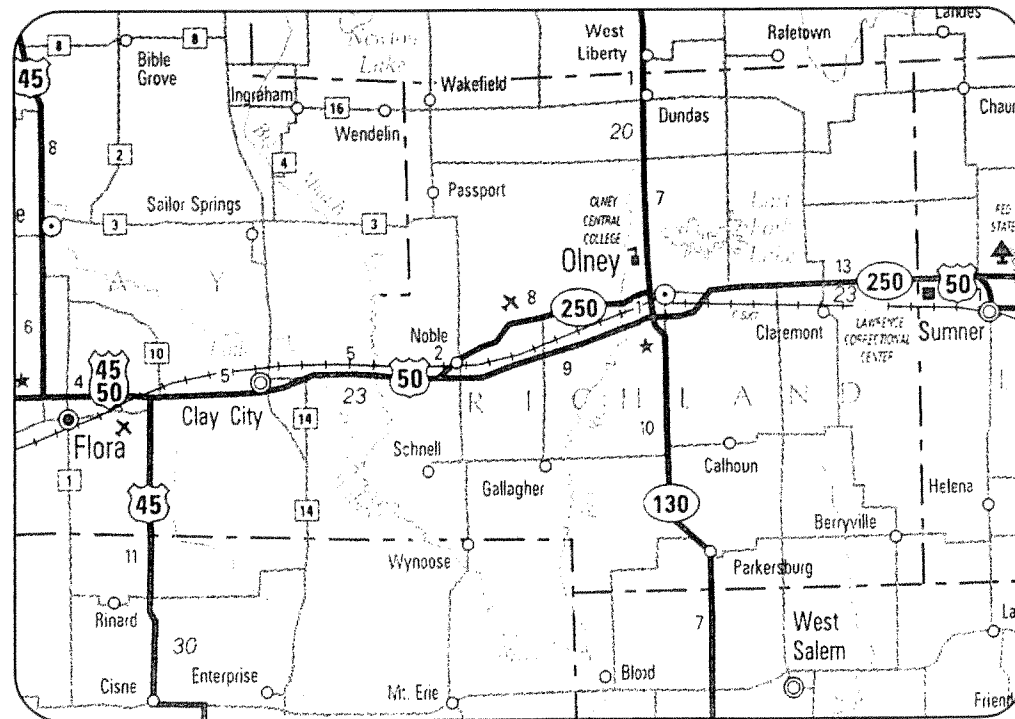


CONSTRUCTION PLANS FOR OLNEY-NOBLE AIRPORT OLNEY, RICHLAND COUNTY, ILLINOIS PFC OVERLAY APRON AND TAXIWAYS

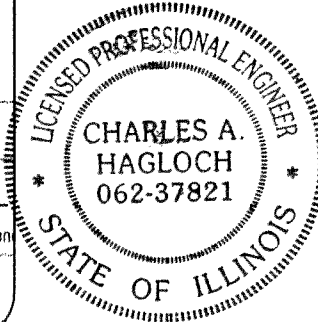
SCOPE OF WORK

THIS PROJECT CONSISTS OF CONSTRUCTING A POROUS FRICTION COURSE OVERLAY ON THE AIRCRAFT PARKING APRON, AND THREE CONNECTING TAXIWAYS. ASSOCIATED WORK INCLUDES CRACK CLEANING AND SEALING, BITUMINOUS REMOVAL AND REPLACEMENT, TIE DOWN REMOVAL AND REPLACEMENT, BITUMINOUS TACK COAT AND PAVEMENT MARKING.



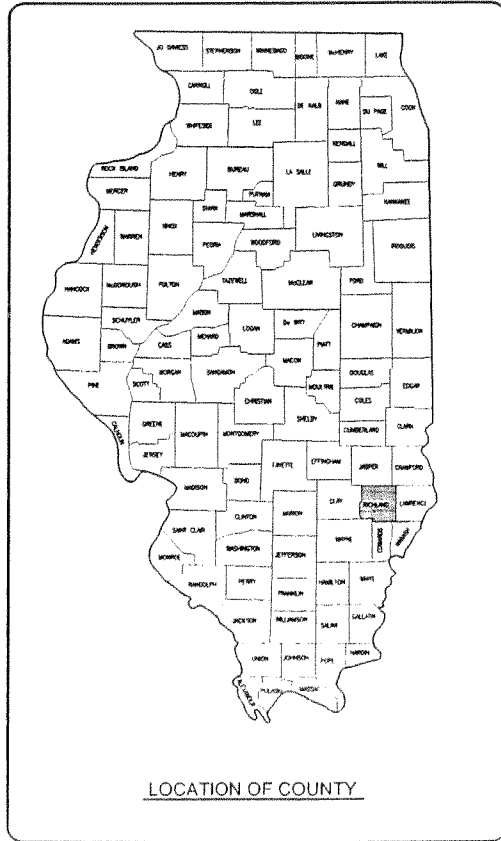
LOCATION

ILL. PROJ.: OLY-3780
A.I.P. PROJ.: 3-17-0076-B8
LATITUDE: 38° 44' 00"
LONGITUDE: 88° 10' 33"
ELEVATION: 481.0' M.S.L.
DATE: MAY 1, 2009



HANSON
Hanson Professional Services Inc.
Submitted by Charles A. Hagloch ENG'R
Date Submitted JUNE 23, 2009
Lic. Exp. Date Nov. 30, 2009

OLNEY-NOBLE AIRPORT AUTHORITY
Approved: [Signature] CHAIRMAN
Date _____
Approved: _____ SECRETARY
Date _____



LOCATION OF COUNTY

11/23/2009 7:52 AM K:\KINCAD0394
R:\P\015\OLNEY\09A0032\AIRPORT\SHEETS\P-001CVR.DWG - COVER

DATE	REVISION	BY
OLNEY-NOBLE AIRPORT OLNEY, ILLINOIS		
ILL. PROJ.: OLY-3780 A.I.P. PROJ.: 3-17-0076-B8		
Hanson Project No. 09A0032D_0800	Filename R-001CVR.DWG	Scale N/A
Date 03/03/09	MDR 03/03/09	CAH xx/xx/xx
 Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 Offices Nationwide		
PFC OVERLAY APRON AND TAXIWAY		COVER SHEET
		1

OL008

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITIES	AS BUILT QUANTITIES
AR150510	ENGINEER'S FEILD OFFICE	L.S.	1	
AR150540	HAUL ROUTE	L.S.	1	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	L.F.	3,349	
AR401655	BUTT JOINT CONSTRUCTION	S.Y.	879	
AR401910	REMOVE & REPLACE BIT. PAVEMENT	S.Y.	136	
AR402622	POROUS FRICTION COURSE, 0.10'	S.Y.	16,313	
AR510510	TIE DOWN	EACH	24	
AR510900	REMOVE TIE DOWN	EACH	5	
AR603510	BITUMINOUS TACK COAT	GAL.	4,078	
AR620520	PAVEMENT MARKING-WATERBORNE	S.F.	4,685	
AR620525	PAVEMENT MARKING-BLACK BORDER	S.F.	4,464	

INDEX TO SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	SUMMARY OF QUANTITIES AND INDEX TO SHEETS
3	PROPOSED SAFETY PLAN
4	PROPOSED CONSTRUCTION PLAN
5	PROPOSED CONSTRUCTION PLAN
6	PROPOSED PAVEMENT PREPARATION PLAN
7	PROPOSED PAVEMENT PREPARATION PLAN
8	PROPOSED TIE DOWN REMOVAL PLAN
9	PROPOSED MARKING AND TIE DOWN PLAN
10	PROPOSED MARKING PLAN

DATE	REVISION	BY

**OLNEY-NOBLE AIRPORT
 OLNEY, ILLINOIS**

IL. PROJ.: 01Y-3780 A.I.P. PROJ.: 3-17-0076-B8

Hanson Project No. 09A0032D_0800			
Filename R-002FLP.DWG			
Scale N/A			
Date 03/03/09			
LAYOUT	MDR	03/03/09	
DRAWN	MDR	03/03/09	
REVIEWED	CAH	xx/xx/xx	



Hanson Professional Services Inc.
 1525 South Sixth Street
 Springfield, Illinois 62705-2886
 Offices Nationwide

PFC OVERLAY
 APRON AND TAXIWAYS
 SUMMARY OF QUANTITIES
 AND
 INDEX TO SHEETS

SCOPE OF WORK

THIS PROJECT CONSISTS OF CONSTRUCTING A POROUS FRICTION COURSE OVERLAY ON THE AIRCRAFT PARKING APRON, AND THREE CONNECTING TAXIWAYS. ASSOCIATED WORK INCLUDES CRACK CLEANING AND SEALING, BITUMINOUS REMOVAL AND REPLACEMENT, TIE DOWN REMOVAL AND REPLACEMENT, BITUMINOUS TACK COAT AND PAVEMENT MARKING.

PROPOSED SAFETY PLAN

GENERAL - THE OLNEY NOBLE AIRPORT IS COMPRISED OF TWO PAVED RUNWAYS. THE PROPOSED CONSTRUCTION WILL NECESSITATE CLOSING BOTH RUNWAYS. RUNWAY 3/21 WILL BE CLOSED DURING PAVING OPERATIONS WHEN UTILIZED AS A HAUL ROUTE. ANY TIME THE CONTRACTOR IS WORKING WITHIN 200' OF EITHER RUNWAY 3/21 OR RUNWAY 11/29 CENTERLINE, THE ASSOCIATED RUNWAY WILL BE CLOSED. AT THE END OF EACH CONSTRUCTION DAY THE CONTRACTOR WILL SMOOTH GRADE ALL AREAS WITHIN THE SAFETY AREA TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. THE RUNWAY WILL BE BROOMED CLEAN. CLOSURE OF RUNWAY 3/21 AND RUNWAY 11/29 WILL BE KEPT TO A MINIMUM. THE AIRPORT WILL ESSENTIALLY NEED TO BE CLOSED DUE TO PROXIMITY TO RUNWAY 11-29 AND RUNWAY 3-21 WILL BE UTILIZED AS THE PAVING HAUL ROUTE. IMMEDIATELY UPON COMPLETION OF THE ROLLING OPERATION WITHIN 200' OF RUNWAY 11-29, THAT RUNWAY (11-29) WILL BE RE-OPENED. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAYS WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE CONTRACTOR IS REQUIRED TO NOTIFY THE AIRPORT MANAGER, THROUGH THE RESIDENT ENGINEER, SEVEN (7) DAYS IN ADVANCE OF THE COMMENCEMENT OF WORK. THIS WILL ALLOW THE AIRPORT MANAGER TIME TO ISSUE ALL NECESSARY NOTAM'S. THE CONTRACTOR IS REQUIRED TO PROVIDE 24 HOURS NOTICE PRIOR TO CLOSING EITHER RUNWAY.

IT IS THE RESPONSIBILITY OF THE AIRPORT MANAGER TO ISSUE ALL NOTAM'S REQUIRED THROUGHOUT THE CONSTRUCTION OF THE PROJECT.

CONTRACTOR STAGING

THE CONTRACTOR WILL SCHEDULE HIS TACK APPLICATION TO OBTAIN THE 48 HR CURE AND PAVE A SUBSTANTIAL PORTION OF THE POROUS FRICTION COURSE (PFC) IN A ONE-WEEK TIME PERIOD (SAT.-SUN.). THIS TIME CONSTRAINT IS TO PROVIDE THE LEAST INCONVENIENCE TO AIR TRAFFIC ACCESSING THE FUEL FACILITY AND T-HANGARS FROM THE RUNWAYS. PRIOR TO THE TACK APPLICATION, A TEMPORARY ACCESS TAXIWAY WILL BE LAID OUT AS SHOWN ON THIS SHEET TO PROVIDE ACCESS FOR AIRCRAFT FROM THE RUNWAYS TO THE T-HANGARS WHILE THE TACK CURES. THIS TEMPORARY ACCESS TAXIWAY WILL BE MAINTAINED AT ALL TIMES UNTIL NORMAL AIRCRAFT TRAFFIC CAN RESUME ON THE OVERLAID PAVEMENT. THIS TEMPORARY ACCESS TAXIWAY WILL BE OPEN EVEN DURING PAVING OPERATIONS. THIS WILL REQUIRE CLOSE COORDINATION BETWEEN THE AIRPORT MANAGER, THE RESIDENT ENGINEER AND THE CONTRACTOR. THE CONTRACTOR WILL HAVE TO STOP PAVING TRAFFIC AND SWEEP ALL PAVEMENT AREAS OF THE TEMPORARY ACCESS TAXIWAY TO ALLOW AIRCRAFT TO PASS. ONCE THE AIRCRAFT HAS PASSED, PAVING OPERATIONS MAY CONTINUE. THE CONTRACTOR WILL, UPON COMPLETION OF THE FIRST DAY OF PAVING, RE-ESTABLISH ACCESS TO THE FUEL FACILITY AND T-HANGARS VIA THE OVERLAID PAVEMENT. ALL WORK AND DELAYS IN PROVIDING AIRCRAFT ACCESS TO THE T-HANGARS AND FUEL FACILITY WILL BE PART OF THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

CONTRACTOR'S RESPONSIBILITIES

IDENTIFICATION - WHEN THE CONTRACTORS VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3') FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE).

ALL CONTRACTOR PERSONNEL SHALL HAVE IDENTIFICATION MAKING IT OBVIOUS THAT THEY ARE A PART OF THE CONSTRUCTION CREW.

THE CONTRACTOR, HIS EMPLOYEES, AND EQUIPMENT SHALL BE RESTRICTED TO THE PROJECT WORK AREA.

RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (123.00 MHZ.) WITH THE AIRPORT UNICOM. THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE OLNEY-NOBLE AIRPORT AND ENABLE THE AIRPORT TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

EQUIPMENT PARKING AND STORAGE - THE CONTRACTOR'S EQUIPMENT PARKING, MATERIAL STORAGE, AND EMPLOYEE PARKING WILL BE AT THE LOCATION SHOWN ON THIS DRAWING. ONLY VEHICLES AND EQUIPMENT NECESSARY FOR CONSTRUCTION WILL BE PERMITTED TO LEAVE THESE AREAS.

THE CONTRACTOR IS REQUIRED TO LIMIT THE USE OF CONSTRUCTION EQUIPMENT ON THE EXISTING PAVEMENTS. ONLY EQUIPMENT NEEDED TO COMPLETE THE SPECIFIC WORK ON THE EXISTING PAVEMENT WILL BE PERMITTED. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ANY DAMAGE TO EXISTING PAVEMENTS CAUSED BY HIS PERSONNEL OR EQUIPMENT.

CERTIFIED PAYROLLS

THE RESIDENT ENGINEER CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THE PERIOD HAVE BEEN RECEIVED.

MATERIAL CERTIFICATION

COMPLETED WORK CANNOT BE PLACED ON A CONSTRUCTION REPORT UNTIL ALL MATERIAL CERTIFICATIONS FOR THAT PAY ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER.

J.U.L.I.E. INFORMATION

COUNTY.....RICHLAND
 CITY.....OLNEY
 TOWNSHIP.....NOBLE
 SECTION NO.....2
 ADDRESS.....OLNEY-NOBLE AIRPORT
 P.O. BOX 717
 OLNEY, ILLINOIS 62450

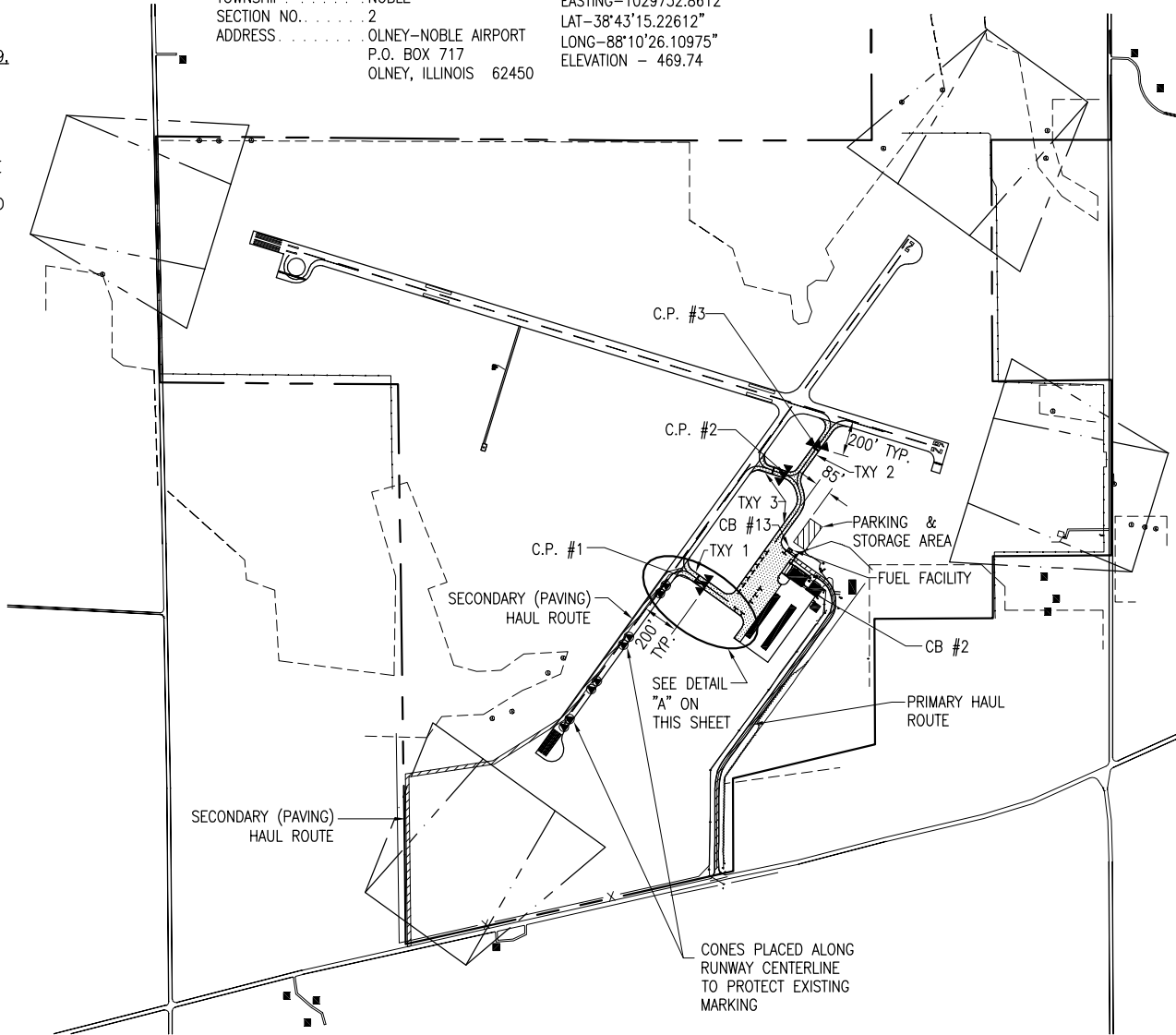
BENCHMARK DATA		
BM #CB-	DESCRIPTION	ELEV.
1	NGS MONUMENT OLNEPORT	469.12
CB #13	CHISELED SQUARE ON NW CORNER FUEL PAD	467.44
CB #2	WEST BOLT ON BEACON BASE	467.79

CRITICAL POINT DATA

CRITICAL PT. NO. 1
 NORTHING-747428.4960
 EASTING-1029283.3853
 LAT-38°43'09.06839"
 LONG-88°10'31.79465"
 ELEVATION - 464.35

CRITICAL PT. NO. 3
 NORTHING-748208.2501
 EASTING-1029906.9248
 LAT-38°43'16.76536"
 LONG-88°10'23.91003"
 ELEVATION - 469.53

CRITICAL PT. NO. 2
 NORTHING-748052.2275
 EASTING-1029732.8612
 LAT-38°43'15.22612"
 LONG-88°10'26.10975"
 ELEVATION - 469.74

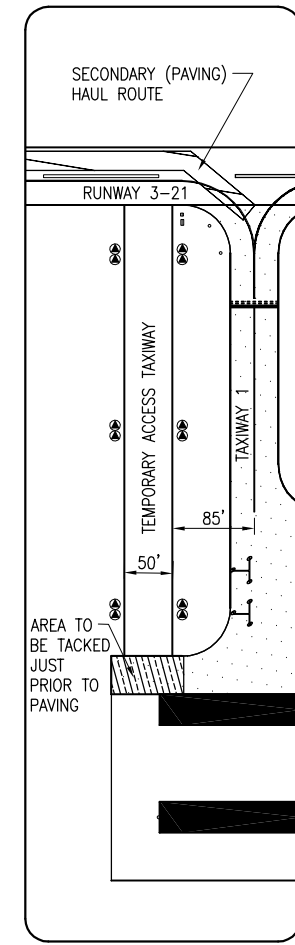


HEIGHT OF CONSTRUCTION EQUIPMENT

THE TALLEST EQUIPMENT EXPECTED TO BE REQUIRED FOR THE PROJECT IS A TRUCK WITH TRAILER IN THE UP POSITION. THE MAXIMUM ANTICIPATED HEIGHT OF THE TRUCK BED IS 25 FT.

EROSION CONTROL

THE PROPOSED PROJECT WILL DISTURB LESS THAN 1 ACRE. THEREFORE, NEITHER A STORM WATER POLLUTION PREVENTION PLAN NOR AN N.P.D.E.S. PERMIT IS REQUIRED.



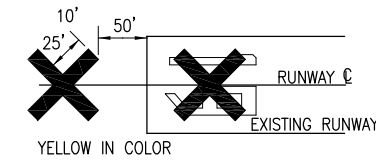
DETAIL "A"
 "NOT TO SCALE"

UTILITY NOTE

THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE UTILITY COMPANIES AND ORGANIZATIONS THAT HAVE LINES OR CONDUITS IN THE PROPOSED CONSTRUCTION AREA AND CONTACTING THEM FOR LOCATION AND DEPTH OF THE LINE. ALL LINES AND CONDUITS ARE TO BE LOCATED AND IDENTIFIED FOR DEPTH PRIOR TO ANY EXCAVATION. THE CONTRACTOR IS REQUIRED TO CONTACT JULIE (1-800-892-0123) TO ASSIST IN THESE REQUIREMENTS. THE CONTRACTOR IS ALSO RESPONSIBLE FOR IDENTIFYING ALL NON-JULIE UTILITIES WITHIN THE CONSTRUCTION LIMITS. THESE UTILITIES MUST ALSO BE LOCATED BEFORE CONSTRUCTION BEGINS.

AIRPORT SECURITY

AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES THROUGHOUT THE PROJECT. THE CONTRACTOR IS RESTRICTED TO THE PROPOSED HAUL ROUTES FOR ACCESS TO THE AIRPORT PROPERTY. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE GATES TO THE ACCESS AT THE END OF EACH DAY. THE CONTRACTOR IS REQUIRED TO PROVIDE LOCKS TO ACCOMPLISH THIS REQUIREMENT. FURNISHING LOCKS AND MAINTAINING AIRPORT SECURITY WILL BE INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



DETAIL OF CROSS FOR CLOSED RUNWAY
 "NOT TO SCALE"

RUNWAY CLOSURE PROCEDURES:
 * CONTACT THE AIRPORT MANAGER OR HIS ASSIGNED REPRESENTATIVE.
 * ISSUANCE OF NOTAM BY THE AIRPORT MANAGER OR HIS ASSIGNED REPRESENTATIVE.
 * PLACEMENT OF CROSSES (SEE DETAIL THIS SHEET).
 * PLACEMENT OF LIGHTED BARRICADES. ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS WITHIN 200 FT. OF THE AFFECTED RUNWAY CENTERLINE BEGIN.

RUNWAY REOPENING PROCEDURES:
 * REMOVE CROSSES.
 * REMOVE LIGHTED BARRICADES.
 * NOTIFY THE AIRPORT MANAGER OR HIS REPRESENTATIVE TO CANCEL THE NOTAM.
 * CANCELLATION OF THE NOTAM. A CLOSED RUNWAY WILL NOT BE REOPENED UNTIL ALL EQUIPMENT AND WORK ARE FURTHER THAN 200 FT. FROM THE AFFECTED RUNWAY CENTERLINE.

150-ENGINEER'S FIELD OFFICE NOTES

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE FURNISHED, MAINTAINED, AND REMOVED IN ACCORDANCE WITH ITEM AR150510 "ENGINEER'S FIELD OFFICE" AS STATED ON PAGE 168 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE LOCATION OF THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.

THE ENGINEERING FIRM WILL MAKE PAYMENT FOR ALL LONG DISTANCE TELEPHONE CALLS IN EXCESS OF ONE HUNDRED DOLLARS (\$100.00) PER MONTH.

THE CONTRACTOR WILL FURNISH A CELL PHONE TO THE RESIDENT ENGINEER FOR HIS EXCLUSIVE USE FOR THE DURATION OF THIS PROJECT. THE RESIDENT ENGINEER WILL USE THIS PHONE FOR PROJECT BUSINESS ONLY. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL CHARGES ASSOCIATED WITH THIS CELL PHONE.

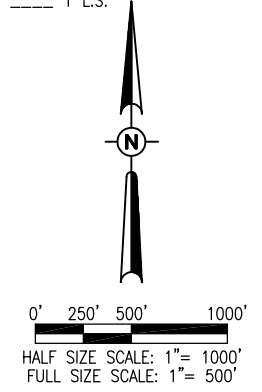
THE PROPOSED ENGINEER'S FIELD OFFICE INCLUDING THE CELL PHONE WILL BE PAID FOR UNDER ITEMS:
 AR150510 ENGINEER'S FIELD OFFICE ____ 1 L.S.

HAUL ROUTE AND EQUIPMENT PARKING

THE CONTRACTOR IS RESTRICTED TO USING THE DESIGNATED SECONDARY HAUL ROUTE FOR CONTINUOUS HAULING OPERATIONS. THE CONTRACTOR WILL USE THE PRIMARY HAUL ROUTE (ENTRANCE ROAD) TO BRING IN THE MAJORITY OF THE EQUIPMENT AND MATERIAL OTHER THAN THE POROUS FRICTION COURSE. THE EQUIPMENT PARKING AREA AND HAUL ROUTES ARE AS SHOWN ON THIS SAFETY PLAN. THE PARKING AREA IS APPROXIMATELY 200 FT. BY 200 FT. THE CONTRACTOR IS REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE AND PARKING AREA THROUGHOUT THE PROJECT DURATION. ALL DISTURBED AREAS WILL BE RETURNED TO THE ORIGINAL CONDITION AT THE CONCLUSION OF THE PROJECT. MAINTENANCE AND RESTORATION OF THE HAUL ROUTE AND EQUIPMENT PARKING AREA WILL BE COMPLETED TO THE SATISFACTION OF THE ENGINEER AND WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

LEGEND

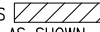
- EXISTING BUILDINGS
- EXISTING PAVEMENT
- EXISTING AIRPORT PROPERTY LINE
- PROPOSED CONSTRUCTION AREA
- BENCHMARKS
- PROPOSED HAUL ROUTE & PARKING
- BARRICADES
- TRAFFIC CONES



BY		REVISION	
DATE		DATE	
OLNEY-NOBLE AIRPORT OLNEY, ILLINOIS		A.I.P. PROJ.: 3-17-0076-B8	
Project No. 09A0032D_0800 File Name R-003SFY.DWG Scale 1" = 500' Date 03/18/09		LAYOUT CAH 03/18/09 DRAWN CAH 03/18/09 REVIEWED CAH xx/xx/xx	IL. PROJ.: 01Y-3780 A.I.P. PROJ.: 3-17-0076-B8
PFC OVERLAY APRON AND TAXIWAYS		PROPOSED SAFETY PLAN	
3		3 of 10 sheets	

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BITUMINOUS PAVEMENT MILLING

THE AREAS DESIGNATED AS  ON THIS SHEET AND SHEET NO. 5 WILL BE CUT OR TRIMMED AS SHOWN. THE MILLING WILL TAPER FROM A DEPTH OF 0.10' AT THE MATCH POINT TO A DEPTH OF 0' IN 10 FEET TO CREATE A BUTT JOINT FOR THE PROPOSED PAVEMENT. THE CUTTING OR TRIMMING WILL BE DONE BY ROTO-MILLING. THE TOLERANCE OF THE MILLING WILL BE AS STATED IN THE STANDARD SPECIFICATIONS.

IF A SQUARE STRAIGHT EDGE IS NOT OBTAINED FROM THE MILLING OPERATIONS, THE EXISTING PAVEMENT WILL BE SAWED AS SHOWN ON THIS SHEET. SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT MILLING AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL MILLED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE.

PRIOR TO APPLYING THE BITUMINOUS OVERLAY ALL MILLED MATERIAL WILL BE BROOMED AND BLOWN CLEAN AND A BITUMINOUS TACK COAT APPLIED. THE VERTICAL FACE OF ALL SAW CUTS WILL BE PAINTED WITH A LIQUID ASPHALT.

ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AS SHOWN AND VERIFIED BY THE RESIDENT ENGINEER.

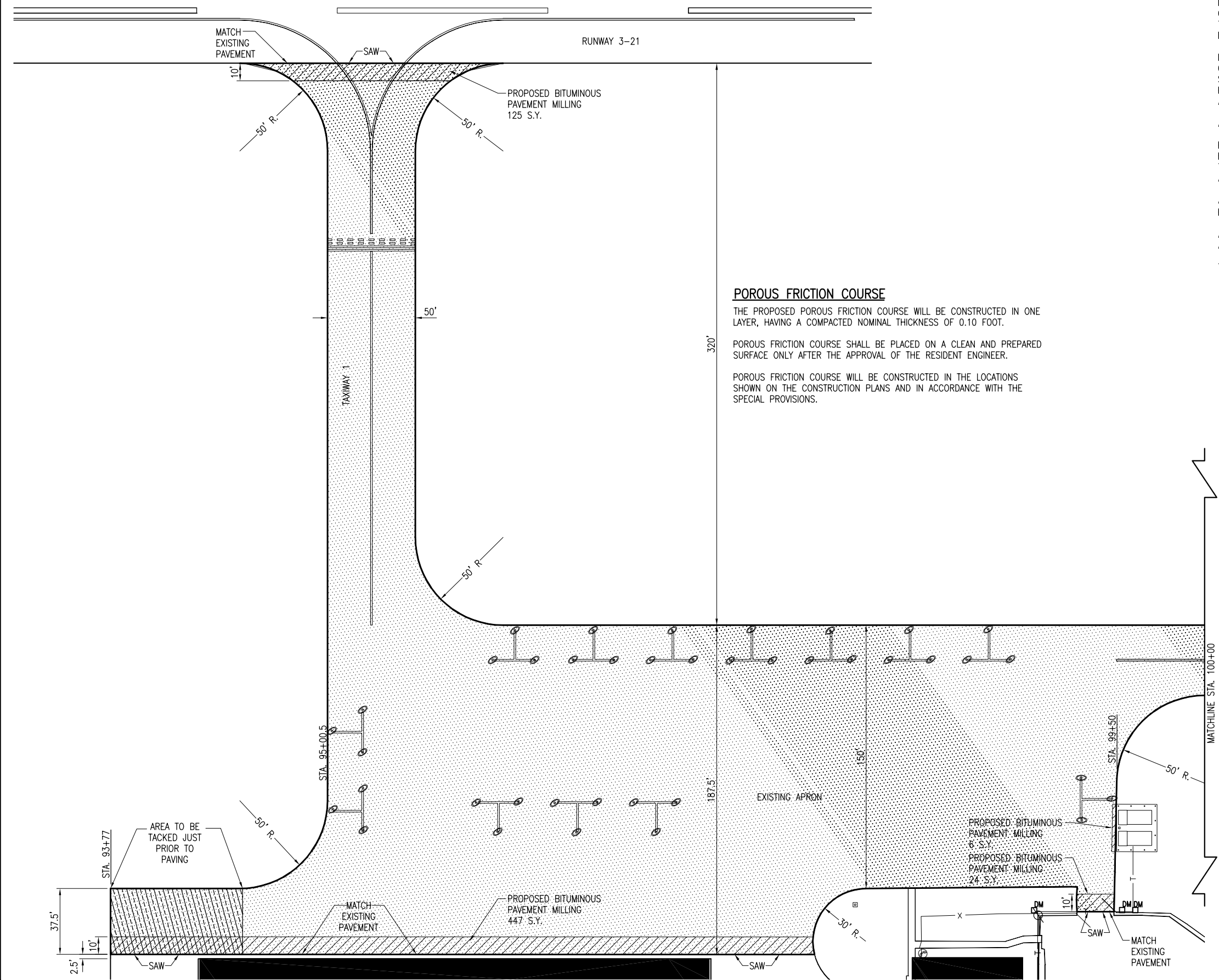
THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:
AR401650 "BITUMINOUS PAVEMENT MILLING" - PER S.Y.

POROUS FRICTION COURSE





THE PROPOSED POROUS FRICTION COURSE WILL BE CONSTRUCTED IN ONE LAYER, HAVING A COMPACTED NOMINAL THICKNESS OF 0.10 FOOT.

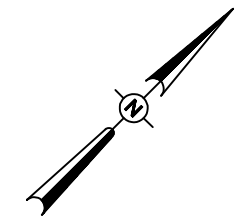
POROUS FRICTION COURSE SHALL BE PLACED ON A CLEAN AND PREPARED SURFACE ONLY AFTER THE APPROVAL OF THE RESIDENT ENGINEER.

POROUS FRICTION COURSE WILL BE CONSTRUCTED IN THE LOCATIONS SHOWN ON THE CONSTRUCTION PLANS AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.



LEGEND

-  EXISTING IMPROVEMENTS
-  PROPOSED IMPROVEMENTS
-  EXISTING BUILDINGS
-  PROPOSED BITUMINOUS PAVEMENT MILLING



DATE	REVISION	BY

**OLNEY-NOBLE AIRPORT
OLNEY, ILLINOIS**

IL. PROJ.: 01Y-3780 A.I.P. PROJ.: 3-17-0076-B8

Hanson Project No. 09A0032D_0800	LAYOUT	CAH	03/18/09
Filename: R-121CON.DWG	DRAWN	CAH	03/18/09
Scale: 1" = 30'	REVIEWED	CAH	xx/xx/xx
Date: 03/18/09			

HANSON

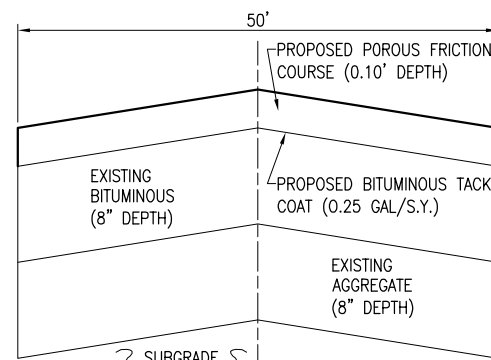
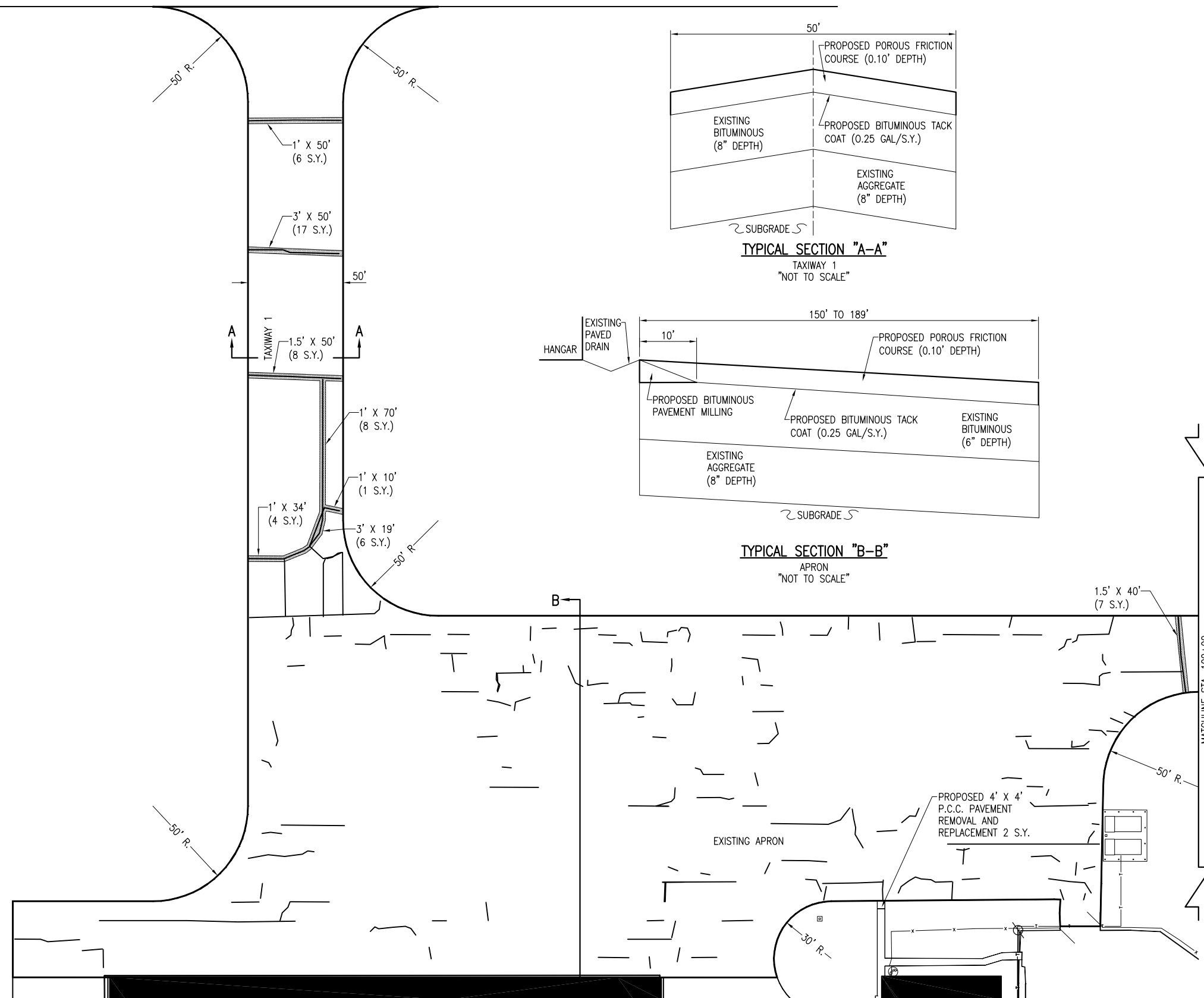
Hanson Professional Services Inc.
1525 South Sixth Street
Springfield, Illinois 62705-2886
Offices Nationwide

**PFC OVERLAY
APRON AND TAXIWAYS**

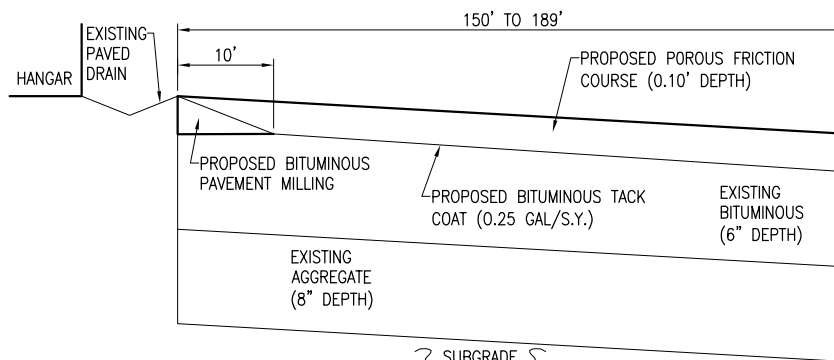
**PROPOSED
CONSTRUCTION
PLAN**

JUL 06 2009 1:20 PM HAGL000382
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RUNWAY 3-21



TYPICAL SECTION "A-A"
TAXIWAY 1
"NOT TO SCALE"



TYPICAL SECTION "B-B"
APRON
"NOT TO SCALE"

CLEANING AND SEALING BITUMINOUS CRACKS

THE PAVEMENT CONDITIONS AND CRACKS WERE IDENTIFIED DURING A SURVEY OF THE EXISTING APRON AREA PERFORMED MARCH, 2009. THE EXACT AMOUNT OF CRACKS TO BE CLEANED AND SEALED WILL BE THE NUMBER OF LINEAR FEET OF CRACKS SHOWN ON THIS SHEET AND THE NEXT SHEET MARKED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

ALL CRACKS DESIGNATED BY THE RESIDENT ENGINEER FOR CLEANING AND SEALING WILL BE DONE SO AS STATED IN THE SPECIAL PROVISIONS.

THE PROPOSED PAVEMENT MILLING WILL BE ACCOMPLISHED BEFORE THE CRACKS ARE CLEANED AND SEALED. THE RESIDENT ENGINEER WILL DETERMINE IF THE CRACKS LOCATED IN A MILLED AREA ARE LARGE ENOUGH TO WARRANT CLEANING AND SEALING.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:
AR201661 "CLEAN & SEAL BITUMINOUS CRACKS" - PER L.F.

P.C.C. PAVEMENT REMOVAL AND REPLACEMENT

THE AREA LABELED AS REMOVE AND REPLACE SIDEWALK ON THIS SHEET WILL HAVE THE EXISTING P.C.C. SIDEWALK ADJUSTED TO MATCH THE PROPOSED PAVEMENT. ALL REMOVED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE.

THE CONCRETE USED FOR THIS PAY ITEM WILL MEET THE SPECIFICATIONS FOR ITEM 610 STRUCTURAL CEMENT CONCRETE.

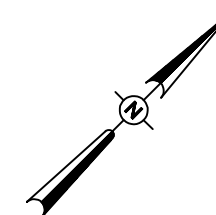
THE EXISTING P.C.C. SIDEWALK WILL BE REMOVED TO THE JOINT THAT IS NEAREST TO THE DIMENSIONS SHOWN ON THIS SHEET.

THE REMOVAL OF THE P.C.C. SIDEWALK WILL BE CONSIDERED AS AN INCIDENTAL ITEM AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:
AR501910 "REMOVE & REPLACE PCC PAVEMENT" - PER S.Y.

LEGEND

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED REMOVE AND REPLACE BITUMINOUS PAVEMENT
- PROPOSED CLEAN & SEAL CRACKS



0' 15' 30' 60'
HALF SIZE SCALE: 1" = 60'
FULL SIZE SCALE: 1" = 30'

DATE	REVISION	BY

**OLNEY-NOBLE AIRPORT
OLNEY, ILLINOIS**

Hanson Project No. 09A0032D_0800	LAYOUT	CAH	03/18/09
Filename: R-111PRP.DWG	DRAWN	CAH	03/18/09
Scale: 1" = 30'	REVIEWED	CAH	xx/xx/xx
Date: 03/18/09			

Hanson Professional Services Inc.
1525 South Sixth Street
Springfield, Illinois 62705-2886
Offices Nationwide

**PFC OVERLAY
APRON AND TAXIWAYS**
**PROPOSED
PAVEMENT PREPARATION
PLAN**

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IL PROJ.: 01Y-3780 A.I.P. PROJ.: 3-17-0076-B8

620-PAVEMENT MARKING-WATERBORNE NOTES

THE PAVEMENT MARKING-WATERBORNE (620) SHALL BE PLACED IN ACCORDANCE WITH ITEM 620 "PAVEMENT MARKING" AS STATED ON PAGE 77 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THIS ITEM SHALL CONSIST OF RUNWAY AND TAXIWAY CENTERLINE, HOLDING LINE AND TIE DOWN MARKING IN ACCORDANCE WITH THESE SPECIFICATIONS AND AT THE LOCATIONS SHOWN ON THE CONSTRUCTION PLANS. ALL TAXIWAY MARKING WILL BE YELLOW IN COLOR WITH A 6-IN BLACK BORDER AND ALL RUNWAY MARKING WILL BE WHITE IN COLOR WITH A 6 IN BLACK BORDER. THE PROPOSED PAVEMENT MARKING WILL BE APPLIED IN TWO APPLICATIONS.

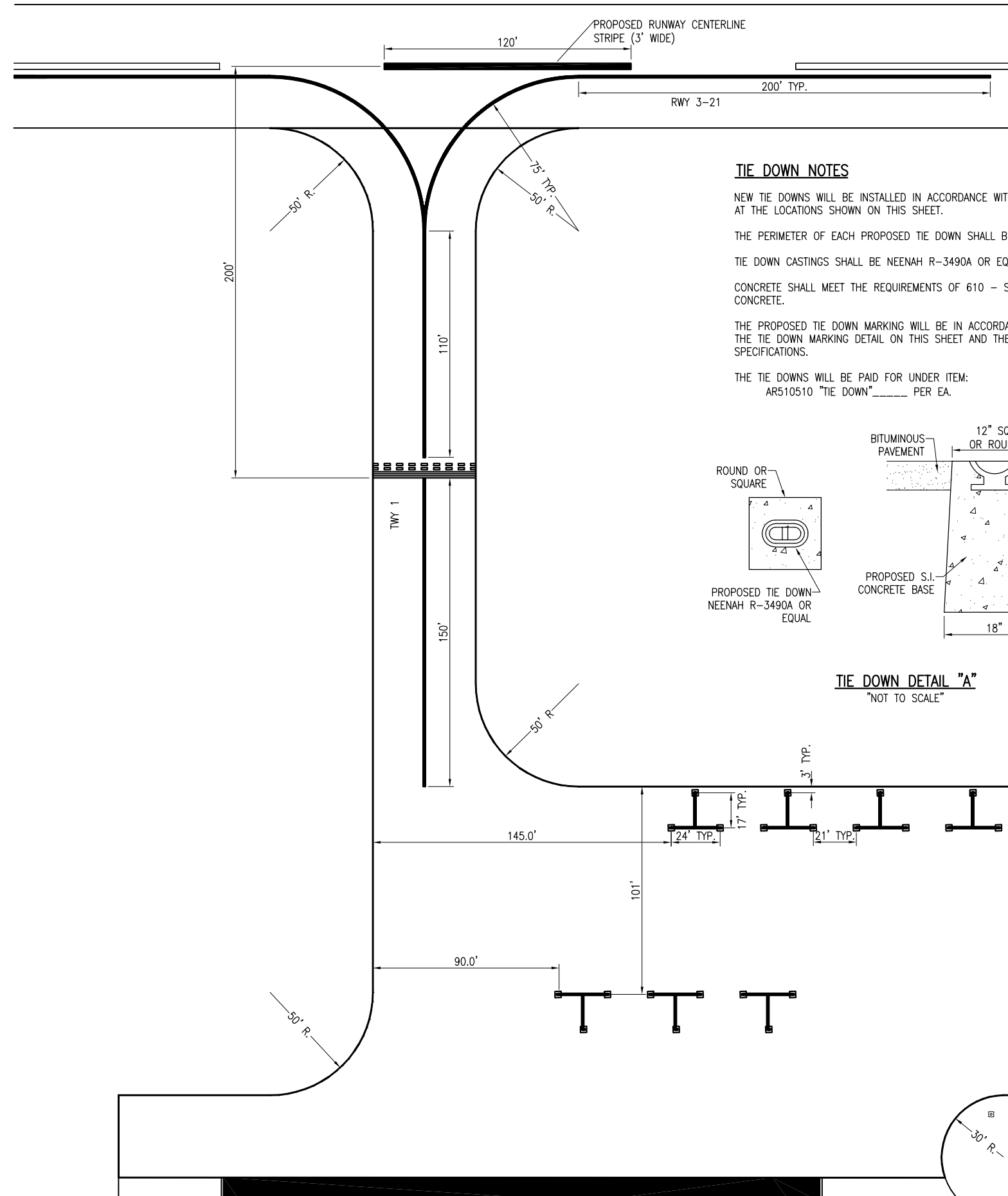
ANY MATERIAL DELIVERED THAT FAILS TO MEET THE SPECIFICATIONS SHALL BE DISPOSED OF BY THE VENDOR AND IMMEDIATELY REPLACED WITH ACCEPTABLE MATERIAL ENTIRELY AT THE VENDOR'S EXPENSE, INCLUDING HANDLING AND TRANSPORTATION CHARGES.

ALL PROPOSED MARKING WILL BE COMPLETED IN ACCORDANCE WITH THE DETAILS SHOWN ON THE CONSTRUCTION PLANS.

GLASS BEADS SHALL BE REQUIRED ONLY ON THE SECOND APPLICATION OF YELLOW AND WHITE MARKING.

CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

THE PROPOSED MARKING WILL BE PAID FOR UNDER ITEM:
 AR620520 PAVEMENT MARKING-WATERBORNE ____ PER S.F.
 AR620525 PAVEMENT MARKING-BLACK BORDER ____ PER S.F.



TIE DOWN NOTES

NEW TIE DOWNS WILL BE INSTALLED IN ACCORDANCE WITH DETAIL "A" AT THE LOCATIONS SHOWN ON THIS SHEET.

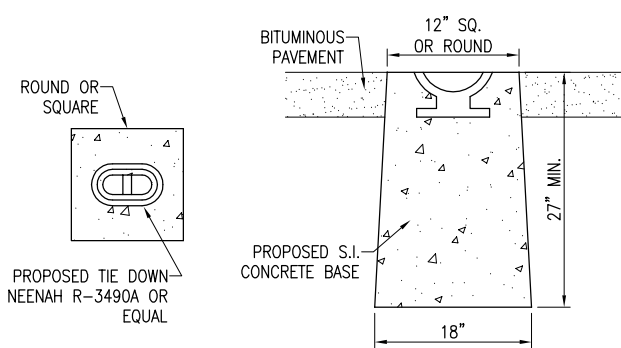
THE PERIMETER OF EACH PROPOSED TIE DOWN SHALL BE SAWED.

TIE DOWN CASTINGS SHALL BE NEENAH R-3490A OR EQUAL.

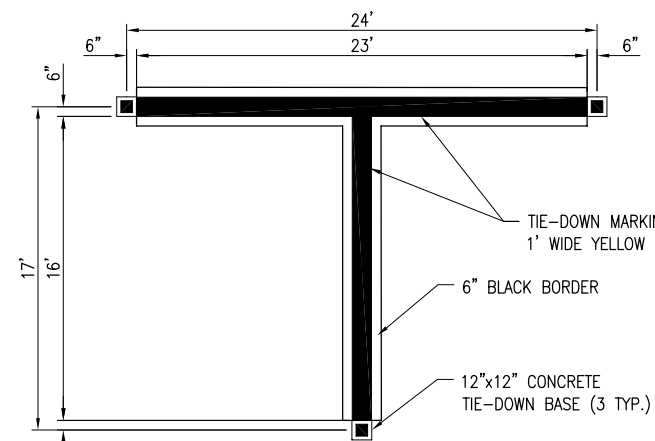
CONCRETE SHALL MEET THE REQUIREMENTS OF 610 - STRUCTURAL CONCRETE.

THE PROPOSED TIE DOWN MARKING WILL BE IN ACCORDANCE WITH THE TIE DOWN MARKING DETAIL ON THIS SHEET AND THE SPECIFICATIONS.

THE TIE DOWNS WILL BE PAID FOR UNDER ITEM:
 AR510510 "TIE DOWN" ____ PER EA.



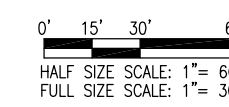
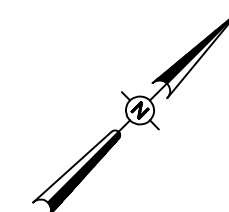
TIE DOWN DETAIL "A"
 "NOT TO SCALE"



TIE DOWN MARKING DETAIL
 "NOT TO SCALE"

LEGEND

	EXISTING PAVEMENT
	PROPOSED TIE DOWN
	EXISTING BUILDINGS
	EXISTING MARKING
	PROPOSED MARKING



DATE	REVISION	BY

**OLNEY-NOBLE AIRPORT
 OLNEY, ILLINOIS**

Hanson Project No. 09A0032D_0800	CAH	03/18/09
Filename: R-151MRK.DWG	CAH	03/18/09
Scale: 1" = 30'	CAH	xx/xx/xx
Date: 03/18/09	REVIEWED	
LAYOUT		

Hanson Professional Services Inc.
 1525 South Sixth Street
 Springfield, Illinois 62705-2886
 Offices Nationwide

**PFC OVERLAY
 APRON AND TAXIWAYS
 PROPOSED
 MARKING AND TIE DOWN
 PLAN**

JUL 06, 2009 1:24 PM HAGL000382
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