

LETTING ITEM NO. 8A  
JULY 31, 2015 LETTING

TOTAL SHEETS: 22  
QU019

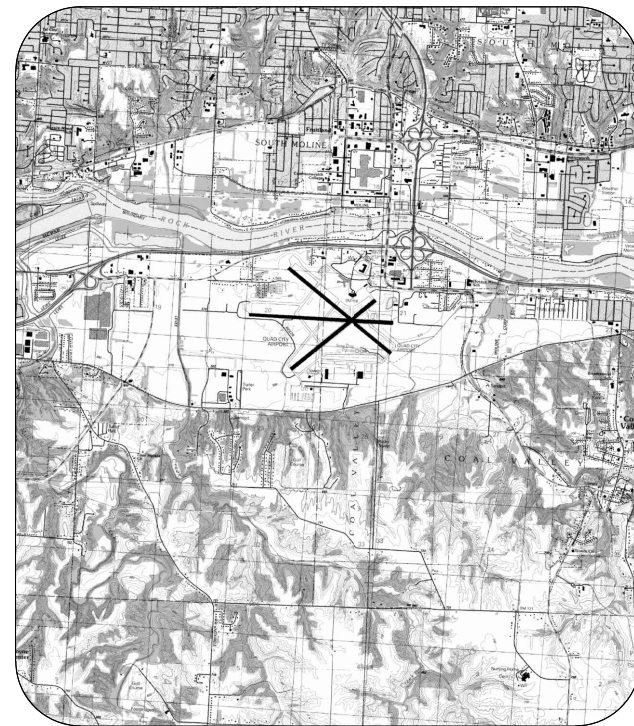
# CONSTRUCTION PLANS FOR QUAD CITY INTERNATIONAL AIRPORT

METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
MOLINE, ILLINOIS

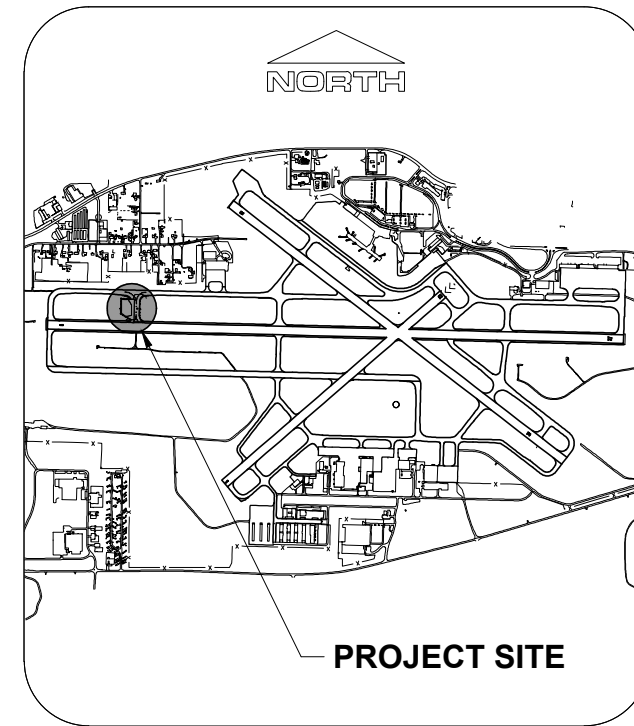
IL. PROJ. NO. MLI-4483  
AIP PROJ. NO. 3-17-0068-XX

## REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING

JUNE 26, 2015



LOCATION MAP



SITE PLAN



### TAXIWAY H2 GEOMETRIC DESIGN CRITERIA

AIRCRAFT DESIGN GROUP IV  
DESIGN APPROACH CATEGORY C & D  
TAXIWAY DESIGN GROUP 5



THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

CALL J.U.L.I.E.  
BEFORE EXCAVATING  
1-800-892-0123

TOWNSHIP: 17 NORTH  
RANGE: 1 WEST  
SECTION: 20  
COUNTY: ROCK ISLAND  
CIVIL TOWNSHIP: UNINCORPORATED  
ROCK ISLAND COUNTY

MAXIMUM EQUIPMENT HEIGHT - 25'  
UNICOM FREQUENCY - 122.95

METROPOLITAN AIRPORT AUTHORITY  
OF ROCK ISLAND  
QUAD CITY INTERNATIONAL AIRPORT  
MOLINE, ILLINOIS

APPROVED: *Bruce Carter*  
DATE: *April 13, 2015*

**CMT**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS

SUBMITTED BY: *Se M*  
SEAN M. SMITH, P.E.  
DATE: *17 April 2015*

CMT JOB NUMBER: 14014-06-00

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FILE: 01 - COVER.dwg  
UPDATE BY: Sean Smith  
PLOT DATE: 6/29/2015 11:00 AM

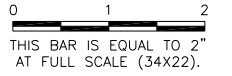
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02	INDEX TO SHEETS AND SUMMARY OF QUANTITIES
03	SITE PLAN
04	PROJECT CONTROL PLAN
05	SUGGESTED SEQUENCE OF CONSTRUCTION
06	SEQUENCE OF CONSTRUCTION GENERAL NOTES
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22	SOIL BORING AND PAVEMENT CORE LOCATIONS

SUMMARY OF QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR108158	1/C #8 5 KV UG CABLE IN UD	LF	1,750	
AR110212	2" STEEL DUCT, DIRECT BURY	LF	30	
AR125415	MITL-BASE MOUNTED	EACH	31	
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	28	
AR125906	REMOVE SPLICE CAN	EACH	1	
AR150520	MOBILIZATION	LS	1	
AR152480	SHOULDER ADJUSTMENT	SY	800	
AR156520	INLET PROTECTION	EACH	4	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	LF	5,000	
AR201663	SAND MIX CRACK REPAIR	LF	500	
AR201670	CRACK CONTROL FABRIC	SY	5,600	
AR401610	BITUMINOUS SURFACE COURSE	TON	2,400	
AR401650	BITUMINOUS PAVEMENT MILLING	SY	5,600	
AR401655	BUTT JOINT CONSTRUCTION	SY	2,570	
AR401910	REMOVE & REPLACE BIT. PAVEMENT	SY	10	
AR603510	BITUMINOUS TACK COAT	GAL	2,700	
AR620520	PAVEMENT MARKING-WATERBORNE	SF	4,700	
AR620900	PAVEMENT MARKING REMOVAL	SF	2,900	
AR800004	REMOVE REFLECTIVE MARKER	EACH	13	
AR800116	PAVEMENT MARKING - PREFORMED THERMOP	SF	525	
AR800117	PAVEMENT MARKING - PREFORMED THERMOPLASTIC (AIRPORT SUPPLIED)	SF	1,450	
AR904510	SODDING	SY	800	

**QU019**


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**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**

**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
 INDEX TO SHEETS AND SUMMARY OF  
 QUANTITIES**

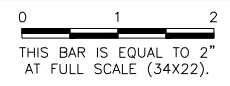

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JOB No:	14014-06-00
IL. PROJ. NO. MLI-4483 AIP PROJ. NO. 3-17-0068-XX	
SHEET 02 OF 22 SHEETS	

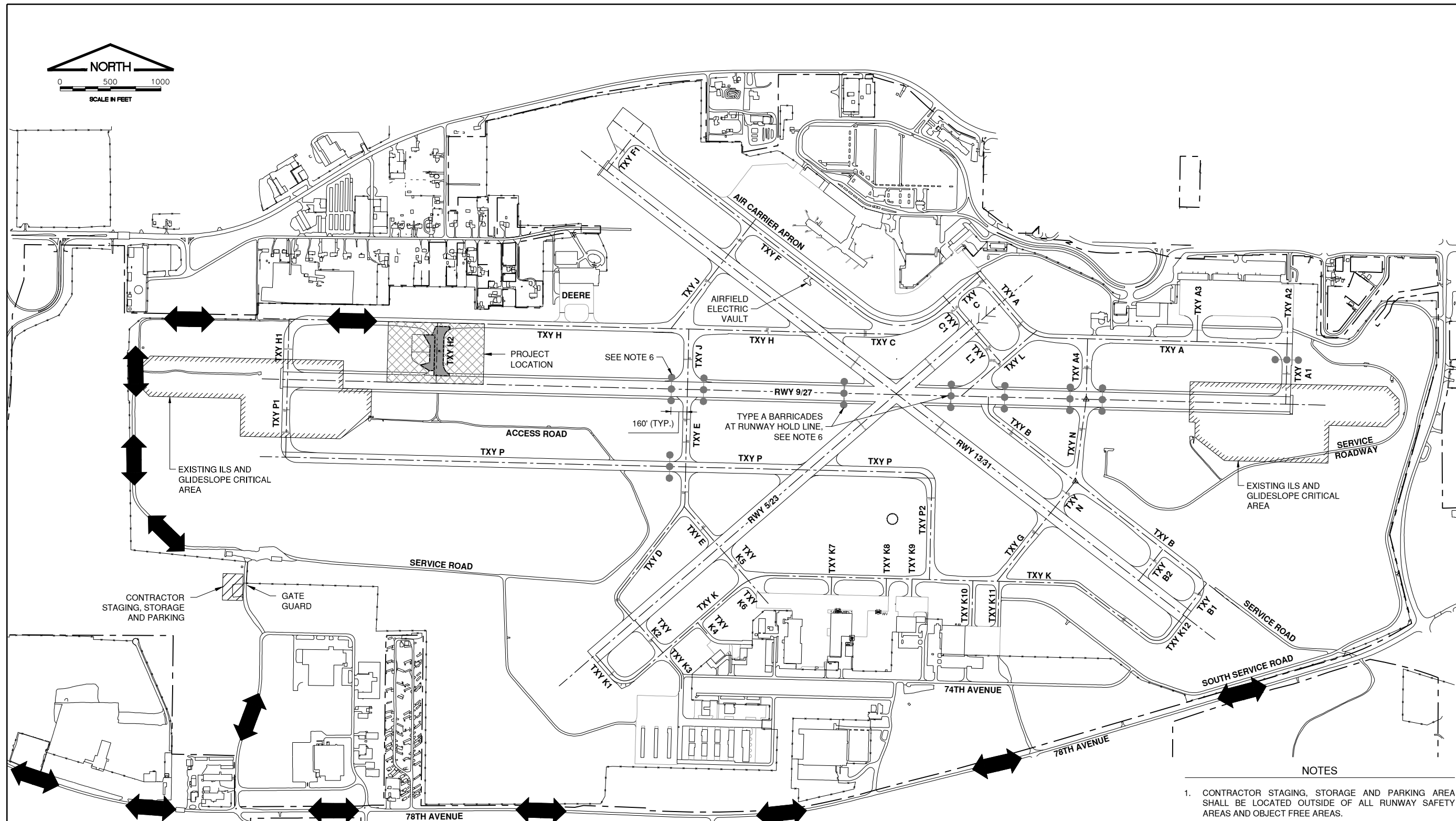
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**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**

**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
 SITE PLAN**



**NOTES**

- CONTRACTOR STAGING, STORAGE AND PARKING AREA SHALL BE LOCATED OUTSIDE OF ALL RUNWAY SAFETY AREAS AND OBJECT FREE AREAS.
- CONTRACTOR SHALL BE REQUIRED TO SECURE HIS ACCESS GATES DURING WORKING AND NON-WORKING HOURS.
- DAMAGE TO AIRPORT SERVICE ROADS AND AIRFIELD PAVEMENTS SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE TO THE SATISFACTION OF THE AIRPORT AND THE ENGINEER.
- CONTRACTOR TO NOTIFY COUNTY, TOWNSHIP AND CITY TRANSPORTATION DEPARTMENTS AS REQUIRED FOR OFF AIRFIELD ACCESS ROUTES.
- TAXIWAY NAMES SHOWN IN THESE DOCUMENTS REPRESENT FUTURE TAXIWAY NAMING PER THE AIRPORT'S UPDATED SIGNAGE PLAN. SIGNAGE UPDATES ARE SCHEDULED TO BE COMPLETED BY FALL 2015.
- IN ADDITION TO THE BARRICADES SHOWN ON THE SEQUENCE OF CONSTRUCTION PLAN, THE CONTRACTOR SHALL INSTALL TYPE A BARRICADES WITH RED, STEADY BURN LIGHTS ON 15-FOOT CENTERS AT THE LOCATIONS SHOWN ON THIS SHEET WHEN RUNWAY 9/27 IS CLOSED.

**LEGEND**

- CONSTRUCTION WORK AREA
- PAVEMENT REHABILITATION LIMITS
- CONTRACTOR STAGING, STORAGE AND PARKING
- PREFERRED CONTRACTOR ACCESS ROUTE
- EXISTING FENCELINE
- EXISTING AIRPORT PROPERTY LINE
- TYPE A BARRICADES REQUIRED DURING RUNWAY 9/27 CLOSURES

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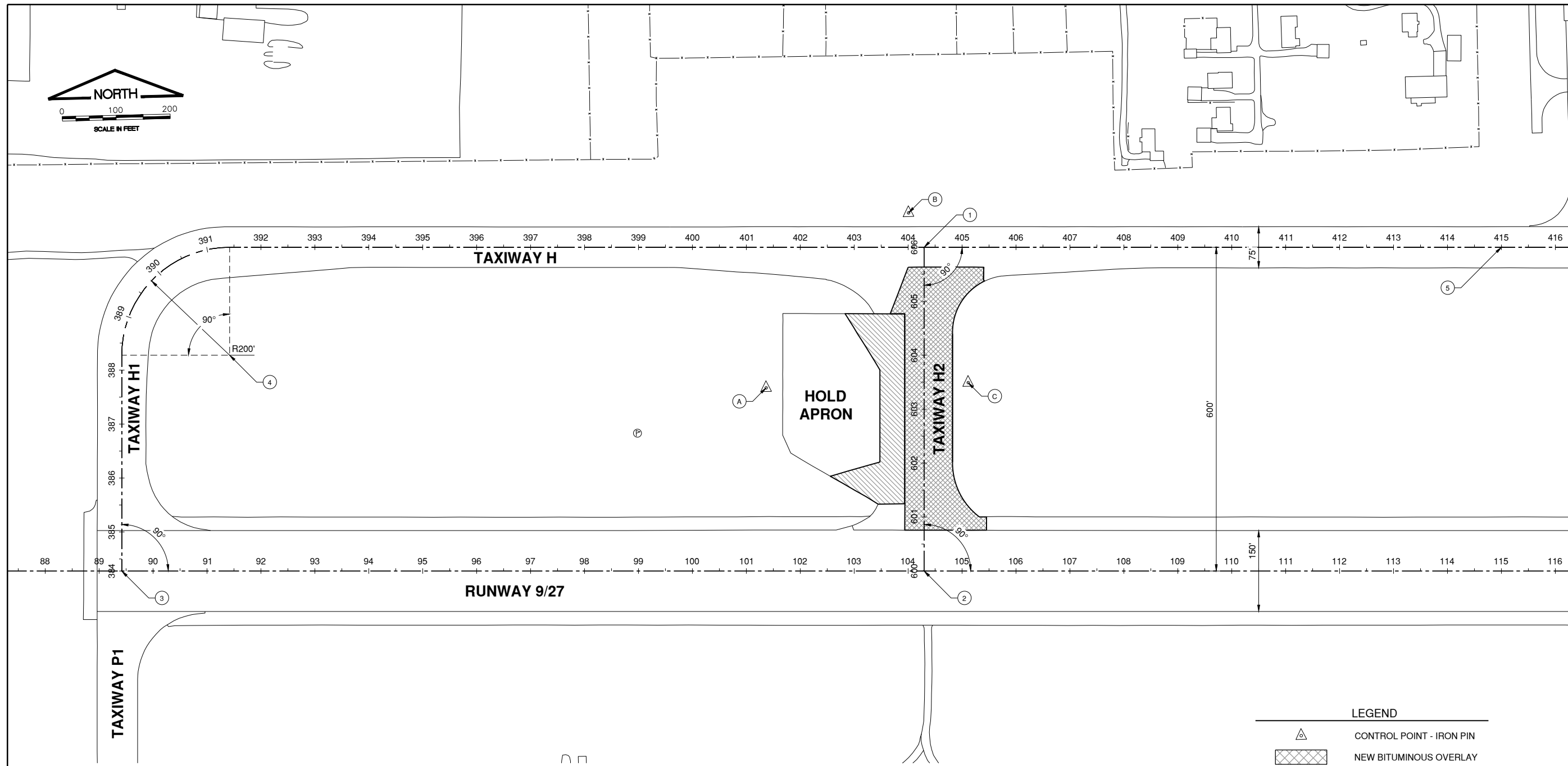
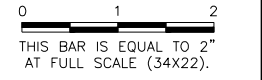
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SHEET 03 OF 22 SHEETS	

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**LEGEND**

	CONTROL POINT - IRON PIN
	NEW BITUMINOUS OVERLAY
	NEW BITUMINOUS TRANSITION

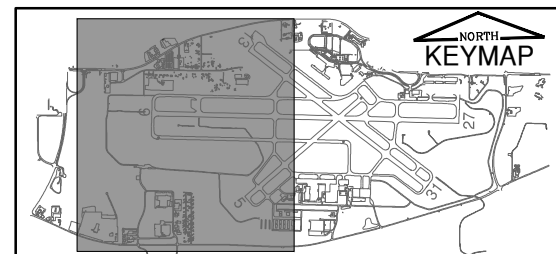
- NOTES**
1. THE CONTRACTOR SHALL BE REQUIRED TO VERIFY THE HORIZONTAL AND VERTICAL CONTROL POINTS PRIOR TO BEGINNING CONSTRUCTION.
  2. DISCREPANCIES IN THE CONTROL POINTS SHALL BE BROUGHT TO THE ATTENTION OF THE RESIDENT ENGINEER AS SOON AS THEY ARE DISCOVERED. THE CONTRACTOR SHALL NOT PROCEED WITH ANY WORK THAT COULD BE IMPACTED BY CONTROL POINT DISCREPANCIES UNTIL RESOLUTION IS COMPLETE.

**HORIZONTAL CONTROL POINTS**

POINT	€ STATION 1	€ STATION 2	NORTHING	EASTING
1	TAXIWAY H STA. 404+29.80	TAXIWAY H2 STA. 606+00.00	1,742,589.49	2,199,344.94
2	RUNWAY 9/27 STA. 104+29.80	TAXIWAY H2 STA. 600+00.00	1,741,989.67	2,199,330.12
3	RUNWAY 9/27 STA 89+42.00	TAXIWAY H1 STA. 384+27.84	1,742,026.42	2,197,842.78
4	TAXIWAY H1 STA. 388+27.84	TAXIWAY H STA. 391+41.99	1,742,421.36	2,198,052.60
5	TAXIWAY H STA. 415+00.00		1,742,563.06	2,200,414.82

**VERTICAL CONTROL POINTS**

POINT	€ STATION 1	€ STATION 2	NORTHING	EASTING	ELEVATION
A - IRON PIN	RUNWAY 9/27 STA. 101+36.54, 339.40' LT.	TAXIWAY H2 STA. 603+39.40, 293.26' LT.	1,742,336.22	2,199,045.34	577.77
B - IRON PIN	TAXIWAY H STA. 404+00.60, 63.52' LT.	TAXIWAY H2 STA. 606+63.52, 29.20' LT.	1,742,653.72	2,199,317.32	580.56
C - IRON PIN	RUNWAY 9/27 STA. 105+11.31, 349.04' LT.	TAXIWAY H2 STA. 603+49.04, 81.51' RT.	1,742,336.60	2,199,420.23	580.05



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND**  
**QUAD CITY INTERNATIONAL AIRPORT**  
**MOLINE, ILLINOIS**

**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING**  
**PROJECT CONTROL PLAN**

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SHEET	04 OF 22 SHEETS

# CRITICAL POINTS

POINT NO.	LATITUDE	LONGITUDE	ELEV.*
A	41° 26' 57.51"	90° 31' 55.98"	598.00
B	41° 26' 59.53"	90° 31' 36.61"	606.00
C	41° 26' 59.23"	90° 31' 17.24"	610.00
D	41° 27' 02.47"	90° 31' 03.00"	614.00
E	41° 26' 36.97"	90° 31' 42.47"	607.00
F	41° 26' 34.37"	90° 31' 42.47"	608.00
G	41° 26' 56.28"	90° 30' 46.66"	615.00
H	41° 26' 48.68"	90° 30' 46.87"	609.00

\* ELEVATION INCLUDES 25' ANTICIPATED MAXIMUM EQUIPMENT HEIGHT

## PHASING NOTES (ALL PHASES)

- ANY TIME THE HAUL ROUTE USES OR CROSSES EXISTING AIRPORT PAVEMENT, THE PAVEMENT SHALL BE SWEEPED AND CLEANED AS REQUIRED TO THE SATISFACTION OF THE AIRPORT AND/OR RESIDENT ENGINEER.
- DAMAGE TO AIRPORT SERVICE ROADS, CONTRACTOR STAGING AREA AND AIRFIELD PAVEMENTS SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- ALL CLOSED TAXIWAY SECTIONS SHALL HAVE THE TAXIWAY EDGE LIGHTS AND TAXIWAY GUIDANCE SIGNS REMOVED FROM SERVICE. IF DEACTIVATING THE CIRCUIT IMPACTS AN OPEN TAXIWAY SECTION, THE EDGE LIGHTS AND GUIDANCE SIGNS MAY BE COVERED IN A MANNER THAT PREVENTS VISIBLE LIGHT. ADDITIONALLY, A TEMPORARY JUMPER MAY BE INSTALLED TO REMOVE THE LIGHTS FROM THE ACTIVE CIRCUIT. METHOD OF DEACTIVATION SHALL BE APPROVED BY THE RESIDENT ENGINEER. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- CONTRACTOR SHALL INSTALL TAXIWAY CLOSURE MARKERS AND REMOVE TAXIWAY LEAD-IN LINES AS SHOWN OR AS DIRECTED BY THE RESIDENT ENGINEER, TO MINIMIZE IMPACT TO THE AIRPORT THIS WORK SHALL BE EXPEDITED AND COORDINATED TO BE COMPLETED WITHIN THE SAME CALENDAR DAY. SEE SEQUENCE OF CONSTRUCTION DETAILS SHEET FOR TAXIWAY CLOSURE MARKER DETAILS AND NOTES.
- CONTRACTOR SHALL INSTALL AND MAINTAIN RUNWAY CLOSURE MARKERS AT BOTH ENDS OF THE RUNWAY WHEN THE RUNWAY IS CLOSED. SEE SEQUENCE OF CONSTRUCTION DETAILS SHEET FOR RUNWAY CLOSURE MARKER DETAILS AND NOTES.

- CONSTRUCTION ACTIVITIES WILL REQUIRE THE CLOSURE OF VARIOUS AIRFIELD PAVEMENTS THROUGHOUT THE DURATION OF THE PROJECT. ALL PAVEMENT CLOSURES SHALL BE COORDINATED WITH THE AIRPORT AND/OR RESIDENT ENGINEER A MINIMUM OF 72 HOURS PRIOR TO CLOSURE. SEE PHASING SUMMARY TABLE LOCATED ON SEQUENCE OF CONSTRUCTION DETAILS SHEET FOR DETAILED PAVEMENT CLOSURE INFORMATION.

- THE LOCATION OF THE BARRICADES SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. SHOULD THE PHASING REQUIRE THE BARRICADES TO BE REPOSITIONED THE WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

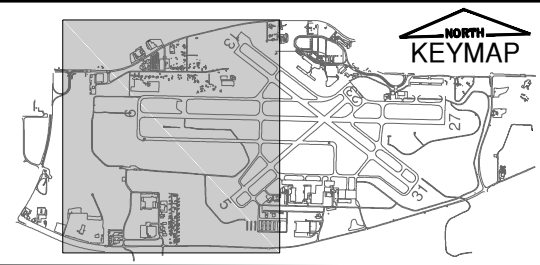
## PHASE A: SUGGESTED SEQUENCE OF CONSTRUCTION

- MARK RUNWAY SAFETY AREA (R.S.A) WITH LATH AND RIBBON AND PLACE BARRICADES AS SHOWN OR AS DIRECTED BY THE ENGINEER.
- REMOVE TAXIWAY LEAD-IN LINE MARKINGS AND INSTALL TEMPORARY TAXIWAY CLOSURE MARKINGS AS SHOWN ON SEQUENCE OF CONSTRUCTION DETAILS SHEET. COORDINATE RUNWAY ACCESS WITH AIRPORT TO ISSUE APPROPRIATE NOTAMS.
- INSTALL EROSION CONTROL MEASURES AS REQUIRED FOR PHASE A.
- MILL EXISTING BITUMINOUS PAVEMENT.
- CLEAN, SEAL AND REPAIR BITUMINOUS CRACKS. PLACE CRACK CONTROL FABRIC.
- INSTALL LIGHTING AND CABLING.

- PLACE NEW BITUMINOUS PAVEMENT (LIFT ONE).

## PHASE B: SUGGESTED SEQUENCE OF CONSTRUCTION

- PLACE RUNWAY CLOSURE MARKERS ON BOTH ENDS OF RUNWAY 9/27 AND PLACE BARRICADES AS SHOWN OR AS DIRECTED BY THE ENGINEER.
- INSTALL EROSION CONTROL MEASURES AS REQUIRED FOR PHASE B.
- MILL EXISTING BITUMINOUS PAVEMENT.
- CLEAN, SEAL AND REPAIR BITUMINOUS CRACKS. PLACE CRACK CONTROL FABRIC.
- INSTALL LIGHTING AND CABLING.
- PLACE NEW BITUMINOUS PAVEMENT (LIFT ONE).
- PLACE NEW BITUMINOUS SURFACE COURSE (PHASE A & B).
- PLACE NEW PAVEMENT MARKINGS (PHASE A & B).
- ADJUST SHOULDERS AND RESTORE WITH SOD (PHASE A & B).
- CLEAN PAVEMENTS, REMOVE EROSION CONTROL MEASURES, REMOVE RUNWAY AND TAXIWAY CLOSURE MARKERS, REMOVE BARRICADES AND LATH (PHASE A & B) AND REOPEN ALL TAXIWAYS AND RUNWAY 9/27.



### LEGEND

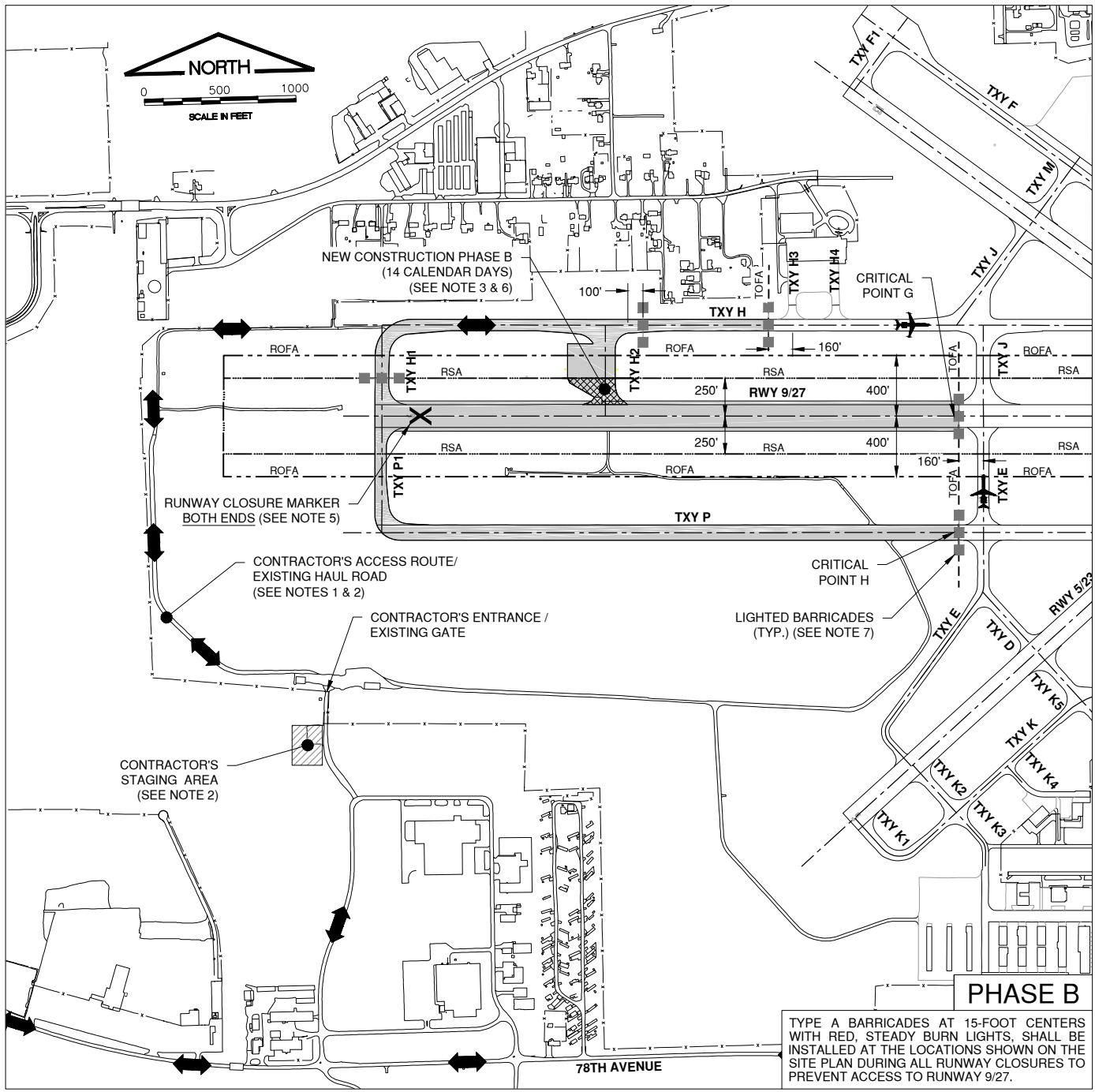
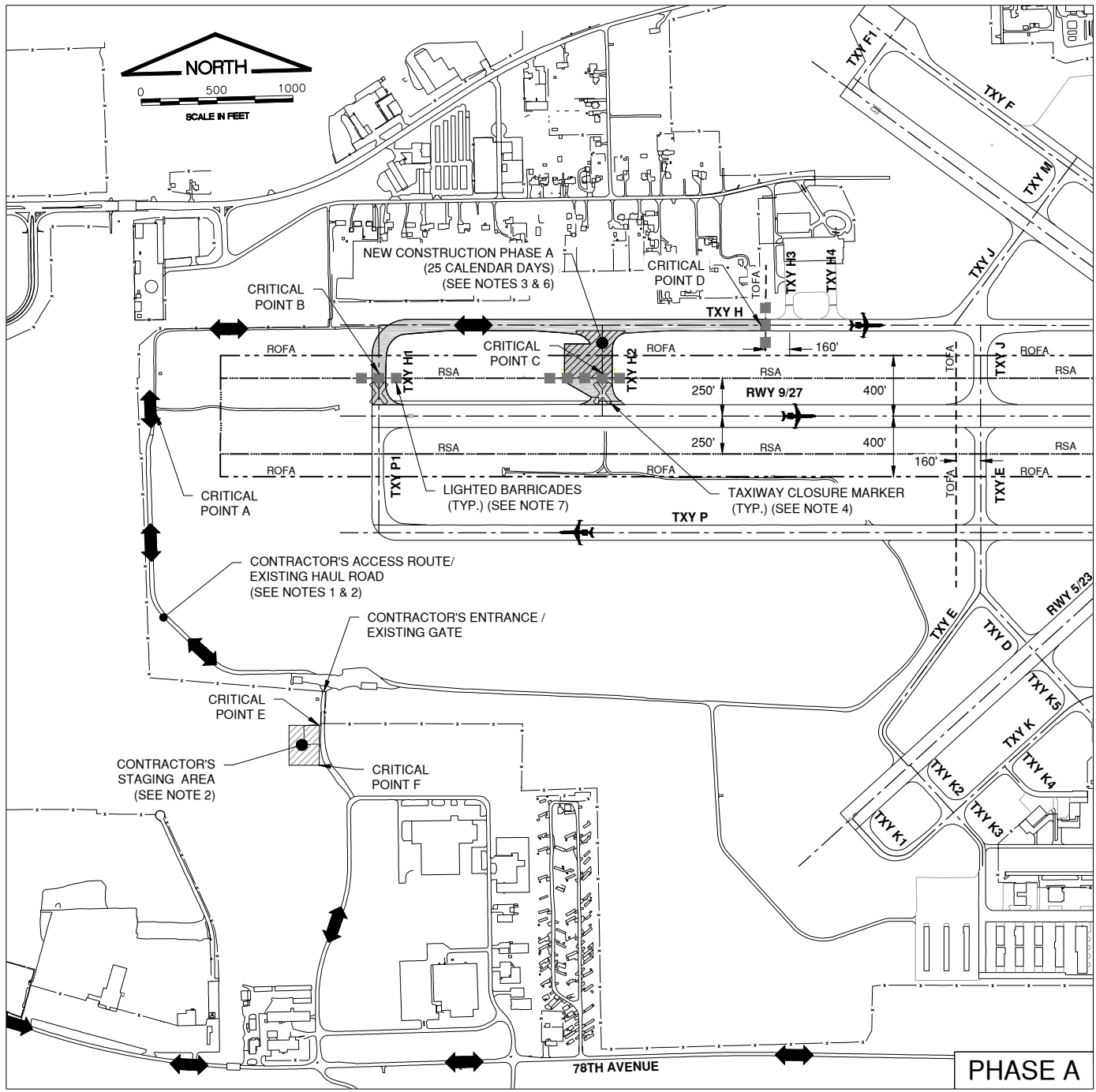
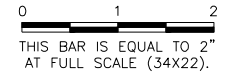
	CLOSED AIRFIELD PAVEMENTS		LIGHTED BARRICADES (R.S.A.)
	CONSTRUCTION WORK AREA PHASE A		TAXIWAY OBJECT FREE AREA (T.O.F.A.)
	CONSTRUCTION WORK AREA PHASE B		RUNWAY OBJECT FREE AREA (R.O.F.A.)
	CONTRACTOR'S CONSTRUCTION ACCESS / HAUL ROUTE		AIRCRAFT MOVEMENT AREAS
	RUNWAY CLOSURE MARKER		TAXIWAY CLOSURE MARKER

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 UPDATE BY: Sean Smith  
 PLOT DATE: 6/29/2015 11:02 AM

Taxiway K Base

## QU019

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NUMBER	BY	DATE



TYPE A BARRICADES AT 15-FOOT CENTERS WITH RED, STEADY BURN LIGHTS, SHALL BE INSTALLED AT THE LOCATIONS SHOWN ON THE SITE PLAN DURING ALL RUNWAY CLOSURES TO PREVENT ACCESS TO RUNWAY 9/27.

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND**  
**QUAD CITY INTERNATIONAL AIRPORT**  
**MOLINE, ILLINOIS**  
**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING**  
**SUGGESTED SEQUENCE OF CONSTRUCTION**

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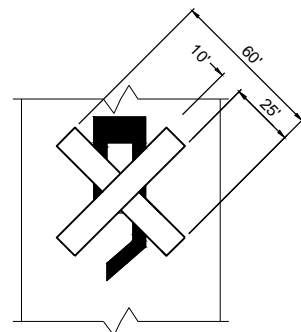
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**LIGHTED RUNWAY CLOSURE MARKERS NOTES**

1. THE AIRPORT SHALL FURNISH TWO PORTABLE, LIGHTED RUNWAY CLOSURE MARKERS FOR THE DURATION OF THE PROJECT. IF LIGHTED RUNWAY CLOSURE MARKERS ARE NOT AVAILABLE, THE CONTRACTOR SHALL REFER TO THE "CLOSED RUNWAY MARKER DETAIL" ON THIS SHEET.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRANSPORTING, INSTALLING, MAINTAINING, REFUELING, REPOSITIONING AND REMOVING THE LIGHTED RUNWAY CLOSURE MARKERS AS SHOWN IN THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
3. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS. WHEN CONSTRUCTION OPERATIONS CONFLICT, THE CLOSURE MARKERS SHALL BE MOVED TO AN ALTERNATE LOCATION AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER.
4. IT WILL BE NECESSARY TO CLOSE RUNWAY 9/27 TO AIR TRAFFIC FOR THE DURATION OF PHASE B CONSTRUCTION. THE CONTRACTOR SHALL MARK RUNWAY 9/27 CLOSED BY PLACING LIGHTED RUNWAY CLOSURE MARKER AT THE LOCATION DETAILED IN THE PLANS OR AS DIRECTED BY THE ENGINEER AND AIRPORT. THE LIGHTED RUNWAY CLOSURE MARKERS ARE REQUIRED TO BE IN OPERATION ANYTIME THE RUNWAY IS CLOSED.
5. UPON COMPLETION OF THE PROJECT, THE MARKERS SHALL BE RETURNED TO THE AIRPORT IN GOOD CONDITION.
6. ALL COST ASSOCIATED WITH THE LIGHTED RUNWAY CLOSURE MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

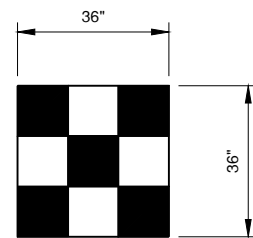


**CLOSED RUNWAY MARKER DETAIL**

ON PAVEMENT - NO SCALE

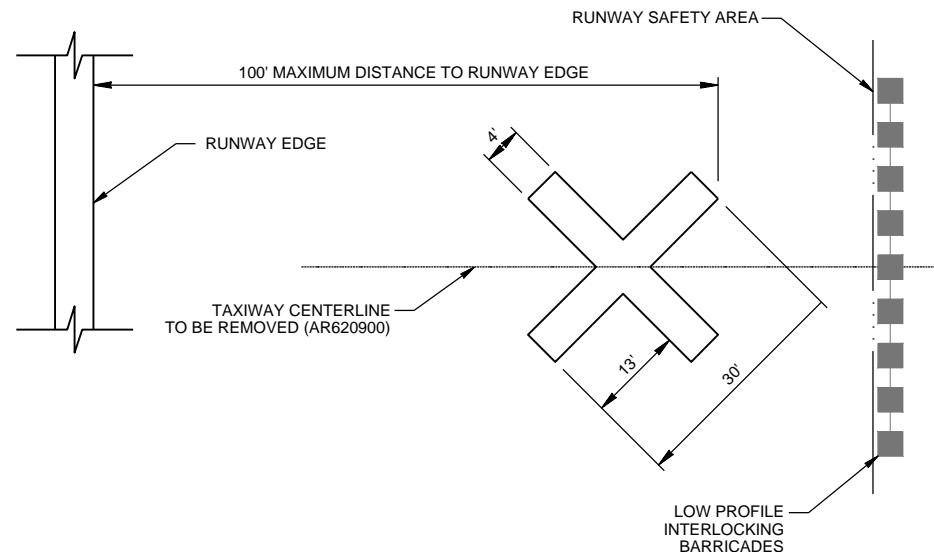
**RUNWAY CLOSURE MARKERS NOTES**

1. THE CONTRACTOR SHALL USE THE LIGHTED RUNWAY CLOSURE MARKERS PROVIDED BY THE AIRPORT. IF THE LIGHTED RUNWAY CLOSURE MARKERS ARE NOT AVAILABLE, THE CONTRACTOR WILL BE REQUIRED TO INSTALL RUNWAY CLOSURE MARKERS AS DETAILED ABOVE.
2. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
3. MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER AND THE AIRPORT.
4. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
5. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
6. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
7. IT WILL BE NECESSARY TO CLOSE RUNWAY 9/27 TO AIR TRAFFIC FOR THE DURATION OF PHASE B CONSTRUCTION. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET OR AS DIRECTED BY THE ENGINEER AND AIRPORT. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.



**CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG**

NOT TO SCALE

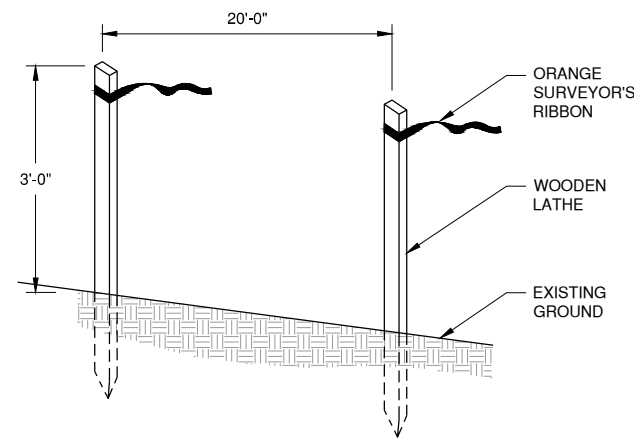


**TYPICAL TAXIWAY CLOSURE MARKER DETAIL**

NOT TO SCALE

**TAXIWAY CLOSURE MARKER NOTES**

1. THE TAXIWAY CLOSURE MARKER CAN BE PAINTED WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING, OR CAN BE ANOTHER MATERIAL THAT DOES NOT VIOLATE THE OFA CRITERIA AND IS APPROVED BY THE ENGINEER AND THE AIRPORT.
2. THE TAXIWAY CLOSURE MARKER SHALL BE YELLOW AND ADEQUATELY SECURED TO WITHSTAND JET BLAST OF 100 MPH.
3. THE MARKER SHALL BE PLACED OVER THE TAXIWAY CENTERLINE.
4. THE TAXIWAY LEAD-IN LINES AND CENTERLINE WITHIN THE RUNWAY SAFETY AREA (R.S.A.) SHALL BE REMOVED. THE REMOVAL OF THESE MARKINGS WILL BE PAID FOR PER PAY ITEM "AR620900 PAVEMENT MARKING REMOVAL."
5. THE INSTALLATION AND REMOVAL OF THE TAXIWAY CLOSURE MARKERS SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

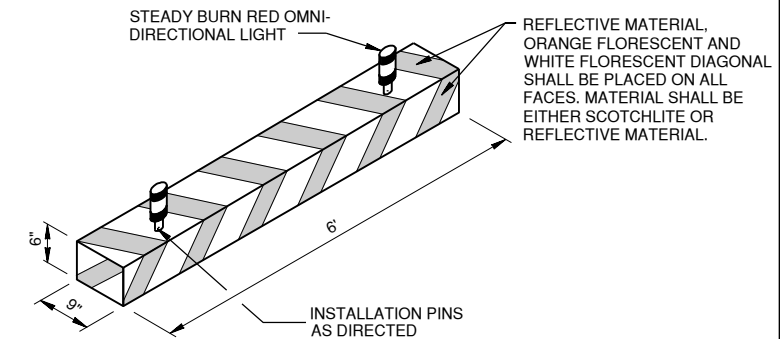


**CONSTRUCTION SETBACK LINE DETAIL**

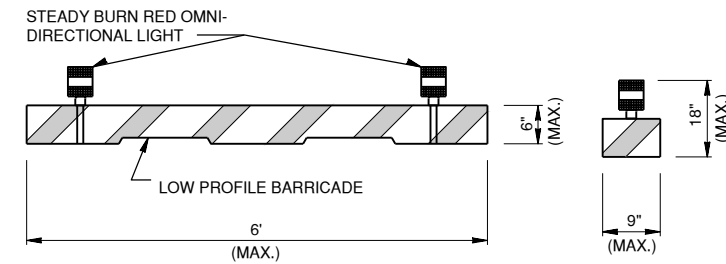
NOT TO SCALE

**CONSTRUCTION SETBACK NOTES**

1. CONTRACTOR SHALL MARKER THE RUNWAY SAFETY AREA PER THE CONSTRUCTION SETBACK DETAIL AS DIRECTED BY THE RESIDENT ENGINEER.
2. ALL COST ASSOCIATED WITH THE CONSTRUCTION SETBACK LINE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



**ISOMETRIC**



**FRONT ELEVATION**

**SIDE ELEVATION**

**INTERLOCKING LOW PROFILE BARRICADE DETAILS**

NOT TO SCALE

**INTERLOCKING LOW PROFILE BARRICADE NOTES**

1. LOW PROFILE BARRICADES SHALL BE PLACED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THE BARRICADES SHALL BE INTERLOCKED WITH NO GAPS BETWEEN BARRICADES.
2. BARRICADES SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
2. THE BARRICADE LINE SHALL EXTEND ONE BARRICADE PAST THE EDGE OF PAVEMENT INTO THE TURF.
4. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR AS LOW POSSIBLE, BUT NOT TO EXCEED 3 INCHES ABOVE THE GROUND.
6. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

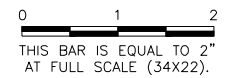
**PHASING SUMMARY**

PHASE / CALENDAR DAYS	WORK AREA	AIRPORT OPERATIONAL RESTRICTIONS
PHASE A / 25 CALENDAR DAYS	TAXIWAY CONNECTOR H2 WORK INSIDE TAXIWAY H SAFETY AREA AND OUTSIDE RUNWAY 9/27 SAFETY AREA.	<ul style="list-style-type: none"> <li>• TAXIWAY H WEST OF TAXIWAY CONNECTOR H3 CLOSED.</li> <li>• TAXIWAY CONNECTOR H1 CLOSED.</li> <li>• TAXIWAY CONNECTOR H2 CLOSED.</li> </ul>
PHASE B / 14 CALENDAR DAYS	TAXIWAY CONNECTOR H2 WORK INSIDE RUNWAY 9/27 SAFETY AREA.	<ul style="list-style-type: none"> <li>• RUNWAY 9/27 CLOSED.</li> <li>• TAXIWAY P WEST OF TAXIWAY E CLOSED.</li> <li>• TAXIWAY CONNECTOR P1 CLOSED</li> <li>• TAXIWAY H WEST OF TAXIWAY CONNECTOR H3 CLOSED.</li> <li>• TAXIWAY CONNECTOR H1 CLOSED.</li> <li>• TAXIWAY CONNECTOR H2 CLOSED.</li> </ul>
39 CALENDAR DAYS		

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**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND**  
**QUAD CITY INTERNATIONAL AIRPORT**  
**MOLINE, ILLINOIS**

**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING**  
**SEQUENCE OF CONSTRUCTION DETAILS**








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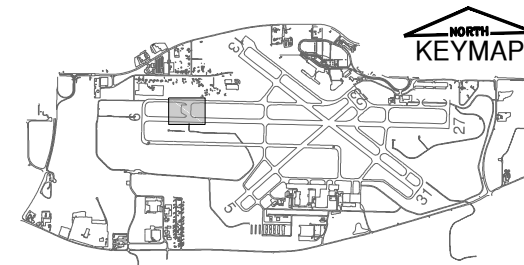
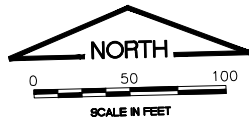


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JOB No:	14014-06-00

IL PROJ. NO. MLI-4483  
 AIP PROJ. NO. 3-17-0068-XX

**LEGEND**

-  NEW BITUMINOUS PAVEMENT
-  NEW BITUMINOUS TRANSITION
-  NEW SODDING
-  NEW GRADING AND LANDSCAPING LIMITS
-  INLET PROTECTION
-  EXISTING MANHOLE / INLET
-  EXISTING STORM SEWER



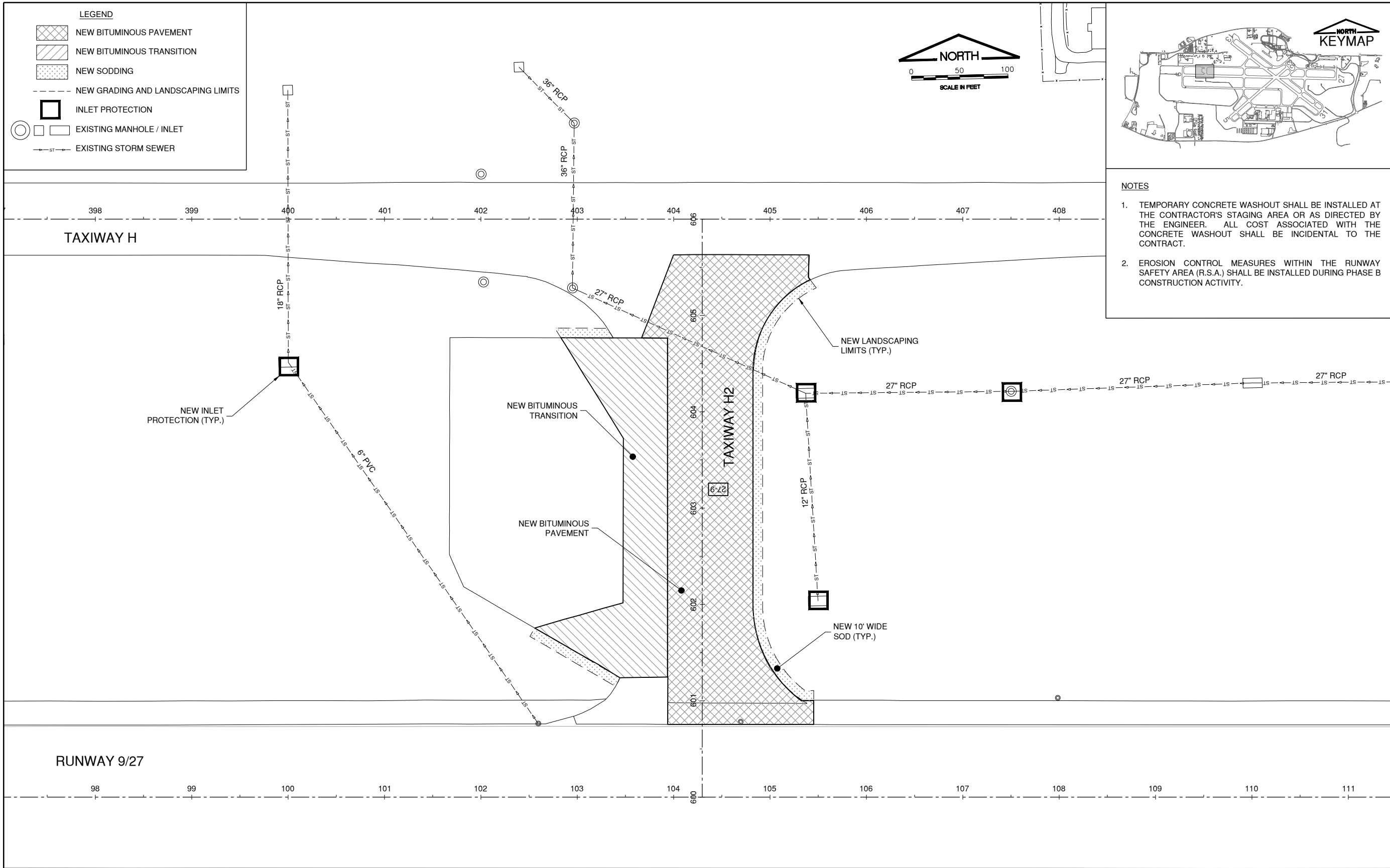
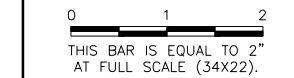
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 PLOT DATE: 6/29/2015 11:02 AM  
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 PROP-GEOMETRY

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**NOTES**

- TEMPORARY CONCRETE WASHOUT SHALL BE INSTALLED AT THE CONTRACTOR'S STAGING AREA OR AS DIRECTED BY THE ENGINEER. ALL COST ASSOCIATED WITH THE CONCRETE WASHOUT SHALL BE INCIDENTAL TO THE CONTRACT.
- EROSION CONTROL MEASURES WITHIN THE RUNWAY SAFETY AREA (R.S.A.) SHALL BE INSTALLED DURING PHASE B CONSTRUCTION ACTIVITY.




**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**

**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
 STORMWATER POLLUTION PREVENTION PLAN**

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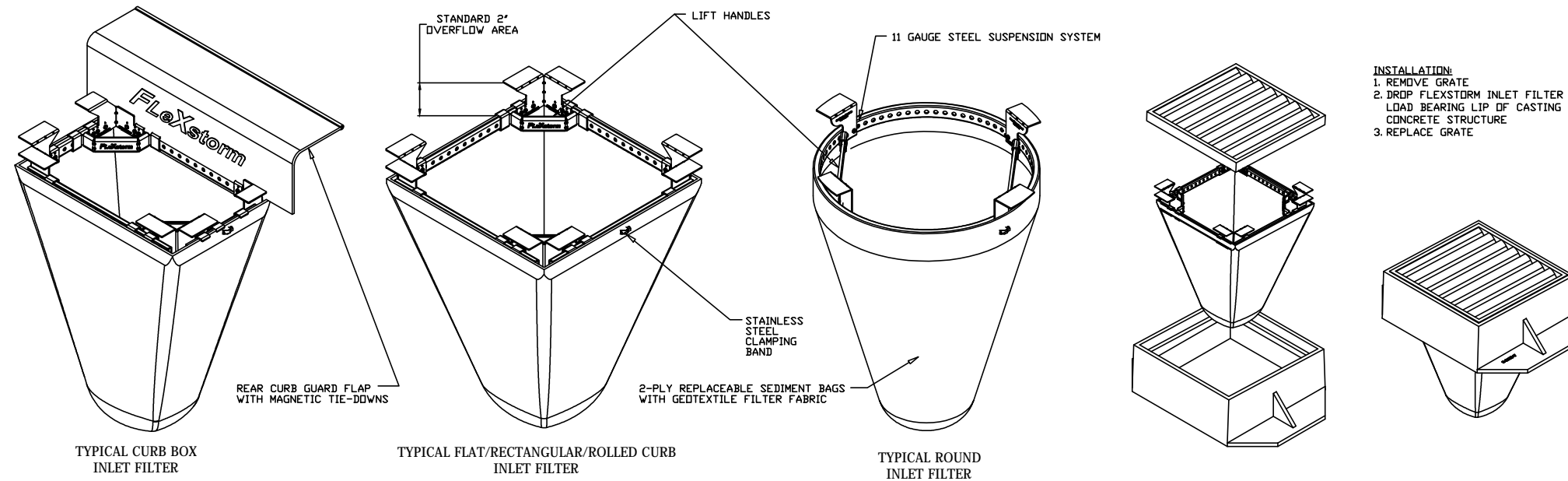
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SHEET	08 OF 22 SHEETS

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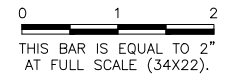
**INSTALLATION:**  
 1. REMOVE GRATE  
 2. DROP FLEXSTORM INLET FILTER INTO LOAD BEARING LIP OF CASTING OR CONCRETE STRUCTURE  
 3. REPLACE GRATE

IPP Flexstorm Inlet Filter Specifications		
Material Property	Test Method	Value (min ave)
<b>&gt; Inner Filter Bag Specs (2 ft<sup>3</sup> min vol)</b>		
Grab Tensile	ASTM D 4632	100 lbs    200 lbs
Puncture Strength	ASTM D 4833	65 lbs    90 lbs
Trapezoidal Tear	ASTM D 4533	45 lbs    75 lbs
UV Resistance	ASTM D 4355	70% at 500 hrs    90%
App Open Size (AOS)	ASTM D 4751	70 sieve (.212 mm)    40 sieve (.425 mm)
Permittivity	ASTM D 4491	2.0 /sec    2.1/sec
Water Flow Rate	ASTM D 4491	145 gpm/sqft    145 gpm/sqft
<b>&gt; Polyester Outer Reinforcement Bag Specifications</b>		
Weight	ASTM D 3776	4.55 oz/sqyd +/- 15%
Thickness	ASTM D 1777	.040 +/- .005
<b>&gt; Frame Construction</b>		
A36 Structural Steel; 11 Gauge; Zinc Plated	ASTM A 576	Tensile Strength > 58,000 psi; Yield Strength > 36,000 psi

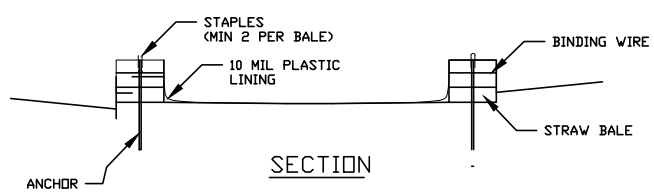
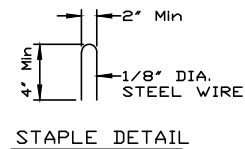
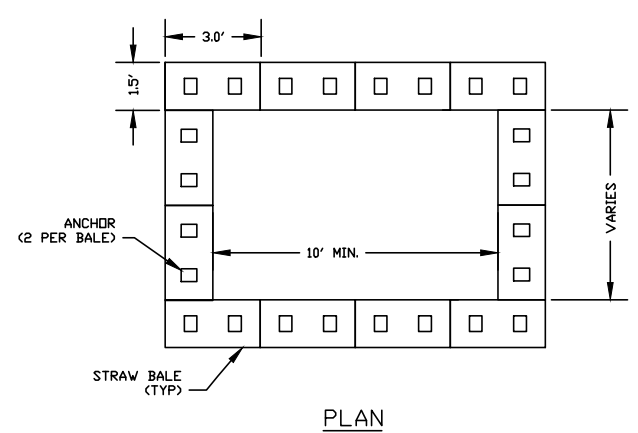
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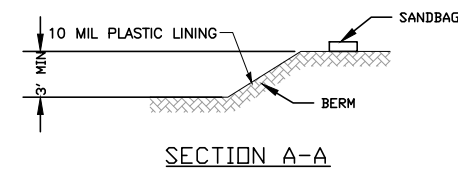
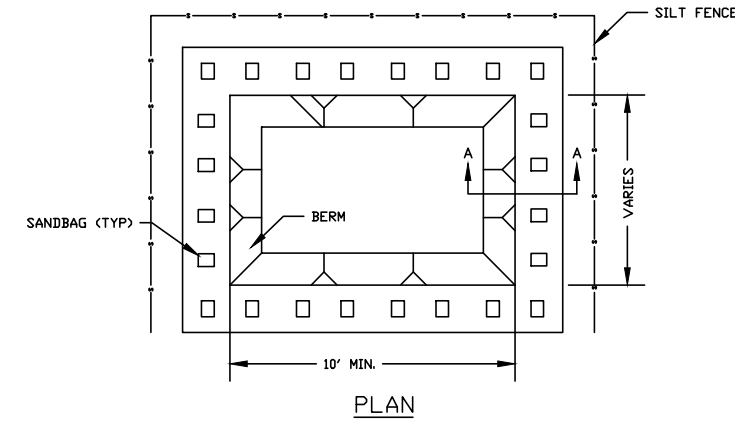


**INLET PROTECTION / SEDIMENT TRAP**  
 NOT TO SCALE  
 STORM SEWER INLET PROTECTION SHALL BE FLEXSTORM INLET FILTERS AS DETAILED HEREIN OR APPROVED EQUAL



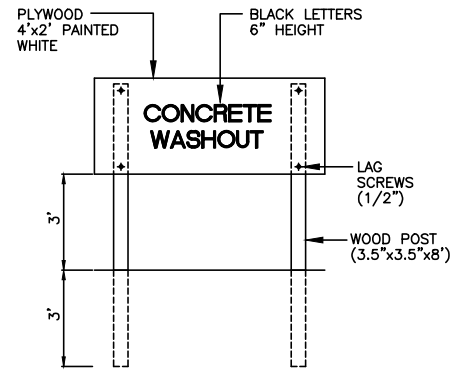
**ABOVE GROUND TEMPORARY WASHOUT**

- NOTES:**
- CONTRACTOR SHALL DETERMINE LOCATION AND SIZE OF WASHOUT.
  - WASHOUT SIZE AND LOCATION SHALL BE APPROVED BY THE ENGINEER.
  - A CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 20 FEET OF THE TEMPORARY CONCRETE WASHOUT FACILITY. AT A MINIMUM, THE SIGN SHALL READ "CONCRETE WASHOUT" IN 6" TALL LETTERS.
  - INSPECTION SHALL OCCUR ONCE PER WEEK AND DAILY DURING CONCRETE OPERATIONS. REPAIR/REPLACEMENT OF THE FACILITY SHALL BE MADE SUCH THAT CONCRETE WASTE IS CONTAINED.
  - MEDIA SHALL BE REMOVED AND DISPOSED OF AT A LEGAL OFF-SITE LOCATION WHEN THE FACILITY HAS REACHED 50% CAPACITY.
  - UPON COMPLETION OF CONCRETE OPERATIONS, THE CONCRETE WASHOUT AND ALL MATERIALS CONTAINED WITHIN SHALL BE DISPOSED OF AT A LEGAL OFF-SITE LOCATION.



**BELOW GROUND TEMPORARY WASHOUT**

- NOTES:**
- CONTRACTOR SHALL DETERMINE LOCATION AND SIZE OF WASHOUT.
  - WASHOUT SIZE AND LOCATION SHALL BE APPROVED BY THE ENGINEER.
  - SANDBAGS SHALL BE INSTALLED TO ANCHOR THE LINING. THE NUMBER OF SANDBAGS SHALL BE DETERMINED BY THE CONTRACTOR. THE CONTRACTOR SHALL ADD SANDBAGS SO AS TO MAINTAIN ANCHORING OF THE LINING.
  - A CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 20 FEET OF THE TEMPORARY CONCRETE WASHOUT FACILITY. AT A MINIMUM, THE SIGN SHALL READ "CONCRETE WASHOUT" IN 6" TALL LETTERS.
  - THE TEMPORARY WASHOUT FACILITY SHALL BE SURROUNDED BY SILT FENCE ON ALL SIDES.
  - INSPECTION SHALL OCCUR ONCE PER WEEK AND DAILY DURING CONCRETE OPERATIONS. REPAIR/REPLACEMENT OF THE FACILITY SHALL BE MADE SUCH THAT CONCRETE WASTE IS CONTAINED.
  - MEDIA SHALL BE REMOVED AND DISPOSED OF AT A LEGAL OFF-SITE LOCATION WHEN THE FACILITY HAS REACHED 50% CAPACITY.
  - UPON COMPLETION OF CONCRETE OPERATIONS, THE CONCRETE WASHOUT AND ALL MATERIALS CONTAINED WITHIN SHALL BE DISPOSED OF AT A LEGAL OFF-SITE LOCATION.



**CONCRETE WASHOUT SIGN DETAIL (OR EQUIVALENT)**

**CONCRETE WASHOUT**  
 NOT TO SCALE

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**

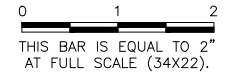
**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
 STORMWATER POLLUTION PREVENTION PLAN  
 NOTES AND DETAILS**

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IL PROJ. NO.	MLI-4483
AIP PROJ. NO.	3-17-0068-XX
SHEET	09 OF 22 SHEETS

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**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**  
**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
 TYPICAL SECTIONS SHEET 1**

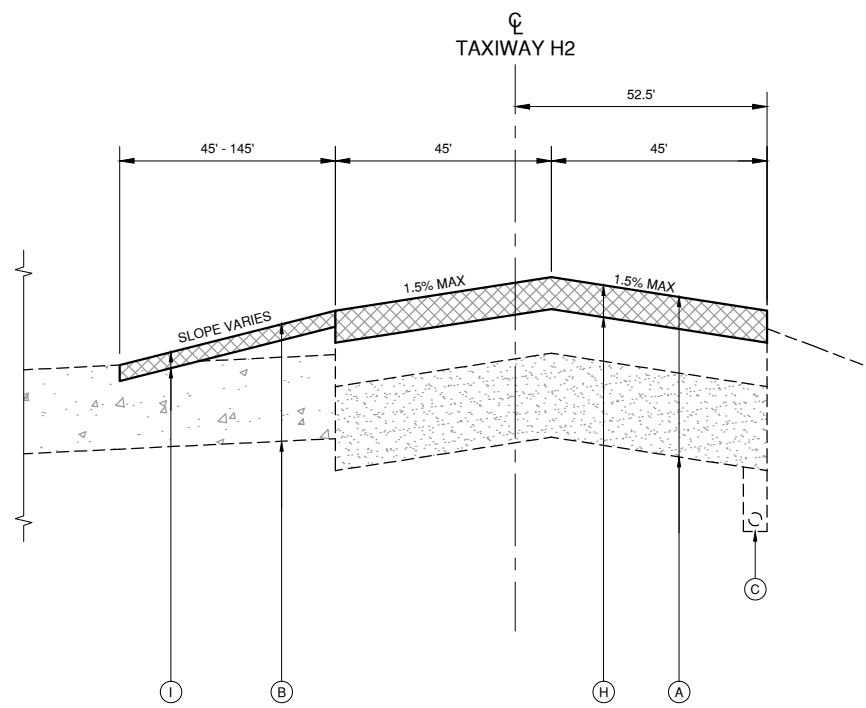
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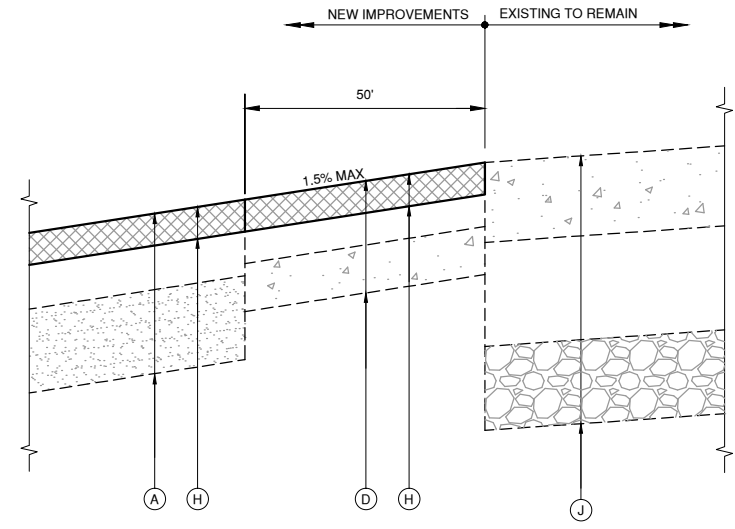
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JOB No:	14014-06-00
IL PROJ. NO. MLI-4483 AIP PROJ. NO. 3-17-0068-XX	
SHEET 10 OF 22 SHEETS	

**LEGEND**

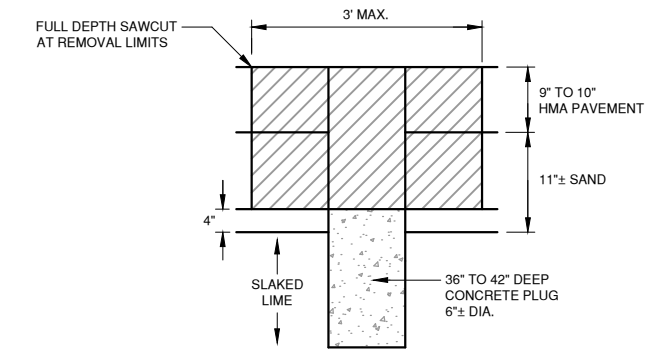
- (A) EXISTING 9.5" BITUMINOUS PAVEMENT  
EXISTING 10.5" SAND SUBBASE COURSE
- (B) EXISTING 5.5" BITUMINOUS PAVEMENT  
EXISTING 10.5" PCC PAVEMENT
- (C) EXISTING UNDERDRAIN
- (D) EXISTING 5" - 8" BITUMINOUS OVERLAY  
EXISTING 7" PCC PAVEMENT
- (E) EXISTING 17" PCC PAVEMENT
- (F) EXISTING 13" - 17" PCC PAVEMENT  
EXISTING 6" - 10" CRUSHED AGGREGATE BASE COURSE
- (G) EXISTING 14" PCC  
EXISTING 5" BITUMINOUS PAVEMENT  
EXISTING 8" CRUSHED AGGREGATE BASE COURSE
- (H) NEW 5" AND VARIABLE BITUMINOUS PAVEMENT MILLING (AR401650)
- (I) NEW 1" - 3" BITUMINOUS PAVEMENT MILLING PAID AS BUTT JOINT CONSTRUCTION (AR401655)
- (J) EXISTING 10" PCC  
EXISTING 13" BITUMINOUS PAVEMENT  
EXISTING 10" CRUSHED AGGREGATE BASE COURSE



**TYPICAL SECTION A-A (REMOVAL LIMITS)  
 TAXIWAY H2**  
 NOT TO SCALE



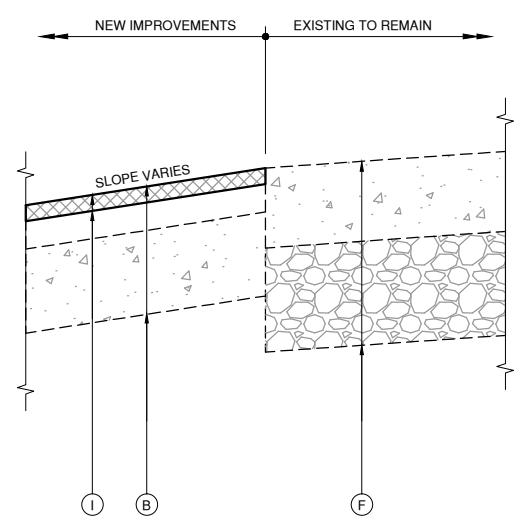
**TYPICAL SECTION B-B (REMOVAL LIMITS)  
 TAXIWAY H AND TAXIWAY H2 INTERFACE**  
 NOT TO SCALE



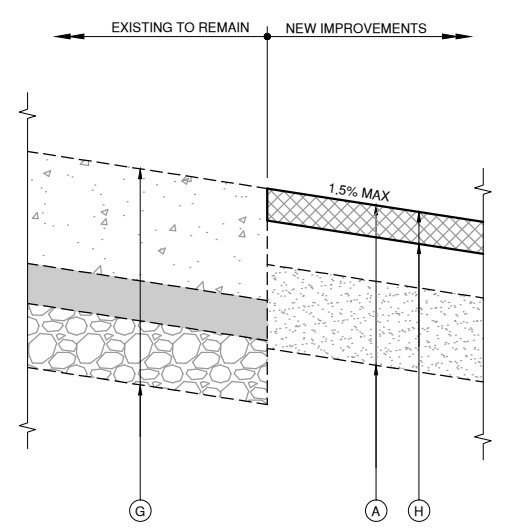
**CONCRETE PLUG REMOVAL**  
 NOT TO SCALE

**NOTES**

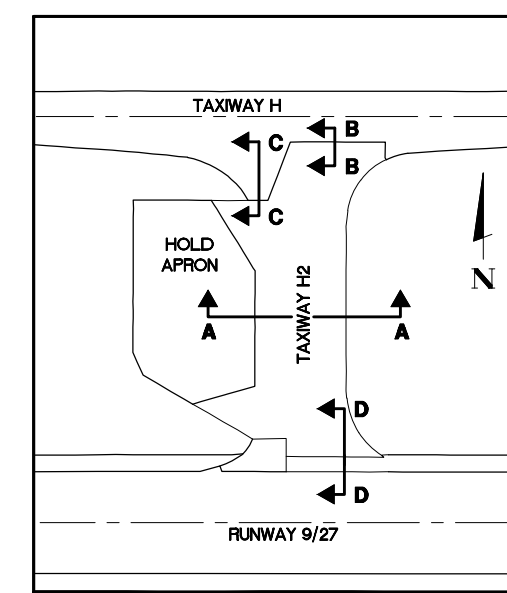
- REMOVE HMA PAVEMENT, SAND SUBBASE AND CONCRETE PLUG IN LIMITS SHOWN.
- REPLACE SAND SUBBASE WITH NEW CA-6 CRUSHED AGGREGATE BASE COURSE MEETING THE REQUIREMENTS OF AR209.
- REPLACE HMA PAVEMENT WITH BITUMINOUS SURFACE COURSE. THE THICKNESS OF THE FINAL LIFT OF SURFACE COURSE SHALL BE 3" AND BE PLACED DURING MAINLINE PAVING OPERATIONS.
- PAVEMENT REMOVAL AND REPLACEMENT SHALL BE PAID UNDER AR401910, REMOVE AND REPLACE BITUMINOUS PAVEMENT.
- SEE SOIL BORING AND PAVEMENT CORE LOCATIONS SHEET FOR LOCATIONS OF CONCRETE PLUGS TO BE REMOVED.



**TYPICAL SECTION C-C (REMOVAL LIMITS)  
 TAXIWAY H AND TAXIWAY H2 INTERFACE**  
 NOT TO SCALE



**TYPICAL SECTION D-D REMOVAL LIMITS  
 RUNWAY 9/27 AND TAXIWAY H2 INTERFACE**  
 NOT TO SCALE

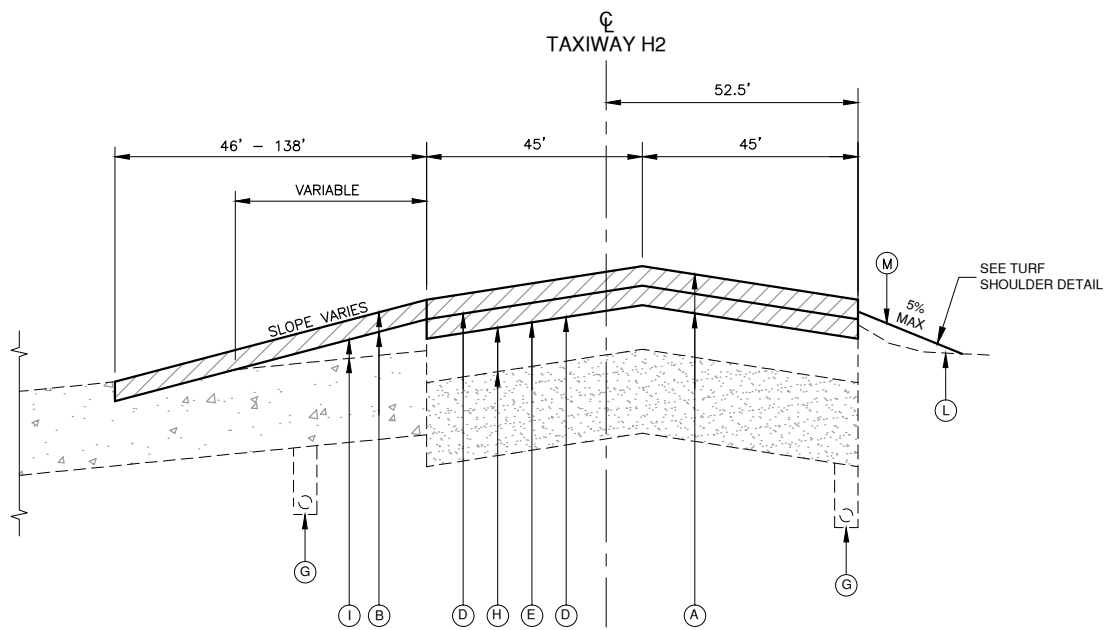
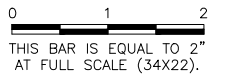


**KEY MAP**

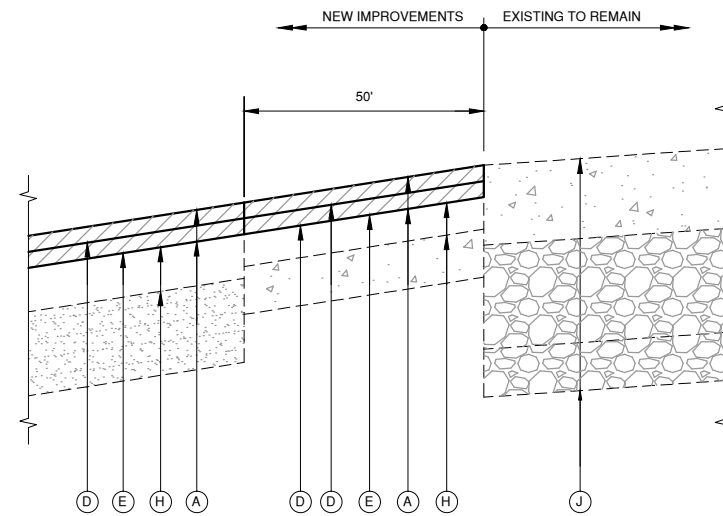
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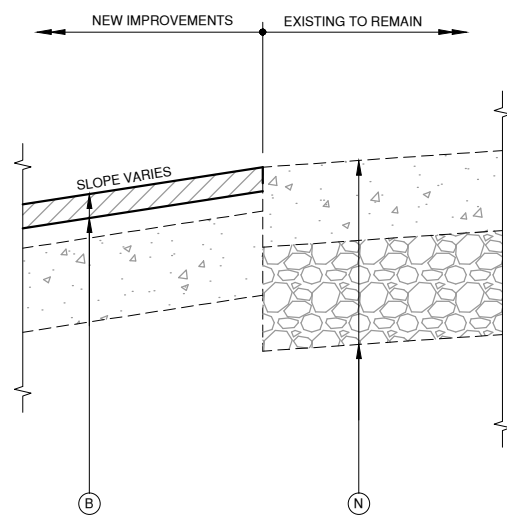


**TYPICAL SECTION A-A (NEW IMPROVEMENTS)  
 TAXIWAY H2**  
 NOT TO SCALE

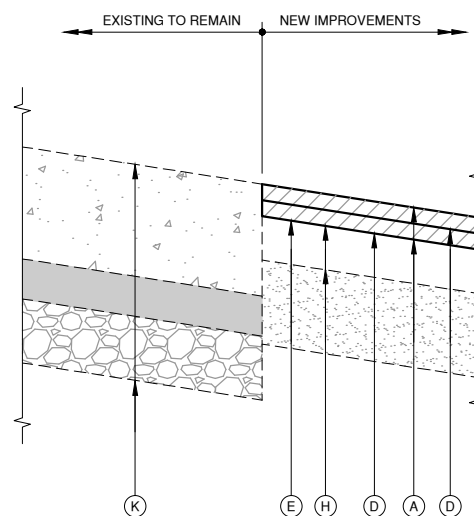


**TYPICAL SECTION B-B (NEW IMPROVEMENTS)  
 TAXIWAY H AND TAXIWAY H2 INTERFACE**  
 NOT TO SCALE

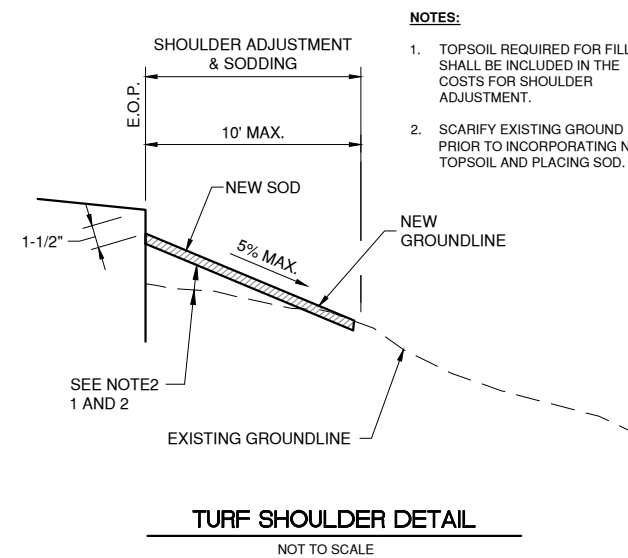
- LEGEND**
- (A) NEW 6" BITUMINOUS SURFACE COURSE (AR401), PLACE IN 2-3" LIFTS WITH TACK COAT BETWEEN LIFTS
  - (B) NEW 3"± BITUMINOUS SURFACE COURSE (AR401)
  - (C) NOT USED
  - (D) NEW BITUMINOUS TACK COAT
  - (E) NEW CRACK CONTROL FABRIC (AR201670)
  - (F) NEW SHOULDER FILL (152)
  - (G) EXISTING UNDERDRAIN
  - (H) EXISTING BITUMINOUS PAVEMENT TO REMAIN AFTER 5" AND VARIABLE BITUMINOUS PAVEMENT MILLING (AR401650)
  - (I) EXISTING BITUMINOUS PAVEMENT TO REMAIN AFTER 1" - 3" BITUMINOUS PAVEMENT MILLING (AR401655)
  - (J) EXISTING 10" PCC PAVEMENT  
EXISTING 13" BITUMINOUS PAVEMENT  
EXISTING 10" CRUSHED AGGREGATE BASE COURSE
  - (K) EXISTING 14" PCC  
EXISTING 5" BITUMINOUS PAVEMENT  
EXISTING 8" CRUSHED AGGREGATE BASE COURSE
  - (L) EXISTING GROUND
  - (M) NEW 10" WIDE SODDING (AR904510) AND SHOULDER ADJUSTMENT (AR152480)
  - (N) EXISTING 13" - 17" PCC PAVEMENT  
EXISTING 6" - 10" CRUSHED AGGREGATE BASE COURSE



**TYPICAL SECTION C-C (NEW IMPROVEMENTS)  
 TAXIWAY H AND TAXIWAY H2 INTERFACE**  
 NOT TO SCALE

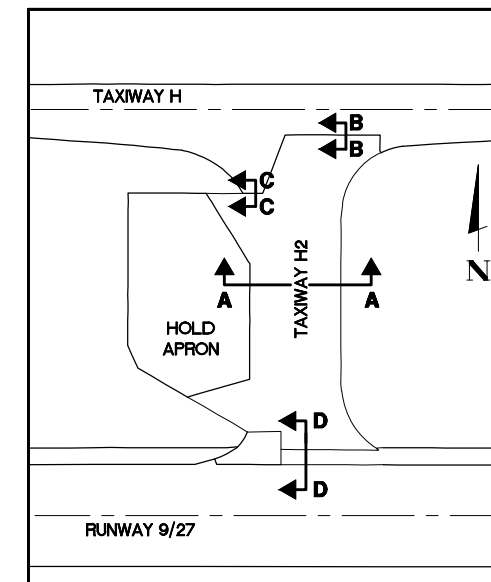


**TYPICAL SECTION D-D (NEW IMPROVEMENTS)  
 RUNWAY 9/27 AND TAXIWAY H2 INTERFACE**  
 NOT TO SCALE



**NOTES:**

1. TOPSOIL REQUIRED FOR FILL SHALL BE INCLUDED IN THE COSTS FOR SHOULDER ADJUSTMENT.
2. SCARIFY EXISTING GROUND PRIOR TO INCORPORATING NEW TOPSOIL AND PLACING SOD.



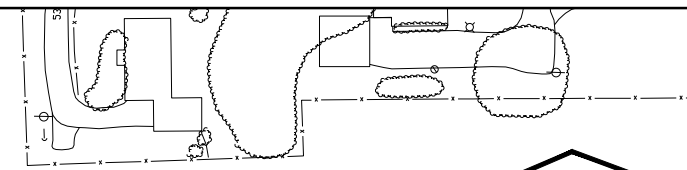
**KEY MAP**

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**  
**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING**  
**TYPICAL SECTIONS SHEET 2**

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SHEET	11 OF 22 SHEETS

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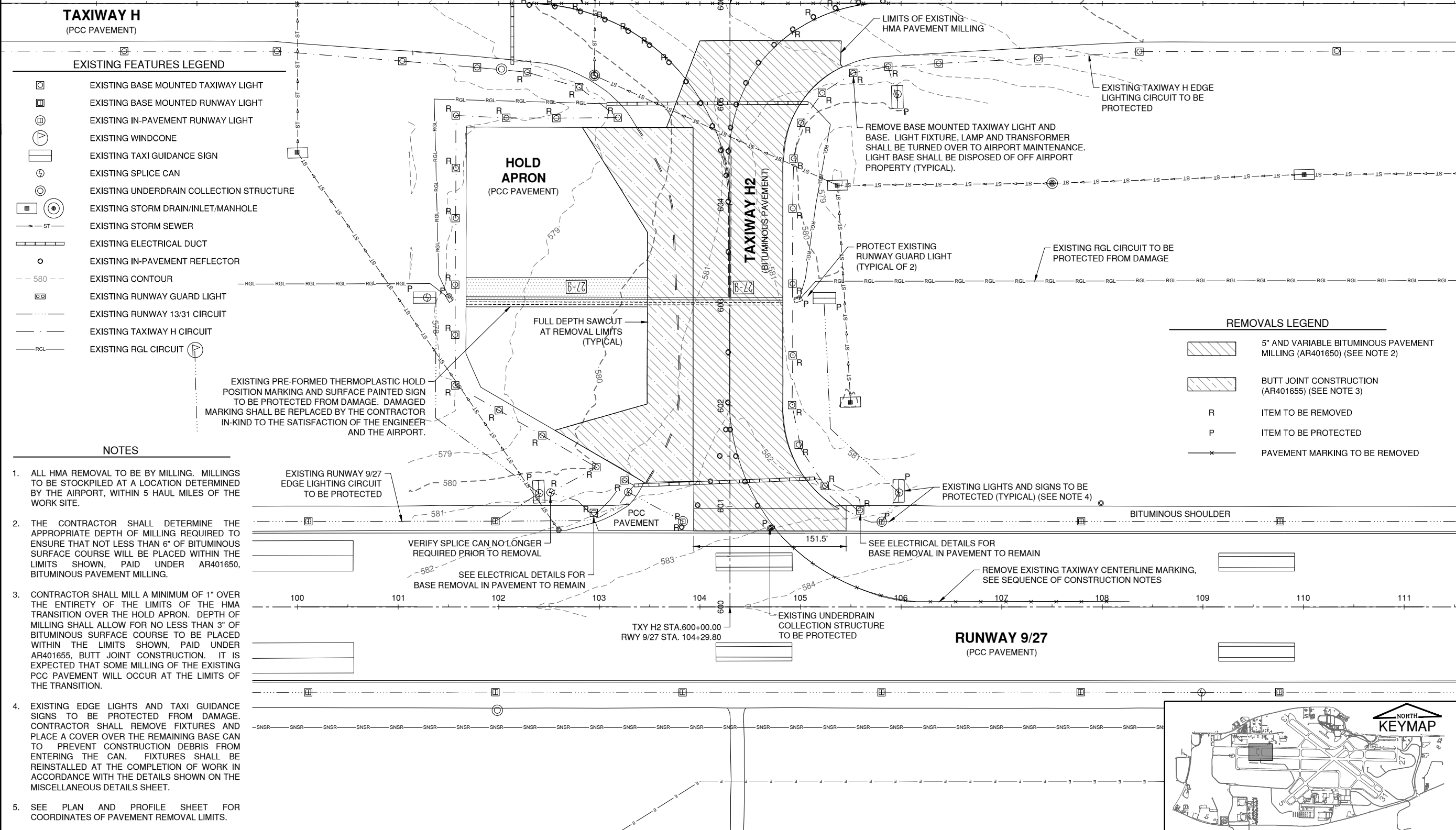
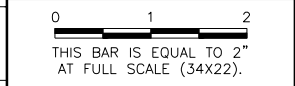


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 PLOT DATE: 6/29/2015 11:04 AM

Taxiway H1 Rehab Base  
 PROP - GEOMETRY  
 MLI Alignments  
 CONTOURS

**QU019**

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**EXISTING FEATURES LEGEND**

- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING IN-PAVEMENT RUNWAY LIGHT
- EXISTING WINDCONE
- EXISTING TAXI GUIDANCE SIGN
- EXISTING SPLICE CAN
- EXISTING UNDERDRAIN COLLECTION STRUCTURE
- EXISTING STORM DRAIN/INLET/MANHOLE
- EXISTING STORM SEWER
- EXISTING ELECTRICAL DUCT
- EXISTING IN-PAVEMENT REFLECTOR
- EXISTING CONTOUR
- EXISTING RUNWAY GUARD LIGHT
- EXISTING RUNWAY 13/31 CIRCUIT
- EXISTING TAXIWAY H CIRCUIT
- EXISTING RGL CIRCUIT

**NOTES**

1. ALL HMA REMOVAL TO BE BY MILLING. MILLINGS TO BE STOCKPILED AT A LOCATION DETERMINED BY THE AIRPORT, WITHIN 5 HAUL MILES OF THE WORK SITE.
2. THE CONTRACTOR SHALL DETERMINE THE APPROPRIATE DEPTH OF MILLING REQUIRED TO ENSURE THAT NOT LESS THAN 6" OF BITUMINOUS SURFACE COURSE WILL BE PLACED WITHIN THE LIMITS SHOWN, PAID UNDER AR401650, BITUMINOUS PAVEMENT MILLING.
3. CONTRACTOR SHALL MILL A MINIMUM OF 1" OVER THE ENTIRETY OF THE LIMITS OF THE HMA TRANSITION OVER THE HOLD APRON. DEPTH OF MILLING SHALL ALLOW FOR NO LESS THAN 3" OF BITUMINOUS SURFACE COURSE TO BE PLACED WITHIN THE LIMITS SHOWN, PAID UNDER AR401655, BUTT JOINT CONSTRUCTION. IT IS EXPECTED THAT SOME MILLING OF THE EXISTING PCC PAVEMENT WILL OCCUR AT THE LIMITS OF THE TRANSITION.
4. EXISTING EDGE LIGHTS AND TAXI GUIDANCE SIGNS TO BE PROTECTED FROM DAMAGE. CONTRACTOR SHALL REMOVE FIXTURES AND PLACE A COVER OVER THE REMAINING BASE CAN TO PREVENT CONSTRUCTION DEBRIS FROM ENTERING THE CAN. FIXTURES SHALL BE REINSTALLED AT THE COMPLETION OF WORK IN ACCORDANCE WITH THE DETAILS SHOWN ON THE MISCELLANEOUS DETAILS SHEET.
5. SEE PLAN AND PROFILE SHEET FOR COORDINATES OF PAVEMENT REMOVAL LIMITS.

**REMOVALS LEGEND**

- 5" AND VARIABLE BITUMINOUS PAVEMENT MILLING (AR401650) (SEE NOTE 2)
- BUTT JOINT CONSTRUCTION (AR401655) (SEE NOTE 3)
- R** ITEM TO BE REMOVED
- P** ITEM TO BE PROTECTED
- x** PAVEMENT MARKING TO BE REMOVED

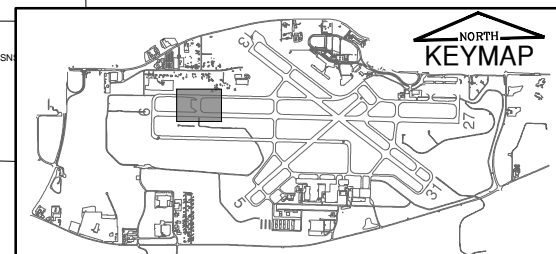
**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**

**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
 EXISTING CONDITIONS AND PROPOSED  
 REMOVALS**

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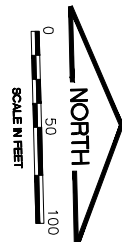
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SHEET 12 OF 22 SHEETS	



K:\Moline\14014-06\_Taxiway H2\Drawn Sheets

K:\Moineap\14014-06\_Taxiway\Drawn\Sheets

SERVICE ROAD



RUNWAY 9/27  
(PCC PAVEMENT)

TXY H2 STA. 600+00.00  
RWY 9/27 STA. 104+29.80

HOLD  
APRON  
(PCC PAVEMENT)

LIMITS OF BITUMINOUS  
TRANSITION, SEE NOTE 1

TAXIWAY H2  
(BITUMINOUS PAVEMENT)

TAXIWAY H  
(PCC PAVEMENT)

TXY H2 STA. 606+00.00  
TXY H STA. 404+29.80

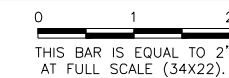
GEOMETRIC CONTROL POINTS

POINT	NORTHING	EASTING	REMARKS
1	1,742,066.17	2,199,296.02	MATCH EXISTING PCC
2	1,742,115.29	2,199,297.22	MATCH EXISTING PCC
3	1,742,115.62	2,199,247.84	MATCH EXISTING PCC
4	1,742,169.48	2,199,160.58	END OF TRANSITION
5	1,742,193.42	2,199,252.95	END OF TRANSITION
6	1,742,364.18	2,199,257.36	END OF TRANSITION
7	1,742,470.03	2,199,195.25	END OF TRANSITION
8	1,742,467.80	2,199,278.86	MATCH EXISTING PCC
9	1,742,467.01	2,199,305.91	EDGE OF OVERLAY
10	1,742,553.08	2,199,314.19	MATCH EXISTING PCC
11	1,742,549.72	2,199,454.66	MATCH EXISTING PCC
12	1,742,523.87	2,199,455.34	MATCH EXISTING PCC
13	1,742,087.12	2,199,448.07	MATCH EXISTING HMA
14	1,742,062.30	2,199,447.47	MATCH EXISTING HMA
15	1,742,185.23	2,199,512.51	RADIUS POINT, R=125'
16	1,742,428.15	2,199,498.51	RADIUS POINT, R=105'

QU019

REVISIONS

NUMBER	BY	DATE

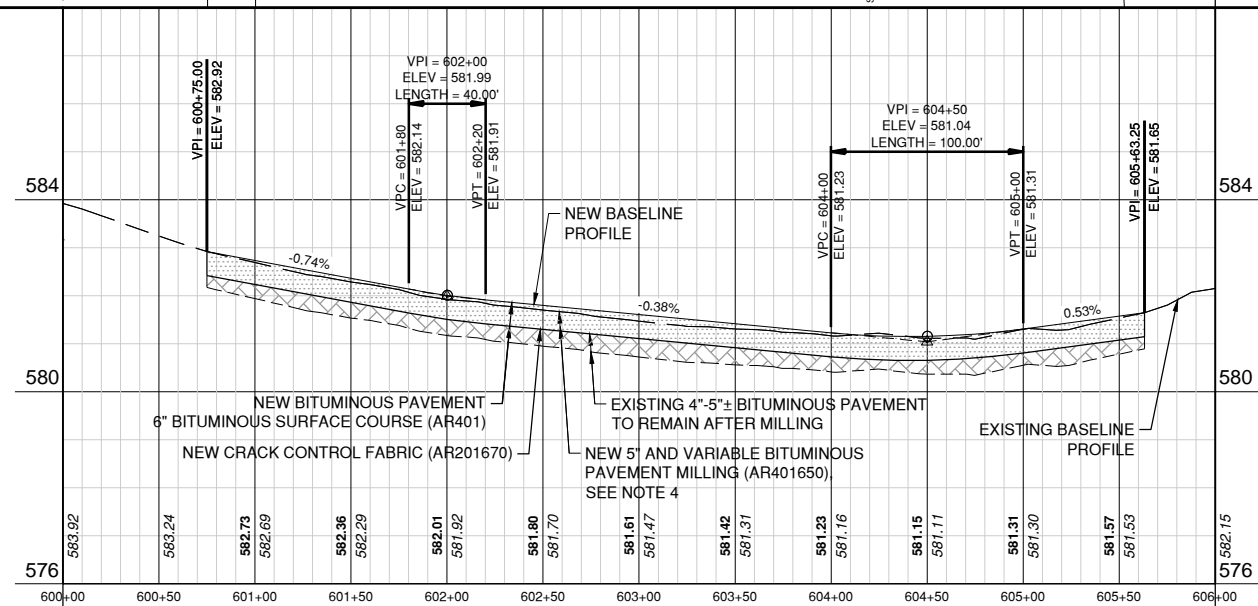


METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
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MOLINE, ILLINOIS

REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
PLAN AND PROFILE

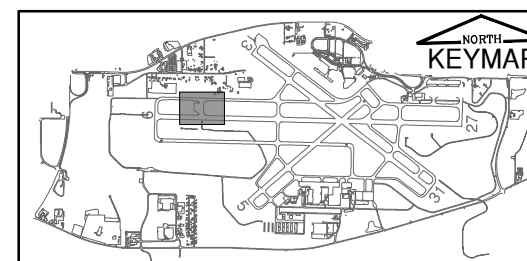
NOTES

1. SAW AND SEAL INTERFACE BETWEEN NEW BITUMINOUS PAVEMENT AND EXISTING PCC PAVEMENT. SEE DETAIL ON MISCELLANEOUS DETAILS SHEET.
2. SAW AND SEAL INTERFACE BETWEEN NEW BITUMINOUS PAVEMENT AND EXISTING BITUMINOUS PAVEMENT. SEE DETAIL ON MISCELLANEOUS DETAILS SHEET.
3. PROTECT EXISTING COLLECTION STRUCTURE. CONTRACTOR TO MILL AS CLOSE TO COLLECTION STRUCTURE AS POSSIBLE WITHOUT DAMAGING THE STRUCTURE.
4. CONTRACTOR TO DETERMINE DEPTH OF MILLING IN ORDER TO PLACE 6" BITUMINOUS SURFACE COURSE. MILLING DEPTH WILL BE VARIABLE, WITH AN AVERAGE DEPTH OF 5 INCHES.
5. PRIOR TO THE PLACEMENT OF THE CRACK CONTROL FABRIC, THE PAVEMENT SHALL BE SWEEPED CLEAN AND THE CONTRACTOR AND THE RESIDENT ENGINEER SHALL EVALUATE THE PAVEMENT SURFACE TO IDENTIFY CRACKS REQUIRING REPAIR. CRACKS IN EXCESS OF 1" MINIMUM WIDTH SHALL BE FILLED WITH SAND MIX (AR201663). CRACKS OF SMALLER WIDTH SHALL BE CLEANED AND SEALED (AR201661).



LEGEND

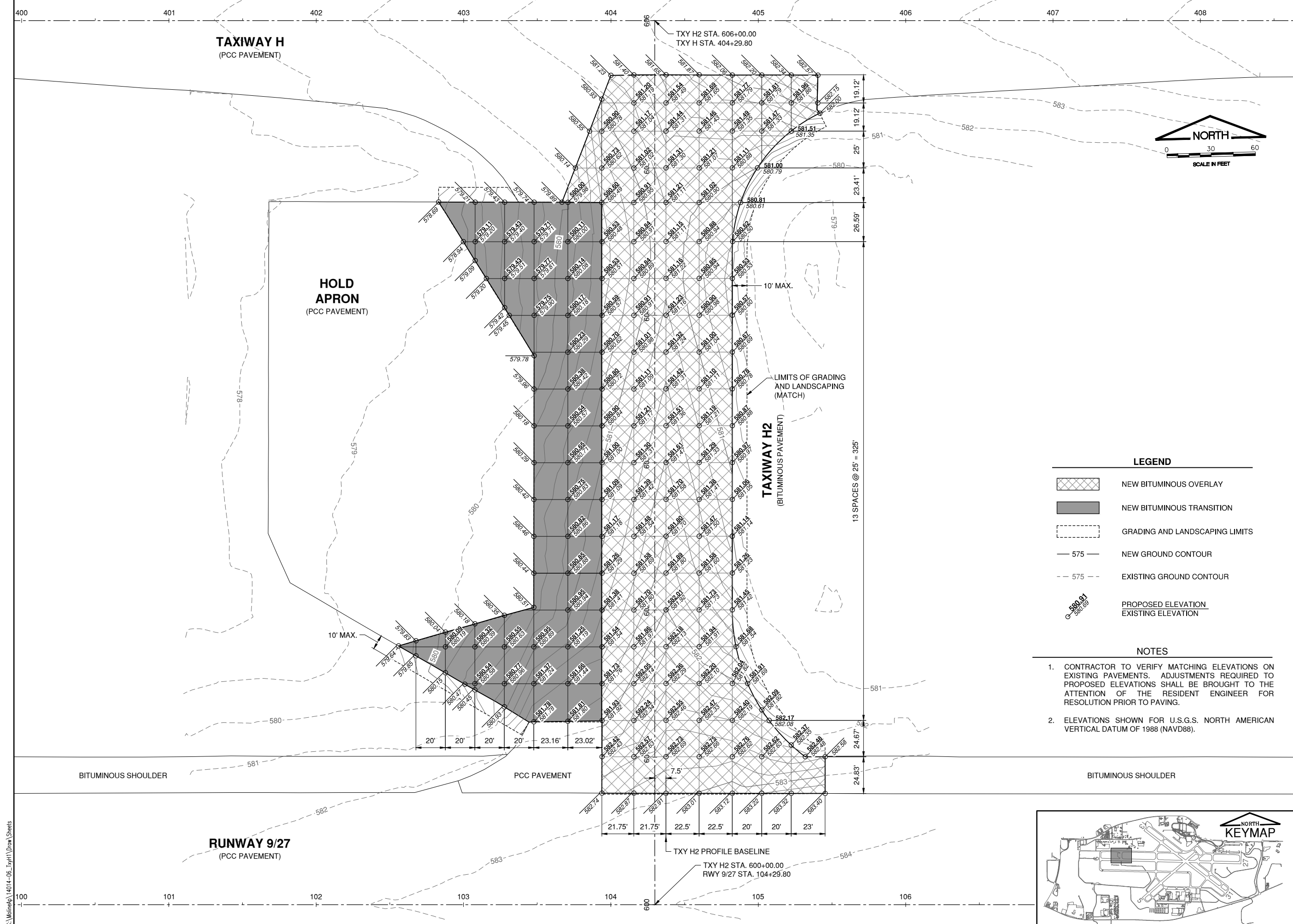
- [Cross-hatched pattern] NEW BITUMINOUS OVERLAY  
6" BITUMINOUS SURFACE COURSE (AR401)  
CRACK CONTROL FABRIC (AR201670)
- [Diagonal hatched pattern] NEW BITUMINOUS TRANSITION  
3"± BITUMINOUS SURFACE COURSE (AR401)



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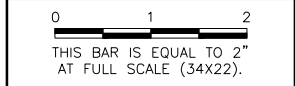


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SHEET	13 OF 22 SHEETS



**QU019**

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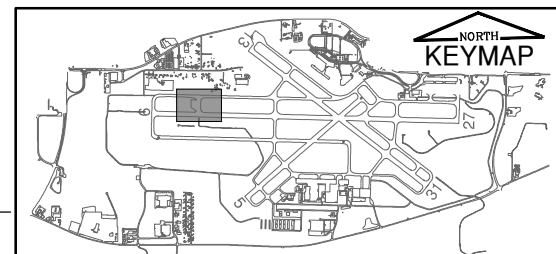
**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**

**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
 PAVEMENT ELEVATION PLAN**

**LEGEND**

	NEW BITUMINOUS OVERLAY
	NEW BITUMINOUS TRANSITION
	GRADING AND LANDSCAPING LIMITS
	NEW GROUND CONTOUR
	EXISTING GROUND CONTOUR
	PROPOSED ELEVATION EXISTING ELEVATION

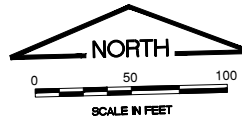
- NOTES**
- CONTRACTOR TO VERIFY MATCHING ELEVATIONS ON EXISTING PAVEMENTS. ADJUSTMENTS REQUIRED TO PROPOSED ELEVATIONS SHALL BE BROUGHT TO THE ATTENTION OF THE RESIDENT ENGINEER FOR RESOLUTION PRIOR TO PAVING.
  - ELEVATIONS SHOWN FOR U.S.G.S. NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).



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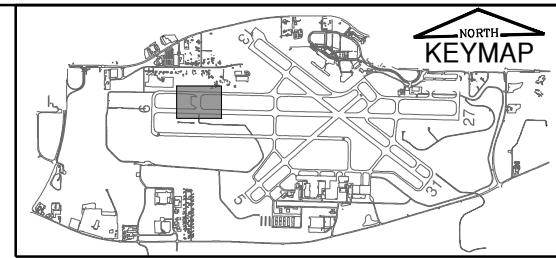
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SHEET 14 OF 22 SHEETS	



MARKING CONTROL POINTS		
POINT	STATION	OFFSET
1	TXY H2 602+06.00	178.50' LT.
2	TXY H2 603+53.89	267.61' LT.
3	TXY H2 604+00.00	200' LT.
4	TXY H2 605+05.00	95' LT
5	TXY H2 605+05.00	95' RT
6	TXY H2 601+00.00	95' RT

**LEGEND**

- PROPOSED PAVEMENT MARKING
- EXISTING PAVEMENT MARKING
- PROPOSED BASE MOUNTED TAXIWAY LIGHT
- - - PROPOSED TAXIWAY H EDGE LIGHTING CIRCUIT



FILE: 15 - MARKING PLAN.dwg  
 UPDATE BY: Sean Smith  
 PLOT DATE: 6/29/2015 11:06 AM

Taxiway H1 Rehab Base  
 PROP - GEOMETRY

**QU019**

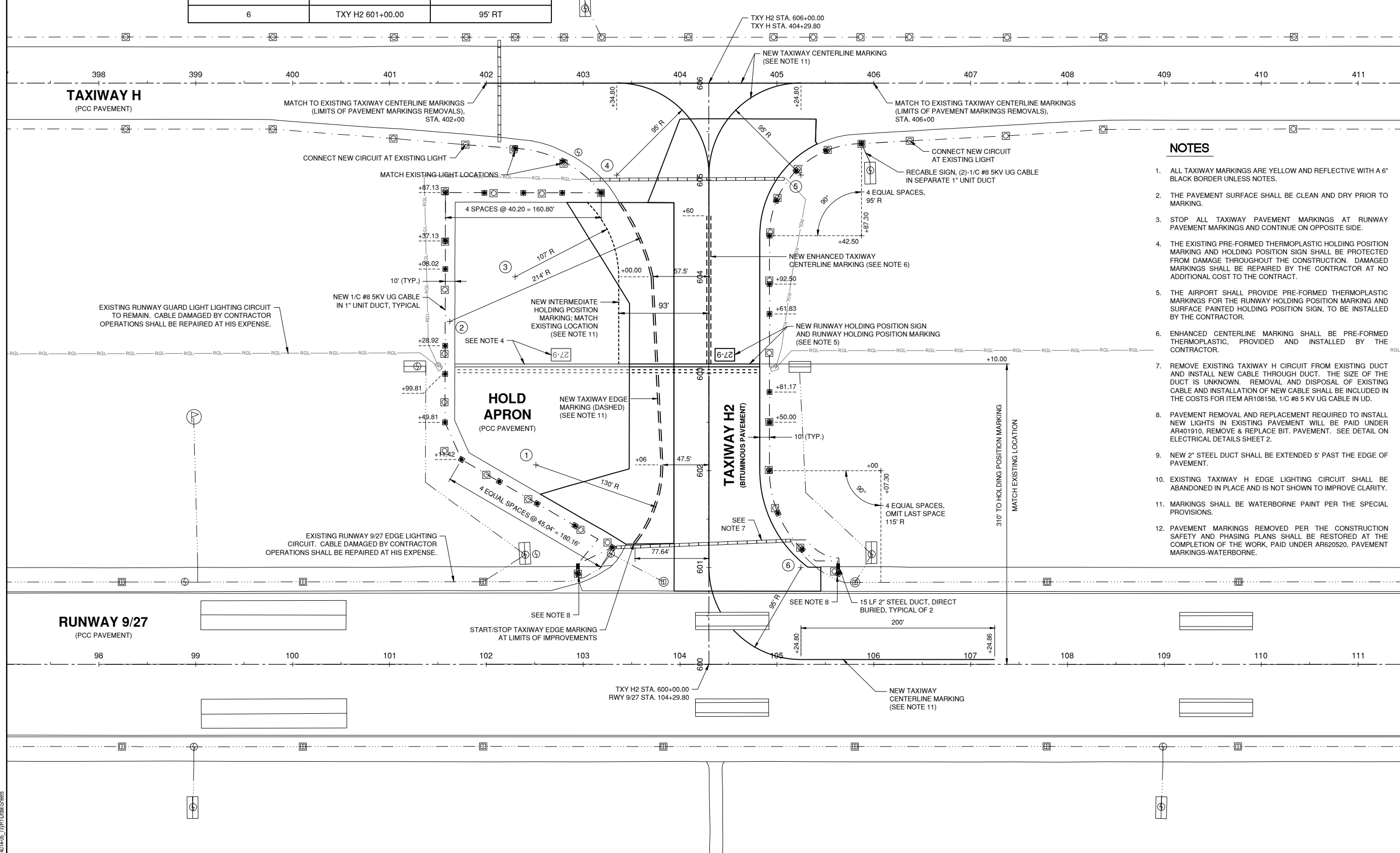
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0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

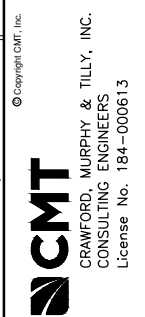
**NOTES**

- ALL TAXIWAY MARKINGS ARE YELLOW AND REFLECTIVE WITH A 6" BLACK BORDER UNLESS NOTES.
- THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
- STOP ALL TAXIWAY PAVEMENT MARKINGS AT RUNWAY PAVEMENT MARKINGS AND CONTINUE ON OPPOSITE SIDE.
- THE EXISTING PRE-FORMED THERMOPLASTIC HOLDING POSITION MARKING AND HOLDING POSITION SIGN SHALL BE PROTECTED FROM DAMAGE THROUGHOUT THE CONSTRUCTION. DAMAGED MARKINGS SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.
- THE AIRPORT SHALL PROVIDE PRE-FORMED THERMOPLASTIC MARKINGS FOR THE RUNWAY HOLDING POSITION MARKING AND SURFACE PAINTED HOLDING POSITION SIGN, TO BE INSTALLED BY THE CONTRACTOR.
- ENHANCED CENTERLINE MARKING SHALL BE PRE-FORMED THERMOPLASTIC, PROVIDED AND INSTALLED BY THE CONTRACTOR.
- REMOVE EXISTING TAXIWAY H CIRCUIT FROM EXISTING DUCT AND INSTALL NEW CABLE THROUGH DUCT. THE SIZE OF THE DUCT IS UNKNOWN. REMOVAL AND DISPOSAL OF EXISTING CABLE AND INSTALLATION OF NEW CABLE SHALL BE INCLUDED IN THE COSTS FOR ITEM AR108158, 1/C #8 5 KV UG CABLE IN UD.
- PAVEMENT REMOVAL AND REPLACEMENT REQUIRED TO INSTALL NEW LIGHTS IN EXISTING PAVEMENT WILL BE PAID UNDER AR401910, REMOVE & REPLACE BIT. PAVEMENT. SEE DETAIL ON ELECTRICAL DETAILS SHEET 2.
- NEW 2" STEEL DUCT SHALL BE EXTENDED 5' PAST THE EDGE OF PAVEMENT.
- EXISTING TAXIWAY H EDGE LIGHTING CIRCUIT SHALL BE ABANDONED IN PLACE AND IS NOT SHOWN TO IMPROVE CLARITY.
- MARKINGS SHALL BE WATERBORNE PAINT PER THE SPECIAL PROVISIONS.
- PAVEMENT MARKINGS REMOVED PER THE CONSTRUCTION SAFETY AND PHASING PLANS SHALL BE RESTORED AT THE COMPLETION OF THE WORK. PAID UNDER AR620520, PAVEMENT MARKINGS-WATERBORNE.



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
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**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
 ELECTRICAL AND PAVEMENT MARKING PLAN**

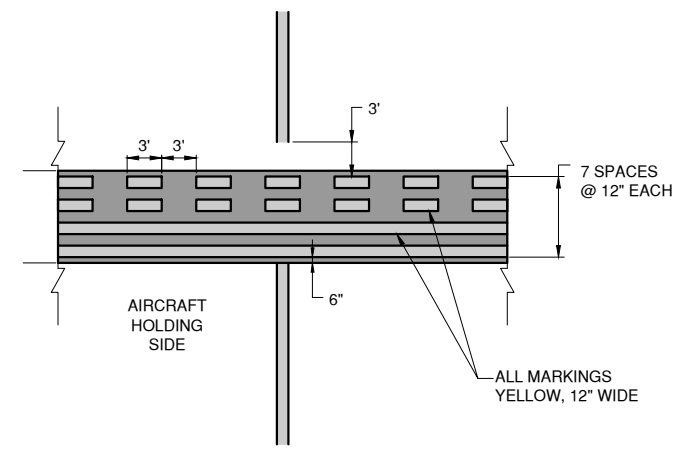
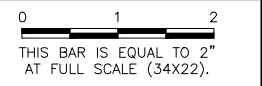


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SHEET	15 OF 22 SHEETS

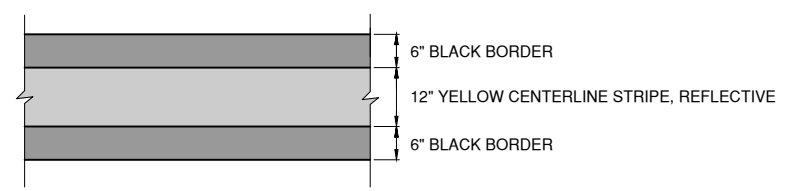
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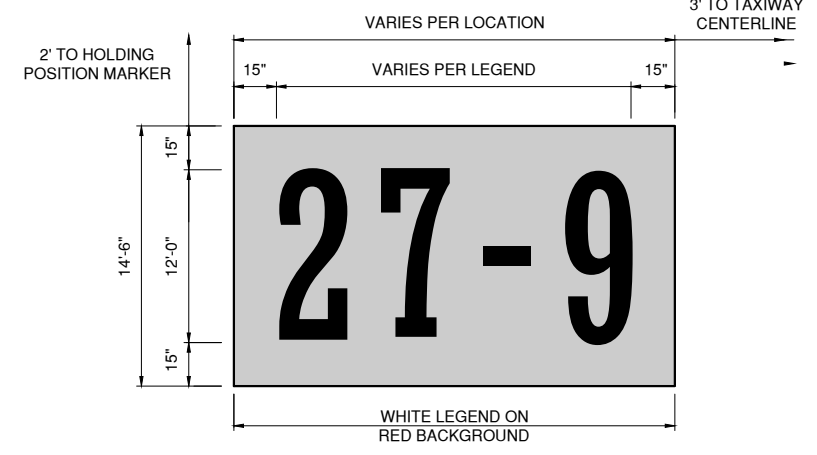
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**RUNWAY HOLDING POSITION MARKING  
 (PRE-FORMED THERMOPLASTIC - OWNER SUPPLIED)**  
 NOT TO SCALE

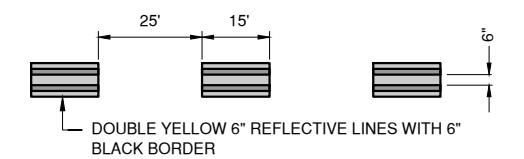


**TAXIWAY CENTERLINE DETAIL**  
 NOT TO SCALE



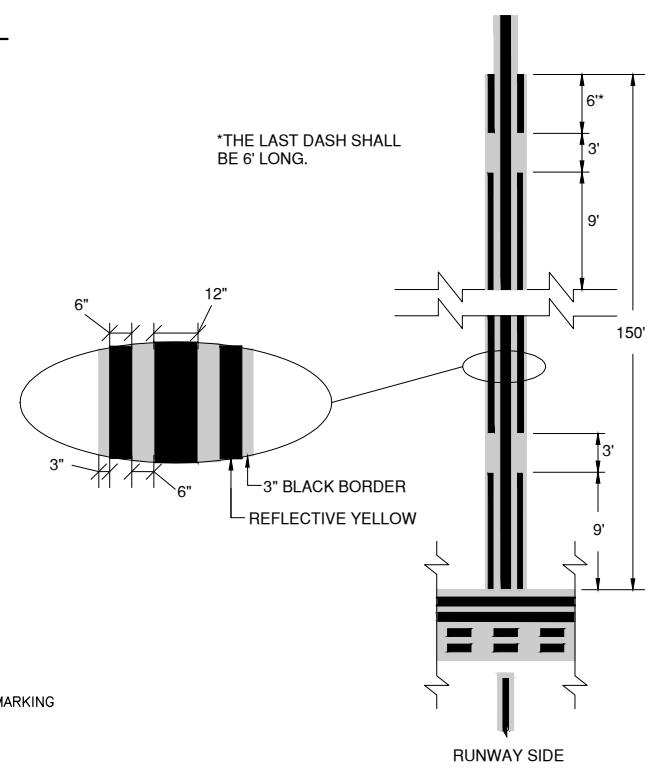
**SURFACE PAINTED HOLDING POSITION SIGN  
 (PRE-FORMED THERMOPLASTIC - OWNER SUPPLIED)**  
 NOT TO SCALE

1. ALL SURFACE PAINTED HOLDING POSITION SIGNS SHALL HAVE A RED REFLECTIVE BACKGROUND WITH A WHITE REFLECTIVE INSCRIPTION.
2. ALL SURFACE PAINTED SIGNS SHALL BE OUTLINED WITH A 6" BLACK BORDER.
3. ALL SURFACE PAINTED SIGNS SHALL BE OUTLINED WITH A 6" BLACK BORDER.
4. LEGENDS SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE AIRPORT MANAGER.
5. ALL LETTERS, NUMBERS AND SYMBOLS SHALL CONFORM TO FAA ADVISORY CIRCULAR 150/5340-1 (LATEST EDITION).

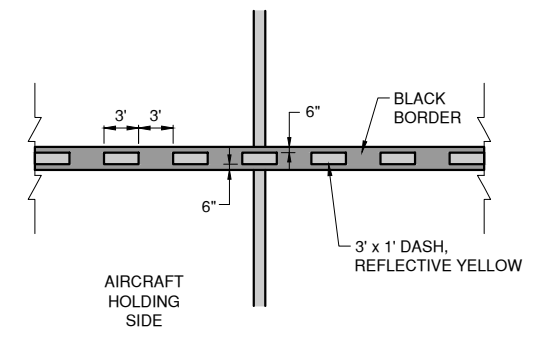


**TAXIWAY EDGE MARKING (DASHED) DETAIL**  
 NOT TO SCALE

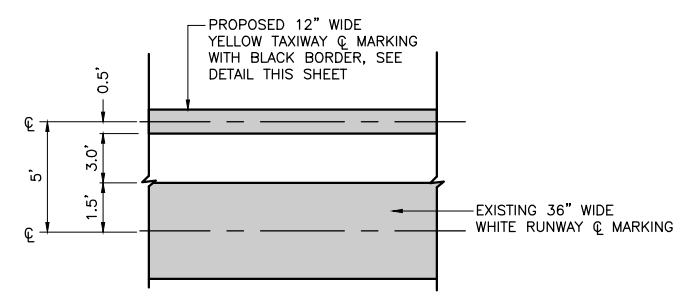
**BLACK BORDER SHALL NOT REQUIRE REFLECTIVE MEDIA**



**ENHANCED TAXIWAY CENTERLINE MARKING**  
 NOT TO SCALE



**INTERMEDIATE HOLDING POSITION MARKING**  
 NOT TO SCALE



**TAXIWAY TANGENT DETAIL**  
 NOT TO SCALE

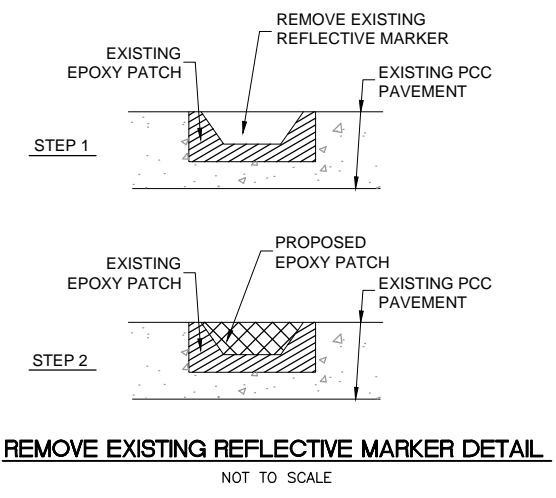
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**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
 MISCELLANEOUS DETAILS**

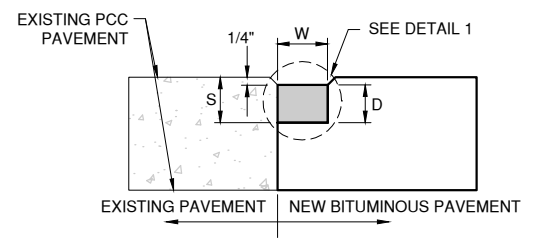
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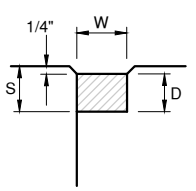
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SHEET	16 OF 22 SHEETS



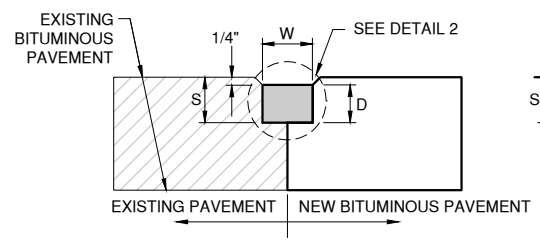
**REMOVE EXISTING REFLECTIVE MARKER DETAIL**  
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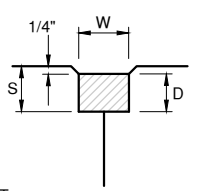
**CONCRETE / BITUMINOUS INTERFACE JOINT SEALING**  
 NOT TO SCALE



**DETAIL 1**



**BITUMINOUS / BITUMINOUS INTERFACE JOINT SEALING**  
 NOT TO SCALE



**DETAIL 2**

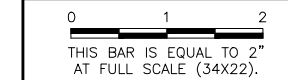
	DETAIL 1 AND 2	
	HOT/COLD POUR	SILICONE
W=WIDTH OF SEALANT RESERVOIR (IN.)	1/2	3/8
D=DEPTH OF SEALANT RESERVOIR (IN.)	1/2	1/4
B=BACKER ROD DIAMETER (IN.)	N/A	N/A
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	3/4	1/2

NOTE: JOINT SEALING SHALL BE PER SECTION 605 OF SPECIAL PROVISIONS.



**QU019**

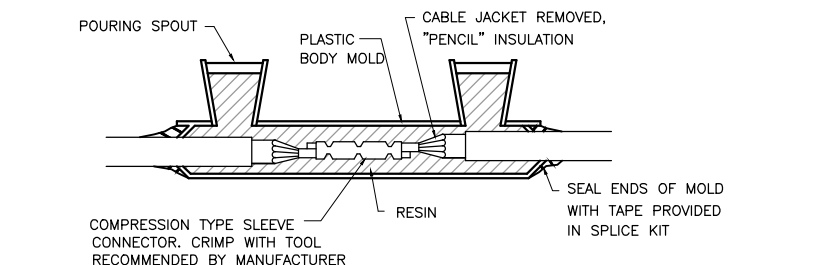
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**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
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 MOLINE, ILLINOIS**  
  
**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
 ELECTRICAL DETAILS SHEET 1**

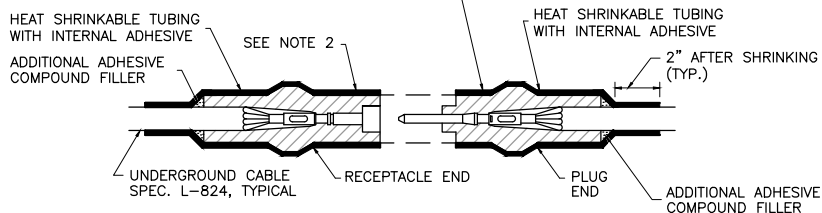
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SHEET 17 OF 22 SHEETS	



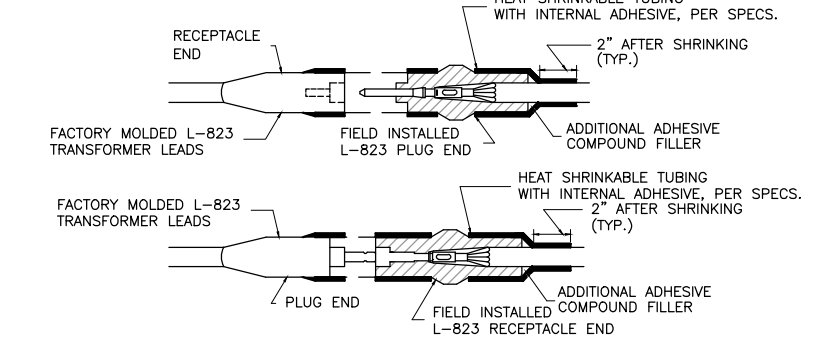
**TYPE A - CABLE SPLICE**

FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY  
 N.T.S.



**TYPE B - CABLE SPLICE**

FOR SPLICES FOR USE AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT  
 N.T.S.



**TYPE C AND D - CABLE SPLICE**

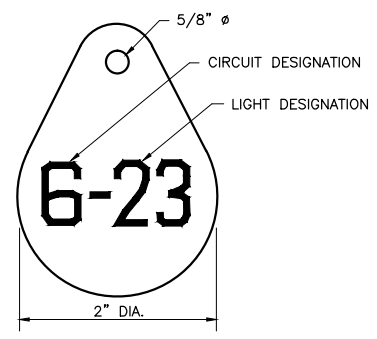
FOR SPLICES AT RUNWAY/TAXIWAY LIGHTS AND SIGNS  
 N.T.S.

**NOTES**

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.
- THE CONTRACTOR MAY ELECT TO USE AN FAA APPROVED PRIMARY CONNECTOR KIT IN LIEU OF HEAT SHRINK KIT AT NO ADDITIONAL COST.

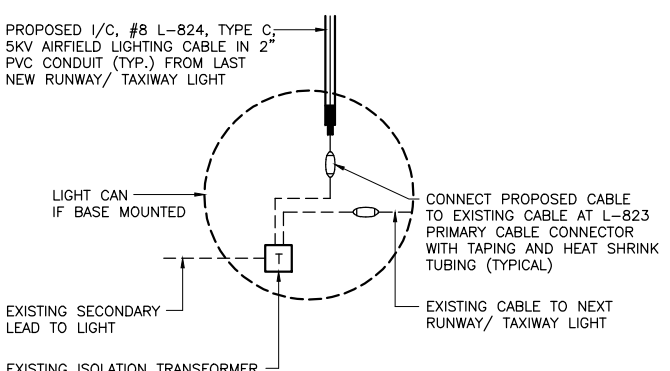
**NOTES:**

- INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH A SET SCREW.
- NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND PROPOSED TAXIWAY AND RUNWAY LIGHTS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (NEW OR RELOCATED LIGHTS) SHALL BE RETAGGED.
- COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



**LIGHT IDENTIFICATION DETAIL**

NOT TO SCALE

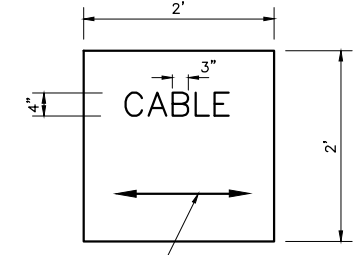
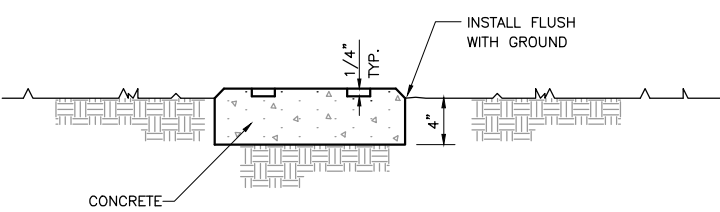


**RUNWAY/TAXIWAY LIGHTING CIRCUIT CONNECTION DETAIL**

NOT TO SCALE

**GENERAL NOTES:**

- THE CONCRETE BASE FOR BASE MTD. LIGHTS AND SIGNS SHALL BE TROWEL FINISHED WITH A 45° BEVELED EDGE. SLOPE TO DRAIN (610).
- TRANSFORMER HOLDER SHALL BE ANY COMMERCIALY AVAILABLE BRICK.
- BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
- ISOLATION TRANSFORMERS COME WITH A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/C, No. 8, 5000 V., L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
- TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
- ALL SIGNS, LIGHTS, CABLES AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT DIRECTOR, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE.
- CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.
- ALL RUNWAY/TAXIWAY EDGE LIGHTS SHALL HAVE 2" DIA. COLUMN AND FRANGIBLE COUPLINGS, UNLESS NOTED OTHERWISE.
- INSTALL SAFETY GROUND TO EXISTING SIGNS AND L-867 CANS. ATTACH GROUND LUG TO EXISTING CAN AND INSTALL GROUND ROD AS SHOWN ON PLANS.

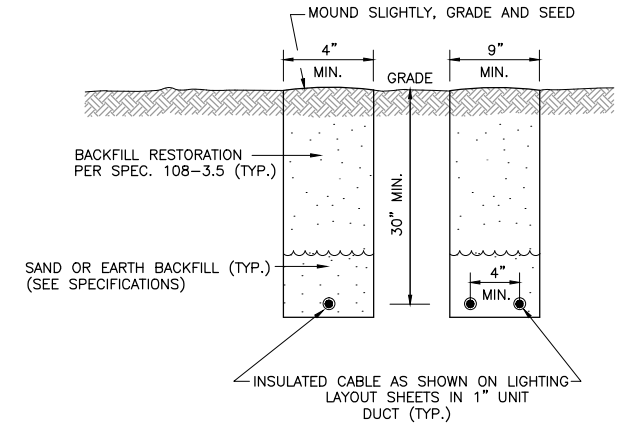


**TURF CABLE MARKER DETAIL**

NOT TO SCALE

**NOTES:**

- CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE CABLE RUN.
- ITEM 610 CONCRETE SHALL BE USED.
- ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
- THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- 0.049 CU. YD. CONCRETE PER MARKER.

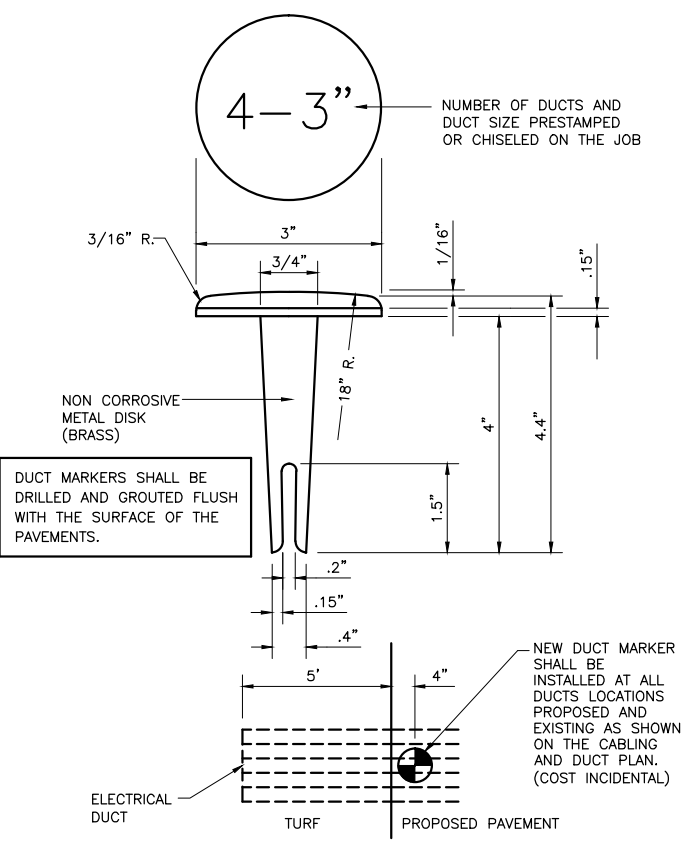


**TURF AREA CABLE TRENCH DETAIL**

NOT TO SCALE

**NOTES**

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION, COST INCLUDED IN THE CABLE.



**DUCT MARKER DETAIL**

NOT TO SCALE

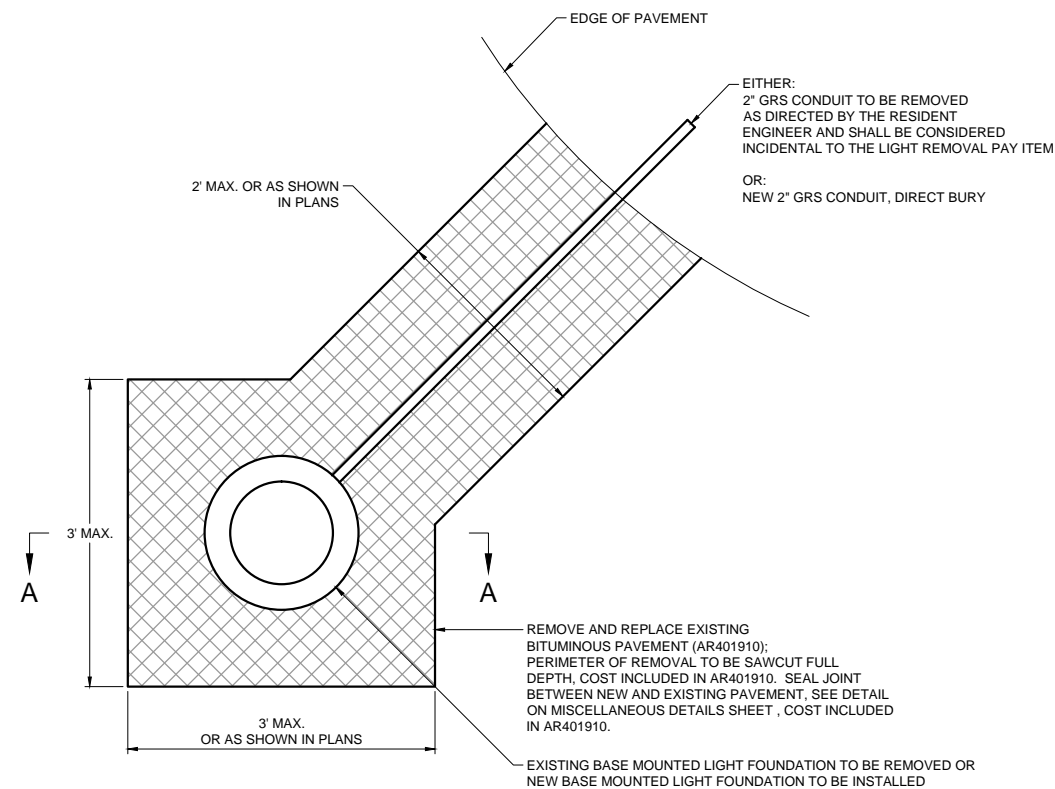
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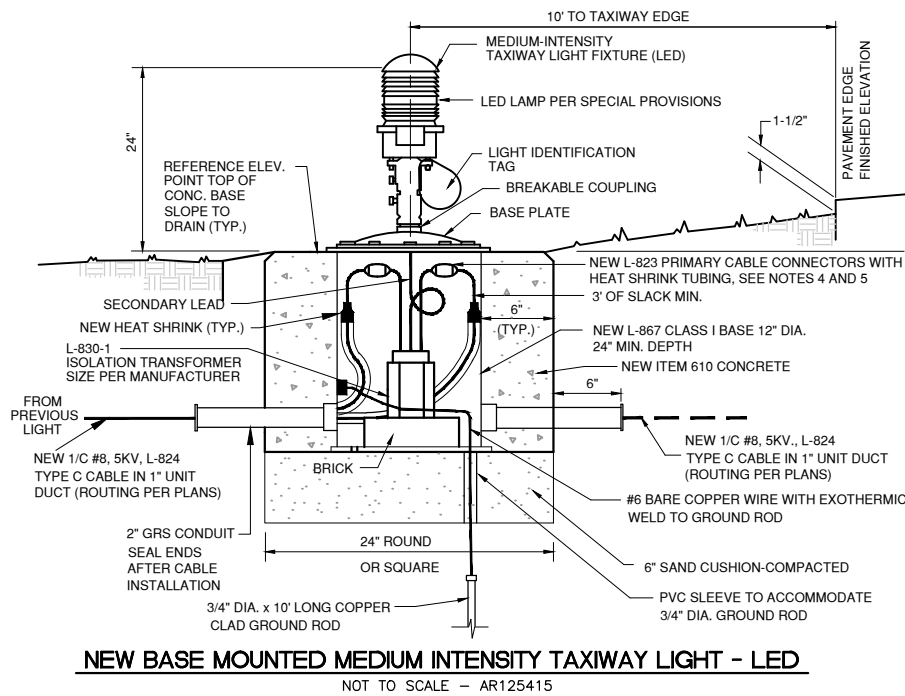
0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

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 QUAD CITY INTERNATIONAL AIRPORT  
 MOLINE, ILLINOIS**

**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
 ELECTRICAL DETAILS SHEET 2**

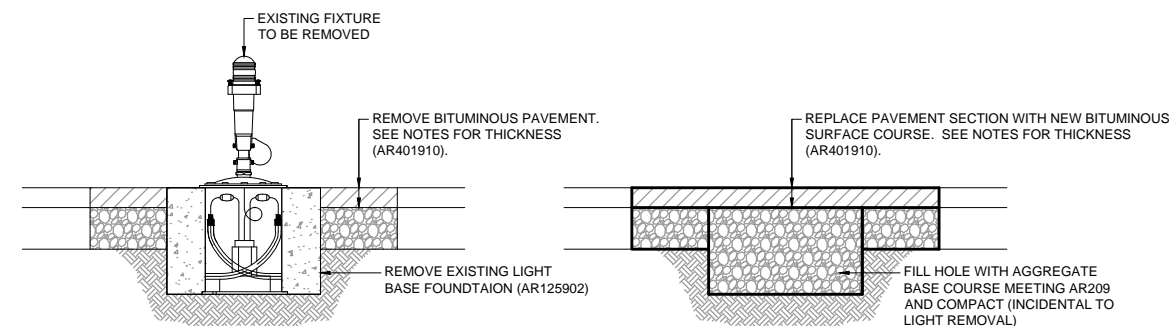


PLAN VIEW



**NEW BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT - LED**

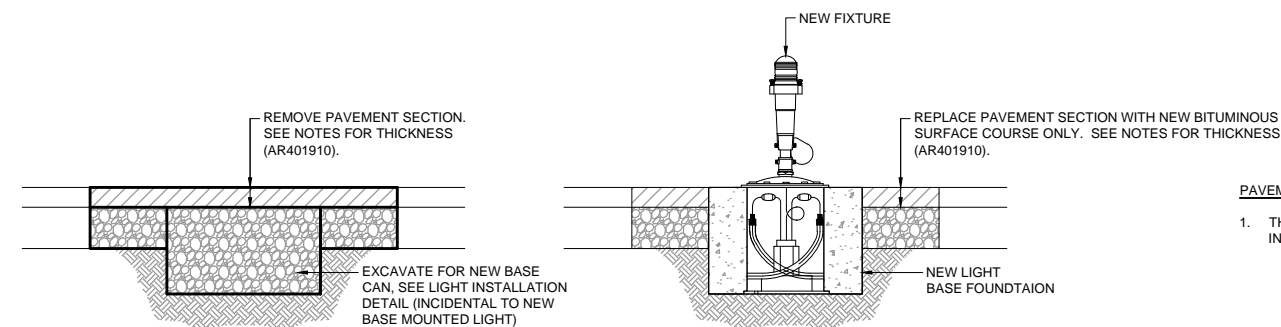
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STEP 1

STEP 2

REMOVAL SECTION A-A



STEP 1

STEP 2

INSTALLATION SECTION A-A

**LIGHT BASE REMOVAL OR INSTALLATION IN BITUMINOUS PAVEMENT TO REMAIN**

NOT TO SCALE

**GENERAL NOTES**

1. TRANSFORMER HOLDER MAY BE ANY COMMERCIALY AVAILABLE BRICK.
2. BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
3. ISOLATION TRANSFORMERS COME WITH A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9), A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/C, No. 8, 5000 V., L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
4. TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
5. AT THE CONTRACTOR'S OPTION, IN LIEU OF TAPE AND HEAT SHRINKABLE TUBING, A SELF-SEALING STYLE CONNECTOR KIT MAY BE USED, SUCH AS AMERACE 54SUPER SERIES, INTEGRO L-823 "COMPLETE KITS" OR FAA APPROVED EQUAL.
6. LAMP FOR FIXTURES SHALL BE AS SPECIFIED IN THE SPECIAL PROVISIONS. ISOLATION TRANSFORMERS SHALL BE SIZED PER THE FIXTURE MANUFACTURER, 6.6 AMP.
7. ALL LIGHTS, CABLES AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE.
8. DUCT MARKERS SHALL BE INSTALLED AT EVERY NEW DUCT AND AT EVERY EXISTING DUCT USED FOR THIS PROJECT.

**PAVEMENT REMOVAL AND REPLACEMENT (AR401910) NOTES:**

1. THICKNESS OF BITUMINOUS PAVEMENT TO BE REMOVED AND REPLACED WAS OBTAINED FROM RECORD DRAWING INFORMATION PROVIDED BY OTHERS:  
 3" BITUMINOUS SURFACE COURSE  
 3" BITUMINOUS BASE COURSE (REPLACE WITH BITUMINOUS SURFACE COURSE)  
 17"-25" CRUSHED AGGREGATE BASE COURSE
2. PAVEMENT REMOVED IN EXCESS OF THE AREA SHOWN WITHOUT PRIOR APPROVAL OF THE RESIDENT ENGINEER SHALL NOT BE CONSIDERED FOR PAYMENT, AND THE PAVEMENT SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
3. WHEN A NEW FOUNDATION IS TO BE INSTALLED AT THE SAME LOCATION AS THE REMOVAL, THE NEW FOUNDATION SHALL BE INSTALLED PRIOR TO PAVEMENT REPLACEMENT.
4. LIGHT BASE REMOVAL TO BE PAID FOR UNDER AR125902. SEE PLAN SHEETS FOR LOCATIONS.
5. WHEN NO NEW FOUNDATION OR FIXTURE WILL BE INSTALLED AT THE SAME LOCATION AS THE REMOVAL, THE HOLE SHALL BE FILLED WITH AGGREGATE BASE COURSE (AR209) AND COMPACTED TO 95% MODIFIED PROCTOR (ASTM D-1557) IN LIFTS NOT EXCEEDING 8-INCH LOOSE MEASURE.
6. WHEN A NEW FIXTURE WILL BE INSTALLED AT THE SAME LOCATION AS THE REMOVAL, THE INSTALLATION SHALL BE COMPLETED IN ACCORDANCE WITH THE DETAILS AS SHOWN FOR A NEW BASE MOUNTED LIGHT.

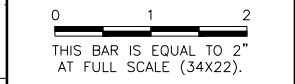
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AIP PROJ. NO.	3-17-0068-XX
SHEET	18 OF 22 SHEETS


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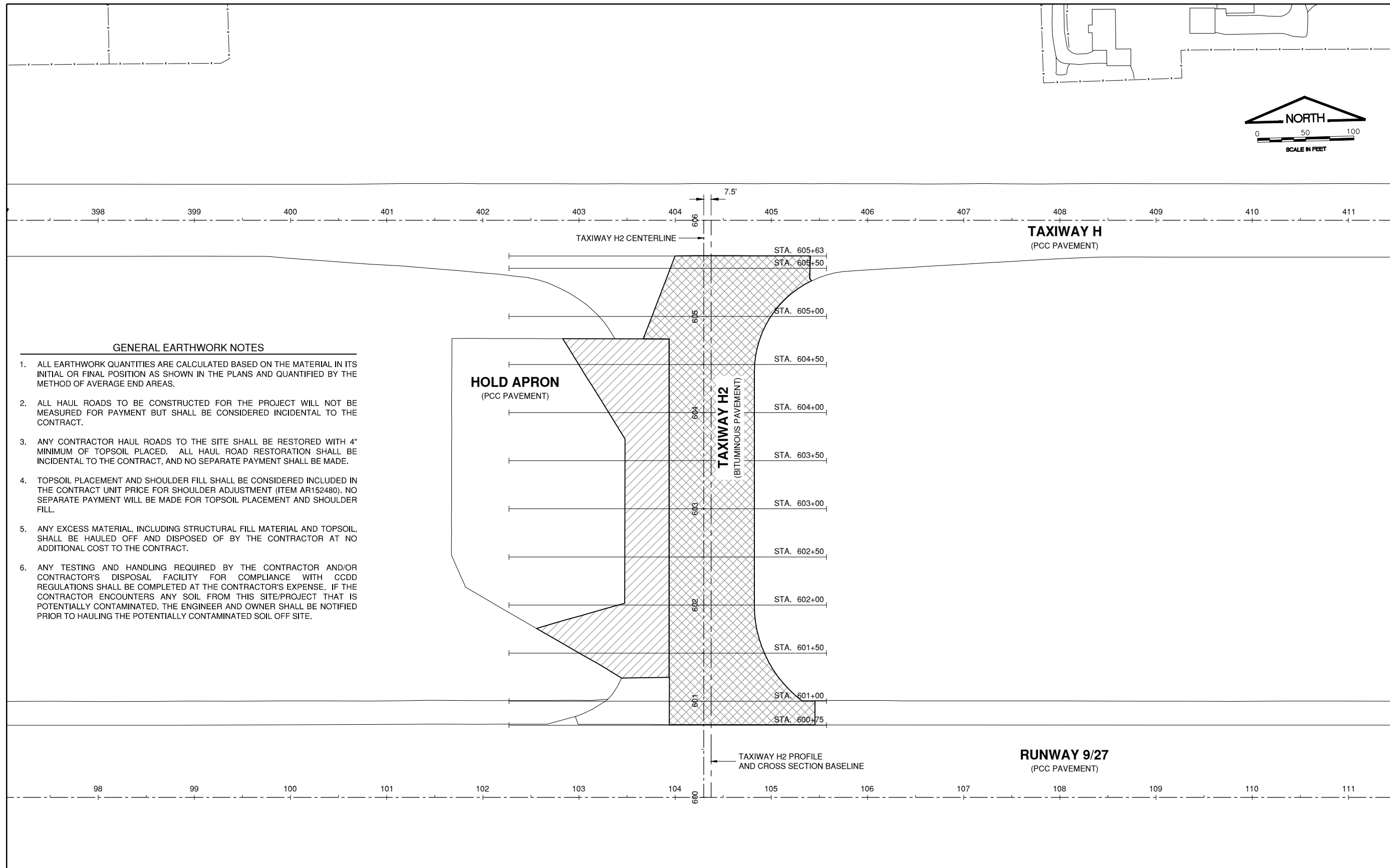
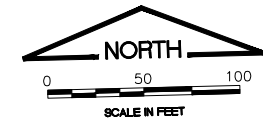


**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
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 MOLINE, ILLINOIS**  
**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
 INDEX TO CROSS SECTIONS**

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
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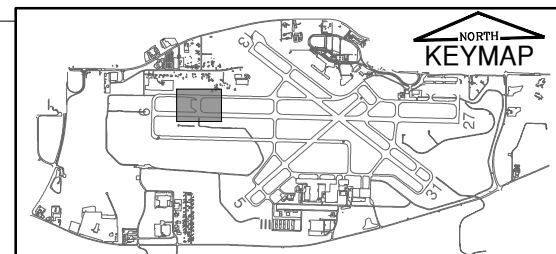


**GENERAL EARTHWORK NOTES**

1. ALL EARTHWORK QUANTITIES ARE CALCULATED BASED ON THE MATERIAL IN ITS INITIAL OR FINAL POSITION AS SHOWN IN THE PLANS AND QUANTIFIED BY THE METHOD OF AVERAGE END AREAS.
2. ALL HAUL ROADS TO BE CONSTRUCTED FOR THE PROJECT WILL NOT BE MEASURED FOR PAYMENT BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
3. ANY CONTRACTOR HAUL ROADS TO THE SITE SHALL BE RESTORED WITH 4" MINIMUM OF TOPSOIL PLACED. ALL HAUL ROAD RESTORATION SHALL BE INCIDENTAL TO THE CONTRACT, AND NO SEPARATE PAYMENT SHALL BE MADE.
4. TOPSOIL PLACEMENT AND SHOULDER FILL SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE FOR SHOULDER ADJUSTMENT (ITEM AR152480). NO SEPARATE PAYMENT WILL BE MADE FOR TOPSOIL PLACEMENT AND SHOULDER FILL.
5. ANY EXCESS MATERIAL, INCLUDING STRUCTURAL FILL MATERIAL AND TOPSOIL, SHALL BE HAULED OFF AND DISPOSED OF BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.
6. ANY TESTING AND HANDLING REQUIRED BY THE CONTRACTOR AND/OR CONTRACTOR'S DISPOSAL FACILITY FOR COMPLIANCE WITH CCDD REGULATIONS SHALL BE COMPLETED AT THE CONTRACTOR'S EXPENSE. IF THE CONTRACTOR ENCOUNTERS ANY SOIL FROM THIS SITE/PROJECT THAT IS POTENTIALLY CONTAMINATED, THE ENGINEER AND OWNER SHALL BE NOTIFIED PRIOR TO HAULING THE POTENTIALLY CONTAMINATED SOIL OFF SITE.

**LEGEND**

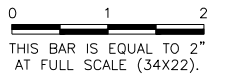
	NEW BITUMINOUS OVERLAY
	NEW BITUMINOUS TRANSITION



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 MOLINE, ILLINOIS**

**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
 CROSS SECTIONS SHEET 1**

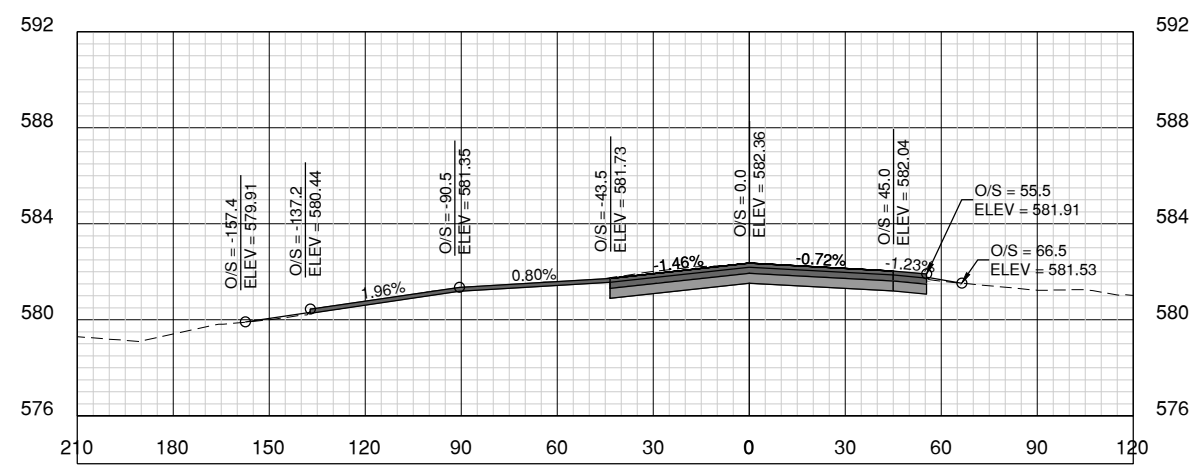
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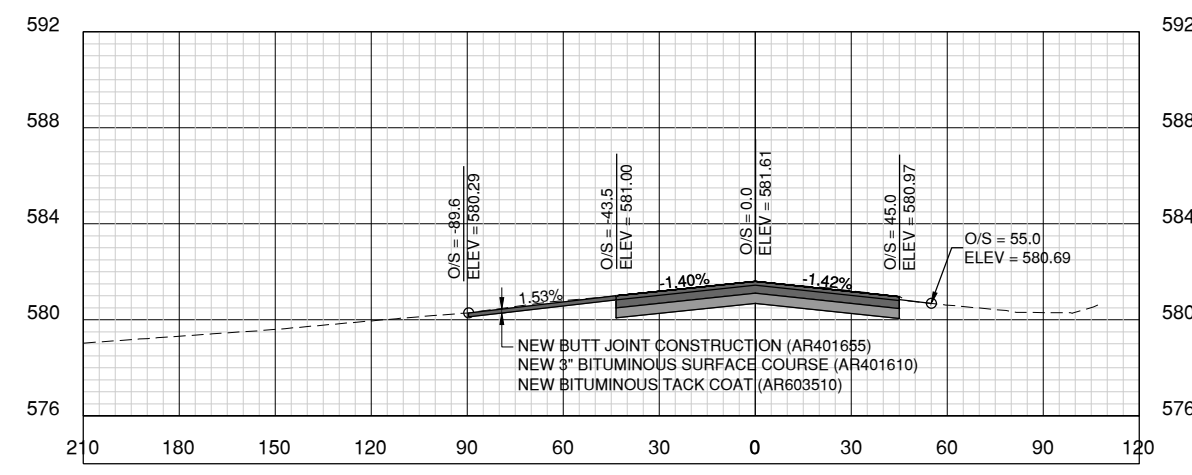


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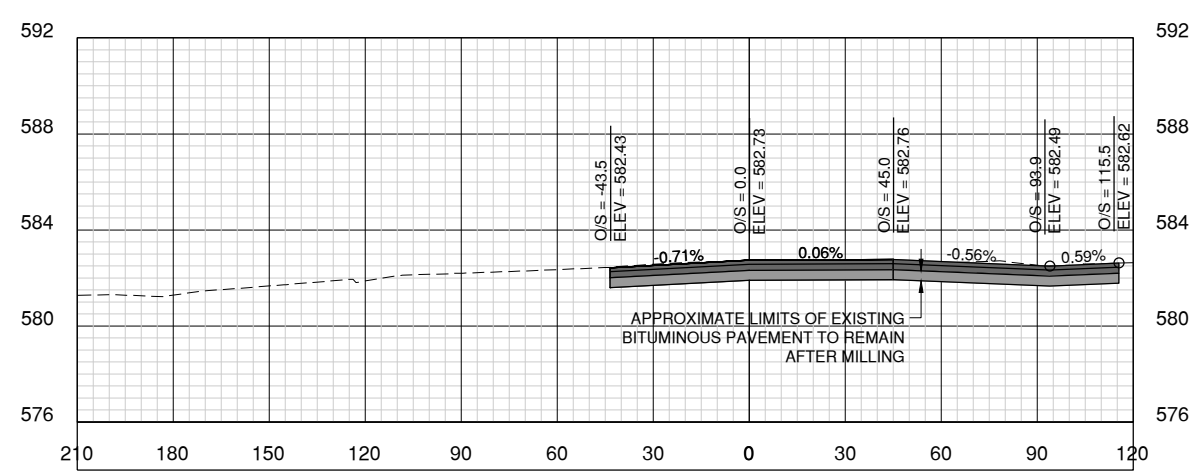
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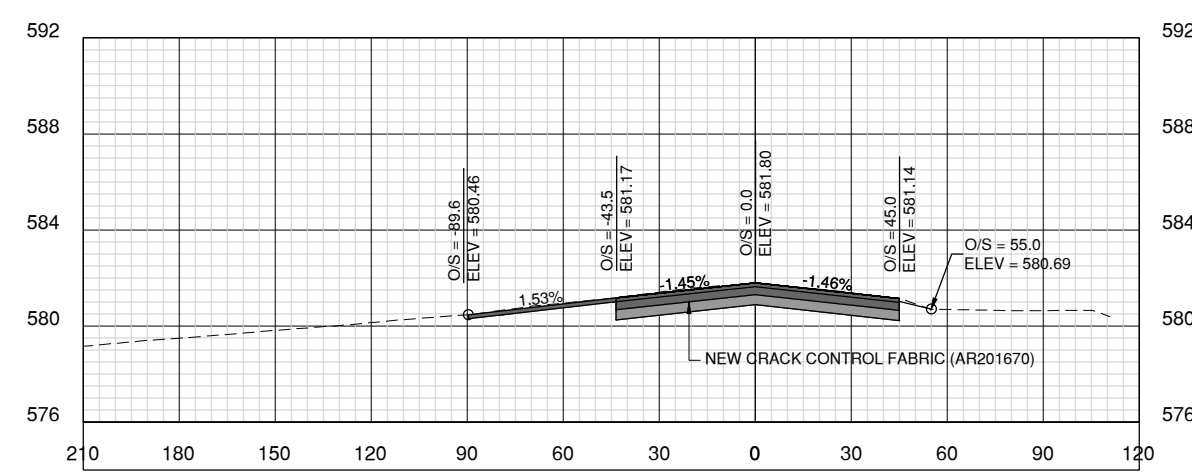
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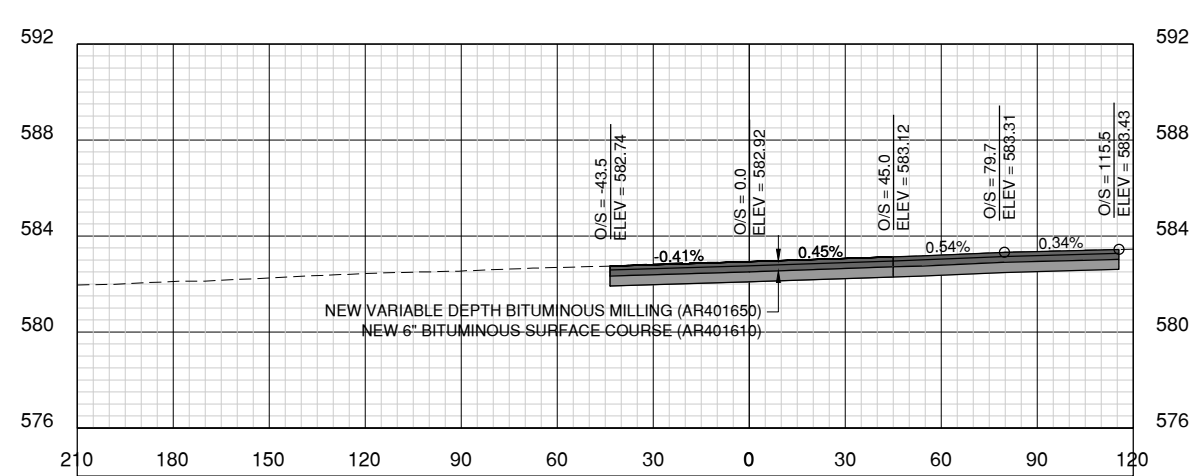
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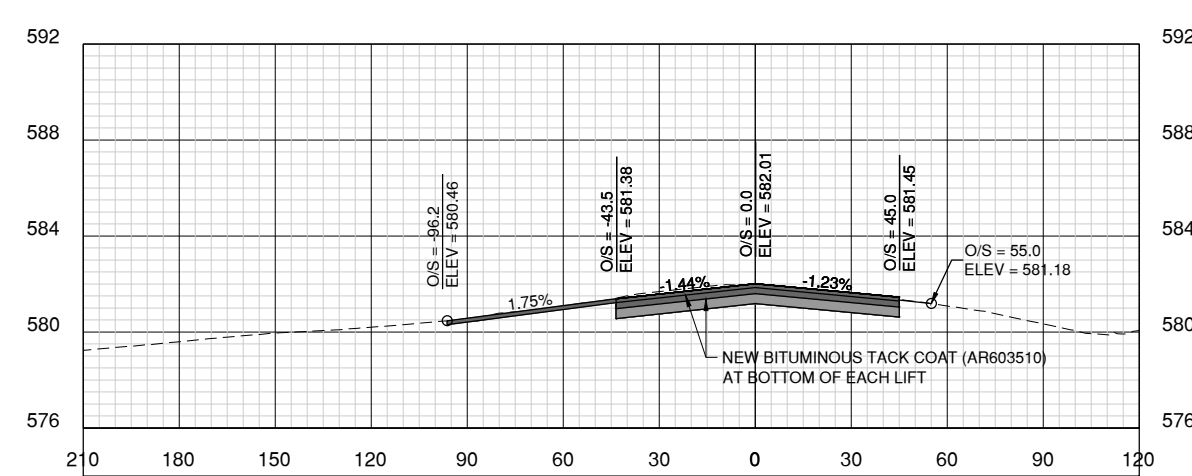
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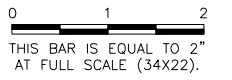
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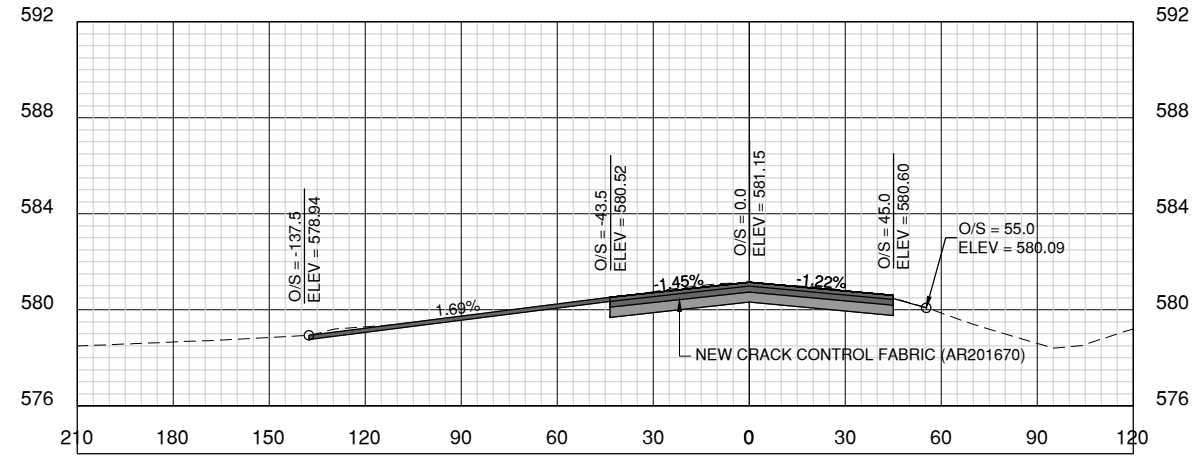
**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
 CROSS SECTIONS SHEET 2**

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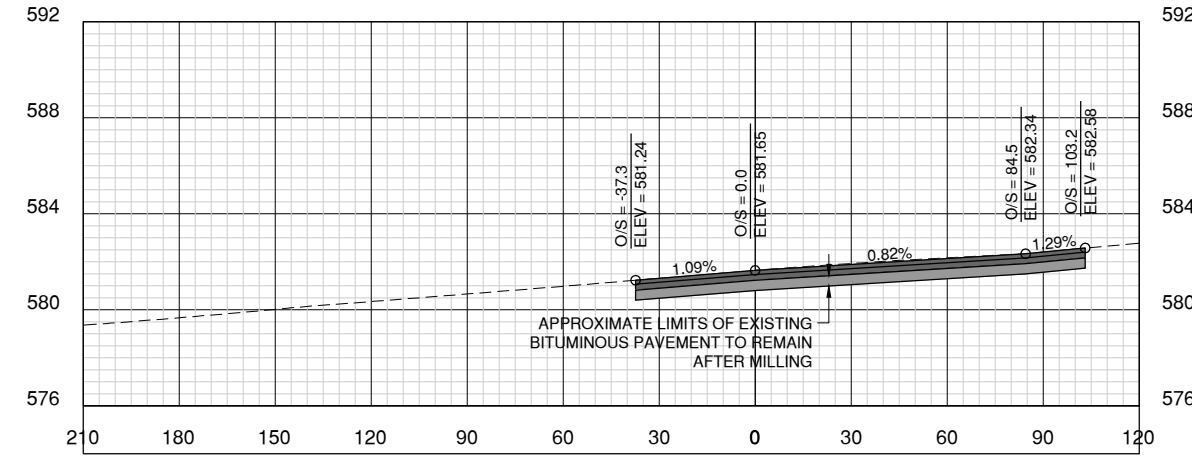
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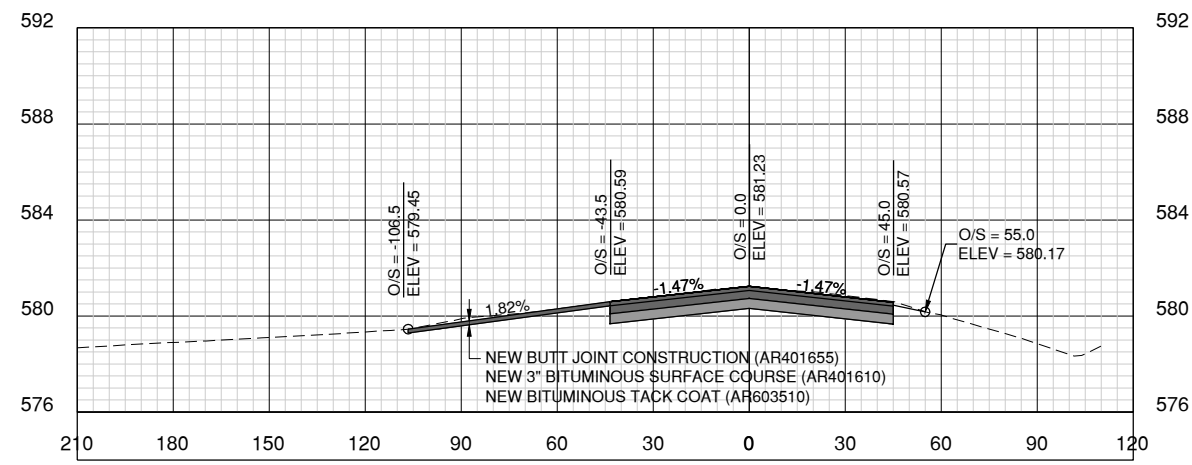
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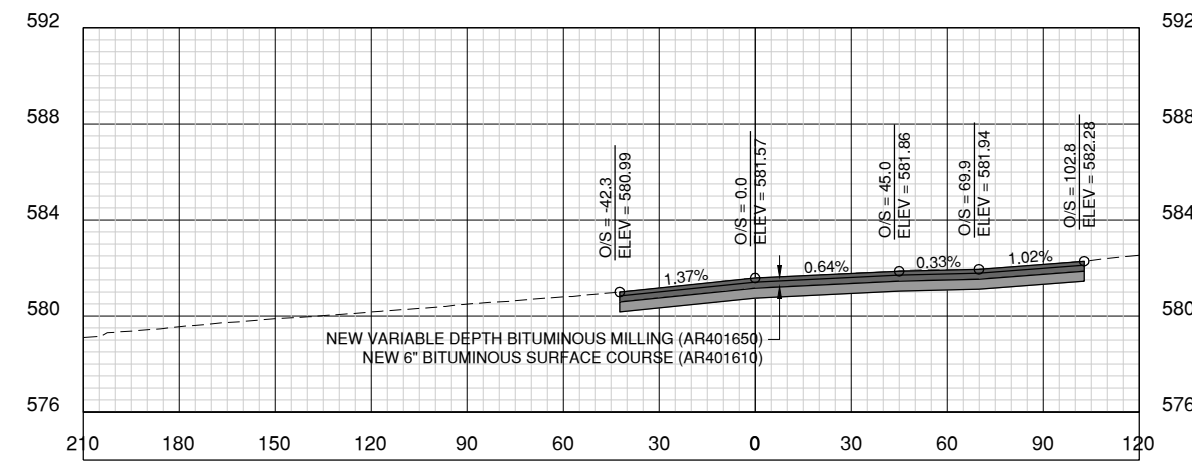
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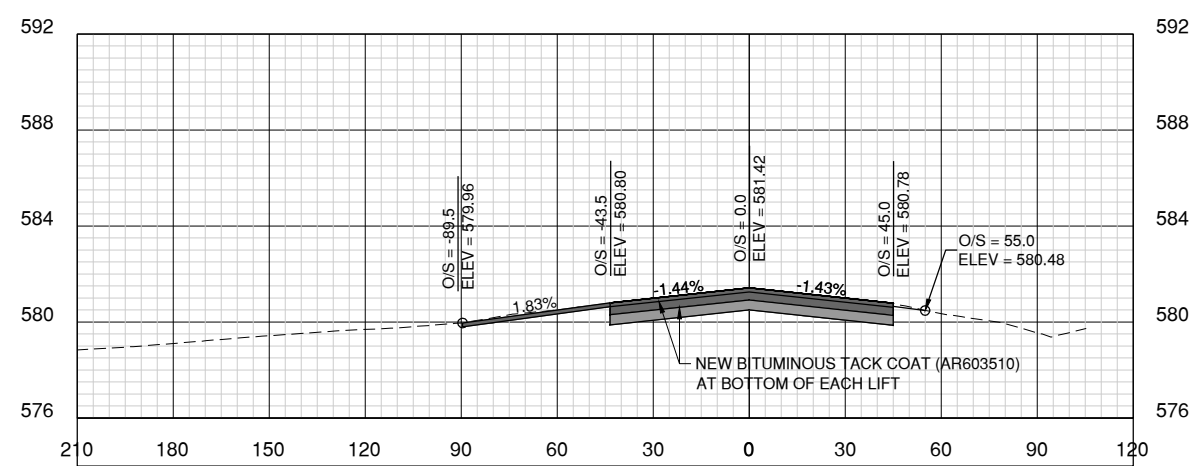
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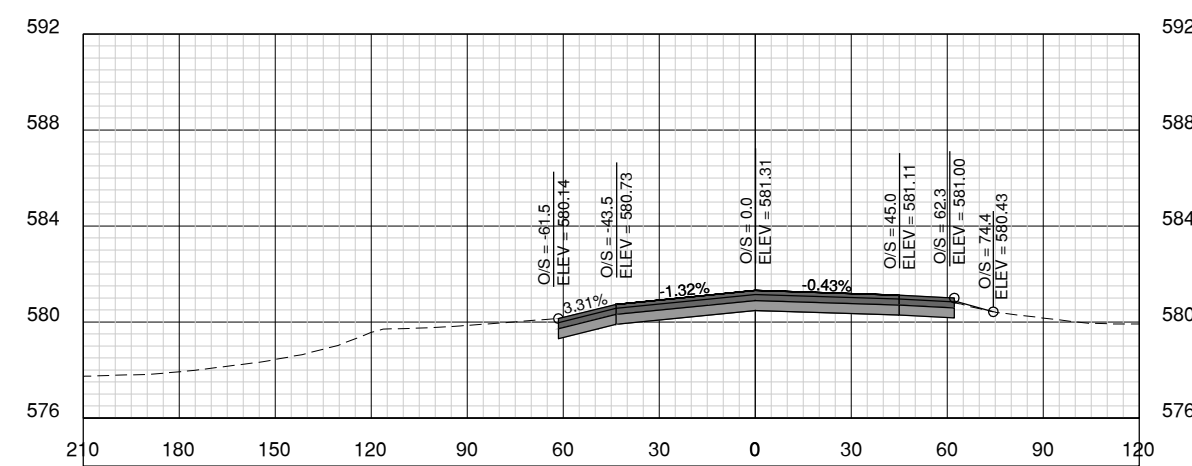
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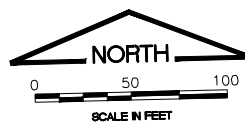
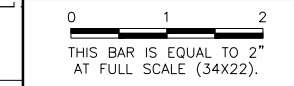
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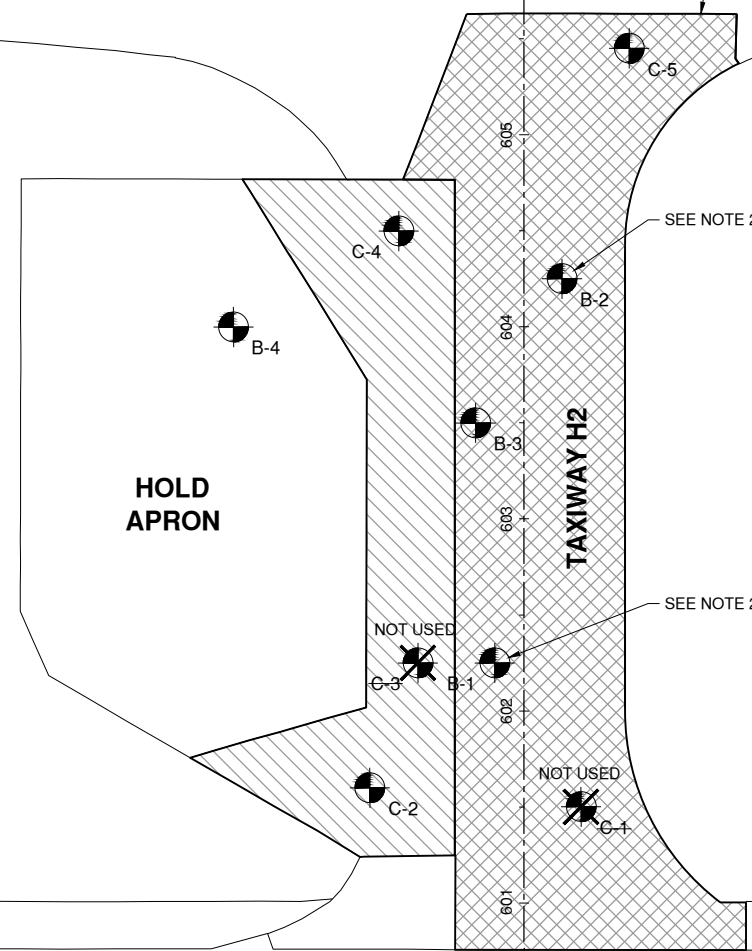
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**TAXIWAY H**

**LEGEND**

	NEW BITUMINOUS OVERLAY
	NEW BITUMINOUS TRANSITION



GEOTECHNICAL LAYOUT TABLE				
BORING/CORE	STATION/OFFSET	NORTHING	EASTING	BORING/CORE SUMMARY
B-1	STA. 2+25, 15' LT.	1742214.97	2199320.38	10" BITUMINOUS PAVEMENT 10" SAND SUBBASE COURSE
B-2	STA. 4+25, 20' RT.	1742414.07	2199360.33	9" BITUMINOUS PAVEMENT 11" SAND SUBBASE COURSE
B-3	STA. 3+50, 25' LT.	1742340.17	2199313.54	9" BITUMINOUS PAVEMENT 10.5" SAND SUBBASE COURSE
B-4	STA. 4+00, 150' LT.	1742393.18	2199188.71	8.5" PCC PAVEMENT 10" SAND SUBBASE COURSE
C-2	STA. 1+60, 80' LT.	1742151.55	2199253.76	5" BITUMINOUS PAVEMENT 10.5" PCC PAVEMENT
C-3	STA. 2+25, 55' LT.	1742215.93	2199280.39	7" BITUMINOUS PAVEMENT 10.5" PCC PAVEMENT
C-4	STA. 4+50, 65' LT.	1742441.10	2199275.95	5.5" BITUMINOUS PAVEMENT 10" PCC PAVEMENT
C-5	STA. 5+45, 55' RT.	1742533.20	2199398.28	6" BITUMINOUS PAVEMENT 7" PCC PAVEMENT

- NOTES:
- SEE SPECIAL PROVISIONS FOR GEOTECHNICAL ENGINEERING REPORT.
  - A 36"-42" DEEP, 6" DIAMETER CONCRETE PLUG WAS INSTALLED TO PATCH BORINGS B-1 AND B-2. CONTRACTOR TO REMOVE CONCRETE PLUGS PRIOR TO PAVING NEW SURFACE COURSE. SEE DETAIL ON TYPICAL SECTIONS SHEET 1.

**RUNWAY 9/27**

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND  
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**REHABILITATE TAXIWAY H2, INCLUDING AIRFIELD LIGHTING  
 SOIL BORING AND PAVEMENT CORE  
 LOCATIONS**

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