

07-30-2021 LETTING ITEM 010

FOR INDEX OF SHEETS, SEE SHEET NO. 2

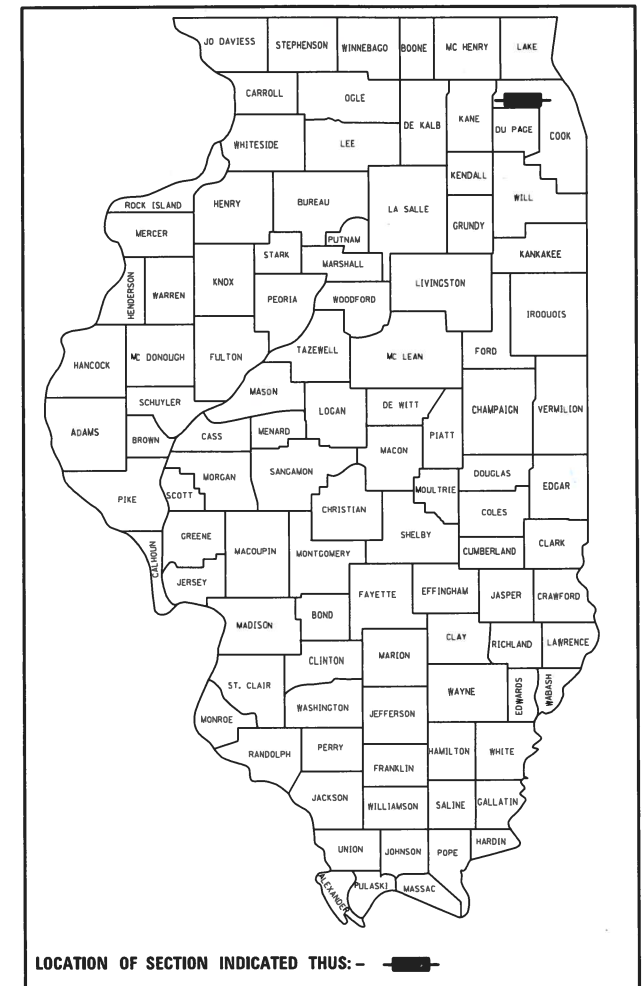
FOR LIST OF APPLICABLE HIGHWAY STANDARDS
SEE SHEET 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

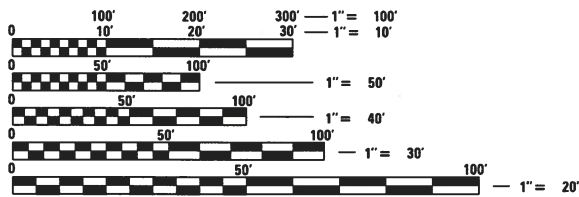
ROUTE MUN 2134
SUMMIT DRIVE
FROM WISE ROAD TO SCHAUMBURG ROAD
ROADWAY IMPROVEMENT PROJECT
SECTION NO.: 20-00135-00-RS
PROJECT NO: T007(396)
VILLAGE OF SCHAUMBURG
COOK COUNTY
JOB NO. C-91-168-21

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-RS	COOK	53	1
		ILLINOIS	CONTRACT NO. 61H16	



TRAFFIC DATA

SUMMIT DRIVE
ADT (YEAR) = 3600 (2020)
SPEED LIMIT = 25 MPH
DESIGN DESIGNATION: MINOR COLLECTOR



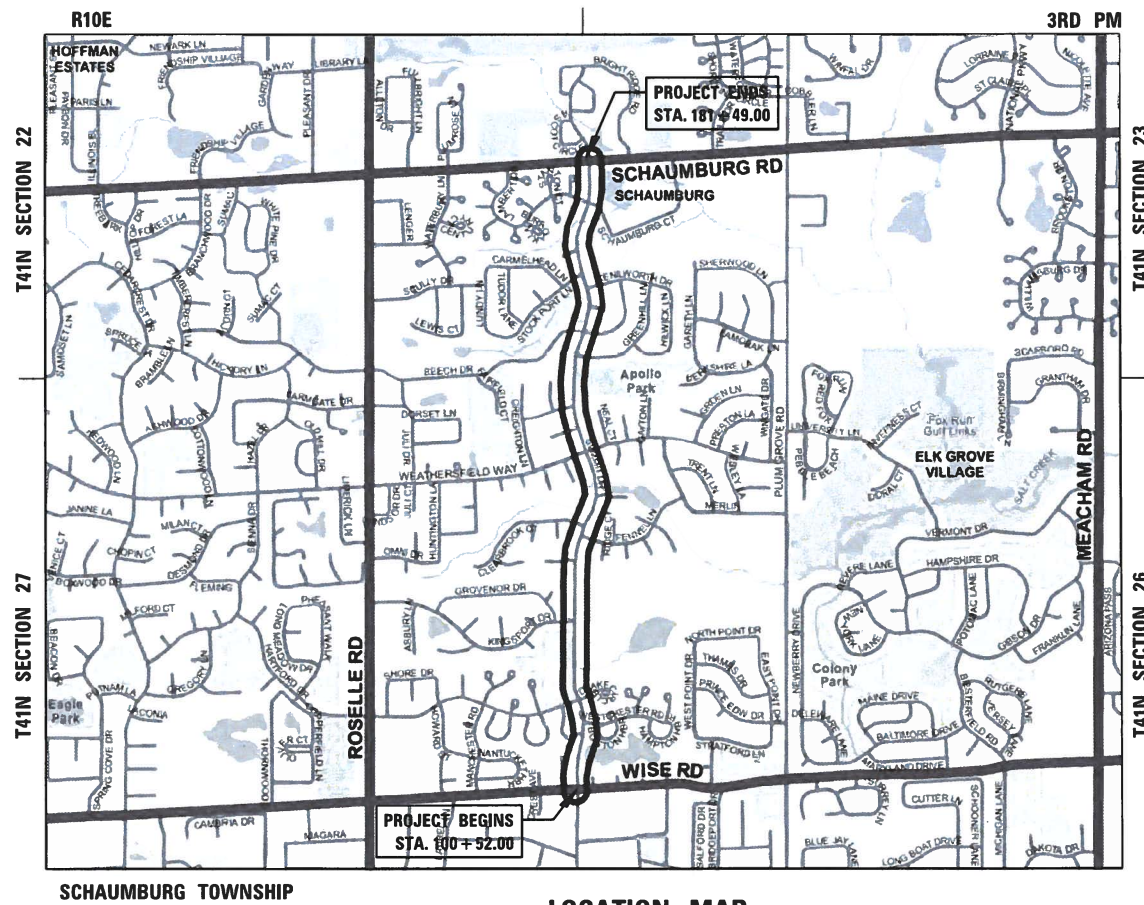
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CB CHRISTOPHER B. BURKE ENGINEERING, LTD.
9575 W. Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

PROFESSIONAL DESIGN FIRM NO. 184-001175
EXPIRATION DATE: 04/30/2021

CONTRACT NO. 61H16



SCHAUMBURG TOWNSHIP

LOCATION MAP
(NOT TO SCALE)

GROSS LENGTH = 8,097 FT. = 1.53 MILES
NET LENGTH = 8,097 FT. = 1.53 MILES

Gary Rozwadowski
ENGINEER DATE _____
GARY ROZWADOWSKI
ILLINOIS REGISTRATION No. 062-054957
EXPIRATION DATE: 11/30/21

AGENCY RESPONSIBLE FOR LETTING

APPROVED April 12 2021
Lisa M. Kiser
ENGINEERING DIVISION MANAGER, VILLAGE OF SCHAUMBURG

PASSED MAY 18 2021
Chris...
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASED FOR BID
BASED ON LIMITED REVIEW MAY 18 2021
Jose Rios
REGIONAL ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

FEDERAL AID ENGINEER: CARMEN E. RAMOS, P.E. SCHAUMBURG, ILLINOIS

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS
3	GENERAL NOTES
4-5	SUMMARY OF QUANTITIES
6	TYPICAL SECTIONS
7-8	ALIGNMENT AND TIES
9-23	EXISTING CONDITIONS AND REMOVAL PLANS
24-38	PROPOSED ROADWAY PLANS AND STRIPING
39-43	ADA RAMP GRADING PLANS
44-45	SCHAUMBURG CONSTRUCTION DETAILS
46-53	DISTRICT ONE DETAILS

IDOT STANDARDS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-11	PERPENDICULAR CURB RAMP FOR SIDEWALKS
424016-05	MID-BLOCK CURB RAMP FOR SIDEWALKS
424021-06	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-05	FRAME AND LIDS TYPE 1
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
780001-05	TYPICAL PAVEMENT MARKINGS

DISTRICT ONE STANDARDS

BD-08	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
BD-24	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-16	SHORT TERM PAVEMENT MARKINGS LETTERS AND SYMBOLS
TC-22	ARTERIAL ROAD INFORMATION SIGN

FILE NAME = N:\SCHAUMBURG\200227\Civi\NOT-01_200227	USER NAME = esidorozuk	DESIGNED - GR	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS AND IDOT HIGHWAY STANDARDS	MUN. RTE. 2134	SECTION 20-00135-00-RS	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 2	
		DRAWN - PM	REVISED -			SCALE: 10' SHEET OF SHEETS STA. TO STA.					
		CHECKED - GR	REVISED -			ILLINOIS FED. AID PROJECT					
Default		DATE - 6/4/2021	REVISED -			CONTRACT NO. 61H16					

GENERAL NOTES

- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2021, THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (IMUTCD), THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS" (SSTCI), "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JULY 2014 SEVENTH EDITION, THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS", IDOT HIGHWAY STANDARDS, AND "VILLAGE OF SCHAUMBURG STANDARD DRAWINGS" INCLUDED IN THE CONTRACT DOCUMENTS.
- THE CONTRACTOR SHALL NOTIFY THE VILLAGE OF SCHAUMBURG AT (847) 895 7100, AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK AND COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER.
- PRIOR TO THE START OF CONSTRUCTION, THE ENGINEER AND THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION MEETING. THE PURPOSE OF THE MEETING IS TO REVIEW ACCEPTABLE CONSTRUCTION PRACTICES IN ACCORDANCE WITH THE TRAFFIC CONTROL HIGHWAY STANDARDS AND VILLAGE ORDINANCES AND POLICIES.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THIS PROJECT.
- IF THERE ARE DISCREPANCIES BETWEEN THE JOB SITE AND WHAT IS SHOWN ON THE CONSTRUCTION PLANS, THE CONTRACTOR MUST IMMEDIATELY REPORT TO THE ENGINEER BEFORE DOING ANY WORK. THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTION FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECT BY OMISSIONS OR DISCREPANCIES. IN FAILING TO SECURE SUCH INSTRUCTION, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS/HER OWN RISK. IN THE EVENT OF ANY DOUBT OR QUESTIONS ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTIONS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- THE CONTRACTOR SHALL COMPLY WITH ALL STATE AND FEDERAL SAFETY REGULATIONS AS OUTLINED IN THE LATEST REVISIONS OF THE FEDERAL CONSTRUCTION SAFETY STANDARDS (SERIES 1926) AND THE APPLICABLE PROVISIONS AND REGULATIONS OF THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA STANDARDS OF THE WILLIAMS STEGER OCCUPATIONAL HEALTH STATE AND SAFETY ACT OF 1970).
- NEITHER THE ENGINEER NOR THE VILLAGE IS RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL LIMIT HIS/HER CONSTRUCTION ACTIVITIES TO THE WORK AREAS DESIGNATED ON THE PLANS. ANY DAMAGE TO AREAS OUTSIDE OF THESE LIMITS SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER.
- THE CONTRACTOR WILL BE REQUIRED TO RELOCATE OR REMOVE AND REPLACE SIGNS WHICH INTERFERE WITH CONSTRUCTION OPERATIONS, AND TO TEMPORARILY RESET ALL SUCH SIGNS DURING CONSTRUCTION OPERATIONS. IF EXISTING SIGNS ARE DAMAGED DURING THE REMOVAL AND REPLACEMENT PROCESS, THE SIGNS SHALL BE REPLACED BY THE CONTRACTOR. ANY SIGN DAMAGED DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR.
- DURING THE CONSTRUCTION OPERATIONS WHEN ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR DRAINAGE STRUCTURES SO THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS ALL DRAINAGE STRUCTURES SHALL BE FREE FROM ALL DIRT AND DEBRIS CAUSED BY THE CONSTRUCTION.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH ALL OTHER ROADWAY PROJECTS WITHIN THE AREA THAT ARE UNDER CONSTRUCTION AT THE SAME TIME.
- A QUANTITY OF AGGREGATE SUBGRADE IMPROVEMENT AND REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL HAVE BEEN PROVIDED FOR USE AT LOCATIONS OF UNSUITABLE OR UNSTABLE SOIL. THE NEED FOR REMOVAL AND REPLACEMENT WITH AGGREGATE SUBGRADE IMPROVEMENT WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. IF UNSUITABLE OR UNSTABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED.
- THE CONTRACTOR(S) SHALL KEEP EXISTING ADJACENT STREET PAVEMENTS CLEAN OF DIRT AND DEBRIS AND CLEAN PAVEMENTS ON A DAILY BASIS OR MORE OFTEN WHEN NECESSARY AND AS DIRECTED BY THE ENGINEER. ALL SAWCUTTING SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.
- THE THICKNESS OF HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.
- THE SUBGRADE STABILITY SHALL BE VERIFIED BY PROOF-ROLLING WITH A FULLY LOADED TANDEM-AXLE TRUCK TO THE APPROVAL OF THE ENGINEER.
- PAVING OF THE FULL ROADWAY WIDTH SHALL BE COMPLETED AT THE END OF EACH DAY OF PAVING TO PREVENT A LONGITUDINAL COLD JOINT FROM APPEARING WHEN OPPOSITE SIDES OF THE ROAD ARE PAVED ON DIFFERENT DAYS. THE CONTRACTOR SHALL ALSO ENSURE THAT AT THE END OF EACH DAY EACH PASS ENDS AT APPROXIMATELY THE SAME STATION TO PREVENT A COLD JOINT.
- IF, DURING CONSTRUCTION, THE CONTRACTOR ENCOUNTERS OR OTHERWISE BECOMES AWARE OF ANY SEWERS OR UNDERDRAINS OTHER THAN THOSE SHOWN ON THE PLANS, HE/SHE SHALL INFORM THE ENGINEER, WHO SHALL DIRECT THE WORK NECESSARY TO MAINTAIN OR REPLACE THE FACILITIES IN SERVICE AND TO PROTECT THEM FROM DAMAGE DURING CONSTRUCTION (IF MAINTAINED).

EXISTING FACILITIES TO BE MAINTAINED THAT ARE DAMAGED BECAUSE OF NON-COMPLIANCE WITH THIS PROVISION SHALL BE REPLACED.

- THE CONTRACTOR SHALL PROVIDE TEMPORARY TOILET FACILITIES AND HAND SANITIZING STATIONS FOR THE USE OF ALL OF THE CONTRACTOR'S PERSONNEL EMPLOYED ON THE WORK SITE. THE FACILITIES SHALL BE MAINTAINED IN PROPER SANITARY CONDITION THROUGHOUT THE PROJECT. THE LOCATION OF THE TEMPORARY FACILITIES SHALL BE APPROVED BY THE ENGINEER.

- ALL POTENTIALLY UNSTABLE SOIL SHOULD BE TESTED WITH A STATIC AND/OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC STANDARD AND CURRENT IDOT SUBGRADE STABILITY MANUAL.

SURVEY

- WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE THE MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL CAREFULLY PRESERVE ALL PROPERTY MARKS AND MONUMENTS UNTIL THE ENGINEER, AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION.

- ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.

- THE STATION/OFFSET/ELEVATIONS NOTED FOR ALL DRAINAGE STRUCTURES LOCATED IN THE CURB LINE REFER TO THE POSITION OF THE ADJACENT PROPOSED EDGE OF PAVEMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE OFFSET NECESSARY FOR EACH STRUCTURE TO SET THE FRAME AND GRATE IN THE PROPER LOCATION. ALL OTHER STRUCTURES ARE DIMENSIONED TO THE CENTER OF STRUCTURE.

- PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT, UNLESS OTHERWISE INDICATED.

UTILITIES

- ALL UNDERGROUND UTILITY LOCATIONS, INCLUDING BUT NOT LIMITED TO SANITARY AND STORM SEWERS, WATER MAINS AND THEIR RESPECTIVE SERVICE LINES, SHOWN ON THE PLANS ARE APPROXIMATE ONLY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL UTILITIES AS NECESSARY, PRIOR TO STARTING CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY J.U.L.I.E. AT (800) 892-0123, (OR 811) AND ALL PUBLIC AND PRIVATE UTILITIES BEFORE STARTING CONSTRUCTION (48 HOUR NOTICE IS REQUIRED).

- THE CONTRACTOR SHALL TEMPORARILY BRACE/SUPPORT EXISTING UTILITIES DURING THE CONSTRUCTION OF THE PROPOSED IMPROVEMENTS.

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. IF EXISTING UTILITY LINES OF ANY NATURE ARE ENCOUNTERED WHICH CONFLICT WITH NEW CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT MAY BE RESOLVED.

SOIL EROSION AND SEDIMENT CONTROL

- SOIL EROSION AND SEDIMENT CONTROL (SESC) FEATURES MUST BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF UPLAND DISTURBANCE. SOIL DISTURBANCE MUST BE PHASED OR ENACTED IN SUCH A MANNER AS TO MINIMIZE EROSION SOIL STABILIZATION MEASURES. CONTRACTOR MUST CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY AND/OR PERMANENT MEASURES.

- UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE INSTALLED AT MINIMUM ACCORDING TO THE STANDARDS AND SPECIFICATIONS IN THE ILLINOIS URBAN MANUAL, REVISED TO LATEST VERSION AS AMENDED. A COPY OF THE APPROVED SOIL EROSION AND SEDIMENT CONTROL (SESC) PLAN MUST BE MAINTAINED ON THE SITE AT ALL TIMES.

- THE CONTRACTOR MUST CLEAN UP, GRADE THE WORK AREAS AS THE PROJECT PROGRESSES, AND INSTALL EROSION PROTECTION TO ELIMINATE THE CONCENTRATION OF RUNOFF, OR MUST INSTALL APPROPRIATE SEDIMENT CONTROL DEVICES TO TRAP SEDIMENT. PAVEMENT MUST BE CLEANED DAILY OR AS NECESSARY TO REMOVE TRACK-OUT MATERIAL.

- AFTER ALL PERIMETER SEDIMENT BARRIER IS REMOVED, THE AREAS DAMAGED BY THE PERIMETER SEDIMENT BARRIER MUST BE RESTORED.

- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DIVERT ALL WATER (GROUND, STORM, AND CONSTRUCTION) DURING CONSTRUCTION IN ORDER TO KEEP THE CONSTRUCTION AREAS FREED OF WATER. BYPASS PUMPING, INCLUDING SILT BAGS AND AN ENERGY DISSIPATION SURFACE FOR THE PUMPS, MAY BE REQUIRED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SIZE THE PUMPS APPROPRIATELY.

- DURING DE-WATERING/PUMPING OPERATIONS, ONLY UNCONTAMINATED WATER SHOULD BE ALLOWED TO DISCHARGE TO PROTECTED NATURAL AREAS, WATERS OF THE STATE, OR TO A STORM SEWER SYSTEM (IN ACCORDANCE WITH LOCAL PERMITS). INLET HOSES SHOULD BE PLACED IN A STABILIZED SUMP PIT OR FLOATED AT THE SURFACE OF THE WATER IN ORDER TO LIMIT THE AMOUNT OF SEDIMENT INTAKE. PUMPING OPERATIONS MAY BE DISCHARGED TO A STABILIZED AREA THAT CONSISTS OF AN ENERGY DISSIPATING DEVICE (E.G., STONE), SEDIMENT FILTER BAG, OR

BOTH. ADEQUATE EROSION AND SEDIMENT CONTROLS SHOULD BE USED DURING DE-WATERING OPERATIONS, AS NECESSARY. DEWATERING SEDIMENT LADEN WATER DIRECTLY INTO FIELD TILES, STORM WATER STRUCTURES, OR "WATERS OF THE US" IS PROHIBITED.

- SEDIMENT CONTROL BMPs (CONSTRUCTION ENTRANCES) SHALL BE CONSTRUCTED AT ALL LOCATIONS WHERE CONSTRUCTION TRAFFIC ENTERS OR LEAVES THE SITE. THESE LOCATIONS SHALL BE DETERMINED IN THE FIELD, AS NEEDED, BY THE ENGINEER. GRAVELED ROADS, RUMBLE STRIPS, ACCESS DRIVES, PARKING AREAS OF SUFFICIENT WIDTH AND LENGTH, AND VEHICLE WASH DOWN FACILITIES IF NECESSARY, MUST BE PROVIDED TO PREVENT THE DEPOSIT OF SOIL FROM BEING TRACKED ONTO PUBLIC OR PRIVATE ROADWAYS. ANY SOIL REACHING PUBLIC OR PRIVATE ROADWAY MUST BE REMOVED IMMEDIATELY.

- STOCKPILES OR SOIL MUST NOT BE LOCATED IN FLOOD PLAINS, RIPARIAN AREAS (VEGETATED FLOOD PLAINS), WETLANDS AND WATERS OF THE U.S., UNLESS OTHERWISE AUTHORIZED BY THE RELEVANT PERMITTING AUTHORITY. IF A STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN THREE DAYS, PERIMETER SEDIMENT BARRIER MUST BE PROVIDED.

- CONTRACTOR MUST INSTALL PERIMETER SEDIMENT BARRIER AT ANY LOCATION IN WHICH SHEET FLOWS MAY RESULT IN SEDIMENT RUNOFF OUTSIDE THE CONSTRUCTION LIMITS. THE CONTRACTOR MAY USE OTHER METHODS TO CONTROL RUNOFF, INCLUDING, BUT NOT LIMITED TO, TEMPORARY DIVERSION SWALES, TEMPORARY SEDIMENT TRAPS, SHAPED DITCHES TO CONVEY WATER, ETC.

- ALL PROPOSED AND EXISTING STORM SEWER INLET STRUCTURES (INCLUDING INLETS LOCATED WITHIN THE HAUL ROUTES) MUST BE PROTECTED WITH STORM SEWER INLET PROTECTION (I.E. INLET FILTERS) PER INLET PROTECTION DETAILS IN THE PLANS.

- STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.

- ALL TEMPORARY SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.

- NO PHOSPHOROUS SHALL BE ALLOWED IN ANY SEED FERTILIZERS, TEMPORARY OR PERMANENT, THROUGHOUT THE ENTIRE PROJECT LIMITS.

STORM SEWER

- FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES OF ALL NEW, ADJUSTED, OR RECONSTRUCTED STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION APPROPRIATE TO THEIR LOCATION.

VILLAGE OF SCHAUMBURG GENERAL NOTES

TRAFFIC CONTROL PROTECTION:

IMMEDIATELY AFTER THE EXISTING HOT-MIX ASPHALT STREET SURFACE HAS BEEN REMOVED; TEMPORARY RAMPS SHALL BE CONSTRUCTED WHEN REQUIRED BY THE ENGINEER TO ELIMINATE EXCESSIVE DROP-OFFS. THE TEMPORARY RAMPS SHALL BE REMOVED BEFORE PLACING THE HOT-MIX ASPHALT REPLACEMENT COURSES.

DISPOSAL OF EXCESS MATERIAL:

ALL EXCESS MATERIAL AND ANY OTHER DEBRIS SHALL BE DISPOSED OF BY THE CONTRACTOR AT AN OFF-SITE DISPOSAL AREA. EXISTING FRAMES AND GRATES IN GOOD CONDITION, AND NOT SCHEDULED FOR REUSE, SHALL BE LOADED AND DELIVERED TO THE VILLAGE OF SCHAUMBURG STORAGE YARD AT 714 SOUTH PLUM GROVE ROAD.

EXISTING STREET CLEANLINESS:

THE CONTRACTOR SHALL KEEP EXISTING ADJACENT STREETS CLEAN OF DIRT, MUD, AND OTHER DEBRIS AND, WHEN NECESSARY, CLEAN SAID PAVEMENTS ON A DAILY BASIS OR WHEN DIRECTED BY THE ENGINEER.

CLEAN-UP AND DISPOSAL:

THE CONTRACTOR SHALL MAINTAIN THE SITES IN A CLEAN AND ORDERLY MANNER. DEBRIS AND ANY SURPLUS MATERIAL SHALL BE REMOVED AS THE WORK PROCEEDS. RESTORATION SHALL PROCEED IN ACCORDANCE WITH THE PROJECT SPECIAL PROVISIONS WITH SODDING TO BE PLACED AFTER SEPTEMBER 1ST. IF THE ENGINEER SO DIRECTS, THE CONTRACTOR SHALL STOP ALL OTHER WORK AND CONCENTRATE ON CLEAN-UP AND RESTORATION. DEBRIS AND SURPLUS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR OFF-SITE. PILES OF EXCAVATED MATERIAL (PAVEMENT, STONE, SOIL, ETC.) SHALL BE REMOVED FROM THE SITE AT THE END OF THE WORKDAY. CONTRACTOR SHALL KEEP EXISTING ADJACENT STREETS CLEAN OF DIRT, MUD AND OTHER DEBRIS.

DRIVEWAY ACCESS

THIS PROJECT IS LOCATED WITHIN A FULLY DEVELOPED RESIDENTIAL AREA. IN ORDER TO MINIMIZE THE IMPACT OF CONSTRUCTION TO RESIDENTS, THE CONTRACTOR SHALL ADHERE TO THE FOLLOWING:

ACCESS TO ABUTTING PROPERTY SHALL BE PROVIDED AT ALL TIMES DURING CONSTRUCTION OPERATIONS. TIME REQUIRED FOR CONSTRUCTION AT DRIVEWAYS SHALL BE LIMITED TO THE MINIMUM AMOUNT PRACTICABLE FOR SAID CONSTRUCTION. THE ENGINEER OR REPRESENTATIVE OF THE ENGINEER WILL COORDINATE ANY RESIDENT SPECIAL NEEDS. THE CONTRACTOR SHALL, WHERE DIRECTED BY THE ENGINEER OR REPRESENTATIVE OF THE ENGINEER, PROVIDE IMMEDIATE ACCESS TO DRIVEWAYS AND INTERSECTING STREETS USING TEMPORARY AGGREGATE. THE CONTRACTOR SHALL AT ALL TIMES PROVIDE ACCESS FOR EMERGENCY VEHICLES DURING CONSTRUCTION.

ANY DRIVEWAY APRON THAT IS DISTURBED OR SCHEDULED FOR REPLACEMENT SHALL BE RESTORED TO THE SATISFACTION OF THE ENGINEER OR REPRESENTATIVE OF THE ENGINEER AFTER THE NEW CURB AND GUTTER HAS BEEN CONSTRUCTED. THE CONTRACTOR MUST SCHEDULE ALL CURB AND GUTTER, DRIVEWAY, AND SIDEWALK WORK SUCH THAT ONLY ONE SIDE OF THE STREET WILL BE UNDER CONSTRUCTION AT ANY ONE TIME. IN NO CASE SHALL AN OPEN EXCAVATION CAUSED BY REMOVAL OF EXISTING CURB AND GUTTER, DRIVEWAY, OR SIDEWALK, WHETHER FORMED OR NOT FORMED, REMAIN OPEN FOR MORE THAN 2 WORKING DAYS. FOR EXAMPLE, IF CURB AND GUTTER IS REMOVED MONDAY, IT MUST BE REPLACED NO LATER THAN WEDNESDAY OF THE SAME WEEK.

PRIOR TO REMOVING ANY DRIVEWAY ACCESS, THE CONTRACTOR SHALL PROVIDE THE VILLAGE SUFFICIENT TIME TO PROVIDE A 24 HOURS ADVANCED WRITTEN NOTICE TO THE RESIDENT/OWNER OF THE DRIVEWAY, ALLOWING THE RESIDENT/OWNER TIME TO REMOVE ANY VEHICLES.

BACKFILLING OF DISTURBED PAVEMENT SHALL BE COMPLETED NO MORE THAN 3 DAYS AFTER CURB AND GUTTER HAS BEEN PLACED TO ALLOW DRIVEWAY ACCESS. IF NECESSARY, THE CONTRACTOR SHALL PLACE TEMPORARY AGGREGATE BEHIND THE NEW CURB AND GUTTER UNTIL THE DRIVEWAY IS RESTORED.

SOD WATERING

THE CONTRACTOR WILL BE REQUIRED TO UTILIZE WATER FOR SOD INSTALLATION FROM EITHER THE SCHAUMBURG PUBLIC WORKS BUILDING AT 714 SOUTH PLUM GROVE ROAD OR AN ACCEPTABLE OUTSIDE SOURCE. VILLAGE POTABLE WATER USE WILL NOT BE ALLOWED. UNDER NO CIRCUMSTANCES SHALL A CONTRACTOR OBTAIN WATER BY OPENING A VILLAGE FIRE HYDRANT.

SOD INSTALLATION

A PRE-SOD INSTALLATION INSPECTION SHALL BE HELD WITH THE ENGINEER, GENERAL CONTRACTOR, AND LANDSCAPE SUB-CONTRACTOR. APPROVAL BY THE ENGINEER WILL BE REQUIRED PRIOR TO THE INSTALLATION OF SOD.

SOD AND TOPSOIL SHALL BE OBTAINED FROM AN I. D.O.T. APPROVED SUPPLIER. MATERIAL CERTIFICATIONS SHALL BE SUBMITTED TO THE ENGINEER FOR ALL MATERIALS PLACED.

CURB AND GUTTER REPLACEMENT

AFTER SPOT REMOVAL AND REPLACEMENT OF CURB AND GUTTER ON ALL VILLAGE STREETS TO BE REPAIRED, THE CONTRACTOR SHALL FRONT FILL THE DAMAGED PAVEMENT WITH 1/4" TO 1/2" COARSE FRAP, OR CM 16 (3/8") CRUSHED CHIP OR ANOTHER AGGREGATE APPROVED BY THE ENGINEER. PRIOR TO FRONT FILLING THE DAMAGED PAVEMENT, THE CONTRACTOR SHALL CLEAN ALL DEBRIS FROM THE AREA TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE DONE PRIOR TO THE PLACEMENT OF LEVELING BINDER.

MISCELLANEOUS

- WHENEVER NEW CONCRETE ABUTS EXISTING/NEW CONCRETE, SET A 1/2" THICK PRE-MOLDED FIBER EXPANSION JOINT AND DOWEL WITH SMOOTH 12" #4 BARS @ 24" O.C. THIS INCLUDES CONCRETE POURED ADJACENT TO ANY EXISTING SIDEWALKS AND CURBS. THE DOWEL BARS SHOULD BE 4" INTO EXISTING CONCRETE WITH 8" EXTENDING INTO NEW CONCRETE.

- PER VILLAGE OF SCHAUMBURG ORDINANCE 92-133 CONSTRUCTION HOURS ARE WEEKDAYS AND SATURDAYS 7:00 A.M. – 6:00 P.M.

- TRAFFIC SIGNAL MAINTENANCE: THE CONTRACTOR SHALL INFORM THE CCDOTH DESIGN ENGINEER AT (312) 603-1730 PRIOR TO THE START OF ANY WORK ON THE CONTRACT WITHIN COOK COUNTY RIGHT OF WAY REGARDING THE REMOVAL OF THE DETECTOR LOOPS ON SUMMIT DRIVE AT THE SCHAUMBURG ROAD TRAFFIC SIGNAL.

- PIPE UNDERDRAIN SHALL BE INSTALLED ACCORDING TO SECTION 601 OF THE SSRBC AND STANDARD 601001-05. TOP OF THE PIPE UNDERDRAIN SHALL BE PLACE MINIMUM 6" BELOW THE AGGREGATE SUBGRADE IMPROVEMENT LAYER. THE COST OF MAKING PIPE UNDERDRAIN CONNECTIONS TO DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE COST OF UNDERDRAINS.

FILE NAME =	USER NAME =	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES	MUN RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\SCHAUMBURG\200227\Civil\NOT-02.200227.sht	= esidorozuk	DRAWN -	REVISED -			2134	20-00135-00-R5	COOK	53	3
Default		CHECKED -	REVISED -			CONTRACT NO. 61H16				
		DATE -	REVISED -			SCALE: 10'	SHEET OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT

SUMMARY OF QUANTITIES

SUMMARY OF QUANTITIES			0005
CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY
20200100	EARTH EXCAVATION	CU YD	160
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	600
20800150	TRENCH BACKFILL	CU YD	122
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	5446
* 25200110	SODDING, SALT TOLERANT	SQ YD	5446
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	110
* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	110
* 25200200	SUPPLEMENTAL WATERING	UNIT	100
28000400	PERIMETER EROSION BARRIER	FOOT	1500
28000510	INLET FILTERS	EACH	31
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	600
35101400	AGGREGATE BASE COURSE, TYPE B	TON	90
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	35
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	25375
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	40
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	1580
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	4210
42400800	DETECTABLE WARNINGS	SQ FT	388

* = Specialty Item
 SP = Special Provision
 † = Construction Code 0042

SUMMARY OF QUANTITIES			0005
CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY
44000300	CURB REMOVAL	FOOT	36
44000600	SIDEWALK REMOVAL	SQ FT	6469
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	36500
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	910
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	910
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	910
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	910
60108204	PIPE UNDERDRAINS, TYPE 2, 4"	FOOT	150
SP 60202405	CATCH BASINS, TYPE A, 4'-DIAMETER	EACH	18
SP 60238800	INLETS, TYPE A	EACH	2
SP 60257900	MANHOLES TO BE RECONSTRUCTED	EACH	4
SP 60261300	INLETS TO BE ADJUSTED WITH NEW TYPE 11 FRAME AND GRATE	EACH	2
SP 60263900	INLETS TO BE RECONSTRUCTED WITH NEW TYPE 11 FRAME AND GRATE	EACH	5
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	30
60404500	FRAMES AND GRATES, TYPE 6	EACH	2
SP 60404800	FRAMES AND GRATES, TYPE 11	EACH	18
60600605	CONCRETE CURB, TYPE B	FOOT	80
67100100	MOBILIZATION	L. SUM	1

* = Specialty Item
 SP = Special Provision
 † = Construction Code 0042

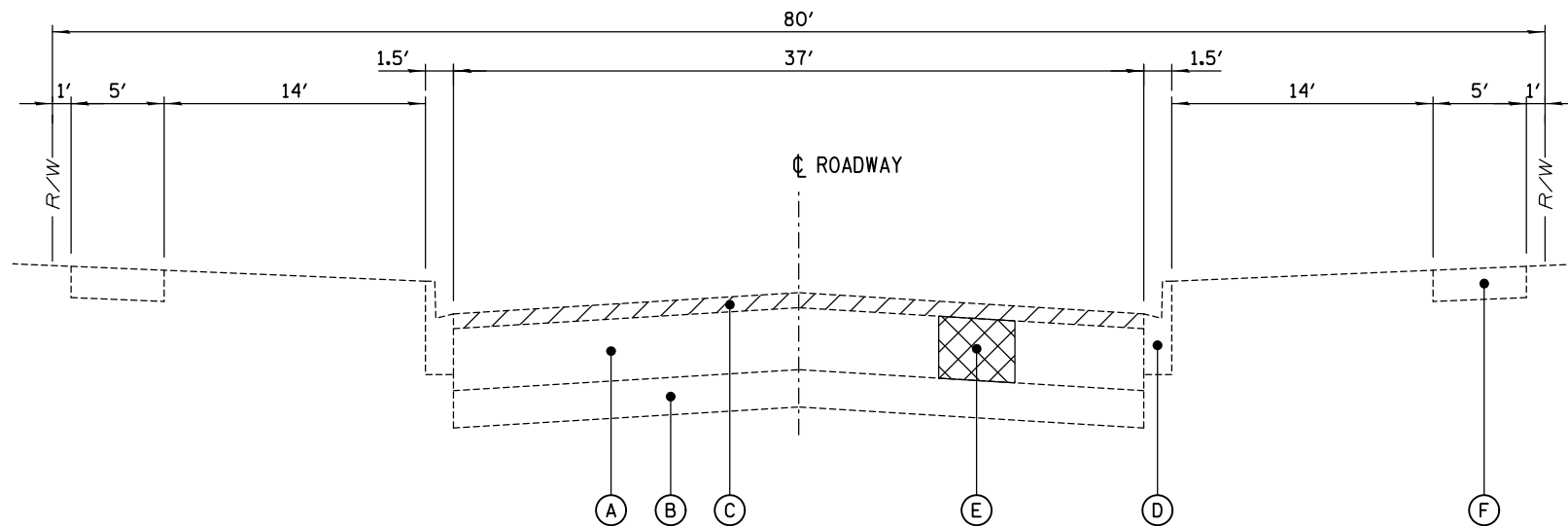
SUMMARY OF QUANTITIES

SUMMARY OF QUANTITIES			0005
CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY
70107025	CHANGEABLE MESSAGE SIGN	CAL DAY	126
72000100	SIGN PANEL - TYPE 1	SQ FT	176
72400310	REMOVE SIGN PANEL - TYPE 1	SQ FT	72
72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	160
70300100	SHORT TERM PAVEMENT MARKING	FOOT	670
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	225
78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4400
78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	24500
78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	3200
78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	430
78006100	PREFORMED THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS	SQ FT	964
78008210	POLYUREA PAVEMENT MARKING, TYPE 1 - LINE 4"	FOOT	32
78008230	POLYUREA PAVEMENT MARKING, TYPE 1 - LINE 6"	FOOT	32
78008270	POLYUREA PAVEMENT MARKING, TYPE 1 - LINE 24"	FOOT	21
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	60
SP X0301271	REMOVE EXISTING UNDERDRAINS	FOOT	150
SP X4240430	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH (SPECIAL)	SQ FT	5151
SP X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	500

* = Specialty Item
 SP = Special Provision
 † = Construction Code 0042

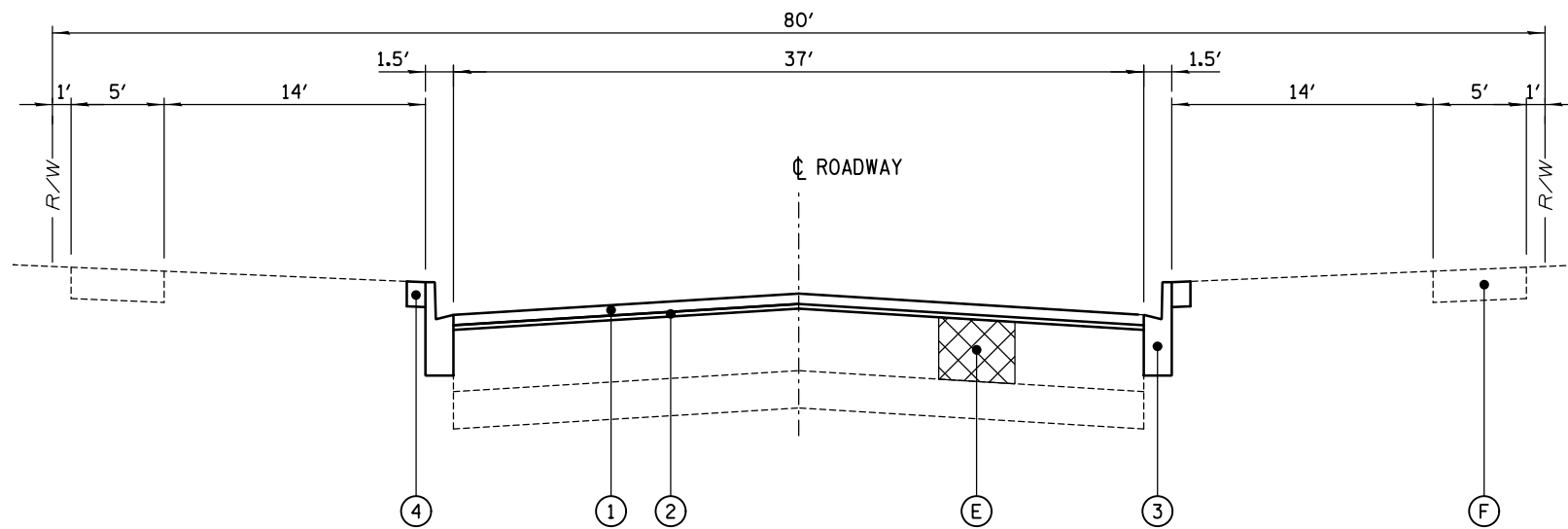
SUMMARY OF QUANTITIES			0005
CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY
SP X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1
SP XX003435	PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	950
SP XX006947	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	180
SP Z0004562	COMBINATION CONCRETE CURB & GUTTER REMOVAL & REPLACEMENT	FOOT	3010
SP Z0013798	CONSTRUCTION LAYOUT	L SUM	1
SP Z0018700	DRAINAGE STRUCTURE TO BE REMOVED	EACH	18
† SP Z0076600	TRAINEES	HOUR	500
† SP Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500

* = Specialty Item
 SP = Special Provision
 † = Construction Code 0042



EXISTING TYPICAL SECTION

SUMMIT DRIVE STA. 100+52 TO STA. 181+49



PROPOSED TYPICAL SECTION

SUMMIT DRIVE STA. 100+52 TO STA. 181+49

NOTES:

- LOCATIONS FOR THE FOLLOWING ITEMS DETERMINED IN THE FIELD BY THE ENGINEER:
 - COMB. CONCRETE CURB AND GUTTER REMOVE AND REPLACE
 - CLASS D PATCHING
 - ALL SIDEWALK REMOVE AND REPLACE
 - SODDING, TOPSOIL LIMITED TO 18" ADJACENT TO SIDEWALK AND CURB AND GUTTER
- PAVEMENT SHALL BE MILLED AND SWEPT PRIOR TO PATCHING

LEGEND

EXISTING:

- (A) EXISTING HOT-MIX ASPHALT PAVEMENT
- (B) EXISTING AGGREGATE SUBBASE
- (C) HOT-MIX ASPHALT SURFACE COURSE REMOVAL, 2 1/2"
- (D) COMB. CONCRETE CURB AND GUTTER REMOVAL, (LOCATIONS DETERMINED BY ENGINEER)
- (E) CLASS D PATCHES, SPECIAL (10 INCH)
- (F) EXISTING SIDEWALK

PROPOSED:

- (1) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5 MIX D N70, 2"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75 N50, 3/4"
- (3) COMB. CONCRETE CURB AND GUTTER, TYPE B-6.12, (INCLUDES 4" SUBBASE GRANULAR MATERIAL, TYPE B) (LOCATIONS DETERMINED BY ENGINEER)
- (4) SODDING, SALT TOLERANT, TOPSOIL FURNISH AND PLACE, 4"

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE:	
MAINLINE RESURFACING	
HMA SURFACE COURSE, IL-9.5 MIX "D", N70, 2"	4.0% @ 70 GYR.
POLYMERIZED HMA BINDER COURSE, IL-4.75 N50, 3/4"	3.5% @ 50 GYR.
HMA DRIVEWAY REMOVE AND REPLACE	
RESIDENTIAL ENTRANCE	
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5mm), 3"	4.0% @ 50 GYR.
CLASS D PATCHES	
HMA BINDER COURSE, IL-19, N70, 10"	4.0% @ 70 GYR.

NOTES:

- THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE QUANTITIES IS 112 LBS/SO SY/IN.
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY SPECIAL PROVISIONS.
- FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

SUMMIT DRIVE CORE DETAILS:									
CORE No.	1	2	3	4	5	6	7	8	9
TOTAL BITUMINOUS THICKNESS (INCH)	13 1/4	11 3/4	13 1/2	12	12	11 1/2	12	13	12
AGGREGATE BASE COURSE THICKNESS (INCH)	0	0	0	2	2	2	1	1	4

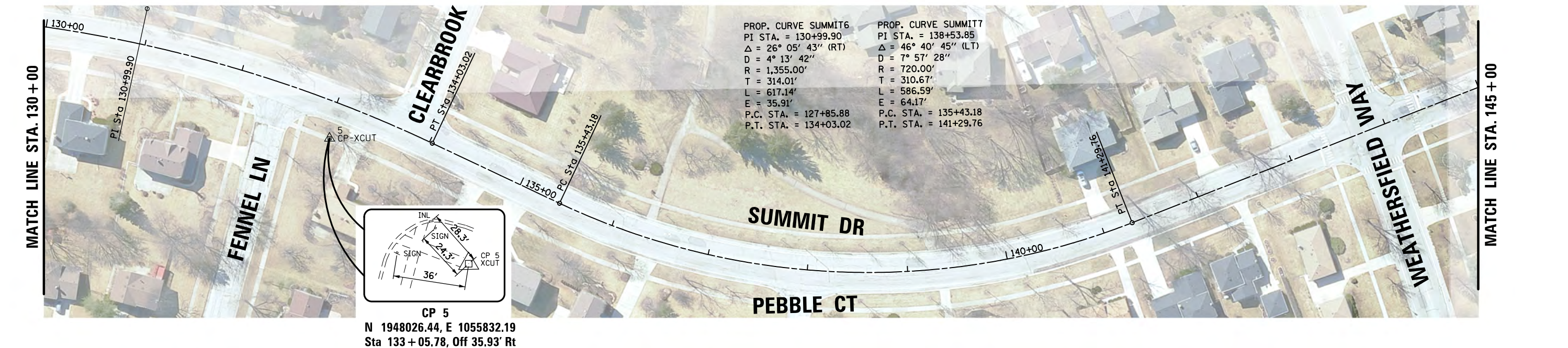
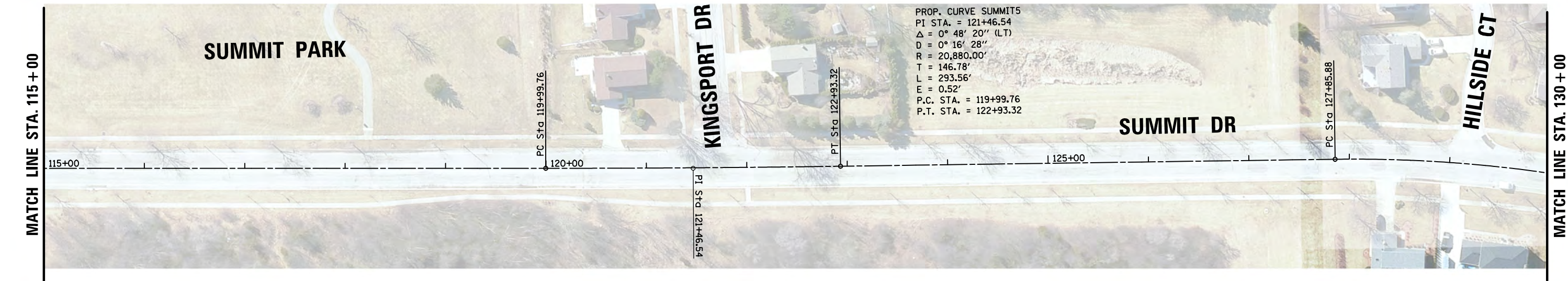
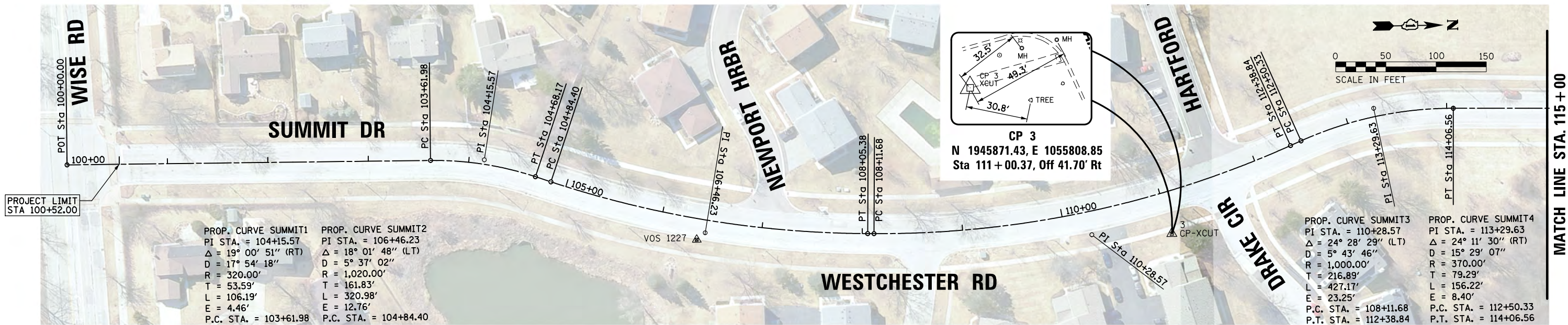
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\TYP-01.200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 10'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

SCALE: 10' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	6
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61H16	

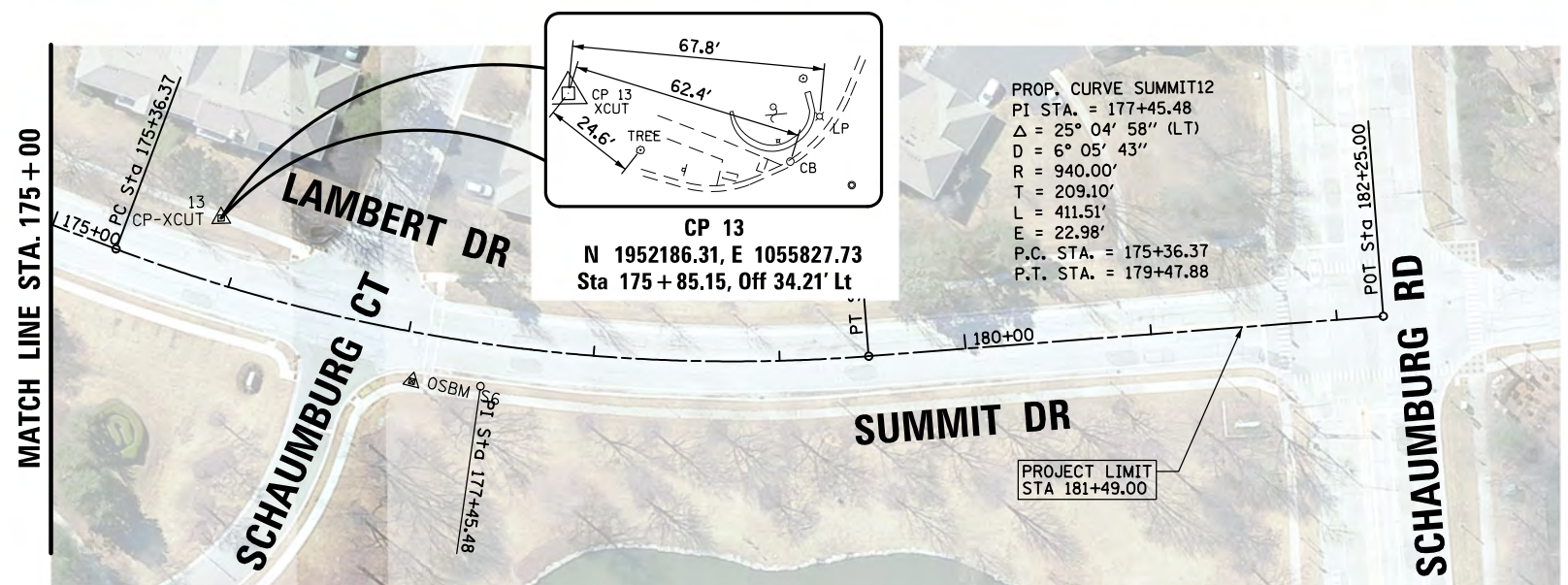
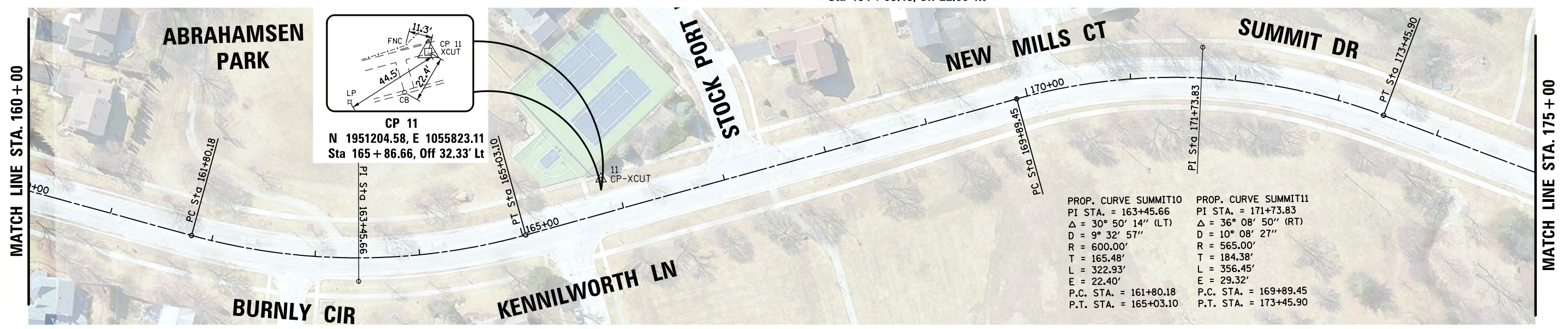
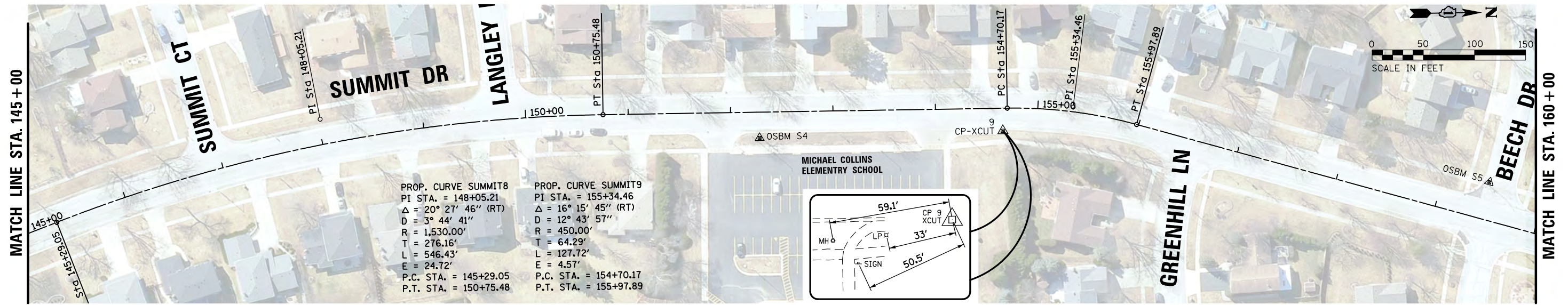


FILE NAME = N:\SCHAUMBURG\200227\Civil\1-01.200227.sht	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
Default	PLOT SCALE = 100'	DRAWN - PM	REVISED -
	PLOT DATE = 6/4/2021	CHECKED - GR	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ALIGNMENT AND TIES			
SCALE: 100'	SHEET	OF SHEETS	STA. TO STA.

MUN RFE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	7
CONTRACT NO. 61H16				
ILLINOIS FED. AID PROJECT				

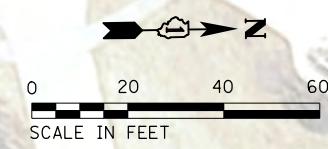
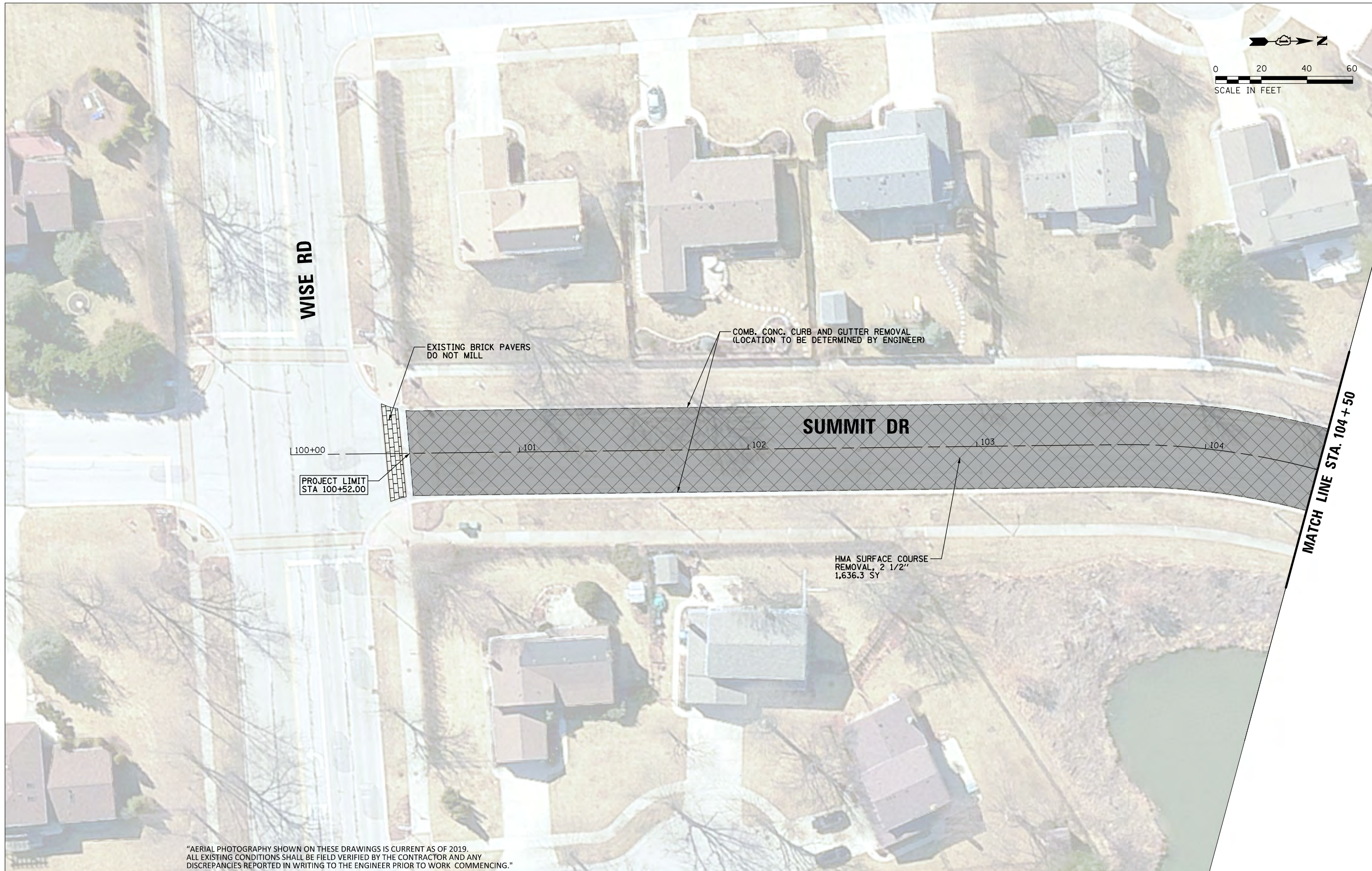


ELEVATION BENCHMARKS		
DATUM: VILLAGE OF SCHAUMBURG (NAVD'88)		
NO.	DESCRIPTION	ELEV.
VOS 1227	SCHAUMBURG DISK APPROXIMATELY 200' ± SOUTH OF WESTCHESTER ROAD ON THE EASTERLY CURB LINE OF SUMMIT DRIVE	761.01
OSBM S1	NORTHEAST BOLT OF FIRE HYDRANT 200' ± NORTH OF KINGSFORT ON EAST SIDE OF SUMMIT DRIVE	746.44
OSBM S2	ALUMINUM DISK ON TOP OF CURB AT THE NORTHWEST CORNER OF CLEARBOOK COURT & SUMMIT DRIVE	751.30
OSBM S3	NORTHWEST BOLT OF FIRE HYDRANT IN FRONT OF HOUSE #534 SUMMIT DRIVE	749.12
OSBM S4	NORTHWEST BOLT OF FOUNDATION TO LIGHT POLE OPPOSITE HOUSE #414 SUMMIT DRIVE	760.46
OSBM S5	EASTERLY BOLT OF STREET LIGHT FOUNDATION ON THE SOUTHWEST CORNER OF SUMMIT DRIVE & BEECH DRIVE	761.98
OSBM S6	NORTHWEST CORNER OF STEEL BASE OF STREET LIGHT ON THE NORTHEAST CORNER OF SUMMIT DRIVE & LAMBERT DRIVE	745.58

FILE NAME = N:\SCHAUMBURG\200227\Civil\1-02-200227.sht	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
Default	PLOT SCALE = 100'	DRAWN - PM	REVISED -
	PLOT DATE = 6/4/2021	CHECKED - GR	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ALIGNMENT AND TIES			
MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS
2134	20-00135-00-R5	COOK	53
			SHEET NO. 8
CONTRACT NO. 61H16			
SCALE: 100'	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT



"AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019. ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING."

FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\REM01_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48"	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN
SUMMIT DRIVE**

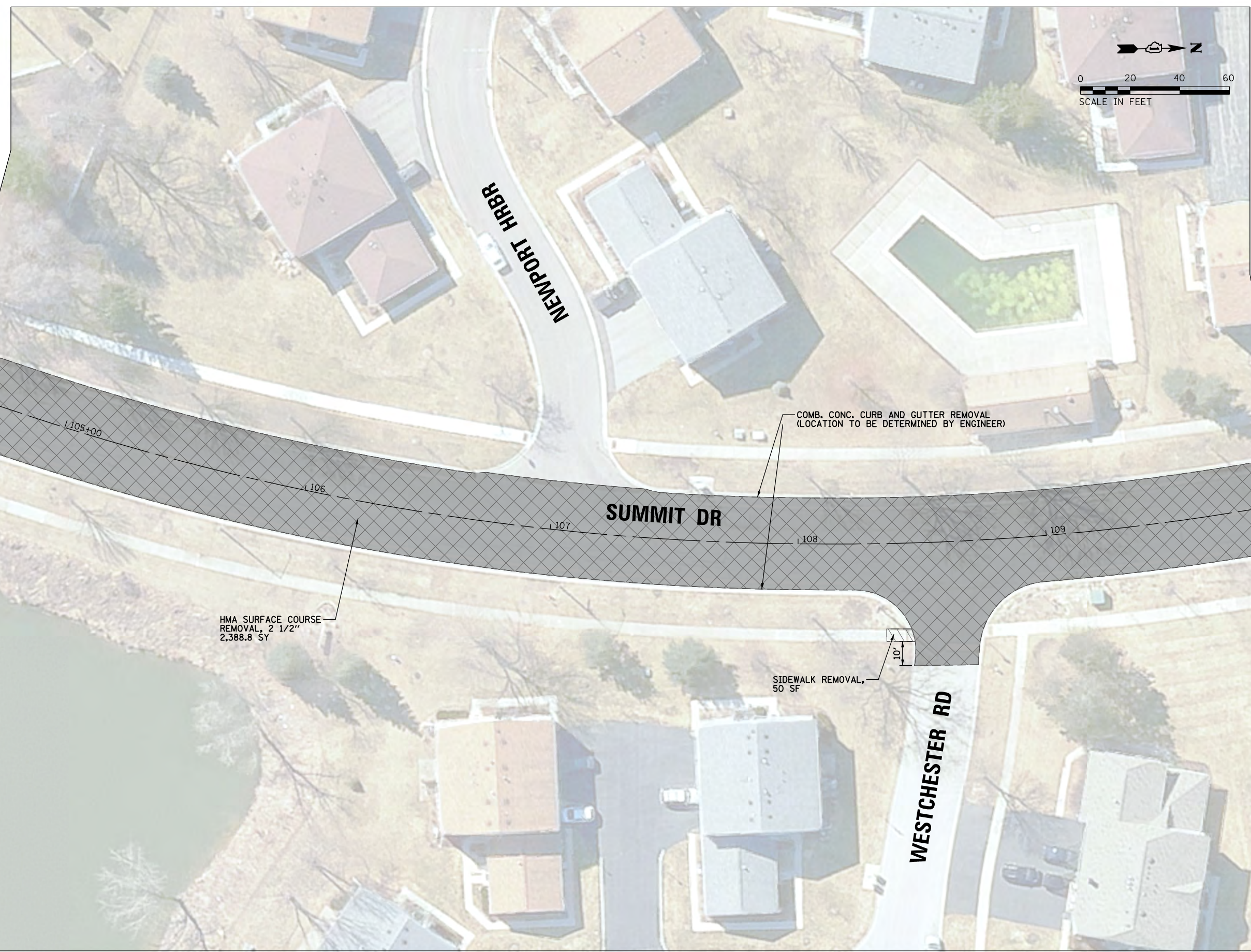
SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	9
CONTRACT NO. 61H16				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 104 + 50

MATCH LINE STA. 110 + 00



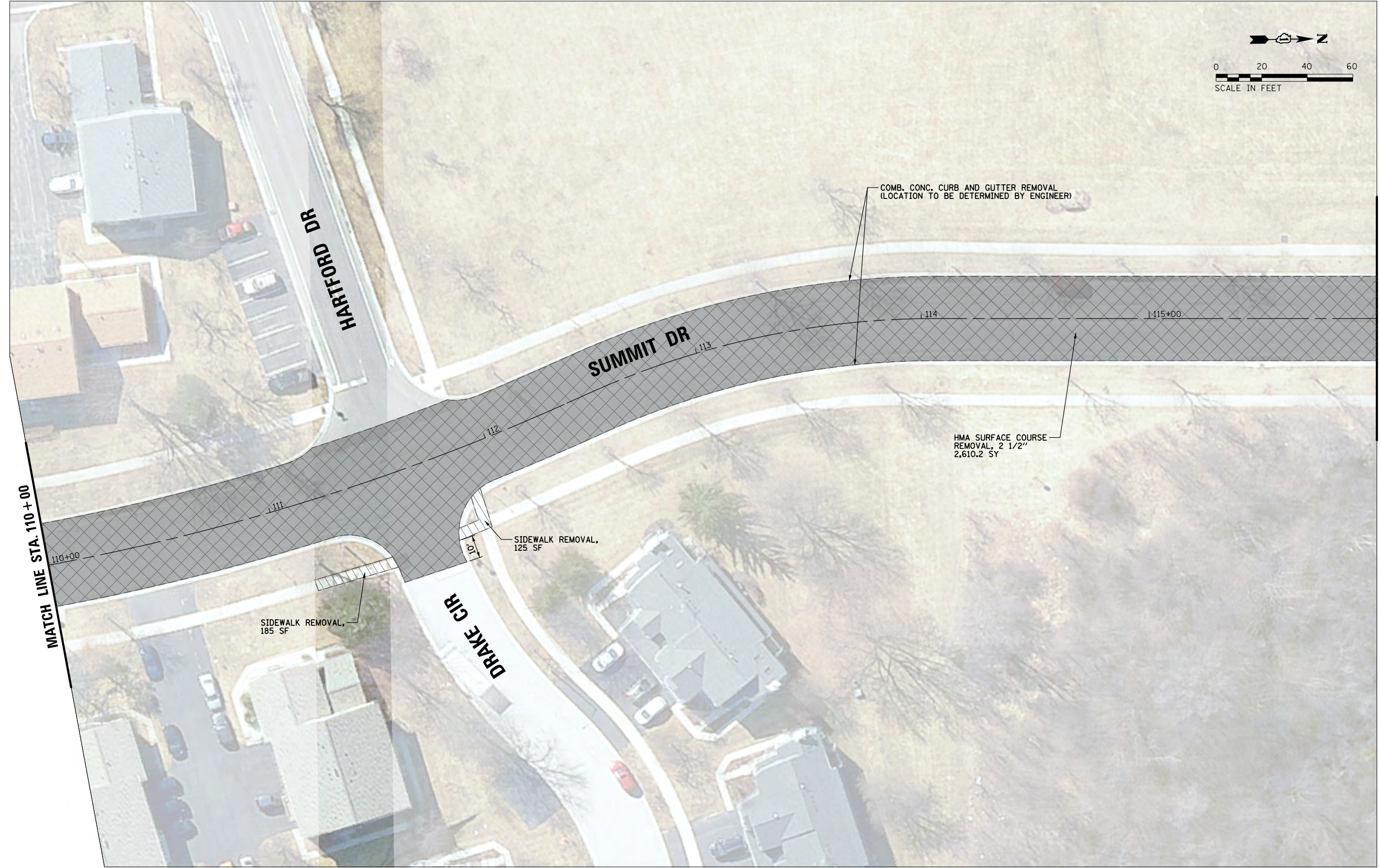
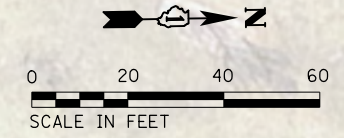
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN
SUMMIT DRIVE**

FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civi\REM02.200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 40'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	10
				CONTRACT NO. 61H16
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 110 + 00

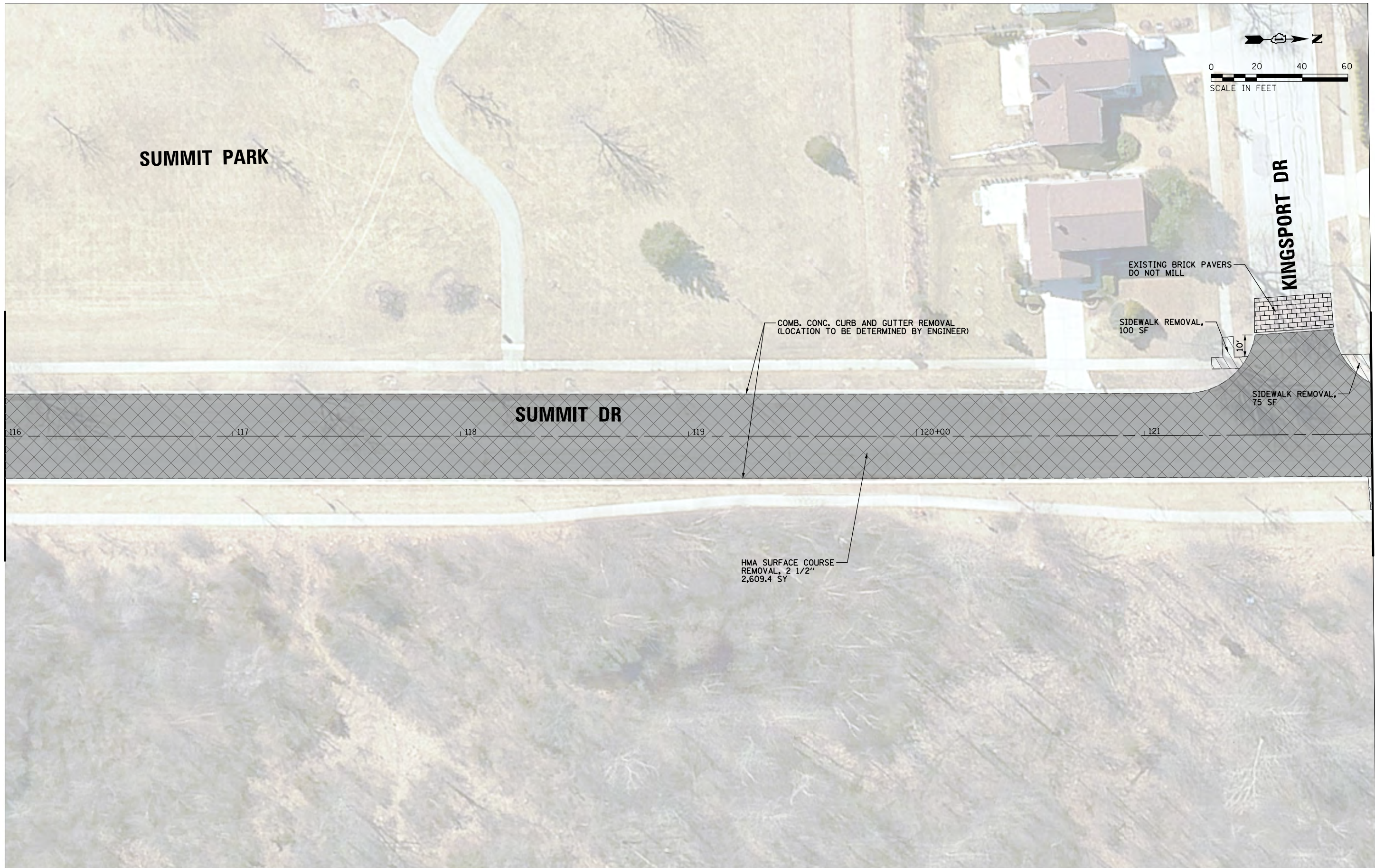
MATCH LINE STA. 116 + 00

FILE NAME = N:\SCHAUMBURG\200227\Civil\REM03_200227.sht	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
Default	PLOT SCALE = 48'	DRAWN - PM	REVISED -
	PLOT DATE = 6/4/2021	CHECKED - GR	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

REMOVAL PLAN SUMMIT DRIVE			
SCALE: 40'	SHEET	OF SHEETS	STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	11
				CONTRACT NO. 61H16
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 116 + 00

MATCH LINE STA. 122 + 00

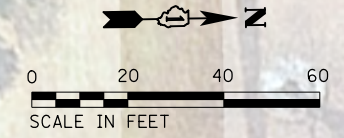
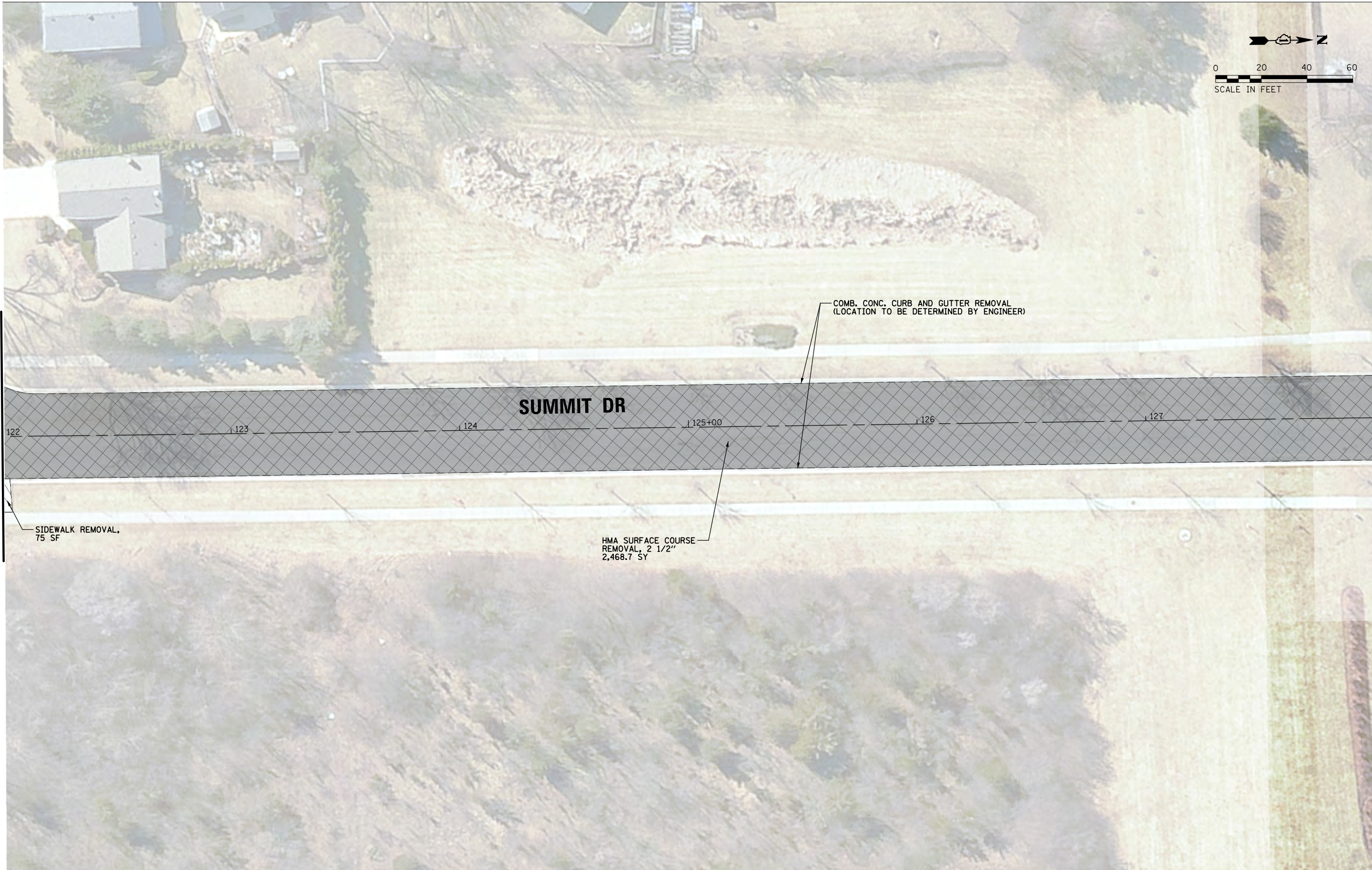
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\REM04_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 40'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN
SUMMIT DRIVE**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	12
CONTRACT NO. 61H16			ILLINOIS FED. AID PROJECT	



MATCH LINE STA. 122+00

MATCH LINE STA. 128+00

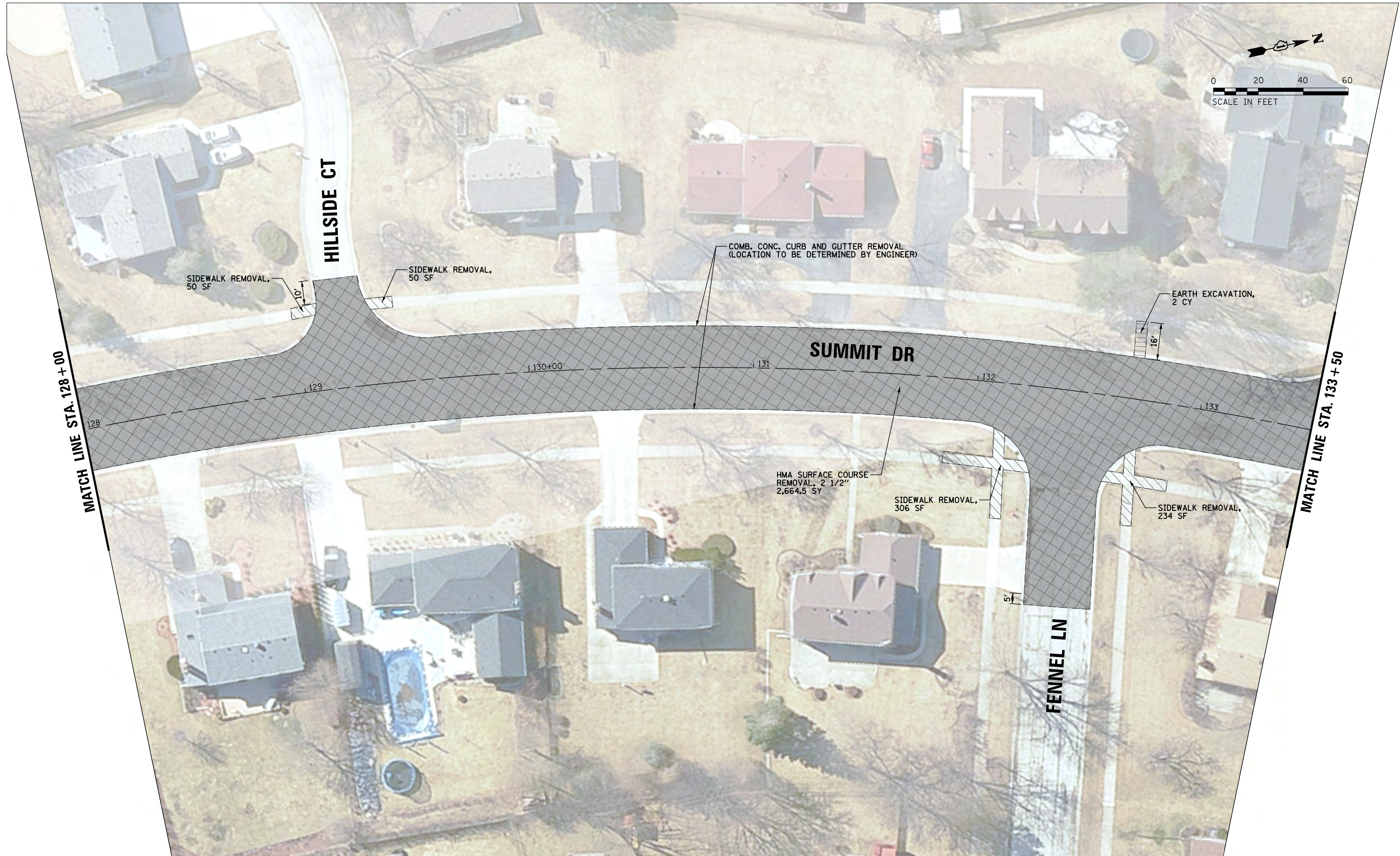
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\REM05_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 40'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN
SUMMIT DRIVE**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	13
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61H16	



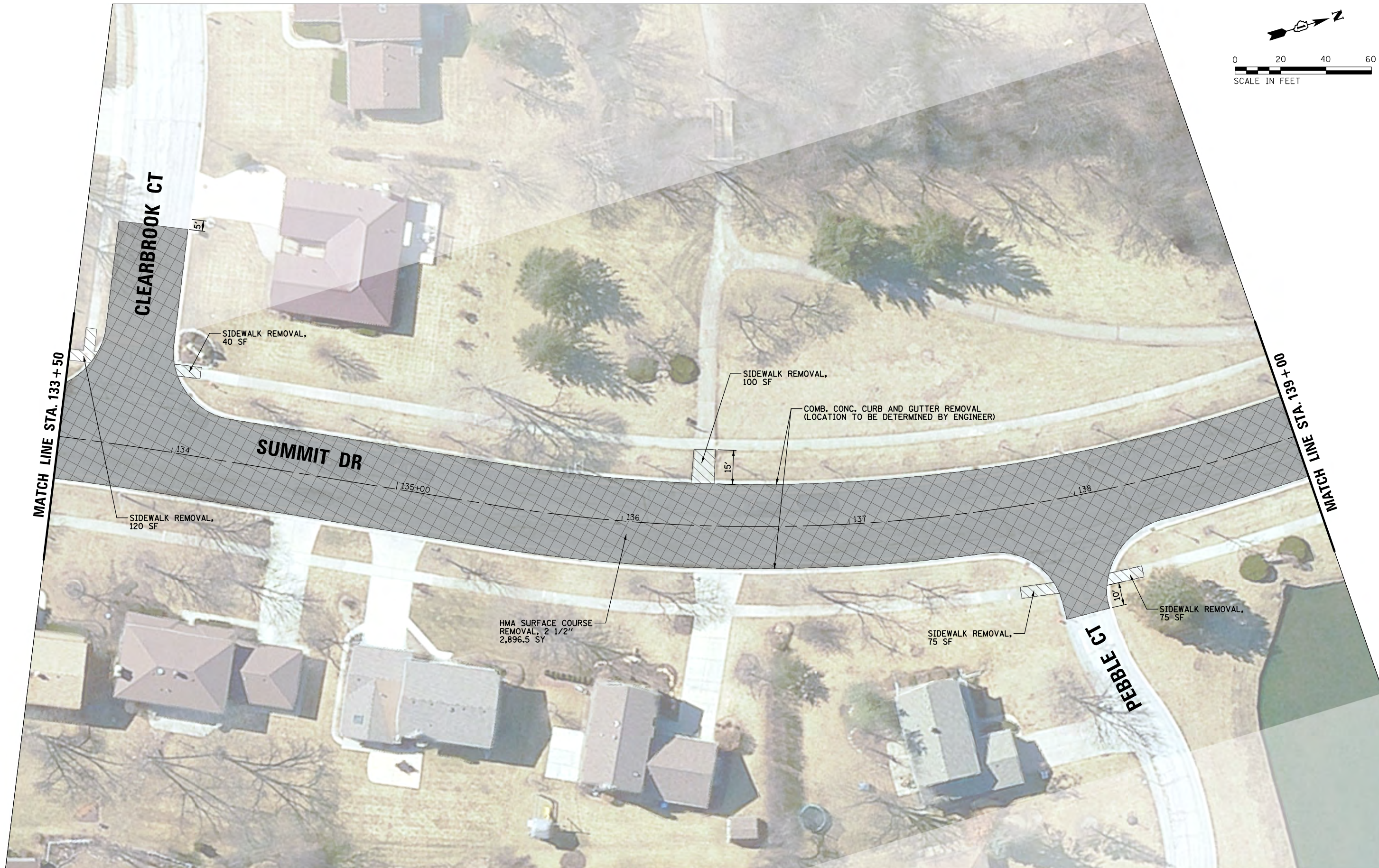
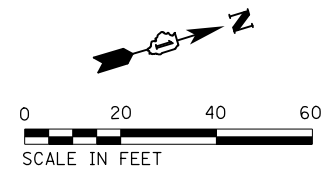
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\REM06.200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN
SUMMIT DRIVE**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	14
CONTRACT NO. 61H16				
ILLINOIS FED. AID PROJECT				

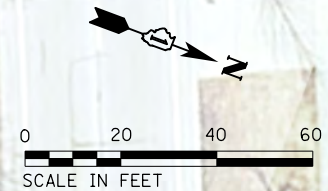


FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\REM07_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48"	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

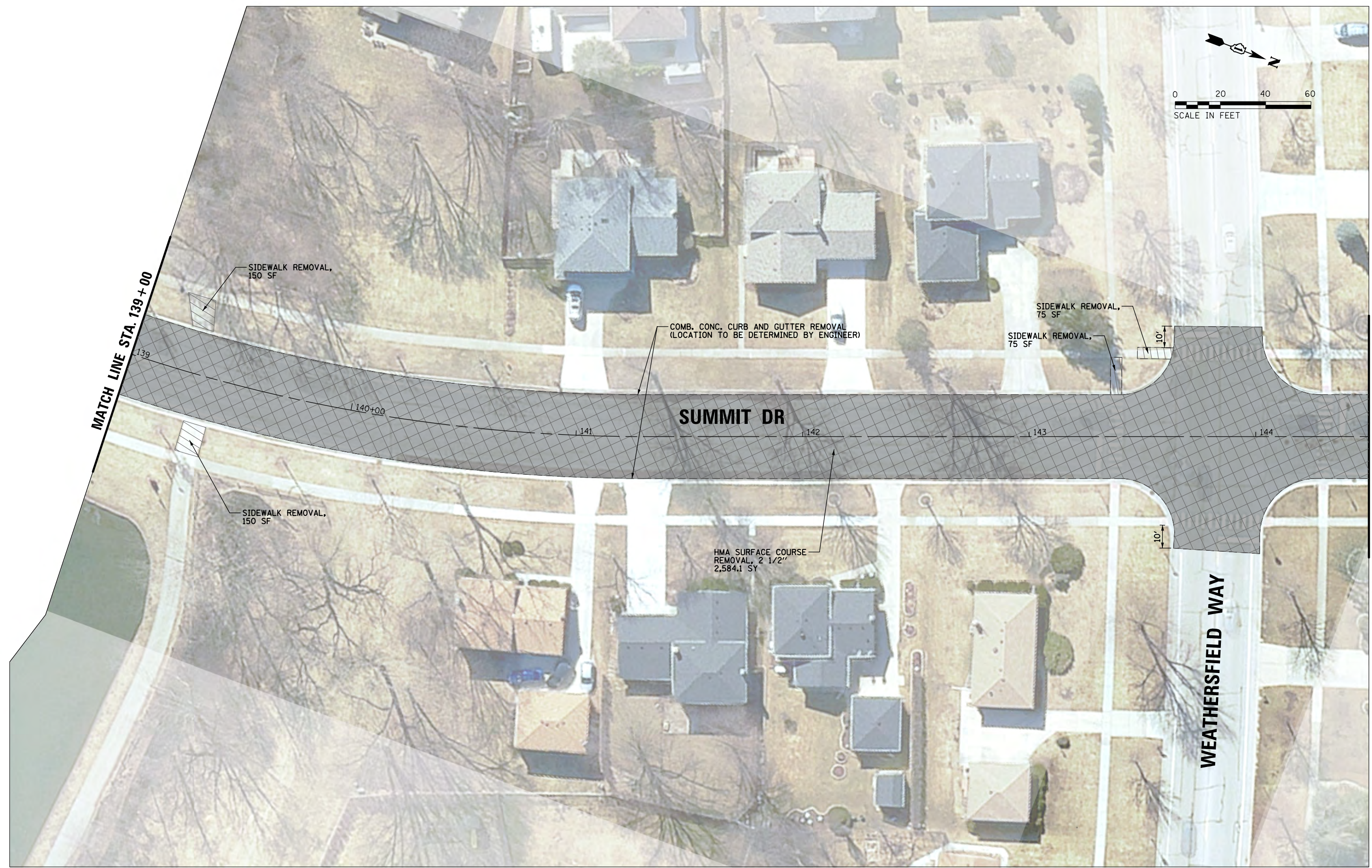
REMOVAL PLAN SUMMIT DRIVE			
SCALE: 40'	SHEET	OF SHEETS	STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	15
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61H16	



MATCH LINE STA. 139 + 00

MATCH LINE STA. 144 + 50



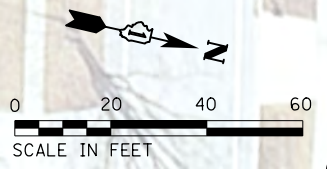
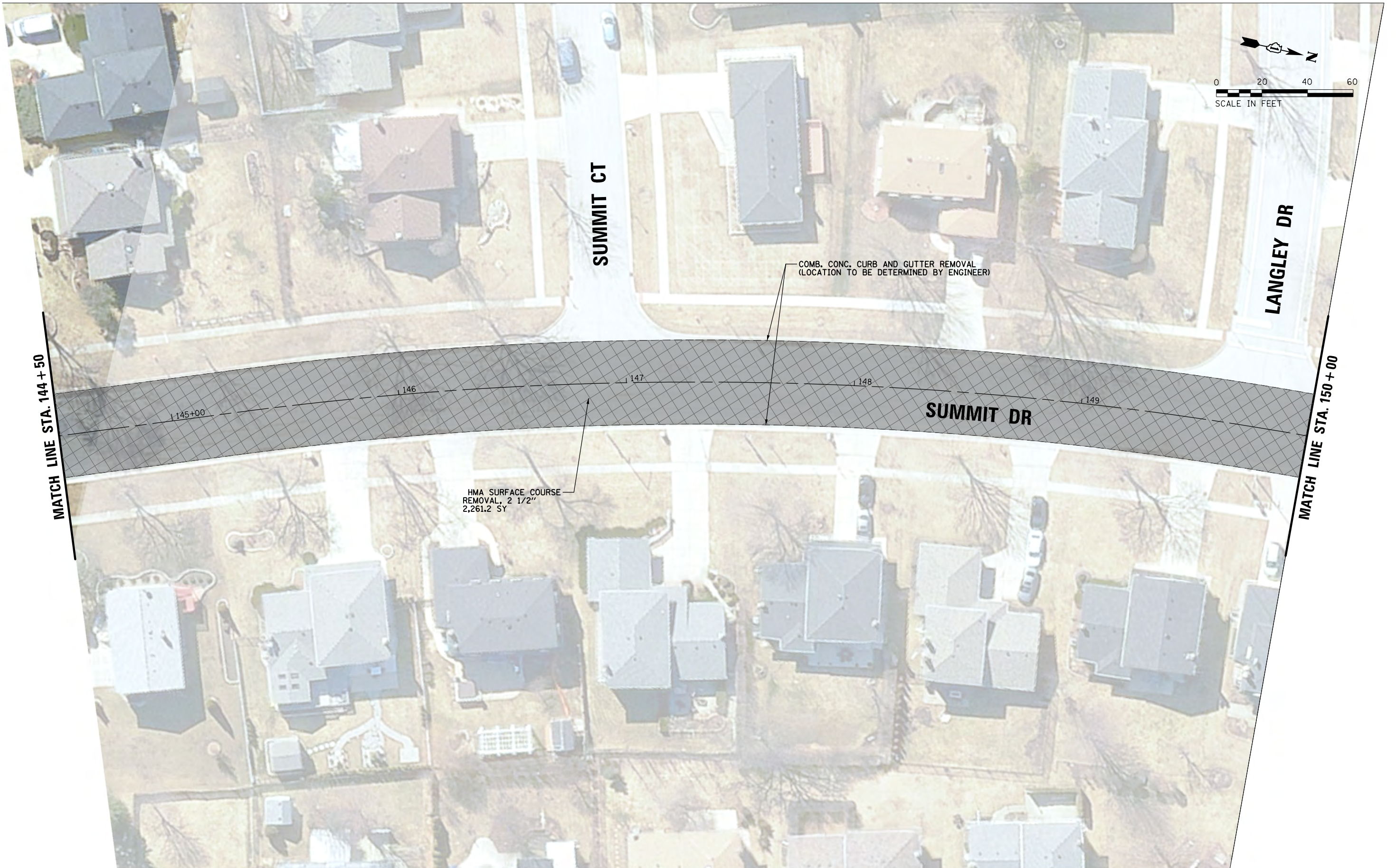
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\REM08_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48"	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN
SUMMIT DRIVE**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	16
CONTRACT NO. 61H16			ILLINOIS FED. AID PROJECT	



MATCH LINE STA. 144 + 50

MATCH LINE STA. 150 + 00

FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\REM09_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48"	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

REMOVAL PLAN SUMMIT DRIVE			
SCALE: 40'	SHEET	OF SHEETS	STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	17
CONTRACT NO. 61H16				
ILLINOIS FED. AID PROJECT				



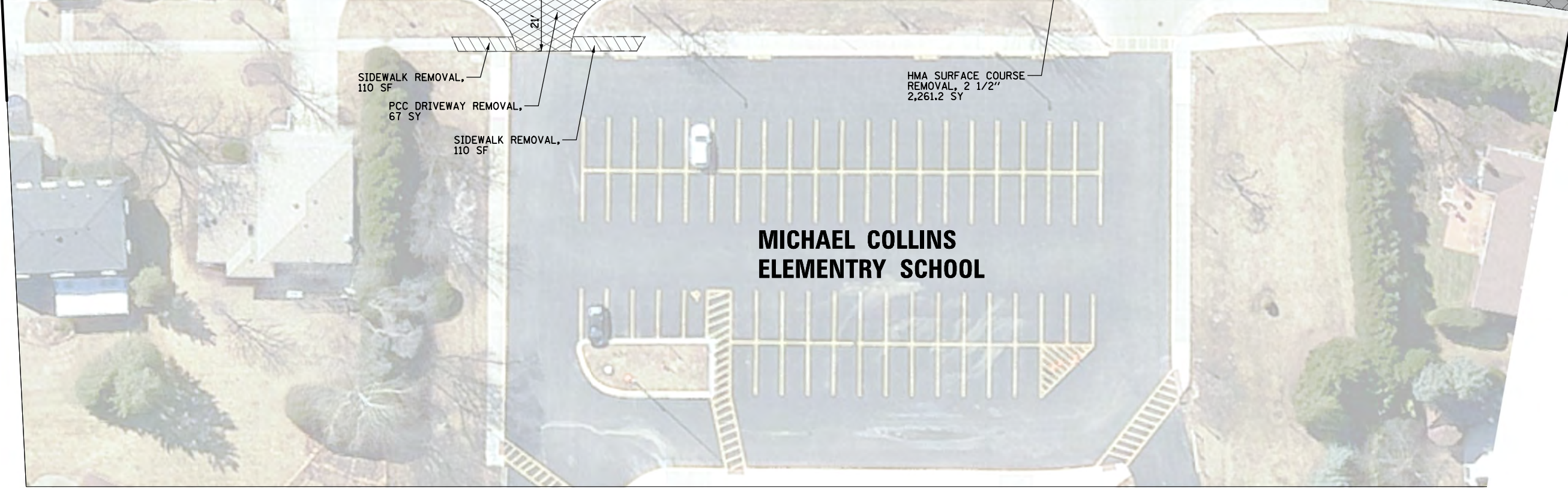
MATCH LINE STA. 150+00

MATCH LINE STA. 155+50

COMB. CONC. CURB AND GUTTER REMOVAL
(LOCATION TO BE DETERMINED BY ENGINEER)



SUMMIT DR



SIDEWALK REMOVAL,
110 SF

PCC DRIVEWAY REMOVAL,
67 SY

SIDEWALK REMOVAL,
110 SF

HMA SURFACE COURSE
REMOVAL, 2 1/2"
2,261.2 SY

**MICHAEL COLLINS
ELEMENTARY SCHOOL**

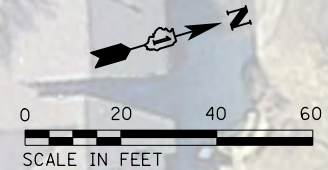
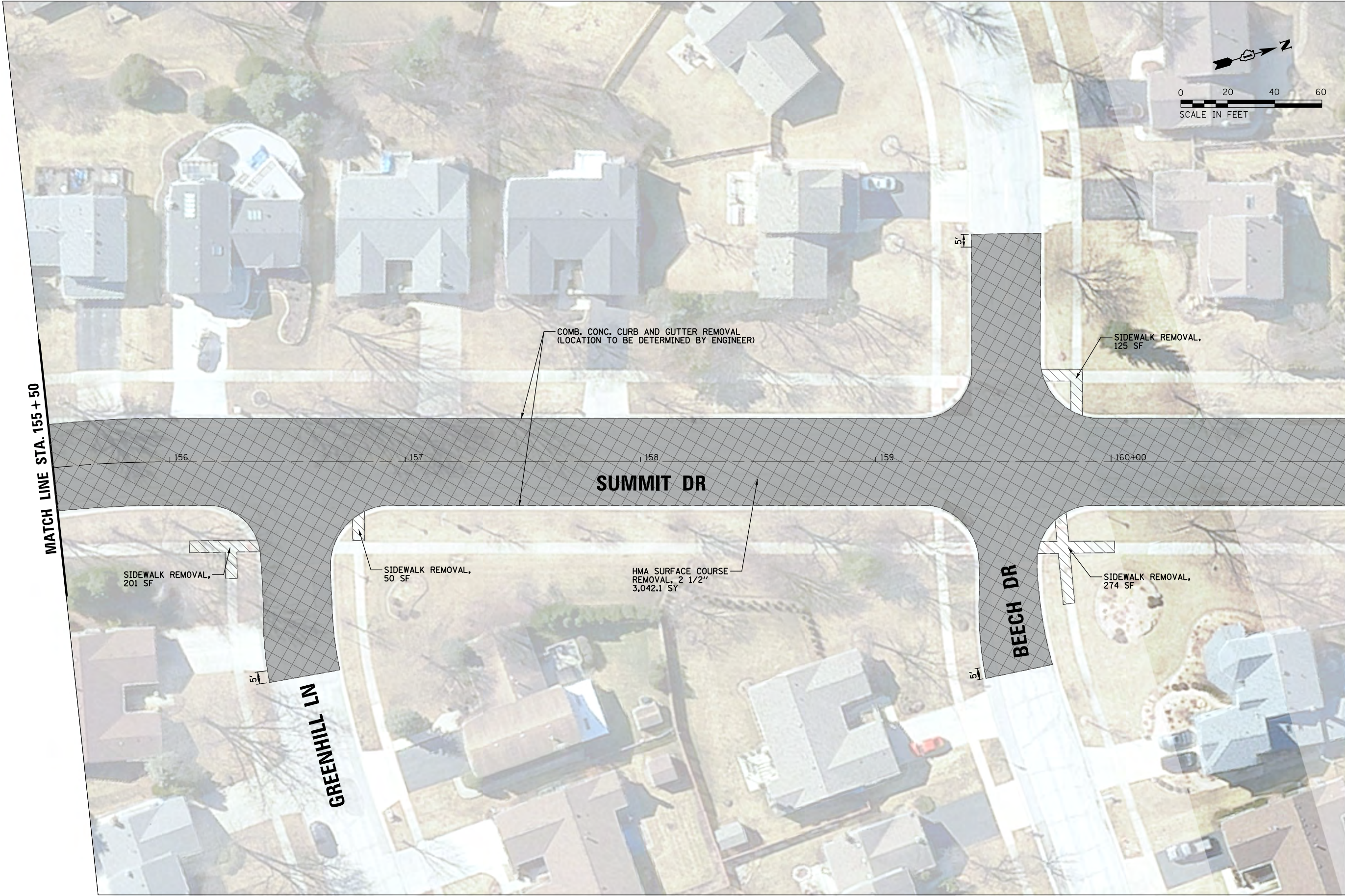
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\REM10_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 40'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN
SUMMIT DRIVE**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	18
			CONTRACT NO. 61H16	
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 155 + 50

MATCH LINE STA. 161 + 00

SUMMIT DR

BEECH DR

GREENHILL LN

FILE NAME = N:\SCHAUMBURG\200227\Civil\REM11.200227.sht	USER NAME = esidorczuk	DESIGNED - GR	REVISED -
		DRAWN - PM	REVISED -
	PLOT SCALE = 48"	CHECKED - GR	REVISED -
Default	PLOT DATE = 6/4/2021	DATE -	REVISED -

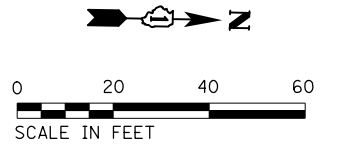
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN
SUMMIT DRIVE**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

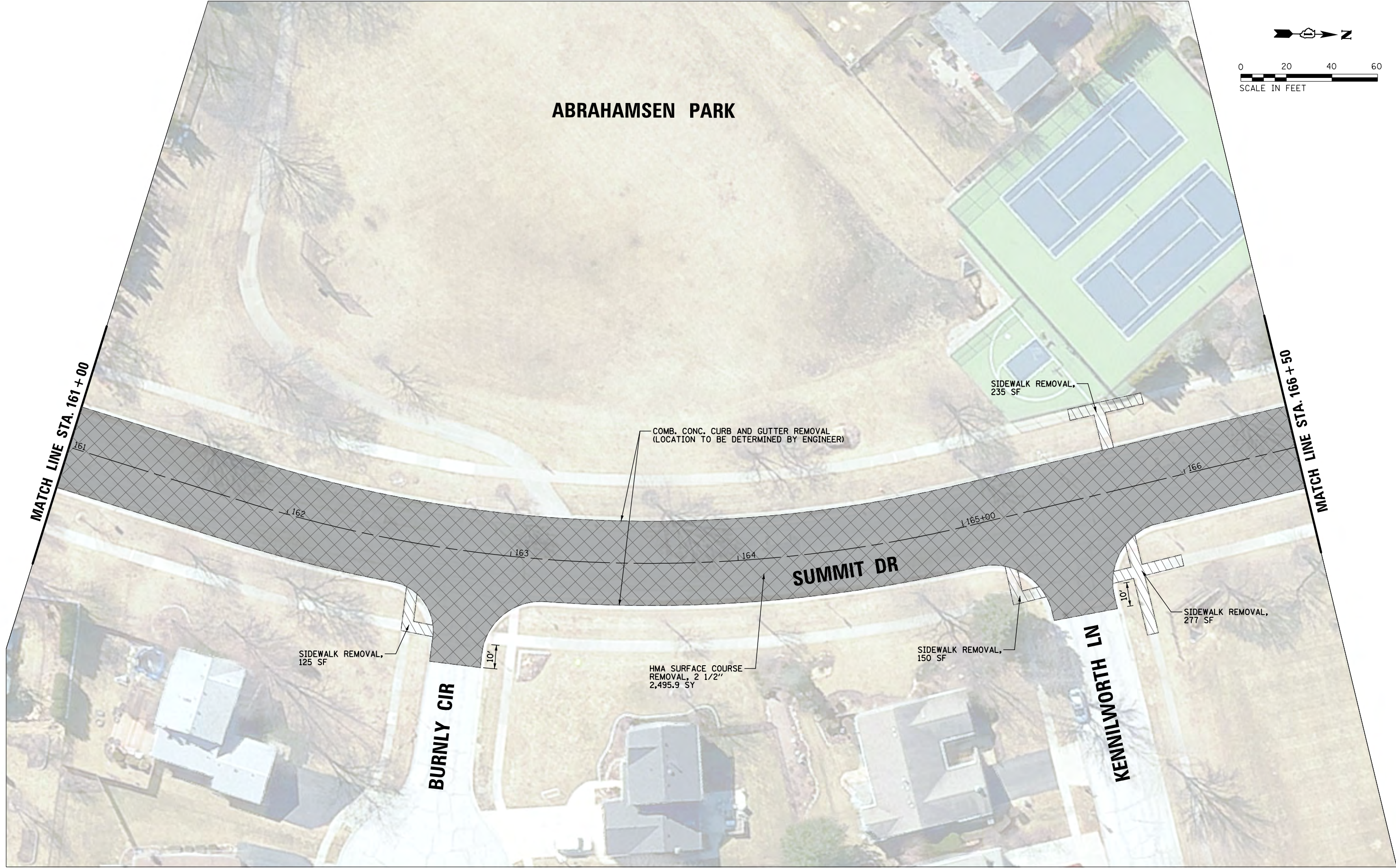
MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	19
			CONTRACT NO. 61H16	
ILLINOIS FED. AID PROJECT				

ABRAHAMSEN PARK



MATCH LINE STA. 161+00

MATCH LINE STA. 166+50



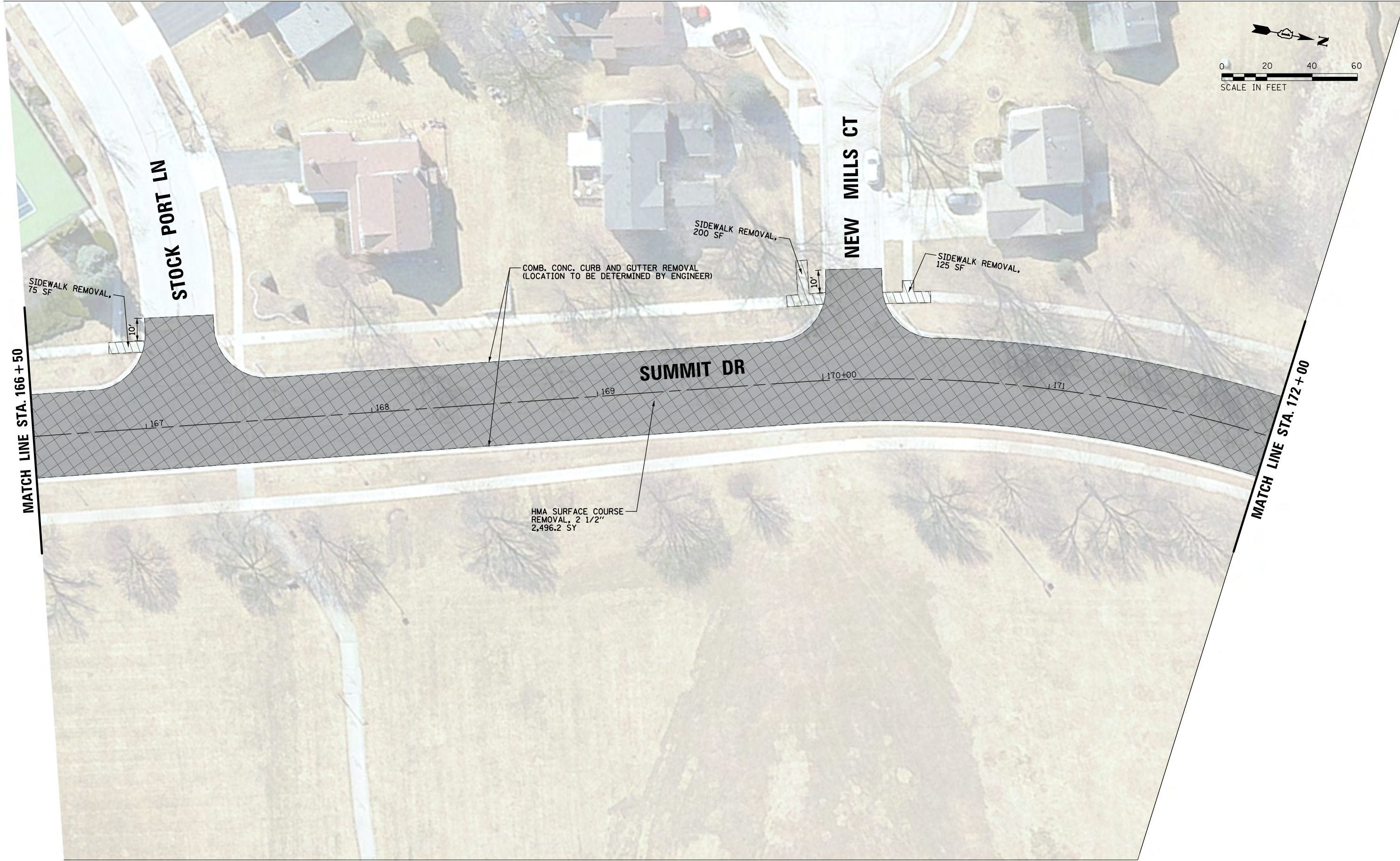
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\REM12.200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVAL PLAN
SUMMIT DRIVE

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	20
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61H16	



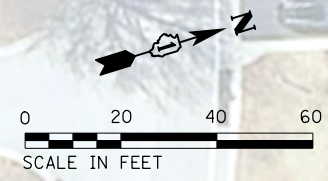
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\REM13_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 40'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN
SUMMIT DRIVE**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	21
CONTRACT NO. 61H16				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 172+00

MATCH LINE STA. 177+50

COMB. CONC. CURB AND GUTTER REMOVAL
(LOCATION TO BE DETERMINED BY ENGINEER)

SIDEWALK REMOVAL,
222 SF

24'

SUMMIT DR

SCHAUMBURG CT

HMA SURFACE COURSE
REMOVAL, 2 1/2"
2,299.9 SY



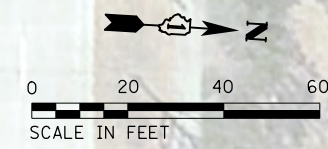
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\REM14_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48"	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN
SUMMIT DRIVE**

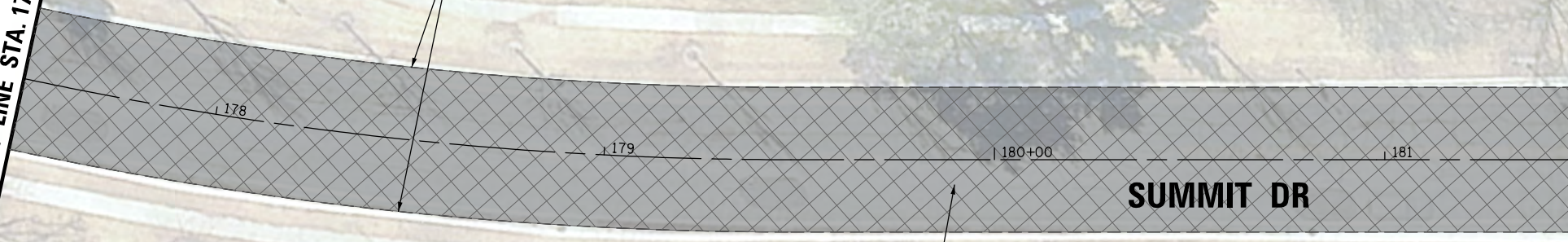
SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	22
				CONTRACT NO. 61H16
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 177+50

COMB. CONC. CURB AND GUTTER REMOVAL
(LOCATION TO BE DETERMINED BY ENGINEER)



SUMMIT DR

PROJECT LIMIT
STA 181+49+00

EXISTING CROSSWALK

HMA SURFACE COURSE
REMOVAL, 2 1/2"
1,638.9 SY

SCHAUMBURG RD

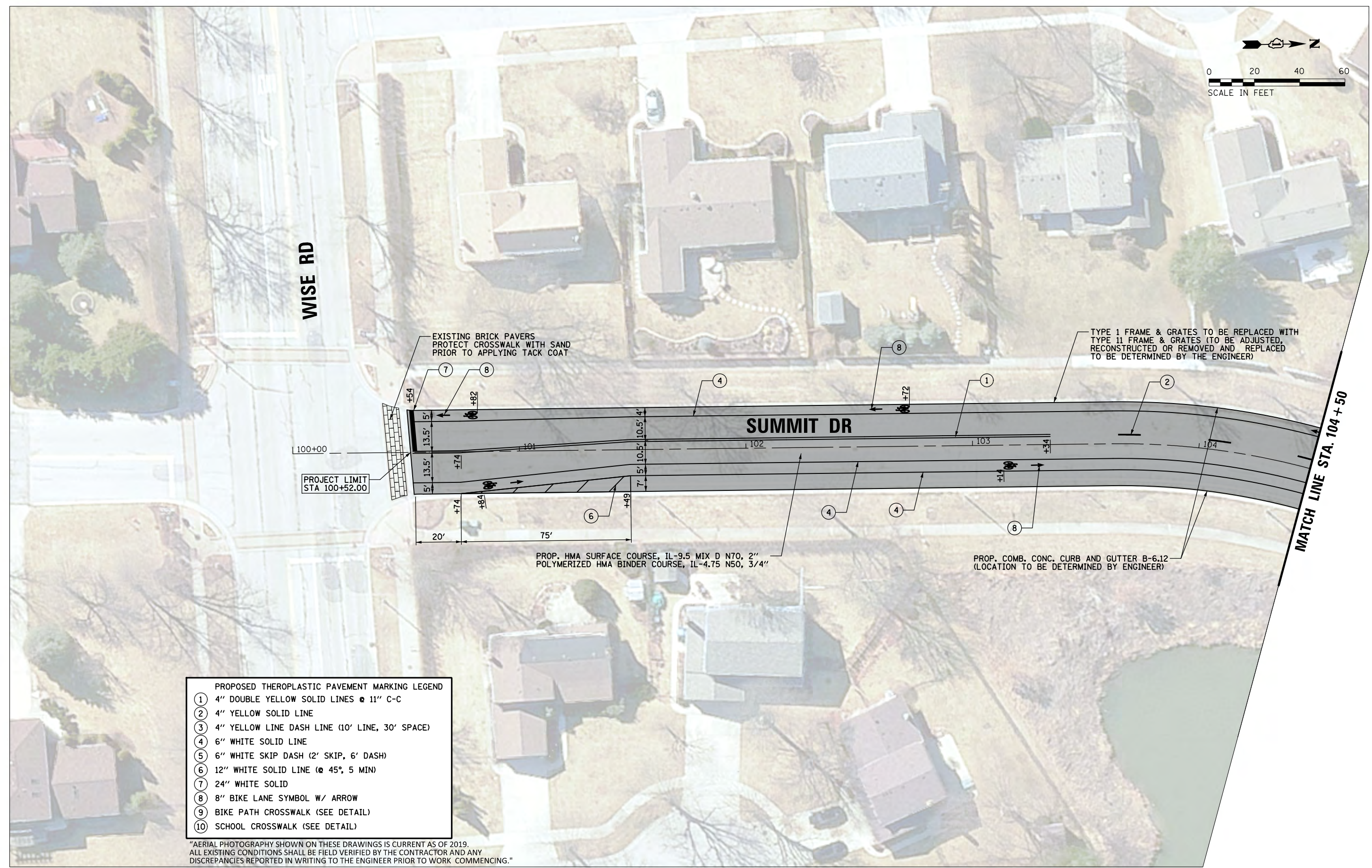
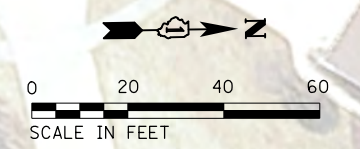
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\REM15_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 40'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVAL PLAN
SUMMIT DRIVE

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	23
				CONTRACT NO. 61H16
ILLINOIS FED. AID PROJECT				



WISE RD

SUMMIT DR

MATCH LINE STA. 104+50

PROJECT LIMIT
STA 100+52.00

EXISTING BRICK PAVERS
PROTECT CROSSWALK WITH SAND
PRIOR TO APPLYING TACK COAT

TYPE 1 FRAME & GRATES TO BE REPLACED WITH
TYPE 11 FRAME & GRATES (TO BE ADJUSTED,
RECONSTRUCTED OR REMOVED AND REPLACED
TO BE DETERMINED BY THE ENGINEER)

PROP. HMA SURFACE COURSE, IL-9.5 MIX D N70, 2"
POLYMERIZED HMA BINDER COURSE, IL-4.75 N50, 3/4"

PROP. COMB. CONC. CURB AND GUTTER B-6.12
(LOCATION TO BE DETERMINED BY ENGINEER)

- PROPOSED THEROPLASTIC PAVEMENT MARKING LEGEND**
- ① 4" DOUBLE YELLOW SOLID LINES @ 11" C-C
 - ② 4" YELLOW SOLID LINE
 - ③ 4" YELLOW LINE DASH LINE (10' LINE, 30' SPACE)
 - ④ 6" WHITE SOLID LINE
 - ⑤ 6" WHITE SKIP DASH (2' SKIP, 6' DASH)
 - ⑥ 12" WHITE SOLID LINE (@ 45°, 5 MIN)
 - ⑦ 24" WHITE SOLID
 - ⑧ 8" BIKE LANE SYMBOL W/ ARROW
 - ⑨ BIKE PATH CROSSWALK (SEE DETAIL)
 - ⑩ SCHOOL CROSSWALK (SEE DETAIL)

"AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019.
ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY
DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING."

FILE NAME = N:\SCHAUMBURG\200227\Civil\PLN01_200227.sht	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
PLOT SCALE = 48"	CHECKED - GR	DRAWN - PM	REVISED -
Default	DATE -	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN
SUMMIT DRIVE**

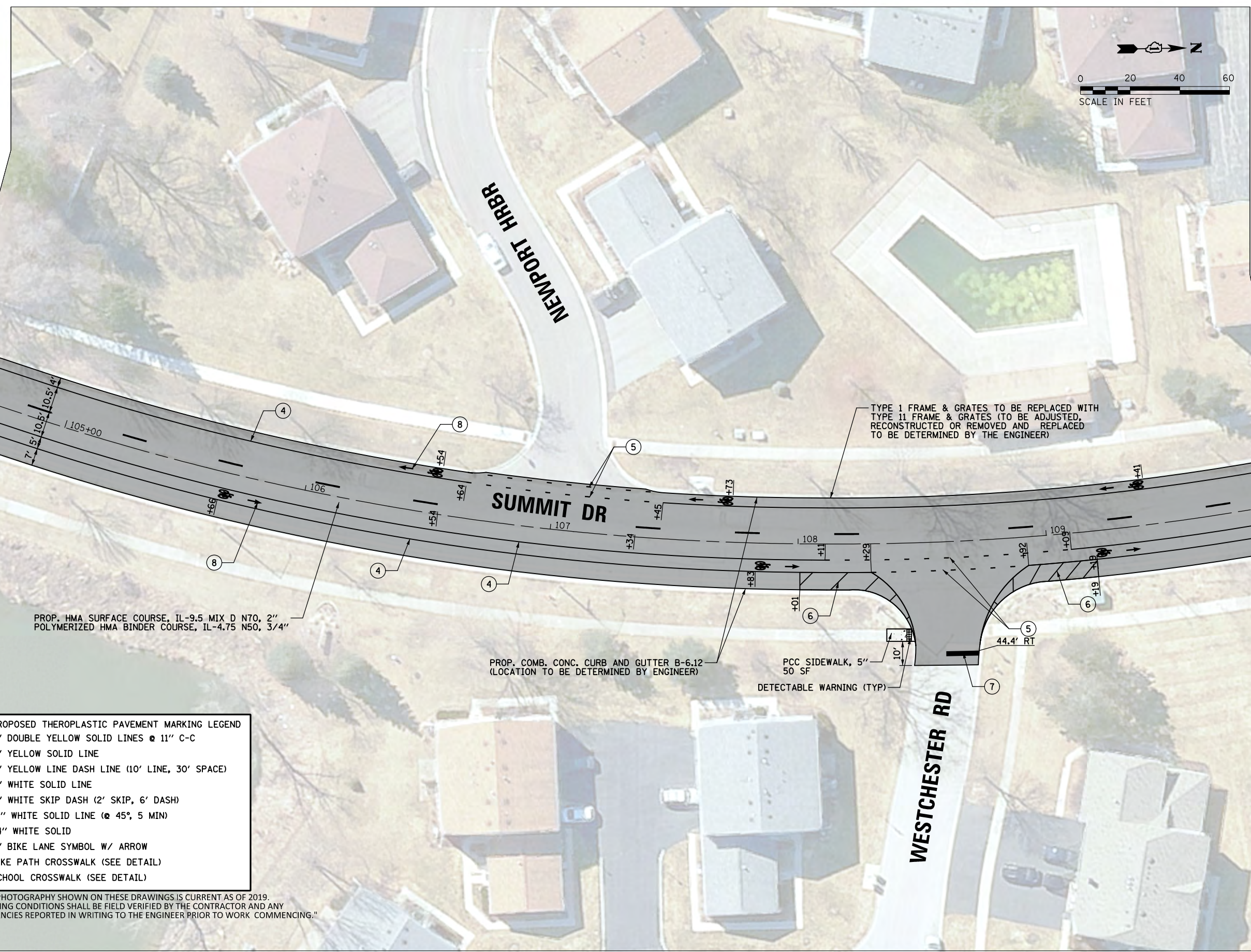
SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	24
CONTRACT NO. 61H16				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 104 + 50

MATCH LINE STA. 110 + 00



PROP. HMA SURFACE COURSE, IL-9.5 MIX D N70, 2"
POLYMERIZED HMA BINDER COURSE, IL-4.75 N50, 3/4"

- PROPOSED THEROPLASTIC PAVEMENT MARKING LEGEND
- ① 4" DOUBLE YELLOW SOLID LINES @ 11" C-C
 - ② 4" YELLOW SOLID LINE
 - ③ 4" YELLOW LINE DASH LINE (10' LINE, 30' SPACE)
 - ④ 6" WHITE SOLID LINE
 - ⑤ 6" WHITE SKIP DASH (2' SKIP, 6' DASH)
 - ⑥ 12" WHITE SOLID LINE @ 45°, 5 MIN
 - ⑦ 24" WHITE SOLID
 - ⑧ 8" BIKE LANE SYMBOL W/ ARROW
 - ⑨ BIKE PATH CROSSWALK (SEE DETAIL)
 - ⑩ SCHOOL CROSSWALK (SEE DETAIL)

"AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019.
ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY
DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING."

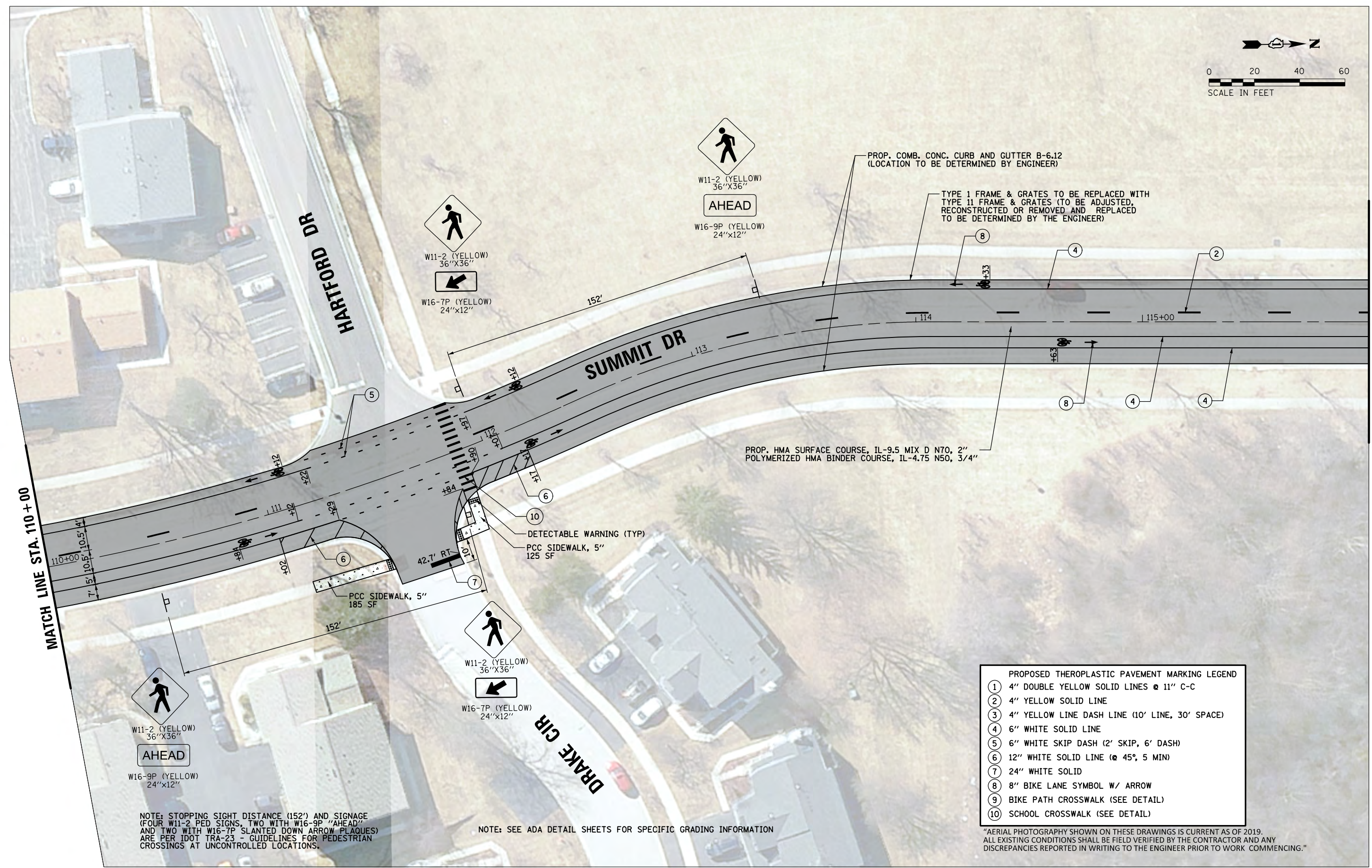
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\PLN\02.200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48"	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED PLAN
SUMMIT DRIVE

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	25
CONTRACT NO. 61H16			ILLINOIS FED. AID PROJECT	



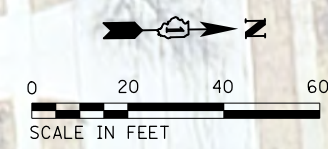
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\PLN03.200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 40'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN
SUMMIT DRIVE**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	26
			CONTRACT NO. 61H16	
ILLINOIS FED. AID PROJECT				



SUMMIT PARK

KINGSPORT DR

SUMMIT DR

MATCH LINE STA. 116 + 00

MATCH LINE STA. 122 + 00

EXISTING BRICK PAVERS
PROTECT CROSSWALK WITH SAND
PRIOR TO APPLYING TACK COAT

PCC SIDEWALK, 5"
100 SF

DETECTABLE WARNING (TYP)

38.4' LT

PCC SIDEWALK, 5"
75 SF

TYPE 1 FRAME & GRATES TO BE REPLACED WITH
TYPE 11 FRAME & GRATES (TO BE ADJUSTED,
RECONSTRUCTED OR REMOVED AND REPLACED
TO BE DETERMINED BY THE ENGINEER)

PROP. HMA SURFACE COURSE, IL-9.5 MIX D N70, 2"
POLYMERIZED HMA BINDER COURSE, IL-4.75 N50, 3/4"

PROP. COMB. CONC. CURB AND GUTTER B-6.12
(LOCATION TO BE DETERMINED BY ENGINEER)

PROPOSED THEROPLASTIC PAVEMENT MARKING LEGEND

- ① 4" DOUBLE YELLOW SOLID LINES @ 11" C-C
- ② 4" YELLOW SOLID LINE
- ③ 4" YELLOW LINE DASH LINE (10' LINE, 30' SPACE)
- ④ 6" WHITE SOLID LINE
- ⑤ 6" WHITE SKIP DASH (2' SKIP, 6' DASH)
- ⑥ 12" WHITE SOLID LINE (@ 45°, 5 MIN)
- ⑦ 24" WHITE SOLID
- ⑧ 8" BIKE LANE SYMBOL W/ ARROW
- ⑨ BIKE PATH CROSSWALK (SEE DETAIL)
- ⑩ SCHOOL CROSSWALK (SEE DETAIL)

"AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019.
ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY
DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING."

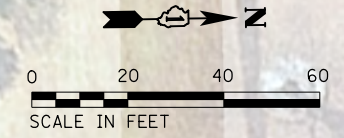
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\PLN04_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48"	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN
SUMMIT DRIVE**

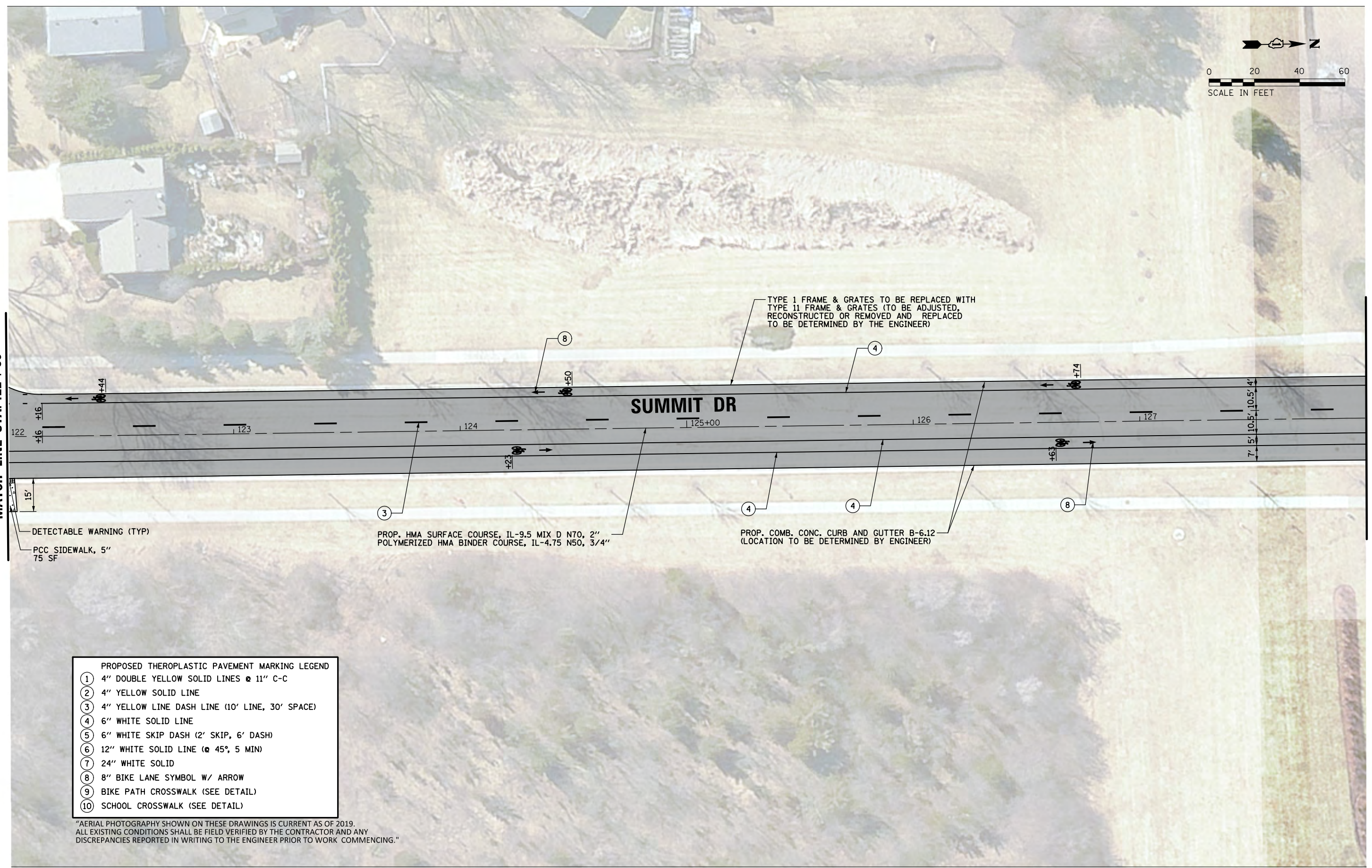
SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	27
CONTRACT NO. 61H16				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 122 + 00

MATCH LINE STA. 128 + 00



PROPOSED THEROPLASTIC PAVEMENT MARKING LEGEND

①	4" DOUBLE YELLOW SOLID LINES @ 11" C-C
②	4" YELLOW SOLID LINE
③	4" YELLOW LINE DASH LINE (10' LINE, 30' SPACE)
④	6" WHITE SOLID LINE
⑤	6" WHITE SKIP DASH (2' SKIP, 6' DASH)
⑥	12" WHITE SOLID LINE (@ 45°, 5 MIN)
⑦	24" WHITE SOLID
⑧	8" BIKE LANE SYMBOL W/ ARROW
⑨	BIKE PATH CROSSWALK (SEE DETAIL)
⑩	SCHOOL CROSSWALK (SEE DETAIL)

"AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019. ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING."

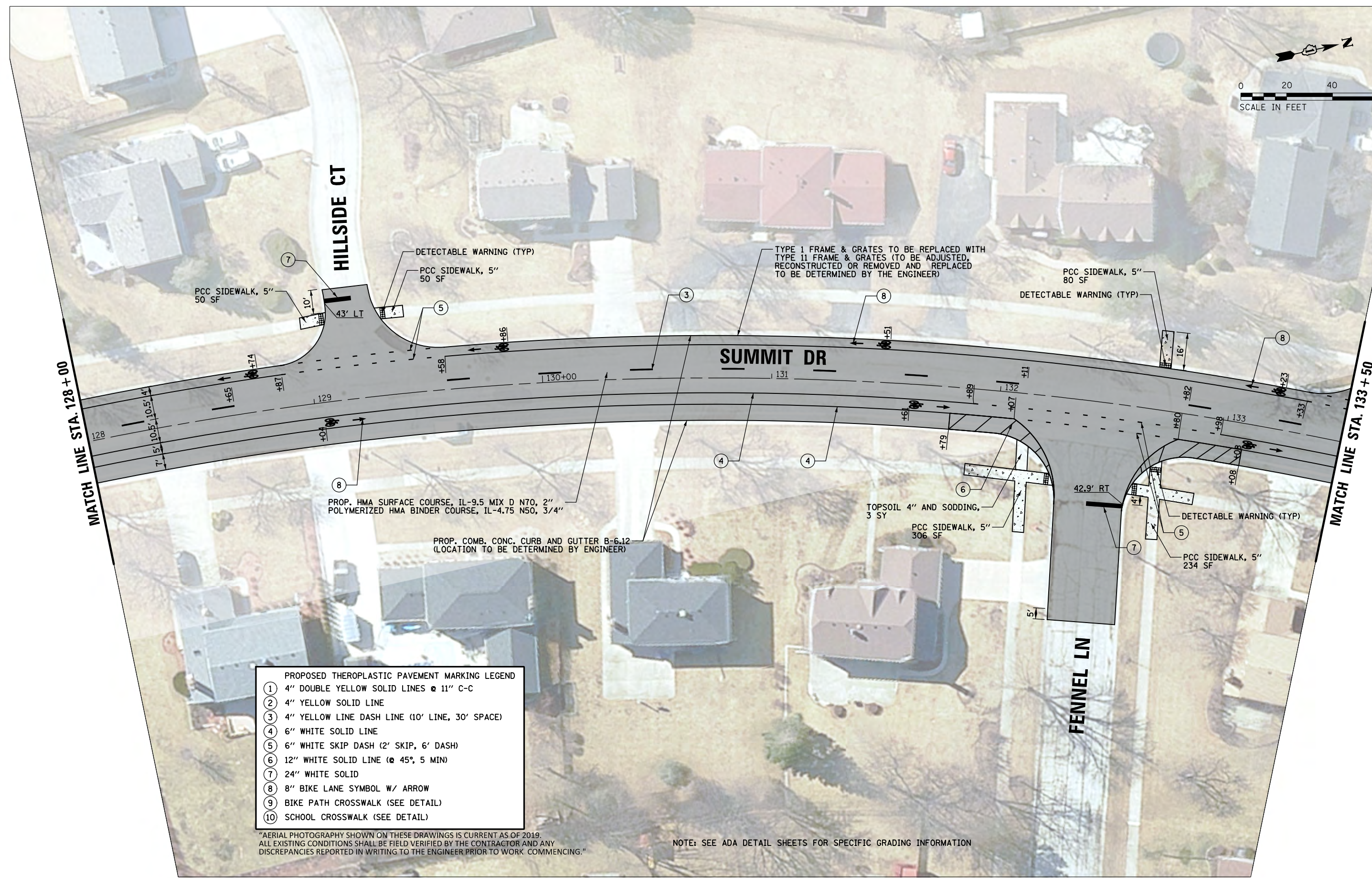
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\PLN05_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48"	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN
SUMMIT DRIVE**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	28
CONTRACT NO. 61H16			ILLINOIS FED. AID PROJECT	



- PROPOSED THEROPLASTIC PAVEMENT MARKING LEGEND**
- ① 4" DOUBLE YELLOW SOLID LINES @ 11" C-C
 - ② 4" YELLOW SOLID LINE
 - ③ 4" YELLOW LINE DASH LINE (10' LINE, 30' SPACE)
 - ④ 6" WHITE SOLID LINE
 - ⑤ 6" WHITE SKIP DASH (2' SKIP, 6' DASH)
 - ⑥ 12" WHITE SOLID LINE (@ 45°, 5 MIN)
 - ⑦ 24" WHITE SOLID
 - ⑧ 8" BIKE LANE SYMBOL W/ ARROW
 - ⑨ BIKE PATH CROSSWALK (SEE DETAIL)
 - ⑩ SCHOOL CROSSWALK (SEE DETAIL)

"AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019. ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING."

NOTE: SEE ADA DETAIL SHEETS FOR SPECIFIC GRADING INFORMATION

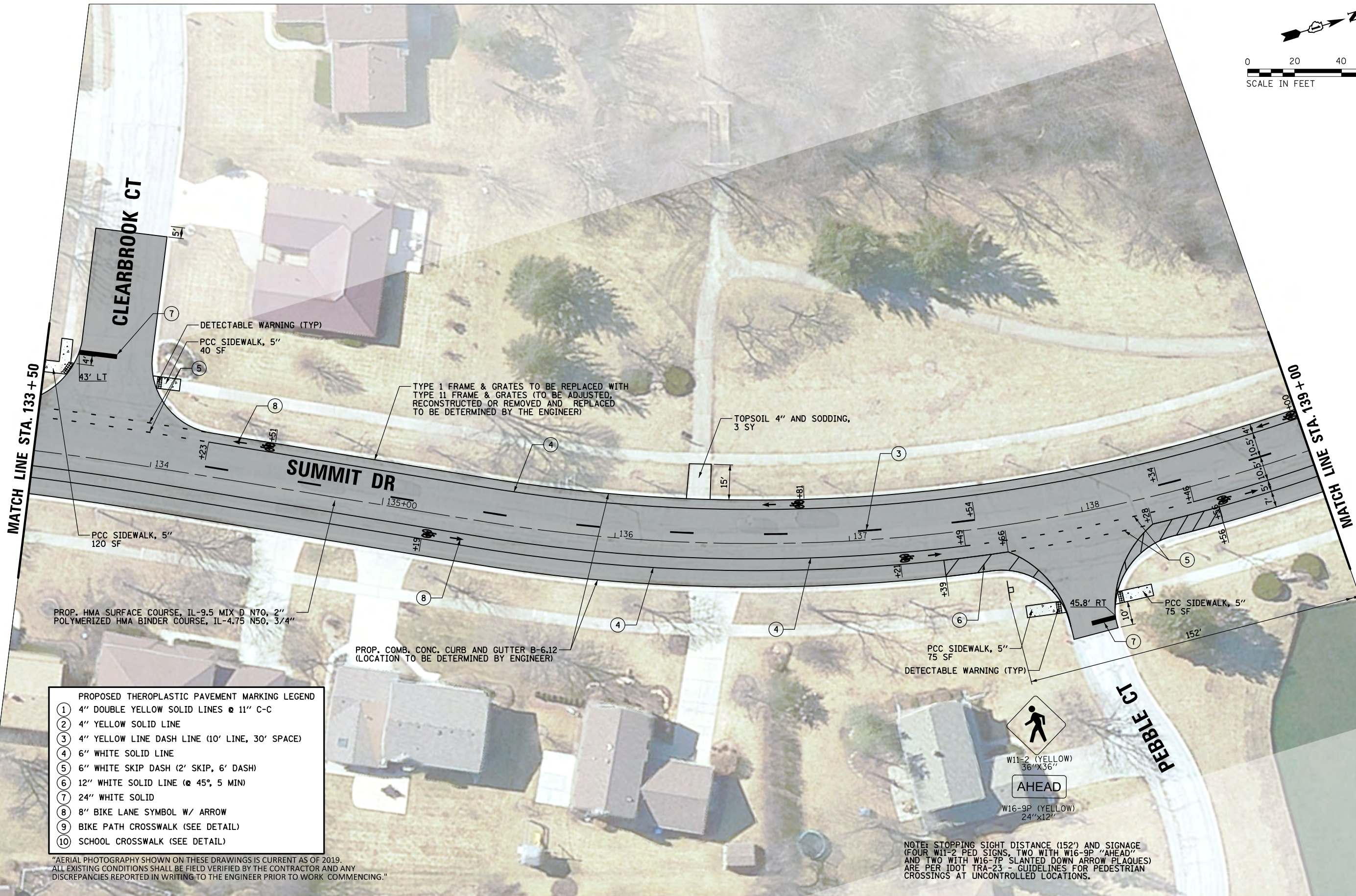
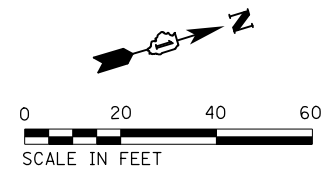
FILE NAME =	USER NAME = esidorczuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\PLN06_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48"	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN
SUMMIT DRIVE**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	29
CONTRACT NO. 61H16			ILLINOIS FED. AID PROJECT	



- PROPOSED THEROPLASTIC PAVEMENT MARKING LEGEND**
- ① 4" DOUBLE YELLOW SOLID LINES @ 11" C-C
 - ② 4" YELLOW SOLID LINE
 - ③ 4" YELLOW LINE DASH LINE (10' LINE, 30' SPACE)
 - ④ 6" WHITE SOLID LINE
 - ⑤ 6" WHITE SKIP DASH (2' SKIP, 6' DASH)
 - ⑥ 12" WHITE SOLID LINE (@ 45°, 5 MIN)
 - ⑦ 24" WHITE SOLID
 - ⑧ 8" BIKE LANE SYMBOL W/ ARROW
 - ⑨ BIKE PATH CROSSWALK (SEE DETAIL)
 - ⑩ SCHOOL CROSSWALK (SEE DETAIL)

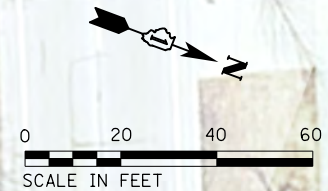
"AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019. ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING."

NOTE: STOPPING SIGHT DISTANCE (152') AND SIGNAGE (FOUR W11-2 PED SIGNS, TWO WITH W16-9P "AHEAD" AND TWO WITH W16-7P SLANTED DOWN ARROW PLAQUES) ARE PER IDOT TRA-23 - GUIDELINES FOR PEDESTRIAN CROSSINGS AT UNCONTROLLED LOCATIONS.

FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\PLN07_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48"	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED PLAN		MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SUMMIT DRIVE		2134	20-00135-00-R5	COOK	53	30
SCALE: 40'	SHEET OF SHEETS	STA. TO STA.	CONTRACT NO. 61H16			
ILLINOIS FED. AID PROJECT						



MATCH LINE STA. 139 + 00

MATCH LINE STA. 144 + 50



W11-2 (YELLOW)
36"X36"



W16-7P (YELLOW)
24"X12"



W11-2 (YELLOW)
36"X36"



W16-9P (YELLOW)
24"X12"



W11-2 (YELLOW)
36"X36"



W16-7P (YELLOW)
24"X12"

TYPE 1 FRAME & GRATES TO BE REPLACED WITH TYPE 11 FRAME & GRATES (TO BE ADJUSTED, RECONSTRUCTED OR REMOVED AND REPLACED TO BE DETERMINED BY THE ENGINEER)

DETECTABLE WARNING (TYP)
PCC SIDEWALK, 5
75 SF

PROP. HMA SURFACE COURSE, IL-9.5 MIX D N70, 2"
POLYMERIZED HMA BINDER COURSE, IL-4.75 N50, 3/4"

PROP. COMB. CONC. CURB AND GUTTER B-6.12
(LOCATION TO BE DETERMINED BY ENGINEER)

SUMMIT DR

WEATHERSFIELD WAY

- PROPOSED THEROPLASTIC PAVEMENT MARKING LEGEND
- ① 4" DOUBLE YELLOW SOLID LINES @ 11" C-C
 - ② 4" YELLOW SOLID LINE
 - ③ 4" YELLOW LINE DASH LINE (10' LINE, 30' SPACE)
 - ④ 6" WHITE SOLID LINE
 - ⑤ 6" WHITE SKIP DASH (2' SKIP, 6' DASH)
 - ⑥ 12" WHITE SOLID LINE @ 45°, 5 MIN
 - ⑦ 24" WHITE SOLID
 - ⑧ 8" BIKE LANE SYMBOL W/ ARROW
 - ⑨ BIKE PATH CROSSWALK (SEE DETAIL)
 - ⑩ SCHOOL CROSSWALK (SEE DETAIL)

NOTE: STOPPING SIGHT DISTANCE (152') AND SIGNAGE (FOUR W11-2 PED SIGNS, TWO WITH W16-9P "AHEAD" AND TWO WITH W16-7P SLANTED DOWN ARROW PLAQUES) ARE PER IDOT TRA-23 - GUIDELINES FOR PEDESTRIAN CROSSINGS AT UNCONTROLLED LOCATIONS.

"AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019. ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING."

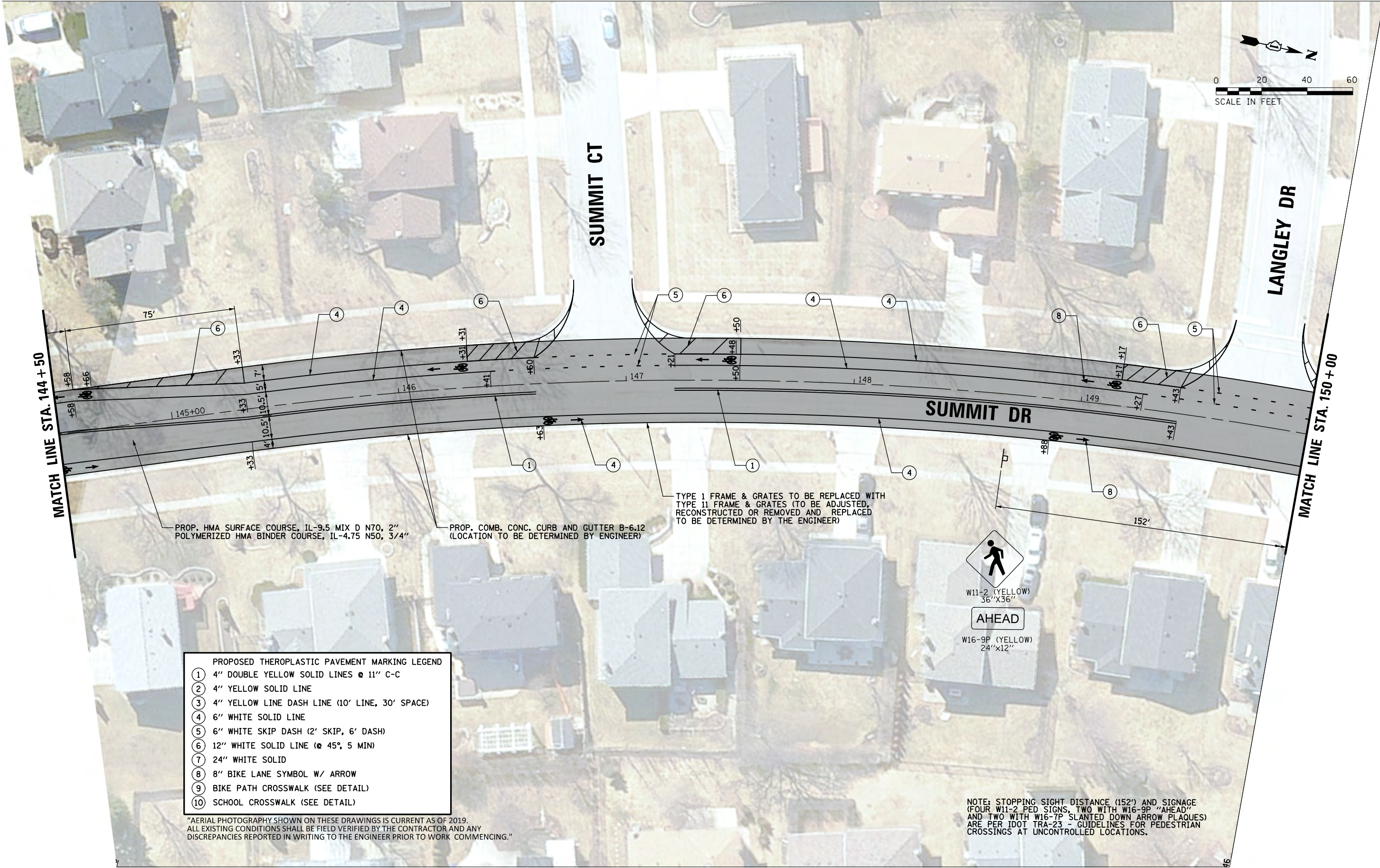
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\PLN08_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED PLAN
SUMMIT DRIVE

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	31
CONTRACT NO. 61H16			ILLINOIS FED. AID PROJECT	



PROP. HMA SURFACE COURSE, IL-9.5 MIX D N70, 2" POLYMERIZED HMA BINDER COURSE, IL-4.75 N50, 3/4"

PROP. COMB. CONC. CURB AND GUTTER B-6.12 (LOCATION TO BE DETERMINED BY ENGINEER)

TYPE 1 FRAME & GRATES TO BE REPLACED WITH TYPE 11 FRAME & GRATES (TO BE ADJUSTED, RECONSTRUCTED OR REMOVED AND REPLACED TO BE DETERMINED BY THE ENGINEER)

- PROPOSED THEROPLASTIC PAVEMENT MARKING LEGEND**
- ① 4" DOUBLE YELLOW SOLID LINES @ 11" C-C
 - ② 4" YELLOW SOLID LINE
 - ③ 4" YELLOW LINE DASH LINE (10' LINE, 30' SPACE)
 - ④ 6" WHITE SOLID LINE
 - ⑤ 6" WHITE SKIP DASH (2' SKIP, 6' DASH)
 - ⑥ 12" WHITE SOLID LINE (@ 45°, 5 MIN)
 - ⑦ 24" WHITE SOLID
 - ⑧ 8" BIKE LANE SYMBOL W/ ARROW
 - ⑨ BIKE PATH CROSSWALK (SEE DETAIL)
 - ⑩ SCHOOL CROSSWALK (SEE DETAIL)

"AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019. ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING."



NOTE: STOPPING SIGHT DISTANCE (152') AND SIGNAGE (FOUR W11-2 PED SIGNS, TWO WITH W16-9P "AHEAD" AND TWO WITH W16-7P SLANTED DOWN ARROW PLAQUES) ARE PER IDOT TRA-23 - GUIDELINES FOR PEDESTRIAN CROSSINGS AT UNCONTROLLED LOCATIONS.

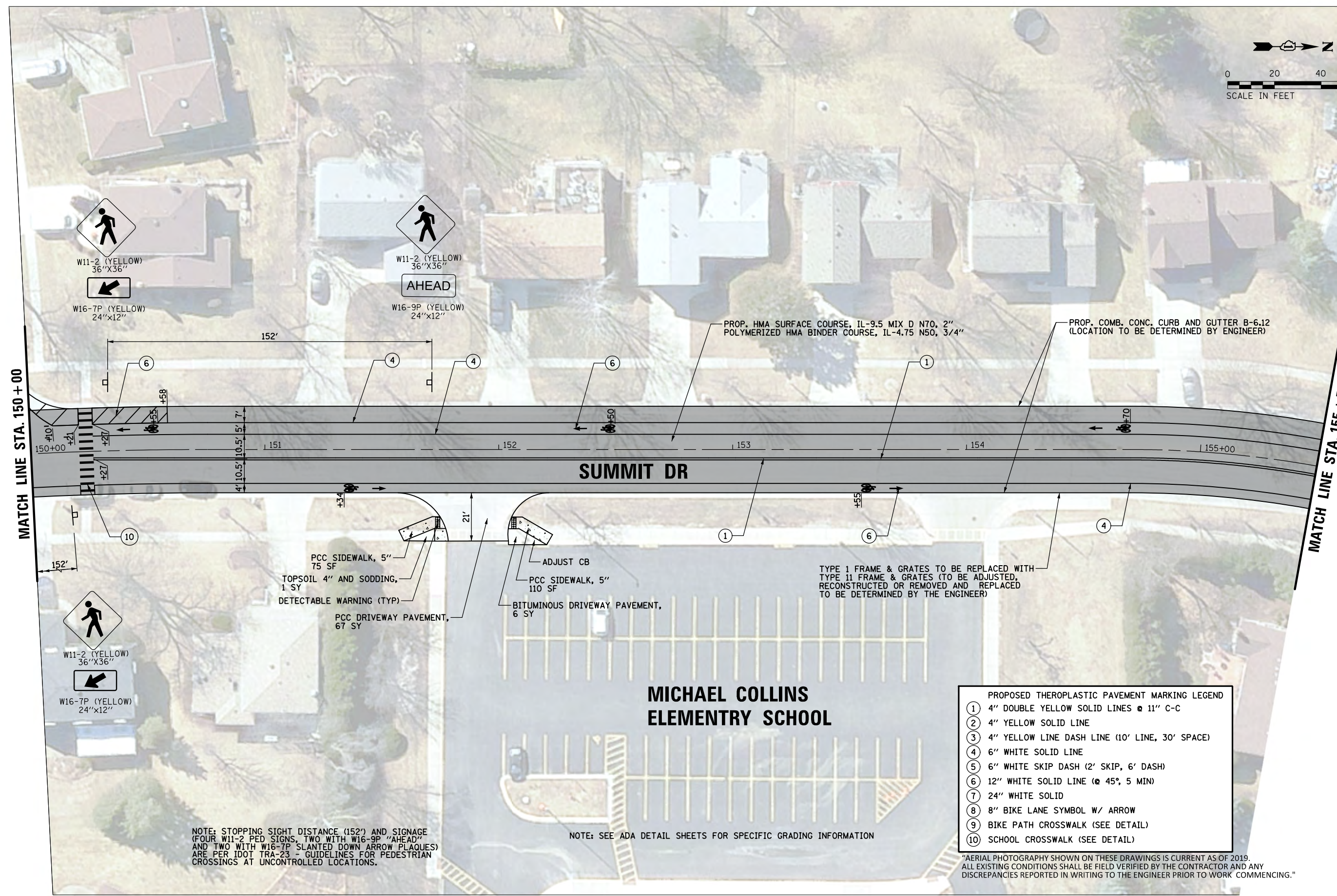
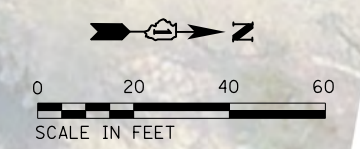
FILE NAME = N:\SCHAUMBURG\200227\Civil\PLN\09_200227.sht	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
Default	PLOT SCALE = 48"	DRAWN - PM	REVISED -
	PLOT DATE = 6/4/2021	CHECKED - GR	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN
SUMMIT DRIVE**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	32
CONTRACT NO. 61H16				
ILLINOIS FED. AID PROJECT				



W11-2 (YELLOW)
36"X36"
W16-7P (YELLOW)
24"X12"

W11-2 (YELLOW)
36"X36"
AHEAD
W16-9P (YELLOW)
24"X12"

PROP. HMA SURFACE COURSE, IL-9.5 MIX D N70, 2"
POLYMERIZED HMA BINDER COURSE, IL-4.75 N50, 3/4"

PROP. COMB. CONC. CURB AND GUTTER B-6.12
(LOCATION TO BE DETERMINED BY ENGINEER)

PCC SIDEWALK, 5"
75 SF
TOPSOIL 4" AND SODDING,
1 SY
DETECTABLE WARNING (TYP)
PCC DRIVEWAY PAVEMENT,
67 SY

ADJUST CB
PCC SIDEWALK, 5"
110 SF
BITUMINOUS DRIVEWAY PAVEMENT,
6 SY

TYPE 1 FRAME & GRATES TO BE REPLACED WITH
TYPE 11 FRAME & GRATES (TO BE ADJUSTED,
RECONSTRUCTED OR REMOVED AND REPLACED
TO BE DETERMINED BY THE ENGINEER)

**MICHAEL COLLINS
ELEMENTARY SCHOOL**

- PROPOSED THEROPLASTIC PAVEMENT MARKING LEGEND**
- ① 4" DOUBLE YELLOW SOLID LINES @ 11" C-C
 - ② 4" YELLOW SOLID LINE
 - ③ 4" YELLOW LINE DASH LINE (10' LINE, 30' SPACE)
 - ④ 6" WHITE SOLID LINE
 - ⑤ 6" WHITE SKIP DASH (2' SKIP, 6' DASH)
 - ⑥ 12" WHITE SOLID LINE (@ 45°, 5 MIN)
 - ⑦ 24" WHITE SOLID
 - ⑧ 8" BIKE LANE SYMBOL W/ ARROW
 - ⑨ BIKE PATH CROSSWALK (SEE DETAIL)
 - ⑩ SCHOOL CROSSWALK (SEE DETAIL)

NOTE: STOPPING SIGHT DISTANCE (152') AND SIGNAGE (FOUR W11-2 PED SIGNS, TWO WITH W16-9P "AHEAD" AND TWO WITH W16-7P SLANTED DOWN ARROW PLAQUES) ARE PER IDOT TRA-23 - GUIDELINES FOR PEDESTRIAN CROSSINGS AT UNCONTROLLED LOCATIONS.

NOTE: SEE ADA DETAIL SHEETS FOR SPECIFIC GRADING INFORMATION

"AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019. ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING."

FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\PLN10_200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48"	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

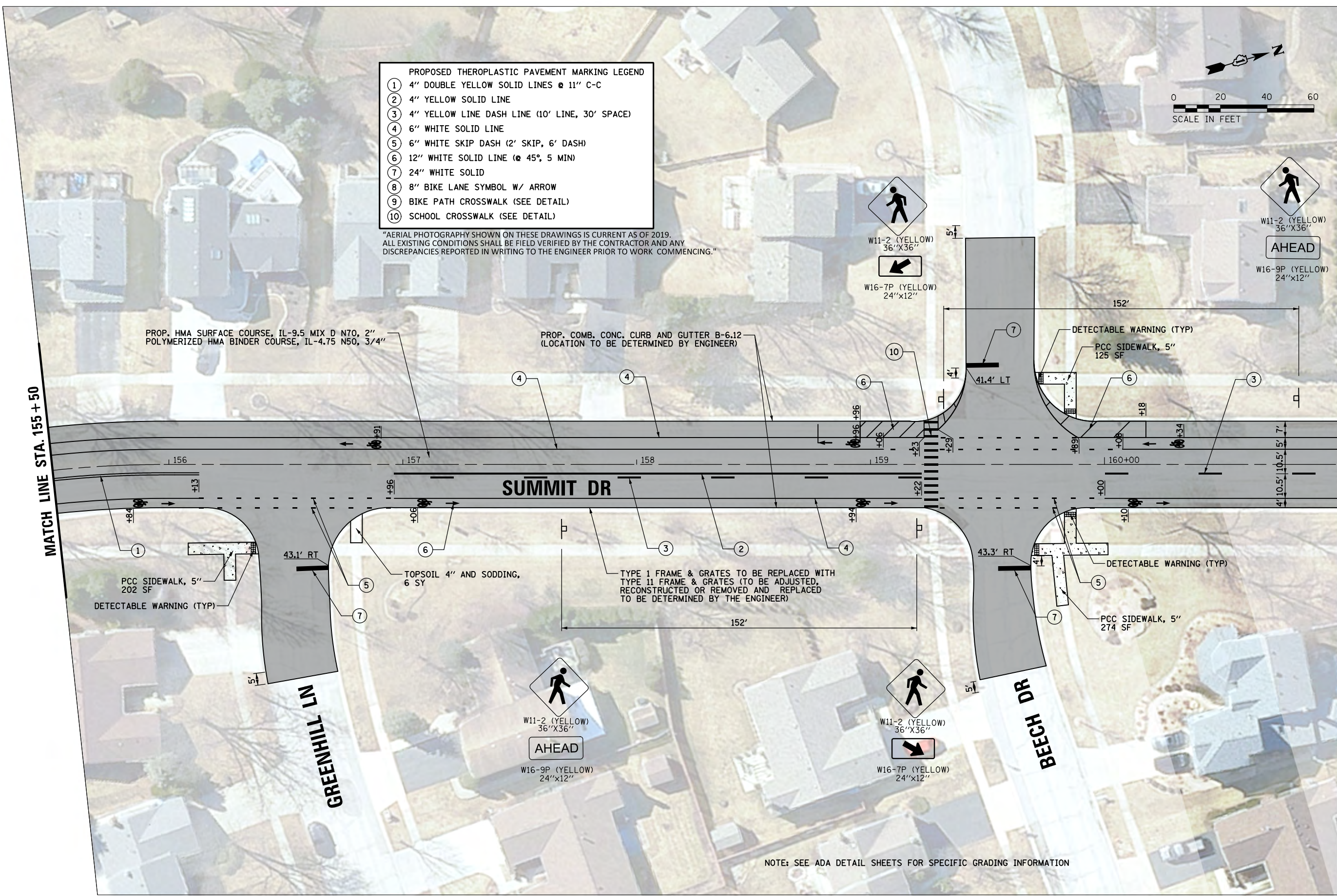
**PROPOSED PLAN
SUMMIT DRIVE**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	33
CONTRACT NO. 61H16			ILLINOIS FED. AID PROJECT	

- PROPOSED THEROPLASTIC PAVEMENT MARKING LEGEND**
- ① 4" DOUBLE YELLOW SOLID LINES @ 11" C-C
 - ② 4" YELLOW SOLID LINE
 - ③ 4" YELLOW LINE DASH LINE (10' LINE, 30' SPACE)
 - ④ 6" WHITE SOLID LINE
 - ⑤ 6" WHITE SKIP DASH (2' SKIP, 6' DASH)
 - ⑥ 12" WHITE SOLID LINE @ 45°, 5 MIN
 - ⑦ 24" WHITE SOLID
 - ⑧ 8" BIKE LANE SYMBOL W/ ARROW
 - ⑨ BIKE PATH CROSSWALK (SEE DETAIL)
 - ⑩ SCHOOL CROSSWALK (SEE DETAIL)

"AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019. ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING."



MATCH LINE STA. 155 + 50

MATCH LINE STA. 161 + 00

PROP. HMA SURFACE COURSE, IL-9.5 MIX D N70, 2"
POLYMERIZED HMA BINDER COURSE, IL-4.75 N50, 3/4"

PROP. COMB. CONC. CURB AND GUTTER B-6.12
(LOCATION TO BE DETERMINED BY ENGINEER)

PCC SIDEWALK, 5"
202 SF

TOPSOIL 4" AND SODDING,
6 SY

TYPE 1 FRAME & GRATES TO BE REPLACED WITH
TYPE 11 FRAME & GRATES (TO BE ADJUSTED,
RECONSTRUCTED OR REMOVED AND REPLACED
TO BE DETERMINED BY THE ENGINEER)

PCC SIDEWALK, 5"
274 SF

NOTE: SEE ADA DETAIL SHEETS FOR SPECIFIC GRADING INFORMATION

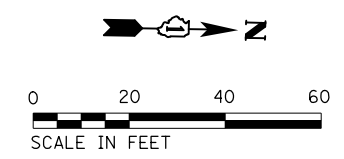
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\PLN11.200227.dht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48"	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN
SUMMIT DRIVE**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

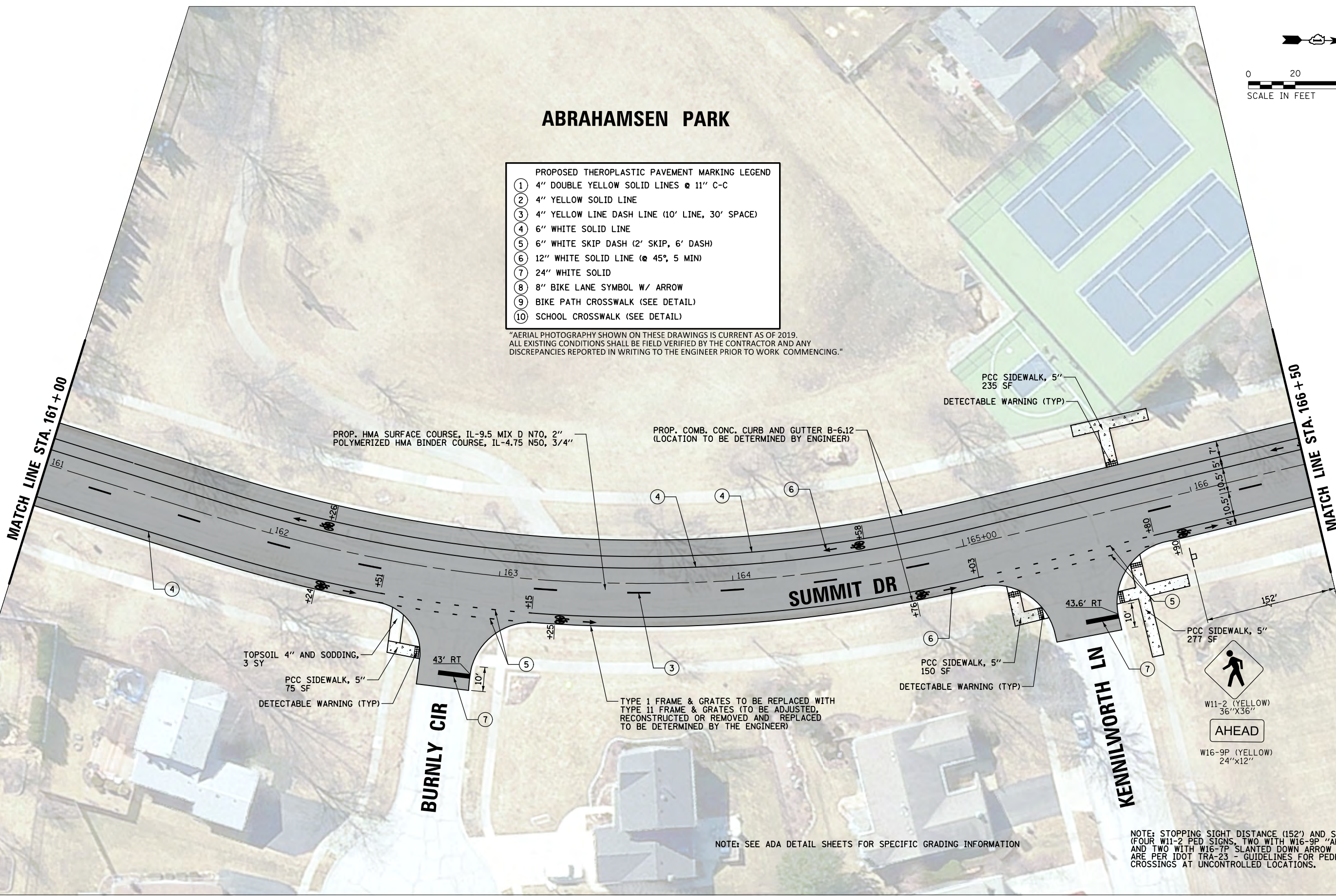
MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	34
				CONTRACT NO. 61H16
ILLINOIS FED. AID PROJECT				



ABRAHAMSEN PARK

- PROPOSED THEROPLASTIC PAVEMENT MARKING LEGEND**
- ① 4" DOUBLE YELLOW SOLID LINES @ 11" C-C
 - ② 4" YELLOW SOLID LINE
 - ③ 4" YELLOW LINE DASH LINE (10' LINE, 30' SPACE)
 - ④ 6" WHITE SOLID LINE
 - ⑤ 6" WHITE SKIP DASH (2' SKIP, 6' DASH)
 - ⑥ 12" WHITE SOLID LINE (@ 45°, 5 MIN)
 - ⑦ 24" WHITE SOLID
 - ⑧ 8" BIKE LANE SYMBOL W/ ARROW
 - ⑨ BIKE PATH CROSSWALK (SEE DETAIL)
 - ⑩ SCHOOL CROSSWALK (SEE DETAIL)

"AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019. ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING."

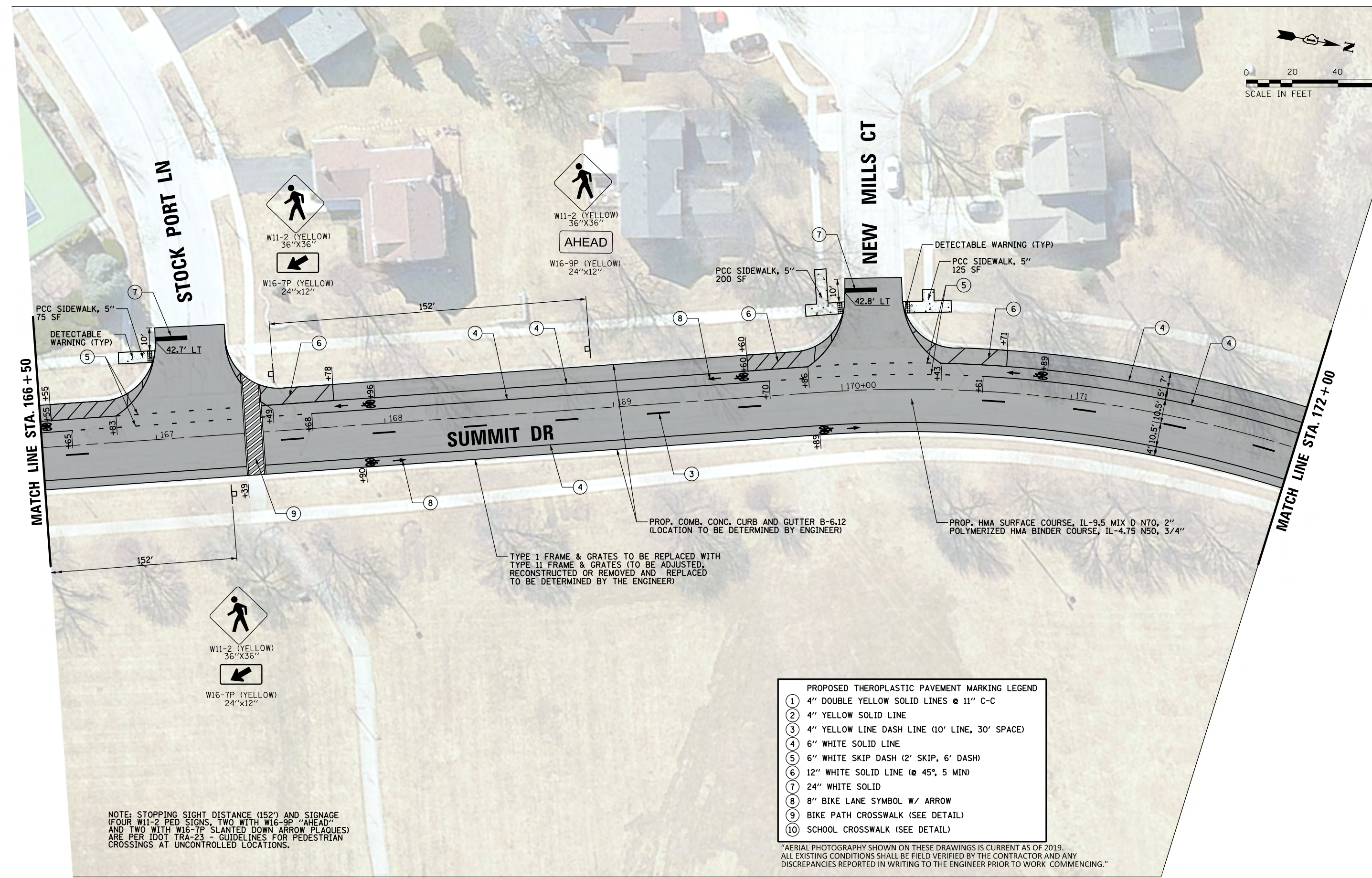
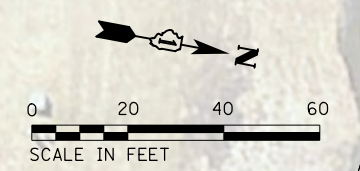


FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\PLN12.200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED PLAN			
SUMMIT DRIVE			
SCALE: 40'	SHEET	OF SHEETS	STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	35
				CONTRACT NO. 61H16
ILLINOIS FED. AID PROJECT				



NOTE: STOPPING SIGHT DISTANCE (152') AND SIGNAGE (FOUR W11-2 PED SIGNS, TWO WITH W16-9P "AHEAD" AND TWO WITH W16-7P SLANTED DOWN ARROW PLAQUES) ARE PER IDOT TRA-23 - GUIDELINES FOR PEDESTRIAN CROSSINGS AT UNCONTROLLED LOCATIONS.

- PROPOSED THEROPLASTIC PAVEMENT MARKING LEGEND**
- ① 4" DOUBLE YELLOW SOLID LINES @ 11" C-C
 - ② 4" YELLOW SOLID LINE
 - ③ 4" YELLOW LINE DASH LINE (10' LINE, 30' SPACE)
 - ④ 6" WHITE SOLID LINE
 - ⑤ 6" WHITE SKIP DASH (2' SKIP, 6' DASH)
 - ⑥ 12" WHITE SOLID LINE (@ 45°, 5 MIN)
 - ⑦ 24" WHITE SOLID
 - ⑧ 8" BIKE LANE SYMBOL W/ ARROW
 - ⑨ BIKE PATH CROSSWALK (SEE DETAIL)
 - ⑩ SCHOOL CROSSWALK (SEE DETAIL)

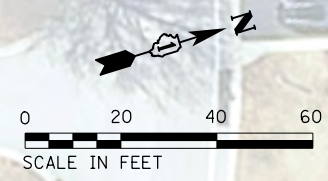
"AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019. ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING."

FILE NAME = N:\SCHAUMBURG\200227\Civi\PLN\3.200227.sht	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
PLOT SCALE = 48"	CHECKED - GR	DRAWN - PM	REVISED -
PLOT DATE = 6/4/2021	DATE -		REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

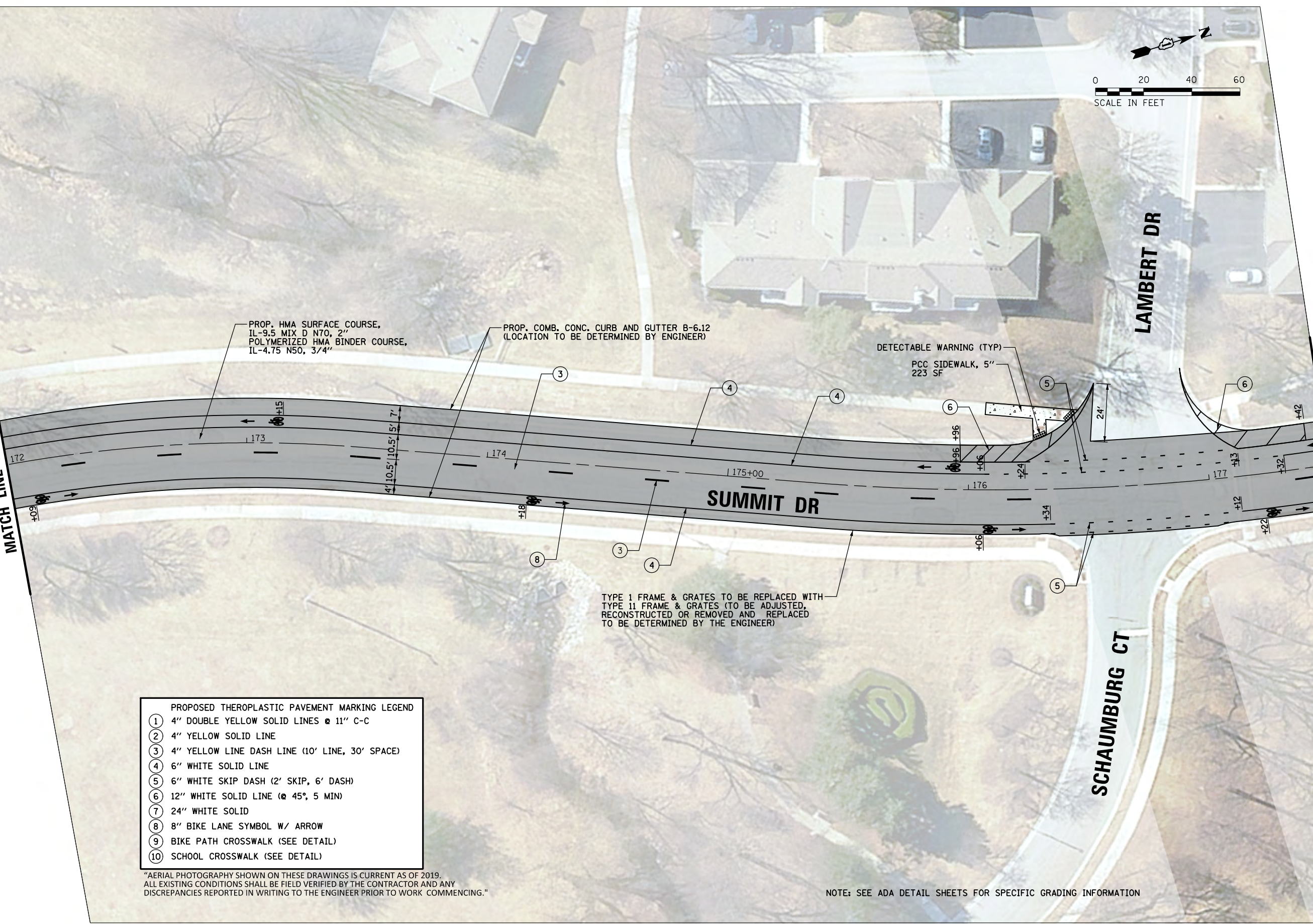
PROPOSED PLAN SUMMIT DRIVE	
SCALE: 40'	SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	36
CONTRACT NO. 61H16				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 172+00

MATCH LINE STA. 177+50



PROP. HMA SURFACE COURSE,
IL-9.5 MIX D N70, 2"
POLYMERIZED HMA BINDER COURSE,
IL-4.75 N50, 3/4"

PROP. COMB. CONC. CURB AND GUTTER B-6.12
(LOCATION TO BE DETERMINED BY ENGINEER)

DETECTABLE WARNING (TYP)
PCC SIDEWALK, 5"
223 SF

TYPE 1 FRAME & GRATES TO BE REPLACED WITH
TYPE 11 FRAME & GRATES (TO BE ADJUSTED,
RECONSTRUCTED OR REMOVED AND REPLACED
TO BE DETERMINED BY THE ENGINEER)

- PROPOSED THEROPLASTIC PAVEMENT MARKING LEGEND**
- ① 4" DOUBLE YELLOW SOLID LINES @ 11" C-C
 - ② 4" YELLOW SOLID LINE
 - ③ 4" YELLOW LINE DASH LINE (10' LINE, 30' SPACE)
 - ④ 6" WHITE SOLID LINE
 - ⑤ 6" WHITE SKIP DASH (2' SKIP, 6' DASH)
 - ⑥ 12" WHITE SOLID LINE (@ 45°, 5 MIN)
 - ⑦ 24" WHITE SOLID
 - ⑧ 8" BIKE LANE SYMBOL W/ ARROW
 - ⑨ BIKE PATH CROSSWALK (SEE DETAIL)
 - ⑩ SCHOOL CROSSWALK (SEE DETAIL)

"AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019.
ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY
DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING."

NOTE: SEE ADA DETAIL SHEETS FOR SPECIFIC GRADING INFORMATION

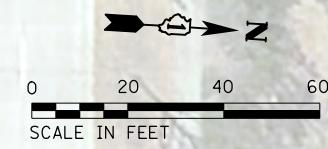
FILE NAME = N:\SCHAUMBURG\200227\Civil\PLN14_200227	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
Default	PLOT SCALE = 48"	DRAWN - PM	REVISED -
	PLOT DATE = 6/4/2021	CHECKED - GR	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

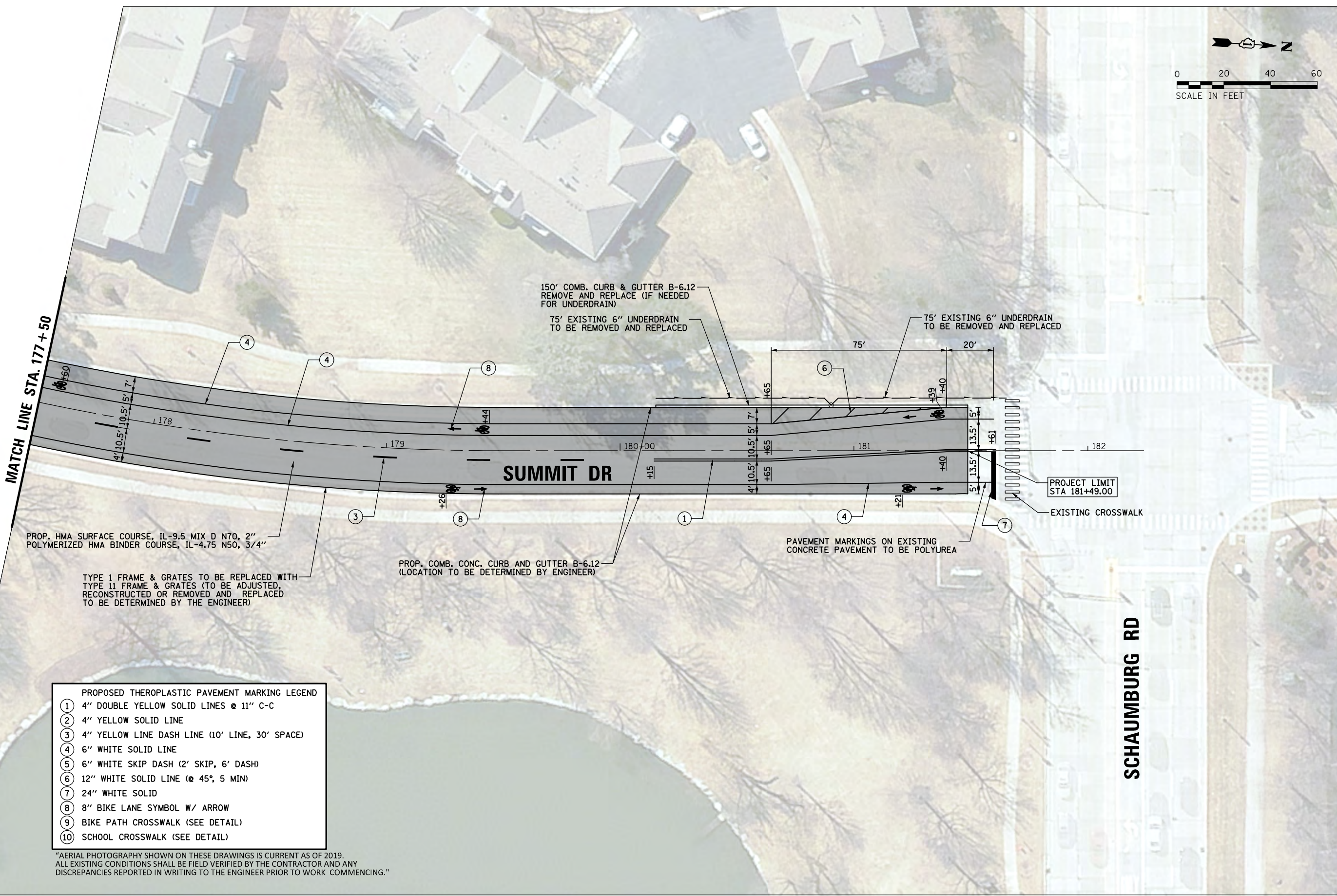
**PROPOSED PLAN
SUMMIT DRIVE**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	37
CONTRACT NO. 61H16				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 177+50



PROP. HMA SURFACE COURSE, IL-9.5 MIX D N70, 2"
POLYMERIZED HMA BINDER COURSE, IL-4.75 N50, 3/4"

TYPE 1 FRAME & GRATES TO BE REPLACED WITH
TYPE 11 FRAME & GRATES (TO BE ADJUSTED,
RECONSTRUCTED OR REMOVED AND REPLACED
TO BE DETERMINED BY THE ENGINEER)

PROP. COMB. CONC. CURB AND GUTTER B-6.12
(LOCATION TO BE DETERMINED BY ENGINEER)

PAVEMENT MARKINGS ON EXISTING
CONCRETE PAVEMENT TO BE POLYUREA

PROJECT LIMIT
STA 181+49.00

EXISTING CROSSWALK

- PROPOSED THEROPLASTIC PAVEMENT MARKING LEGEND
- ① 4" DOUBLE YELLOW SOLID LINES @ 11" C-C
 - ② 4" YELLOW SOLID LINE
 - ③ 4" YELLOW LINE DASH LINE (10' LINE, 30' SPACE)
 - ④ 6" WHITE SOLID LINE
 - ⑤ 6" WHITE SKIP DASH (2' SKIP, 6' DASH)
 - ⑥ 12" WHITE SOLID LINE @ 45°, 5 MIN
 - ⑦ 24" WHITE SOLID
 - ⑧ 8" BIKE LANE SYMBOL W/ ARROW
 - ⑨ BIKE PATH CROSSWALK (SEE DETAIL)
 - ⑩ SCHOOL CROSSWALK (SEE DETAIL)

"AERIAL PHOTOGRAPHY SHOWN ON THESE DRAWINGS IS CURRENT AS OF 2019.
ALL EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR AND ANY
DISCREPANCIES REPORTED IN WRITING TO THE ENGINEER PRIOR TO WORK COMMENCING."

SCHAUMBURG RD

FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\PLN15.200227.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 48"	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

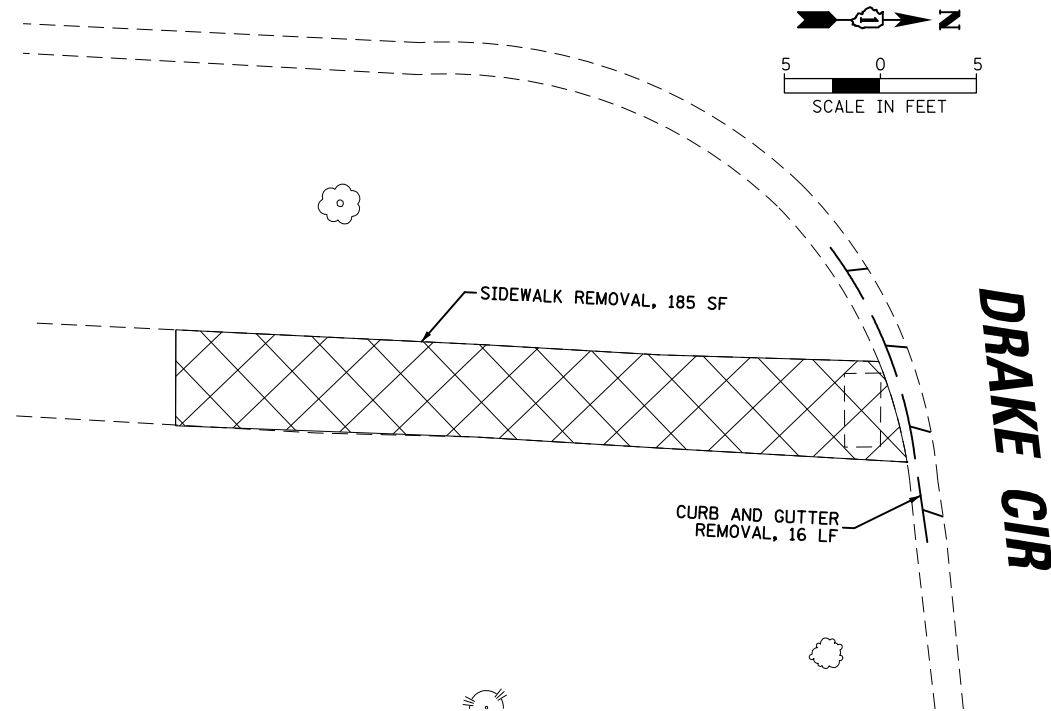
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED PLAN
SUMMIT DRIVE

SCALE: 40' SHEET OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	38
CONTRACT NO. 61H16				
ILLINOIS FED. AID PROJECT				

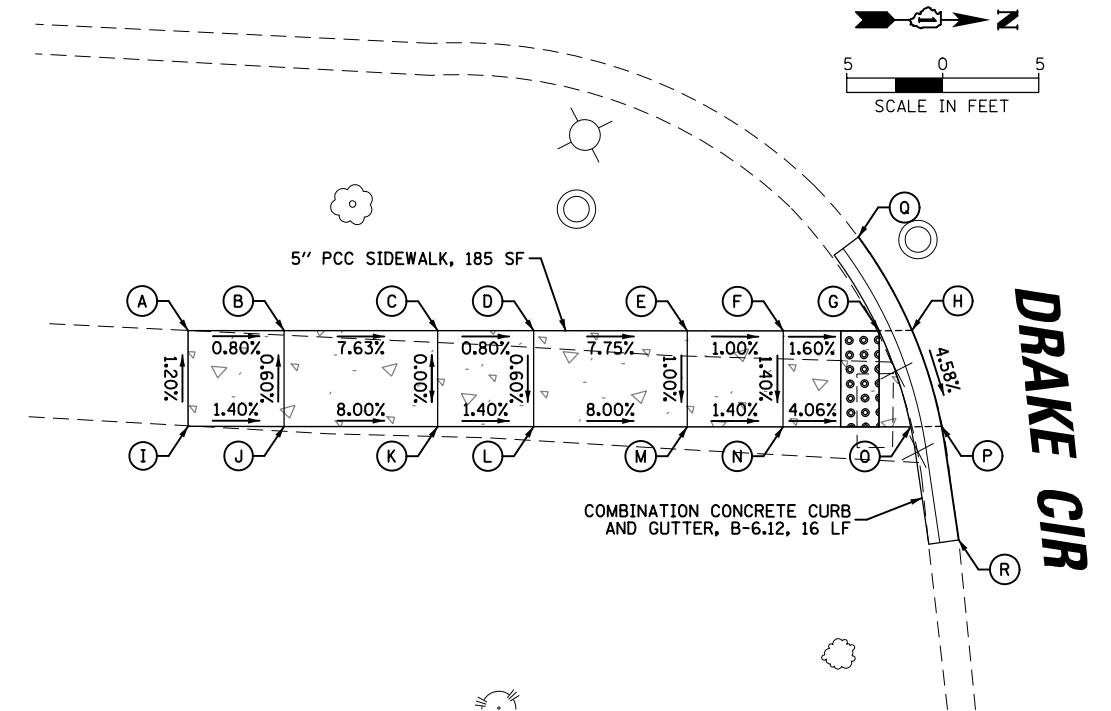
SUMMIT DR



DRAKE CIR

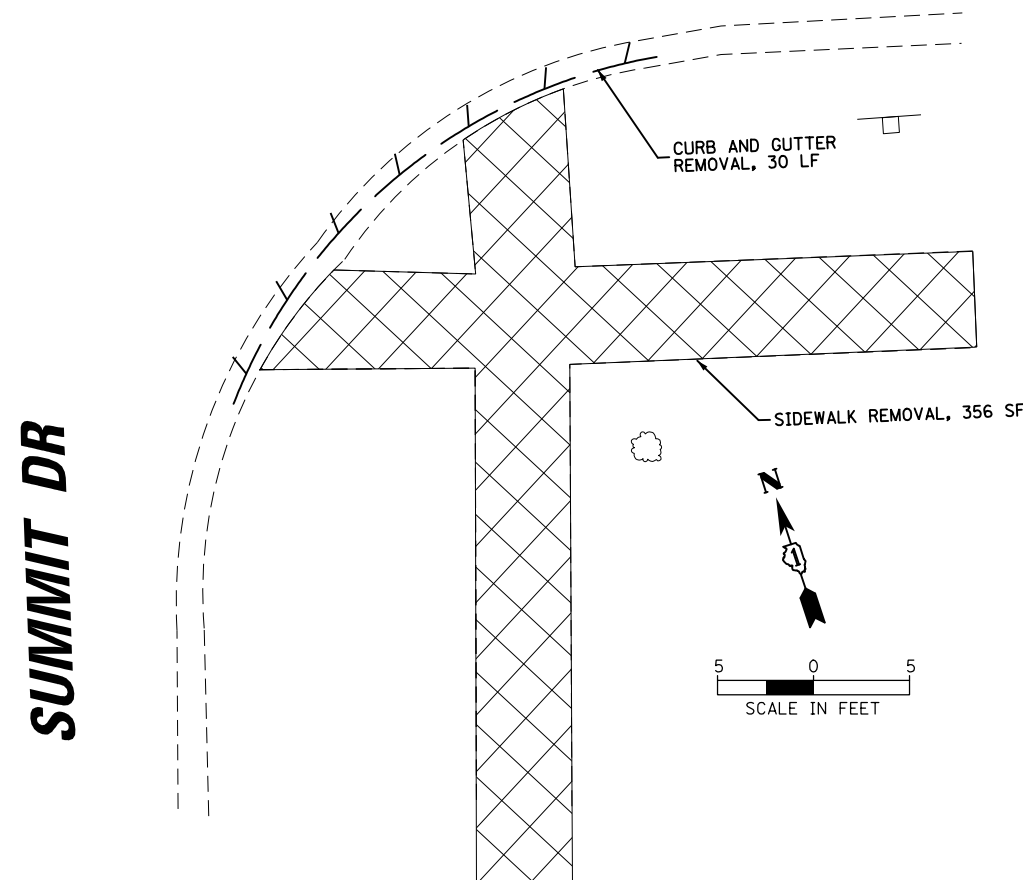
SE QUADRANT OF DRAKE CIR & SUMMIT DR			
POINT	NORTH	EAST	ELEV
A	1945880.13	1055801.64	769.84
B	1945884.90	1055800.12	769.80
C	1945892.52	1055797.69	769.19
D	1945897.28	1055796.17	769.15
E	1945904.90	1055793.74	768.53
F	1945909.67	1055792.22	768.48
G	1945914.43	1055790.71	768.40
H	1945916.06	1055790.15	768.41
I	1945881.65	1055806.40	769.90
J	1945886.41	1055804.88	769.83
K	1945894.04	1055802.45	769.19
L	1945898.80	1055800.93	769.12
M	1945906.42	1055798.51	768.48
N	1945911.19	1055796.99	768.41
O	1945917.53	1055794.97	768.14
P	1945919.05	1055794.45	768.17
Q	1945911.91	1055786.37	768.65
R	1945921.70	1055799.81	767.99

SUMMIT DR



DRAKE CIR

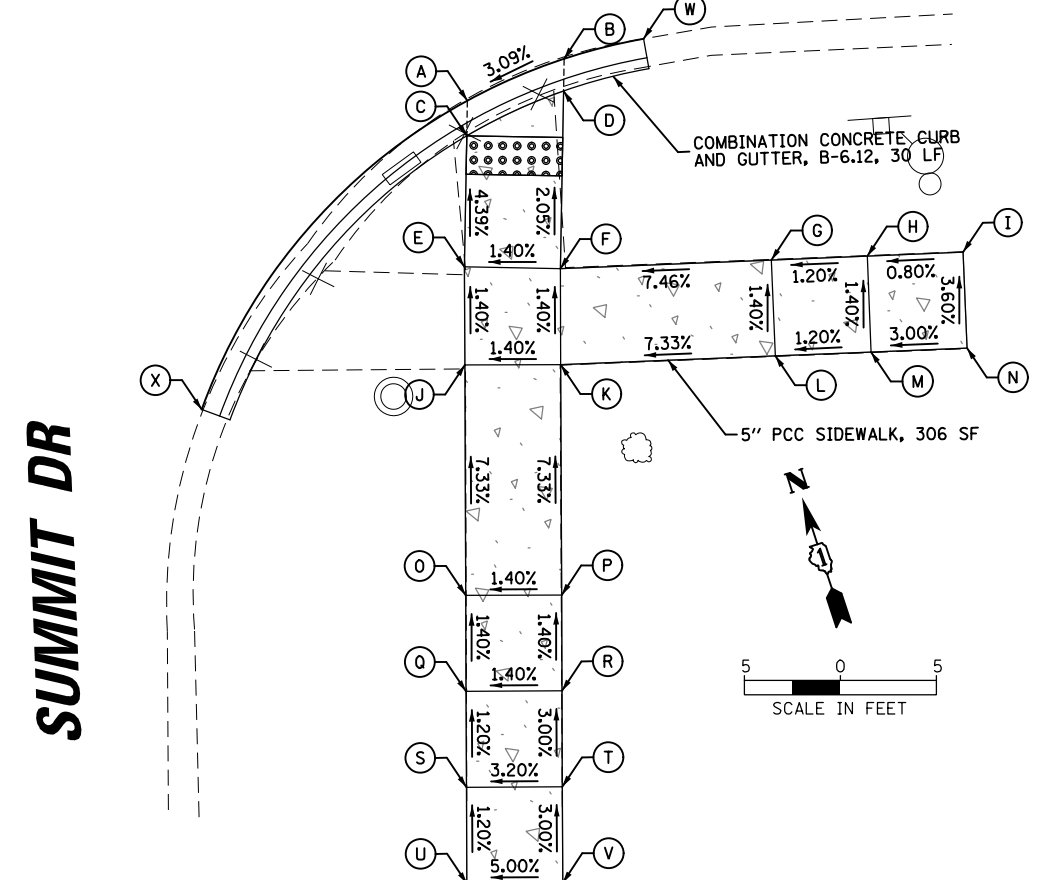
FENNEL LN



SUMMIT DR

SE QUADRANT OF FENNEL LN & SUMMIT DR			
POINT	NORTH	EAST	ELEV
A	1947956.91	1055804.58	753.53
B	1947957.43	1055810.05	753.70
C	1947955.18	1055803.98	753.46
D	1947955.84	1055809.50	753.64
E	1947948.72	1055801.76	753.76
F	1947947.09	1055806.48	753.83
G	1947944.12	1055817.07	754.65
H	1947942.77	1055821.89	754.71
I	1947941.41	1055826.70	754.75
J	1947943.86	1055800.19	753.83
K	1947942.33	1055804.95	753.90
L	1947939.30	1055815.72	754.72
M	1947937.95	1055820.54	754.78
N	1947936.60	1055825.35	754.93
O	1947932.43	1055796.53	754.71
P	1947930.90	1055801.29	754.78
Q	1947927.67	1055795.00	754.78
R	1947926.14	1055799.76	754.85
S	1947922.91	1055793.47	754.84
T	1947921.38	1055798.23	755.00
U	1947918.15	1055791.94	754.90
V	1947916.62	1055796.70	755.15
W	1947957.13	1055814.31	753.87
X	1947945.89	1055786.43	753.59

FENNEL LN



SUMMIT DR

FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\ADA.01.200227.dgn		DRAWN - PM	REVISED -
Default	PLOT SCALE = 18'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

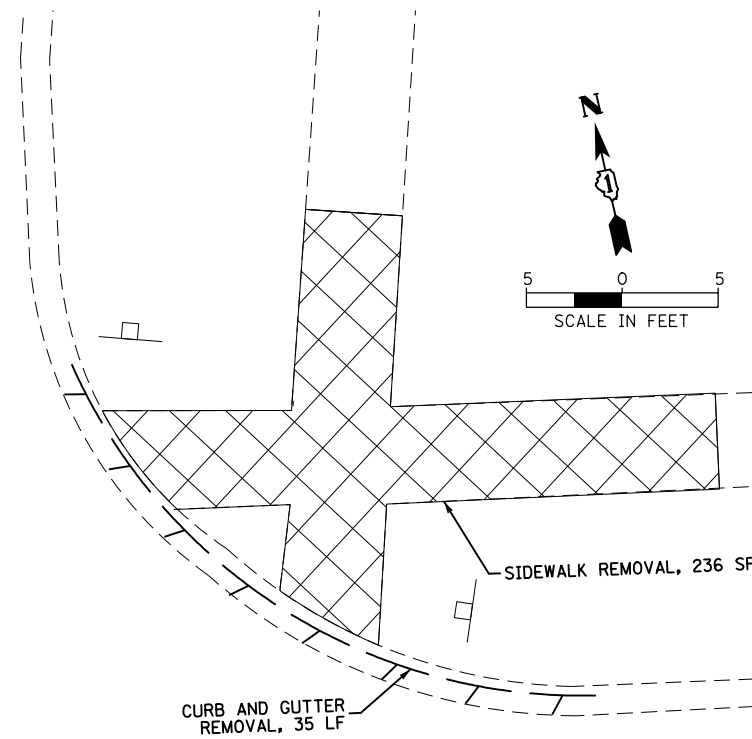
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP GRADING PLAN
SUMMIT DR AND FENNEL LN

SCALE: 10' SHEET 1 OF 5 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	39
CONTRACT NO. 61H16				
ILLINOIS FED. AID PROJECT				

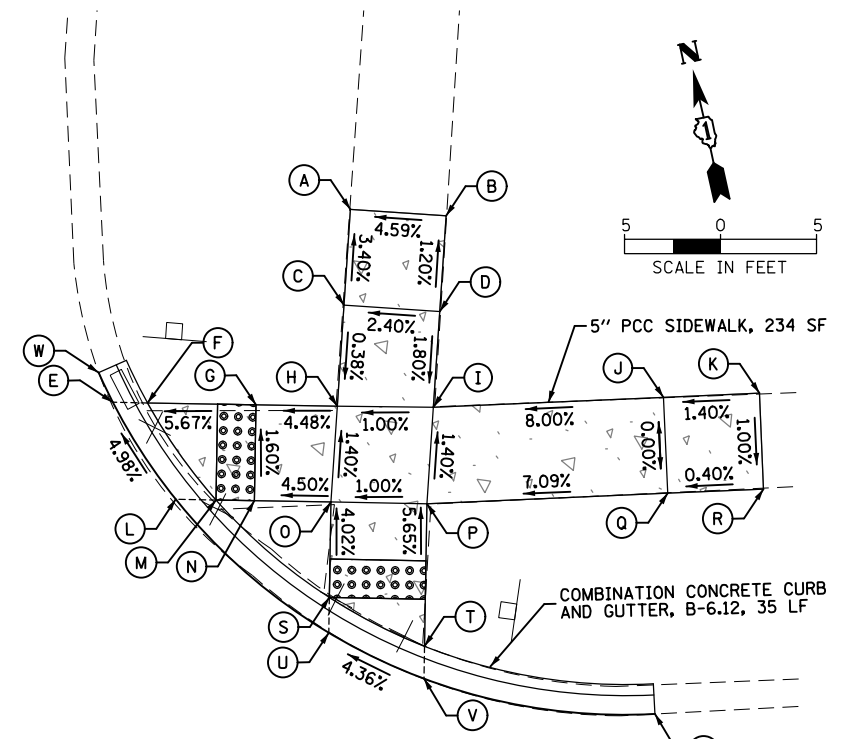
SUMMIT DR



FENNEL LN

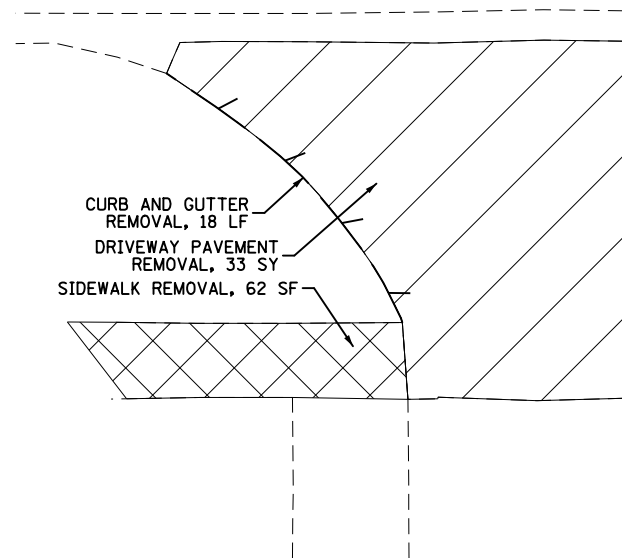
NE QUADRANT OF FENNEL LN & SUMMIT DR			
POINT	NORTH	EAST	ELEV
A	1948013.33	1055824.81	752.95
B	1948011.47	1055829.46	753.18
C	1948008.69	1055822.96	753.12
D	1948006.83	1055827.60	753.24
E	1948007.60	1055809.94	752.59
F	1948007.01	1055811.65	752.59
G	1948005.17	1055816.99	752.91
H	1948003.79	1055821.00	753.10
I	1948002.19	1055825.74	753.15
J	1947998.97	1055837.30	754.11
K	1947997.63	1055842.12	754.18
L	1948001.78	1055811.49	752.89
M	1948001.10	1055813.47	752.92
N	1948000.45	1055815.36	752.99
O	1947999.14	1055819.14	753.17
P	1947997.52	1055823.87	753.22
Q	1947994.16	1055835.96	754.11
R	1947992.82	1055840.78	754.13
S	1947994.44	1055817.52	753.37
T	1947990.49	1055821.45	753.64
U	1947992.70	1055816.92	753.46
V	1947988.90	1055820.90	753.70
W	1948009.30	1055809.71	752.51
X	1947983.42	1055831.77	754.02

SUMMIT DR



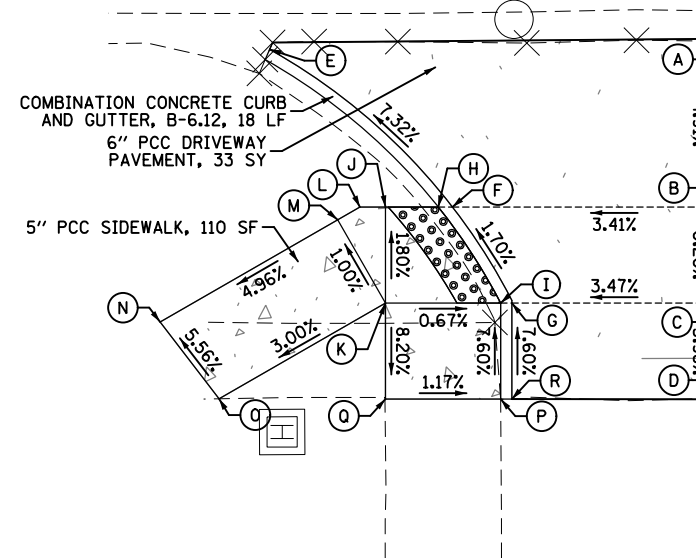
FENNEL LN

SUMMIT DRIVE



**MICHAEL COLLINS SCHOOL
SOUTHERN ENTRANCE**

SUMMIT DRIVE



**MICHAEL COLLINS SCHOOL
SOUTHERN ENTRANCE**

S SIDE OF DRIVEWAY			
POINT	NORTH	EAST	ELEV
A	1949849.75	1055712.99	758.20
B	1949849.72	1055721.75	758.63
C	1949849.70	1055726.75	758.64
D	1949849.68	1055731.77	758.84
E	1949826.46	1055713.46	757.25
F	1949835.95	1055721.71	758.17
G	1949839.03	1055726.72	758.27
H	1949835.22	1055721.70	758.17
I	1949838.45	1055726.72	758.27
J	1949832.47	1055721.69	758.22
K	1949832.45	1055726.69	758.31
L	1949831.13	1055721.69	758.24
M	1949829.97	1055722.35	758.26
N	1949820.70	1055727.65	757.73
O	1949823.76	1055731.65	758.01
P	1949838.43	1055731.72	758.65
Q	1949832.43	1055731.69	758.72
R	1949839.01	1055731.72	758.65

FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\ADA_02_200227.dgn		DRAWN - PM	REVISED -
Default	PLOT SCALE = 10'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

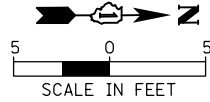
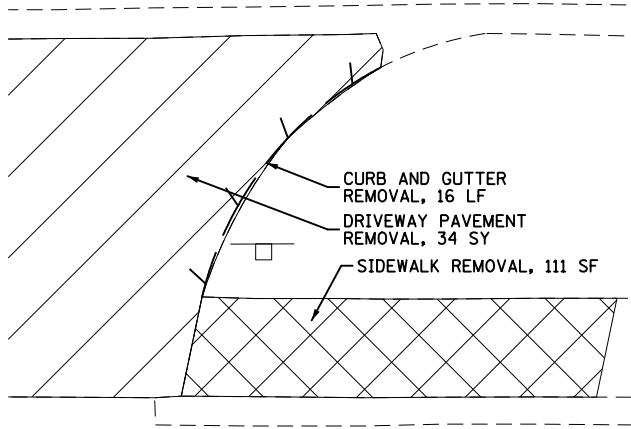
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP GRADING PLAN
SUMMIT DR AND FENNEL LN**

SCALE: 10' SHEET 2 OF 5 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	40
			CONTRACT NO. 61H16	
ILLINOIS FED. AID PROJECT				

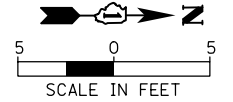
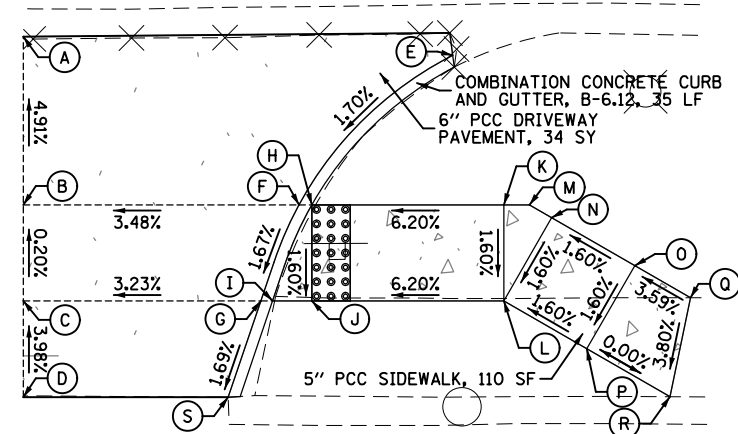
SUMMIT DR



MICHAEL COLLINS SCHOOL
SOUTHERN ENTRANCE

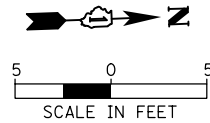
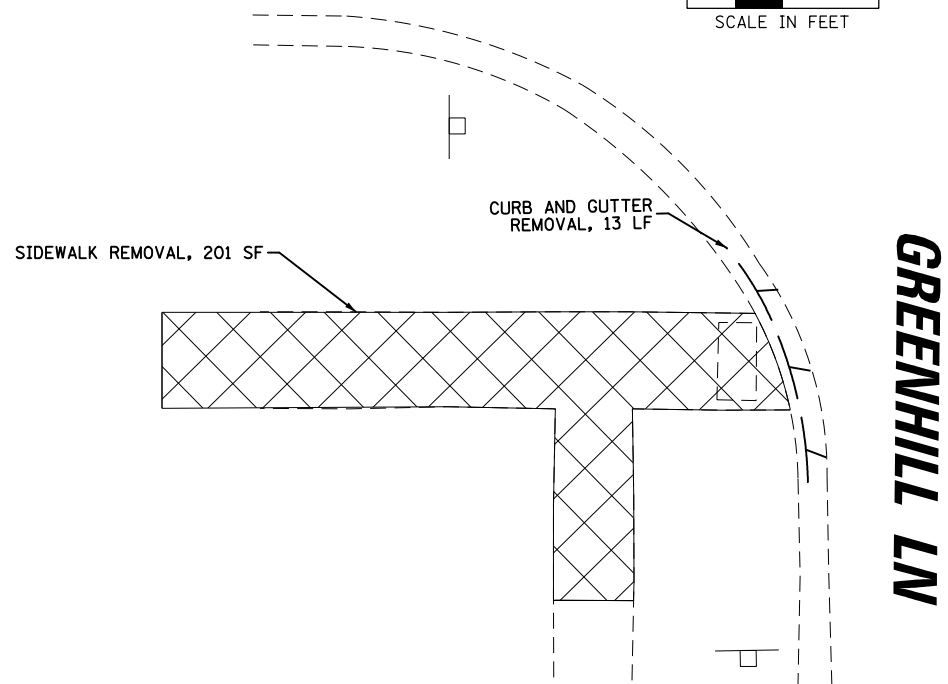
N SIDE OF DRIVEWAY			
POINT	NORTH	EAST	ELEV
A	1949849.75	1055712.99	758.20
B	1949849.72	1055721.75	758.63
C	1949849.70	1055726.75	758.64
D	1949849.68	1055731.77	758.84
E	1949872.12	1055714.04	759.32
F	1949864.09	1055721.80	759.13
G	1949862.08	1055726.80	759.04
H	1949864.75	1055721.81	759.13
I	1949862.69	1055726.80	759.04
J	1949864.73	1055726.81	759.05
K	1949874.75	1055721.84	759.75
L	1949874.73	1055726.84	759.67
M	1949876.09	1055721.85	759.75
N	1949877.24	1055722.52	759.75
O	1949881.57	1055725.03	759.83
P	1949879.06	1055729.35	759.75
Q	1949884.46	1055726.71	759.95
R	1949883.38	1055731.86	759.75
S	1949860.35	1055731.82	758.95

SUMMIT DR



MICHAEL COLLINS SCHOOL
SOUTHERN ENTRANCE

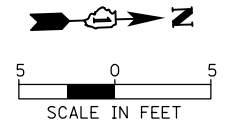
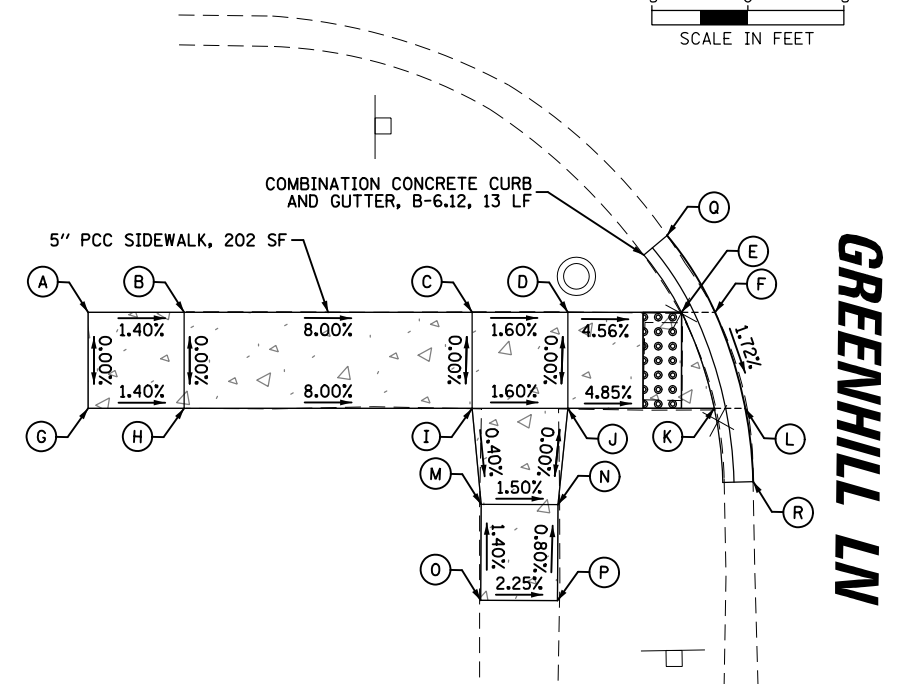
SUMMIT DR



GREENHILL LN

SE QUADRANT OF GREENHILL LN & SUMMIT DR			
POINT	NORTH	EAST	ELEV
A	1950254.08	1055744.73	772.20
B	1950258.88	1055746.12	772.13
C	1950273.28	1055750.31	770.93
D	1950278.09	1055751.71	770.85
E	1950283.77	1055753.36	770.58
F	1950285.44	1055753.85	770.60
G	1950252.68	1055749.53	772.20
H	1950257.48	1055750.92	772.13
I	1950271.89	1055755.11	770.93
J	1950276.69	1055756.51	770.85
K	1950284.01	1055758.64	770.48
L	1950285.55	1055759.09	770.51
M	1950270.95	1055760.05	770.91
N	1950274.78	1055761.19	770.85
O	1950269.53	1055764.85	770.98
P	1950273.37	1055765.98	770.89
Q	1950284.16	1055749.31	770.66
R	1950284.88	1055762.88	770.38

SUMMIT DR



GREENHILL LN

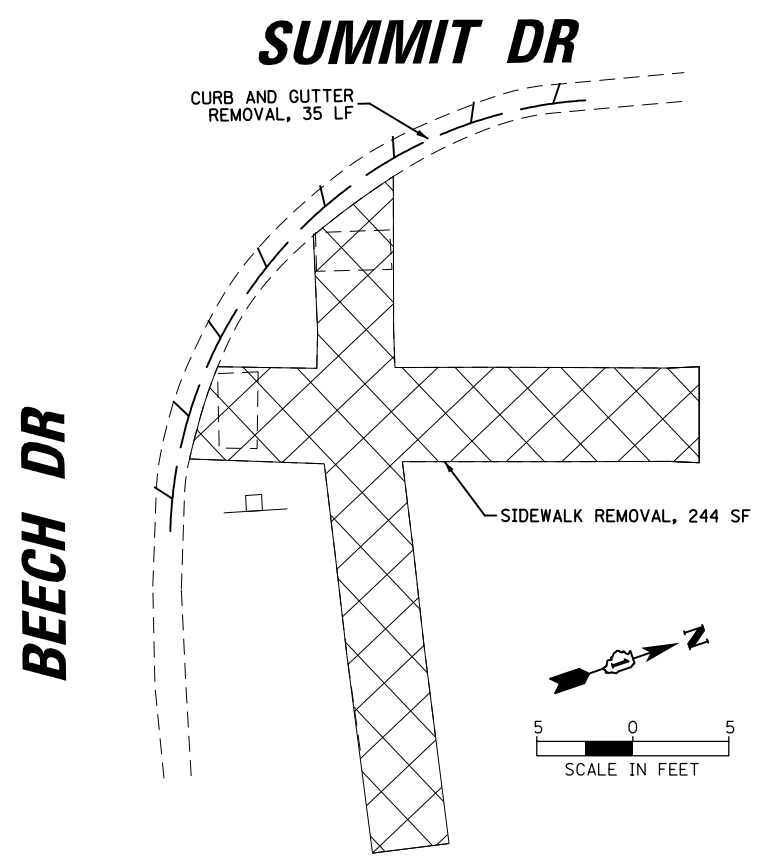
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\ADA_03.200227.dgn		DRAWN - PM	REVISED -
Default	PLOT SCALE = 10'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

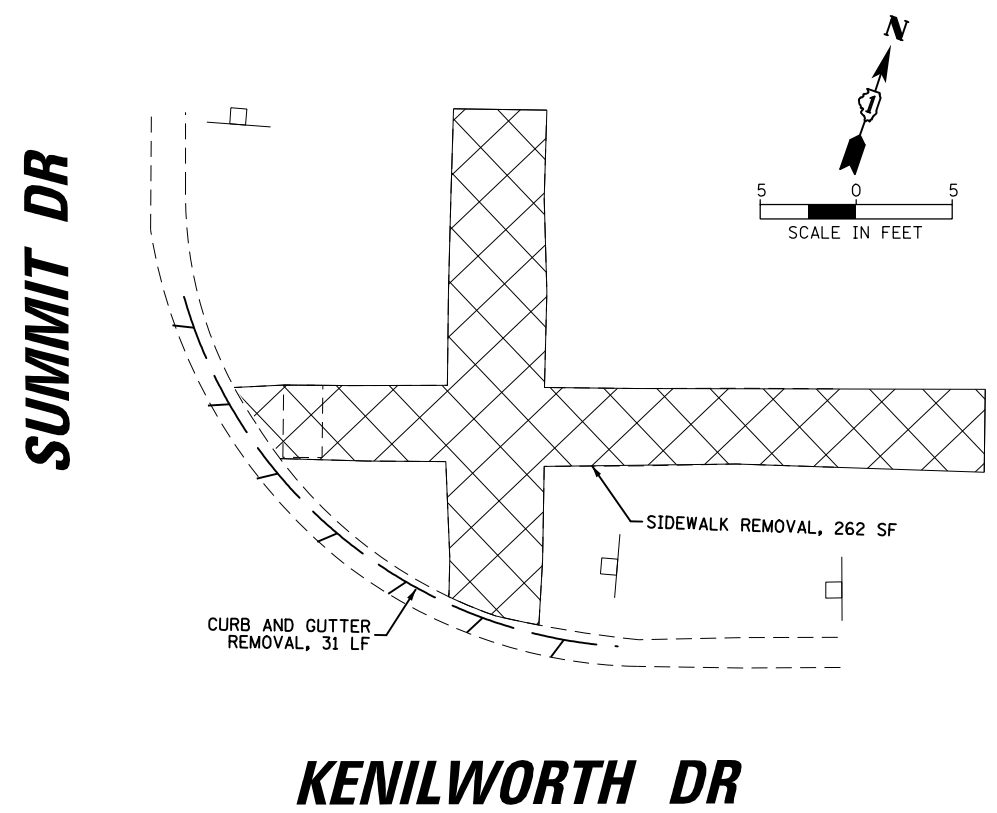
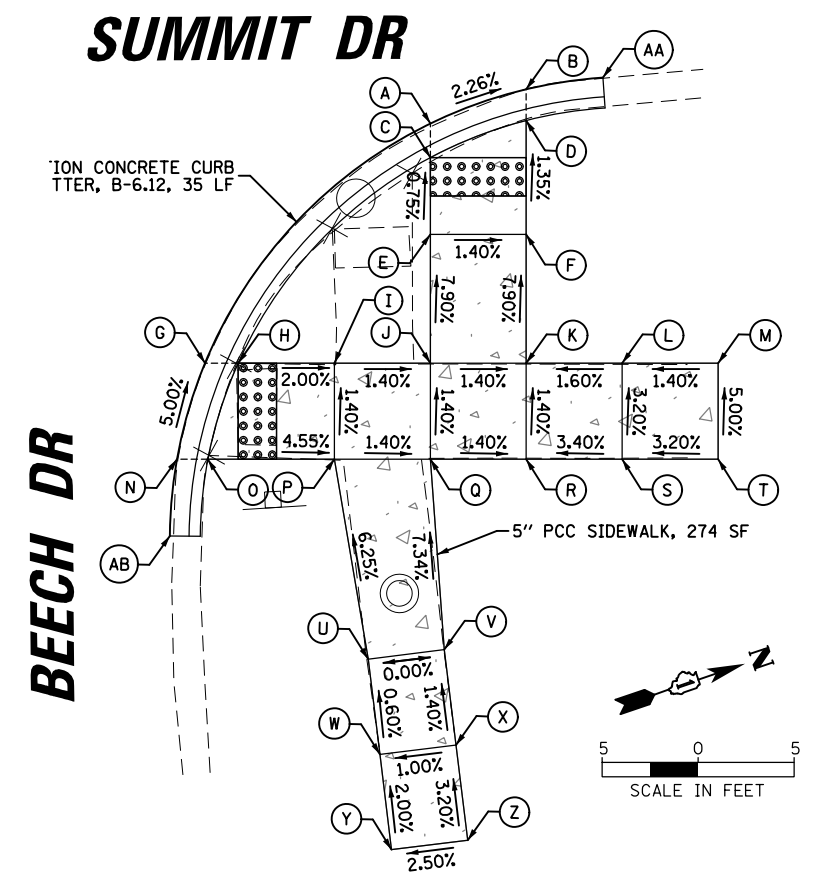
ADA RAMP GRADING PLAN
SUMMIT DR AND GREENHILL LN

SCALE: 10' SHEET 3 OF 5 SHEETS STA. TO STA.

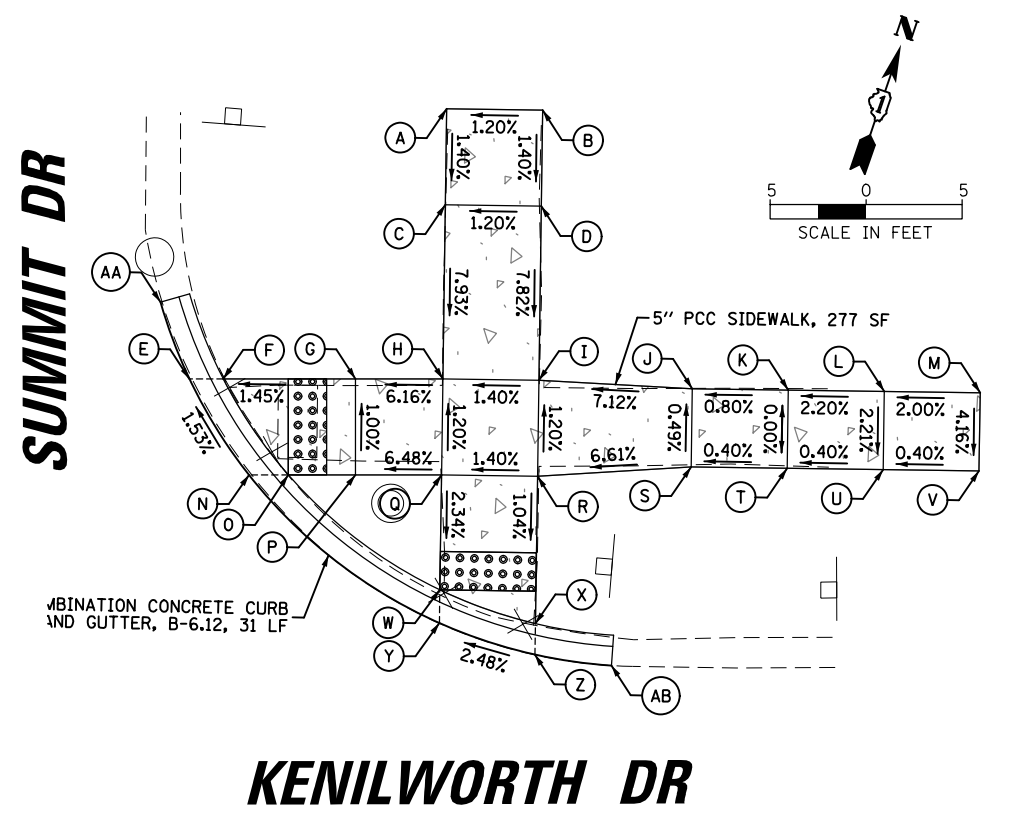
MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	41
CONTRACT NO. 61H16				
ILLINOIS FED. AID PROJECT				



NE QUADRANT OF BEECH DR & SUMMIT DR			
POINT	NORTH	EAST	ELEV
A	1950620.73	1055838.44	760.81
B	1950626.03	1055838.10	760.69
C	1950620.23	1055840.14	760.81
D	1950625.57	1055839.67	760.69
E	1950619.12	1055843.98	760.84
F	1950623.93	1055845.37	760.77
G	1950605.99	1055847.18	761.61
H	1950607.64	1055847.65	761.54
I	1950612.46	1055849.04	761.44
J	1950617.26	1055850.43	761.37
K	1950622.07	1055851.82	761.30
L	1950626.87	1055853.21	761.38
M	1950631.67	1055854.59	761.45
N	1950603.20	1055851.57	761.87
O	1950604.74	1055852.02	761.81
P	1950611.07	1055853.85	761.51
Q	1950615.87	1055855.24	761.44
R	1950620.68	1055856.62	761.37
S	1950625.48	1055858.01	761.54
T	1950630.29	1055859.40	761.70
U	1950609.88	1055864.34	762.17
V	1950613.83	1055864.98	762.17
W	1950609.08	1055869.28	762.20
X	1950613.03	1055869.91	762.24
Y	1950608.29	1055874.22	762.30
Z	1950612.24	1055874.85	762.40
AA	1950630.04	1055838.64	760.62
AB	1950601.73	1055855.31	761.97



NE QUADRANT OF KENILWORTH DR & SUMMIT DR			
POINT	NORTH	EAST	ELEV
A	1951225.40	1055889.53	748.07
B	1951226.63	1055894.37	748.13
C	1951220.56	1055890.76	748.00
D	1951221.79	1055895.60	748.06
E	1951208.33	1055880.24	746.93
F	1951208.79	1055881.94	746.90
G	1951210.58	1055888.60	747.00
H	1951211.76	1055892.99	747.28
I	1951212.99	1055897.84	747.35
J	1951214.65	1055905.67	747.92
K	1951215.88	1055910.51	747.96
L	1951217.11	1055915.36	748.07
M	1951218.35	1055920.21	748.17
N	1951204.31	1055884.53	747.02
O	1951204.84	1055886.52	747.00
P	1951205.75	1055889.90	747.05
Q	1951206.92	1055894.22	747.34
R	1951208.15	1055899.07	747.41
S	1951210.70	1055906.67	747.94
T	1951211.93	1055911.52	747.96
U	1951213.16	1055916.36	747.98
V	1951214.39	1055921.21	748.00
W	1951201.11	1055895.69	747.20
X	1951200.71	1055900.95	747.33
Y	1951199.44	1055896.12	747.25
Z	1951199.14	1055901.35	747.38
AA	1951211.80	1055877.74	746.87
AB	1951199.63	1055905.36	747.49



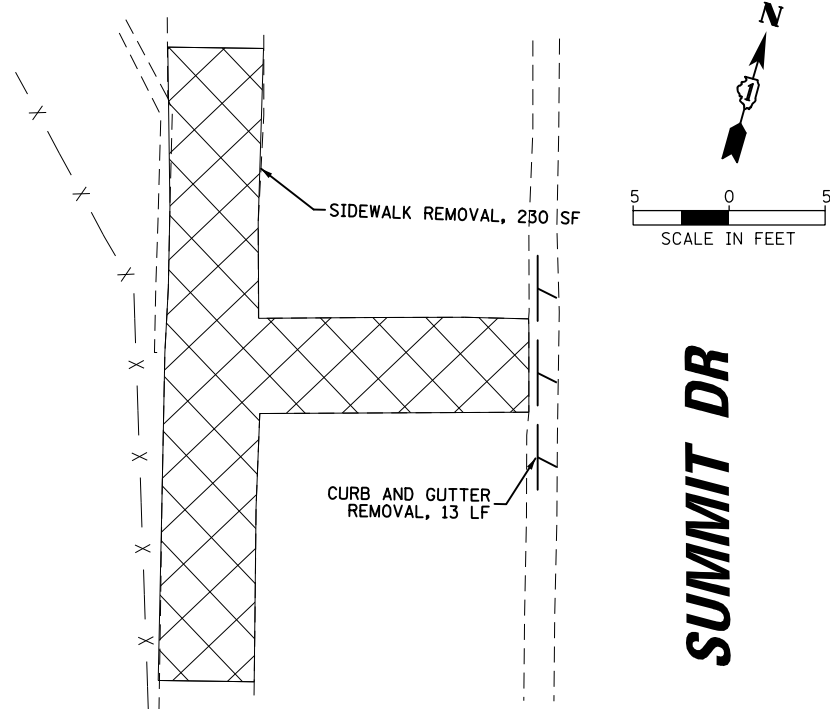
FILE NAME = N:\SCHAUMBURG\200227\Civi\ADA_04_200227.dgn	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
		DRAWN - PM	REVISED -
		CHECKED - GR	REVISED -
		DATE -	REVISED -
Default	PLOT DATE = 6/4/2021		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP GRADING PLAN
SUMMIT DR AND BEECH DR
SUMMIT DR AND KENILWORTH DR

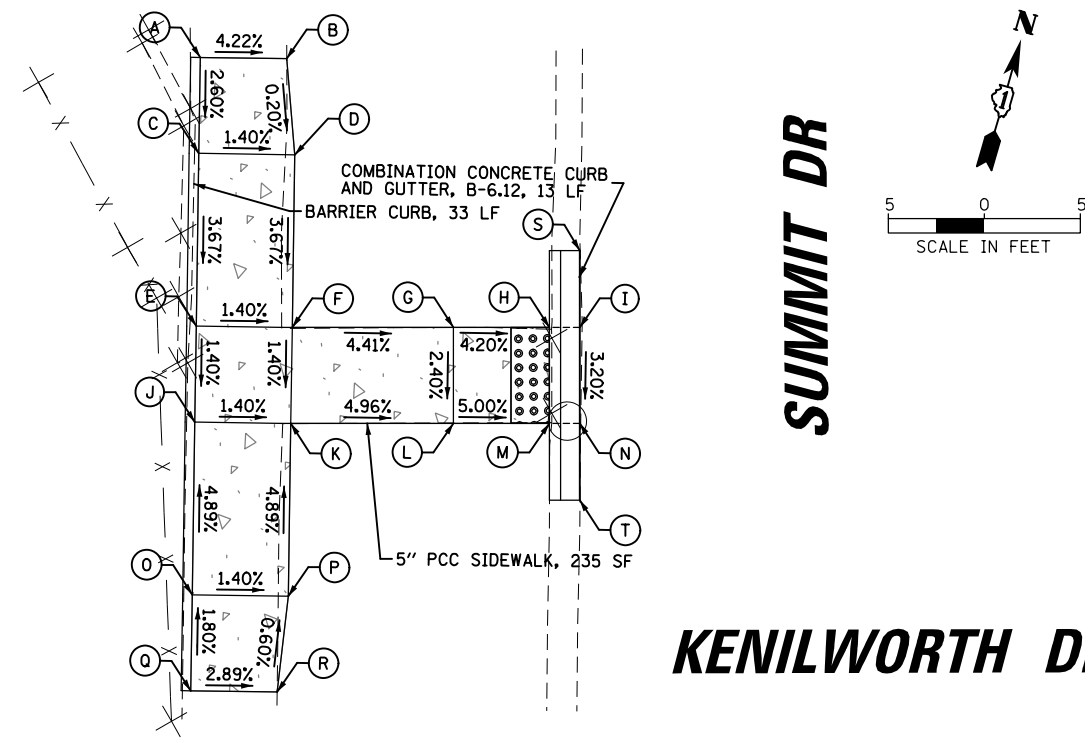
SCALE: 10' SHEET 4 OF 5 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	42
CONTRACT NO. 61H16				
ILLINOIS FED. AID PROJECT				

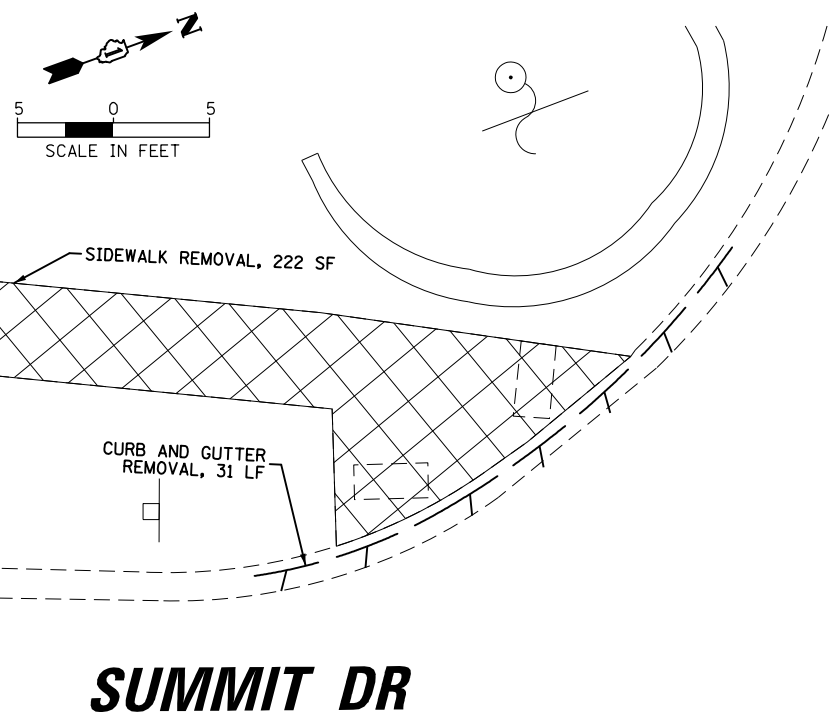


SUMMIT DR

W SIDE OF KENILWORTH DR & SUMMIT DR			
POINT	NORTH	EAST	ELEV
A	1951206.39	1055818.95	747.93
B	1951207.50	1055823.31	747.74
C	1951201.54	1055820.18	747.80
D	1951202.77	1055825.02	747.73
E	1951192.82	1055822.39	747.47
F	1951194.06	1055827.23	747.40
G	1951196.24	1055835.34	747.03
H	1951197.54	1055840.17	746.82
I	1951197.95	1055841.70	746.86
J	1951187.97	1055823.62	747.40
K	1951189.21	1055828.46	747.33
L	1951191.41	1055836.64	746.91
M	1951192.71	1055841.47	746.66
N	1951193.12	1055843.00	746.70
O	1951179.25	1055825.83	747.84
P	1951180.48	1055830.67	747.77
Q	1951174.40	1055827.06	747.93
R	1951175.51	1055831.42	747.80
S	1951201.81	1055840.66	746.80
T	1951189.26	1055844.04	746.76



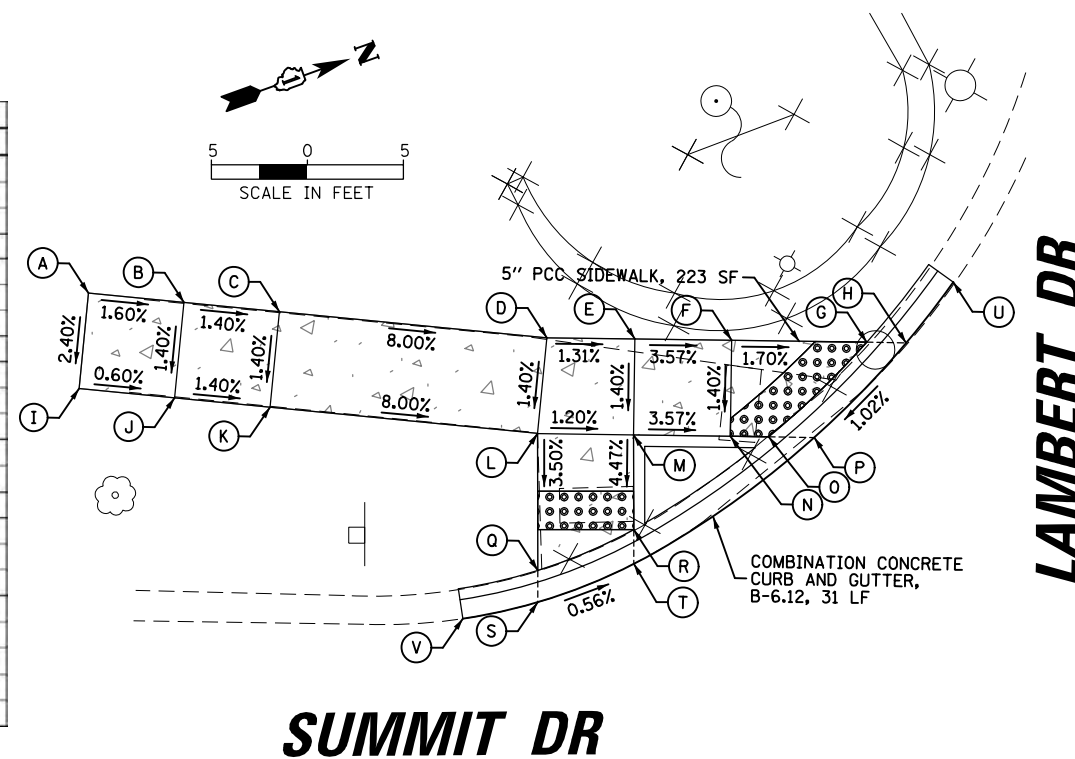
SUMMIT DR
KENILWORTH DR



SUMMIT DR

LAMBERT DR

SW QUADRANT OF LAMBERT DR & SUMMIT DR			
POINT	NORTH	EAST	ELEV
A	1952207.04	1055832.75	747.52
B	1952211.69	1055834.59	747.44
C	1952216.34	1055836.44	747.37
D	1952229.35	1055841.61	746.25
E	1952233.73	1055842.95	746.19
F	1952238.55	1055844.41	746.01
G	1952245.29	1055846.46	745.89
H	1952247.26	1055847.06	745.97
I	1952205.20	1055837.39	747.40
J	1952209.84	1055839.24	747.37
K	1952214.49	1055841.09	747.30
L	1952227.50	1055846.26	746.18
M	1952232.28	1055847.73	746.12
N	1952237.10	1055849.20	745.94
O	1952239.01	1055849.78	745.87
P	1952241.29	1055850.47	745.90
Q	1952225.53	1055853.13	745.93
R	1952230.92	1055852.46	745.90
S	1952225.07	1055854.71	745.97
T	1952230.43	1055854.18	745.94
U	1952250.50	1055844.70	746.10
V	1952221.08	1055854.46	746.01



SUMMIT DR

LAMBERT DR

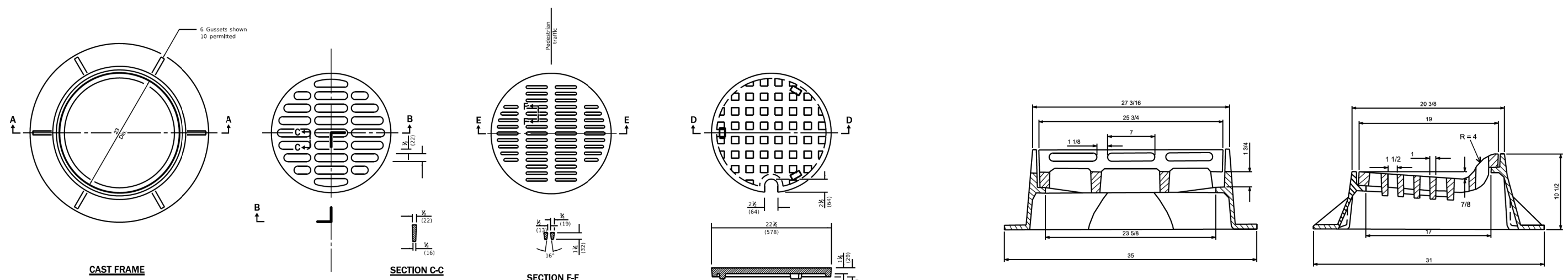
FILE NAME = N:\SCHAUMBURG\200227\Civil\ADA_05_200227.dgn	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
Default	PLOT SCALE = 1/8"	DRAWN - PM	REVISED -
	PLOT DATE = 6/4/2021	CHECKED - GR	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

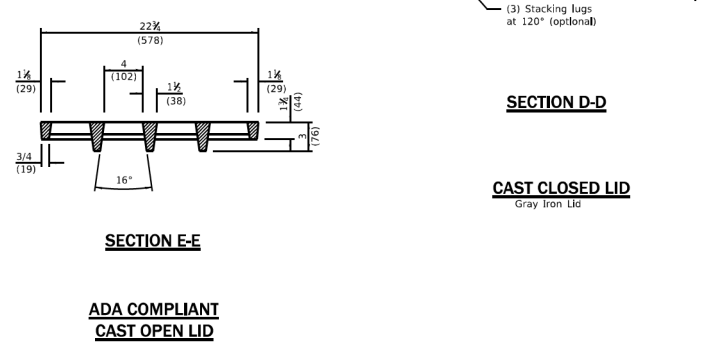
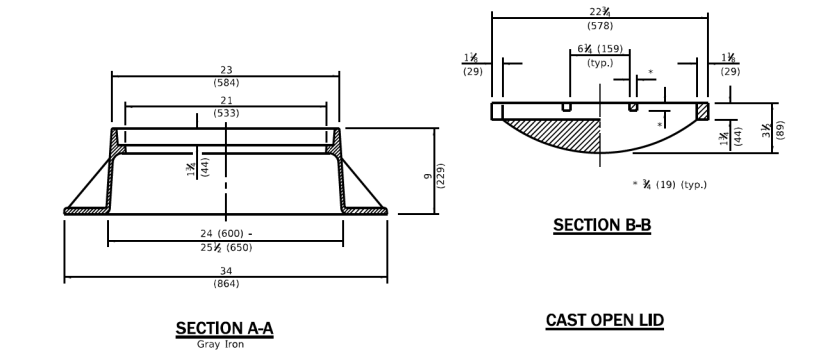
ADA RAMP GRADING PLAN
SUMMIT DR AND LAMBERT DR

SCALE: 10' SHEET 5 OF 5 SHEETS STA. TO STA.

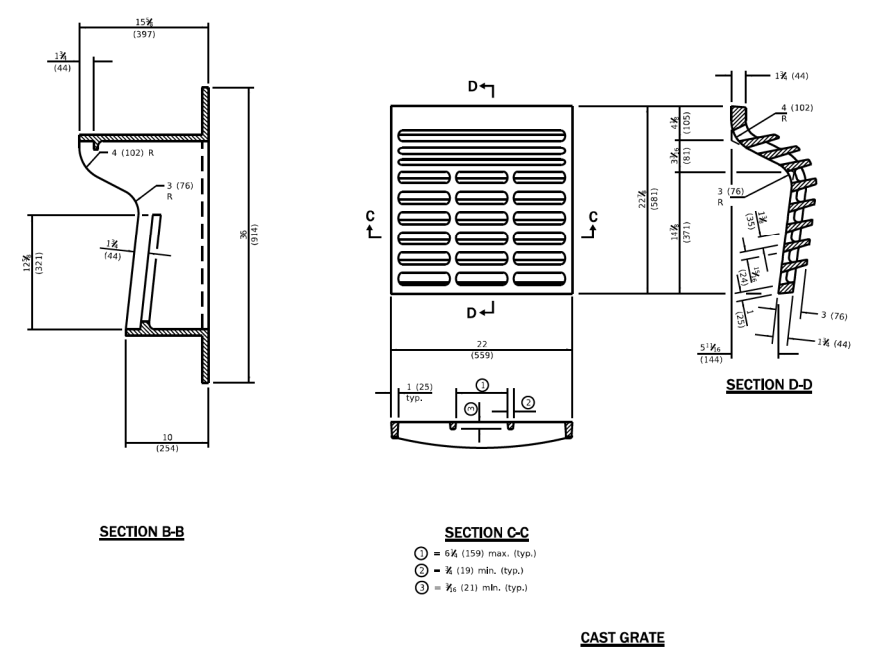
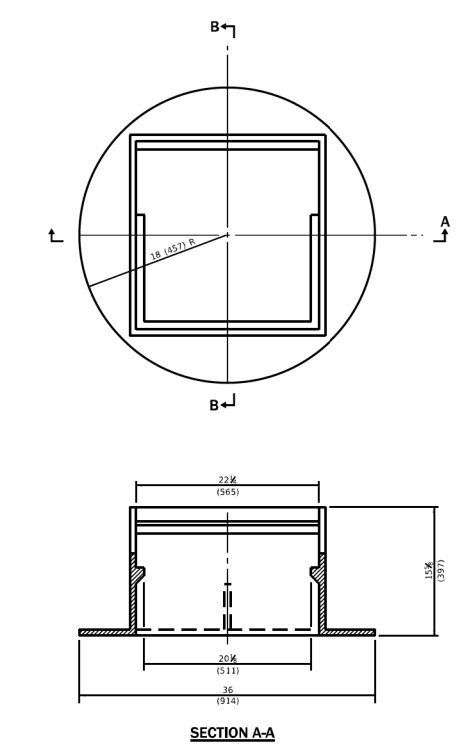
MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	43
CONTRACT NO. 61H16				
ILLINOIS FED. AID PROJECT				



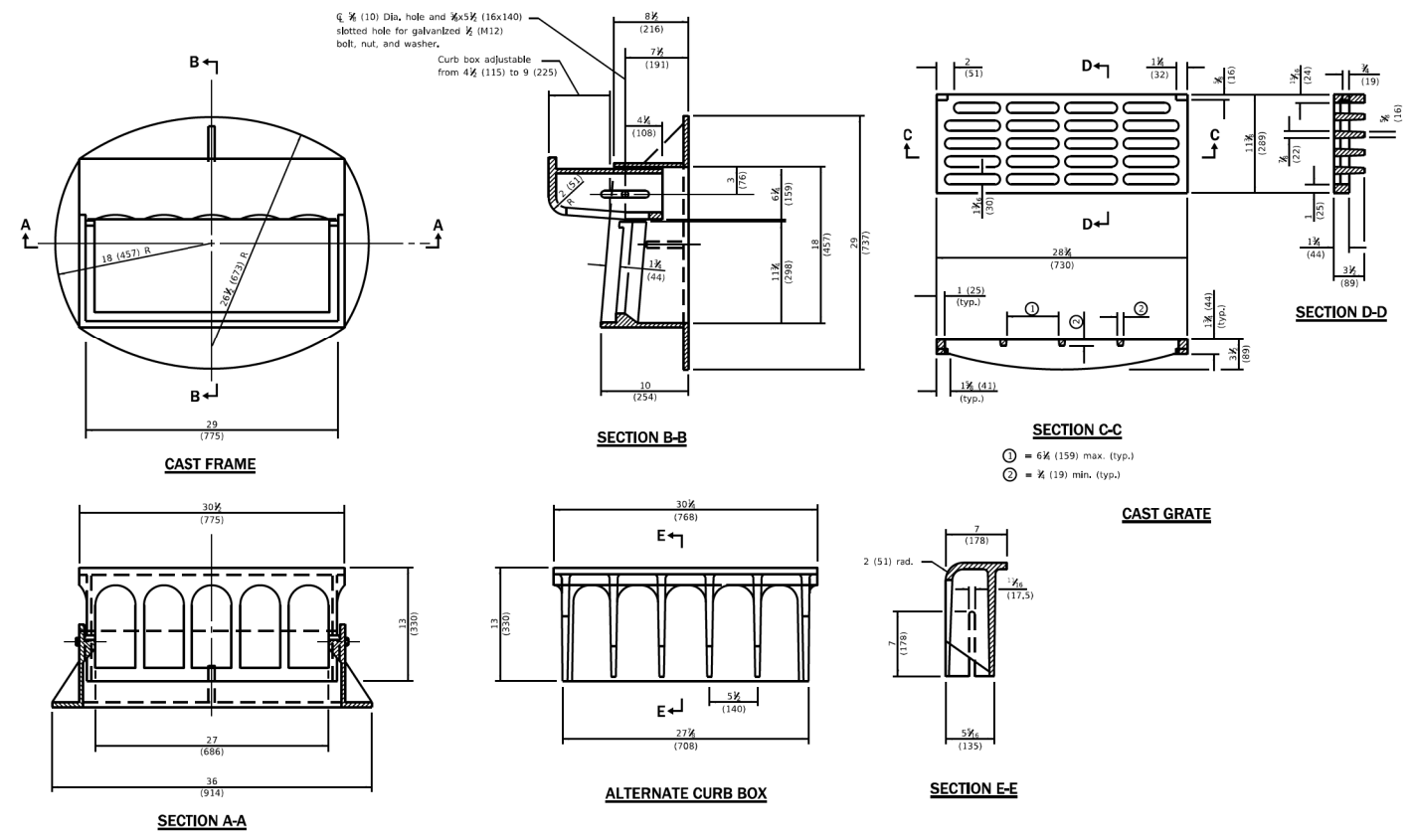
FRAME AND GRATE TYPE 6 (SPECIAL)
ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE SHOWN



FRAME AND LIDS TYPE 1
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

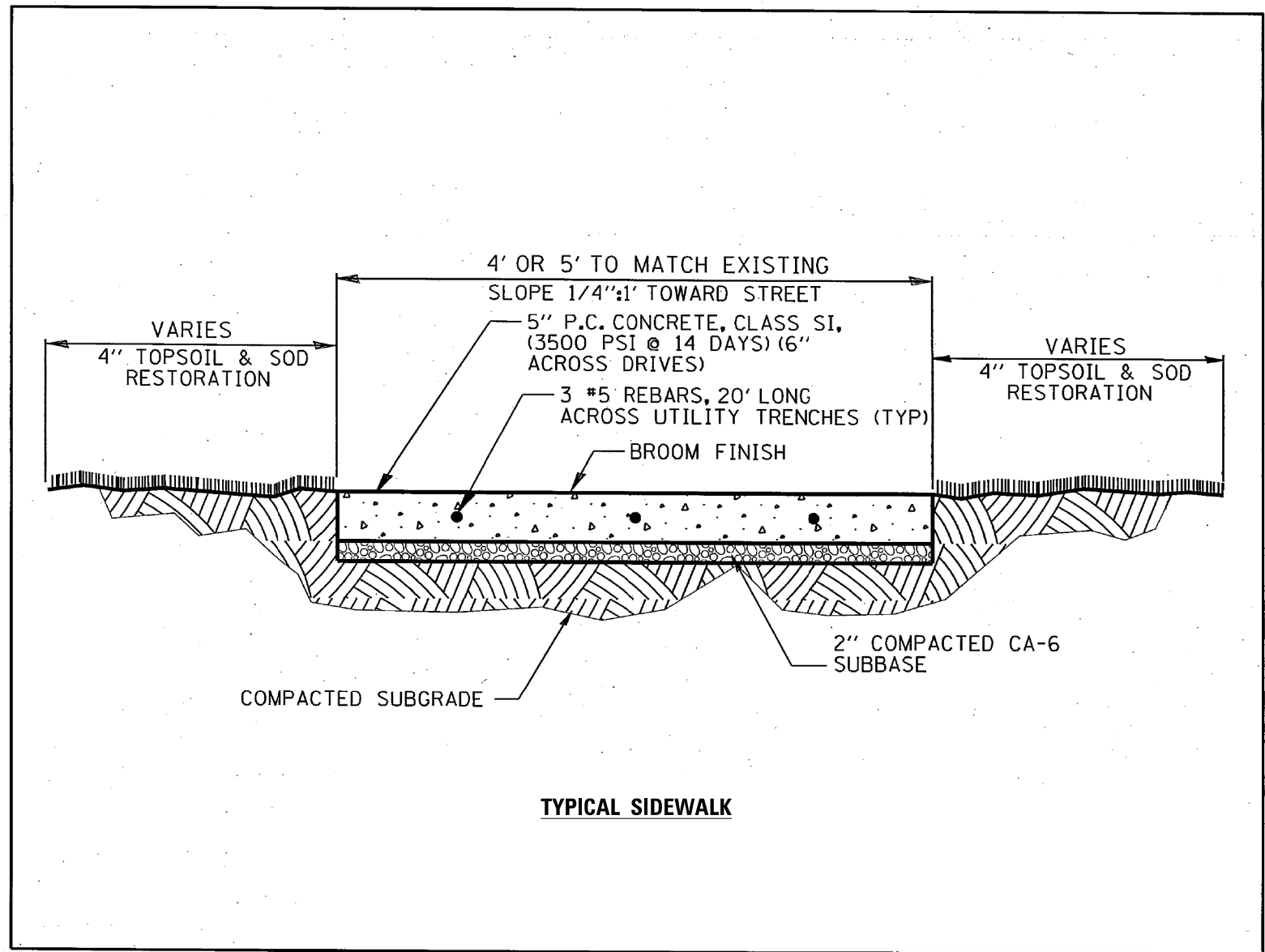


FRAME AND GRATE TYPE 6
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN



FRAME AND GRATE TYPE 11
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHAUMBURG CONSTRUCTION DETAILS	MUN R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\SCHAUMBURG\200227\Civil\DET_200227.dwg		DRAWN - PM	REVISED -			2134	20-00135-00-R5	COOK	53	44	
Default	PLOT SCALE = 10'	CHECKED - GR	REVISED -			CONTRACT NO. 61H16					
	PLOT DATE = 6/4/2021	DATE -	REVISED -			SCALE: 10'	SHEET 1 OF 2 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	



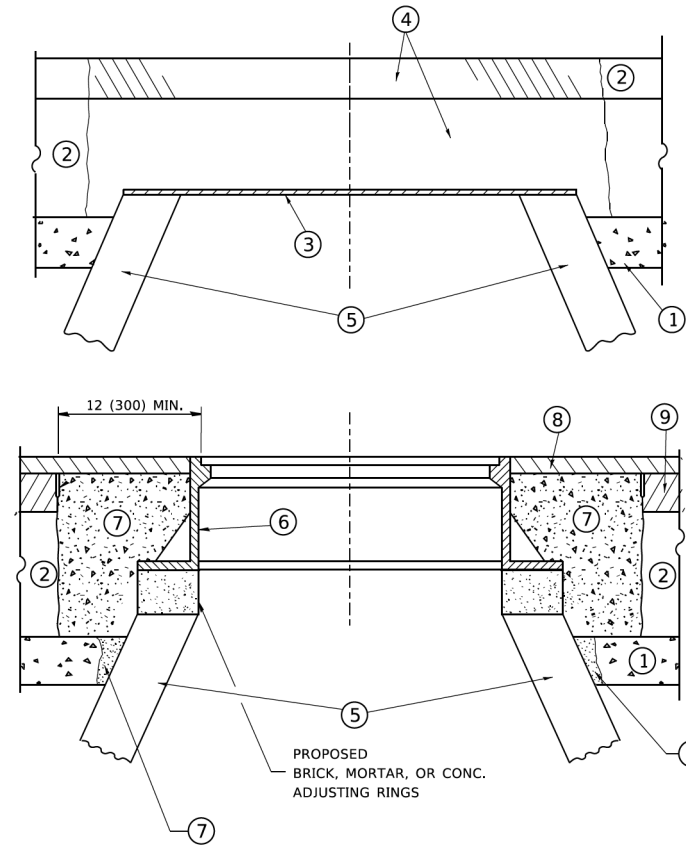
FILE NAME =	USER NAME = esidorozuk	DESIGNED - GR	REVISED -
N:\SCHAUMBURG\200227\Civil\DET_200227_02.sht		DRAWN - PM	REVISED -
Default	PLOT SCALE = 10'	CHECKED - GR	REVISED -
	PLOT DATE = 6/4/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF SCHAUMBURG
TYPICAL SIDEWALK**

SCALE: 10' SHEET 2 OF 2 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-R5	COOK	53	45
			CONTRACT NO. 61H16	
ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 * CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT *THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER.*

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1 *CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR *FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).*

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

MODEL: Default
FILE NAME: p:\uplancom\dot_illinois\gov\p\DOT\Documents\DOT_Offices\District_1\Projects\DHHS\022\31\CADD\ba\CAD\sheet\bd08.dgn

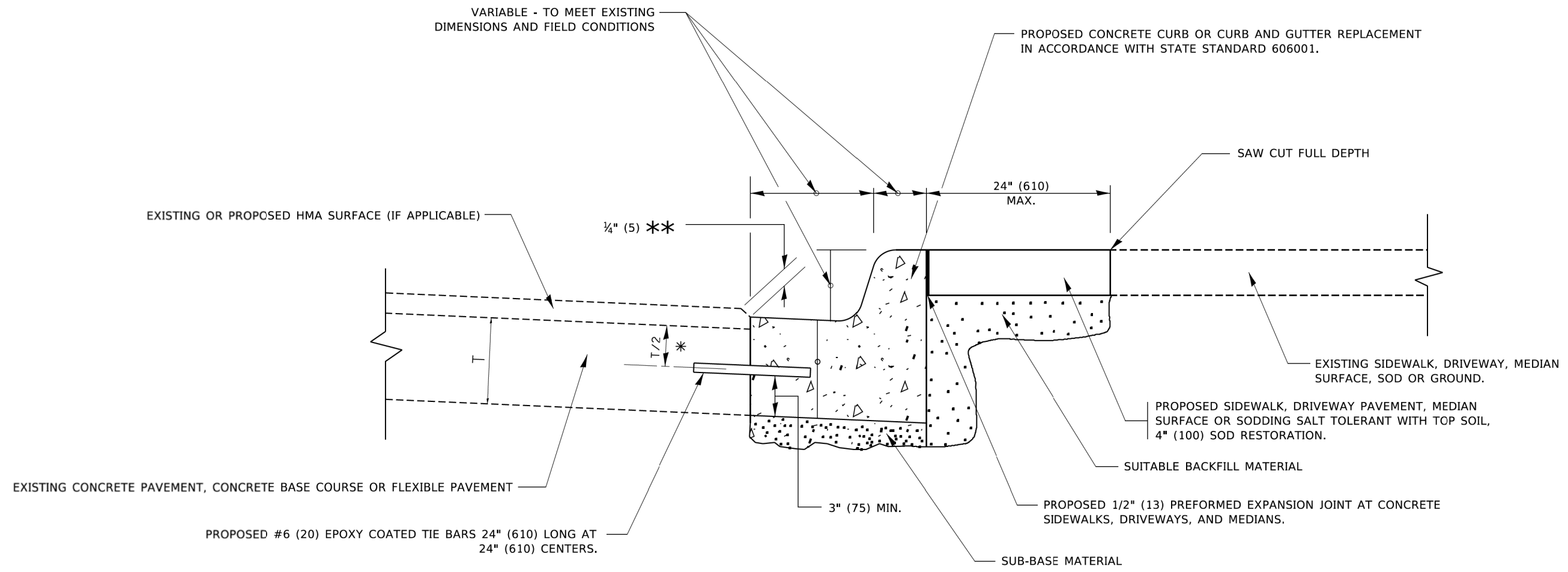
USER NAME = footemj	DESIGNED - R. SHAH	REVISED - R. WEDEMAN 05-14-04
	DRAWN -	REVISED - R. BORO 01-01-07
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - R. BORO 03-09-11
PLOT DATE = 3/27/2019	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-RS	COOK	53	46
BD600-03 (BD-8)		CONTRACT NO.	61H16	
ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
 FILE NAME: Proj\plancom.ecd.illinois.gov\PIV\DOT\Documents\DOT_Offices\District_1\Projects\DH\5422\31\CAD\Drawn\CD\sheet1b24.dgn

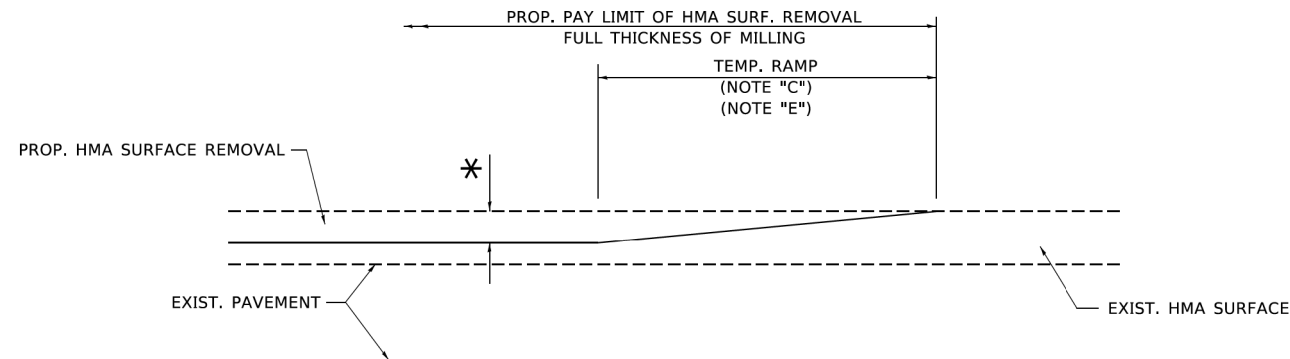
USER NAME = footemj	DESIGNED - A. HOUSEH	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 01-22-01
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - R. BORO 12-15-09
PLOT DATE = 7/11/2019	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

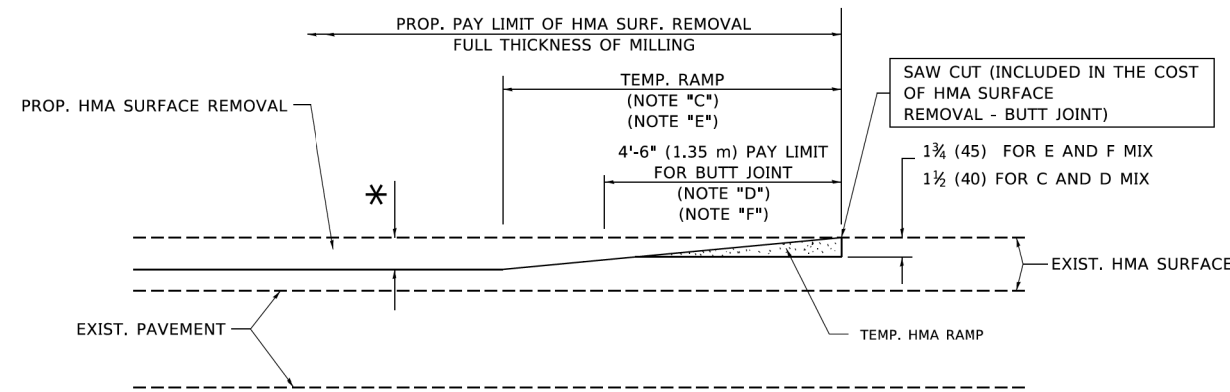
MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-RS	COOK	53	48
BD600-06 (BD-24)			CONTRACT NO. 61H16	
ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

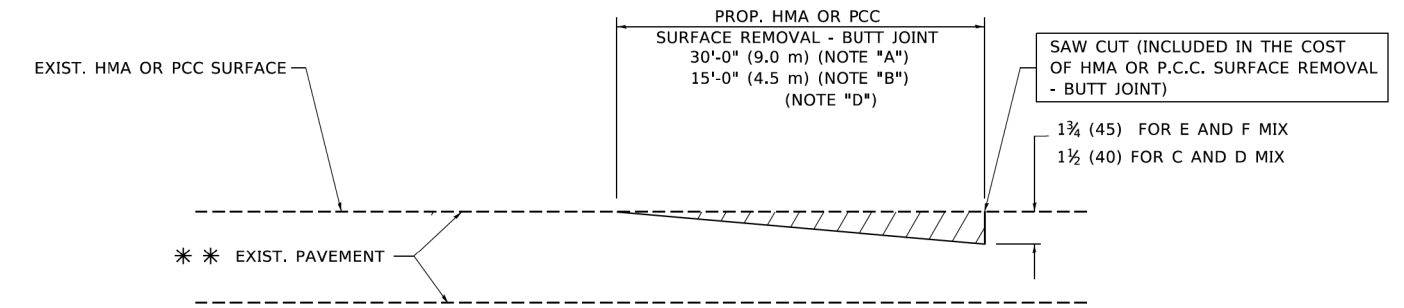


HMA CONSTRUCTED TEMPORARY RAMP

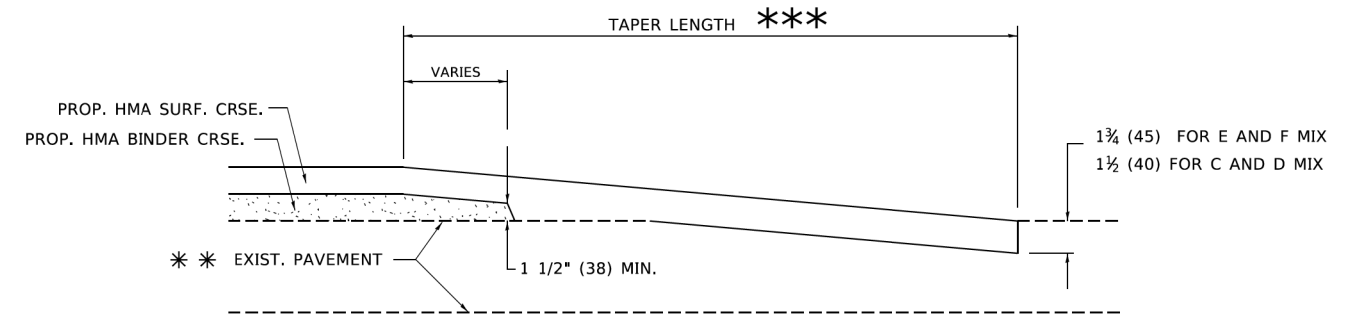
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

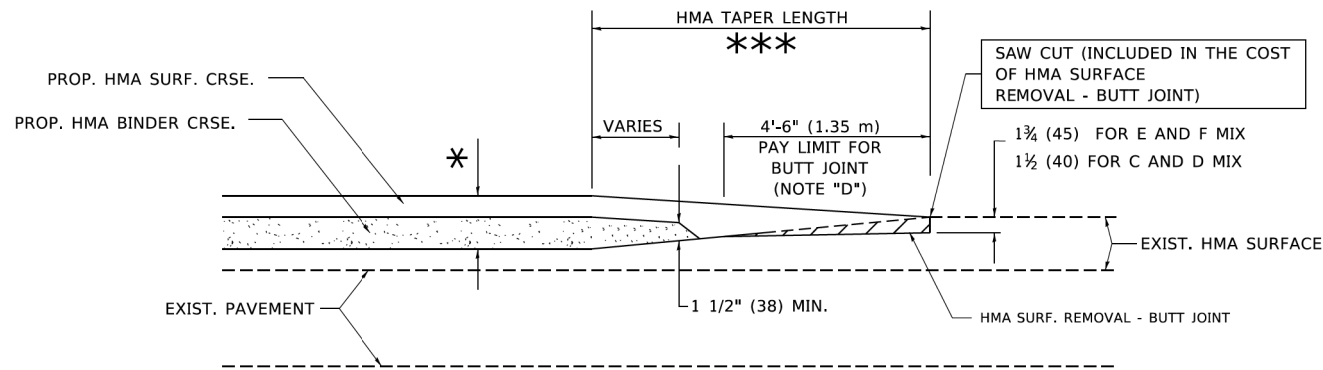
NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

MODEL: Default
FILE NAME: Proj\plancom.ecd\illinois.gov\PI\DOT\Documents\DOT_Offices\District_1\Projects\DH\5422\21\CAD\Drawn\CADsheets\B432.dgn

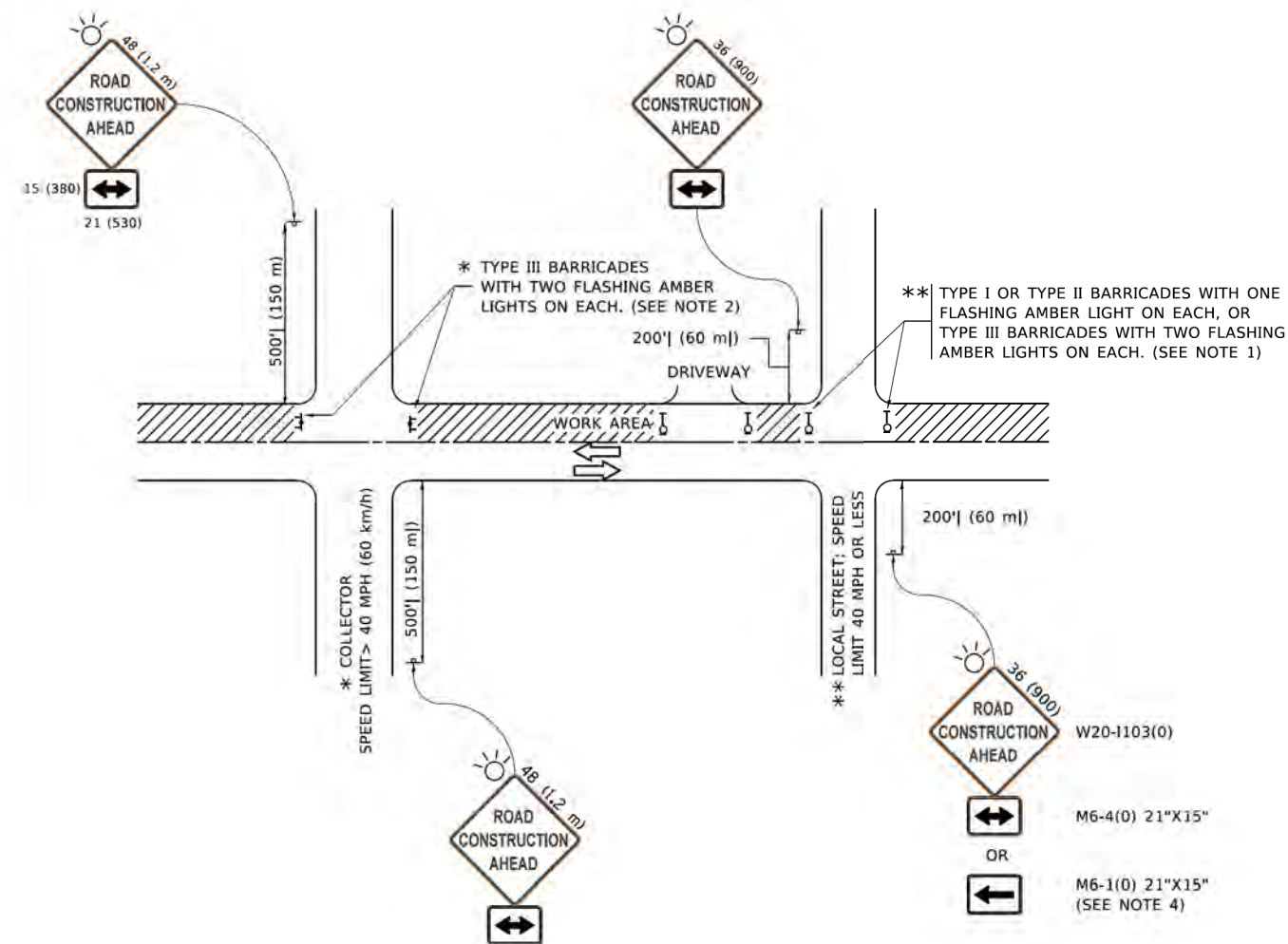
USER NAME = footemj	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
PLOT SCALE = 50.0000' / in.	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT DATE = 3/27/2019	CHECKED -	REVISED - M. GOMEZ 04-06-01
	DATE - 06-13-90	REVISED - R.BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-RS	COOK	53	49
BD400-05 BD32		CONTRACT NO.		61H16
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
 FILE NAME: P:\110848\BID\NTEG\Illinois.gov\PHIDOT\Documents\DOT Office\Drifct 1\Projects\010522\21CAD\Drawings\CAD\Sheet11C.dgn

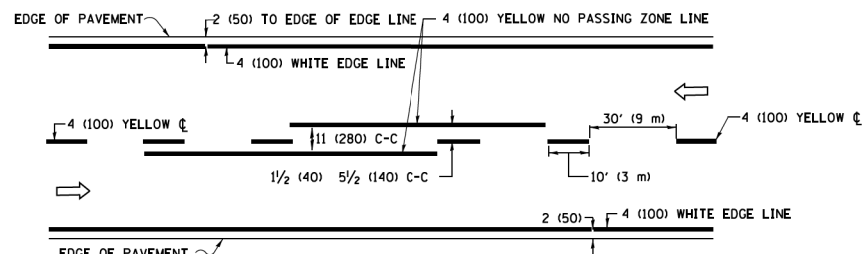
USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
PLOT DATE = 3/4/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

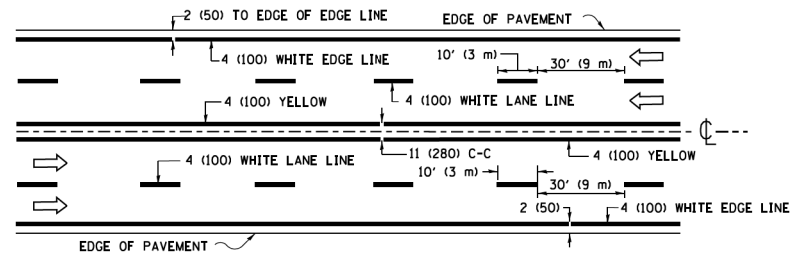
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

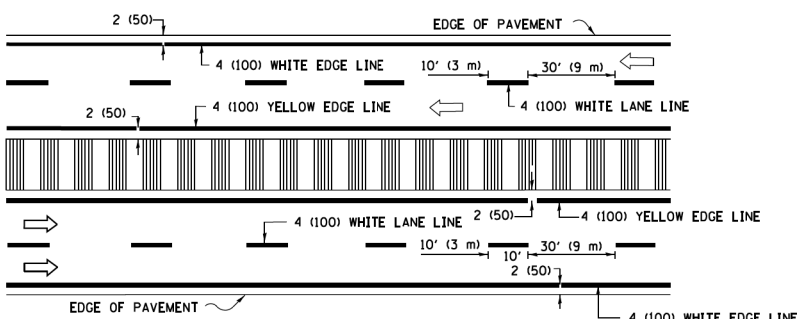
MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-RS	COOK	53	50
TC-10			CONTRACT NO. 61H16	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

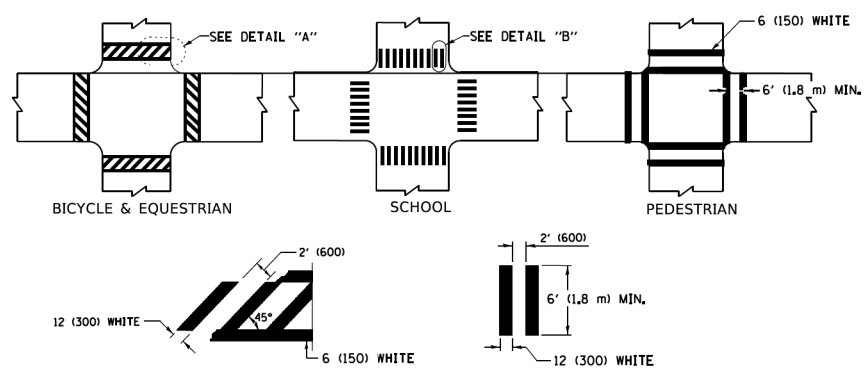


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

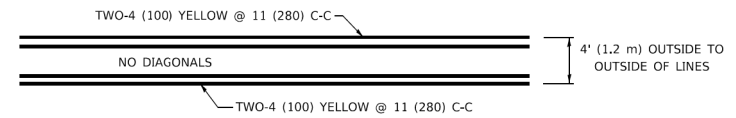
TYPICAL LANE AND EDGE LINE MARKING



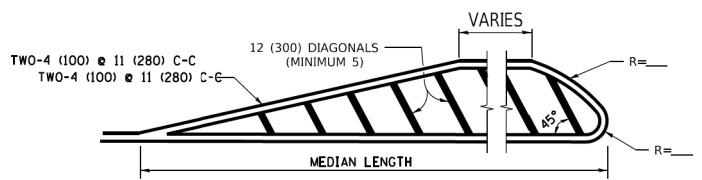
DETAIL "A" DETAIL "B"

TYPICAL CROSSWALK MARKING

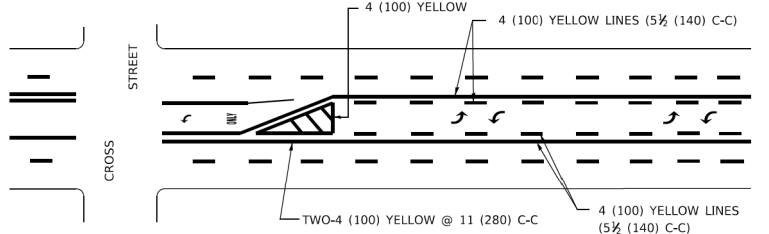
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



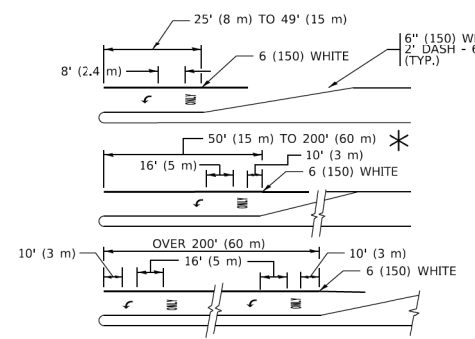
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE



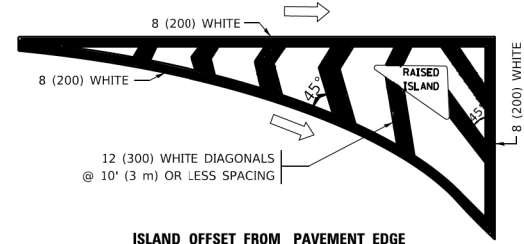
**MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING**



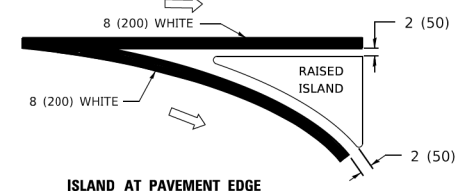
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

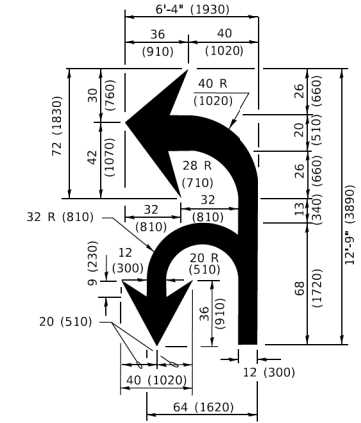


ISLAND OFFSET FROM PAVEMENT EDGE

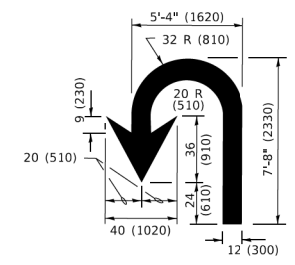


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" 15 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m ²) EACH *X*=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
FILE NAME: P:\110848\BID\NITEC\Illinois.gov\NIDOT\Documents\DOT Office\District 1\Projects\Dist1\22-23\CADD\Dist1\CD\Sheet\TC13.dwg

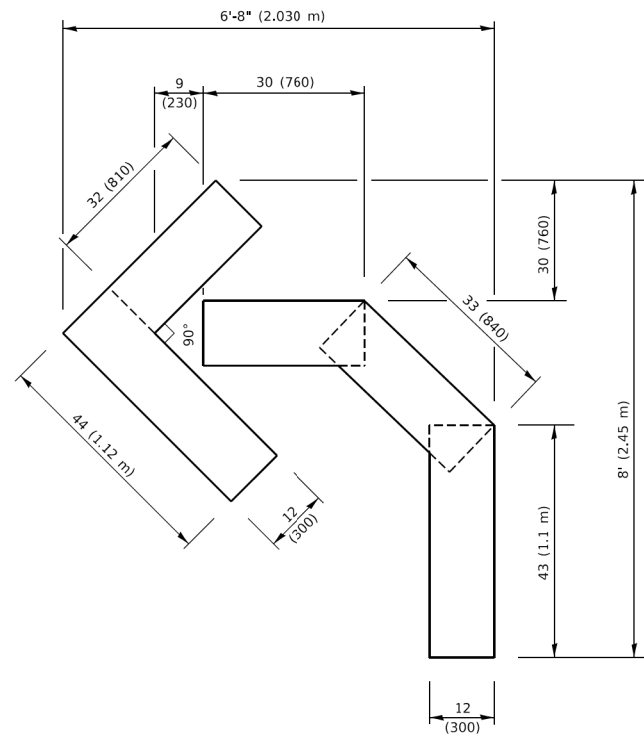
USER NAME = footemj	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
PLOT SCALE = 50.0000" / 1 in.	DRAWN -	REVISED - C. JUCIUS 07-01-13
PLOT DATE = 3/4/2019	CHECKED -	REVISED - C. JUCIUS 12-21-15
	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

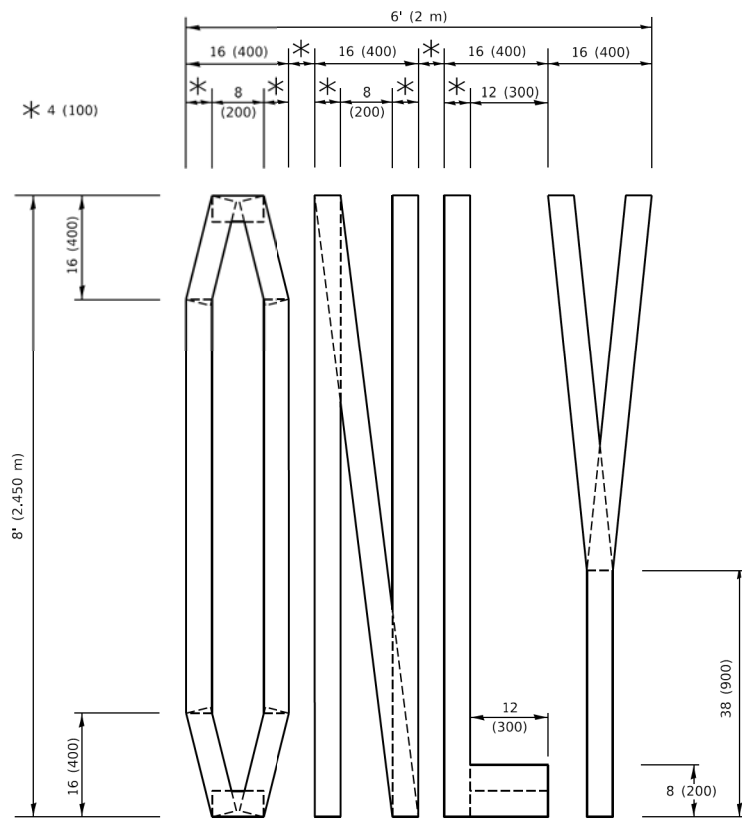
SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

MUN. RTE. 2134	SECTION 20-00135-00-RS	COUNTY COOK	TOTAL SHEETS 53	SHEET NO. 51
TC-13		CONTRACT NO. 61H16	ILLINOIS FED. AID PROJECT	



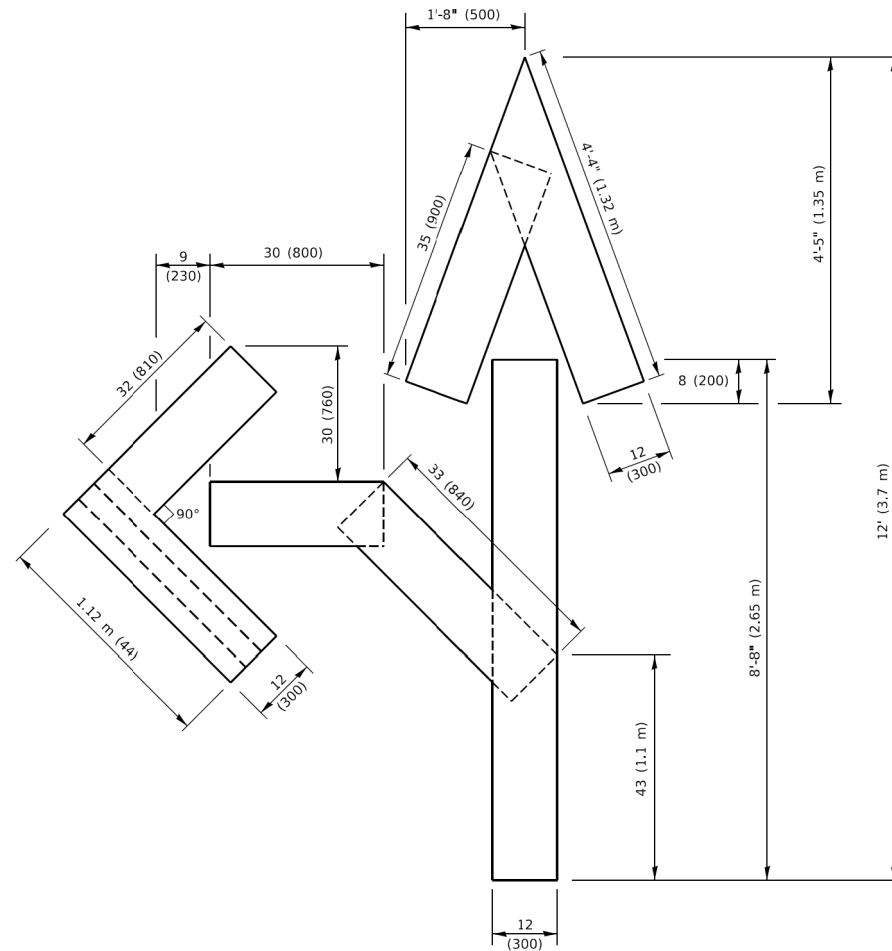
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

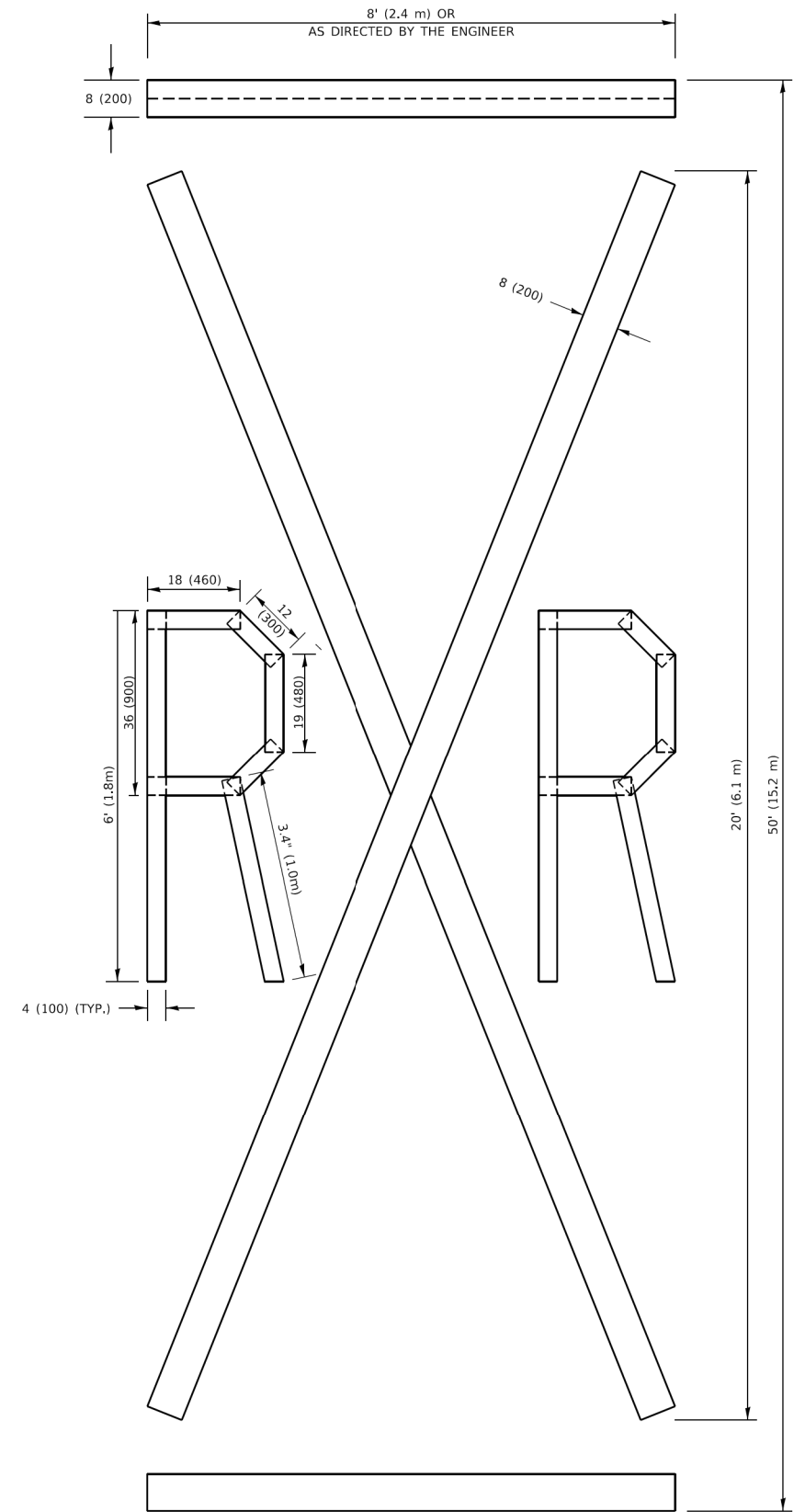


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
FILE: \\miller\c84\FEB\INTEG\Illinois.gov\PIV\DOT\Documents\DOT_Offices\Bartlett_1\Projects\018\54272-24\CADD\DATA\CAD\Sheets\12.16.dgn

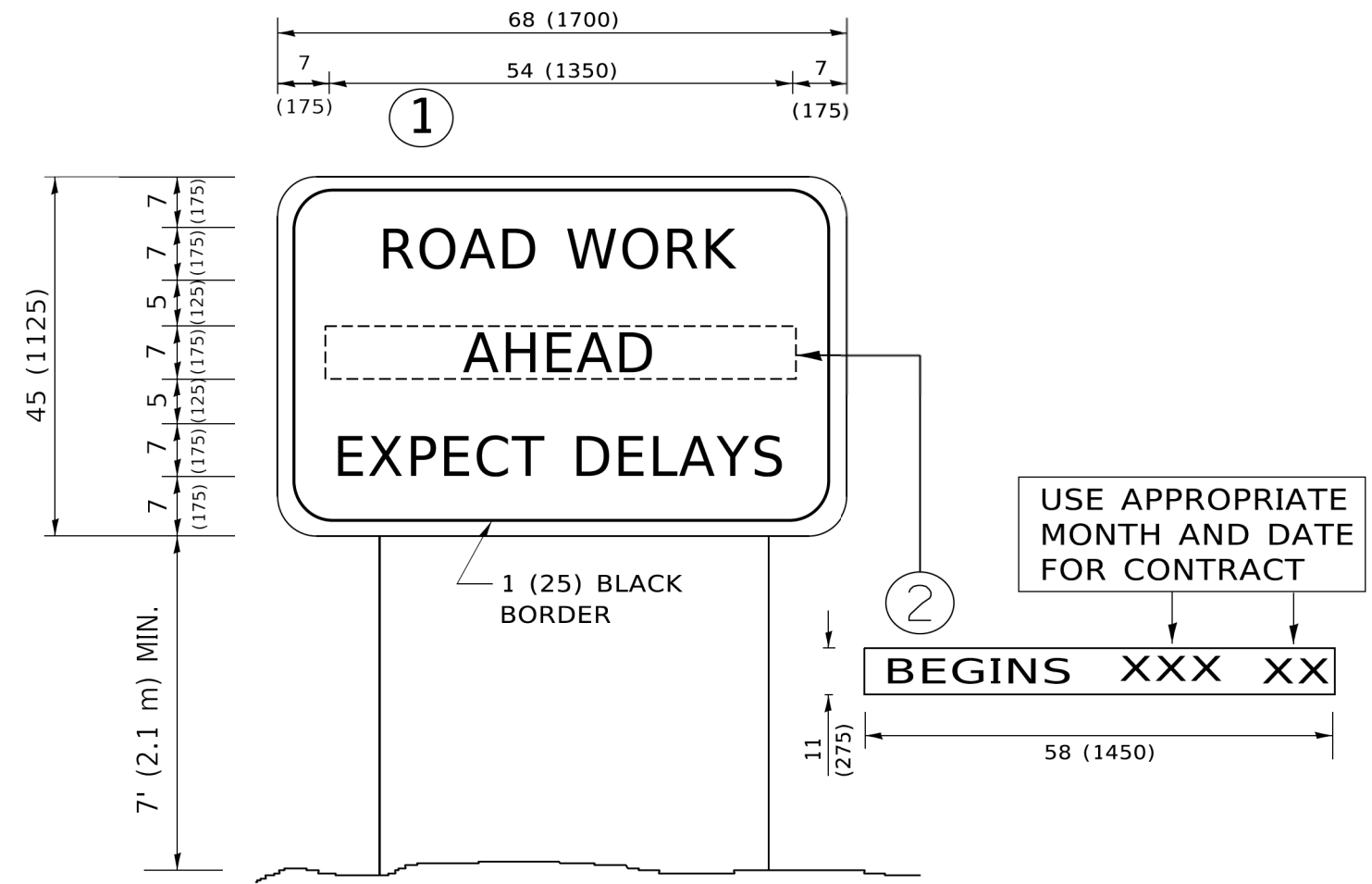
USER NAME = footemj	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
PLOT SCALE = 50.0068 ' / ft.	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 3/4/2019	CHECKED -	REVISED - E. GOMEZ 08-28-00
	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-RS	COOK	53	52
TC-16		CONTRACT NO. 61H16		
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
 FILE NAME: P:\1110848\BID\NTEG\Illinois.gov\PHIDOT\Documents\DOT Offices\District 1\Projects\DUHS22\21\CADD\Info\CAD\Sheet122.dgn

USER NAME = footemj	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2134	20-00135-00-RS	COOK	53	53
TC-22			CONTRACT NO. 61H16	
ILLINOIS FED. AID PROJECT				