

T.B.M. "10" - TOP VALVE STEM FIRE HYD.
 STA. 16+429±, 46m± RT.
 ELEV.= 238.391

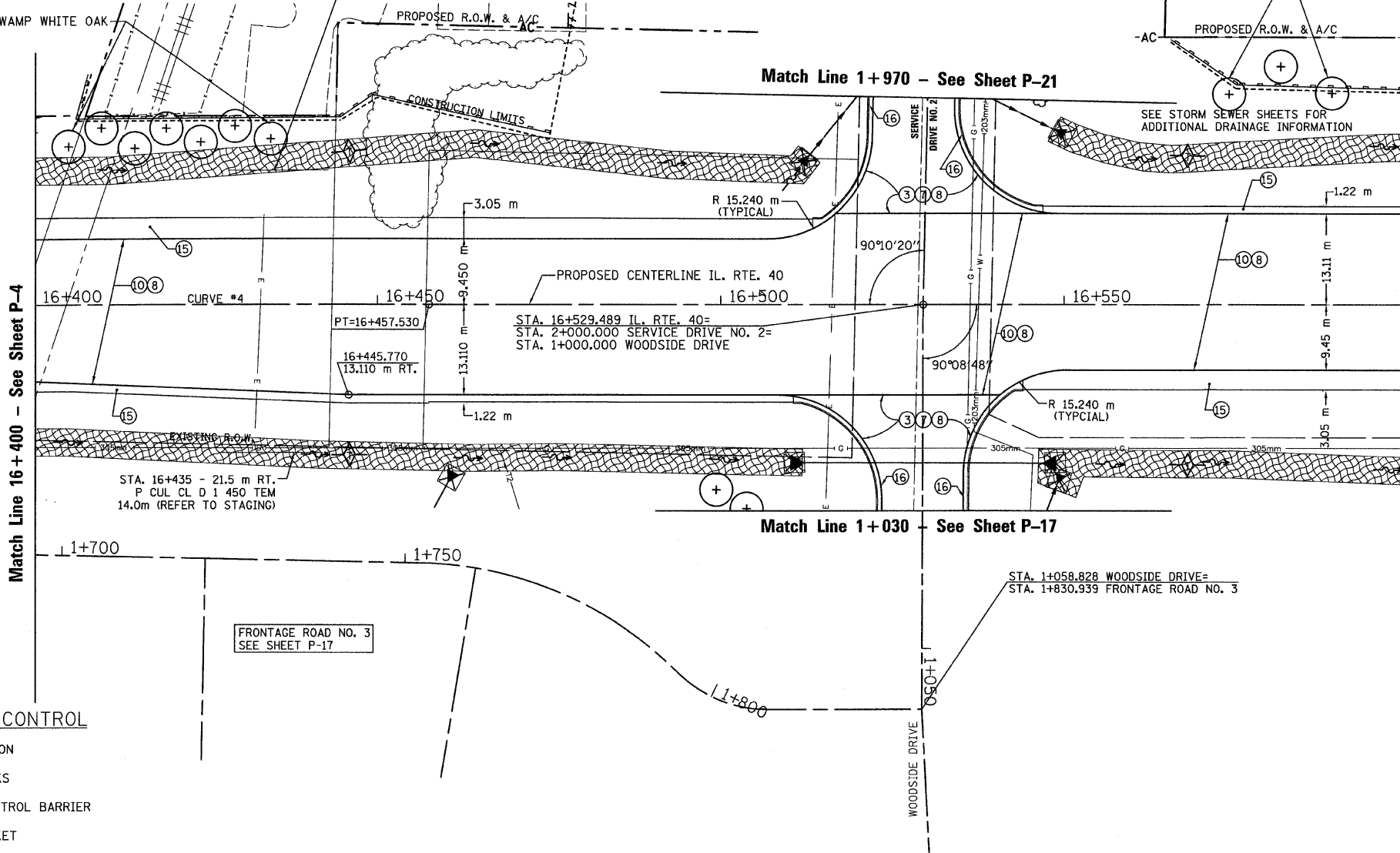
STA. 16+435 - 19.4 m LT.
 P CUL CL D 1 450 TEM
 14.0 m (REFER TO STAGING)

SEC. 20 T.10N., R.8E., 4th P.M.

RTE NO.	SECTION	COUNTY	TOTAL	SHEET #
FA 646	125W-1, RS-2	PEORIA	256	92
STATION 16+400 TO STATION 16+600				

7 SWAMP WHITE OAK

3 BUR OAK



LEGEND

- ① FABRIC FORMED CONCRETE REVETMENT MAT
- ② AGGREGATE BASE COURSE
- ③ HOT-MIX ASPHALT BASE COURSE
- ④ HOT-MIX ASPHALT BASE COURSE WIDENING
- ⑤ AGGREGATE SURFACE COURSE
- ⑥ LEVELING BINDER (MACHINE METHOD)
- ⑦ HOT-MIX ASPHALT BINDER COURSE
- ⑧ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE
- ⑨ HOT-MIX ASPHALT REMOVAL-BUTT JOINT
- ⑩ HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH)
- ⑪ PORTLAND CEMENT CONCRETE PAVEMENT
- ⑫ PORTLAND CEMENT CONCRETE SIDEWALK
- ⑬ HOT-MIX ASPHALT REMOVAL (VAR. DEPTH)
- ⑭ AGGREGATE SHOULDERS
- ⑮ HOT-MIX ASPHALT SHOULDERS
- ⑯ COMBINATION CONCRETE CURB AND GUTTER

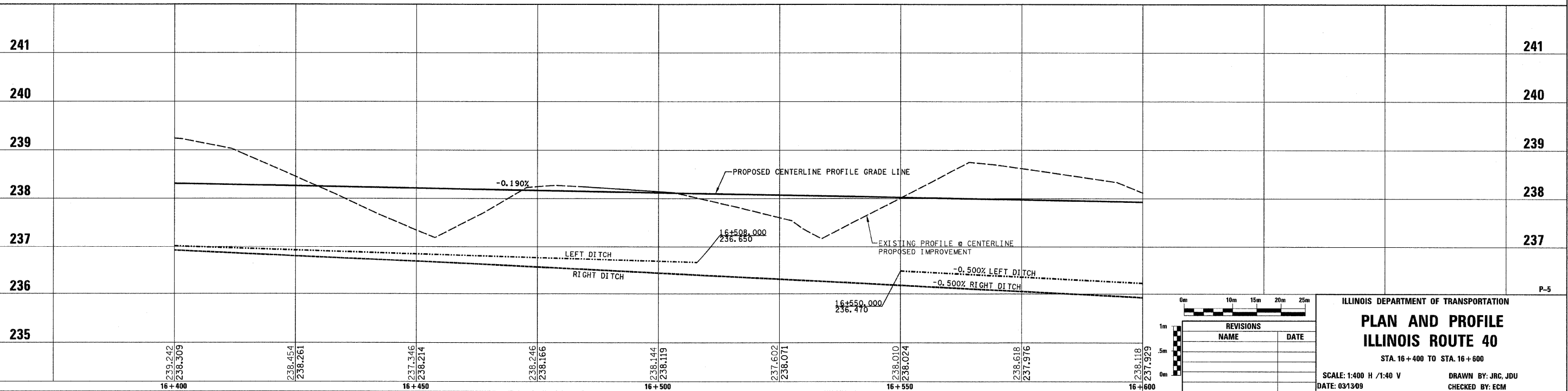
WOODSIDE DRIVE
 STA. 1+023.000
 34.5 m - P CUL CL A 2 450
 W/ PRC FL-END SEC 450 - EACH END
 USFL = 236.374
 DSFL = 236.183

SEE SHEET TS-3 FOR TRAFFIC SIGNAL PLAN F.A. RTE. 646 (IL. RTE. 40) & WOODSIDE DRIVE/SERVICE DRIVE NO. 2

CENTERLINE CURVE DATA
 CURVE #4
 PI STA 16+353.185
 PC STA 16+248.825
 PT STA 16+457.530
 Δ = 01°42'25" (RT.)
 R = 7,005.438 m
 T = 104.360 m
 L = 208.705 m
 SE = N.C.

LEGEND - EROSION CONTROL

- ⊠ INLET & PIPE PROTECTION
- ◇ TEMPORARY DITCH CHECKS
- PERIMETER EROSION CONTROL BARRIER
- ▨ EROSION CONTROL BLANKET



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAN AND PROFILE
ILLINOIS ROUTE 40
 STA. 16+400 TO STA. 16+600
 SCALE: 1:400 H / 1:40 V
 DATE: 03/13/09
 DRAWN BY: JRC, JDU
 CHECKED BY: ECM