# CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS

CONSTRUCTION PLANS
FOR
CHICAGO EXECUTIVE AIRPORT





J.U.L.I.E.
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS
www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL INMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL BE RESTORED TO SERVICES CAND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811

## TOWNSHIP: 42 NORTH WHEELING TOWNSHIP RANGE: 11 EAST (SECTION: 13) COOK COUNTY

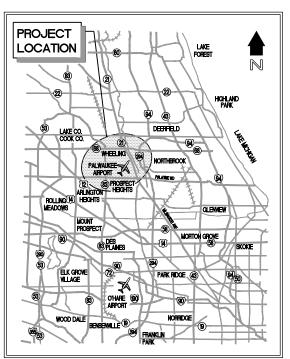
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Ш	KRIS SALVATERA, PE
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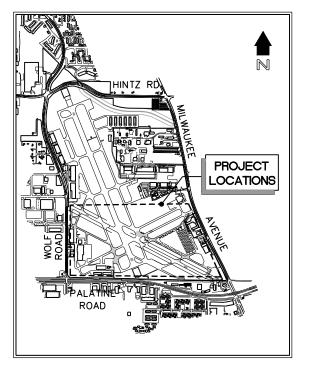
REHABILITATE AIRFIELD LIGHTING - PHASE 1
AND INSTALL ALCMS

ILLINOIS PROJECT: PWK-4843 S.B.G. PROJECT: 3-17-SBGP-TBD

JUNE 25, 2021



**LOCATION MAP** 



SITE PLAN



LICENSE EXPIRATION
DATE: 11/30/2021
DATE SIGNED: 06/24/21



LICENSE EXPIRATION
DATE: 11/30/2021
DATE SIGNED: 06/24/21

#### PROJECT INFORMATION

CONTRACTOR:
RESIDENT ENGINEER:
ORIGINAL CONTRACT AMOUNT:
FINAL CONSTRUCTION COST:
IDOT LETTING DATE:
IDOT AWARD DATE:
NOTICE TO PROCEED:
START OF CONSTRUCTION:
SUBSTANTIAL COMPLETION:

LOCAL AGENCY CONTACT INFORMATION

VILLAGE OF WHEELING - 847.459.2600

CITY OF PROSPECT HEIGHTS - 847.398.6070

ENGINEER'S PROJECT PERMIT LOG

NPDES #
FAA AIRSPACE #
CCDD LPC-663 DATED
MWRDGC PERMIT # 03-246 & RL 09-063
VILLAGE APP FOR CONSTRUCTION PERMIT #
VILLAGE FLOODPLAIN PERMIT #
CONTRACTOR'S REGISTRATION WITH VILLAGE
VILLAGE SITE ALTERATION PERMIT #
CITY APPLICATION FOR PERMIT #
CITY FLOODPLAIN PERMIT #
CITY SITE GRADING PERMIT #
CONTRACTOR'S REGISTRATION WITH CITY

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#### NOTES

1. SPECIAL ATTENTION IS NECESSARY WHEN WORKING NEAR FAA POWER AND CONTROL CABLES. ANY FAA UTILITY THAT IS DAMAGED OR CUT DURING CONSTRUCTION SHALL BE REPAIRED IMMEDIATELY. FAA REQUIRES THAT ANY DAMAGED CABLE BE REPLACED IN ITS ENTIRETY, FROM POWER/CONTROL SOURCE TO THE EQUIPMENT/SERVICE. SPLICES OF ANY KIND WILL NOT BE PERMITTED. EXPOSURES OF ANY FAA CABLES MUST BE DONE BY HAND DIGGING OR HYDRO-EXCAVATION. NO ADDITIONAL COMPENSATION WILL BE MADE FOR LOCATING, REPLACEMENT OR REPAIR OF FAA FACILITIES OR CABLES BUT, SHALL BE INCIDENTAL TO THE CONTRACT.

ADDITIVE ALTERNATE #1 - BEACON - ATCT MODIFICATIONS

- 2. WHEN FAA CABLES ARE REQUIRED TO BE LOCATED, OR THE CONTRACTOR IS PLANNING ON WORKING ON OR AROUND FAA CABLES, CONDUITS OR EQUIPMENT, A 10 WORKING DAY ADVANCED NOTICE SHALL BE GIVEN TO THE FAA BEFORE ANY SUCH MARKINGS ARE REQUIRED. ONCE FAA MARKS THE CABLES, THE CONTRACTOR WILL BE REQUIRED TO SURVEY THE FAA UTILITIES SO THEY CAN BE REPLACED DURING CONSTRUCTION WITHOUT REMARKING BY THE FAA. THIS SHALL BE INCIDENTAL TO THE CONTRACT. THE FAA PERSONNEL ARE ONLY AVAILABLE FROM 9 AM TO 3 PM, MONDAY THROUGH FRIDAY WITH ADVANCED NOTICE.
- 3. ALL ELEVATIONS SHOWN ON PLANS ARE IN 1929 DATUM. SUBTRACT 0.24 FEET FROM ELEVATIONS SHOWN TO OBTAIN 1988 NAVD.

#### SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR108108	1/C #8 5 KV UG CABLE	LF	29,850	
AR108960	REMOVE CABLE	LF	29,000	
AR109210	VAULT MODIFICATIONS	LS	1	
AR109331	15 KW REGULATOR, STYLE 1	EACH	2	
AR109361	30 KW REGULATOR, STYLE 1	EACH	1	
AR110012	2" DIRECTIONAL BORE	LF	400	
AR110202	2" PVC DUCT, DIRECT BURY	LF	5,800	
AR110900	REMOVE DUCT	LF	5,500	
AR115610	ELECTRICAL HANDHOLE	EACH	3	
AR125100	ELEVATED RETROREFLECTIVE MARKER	EA	12	
AR125416	MITL-BASE MOUNTED-LED	EACH	84	
AR125442	TAXI GUIDANCE SIGN, 2 CHARACTER	EACH	1	
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EACH	1	
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	1	
AR125565	SPLICE CAN	EACH	1	
AR125901	REMOVE STAKE MOUNTED LIGHT	EACH	77	
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	2	
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	3	
AR125912	REMOVE RETROREFLECTIVE MARKER	EACH	2	
AR125922	REPLACE BASE MOUNTED LIGHT	EACH	242	
AR125962	RELOCATE BASE MOUNTED LIGHT	EACH	4	
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR150520	MOBILIZATION	LS	1	
AR156520	INLET PROTECTION	EACH	16	
AR401910	REMOVE & REPLACE BIT. PAVEMENT	SY	11	
AR800085	RETROFIT EXIST EDGE LIGHT OR GUIDANCE SIGN	EACH	49	
AR800178	FIBER OPTIC CABLE	LF	300	
AR800192	INSTALL ALCMS L-890	LS	1	
AR800816	L-804 RGL ELEVATED, BASE MOUNTED	EACH	2	
ADDITIVE AT	   TERNATE #1 - RELOCATE BEACON			
ADDITIVE AL	ILDINALE #1-NELOCATE BEACON		ESTIMATED	RECORD
ITEM NO.	DESCRIPTION	UNIT	QUANTITY	QUANTITY
AS101510	AIRPORT ROTATING BEACON	EACH	1	
AS101900	BEACON REMOVAL	EACH	1	

#### MUNICIPALITIES GENERAL NOTES

- THE CHICAGO EXECUTIVE AIRPORT IS A JOINT OWNERSHIP BY BOTH THE VILLAGE OF WHEELING AND CITY OF PROSPECT HEIGHTS. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH VILLAGE AND CITY CODES, ORDINANCES AND STANDARDS AS APPLICABLE.
- 2. ALL CONTRACTORS AND SUBCONTRACTORS SHALL BE REGISTERED WITH THE VILLAGE AND CITY PRIOR TO THE NOTICE TO PROCEED. ALL REGISTRATION FEES SHALL BE INCIDENTAL TO THE CONTRACT.
- 3. THE CONTRACTOR SHALL WORK WITH THE AIRPORT AND ENGINEER TO SECURE THE REQUIRED VILLAGE AND CITY LOCAL CONSTRUCTION PERMITS PRIOR TO THE NOTICE TO PROCEED.
- 4. THE CONTRACTOR SHALL COORDINATE WITH THE VILLAGE AND CITY AT THE WEEKLY PROGRESS MEETINGS AND SHALL NOTIFY THE CITY OF PROSPECT HEIGHTS (847.398.6700) AND THE VILLAGE OF WHEELING (847.459.2600) A MINIMUM OF 48 HOURS PRIOR TO ANY REQUIRED VILLAGE/CITY INSPECTIONS.
- 5. ALL STORM SEWERS AND SANITARY SEWERS ON THE AIRPORT SITE ARE OWNED, OPERATED AND MAINTAINED BY THE CHICAGO EXECUTIVE AIRPORT UNLESS LABELED OTHERWISE.

IL. CONTRACT: **PA063**IL. LETTING ITEM: **08A**IL. PROJECT: **PWK-4843**S.B.G. PROJECT:

SURVEY BOOK #

REVISIONS				
NUMBER	BY	DATE		

THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).

ALCMS

CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL

AT FULL SCALE (34X22).

SHEETS, SUMMARY OF QUANTITIES, AND GENERAL NOTES

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CAIRPORT

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DRAWN BY: JRO

CHECKED BY: KWS

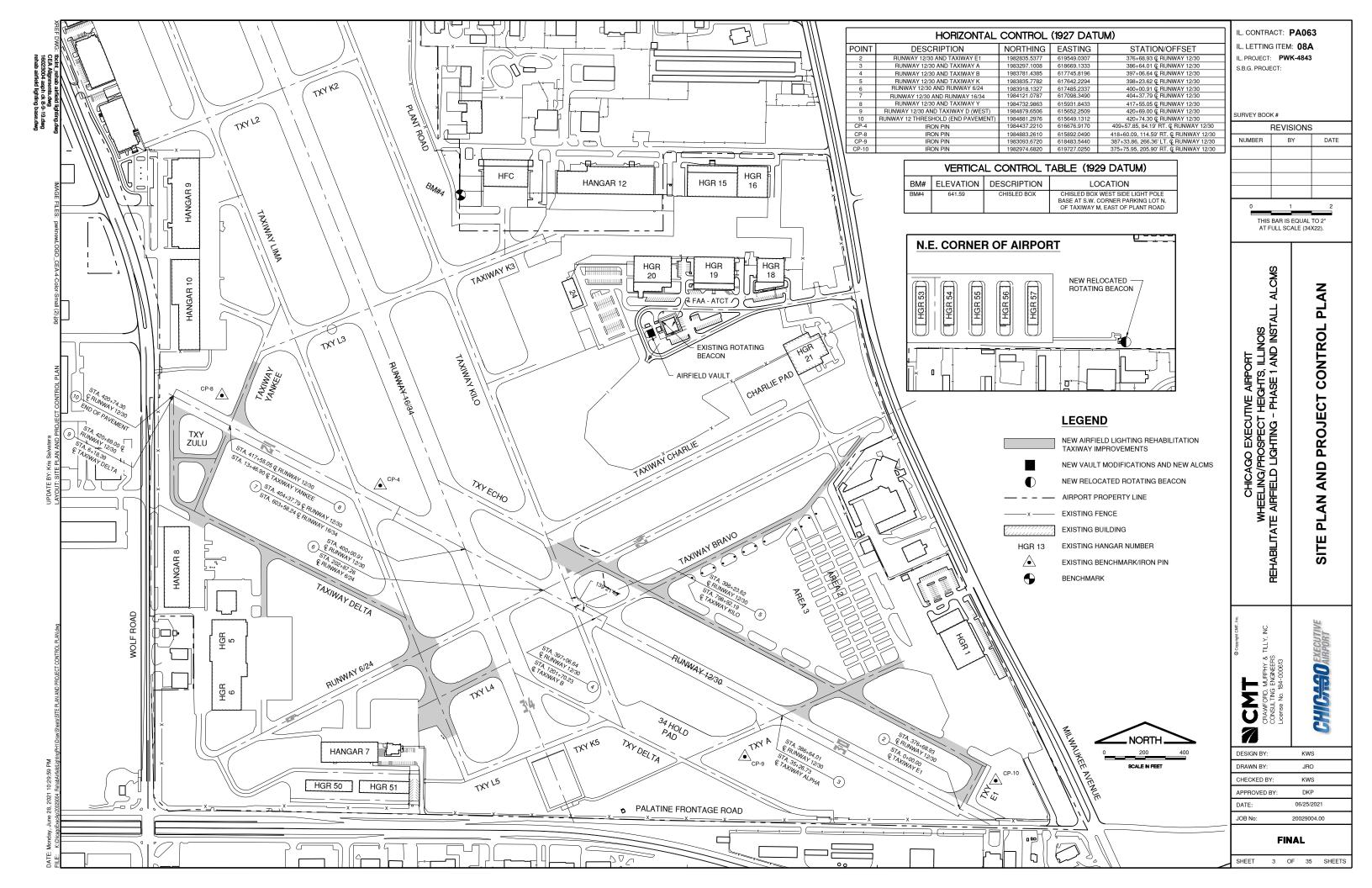
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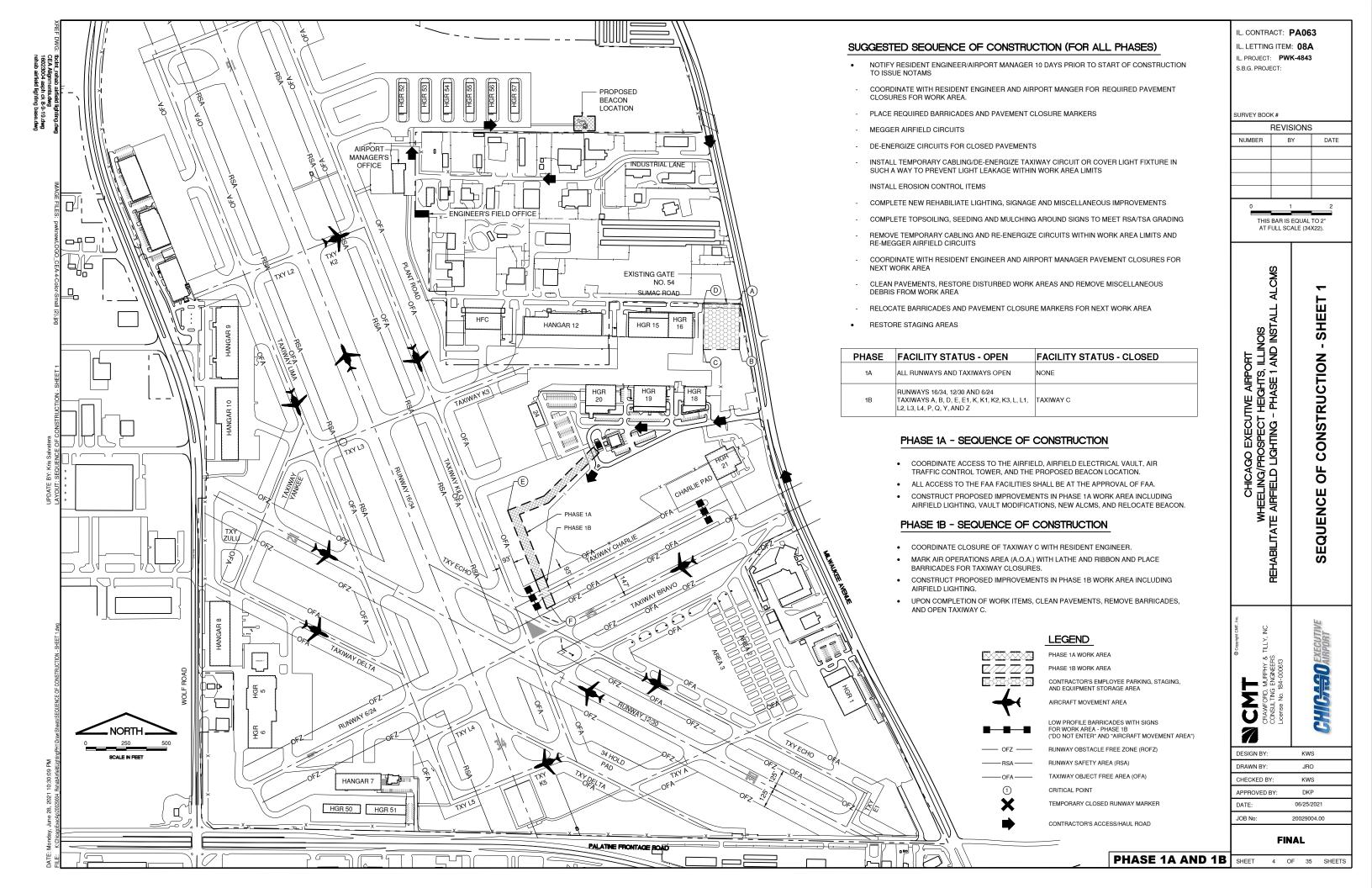
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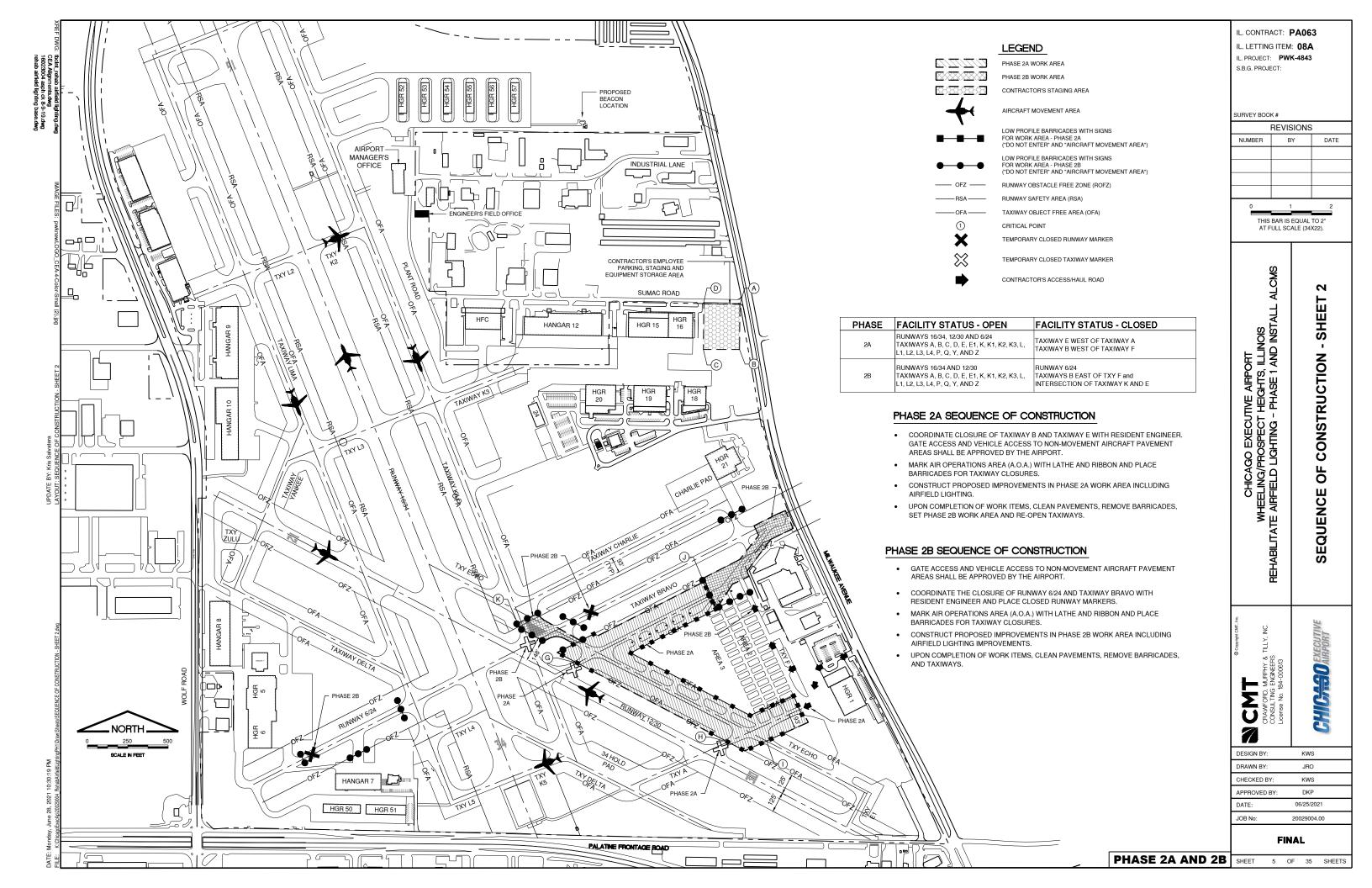
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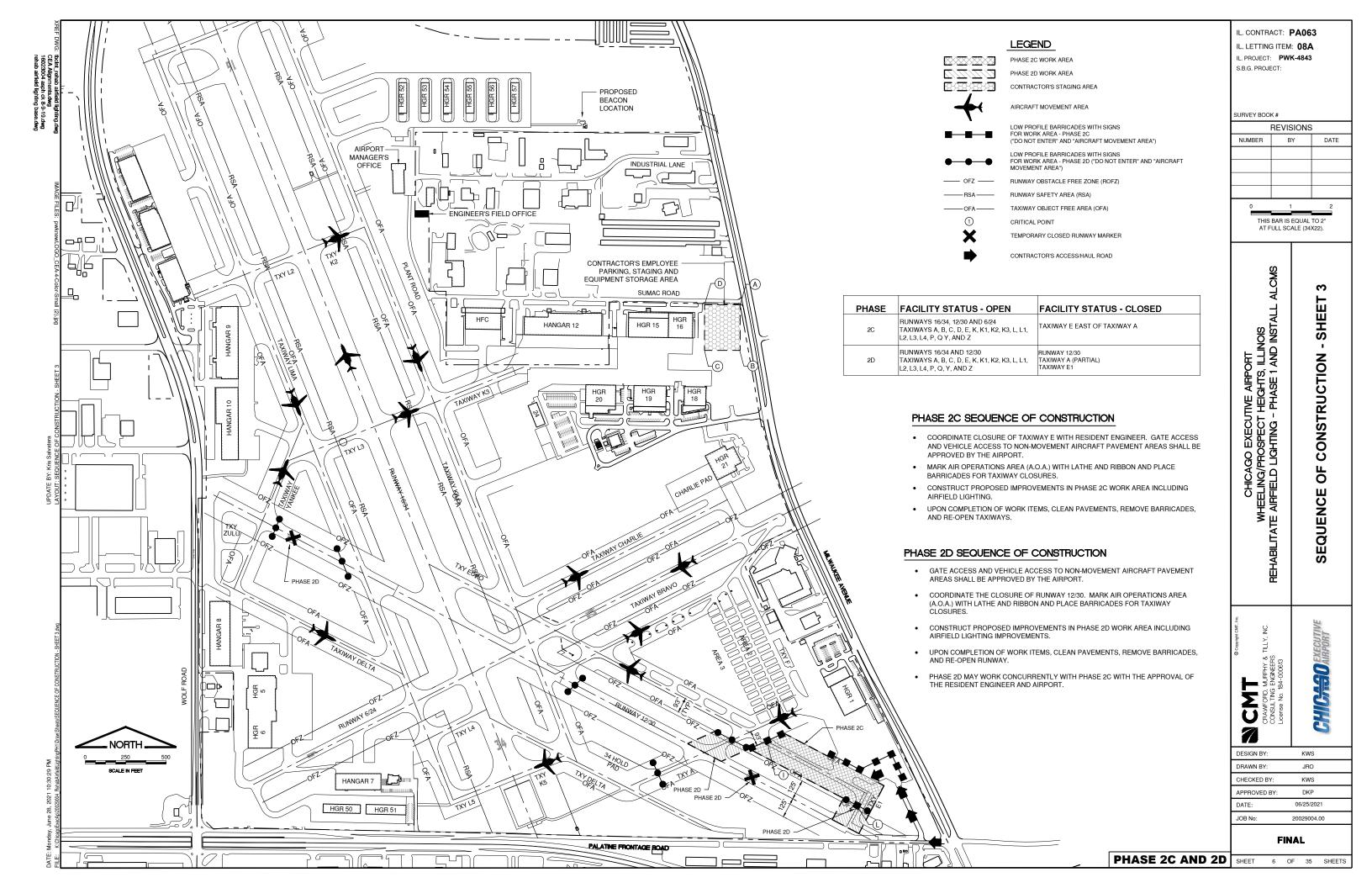
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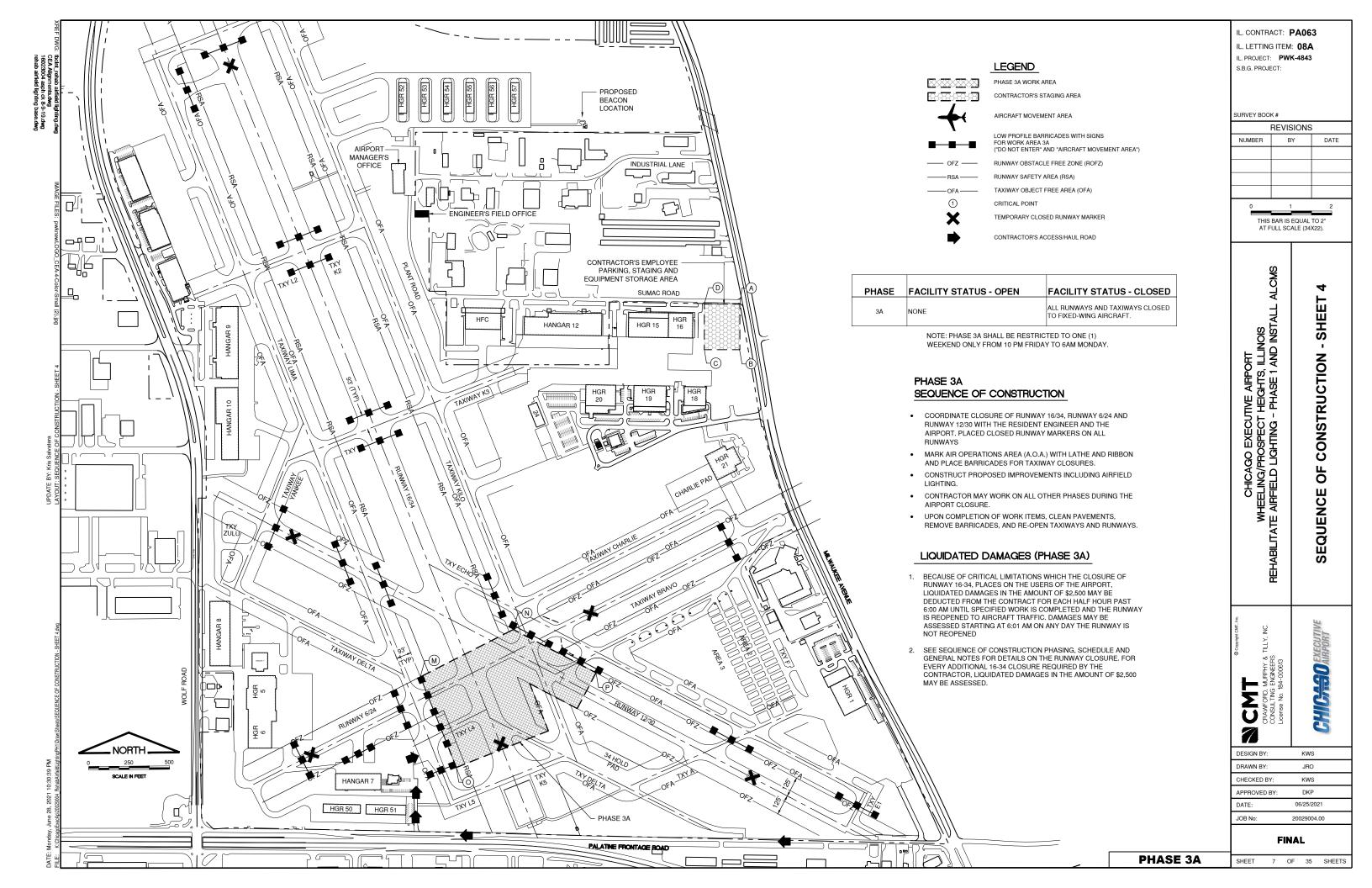
SHEET 2 OF 35 SHEETS

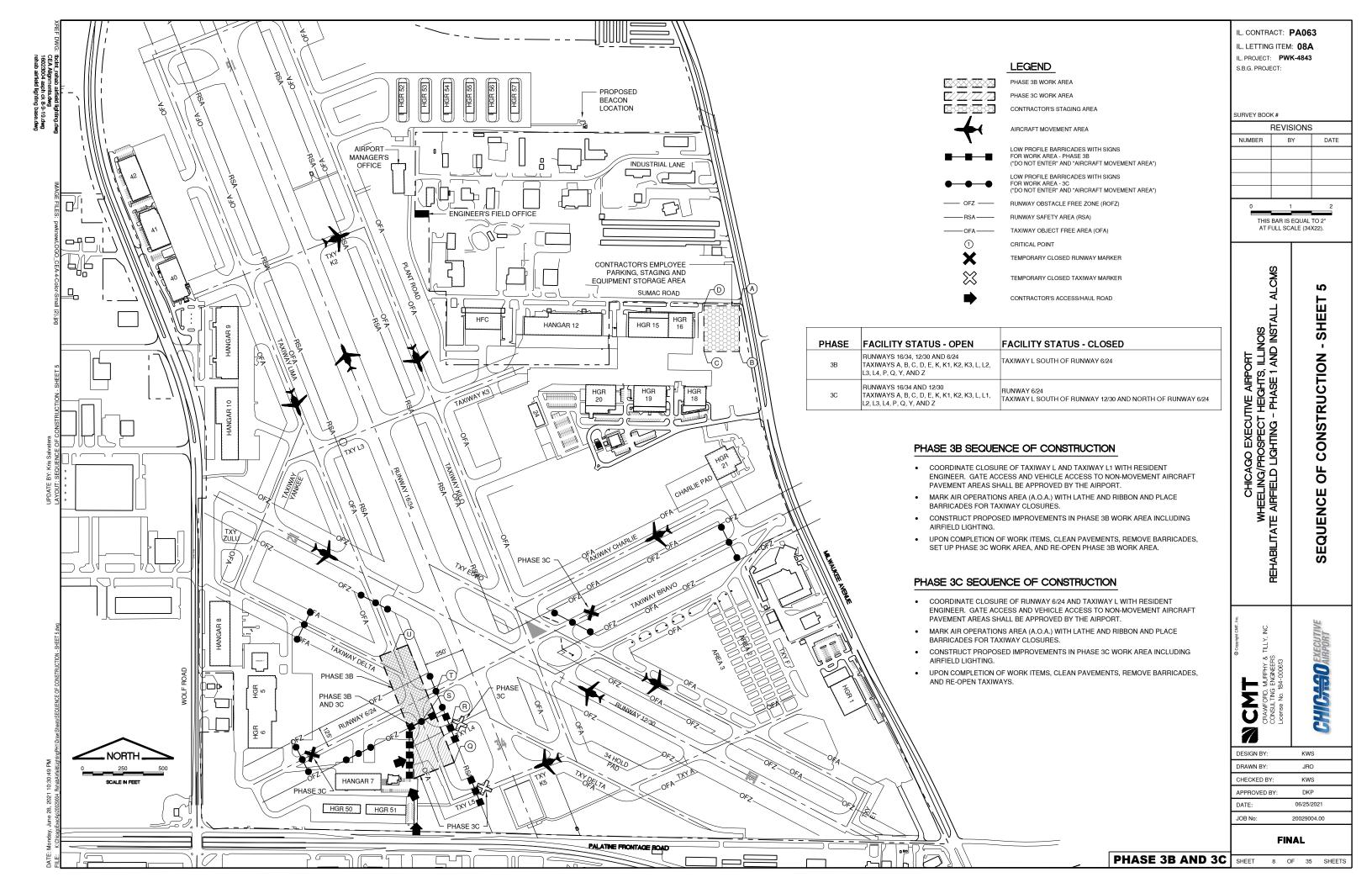


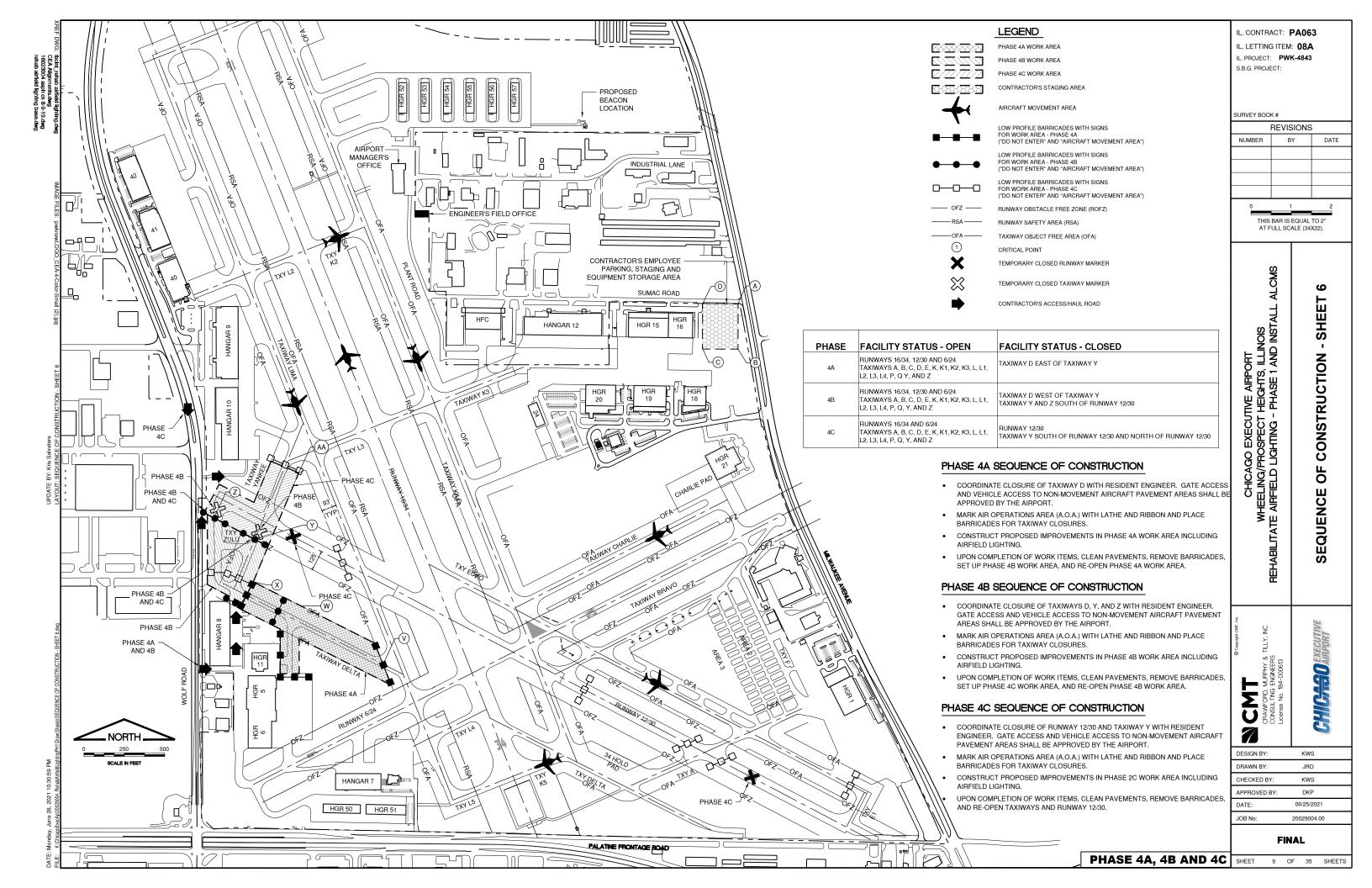












ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2G (LATEST EDITION) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION"

- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS. NO EXCEPTIONS FOR SLOW MOVING EQUIPMENT SHALL BE ALLOWED
- THE AIRPORT MANAGER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATION PHASING AND SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL BOAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER AND AIRPORT
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE LINDER SLIFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTORS WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER. ANY DEFECIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 10. MATERIALS REMOVED FROM THE PROJECT WILL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
- 11. PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, CONSTRUCTION FENCE, SIGNING, RUNWAY AND TAXIWAY CLOSED MARKERS, SAFETY AND OBJECT FREE AREAS, LATHE AND RIBBON, ETC SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. LOW PROFILE BARRICADES SHALL CONFORM TO THE DETAILS IN THE PLANS AND SEQUENCE OF CONSTRUCTION, BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE WORK AREA BY CONTRACTOR'S WORKERS EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. ALL BARRICADES SHALL BE PLACED OUTSIDE OF ACTIVE SAFETY AREAS AND OBJECT FREE AREAS.
- 12. THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER TEN (10) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE

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- 13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON-WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT. ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR
- 14. CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS OR A WORKING BEACON LIGHT ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.
- 15. IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT ENGINEER
- 16. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK
- 17. THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN ASPHALT/STONE TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEET IN A DUMP POSITION.
- 18. IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED. RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS
- 19. CHICAGO EXECUTIVE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT
- 20. APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES
- MOBILIZATION/FOLIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS, THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.

- 22. LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR, REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED, ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO PREVIOUS EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.
- 23. COORDINATION MEETINGS THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS, MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ILL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT
- 24. THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- 25. DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO
- 26. VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA AND RUNWAY SAFETY AREA OF ACTIVE TAXIWAYS AND RUNWAYS.
- 27. CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 IMAGINARY SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS. 28. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES
- SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE. ANY NECESSARY TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT
- 29. COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF LINDERGROUND LITH ITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS, NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT

#### CONTRACTOR CROSSING RUNWAY SAFETY AREAS (RSA) AND TAXIWAY OBJECT FREE AREAS (TOFA)

- 31. ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIREIFI D PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE CONTROL TOWER SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS, THIS COST SHALL BE CONSIDERED NCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS,
- 32. ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER.
  PAVEMENT SHALL BE CONTINUALLY SWEPT TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE

#### LIMITATIONS ON CONSTRUCTION WITHIN RUNWAY SAFETY AREAS (RSA) / RUNWAY OBSTACLE FREE ZONE (OFZ) AND TAXIWAY OBJECT FREE AREAS (TOFA)

#### RUNWAYS:

33. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. ANY WORK WITHIN THE RUNWAY SAFETY AREA OR RUNWAY OBSTACLE FREE AREA WILL REQUIRE A RUNWAY CLOSURE. WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING PERIOD THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED PER FAA REQUIREMENTS, AT LEAST ONE OF THE RUNWAYS SHALL REMAIN IN OPERATION AT ALL TIMES. IF NECESSARY, STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE RSA AT NO ADDITIONAL COST TO THE CONTRACT. NO MATERIAL SHALL BE STOCKPILED WITHIN THE RSA. IF DURING RUNWAY CLOSURE AN EMERGENC IS DECLARED. THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES. MEN. EQUIPMENT AND BARRICADES

#### **TAXIWAYS**

34. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS IN ADVANCE FOR WORK WITHIN THE TAXIWAY OBJECT FREE AREA. ANY WORK WITHIN THE TAXIWAY OBJECT FREE AREA WILL REQUIRE A TAXIWAY CLOSURE. WORK WITHIN THE TAXIWAY OBJECT FREE AREA SHALL BE EXPEDITED AND AT THE END OF EACH WORKING PERIOD THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE TAXIWAY TO BE REOPENED PER FAA REQUIREMENTS. IF NECESSARY, STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE TOFA AT NO ADDITIONAL COST TO THE CONTRACT. NO MATERIAL OR EQUIPMENT SHALL BE STOCKPILED OR STORED WITHIN THE TOFA. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT.

#### ALLOWABLE CONSTRUCTION HOURS

THE ALLOWABLE CONSTRUCTION HOURS FOR THE VILLAGE OF WHEELING AND THE CITY OF PROSPECT HEIGHTS ARE FROM 7 AM TO 6 PM, MONDAY THROUGH SATURDAY. THE AIRPORT WILL SEEK A WAIVER WITH THE VILLAGE AND CITY TO ALLOW CONSTRUCTION OUTSIDE OF THOSE HOURS FOR THE PHASES SHOWN TO BE COMPLETED OVER WEEKENDS ONLY. AT ALL OTHER TIMES, IT IS EXPECTED THE CONTRACTOR WILL ADHERE TO THE VILLAGE AND CITY NOISE ORDINANCE AND ALLOWABLE CONSTRUCTION HOUR POLICIES. SHOULD THE CONTRACTOR REQUIRE ADDITIONAL WORKING HOURS, HE SHALL REQUEST, THROUGH THE RESIDENT ENGINEER, THAT THE VILLAGE AND CITY BE CONTACTED TO REQUEST ADDITIONAL WAIVER OF THE NOISE ORDINANCE POLICY. ANY FINES LEVIED BY THE VILLAGE OR CITY TO THE AIRPORT FOR VIOLATIONS OF THE NOISE ORDINANCE AND ALLOWABLE CONSTRUCTION HOURS SHALL BE PAID BY THE CONTRACTOR

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS WORK OF HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05), THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION
ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR ANY EFFORTS TO COORDINATE AND ACCESS THE TAXIWAY SITE DUE TO ADJACENT BUILDING CONSTRUCTION

GROUND CONTROL FREQUENCY: 121.7 MHz AIR CONTROL FREQUENCY: 119.9 MHz

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION **FQUIPMENT: DUMP TRUCK IN DUMP POSITION - 25** 

IN THE EVENT THE CONTRACTOR PROPOSES TO UTILIZE CONSTRUCTION FOUIPMENT THAT IS TALLER THAN WHAT IS LISTED, THE CONTRACTOR WILL BE RESPONSIBLE TO SUBMI FAA FORM 7460 FOR AIRSPACE APPROVAL. THE RESIDENT ENGINEER WILL PROVIDE BASE AIRPORT INFORMATION FOR THE CONTRACTOR'S USE

#### **ELECTRICAL NOTES - ALL PHASES**

- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.
- WHEN FAA CABLES ARE REQUIRED TO BE LOCATED. A 10 WORKING DAY ADVANCED NOTICE SHALL BE GIVEN TO THE FAA BEFORE ANY SUCH MARKINGS ARE REQUIRED. ONCE FAA MARKS THE CARLES THE CONTRACTOR WILL BE REQUIRED TO SURVEY THE FAA UTILITIES SO THEY CAN BE REPLACED DURING CONSTRUCTION WITHOUT REMARKING BY THE FAA THIS SHALL BE INCIDENTAL AND AT THE CONTRACTOR'S EXPENSE. THE FAA PERSONNEL ARE ONLY AVAILABLE FROM S AM TO 3 PM, MONDAY THROUGH FRIDAY WITH ADVANCED
- SPECIAL ATTENTION IS NECESSARY WHEN WORKING NEAR FAA POWER AND CONTROL CABLES. ANY FAA UTILITY THAT IS DAMAGED OR CUT DURING CONSTRUCTION SHALL BE REPAIRED IMMEDIATELY. FAA REQUIRES THAT ANY DAMAGE CABLE BE REPLACED IN ITS ENTIRETY, FROM POWER/CONTROL SOURCE TO THE EQUIPMENT/SERVICE. SPLICES OF ANY KIND WILL NOT BE PERMITTED. EXPOSURES OF ANY FAA CABLES MUST BE DONE BY HAND DIGGING OF HYDRO-EXCAVATION. NO ADDITIONAL COMPENSATION WILL BE MADE FOR LOCATING, REPLACEMENT OR REPAIR OF FAA FACILITIES OR CABLES BUT, SHALL BE INCIDENTAL AND AT THE CONTRACTOR'S EXPENSE

	EXISTING CRITICAL AIRCF	UIRED SAFET	Y AREAS	
	RUNWAY	16/34	12/30	6/24
	APPROACH CATEGORY	D	В	В
	DESIGN GROUP	III	II	I
	DESIGN AIRCRAFT	GULFSTREAM 550	KING AIR B200	CESSNA 421
7	APPROACH SPEED	141 KNOTS	103 KNOTS	96 KNOTS
U ALP	WINGSPAN	94 FEET	55 FEET	42 FEET
2	TAIL HEIGHT	25.8 FEET	15.0 FEET	11.6 FEET
APPROVED	STRENGTH (MGTW)	90,500 LBS.	12,500 LBS.	7,450 LBS.
	LENGTH	97 FEET	44 FEET	37 FEET
ZUUS CEA	RUNWAY SAFETY AREA WIDTH (RSA)	500 (250' FROM φ)	150 (75' FROM <b>©</b> )	120 (60' FROM <b>©</b> )
	RWY OBJECT FREE AREA WIDTH (ROFA)	800	500	400
Ž.	RWY OBSTACLE FREE ZONE WIDTH (ROFZ)	400 (200' FROM ငု)	250 (125' FROM Ç)(2)	250 (125' FROM ©)(2)
DAIA	TAXIWAY SAFETY AREA WIDTH (TSA)	118	79	49
	(2) TXY OBJECT FREE AREA WIDTH (TOFA)	186 (93' FROM ပု)	131(65.5' FROM Ç)	89 (44.5' FRM Ç)

- TOFA, ROFA, RSA, CRITICAL AREAS, PART 77 AND DEPARTURE/APPROACH SURFACES ARE SHOWN FOR REFERENCE FOR BUNWAY TO BE CLOSED DURING VARIOUS CONSTRUCTION PHASES
- 2. NO CONSTRUCTION WITHIN THESE LIMITS WILL BE ALLOWED WITHOUT THE RUNWAY AND/OR TAXIWAY CLOSED.
- AT THE END OF THE WORK DAY NO CONSTRUCTION FOUIPMENT SHALL BE PARKED/STORED WITHIN THE OBJECT FREE AREA(S) OF OPEN OR CLOSED RUNWAYS AND/OR TAXIWAYS. PARKED/STORED EQUIPMENT HEIGHT SHALL NOT PENETRATE PART 77 SURFACE(S).

CRITICAL POINTS TABLE						
WORK AREA	POINT	APPROXIMATE ELEVATION OF GROUND (1929 DATUM)	ANTICIPATED EQUIPMENT AND HEIGHT	APPROXIMATE ELEVATION OF EQUIPMENT (1929 DATUM)	LATITUDE (NAD 83)	LONGITUDE (NAD83)
STAGING	Α	640	MINI-EXCAVATOR - 25'	665	N042° 07' 06.92"	W087° 53' 42.49"
STAGING	В	640	MINI-EXCAVATOR - 25'	665	N042° 07' 04.16"	W087° 53' 42.55"
STAGING	С	640	MINI-EXCAVATOR - 25'	665	N042° 07' 04.17"	W087° 53' 45.52"
STAGING	D	640	MINI-EXCAVATOR - 25'	665	N042° 07' 07.05"	W087° 53' 45.46"
PHASE 1A	Е	635	MINI-EXCAVATOR - 25'	660	N042° 06' 54.39"	W087° 54' 01.67"
PHASE 1B	F	635	MINI-EXCAVATOR - 25'	660	N042° 06' 48.50"	W087° 53' 58.16"
PHASE 2A	G	637	MINI-EXCAVATOR - 25'	662	N042° 06' 45.03"	W087° 53' 56.00"
PHASE 2A	Н	637	MINI-EXCAVATOR - 25'	662	N042° 06' 40.15"	W087° 53' 43.67"
PHASE 2A/2C	I	637	MINI-EXCAVATOR - 25'	662	N042° 06' 40.19"	W087° 53' 40.48"
PHASE 2A	J	635	MINI-EXCAVATOR - 25'	660	N042° 06' 50.00"	W087° 53' 45.62"
PHASE 2B	К	638	MINI-EXCAVATOR - 25'	663	N042° 06' 47.48"	W087° 54' 00.90'
PHASE 2C/2D	L	638	MINI-EXCAVATOR - 25'	663	N042° 06' 36.19"	W087° 53' 33.05"
PHASE 3A	М	638	MINI-EXCAVATOR - 25'	663	N042° 06' 44.23"	W087° 54' 08.92'
PHASE 3A	N	637	MINI-EXCAVATOR - 25'	662	N042° 06' 47.10"	W087° 54' 01.25"
PHASE 3A	0	640	MINI-EXCAVATOR - 25'	665	N042° 06' 38.68"	W087° 54' 06.00"
PHASE 3A	Р	635	MINI-EXCAVATOR - 25'	660	N042° 06' 44.43"	W087° 53' 54.48"
PHASE 3B	Q	639	MINI-EXCAVATOR - 25'	664	N042° 06' 12.83"	W089° 41' 10.28"
PHASE 3B	R	639	MINI-EXCAVATOR - 25'	664	N042° 06' 14.95"	W089° 41' 10.65"
PHASE 3B/3C	S	639	MINI-EXCAVATOR - 25'	664	N042° 06' 15.26"	W089° 41' 13.08"
PHASE 3C	Т	639	MINI-EXCAVATOR - 25'	664	N042° 06' 17.15"	W089° 41' 11.88"
PHASE 3C	U	639	MINI-EXCAVATOR - 25'	664	N042° 06' 19.61"	W089° 41' 15.40"
PHASE 4A	٧	640	MINI-EXCAVATOR - 25'	665	N042° 06' 45.00"	W087° 54' 11.97'
PHASE 4A	W	640	MINI-EXCAVATOR - 25'	665	N042° 06' 48.60"	W087° 54' 18.68'
PHASE 4A/4B	Х	640	MINI-EXCAVATOR - 25'	665	N042° 06' 50.10"	W087° 54' 22.61"
PHASE 4B/4C	Υ	640	MINI-EXCAVATOR - 25'	665	N042° 06' 52.70"	W087° 54' 22.47'
PHASE 4B/4C	Z	640	MINI-EXCAVATOR - 25'	665	N042° 06' 54.01"	W087° 54' 25.80'
PHASE 4C	AA	640	MINI-EXCAVATOR - 25'	665	N042° 06' 56.74"	W087° 54' 18.84"

L. CONTRACT: PA063 IL. LETTING ITEM: 08A IL. PROJECT: PWK-4843 S.B.G. PROJECT

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CONSTRUCTION ND DETAILS - SHE PA

CHICAGO EXECUTIVE AIRPC WHEELING/PROSPECT HEIGHTS, REHABILITATE AIRPIELD LIGHTING - PHASE 1 SEQUENCE O Ш G

CHICAGO EXECUTA

RPHY & GINEERS

Συ

DESIGN BY KWS JRO DRAWN BY KWS CHECKED BY APPROVED BY 06/25/2021 JOB No: 20029004.00

**FINAL** 

SHEET 10 OF 35 SHEETS

#### PHASING NOTES (ALL PHASES)

- 1. THE INTENT OF THE PHASING PLANS IS TO MINIMIZE THE IMPACT OF CONSTRUCTION ON THE OPERATION OF THE AIRPORT. THE CONTRACTOR SHALL CONSTRUCT THE PROJECT IN CONSCCUTIVE PHASES AS OUTLINED IN THE PLANS UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT EXECUTIVE DIRECTOR.
- 2. PRIOR TO REOPENING A CLOSED RUNWAY, THE ENTIRE (RSA) RUNWAY SAFETY AREA (250 FEET FROM CENTERLINE AND INCLUDING BEYOND THE END OF THE RUNWAY WITHIN AIRPORT PROPERTY) AND (TOFA) TAXIWAY OBJECT FREE AREA MUST MEET FAA CRITERIA. FAA CRITERIA REQUIRES THAT THERE BE NO OPEN EXCAVATIONS OR TRENCHES IN THE SAFETY AREA(S), THE MAXIMUM PAVEMENT DROP OFF BE 3 INCHES. AND ALL GRADES IN ANY DIRECTION BE LESS THAN 3 PERCENT, STEEL PLATES TEMPORARY WEDGING OF BASE COURSE AND BITUMINOUS CONCRETE MAY BE REQUIRED TO MEET CRITERIA. ALL NECESSAR TEMPORARY MEASURES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 3. THE CONTRACTOR SHALL SUBMIT A CONSTRUCTION SCHEDULE, STRICT ADHERENCE TO THE APPROVED SCHEDULE WILL BE ENFORCED TO AVOID CONFLICTS WITH OTHER CONSTRUCTION ACTIVITIES ON THE AIRPORT AND THE ADVERSE EFFECTS THEY COULD HAVE ON AIRPORT OPERATIONS.
- THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT STAFF TO SCHEDULE THE RUNWAY/TAXIWAY CLOSURES. ITEMS SUCH AS THE EXTENDED WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT DEPENDABILITY AND MANPOWER AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THIS CRITICAL CLOSURE. THE AIRPORT EXECUTIVE DIRECTOR AND THE CONTRACTOR SHALL MUTUALLY AGREE ON THE EXACT DATES AND TIMES OF THE CLOSURE(S).
- 5. CONTRACTOR MUST MAINTAIN ACCESS TO ALL ACTIVE AND OPEN AREAS AT ALL TIMES. CONTRACTOR SHALL BELOCATE EQUIPMENT AT NO ADDITIONAL COST TO CONTRACT TO ALLOW AIRCRAFT TO PROVIDE MINIMAL DISRUPTIONS TO AIRCRAFT MOVEMENT IN
- 6. FAA AND AIRPORT ACCESS ROAD(S) SHALL NOT BE USED AS A HAUL ROAD BY THE CONTRACTOR WITHOUT PRIOR APPROVAL.
- 7. TO THE EXTENT POSSIBLE THE CONTRACTOR SHALL HAVE ALL EMPLOYEE PARKING OUTSIDE OF AIRPORT FENCE OR AS INDICATED AT THE LOCATION SHOWN
- THE AIRPORT RESERVES THE RIGHT TO MODIFY THE SEQUENCE OF CONSTRUCTION INCLUDING BUT NOT LIMITED TO PHASING, WORK AREAS, BARRICADE PLACEMENT, ACCESS AND HAUL ROUTES, AND CONTRACTOR MOVEMENTS AT ANY TIME DURING THE PROJECT WITH FAA, IDA AND ATCT APPROVAL.
- 9. CONTRACTOR MAY REQUEST TO COMBINE WORK PHASES/AREAS. THE AIRPORT WILL DETERMINE IF THE REQUEST IS
- 10. WHEN HAUL ROUTE IS IN USE ON ACTIVE AIRFIELD PAVEMENTS, THE CONTRACTOR WILL BE REQUIRED TO BE UNDER CONTROL BY A CROSSING GUARD OR ESCORT IN RADIO CONTACT WITH THE ATCT FOR CONSTRUCTION PERSONNEL, CONSTRUCTION TRAFFIC, CONTRACTORS VEHICLES AND EQUIPMENT CROSSING BY, TO OR FROM WORK ZONE. STOP SIGNS SHALL BE IN PLACE AT ALL TIMES IN THIS AREA. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.
- 11. TEMPORARY CLOSED TAXIWAY MARKERS ARE ONLY REQUIRED WHEN THE TAXIWAY WILL REMAIN CLOSED FOR 3 CONSECUTIVE

#### AIRFIELD LIGHTS AND SIGNS NOTES

- CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL, COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.
- CONTRACTOR SHALL TURN OFF RUNWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY CLOSURES. CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY, THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.

CLOSED RUNWAY MARKER DETAIL CLOSED RUNWAY MARKER DETAIL

END OF RUNWAY

OFF PAVEMENT TEMPORARY

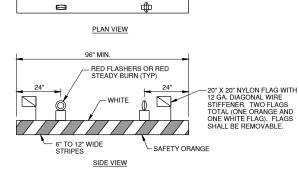
1. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE

2. FOR RUNWAY CLOSURES INVOLVING A SINGLE RUNWAY, IT IS ANTICIPATED THAT THE AIRPORT SHALL MOBILIZE THE AIRPORT OWNED LIGHTED "X"S ON EACH END OF THE CLOSED RUNWAY. THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN THE LIGHTED "X"S DURING EACH CLOSURE PERIOD.

CONTRACTOR SUPPLIED MAINTENANCE SHALL INCLUDE, BUT NOT BE LIMITED TO, FUELING, REPLACING LAMPS, CHECKING AND SUPPLYING OIL, ETC. IF
ANY DAMAGE IS INCURRED TO THE AIRPORT LIGHTED "X"S DURING CONSTRUCTION BY THE CONTRACTOR, THE CONTRACTOR SHALL REPLACE THE LIGHTED

3. FOR RUNWAY CLOSURES INVOLVING MORE THAN ONE RUNWAY, OR IF THE AIRPORT OWNED LIGHTED "X"S ARE NOT AVAILABLE. THE CONTRACTOR SHALL

MARK THE RUNWAYS TO BE CLOSED BY PLACING YELLOW CROSSES AT THE LOCATION AND DIMENSIONS DETAILED ON THE SEQUENCE OF CONSTRUCTION AND PER AC 150/5370-2 (LATEST EDITION). THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS



#### AIRSIDE LOW PROFILE LIGHTED BARRICADE

#### **BARRICADE NOTES**

CONSTRUCTION EQUIPMENT

AND TRUCK SIGNAL FLAG

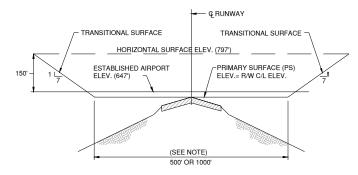
- 1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90^.
- 2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT
- BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°
- FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
- BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
- 6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
- 7. PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
- 8. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

## RUNWAY CENTERLINE RUNWAY FDGF TO BLINWAY CONTRACTOR TO REMARK TAXIWAY CENTERI INF WHEN "X" IS REMOVED (INCIDENTAL) BUNWAY SAFFTY AREA TAXIWAY CENTERLINE BARRICADES



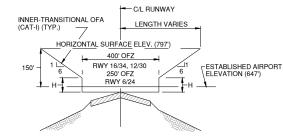
#### **CLOSED TAXIWAY MARKER DETAIL NOTES**

- 1. CLOSED TAXIWAY MARKERS SHALL BE PAINTED YELLOW WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING OR OTHER MATERIAL THAT DOES NOT VIOLATE THE OBJECT FREE AREA CRITERIA AND RUNWAY SAFETY AREA CRITERIA PER ADVISORY CIRCULAR 150/5300-13A (LATEST EDITION) AND ARE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT
- 2. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
- COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 4. PLACE MARKERS OVER TAXIWAY CENTERLINE
- 5. MARKERS SHALL BE ADEQUATELY SECURED TO PREVENT MOVEMENT BY PROPELLER WASH, JET BLAST OR OTHER WIND CURRENTS.
- 6. MARKERS ARE ONLY REQUIRED FOR CLOSURES EXCEEDING 72 HOURS



## TYPICAL SECTION F.A.R. PART 77 IMAGINARY SURFACES

IMAGINARY SURFACE REQUIREMENTS FOR EXISTING ACTIVE RUNWAYS (R/W) ARE SIMILAR EXCEPT PRIMARY SURFACE (PS) DIMENSIONS VARY R/W 6/24 & 12/30 500' PS (250' LT & RT OF €) R/W 16/34 1000' PS (500' LT & RT OF €)



#### TYPICAL SECTION OBSTACLE FREE ZONE (OFZ)

#### NO SCALE

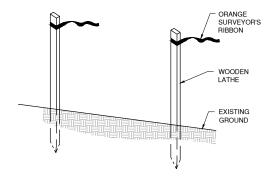
RUNWAY	TYPE OF RUNWAY	H (FEET)
16-34	PRECISION (CAT I)	48
6-24 12-30	VISUAL	150



#### TYPICAL PROFILE F.A.R. PART 77 IMAGINARY SURFACES

#### NO SCALE

RUNWAY END	ELEVATION	APPROACH SLOPE
16	643	50:1
34	644	20:1
12	645	20:1
30	640	20:1
6	647	20:1
24	638	20:1



#### CONSTRUCTION SETBACK LINE DETAIL

#### NOT TO SCALE

#### CONSTRUCTION SETBACK NOTES

- 1. CONTRACTOR SHALL MARKER THE RUNWAY SAFETY AREA, TAXIWAY OBJECT FREE AREA AND RUNWAY OBSTACLE FREE ZONE PER THE CONSTRUCTION SETBACK DETAIL AS DIRECTED BY THE
- 2. ALL COST ASSOCIATED WITH THE CONSTRUCTION SETBACK LINE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

L. CONTRACT: PA063 IL. LETTING ITEM: 08A IL. PROJECT: PWK-4843 S.B.G. PROJECT

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AGO EXECUTIVE AIRPOI /PROSPECT HEIGHTS, II > LIGHTING - PHASE 1 A

CHICAGO I MEELING/PRO? AIRFIELD LIGH

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DESIGN BY KWS CHECKED BY KWS APPROVED BY DKF 06/25/2021 IOB No: 20029004.00

#### **FINAL**

SHEET 11 OF 35 SHEETS

### 7. MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.

**TEMPORARY** 

AIRPORT AND TOWER PERSONNEL

**CLOSED RUNWAY MARKER NOTES** 

"X" IN KIND AT NO COST TO THE CONTRACT OR AIRPORT.

DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.

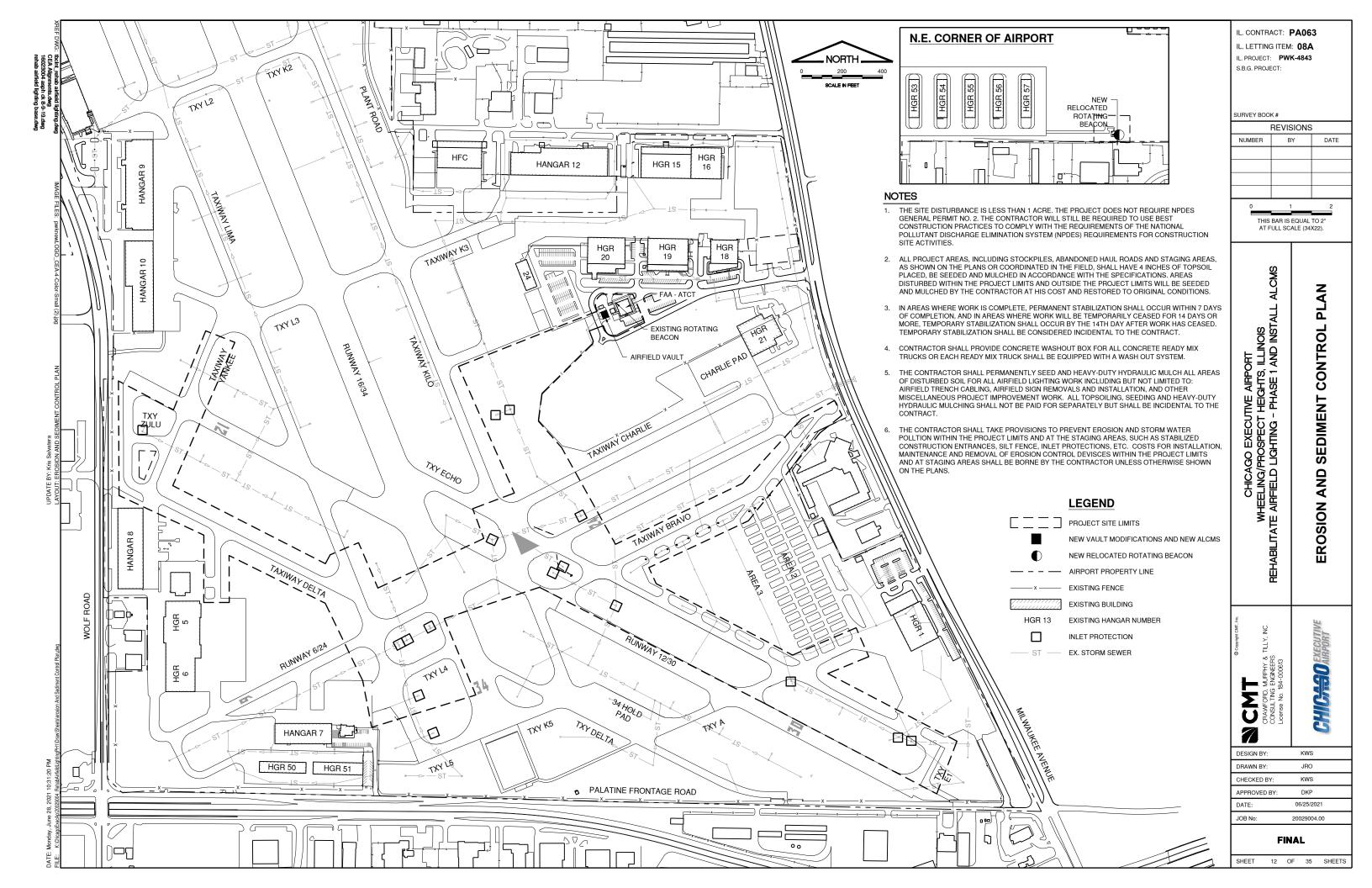
5. TEMPORARY MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.

TEMPORARY CLOSED RUNWAY MARKERS SHALL BE YELLOW.

8. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

6. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION





## STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE AT THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE

#### SITE DESCRIPTION:

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATION OF AN EXISTING BITUMINOUS PAVEMENT AT THE CHICAGO EXECUTIVE AIRPORT. THE PROJECT INCLUDES TURF SHOULDER ADJUSTMENT, VARIOUS PAVEMENT ITEMS, ELECTRICAL WORK, PAVEMENT MARKING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL.

REMOVAL, ADJUSTMENTS AND INSTALLATION OF ELECTRICAL AND MISCELLANEOUS ITEMS

TURF SHOULDER ADJUSTMENT, SEEDING AND MULCHING

REMOVAL AND DISPOSAL OF TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES.

#### AREA OF CONSTRUCTION SITE:

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 55 ACRES OF WHICH LESS THAN 1 ACRE WILL BE DISTURBED BY GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

- INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION
- 2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE DES PLAINES RIVER THROUGH A STORM SEWER SYSTEM

#### EROSION AND SEDIMENT CONTROL

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION

THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, PERMANENT SEEDING, MUCHING, SOD, PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND ALL OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR

AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION

DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

#### DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTSIDED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE <u>TEMPORARILY SEEDED</u>, <u>AT THE CONTRACTOR'S EXPENSE</u>, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN SEVEN (7) DAYS.

THE DOWN STREAM SIDE OF ALL STOCKPILES SHALL BE ENCOMPASSED WITH EROSION CONTROL BARRIER

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS WITHIN THE STAGING AREA. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE

THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT PERIODICALLY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2" OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE RESIDENT ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT THE EROSION AND SEDIMENT CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.

SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCIDENTAL TO THE CONTRACT.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING SOIL CONTAMINATION FROM BUILDING MATERIALS, FERTILIZERS, CHEMICALS, PAVEMENT MARKING, WASTE PILES, FUEL CONTAINMENT, AND ANY OTHER POTENTIAL HAZARDOUS MATERIALS THAT MAY EXIST ONSITE.

NO DEDICATED CONCRETE OR ASPHALT BATCH PLANTS SHALL BE LOCATED ON THIS SITE.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING

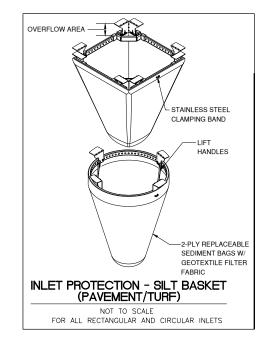
TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE

COST OF MAINTAINING THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE INCIDENTAL TO THE CONTRACT.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RE-SEEDED AND/OR SODDED.

#### MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE OF TEMPORARY AND PERMANENT EROSION CONTROL SYSTEMS UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.



SPACERS

INLET PROTECTION

(INLET/MANHOLES - IN TURF)

IDOT STANDARD 280001-07

SPACERS

FLOW

INLET PROTECTION (END SECTION)

IDOT STANDARD 280001-03

SILT FENCE

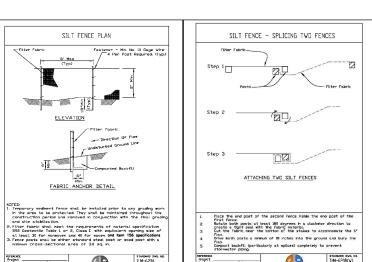
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PERIMETER EROSION BARRIER

GENERAL NOTES FOR SOIL EROSION AND SEDIMENT CONTROL:

- ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
- 2 NO WORK SHALL BE PERFORMED IN FLOWING WATER WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
- 3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
- 4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS
- 5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED
- 6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
- 7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS. THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO
- 8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, COOK COUNTY, CHICAGO EXECUTIVE AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
- 9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY
- 10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
- 11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
- 12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE
- 13. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE—HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
- ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL
- 15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING
- 16. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.





L. CONTRACT: PA063

IL. LETTING ITEM: 08A

IL. PROJECT: PWK-4843

**REVISIONS** 

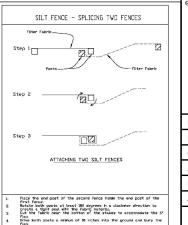
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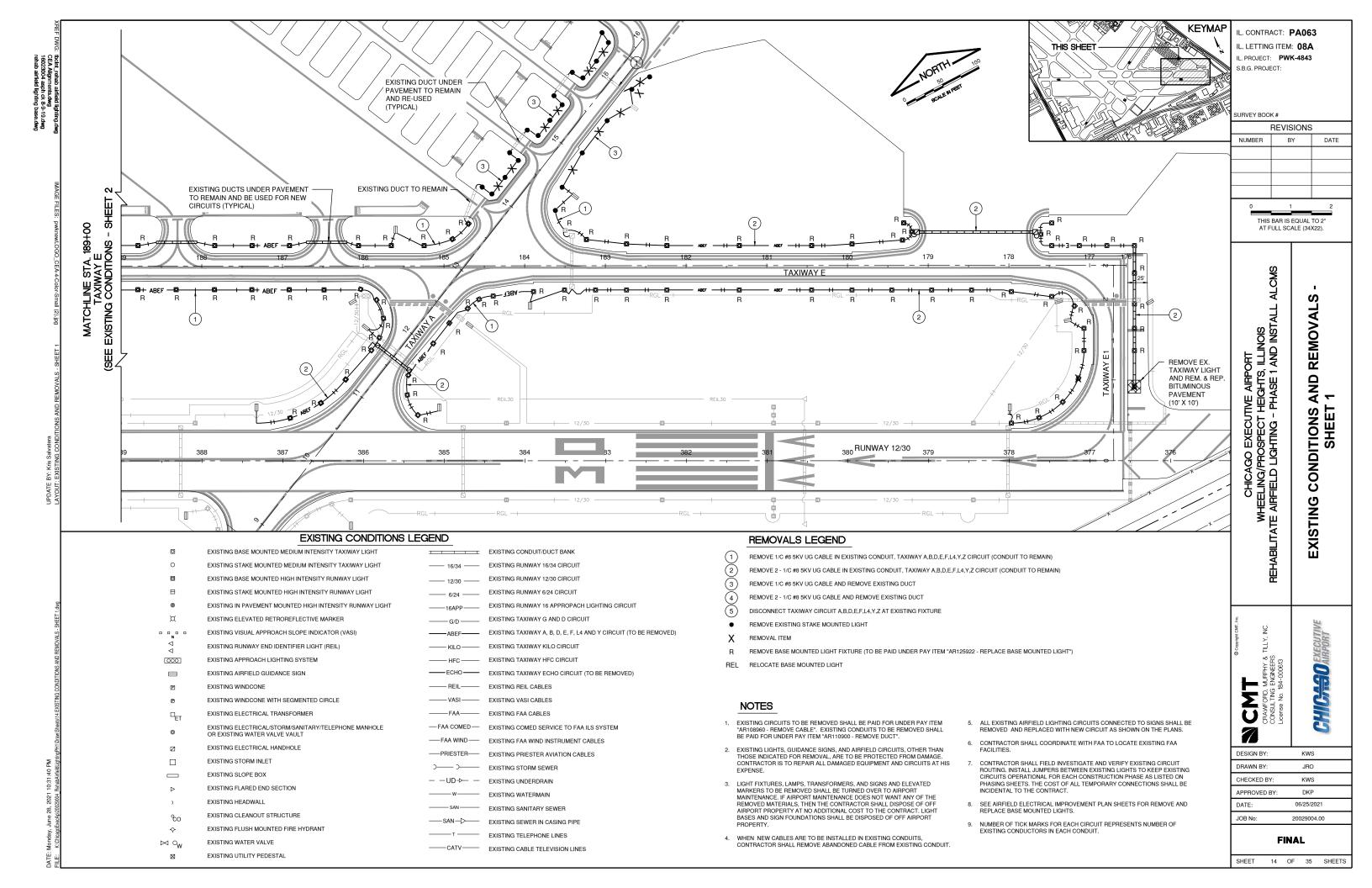


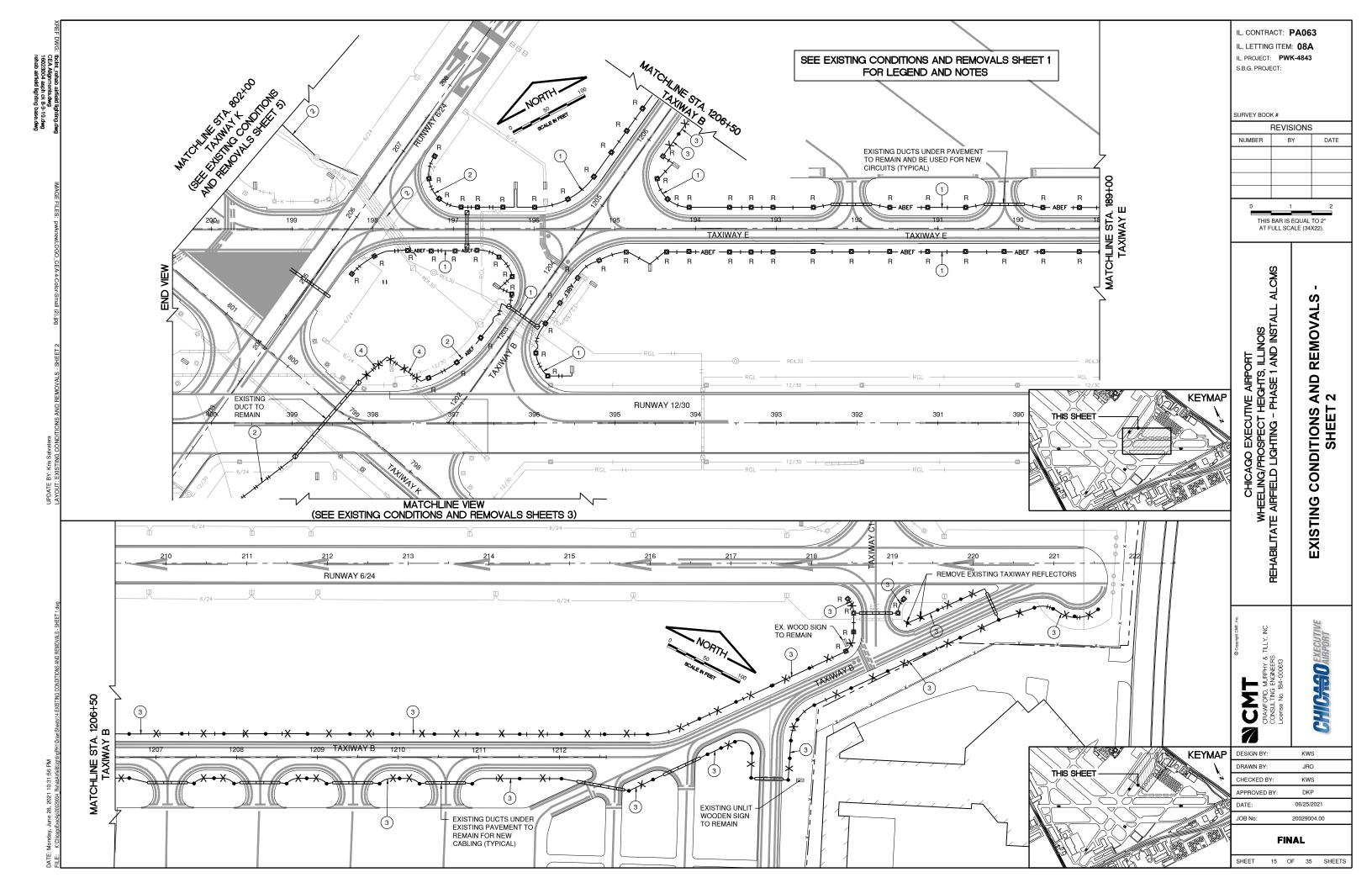
DESIGN BY KWS CHECKED BY KWS APPROVED BY 06/25/2021 JOB No: 20029004.00

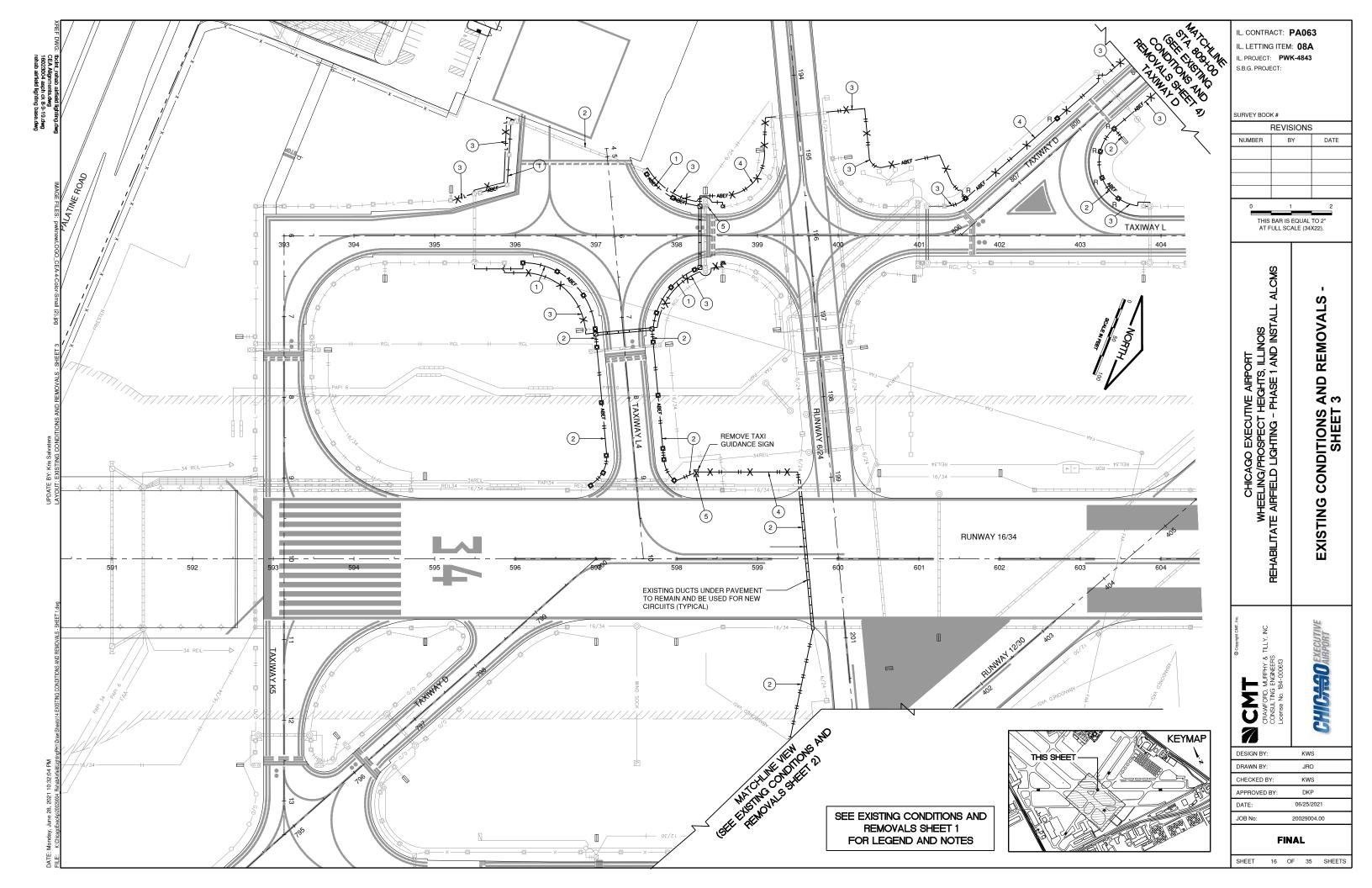
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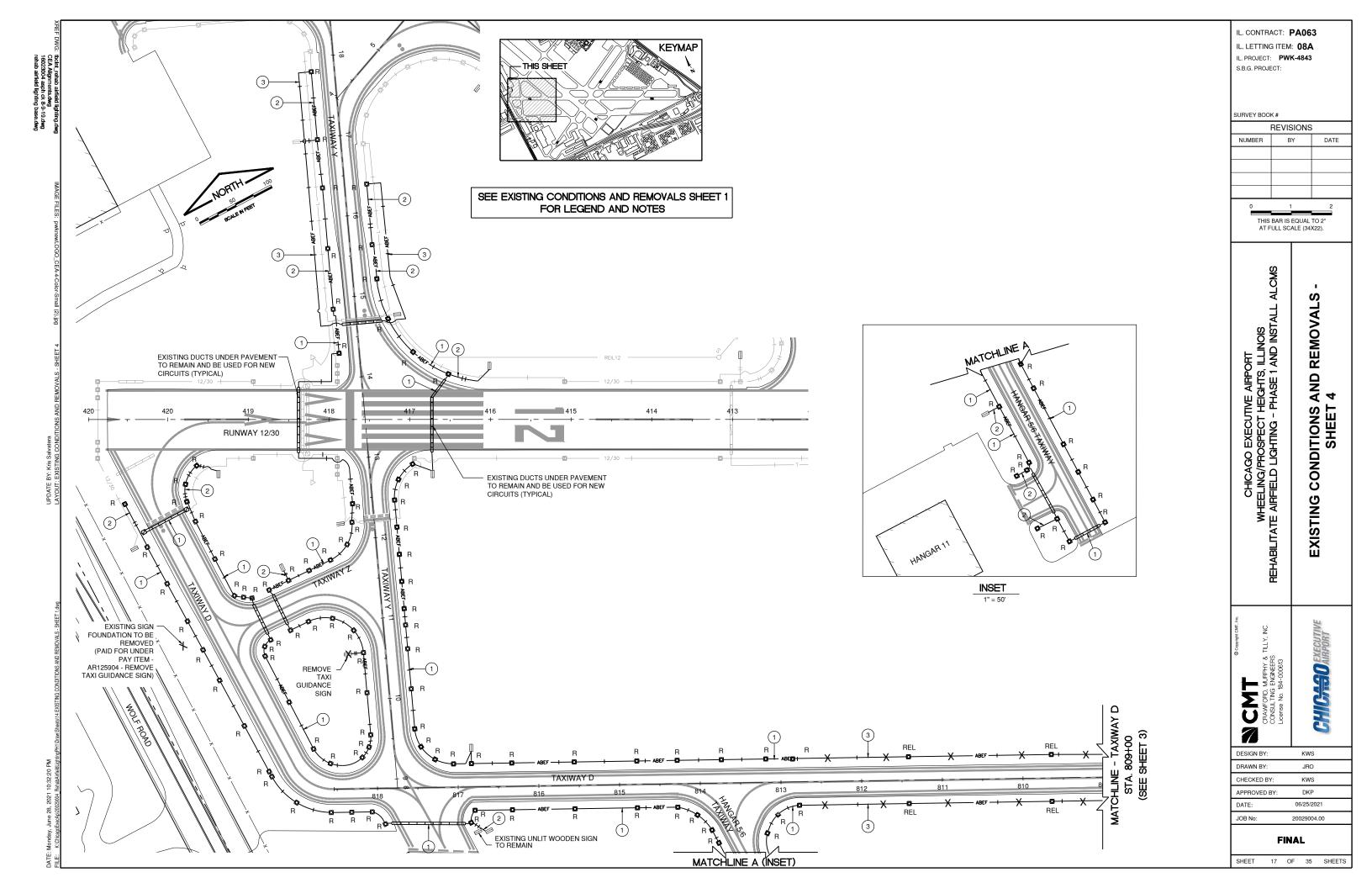
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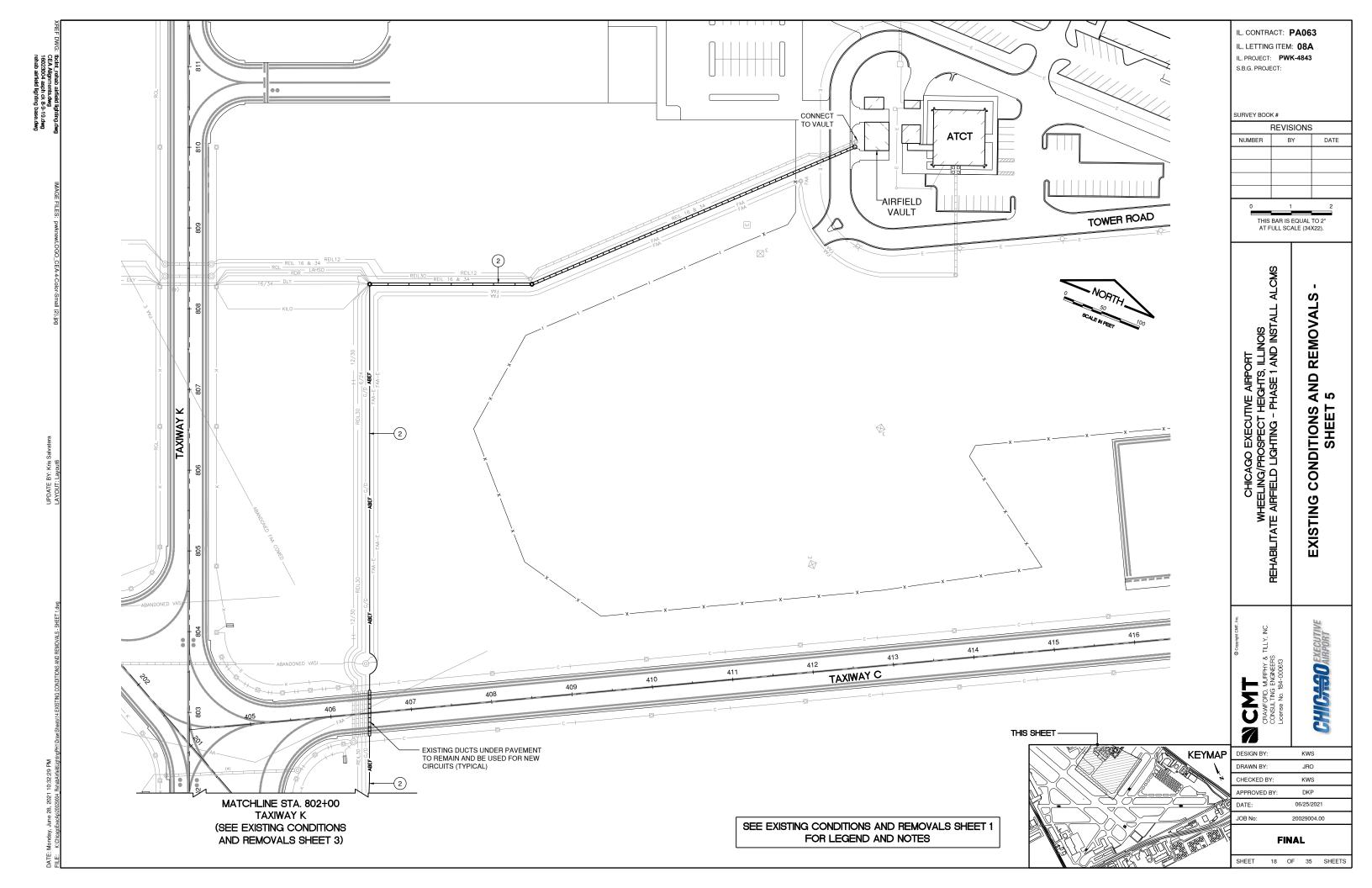
**FINAL** SHEET 13 OF 35 SHEETS

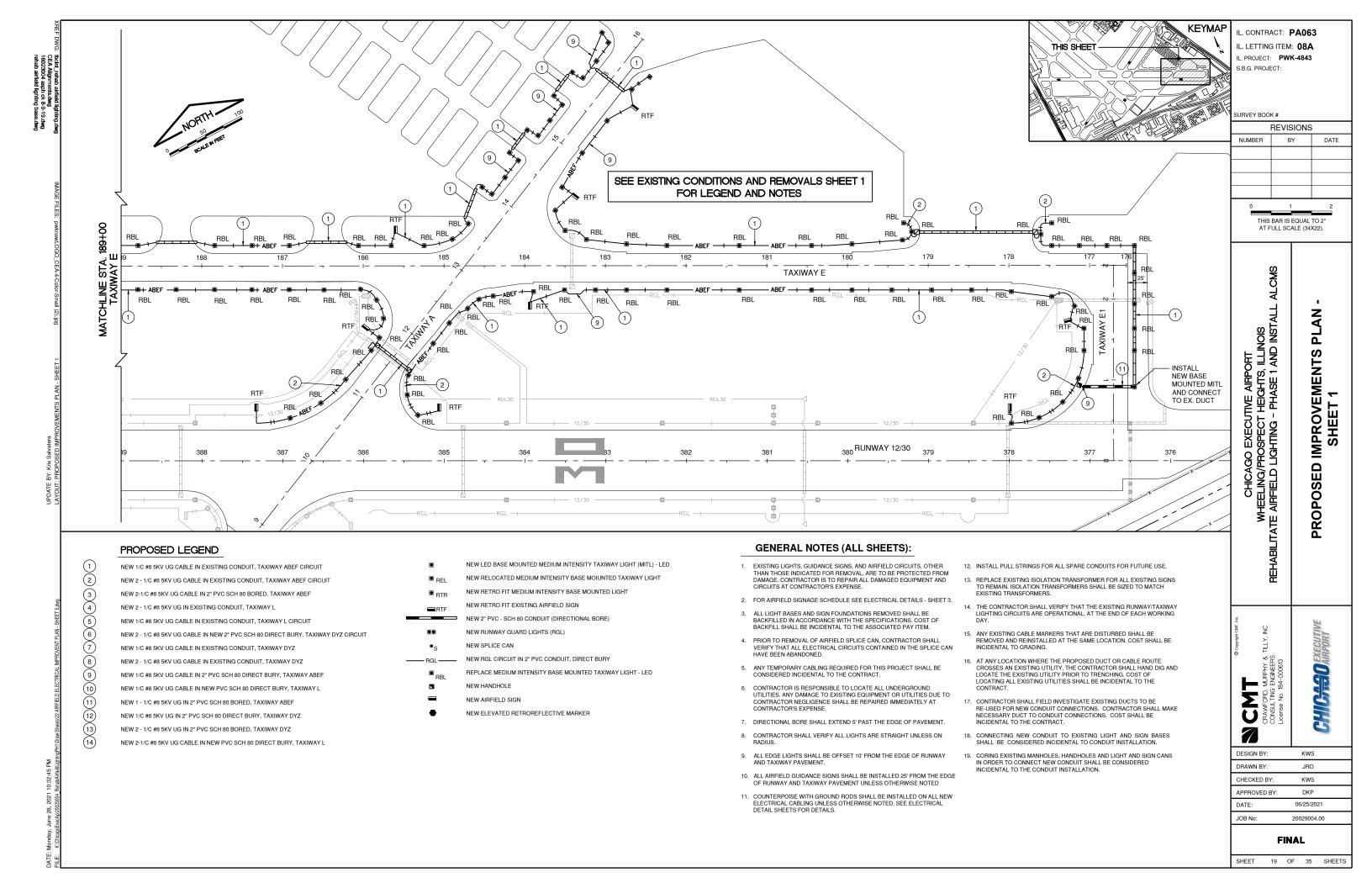


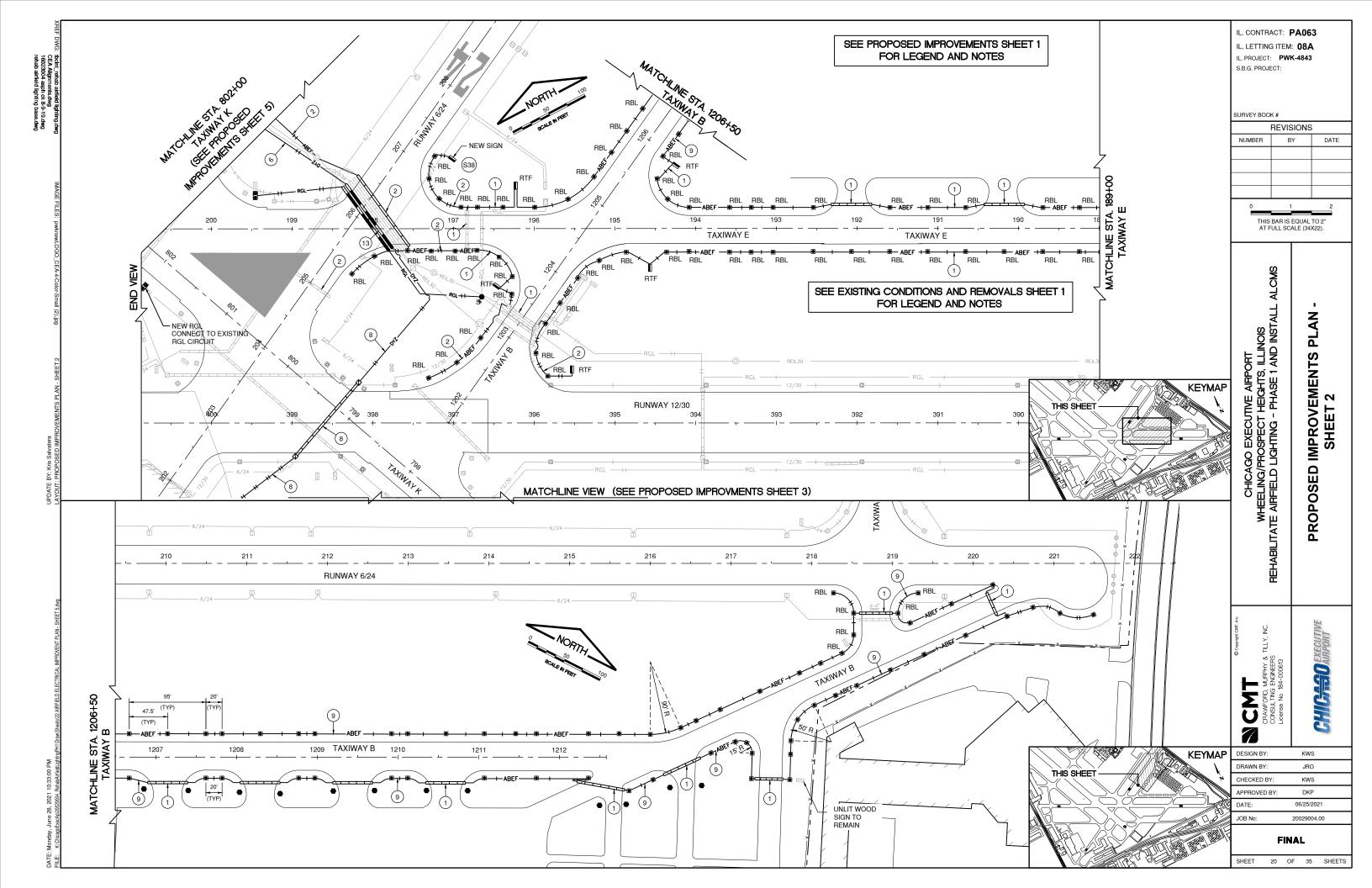


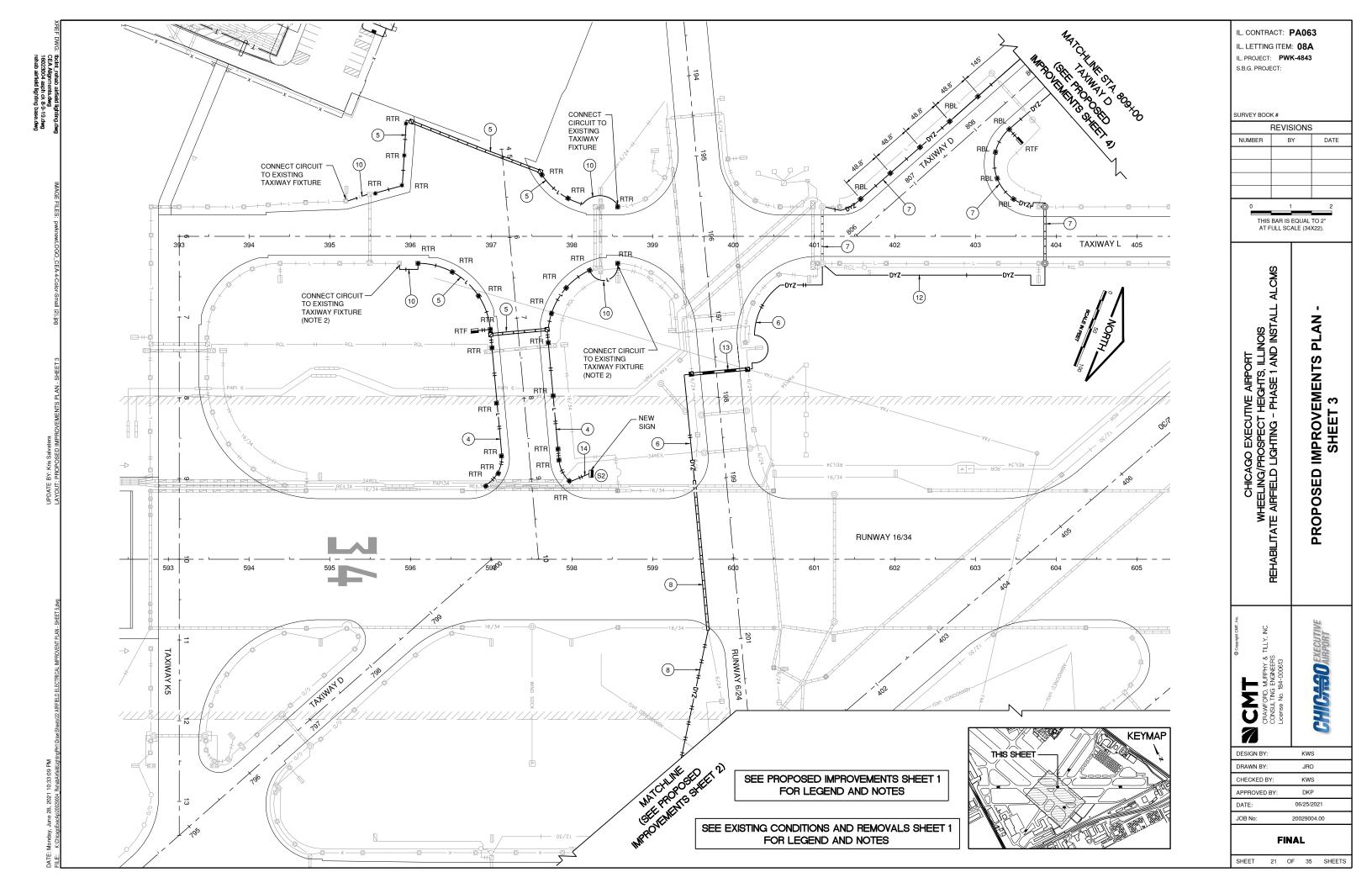


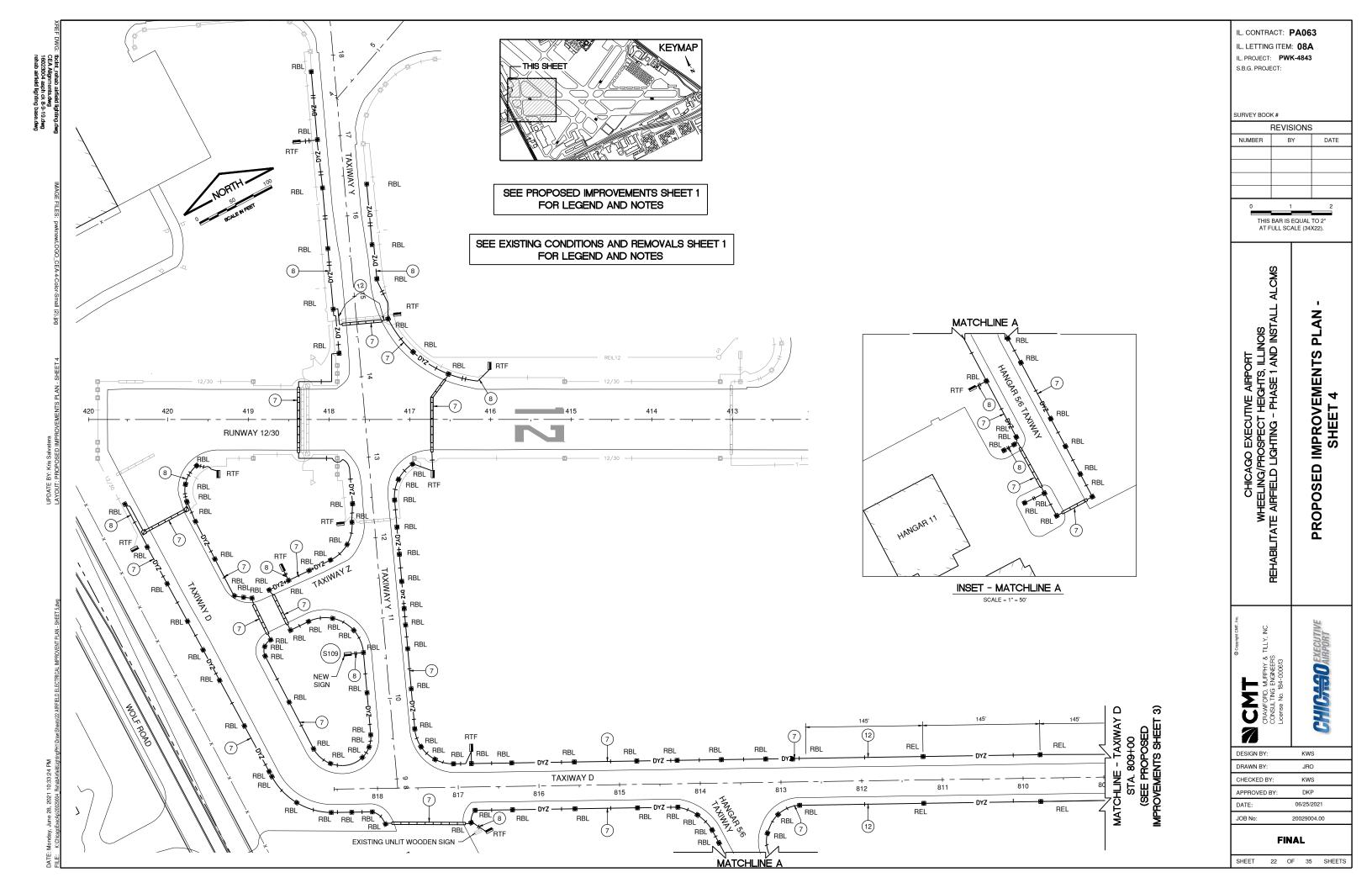


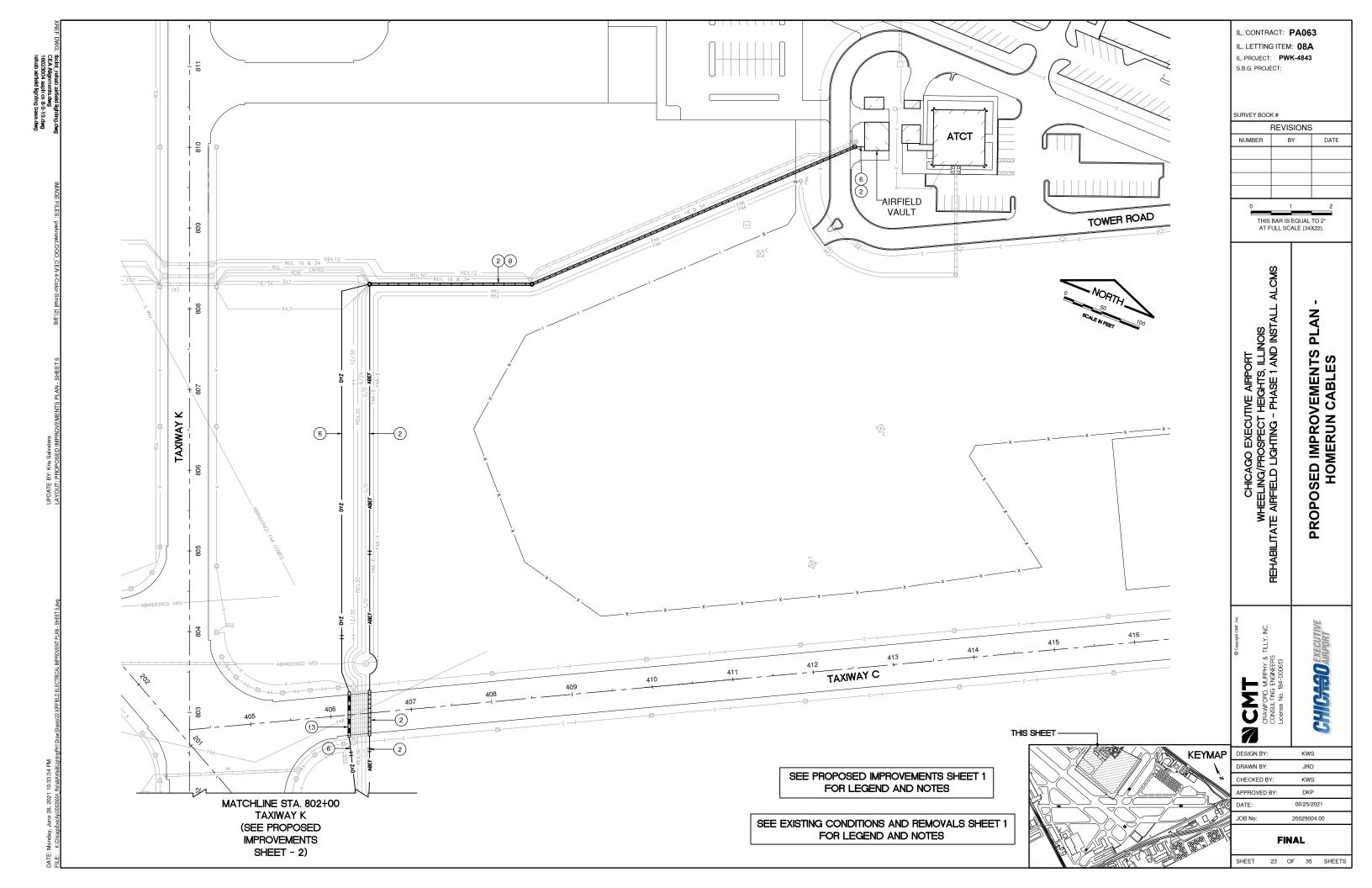


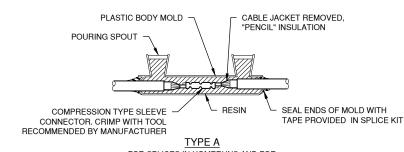




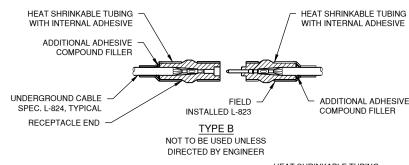


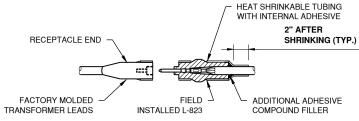




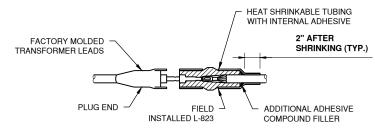


FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY





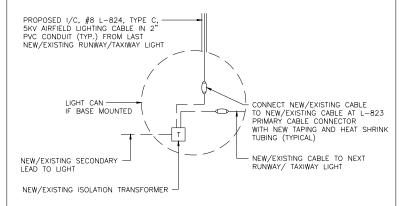
TYPE C
FOR SPLICES AT RUNWAY LIGHTS,
TAXIWAY LIGHT AND SIGNS



TYPE D FOR SPLICES AT RUNWAY LIGHTS, TAXIWAY LIGHT AND SIGNS

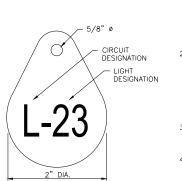
#### SPLICE NOTES

- 1. INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- 2. WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- 4. THE CONTRACTOR SHALL HAVE A MINIMUM OF FIVE (5) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.
- THE CONTRACTOR MAY ELECT TO USE AN FAA APPROVED "COMPLETE" KIT IN LIEU OF HEAT SHRINK KIT AT NO ADDITIONAL COST.



#### TAXIWAY LIGHTING CIRCUIT CONNECTION DETAIL

NOT TO SCALE

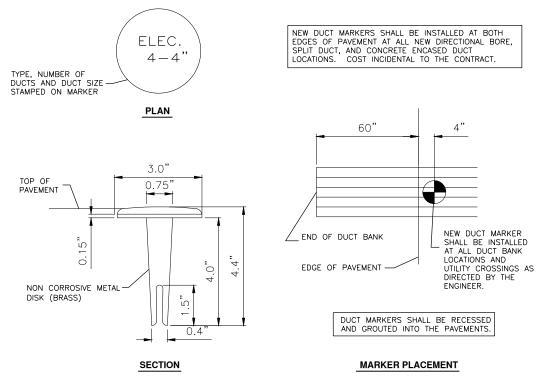


#### NOTES

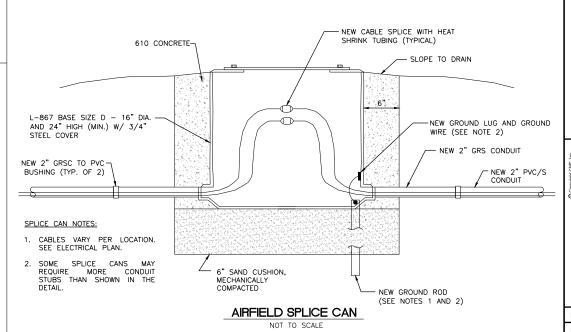
- INSTALL A NON-CORROSIVE DISC OF 2"
   MINIMUM DIAMETER WITH THE NUMBER
   PERMANENTLY STAMPED, CUT OUT, OR
   ENGRAVED UNDER THE HEAD OF THE BASE
   PLATE BOLT OR ATTACHED TO LIGHT FLANGE
   WITH A SET SCREW.
- 2. NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND PROPOSED TAXIMAY AND RUNWAY LIGHTS AND SIGNS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTIS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (PROPOSED OR RELOCATED LIGHTS) SHALL BE RETAGGED.
- 3. COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 4. COORDINATE WITH RESIDENT ENGINEER FOR TAXIWAY LIGHTING TAG NUMBERS.

#### LIGHT IDENTIFICATION DETAIL

NOT TO SCALE



DUCT MARKER DETAILS



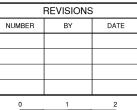
#### NOTES:

- 1. INSTALL SAFETY GROUND TO EXISTING SIGNS AND L-867 CANS. ATTACH GROUND LUG TO EXISTING CAN AND INSTALL GROUND ROD AS SHOWN ON PLANS.
- 2. INSTALL 1/C #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN AND EXOTHERMICALLY WELDED TO GROUND ROD. INSTALL GROUND LUG FOR EXISTING CANS, IF REQUIRED.

IL. CONTRACT: PA063
IL. LETTING ITEM: 08A

IL. PROJECT: **PWK-4843** S.B.G. PROJECT:

SURVEY BOOK #



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL AL

ELECTRICAL DETAILS - SHEET 1

CAMPHY & TILY, NC. CONSULTING ENGINEERS LICENSE No. 184-000613

HIGHER PARENCULINE

DESIGN BY: KWS

DRAWN BY: JRO

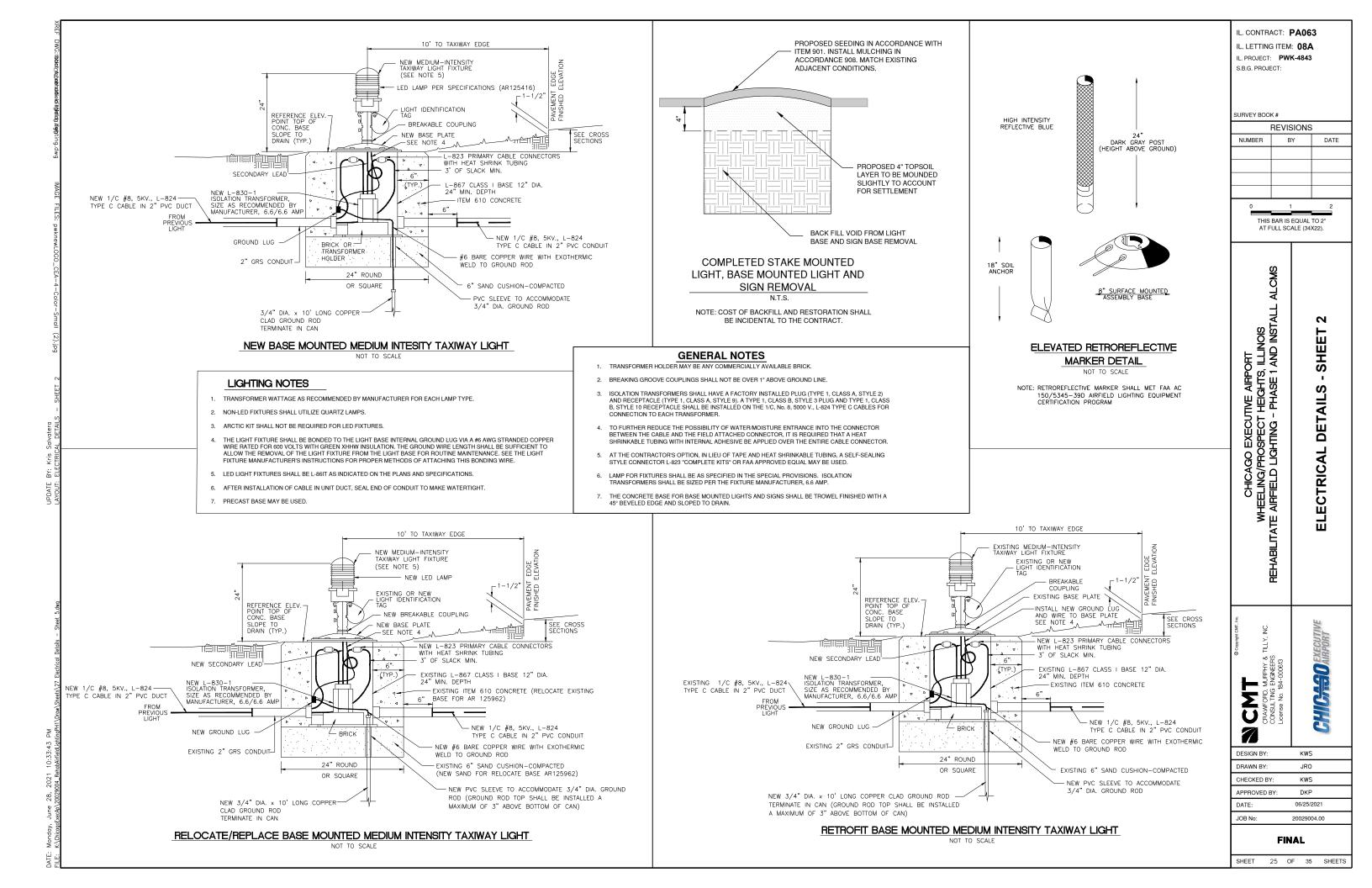
CHECKED BY: KWS

APPROVED BY: DKP

DATE: 06/25/2021

JOB No: 20029004.00

FINAL
SHEET 24 OF 35 SHEETS



#### NEW AIRFIELD GUIDANCE SIGN, L-858 (LED) DETAIL

NOT TO SCALE

#### SIGNAGE NOTES

- ALL SIGNS ARE 2-SIDED LED SIGNS, CLASS 2 AS MANUFACTURED BY ADB OR APPROVED EQUAL. SIGNS SHALL BE CAPABLE OF BEING CONNECTED TO A 3-STEP OR 5-STEP REGULATOR.
- 2. TRANSFORMER WATTAGE AS RECOMMENDED BY MANUFACTURER FOR LED SIGNS.
- 3. LIGHTED SIGNS SHALL BE BASE MOUNTED ONLY
- 4. THE NUMBER OF MODULES PER SIGN SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
- 5. CONTRACTOR SHALL SUBMIT DETAILED SHOP DRAWINGS INCLUDING SIGN, COLOR, SIZE AND PROPOSED LEGEND, IN ENOUGH DETAIL TO DETERMINE PROPOSED SPACING AND OTHER INFORMATION REQUIRED BY THE SPECIAL PROVISIONS
- 6. CONTRACTOR TO VERIFY PROPOSED SIGN LOCATIONS AND ORIENTATIONS WITH RESIDENT ENGINEER PRIOR TO INSTALLATION.

	AIRFIELD SIGNAGE SCHEDULE					
NEW SIGN NUMBER	NEW SIGN FACE	NEW SIGN LEGEND	NEW SIGN TYPE	NEW SIGN LOCATION	NOTES	
S2	N S	<b>◆</b> ⊅7 6-24	3 1	RUNWAY 16/34	NEW 4 CHARACTER SIGN	
S38	W E	BLANK 3 ♣	0 3	RUNWAY 6/24	NEW 2 CHARACTER SIGN	
S109	N S	↓ BΓ∀ИK Y Σ <b>⊕</b>	2,0 3,2	TAXIWAY Y	NEW 3 CHARACTER SIGN	

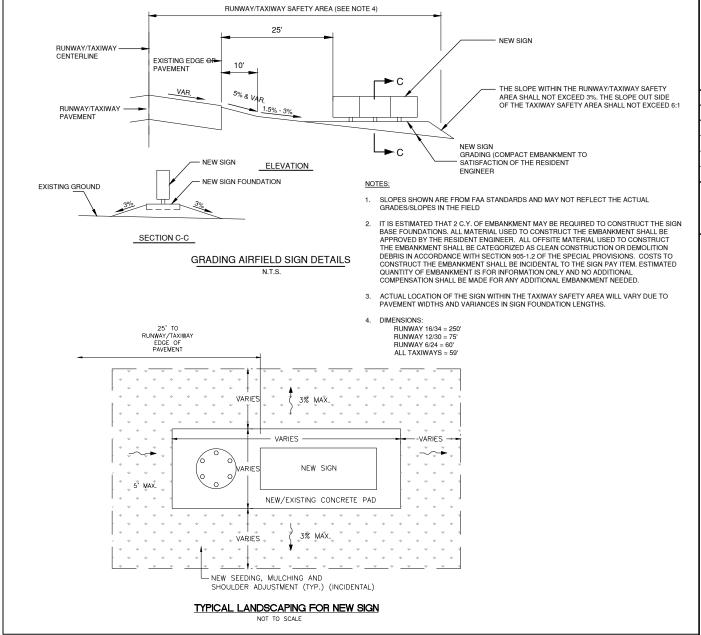
#### **NEW SIGN TYPE LEGEND**

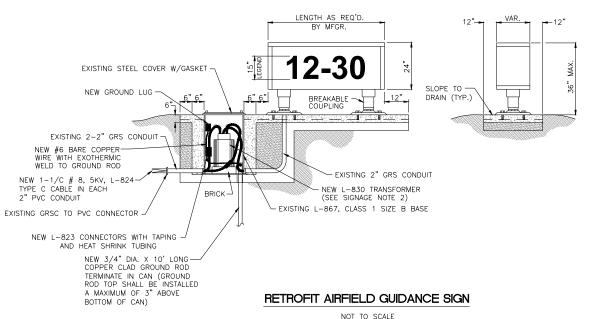
0 — BLANK PANEL – BLACK 1 — RUNWAY/TAXIWAY HOLDLINE – WHITE LEGEND ON RED BACKGROUND

2 — LOCATION SIGN — YELLOW LEGEND ON BLACK BACKGROUND 3 — DIRECTION/INFORMATION SIGN — BLACK LEGEND ON YELLOW BACKGROUND

#### NOTE

1. GUIDANCE SIGN PANEL SIZE WILL BE BASED ON THE MANUFACTURER'S RECOMMENDATION.





IL. CONTRACT: PA063 IL. LETTING ITEM: **08A** IL. PROJECT: PWK-4843 S.B.G. PROJECT:

URVEY BOOK #

**REVISIONS** NUMBER BY DATE

> THIS BAR IS FOLIAL TO 2" AT FULL SCALE (34X22).

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, ILLINOIS AND INSTALL က SHEE. **DETAILS** LECTRICAL

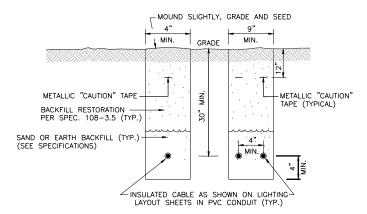
CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILL REHABILITATE AIRFIELD LIGHTING - PHASE 1 ANI

SO EXECUTI JAPHY & ... ΣU

DESIGN BY KWS DRAWN BY JRO CHECKED BY KWS APPROVED BY DKP 06/25/2021 JOB No: 20029004.00

**FINAL** 

SHEET 26 OF 35 SHEETS

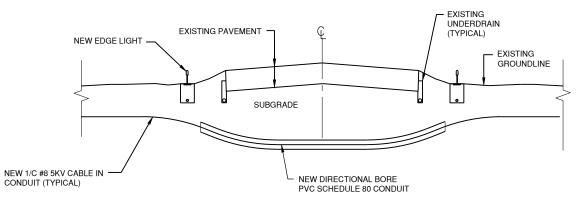


#### TRENCH NOTES

- 1. TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 4" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- 2. DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- 3. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- 4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO THE CONDUIT AND CABLE INSTALLATION.

#### TRENCH DETAIL

NOT TO SCALE

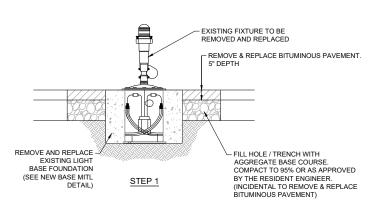


#### **DIRECTIONAL BORE DETAIL**

N.T.S.

#### **NOTES**

- THE DEPTH OF THE DIRECTIONAL BORE SHALL BE NO LESS THAN 4.0'
   FROM THE PAVEMENT SURFACE AND SHALL NOT DISTURB EXISTING UNDERDRAINS/UTILITIES OR NEW LIGHTS/CABLING.
- 2. REFER TO LIGHTING PLANS FOR ADDITIONAL CABLE AND CONDUIT INFORMATION.

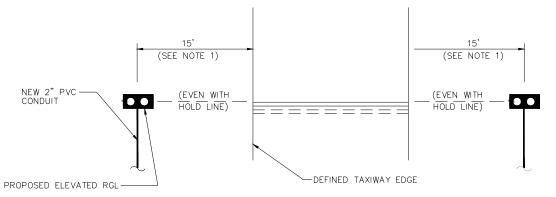


#### NOTES:

- SEE PLAN SHEETS FOR DIMENSIONS OF ASPHALT PAVEMENT TO BE REMOVED AND REPLACED.
- WHEN A NEW FIXTURE WILL BE INSTALLED AT THE SAME LOCATION AS THE REMOVAL, THE INSTALLATION SHALL BE COMPLETED IN ACCORDANCE WITH THE DETAILS AS SHOWN FOR A NEW BASE MOUNTED OR IN-PAVEMENT LIGHT. SEE PLAN FOR INSTALLATION TYPE AND LOCATIONS.

LIGHT BASE REMOVAL IN ASPHALT PAVEMENT (NEW/EXISTING)

NOT TO SCALE

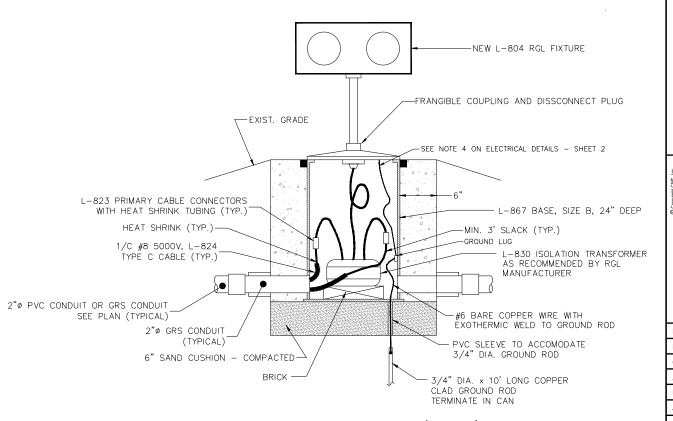


#### ELEVATED RGL LAYOUT DETAIL (TYPICAL)

NOT TO SCALE

#### NOTES

 DISTANCE FROM TAXIWAY EDGE MAY BE INCREASED UP TO A MAXIMUM OF 17' AND A MINIMUM OF 12'. KEEP BOTH FIXTURES AT SAME DISTANCE FROM TAXIWAY PAVEMENT. CONTRACTOR SHALL VERIFY THE LOCATION LAYOUT WITH THE RESIDENT ENGINEER.



#### INSTALLATION OF ELEVATED RGL (TYPICAL)

NOT TO SCALE

IL. CONTRACT: PA063
IL. LETTING ITEM: 08A
IL. PROJECT: PWK-4843
S.B.G. PROJECT:

SURVEY BOOK #

REVISIONS					
NUMBER	BY	DATE			
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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL ALC
ELECTRICAL DETAILS - SHEET 4

NC. REHABILIT

CRAWFORD, MAPPHY & TILTY, IN CONSULTING ENGINEERS
LICENSE No. 184-000613

CHICAGO EXECUTA

 DESIGN BY:
 KWS

 DRAWN BY:
 JRO

 CHECKED BY:
 KWS

 APPROVED BY:
 DKP

 DATE:
 06/25/2021

 JOB No:
 20029004,00

FINAL

SHEET 27 OF 35 SHEETS

(3) EXISTING 10KW (3-STEP) REGULATOR FOR RUNWAY 12/30

(4) EXISTING 10KW (3-STEP) REGULATOR FOR TAXIWAY G,D AND 34 HOLD APRON.

5 EXISTING 10KW (3-STEP) REGULATOR FOR RUNWAY 16/34 RDR

(6) EXITSTING 15KW (3-STEP) REGULATOR FOR RUNWAY 16/34 RGL.

(7) EXISTING 30 KW (5-STEP) REGULATOR (SPARE).

(8) EXISTING 30 KW (3-STEP) REGULATOR FOR A, B, D, E, F, Y AND L4 TO BE REMOVED (SEE NOTE 7).

(9) EXISTING 30KW (5-STEP) SPARE

(10) EXISTING 20KW (5-STEP) REGULATOR FOR RUNWAY 16 APPROACH.

 $(\mathfrak{1})$  existing 30 kw (3–step) regulator for lima to be replaced with new 30kw regulator.

(12) EXISTING 30KW (3-STEP) REGULATOR FOR TAXIWAY K.

(13) EXISTING 10KW (3-STEP) REGULATOR FOR TAXIWAY C.

(14) EXISTING 30kW (3-STEP) FUTURE AIRFIELD LIGHTING REGULATOR.

(15) EXISTING MAIN UTILITY SERVICE CIRCUIT BREAKER DISCONNECT. 800A, 480V, 3-POLE.

(16) EXISTING AUTOMATIC TRANSFER SWITCH. 800A, 480V, 3-POLE. PROVIDE ALCMS INTERFACE.

17 EXISTING HIGH VOLTAGE POWER DISTRIBUTION PANEL. 480V, 3-PHASE WITH 800AMP MAIN CIRCUIT BREAKER (SEE NOTE 3).

(18) EXISTING 150KVA, 480V-280Y/120V, 3Ø, 4-WIRE TRANSFORMER

⑭ EXISTING LOW VOLTAGE LIGHTING PANEL. 208Y/120V, 3−PHASE WITH 400AMP MAIN CIRCUIT BREAKER (SEE NOTE 3).

20 EXISTING PLC CONTROL CABINET TO BE REMOVED.

(21) EXISTING 800AMP CT CABINET

22 EXISTING 12"x12"x12' LONG LOW VOLTAGE WIREWAY.

(23) EXISTING 12"X12"x12' LONG HIGH VOLTAGE WIREWAY.

24 EXISTING 12"x12"x9' LONG HIGH VOLTAGE WIREWAY (SEE NOTE 8).

(25) EXISTING 12"x12"x9' LONG LOW VOLTAGE WIREWAY

(SEE NOTE 7).

 $\ensuremath{\textcircled{\sc D}}$  install new 2-1/c #8 5kV, L-824 cables in existing conduit from New 15kW abef regulator to high voltage wireway, install L-823 connectors.

(28) NEW 15KW, 480V (3-STEP) L-829 FERRORESONANT REGULATOR FOR TAXIWAY A, B, E, F CIRCUIT.

29 NEW 15KW, 480V (3-STEP) L-829 FERRORESONANT REGULATOR FOR TAXIWAY D, Y, Z CIRCUIT.

30 CONNECT NEW REGULATOR TO POWER DISTRIBUTION PANEL.

(3) NEW DISTRIBUTED CONTROL AND MONITORING EQUIPMENT (DCME) MOUNTED ON EXISTING EQUIPMENT PLATE OR WALL. TYPICAL FOR EACH CCR (TOTAL OF 15), A.T.S., LAHSO AND REIL CONTROLLER (TOTAL OF 3)(SEE NOTE 9).

32 NEW (2) #24 AWG, SHIELDED, TWO TWISTED PAIR, BELDEN 9842 OR AS REQUIRED BY ALCMS MANUFACTURER IN 1" GRS CONDUIT (TYPICAL FOR ALL DCME UNITSI).

33 NEW 2 #10 THWN, 1 #10 GND. FOR DCME UNIT UPS POWER OR AS REQUIRED BY ALCMS MANUFACTURER IN 1" GRS CONDUIT (TYPICAL FOR ALL DCME).

(34) NEW 2 #12 THWN, 1 #12 GND. IN 1" GRS CONDUIT TO LOW VOLTAGE LIGHTING PANEL (SEE NOTE 3).

(35) NEW 14 #18 AWG OR AS REQUIRED BY ALCMS IN 3/4" FLEXIBLE CONDUIT (TYPICAL FOR ALL CCR'S)

(36) NEW FIBER OPTIC PATCH PANEL AND FIBER OPTIC JUMPER CABLES AS REQUIRED BY ALCMS MANUFACTURER.

(37) NEW 1-12 STRAND MULTI-MODE FIBER OPTIC CABLE IN EXISTING CONDUIT TO ATCT.

(38) NEW ALCMS RACK (SEE NOTE 2).

(40) NEW 2 #2, 4 #4 THWN, 3 #6 GND. IN EXISTING CONDUIT

(41) NEW 2 #4 THWN, 1 #6 IN FLEXIBLE CONDUIT.

(42) NEW 2 #1/C #8, 5KV L-824 AIRFIELD LIGHTING CABLE IN FLEXIBLE CONDUIT. (SEE NOTE 9).

(43) REMOVE EXISTING LOUVERS, MOTORIZED DAMPERS AND FILTERS AND REPLACE WITH NEW STAIONARY LOUVERS SL-1 AND SL-2 WITH NEW MOTORIZED DAMPERS AND FILTERS. FURNISH AND INSTALL NEW ALUMINUM SLEEVE TO MOUNT MOTORIZED DAMPERS AND FILTERS IN BEHIND LOUVER. SEE STATIONARY LOUVER SCHEDULE SHEET 29.

(4) REMOVE EXISTING LAHSO CONTROLLER AND REPLACE WITH NEW LAHSO CONTROLLER.

(45) NEW 2 #10 THWN, 1 #10 GND. IN 1" CONDUIT TO LIGHTING PANEL

(46) EXISTING L-854 RADIO CONTROLLER, PROVIDE ALCMS INTERFACE.

(47) NEW 2 #2 THWN, 1 #6 GND. IN EXISTING CONDUIT

48 REMOVE EXISTING LOUVER, PLENUM AND EXHAUST FAN. INSTALL NEW ANGLE IRON FRAME AND NEW EXHAUST FANS EF-1 AND EF-2. SEE EXHAUST FAN SCHEDULE SHEET 29. SEE EXHAUST FAN MOUNTING DETAIL SHEET 29.

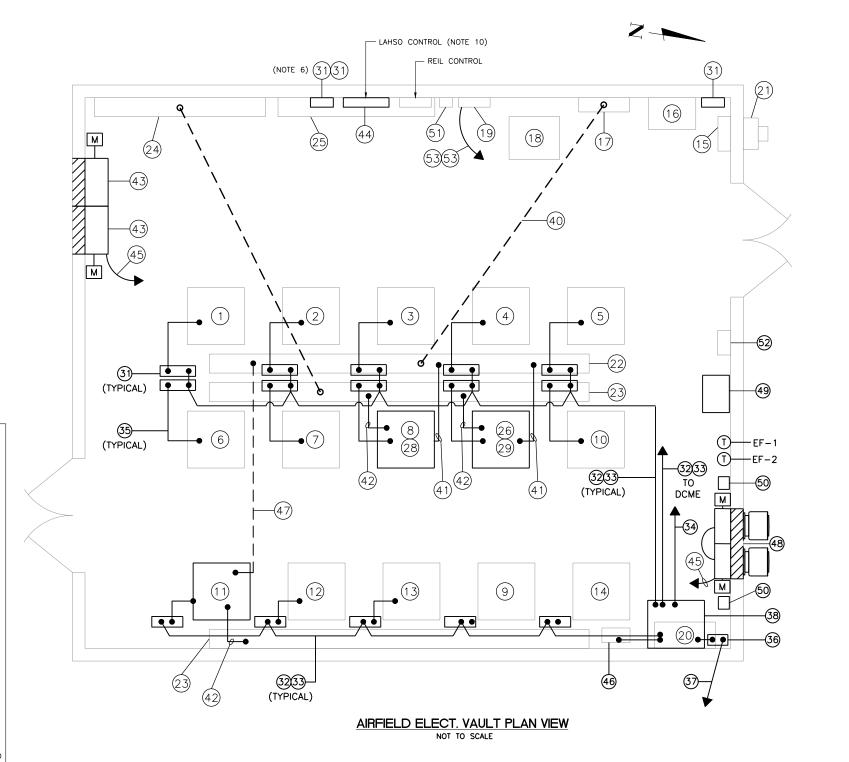
 $\stackrel{\textstyle (49)}{}$  REUSE EXISTING INTERLOCK CONTROL BOX AND CONDUIT TO WIRE NEW LINE VOLTAGE THERMOSTAT CONTROL FROM EF-1 TO SL-1 AND SEPARATE THERMOSTAT CONTROL FROM EF-2 TO SL-2.

(50) MOUNT DISCONNECT AND SPEED CONTROL ADJACENT TO NEW EXHAUST FAN

(5) EXISTING TOWER ROAD STREET LIGHTS SWITCH TO REMAIN

(2) EXISTING TYCO FIRE ALARM AND VIDEO SURVEILLANCE EQUIPMENT TO REMAIN.

(53) NEW 2 #12 THWN, 1 #12 GND. IN 1" CONDUIT TO EXHAUST FAN.



ALL PROPOSED WORK OR ITEMS BEING MODIFIED ARE SHOWN IN BOLD. ALL OTHER ITEMS SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY.

PROPOSED ALCMS RACK SHALL BE SUPPLIED WITH WHEELS. NEW CONTROL SYSTEM SHALL BE OPERATIONAL AND TESTED PRIOR TO THE REMOVAL OF EXISTING PLC CABINET. PROVIDE J-BOX, FLEX CONDUIT AND SUFFICIENT CABLE SLACK REQUIRED FOR ALCMS RACK TO BE OPERATIONAL.

SEE PANEL SCHEDULE FOR REGULATOR, ALCMS AND HVAC CIRCUITS

INSTALL POWER AND CONTROL WIRES BETWEEN INTERFACE CONTROL PANEL AND A.T.S., BEACON LAHSO CONTROL, L-854 RADIO CONTROLLER AND REIL CONTROLLER PER ALCMS MANUFACTURER

5. INSTALL AND WIRE PROPOSED DCME FOR FUTURE REGULATORS.

6. RELOCATE EXISTING WALL MOUNTED TRANSFORMER BELOW LAHSO CONTROLLER. MOUNT (2) ACE—II UNITS ON TOP OF EACH OTHER BETWEEN LAHSO AND REIL CONTROLLERS.

7. REMOVE EXISTING REGULATOR AND INSTALL PROPOSED REGULATOR INSTALL PROPOSED POWER AND CONTROL WIRES IN FLEXIBLE CONDUIT. MATCH EXISTING CONDITIONS.

8. INSTALL L-823 CONNECTORS IN HIGH VOLTAGE WIREWAY.

9. RELOCATE EXISTING INDICATOR UNIT ON EXISTING PANEL TO PROVIDE SPACE

10. CONNECT EXISTING INPUT POWER AND OUTPUT AIRFIELD CIRCUIT TO NEW LAHSO CONTROLLER.

L. CONTRACT: PA063 IL. LETTING ITEM: 08A IL. PROJECT: PWK-4843

URVEY BOOK #

S.B.G. PROJECT:

**REVISIONS** NUMBER

> THIS BAR IS FOLIAL TO 2" AT FULL SCALE (34X22)

ROVEME , ILLINOIS AND INSTALL AGO EXECUTIVE AIRPORT /PROSPECT HEIGHTS, ILLIN ) LIGHTING - PHASE 1 AND <u>≅</u> ∞ ALCM

CTRICAL VA CHICAGO F HEELING/PROS AIRFIELD LIGH Ш 2 AIRFIEI

SO EXE

DESIGN BY JOB No: 20029004.00

**FINAL** 

SHEET 28 OF 35 SHEETS

BOND NEUTRAL AND GROUND BAR: NO NEUTRAL BUS RATING: SERVICE ENTRANCE RATED: NO

MOUNTING: SURFACE

ENCL RATING: NEMA 1

PANELBOARD SCHEDULE

POLE: 30 SHORT CIRCUIT RATING: SERIES OR FULLY RATED: SERIES TVSS & DISCONNECT REQUIRED: NO

BUS RATING (AMPS): 800 BUS: COPPER

	W	RE: 3									MAIN C	IRCUIT B	REAKER: A	MP/POLE	800		
CKT		BREAKER	LOAD	USAGE	PHASE	AMPS (	USAGE)	PC	DLE	PHASE	AMPS (	JSAGE)	USAGE	LOAD	BREAKER		CKT
NO.	LOAD	SIZE	AMPS	FACTOR	Α	В	С	N	Ю.	Α	В	С	FACTOR	AMPS	SIZE		NO.
1			10.2	0.5	5.1			1	2	7.5			0.5	15	100A/2P	RWY 16/34	2
3	TRANSFORMER	225A/3P	10.2	0.5		5.1		3	4		7.5		0.5	15	100A/2F	RWT 10/34	4
5			10.2	0.5			5.1	5	6			7.5	0.5	15	30A/2P	RWY 12/30	6
7	TWY LIMA	100A/2P	6.8	0.5	3.4			7	8	6			0.5	12	00/1/21	111111200	8
9		1001.02	6.8	0.5		3.4		9	10		6		0.5	12	50A/2P	RGL	10
11	TWY DYZ	60A/2P	6.8	0.5			3.4	11	12			6	0.5	12	00/112/		12
13			4.2	0.5	2.1			13	14	10			0.5	20	100A/2P	STAND BY	14
15	TWY AEBF	100A/2P	4.2	0.5		2.1		15	16		10		0.5	20			16
17			4.2	0.5			2.1	17	18			10	0.5	20			18
19			27	0.5	13.5			19	20	10			0.5	20	50A/3P	UNIT HEATER	20
21	UNIT HEATER	50A/3P	27	0.5		13.5		21	22		10		0.5	20			22
23			27	0.5			13.5	23	24			10	0.5	20	60A/2P	RWY 6/24	24
25			3.4	0.5	1.7			25	26	10			0.5	20			26
27	STREET LIGHTS 20A	SHTS 20A/3P	3.4	0.5		1.7		27	28		10		0.5	20	60A/2P	16 APPROACH	28
29			3.4	0.5			1.7	29	30			10	0.5	20			30
31	KILO	100A/2P						31	32						30A/2P	RDR	32
33								33	34								34
35	REIL 12	20A/2P						35	36				8		20A/2P	REIL 16	36
37								37	38								38
39	REIL 30	20A/2P						39	40						20A/2P	REIL 34	40
41								41	42								42
43 45	TWY C	30A/2P		-				43 45	46						20A/2P	LAHSO STEP DOWN TRANSFORMER	44 46
45				l				45	46						1		46
:	SECTION TOTAL:				25.8	25.8	25.8			43.5	43.5	43.5					
								•		A	В	С	-			TOTAL USAGE LOAD:	

#### PANELBOARD SCHEDULE

69.3 69.3 69.3

19196.1 19196.1 19196.1

В

PANEL DESIGNATION: LP LOCATION: VAULT MFR & TYPE: SQUARE D BOND NEUTRAL AND GROUND BAR: NO NEUTRAL BUS RATING: SERVICE ENTRANCE RATED: NO

POLE: 30 SHORT CIRCUIT RATING: SERIES OR FULLY RATED: SERIES TVSS & DISCONNECT REQUIRED: NO

VOLTS: 208Y/120V	MOUNTING: SURFACE	BUS RATING (AMPS): 400
PHASE: 3	ENCL RATING: NEMA 1	BUS: COPPER
WIRE: 4		MAIN CIRCUIT BREAKER: AMP/POLE 400

PHASE TOTAL AMPS:

PHASE TOTAL VA:

PHASE TOTAL AMPS:

PHASE TOTAL VA:

CKT	LOAD	BREAKER	LOAD	USAGE	PHASE	AMPS (	USAGE)	PC	LE	PHASE	AMPS (	USAGE)	USAGE	LOAD	BREAKER	LOAD	CKT
NO.	LOAD	SIZE	AMPS	FACTOR	Α	В	С	N	Э.	Α	В	С	FACTOR	AMPS	SIZE	LOAD	NO.
1	REGULATOR TWY GD , 34 HOLD	20A/2P	0	0	0			1	2	0			0	0	20A/2P	EXHAUST FAN	2
3	REGOLATOR TWT GD , 34 HOLD	20/02/	0	0		0		3	4		0		0	0	20/1/21	EXIAGSTIAN	4
5	GEN- SET-PANEL	100A/2P	0	0			0	5	6			0	0	0	20A/1P	OUTLETS - NORTH - EAST	6
7	GEN- 3E I-PANEL	100A/2F	0	0	0			7	8	0			0	0	20A/1P	LOUVERS - FAN	8
9	OUTLET - WEST	20A/1P	0	0		0		9	10		0		0	0	20A/1P	OUTSIDE LIGHTS	10
11	OUTLET - SOUTH	20A/1P	0	0			0	11	12			0	0	0	20A/1P	VAULT LIGHTS	12
13	ALCMS POWER	20A/1P	0	0	0			13	14	0			0	0	20A/1P	BEACON CONTROLLER	14
15	RADIO CONTROL UNIT	20A/1P	0	0		0		15	16		0		0	0	20A/1P	SPARE	16
17	SPARE	20A/1P	0	0			0	17	18			0	0	0	60A/2P	TRAILER TEMP SERVICE	18
19	ST LIGHTING CONTROL	20A/1P	0	0	0			19	20	0			0	0	00A/2F	TRAILER TEIVIF SERVICE	20
21	LAHSO CONTROL	20A/1P	0	0		0		21	22		0		0	0	60A/2P	RWY 6/24	22
23	BLANK	20A/1P	0	0			0	23	24			0	0	0	00A/2F	RVV1 0/24	24
25	REIL CONTROL	15A/1P	0	0	0			25	26	0			0	0	20A/2P	LAHSO	26
27	EXHAUST FAN	20A/2P				0		27	28		0		0	0	20A/2P	LARSO	28
29	EXTAUST FAN	20A/2P					0	29	30			0			15A/1P	DAMPER MOTORS	30
	SECTIO	N TOTAL:		•	0	0	0			0	0	0					
										Α	В	С	-			TOTAL USAGE LOAD:	

#### POWER DISTRIBUTION NOTES

- CONNECT NEW TAXIWAY LIMA REGULATOR CABLES TO EXISTING CIRCUIT BREAKER.
- REPLACE EXISTING 60A SPARE CIRCUIT BREAKER WITH NEW 50A, 2-POLE BREAKER FOR TAXIWAY D,Y,Z REGULATOR.
- 3. REPLACE EXISTING 100A TAXIWAY A,B,E,F CIRCUIT BREAKER WITH NEW 50A, 2-POLE BREAKER FOR TAXIWAY A,B,E,F REGULATOR.

<b>EXHA</b>	USIF	AN SCHED	ULE
MARK		EF-1	EF-2
SERVICE		VAULT	VAULT
LOCATION	1	SIDEWALL	SIDEWALL
MANUFAC	TURER	GREENHECK	GREENHECK
MODEL		CUE-180-VG	CUE-180-VG
TYPE		SIDEWALL	SIDEWALL
CFM		4000	4000
S.P.		0.75"	0.75"
FAN RPM		1239	1239
	HP	2	2
MOTOR	VOLT	208	208
	PHASE	1	1
	RPM	1325	1325
ACCESSO	RIES	SEE NOTES	
		1 THRU 6	1 THRU 6
REMARKS	3	INTERLOCK WITH	INTERLOCK WITH
		SL-1	SL-2

- - 2. ALUMINUM BIRDSCREEN
  - 3. LOW LEAK INSULATED 115V MOTORIZED DAMPER
  - 4. SPEED CONTROL
  - 5. LINE VOLTAGE COOLING THERMOSTAT
  - 6. MAKE/MODEL NUMBER IS SHOWN FOR INFORMATION ONLY. APPROVED EQUAL EQUIPMENT SHALL BE ACCEPTABLE.

STATI	ONAR	Y LOUVER S	CHEDULE		
MARK		SL-1	SL-2		
SERVICE		VAULT	VAULT		
TYPE		DRAINABLE	DRAINABLE		
MANUFACT	TURER	GREENHECK	GREENHECK		
MODEL		ESD-635	ESD-635		
CFM		4000	4000		
MAX. ∆ S.P	. (IN)	0.1	0.1		
	WIDTH	36	36		
NOM. SIZE	HEIGHT	32	32		
	DEPTH	6"	6"		
AREA	NET	7.50	7.50		
(SQ. FT.)	FREE	3.845	3.845		
FRAME		CHANNEL	CHANNEL		
MATERIAL		ALUMINUM	ALUMINUM		
FINISH		KYNAR	KYNAR		
ACCESSO	RIES	SEE NOTES	SEE NOTES		
		1 THRU 7	1 THRU 7		
REMARKS		INTERLOCK WITH	INTERLOCK WITH		
		EF-1	EF-2		

NOTES: 1. COLOR TO BE SELECTED BY OWNER 2. BIRD SCREEN

- 3. EXTENDED SILL
- 4. 115V LOW LEAK INSULATED MOTORIZED DAMPER
- 5. WASHABLE FILTER
- MAKE/MODEL NUMBER IS SHOWN FOR INFORMATION ONLY, APPROVED EQUAL EQUIPMENT SHALL BE ACCEPTABLE.

EXHAUST FAN SCHEDULE					
MARK		EF-1	EF-2		
SERVICE		VAULT	VAULT		
LOCATION	l	SIDEWALL	SIDEWALL		
MANUFAC	TURER	GREENHECK	GREENHECK		
MODEL		CUE-180-VG	CUE-180-VG		
TYPE		SIDEWALL	SIDEWALL		
CFM		4000	4000		
S.P.		0.75"	0.75"		
FAN RPM		1239	1239		
	HP	2	2		
MOTOR	VOLT	208	208		
	PHASE	1	1		
	RPM	1325	1325		
ACCESSO	RIES	SEE NOTES	SEE NOTES		
		1 THRU 6	1 THRU 6		
REMARKS	3	INTERLOCK WITH	INTERLOCK WITH		

NOTES: 1. WEATHER TIGHT DISCONNECT SWITCH

INSULATE INSIDE OF FRAME WITH 2" RIGID INSULATION AND SKIN WITH 16 GAUGE SHEET METAL.	© Copyright CMT, Inc.
PRAME TO ATTACH EXHAUST FAN TO SLEEVE	Σ
1. X 8' PAINTED PLATE STEEL AROUND PERIMETER DE EXISTING 72"X32' DENING. ATTACH TO WALL. FIELD VERIFY EXACT OPENING SIZE.	\ \( \)
	7
( EF-1 ) ( EF-2 )	DESIG
	DRAW
	CHEC
	APPRO
	DATE:
SKIN THE EXTERIOR OF FRAME WITH 16 GAUGE SHEET METAL	JOB N
AND SEAL WATER TIGHT.	
EXHAUST FAN DETAIL	

IL. CONTRACT: PA063 IL. LETTING ITEM: 08A

IL. PROJECT: PWK-4843 S.B.G. PROJECT:

**REVISIONS** 

BY

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

ALCMS

CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL

PANEL SCHEDULE

DATE

SURVEY BOOK #

NUMBER

SIGN BY: AB WN BY: JRO CKED BY: AB PROVED BY: DKP 06/25/2021 20029004.00

**FINAL** 

SHEET 29 OF 35 SHEETS

CHICAGO EXECUTIVE CONTROLLE CONTROLL

## LIGHTING PANEL NOTES

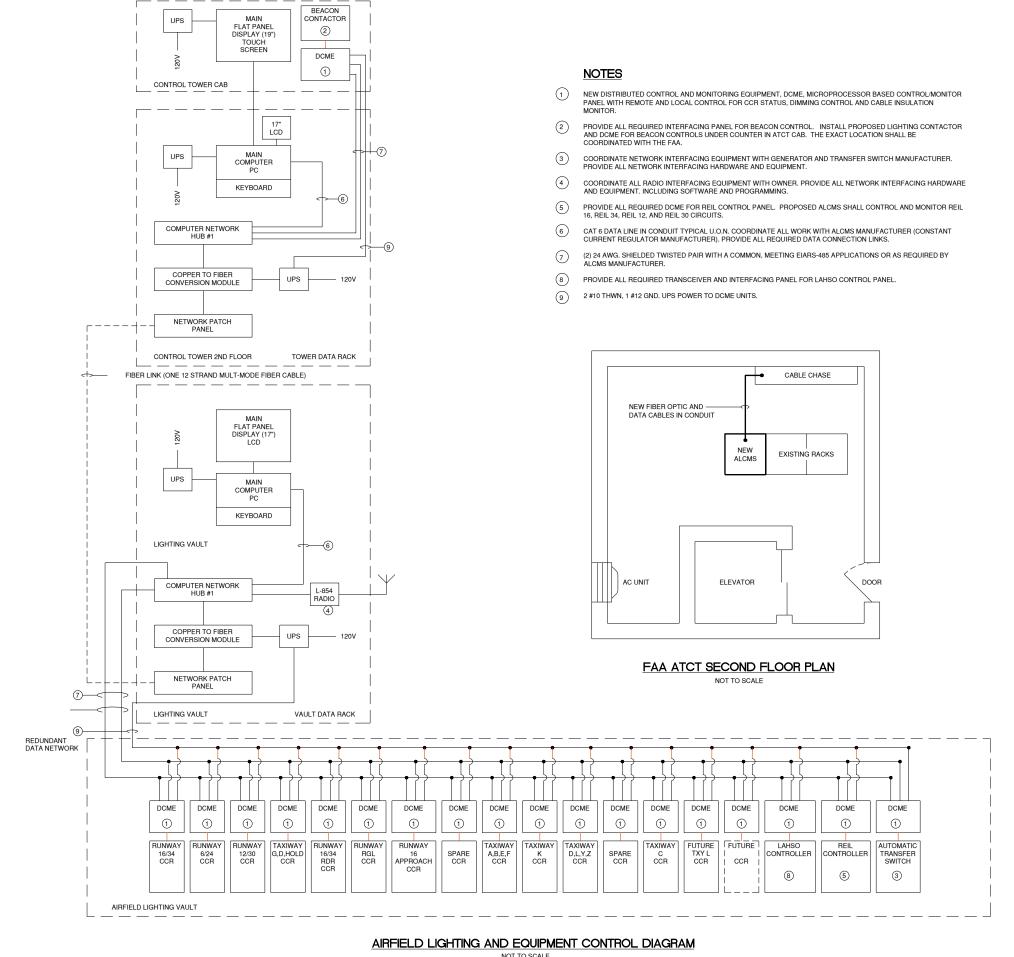
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0 VA

- REPLACE EXISTING 20A PLC-POWER CIRCUIT BREAKER WITH NEW 20A, 1-POLE BREAKER FOR ALCMS.
- 2. REPLACE HFC CIRCUIT BREAKER WITH 20A, 2-POLE BREAKER FOR EXHAUST FAN.

REVISIONS									
	NUMBER	BY	DATE						
6									

SHEET 30 OF 35 SHEETS



IL. CONTRACT: PA063 IL. LETTING ITEM: 08A IL. PROJECT: PWK-4843 S.B.G. PROJECT: SURVEY BOOK # **REVISIONS** NUMBER BY DATE

> THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

> > ₹

**BLOCK DIAGRAM** EQUIPMENT

**ALCMS** 

SO EXECUTI

CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL

JRPHY & ... L W U

DESIGN BY DRAWN BY CHECKED BY APPROVED BY 06/25/2021 JOB No: 20029004.00

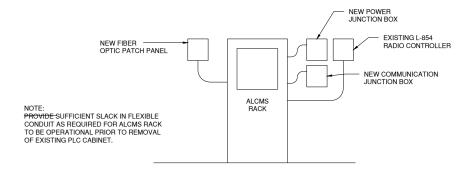
**FINAL** SHEET 31 OF 35 SHEETS

#### **NOTES**

- INSTALL NEW DISTRIBUTED CONTROL AND MONITORING EQUIPMENT (DCME) ON UNITSTRUT. MATCH EXISTING CONDITIONS. COORDINATE SIZE AND CONDUIT OPENINGS WITH ALCMS MANUFACTURER.
- INSTALL MALE AND FEMALE L-823 CONNECTOR TO CONNECT AIRFIELD LIGHTING CIRCUITS IN HIGH VOLTAGE WIREWAY.

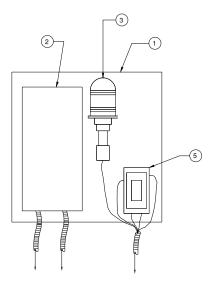
#### EQUIPMENT NOMENCLATURE

- EXISTING EQUIPMENT MOUNTING PLATE TO REMAIN.
- 2 EXISTING REGULATOR DISCONNECT SWITCH TO REMAIN.
- 3 EXISTING RUNWAY/TAXIWAY INDICATOR LIGHT TO BE RELOCATED.
- 4 RELOCATED S-1 CUT-OUT.
- 5 EXISTING S-1 PLUG CUT-OUT TO BE RELOCATED.
- 6 NEW DISTRIBUTED CONTROL AND MONITORING EQUIPMENT (DCME) SEE NOTE 1.
- $\begin{picture}(60,0)\put(0,0){\line(1,0){10}} \put(0,0){\line(1,0){10}} \put(0,0$
- 8 EXISTING LOW VOLTAGE AND HIGH VOLTAGE 12"x12" WIREWAY SEE NOTE 2.
- (9) EXISTING INCOMING AND OUTGOING 600V POWER CABLES IN FLEX CONDUIT TO LOW VOLTAGE WIREWAY.
- (10) EXISTING 4 #8 5KV AIRFIELD LIGHTING CABLES IN FLEX CONDUIT TO HIGH VOLTAGE WIREWAY.
- (11) NEW (2) 24 AWG SHIELDED TWISTED PAIRS OR AS REQUIRED BY ALCMS MANUFACTURER IN 1" GRS CONDUIT.
- 12 NEW 1" TX CONDUIT BODY WITH GASKET AND COVER.
- (13) NEW 2 #8 5KV, L-824 TYPE C AIRFIELD LIGHTING CABLES IN 1" FLEX CONDUIT TO HIGH VOLTAGE WIREWAY.
- (14) NEW 2 #10 THWN, 1 #12 GND. IN 3/4" FLEX CONDUIT TO LOW VOLTAGE WIREWAY FOR UPS POWER.
- (15) NEW 2 #8 5KV, L-824 TYPE C CABLES IN 1" FLEX CONDUIT TO S-1 PLUG CUT-OUT.
- (16) NEW 2 #10 THWN, 1 #12 GND. IN 1" GRS CONDUIT.



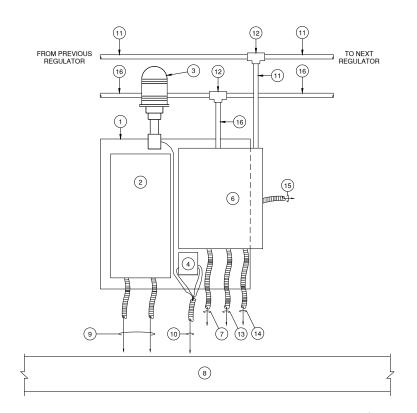
#### VAULT ALCMS RACK ELEVATION

NOT TO SCALE



#### **EXISTING EQUIPMENT MOUNTING PLATE DETAIL**

NOT TO SCALE



## TYPICAL INTERFACE CONTROL AND IRMS PANEL (ACE-II) MOUNTING DETAIL

NOT TO SCALE

IL. CONTRACT: PA063
IL. LETTING ITEM: 08A
IL. PROJECT: PWK-4843
S.B.G. PROJECT:

SURVEY BOOK #

REVISIONS								
NUMBER	BY	DATE						

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL
ALCMS DETAILS

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PHY & TILLY, INC.
VEERS

CRAWFORD, MURPHY & TILL CONSULTING ENGINEERS LICERSE No. 184-000613

DESIGN BY: AB

DRAWN BY: JRO

CHECKED BY: AB

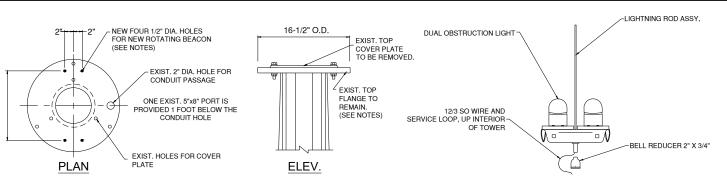
APPROVED BY: DKP

DATE: 06/25/2021

JOB No: 20029004.00

FINAL

SHEET 32 OF 35 SHEETS

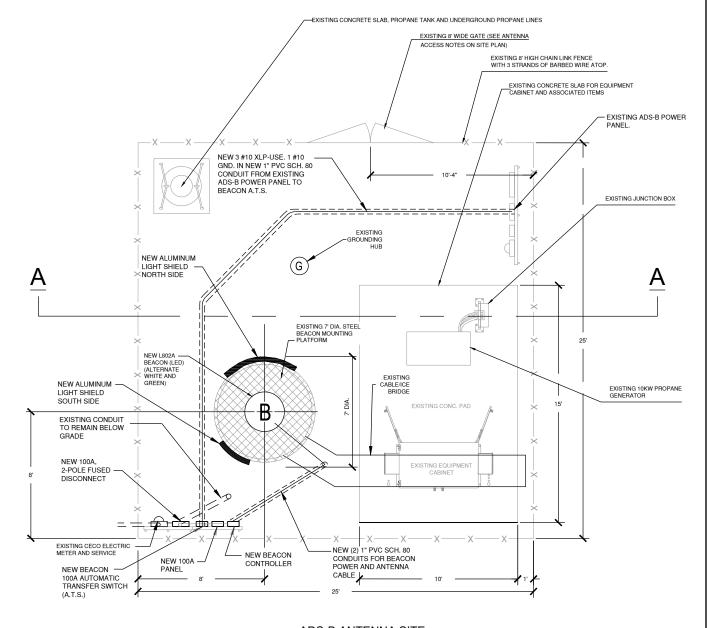


#### EXISTING TOP FLANGE DETAIL

THE EXISTING TOP FLANGE BOLT PATTERN SHALL BE MODIFIED PER MANUFACTURER RECOMMENDATIONS, CONTRACTOR SHALL VERIFY CORRECT BOLT PATTERN AND REQUIREMENTS FOR NEW BEACON.

#### **EXISTING FAA OBSTRUCTION LIGHT MOUNT DETAIL** (TO BE RELOCATED)

- 1. THE RELOCATION OF THE EXISTING FAA OBSTRUCTION LIGHTS SHALL BE INCIDENTAL TO THE NEW BEACON PAY ITEM.
- 2. OBSTRUCTION LIGHT SHALL BE ON 24 HOURS (DAY AND NIGHT).



#### ADS-B ANTENNA SITE

NOT TO SCALE

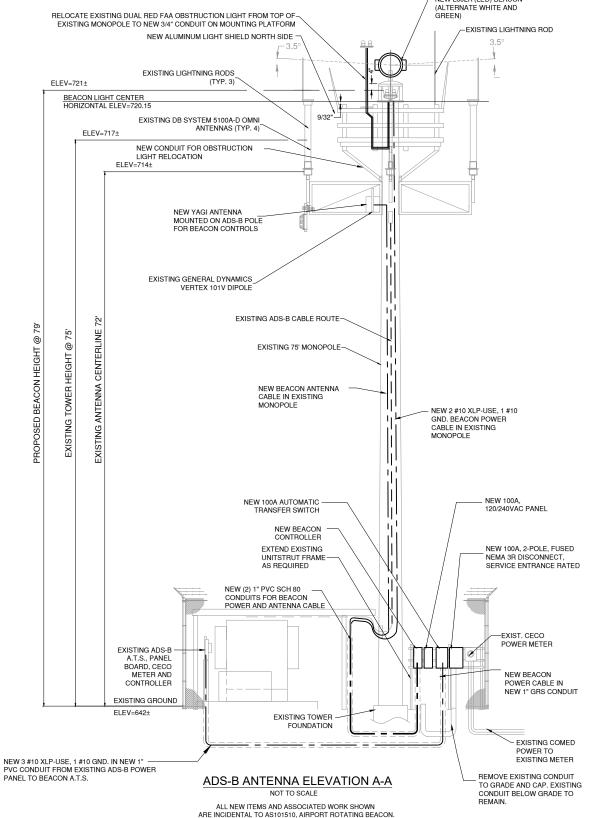
ALL NEW ITEMS AND ASSOCIATED WORK SHOWN ARE INCIDENTAL TO AS101510, AIRPORT ROTATING BEACON.

#### NOTES:

- ANY EXISTING UTILITY, CABLE, GROUNDING, AND CONDUIT LOCATIONS SHALL BE LOCATED BY THE CONTRACTOR PRIOR TO NEW INSTALLATION. LOCATIONS NOT SHOWN FOR CLARITY
- ALL EXISTING UTILITY VERIFICATION AND NEW UNDERGROUND INSTALLATION SHALL BE COMPLETED BY HAND DIGGING TRENCHES.
- NEW L802A BEACON (LED) SHALL BE HALI-BRITE L802AL216 CIVILIAN VERSION. 120 VAC, 60 HZ, CLASS II, OR APPROVED EQUAL
- THE MOUNTING PLATFORM SHALL BE HALL-BRITE 440C98-1 7 FOOT DIA. ROUND BEACON BASKET OR APPROVED EQUAL. ALL PLATFORM SUPPORTS AND INSTALLATION SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
- THE NEW LIGHT SHIELDS SHALL PROTECT GLARE FROM THE NEW BEACON TO THE FAA AIR TRAFFIC CONTROL TOWER AND THE RESIDENCE TO THE NORTH OF HINTZ ROAD AS SHOWN ON SHEET 3, SEQUENCE OF CONSTRUCTION. THE LIGHT SHIELDS SHALL BE VARIOUS SIZED ALUMINUM PANELS OR APPROVED EQUAL AND SHALL BE INSTALLED TO THE PLATFORM TO THE SATISFACTION OF THE AIRPORT AND ENGINEER. AFTER INSTALLATION, THE CONTRACTOR MAY NEED TO ADJUST THE SHIELDS UP TO TWO TIMES TO THE SATISFACTION OF THE FAA TOWER CHIEF, THE AIRPORT AND THE ENGINEER.
- THE BEACON LIGHT BEAM ANGLE SHALL BE SET TO 3.5 DEGREES ABOVE HORIZONTAL OR AS DETERMINED BY THE AIRPORT.

COORDINATE ALL WORK AT ADSB TOWER WITH L3 HARRIS TECHNOLOGIES, INC.

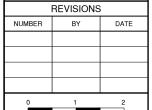
NEW I 802A (LED) BEACON RELOCATE EXISTING DUAL RED FAA OBSTRUCTION LIGHT FROM TOP OF



L. CONTRACT: PA063 IL. LETTING ITEM: **08A** 

IL. PROJECT: PWK-4843 S.B.G. PROJECT:

URVEY BOOK #



THIS BAR IS FOLIAL TO 2" AT FULL SCALE (34X22).

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GO EXECUTIVE AIRPORT PROSPECT HEIGHTS, ILLINOIS LIGHTING - PHASE 1 AND INSTALL

CHICAGO WHEELING/PRO REHABILITATE AIRFIELD LIGI

ALTERNATE #1 LOCATION PLAN

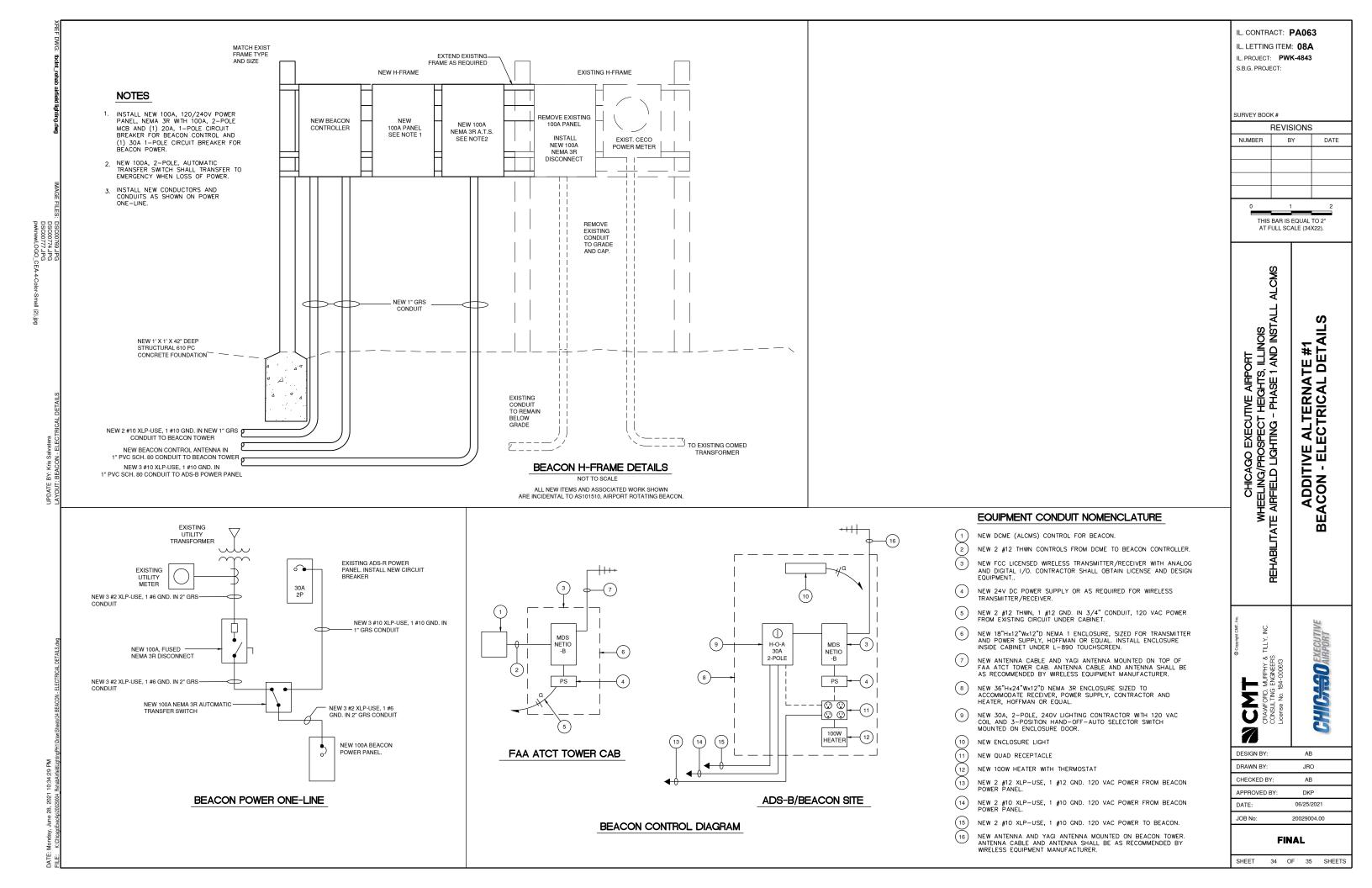
ADDITIVE A

CHICAGO EXECUTI RPHY & GINEERS

DESIGN BY AB DRAWN BY JRO CHECKED BY APPROVED BY DKP 06/25/2021 JOB No: 20029004.00

**FINAL** 

SHEET 33 OF 35 SHEETS

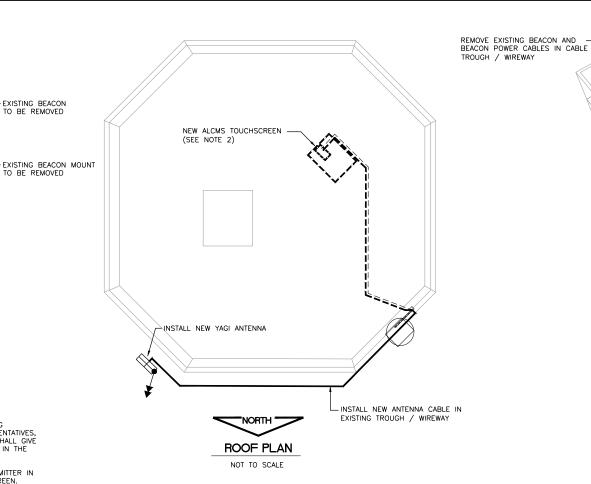




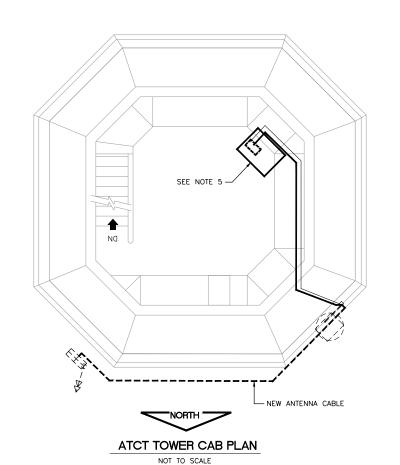
EXISTING BEACON

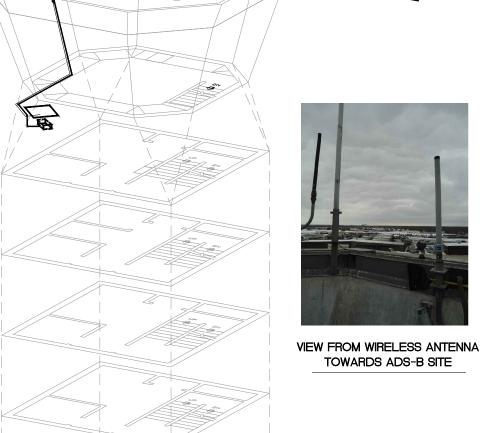
#### NOTES:

- CONTRACTOR SHALL COORDINATE ALL WORK IN THE EXISTING CONTROL TOWER WITH THE FAA AIRWAYS FACILITIES REPRESENTATIVES, THE AIRPORT AND THE RESIDENT ENGINEER. CONTRACTOR SHALL GIVE A MINIMUM OF 7 DAYS NOTICE PRIOR TO BEGINNING WORK IN THE EXISTING TOWER.
- CONTRACTOR SHALL INSTALL NEW BEACON WIRELESS TRANSMITTER IN ENCLOSURE INSIDE CABINET UNDER NEW ALCMS TOUCH SCREEN. CONNECT DIGITAL INPUT FROM BEACON PUSH BUTTON TO WIRELESS TRANSMITTER.
- 3. CONTRACTOR SHALL REMOVE EXISTING BEACON AND ASSOCIATED CONDUCTORS AFTER NEW BEACON IS OPERATIONAL.
- 4. CONTRACTOR TO PERFORM RADIO PATH STUDY FOR BEACON WIRELESS FOR ICC LICENSED COMMUNICATION AND ADJUST ANTENNA TYPE, HEIGHT AND DIRECTION AS NECESSARY, INCIDENTAL TO AS101510.
- 5. CONTRACTOR SHALL INSTALL NEW BEACON CONTROLLER, DCME, CONTROLLER POWER, ANTENNA, ANTENNA CABLE AND REMOVAL OF EXISTING BEACON TO PROVIDE COMPLETE AND OPERATIONAL SYSTEM TO THE SATISFACTION OF THE TOWER CHIEF, AIRPORT AND ENGINEER. ALL WORK TO BE PAID UNDER AS101510.
- CONTRACTOR SHALL OBTAIN FCC LICENSE FROM RADIO CONTROL RADIO SERVICE (RCRS) FOR AUTHORIZED FREQUENCY TO OPERATE RADIO CONTROL. NEW WIRELESS EQUIPMENT SHALL BE DESIGNED TO OPERATE ON SPECIFIC FREQUENCY.



ALL NEW ITEMS AND ASSOCIATED WORK SHOWN ARE INCIDENTAL TO AS101510, AIRPORT ROTATING BEACON.





IL. CONTRACT: PA063
IL. LETTING ITEM: 08A
IL. PROJECT: PWK-4843
S.B.G. PROJECT:

SURVEY BOOK #

INSTALL NEW YAGI ANTENNA AND CABLES. ANTENNA MOUNTING SHALL BE AS RECOMMENDED BY MANUFACTURER

REVISIONS						
NUMBER	BY	DATE				

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

AT FULL SCALE (34X22).

ADDITIVE ALTERNATE #1
BEACON - ATCT MODIFICATIONS

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE AIRFIELD LIGHTING - PHASE 1 AND INSTALL

CRAWFORD, MAPRHY & TILLY, N CONSULTING ENGINEERS License No. 184-000613

DESIGN BY: AB

DRAWN BY: JRO

CHECKED BY: AB

APPROVED BY: DKP

DATE: 06/25/2021

JOB No: 20029004.00

FINAL
SHEET 35 OF 35 SHEETS

ATCT ELEVATION

NOT TO SCALE