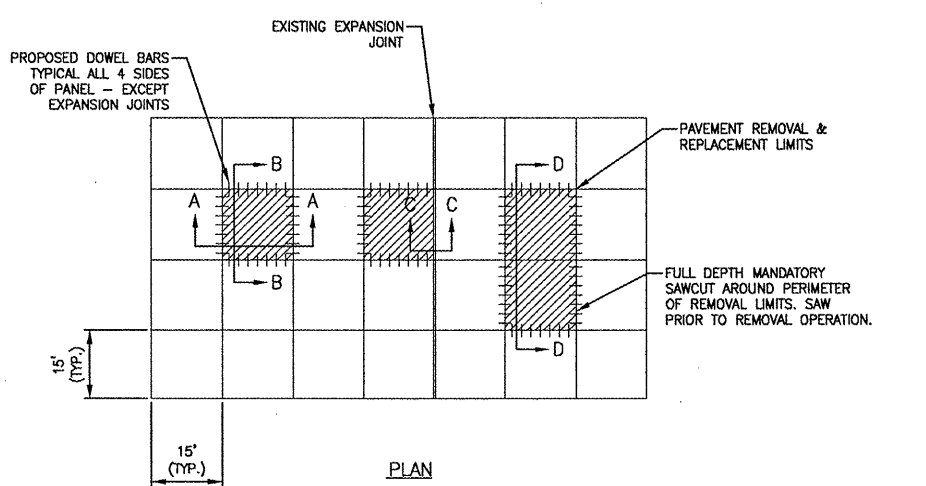
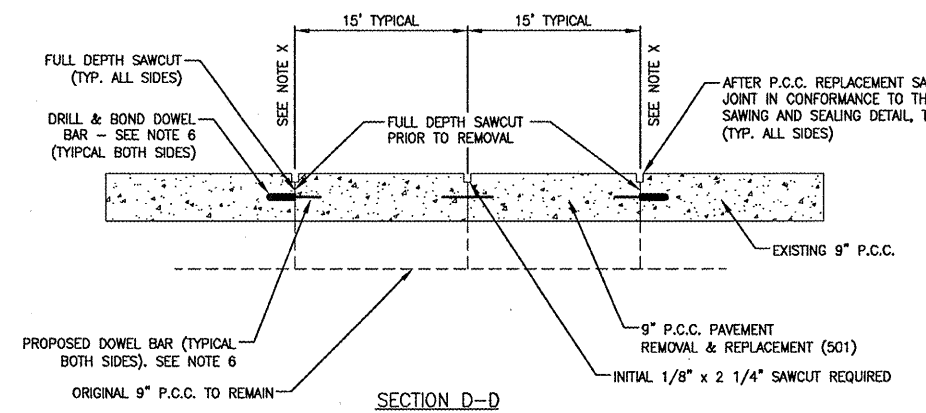
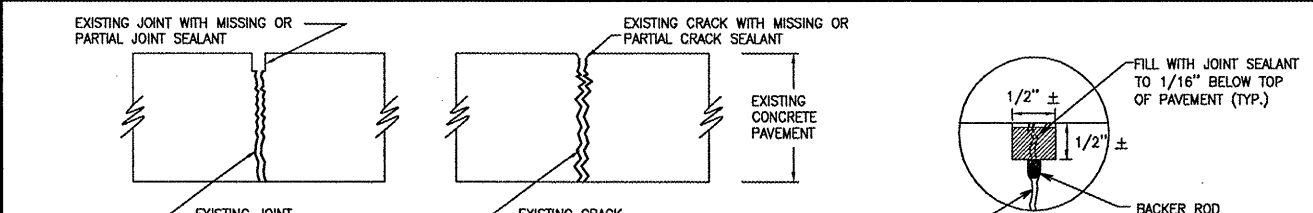


NOTE 'X':
THE CONTRACTOR SHALL SAWCUT 8" AT THE EXISTING PAVEMENT JOINTS THROUGH EXISTING DOWEL AND TIE BARS. THE CONTRACTOR SHALL DRILL AND BOND NEW DOWEL BARS IN THE EXISTING CONCRETE. THE NEW DOWELS SHALL BE 1" IN DIAMETER, 19" LONG PLACED AT 12" CENTERS. DOWEL BARS ARE REQUIRED REGARDLESS IF THE EXISTING JOINT HAS TIE BARS, DOWEL BARS, NO DOWELS SHALL BE INSTALLED AT EXPANSION JOINTS.

- NOTES:
- PANEL REMOVAL AND REPLACEMENT SHALL BE REQUIRED AT THE LOCATIONS SHOWN OR AS DESIGNATED BY THE RESIDENT ENGINEER.
 - THE RESIDENT ENGINEER SHALL DETERMINE THE ACTUAL LIMITS OF PANEL REMOVAL AND REPLACEMENT.
 - THE CONTRACTOR SHALL USE CARE TO NOT DAMAGE THE UNDERLYING P.C.C. PAVEMENT WHEN REMOVING THE TOP LAYER OF P.C.C. PAVEMENT. ANY DAMAGE CAUSED SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
 - THE JOINT PATTERN IN THE NEW PAVEMENT REPLACEMENT SHALL MATCH THAT OF THE ORIGINAL PAVEMENTS. JOINTS IN THE ORIGINAL PAVEMENT SHALL BE RE-SAWED AND SEALED IN THE NEW CONCRETE. SAWED JOINT DEPTHS AND DETAILS SHALL CONFORM TO THE JOINT SAWING AND SEALING DETAILS SHOWN ON SHEET 16. THE JOINT AROUND THE PERIMETER OF THE REPLACEMENT SHALL BE SAWED AND SEALED CONFORMING TO THIS SAME DETAIL.
 - A FULL DEPTH SAWCUT AROUND THE PERIMETER SHALL BE REQUIRED PRIOR TO PANEL REMOVAL AND REPLACEMENT.
 - DRILL HARDENED P.C.C. AND BOND DOWEL BARS IN PLACE USING A POLYESTER RESIN MATERIAL CONFORMING TO PORTLAND CEMENT CONCRETE PAVEMENT SECTION IN THE TECHNICAL SPECIFICATIONS.
 - IN THE EVENT THAT THE RESIDENT ENGINEER DETERMINES THAT THE UNDERLYING P.C.C. PAVEMENT IS FRACTURED OR IN A CONDITION SUCH THAT THERE IS THE POTENTIAL FOR REFLECTIVE CRACKING IN THE REPLACEMENT SURFACE P.C.C. LAYER, THE SURFACE P.C.C. LAYER SHALL BE REINFORCED WITH #5 BARS AT 8" O.C. E.W. THIS REINFORCEMENT, IF NECESSARY WILL BE PAID FOR UNDER ITEM AR801704 PCC PANEL REINFORCEMENT (SPECIAL).

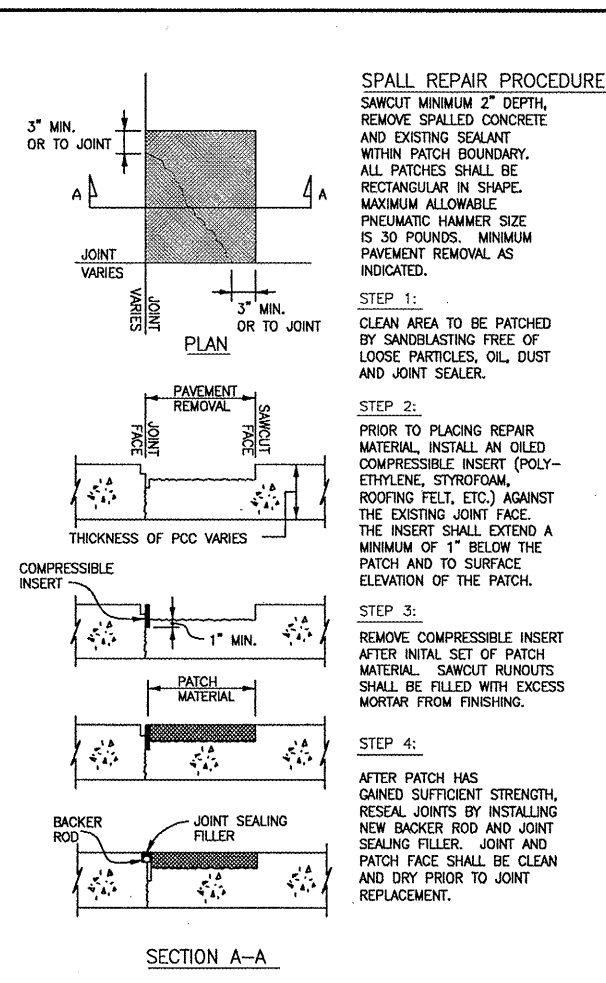


PANEL REMOVAL & REPLACEMENT DETAILS



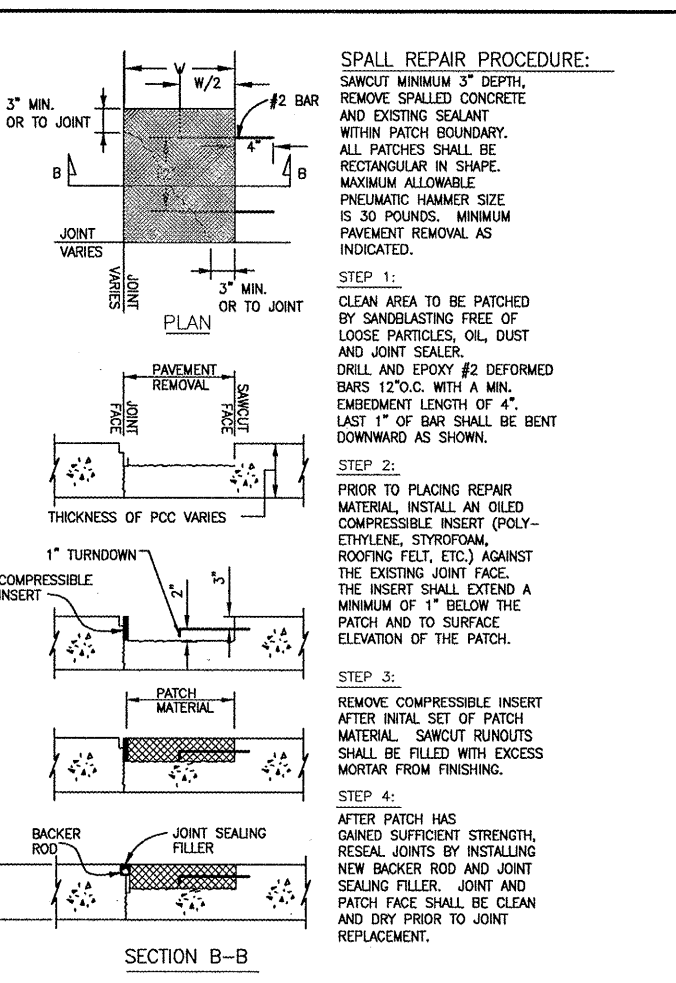
- NOTES:
- ROUTE OR SAWCUT EXISTING CRACK/JOINT.
 - CLEAN AND DRY EXISTING JOINT OR CRACK BY SANDBLASTING AND AIRBLASTING. CRACK OR JOINT SHALL BE DUST-FREE PRIOR TO INSTALLATION OF SEALANT.
 - RESEAL CRACK OR JOINT WITH HOT-POUR JET FUEL RESISTANT SEALANT (ASTM D-1854 OR D-3581) OR DOW CORNING 888 NON-SAG SILICONE JOINT SEALANT.
 - THIS WORK SHALL BE PAID FOR UNDER AR605541 CLEAN AND SEAL CRACKS.

CRACK ROUTING AND SEALING DETAILS



SHALLOW SPALL REPAIR DETAILS

- NOTES:
- SPALL REPAIR PATCH MATERIAL SHALL BE ITEM 610 PCC WITH POLY FIBERS AND 3/4" NOMINAL AGGREGATE SIZE.
 - COAT CAVITY WITH EPOXY BONDING AGENT.
 - MAX DEPTH IS 4".
 - THIS WORK SHALL BE PAID FOR UNDER ITEM AR801702 PART DEPTH PCC PAVEMENT REPAIR, 2"-4".



DEEP SPALL REPAIR DETAILS

- NOTES:
- SPALL REPAIR PATCH MATERIAL SHALL BE ITEM 610 PCC WITH POLY FIBERS AND 3/4" NOMINAL AGGREGATE SIZE.
 - COAT CAVITY WITH EPOXY BONDING AGENT.
 - MAX DEPTH IS 0.67xT (T=CONCRETE PAVEMENT THICKNESS).
 - DEEPER REPAIR REQUIRES PARTIAL PANEL REPLACEMENT.
 - THIS WORK SHALL BE PAID FOR UNDER ITEM AR801703 PART DEPTH PCC PAVEMENT REPAIR, 4"-6".

PE087



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SHEET TITLE

TYPICAL PANEL
REMOVAL AND
REPAIR DETAILS

SHEET NUMBER

18 OF 43

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