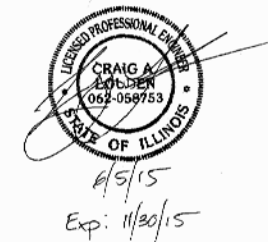


CONSTRUCTION PLANS FOR QUAD CITY INTERNATIONAL AIRPORT

METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND COUNTY
MOLINE, ILLINOIS

IL. PROJ. NO. MLI-4463
AIP PROJ. NO. 3-17-0068-XX

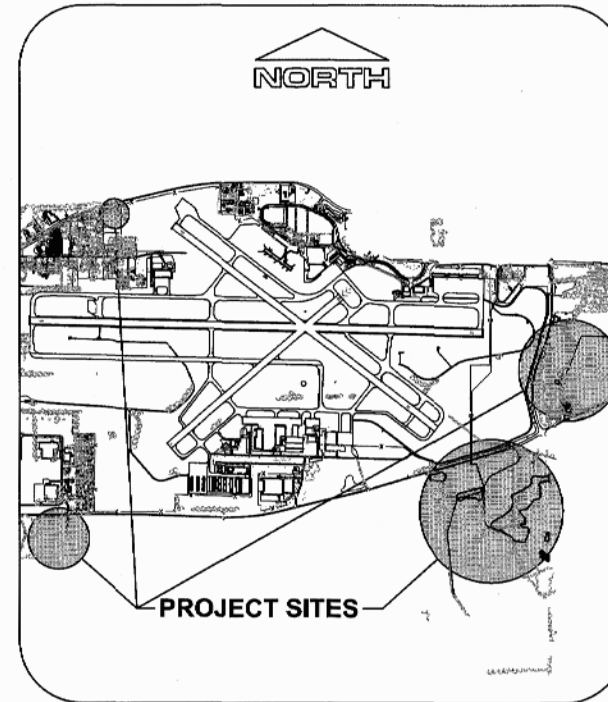
OBSTRUCTION REMOVAL - PHASE 1



JUNE 5, 2015



LOCATION MAP



SITE PLAN

811 Know what's below.
Call before you dig.
J.U.L.I.E.
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS
www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

CALL J.U.L.I.E.
BEFORE EXCAVATING
1-800-892-0123
TOWNSHIP: 17 NORTH
RANGE: 1 WEST
SECTION: 20
COUNTY: ROCK ISLAND
CIVIL TOWNSHIP: UNINCORPORATED
ROCK ISLAND COUNTY

MAXIMUM EQUIPMENT HEIGHT - 25'
UNICOM FREQUENCY - 122.95

METROPOLITAN AIRPORT AUTHORITY
OF ROCK ISLAND COUNTY
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS

APPROVED Bruce Carter
DATE 6/5/15

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS

SUBMITTED BY Craig Louden
CRAIG LOUDEN, P.E.

DATE 6/5/15

CMT JOB NUMBER: 13014-06-00

INDEX TO SHEETS

1. COVER SHEET
2. INDEX TO SHEETS AND SUMMARY OF QUANTITIES
3. SITE PLAN/PROJECT CONTROL PLAN
4. SEQUENCE OF CONSTRUCTION AND DETAILS
5. SEQUENCE OF CONSTRUCTION GENERAL NOTES
6. RUNWAY 5 OBSTRUCTION REMOVAL
7. RUNWAY 13 OBSTRUCTION REMOVAL/
STORMWATER POLLUTION PREVENTION PLAN
8. RUNWAY 27 OBSTRUCTION REMOVAL/
STORMWATER POLLUTION PREVENTION PLAN
9. RUNWAY 31 OBSTRUCTION REMOVAL/
STORMWATER POLLUTION PREVENTION PLAN
10. RUNWAY 31 OBSTRUCTION REMOVAL/
STORMWATER POLLUTION PREVENTION PLAN
11. STORMWATER POLLUTION PREVENTION PLAN
NOTES AND DETAILS

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR150520	MOBILIZATION	LS	1	
AR151410	CLEARING	ACRE	2.5	
AR151430	CLEARING TREES 2.5'-5' BUTT DIA.	EA	10	
AR151450	CLEARING AND GRUBBING	ACRE	18.6	
AR152410	UNCLASSIFIED EXCAVATION	CY	1,950	
AR156510	SILT FENCE	LF	4,800	
AR156531	EROSION CONTROL BLANKET	SY	2,800	
AR901510	SEEDING	ACRE	19.2	
AR908510	MULCHING	ACRE	18.6	

QU021

REVISIONS

NUMBER	BY	DATE

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 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS

OBSTRUCTION REMOVAL - PHASE 1
 INDEX TO SHEETS AND
 SUMMARY OF QUANTITIES

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DATE: 06/02/2015

JOB No: 13014-06

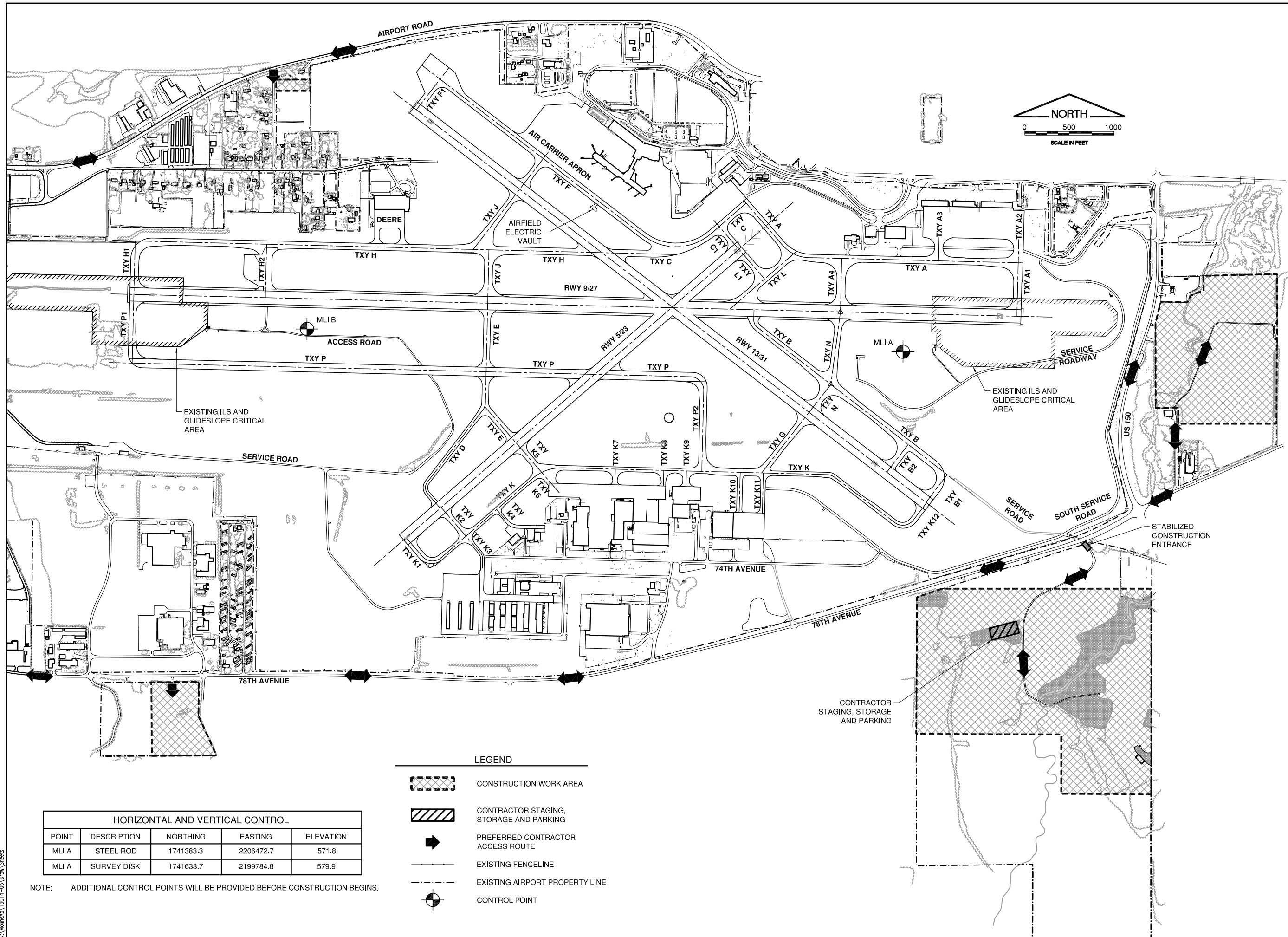
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SHEET 2 OF 11 SHEETS

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**QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**OBSTRUCTION REMOVAL - PHASE 1
 SITE PLAN/PROJECT CONTROL PLAN**

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SHEET 3 OF 11 SHEETS	

LEGEND

	CONSTRUCTION WORK AREA
	CONTRACTOR STAGING, STORAGE AND PARKING
	PREFERRED CONTRACTOR ACCESS ROUTE
	EXISTING FENCELINE
	EXISTING AIRPORT PROPERTY LINE
	CONTROL POINT

HORIZONTAL AND VERTICAL CONTROL

POINT	DESCRIPTION	NORTHING	EASTING	ELEVATION
MLI A	STEEL ROD	1741383.3	2206472.7	571.8
MLI A	SURVEY DISK	1741638.7	2199784.8	579.9

NOTE: ADDITIONAL CONTROL POINTS WILL BE PROVIDED BEFORE CONSTRUCTION BEGINS.

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**QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

**OBSTRUCTION REMOVAL - PHASE 1
SEQUENCE OF CONSTRUCTION
AND DETAILS**

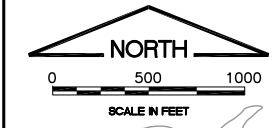
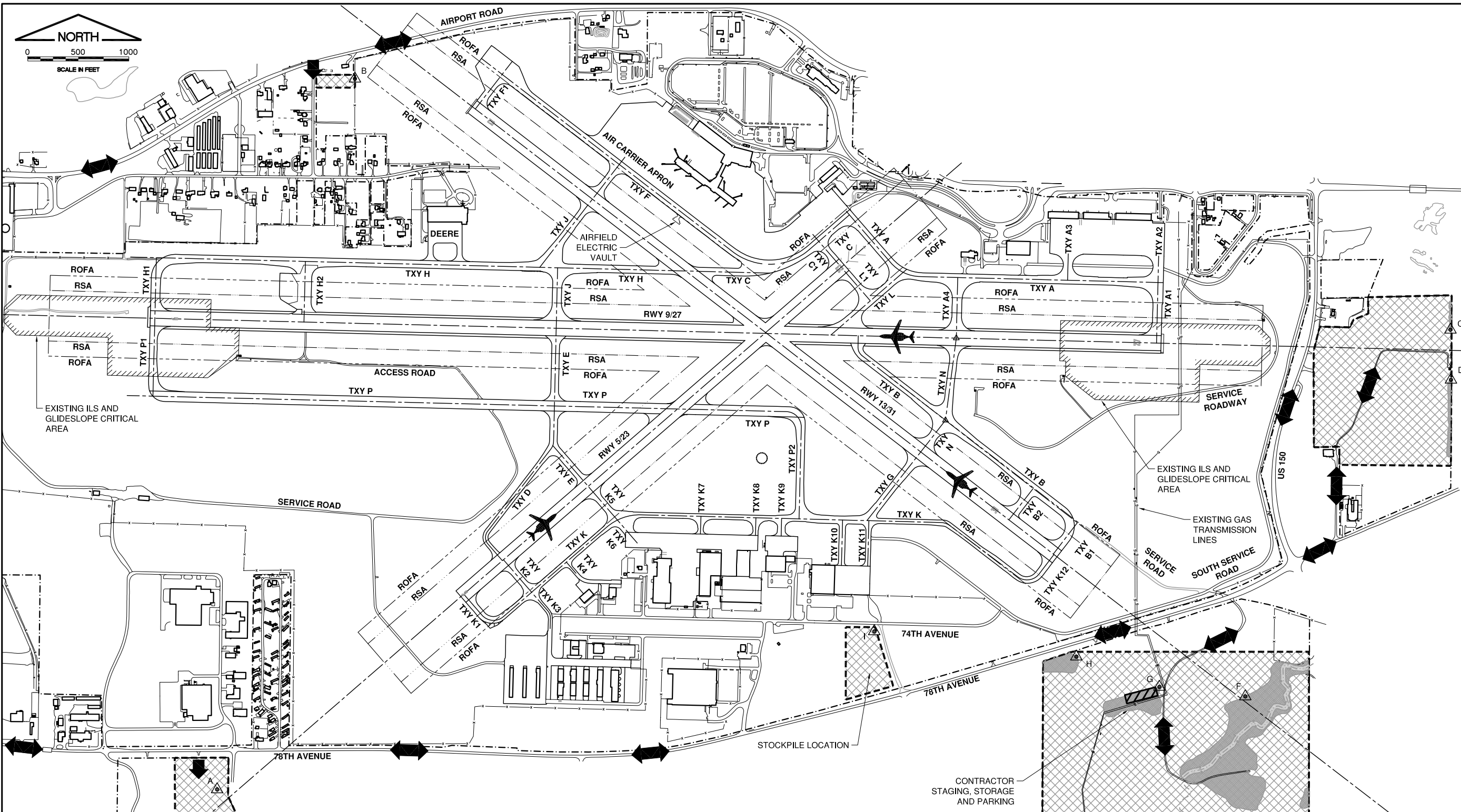
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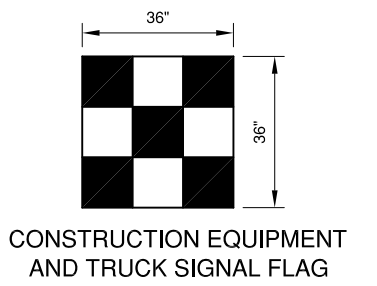


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SHEET	04 OF 11 SHEETS



ALL RUNWAYS WILL BE OPEN FOR THE DURATION OF THE PROJECT

ALL WORK TO TAKE PLACE OUTSIDE AIRPORT PERIMETER FENCE



LEGEND

	CONSTRUCTION WORK AREA
	CONTRACTOR STAGING, STORAGE AND PARKING
	PREFERRED CONTRACTOR ACCESS ROUTE
	EXISTING FENCELINE
	EXISTING AIRPORT PROPERTY LINE
	CRITICAL POINT
	RUNWAY SAFETY AREA
	RUNWAY OBJECT FREE AREA

CRITICAL POINTS

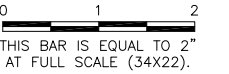
POINT #	LATITUDE	LONGITUDE	ELEVATION*
A	41°26'11.02"	90°31'28.35"	685.0
B	41°27'20.57"	90°31'10.83"	593.0
C	41°26'56.44"	90°28'48.37"	595.0
D	41°26'51.54"	90°28'48.29"	600.0
E	41°26'07.33"	90°29'08.14"	703.0
F	41°26'20.50"	90°29'14.85"	611.0
G	41°26'21.41"	90°29'26.02"	627.0
H	41°26'24.33"	90°29'36.82"	620.0
I	41°26'26.73"	90°30'03.07"	601.0

* ELEVATION INCLUDES 25' ANTICIPATED MAXIMUM EQUIPMENT HEIGHT

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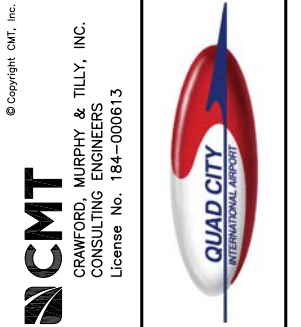
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**QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

**OBSTRUCTION REMOVAL - PHASE 1
SEQUENCE OF CONSTRUCTION
GENERAL NOTES**



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SHEET	05 OF 11 SHEETS

11. PENALTIES (CONT.)

- THE CONTRACTOR SHALL RESTRICT ALL CONSTRUCTION ACTIVITIES TO THE CONSTRUCTION AREA DETAILED IN THE CONSTRUCTION SAFETY AND PHASING PLAN. ANY UNAUTHORIZED MOVEMENTS, PEDESTRIAN OR VEHICULAR, BEYOND THE CONSTRUCTION LIMITS SHOWN SHALL BE CONSIDERED AN AIRFIELD INCURSION. AIRFIELD INCURSIONS, AT THE DISCRETION OF THE AIRPORT DIRECTOR OF AVIATION, MAY BE FINED \$10,000.00 PER INCIDENT. INCURSION FINES WILL BE ASSESSED IMMEDIATELY AND TAKEN FROM MONIES DUE THE CONTRACTOR ON THE NEXT CONSTRUCTION PAYMENT.

12. SPECIAL CONDITIONS

- ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

13. RUNWAY AND TAXIWAY VISUAL AIDS

- RUNWAY OR TAXIWAY CLOSURES ARE AS DETAILED IN THE CONSTRUCTION SAFETY AND PHASING PLAN FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2F.

14. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL PLACE ALL BARRICADES AND CONSTRUCTION SETBACK LINES ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, REPOSITION, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
- ACCESS TO ACTIVE RUNWAY AND TAXIWAY PAVEMENTS (TOWER CONTROLLED AREAS) SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON TYPE II BARRICADES (2 EACH, RIGHT AND LEFT). IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "TOWER CONTROL AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE".

15. PROTECTION

- CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS AND THE RUNWAY SAFETY AREA (RSA) OF ACTIVE RUNWAYS.

16. OTHER LIMITATIONS ON CONSTRUCTION

- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- THE CONTRACTOR MAY BE REQUIRED TO SUBMIT A REVISED PROGRESS SCHEDULE TO ACCOMMODATE AIRPORT EVENTS (I.E. AIRSHOW). SHOULD A REVISED SCHEDULE BE REQUIRED, THE REVISION SHALL BE COMPLETED AT NO ADDITIONAL COST TO THE CONTRACT.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS.
- THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.
- BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

7. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

8. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
- CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.

AIRPORT OPERATOR
BRUCE CARTER - DIRECTOR OF AVIATION (309) 757-1732

ENGINEER
CMT - RESIDENT ENGINEER (630) 820-1022

9. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

10. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. SEE SECTION 70-17 OF THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.
- SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

11. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.
- THE GATE SHALL BE MAINTAINED, CLOSED AND LOCKED AS DIRECTED BY THE AIRPORT DIRECTOR OF AVIATION. SHOULD THE CONTRACTOR'S OPERATIONS REQUIRE THE GATE TO REMAIN OPEN TO PROVIDE ACCESS TO HAULING OPERATIONS, A COMPETENT GATE GUARD SHALL BE REQUIRED TO CONTROL ACCESS TO THE AIRFIELD. A \$1,000 FINE SHALL BE ASSESSED FOR ANY OCCURRENCE OF AN UNSECURE GATE THAT IS THE CONTRACTOR'S RESPONSIBILITY. AN UNSECURED GATE SHALL BE DEFINED AS ANY GATE THAT IS NOT WITHIN THE SIGHT AND PHYSICAL CONTROL OF THE CONTRACTOR'S GUARD. IN THE EVENT THAT THE GATE MAY NOT BE SECURED, THE CONTRACTOR WILL BE CHARGED FOR AIRPORT PERSONNEL TO REMAIN AT THE GATE UNTIL SECURED.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.
- SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

- THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE EXISTING LOCATIONS SHOWN. THE ENTRANCE SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLES ACCESS AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO CITY OF MOLINE AND IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.
- CERTAIN CONTRACTOR EMPLOYEES SHALL OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET CERTAIN BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTAIN CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
- ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATION AREA (AOA) SHALL ALSO ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT WITHOUT AN ESCORT. ALL COST ASSOCIATED WITH THE DRIVERS TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
- DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE.
- THE CONTRACTOR'S STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN AND CONSTRUCTION PHASING PLAN.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS, STORAGE AREAS AND/OR STAGING AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SAFETY REQUIREMENTS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING SAFETY REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL SUBMIT THE SPCD FOR APPROVAL.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE RESIDENT ENGINEER. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ONSITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE AIRPORT. PAYMENT FOR DUST CONTROL SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, AIR OPERATIONS AREA (A.O.A) LATH AND RIBBON, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.
- CONTRACTOR SHALL PLAN HIS/HER WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORT AND SPECIAL PROVISIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT:

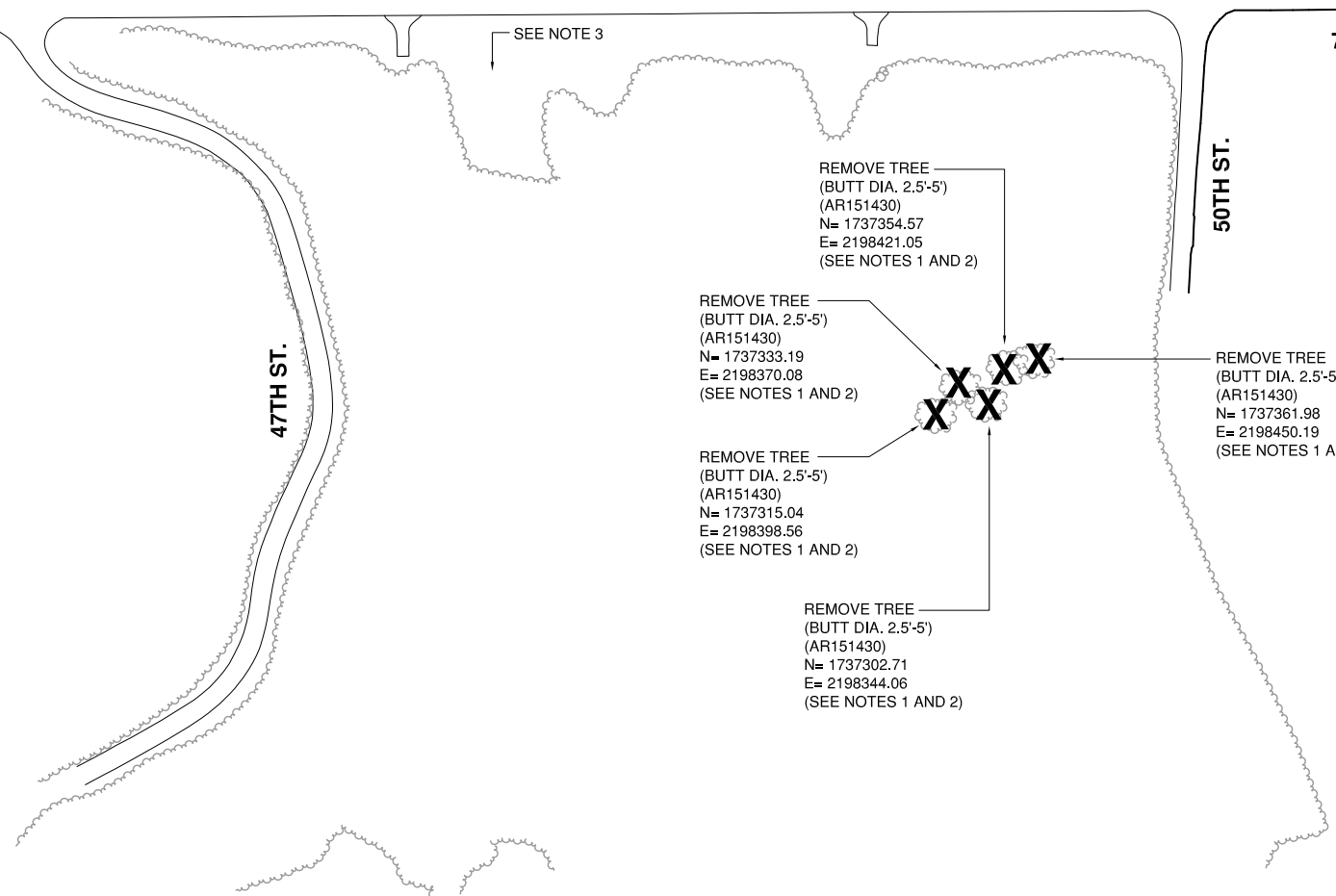
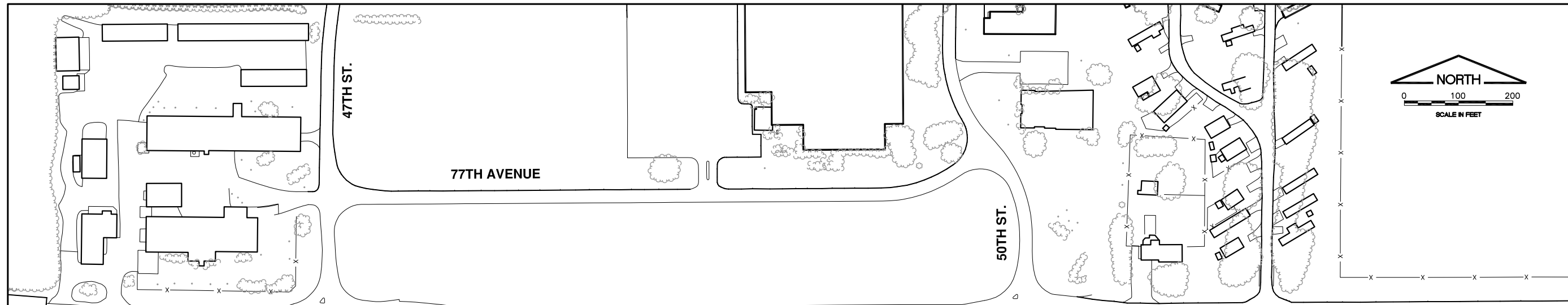
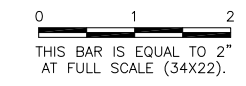
- INSTALL RWY 9 PAPI AND AIRFIELD SIGNAGE UPGRADES
- REHABILITATE TAXIWAY H2
- CONSTRUCT TAXIWAY K CONNECTOR
- AIRFIELD SECURITY IMPROVEMENTS - PHASE 1

2. PHASING

- TOTAL CONTRACT TIME SHALL BE 38 CALENDAR DAYS.
- PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.

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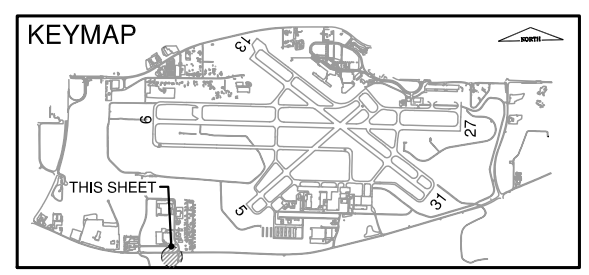


LEGEND

	AIRPORT PROPERTY LINE
	AIRPORT PERIMETER FENCE
	EXISTING STRUCTURE
	TREE TO BE REMOVED

NOTES

- NO SOIL DISTURBANCE ALLOWED IN TREE CLEARING AREA. TREES SHALL BE REMOVED WITHOUT MECHANIZED EQUIPMENT. FELLED TREES AND STUMPS SHALL BE LEFT IN PLACE.
- TREE REMOVAL AND CLEARING AND GRUBBING LOCATIONS HAVE BEEN DETERMINED FROM AERIAL LIDAR SURVEYS AND ARE APPROXIMATE. CONTRACTOR SHALL COORDINATE LOCATIONS/LIMITS WITH THE RESIDENT ENGINEER BEFORE REMOVING TREES.
- RESIDENT ENGINEER SHALL SUPPLY CONTRACTOR WITH EXISTING PROPERTY LINE CONTROL POINTS. CONTRACTOR SHALL LAYOUT EXISTING PROPERTY LINE IN AREA ADJACENT TO TREE REMOVALS AND/OR CLEARING AND GRUBBING AREAS.
- UPON COMPLETION OF TREE REMOVALS AND/OR CLEARING AND GRUBBING, CONTRACTOR SHALL PROVIDE SURVEYED COORDINATES (NORTHING AND EASTING IN ILLINOIS STATE PLANE WEST) OF REMOVED TREES AND/OR LIMITS OF CLEARING AND GRUBBING AREAS TO RESIDENT ENGINEER.



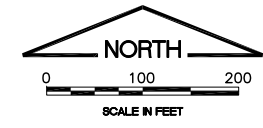
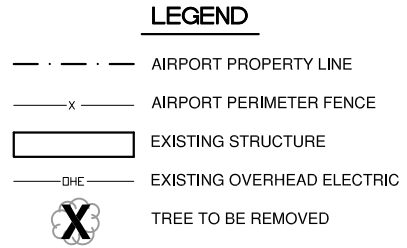
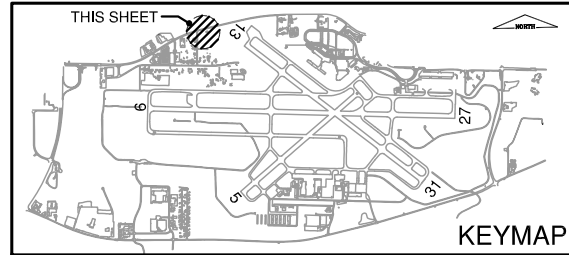
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OBSTRUCTION REMOVAL - PHASE 1
 RUNWAY 5 OBSTRUCTION REMOVAL

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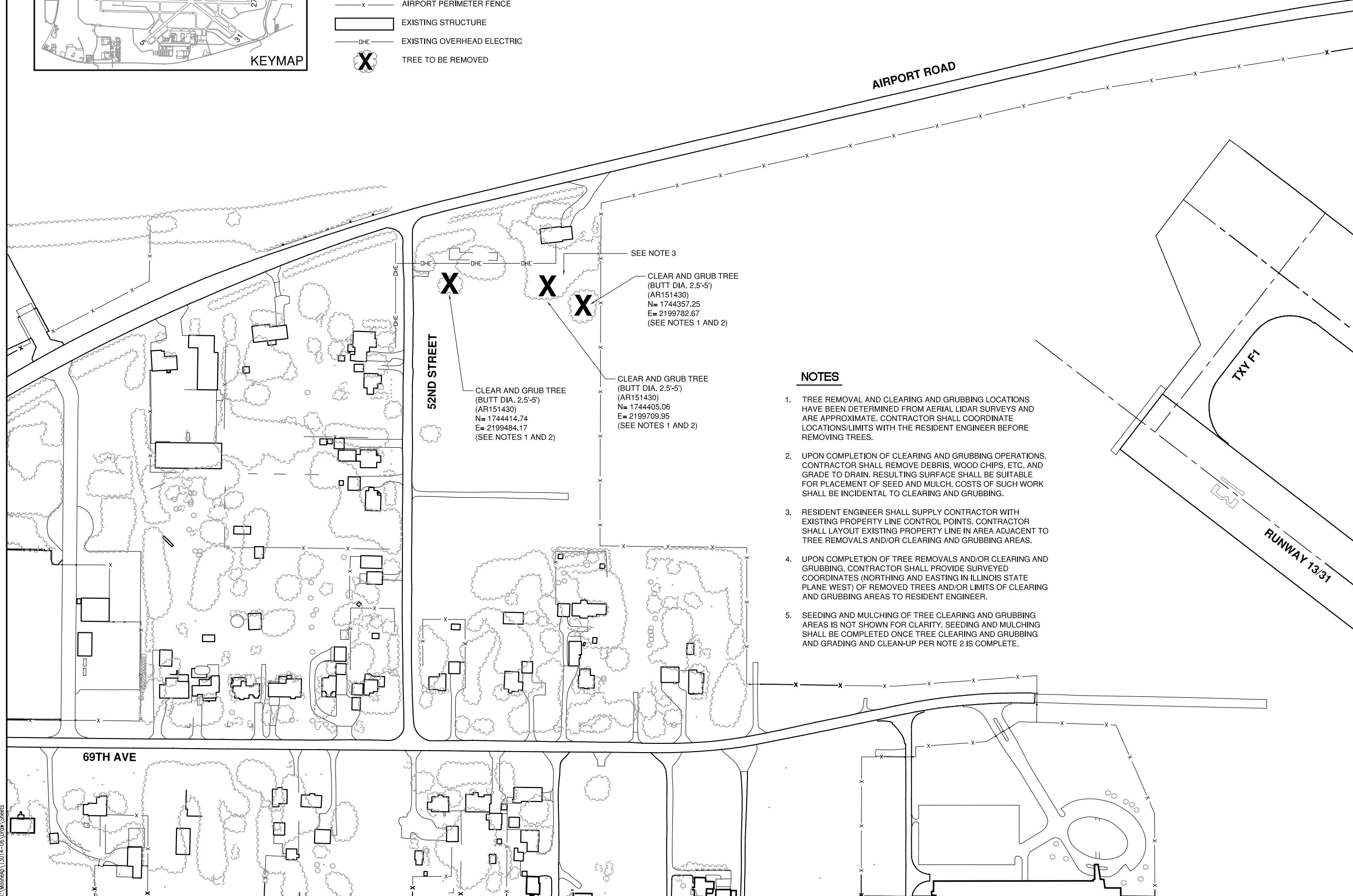
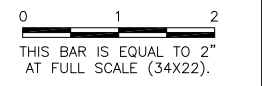
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NOTES

- TREE REMOVAL AND CLEARING AND GRUBBING LOCATIONS HAVE BEEN DETERMINED FROM AERIAL LIDAR SURVEYS AND ARE APPROXIMATE. CONTRACTOR SHALL COORDINATE LOCATIONS/LIMITS WITH THE RESIDENT ENGINEER BEFORE REMOVING TREES.
- UPON COMPLETION OF CLEARING AND GRUBBING OPERATIONS, CONTRACTOR SHALL REMOVE DEBRIS, WOOD CHIPS, ETC. AND GRADE TO DRAIN. RESULTING SURFACE SHALL BE SUITABLE FOR PLACEMENT OF SEED AND MULCH. COSTS OF SUCH WORK SHALL BE INCIDENTAL TO CLEARING AND GRUBBING.
- RESIDENT ENGINEER SHALL SUPPLY CONTRACTOR WITH EXISTING PROPERTY LINE CONTROL POINTS. CONTRACTOR SHALL LAYOUT EXISTING PROPERTY LINE IN AREA ADJACENT TO TREE REMOVALS AND/OR CLEARING AND GRUBBING AREAS.
- UPON COMPLETION OF TREE REMOVALS AND/OR CLEARING AND GRUBBING, CONTRACTOR SHALL PROVIDE SURVEYED COORDINATES (NORTHING AND EASTING IN ILLINOIS STATE PLANE WEST) OF REMOVED TREES AND/OR LIMITS OF CLEARING AND GRUBBING AREAS TO RESIDENT ENGINEER.
- SEEDING AND MULCHING OF TREE CLEARING AND GRUBBING AREAS IS NOT SHOWN FOR CLARITY. SEEDING AND MULCHING SHALL BE COMPLETED ONCE TREE CLEARING AND GRUBBING AND GRADING AND CLEAN-UP PER NOTE 2 IS COMPLETE.

**QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**OBSTRUCTION REMOVAL - PHASE 1
 RUNWAY 13 OBSTRUCTION REMOVAL/
 STORMWATER POLLUTION PREVENTION PLAN**

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**QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**OBSTRUCTION REMOVAL - PHASE 1
 RUNWAY 27 OBSTRUCTION REMOVAL
 STORMWATER POLLUTION PREVENTION PLAN**

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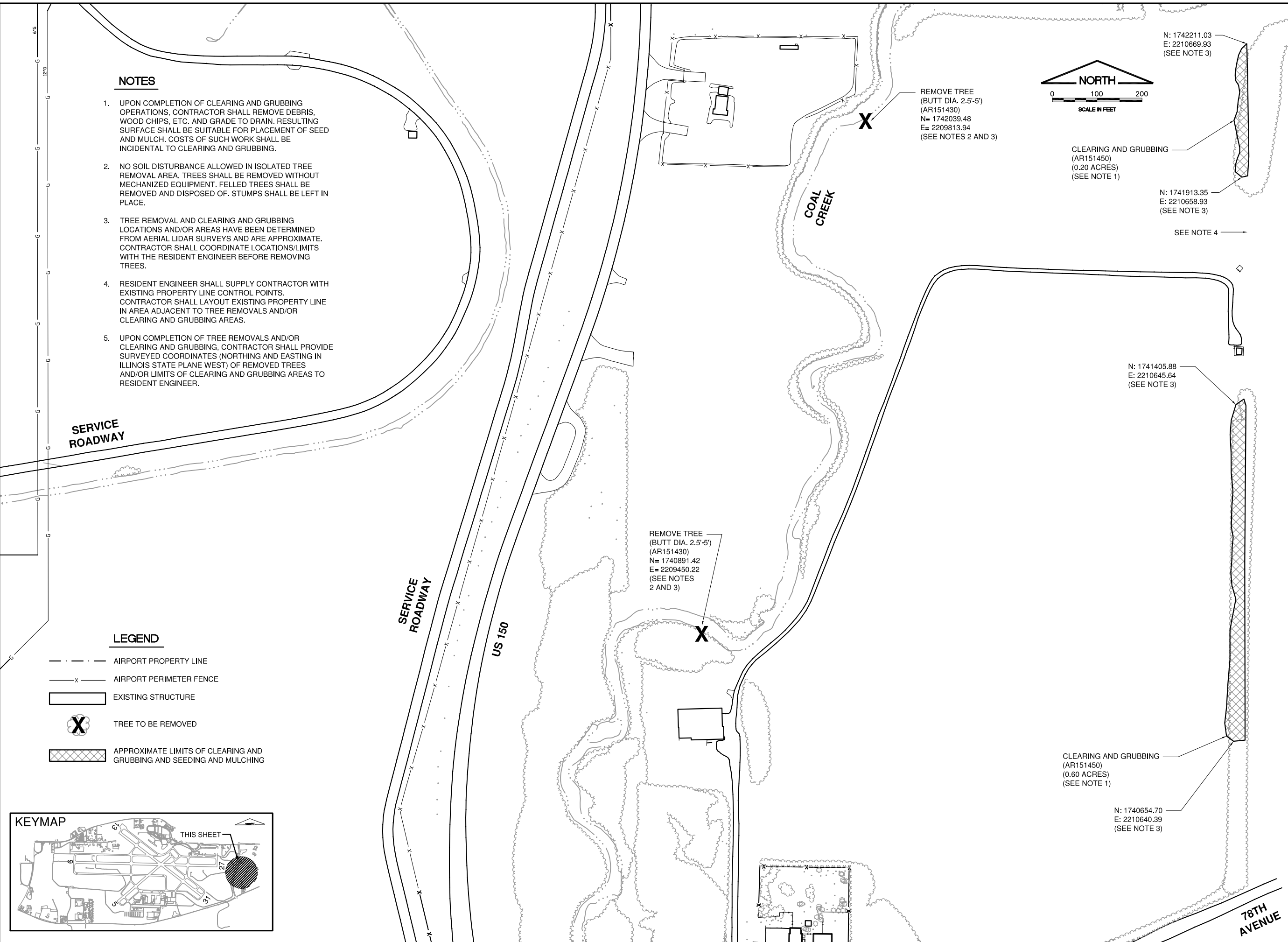
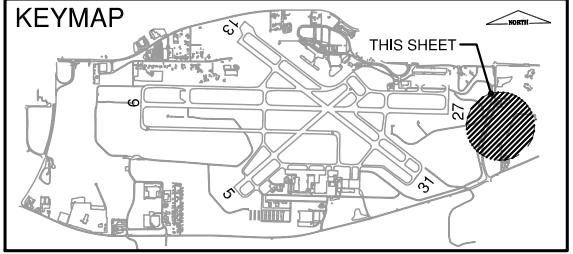
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NOTES

- UPON COMPLETION OF CLEARING AND GRUBBING OPERATIONS, CONTRACTOR SHALL REMOVE DEBRIS, WOOD CHIPS, ETC. AND GRADE TO DRAIN. RESULTING SURFACE SHALL BE SUITABLE FOR PLACEMENT OF SEED AND MULCH. COSTS OF SUCH WORK SHALL BE INCIDENTAL TO CLEARING AND GRUBBING.
- NO SOIL DISTURBANCE ALLOWED IN ISOLATED TREE REMOVAL AREA. TREES SHALL BE REMOVED WITHOUT MECHANIZED EQUIPMENT. FELLED TREES SHALL BE REMOVED AND DISPOSED OF. STUMPS SHALL BE LEFT IN PLACE.
- TREE REMOVAL AND CLEARING AND GRUBBING LOCATIONS AND/OR AREAS HAVE BEEN DETERMINED FROM AERIAL LIDAR SURVEYS AND ARE APPROXIMATE. CONTRACTOR SHALL COORDINATE LOCATIONS/LIMITS WITH THE RESIDENT ENGINEER BEFORE REMOVING TREES.
- RESIDENT ENGINEER SHALL SUPPLY CONTRACTOR WITH EXISTING PROPERTY LINE CONTROL POINTS. CONTRACTOR SHALL LAYOUT EXISTING PROPERTY LINE IN AREA ADJACENT TO TREE REMOVALS AND/OR CLEARING AND GRUBBING AREAS.
- UPON COMPLETION OF TREE REMOVALS AND/OR CLEARING AND GRUBBING, CONTRACTOR SHALL PROVIDE SURVEYED COORDINATES (NORTHING AND EASTING IN ILLINOIS STATE PLANE WEST) OF REMOVED TREES AND/OR LIMITS OF CLEARING AND GRUBBING AREAS TO RESIDENT ENGINEER.

LEGEND

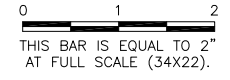
- AIRPORT PROPERTY LINE
- x- AIRPORT PERIMETER FENCE
- ▭ EXISTING STRUCTURE
- X TREE TO BE REMOVED
- ▨ APPROXIMATE LIMITS OF CLEARING AND GRUBBING AND SEEDING AND MULCHING



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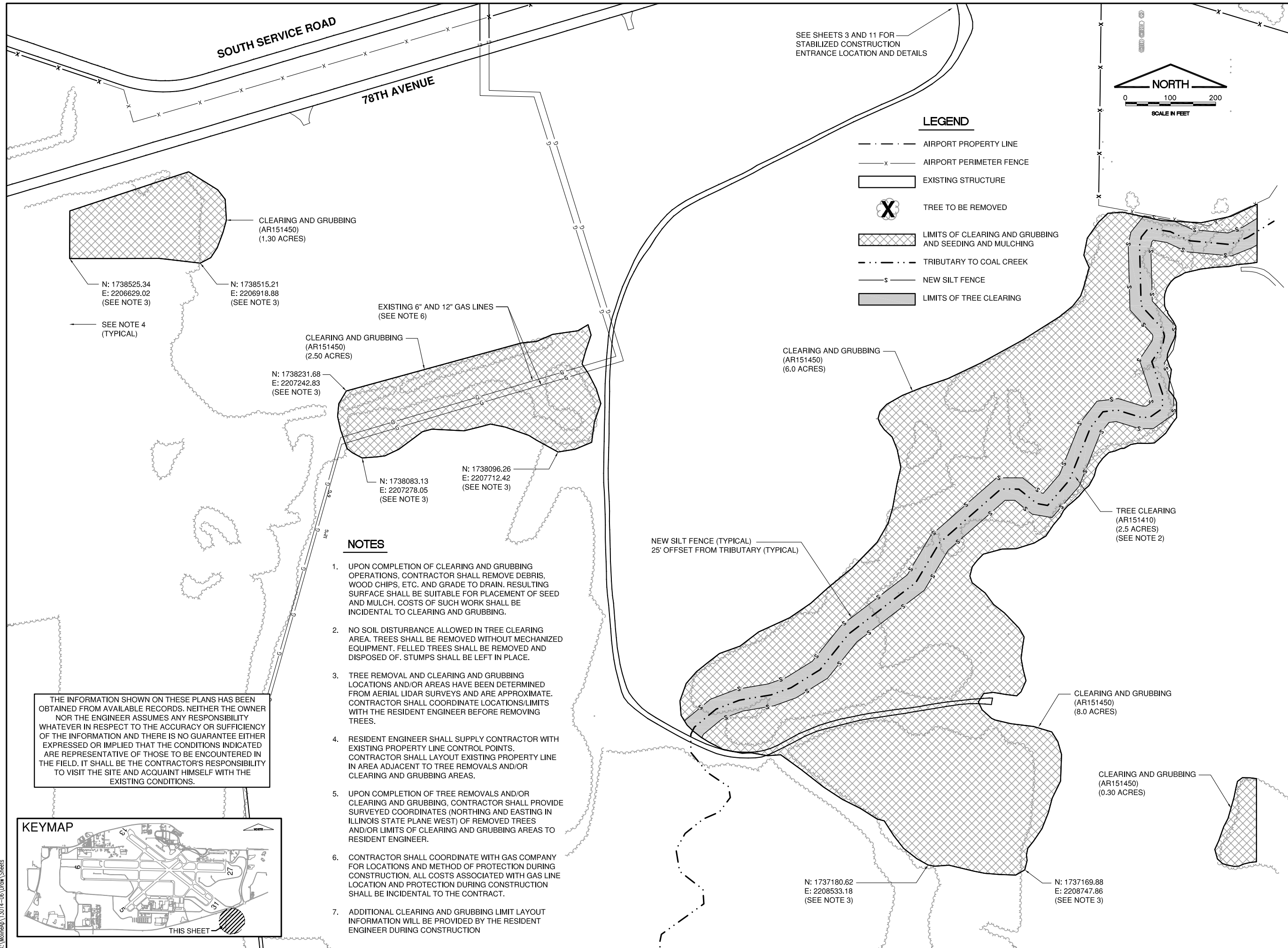
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 MOLINE, ILLINOIS**

**OBSTRUCTION REMOVAL - PHASE 1
 RUNWAY 31 OBSTRUCTION REMOVAL/
 STORMWATER POLLUTION PREVENTION PLAN**

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**QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**OBSTRUCTION REMOVAL - PHASE 1
 RUNWAY 31 GRADING PLAN/
 STORM WATER POLLUTION PREVENTION PLAN**

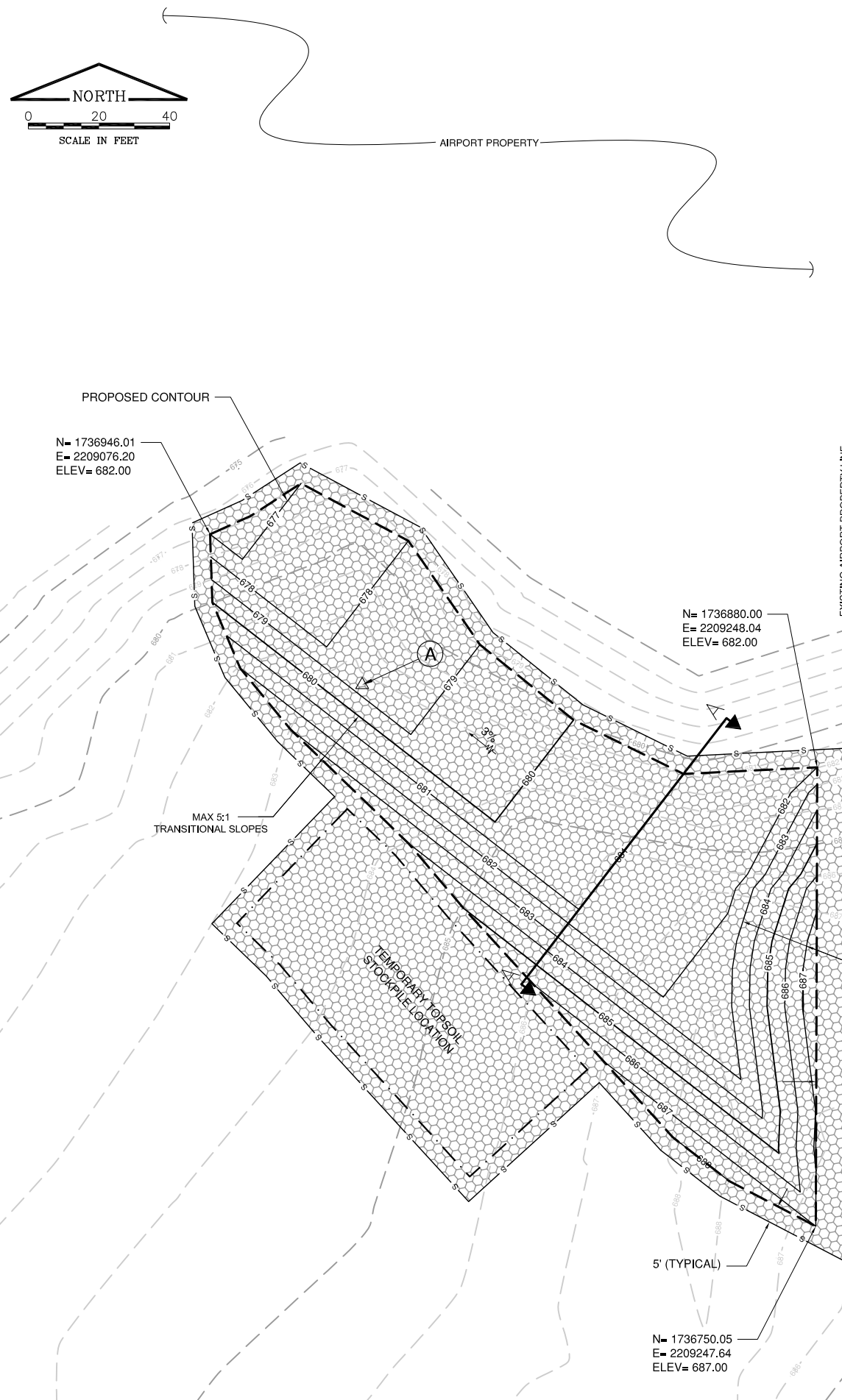
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STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF OBSTRUCTION REMOVAL AT THE QUAD CITY INTERNATIONAL AIRPORT. THE PROJECT INCLUDES GRADING, EXCAVATION, TOPSOIL PLACEMENT, TREE REMOVAL, LANDSCAPING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

1. PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL, SUCH AS PERIMETER SILT FENCE AND INLET PROTECTION.
2. EXCAVATION WILL BE COMPLETED WITHIN THE PROJECT LIMITS.
3. TREE REMOVAL.
4. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
5. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCHING.
6. REMOVAL OF TEMPORARY EROSION CONTROL / PROTECTION FACILITIES.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 30 ACRES OF WHICH 25 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE ROCK RIVER VIA OVERLAND FLOW, CASE AND COAL CREEKS.

CONTROLS - EROSION CONTROLS AND SEDIMENT CONTROL

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION

1. THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS POSSIBLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.
2. AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLANS OR AS DIRECTED BY THE ENGINEER.
3. THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
- B. CONSTRUCT DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.
- C. BUILD NECESSARY EMBANKMENT AT CULVERT/STORM SEWER LOCATIONS AND THEN EXCAVATE AND PLACE PIPE.
- D. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.

4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

5. THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2 INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD.

6. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

7. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

ALLOWABLE NON-STORM WATER DISCHARGES:

1. DISCHARGES FROM FIRE-FIGHTING ACTIVITIES;
2. FIRE HYDRANT FLUSHINGS;
3. WATER USED TO CONTROL DUST;
4. POTABLE WATER INCLUDING UNCONTAMINATED WATER LINE FLUSHINGS;
5. UNCONTAMINATED GROUND WATER OR SPRING WATER;
6. UNCONTAMINATED EXCAVATION DEWATERING;
7. LANDSCAPE IRRIGATION;

RUNOFF COEFFICIENT AFTER CONSTRUCTION = 0.50

MAINTENANCE:

WHEN REQUESTED BY THE CONTRACTOR, THE RESIDENT ENGINEER WILL PROVIDE GENERAL MAINTENANCE GUIDES TO THE CONTRACTOR FOR THE PRACTICES ASSOCIATED WITH THIS PROJECT. THE FOLLOWING ADDITIONAL PROCEDURES WILL BE USED TO MAINTAIN, IN GOOD AND EFFECTIVE OPERATING CONDITIONS, THE VEGETATION, EROSION AND SEDIMENT CONTROL MEASURES AND OTHER PROTECTIVE MEASURES IDENTIFIED IN THIS PLAN. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ATTAIN MAINTENANCE GUIDELINES FOR ANY MANUFACTURED BMP'S WHICH ARE TO BE INSTALLED AND MAINTAINED PER MANUFACTURE'S SPECIFICATIONS.

1. SEEDING - WHERE TEMPORARY CESSATION OF THE EARTH DISTURBING ACTIVITIES OCCURS, STABILIZATION OF DISTURBED AREAS MUST BE INITIATED WITHIN 1 WORKING DAY IF WORK WILL NOT RESUME FOR A PERIOD EXCEEDING 14 DAYS.
2. PERIMETER EROSION BARRIER - SEDIMENT WILL BE REMOVED IF THE INTEGRITY OF THE FENCING IS IN JEOPARDY AND ANY FENCE KNOCKED DOWN WILL BE REPAIRED IMMEDIATELY.
3. DITCH CHECK - SEDIMENT WILL BE REMOVED IF THE INTEGRITY OF THE DITCH CHECK IS IN JEOPARDY. ANY DITCH CHECKS WHICH FAILS WILL BE REPAIRED OR REPLACED IMMEDIATELY.
4. INLET PROTECTION/FILTERS - SEDIMENT WILL BE REMOVED IF THE INTEGRITY OF THE INLET PROTECTION IS IN JEOPARDY. ANY INLET PROTECTION FILTERS WHICH FAILS WILL BE REPLACED IMMEDIATELY.

INSPECTIONS:

QUALIFIED PERSONNEL SHALL INSPECT DISTURBED AREAS OF THE CONSTRUCTION SITE WHICH HAVE NOT YET BEEN FINALLY STABILIZED. STRUCTURAL CONTROL MEASURES, AND LOCATIONS WHERE VEHICLES AND EQUIPMENT ENTER AND EXIT THE SITE USING IDOT STORM WATER POLLUTION PREVENTION PLAN EROSION CONTROL INSPECTION REPORT (BC 2259). SUCH INSPECTIONS SHALL BE CONDUCTED AT LEAST ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN TWENTY-FOUR (24) HOURS OF THE END OF A STORM OR BY THE END OF THE FOLLOWING BUSINESS OR WORK DAY THAT IS 0.5 INCH OR GREATER OR EQUIVALENT SNOWFALL.

INSPECTIONS MAY BE REDUCED TO ONCE PER MONTH WHEN CONSTRUCTION ACTIVITIES HAVE CEASED DUE TO FROZEN CONDITIONS. WEEKLY INSPECTIONS WILL RECOMMENCE WHEN CONSTRUCTION ACTIVITIES ARE CONDUCTED, OR IF THERE IS 0.5" OR GREATER RAIN EVENT, OR A DISCHARGE DUE TO SNOWMELT OCCURS. IF ANY VIOLATION OF THE PROVISIONS OF THIS PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION WORK COVERED BY THIS PLAN, THE RESIDENT ENGINEER SHALL NOTIFY THE APPROPRIATE IEPA FIELD OPERATIONS SECTION OFFICE BY EMAIL AT: EPA.SWNONCOMP@ILLINOIS.GOV. TELEPHONE OR FAX WITHIN TWENTY-FOUR (24) HOURS OF THE INCIDENT. THE RESIDENT ENGINEER SHALL THEN COMPLETE AND SUBMIT AN "INCIDENCE OF NON-COMPLIANCE" (ION) REPORT FOR THE IDENTIFIED VIOLATION WITHIN FIVE (5) DAYS OF THE INCIDENT. THE RESIDENT ENGINEER SHALL USE FORMS PROVIDED BY IEPA AND SHALL INCLUDE SPECIFIC INFORMATION ON THE CAUSE OF NONCOMPLIANCE, ACTIONS WHICH WERE TAKEN TO PREVENT ANY FURTHER CAUSES OF NONCOMPLIANCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NON-COMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI. G OF THE PERMIT ILR10. THE INCIDENCE OF NON-COMPLIANCE SHALL BE MAILED TO THE FOLLOWING ADDRESS:

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY
DIVISION OF WATER POLLUTION CONTROL
ATTN: COMPLIANCE ASSURANCE SECTION
1021 NORTH GRAND EAST
POST OFFICE BOX 19276
SPRINGFIELD, ILLINOIS 62794-9276
ADDITIONAL INSPECTIONS REQUIRED:

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

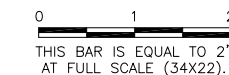
TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED. ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

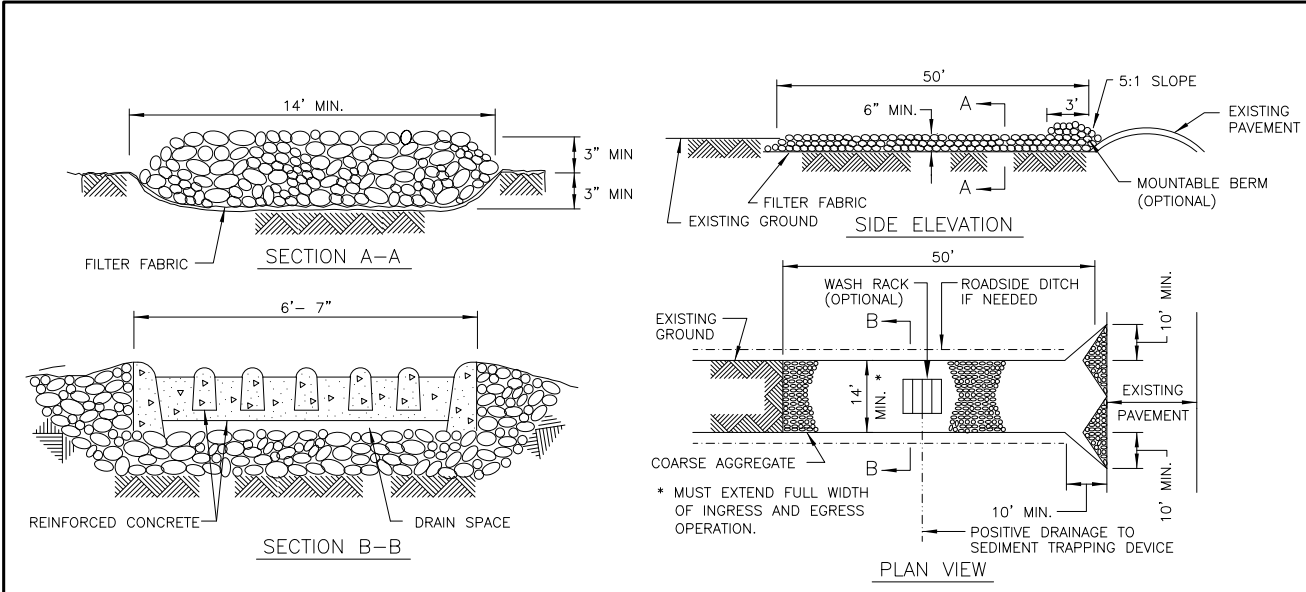
QU021

REVISIONS		
NUMBER	BY	DATE



**QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

**OBSTRUCTION REMOVAL - PHASE 1
STORMWATER POLLUTION PREVENTION PLAN
NOTES AND DETAILS**



STABILIZED CONSTRUCTION ENTRANCE
FROM NRCS STANDARD DRAWING NO. IL-630

STABILIZED CONSTRUCTION ENTRANCE NOTES

1. FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED UNDER SECTION 1080.03. OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.
2. ROCK OR RECLAIMED CONCRETE SHALL MEET ONE OF THE FOLLOWING IDOT COARSE AGGREGATE GRADATION, CA-1, CA-2, CA-3 OR CA-4. COMPACTION SHALL BE TO THE SATISFACTION OF THE ENGINEER.
3. ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHALL BE CONSTRUCTED ACCORDING TO MANUFACTURERS SPECIFICATIONS AND SHALL BE INCIDENTAL TO THE CONTRACT.
4. MINIMUM WIDTH IS 14' FOR ONE-WAY TRAFFIC AND 20' FOR TWO WAY TRAFFIC. TWO-WAY TRAFFIC WIDTHS SHALL BE INCREASED A MINIMUM OF 4' FOR TRAILER TRAFFIC. DEPENDING ON THE TYPE OF VEHICLE OR EQUIPMENT, SPEED, LOADS, CLIMATIC AND OTHER CONDITIONS UNDER WHICH VEHICLES AND EQUIPMENT OPERATE AN INCREASE IN THE MINIMUM WIDTHS MAY BE REQUIRED.
5. ROADWAY SHALL FOLLOW THE CONTOUR OF THE NATURAL TERRAIN TO THE EXTENT POSSIBLE.
6. IF WASH RACK ARE USED THEY SHALL BE INSTALLED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS.
7. THE STABILIZED CONSTRUCTION ENTRANCE INSTALLATION, MAINTENANCE AND REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.
8. SEE SHEET 3 FOR ENTRANCE LOCATION.

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DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	CAL
APPROVED BY:	CAL
DATE:	06/02/2015
JOB No:	13014-06
IL PROJ. NO.	MLI-4463
AIP PROJ. NO.	3-17-0068-XX
SHEET	11 OF 11 SHEETS