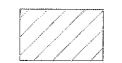


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LEGEND



Hatched area to be poured after superstructure false work has been removed. Quantity of concrete included with Concrete Superstructure.

W. ABUT. BILL OF MATERIAL

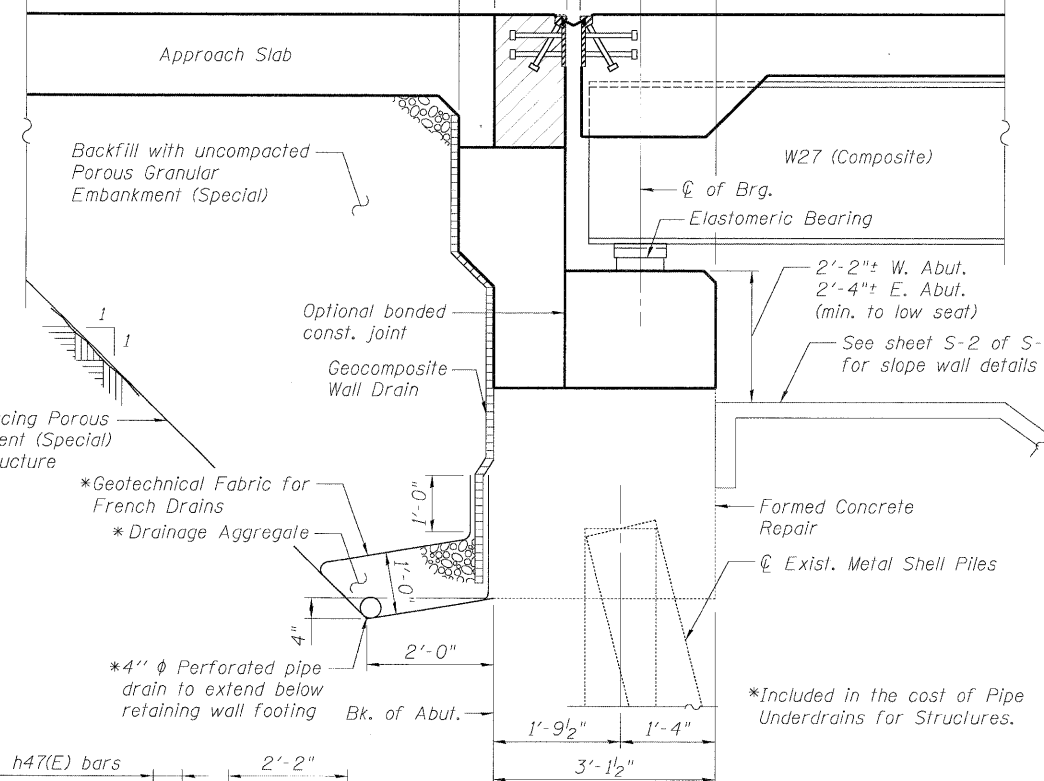
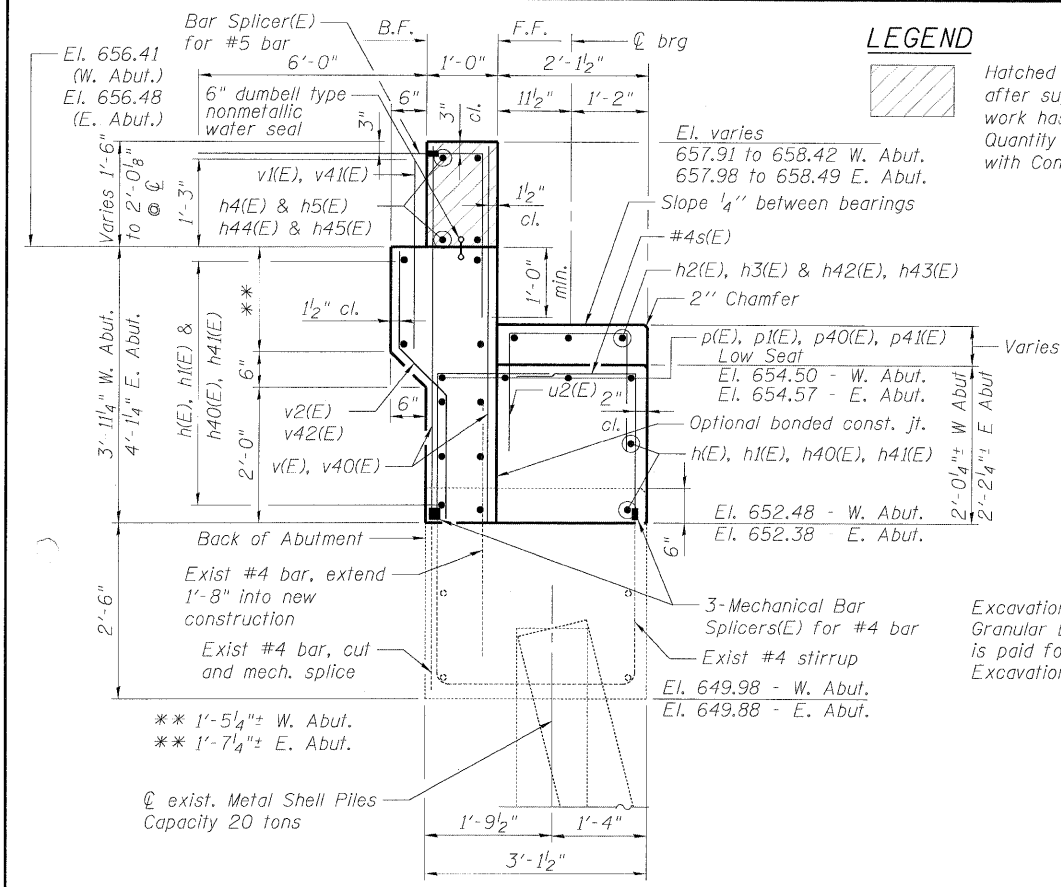
Bar	No.	Size	Length	Shape
d1(E)	6	#5	7'-4"	L
h(E)	12	#5	32'-0"	—
h1(E)	12	#5	35'-4"	—
h2(E)	3	#5	15'-0"	—
h3(E)	3	#5	11'-8"	—
h4(E)	4	#6	30'-7"	—
h5(E)	4	#6	33'-9"	—
h6(E)	4	#5	3'-11"	—
h7(E)	16	#5	3'-0"	—
p(E)	4	#7	32'-0"	—
p1(E)	4	#7	35'-4"	—
s(E)	84	#4	4'-2"	—
u(E)	10	#5	9'-2"	—
u1(E)	6	#5	10'-8"	—
u2(E)	29	#4	5'-2"	—
v(E)	138	#4	5'-1"	—
v1(E)	69	#5	2'-8"	—
v2(E)	69	#4	3'-10"	—
v3(E)	6	#4	4'-9"	—
v4(E)	4	#4	3'-11"	—
Item	Unit	Quantity		
Bar Splicers	Each	92		
Structure Excavation	Cu. Yd.	95		
Concrete Structures	Cu. Yd.	30.5		
Reinforcement Bars, Epoxy Coated	Pound	3350		
Concrete Sealer	Sq. Ft.	535		
Structural Repair of Concrete (Depth equal to or less than 5")	Sq. Ft.	15		
Mechanical Bar Splicers	Each	153		
Geocomposite Wall Drain	Sq. Yd.	55		

E. ABUT. BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d40(E)	6	#5	7'-4"	L
h40(E)	12	#5	32'-0"	—
h41(E)	12	#5	35'-4"	—
h42(E)	3	#5	15'-0"	—
h43(E)	3	#5	11'-8"	—
h44(E)	4	#6	30'-7"	—
h45(E)	4	#6	33'-9"	—
h46(E)	4	#5	3'-11"	—
h47(E)	16	#5	3'-0"	—
p40(E)	4	#7	32'-0"	—
p41(E)	4	#7	35'-4"	—
s40(E)	84	#4	4'-4"	—
u40(E)	10	#5	9'-2"	—
u41(E)	6	#5	10'-8"	—
u42(E)	29	#4	5'-2"	—
v40(E)	138	#4	5'-3"	—
v41(E)	69	#5	2'-10"	—
v42(E)	69	#4	4'-0"	—
v43(E)	6	#4	4'-11"	—
v44(E)	4	#4	4'-1"	—
Item	Unit	Quantity		
Bar Splicers	Each	92		
Structure Excavation	Cu. Yd.	95		
Concrete Structures	Cu. Yd.	31.6		
Reinforcement Bars, Epoxy Coated	Pound	3390		
Concrete Sealer	Sq. Ft.	550		
Structural Repair of Concrete (Depth equal to or less than 5")	Sq. Ft.	3		
Mechanical Bar Splicers	Each	153		
Geocomposite Wall Drain	Sq. Yd.	55		

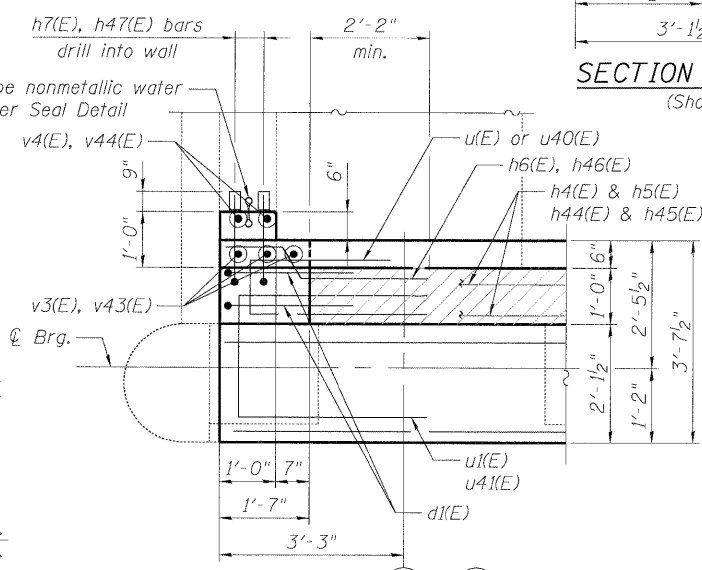
SECTION THRU ABUTMENT

(Showing Reinforcement)



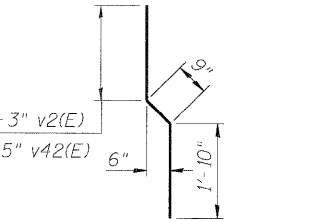
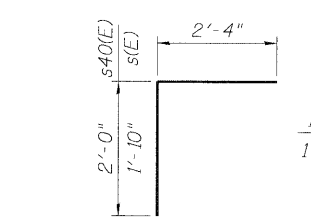
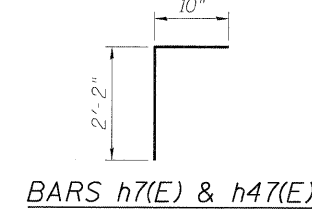
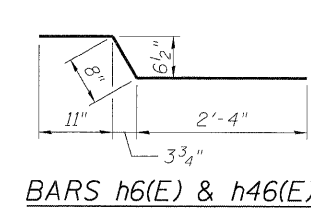
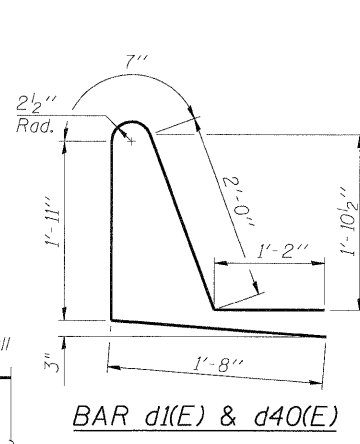
SECTION THRU ABUTMENT

(Showing Drainage)



DETAIL 1

Epoxy new Water Seal into 2" wide sawcut. Epoxy to meet the requirements of ASTM C881, Type III, Grade 3. Prime Groove with an Epoxy meeting ASTM C881, Type III, Grade 1.



BAR d1(E) & d40(E)

BAR s(E) & s40(E)

BAR v2(E) & v42(E)

BAR u(E) & u40(E)

BAR u1(E) & u41(E)

BAR u2(E) & u42(E)

NOTES

- Existing reinforcement extending into new construction shall be cleaned, straightened and incorporated into the new construction. Cost included with Concrete Removal.
- For details of Bar Splicers, see sheet S-28 of S-30.
- All drainage system components shall extend along length of abutments from wingwall to wingwall except a 4" φ outlet pipe shall sleeve below wingwall footing and extend until intersecting with the side slope (20 ft). The pipes shall drain into concrete headwalls (See Special Provisions and Article 601.05 of the Standard Specifications and Highway Standard 601101). Cost included with Pipe Underdrains for Structures.

WATER SEAL DETAIL AT WINGWALL

Costs of water seal, epoxy grout, joint filler, concrete nails and saw cutting included with Concrete Structures.

ABUTMENT DETAILS

STRUCTURE NO. 016-0275

DESIGNED - TAH
CHECKED - DF
DRAWN - LAM
CHECKED - TAH

END VIEW ABUTMENT

For additional reinforcement see above abutment detail

BOWMAN, BARRETT & ASSOCIATES INC.
CONSULTING ENGINEERS
Chicago, Illinois
312.228.0100
www.bbandainc.com
Job No. 910

SHEET NO. S-22	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0353	0303.1B-1	COOK	91	67
S-30 SHEETS	SN 016-0275		CONTRACT NO. 60C05		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			