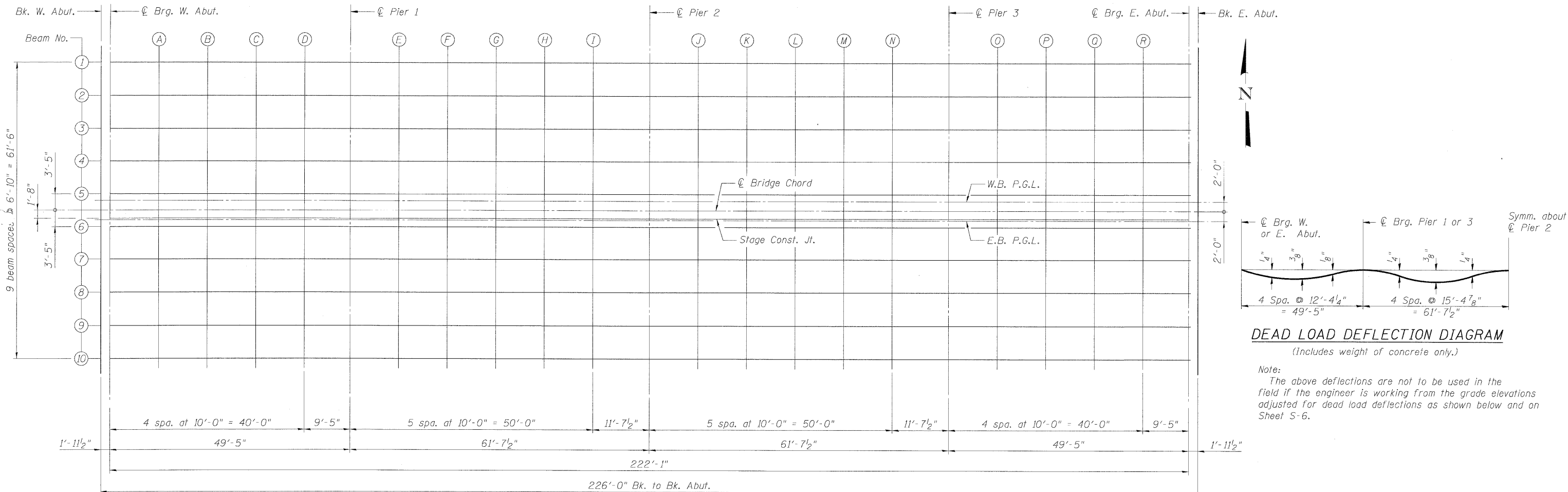


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



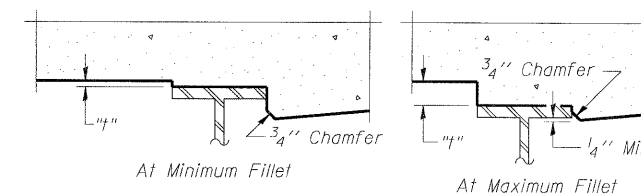
PLAN

BEAM 1

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BK. W. ABUT	1887.00	-30.75	657.94	657.94
☉ BRG. W. ABUT	1888.96	-30.75	657.95	657.95
A	1898.96	-30.75	658.02	658.04
B	1908.96	-30.75	658.08	658.11
C	1918.96	-30.75	658.14	658.16
D	1928.96	-30.75	658.19	658.20
☉ PIER 1	1938.38	-30.75	658.23	658.23
E	1948.38	-30.75	658.27	658.28
F	1958.38	-30.75	658.30	658.33
G	1968.38	-30.75	658.33	658.36
H	1978.38	-30.75	658.35	658.38
I	1988.38	-30.75	658.36	658.38
☉ PIER 2	2000.00	-30.75	658.37	658.37
J	2010.00	-30.75	658.37	658.38
K	2020.00	-30.75	658.36	658.39
L	2030.00	-30.75	658.35	658.39
M	2040.00	-30.75	658.33	658.36
N	2050.00	-30.75	658.31	658.32
☉ PIER 3	2061.63	-30.75	658.27	658.27
O	2071.63	-30.75	658.23	658.24
P	2081.63	-30.75	658.19	658.21
Q	2091.63	-30.75	658.14	658.16
R	2101.63	-30.75	658.08	658.10
☉ BRG. E. ABUT	2111.05	-30.75	658.02	658.02
BK. E. ABUT	2113.00	-30.75	658.00	658.00

BEAM 2

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BK. W. ABUT	1887.00	-23.92	658.04	658.04
☉ BRG. W. ABUT	1888.96	-23.92	658.06	658.06
A	1898.96	-23.92	658.13	658.15
B	1908.96	-23.92	658.19	658.22
C	1918.96	-23.92	658.25	658.27
D	1928.96	-23.92	658.30	658.31
☉ PIER 1	1938.38	-23.92	658.34	658.34
E	1948.38	-23.92	658.38	658.39
F	1958.38	-23.92	658.41	658.44
G	1968.38	-23.92	658.44	658.47
H	1978.38	-23.92	658.46	658.49
I	1988.38	-23.92	658.47	658.48
☉ PIER 2	2000.00	-23.92	658.48	658.48
J	2010.00	-23.92	658.48	658.49
K	2020.00	-23.92	658.47	658.50
L	2030.00	-23.92	658.46	658.49
M	2040.00	-23.92	658.44	658.47
N	2050.00	-23.92	658.41	658.43
☉ PIER 3	2061.63	-23.92	658.38	658.38
O	2071.63	-23.92	658.34	658.35
P	2081.63	-23.92	658.29	658.32
Q	2091.63	-23.92	658.24	658.27
R	2101.63	-23.92	658.18	658.20
☉ BRG. E. ABUT	2111.05	-23.92	658.12	658.12
BK. E. ABUT	2113.00	-23.92	658.11	658.11



To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown below and on Sheet S-6. These elevations subtracted from the "Theoretical Grade Elevations Adjusted For Dead Load Deflection" shown below and on Sheet S-6, minus slab thickness, equals the fillet heights "t" above top flange of beams.

FILLET HEIGHTS

DESIGNED - TL
CHECKED - DF
DRAWN - LAM
CHECKED - DF

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TOP OF SLAB ELEVATIONS LAYOUT
STRUCTURE NO. 016-0275

SHEET NO. S-5	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0353	0303.1B-1	COOK	91	50
S-30 SHEETS		SN 016-0275	CONTRACT NO. 60C05		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			