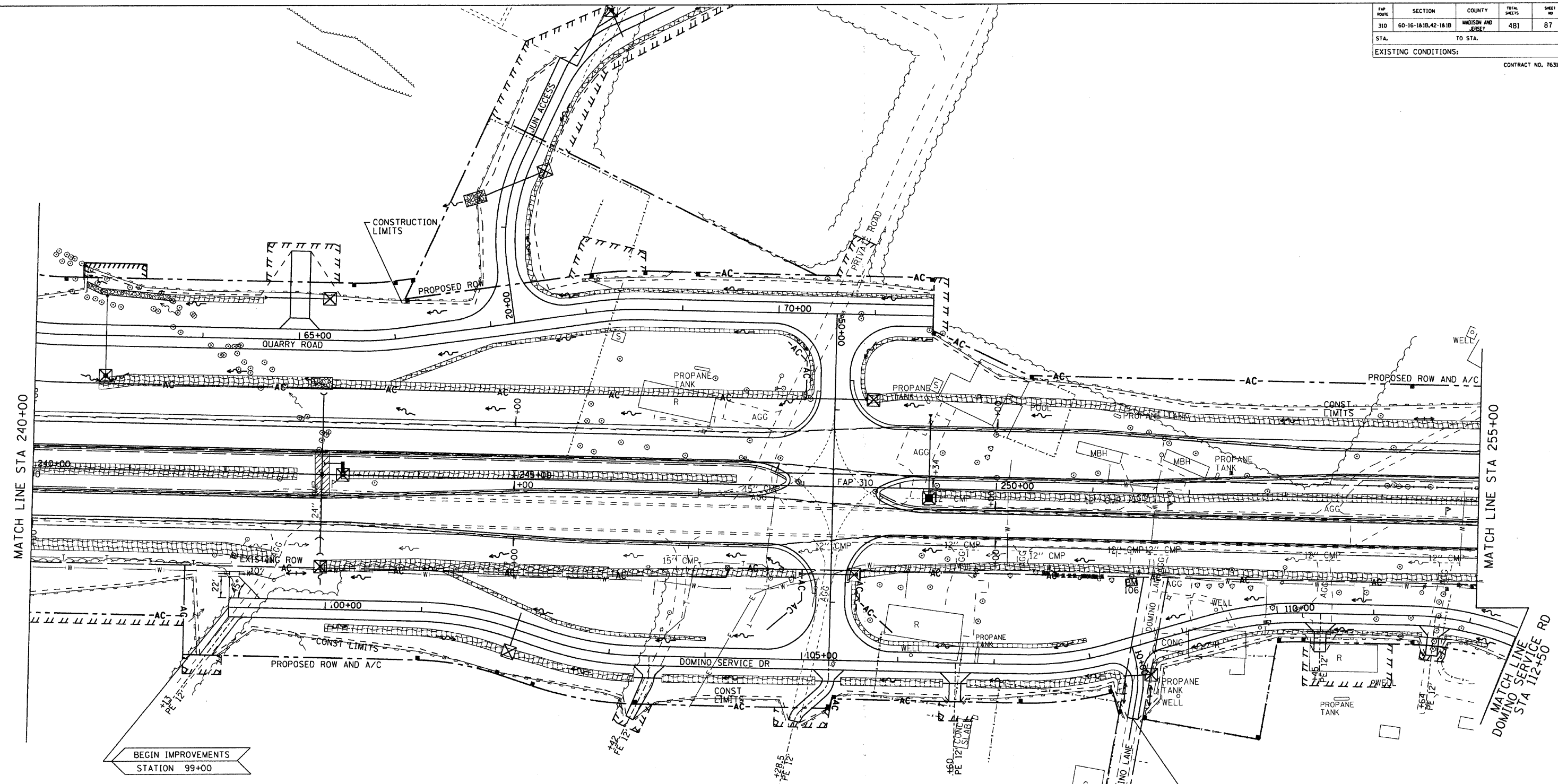
- PERIMETER EROSION BARRIER - SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
- EROSION CONTROL BLANKET
- FLOW LINES
- INLET AND PIPE PROTECTION - STRAW BALES, FILTER FABRIC, AGGREGATES
- PERMANENT EARTH BERM DITCH CHECK
- RIPRAP



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
EROSION CONTROL PLAN
STA 225+00 TO 240+00
 FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON/JERSEY COUNTY
 DRAWN BY: EMR
 CHECKED BY: GLF

PLAN	DATE
SURVEYED	
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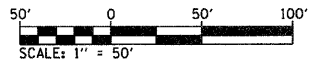


BEGIN IMPROVEMENTS
STATION 99+00

END IMPROVEMENTS
STATION 10+75

**EROSION AND SEDIMENT CONTROL
STANDARD SYMBOLS**

- PERIMETER EROSION BARRIER - SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
- EROSION CONTROL BLANKET
- FLOW LINES
- INLET AND PIPE PROTECTION - STRAW BALES, FILTER FABRIC, AGGREGATES
- PERMANENT EARTH BERM DITCH CHECK
- RIPRAP



ILLINOIS DEPARTMENT OF TRANSPORTATION

**EROSION CONTROL PLAN
STA 240+00 TO 255+00**

FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON/JERSEY COUNTY

DRAWN BY: EMR

CHECKED BY: GLF

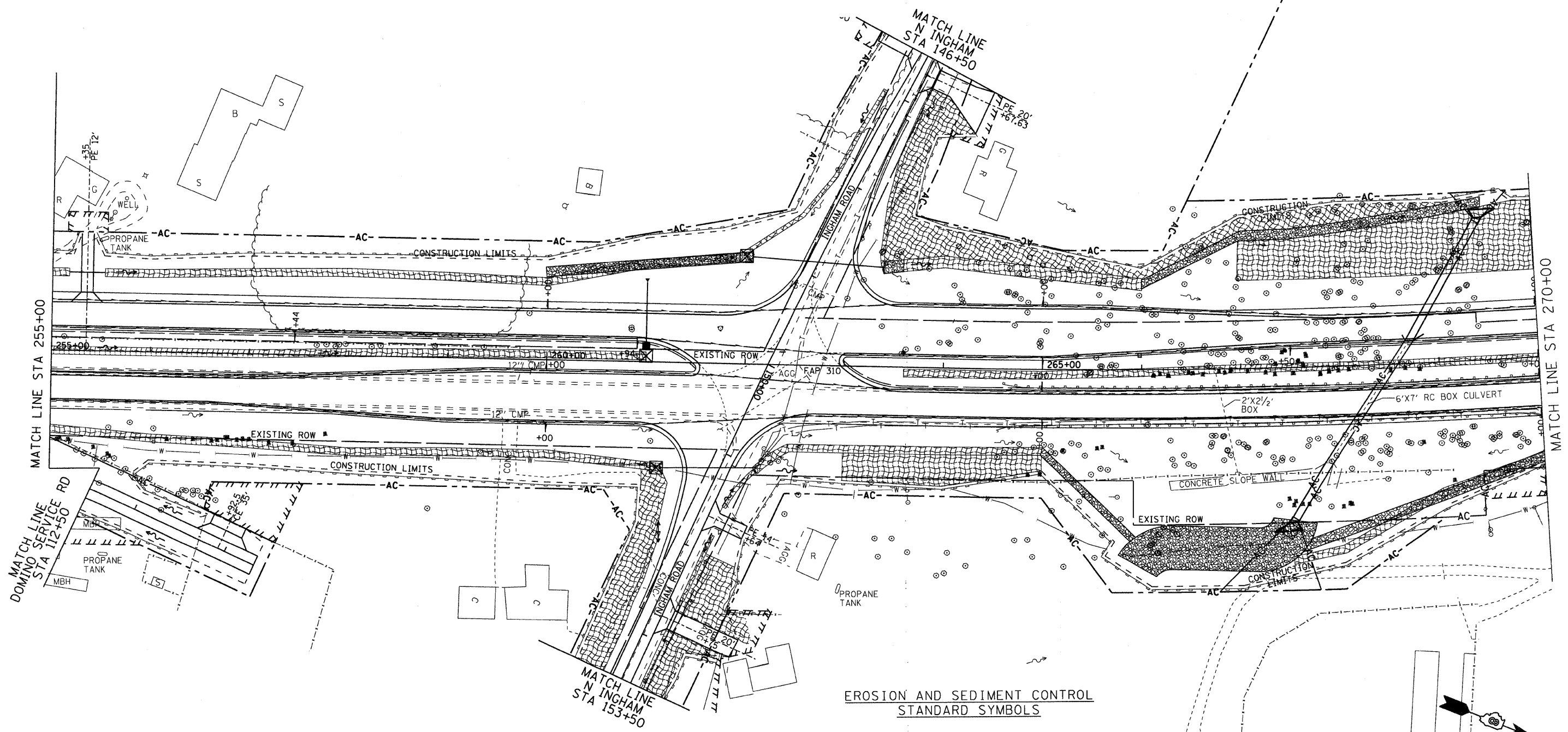
REVISIONS	
NAME	DATE

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&18,42-1&18	MADISON AND JERSEY	481	88

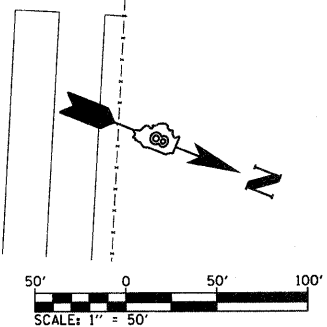
STA. TO STA. CONTRACT NO. 76318

EXISTING CONDITIONS:

PLAN	DATE
SURVEYED	BY
PLOTTED	BY
REVISIONS	NO.
NOTE BOOK	NO.
RT. OF WAY CHECKED	NO.
CADD FILE NAME	



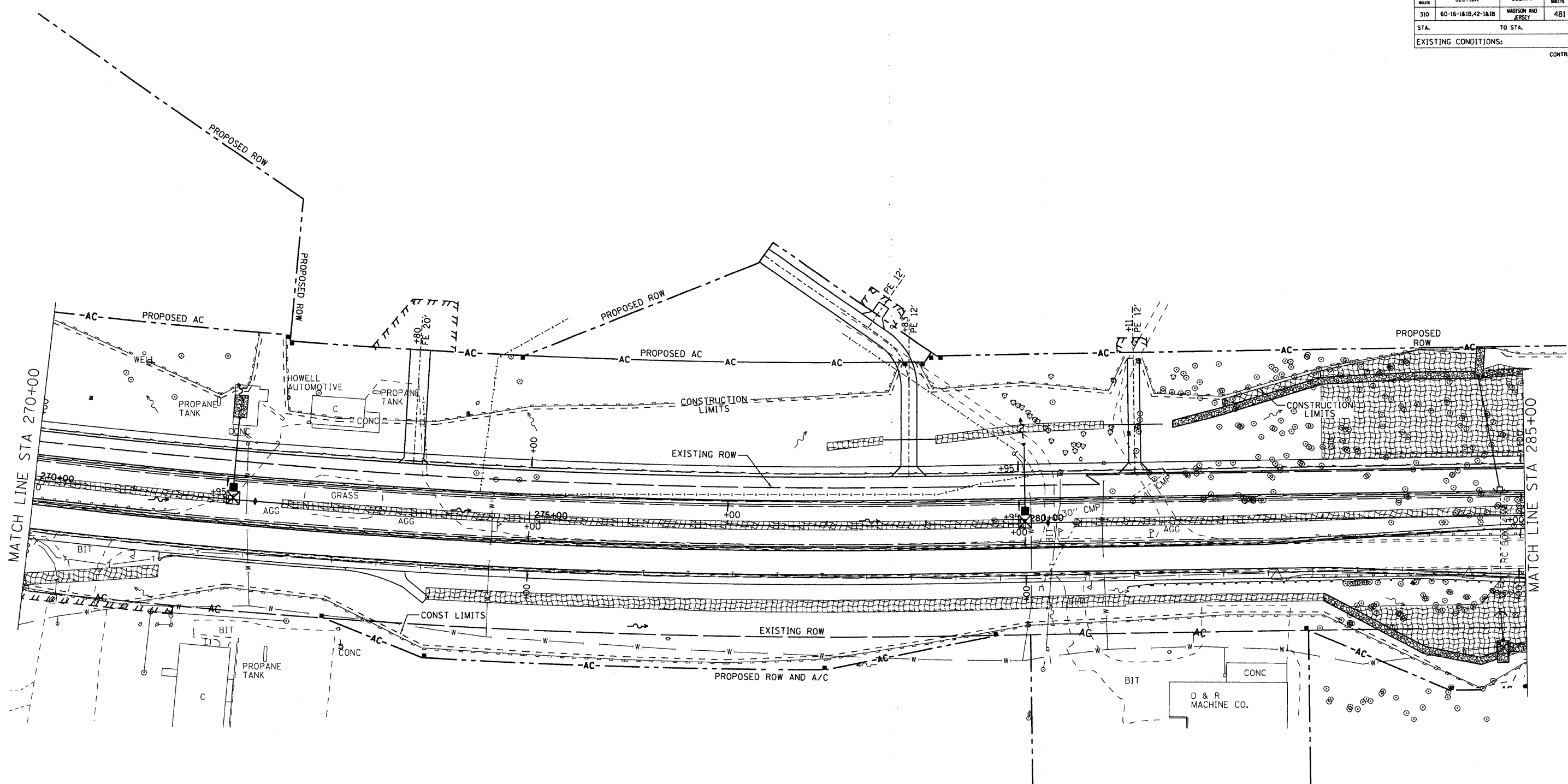
- EROSION AND SEDIMENT CONTROL STANDARD SYMBOLS**
- PERIMETER EROSION BARRIER - SILT FILTER FENCE, OR OTHER AS APPROVED BY THE ENGINEER
 - EROSION CONTROL BLANKET
 - FLOW LINES
 - INLET AND PIPE PROTECTION - STRAW BALES, FILTER FABRIC, AGGREGATES
 - PERMANENT EARTH BERM DITCH CHECK
 - RIPRAP



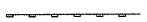





REVISIONS	
NAME	DATE

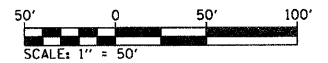
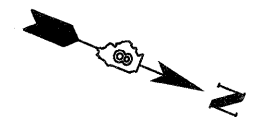
ILLINOIS DEPARTMENT OF TRANSPORTATION
EROSION CONTROL PLAN
STA 255+00 TO 270+00
 FAP ROUTE 310
 SECTION 60-16-1&18,42-1&18
 MADISON/JERSEY COUNTY
 DRAWN BY: EMR
 CHECKED BY: GLF

DATE	
BY	
REVISIONS	
NOTED	
ALIGNED	
CHECKED	
DATE	
FILE NAME	



**EROSION AND SEDIMENT CONTROL
STANDARD SYMBOLS**

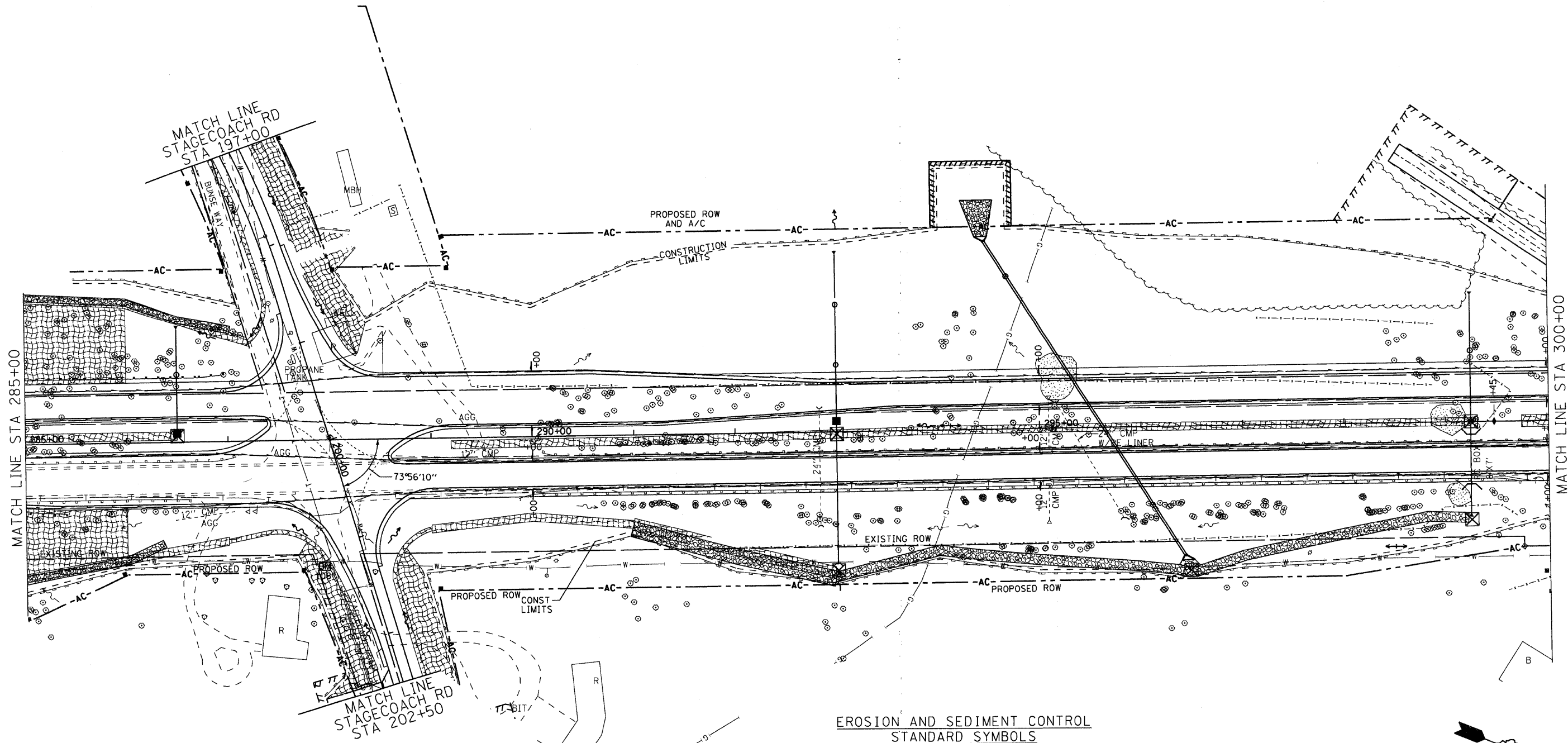
-  PERIMETER EROSION BARRIER - SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
-  EROSION CONTROL BLANKET
-  FLOW LINES
-  INLET AND PIPE PROTECTION - STRAW BALES, FILTER FABRIC, AGGREGATES
-  PERMANENT EARTH BERM DITCH CHECK
-  RIPRAP









REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
EROSION CONTROL PLAN
STA 270+00 TO 285+00
 FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON/JERSEY COUNTY
 DRAWN BY: EMR
 CHECKED BY: GLF

PLAN	SURVEYED	DATE
NO.	BY	
	PLOTTED	
	CHECKED	
	BY	
	DATE	
	FILE NAME	



**EROSION AND SEDIMENT CONTROL
STANDARD SYMBOLS**

-  PERIMETER EROSION BARRIER - SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
-  EROSION CONTROL BLANKET
-  FLOW LINES
-  INLET AND PIPE PROTECTION - STRAW BALES, FILTER FABRIC, AGGREGATES
-  PERMANENT EARTH BERM DITCH CHECK
-  RIPRAP

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**EROSION CONTROL PLAN
STA 285+00 TO 300+00**

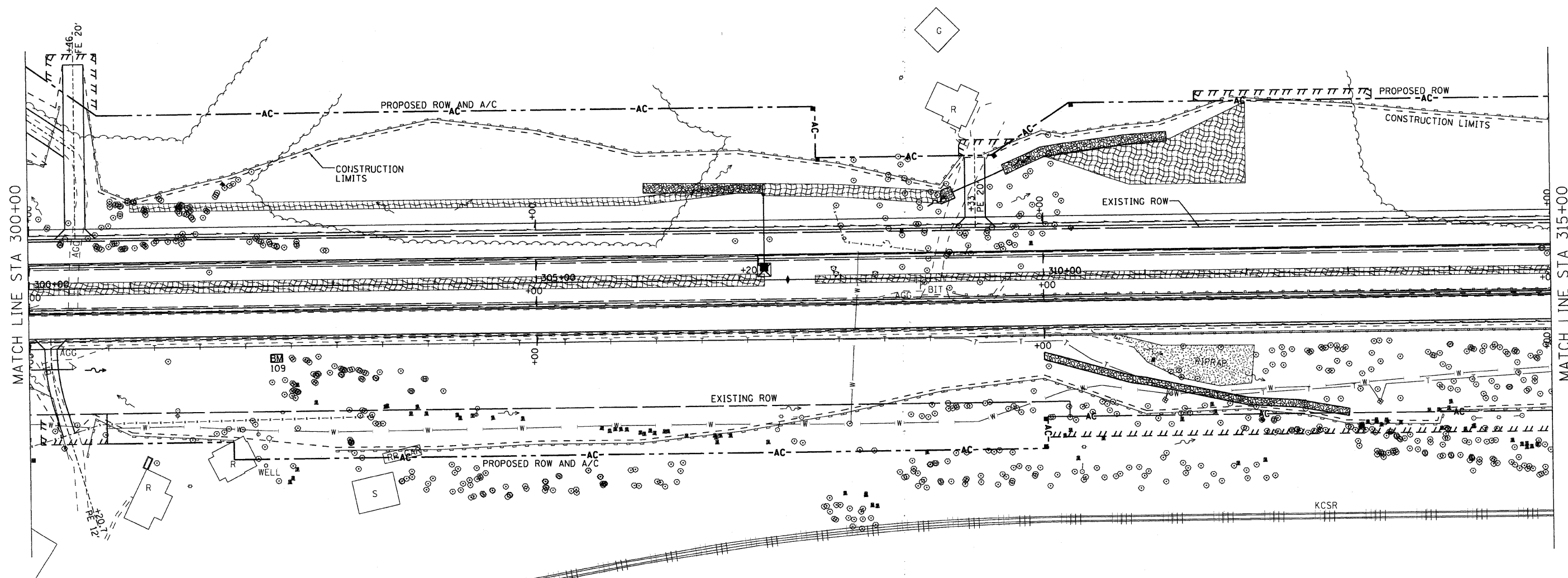
FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON/JERSEY COUNTY

DRAWN BY: EMR CHECKED BY: GLF

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
310	60-16-1&1B,42-1&1B	MADISON/JERSEY	481	91
STA.	TO STA.			
EXISTING CONDITIONS:				

CONTRACT NO. 76318

PLAN	NO.	DATE
DESIGNED		
CHECKED		
BY		









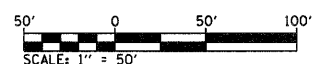
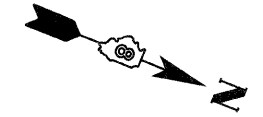
MATCH LINE STA 300+00

MATCH LINE STA 315+00

99+23, 61' RT.
XX.XX

**EROSION AND SEDIMENT CONTROL
STANDARD SYMBOLS**

-  PERIMETER EROSION BARRIER - SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
-  EROSION CONTROL BLANKET
-  FLOW LINES
-  INLET AND PIPE PROTECTION - STRAW BALES, FILTER FABRIC, AGGREGATES
-  PERMANENT EARTH BERM DITCH CHECK
-  RIPRAP



ILLINOIS DEPARTMENT OF TRANSPORTATION

**EROSION CONTROL PLAN
STA 300+00 TO 315+00**

FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON/JERSEY COUNTY

REVISIONS	
NAME	DATE

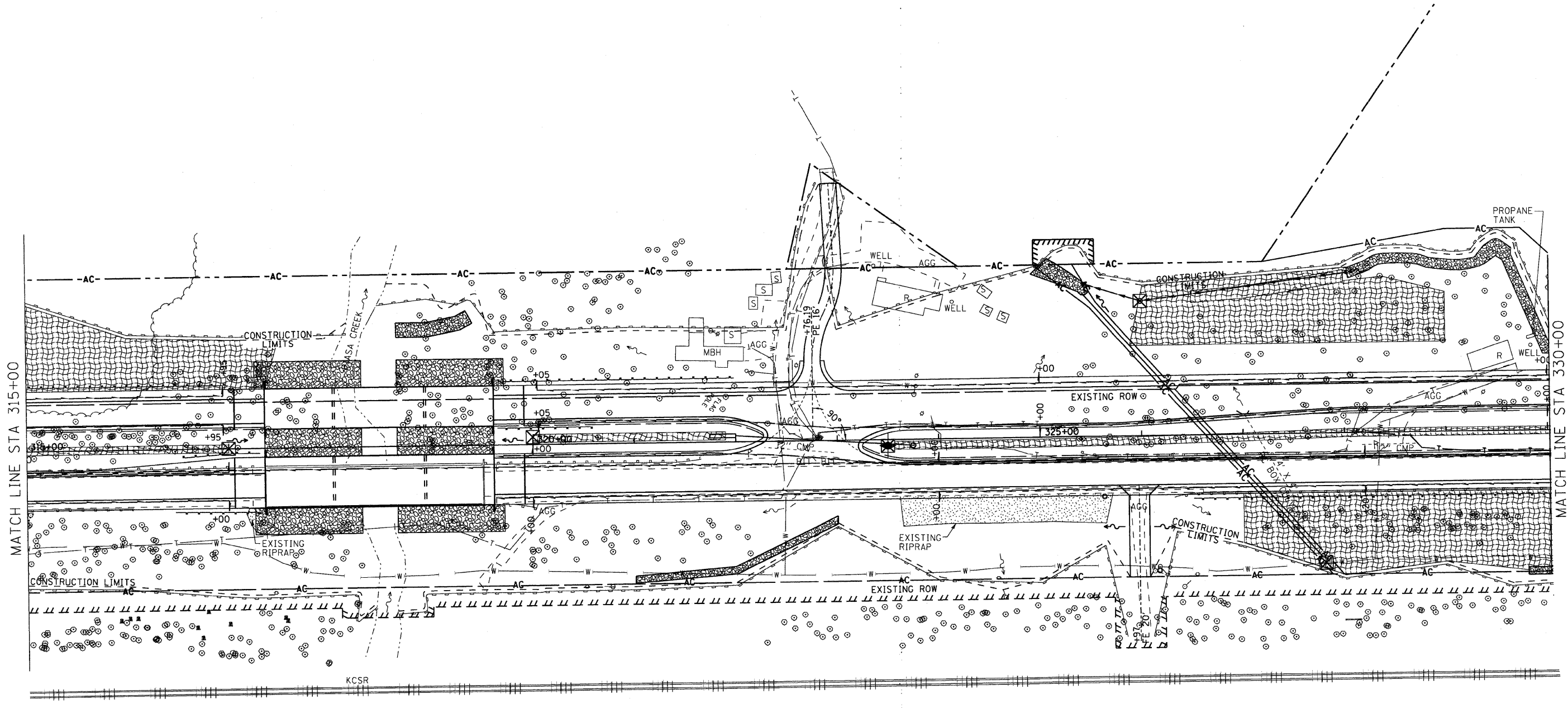
DRAWN BY: EMR

CHECKED BY: GLF

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
310	60-16-1&1B,42-1&1B	MADISON/JERSEY	481	92
STA.		TO STA.		
EXISTING CONDITIONS:				

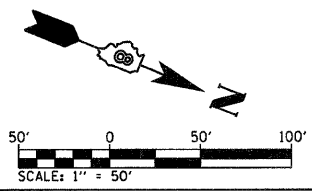
CONTRACT NO. 76318

DATE	
BY	
SURVEYED	
PLOTTED	
CHECKED	
BY	
NO. OF WAY CHECKED	
ADD FILE NAME	
PLAN	
NOTE BOOK	
NO.	



**EROSION AND SEDIMENT CONTROL
STANDARD SYMBOLS**

- PERIMETER EROSION BARRIER - SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
- EROSION CONTROL BLANKET
- FLOW LINES
- INLET AND PIPE PROTECTION - STRAW BALES, FILTER FABRIC, AGGREGATES
- PERMANENT EARTH BERM DITCH CHECK
- RIPRAP



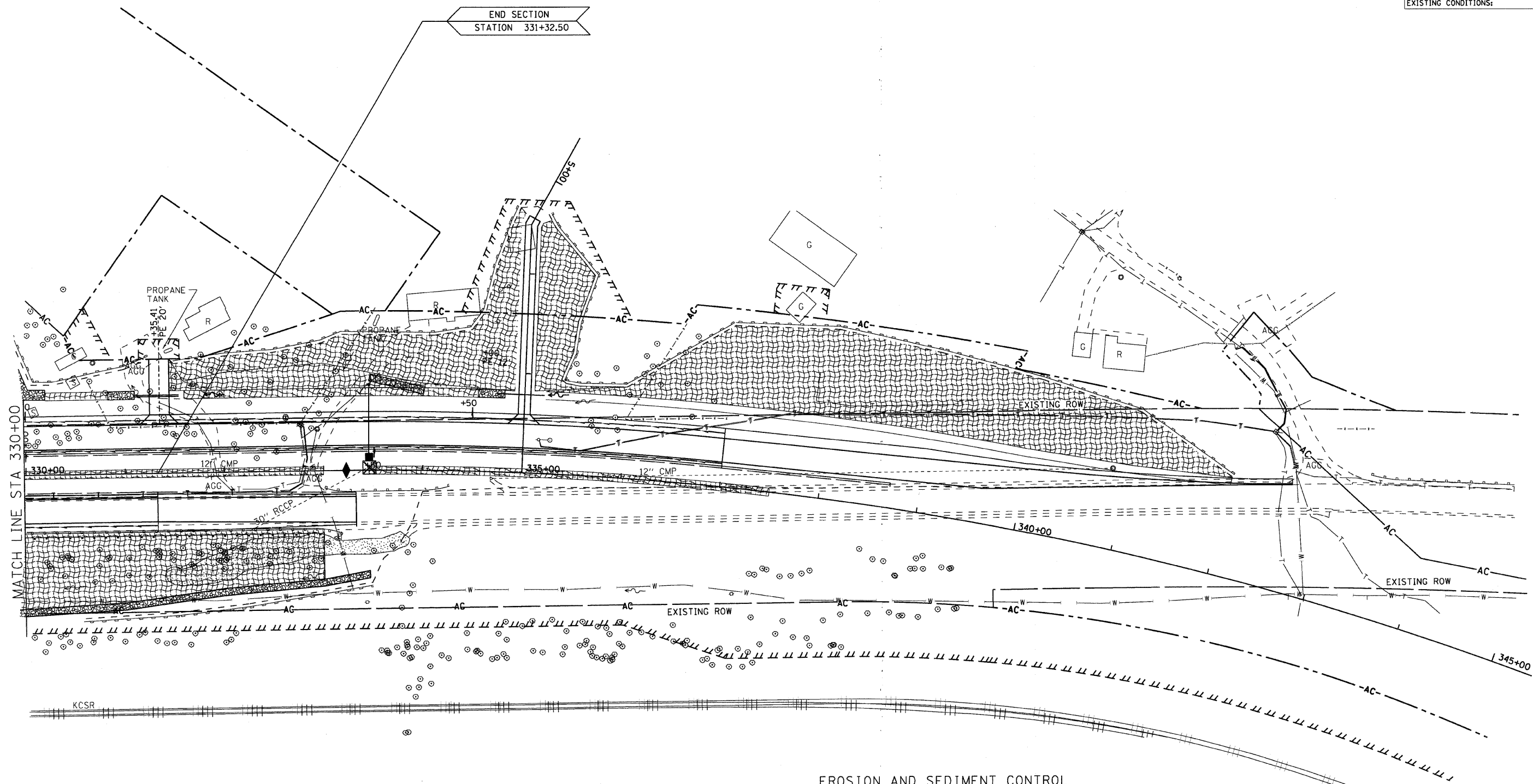
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**EROSION CONTROL PLAN
STA 315+00 TO 330+00**
FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON/JERSEY COUNTY
DRAWN BY: EMR
CHECKED BY: GLF

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON/JERSEY	481	93
STA. TO STA.		EXISTING CONDITIONS:		

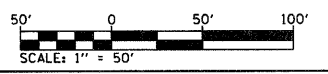
CONTRACT NO. 76318

PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	
	ALL DIMENSIONS CHECKED	
	CADD FILE NAME	



**EROSION AND SEDIMENT CONTROL
STANDARD SYMBOLS**

- PERIMETER EROSION BARRIER - SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
- EROSION CONTROL BLANKET
- FLOW LINES
- INLET AND PIPE PROTECTION - STRAW BALES, FILTER FABRIC, AGGREGATES
- PERMANENT EARTH BERM DITCH CHECK
- RIPRAP



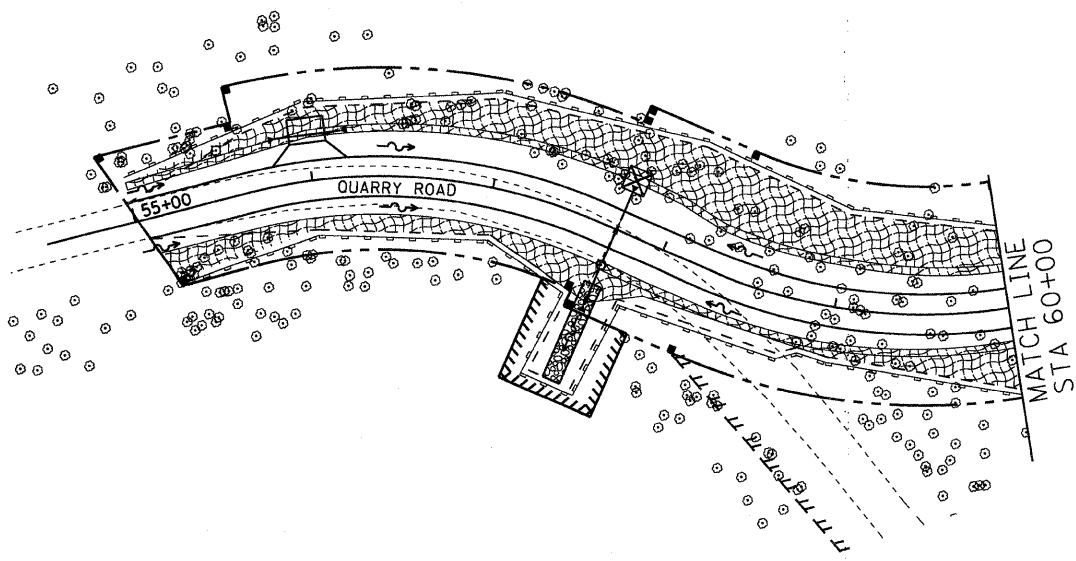
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
EROSION CONTROL PLAN
STA 330+00 TO 337+00
 FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON/JERSEY COUNTY
 DRAWN BY: EMR
 CHECKED BY: GLF

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	94
STA. _____ TO STA. _____				

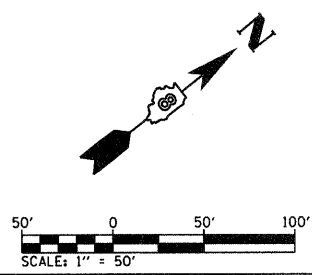
EXISTING CONDITIONS:
CONTRACT NO. 76318

PLAN	REVISIONS	DATE
NO.	NO.	
BY	BY	
DATE	DATE	
NO.	NO.	
BY	BY	
DATE	DATE	
NO.	NO.	
BY	BY	
DATE	DATE	
NO.	NO.	



**EROSION AND SEDIMENT CONTROL
STANDARD SYMBOLS**

- PERIMETER EROSION BARRIER - SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
- EROSION CONTROL BLANKET
- FLOW LINES
- INLET AND PIPE PROTECTION - STRAW BALES, FILTER FABRIC, AGGREGATES
- PERMANENT EARTH BERM DITCH CHECK
- RIPRAP









REVISIONS	
NAME	DATE

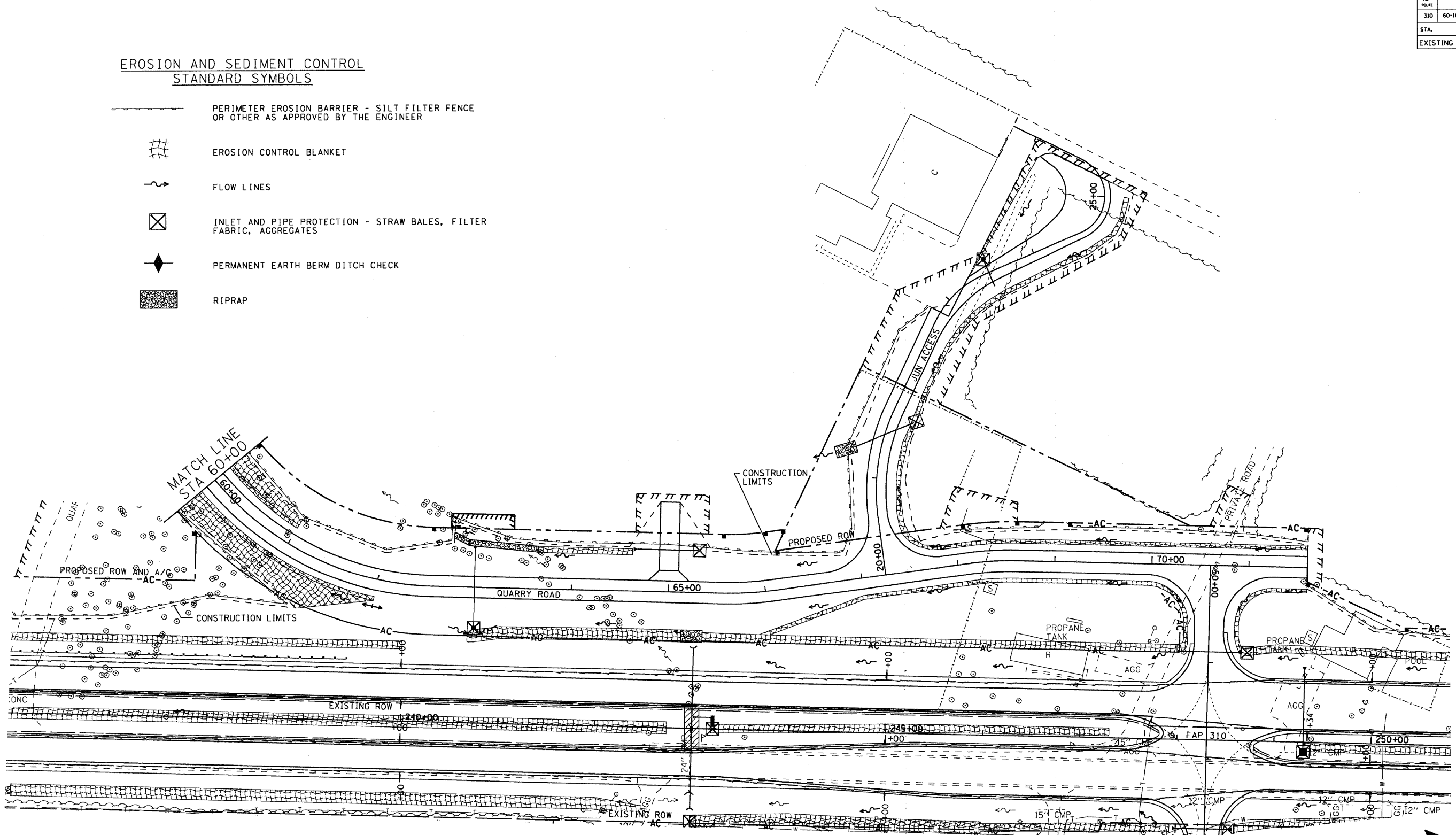
ILLINOIS DEPARTMENT OF TRANSPORTATION
EROSION CONTROL PLAN
QUARRY ROAD
FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON/JERSEY COUNTY
DRAWN BY: EMR
CHECKED BY: GLF

FAP NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	95
STA. TO STA.		EXISTING CONDITIONS:		
CONTRACT NO. 76318				

**EROSION AND SEDIMENT CONTROL
STANDARD SYMBOLS**

-  PERIMETER EROSION BARRIER - SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
-  EROSION CONTROL BLANKET
-  FLOW LINES
-  INLET AND PIPE PROTECTION - STRAW BALES, FILTER FABRIC, AGGREGATES
-  PERMANENT EARTH BERM DITCH CHECK
-  RIPRAP

PLAN	SUBMITTED	DATE
NOTE BOOK NO.	PLOTTED	
	CHECKED	
	BY	
	DATE	
	FILE NAME	



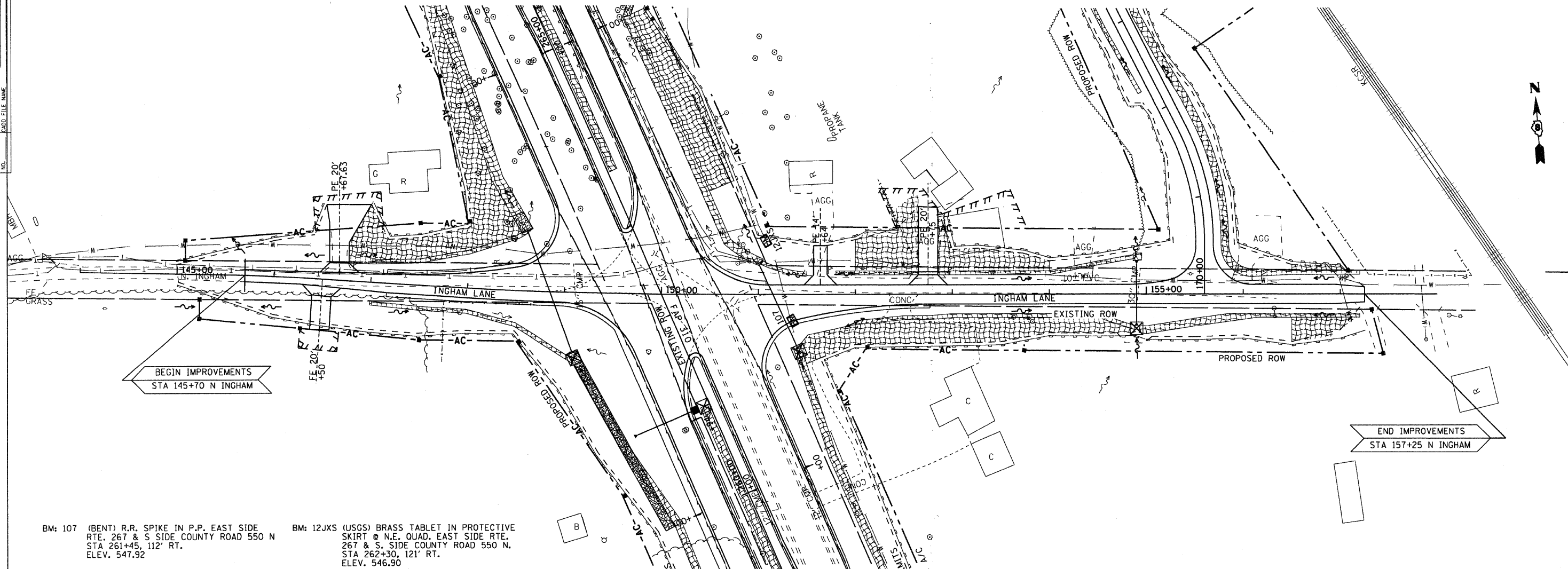
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
EROSION CONTROL PLAN
QUARRY ROAD
 FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON/JERSEY COUNTY
 DRAWN BY: EMR
 CHECKED BY: GLF

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	96
STA.	138+00	TO STA.	150+00	

EXISTING CONDITIONS: CONTRACT NO. 76318

PLAN	DATE
SURVEYED	
ALIGNED	
CHECKED	
BY	
NO.	









BEGIN IMPROVEMENTS
STA 145+70 N INGHAM

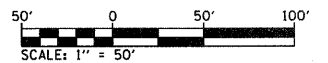
END IMPROVEMENTS
STA 157+25 N INGHAM

BM: 107 (BENT) R.R. SPIKE IN P.P. EAST SIDE
RTE. 267 & S SIDE COUNTY ROAD 550 N
STA 261+45, 112' RT.
ELEV. 547.92

BM: 12JXS (USGS) BRASS TABLET IN PROTECTIVE
SKIRT @ N.E. QUAD. EAST SIDE RTE.
267 & S. SIDE COUNTY ROAD 550 N.
STA 262+30, 121' RT.
ELEV. 546.90

**EROSION AND SEDIMENT CONTROL
STANDARD SYMBOLS**

-  PERIMETER EROSION BARRIER - SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
-  EROSION CONTROL BLANKET
-  FLOW LINES
-  INLET AND PIPE PROTECTION - STRAW BALES, FILTER FABRIC, AGGREGATES
-  PERMANENT EARTH BERM DITCH CHECK
-  RIPRAP



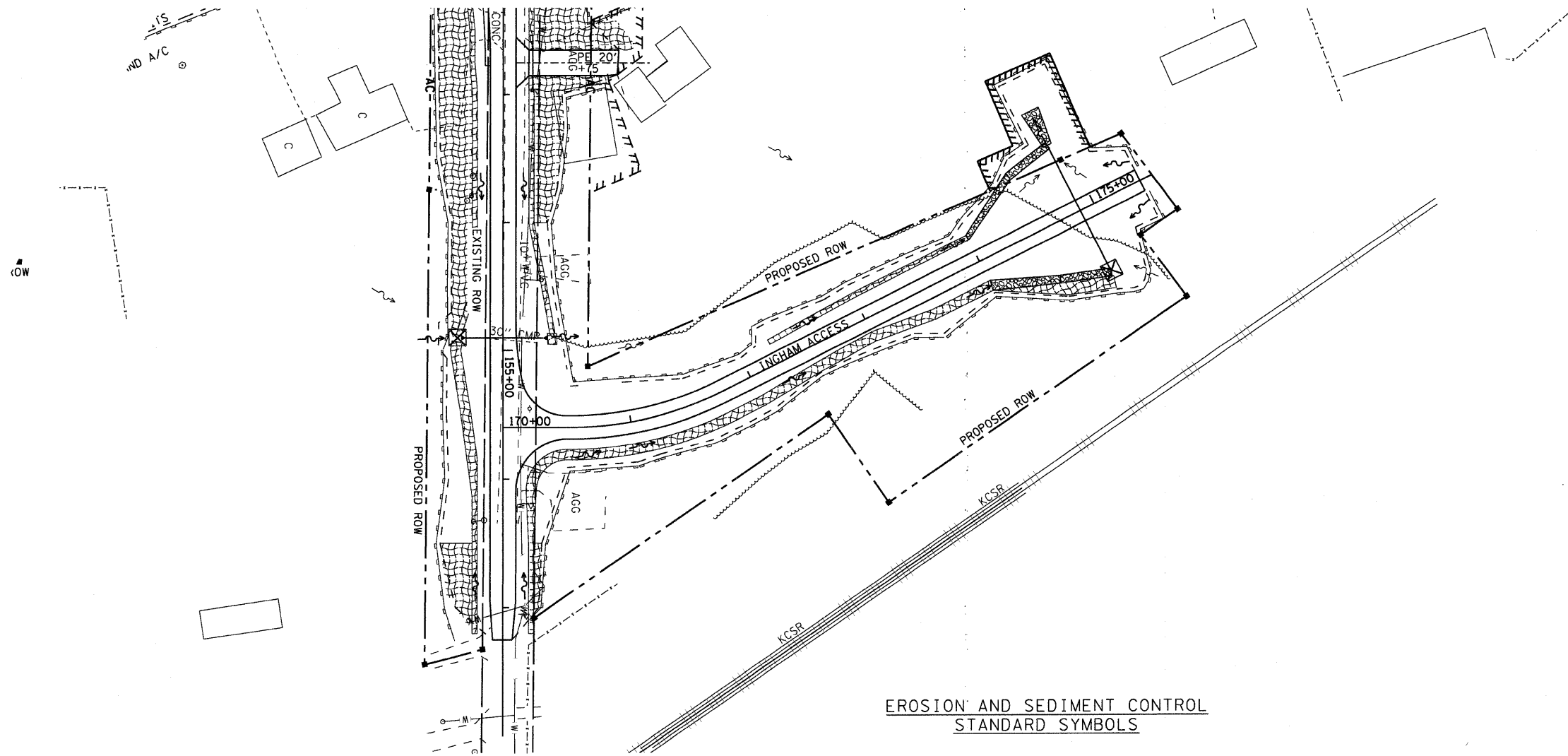
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**EROSION CONTROL PLAN
NORTH INGHAM**
FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON/JERSEY COUNTY
DRAWN BY: EMR
CHECKED BY: GLF

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
310	60-16-1&1B,42-1&1B	MADISON AND JERSEY	481	97
STA. 160+00		TO STA. 174+00		
EXISTING CONDITIONS:				

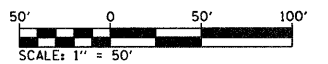
CONTRACT NO. 76318

PLAN	DATE
SURVEYED	
PLOTTED	
ALIGNED	
REVISIONS CHECKED	
CADD FILE NAME	
NO.	



**EROSION AND SEDIMENT CONTROL
STANDARD SYMBOLS**

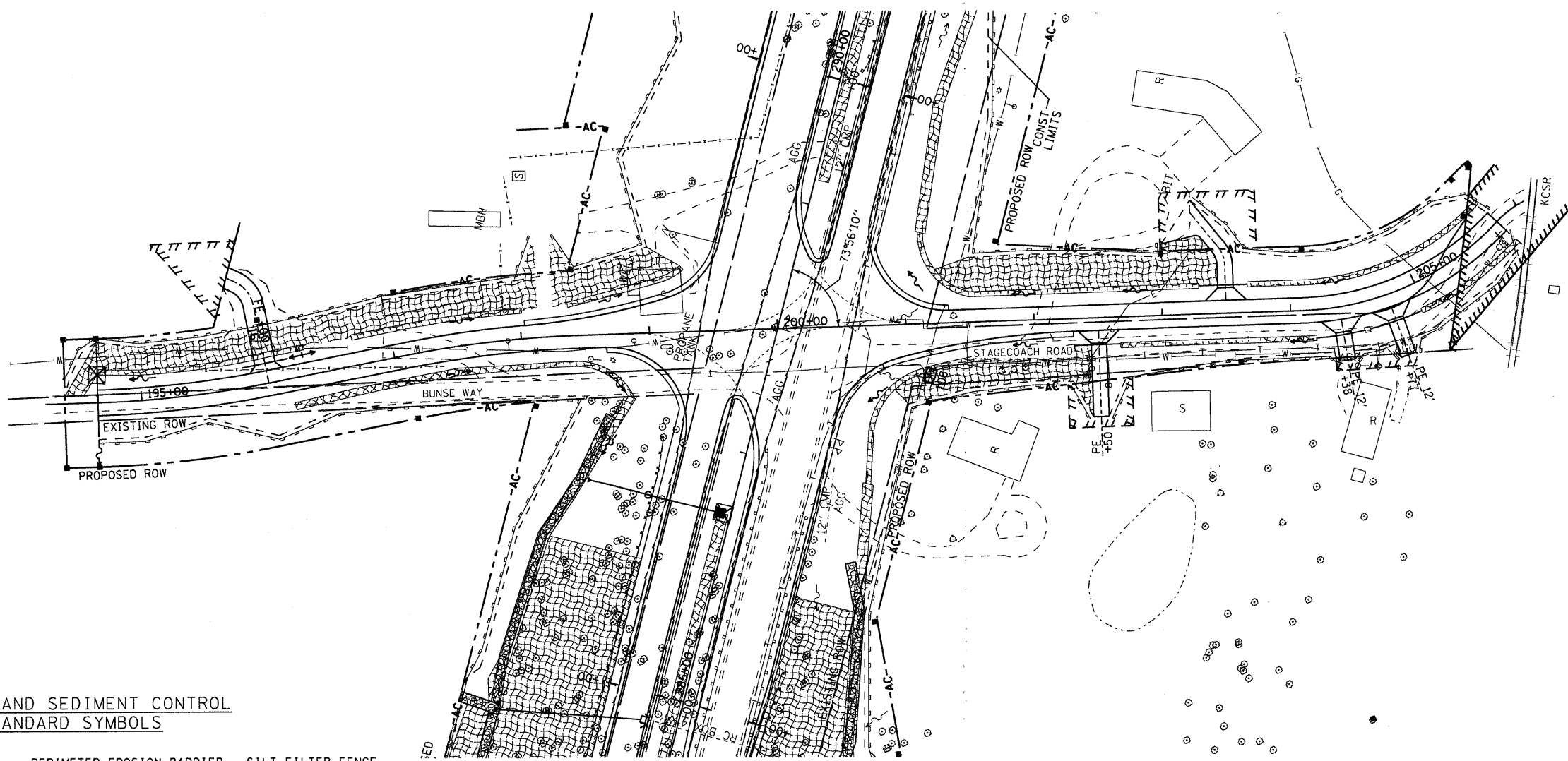
- PERIMETER EROSION BARRIER - SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
- EROSION CONTROL BLANKET
- FLOW LINES
- INLET AND PIPE PROTECTION - STRAW BALES, FILTER FABRIC, AGGREGATES
- PERMANENT EARTH BERM DITCH CHECK
- RIPRAP









REVISIONS	
NAME	DATE

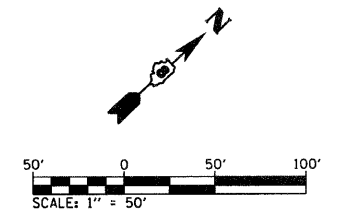
ILLINOIS DEPARTMENT OF TRANSPORTATION
EROSION CONTROL PLAN
INGHAM ACCESS DRIVE
FAP ROUTE 310
SECTION 60-16-1&1B,42-1&1B
MADISON/JERSEY COUNTY
DRAWN BY: EMR
CHECKED BY: GLF

PLAN	SURVEYED	DATE
NO.	BY	
	PLOTTED	
	NOTED	
	REVIEWED	
	BY	
	DATE	



**EROSION AND SEDIMENT CONTROL
STANDARD SYMBOLS**

-  PERIMETER EROSION BARRIER - SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
-  EROSION CONTROL BLANKET
-  FLOW LINES
-  INLET AND PIPE PROTECTION - STRAW BALES, FILTER FABRIC, AGGREGATES
-  PERMANENT EARTH BERM DITCH CHECK
-  RIPRAP



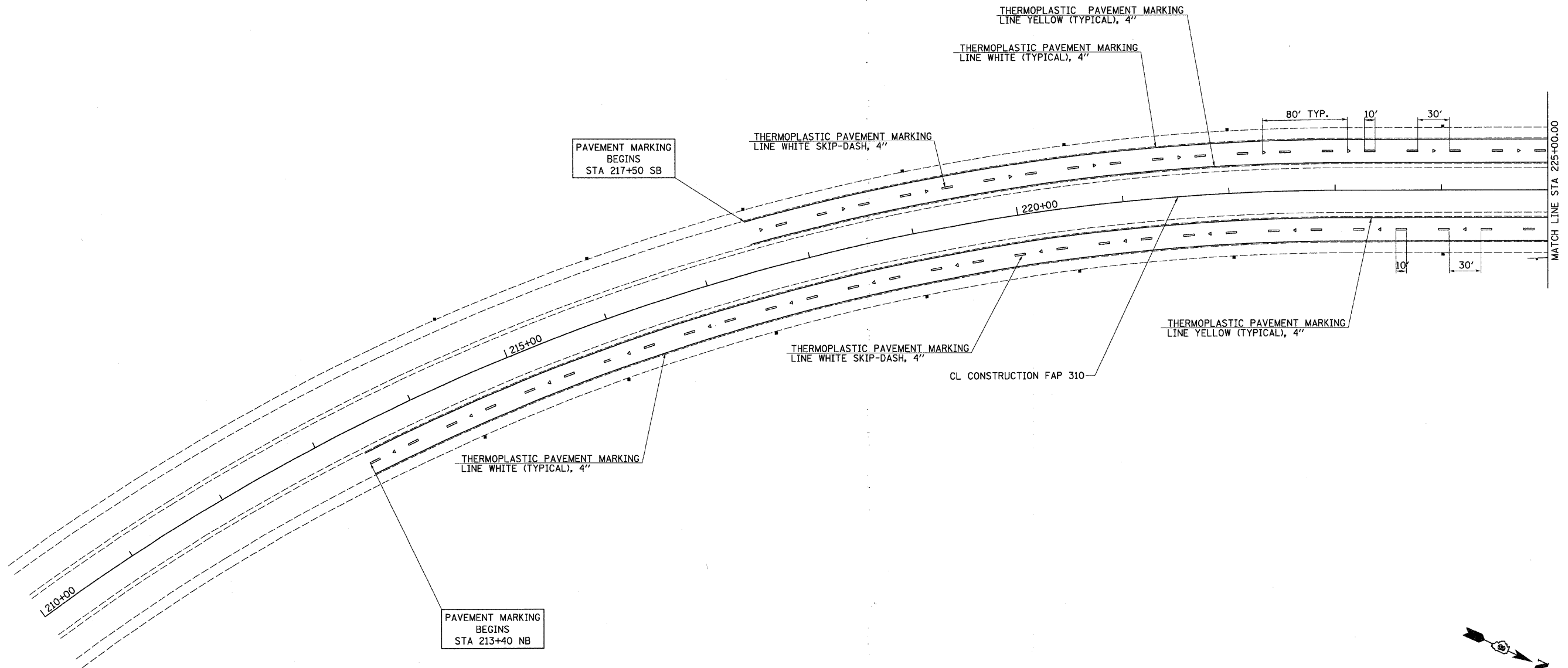
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
EROSION CONTROL PLAN
BUNSE WAY / STAGECOACH ROAD
 FAP ROUTE 310
 SECTION 60-16-1&1B,42-1&1B
 MADISON/JERSEY COUNTY
 DRAWN BY: EMR
 CHECKED BY: GLF

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16-1&1B, 42-1&1B	MADISON & JERSEY	481	99
STA. 210+00.00		TO STA. 225+00.00		

CONTRACT NO. 76318

PLAN	DATE	BY
SURVEYED		
ALIGNED		
CHECKED		
RT. OF WAY CHECKED		
NO. _____		
ADD FILE NAME		



DATE
DRAWN
CHECKED
DATE
SCALE: 1" = 50'

2104pmk001c

REFER TO STANDARDS 635001, 701901, 720001, 720006, 720011, 728001, 780001, 781001 & FOR SIGNING AND STRIPING DETAILS.

- LEGEND**
- ▶ RAISED REFLECTIVE PAVEMENT MARKERS 1-WAY, AMBER
 - ▷ RAISED REFLECTIVE PAVEMENT MARKERS 1-WAY, CRYSTAL
 - DELINEATORS (STD. #635001)

Hutchison Engineering Inc.
Since 1945
Jacksonville * Shorewood

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SIGNING AND STRIPING PLAN
FAP ROUTE 310
SECTION 60-16-1&1B, 42-1&1B
MADISON AND JERSEY COUNTIES

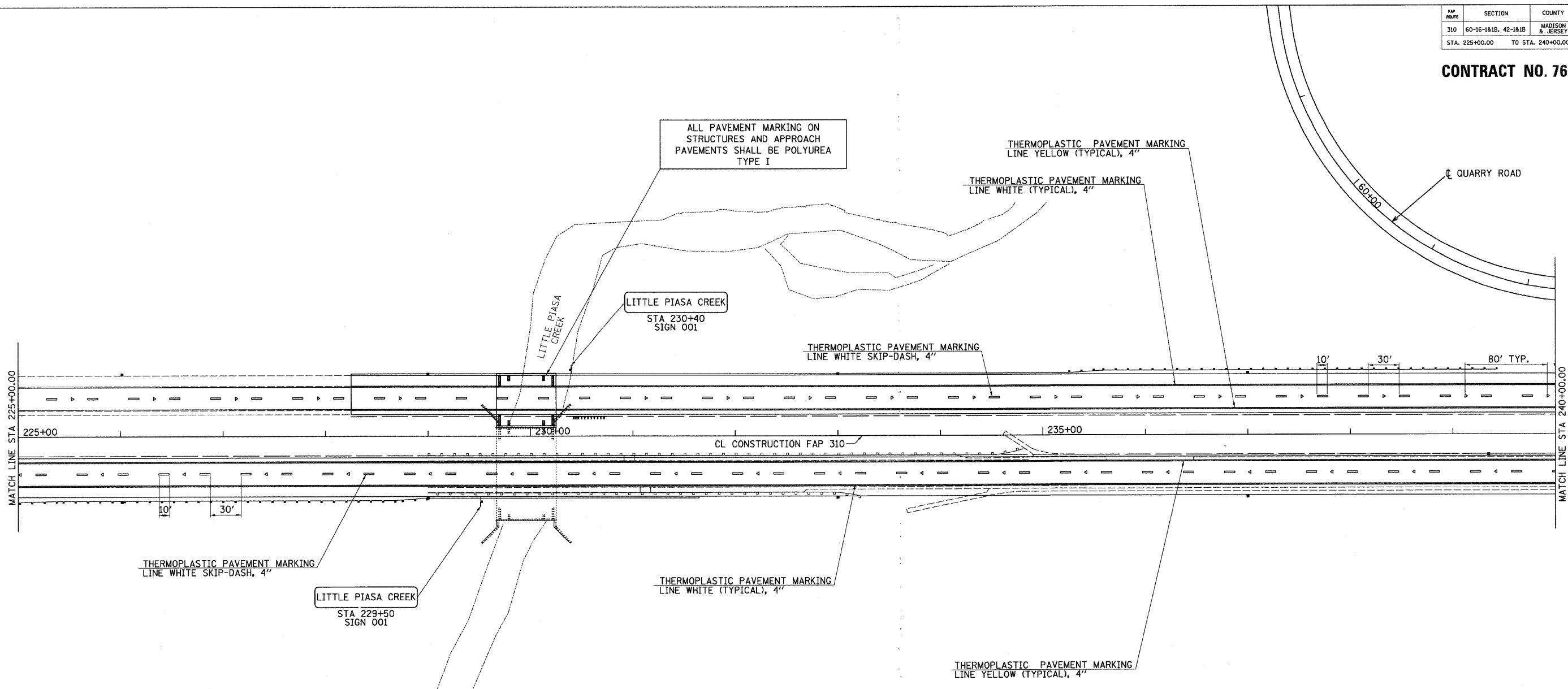
SCALE: VERT. _____
HORIZ. _____
DATE FEBRUARY 25, 2010
DRAWN BY KLS
CHECKED BY AWM

PLOT DATE: *DATE-TIME*

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
310	60-16-1&1B, 42-1&1B	MADISON & JERSEY	481	100
STA. 225+00.00		TO STA. 240+00.00		

CONTRACT NO. 76318

PLAN	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF WAY CHECKED	
	CADD FILE NAME	



ALL PAVEMENT MARKING ON STRUCTURES AND APPROACH PAVEMENTS SHALL BE POLYUREA TYPE I

THERMOPLASTIC PAVEMENT MARKING LINE YELLOW (TYPICAL), 4"

THERMOPLASTIC PAVEMENT MARKING LINE WHITE (TYPICAL), 4"

LITTLE PIASA CREEK
STA 230+40
SIGN 001

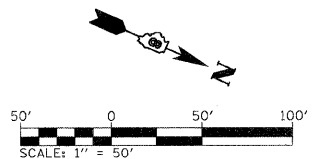
THERMOPLASTIC PAVEMENT MARKING LINE WHITE SKIP-DASH, 4"

THERMOPLASTIC PAVEMENT MARKING LINE WHITE SKIP-DASH, 4"

LITTLE PIASA CREEK
STA 229+50
SIGN 001

THERMOPLASTIC PAVEMENT MARKING LINE WHITE (TYPICAL), 4"

THERMOPLASTIC PAVEMENT MARKING LINE YELLOW (TYPICAL), 4"



LEGEND

- ▶ RAISED REFLECTIVE PAVEMENT MARKERS 1-WAY, AMBER
- ▷ RAISED REFLECTIVE PAVEMENT MARKERS 1-WAY, CRYSTAL
- DELINEATORS (STD. #635001)

REFER TO STANDARDS 635001, 701901, 720001, 720006, 720011, 728001, 780001, 781001 & FOR SIGNING AND STRIPING DETAILS.

Hutchison Engineering Inc.
Since 1945
Jacksonville * Shorewood

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SIGNING AND STRIPING PLAN
FAP ROUTE 310
SECTION 60-16-1&1B, 42-1&1B
MADISON AND JERSEY COUNTIES

SCALE: VERT. _____
HORIZ. _____
DATE: FEBRUARY 25, 2010

DRAWN BY KLS
CHECKED BY AWM

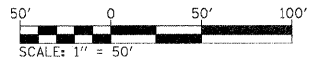
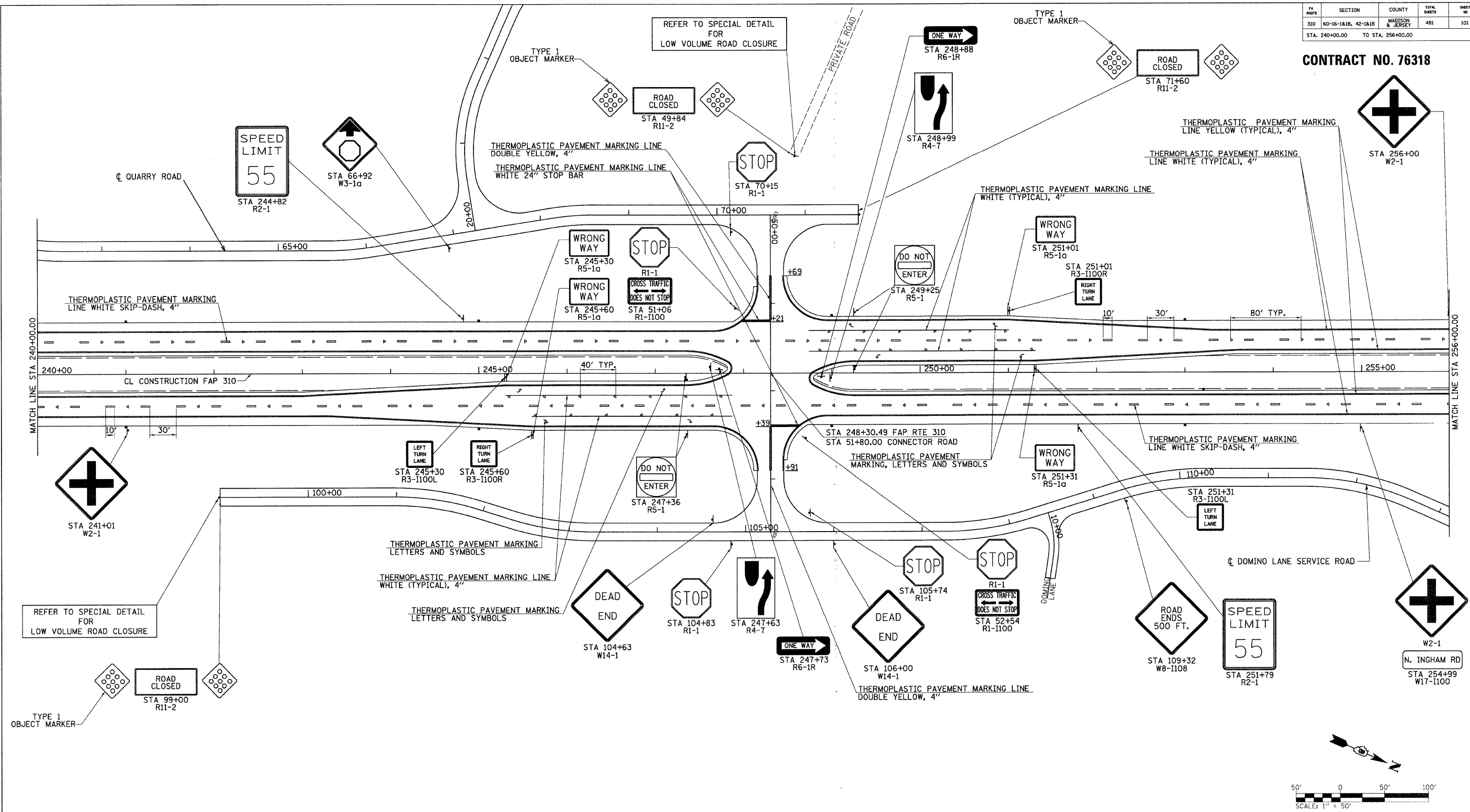
PLOT DATE: *DATE-TIME*

FA ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
310	60-16-1&1B, 42-1&1B	MADISON & JERSEY	481	101
STA. 240+00.00		TO STA. 256+00.00		

CONTRACT NO. 76318

DATE	BY
DATE	BY
DATE	BY
DATE	BY
DATE	BY

DATE TIME
 DATE TIME
 DATE TIME
 DATE TIME
 DATE TIME
 DATE TIME
 DATE TIME
 DATE TIME
 DATE TIME
 DATE TIME
 DATE TIME



REFER TO STANDARDS 635001, 701901, 720001, 720006, 720011, 728001, 780001, 781001 & FOR SIGNING AND STRIPING DETAILS.

- LEGEND**
- ▶ RAISED REFLECTIVE PAVEMENT MARKERS 1-WAY, AMBER
 - ▶ RAISED REFLECTIVE PAVEMENT MARKERS 1-WAY, CRYSTAL
 - DELINEATORS (STD. *635001)

Hutchison Engineering Inc.
 Since 1945
 Jacksonville * Shorewood

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SIGNING AND STRIPING PLAN
 FAP ROUTE 310
 SECTION 60-16-1&1B, 42-1&1B
 MADISON AND JERSEY COUNTIES
 SCALE: VERT. _____
 HORIZ. _____
 DATE: FEBRUARY 25, 2010
 DRAWN BY: KLS
 CHECKED BY: AWM
 PLOT DATE: *DATE-TIME*