

FAU ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1624	08-00122-00-RS	COOK	10	1
ILLINOIS PROJECT				

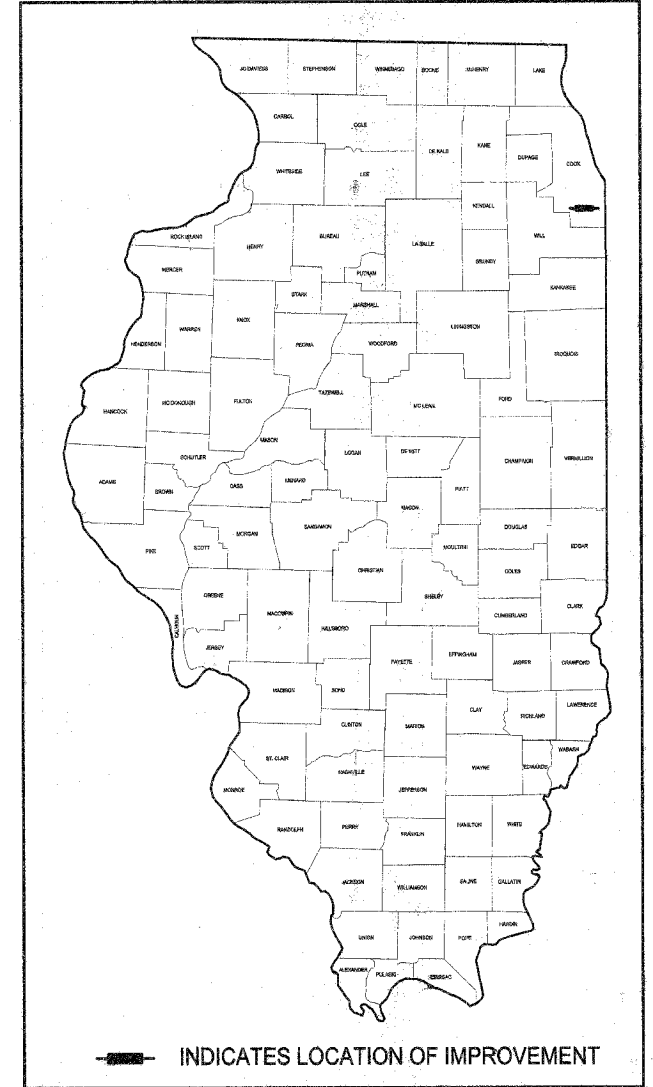
CONTRACT NO.: 63051

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

**PLANS FOR PROPOSED  
FEDERAL AID PROJECT  
FAU 1624 / 187th STREET  
FROM RIEGEL ROAD (FAU 2860) TO HALSTED STREET (FAP 876)  
SECTION : 08-00122-00-RS  
PROJECT NO.: M-8003(970)  
COOK COUNTY  
JOB NO.: C-91-331-08**

**INDEX OF SHEETS**

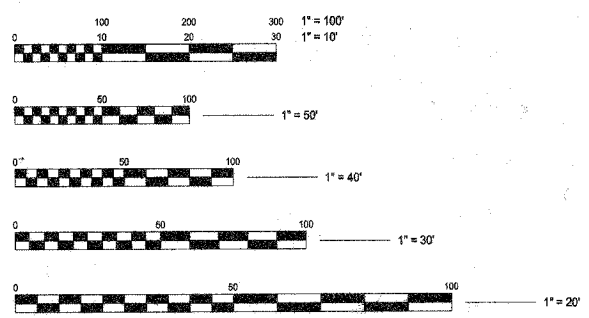
SHEET NO.	TITLE
1	COVER SHEET, INDEX & LOCATION MAP
2	SUMMARY OF QUANTITIES AND GENERAL NOTES
3	EXISTING TYPICAL SECTIONS
4	PROPOSED TYPICAL SECTIONS
5	187th STREET (FAU ROUTE 1624) EXISTING ROADWAY ROADWAY AND PAVEMENT MARKING PLAN, STA. 0+00 STA. 24+00
6	187th STREET (FAU ROUTE 1624) EXISTING ROADWAY ROADWAY AND PAVEMENT MARKING PLAN, STA. 24+00 STA. 48+00
7	IDOT DISTRICT ONE TYPICAL PAVEMENT MARKING DETAIL
8	IDOT BUTT JOINT & BITUMINOUS TAPER DETAIL
9	CURB AND GUTTER REMOVAL AND REPLACEMENT
10	IDOT DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS



INDICATES LOCATION OF IMPROVEMENT

**STANDARD NO.**

SHEET NO.	TITLE
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
606001-03	CONCRETE CURB TYPE-B AND COMBINATION CONCRETE CURB & GUTTER
701501-04	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701801-03	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901	TRAFFIC CONTROL DEVICES

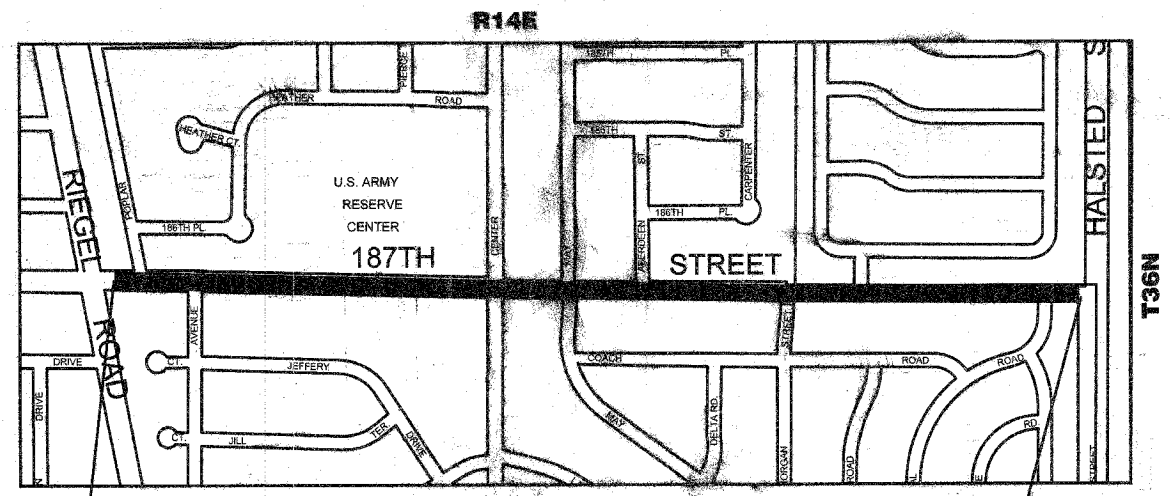


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED SIZE PLANS, THE ABOVE SCALES SHALL BE USED.



CALL JULIE (800) 892-0123 WITH THE FOLLOWING INFORMATION:  
COUNTY = COOK  
48 HOURS BEFORE YOU DIG  
EXCLUDING SAT., SUN. & HOLIDAYS

PROJECT LOCATED IN THE VILLAGE OF HOMEWOOD



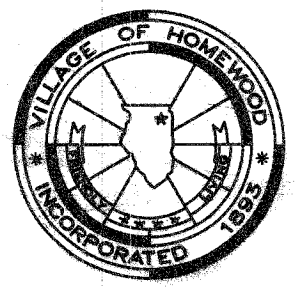
LOCATION: 187th STREET BEGIN STA. 0+51.09

LOCATION: 187th STREET END STA. 45+82.39

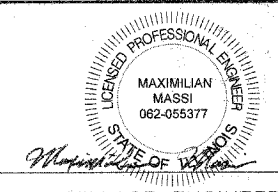
**LOCATION MAP**  
NOT TO SCALE

TOTAL NET & GROSS LENGTH OF PROJECT = 4,531 LINEAL FEET (0.858 MILES)

187th STREET DESIGN DESIGNATION  
POSTED SPEED: 25 MPH  
ADT: 3,930 (1998)  
COLLECTOR



VILLAGE OF HOMEWOOD  
Department of Public Works  
Engineering Division  
17755 S. Ashland Avenue  
Homewood, IL 60445  
(708) 206-3470



VILLAGE ENGINEER  
" LICENSE EXPIRES 11-30-09 "

DATE: 5-14-08

<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS</b>	
APPROVED	<u>May 14 2008</u> <i>John A. Hoff</i> VILLAGE PRESIDENT
PASSED	<u>MAY 22 2008</u> <i>Christopher Holt</i> DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS
RELEASING FOR BID -BASED ON LIMITED REVIEW	<u>MAY 22 2008</u> <i>Diana M. O'Keefe</i> DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

FIELD ENGINEER: MEL MANGOBA, (847) 705-4408

CONTRACT NO.: 63051

FAU ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1624	08-00122-00-RS	COOK	10	2
ILLINOIS PROJECT #				

## GENERAL NOTES

CONTRACT NO.: 63051

## SUMMARY OF QUANTITIES

ITEM NO.	PAY ITEM	UNIT	1000 TOTAL
20101300	TREE PRUNING (1 to 10 INCH DIAMETER)	EA	10
21101815	TOPSOIL FURNISH AND PLACE, 4"	SY	121
25000400	NITROGEN FERTILIZER NUTRIENT	LBS	4
25000500	PHOSPHORUS FERTILIZER NUTRIENT	LBS	4
25000600	POTASSIUM FERTILIZER NUTRIENT	LBS	4
25200110	SODDING, SALT TOLERANT	SY	121
25200200	SUPPLEMENTAL WATERING	UNIT	5
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GAL	2,025
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	825
40600895	CONSTRUCTING TEST STRIP	EACH	1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SY	1064
40603310	HOT-MIX ASPHALT, SURFACE COURSE, MIX "C", N50	TON	1,650
42400800	DETECTABLE WARNINGS	SF	25
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SY	19,184
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FT	400
44300300	AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A	SY	20,248
60255700	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EA	12
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EA	16
60260410	INLETS TO BE ADJUSTED WITH NEW TYPE 2 FRAME AND GRATE	EA	2
60266600	VALVE BOXES TO BE ADJUSTED	EA	5
67100100	MOBILIZATION	LS	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LS	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LS	1
70300100	SHORT-TERM PAVEMENT MARKING	FT	1,400
* 78001100	PAINT PAVEMENT MARKING - LETTERS AND SYMBOLS	SF	110
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FT	1,800
* 78001130	PAINT PAVEMENT MARKING - LINE 6"	FT	132
* 78001150	PAINT PAVEMENT MARKING - LINE 12"	FT	318
* 78001180	PAINT PAVEMENT MARKING - LINE 24"	FT	70
* 88600800	DETECTOR LOOP REPLACEMENT	FT	341
XX001306	SIDEWALK REMOVAL AND REPLACEMENT	SF	1,625
XX003435	PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT	SY	25
XX006947	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	SY	25

\* SPECIALTY ITEM

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION," ADOPTED JANUARY 1, 2007 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS), THE LATEST EDITION OF THE "ILLINOIS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" AND THE MANUAL OF TEST PROCEDURES FOR MATERIALS IN EFFECT ON THE DATE OF INVITATION FOR BID, THE LATEST EDITION OF THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" AND THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" INDICATED ON THE CHECK SHEET INCLUDED HEREIN WHICH APPLY TO AND GOVERN THE CONSTRUCTION OF THE LOCAL AREA PAVEMENT PRESERVATION PROGRAM, FAU ROUTE 1624, SECTION 08-00122-00-RS, PROJECT NO. M-, COOK COUNTY, ILLINOIS AND IN CASE OF CONFLICT WITH ANY PART OR PARTS OF SAID SPECIFICATIONS, THE SAID SPECIAL PROVISIONS SHALL TAKE PRECEDENCE AND GOVERN.

ALL REFERENCES TO THE "VILLAGE" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE VILLAGE OF HOMEWOOD.

THE FINISHED HMA SURFACE SHALL BE CONSTRUCTED 1/4" INCH ABOVE THE GUTTER FLAG.

THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE VILLAGE AND THE ENGINEER DO NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING THE CONSTRUCTION OPERATION SO AS NOT TO DAMAGE THEM. IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 105.07 OF THE "STANDARD SPECIFICATIONS," THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED, OR MOVED, IF NECESSARY, PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE "STANDARD SPECIFICATIONS."

THOSE EXISTING TRAFFIC SIGNS WHICH ARE SO DESIGNATED BY THE ENGINEER SHALL BE REMOVED, STORED AND SUBSEQUENTLY RELOCATED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE VILLAGE. IN ADDITION, ANY SIGNS WHICH ARE DAMAGED BEYOND REPAIR BY THE CONTRACTOR AS DETERMINED BY THE ENGINEER SHALL BE REPLACED IN KIND BY THE CONTRACTOR AND TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE VILLAGE.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE PLANS AND THE IDOT STANDARDS FOR TRAFFIC CONTROL AND PROTECTION.

WHERE THE PROPOSED PAVEMENT ABUTS EXISTING PAVEMENT TO REMAIN IN PLACE (BEGIN, END AND LIMITS OF CONSTRUCTION), EXISTING DRIVEWAY PAVEMENT, SIDEWALK TO REMAIN IN PLACE. THE EXISTING PAVEMENT SHALL BE SAW CUT TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING SURFACES. THIS SAW CUT SHALL BE INCLUDED IN THE COST OF THE PAY ITEM "HMA SURFACE REMOVAL - BUTT JOINT".

CURB & GUTTER, DRIVEWAY AND SIDEWALK REMOVAL AND REPLACEMENT SHALL BE LIMITED TO WORKING ON ONE SIDE OF THE STREET IN EACH DIRECTION AT ALL TIMES TO MINIMIZE CONGESTION. NO CURB & GUTTER SHALL BE REMOVED ON FRIDAYS. CONCRETE SHALL HAVE A MINIMUM OF 72 HOURS CURING TIME AND DRIVEWAYS SHALL BE ACCESSIBLE PRIOR TO REMOVING EXISTING CURB ON THE OTHER SIDE OF THE STREET.

ALL FRAME AND GRATES TO BE REPLACED SHALL REMAIN THE PROPERTY OF THE VILLAGE OF HOMEWOOD AND SHALL BE DELIVERED TO THE PUBLIC WORKS BUILDING BY THE CONTRACTOR.

THE CONTRACTOR(S) AND THEIR SUBCONTRACTOR(S) SHALL CONTACT J.U.L.I.E. (800) 892-0123 AND THE VILLAGE OF HOMEWOOD/DEPARTMENT OF PUBLIC WORKS (708) 208-3470 BEFORE DIGGING.

THE CONTRACTOR SHALL NOTIFY RESIDENTS 24 HOURS PRIOR TO BEGINNING WORK SO THAT CARS CAN BE REMOVED FROM DRIVEWAYS. WORK MUST BE PLANNED TO MINIMIZE THE TIME DRIVEWAYS ARE OUT OF SERVICE. WORK SHALL BE SCHEDULED SO THAT DRIVES PULLED ON THE FIRST WORKING DAY ARE FORMED AND POURED BY THE THIRD WORKING DAY. BEYOND THIS 3 DAY TIME IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE AT HIS OWN EXPENSE TEMPORARY AGGREGATE FOR ACCESS TO THE DRIVEWAY. NO CURB IN FRONT OF DRIVE SHALL BE PULLED ON FRIDAYS.

DEBRIS REMOVAL - MATERIALS RESULTING FROM THE VARIOUS CONSTRUCTION OPERATIONS SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGEMENT OF THE VILLAGE, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE VILLAGE WILL REMOVE DEBRIS AND THE CONTRACTOR SHALL BE BILLED ACCORDINGLY.

WATER SUPPLY - THE CONTRACTOR CAN OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A WATERING BAN IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR. WATER IS AVAILABLE AT 17765 S. ASHLAND AVENUE, MUNICIPAL SERVICE CENTER FIRE HYDRANT. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER. THE SUPPLY OF WATER BY THE CONTRACTOR SHALL BE INCLUDED IN THE COST OF SUPPLEMENTAL WATERING.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 40 MM (1 1/2 INCHES) WHERE THE SPEED LIMIT IS 80 KM/HR (45 MPH) OR LESS AND 25 MM (1 INCH) WHERE THE SPEED LIMIT IS GREATER THAN 80 KM/HR (45 MPH). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 75 MM (3 INCHES) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

ALL TREES SHALL BE SAVED. THE CONTRACTOR SHALL ERECT EITHER TEMPORARY FENCING OR TREE TRUNK PROTECTION AS DIRECTED BY THE ENGINEER PER ARTICLE 201.5 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY AND ALL DAMAGE PER ARTICLE 201.07 OF THE STANDARD SPECIFICATIONS. TREE PROTECTION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE WORK.

PROTECTIVE COAT SHALL BE APPLIED TO THE SURFACE OF ALL COMBINATION CONCRETE AND GUTTER AND PORTLAND CEMENT CONCRETE SIDEWALK AND DRIVEWAYS. THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 420.18 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE WORK TO WHICH IT IS APPLIED.

THE QUANTITIES FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, CLASS D PATCHES OF THE TYPE AND SIZE SPECIFIED, SIDEWALK REMOVAL AND REPLACEMENT, PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT AND HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT SHALL NOT EXCEED THE QUANTITY LISTED IN THE SUMMARY OF QUANTITIES.

PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL PROVIDE TO THE VILLAGE A VIDEO TAPE COVERING FROM RIGHT-OF-WAY TO RIGHT-OF-WAY OF ALL ROADWAYS TO BE IMPROVED. THIS WORK SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

WHEN REMOVING PAVEMENT, CURB AND GUTTER, SIDEWALK, DRIVEWAY PAVEMENT, PARKING LOT PAVEMENT, AND/OR OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKER WHICH MIGHT DISTURB UNDERGROUND PUBLIC UTILITIES WILL NOT BE PERMITTED.

PRIOR TO THE START OF CONSTRUCTION ACTIVITIES, THE ENGINEER AND CONTRACTOR, JOINTLY, SHALL PERFORM A VISUAL INSPECTION OF VILLAGE DRAINAGE AND UTILITY STRUCTURES TO DETERMINE THE AMOUNT OF EXISTING DEBRIS IN EACH STRUCTURE. THE CONTRACTOR WILL BE REQUIRED TO CLEAN THOSE STRUCTURES WITH DEBRIS THAT WERE CLEAN AT THE BEGINNING OF CONSTRUCTION AT NO ADDITIONAL COST TO THE CONTRACT.

MILLING OF PAVEMENT SHALL BE DONE SO AS TO NOT DAMAGE THE ADJACENT CURB OR STRUCTURES. REMOVAL ADJACENT TO THESE STRUCTURES SHALL BE ACCOMPLISHED TO THE SATISFACTION OF THE ENGINEER AND MAY REQUIRE HANDWORK.



**VILLAGE OF HOMEWOOD  
ENGINEERING DIVISION**

DESIGNED BY:	D.J.A.
DRAWN BY:	D.J.A.
CHECKED BY:	M.M.

REVISIONS			
NO.	DATE	BY	DESCRIPTION
1	4-21-08	DJA	IDOT REVISIONS
2			

VILLAGE OF HOMEWOOD  
187th STREET L.A.P.P. PROJECT

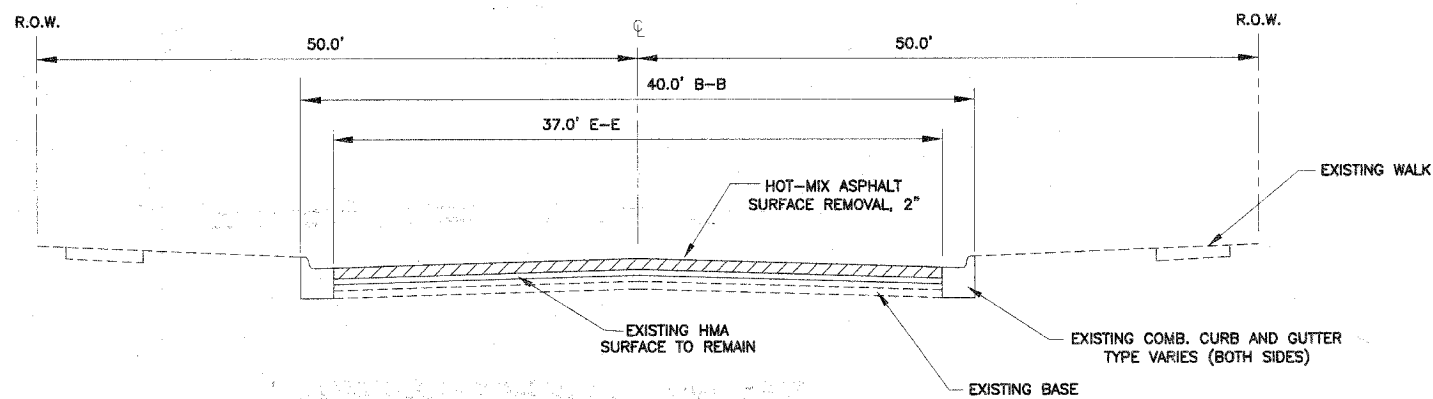
**SUMMARY OF QUANTITIES  
AND GENERAL NOTES**

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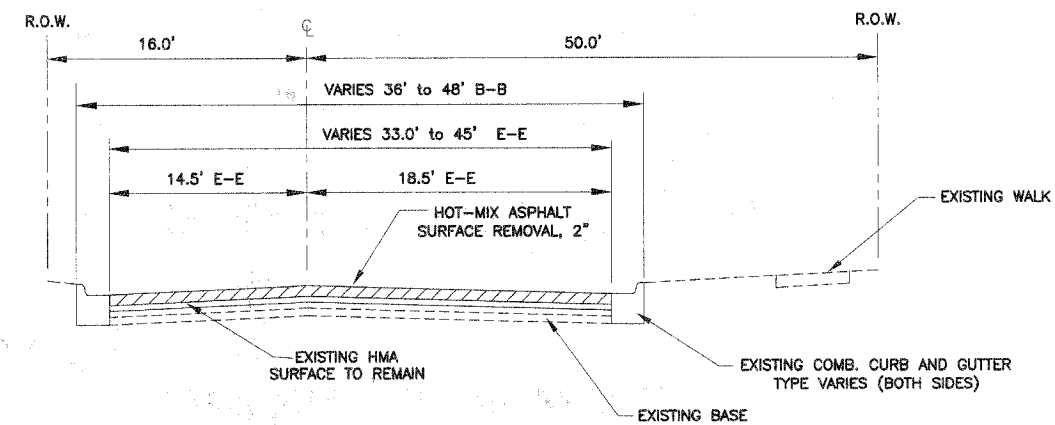
SHEET  
**2**  
OF 12

FAU ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1624	08-00122-00-RS	COOK	10	3

CONTRACT NO.: 63051



**EXISTING TYPICAL SECTION**  
STA. 00+51 to STA. 32+00



**EXISTING TYPICAL SECTION**  
STA. 32+00 to STA. 45+82



**VILLAGE OF HOMEWOOD**  
ENGINEERING DIVISION

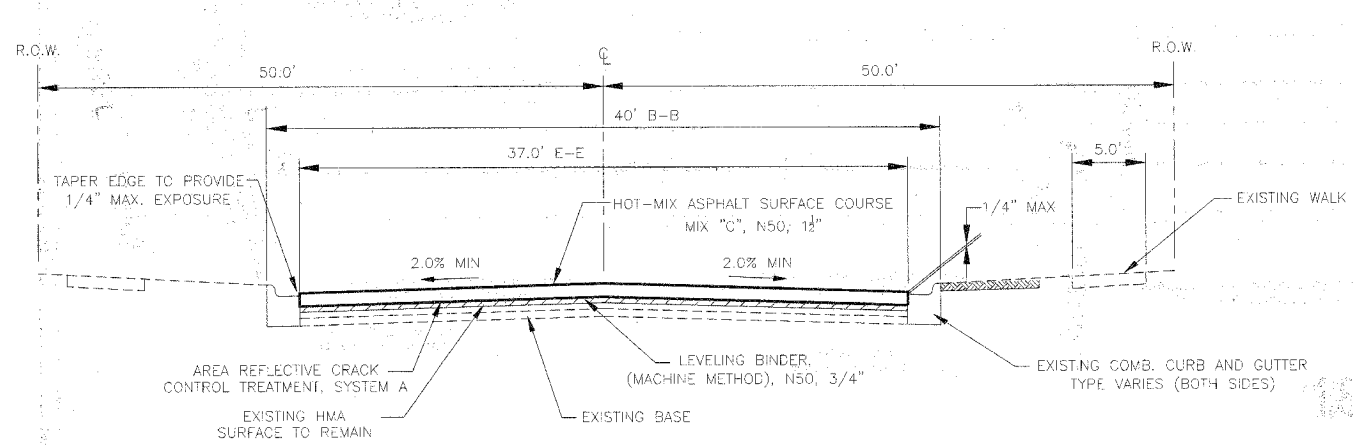
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CHECKED BY:	M.M.

REVISIONS			
NO.	DATE	BY	DESCRIPTION
1	-	-	-
2	-	-	-

VILLAGE OF HOMEWOOD  
187th STREET L.A.P.P. PROJECT  
**187th STREET (FAU ROUTE 1624)**  
EXISTING TYPICAL SECTIONS  
DATE: 01-15-08 SCALE: NOT TO SCALE

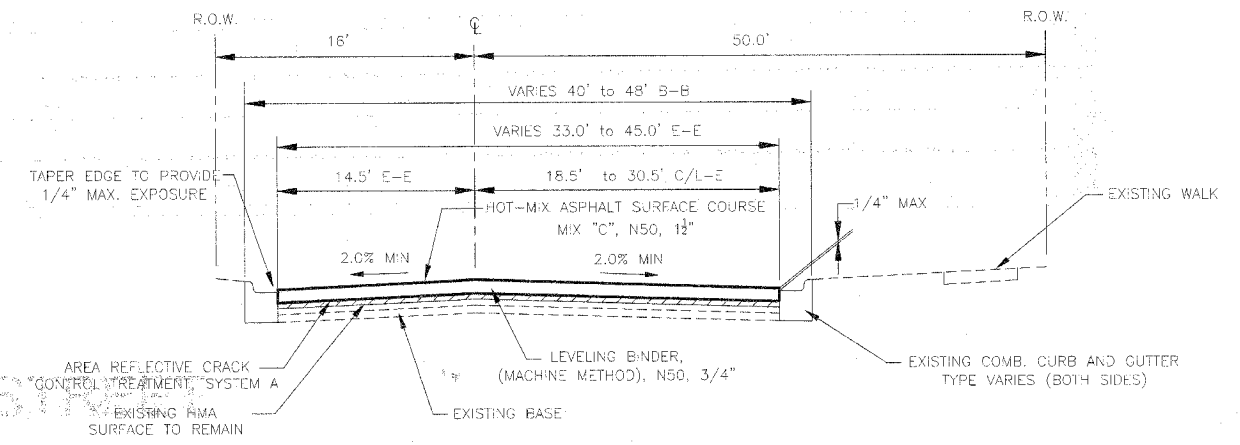
SHEET  
**3**  
OF 12

ILLINOIS PROJECT \*  
CONTRACT NO.: 63051



**PROPOSED TYPICAL SECTION**

STA. 00+51 to 32+00



**PROPOSED TYPICAL SECTION**

STA. 32+00 to STA. 45+82

**HOT-MIX ASPHALT MIXTURE TABLE**

ITEM	AC TYPE	VOIDS
LEVELING BINDER (MACHINE METHOD), N50 (IL-9.5mm)	PG 64-22/58-22*	4% @ 50 GYR
HOT-MIX ASPHALT SURFACE COURSE MIX "C", N50 (IL-9.5mm)	PG 64-22	4% @ 50 GYR
HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	PG 64-22	4% @ 50 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

\*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.



**VILLAGE OF HOMEWOOD**  
ENGINEERING DIVISION

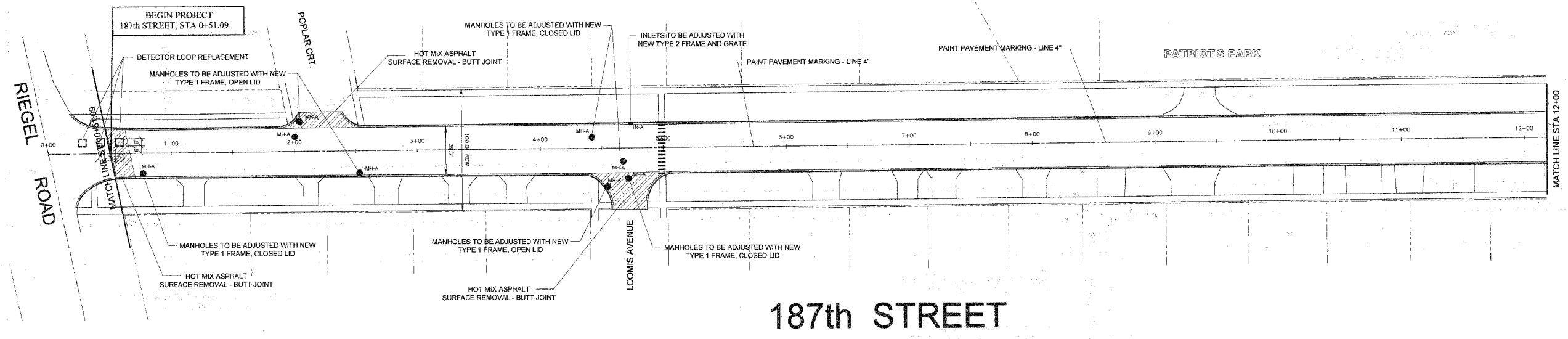
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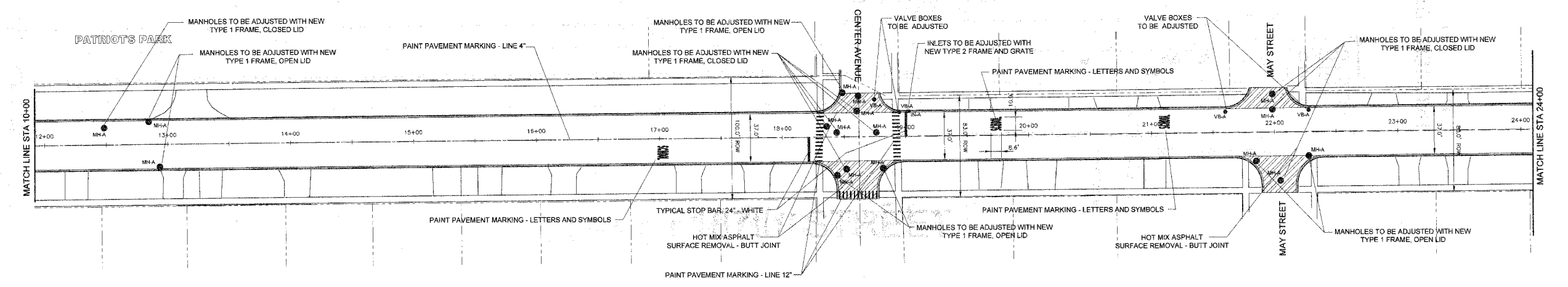
VILLAGE OF HOMEWOOD  
187th STREET L.A.P.P. PROJECT  
**187th STREET (FAU ROUTE 1624)**  
PROPOSED TYPICAL SECTIONS  
DATE: 01-15-08 SCALE: NOT TO SCALE

SHEET  
**4**  
OF 12

FAU ROUTE NO.	DISTRICT	COUNTY	TOTAL SHEETS	SHEET NO.
1624	08-00122-00-RS	COOK	10	5
ELIAB PROJECT				
CONTRACT NO.: 63051				



187th STREET



187th STREET



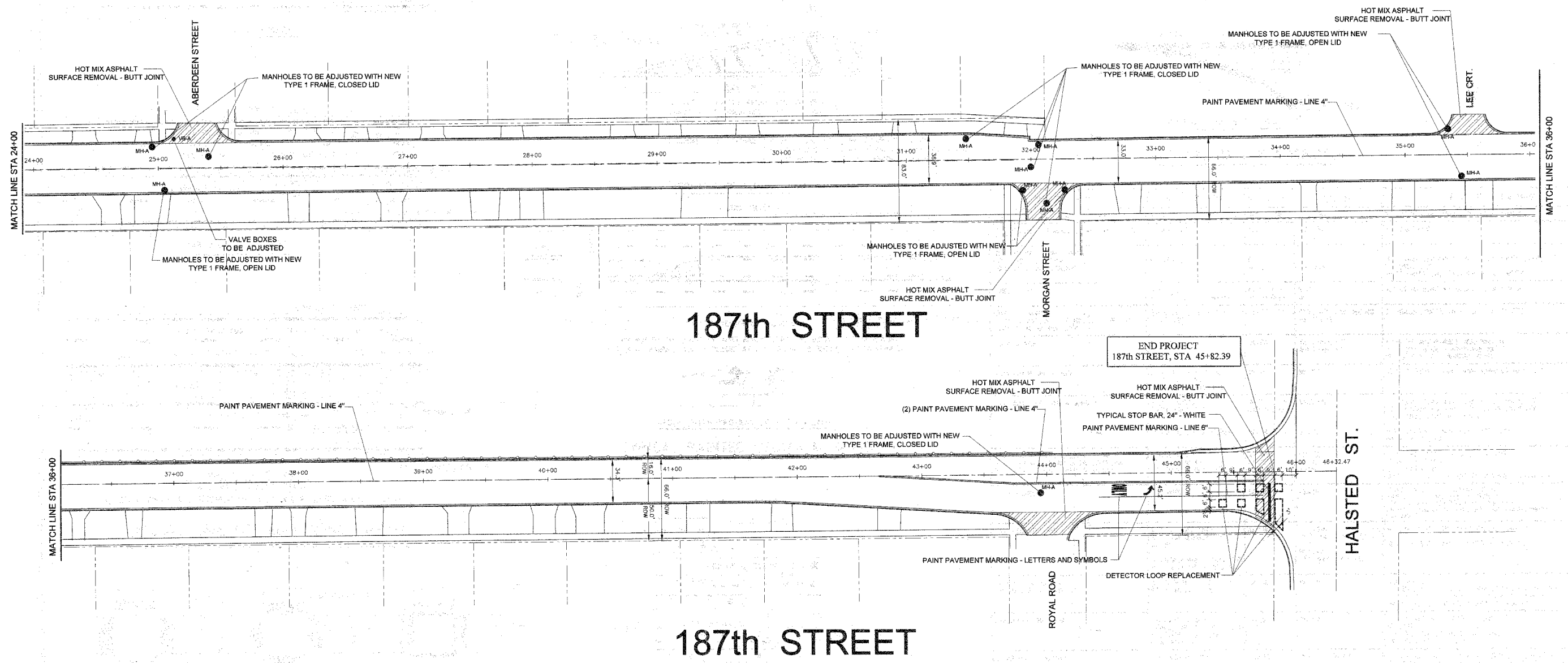
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VILLAGE OF HOMEWOOD  
187th STREET L.A.P.P. PROJECT  
187th STREET (FAU ROUTE 1624) EXISTING & PROPOSED  
ROADWAY AND PAVEMENT MARKING PLAN  
STA. 0+00 TO STA. 24+00  
DATE: 01-15-08 SCALE: 1" = 40'

SHEET  
**5**  
OF 12



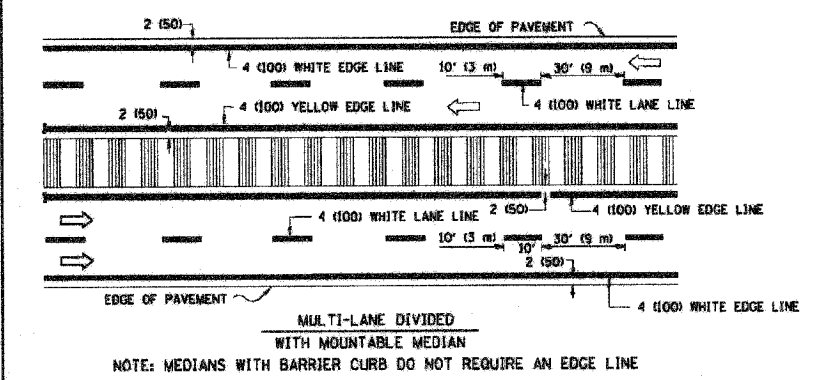
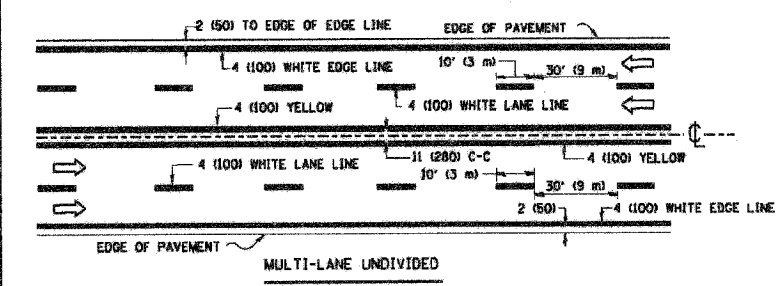
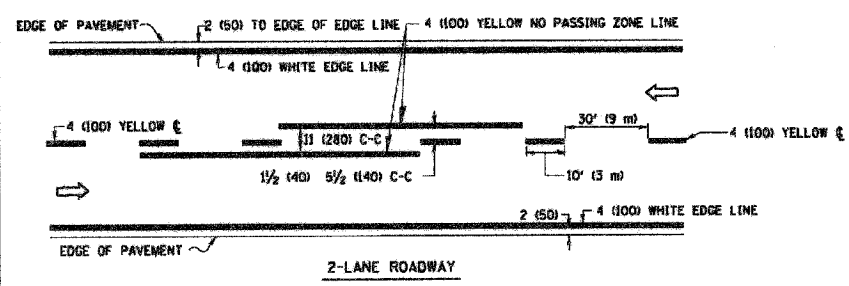
**VILLAGE OF HOMEWOOD  
ENGINEERING DIVISION**

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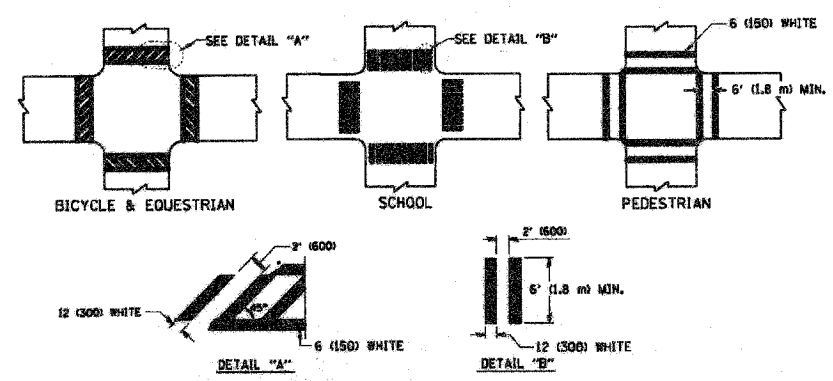
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VILLAGE OF HOMEWOOD  
187th STREET L.A.P.P. PROJECT  
**187th STREET (FAU ROUTE 1624) EXISTING & PROPOSED  
ROADWAY AND PAVEMENT MARKING PLAN  
STA. 24+00 TO STA. 46+00**  
DATE: 01-09-08 SCALE: 1" = 40'

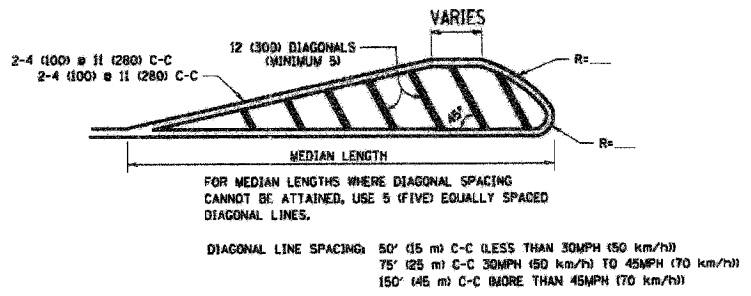
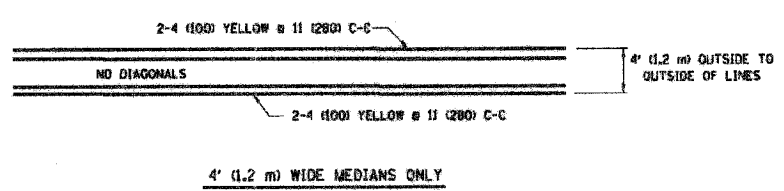
SHEET  
**6**  
OF 12



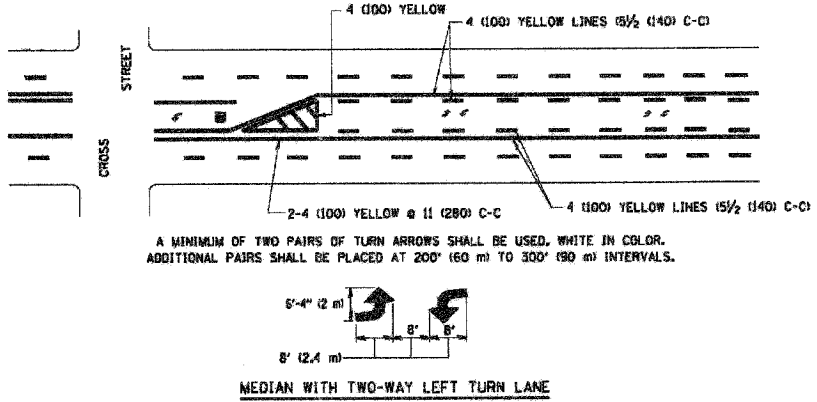
**TYPICAL LANE AND EDGE LINE MARKING**



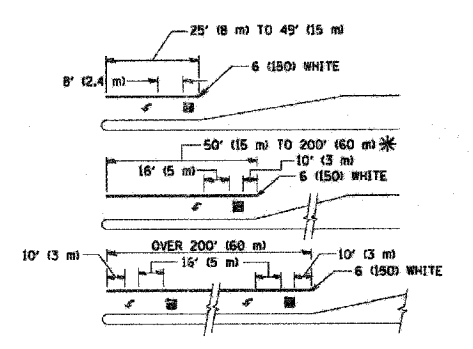
**TYPICAL CROSSWALK MARKING**



**MEDIANS OVER 4' (1.2 m) WIDE**



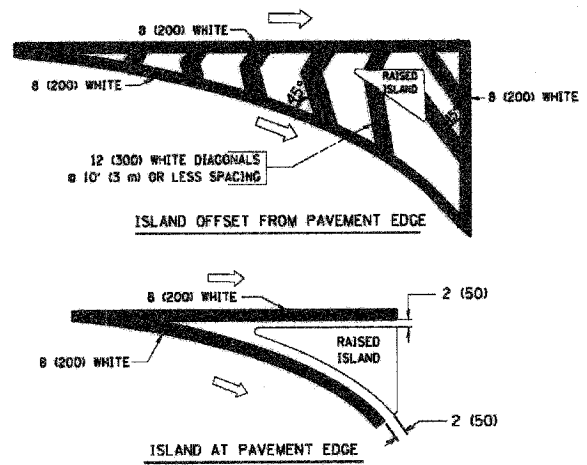
**TYPICAL PAINTED MEDIAN MARKING**



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 \* AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) \* AREA = 20.6 SQ. FT. (1.9 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**



**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
BOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN & EQUESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
ODR MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R" = 3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X" = 54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 78000.

All dimensions are in millimeters (inches) unless otherwise shown.

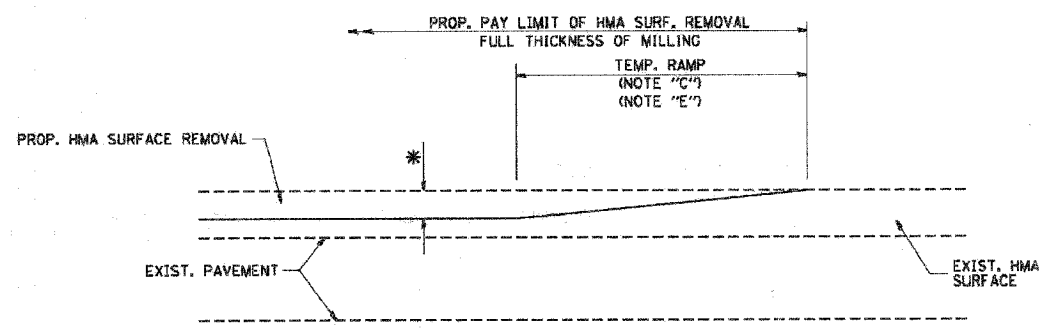
REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEL	10-09-98
ALEX HOUSEL	10-17-98
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DISTRICT ONE**  
**TYPICAL PAVEMENT MARKINGS**

SCALE: NONE  
 DATE: 1/17/2007  
 DRAWN BY CADD  
 CHECKED BY TC-13  
 REVISION DATE: 01/06/00

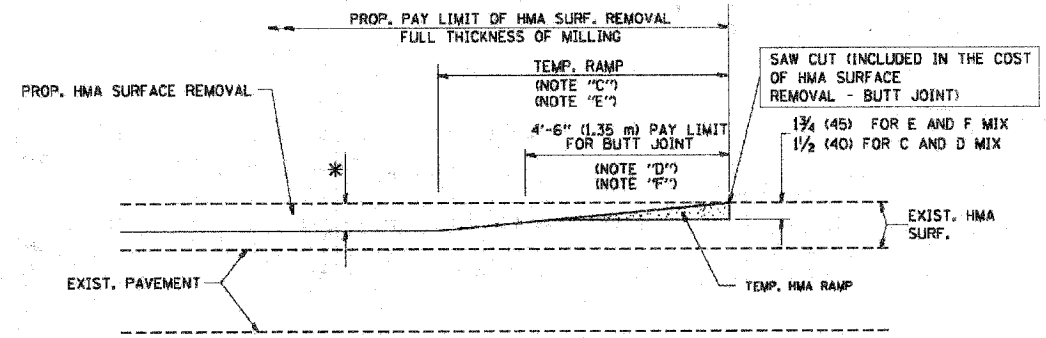
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 FILE NAME: 162408-0122-00-RS  
 PLOT SCALE: 1/8" = 1'-0"  
 USER: WHE

CONTRACT NO. -			
F.A. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
1624	08-00122-00-RS	COOK	10 8
STA.	TO STA.		
			FED. AID PROJECT



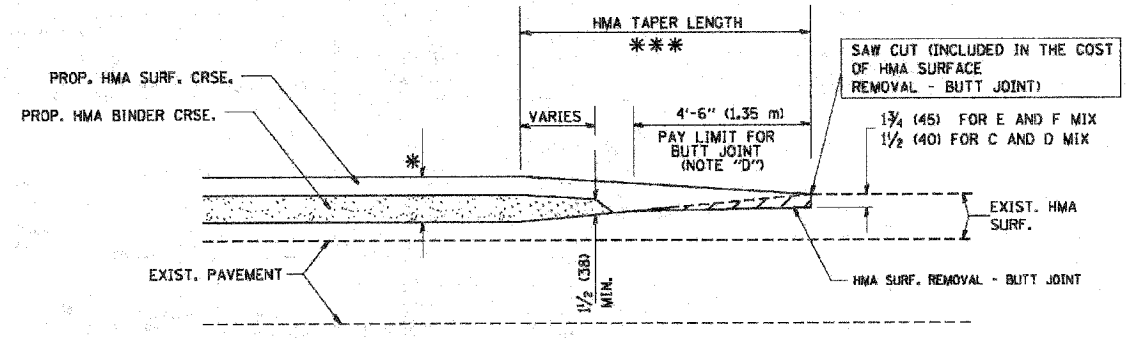
MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

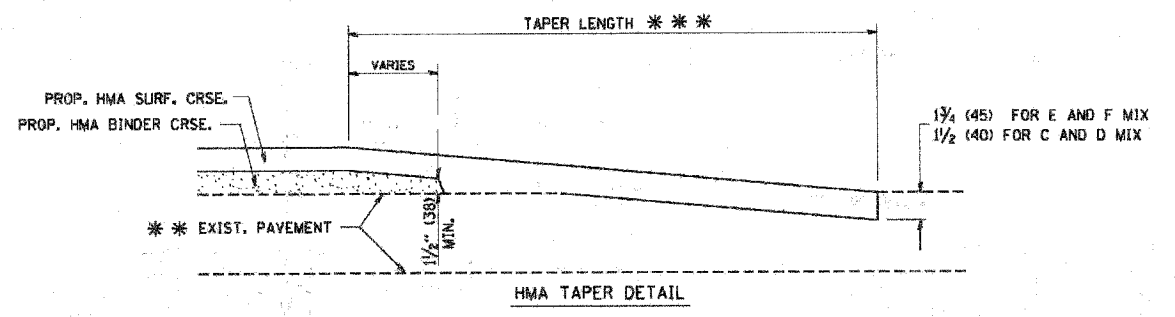
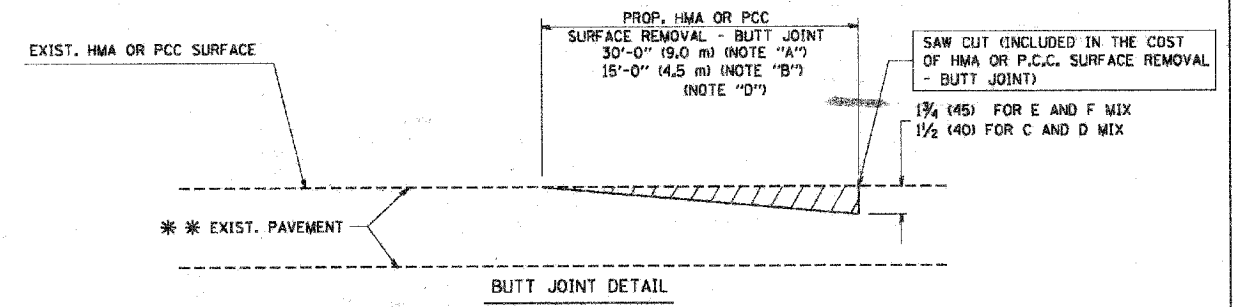


HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2  
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER  
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	5-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABRAS	03/21/97
M. GOMEZ	04/06/00
R. BORD	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: VERT. NONE  
HORIZ. NONE  
PLOT DATE: 1/18/2007

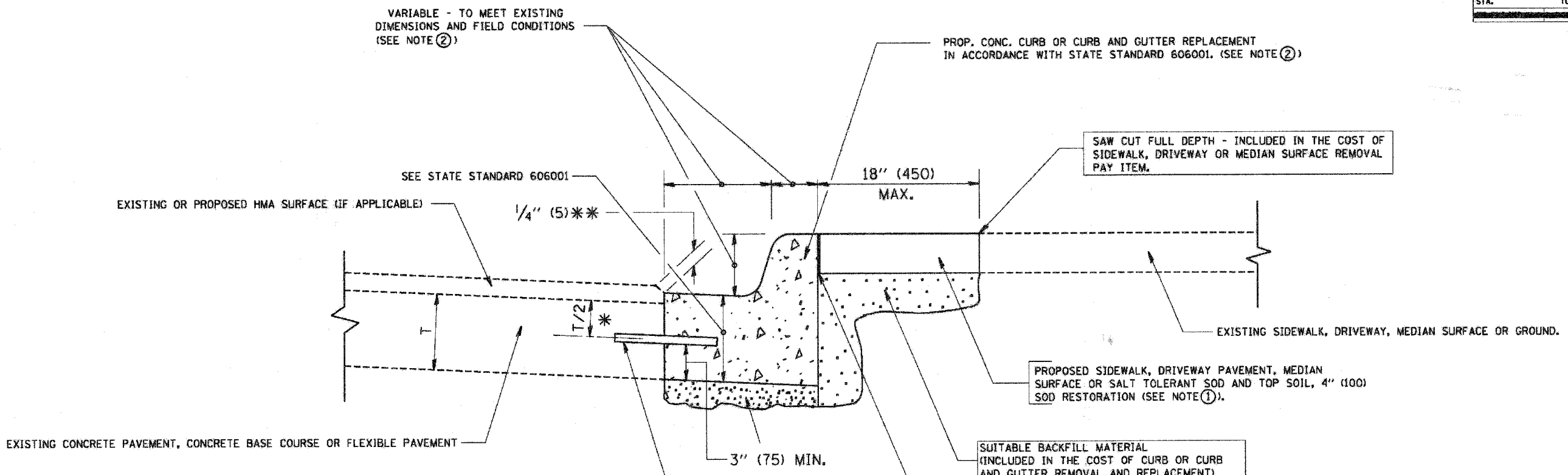
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CHECKED BY

BD400-05 (VI-BD32)  
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 USER NAME = dr1624



CONTRACT NO. -63051				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1824	08-00122-00-RS	COOK	10	9
STA.		TO STA.		FED. AID PROJECT



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \* \* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

- UNSATISFACTORY SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

## CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

REVISIONS	
NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	06/19/96
R. SHAH	06/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

**CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT**

SCALE: VERT. NONE  
HORIZ. 1/8"=1'-0"

PLT DATE: 1/18/2007

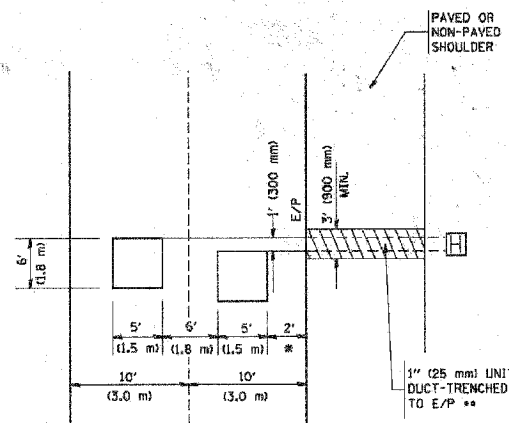
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CHECKED BY: \_\_\_\_\_  
BD600-06 (BD-24)  
REVISION DATE: 01/01/07

PLOT DATE = 1/18/2007  
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 USER = ...  
 USER = ...

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1624	08-00122-00-RS	COOK	10	10
STA.		TO STA.		
FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT  
NOTE WHICH SHOULD EQUAL  
3' (900 mm) X WIDTH OF  
PAVED SHOULDER.

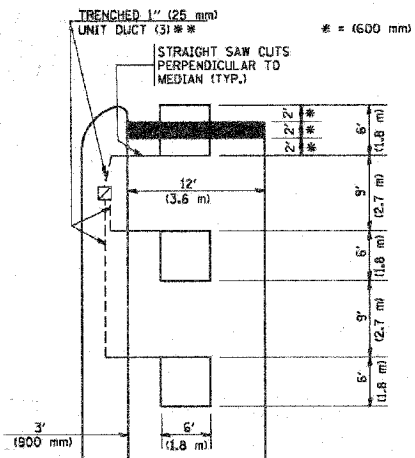


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS  
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)

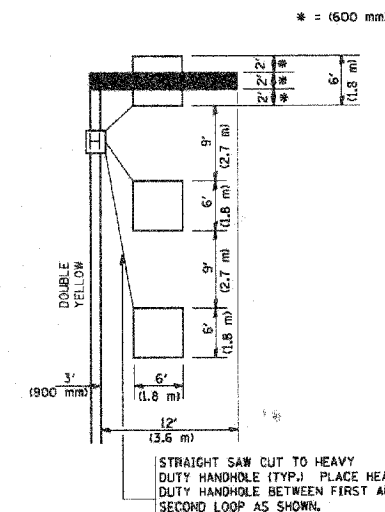
HANDHOLE LOCATION MAY  
VARY DEPENDING ON GEOMETRICS  
AND DESIGN OF TRAFFIC SIGNALS.  
HEAVY-DUTY HANDHOLES TO BE  
USED WHEN THE MEDIAN IS  
MOUNTABLE. REFER TO STANDARD  
814001 TO ENSURE THAT HANDHOLE  
FITS IN MEDIAN.



\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS  
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

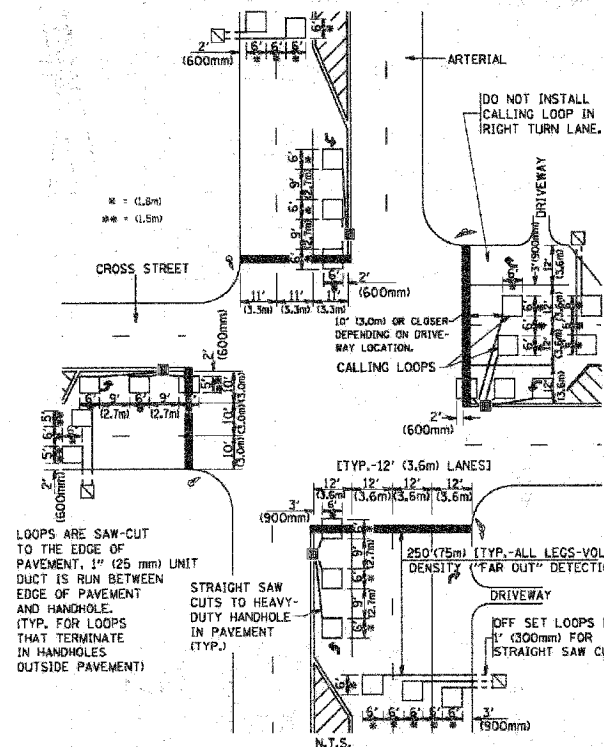
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO  
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)



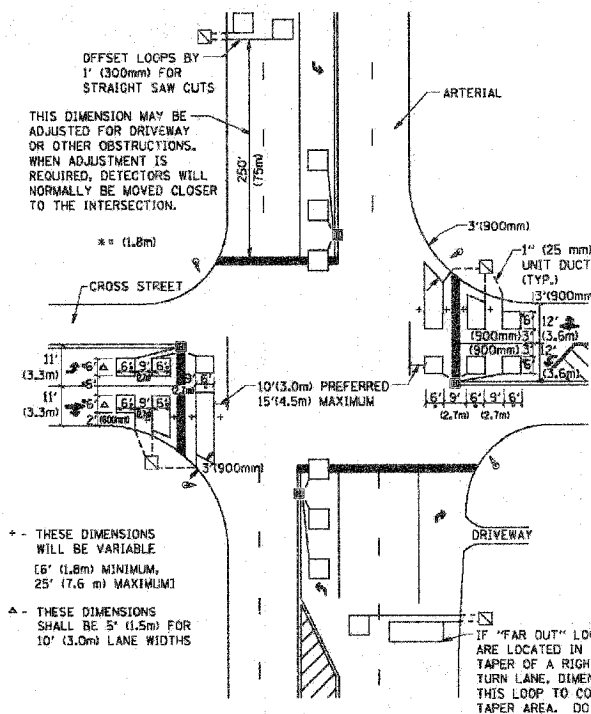
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO  
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1  
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2  
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTES:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING
NAME	DATE	
		DESIGNED BY
		DRAWN BY CADD
		CHECKED BY R.K.F.
		TS07
		REVISION DATE:

PLOT DATE = 2/15/2006  
FILE NAME = s:\projects\1624\1624.dwg  
PLOT SCALE = 1/8" = 1'-0"  
USER NAME = jplum