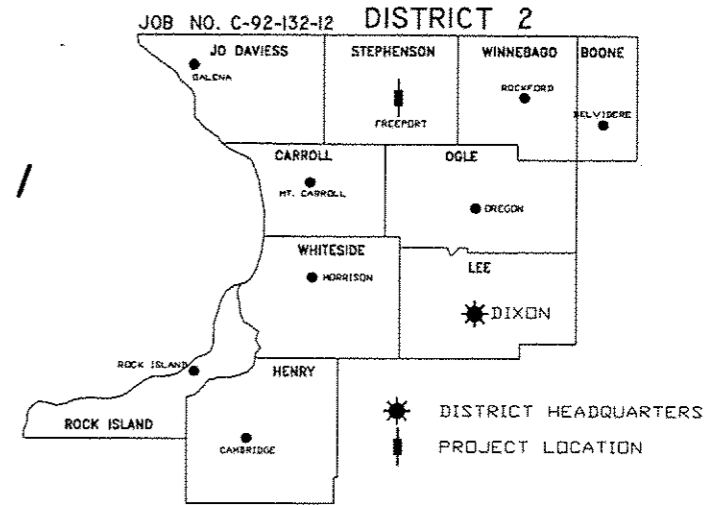


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
STREET IMPROVEMENTS
FAU 5254 (HARLEM AVE.) / FAU 5235 (STEPHENSON ST.) /
FAU 5237 (LINCOLN BLVD.)
CITY OF FREEPORT, STEPHENSON COUNTY
SECTION 14-000161-00-RS
PROJECT NO. M-5024(032)
JOB NO. C-92-044-15
CONTRACT NO. 85626



INDEX OF SHEETS

SHEET NO.	TITLE
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2	GENERAL NOTES
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27-32	STANDARD DETAILS

LIST OF STATE HIGHWAY STANDARDS

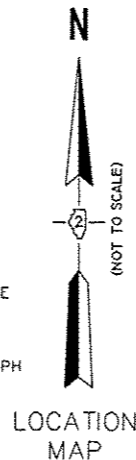
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-02	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-02	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-02	MID-BLOCK CURB RAMP FOR SIDEWALKS
424021-03	DEPRESSED CORNER FOR SIDEWALKS
602601-03	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-04	FRAME AND LIDS TYPE 1
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND CUTTER
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
701101-04	OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701427-03	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS ≤ 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701502-06	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES
720011-01	METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
728001-01	TELESCOPING STEEL SIGN SUPPORT
729001-01	APPLICATIONS OF TYPES A & B METAL POSTS (FOR SIGNS & MARKERS)
780001-05	TYPICAL PAVEMENT MARKINGS

IDOT DISTRICT 2 STANDARDS (INCLUDED IN PLAN SET)

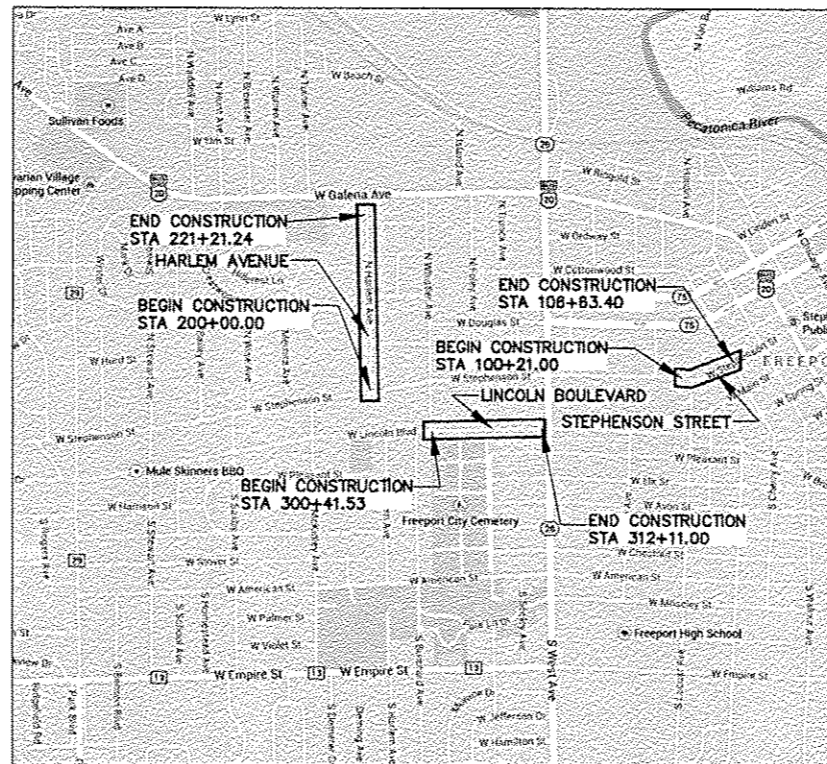
10.2	INLETS, SPECIAL
25.1	ENTRANCE APPROACHES - URBAN AREA
35.4	SIDEWALK AND DRIVEWAY PAVEMENT PAY AREAS
41.1	TYPICAL PAVEMENT MARKINGS
60.2	CURB RAMP DETAILS

CITY OF FREEPORT STANDARD DETAILS (INCLUDED IN PLAN SET)

1	MANHOLE ADJUSTMENT DETAIL
2	WATER VALVE ADJUSTMENT DETAIL



COMMITMENTS - NONE



HARLEM AVENUE
FUNCTIONAL CLASSIFICATION:
MAJOR COLLECTOR
2015 AADT: 2320
1% TRUCKS
FREEPORT TOWNSHIP 27N, RANGE 7E,
SECTION 36 3RD P.M.
STEPHENSON COUNTY

PROJECT LENGTH:
2121 FEET = 0.40 MILES

STEPHENSON STREET
FUNCTIONAL CLASSIFICATION:
MINOR ARTERIAL
2015 AADT: 3920
1% TRUCKS
FREEPORT TOWNSHIP 27N, RANGE 8E,
SECTION 31 3RD P.M.
STEPHENSON COUNTY

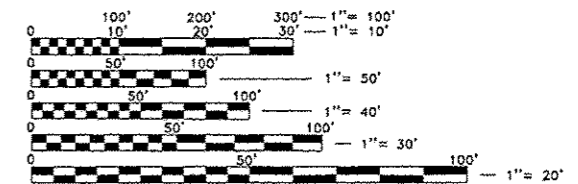
PROJECT LENGTH:
642 FEET = 0.12 MILES

LINCOLN BOULEVARD
FUNCTIONAL CLASSIFICATION:
MAJOR COLLECTOR
2015 AADT: 3090
1% TRUCKS
FREEPORT TOWNSHIP 27N, RANGE 7E,
SECTION 36 3RD P.M.
STEPHENSON COUNTY

PROJECT LENGTH:
1170 FEET = 0.22 MILES

SCALES:

ROADWAY PLAN: 1" = 20'
ADA RAMP DETAILS: 1" = 10'
EROSION CONTROL PLAN: 1" = 20'
CROSS SECTIONS: 1" = 10" H
1" = 5' V



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



811 or
1-800-892-0123
TOLL FREE

AGENCY RESPONSIBLE FOR LETTING	APPROVED: <i>Sh. Galt</i> City of Freeport, City Engineer Local Agency, Position
PASSED: 6/17/15 <i>Andy M. Buntz</i> District 2 Engineer of Local Roads & Streets	RELEASING FOR BID BASED ON LIMITED REVIEW: 6/17/15 <i>Paul A. Fuchs</i> Deputy Director of Highways, Region 2 Engineer
PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS	

I hereby certify that this engineering document was prepared by me or under my direct personal supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Illinois.

Signature: *Kerry R. Lin P.E.*
KERRY R. LIN, P.E.

Date: 6/16/15 Reg. No.: 062-049244
(Month, Day, Year)

My registration renewal date is November 30, 2015.



SHEET REVIEW	
AGENCY	DATE

REVISIONS		
NO.	ITEM	DATE

SCALE:	NA
DRAWN BY:	KRL
CHECKED BY:	BSL
DATE:	6/16/2015

McClure
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7282 Arroyo Drive
Rockford, Illinois 61107-5837
(815) 398-2532 FAX (815) 398-2496
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COVER	
CITY OF FREEPORT	FREEPORT, ILLINOIS
FILE: H:\14-016-FREEPORTSTREETIMP\DESIGN\DRAWINGS\14-016 COVER.DWG	MEAT JOB: 04-28-14-016

SHEET NO.	1
OF	32

GENERAL NOTES

GENERAL

1. IN THE FOLLOWING, THE ILLINOIS DEPARTMENT OF TRANSPORTATION WILL BE REFERRED TO AS IDOT.
2. THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" PREPARED BY IDOT, ADOPTED JANUARY 1, 2012, AND THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" PREPARED BY IDOT, ADOPTED JANUARY 1, 2015. ALL SEWER AND WATER MAIN CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", CURRENT EDITION. ALL WORK ON THE CITY OF FREEPORT SANITARY FACILITIES SHALL BE IN ACCORDANCE WITH THE CITY OF FREEPORT'S GENERAL PROVISIONS AND TECHNICAL SPECIFICATIONS FOR SANITARY SEWER CONSTRUCTION. SIGN CONSTRUCTION AND PAVEMENT MARKINGS SHALL CONFORM TO THE REQUIREMENTS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", CURRENT EDITION.
3. GENERAL SAFETY PROVISION: TO PROVIDE DRIVERS WITH SAFE TRAVEL CONDITIONS DURING THE CONSTRUCTION PROJECT, AND TO PROVIDE SAFE WORKING CONDITIONS FOR ALL EMPLOYEES, THE RULES, REGULATIONS, AND CONDITIONS STATED BELOW WILL PREVAIL FOR THE DURATION OF THIS CONTRACT. ANY EMPLOYEE OF THE CONTRACTOR OR HIS SUBCONTRACTORS WHO REFUSES TO COMPLY WITH THESE GENERAL SAFETY PROVISIONS SHALL BE REMOVED FROM THE JOB SITE IN ACCORDANCE WITH THE IDOT STANDARD SPECIFICATIONS. THE CONTRACTOR AND ANY SUBCONTRACTORS RETAINED BY HIM SHALL COMPLY WITH THE STATE AND FEDERAL REQUIREMENTS OF THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970 (OSHA), AS IT RELATES TO HIS OPERATIONS, REVISED AS OF JULY 1, 1987.
4. THE CONTRACTOR WILL BE REQUIRED TO COMPLY WITH ALL STATE AND LOCAL REGULATIONS REGARDING AIR, WATER, AND NOISE POLLUTION. HE WILL NOT BE ALLOWED TO BUILD FIRES ON THE SITE.
5. WHEN ARTIFICIAL LIGHTING IS UTILIZED DURING NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC, AS WELL AS ADJOINING RESIDENTIAL AREAS.
6. THE SCALE SHOWN ON THE DRAWINGS APPLIES ONLY TO THE FULL SIZE PLANS AND NOT REDUCED SIZE PLANS. DO NOT SCALE REDUCED SIZE PLANS FOR CONSTRUCTION DIMENSIONS.
7. THE IDOT STANDARD DRAWINGS LISTED ON THE COVER SHEET ARE INTENDED TO BE THE LATEST REVISIONS AND SHALL TAKE PRECEDENCE OVER EARLIER DRAWINGS THAT MAY BE REFERRED TO ELSEWHERE IN THE PLANS OR SPECIAL PROVISIONS.
8. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING HIS CONSTRUCTION OPERATIONS WITH OTHER CONSTRUCTION, UTILITY, AND/OR MAINTENANCE OPERATIONS WITHIN OR ADJACENT TO THE PROJECT.
9. WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED OR DISTURBED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS, MONUMENTS AND RIGHT-OF-WAY PINS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR, OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR RE-ESTABLISH ANY SECTION OR SUBSECTION MONUMENTS DISTURBED OR DESTROYED BY HIS OPERATIONS. REPLACEMENT OF MONUMENTS WILL BE DETERMINED BY THE ENGINEER. PROPERTY MARKERS DAMAGED BY THE CONTRACTOR SHALL BE REPLACED AT THE EXPENSE OF THE CONTRACTOR AT NO ADDITIONAL EXPENSE TO THE OWNER.
10. THE CONTRACTOR SHALL FIELD VERIFY THE ELEVATIONS OF THE BENCHMARKS PRIOR TO COMMENCING WORK. THE CONTRACTOR SHALL ALSO FIELD VERIFY LOCATION, ELEVATION AND SIZE OF EXISTING UTILITIES, AND VERIFY PAVEMENT ELEVATIONS WHERE MATCHING INTO EXISTING WORK. THE CONTRACTOR SHALL FIELD VERIFY HORIZONTAL CONTROL BY REFERENCING KNOWN PROPERTY LINES, OR SURVEY MARKERS. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF DISCREPANCIES IN EITHER VERTICAL OR HORIZONTAL CONTROL PRIOR TO PROCEEDING WITH WORK.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL PLAN QUANTITIES.
12. THE CONTRACTOR SHALL REMOVE, STORE, AND RELOCATE TO THE SATISFACTION OF THE ENGINEER ALL EXISTING SIGNS WHICH ARE TO BE RELOCATED IN ACCORDANCE WITH ARTICLE 107.25 OF THE IDOT STANDARD SPECIFICATIONS. THE PLACEMENT OF ADDITIONAL SIGNING SHALL BE THE RESPONSIBILITY OF THE CITY OF FREEPORT, OR THEIR APPROVED REPRESENTATIVES.
13. ANY PRIVATE OR COMMERCIAL SIGNS WHICH NEED TO BE REMOVED FOR CONSTRUCTION PURPOSES SHALL BE HANDLED AS FOLLOWS:

 WITHIN THE EXISTING RIGHT-OF-WAY, SIGNS SHALL BE REMOVED OR REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER PRIOR TO BEGINNING WORK. PAYMENT FOR ALL MATERIALS AND LABOR REQUIRED FOR THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

GENERAL, CONT.

14. ALL ITEMS SHALL INCLUDE ALL THE NECESSARY MATERIALS AND LABOR TO COMPLETE THE ITEM IN PLACE. MATERIALS AND LABOR NOT SPECIFICALLY IDENTIFIED SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF THE ITEM BEING CONSTRUCTED.
15. ALL MATERIALS SHALL MEET OR EXCEED IDOT STANDARDS.
16. THE CONTRACTOR SHALL NOTIFY THE CITY OF FREEPORT, PROPERTY OWNERS, THE ENGINEER, AND ALL UTILITY COMPANIES AT LEAST 48 HOURS PRIOR TO THE START OF CONSTRUCTION OR CHANGE IN CLOSURES OR DETOURS.
17. ALL RADII AND DIMENSIONS ARE REFERENCED TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
18. NO WORK SHALL BE PERFORMED BEYOND THE PROJECT LIMITS WITHOUT PRIOR WRITTEN AUTHORIZATION FROM THE PROPERTY OWNER AND ENGINEER.
19. INSPECTION SHALL BE THE RESPONSIBILITY OF THE OWNER. CONSTRUCTION STAKES SHALL BE REQUESTED A MINIMUM OF 48 HOURS IN ADVANCE AND WILL BE INCLUDED IN THE CONSTRUCTION LAYOUT PAY ITEM. ANY RESTAKING SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC CONTROL

1. THE CONTRACTOR SHALL AT ALL TIMES PROVIDE PROTECTION FOR THE TRAFFIC AS DIRECTED BY THE ENGINEER AND IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
2. ACCESS MUST BE MAINTAINED TO ALL EXISTING PROPERTIES DURING CONSTRUCTION PER ARTICLE 107.09 UNLESS ARRANGEMENTS ARE MADE IN WRITING BY THE CONTRACTOR WITH THE PROPERTY OWNER FOR SHORT-TERM CLOSURES. A COPY OF SAID ARRANGEMENT MUST BE GIVEN TO THE ENGINEER.
3. THE CONTRACTOR IS ADVISED THAT IN THE EVENT OF SNOW, HE WILL ADJUST ANY TRAFFIC CONTROL AND PROTECTION/MAINTENANCE OF TRAFFIC DEVICES REQUIRED FOR HIS OPERATIONS THAT WOULD INTERFERE WITH SNOW REMOVAL OPERATIONS BUT SHALL CONTINUE TO PROVIDE PROTECTION FOR TRAFFIC.
4. ANY DROP-OFF GREATER THAN 1 1/2 INCHES ADJACENT TO THE EDGE OF PAVEMENT SHALL BE PROTECTED WITH BARRICADES, AND SHALL BE INCLUDED IN THE UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION, SPECIAL.
5. ALL TRAFFIC CONTROL DEVICES USED FOR THE MAINTENANCE OF TRAFFIC SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS NECESSARY THROUGHOUT THE DURATION OF THE CONTRACT. ALL SIGNS USED FOR THE MAINTENANCE OF TRAFFIC SHALL BE FURNISHED, INSTALLED AND MAINTAINED BY THE CONTRACTOR. PAYMENT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION, SPECIAL.
6. TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN CONDITIONS MAY REQUIRE THE ENGINEER TO MODIFY THE LOCATION OF THE TRAFFIC CONTROL DEVICES. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND WITHIN 30 MINUTES FROM THE TIME OF NOTIFICATION BY THE ENGINEER TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION, IMPROVEMENT OR MODIFICATION OF THE MAINTENANCE OF TRAFFIC CONTROL DEVICES.
7. THE CONTRACTOR SHALL SUPPLY THE ENGINEER WITH A PRIMARY AND ALTERNATE CONTACT PERSON WITH 24-HOUR, 7-DAY ACCESS WHO CAN REMEDY DEFICIENT OR EMERGENCY TRAFFIC CONTROL MEASURES. THE CONTRACTOR SHALL INSPECT THE TRAFFIC CONTROL DEVICES NOT LESS THAN TWICE DAILY TO ENSURE THAT ALL BARRELS, BARRICADES, AND WARNING DEVICES ARE IN THEIR PROPER PLACE AND OPERATIONAL.
8. DURING CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL TAKE NECESSARY PRECAUTIONS TO PROTECT ADJACENT TRAFFIC LANES OPEN TO TRAFFIC FROM DEBRIS BEING BLOWN OR OTHERWISE REMOVED FROM THE CONSTRUCTION AREAS. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR KEEPING DEBRIS OFF THE ADJACENT TRAVELED LANE SURFACE.

TOPSOIL & SEEDING

1. DISTURBED AREAS ARE LOCATIONS WHERE THE CONTRACTOR'S OPERATIONS HAVE DAMAGED EXISTING GROUND COVER AND/OR TOPSOIL OUTSIDE OF THE LIMITS OF CONSTRUCTION AS SHOWN IN THE PLANS. SEEDING OF THESE DISTURBED AREAS IS INCLUDED IN THE PARKWAY RESTORATION PAY ITEM AND NO ADDITIONAL COMPENSATION IS ALLOWED.
2. MODIFY SECTION 250.07 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION TO INCLUDE THE CONTRACTOR GUARANTEE A 75 PERCENT UNIFORM GROWTH OVER THE ENTIRE SEEDED AREA(S) AFTER ONE GROWING SEASON, WITH NO EXCEPTION TO THE TIMING OF THE SEEDING. AFTER ONE GROWING SEASON, AREAS NOT SUSTAINING A 75 PERCENT UNIFORM GROWTH SHALL BE INTERSEEDED OR RESEEDED, AS DETERMINED BY THE ENGINEER, AT NO ADDITIONAL COST TO THE CONTRACT.

REMOVAL / EXCAVATION / EARTHWORK


1. EXISTING STRUCTURES (INCLUDING FOUNDATIONS, WALLS, CISTERNS, OR OTHER UNDERGROUND STRUCTURES) WITHIN THE RIGHT OF WAY SHALL BE REMOVED IN ACCORDANCE WITH SECTION 501 OF THE STANDARD SPECIFICATIONS, WITHOUT ADDITIONAL COMPENSATION, UNLESS OTHERWISE NOTED IN THE PLANS OR SPECIAL PROVISIONS.
2. UNDERGROUND TANKS OR SEPTIC TANKS WITHIN THE RIGHT-OF-WAY WHICH HAVE NOT BEEN REMOVED AND WILL NOT INTERFERE WITH CONSTRUCTION SHALL BE FILLED WITH FREE-FLOWING SAND AT THE DIRECTION OF THE ENGINEER. THE COST OF THIS WORK SHALL BE IN ACCORDANCE WITH SECTION 109 OF THE STANDARD SPECIFICATIONS.
3. ALL ROADWAY REMOVAL ITEMS SHALL CONFORM TO SECTION 202 AND 440 OF THE IDOT STANDARD SPECIFICATIONS. ALL JOINTS BETWEEN THE PORTION REMOVED AND THAT LEFT IN PLACE SHALL BE SAWCUT TO SUCH A DEPTH THAT A CLEAN, NEAT EDGE WILL RESULT WITH NO SPALLING TO THE REMAINING PORTION. THE COST OF SAW CUTTING SHALL BE INCLUDED IN THE UNIT PRICE OF THE ITEM BEING REMOVED. ADDITIONAL SAW CUTTING MAY BE REQUIRED AS DIRECTED BY THE ENGINEER AT NO ADDITIONAL COST TO THE OWNER.
4. ALL DEBRIS RESULTING FROM CONSTRUCTION OPERATIONS SHALL BE PROPERLY DISPOSED OF OFF-SITE. THIS SHALL INCLUDE BUT NOT BE LIMITED TO DEBRIS FROM PAVEMENT, DRIVEWAY, SIDEWALK, AND CURB AND GUTTER REMOVAL.
5. ALL EXCAVATED EARTH MATERIAL SHALL REMAIN ON-SITE AND BE INCORPORATED INTO THE PROPOSED CONSTRUCTION. THIS WORK SHALL BE COORDINATED WITH THE ENGINEER AND THE CITY OF FREEPORT AND SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR EARTH EXCAVATION.
6. THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS WHICH ARE NOT INDICATED TO BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE OWNER.
7. CLEARING SHALL BE DONE IN ACCORDANCE WITH SECTION 201 OF THE STANDARD SPECIFICATIONS AND SHALL NOT BE MEASURED FOR PAYMENT.

ROADWAY ITEMS

1. PROTECTIVE COAT SHALL BE APPLIED TO ALL CONCRETE SURFACES AND APPURTENANCES PER ARTICLE 420.18 OF THE IDOT "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". THE COST OF THE PROTECTIVE COAT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE TO WHICH IT IS APPLIED.
2. PREFORMED FIBER EXPANSION JOINTS WHERE REQUIRED SHALL BE INCLUDED IN THE CONTRACT UNIT BID PRICE FOR WHICH IT IS APPLIED (CURB & GUTTER, DRIVEWAYS, ETC.)
3. PREVIOUSLY PUGMILLED STOCKPILES OF "TYPE A" AGGREGATE OLDER THAN 1 MONTH WILL NOT BE APPROVED FOR USE UNTIL A MOISTURE CHECK IS RUN TO VERIFY MOISTURE CONTENT. MATERIAL SHIPPED TO PROJECTS WITHOUT BEING TESTED WILL NOT BE ACCEPTED.
4. AGGREGATE BASE COURSE, TYPE B, SHALL BE USED FOR TEMPORARY ACCESS TO ENTRANCES AND ALLEYS WHILE THE PROPOSED PAVEMENT IS UNDER CONSTRUCTION AND SHALL BE USED AS DIRECTED BY THE ENGINEER. THIS AGGREGATE SHALL NOT BE REUSED WITH THE AGGREGATE BASE COURSE BUT IS INCLUDED IN THE CONTRACT UNIT BID PRICE FOR AGGREGATE BASE COURSE, TYPE B.
5. TRENCH BACKFILL HAS BEEN CALCULATED USING THE IDOT TRENCH BACKFILL DETAIL.
6. IN THE EVENT OF DIMINISHED WEATHER CONDITIONS, WARM WATER MIX AND BLANKETS SHALL BE INCLUDED IN ALL APPLICABLE PAY ITEMS WITH NO ADDITIONAL COST TO THE OWNER. USE OF THESE COLD WEATHER PROVISIONS SHALL BE AT THE DIRECTION OF THE ENGINEER AND THE CITY OF FREEPORT.

UTILITIES

1. ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION OR HAVE THE POTENTIAL FOR CREATING FUTURE PROBLEMS SHALL BE REMOVED AND DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT OF WAY AT AN APPROVED LOCATION OBTAINED BY THE CONTRACTOR, ACCORDING TO ARTICLE 202.03 OF THE IDOT STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT BID PRICE FOR EARTH EXCAVATION. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
2. UTILITIES SHOWN ON THE PLANS ARE FOR ILLUSTRATIVE PURPOSES ONLY AND NO GUARANTEE OF THEIR ACCURACY IS MADE OR INFERRED. THE LOCATIONS OF EXISTING UTILITIES AS SHOWN ON THE DRAWINGS REPRESENT DATA RECEIVED FROM VARIOUS SOURCES. IT IS NOT GUARANTEED TO BE CORRECT OR ALL-INCLUSIVE. THE CONTRACTOR SHALL CONDUCT HIS OWN INVESTIGATION INTO THE LOCATION, SIZE, DEPTH, NUMBER AND NATURE OF ANY AND ALL EXISTING UTILITIES WHICH MAY INTERFERE WITH THE WORK UNDER THIS CONTRACT. ANY EXISTING UTILITIES WHICH ARE TO REMAIN IN SERVICE SHALL BE FULLY PROTECTED BY THE CONTRACTOR AND ANY DAMAGE CAUSED BY THE CONSTRUCTION OPERATIONS SHALL BE IMMEDIATELY REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE OWNER. THIS WORK SHALL BE AT THE CONTRACTORS EXPENSE.
3. THE CONTRACTOR SHALL CALL J.U.L.I.E. AT 811 OR 1-800-892-0123 48 HOURS PRIOR TO CONSTRUCTION FOR CONFIRMATION OF CURRENT UTILITY LOCATIONS AND FOR ALL NON-EMERGENCY WORK. UTILITIES WHICH ARE NOT MEMBERS OF J.U.L.I.E. SHOULD BE NOTIFIED INDIVIDUALLY BY THE CONTRACTOR.
4. ALL EXISTING CONDUITS WHICH WILL BE ABANDONED AND LEFT IN PLACE SHALL BE PLUGGED AT ALL OPEN ENDS WITH CONCRETE OR OTHER APPROVED MEANS. THE COST WILL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT BID PRICE FOR THE ITEM BEING REMOVED OR ABANDONED.
5. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN DRAINAGE FLOWS AT ALL TIMES DURING THE PERFORMANCE OF THE WORK. METHODS USED BY THE CONTRACTOR SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER. COST OF MAINTAINING DRAINAGE FLOWS SHALL BE INCLUDED IN THE VARIOUS UNIT PRICES FOR THE ITEMS BEING CONSTRUCTED. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
6. STATIONS AND OFFSETS FOR DRAINAGE STRUCTURES ARE TO THE CENTER OF THE STRUCTURE'S FRAME.
7. NEW MANHOLE AND INLET LIDS ON THIS PROJECT SHALL HAVE THE WORDS "STORM" OR "SANITARY" ON THE LID. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE APPROPRIATE TYPE OF LID. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THIS WORK.
8. IF DURING MANHOLE ADJUSTMENTS, THE CONTRACTOR DAMAGES EITHER THE FRAME OR LID, HE SHALL FURNISH A NEW FRAME AND LID, AT NO ADDITIONAL COST. ALL NEW MANHOLES AND INLETS AND THOSE TO BE RECONSTRUCTED SHALL BE CONSTRUCTED WITH NEW FRAMES AND GRATES.
9. IF DURING INLET ADJUSTMENTS, THE CONTRACTOR DAMAGES EITHER THE FRAME, CURB BOX OR GRATE, HE SHALL FURNISH A NEW FRAME, GRATE AND CURB BOX, AT NO ADDITIONAL COST.
10. ALL SANITARY SEWER MANHOLES ARE TO BE ADJUSTED PER CITY OF FREEPORT STANDARDS. ALL EXISTING CASTINGS SHALL BE RETURNED TO THE WATER AND SEWER COMMISSION IN EXCHANGE FOR NEW CASTINGS PROVIDED BY THE COMMISSION. THE CONTRACTOR SHALL USE HIGH EARLY STRENGTH CONCRETE, PP-2 AND POLYETHYLENE MOLDED ADJUSTING RINGS ON ALL ADJUSTMENTS. THE CONTRACTOR SHALL ADVISE THE CITY WHEN MANHOLE ADJUSTMENTS ARE TO BE MADE, SO THEY MAY WITNESS THEM.
11. THE COST OF MAKING CONNECTIONS TO EXISTING STORM SEWER PIPES OR STRUCTURES SHALL BE INCLUDED IN THE VARIOUS STORM SEWER ITEMS.

SHEET REVIEW	REVISIONS	SCALE: NA		GENERAL NOTES	SHEET NO.
AGENCY	NO.	DRAWN BY: KRL		FREEPORT STREET IMPROVEMENTS	2
DATE	ITEM	CHECKED BY: BSL		CITY OF FREEPORT	OF
		DATE: 6/29/2015		FREEPORT, ILLINOIS	32
			FILE: H:\14-016-FREEPORTSTREETIMP\DESIGN\DRAWING\016 GENERAL NOTES.DWG MEAT JOB 04-28-14-016		

SUMMARY & SCHEDULE OF QUANTITIES

S.P.	ITEM NO.	DESCRIPTION	UNIT	CONSTRUCTION CODE			TOTAL QUANTITY
				HARLEM	STEPHENSON	LINCOLN	
	20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	27			27
	20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	182	40		222
	30200100	EARTH EXCAVATION	CU YD	385	25		420
	30900150	TRENCH BACKFILL	CU YD	91	80		171
	28000400	PERIMETER EROSION BARRIER	FOOT	3787	1150		4937
	28000500	INLET AND PIPE PROTECTION	EACH	7	6	2	15
	35101400	AGGREGATE BASE COURSE, TYPE B	TON	855	275		1130
	40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	3920	1804	2653	8377
	40600825	LEVELING BINDER (MACHINE METHOD), N50	TON	80	13		93
	40600825	POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50	TON	385	180	265	830
	40603575	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	595	275	405	1275
	42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	1079	185		1264
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	3284	2454		5748
	42400800	DETECTABLE WARNINGS	SQ FT	164	80		244
	44000100	PAVEMENT REMOVAL	SQ YD	1098	730		1828
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	997	330		1327
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	3942	1273		5215
	44000900	SIDEWALK REMOVAL	SQ FT	4009	2930		6939
	44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	4211	1300		5511
	500A0040	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	396	397		793
	50100500	STORM SEWER REMOVAL 12"	FOOT	130	97		227
	56109210	WATER VALVES TO BE ADJUSTED	EACH	2	2		4
	56400510	FIRE HYDRANTS TO BE REMOVED AND REPLACED	EACH	2	2		4
	60221100	MANHOLES, TYPE A, 5-DIAMETER, TYPE 1 FRAME CLOSED LID	EACH	2	2		4
	60260100	INLETS TO BE ADJUSTED	EACH	1	1		2
	60266800	VALVE BOXES TO BE ADJUSTED	EACH	2	8		10
	60500040	REMOVING MANHOLES	EACH	2	1		3
	60500060	REMOVING INLETS	EACH	6	2		8
	60609900	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06	FOOT	37			37
	60609900	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.18	FOOT	4185	1205		5390
	60610400	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24	FOOT	31			31
	60625610	ISLAND PAVEMENT (8')	SQ YD	10			10
	67100100	MOBILIZATION	L SUM	0.5	0.2	0.3	1
	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	24			24
	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3572	164	172	3908
	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	539	592	390	1521
	78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	391			391
	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	197	258		455
	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	80	69		149
	A2000116	TREE, ACER X FREEMANII AUTUMN BLAZE (AUTUMN BLAZE FREEMAN MAPLE), 2" CALIPER, BALLED AND BURLAPPED	EACH	4	2		6
	A2008116	TREE, TILIA CORDATA GREENSPIRE (GREENSPIRE LITTLE LEAF LINDEN), 2" CALIPER, BALLED AND BURLAPPED	EACH	4	2		6
	X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	5661	2768	4956	13405
	X4402805	ISLAND REMOVAL	SQ FT	125			125
	X5099900	ABANDON AND FILL EXISTING STORM SEWER	FOOT	28	25		53
	X8024240	INLETS, SPECIAL	EACH	7	4		11
	X8028050	SANITARY MANHOLES TO BE ADJUSTED	EACH	10		7	17
	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	0.5	0.2	0.3	1
	X8000970	PARKWAY RESTORATION	L SUM	0.8	0.2		1
	X8008821	CONCRETE TRUCK WASHOUT	L SUM	0.5	0.5		1
	X8007452	RELOCATE BOLLARDS	EACH	2			2
	Z0013798	CONSTRUCTION LAYOUT	L SUM	0.5	0.2	0.3	1

▲ SPECIALTY ITEMS

* SPECIAL PROVISION

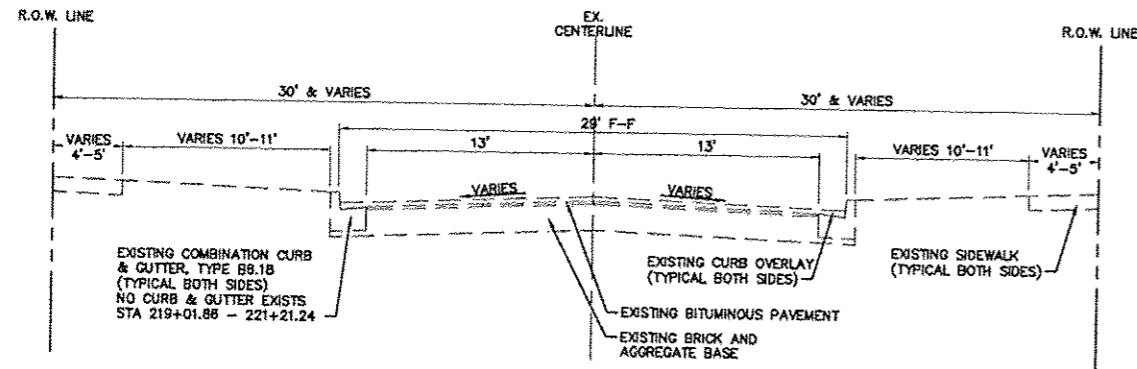
		UTILITY ADJUSTMENTS / REMOVALS						
		65109210	56400510	60260100	60266800	60500040	60500060	X8026050
		WATER VALVES TO BE ADJUSTED	FIRE HYDRANTS TO BE REMOVED AND REPLACED	INLETS TO BE ADJUSTED	VALVE BOXES TO BE ADJUSTED	REMOVING MANHOLES	REMOVING INLETS	SANITARY MANHOLES TO BE ADJUSTED
STATION	OFFSET	EACH	EACH	EACH	EACH	EACH	EACH	EACH
HARLEM								
200+08.11	LT							1
200+08.47	LT				1			
200+08.66	LT				1			
202+62.31	RT							1
204+96.76	RT						1	
205+15.30	LT						1	
206+16.70	RT					1		
206+21.18	RT							1
207+41.28	RT							1
209+51.90	RT							1
211+70.27	RT							1
213+91.83	RT							1
215+16.88	RT						1	
215+32.13	RT					1		
215+48.64	RT							1
215+61.16	LT						1	
215+80.02	RT						1	
215+86.79	LT						1	
216+67.47	RT							1
218+74.06	RT							1
STEPHENSON								
100+91.50	LT							1
101+10.79	RT				1			
101+12.33	LT							1
101+17.73	LT	1						
104+66.51	RT				1			
105+62.50	LT	1						
105+75.92	RT				1			
106+05.96	RT						1	
106+07.26	LT						1	
106+11.59	LT				1			
106+11.67	LT			1				
106+23.94	LT							1
106+24.89	LT							1
106+34.20	LT							1
106+37.17	LT							1
106+41.43	LT							1
LINCOLN								
300+42.27	LT							1
302+70.77	-							1
304+03.09	-							1
305+28.61	-							1
307+26.81	-							1
308+15.15	-							1
310+70.89	RT							1
TOTAL		2	2	1	10	3	8	17

		PAVEMENT MARKING					
		78000100	78000200	78000400	78000500	78000600	78000650
		THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	THERMOPLASTIC PAVEMENT MARKING - LINE 24"
STATION	STATION	SQ FT	FOOT	FOOT	FOOT	FOOT	FOOT
HARLEM							
-	200+15.73					36	
-	200+17.25					37	
-	200+27.16					41	
-	200+23.82					37	
200+15.73	200+17.25						66
200+24.80	200+28.49				36		
200+24.80	200+28.49						59
200+27.16	200+23.82						72
200+31.52	200+34.63			30			
200+36.58	200+36.58						10
200+36.58	204+91.09		900				
205+04.95	205+39.52					35	
205+07.38	205+26.52					29	
205+21.03	205+36.17						15
205+52.67	210+47.66		990				
210+61.81	210+91.73				30		
210+64.04	210+89.41				25		
210+74.53	210+89.25						15
211+05.77	213+64.88		518				
213+80.74	214+02.94				33		
213+81.32	213+91.66						10
213+81.36	214+02.20				25		
214+18.72	215+18.21		199				
215+30.25	215+67.06				37		
215+33.99	215+63.75				30		
215+47.19	215+62.92						18
215+79.47	218+46.20		533				
218+55.74	218+88.27				33		
218+61.05	218+85.83				25		
218+73.88	218+85.69						12
219+01.77	220+64.48		325				
ON STEPHENSON							
PARKING STALLS			98				
STEPHENSON							
100+21.00	100+65.00		88				
100+92.48	101+43.58					55	
100+97.08	101+37.10					43	
101+63.13	106+08.76					110	
103+79.81	-					24	
103+89.81	-					24	
104+39.81	-		24				
106+12.69	106+11.44					41	
106+19.26	106+17.86					47	
106+22.86	106+64.83					42	
106+23.05	106+62.99					40	
106+24.64	106+62.65					38	
106+25.20	106+64.50					39	
106+25.40	106+45.05						20
106+43.78	106+62.54						19
106+68.84	106+67.50					46	
106+74.68	106+74.68				43		
PARKING STALLS							
ISLAND			75				
MIDBLOCK CROSSING						188	102
LINCOLN						203	156
300+41.53		312+11.00					30
PARKING STALLS							
TOTAL		24	3908	1521	391	455	149

STORM SEWER					
		X5099900	50100500	500A0040	20800150
		ABANDON AND FILL EXISTING STORM SEWER	STORM SEWER REMOVAL 12"	STORM SEWERS, CLASS A, TYPE 2 12"	TRENCH BACKFILL
STATION	STATION	FOOT	FOOT	FOOT	CU YD
HARLEM					
204+91.50	205+16.75			28	10
204+96.76	205+16				

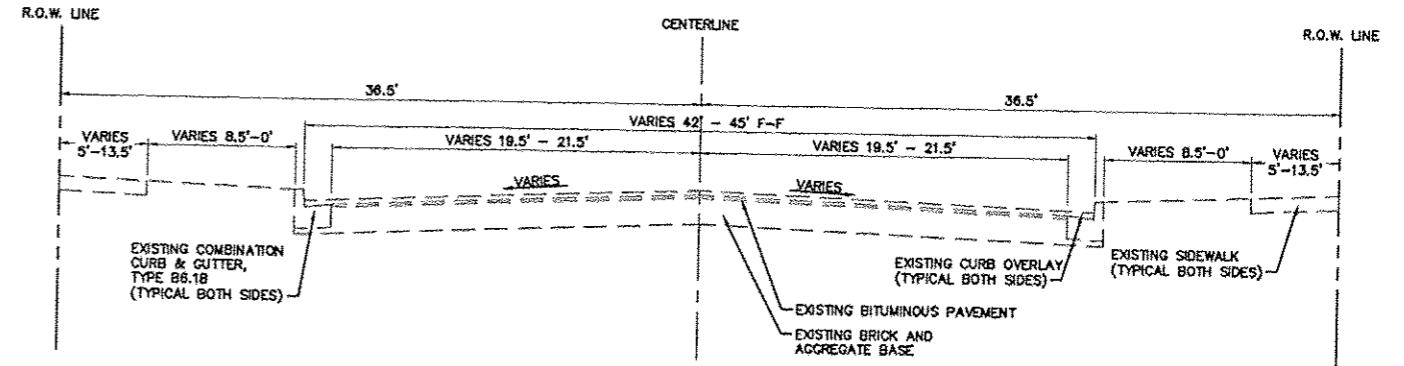
TYPICAL SECTIONS

HARLEM AVENUE

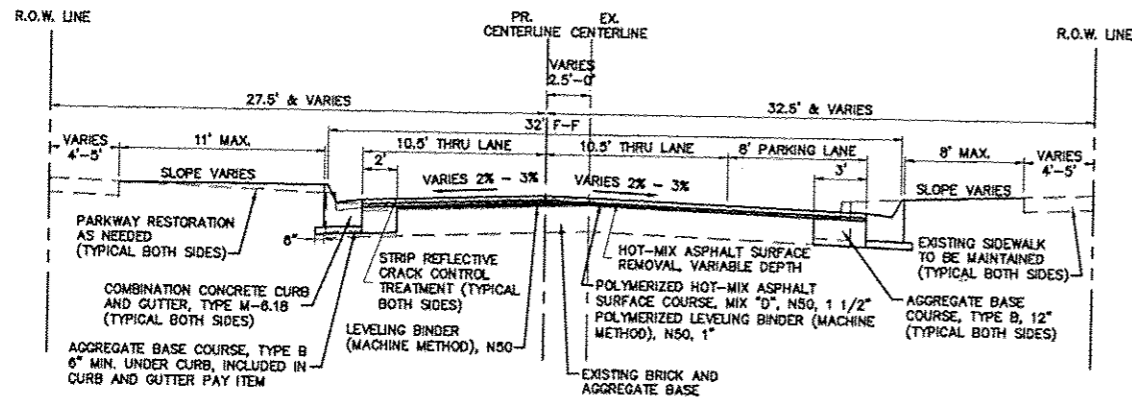


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STA 200+00.00 - 221+21.24

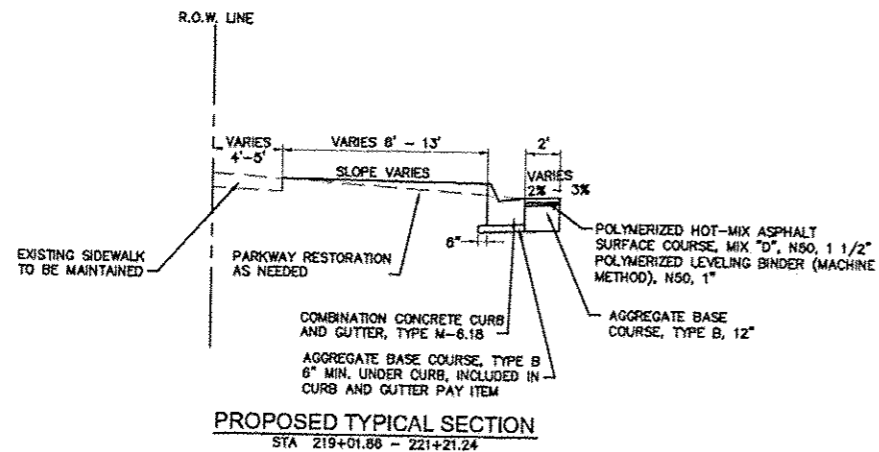
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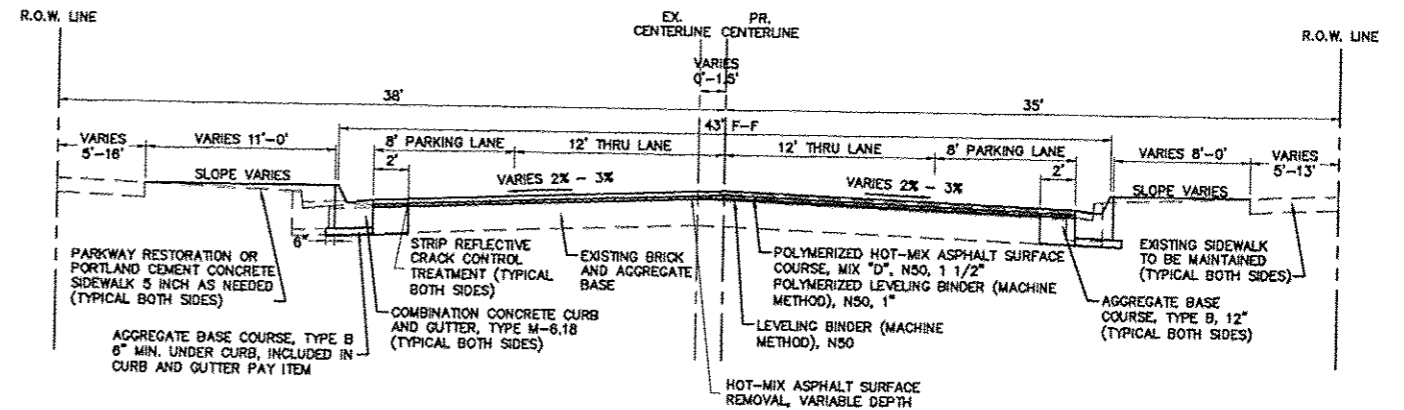
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PROPOSED TYPICAL SECTION
STA 200+00.00 - 219+01.86



PROPOSED TYPICAL SECTION
STA 219+01.86 - 221+21.24



PROPOSED TYPICAL SECTION
STA 100+21.00 - 106+83.59

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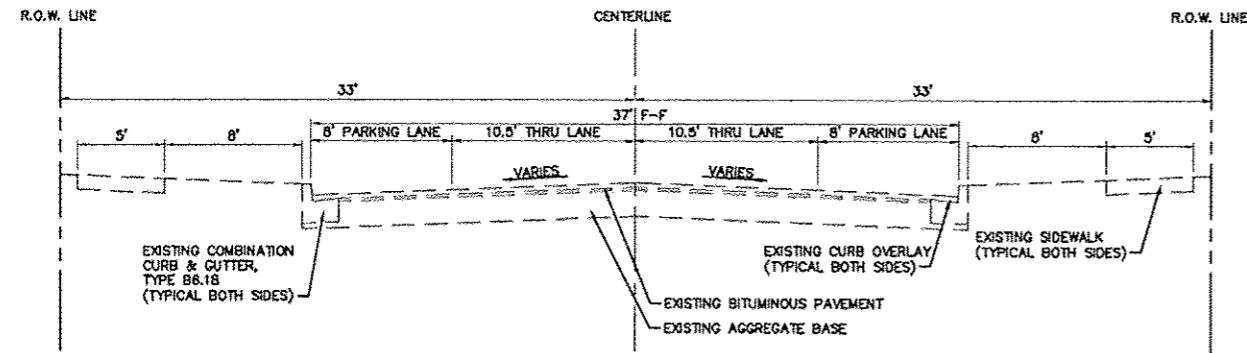
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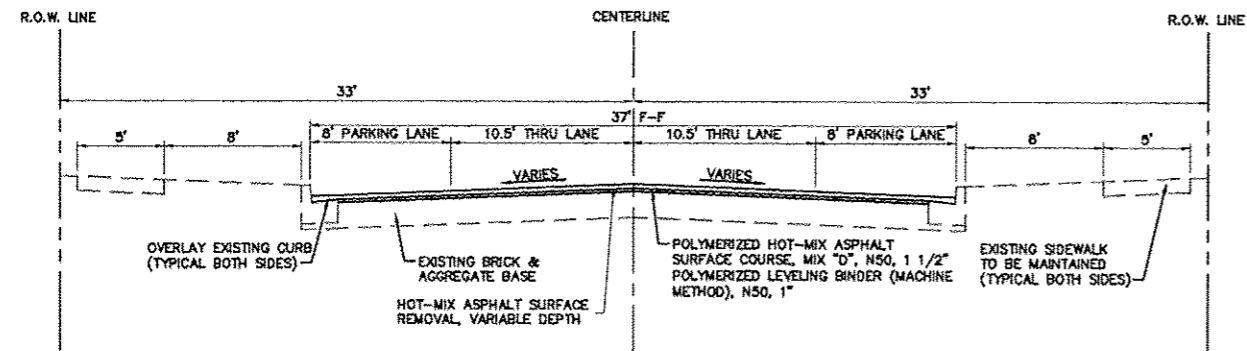
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CITY OF FREEPORT
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FREEPORT, ILLINOIS
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TYPICAL SECTIONS

LINCOLN BOULEVARD

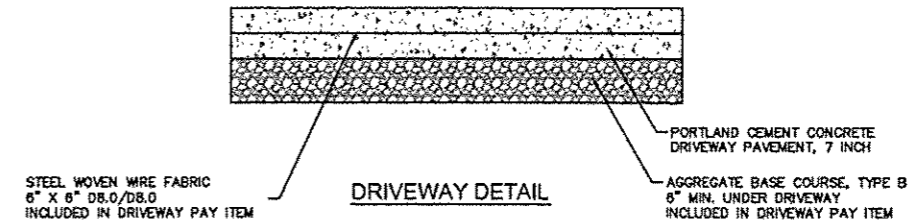


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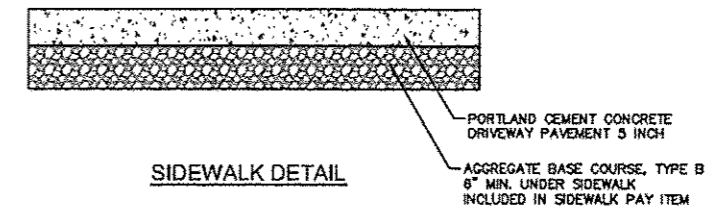


PROPOSED TYPICAL SECTION
STA 300+41.53 - 312+11.00

DETAILS



DRIVEWAY DETAIL



SIDEWALK DETAIL

LOCATION AND MIXTURE USES	RESURFACING	
	SURFACE	LEVELING BINDER
PG:	PG SBS 64-28	PG 64-22
DESIGN AIR Voids	3.0 @ N50	3.0 @ N50
COMPOSITION (GRADATION MIXTURE)	IL 9.5	IL 9.5FG*
FRICTION AGGREGATE	D	D
MIX UNIT WEIGHT	115 LBS/SY-IN	112 LBS/SY-IN
MANAGEMENT PROGRAM TO BE USED	QC/QA	QC/QA

HMA TABLE

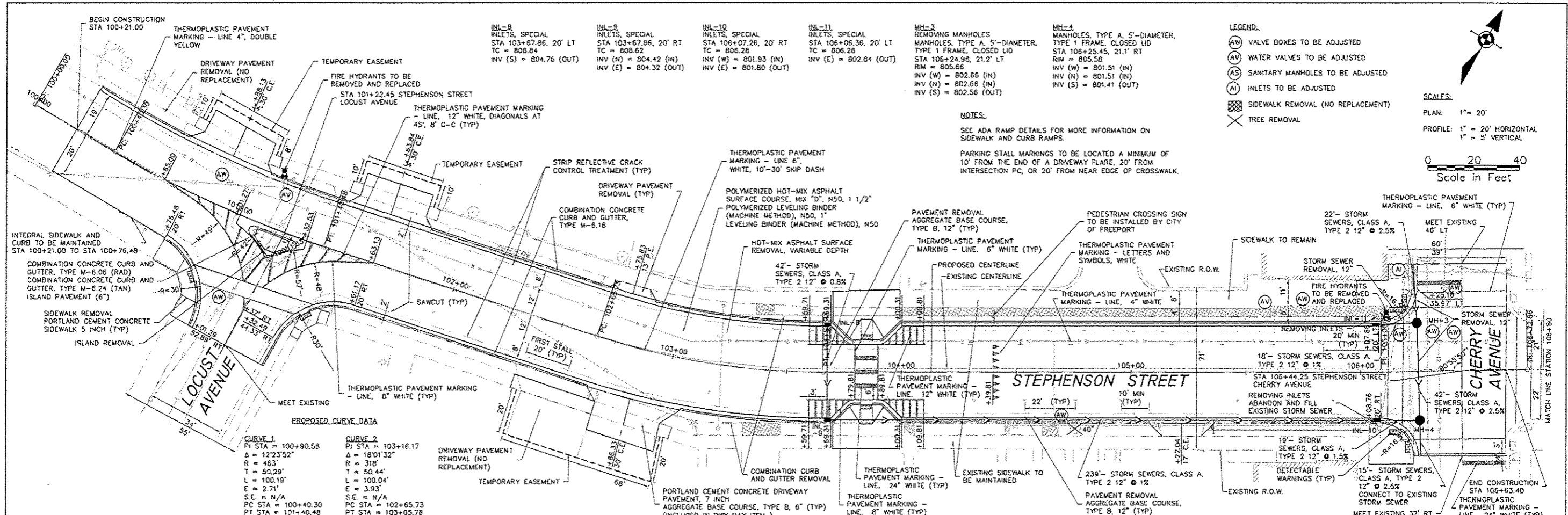
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NO.	ITEM	DATE

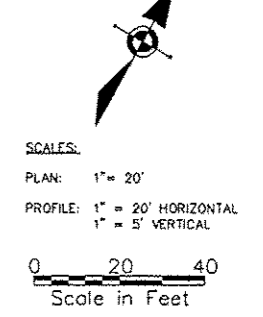
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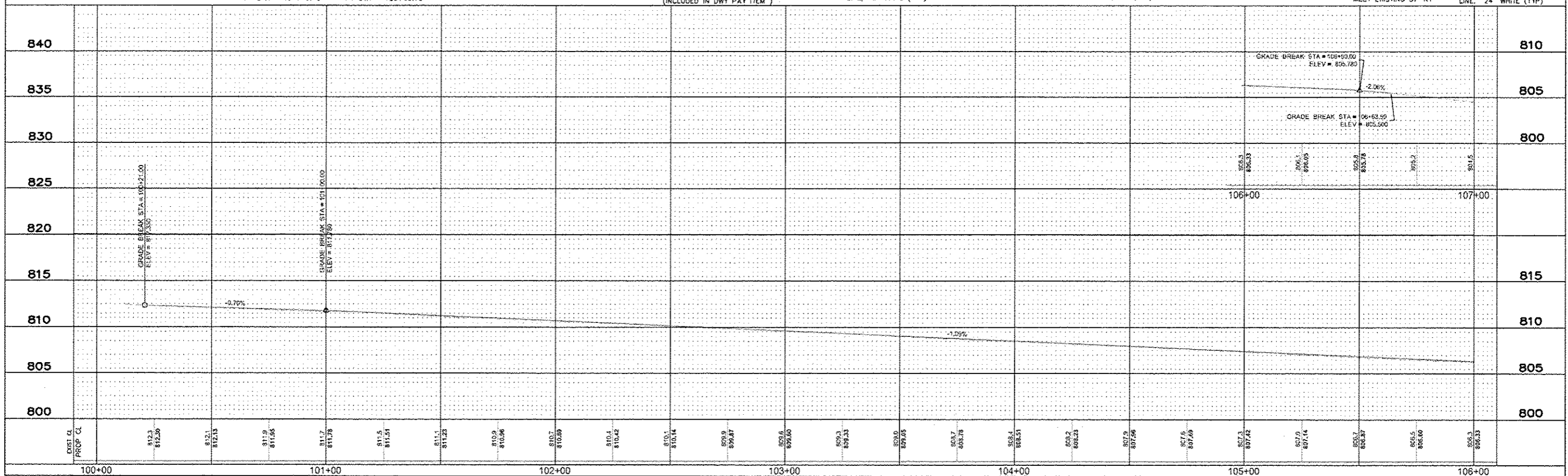


- LEGEND**
- (AW) VALVE BOXES TO BE ADJUSTED
 - (W) WATER VALVES TO BE ADJUSTED
 - (AS) SANITARY MANHOLES TO BE ADJUSTED
 - (AI) INLETS TO BE ADJUSTED
 - (X) SIDEWALK REMOVAL (NO REPLACEMENT)
 - (X) TREE REMOVAL



PROPOSED CURVE DATA

CURVE 1	CURVE 2
PI STA = 100+90.58	PI STA = 103+16.17
Δ = 122°3'52"	Δ = 18°0'32"
R = 463'	R = 318'
T = 50.29'	T = 50.44'
L = 100.19'	L = 100.04'
E = 2.71'	E = 3.93'
S.C. = N/A	S.E. = N/A
PC STA = 100+40.30	PC STA = 102+65.73
PT STA = 101+40.48	PT STA = 103+65.78



STATION	ELEVATION
100+00	812.3
100+10	812.50
100+20	812.13
100+30	811.9
100+40	811.66
100+50	811.7
100+60	811.78
100+70	811.5
100+80	811.51
100+90	811.1
101+00	811.28
101+10	810.9
101+20	810.96
101+30	810.7
101+40	810.69
101+50	810.1
101+60	810.42
101+70	810.1
101+80	810.14
101+90	809.9
102+00	809.87
102+10	809.6
102+20	809.60
102+30	809.3
102+40	809.33
102+50	809.0
102+60	809.05
102+70	808.7
102+80	808.16
102+90	808.4
103+00	808.51
103+10	808.2
103+20	808.28
103+30	807.9
103+40	807.56
103+50	807.5
103+60	807.69
103+70	807.1
103+80	807.14
103+90	806.7
104+00	806.37
104+10	806.5
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104+40	806.33

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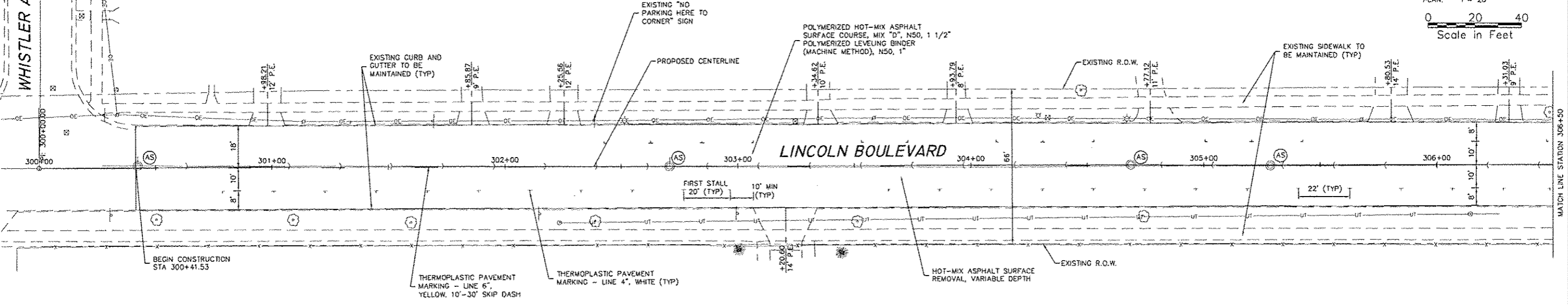
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PLAN AND PROFILE - STEPHENSON STREET
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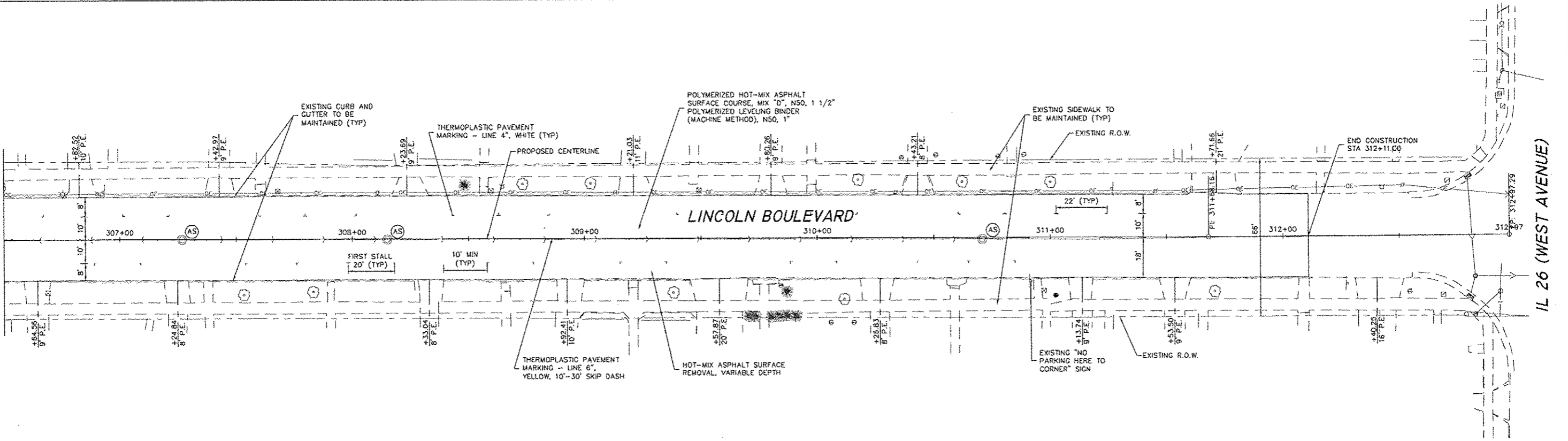
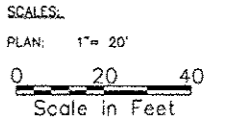
SHEET NO.
 10
 OF
 32

WHISTLER AVENUE



NOTE:
PARKING STALL MARKINGS TO BE LOCATED A MINIMUM OF 10' FROM THE END OF A DRIVEWAY FLARE, 20' FROM INTERSECTION PC, OR 20' FROM NEAR EDGE OF CROSSWALK.

- LEGEND:
- (AW) VALVE BOXES TO BE ADJUSTED
 - (AV) WATER VALVES TO BE ADJUSTED
 - (AS) SANITARY MANHOLES TO BE ADJUSTED
 - (AM) MANHOLES TO BE ADJUSTED (STORM)
 - ☒ SIDEWALK REMOVAL (NO REPLACEMENT)



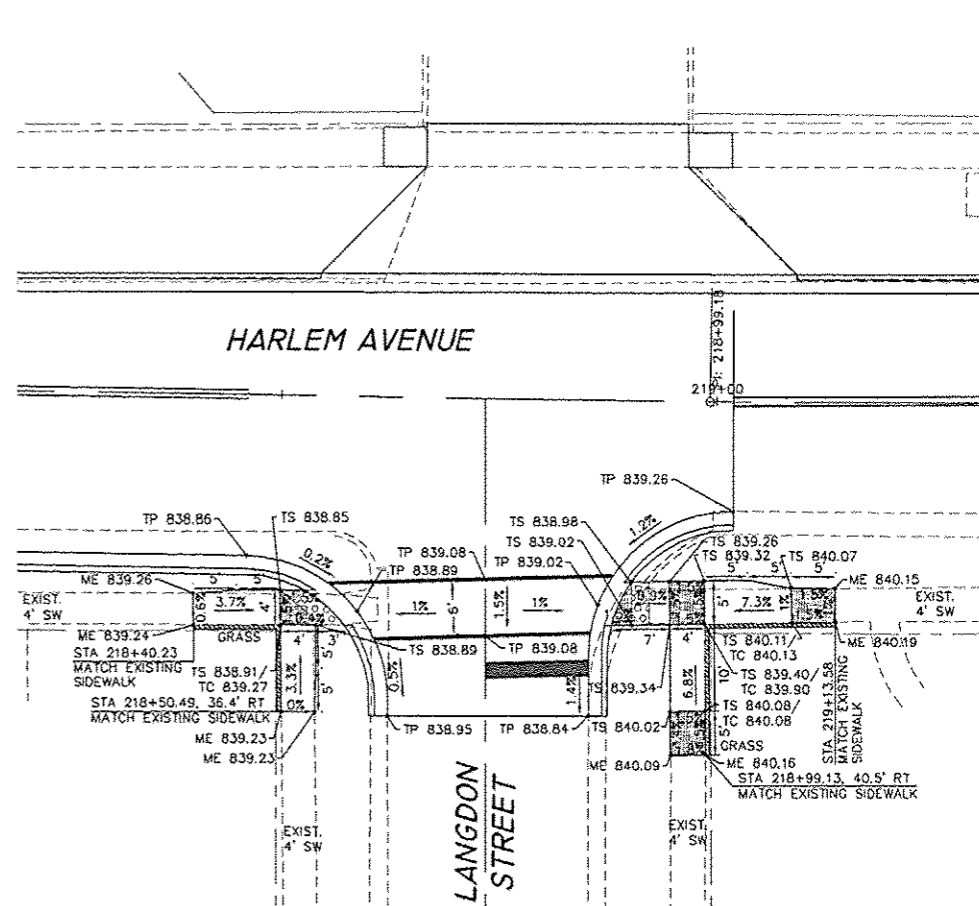
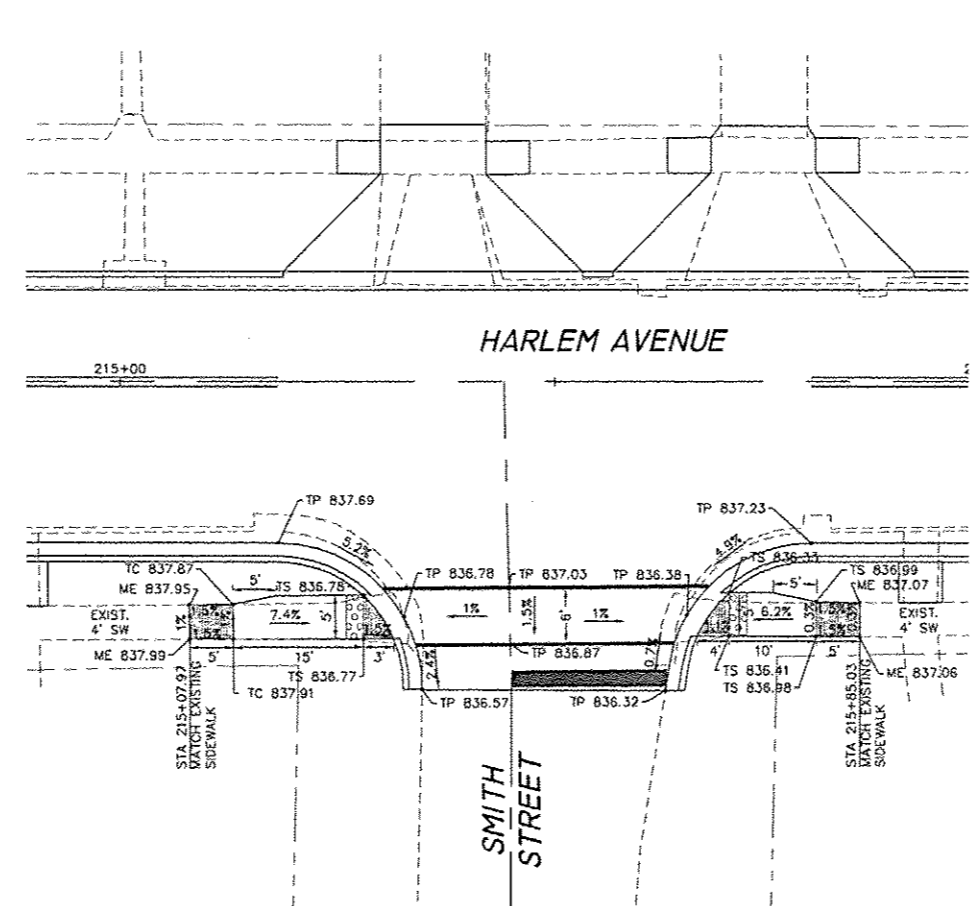
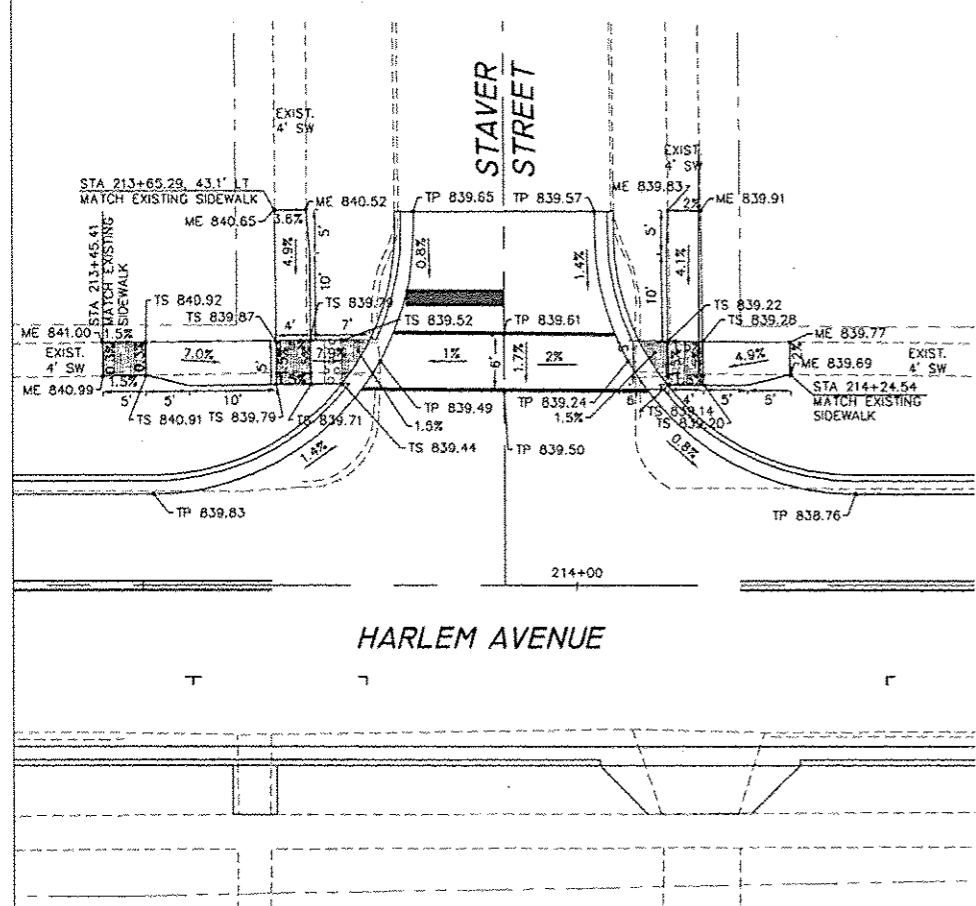
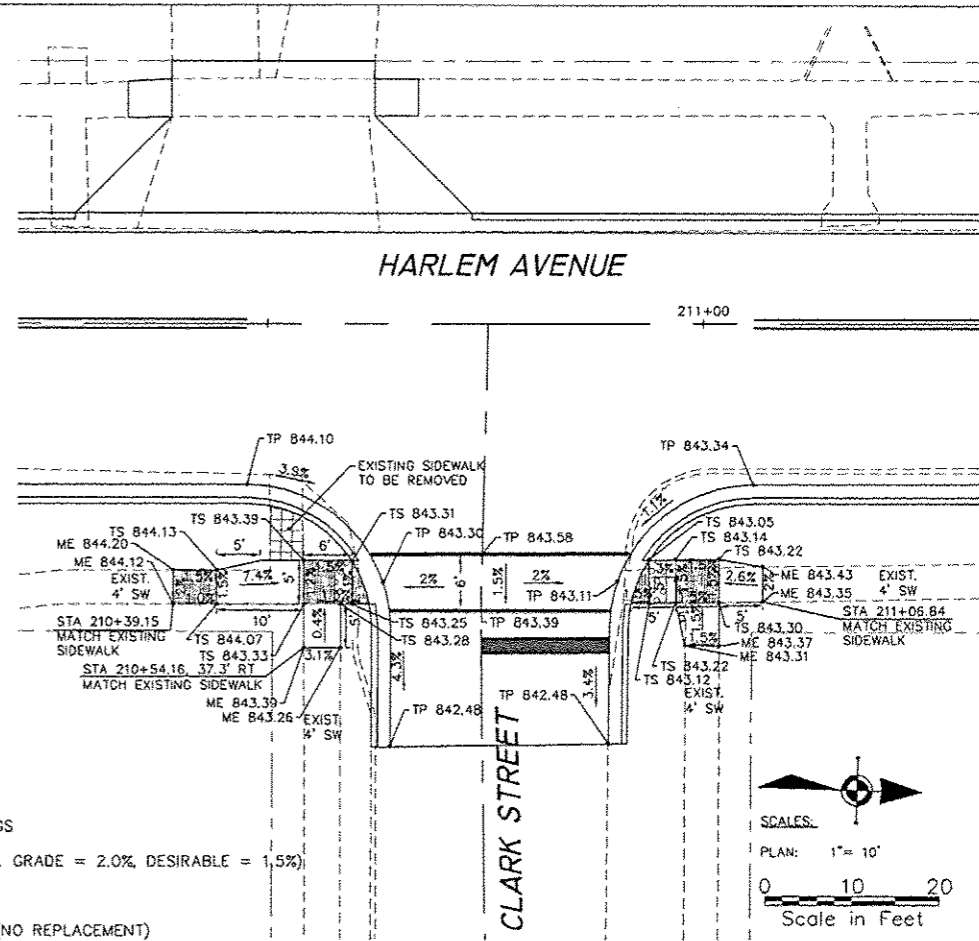
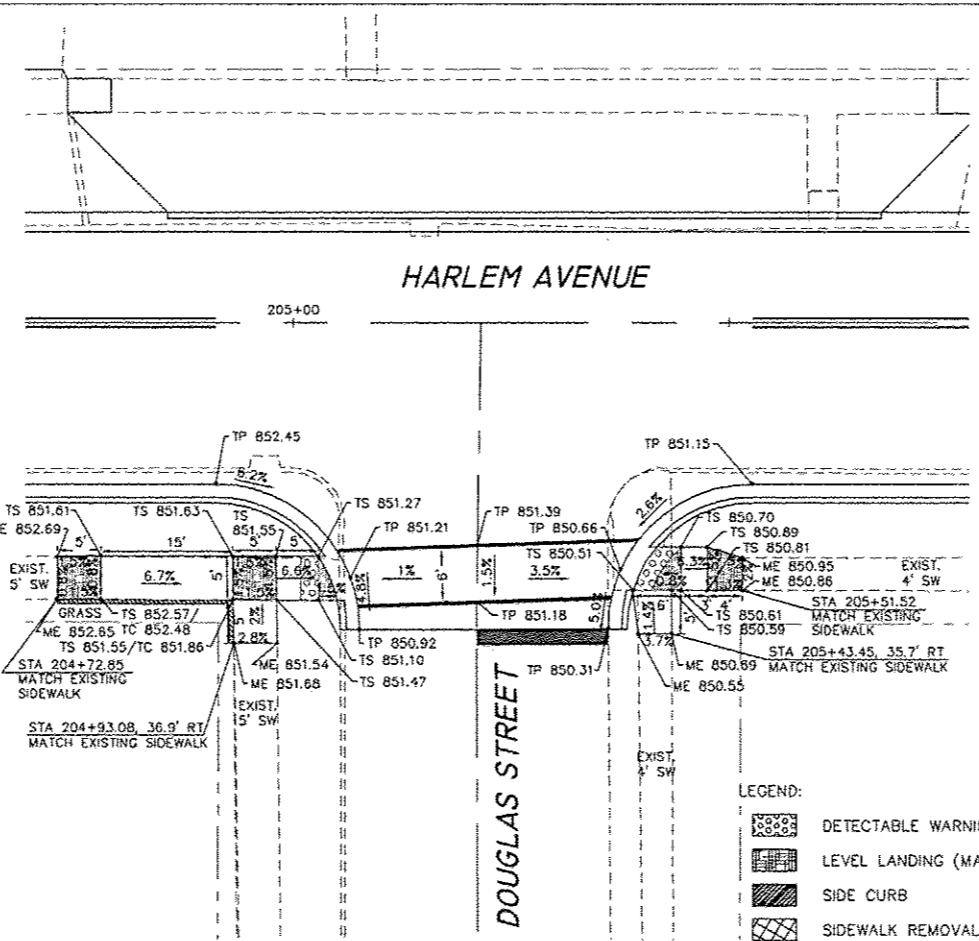
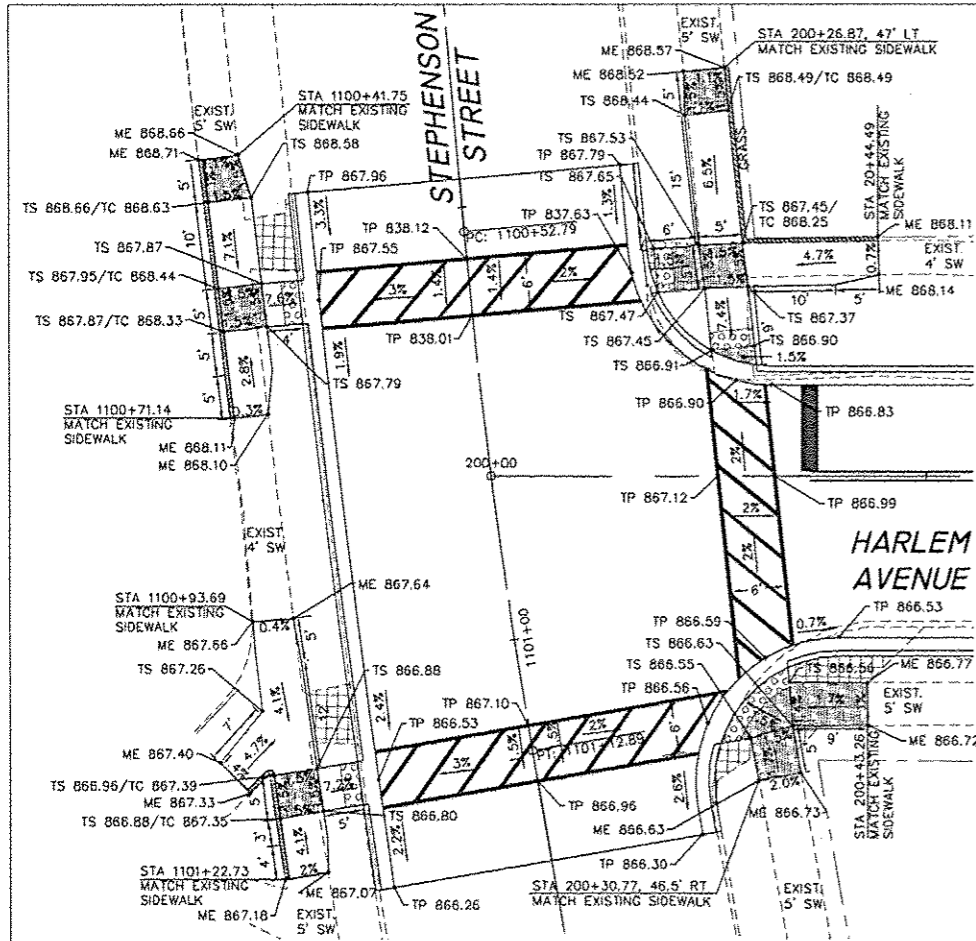
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PLAN AND PROFILE - LINCOLN BOULEVARD
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ADA RAMP DETAILS - HARLEM AVENUE
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SHEET NO.
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 OF
 32

STORM WATER POLLUTION PREVENTION NOTES:

1. THE STORM WATER POLLUTION PREVENTION PLAN IS COMPRISED OF THE DRAWING MAPS, SPECIFICATIONS AND THE PERMIT AND ALL SUBSEQUENT REPORTS AND RELATED DOCUMENTS.
2. ALL CONTRACTORS AND SUBCONTRACTORS INVOLVED WITH STORM WATER POLLUTION PREVENTION SHALL OBTAIN A COPY OF THE STORM WATER POLLUTION PREVENTION PLAN, ILLINOIS EPA NPDES GENERAL PERMIT, CITY OF FREEPORT NPDES PERMIT, AND BECOME FAMILIAR WITH THEIR CONTENTS.
3. CONTRACTOR SHALL IMPLEMENT BEST MANAGEMENT PRACTICES AS REQUIRED BY THE SWPPP AND IN CONFORMANCE WITH FEDERAL, STATE, OR LOCAL REQUIREMENTS OR MANUAL OF PRACTICE, AS APPLICABLE. ADDITIONAL BEST MANAGEMENT PRACTICES SHALL BE IMPLEMENTED AS DICTATED BY CONDITIONS AT NO ADDITIONAL COST TO OWNER THROUGHOUT ALL PHASES OF CONSTRUCTION.
4. GENERAL CONTRACTOR SHALL DENOTE ON EXHIBITS THE TEMPORARY PARKING AND STORAGE AREA WHICH SHALL ALSO BE USED AS THE EQUIPMENT MAINTENANCE AND CLEANING AREA.
5. ALL WASH WATER (CONCRETE TRUCKS, VEHICLE CLEANING, EQUIPMENT CLEANING, ETC.) SHALL BE DETAINED AND PROPERLY TREATED OR DISPOSED. WASHOUTS ARE TO BE PROVIDED AND SHALL BE PAID FOR PER LUMP SUM FOR CONCRETE TRUCK WASHOUTS.
6. SUFFICIENT OIL AND GREASE ABSORBING MATERIALS AND FLOTATION BOOMS SHALL BE MAINTAINED ON SITE OR READILY AVAILABLE TO CONTAIN AND CLEAN-UP FUEL OR CHEMICAL SPILLS AND LEAKS.

STORM WATER POLLUTION PREVENTION NOTES:

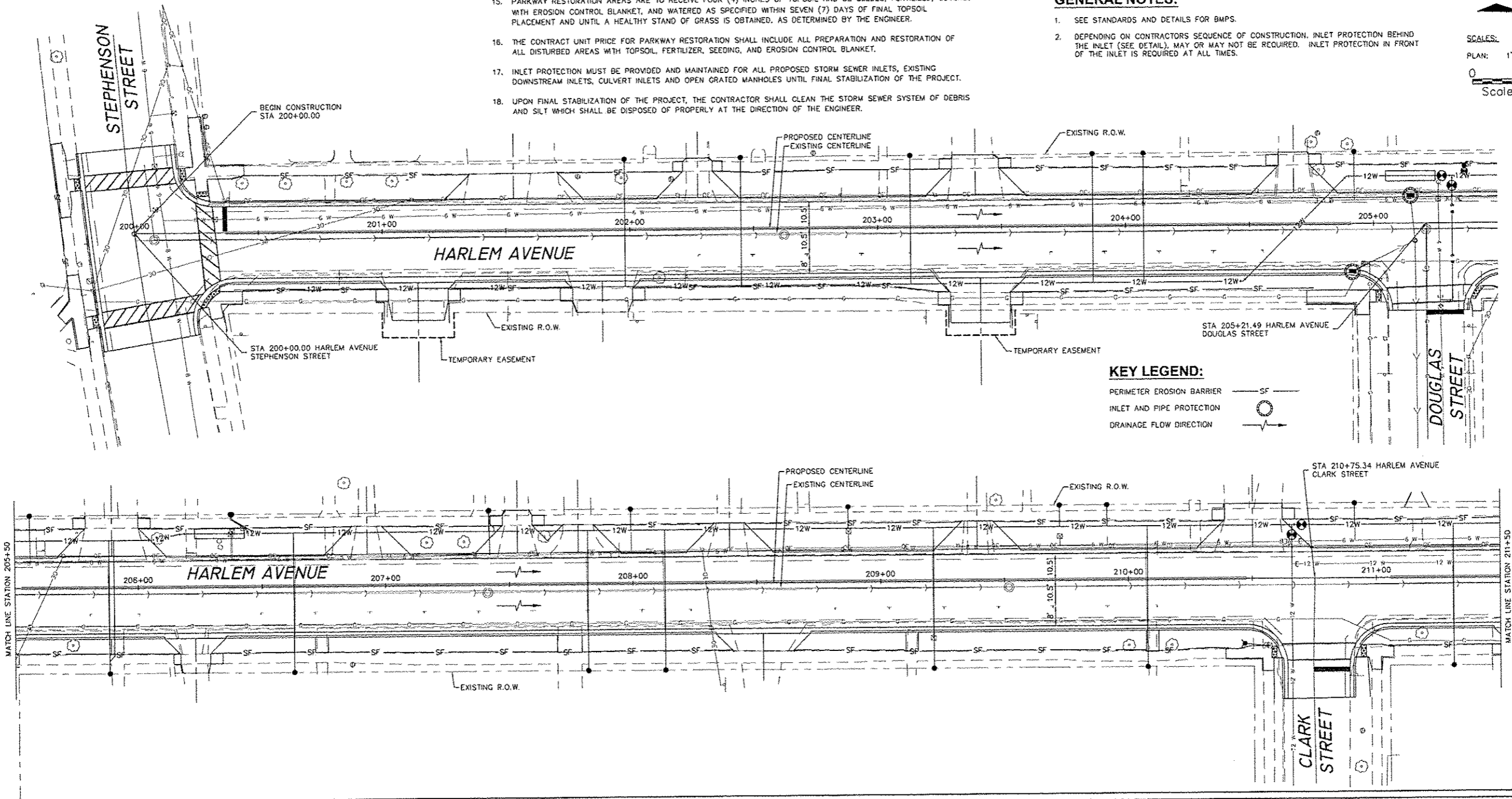
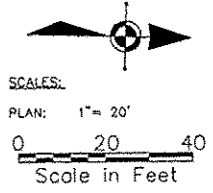
8. ALL MATERIALS SPILLED, DROPPED, WASHED OR TRACKED FROM VEHICLES ONTO ADJACENT ROADWAYS OR INTO STORM DRAINS MUST BE REMOVED IMMEDIATELY. ALL PAVEMENT SHALL BE THOROUGHLY CLEANED AT THE END OF EACH WORKING DAY.
9. CONTRACTORS OR SUBCONTRACTORS WILL BE RESPONSIBLE FOR REMOVING SEDIMENT THAT MAY HAVE COLLECTED IN THE STORM SEWER DRAINAGE SYSTEMS IN CONJUNCTION WITH THE STABILIZATION OF THE SITE.
10. ALL CONSTRUCTION SHALL BE STABILIZED AT THE END OF EACH WORKING DAY, THIS INCLUDES BACKFILLING OF TRENCHES FOR UTILITY CONSTRUCTION AND PLACEMENT OF GRAVEL OR BITUMINOUS PAVING FOR ROAD CONSTRUCTION.
11. THE EROSION CONTROL MEASURES INDICATED ON THE DRAWINGS ARE THE MINIMUM REQUIREMENTS.
12. DETAIL DRAWINGS SHOWN ARE RECOMMENDATIONS ONLY. ALL EROSION CONTROL MEASURES MUST BE INSTALLED, INSPECTED AND APPROVED BY THE CITY OF FREEPORT OR THEIR REPRESENTATIVES, BEFORE CONSTRUCTION ACTIVITIES BEGIN.
13. SILTATION BAGS SHALL BE USED TO PROTECT THE INLETS FROM SILTATION AND TO GATHER SILTATION FOR QUANTIFICATION, DOCUMENTATION AND DISPOSAL OF DEBRIS AND SILT.
14. ALL DISTURBED AREAS THAT WILL BE LEFT IDLE FOR MORE THAN THIRTY (30) DAYS SHALL BE TEMPORARILY SEEDED WITHIN SEVEN (7) DAYS.
15. PARKWAY RESTORATION AREAS ARE TO RECEIVE FOUR (4) INCHES OF TOPSOIL AND BE SEEDED, FERTILIZED, COVERED WITH EROSION CONTROL BLANKET, AND WATERED AS SPECIFIED WITHIN SEVEN (7) DAYS OF FINAL TOPSOIL PLACEMENT AND UNTIL A HEALTHY STAND OF GRASS IS OBTAINED, AS DETERMINED BY THE ENGINEER.
16. THE CONTRACT UNIT PRICE FOR PARKWAY RESTORATION SHALL INCLUDE ALL PREPARATION AND RESTORATION OF ALL DISTURBED AREAS WITH TOPSOIL, FERTILIZER, SEEDING, AND EROSION CONTROL BLANKET.
17. INLET PROTECTION MUST BE PROVIDED AND MAINTAINED FOR ALL PROPOSED STORM SEWER INLETS, EXISTING DOWNSTREAM INLETS, CULVERT INLETS AND OPEN GRATED MANHOLES UNTIL FINAL STABILIZATION OF THE PROJECT.
18. UPON FINAL STABILIZATION OF THE PROJECT, THE CONTRACTOR SHALL CLEAN THE STORM SEWER SYSTEM OF DEBRIS AND SILT WHICH SHALL BE DISPOSED OF PROPERLY AT THE DIRECTION OF THE ENGINEER.

MAINTENANCE NOTES:

- ALL BMP MEASURES STATED ON THIS EROSION CONTROL PLAN SHALL BE MAINTAINED IN FULLY FUNCTIONAL CONDITION UNTIL NO LONGER REQUIRED FOR A COMPLETED PHASE OF WORK OR FINAL STABILIZATION OF THE ROADWAY PROJECT. ALL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE CHECKED BY A QUALIFIED PERSON AT LEAST ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A 0.5" RAINFALL EVENT, AND CLEANED AND REPAIRED IN ACCORDANCE WITH THE FOLLOWING:
- a. INLET PROTECTION DEVICES AND BARRIERS SHALL BE REPAIRED OR REPLACED IF THEY SHOW SIGNS OF UNDERMINING, OR DETERIORATION.
 - b. ALL SEEDED AREAS SHALL BE CHECKED REGULARLY TO SEE THAT A GOOD STAND IS MAINTAINED. AREAS SHOULD BE FERTILIZED, WATERED, AND RE-SEEDED AS NEEDED.
 - c. SILT FENCES SHALL BE REPAIRED TO THEIR ORIGINAL CONDITIONS IF DAMAGED. SEDIMENT SHALL BE REMOVED FROM THE SILT FENCES WHEN IT REACHES ONE-HALF THE HEIGHT OF THE SILT FENCE.
 - d. THE CONSTRUCTION AREA ACCESSES SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOW OF MUD ONTO ADJACENT PUBLIC ROADWAYS AND RIGHTS-OF-WAY OUTSIDE WORK AREA.
 - e. SILT BAGS IN REQUIRED INLET STRUCTURES SHALL BE MAINTAINED IN OPERATIONAL CONDITIONS AT ALL TIMES. SEDIMENT FROM SILT BAGS SHALL BE REMOVED FROM BAGS AND QUANTIFIED AND REPORTED WEEKLY TO THE ENGINEER.

GENERAL NOTES:

1. SEE STANDARDS AND DETAILS FOR BMPs.
2. DEPENDING ON CONTRACTORS SEQUENCE OF CONSTRUCTION, INLET PROTECTION BEHIND THE INLET (SEE DETAIL), MAY OR MAY NOT BE REQUIRED. INLET PROTECTION IN FRONT OF THE INLET IS REQUIRED AT ALL TIMES.



KEY LEGEND:

- PERIMETER EROSION BARRIER — SF
- INLET AND PIPE PROTECTION (circle with cross)
- DRAINAGE FLOW DIRECTION (arrow)

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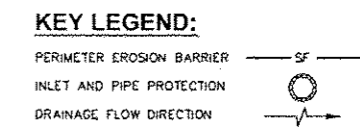
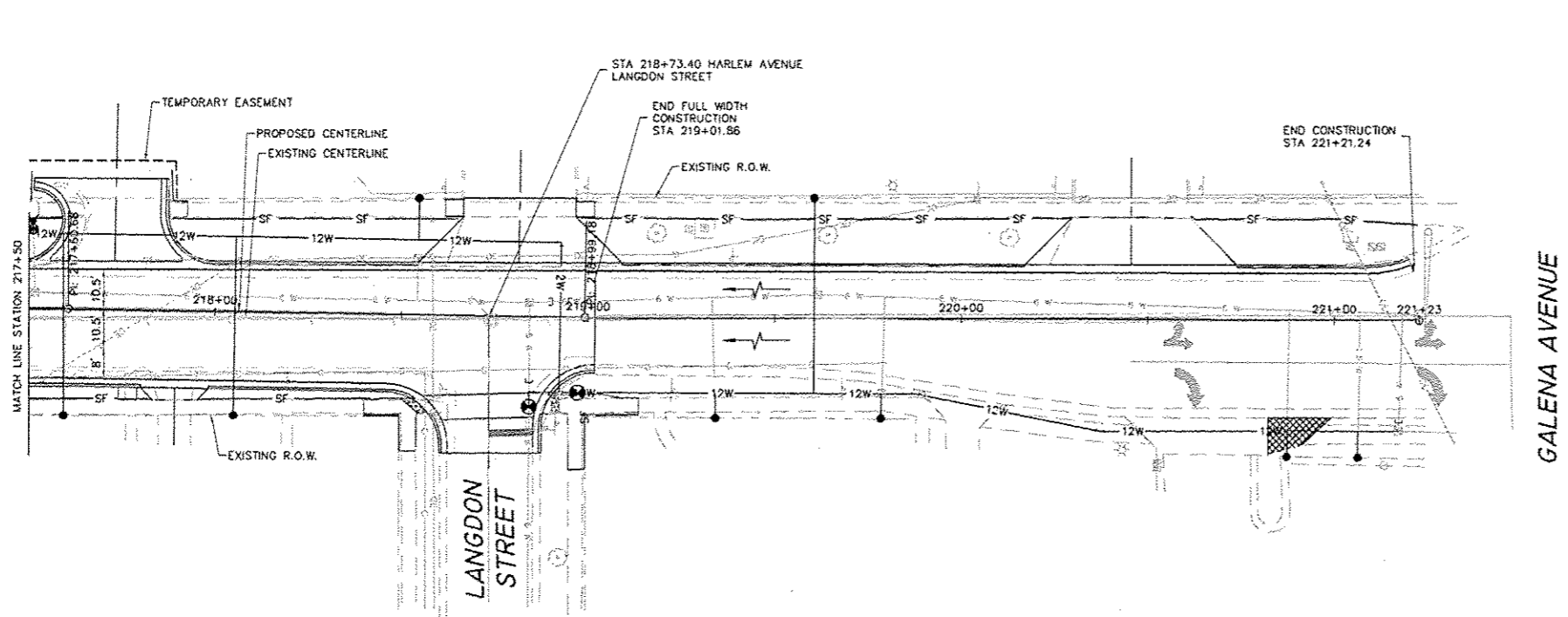
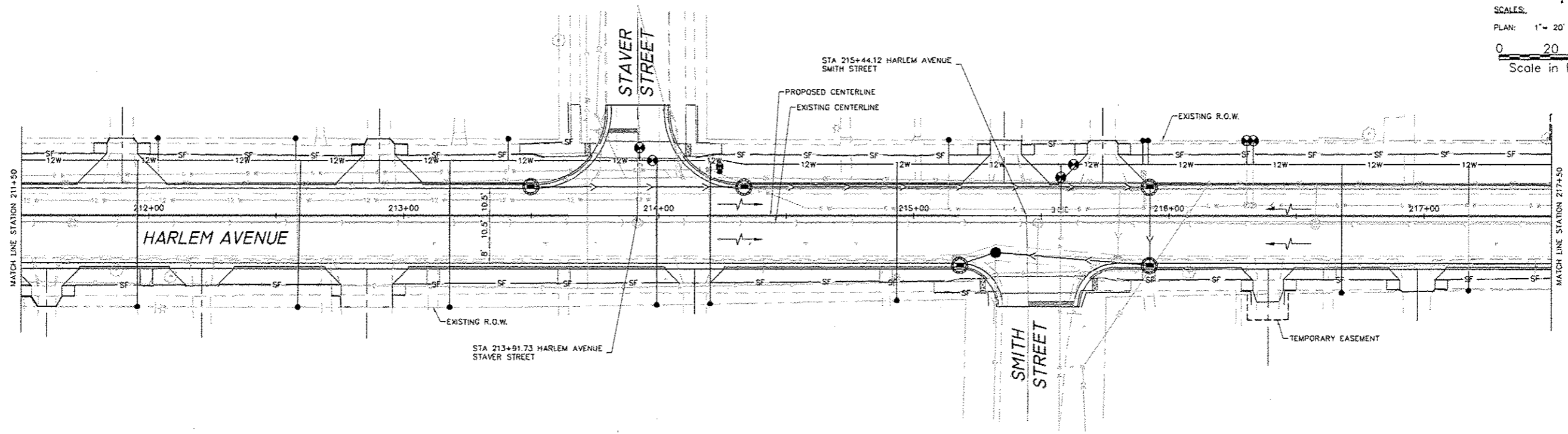
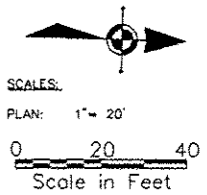
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EROSION CONTROL PLAN - HARLEM AVENUE
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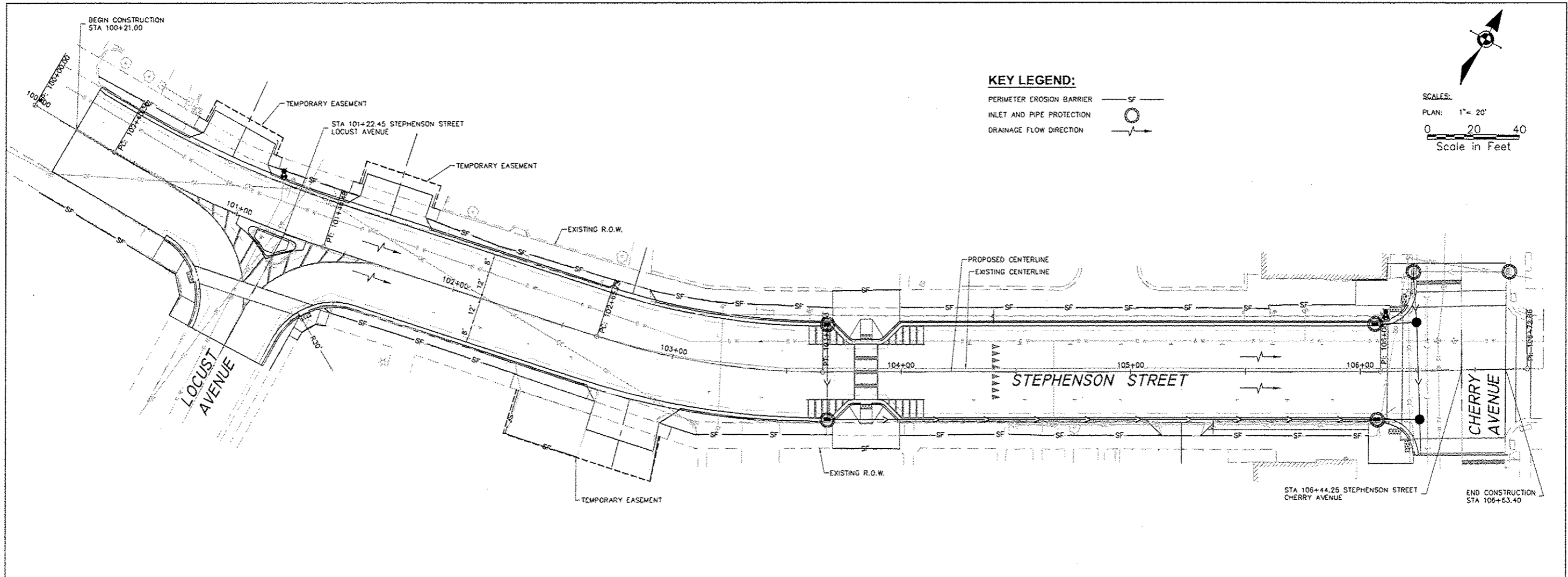
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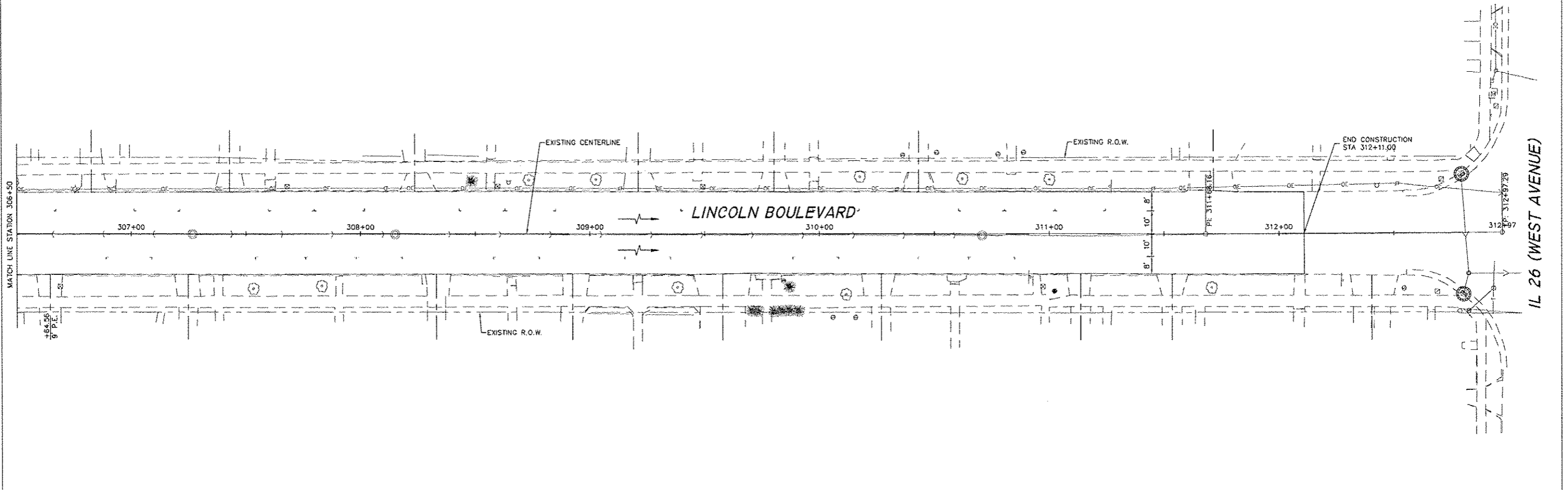
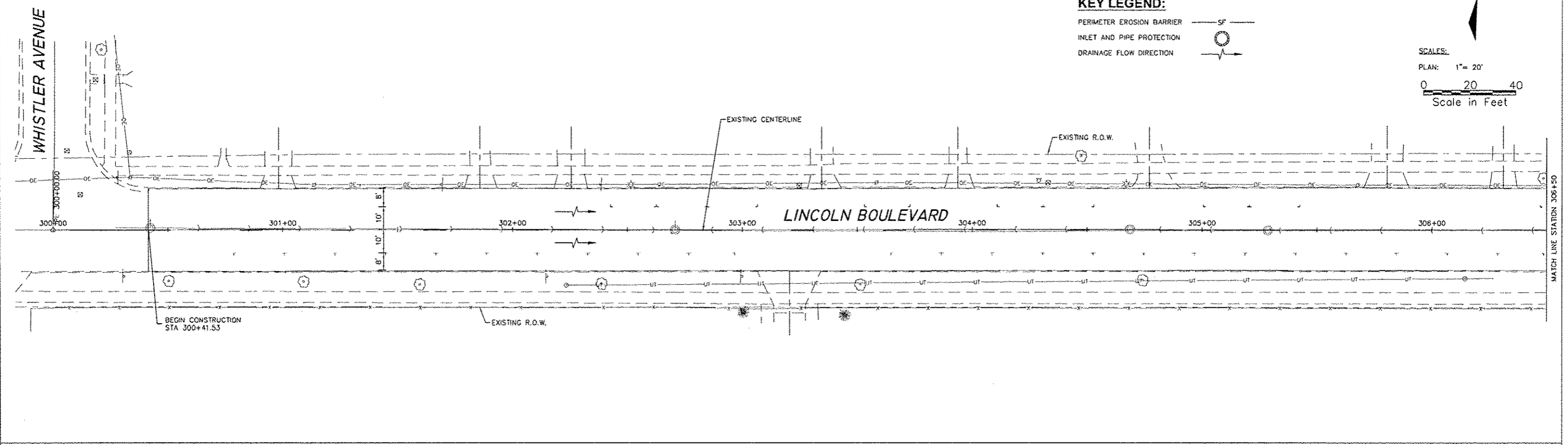
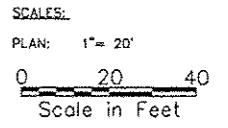
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EROSION CONTROL PLAN - STEPHENSON STREET
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KEY LEGEND:


- PERIMETER EROSION BARRIER — SF —
- INLET AND PIPE PROTECTION 
- DRAINAGE FLOW DIRECTION 



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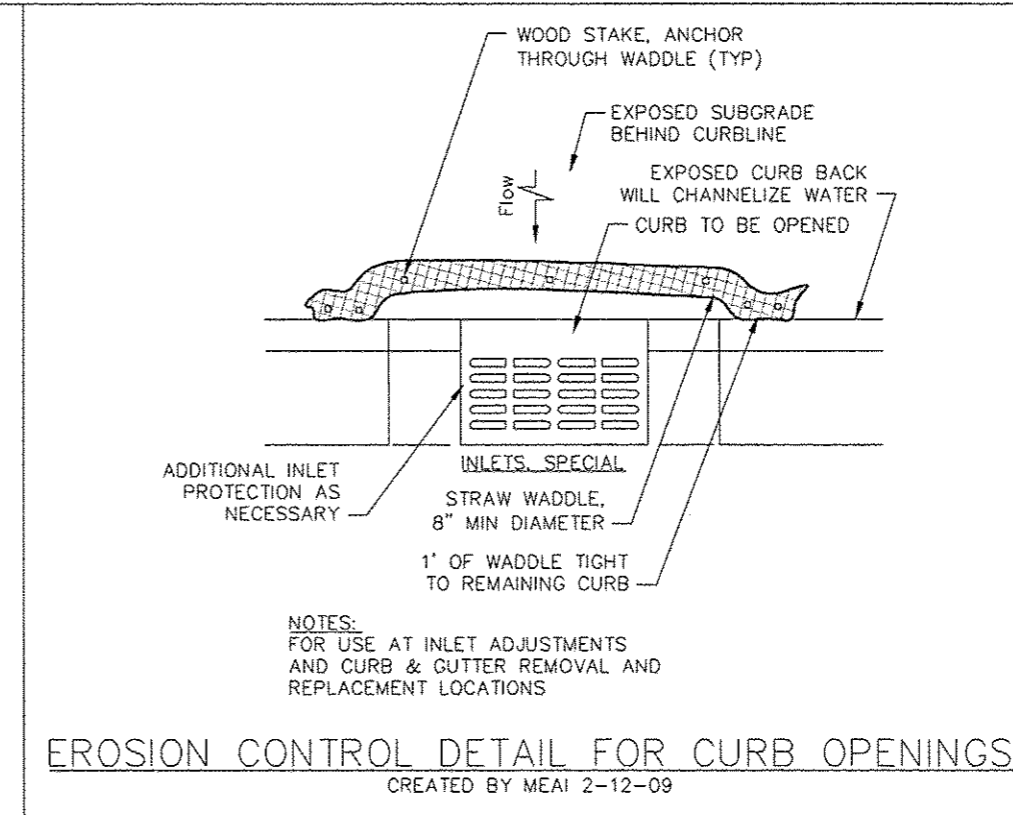
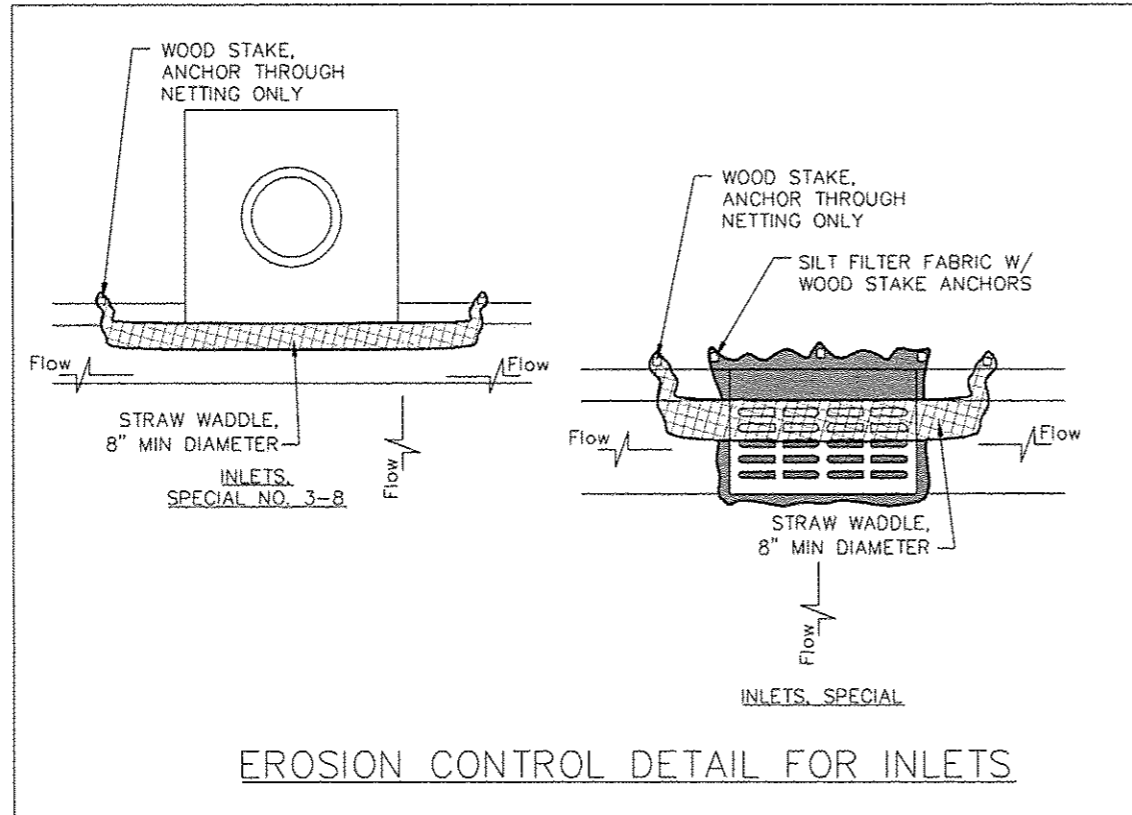
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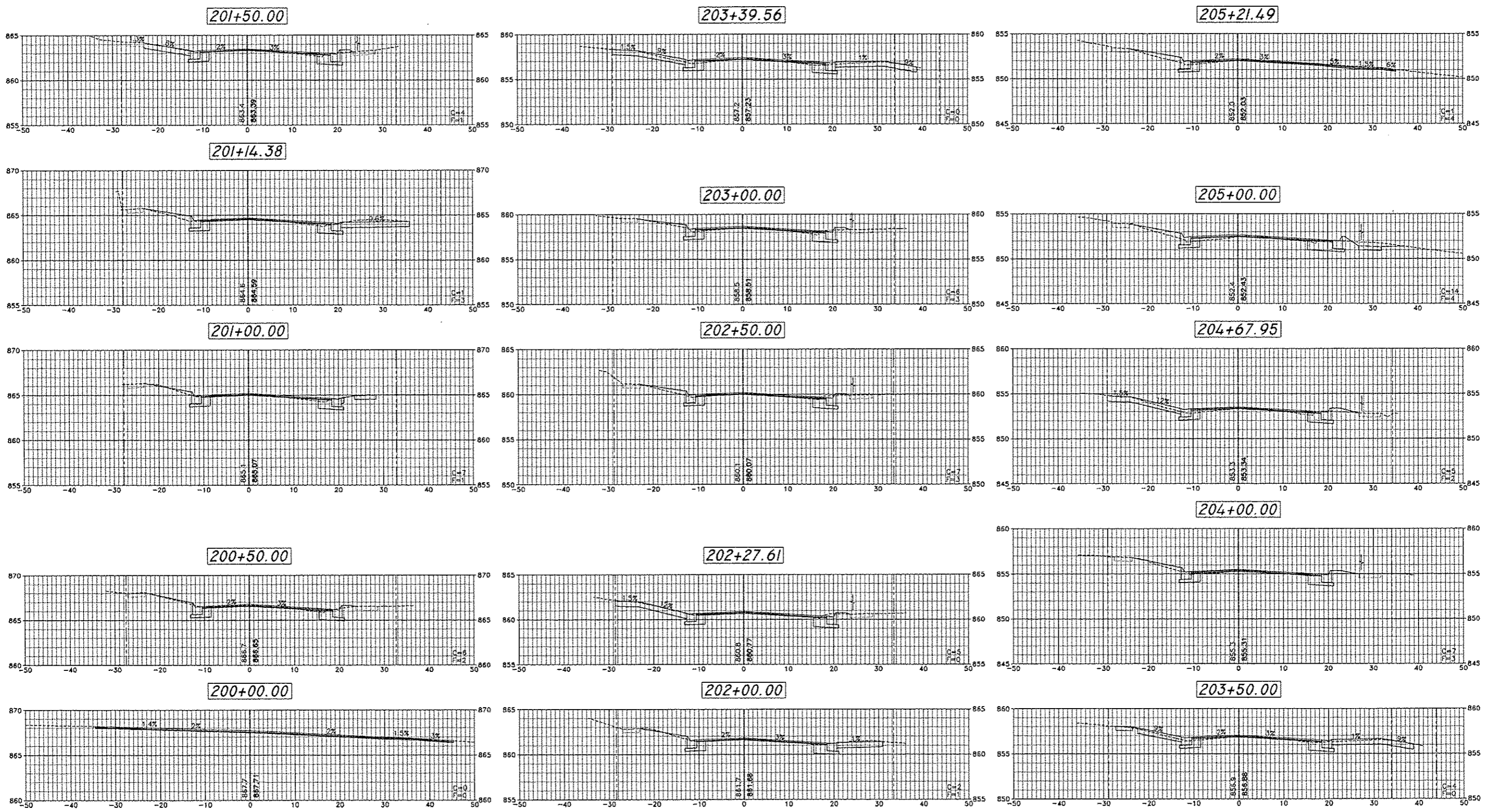
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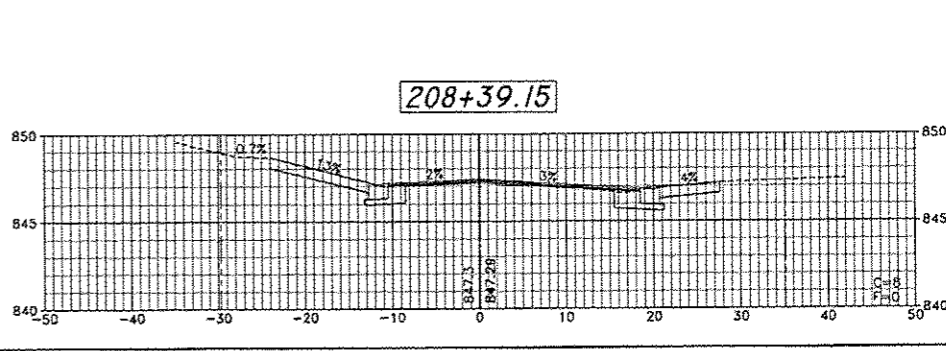
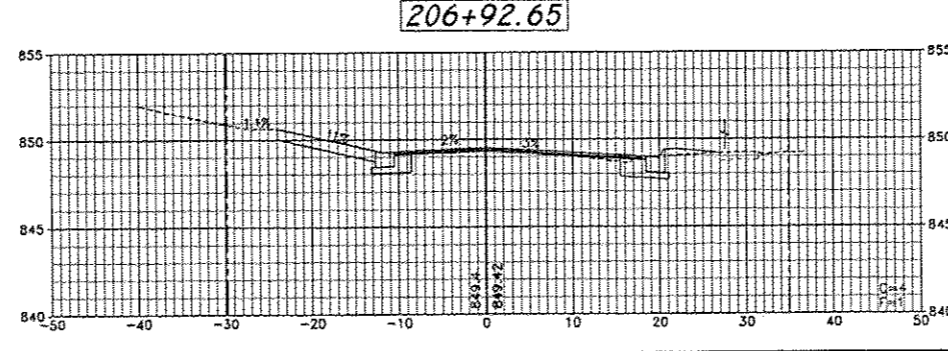
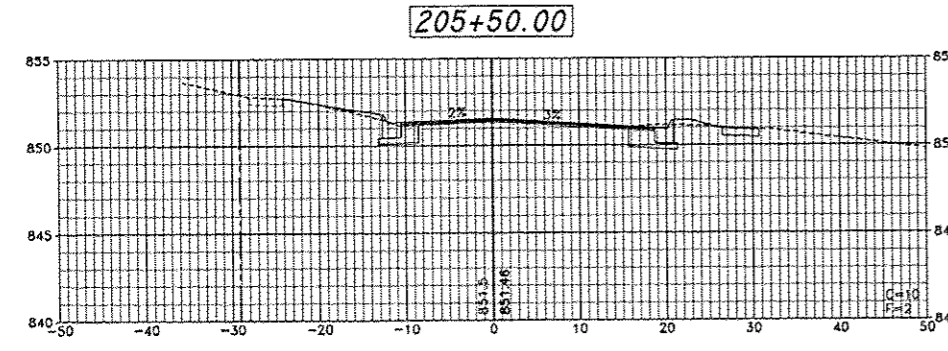
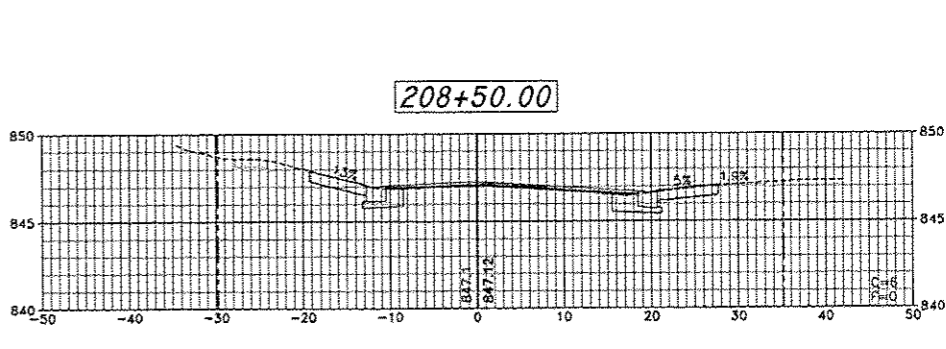
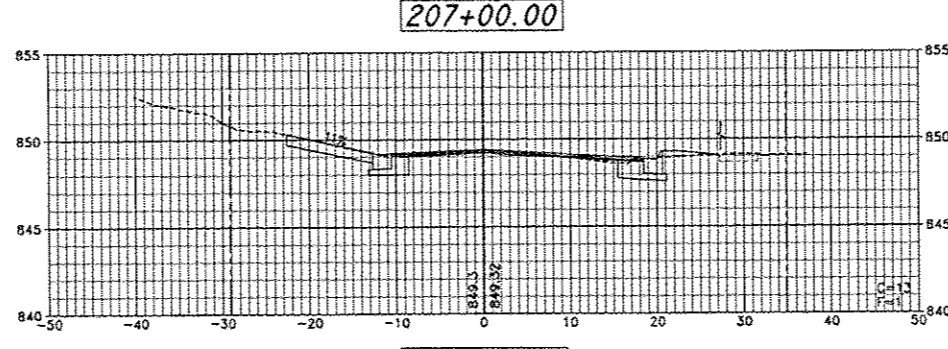
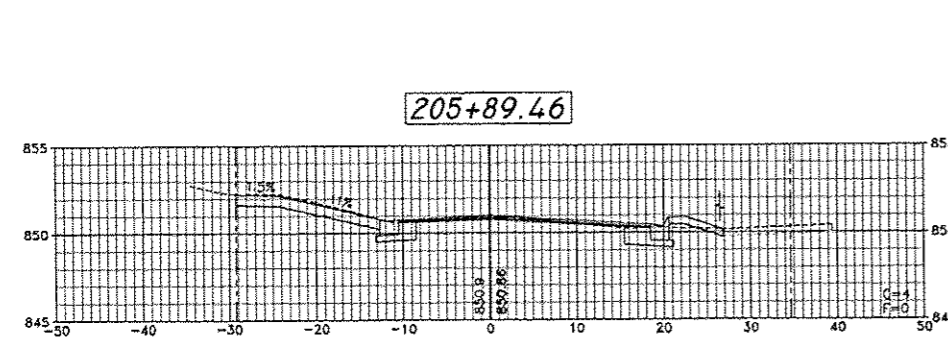
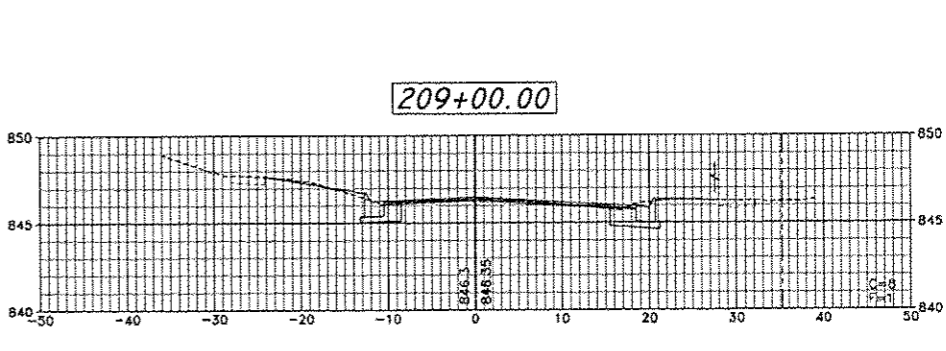
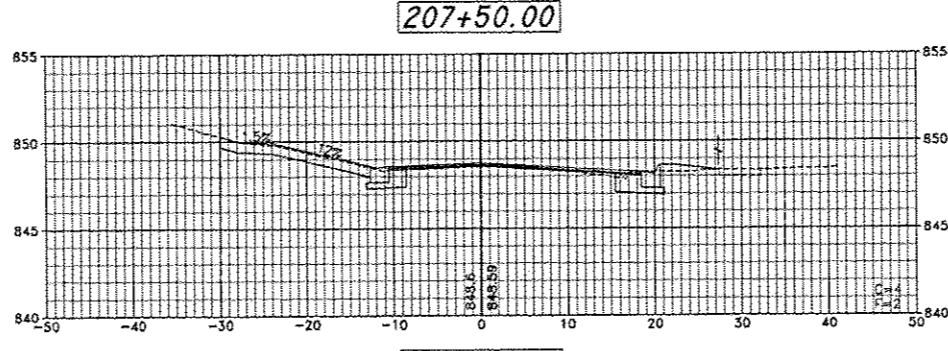
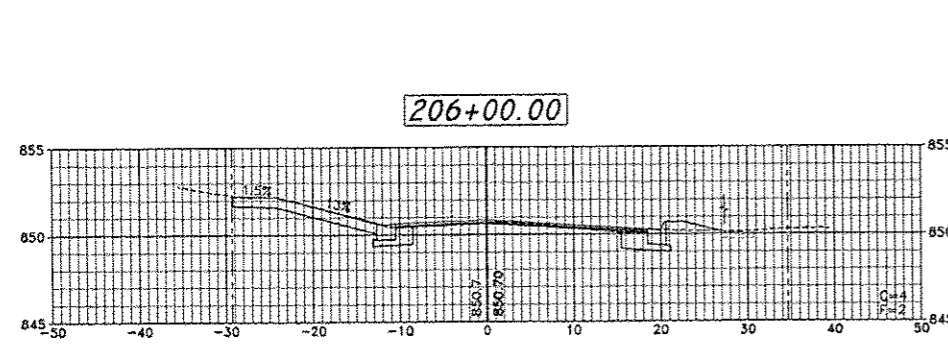
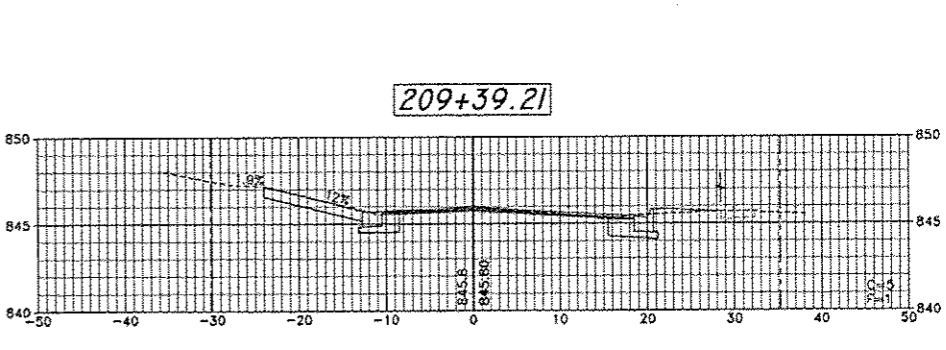
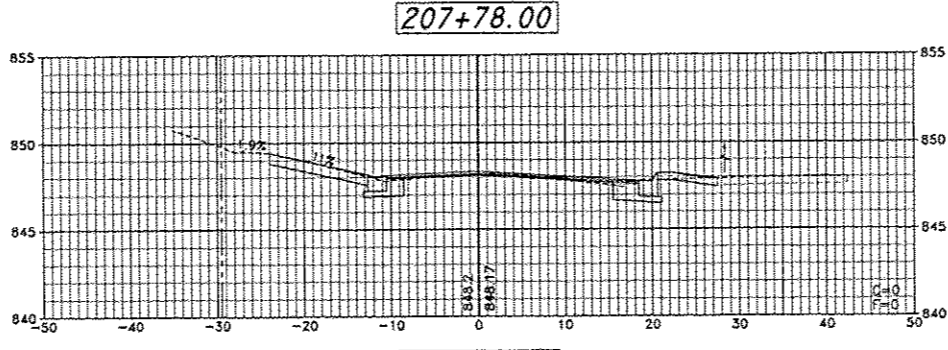
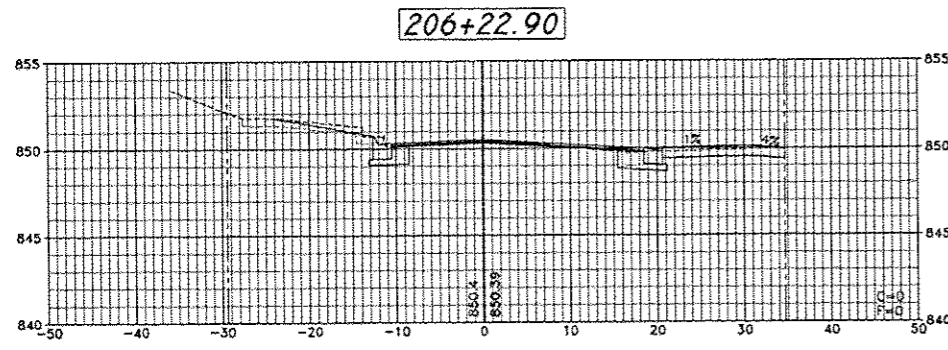
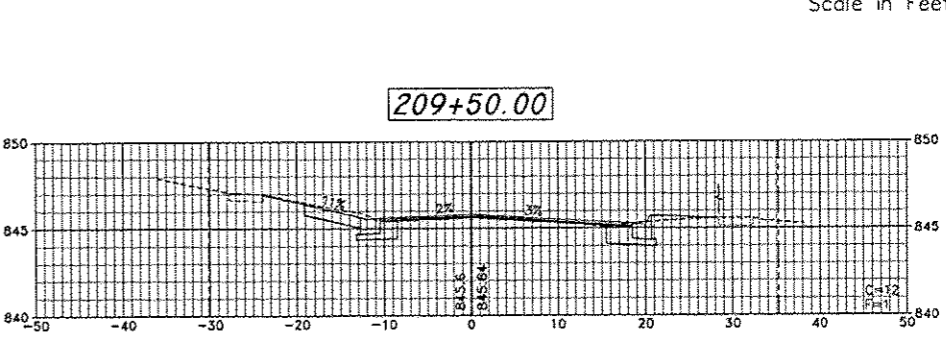
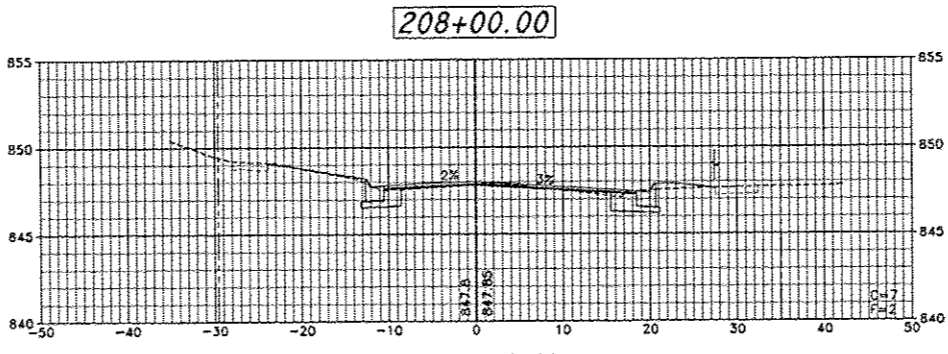
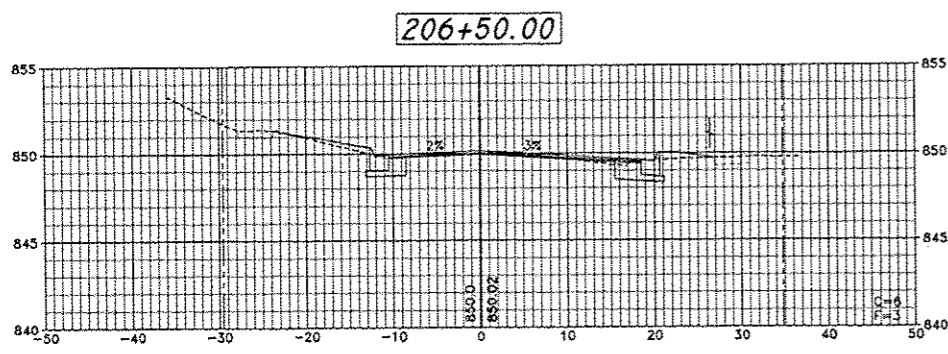
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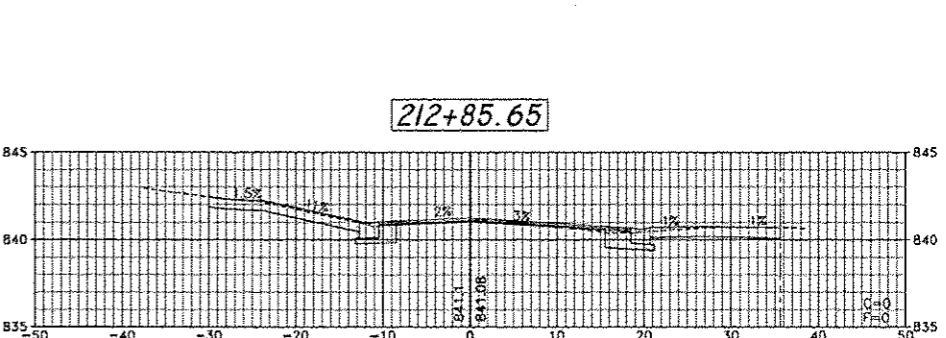
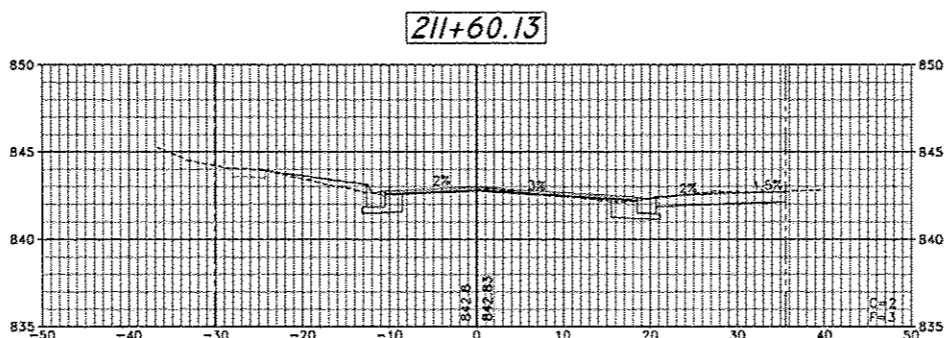
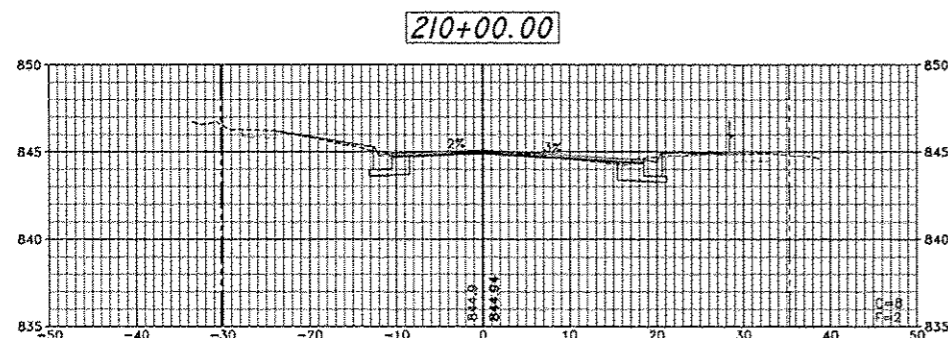
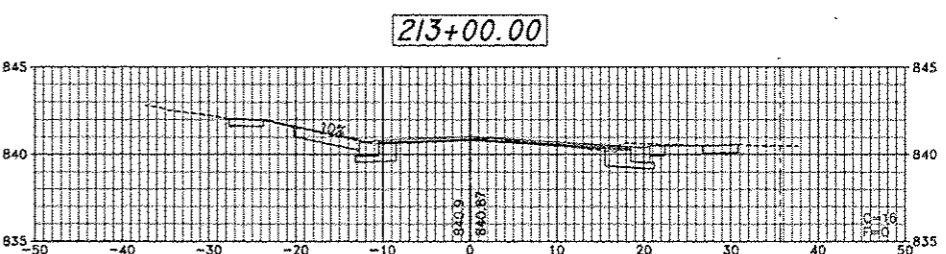
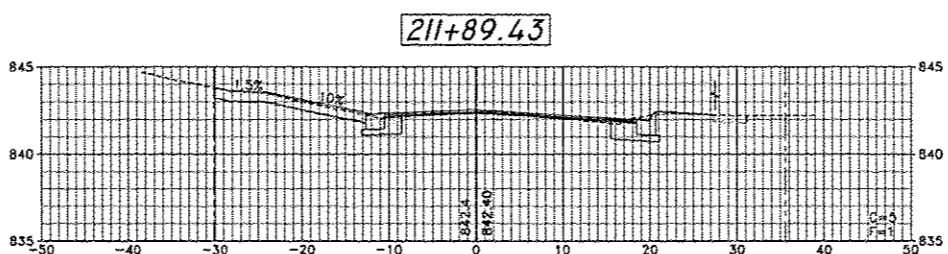
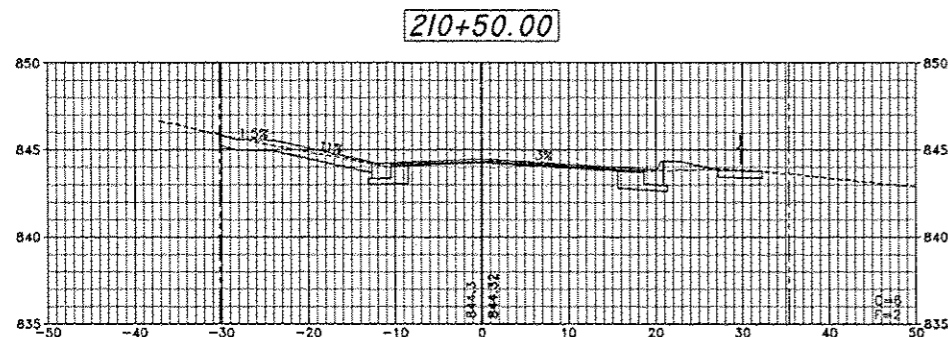
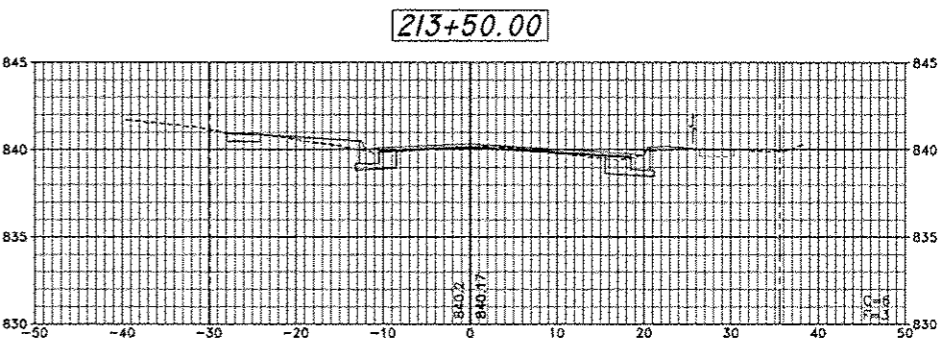
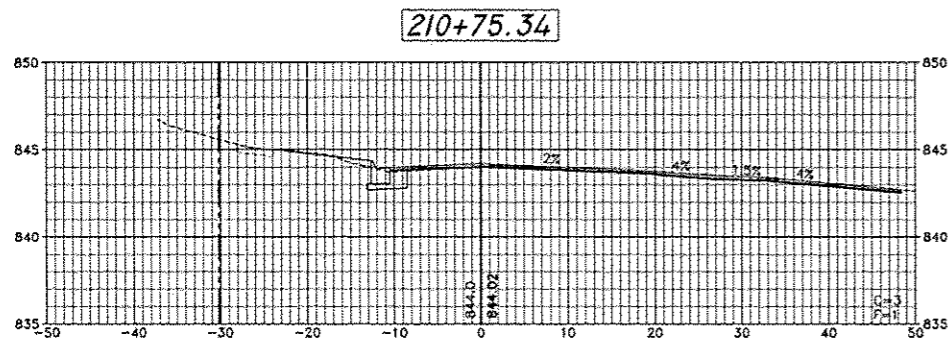
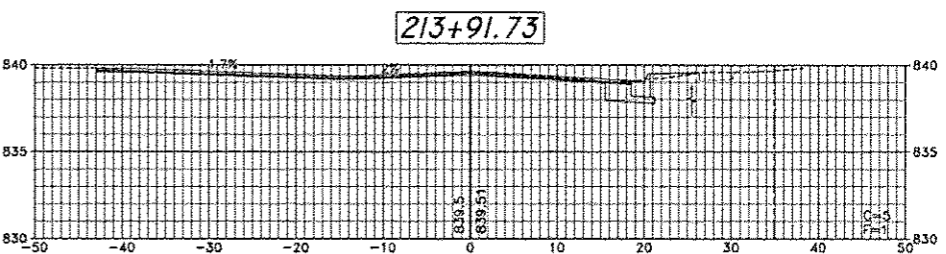
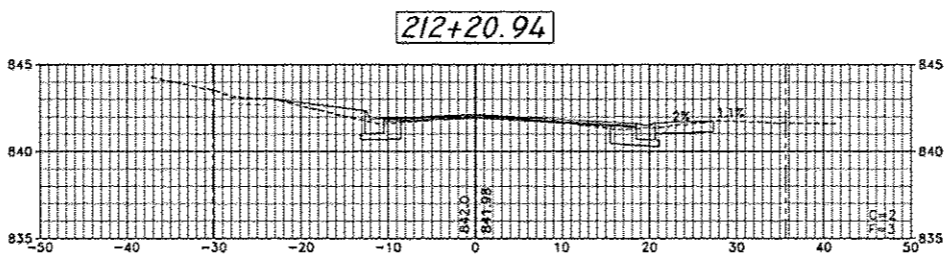
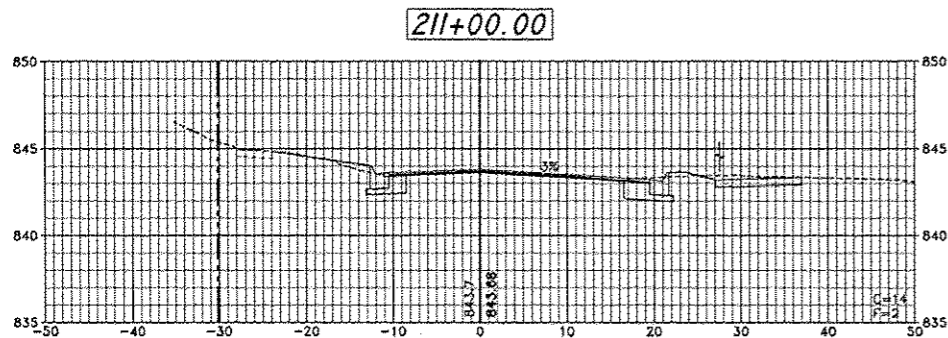
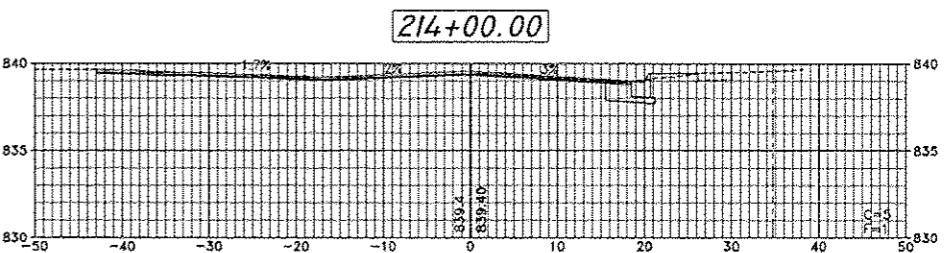
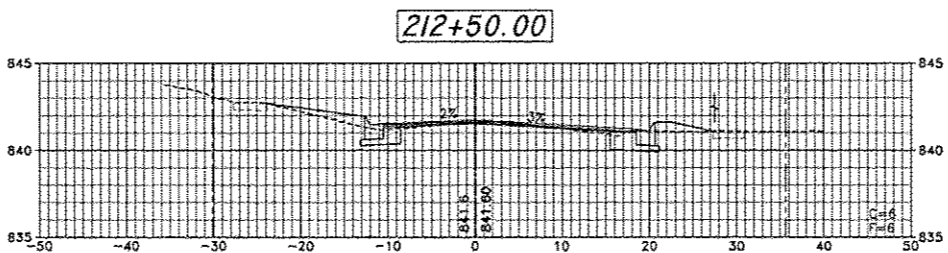
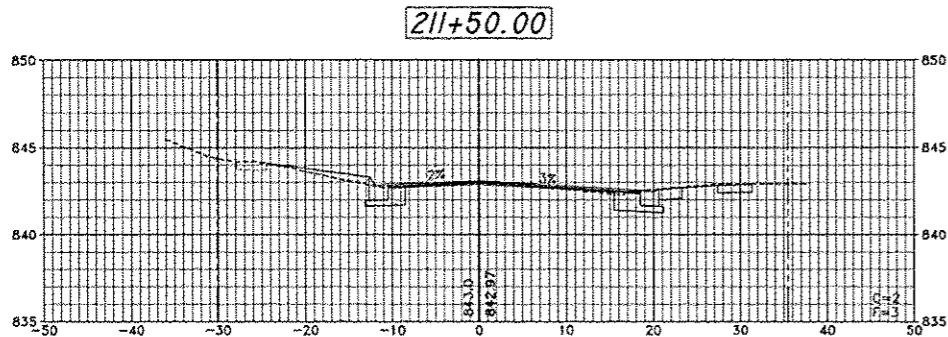
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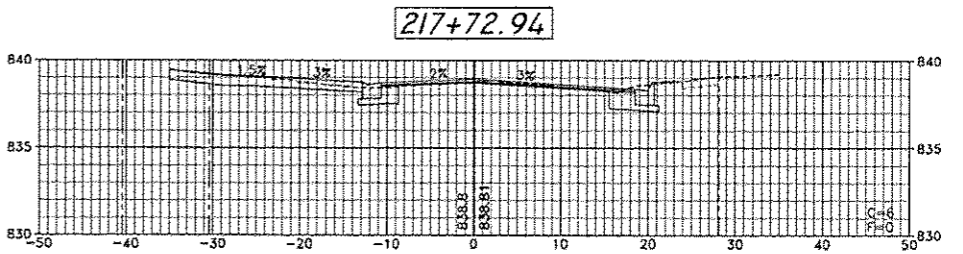
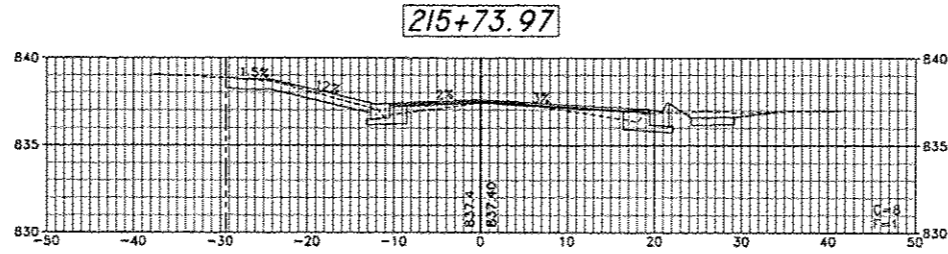
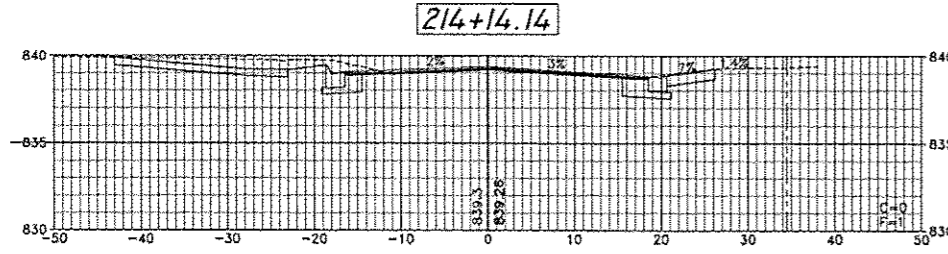
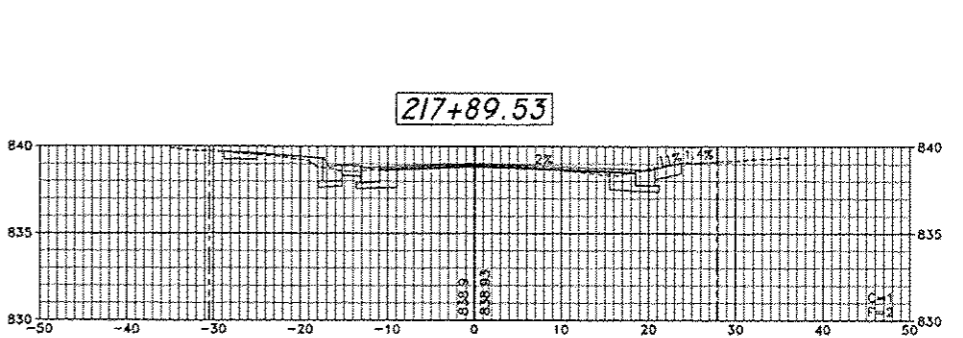
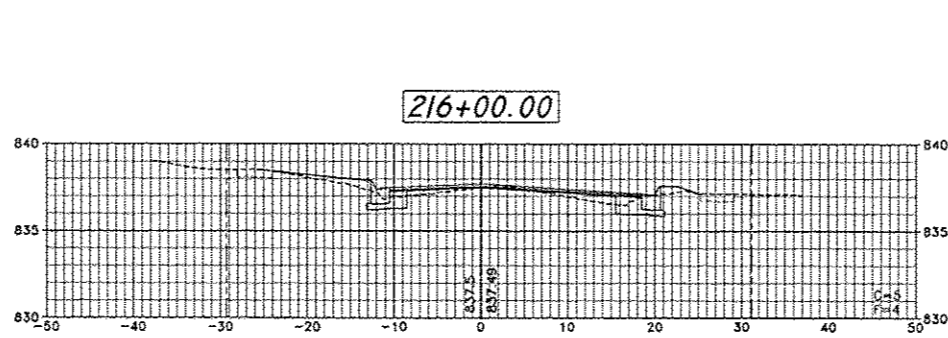
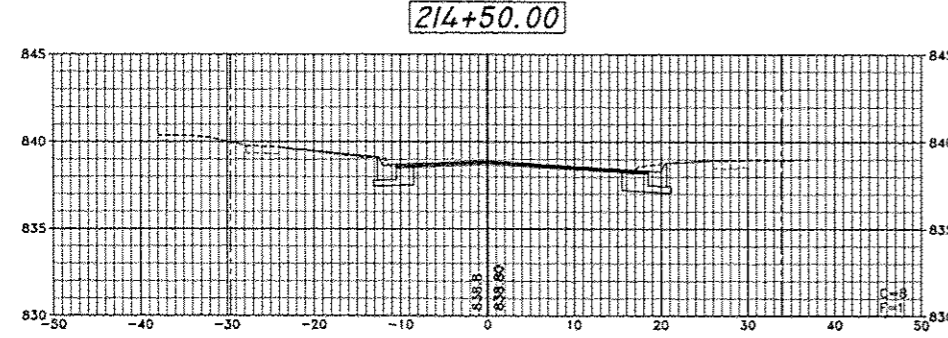
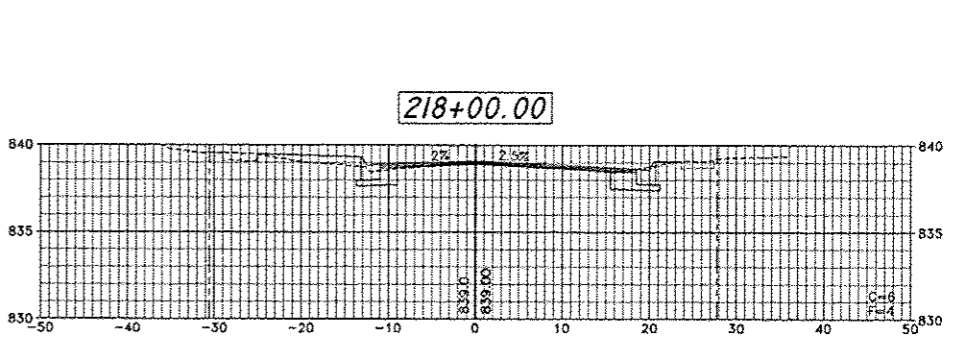
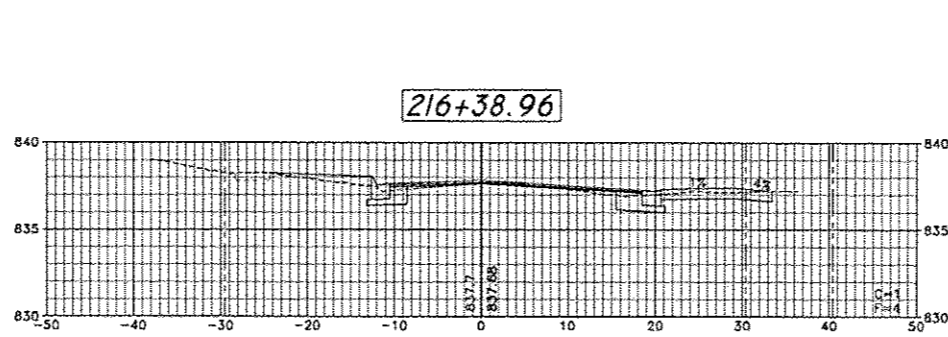
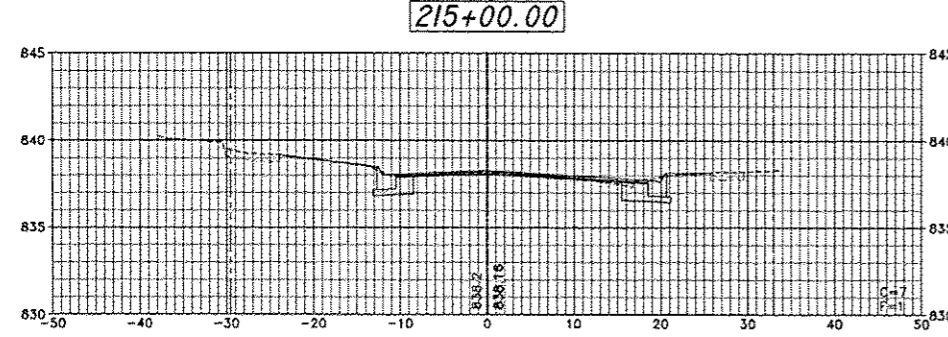
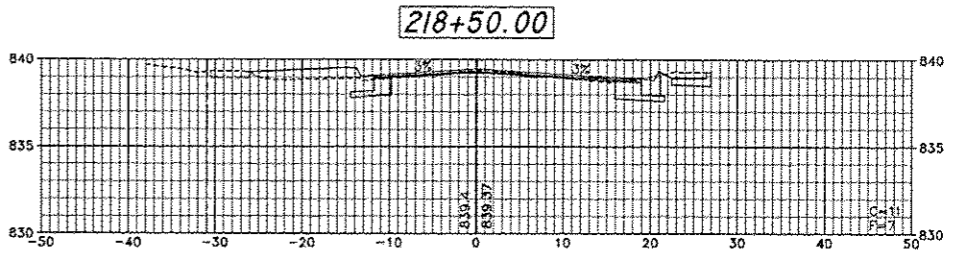
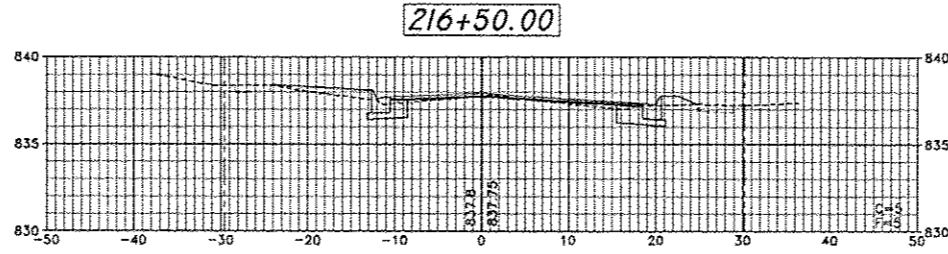
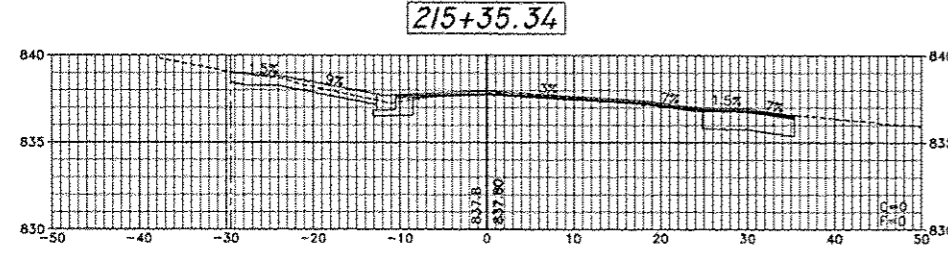
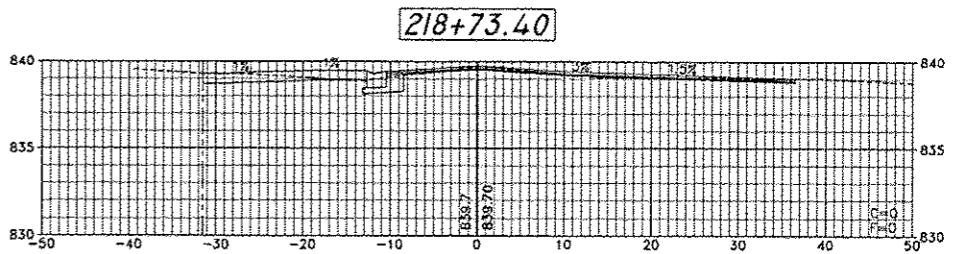
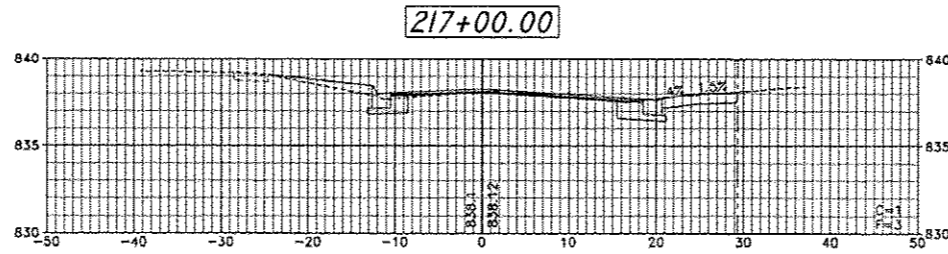
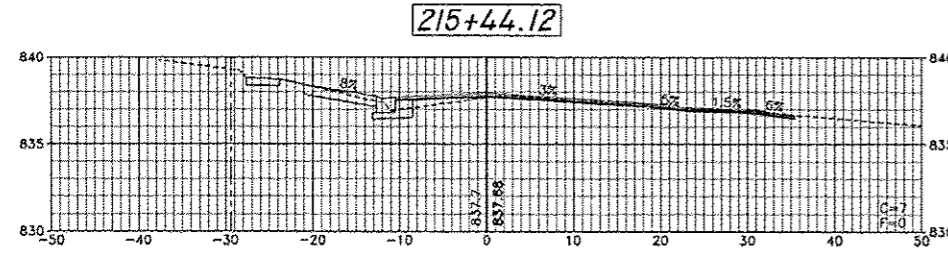
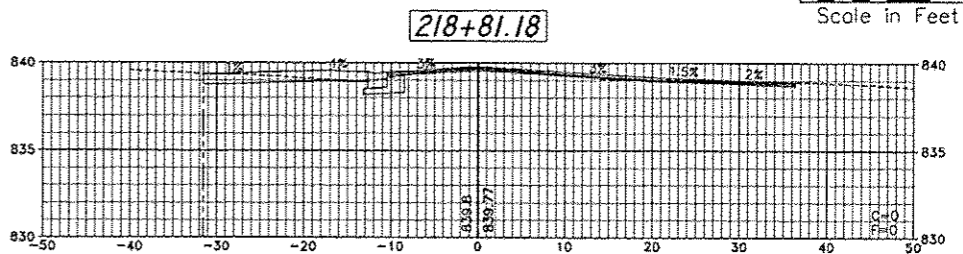
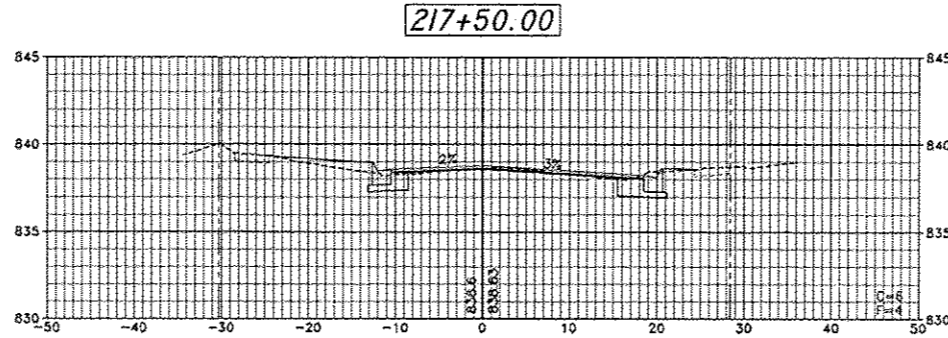
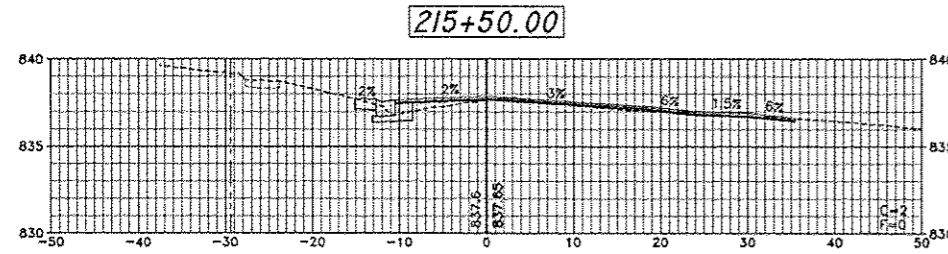
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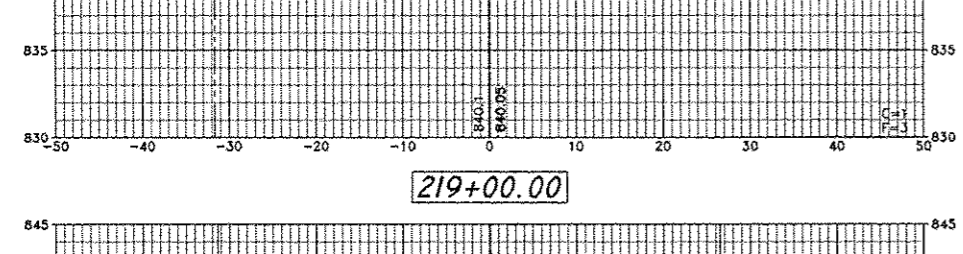
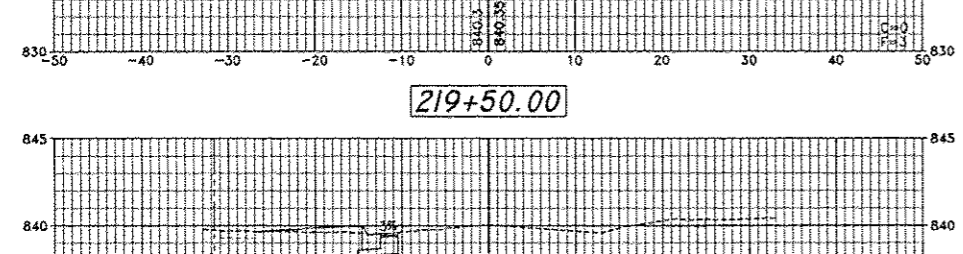
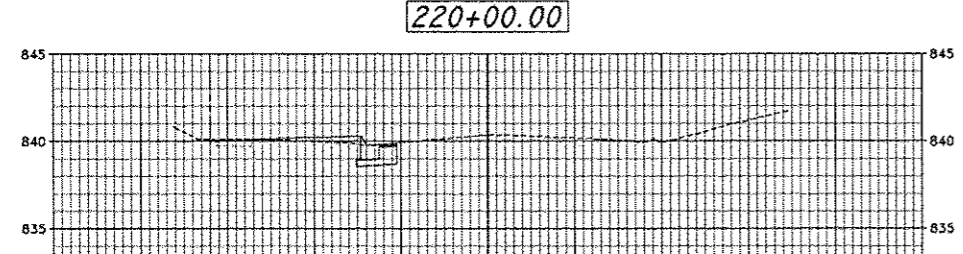
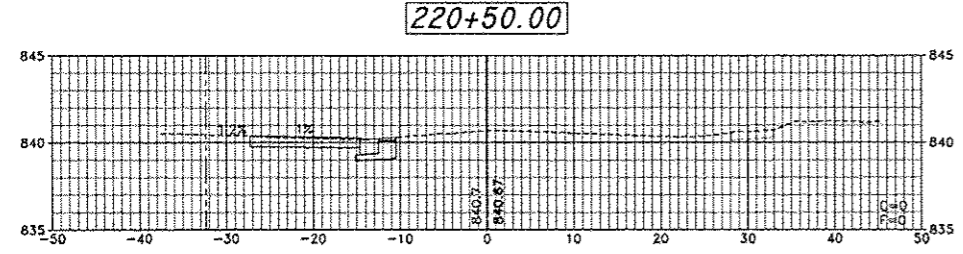
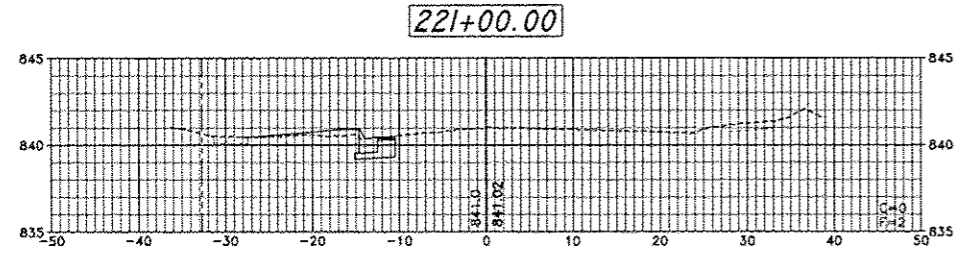
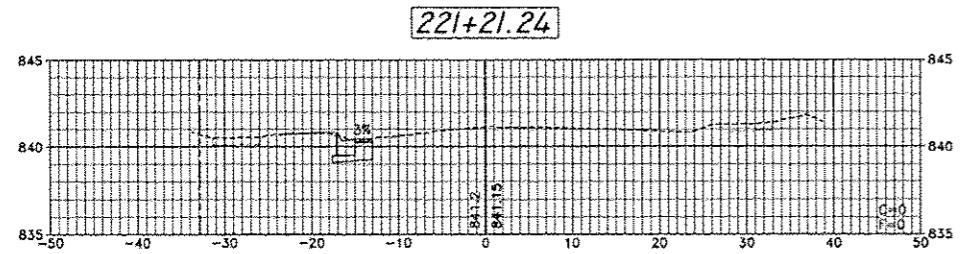
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CROSS SECTIONS - HARLEM AVENUE
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 FILE: H:\14-016-FREEPORTSTREETIMP\DESIGN\DRAWINGS\14-016 HARLEM\SECTS.DWG
 JOB: 04-28-14-016

SHEET NO.
 22
 OF
 32

SCALE:
 1" = 10' HORIZONTAL
 1" = 5' VERTICAL
 0 10 20
 Scale in Feet



SHEET REVIEW	
AGENCY	DATE

REVISIONS	
NO.	DATE

NO.	ITEM	DATE

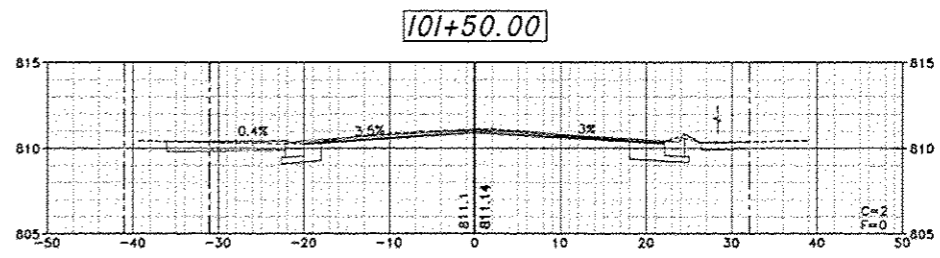
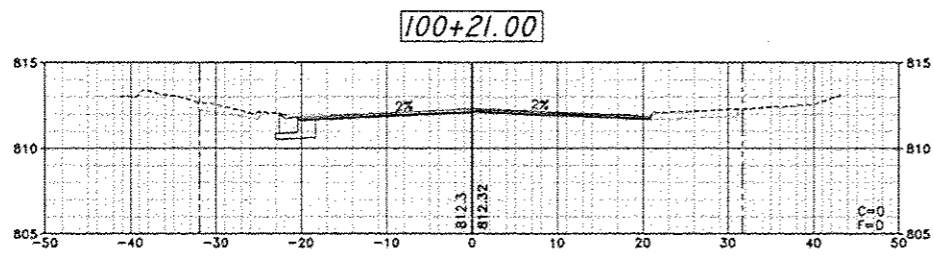
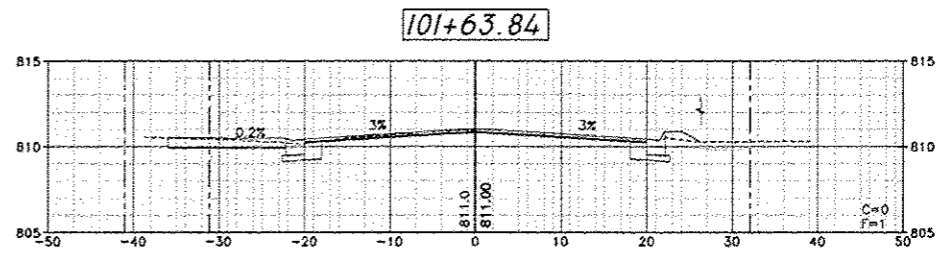
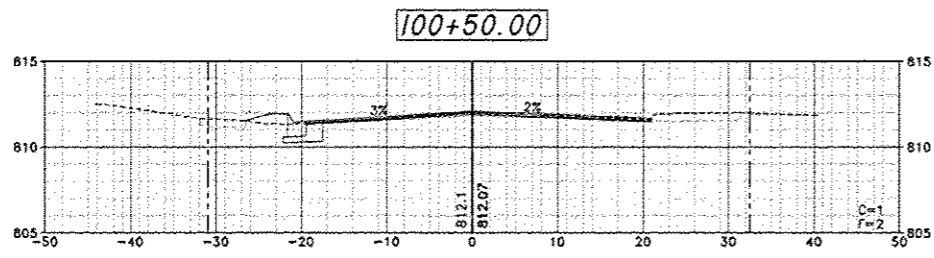
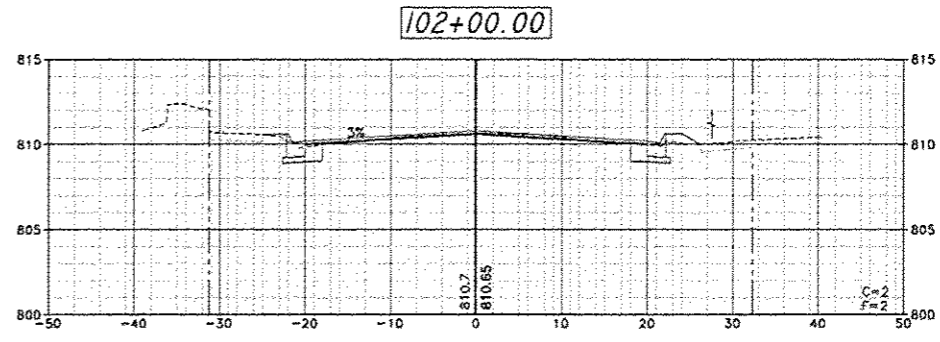
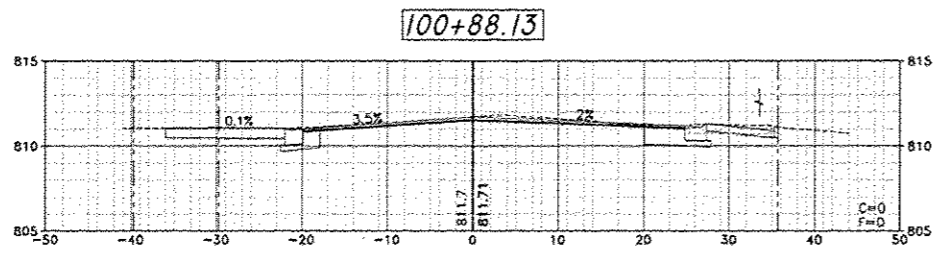
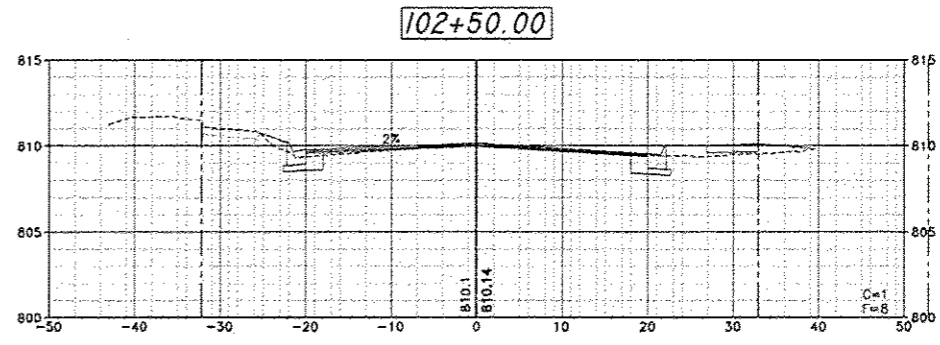
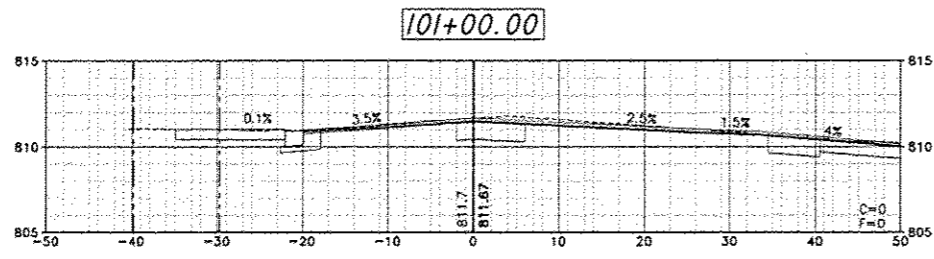
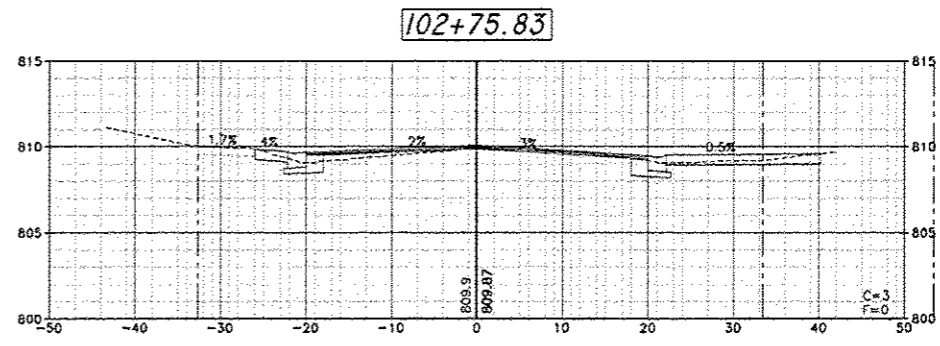
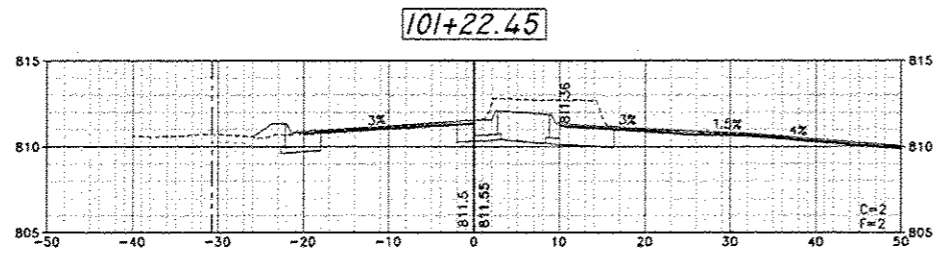
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 FILE: H116-0-6-FREEPORTSTREETIMPDESIGNDRAWINGS114-016 HARLEMSECTS.DWG
 JOB: 04-28-14-016

SHEET NO.
 23
 OF
 32

SCALE:
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 1" = 5' VERTICAL
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 Scale in Feet



SHEET REVIEW	
AGENCY	DATE

REVISIONS		
NO.	ITEM	DATE

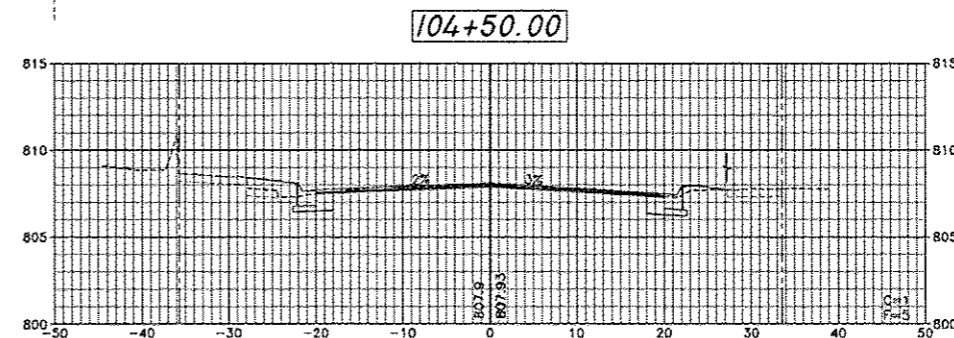
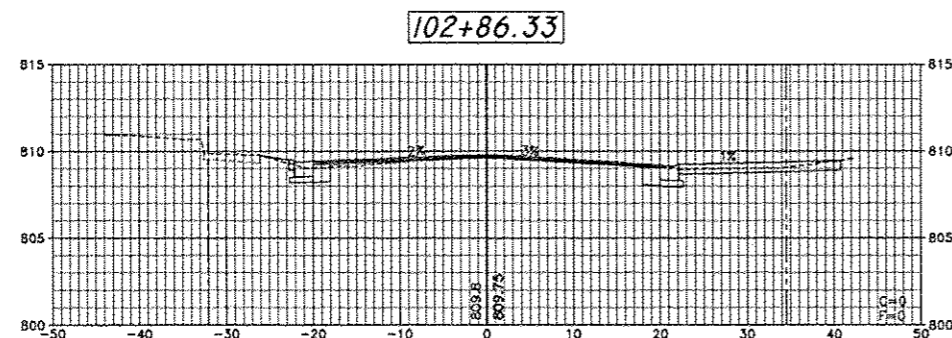
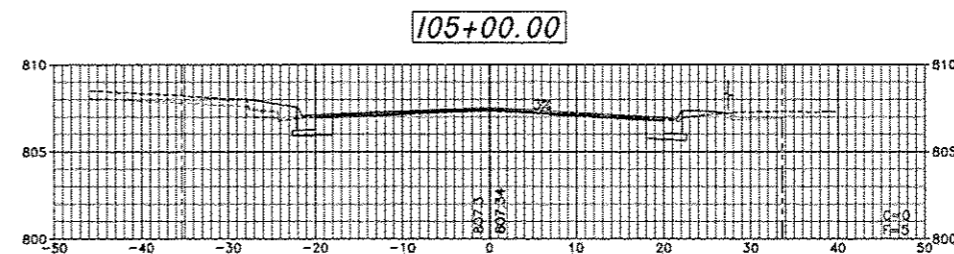
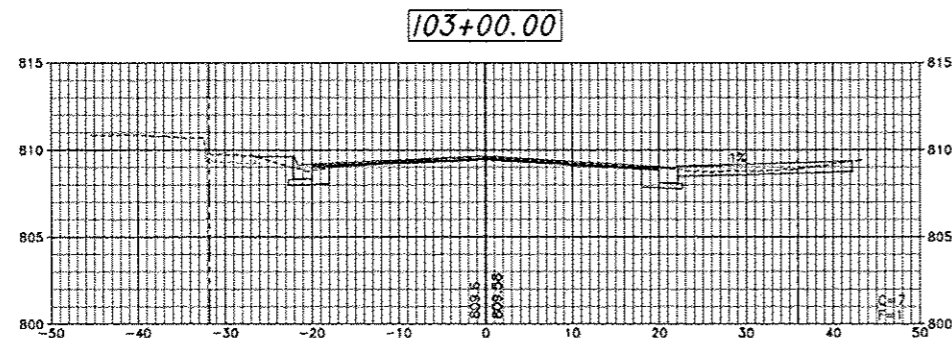
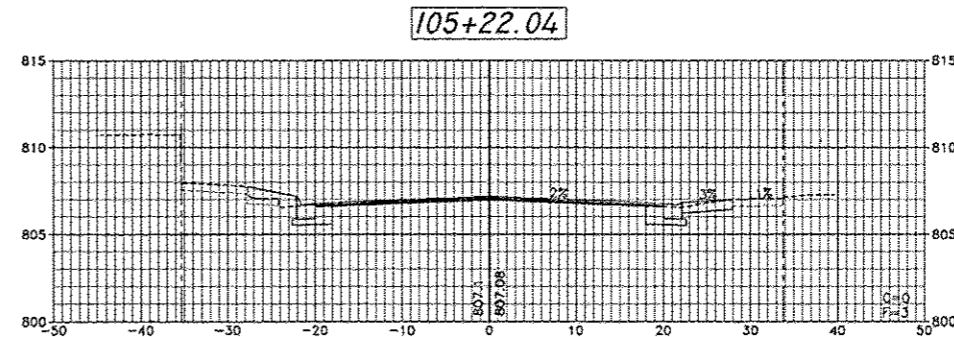
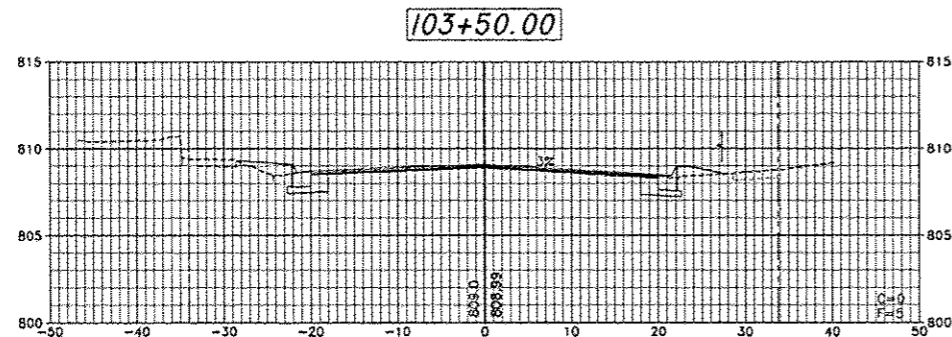
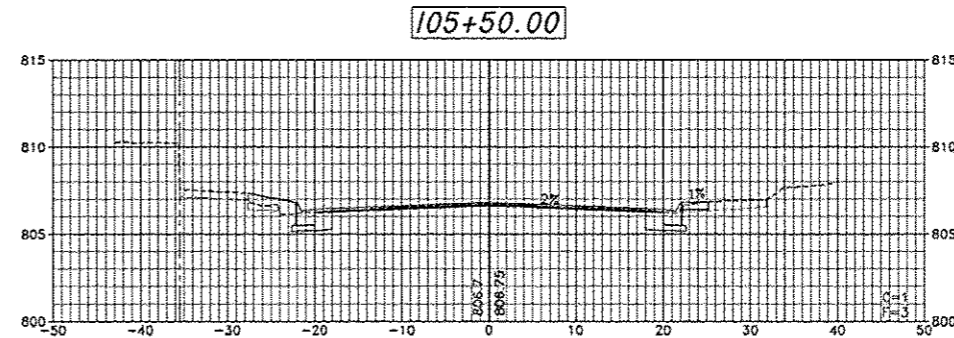
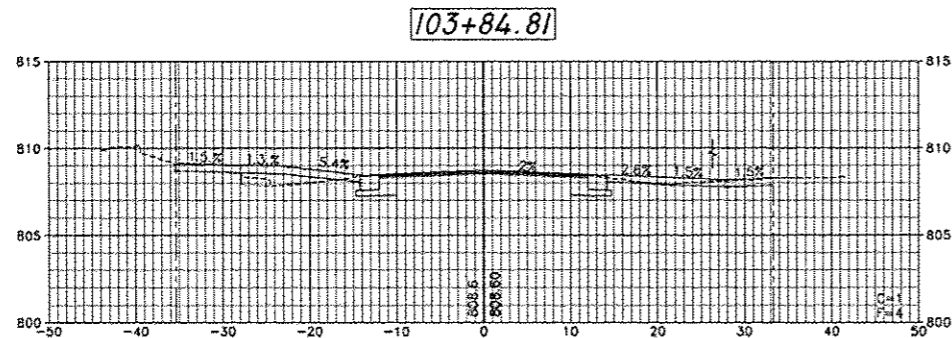
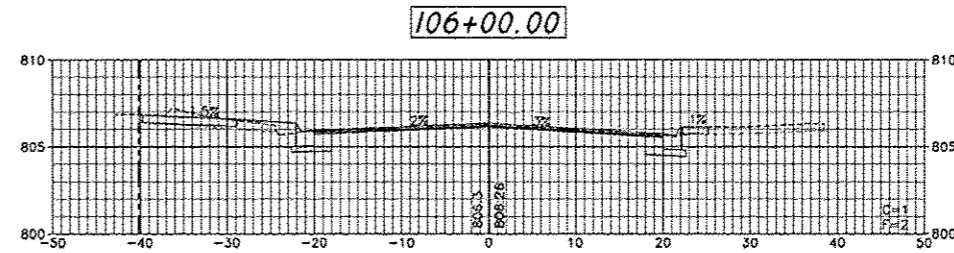
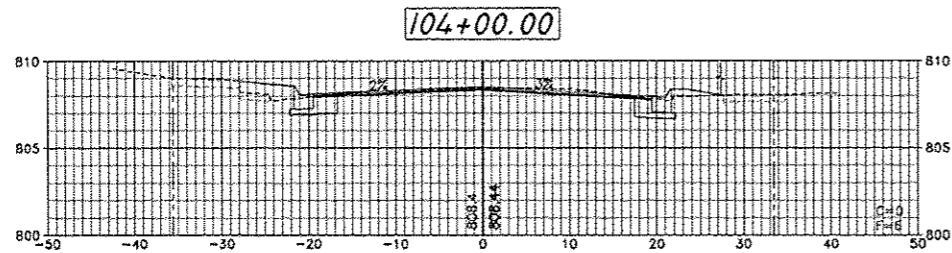
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 JOB: 04-28-14-016

SHEET NO.
 24
 OF
 32

SCALE:
 1" = 10' HORIZONTAL
 1" = 5' VERTICAL
 0 10 20
 Scale in Feet



SHEET REVIEW	
AGENCY	DATE

REVISIONS		
NO.	ITEM	DATE

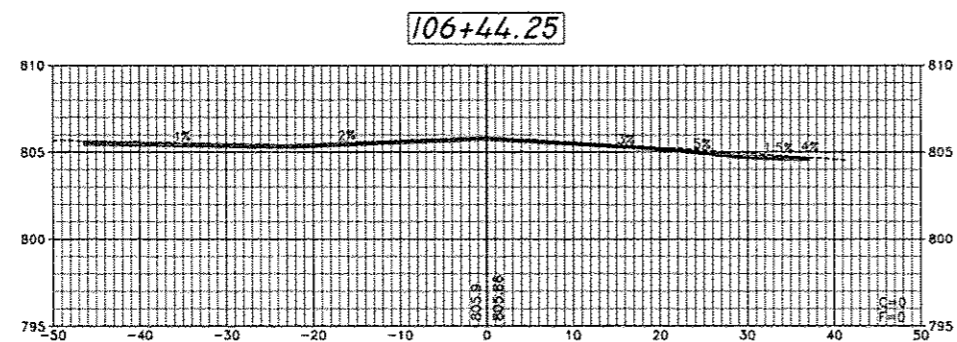
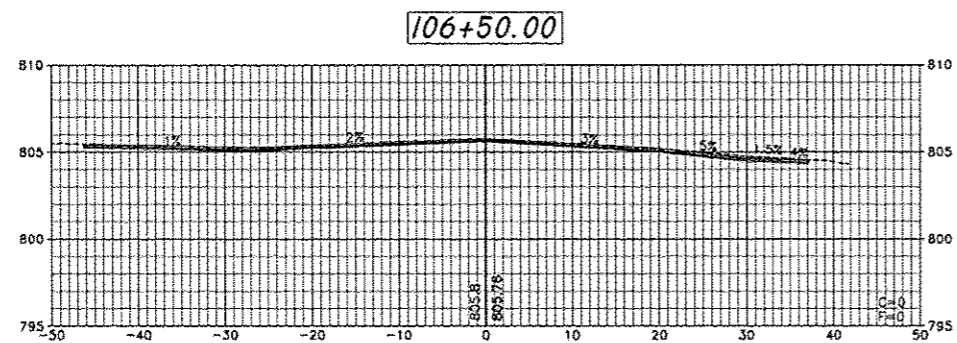
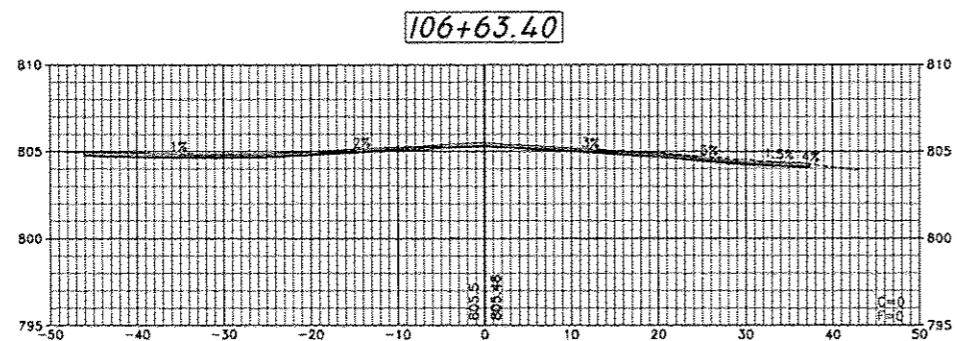
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SHEET NO.
 25
 OF
 32



SHEET REVIEW	
AGENCY	DATE

REVISIONS		
NO.	ITEM	DATE

SCALE:	Hor. 1"=10' Ver. 1"=5'
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DATE:	6/15/2015

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CROSS SECTIONS - STEPHENSON STREET

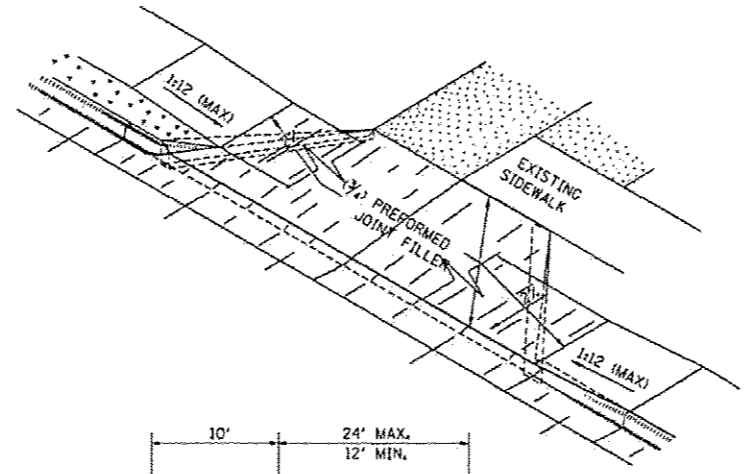
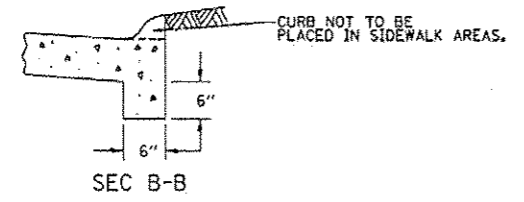
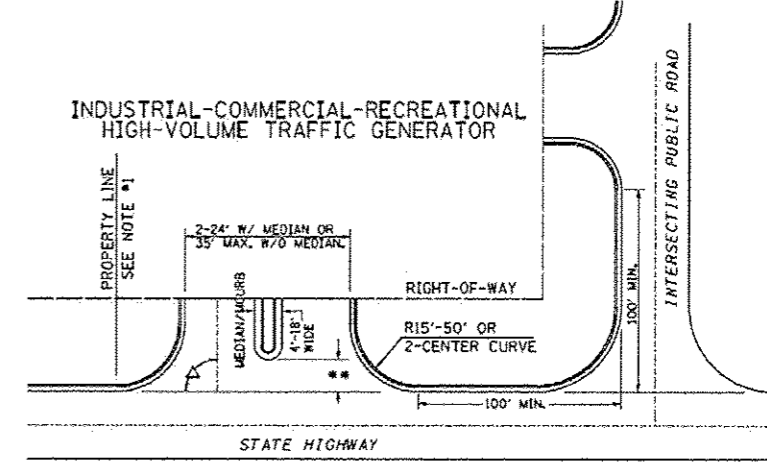
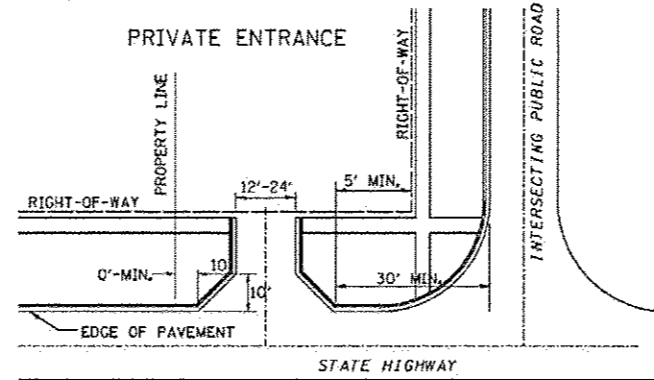
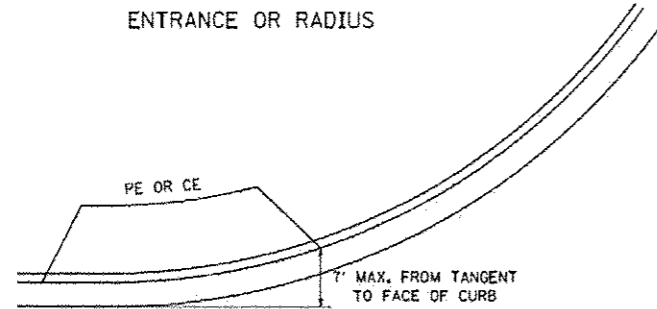
FREEPORT STREET IMPROVEMENTS

CITY OF FREEPORT FREEPORT, ILLINOIS

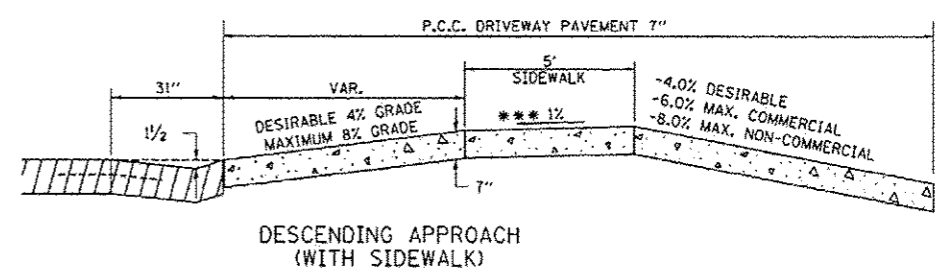
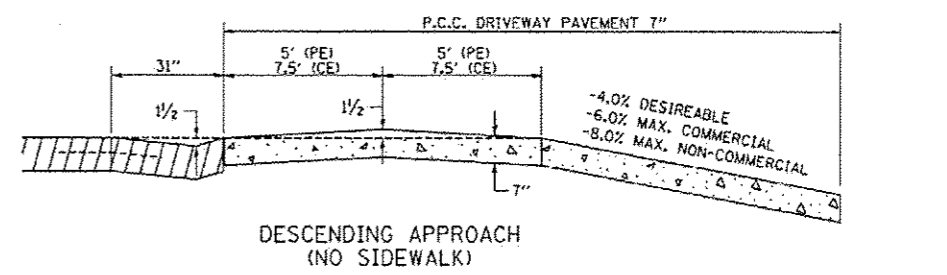
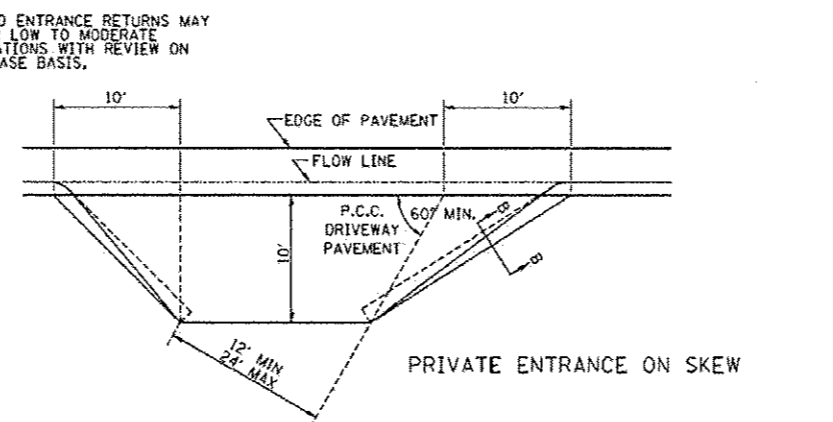
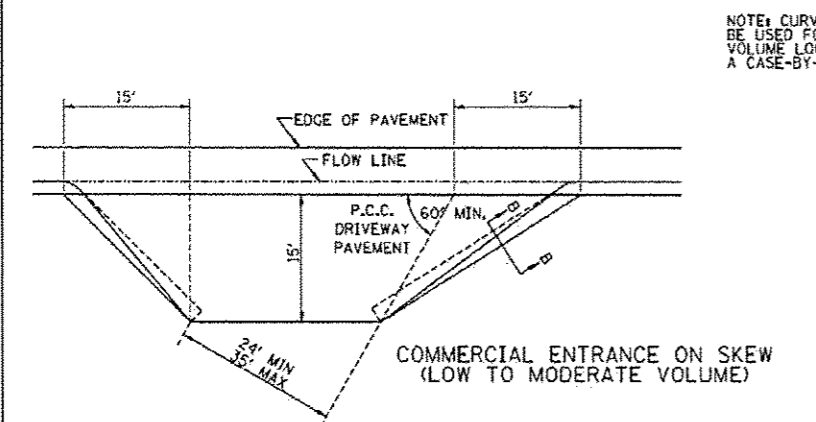
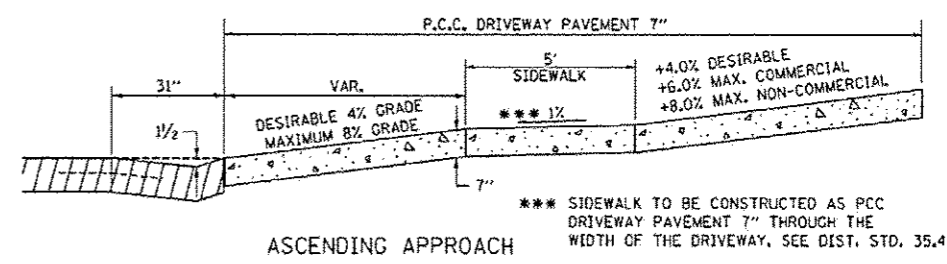
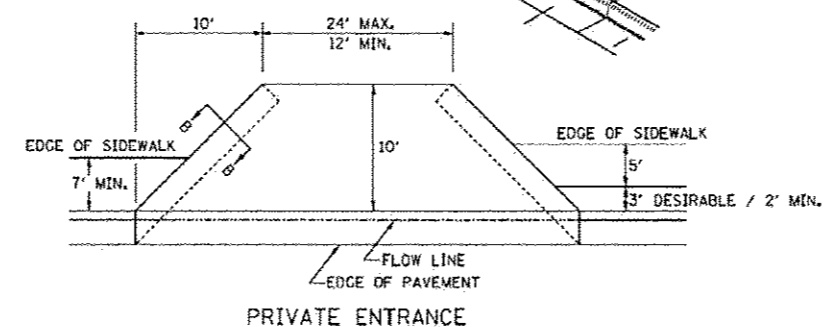
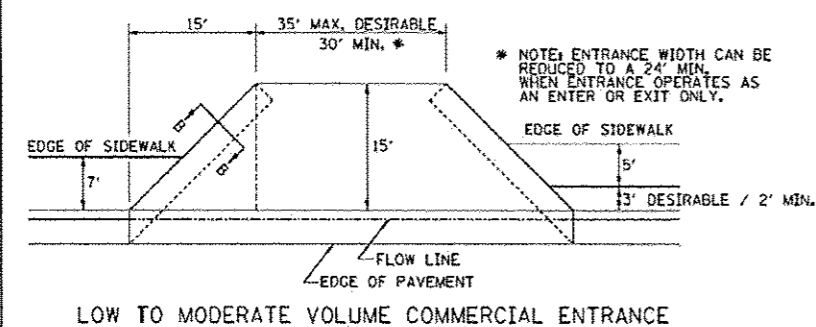
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ENTRANCE APPROACHES – URBAN AREA

ENTRANCE OR RADIUS



A MINIMUM OF 440 FEET SHALL BE MAINTAINED BETWEEN CENTER LINES OF ADJACENT DRIVEWAYS.
 Δ 90° DESIRABLE, 45° MIN. ANGLE PERMITTED ONLY FOR ONE-WAY DRIVEWAYS.
 60° MIN. ANGLE FOR TWO-WAY DRIVEWAYS.
 NOTE: *1 ENCROACHMENT ON THE ADJACENT PROPERTY OWNER LAND REQUIRES HIS OR HER WRITTEN APPROVAL.
 ** 4'-10" IF HIGHWAY CURBED.
 AT EDGE OF SHOULDER IF HIGHWAY UNCURBED.

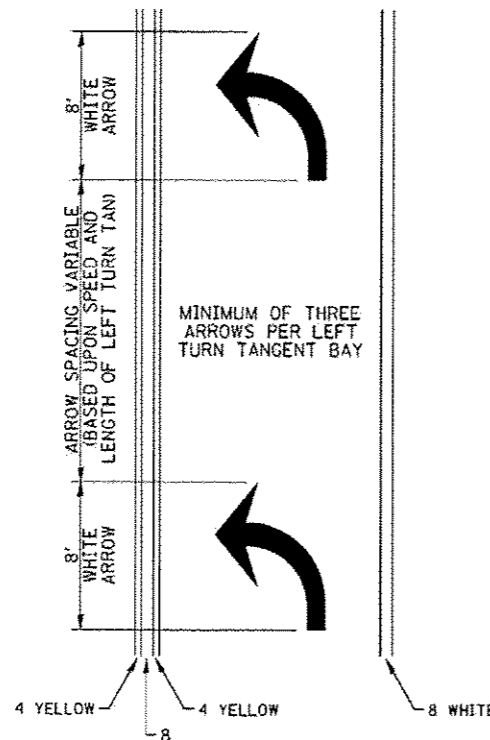


FILE NAME: District 2 Standard	USER NAME: jdot/District 2	DESIGNED: -	REVISED: 6-27-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REGION 2 / DISTRICT 2 STANDARD	P.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN: -	REVISED: 12-07-10			SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	CONTRACT NO.		
		CHECKED: -	REVISED: -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					
		DATE: 6/15/2015	REVISED: -			ENTRANCE APPROACHES – URBAN AREA 25.1					

SHEET REVIEW		REVISIONS		SCALE: NA		STANDARD DETAILS		SHEET NO. 27 OF 32
AGENCY	DATE	NO.	ITEM	DATE		CITY OF FREEPORT	FREEPORT STREET IMPROVEMENTS	
					7202 Argo Drive Rockford, Illinois 61107-5037 (815) 398-2332 Design Firm License: Illinois 184-000818 Copyright 2015 By McClure Engineering Associates, Inc.	FILE: H:\14-016-FREEPORTSTREETIMPDESIGN\DRAWINGS\14-016 D2DETAILS.DWG	FREEPORT, ILLINOIS	MEAT JOB: 04-28-14-06

TYPICAL PAVEMENT MARKINGS

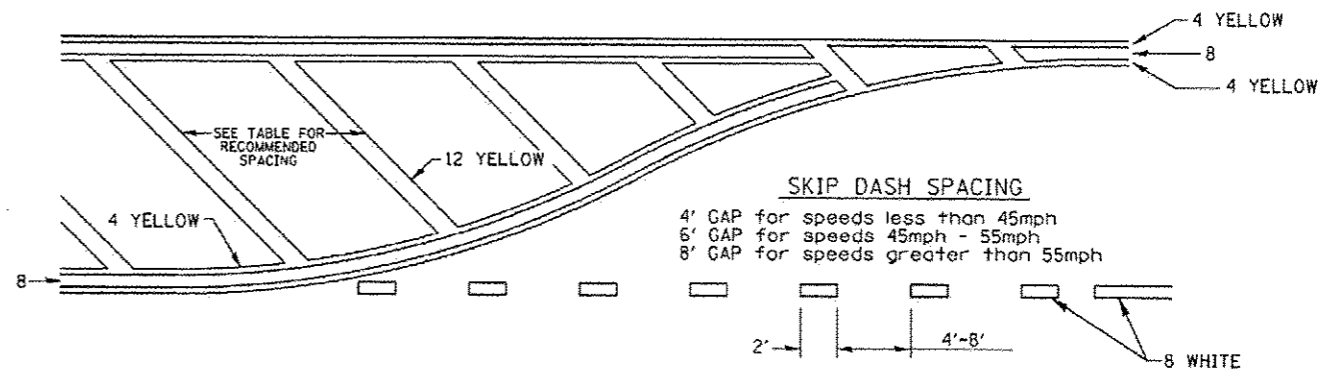
ARROW LAYOUT



- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER
- ◆ TWO-WAY AMBER MARKER

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

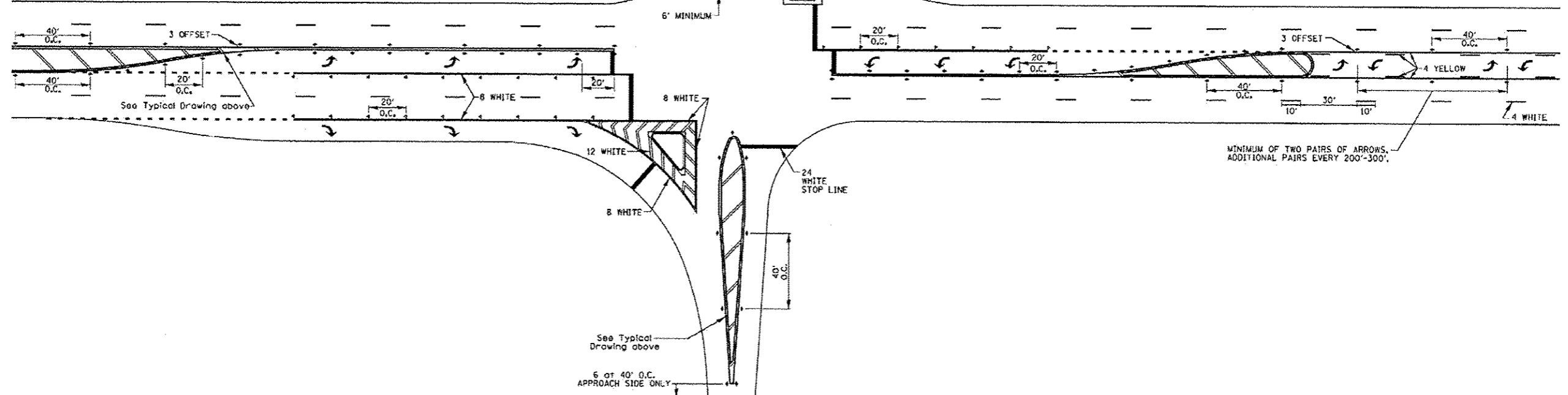
TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN



RECOMMENDED SPACING BETWEEN DIAGONALS (IN FEET)

Speed Limit Range	Continuous Median Area	Intersection Channelization	Objects (Islands)
less than 30MPH	50'	15'	10'
30-40MPH	75'	20'	15'
45MPH & over	75'	30'	20'

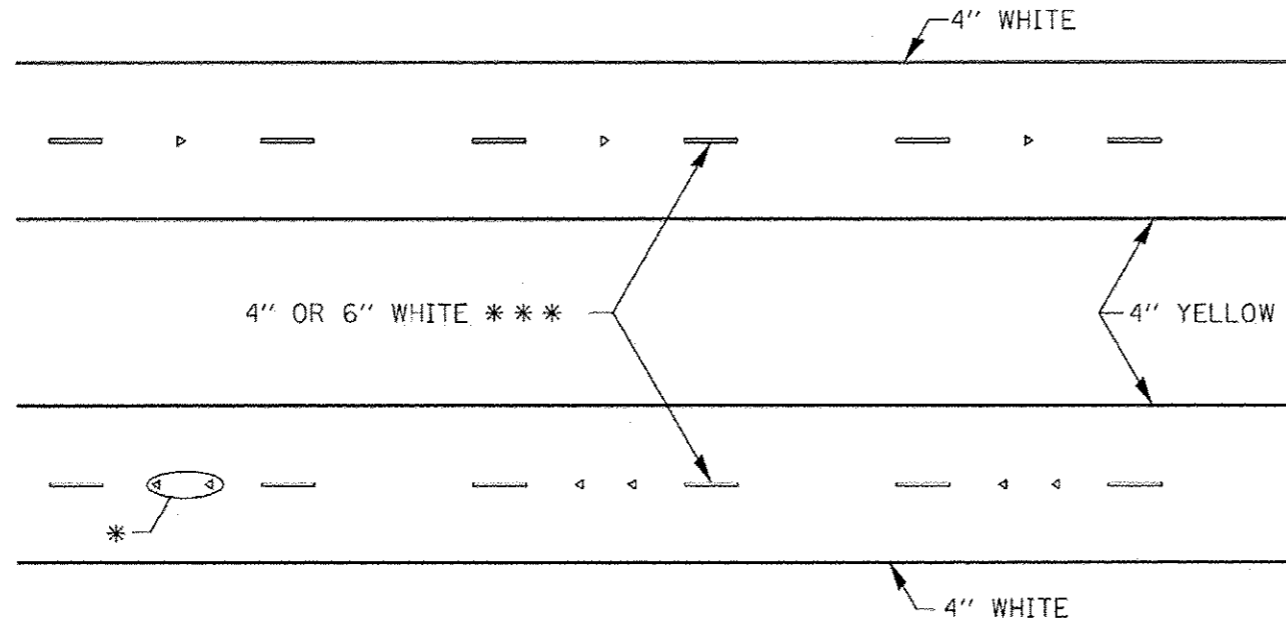
NOTE: If the spacing recommended in the Table does not permit at least five diagonal lines in the area being marked, the spacing from the next lowest speed range should be used. The recommended spacing is measured parallel to the pavement center line.



FILE NAME: District 2 Standard	USER NAME: JDOT/District 2	DESIGNED -	REVISED - 6-27-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REGION 2 / DISTRICT 2 STANDARD	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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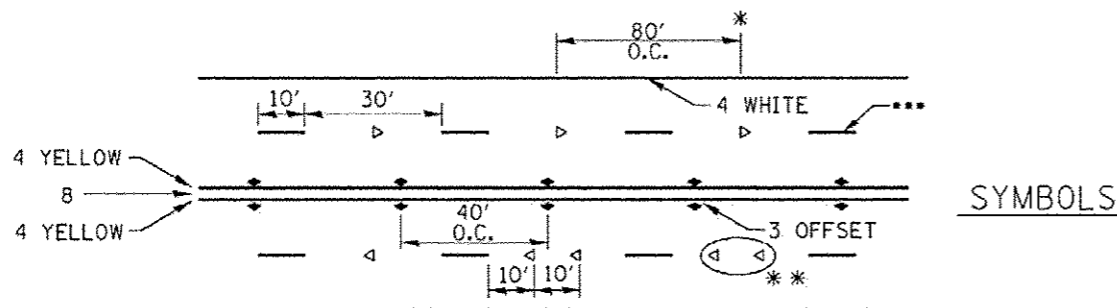
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SHEET REVIEW																																				
AGENCY	DATE																																			
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NO.	ITEM	DATE																																		
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CITY OF FREEPORT	FREEPORT STREET IMPROVEMENTS																																			
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MEAL JOB: 04-28-14-016																																				

TYPICAL PAVEMENT MARKINGS



* SEE HIGHWAY STANDARD 781001 FOR SPACING DETAILS.
USE DOUBLE MARKERS WHEN ADT > 20,000.

MULTI-LANE / DIVIDED

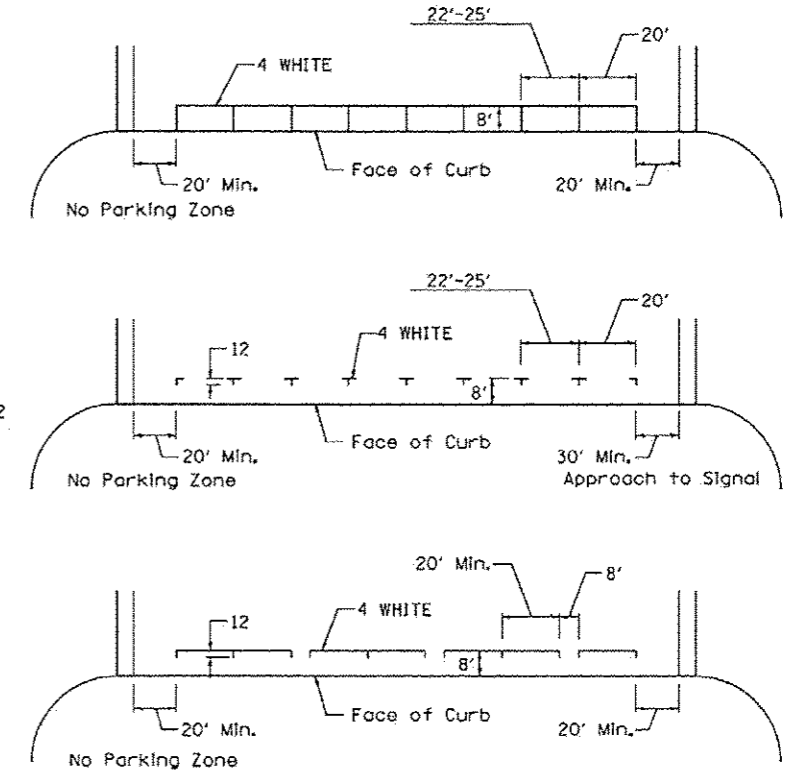


- REDUCE TO 40' O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH LOWER THAN POSTED SPEEDS.
- USE DOUBLE MARKERS WHEN ADT > 20,000
- CENTERLINE SKIP DASH PAVEMENT MARKING SPEED LIMIT LESS THAN 40 MPH USE 4" LINE, SPEED LIMIT 40 MPH AND OVER USE 6" LINE.

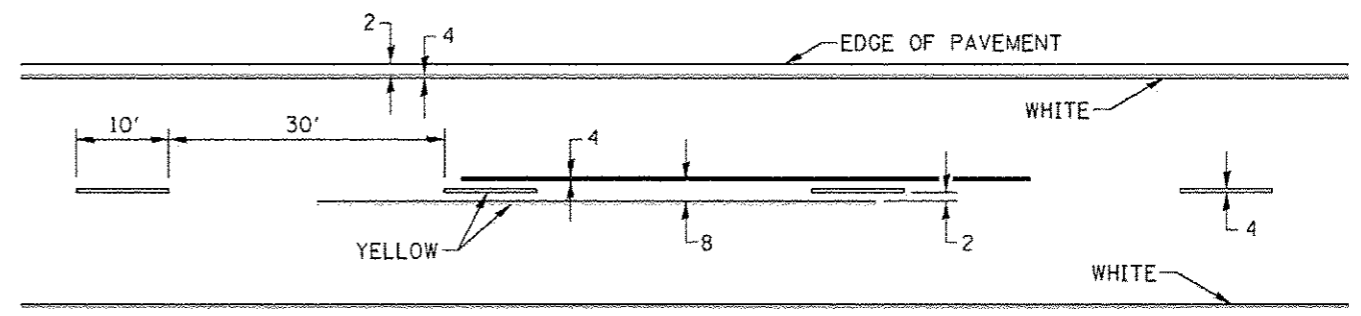
MULTI-LANE / UNDIVIDED & ONE WAY

(FOR MULTI-LANE UNDIVIDED HIGHWAYS USE THIS
DETAIL NOT HIGHWAY STANDARD 781001)

TYPICAL PARKING SPACING



TYPICAL PAVEMENT MARKING FOR TWO LANE SECTION - NO PASSING ZONES



FILE NAME = District 2 Standard	USER NAME = 1001/District 2	DESIGNED -	REVISED - 6-27-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REGION 2 / DISTRICT 2 STANDARD	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
	PLOT SCALE = 1/8" = 1' / 1"	DRAWN -	REVISED - 8-27-13			SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT
	PLOT DATE = Thu Nov 13 11:24:11 2014	CHECKED -	REVISED - 11-28-12									CONTRACT NO.	
		DATE -	REVISED -										

TYPICAL PAVEMENT MARKINGS SHEET 3 OF 3 41.1

SHEET REVIEW	
AGENCY	DATE

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NO.	ITEM	DATE

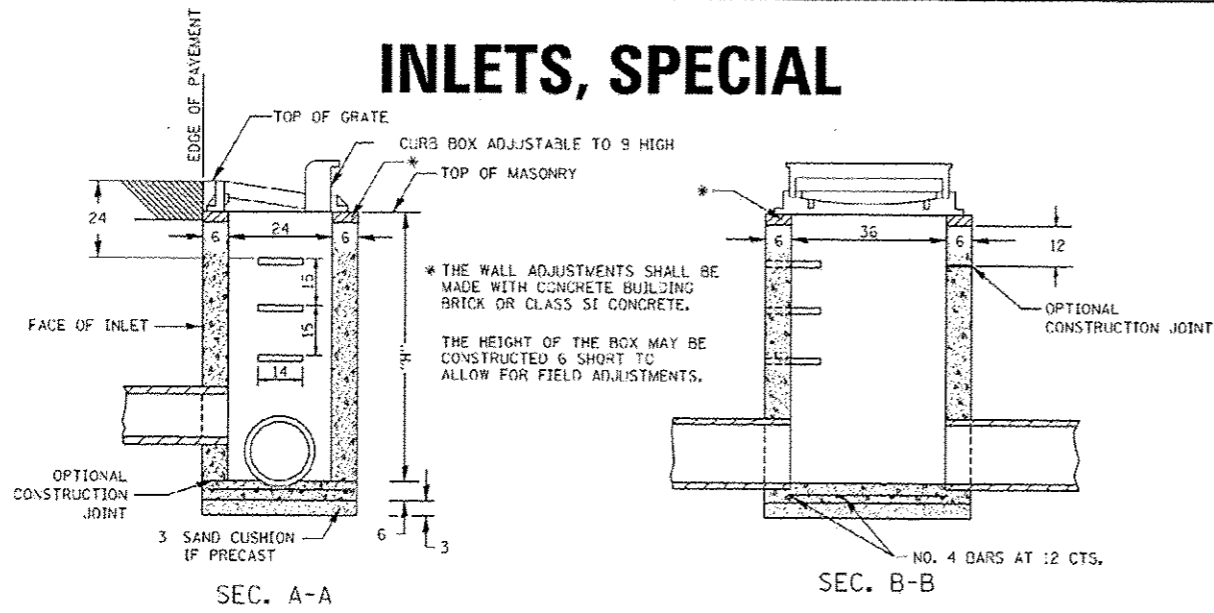
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CHECKED BY:	BSL
DATE:	6/15/2015

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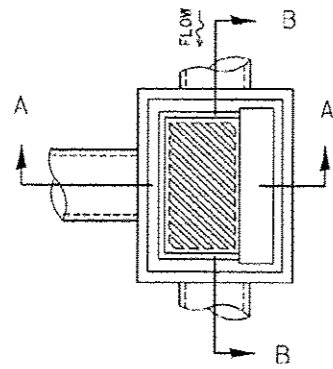
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FREEPORT STREET IMPROVEMENTS		
CITY OF FREEPORT	FREEPORT, ILLINOIS	
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SHEET NO.
30
OF
32

INLETS, SPECIAL



* THE WALL ADJUSTMENTS SHALL BE MADE WITH CONCRETE BUILDING BRICK OR CLASS SI CONCRETE.
THE HEIGHT OF THE BOX MAY BE CONSTRUCTED 6 SHORT TO ALLOW FOR FIELD ADJUSTMENTS.



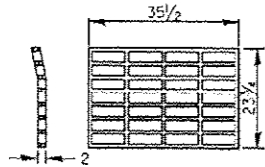
NOTES

- SEE STANDARD 602701 FOR DETAILS OF STEPS.
- EXCEPT AS NOTED HEREON INLET SPECIAL SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 602 OF THE STANDARD SPECIFICATIONS.
- THE SIDE WALLS MAY BE BUILT AS PRECAST SEGMENTED SECTIONS.
- ALL VOIDS AROUND PIPE ENTRANCE, BOTH INSIDE AND OUTSIDE, SHALL BE SEALED WITH MORTAR.
- WEIGHT OF CAST IRON FRAME & GRATE = 530 lbs. ± . STEPS SHALL BE OMITTED WHEN DEPTH OF "H" IS LESS THAN 5 FT.

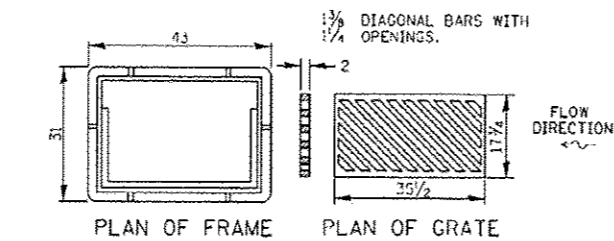
DETAIL OF FRAME & GRATE

NOTES

- CLASS SI CONCRETE OR PRECAST CONCRETE SHALL BE USED THROUGHOUT. PRECAST CONCRETE SHALL BE IN ACCORDANCE WITH SECTION 504.01 THRU 504.05 OF THE STANDARD SPECIFICATIONS EXCEPT THAT CONCRETE STRENGTH SHALL BE 4,000 PSI AFTER 28 DAYS.
- THE CONTRACT UNIT PRICE EACH FOR INLETS, SPECIAL SHALL INCLUDE THE COST OF CONSTRUCTING THE INLET BOX, FURNISHING AND INSTALLING THE FRAME AND GRATE, THE CAST IRON STEPS (IF USED), THE PRECAST FLOOR SLABS, SAND CUSHION (WHEN USED) AND REINFORCEMENT BARS.

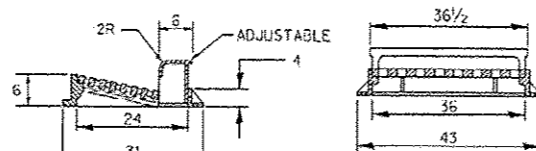


PLAN OF GRATE *



PLAN OF FRAME

PLAN OF GRATE



SECTION A-A

SECTION B-B

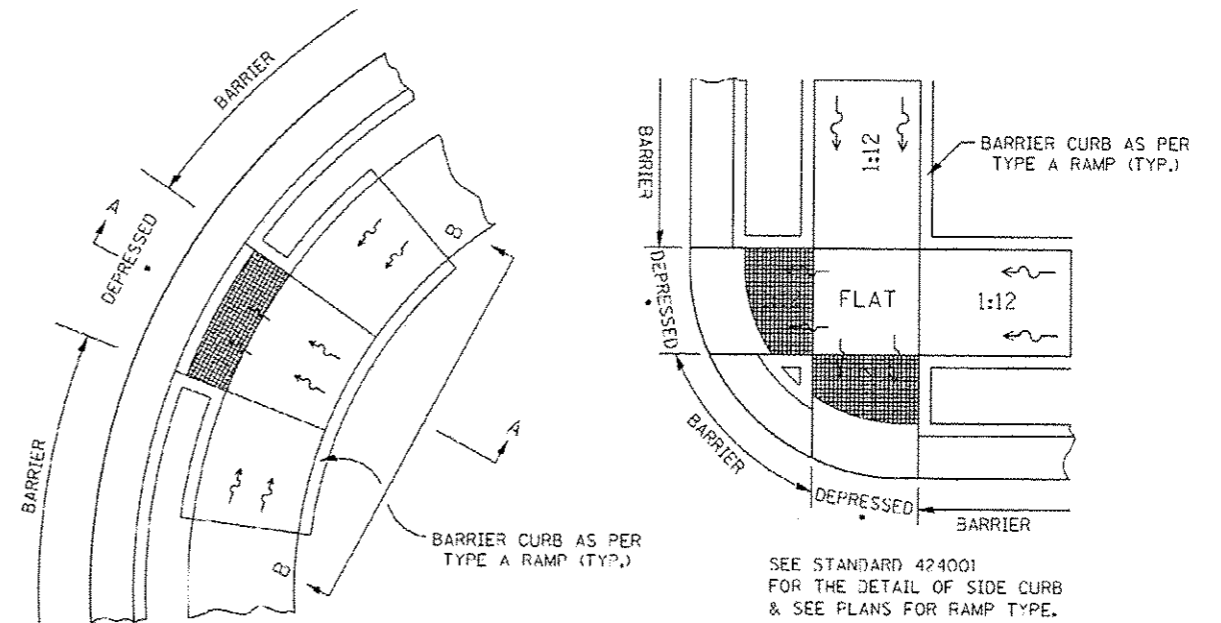
* THIS GRATE TO BE USED WITHOUT CURB BOX WHEN INLET IS IN DRIVEWAY.

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

REVISED - 5-27-14	REGION 2 / DISTRICT 2 STANDARD				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED - 10-13-11									
REVISED -									
REVISED -	SCALE: 1:0000	SHEET NO.	OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO.		

INLETS, SPECIAL 10.2

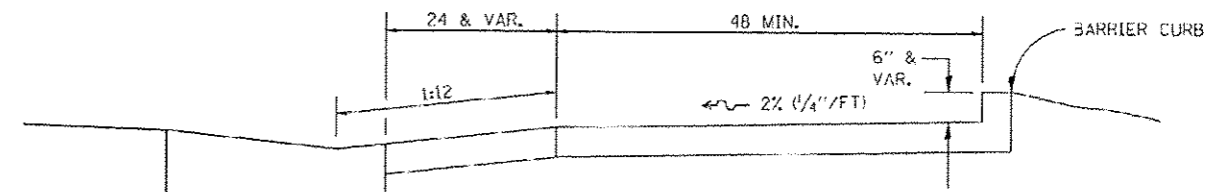
CURB RAMP DETAILS



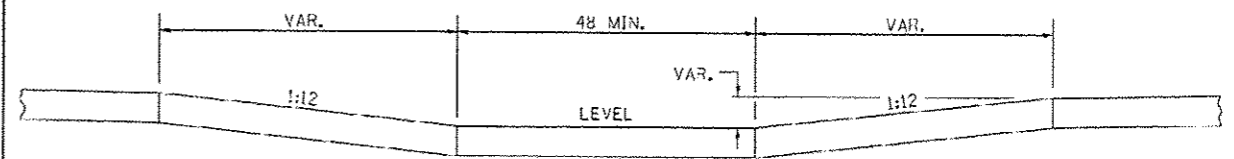
SEE STANDARD 424001 FOR THE DETAIL OF SIDE CURB & SEE PLANS FOR RAMP TYPE.

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

• SEE NOTE BELOW



SECTION A-A



SECTION B-B

- NOTES :
- THIS DETAIL TO BE USED IN CONJUNCTION WITH STATE STANDARD 424001.
 - THE MAXIMUM ALLOWABLE CROSS SLOPE FOR SIDEWALK IS 2% (1/4"/FT).
 - THE MAXIMUM ALLOWABLE SIDEWALK GRADE IS 8% (1/2"/FT). IF SPACE LIMITATIONS PROHIBIT THE USE OF THE 1:12 SLOPE, THEN SLOPES BETWEEN 1:10 AND 1:12 ARE PERMITTED FOR A MAXIMUM RISE OF 6. SLOPES 1:8 AND 1:10 ARE ALLOWED FOR A MAXIMUM RISE OF 3. SLOPES STEEPER THAN 1:8 ARE NOT PERMITTED.
 - THE DEPRESSED CURB IS NOT STANDARD. THE RISE IS 1/2 INSTEAD OF 1/2.

REVISED - 8-27-13	REGION 2 / DISTRICT 2 STANDARD				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED - 10-14-11									
REVISED -									
REVISED -	SCALE: 1:0000	SHEET NO.	OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO.		

CURB RAMP DETAILS 60.2

SHEET REVIEW	
AGENCY	DATE

REVISIONS		
NO.	ITEM	DATE

SCALE:	MA
DRAWN BY:	KRL
CHECKED BY:	OSL
DATE:	6/19/2015





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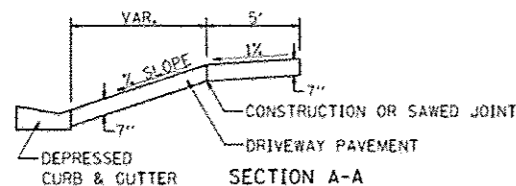
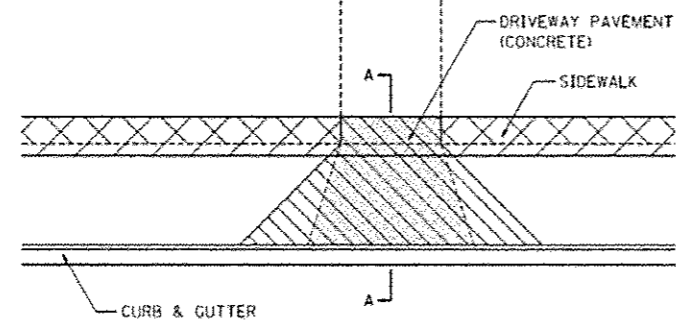
STANDARD DETAILS
FREEPORT STREET IMPROVEMENTS
CITY OF FREEPORT
FREEPORT, ILLINOIS
FILE: H:\14-016-FREEPORT STREET IMPROVEMENTS\DRAWINGS\14-06 DDETAILS.DWG
P&A JOB: 04-28-14-016

SHEET NO.
31
OF
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SIDEWALK AND DRIVEWAY PAVEMENT PAY AREAS

PAY FOR AS

-  SIDEWALK REMOVAL
-  DRIVEWAY PAVEMENT REMOVAL
-  PCC SIDEWALK 5
-  PCC DRIVEWAY PAVEMENT 7

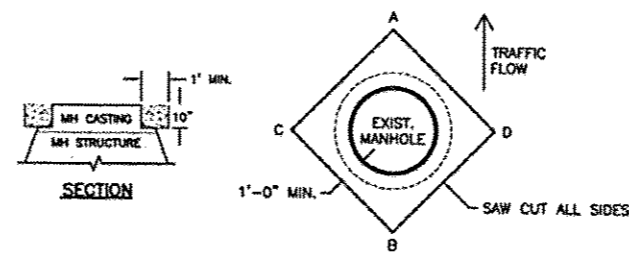


FOR DETAILS ON DIMENSIONS AND GRADES, SEE DISTRICT STANDARD 25.1 OR PLANS.

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

REVISED - 6-27-14	REGION 2 / DISTRICT 2 STANDARD	P.A. REL.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED - 10-03-11						
REVISED -						
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	SCALE: 1/8" = 1'-0"	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO.

SIDEWALK AND DRIVEWAY PAVEMENT PAY AREAS 35.4



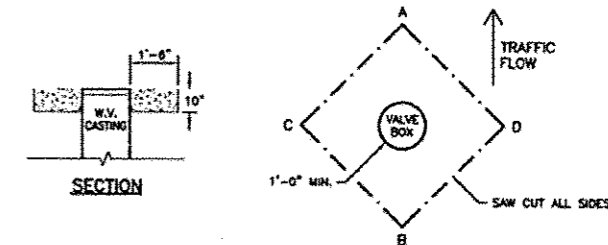
- ADJUST MANHOLE AFTER PAVING IS COMPLETED.
- SAW CUT SQUARE OPENING SET AT DIAGONAL, WITH POINTS OF DIAGONAL ABOUT 1.5' FROM EDGE OF MANHOLE.
- REMOVE PAVEMENT MATERIAL AND ADJUST MANHOLE TO WITHIN 1/4" TO 3/8" OF TOP OF NEW PAVEMENT IF STRING IS PULLED FROM POINT A TO B. CHECK C TO D WITH STRING FOR POTENTIAL CONFLICT AND ADJUST AS NEEDED.
- PRIOR TO PLACEMENT OF CONCRETE, MAKE SURE BASE IS COMPACTED SUFFICIENTLY & OUTSIDE EDGE OF OPENING HAS VERTICAL WALLS.
- FILL VOID WITH MINIMUM OF 10" OF CONCRETE, THIS WORK SHALL MEET THE REQUIREMENTS OF ARTICLE 442.02 NOTE #1 AND ARTICLE 701.17(a)(2)&(3).
- TROWEL OFF CONCRETE FLUSH WITH NEW PAVEMENT AND BROOM FINISH. MANHOLE FRAME SHALL BE RECESSED FROM CONCRETE NO MORE THAN 3/8".
- IF MANHOLE FRAME AND LID IS WITHIN 2" OF FINAL ADJUSTMENT, AN ADJUSTING RING MAY BE USED INSTEAD OF COMPLETE REMOVAL OF CASTING AND ADJUSTMENT. THIS WORK SHALL CONFORM TO SECTION 802 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS", LATEST EDITION. THE WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR MANHOLE TO BE ADJUSTED, WHICH SHALL INCLUDE SAW CUTTING, REMOVAL OF MATERIAL, RISER RING, AND PLACEMENT OF CONCRETE.
- IF MANHOLE FRAME AND LID REQUIRES AN ADJUSTMENT OF GREATER THAN 2", THEN REMOVAL OF CASTING AND LID SHALL BE REQUIRED WITH RECESSED ADJUSTMENT AS PER SECTION 802 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS", LATEST EDITION. THE WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR MANHOLES TO BE ADJUSTED, WHICH SHALL INCLUDE SAW CUTTING, REMOVAL OF MATERIAL, ADJUSTMENT OF MANHOLE FRAME AND LID AND CONCRETE PLACEMENT.
- THE CONTRACTOR SHALL ALSO MARK THE LOCATION OF EACH MANHOLE PRIOR TO PAVING ALONG THE EDGE OF THE PAVEMENT OR CURB WITH A LATH AND OR PAINT MARKS SO THAT IT CAN BE READILY FOUND. THE ABOVE WORK SHALL BE INCIDENTAL TO THE MANHOLE ADJUSTMENT.

NOTE: IF DURING THE PAVING OPERATION AN ADJUSTING RING IS USED AND ADJUSTED MANHOLE MATCHES THE NEW PAVEMENT, THEN SAW CUTTING, REMOVAL OF MATERIAL AND CONCRETE PLACEMENT WILL NOT BE REQUIRED. THIS WORK SHALL BE INCIDENTAL AND SHALL BE CONSIDERED A PART OF THE MANHOLE TO BE ADJUSTED. THIS WORK SHALL CONFORM TO SECTION 802 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS", LATEST EDITION. IF ABOVE WORK DOES NOT MEET GUIDELINES OF ITEM 3 AND 8 AFTER ROLLING OF PAVEMENT, THEN SAW CUTTING AND ADJUSTMENT WILL BE REQUIRED AS PER INSTRUCTIONS ABOVE.

MANHOLES IF CASTING IS REMOVED PRIOR TO PAVEMENT GRINDING OR PAVEMENT RE-SURFACING, A GEO TECHNICAL FABRIC SHALL BE PLACED OVER OPENING OF THE MANHOLE THAT WAS HAMMERED OUT. A PLYWOOD COOKE AND THE MANHOLE LID SHALL BE PLACED OVER THE FABRIC AND BACKFILLED WITH GRAVEL. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CLEAN OUT ANY DEBRIS FROM WITHIN THE MANHOLE PRIOR TO AND DURING THE ADJUSTMENT OF THE CASTING.

MANHOLE ADJUSTMENT DETAIL
N.T.S.

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- WATER VALVE BOX SHALL BE ADJUSTED AFTER PAVING IS COMPLETED.
- SAW CUT SQUARE OPENING SET AT DIAGONAL, WITH POINTS OF DIAGONAL ABOUT 1.5' MINIMUM FROM EDGE OF WATER VALVE BOX.
- REMOVE PAVEMENT MATERIAL AND ADJUST WATER VALVE BOX TO WITHIN 1/4" TO 3/8" OF TOP OF NEW PAVEMENT IF STRING IS PULLED FROM POINT A TO B. CHECK C TO D WITH STRING FOR POTENTIAL CONFLICT AND ADJUST AS NEEDED.
- PRIOR TO PLACEMENT OF CONCRETE MAKE SURE THE BASE IS COMPACTED SUFFICIENTLY & OUTSIDE EDGE OF OPENING HAS VERTICAL WALLS.
- FILL VOID WITH MINIMUM OF 10" OF CONCRETE, THIS WORK SHALL MEET THE REQUIREMENTS OF ARTICLE 442.02 NOTE #1 AND ARTICLE 701.17(a)(2)&(3).
- TROWEL OFF CONCRETE FLUSH WITH NEW PAVEMENT AND BROOM FINISH. WATER VALVE BOX FRAME SHALL BE RECESSED FROM CONCRETE NO MORE THAN 3/8".
- WATER VALVE BOX ADJUSTMENT MAY BE ACCOMPLISHED BY RAISING TOP SECTION OR BY PLACEMENT OF RISER RING. THIS WORK SHALL CONFORM TO SECTION 802 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS", LATEST EDITION. THE WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR WATER VALVES TO BE ADJUSTED.
- THE CONTRACTOR SHALL ALSO MARK THE LOCATION OF EACH VALVE PRIOR TO PAVING ALONG THE EDGE OF THE PAVEMENT OR CURB WITH A LATH AND OR PAINT MARKS SO THAT IT CAN BE READILY FOUND. THE ABOVE WORK SHALL BE INCIDENTAL TO THE WATER VALVE ADJUSTMENT.

NOTE: IF AN ADJUSTING RING IS USED DURING THE PAVING OPERATION, AND ADJUSTED WATER VALVE BOX MATCHES THE NEW PAVEMENT WITHOUT FURTHER ADJUSTMENT, THEN SAW CUTTING, REMOVAL OF MATERIAL, AND CONCRETE PLACEMENT WILL NOT BE REQUIRED. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO WATER VALVE TO BE ADJUSTED.

WATER VALVES IF CASTING IS REMOVED PRIOR TO PAVEMENT GRINDING OR PAVEMENT RE-SURFACING, A GEO TECHNICAL FABRIC SHALL BE PLACED OVER OPENING OF THE WATER VALVE THAT WAS HAMMERED OUT. A PLYWOOD COOKE AND THE WATER VALVE LID SHALL BE PLACED OVER THE FABRIC AND BACKFILLED WITH GRAVEL. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CLEAN OUT ANY DEBRIS FROM THE VALVE BOX PRIOR TO AND DURING THE ADJUSTMENT OF THE CASTING.

WATER VALVE ADJUSTMENT DETAIL
N.T.S.

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SHEET REVIEW	
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SHEET NO.
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