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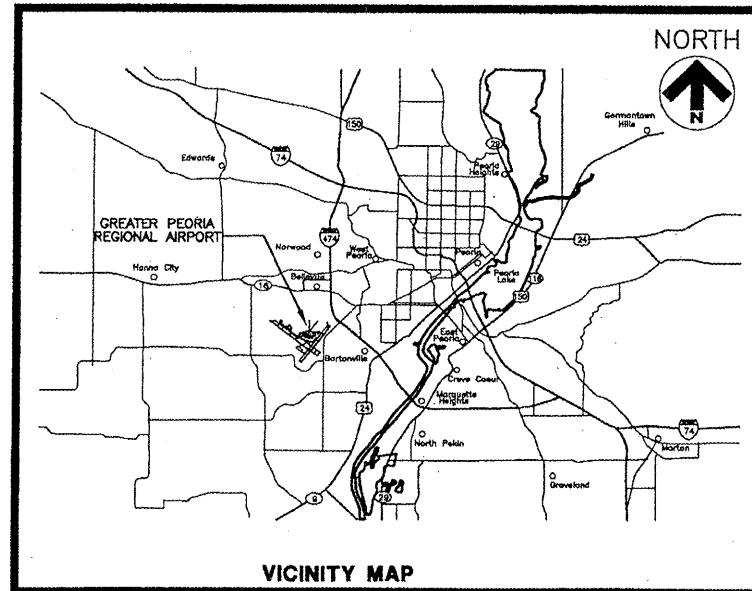
GREATER PEORIA REGIONAL AIRPORT

PEORIA, ILLINOIS

PE087
TOTAL SHEETS: 43

TERMINAL APRON EXPANSION

IL PROJECT NUMBER: PIA-3812
AIP PROJECT NUMBER: 3-17-0080-46



FOR THE
METROPOLITAN AIRPORT AUTHORITY OF PEORIA

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PEORIA, ILLINOIS 61607
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ISSUED FOR BID DOCUMENTS



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JUNE 27, 2008

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METROPOLITAN AIRPORT AUTHORITY OF PEORIA

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METROPOLITAN AIRPORT AUTHORITY OF PEORIA
DOCUMENT APPROVAL

BY: [Signature]
TITLE: Director of Airports
DATE: 7/14/08

DRAWINGS PREPARED BY
RS&H ILLINOIS, INC.

SUBMITTED BY: [Signature]
DATE: 7/14/08
RS&H PROJECT No. 213-6945-000

SUMMARY OF QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL ESTIMATED QUANTITY	AS-BUILT QUANTITY
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	1,400	
AR152460	TOPSOIL STRIPPING	C.Y.	4,100	
AR152441	ON-SITE BORROW	C.Y.	14,500	
AR155712	LIME-MODIFIED SUBGRADE - 12"	S.Y.	12,200	
AR209608	CRUSHED AGG. BASE COURSE - 8"	S.Y.	12,250	
AR209612	CRUSHED AGG. BASE COURSE - 12"	S.Y.	235	
AR401900	REMOVE BITUMINOUS PAVEMENT	S.Y.	1,650	
AR501514	14" PCC PAVEMENT	S.Y.	11,100	
AR501518	18" PCC PAVEMENT	S.Y.	700	
AR501530	PCC TEST BATCH	EACH	1	
AR501900	REMOVE PCC PAVEMENT	S.Y.	2,900	
AR501910	REMOVE & REPLACE PCC PAVEMENT	S.Y.	750	
AR805541	CLEAN & SEAL CRACKS	L.F.	2,000	
AR162506	CLASS E FENCE 6'	L.F.	800	
AR162900	REMOVE CLASS E FENCE	L.F.	800	
AR701512	12" RCP, CLASS IV	L.F.	120	
AR701524	24" RCP, CLASS IV	L.F.	325	
AR701900	REMOVE PIPE	L.F.	250	
AR705526	6" PERFORATED UNDERDRAIN W/ SOCK	L.F.	1,100	
AR751540	MANHOLE 4'	EACH	1	
AR751550	MANHOLE 5'	EACH	1	
AR751803	UNDERDRAIN CLEANOUT	EACH	4	
AR751903	REMOVE MANHOLE	EACH	2	
AR751906	REMOVE CATCH BASIN	EACH	1	
AR751943	ADJUST MANHOLE	EACH	1	
AR752412	PRECAST REINFORCED CONC. FES 12"	EACH	2	
AR752424	PRECAST REINFORCED CONC. FES 24"	EACH	3	
AR752903	REMOVE HEADWALL	EACH	3	
AR754904	REMOVE COMB CURB & GUTTER	L.F.	300	
AR801510	SEEDING	ACRE	1.8	
AR108108	1/C #8 5 KV UG CABLE	L.F.	1,600	
AR108706	1/C #6 COUNTERPOISE	L.F.	800	
AR110202	2" PVC DUCT, DIRECT BURY	L.F.	1,000	
AR110508	8-WAY CONCRETE ENCASED DUCT	L.F.	350	
AR110610	ELECTRICAL HANDHOLE	EACH	4	
AR125415	MIL-BASE MOUNTED	EACH	13	
AR125442	TAXIWAY GUIDANCE SIGN, 2 CHARACTER	EACH	1	
AR125445	TAXIWAY GUIDANCE SIGN, 5 CHARACTER	EACH	1	
AR150520	MOBILIZATION	L.S.	1	
AR150530	TRAFFIC MAINTENANCE	L.S.	1	
AR156510	SILT FENCE	L.F.	800	
AR156511	DITCH CHECK	EACH	1	
AR156520	INLET PROTECTION	EACH	5	
AR156532	EXCELSIOR BLANKET	S.Y.	8,700	
AR156540	RIPRAP	S.Y.	140	
AR801700	REMOVE GUARDRAIL	L.F.	30	
AR801701	ECONCRETE BASE COURSE - 6"	S.Y.	11,950	
AR801702	PART DEPTH PCC PAVT. REPAIR - 2'-4"	S.F.	250	
AR801703	PART DEPTH PCC PAVT. REPAIR - 4'-6"	S.F.	250	
AR801704	PCC PANEL REINFORCEMENT (SPECIAL)	S.Y.	750	
AR801705	RELOCATE STORAGE TANKS	L.S.	1	

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**TERMINAL
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SUBCONSULTANTS

PSA Dewberry

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REVISIONS

NO.	DESCRIPTION	DATE

DATE ISSUED: 06/27/08
 REVIEWED BY: PCB
 DRAWN BY: MJL
 DESIGNED BY: NDO

**AEP PROJECT NUMBER
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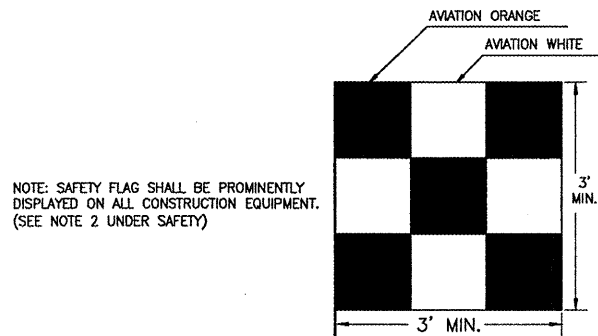
SHEET TITLE

**SUMMARY OF
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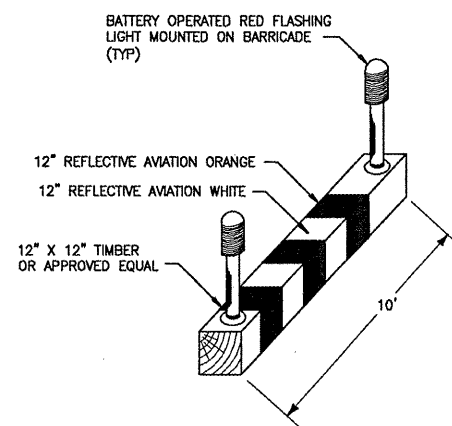
SHEET NUMBER

2 OF 43

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CONSTRUCTION SAFETY FLAG
N.T.S.



LOW LEVEL LIGHTED BARRICADES
N.T.S.

NOTES:

1. LOW LEVEL LIGHTED BARRICADES SHALL BE PLACED AROUND ALL OPEN EXCAVATIONS, HOLES, TRENCHES, PAVEMENT DROPOFFS, AND AREAS IDENTIFIED ON THE PHASING PLAN.
2. PLACE BARRICADES @ 20 FT. O.C. UNLESS OTHERWISE NOTED.
3. 20 BARRICADES SHALL BE TURNED OVER TO AND BECOME THE PROPERTY OF THE AIRPORT UPON COMPLETION OF THE PROJECT.

CONTRACTOR'S SAFETY AND SECURITY REQUIREMENTS

SAFETY

1. THE CONTRACTOR SHALL ACQUAINT HIS SUPERVISORS AND EMPLOYEES OF THE AIRPORT ACTIVITY AND OPERATIONS THAT ARE INHERENT TO THIS ACTIVE AIR CARRIER AIRPORT AND SHALL CONDUCT THE CONSTRUCTION ACTIVITIES TO CONFORM TO ALL ROUTINE AND EMERGENCY AIR TRAFFIC REQUIREMENTS AND GUIDELINES ON SAFETY AS SPECIFIED IN PARAGRAPH NO. 11 BELOW.
2. ALL CONTRACTOR VEHICLES THAT ARE AUTHORIZED TO OPERATE ON THE AIRPORT IN THE ACTIVE AIRCRAFT OPERATIONS AREA (AOA) SHALL DISPLAY IN FULL VIEW A FLASHING AMBER (YELLOW) DOME-TYPE LIGHT OR ABOVE THE VEHICLE A 3' X 3' OR LARGER, ORANGE AND WHITE CHECKERBOARD FLAG, EACH CHECKERBOARD COLOR BEING 1-FOOT SQUARE, (SEE CONSTRUCTION SAFETY FLAG DETAIL, THIS SHEET) AND ESCORTED UNDER THE CONTROL OF ONE CONTRACTOR MOBILE (TWO-WAY) RADIO OPERATOR ON THE JOB AT ALL TIMES. ANY VEHICLE OPERATING IN THE ACTIVE AOA DURING THE HOURS OF DARKNESS SHALL BE EQUIPPED WITH A FLASHING AMBER (YELLOW) DOME LIGHT, MOUNTED ON TOP OF THE VEHICLE AND OF SUCH INTENSITY TO CONFORM TO LOCAL CODES FOR MAINTENANCE AND EMERGENCY VEHICLES.
3. ALL NON-RADIO EQUIPPED CONTRACTOR VEHICLES THAT ARE REQUIRED TO OPERATE ON OR ACROSS ACTIVE RUNWAYS, TAXIWAYS, APRONS AND RUNWAY APPROACH AND PROTECTION ZONES SHALL DO SO UNDER THE DIRECT CONTROL OF A RADIO EQUIPPED ESCORT VEHICLE. ESCORT VEHICLES, OPERATORS, AND RADIO EQUIPMENT SHALL BE APPROVED BY THE AIRPORT. ESCORT VEHICLES SHALL BE MARKED AND LIGHTED AS DESCRIBED IN PARAGRAPH NO. 2 ABOVE AND SHALL BE EQUIPPED WITH AN APPROVED AVIATION BAND. RADIO EQUIPPED VEHICLES SHALL CONTINUOUSLY MONITOR GROUND CONTROL FREQUENCY 121.85 MHZ. ALL AIRCRAFT TRAFFIC ON RUNWAYS, TAXIWAYS AND APRONS SHALL HAVE PRIORITY OVER CONTRACTOR'S TRAFFIC.
4. NO RUNWAY, TAXIWAY, APRON OR AIRPORT ROADWAY SHALL BE CLOSED WITHOUT WRITTEN APPROVAL OF AIRPORT OPERATIONS. TO ENABLE NECESSARY "NOTICES TO AIRMEN" (NOTAM) OR ADVISORIES TO AIRPORT SERVICES OR TENANTS, A MINIMUM OF 72 HOURS WRITTEN NOTICE REQUESTING CLOSING SHALL BE DIRECTED TO THE RESIDENT PROJECT REPRESENTATIVE (RPR) AND/OR RESIDENT ENGINEER WHO WILL COORDINATE THE REQUEST WITH AIRPORT OPERATIONS.
5. ANY CONSTRUCTION ACTIVITY WITHIN 175 FEET OF AN ACTIVE RUNWAY EDGE OR 50 FEET FROM AN ACTIVE TAXIWAY EDGE OR OPEN EXCAVATIONS IN EXCESS OF THREE INCHES DEEP WITHIN THE ABOVE AREAS, WILL REQUIRE CLOSURE OF THE AFFECTED RUNWAY OR TAXIWAY, UNLESS OTHERWISE APPROVED BY AIRPORT OPERATIONS. CLOSURE REQUIRES THE SAME PROVISIONS AS PARAGRAPH NO. 4 ABOVE.
6. OPEN FLAME WELDING OR TORCH CUTTING OPERATIONS ARE PROHIBITED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS HAVE BEEN TAKEN AND THE PROCEDURE APPROVED BY THE RPR AND/OR AIRPORT OPERATIONS.
7. STOCKPILED MATERIAL SHALL BE CONSTRAINED IN A MANNER TO PREVENT MOVEMENT RESULTING FROM AIRCRAFT JET BLAST OR WIND CONDITIONS IN EXCESS OF TEN KNOTS. STOCKPILE HEIGHT SHALL BE LESS THAN 15 FEET, AND OUTSIDE THE RUNWAY AND TAXIWAY OBJECT FREE AREAS. IN ADDITION, STOCKPILED MATERIAL SHALL HAVE SILT FENCE LOCATED AROUND THE MATERIAL TO PREVENT FOD FROM MOVING ONTO THE AIRFIELD PAVEMENTS.
8. OPEN TRENCHES, EXCAVATIONS AND STOCKPILED MATERIAL LOCATED IN THE AOA SHALL BE PROMINENTLY MARKED WITH FLAGS AND LIGHTED BY APPROVED LIGHT UNITS DURING HOURS OF RESTRICTED VISIBILITY AND DARKNESS.
9. DEBRIS, WASTE AND LOOSE MATERIAL CAPABLE OF CAUSING DAMAGE TO AIRCRAFT LANDING GEARS, PROPELLERS OR BEING INGESTED IN JET ENGINES SHALL NOT BE ALLOWED ON ACTIVE AIRCRAFT MOVEMENT AREAS. IF THESE MATERIALS ARE OBSERVED TO BE ON ACTIVE AIRCRAFT MOVEMENT AREAS, THEY WILL BE REMOVED IMMEDIATELY AND/OR CONTINUOUSLY DURING CONSTRUCTION.
10. THE RESIDENT ENGINEER WILL ARRANGE WITH AIRPORT OPERATIONS FOR INSPECTION PRIOR TO OPENING FOR AIRCRAFT USE ANY RUNWAY OR TAXIWAY THAT HAS BEEN CLOSED FOR WORK, ON OR ADJACENT THERETO, OR THAT HAS BEEN USED FOR A CROSSING POINT OR HAUL ROUTE BY THE CONTRACTOR.
11. THE CONTRACTOR IS DIRECTED TO COMPLY WITH AND ACQUAINT HIS/HER EMPLOYEES WITH THE FOLLOWING SAFETY GUIDELINES, RELATED MATERIALS AND FAA ADVISORY CIRCULARS:

FAA ORDER	"SAFETY REQUIREMENT ON AIRPORTS DURING AGENCY FUNDED CONSTRUCTION AND MAINTENANCE ACTIVITIES"
150/5200-18C	"AIRPORT SAFETY-SELF INSPECTION"
150/5210-5B	"PAINTING, MARKING & LIGHTING OF VEHICLES USED ON AIRPORTS"
150/5370-2E	"OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION"
150/5370-4	"APPENDIX 3. SUGGESTED SPECIAL PROVISIONS FOR PROTECTION OF CABLES, CONTROLS, NAVAIDS AND WEATHER BUREAU FACILITIES"
150/5370-7	"AIRPORT CONSTRUCTION CONTROLS TO PREVENT AIR AND WATER POLLUTION"
12. CONSTRUCTION DURING THE PROJECT MAY BE HALTED AT ANY TIME BY RPR, ENGINEER, AND/OR AIRPORT OPERATIONS IF IT IS DETERMINED TO BE IN THE BEST INTEREST OF AIRPORT OPERATIONS OR SAFETY. THE CONTRACTOR MAY BE DIRECTED TO REMOVE EQUIPMENT AND/OR EVACUATE THE SITE IN ORDER TO ENABLE AIRCRAFT OPERATIONS. NECESSARY EXTENSIONS IN CONTRACT TIME WILL BE GRANTED OR A STOP WORK ORDER WILL BE ISSUED DUE TO THESE DELAYS. HOWEVER, THERE WILL BE NO ADJUSTMENTS IN CONTRACT PRICE DUE TO THESE DELAYS. ANY DELAY LONGER THAN 8 HOURS WILL RESULT IN AN EXTENSION OF CONTRACT TIME IF THE DELAY IS THROUGH NO FAULT OF THE CONTRACTOR.

IN ADDITION TO THE ABOVE, THE FOLLOWING SPECIAL REQUIREMENTS WILL APPLY FOR NIGHT CONSTRUCTION:
13. WHEN APPLICABLE, A DAILY SAFETY AND PROGRESS MEETING SHALL BE HELD BETWEEN THE RESIDENT ENGINEER AND THE CONTRACTOR'S SUPERINTENDENT TO DISCUSS REQUIREMENTS FOR THE NIGHTTIME WORK PERIODS.
14. WHEN APPLICABLE, THE CONTRACTOR SHALL PREPARE A SAFETY PLAN SPECIFIC TO NIGHTTIME CONSTRUCTION OPERATIONS, AS WELL AS A CONTINGENCY PLAN TO ADDRESS CASES OF ABNORMAL FAILURES OR UNEXPECTED DISASTERS.
15. TRUCK HAUL ROUTES ON THE AIRFIELD SHALL BE DELINEATED WITH LIGHTED BARRICADES. OTHER MEANS TO CLEARLY MARK THE ROUTES TO THE WORK SITE MAY BE APPROVED BY THE RPR, ENGINEER, AND/OR AIRPORT OPERATIONS.
16. ALL COMMUNICATION WITH THE AIR TRAFFIC CONTROL TOWER OR OTHER ELEMENTS OF THE AIRPORT SHALL BE THROUGH THE RESIDENT ENGINEER AND/OR AIRPORT OPERATIONS OR THE CONTRACTOR'S BADGED PERSONNEL. IF THE CONTRACTOR IS REQUIRED TO CROSS ACTIVE MOVEMENT AREAS THE CONTRACTOR'S SUPERINTENDENT AND FOREMAN SHALL BE BADGED BY THE AIRPORT. SEE SECURITY NOTE 6.

SECURITY

1. **GENERAL INTENT:** IT IS INTENDED THAT THE CONTRACTOR SHALL COMPLY WITH ALL REQUIREMENTS OF THE AIRPORT SECURITY PLAN AND WITH THE SECURITY REQUIREMENTS SPECIFIED HEREIN BY AIRPORT OPERATIONS. THE CONTRACTOR SHALL DESIGNATE TO THE RESIDENT ENGINEER AND AIRPORT OPERATIONS, IN WRITING, THE NAME OF HIS "CONTRACTOR SECURITY OFFICER (CSO)." THE CSO SHALL REPRESENT THE CONTRACTOR ON THE SECURITY REQUIREMENTS FOR THE CONTRACT.
2. **CONTRACTOR PERSONNEL SECURITY ORIENTATION:** THE CSO SHALL BE RESPONSIBLE FOR BRIEFING ALL CONTRACTOR PERSONNEL ON SECURITY REQUIREMENTS. ALL NEW CONTRACTOR EMPLOYEES SHALL BE BRIEFED ON SECURITY REQUIREMENTS PRIOR TO WORKING IN THE CONSTRUCTION AREA.
3. **ACCESS TO THE SITE:** CONTRACTOR'S ACCESS TO THE SITE SHALL BE AS SHOWN ON THE PLANS. NO OTHER ACCESS POINTS SHALL BE ALLOWED UNLESS APPROVED BY AIRPORT OPERATIONS. ALL CONTRACTOR TRAFFIC AUTHORIZED TO ENTER THE SITE SHALL BE EXPERIENCED IN THE ROUTE OR GUIDED BY CONTRACTOR PERSONNEL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL TO AND FROM THE VARIOUS CONSTRUCTION AREAS ON THE SITE, AND FOR THE OPERATION AND SECURITY OF THE ACCESS GATE TO THE SITE. A CONTRACTOR'S FLAGMAN OR TRAFFIC CONTROL PERSON SHALL MONITOR AND COORDINATE ALL CONTRACTOR TRAFFIC AT THE ACCESS GATE WITH SECURITY. THE CONTRACTOR SHALL NOT PERMIT ANY UNAUTHORIZED CONSTRUCTION PERSONNEL OR TRAFFIC ON THE SITE. ACCESS GATES TO THE SITE SHALL BE LOCKED AND SECURED AT ALL TIMES WHEN NOT ATTENDED BY THE CONTRACTOR. IF THE CONTRACTOR CHOOSES TO LEAVE ANY ACCESS GATE OPEN, IT SHALL BE ATTENDED BY CONTRACTOR PERSONNEL WHO ARE FAMILIAR WITH THE REQUIREMENTS OF THE AIRPORT OPERATIONS SECURITY PROGRAM. THE CONTRACTOR IS RESPONSIBLE FOR THE IMMEDIATE CLEANUP OF ANY DEBRIS DEPOSITED ALONG THE ACCESS ROUTE AS A RESULT OF HIS CONSTRUCTION TRAFFIC. DIRECTIONAL SIGNING FROM THE ACCESS GATE ALONG THE DELIVERY ROUTE TO THE STORAGE AREA, PLANT SITE OR WORK SITE SHALL BE AS DIRECTED BY AIRPORT OPERATIONS.
4. **MATERIALS DELIVERY TO THE SITE:** ALL CONTRACTOR'S MATERIAL ORDERS FOR DELIVERY TO THE WORK SITE WILL USE AS A DELIVERY ADDRESS, THE STREET NAME ASSIGNED TO THE ACCESS POINT AT THE CONTRACTOR'S STAGING SITE AT THE AIRPORT. THE AIRPORT NAME SHALL NOT BE USED IN THE DELIVERY ADDRESS AT ANY TIME. THIS WILL PRECLUDE DELIVERY TRUCKS FROM ENTERING INTO THE TERMINAL COMPLEX, OR TAKING SHORT CUTS THROUGH THE PERIMETER GATES AND ENTERING INTO AIRCRAFT OPERATIONS AREAS INAPPROPRIATELY.
5. **CONSTRUCTION AREA LIMITS:** THE LIMITS OF CONSTRUCTION, MATERIAL STORAGE AREAS, PLANT SITE, EQUIPMENT STORAGE AREA, PARKING AREA AND OTHER AREAS DEFINED AS REQUIRED FOR THE CONTRACTOR'S EXCLUSIVE USE DURING CONSTRUCTION SHALL BE MARKED BY THE CONTRACTOR. THE CONTRACTOR SHALL ERECT AND MAINTAIN AROUND THE PERIMETER OF THESE AREAS SUITABLE FENCING, MARKING AND/OR WARNING DEVICES VISIBLE FOR DAY/NIGHT USE. TEMPORARY BARRICADES, FLAGGING AND FLASHING WARNING LIGHTS WILL BE REQUIRED AT CRITICAL ACCESS POINTS. TYPE OF MARKING AND WARNING DEVICES SHALL BE APPROVED BY AIRPORT OPERATIONS.
6. **IDENTIFICATION--PERSONNEL:** ALL EMPLOYEES, AGENTS, VENDORS, INVITEES, ETC. OF THE CONTRACTOR OR SUBCONTRACTORS REQUIRING ACCESS TO THE CONSTRUCTION SITE SHALL, IN ACCORDANCE WITH THE AIRPORT OPERATIONS SECURITY PROGRAM, BE REQUIRED TO DISPLAY IDENTIFICATION OR BE UNDER ESCORT BY PROPERLY BADGED PERSONNEL. THESE BADGES WILL BE IDENTIFIED NUMERICALLY AND ISSUED TO INDIVIDUAL EMPLOYEES WITH A PERMANENT RECORD MAINTAINED ON EACH INDIVIDUAL TO WHOM A BADGE IS ISSUED. AT THE COMPLETION OF THE CONTRACT ALL BADGES WILL BE RETURNED TO THE AIRPORT AND A CHARGE OF \$100 PER BADGE WILL BE ASSESSED FOR ALL BADGES NOT RETURNED. NO BADGE WILL BE ISSUED TO ANY PERSON UNTIL A REVIEW OF THE TSA REQUIRED PAPERWORK BY AIRPORT SECURITY AND ALL REQUIREMENTS ARE MET. PAPERWORK SHALL BE SUBMITTED A MINIMUM OF 7 DAYS BEFORE ISSUANCE OF BADGE. IDENTIFIABLE HARD HATS OR OTHER IDENTIFICATION SHALL ALSO BE WORN AT ALL TIMES IF REQUIRED BY AIRPORT OPERATIONS. THE CONTRACTOR'S SUPERINTENDENT AND FOREMAN ARE RESPONSIBLE FOR ATTENDING TRAINING AND COMPLETING SECURITY BADGE APPLICATIONS, WHICH WILL INCLUDE AIR/GROUND RADIO COMMUNICATION, TAXIWAY AND AIRPORT FAMILIARIZATION. ESTIMATED TIME FOR COMPLETION IS 3 HOURS.
7. **IDENTIFICATION--VEHICLES:** THE CONTRACTOR, THROUGH THE CSO, SHALL ESTABLISH AND MAINTAIN A LIST OF CONTRACTOR AND SUBCONTRACTOR VEHICLES AUTHORIZED TO OPERATE ON THE SITE. PERSONAL VEHICLES OF CONTRACTOR EMPLOYEES SHALL BE RESTRICTED TO THE CONTRACTOR'S EMPLOYEE PARKING AREA AND ARE NOT ALLOWED ON OTHER AREAS OF THE AOA AT ANY TIME.
8. **FINES:** PAYMENT OF ALL FINES ASSESSED TO THE AIRPORT DUE TO VIOLATIONS BY THE CONTRACTOR OF TSA/FAA SECURITY OR SAFETY REQUIREMENTS SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. IF THE RESTRICTED AREA GATE IS FOUND TO BE OPEN OR UNLOCKED AND UNATTENDED, AIRPORT SECURITY POLICE MAY ISSUE THE CONTRACTOR A CITATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS AND IMPOSED FINES. IN ADDITION, A \$1,000.00 CHARGE MAY BE LEVIED BY THE AIRPORT FOR EACH VIOLATION SO DOCUMENTED AND UPON FINAL PAYMENT THE TOTAL OF ANY SUCH CHARGES WILL BE DEDUCTED FROM MONEYS DUE THE CONTRACTOR.
9. A MINIMUM OF 48 HOURS IN ADVANCE OF ANY EXCAVATION OR BORINGS, THE CONTRACTOR SHALL CONTACT LOCAL CABLE OWNERS TO VERIFY ALL UNDERGROUND CABLE LOCATIONS IN THE VICINITY OF THE PROPOSED WORK.

SAFETY PLAN NARRATIVE

1. THIS PROJECT INCLUDES APPROXIMATELY 11,600 SY OF NEW CONCRETE APRON PAVEMENT, EXCAVATION, DRAINAGE IMPROVEMENTS, ELECTRICAL IMPROVEMENTS AND SITE RESTORATION.
2. AIRCRAFT OPERATIONS WILL CONTINUE DURING CONSTRUCTION. TYPE OF AIRCRAFT OPERATIONS DURING CONSTRUCTION INCLUDE DESIGN GROUPS I, II, III AND IV AND APPROACH CATEGORY A, B AND C.
3. BASIS FOR TEMPORARY DISPLACED OR RELOCATED THRESHOLDS: NOT APPLICABLE.
4. BASIS FOR DEVIATING FROM STANDARDS: NOT APPLICABLE.
5. TYPE AND HEIGHT OF CONSTRUCTION EQUIPMENT:
 - A. TRUCKS (DUMP, FLATBED, CONCRETE) - 15 FEET
 - B. FRONT END LOADERS - 20 FEET
 - C. BULLDOZERS - 15 FEET
 - D. ROLLERS AND COMPACTORS - 15 FEET
 - E. EXCAVATORS - 25 FEET
6. DATES FOR START AND COMPLETION OF WORK:

ESTIMATED START:	SEPTEMBER 15, 2008
ESTIMATED COMPLETE:	JULY 3, 2009
CONSTRUCTION TIME:	150 CALENDAR DAYS

PE087

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IMPROVING YOUR WORLD

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DESIGNED BY: NDO

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SHEET TITLE

SAFETY AND SECURITY
NOTES AND DETAILS

SHEET NUMBER

3 OF 43

ISSUED FOR
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GENERAL CONTRACT NOTES

1. **HAUL ROUTES:** LOCATION OF HAUL ROUTES ON THE AIRPORT SITE SHALL BE AS SPECIFIED ON THE PLANS OR AS APPROVED BY THE AIRPORT OWNER. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. BEFORE AND AFTER CONSTRUCTION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR, AND AIRPORT REPRESENTATIVES. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S RESPONSIBILITY AND SHALL BE APPROVED BY THE AIRPORT OWNER PRIOR TO THE WORK. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ANY DAMAGE TO HAUL ROUTES RESULTING FROM CONSTRUCTION TRAFFIC. THE CONTRACTOR SHALL PROVIDE A STREET SWEEPER TO CLEAN HAUL ROUTES ON A DAILY BASIS AS NECESSARY OR AS DIRECTED BY THE ENGINEER.

2. **WASTE DISPOSAL AND BORROW AREAS:** CONCRETE RUBBLE AND EXCAVATION WASTE MATERIAL REMOVED FROM THE CONSTRUCTION AREA SHALL BE DISPOSED OFF OF THE AIRPORT PROPERTY. NO MATERIAL SHALL BE WASTED ON THE AIRPORT SITE UNLESS APPROVED BY THE AIRPORT OWNER. WASTE AND DISPOSAL AREAS SHALL BE SEDED AND RESTORED IN A SMOOTH, GRADED AND DRAINABLE CONDITION. BORROW AREAS, IF REQUIRED, SHALL BE LOCATED AS SHOWN ON THE PLANS AND SHALL ALSO BE RESTORED IN A SMOOTH GRADED AND DRAINABLE CONDITION.

3. **CONTRACTOR UTILITIES:** STAGING AREAS DO NOT HAVE UTILITIES. ANY UTILITIES REQUIRED BY THE CONTRACTOR SHALL BE COORDINATED WITH THE UTILITY COMPANIES AND SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

4. **PROTECTION AND REPAIR OF DAMAGE TO EXISTING CABLES:** ALL UNDERGROUND CABLES SHALL BE PROTECTED AND DAMAGES REPAIRED EXPEDITIOUSLY AT THE CONTRACTOR'S EXPENSE AT NO ADDITIONAL COST TO THE OWNER.





GENERAL CONTRACT NOTES (cont.)

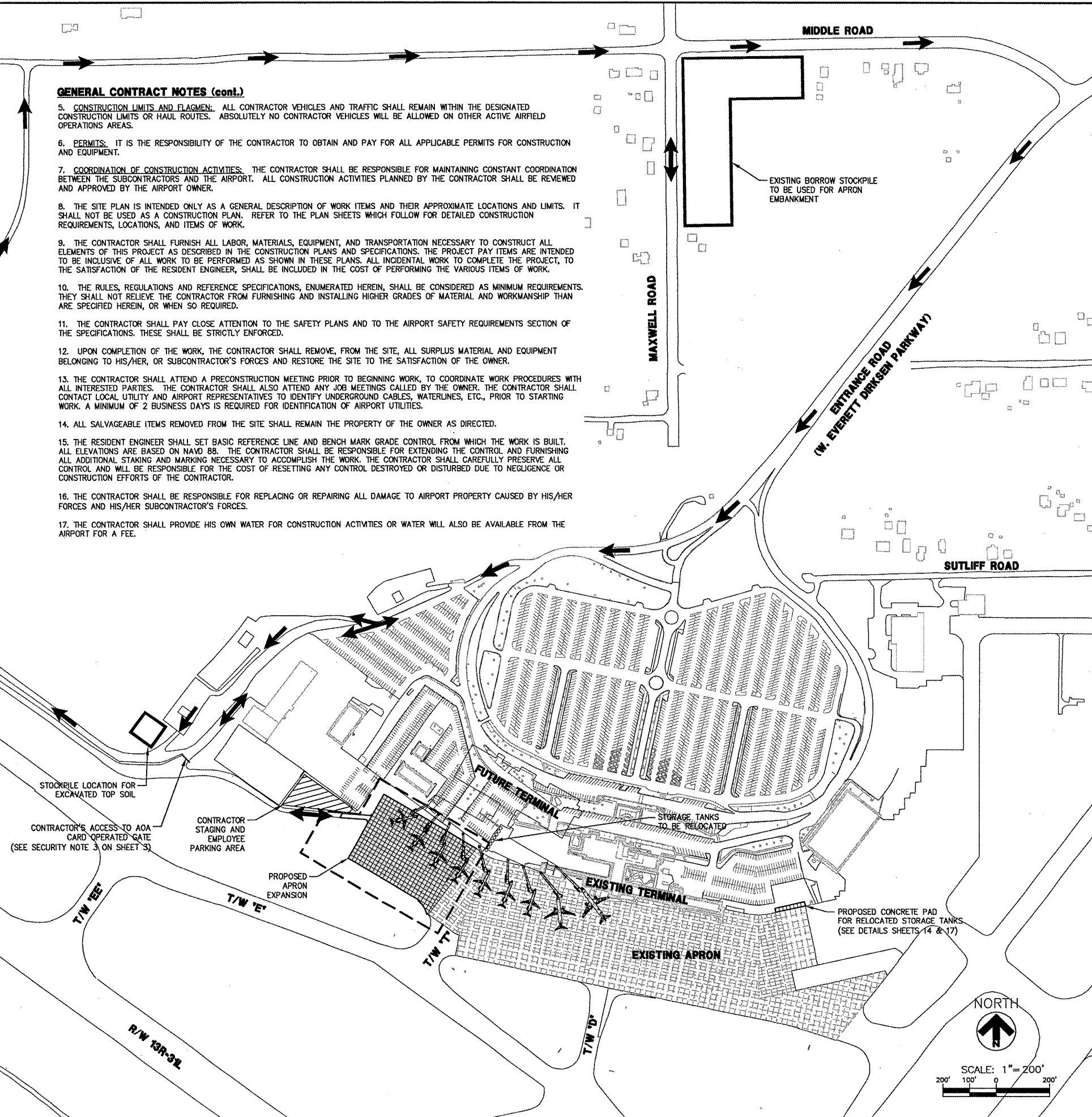
5. **CONSTRUCTION LIMITS AND FLAGMEN:** ALL CONTRACTOR VEHICLES AND TRAFFIC SHALL REMAIN WITHIN THE DESIGNATED CONSTRUCTION LIMITS OR HAUL ROUTES. ABSOLUTELY NO CONTRACTOR VEHICLES WILL BE ALLOWED ON OTHER ACTIVE AIRFIELD OPERATIONS AREAS.
6. **PERMITS:** IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN AND PAY FOR ALL APPLICABLE PERMITS FOR CONSTRUCTION AND EQUIPMENT.
7. **COORDINATION OF CONSTRUCTION ACTIVITIES:** THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING CONSTANT COORDINATION BETWEEN THE SUBCONTRACTORS AND THE AIRPORT. ALL CONSTRUCTION ACTIVITIES PLANNED BY THE CONTRACTOR SHALL BE REVIEWED AND APPROVED BY THE AIRPORT OWNER.
8. THE SITE PLAN IS INTENDED ONLY AS A GENERAL DESCRIPTION OF WORK ITEMS AND THEIR APPROXIMATE LOCATIONS AND LIMITS. IT SHALL NOT BE USED AS A CONSTRUCTION PLAN. REFER TO THE PLAN SHEETS WHICH FOLLOW FOR DETAILED CONSTRUCTION REQUIREMENTS, LOCATIONS, AND ITEMS OF WORK.
9. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIALS, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THIS PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS. THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK TO COMPLETE THE PROJECT, TO THE SATISFACTION OF THE RESIDENT ENGINEER, SHALL BE INCLUDED IN THE COST OF PERFORMING THE VARIOUS ITEMS OF WORK.
10. THE RULES, REGULATIONS AND REFERENCE SPECIFICATIONS, ENUMERATED HEREIN, SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT RELIEVE THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL AND WORKMANSHIP THAN ARE SPECIFIED HEREIN, OR WHEN SO REQUIRED.
11. THE CONTRACTOR SHALL PAY CLOSE ATTENTION TO THE SAFETY PLANS AND TO THE AIRPORT SAFETY REQUIREMENTS SECTION OF THE SPECIFICATIONS. THESE SHALL BE STRICTLY ENFORCED.
12. UPON COMPLETION OF THE WORK, THE CONTRACTOR SHALL REMOVE, FROM THE SITE, ALL SURPLUS MATERIAL AND EQUIPMENT BELONGING TO HIS/HER, OR SUBCONTRACTOR'S FORCES AND RESTORE THE SITE TO THE SATISFACTION OF THE OWNER.
13. THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION MEETING PRIOR TO BEGINNING WORK, TO COORDINATE WORK PROCEDURES WITH ALL INTERESTED PARTIES. THE CONTRACTOR SHALL ALSO ATTEND ANY JOB MEETINGS CALLED BY THE OWNER. THE CONTRACTOR SHALL CONTACT LOCAL UTILITY AND AIRPORT REPRESENTATIVES TO IDENTIFY UNDERGROUND CABLES, WATERLINES, ETC., PRIOR TO STARTING WORK. A MINIMUM OF 2 BUSINESS DAYS IS REQUIRED FOR IDENTIFICATION OF AIRPORT UTILITIES.
14. ALL SALVAGEABLE ITEMS REMOVED FROM THE SITE SHALL REMAIN THE PROPERTY OF THE OWNER AS DIRECTED.
15. THE RESIDENT ENGINEER SHALL SET BASIC REFERENCE LINE AND BENCH MARK GRADE CONTROL FROM WHICH THE WORK IS BUILT. ALL ELEVATIONS ARE BASED ON NAVD 88. THE CONTRACTOR SHALL BE RESPONSIBLE FOR EXTENDING THE CONTROL AND FURNISHING ALL ADDITIONAL STAKING AND MARKING NECESSARY TO ACCOMPLISH THE WORK. THE CONTRACTOR SHALL CAREFULLY PRESERVE ALL CONTROL AND WILL BE RESPONSIBLE FOR THE COST OF RESETTling ANY CONTROL DESTROYED OR DISTURBED DUE TO NEGLIGENCE OR CONSTRUCTION EFFORTS OF THE CONTRACTOR.
16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPLACING OR REPAIRING ALL DAMAGE TO AIRPORT PROPERTY CAUSED BY HIS/HER FORCES AND HIS/HER SUBCONTRACTOR'S FORCES.
17. THE CONTRACTOR SHALL PROVIDE HIS OWN WATER FOR CONSTRUCTION ACTIVITIES OR WATER WILL ALSO BE AVAILABLE FROM THE AIRPORT FOR A FEE.

GENERAL CONTRACT NOTES (cont.)

17. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION WHEN EXCAVATING IN AREAS OF EXISTING UTILITIES. EXISTING UTILITIES SHALL BE LOCATED AND MARKED IN ADVANCE OF EXCAVATION IN ALL AREAS. ANY DAMAGE DONE TO FUNCTIONING UTILITIES SHALL BE REPAIRED IMMEDIATELY BY THE CONTRACTOR AT HIS/HER EXPENSE AND AS DIRECTED. THE LOCATION OF ANY UTILITIES SHOWN ON THE PLANS IS APPROXIMATE ONLY, DEPTHS ARE NOT KNOWN.
18. ANY SPILLAGE ON THE PARKWAY, CURB FRONT OR EXIT ROADWAYS SHALL BE IMMEDIATELY CLEANED UP OR THE AIRPORT MAY DO SO AND REDUCE MONIES DUE TO THE CONTRACTOR TO USE EQUIPMENT AND PERSONNEL FOR THIS CLEAN UP.
19. THE CONTRACTOR SHALL PROVIDE TEMPORARY TOILET FACILITIES AT THE APRON CONSTRUCTION SITE FOR USE BY HIS EMPLOYEES THAT MEET ALL APPLICABLE REQUIREMENTS. CONTRACTOR EMPLOYEES SHALL NOT BE ALLOWED TO USE THE AIRPORT TERMINAL FACILITIES.

LEGEND

-  NEW APRON, THIS CONTACT
-  CONTRACTOR STAGING/PARKING AREA
-  CONTRACTOR HAUL/ACCESS ROUTE
-  LIMITS OF CONSTRUCTION



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IL PROJECT, PIA-3812
AIP PROJECT, 3-17-0080-46

REVISIONS

NO.	DESCRIPTION	DATE

DATE ISSUED: 06/27/08

REVIEWED BY: PCB

DRAWN BY: MJL

DESIGNED BY: NDO

AEP PROJECT NUMBER

213-6945-000

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SHEET TITLE

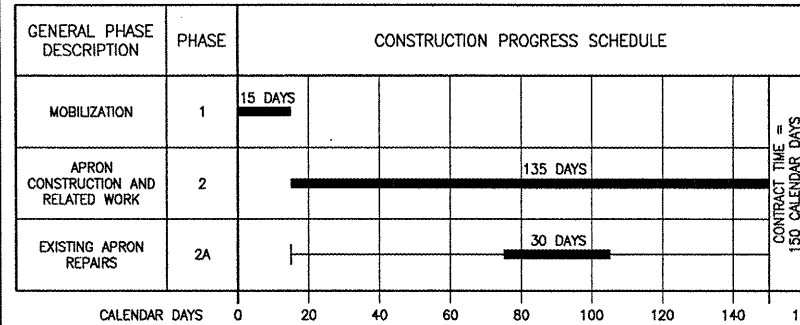
GENERAL CONTRACT
LAYOUT PLAN
AND NOTES

SHEET NUMBER

4 OF 43

ISSUED FOR
BID DOCUMENTS

SCHEMATIC CONSTRUCTION PHASING



CONSTRUCTION PHASING NOTES:

CONSTRUCTION TIME: 150 CALENDAR DAYS
ANTICIPATED START DATE: SEPTEMBER 15, 2008
ANTICIPATED COMPLETION DATE: JULY 3, 2009

PHASE 1 (15 DAYS)

ITEMS OF WORK IN THIS PHASE INCLUDE:
- CONTRACTOR MOBILIZATION
- SHOP DRAWING SUBMITTALS

PHASE 2 (135 DAYS)

PHASE 2 WORK CONSISTS OF CONSTRUCTION OF THE NEW APRON AND RELATED WORK. MAJOR ITEMS OF WORK IN THIS PHASE INCLUDE:

- DEMOLITION OF EXISTING FACILITIES
- TOPSOIL STRIPPING
- DRAINAGE MODIFICATIONS
- CONSTRUCTION OF EMBANKMENT
- CONSTRUCTION OF AGGREGATE BASE
- CONSTRUCTION OF ECONCRETE BASE
- CONSTRUCTION OF PCC PAVEMENT
- RECONSTRUCTION OF PERIMETER ACCESS ROAD
- CONSTRUCTION OF NEW EDGE LIGHTING
- LANDSCAPING AND SITE RESTORATION
- CONSTRUCTION OF CONCRETE PAD FOR RELOCATION OF STORAGE TANKS

PHASE 2A (30 DAYS)

PHASE 2A WORK SHALL OCCUR DURING A 30 DAY PERIOD CONCURRENT WITH PHASE 2. MAJOR ITEMS OF WORK IN THIS PHASE INCLUDE:
- PANEL REPLACEMENT AND REPAIRS IN EXISTING CONCRETE APRON

GENERAL PHASING NOTES:

1. THE CONSTRUCTION PHASING OUTLINED IN THE ATTACHED PLANS IS BROKEN INTO TWO PHASES OF WORK. THE PHASING OF EACH PHASE ARE ESTABLISHED, AS SUCH, TO MINIMIZE THE DISRUPTION OF AIRFIELD OPERATIONS DURING CONSTRUCTION. ALL PHASES OF WORK ARE WITHIN THE AIRCRAFT OPERATIONS AREA (AOA) BUT OUTSIDE RUNWAY AND TAXIWAY SAFETY AREAS. DUE TO THE IMPORTANCE OF MAINTAINING AIRFIELD OPERATIONS AND SAFETY AND SECURITY DURING CONSTRUCTION, THE CONTRACTOR IS REMINDED THAT WHILE WORKING WITHIN THESE AREAS, IT IS THE CONTRACTOR'S RESPONSIBILITY TO BE APPRAISED OF, AND TO IMPLEMENT, THE GUIDELINES ESTABLISHED IN THE PROVISIONS UNDER SAFETY AND SECURITY.

2. THE CONTRACTOR SHALL BE GIVEN A SPECIFIC NUMBER OF CALENDAR DAYS TO PERFORM THE WORK AND IF THE CONTRACTOR DOES NOT COMPLETE THE CONSTRUCTION WITHIN THAT TIME FRAME, LIQUIDATED DAMAGES WILL BE ASSESSED UNLESS THE CONTRACTOR CAN SHOW JUST CAUSE FOR THE DELAYS. IF THE RESIDENT ENGINEER, OR AIRPORT MANAGEMENT FEELS THAT CONSTRUCTION IS PROCEEDING AT TOO SLOW A PACE, THE RESIDENT ENGINEER SHALL NOTIFY THE CONTRACTOR IN WRITING AND THE CONTRACTOR SHALL BE REQUIRED TO RESPOND IN WRITING JUSTIFYING THE IDENTIFIED DELAYS AND/OR LACK OF ADEQUATE EQUIPMENT. IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO PROVIDE JUSTIFICATION FOR WAIVING OF ANY LIQUIDATED DAMAGES CHARGED TO THE CONTRACTOR.

3. THE CONTRACTOR SHALL, AT ALL TIMES, COORDINATE HIS EFFORTS WITH THE RESIDENT ENGINEER. IF ANY PROBLEMS ARISE DURING THE CONSTRUCTION SEQUENCING, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE RESIDENT ENGINEER TO RESOLVE SAID PROBLEMS PRIOR TO CONTINUING THE WORK.

4. THE CONTRACTOR SHALL PERFORM ALL FINAL CLEANUP WORK PRIOR TO A FINAL INSPECTION. THE CONTRACTOR SHALL ALSO CONTINUOUSLY CLEAN UP DURING EACH PHASE OF THE PROJECT.

5. THE CONTRACTOR SHALL PROVIDE FLAGMEN DESIGNATED TO OBSERVE ALL AIRCRAFT ACTIVITIES AROUND THE CONSTRUCTION AREAS FOR AIRCRAFT ARRIVING OR PREPARING TO DEPART FROM SAID AREAS NEAR THE CONSTRUCTION. THE FLAGMEN WILL STOP ANY CONSTRUCTION TRAFFIC WHERE THE AIRCRAFT CROSSES NEAR THE CONSTRUCTION, AND IF NECESSARY, REQUIRE THE CONSTRUCTION EQUIPMENT TO MOVE TO ALLOW SAFE PASSAGE OF THE AIRCRAFT. IF IT IS DETERMINED THAT A CONSIDERABLE AMOUNT OF AIRCRAFT ACTIVITY IS OCCURRING AND THE TRAFFIC WILL CAUSE CONSIDERABLE DELAYS FOR THE CONTRACTOR, THE CONTRACTOR SHALL REQUEST A MEETING WITH THE AIRPORT MANAGEMENT AND RESIDENT ENGINEER TO COORDINATE AND ALLMATE THE DELAYS.

6. THE CONTRACTOR SHALL SWEEP ON A DAILY BASIS, AS NECESSARY OR AS DIRECTED BY THE RESIDENT ENGINEER. THE ACTIVE AIRPORT AREAS NEAR CONSTRUCTION ACTIVITIES WHICH HAVE HAD ANY FOREIGN OBJECT DEBRIS (FOD) DEPOSITED BY AUTOMOBILE OR CONSTRUCTION EQUIPMENT OF BY WIND BLOWING DEBRIS OR MATERIALS ONTO THOSE ACTIVE AREAS. IT IS IMPERATIVE THAT NO DAMAGE IS DONE TO ANY AIRCRAFT DUE TO FOD. ANY DAMAGE DONE TO AIRCRAFT WHICH IS ATTRIBUTABLE TO FOD FROM THE CONSTRUCTION AREAS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE WITH NO REIMBURSEMENT BY THE AIRPORT, RESIDENT ENGINEER OR THEIR AUTHORIZED REPRESENTATIVES.

7. THE CONTRACTOR SHALL BE REQUIRED TO SUBMIT HIS RECOMMENDED FIELD OPERATIONS AREAS FOR STORAGE OF EQUIPMENT, SUPPLIES AND FIELD OFFICES TO THE RESIDENT ENGINEER AND AIRPORT MANAGEMENT AT THE PRE-CONSTRUCTION CONFERENCE FOR REVIEW, COMMENTS AND APPROVAL. ANY AREAS RECOMMENDED FOR STORAGE OF FUELING FACILITIES, MATERIALS AND OFFICES SHALL BE APPROVED BY THE AIRPORT MANAGEMENT AND RESIDENT ENGINEER PRIOR TO MOBILIZATION OF ANY EQUIPMENT OR FIELD OFFICES AND CERTIFIED BY THE CONTRACTOR THAT THE FACILITIES MEET ALL APPLICABLE LOCAL, STATE, AND FEDERAL REQUIREMENTS.

8. ANY AREAS UTILIZED AS FIELD OPERATIONS AREAS SHALL BE MAINTAINED AT ALL TIMES IN A CLEAN AND ENVIRONMENTALLY SAFE CONDITION. IF THE CONTRACTOR MUST UTILIZE AGGREGATE BASE MATERIALS TO PROVIDE A STABLE SURFACE FOR EQUIPMENT STORAGE, THEN ANY MATERIALS UTILIZED WILL BE REMOVED AT THE END OF THE PROJECT AND DISPOSED OF AT A LOCATION OFF-AIRPORT ACCEPTABLE TO THE OWNER. A STABLE BASE EXTENDING FROM EXISTING PAVEMENT, NOT SCHEDULED FOR RECONSTRUCTION, TO THE FIELD OFFICES SHALL BE PROVIDED FOR CLEAN ACCESS.

9. APPROPRIATE EROSION CONTROL MEASURES SHALL BE ACCOMPLISHED PRIOR TO BEGINNING THE RESPECTIVE PHASE. REMOVAL OF TEMPORARY EROSION CONTROL SHALL BE ACCOMPLISHED BY THE CONTRACTOR EITHER AT THE COMPLETION OF THE ASSOCIATED PHASE OR THEREAFTER AS DIRECTED BY THE OWNERS REPRESENTATIVE.

10. THE COMPLETION OF ANY PHASE OF WORK AND SUBSEQUENT USAGE BY THE OWNER DOES NOT DEFINE ACCEPTANCE OF THE WORK IN THAT PHASE. WHEN ALL PHASES ARE COMPLETE AND A FINAL INSPECTION OF THE ENTIRE PROJECT HAS OCCURED AND ALL ASSOCIATED PUNCH LIST ITEMS HAVE BEEN COMPLETED TO THE SATISFACTION OF THE AIRPORT MANAGEMENT AND ENGINEER, THEN THE ENTIRE PROJECT WILL BE ACCEPTED.

BARRICADE PLACEMENT NOTES:

1. BARRICADES SHALL BE INSTALLED AT THE LOCATIONS SHOWN ON THE PLANS. THE PHASE DURING WHICH EACH BARRICADE IS TO REMAIN IS INDICATED BY THE BARRICADE LABELS. BARRICADES SHALL BE INSTALLED AT THE BEGINNING OF EACH PHASE AND SHALL REMAIN IN PLACE THROUGHOUT THE PHASE. IF THE CONTRACTOR MUST TEMPORARILY REMOVE ANY OF THE BARRICADES (TO ALLOW CONSTRUCTION TRAFFIC TO TRAVEL INTO OR OUT OF THE CONSTRUCTION AREA, OR TO COMPLETE CONSTRUCTION AT THE PHASE BOUNDARY), THE CONTRACTOR SHALL PROVIDE A FLAGMAN TO PREVENT AIRCRAFT FROM INADVERTENTLY TRAVELING INTO THE CONSTRUCTION AREA UNTIL THE BARRICADE IS REPLACED TO ITS ORIGINAL POSITION.

2. BARRICADES SHALL CONFORM TO THE LOW PROFILE BARRICADE DETAILS SHOWN ON THE PLANS. THE BARRICADES SHALL BE SPACED AT TWENTY FOOT CENTERS WITH OMNIDIRECTIONAL RED FLASHING LIGHT UNITS AND WEIGHTED DOWN TO PREVENT MOVEMENT FROM JET OR PROP BLAST. BARRICADES SHALL EXTEND FROM LIGHT LINE TO LIGHT LINE, AS A MINIMUM.

3. UPON THE COMPLETION OF THE PROJECT, THE AIRPORT SHALL RETAIN OWNERSHIP OF 20 LOW-LEVEL BARRICADES.

PROTECTION AND REPAIR OF DAMAGE TO EXISTING CABLES:

1. PRIOR TO INITIATION OF ANY CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL PROVIDE A WRITTEN NOTICE (RETURN RECEIPT REQUESTED) TO EACH UTILITY COMPANY, ALONG WITH THE VARIOUS DEPARTMENTS OF THE AIRPORT AS PRESCRIBED BY STATE LAW THAT MAY BE IMPACTED BY CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE THE OWNER AND THE RESIDENT ENGINEER WITH A COPY OF THE RECEIPT OF SAID WRITTEN NOTIFICATION TO EACH OF THE UTILITY COMPANIES AND/OR AIRPORT OPERATIONS. LOCATION OF EXISTING FAA UNDERGROUND CABLES WILL BE FLAGGED ONE TIME BY THE LOCAL FAA AIRWAY FACILITIES SECTOR OFFICE PERSONNEL THROUGH COORDINATION WITH THE RESIDENT ENGINEER AND AIRPORT OPERATIONS. THESE FLAGS SHALL BE PROTECTED AND MAINTAINED BY THE CONTRACTOR AT ALL TIMES. IF THE FLAG ARE LOST OR REMOVED BY THE CONTRACTOR, THEY WILL BE FLAGGED AGAIN AT THE CONTRACTOR'S EXPENSE. ALL UNDERGROUND CABLES SHALL BE PROTECTED AND DAMAGES REPAIRED EXPEDITIOUSLY AT THE CONTRACTOR'S EXPENSE AND AT NO ADDITIONAL COST TO THE OWNER. THIS REQUIREMENT IS IN ADDITION TO ANY OTHER STATE LAWS REGARDING PUBLIC NOTIFICATION PRIOR TO EXCAVATION.

PROPOSED ACCESS ROAD RECONSTRUCTION

PROPOSED PCC APRON EXPANSION

DIRKSEN PARKWAY

EXISTING TERMINAL

PROPOSED CONCRETE PAD FOR DEICING TANKS

EXISTING CONCRETE APRON

T/W 'E'

T/W 'T'

T/W 'D'

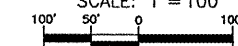
T/W 'A'

T/W 'E'

NORTH



SCALE: 1" = 100'



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SUBCONSULTANTS

PSA Dewberry

STS AECOM

IL PROJECT: PIA-3812
AIP PROJECT: 3-17-0080-46

REVISIONS

NO.	DESCRIPTION	DATE

DATE ISSUED: 06/27/08

REVIEWED BY: PCB

DRAWN BY: MJL

DESIGNED BY: NDO

AEP PROJECT NUMBER

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SHEET TITLE

PHASING PLAN
AND NOTES

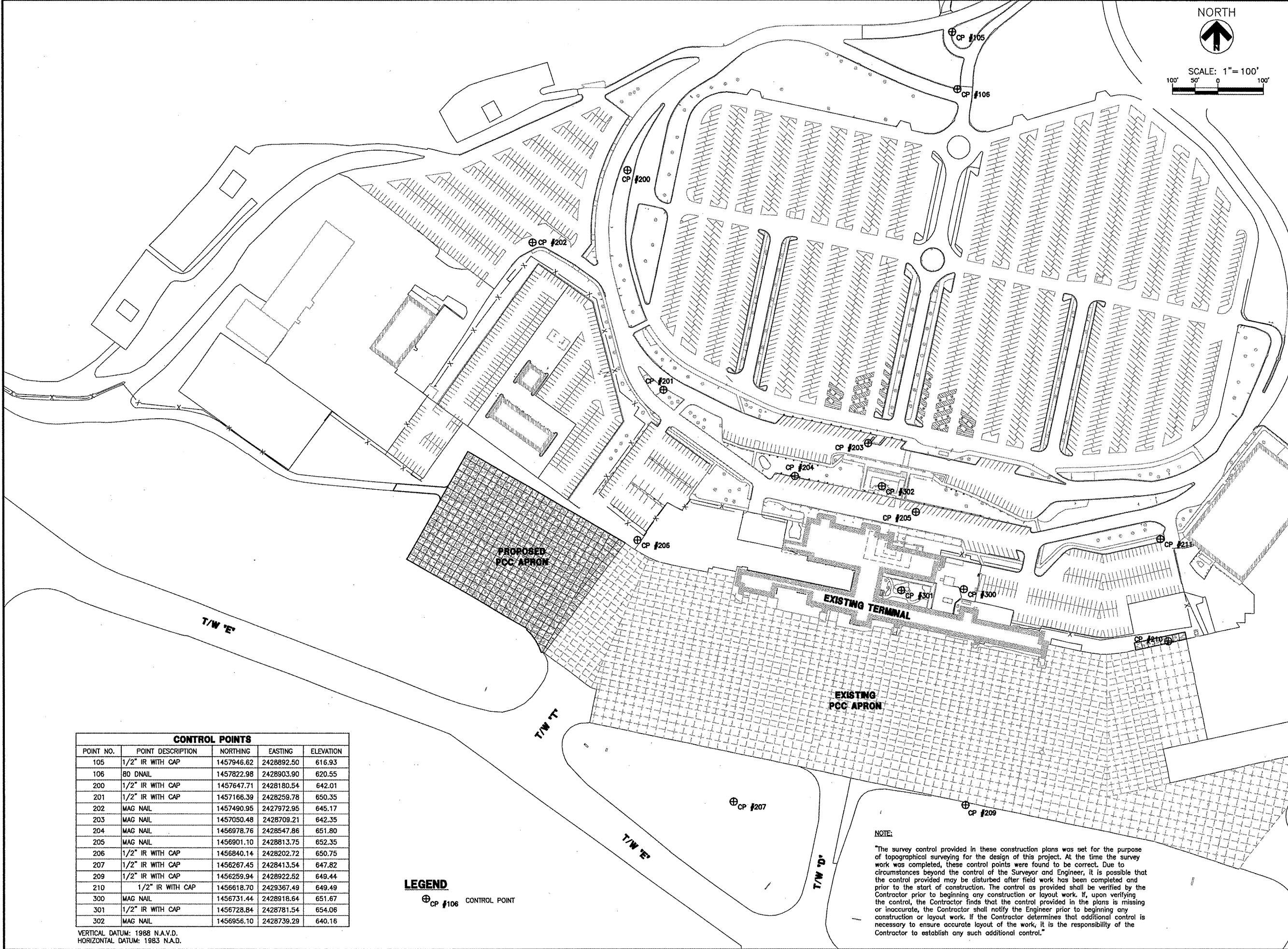
SHEET NUMBER

5 OF 43

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LEGEND

- ② BARRICADE PLACEMENT BY PHASE
- PROPOSED PCC PAVEMENT
- TSA TAXIWAY SAFETY AREA



NORTH
 SCALE: 1" = 100'
 100' 50' 0 100'

CONTROL POINTS				
POINT NO.	POINT DESCRIPTION	NORTHING	EASTING	ELEVATION
105	1/2" IR WITH CAP	1457946.62	2428892.50	616.93
106	80 DNAIL	1457822.98	2428903.90	620.55
200	1/2" IR WITH CAP	1457647.71	2428180.54	642.01
201	1/2" IR WITH CAP	1457166.39	2428259.78	650.35
202	MAG NAIL	1457490.95	2427972.95	645.17
203	MAG NAIL	1457050.48	2428709.21	642.35
204	MAG NAIL	1456978.76	2428547.86	651.80
205	MAG NAIL	1456901.10	2428813.75	652.35
206	1/2" IR WITH CAP	1456840.14	2428202.72	650.75
207	1/2" IR WITH CAP	1456267.45	2428413.54	647.82
209	1/2" IR WITH CAP	1456259.94	2428922.52	649.44
210	1/2" IR WITH CAP	1456618.70	2429367.49	649.49
300	MAG NAIL	1456731.44	2428918.64	651.67
301	1/2" IR WITH CAP	1456728.84	2428781.54	654.06
302	MAG NAIL	1456956.10	2428739.29	640.16

LEGEND
 ⊕ CP #106 CONTROL POINT

NOTE:
 "The survey control provided in these construction plans was set for the purpose of topographical surveying for the design of this project. At the time the survey work was completed, these control points were found to be correct. Due to circumstances beyond the control of the Surveyor and Engineer, it is possible that the control provided may be disturbed after field work has been completed and prior to the start of construction. The control as provided shall be verified by the Contractor prior to beginning any construction or layout work. If, upon verifying the control, the Contractor finds that the control provided in the plans is missing or inaccurate, the Contractor shall notify the Engineer prior to beginning any construction or layout work. If the Contractor determines that additional control is necessary to ensure accurate layout of the work, it is the responsibility of the Contractor to establish any such additional control."

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**TERMINAL
 APRON
 EXPANSION**

SUBCONSULTANTS
 PSA Dewberry
 STS AECOM

IL PROJECT: PIA-3812
 AIP PROJECT: 3-17-0080-46

REVISIONS

NO.	DESCRIPTION	DATE

DATE ISSUED: 06/27/08
 REVIEWED BY: PCB
 DRAWN BY: MJL
 DESIGNED BY: NDO

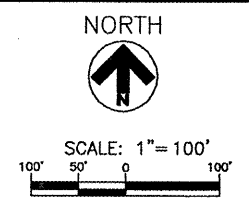
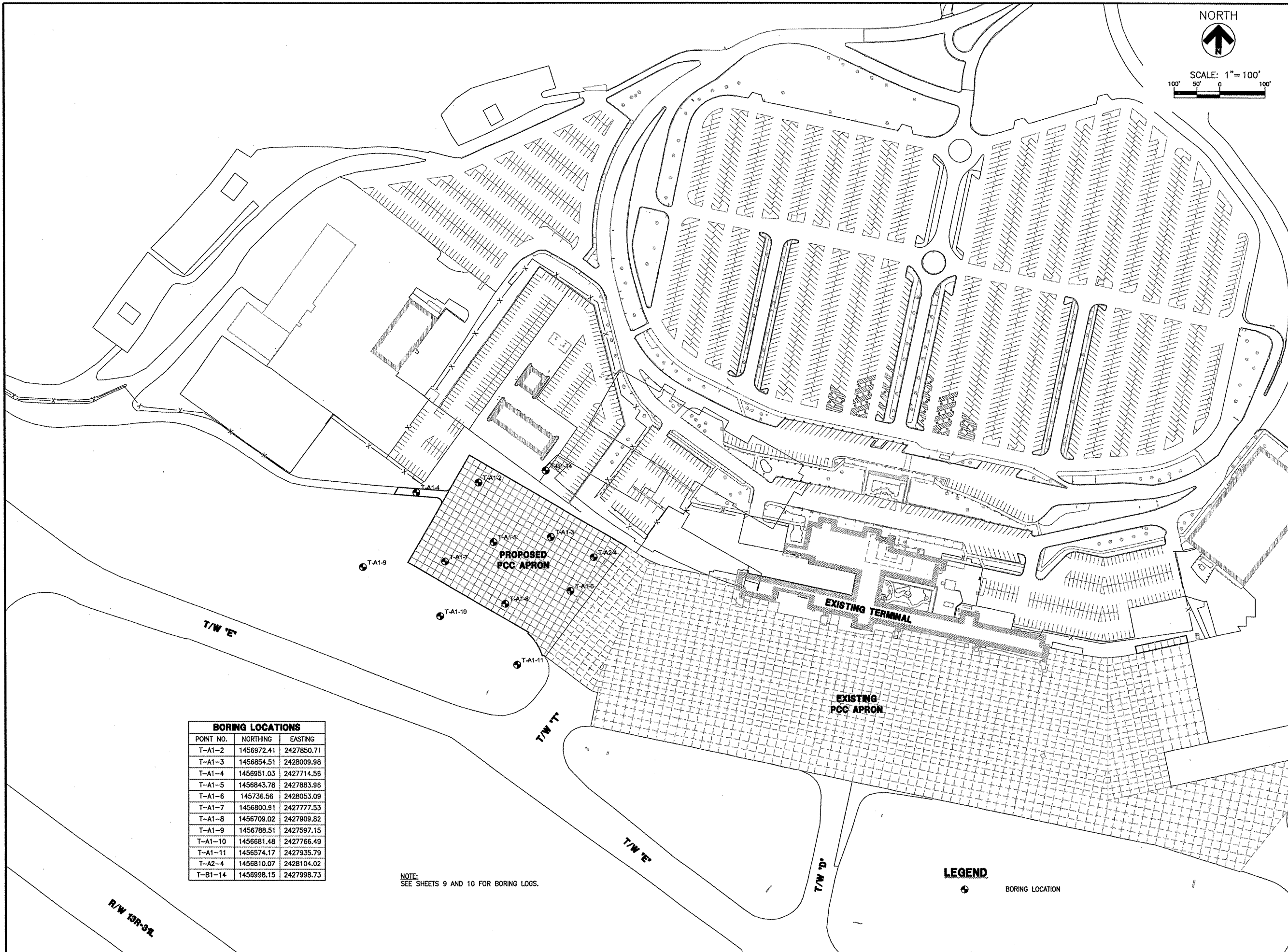
AEP PROJECT NUMBER
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 SHEET TITLE

**HORIZONTAL AND
 VERTICAL CONTROL
 PLAN**

SHEET NUMBER
 7 OF 43

**ISSUED FOR
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BORING LOCATIONS

POINT NO.	NORTHING	EASTING
T-A1-2	1456972.41	2427850.71
T-A1-3	1456854.51	2428009.98
T-A1-4	1456951.03	2427714.56
T-A1-5	1456843.78	2427883.96
T-A1-6	145736.56	2428053.09
T-A1-7	1456800.91	2427777.53
T-A1-8	1456709.02	2427909.82
T-A1-9	1456788.51	2427597.15
T-A1-10	1456681.48	2427766.49
T-A1-11	1456574.17	2427935.79
T-A2-4	1456810.07	2428104.02
T-B1-14	1456998.15	2427998.73

NOTE:
SEE SHEETS 9 AND 10 FOR BORING LOGS.

LEGEND

⊕ BORING LOCATION

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DESIGNED BY: NDO

AEP PROJECT NUMBER
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SHEET TITLE

**GEOTECHNICAL
BORING LAYOUT**

SHEET NUMBER

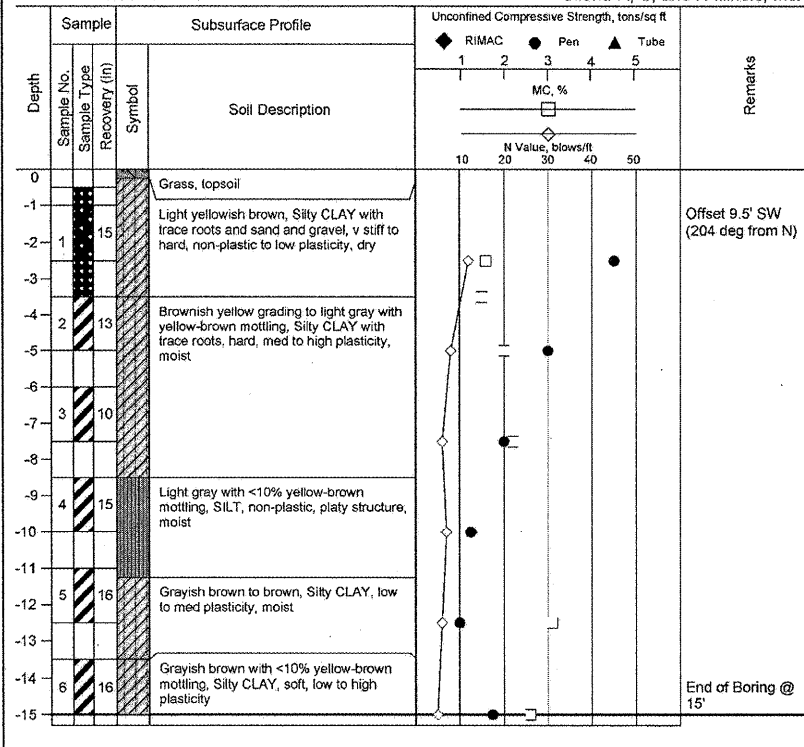
8 OF 43

**ISSUED FOR
BID DOCUMENTS**



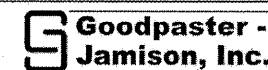
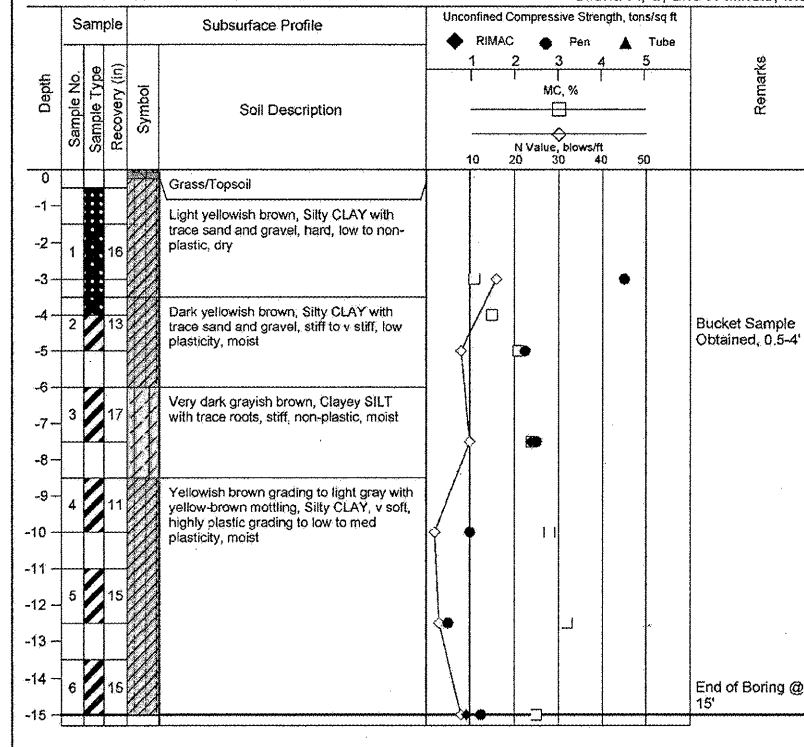
Borehole Log Boring Number T-A1-2

Project: PIA Terminal Expansion Location: Greater Peoria Regional Airport, Peoria, IL
Surface Elevation: 645.6 N: 1456972 E: 2427851 Client: R, S, and H Illinois, Inc.



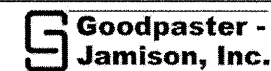
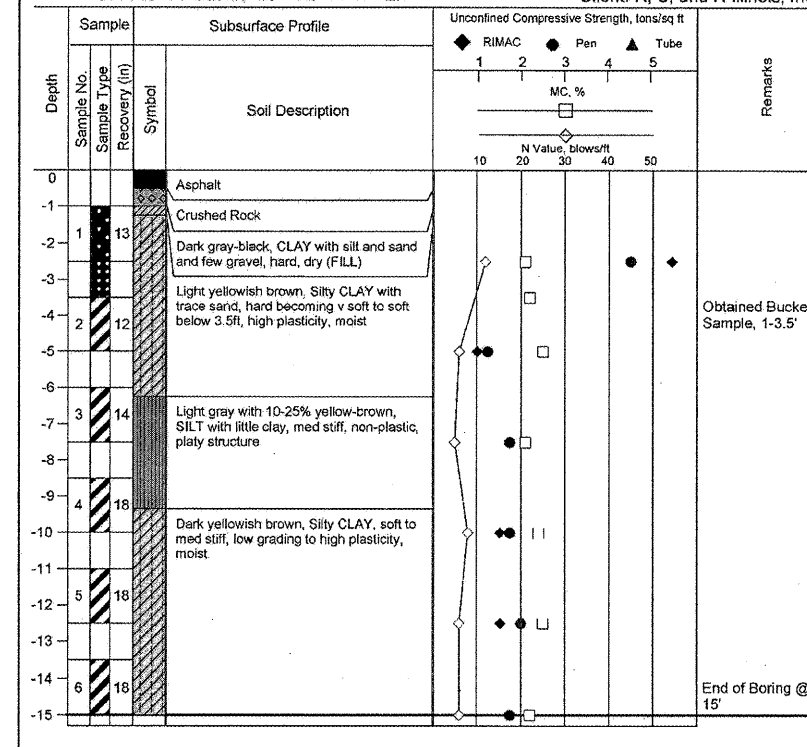
Borehole Log Boring Number T-A1-3

Project: PIA Terminal Expansion Location: Greater Peoria Regional Airport, Peoria, IL
Surface Elevation: 648.1 N: 1456855 E: 2428010 Client: R, S, and H Illinois, Inc.



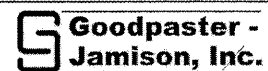
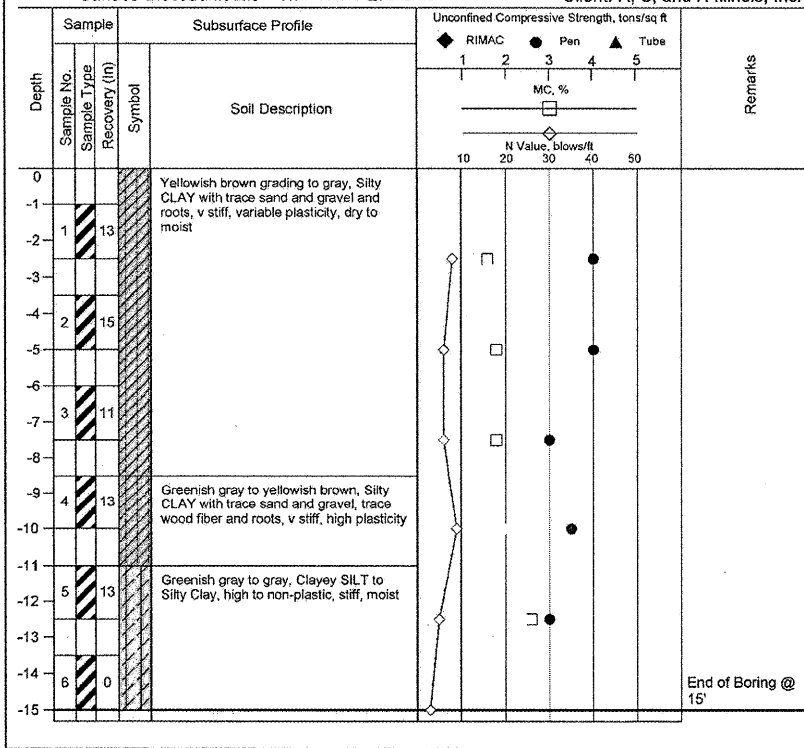
Borehole Log Boring Number T-A1-4

Project: PIA Terminal Expansion Location: Greater Peoria Regional Airport, Peoria, IL
Surface Elevation: 642.9 N: 1456951 E: 2427715 Client: R, S, and H Illinois, Inc.



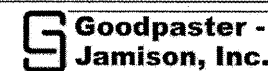
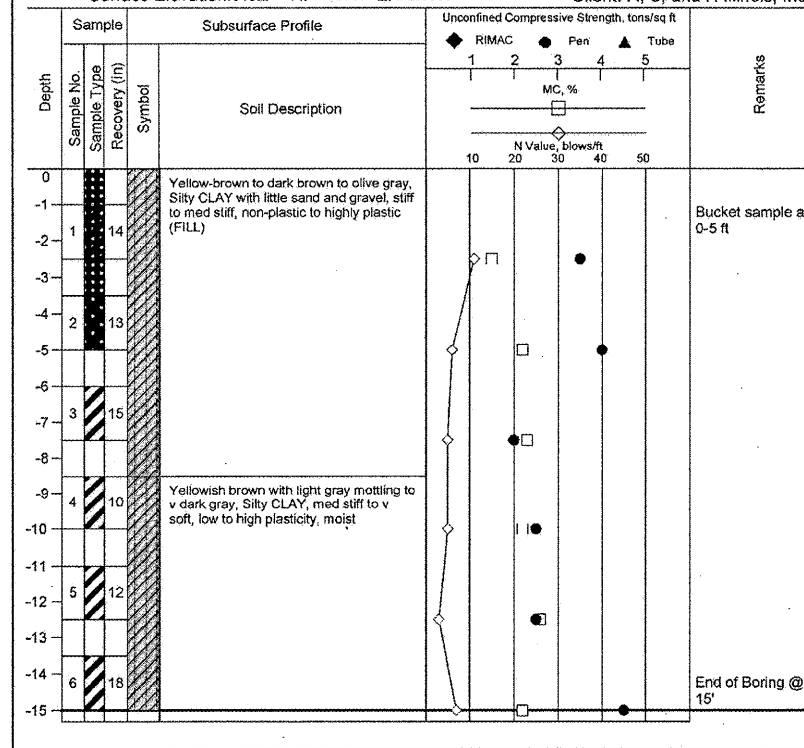
Borehole Log Boring Number T-A1-5

Project: PIA Terminal Expansion Location: Greater Peoria Regional Airport, Peoria, IL
Surface Elevation: 642.8 N: 1456844 E: 2427884 Client: R, S, and H Illinois, Inc.



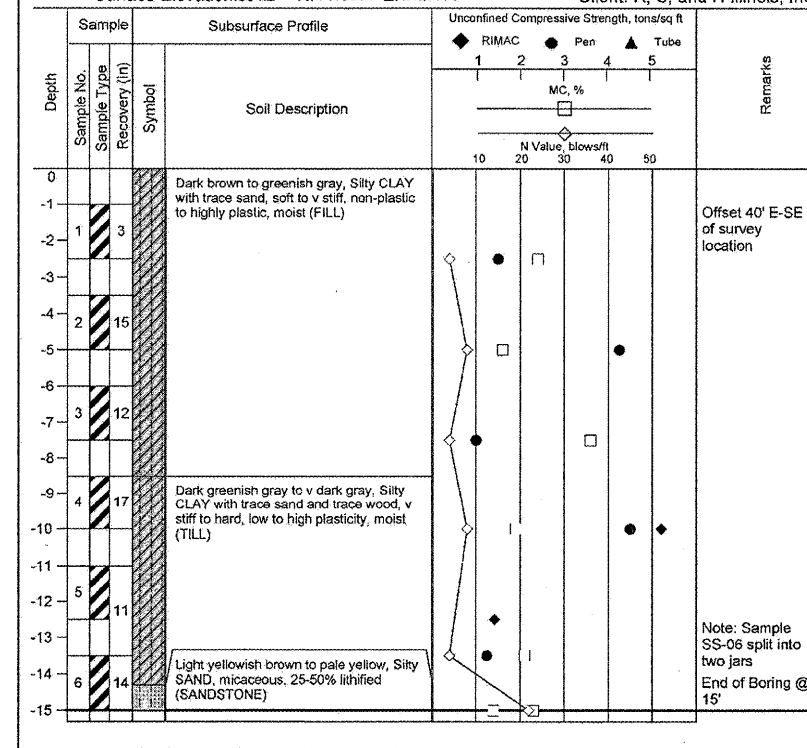
Borehole Log Boring Number T-A1-6

Project: PIA Terminal Expansion Location: Greater Peoria Regional Airport, Peoria, IL
Surface Elevation: 648.2 N: 1456737 E: 2428053 Client: R, S, and H Illinois, Inc.



Borehole Log Boring Number T-A1-7

Project: PIA Terminal Expansion Location: Greater Peoria Regional Airport, Peoria, IL
Surface Elevation: 634.2 N: 1456801 E: 2427778 Client: R, S, and H Illinois, Inc.



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IL PROJECT: PIA-3812
AIP PROJECT: 3-17-0080-46

REVISIONS

NO.	DESCRIPTION	DATE

DATE ISSUED: 06/27/08
REVIEWED BY: PCB
DRAWN BY: MJL
DESIGNED BY: NDO

AEP PROJECT NUMBER
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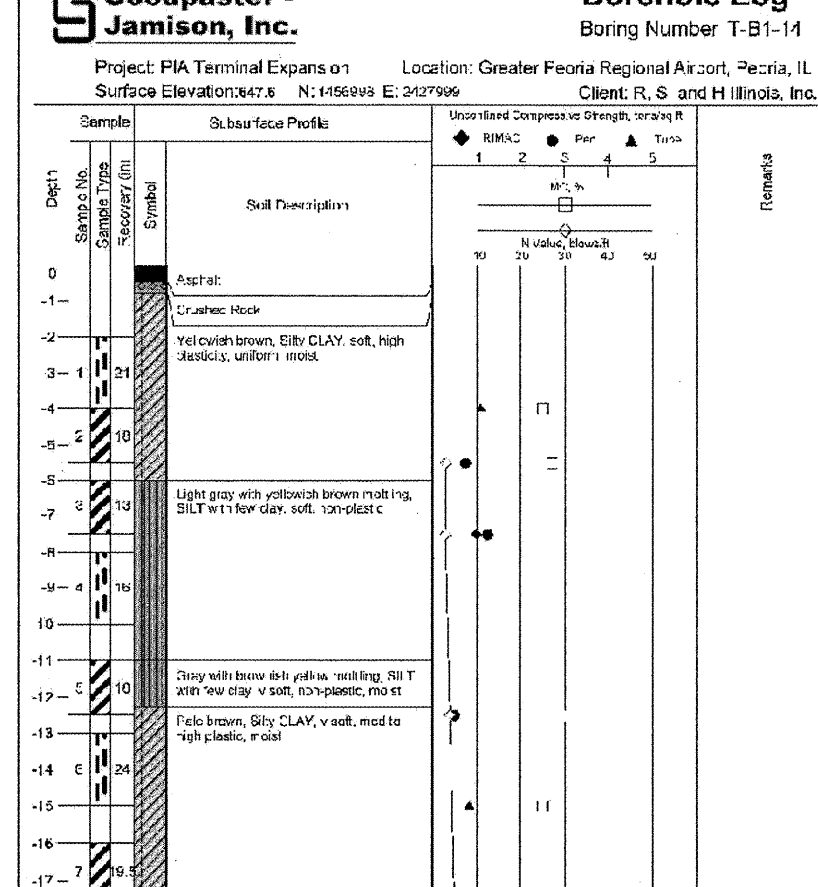
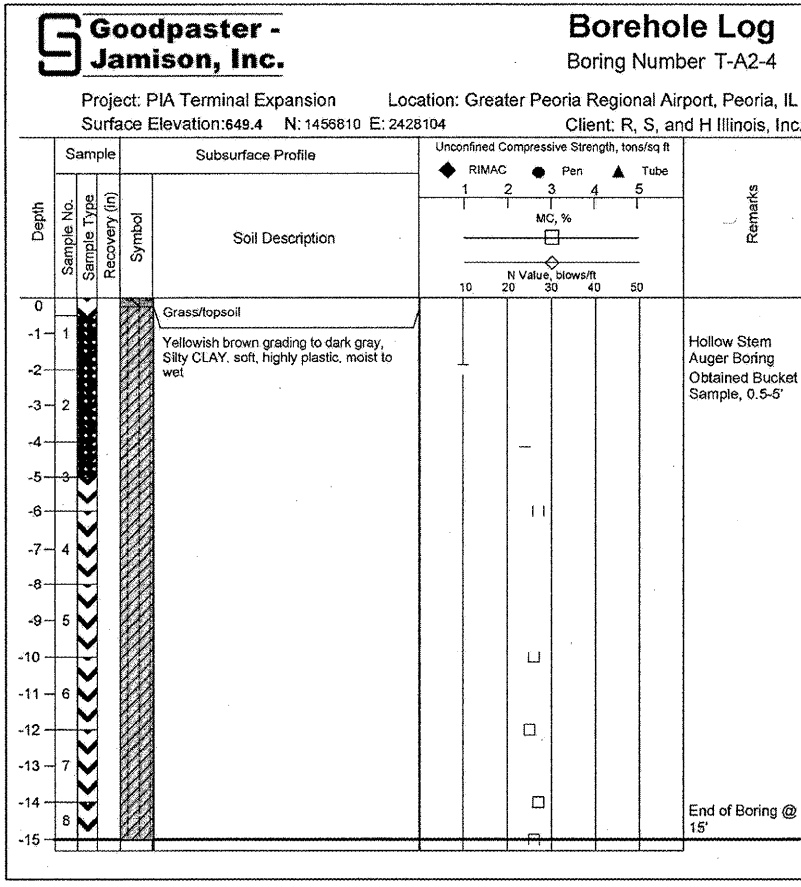
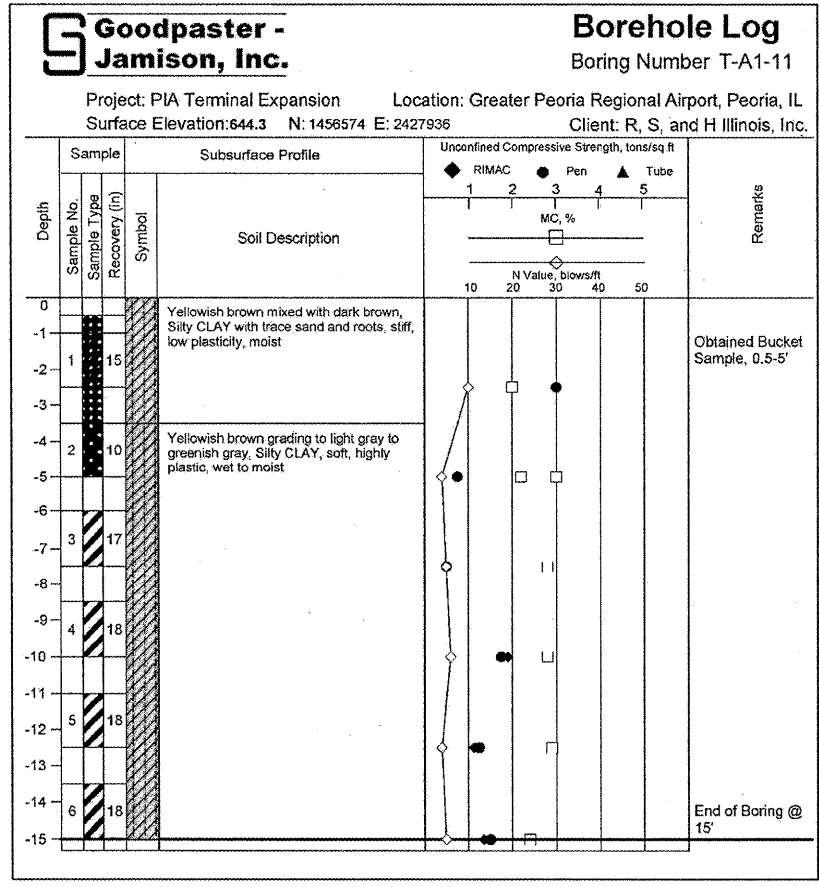
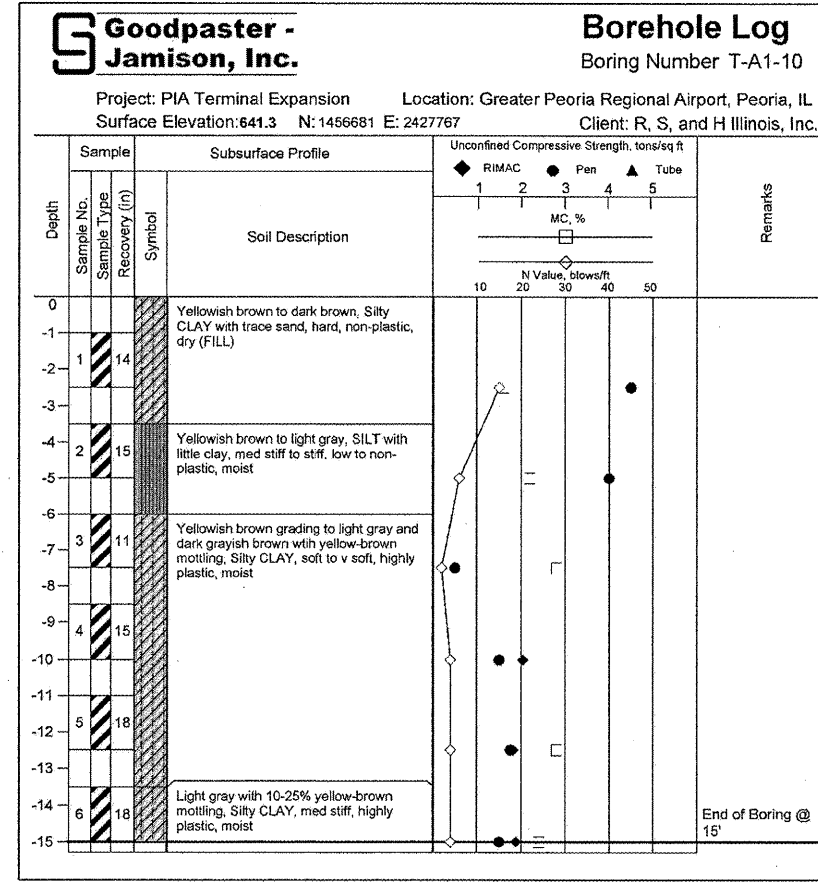
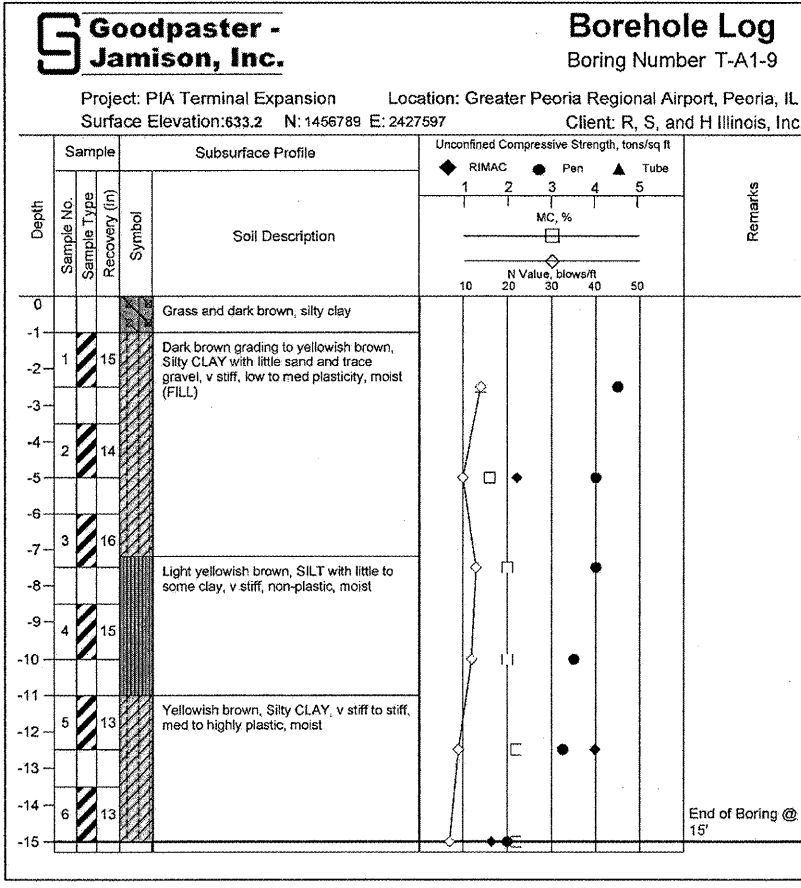
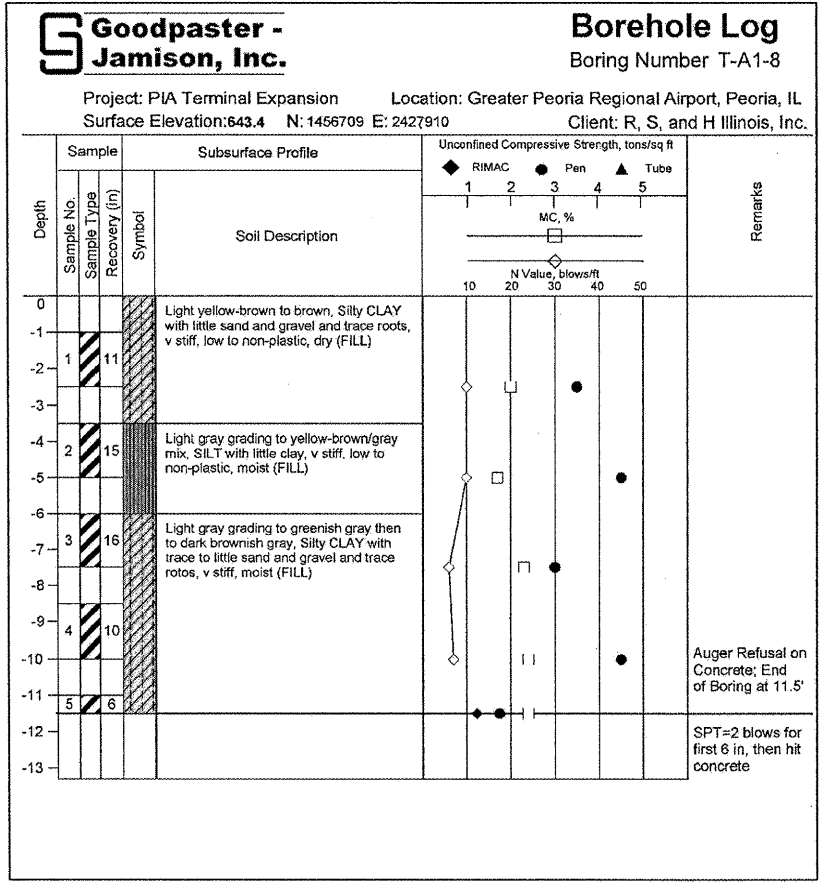
SHEET TITLE

GEOTECHNICAL
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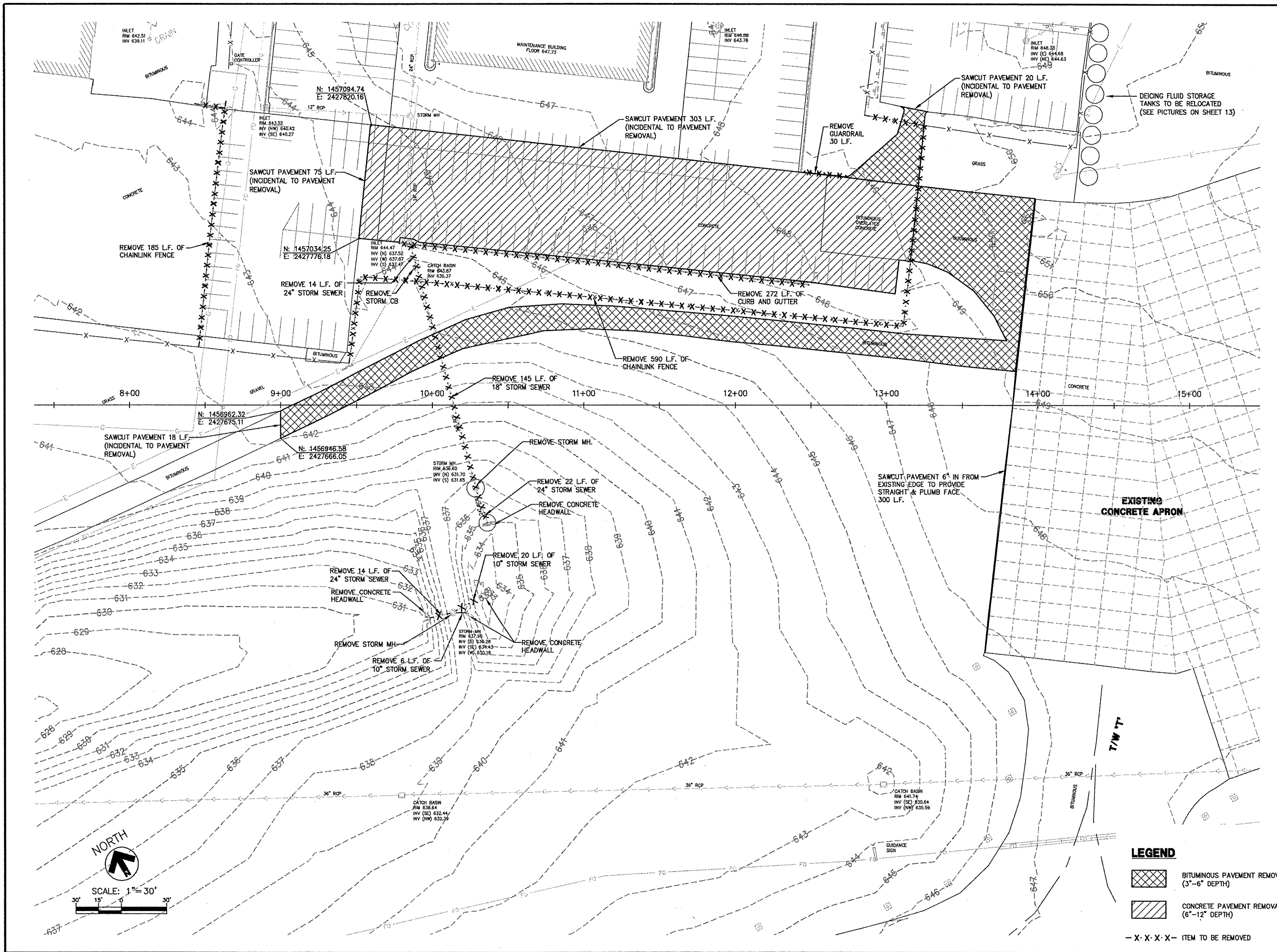
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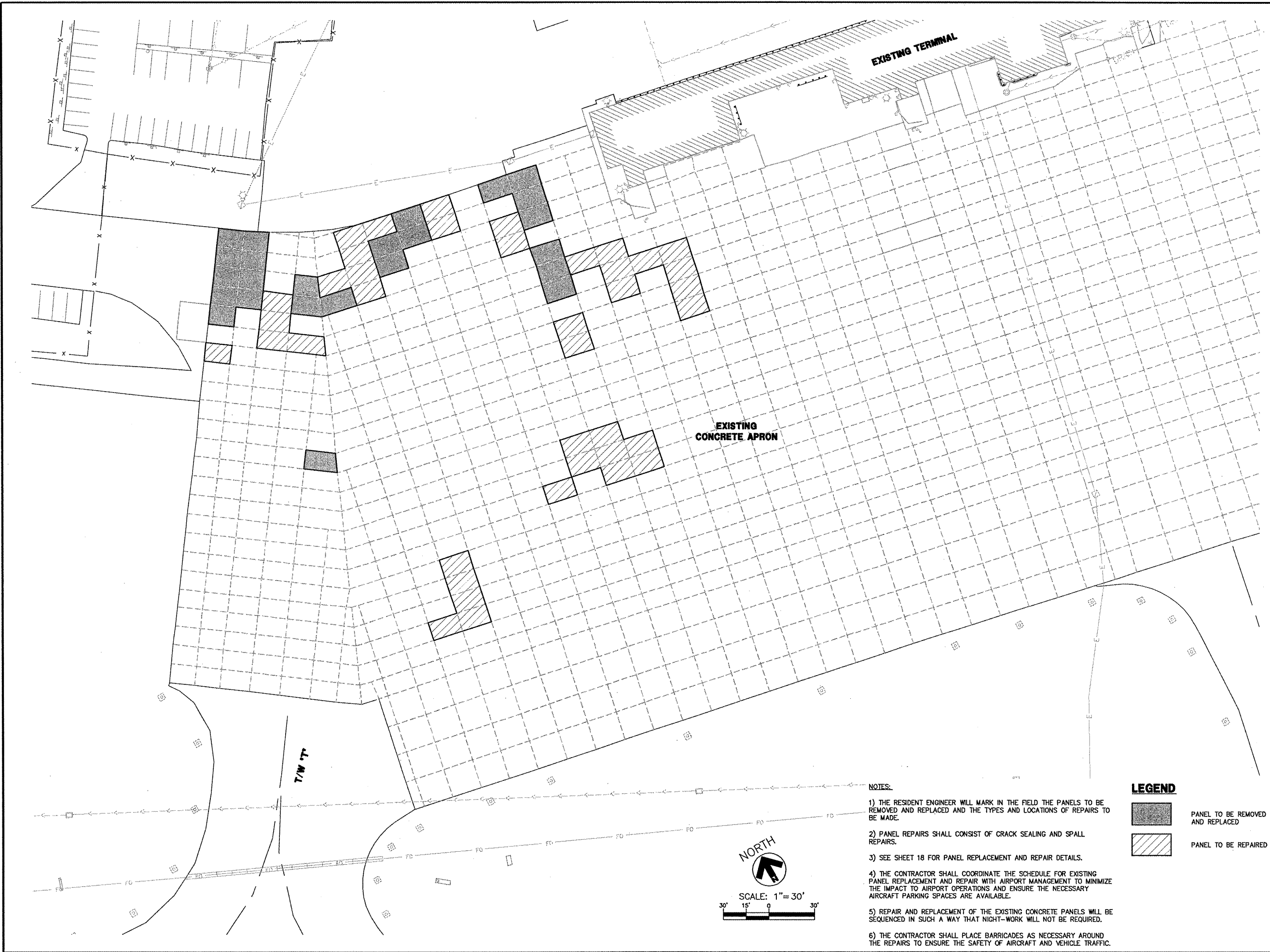
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EXISTING CONDITIONS
AND
DEMOLITION PLAN

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**EXISTING APRON
REPAIR PLAN**



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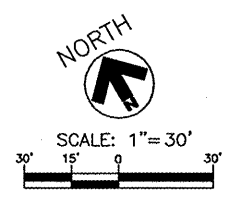
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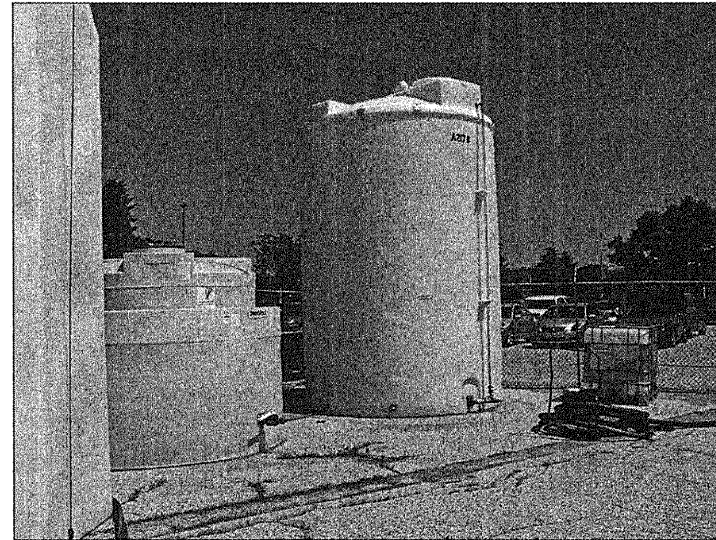
NOTES:

- 1) THE RESIDENT ENGINEER WILL MARK IN THE FIELD THE PANELS TO BE REMOVED AND REPLACED AND THE TYPES AND LOCATIONS OF REPAIRS TO BE MADE.
- 2) PANEL REPAIRS SHALL CONSIST OF CRACK SEALING AND SPALL REPAIRS.
- 3) SEE SHEET 18 FOR PANEL REPLACEMENT AND REPAIR DETAILS.
- 4) THE CONTRACTOR SHALL COORDINATE THE SCHEDULE FOR EXISTING PANEL REPLACEMENT AND REPAIR WITH AIRPORT MANAGEMENT TO MINIMIZE THE IMPACT TO AIRPORT OPERATIONS AND ENSURE THE NECESSARY AIRCRAFT PARKING SPACES ARE AVAILABLE.
- 5) REPAIR AND REPLACEMENT OF THE EXISTING CONCRETE PANELS WILL BE SEQUENCED IN SUCH A WAY THAT NIGHT-WORK WILL NOT BE REQUIRED.
- 6) THE CONTRACTOR SHALL PLACE BARRICADES AS NECESSARY AROUND THE REPAIRS TO ENSURE THE SAFETY OF AIRCRAFT AND VEHICLE TRAFFIC.

LEGEND

-  PANEL TO BE REMOVED AND REPLACED
-  PANEL TO BE REPAIRED

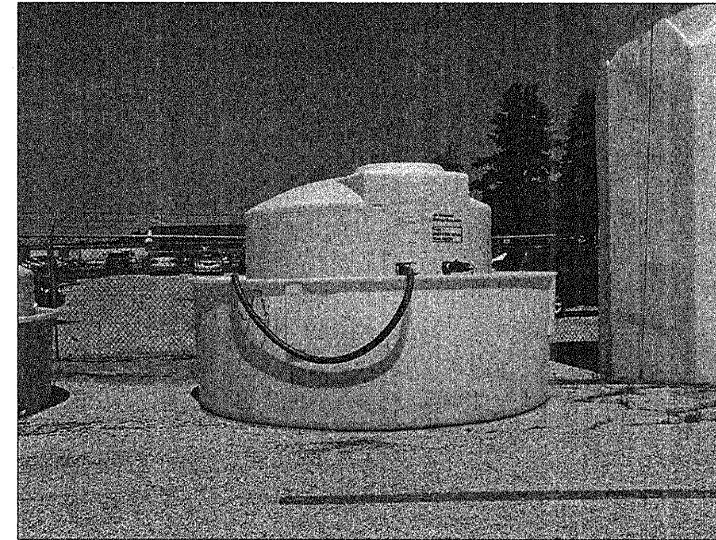




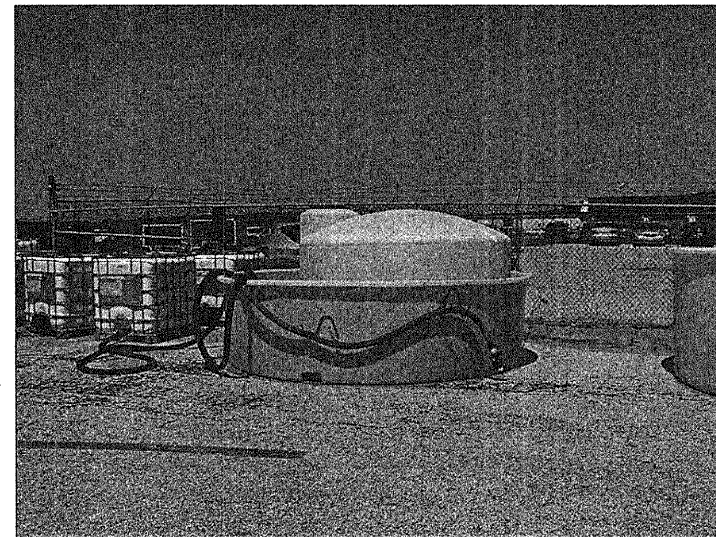
LARGE DEICING STORAGE TANKS



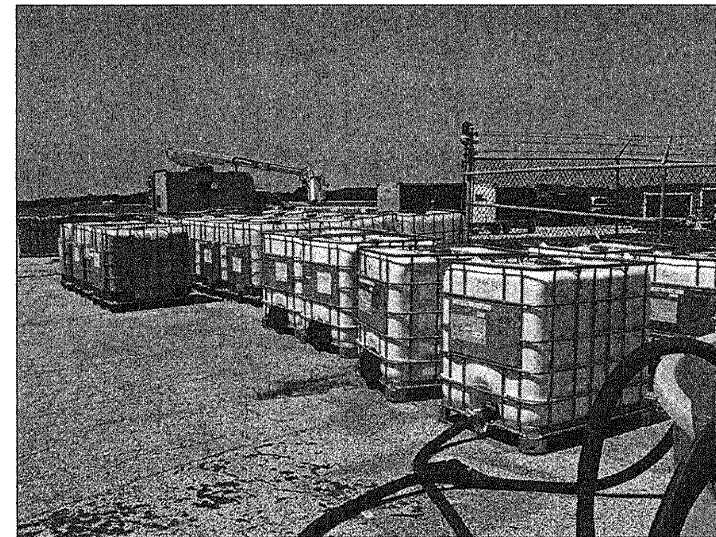
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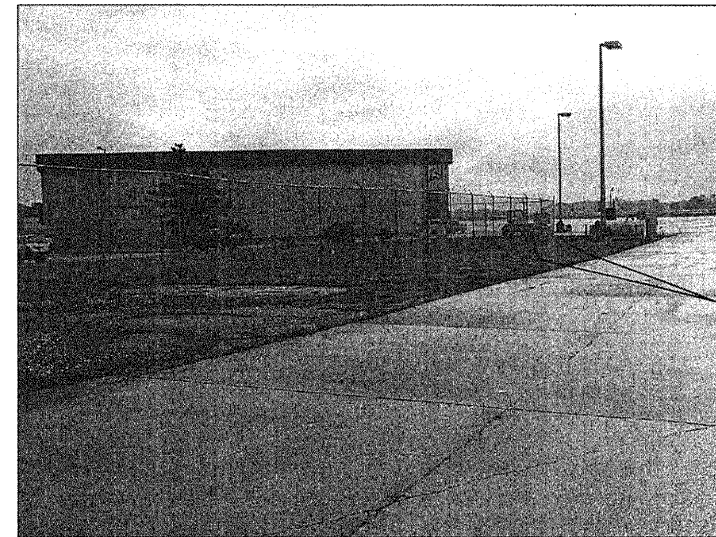
LARGE DEICING STORAGE TANKS



LARGE DEICING STORAGE TANK



SMALL DEICING STORAGE TANKS



PROPOSED RELOCATION AREA

ELECTRICAL OUTLETS
TO BE REMOVED

NOTES:

- 1) RELOCATION OF DEICING FLUID STORAGE TANKS SHALL BE COORDINATED WITH THE RESIDENT ENGINEER AND AIRPORT OPERATIONS.
- 2) CONSTRUCTION OF NEW CONCRETE PAD AND RELOCATION OF STORAGE TANKS SHALL TAKE PLACE WITHIN THE FIRST 30 DAYS OF PHASE 2 IN ORDER TO FACILITATE OTHER CONSTRUCTION IN THE AREA.
- 3) LARGE STORAGE TANKS WILL BE PUMPED OUT BY THE AIRPORT/AIRLINES PRIOR TO RELOCATION.
- 4) CARE SHALL BE TAKEN BY THE CONTRACTOR TO NOT DAMAGE ANY OF THE TANKS DURING THE RELOCATION PROCESS. DAMAGED TANKS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- 5) EXISTING ELECTRICAL OUTLETS MOUNTED ON WOOD POSTS (SEE PHOTO ABOVE) SHALL BE REMOVED BY THE CONTRACTOR. COST SHALL BE INCIDENTAL TO CONSTRUCTION OF NEW CONCRETE PAD.

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REVISIONS

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DESIGNED BY: NDO

AEP PROJECT NUMBER
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SHEET TITLE

**PHOTOS OF
STORAGE TANKS
TO BE RELOCATED**

SHEET NUMBER

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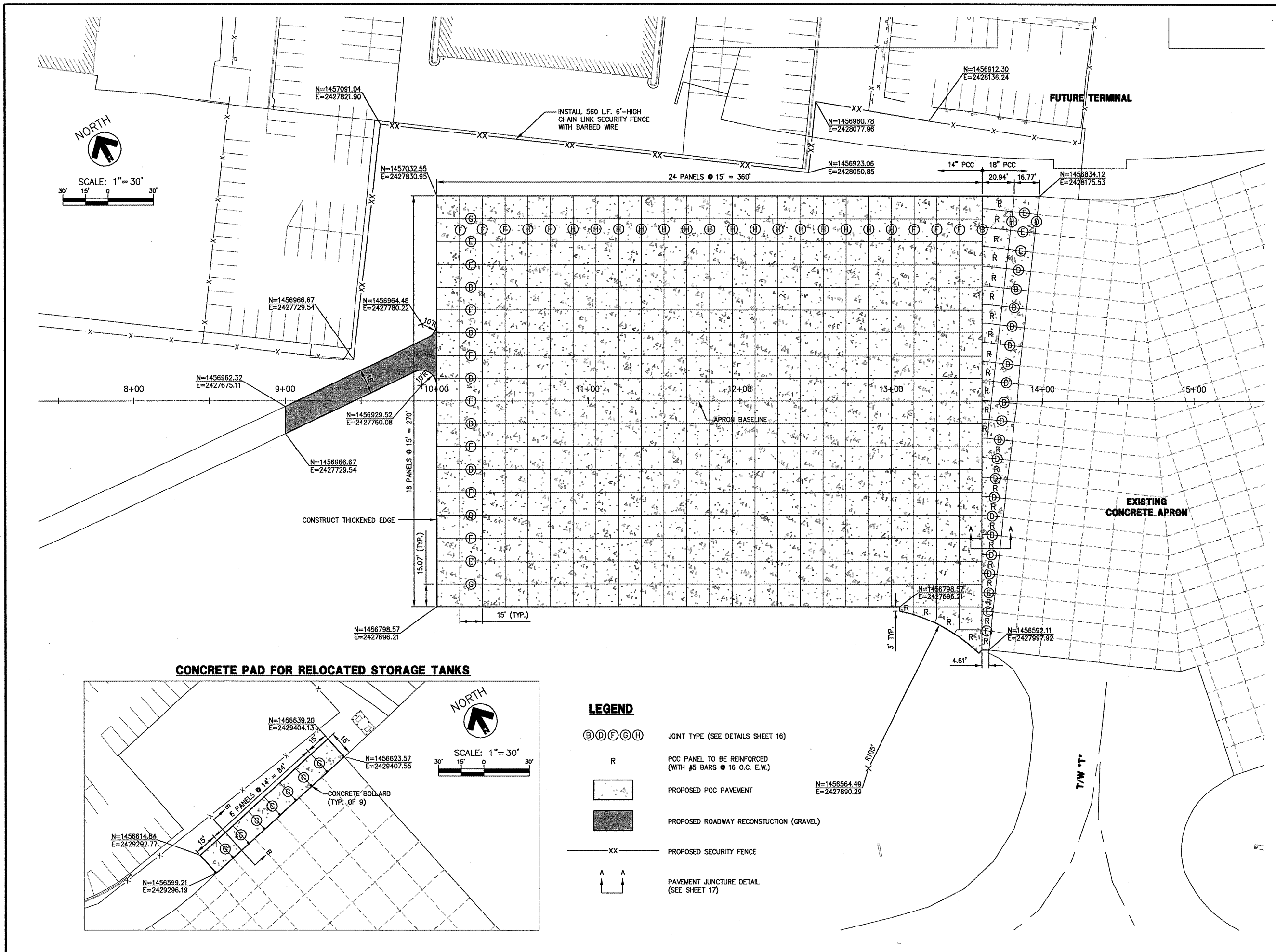
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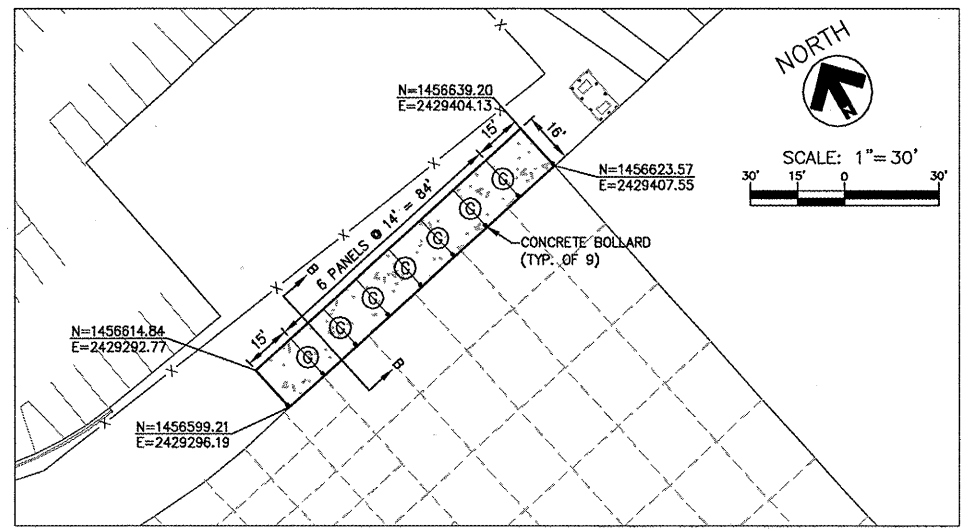
**GEOMETRY AND
JOINT LAYOUT
PLAN**

SHEET NUMBER
14 OF 43

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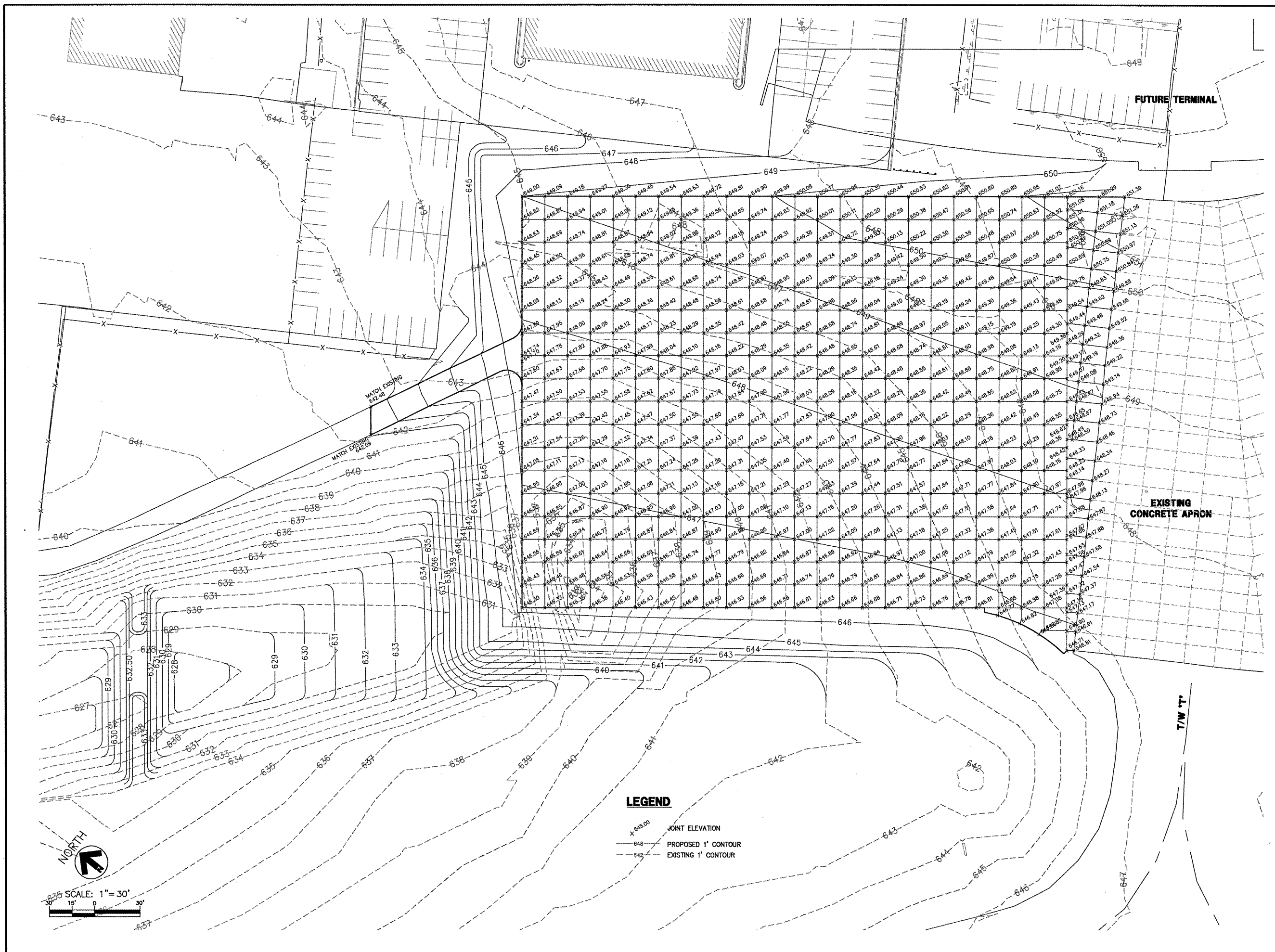


CONCRETE PAD FOR RELOCATED STORAGE TANKS



LEGEND

- (B) (D) (F) (G) (H) JOINT TYPE (SEE DETAILS SHEET 16)
- R PCC PANEL TO BE REINFORCED (WITH #5 BARS @ 16 O.C. E.W.)
- [Patterned Box] PROPOSED PCC PAVEMENT
- [Shaded Box] PROPOSED ROADWAY RECONSTRUCTION (GRAVEL)
- XX PROPOSED SECURITY FENCE
- A-A PAVEMENT JUNCTURE DETAIL (SEE SHEET 17)



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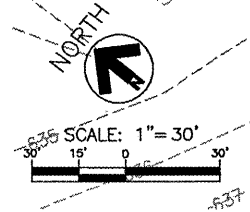
JOINT ELEVATION
 PLAN

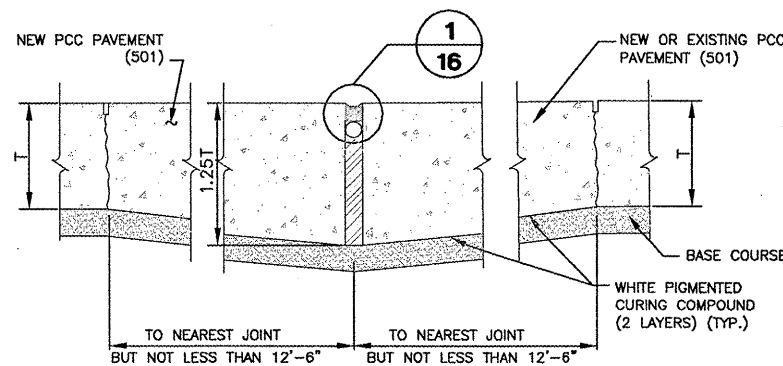
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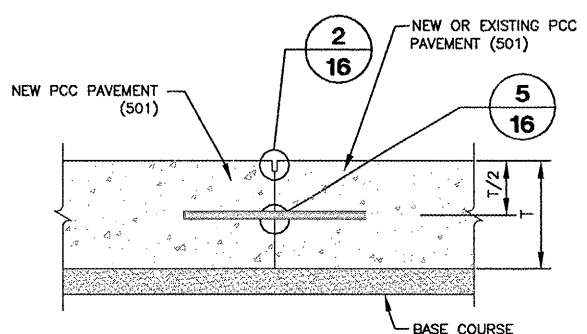
LEGEND

- 645.00 — JOINT ELEVATION
- - - 648 - - - PROPOSED 1' CONTOUR
- - - 642 - - - EXISTING 1' CONTOUR

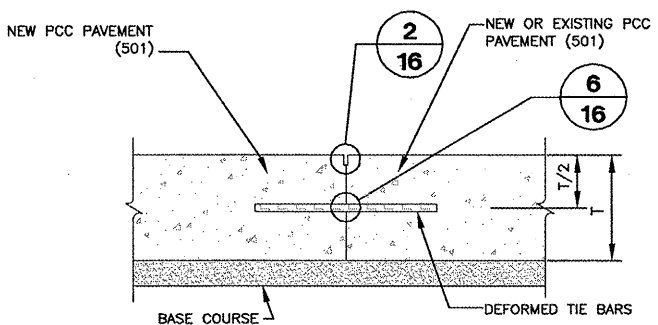




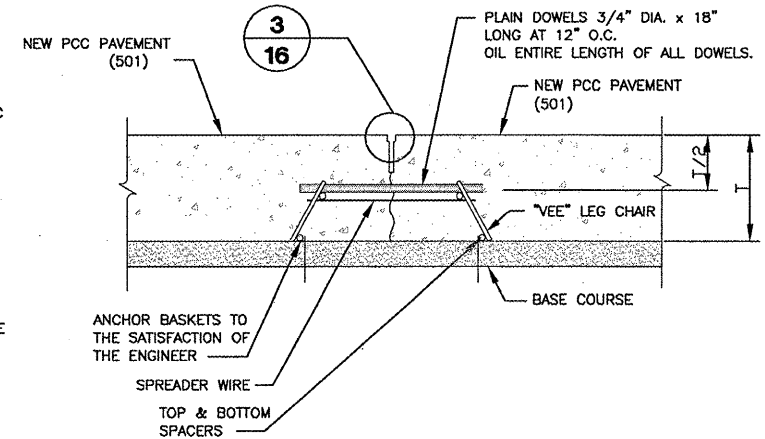
B TYPE B THICKENED EDGE EXPANSION JOINT DETAIL
NTS



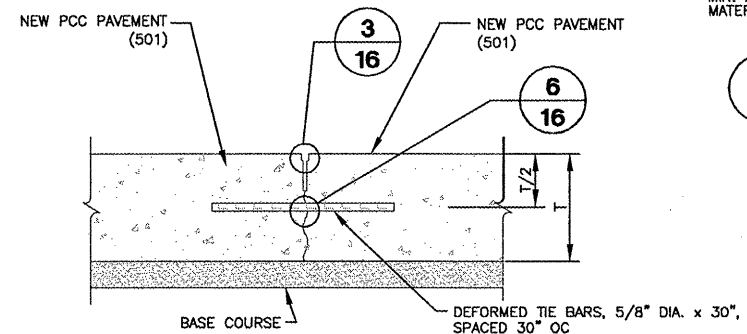
D TYPE D CONSTRUCTION JOINT DETAIL
NTS



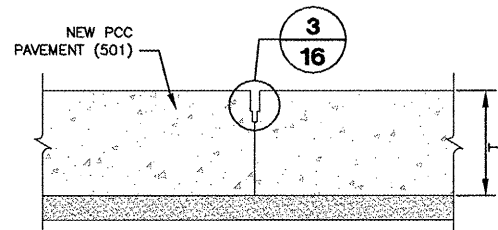
E TYPE E HINGED CONSTRUCTION JOINT DETAIL
NTS



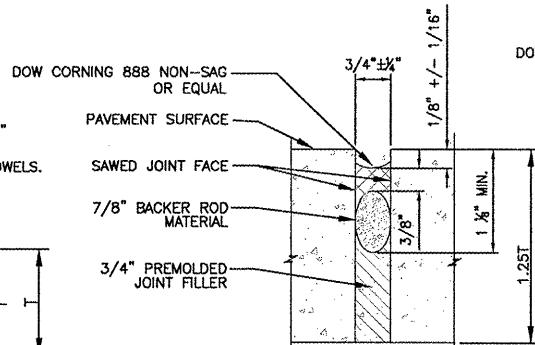
F TYPE F CONTRACTION JOINT DETAIL
NTS



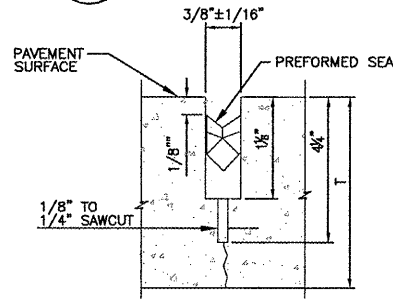
G TYPE G HINGED CONTRACTION JOINT DETAIL
NTS



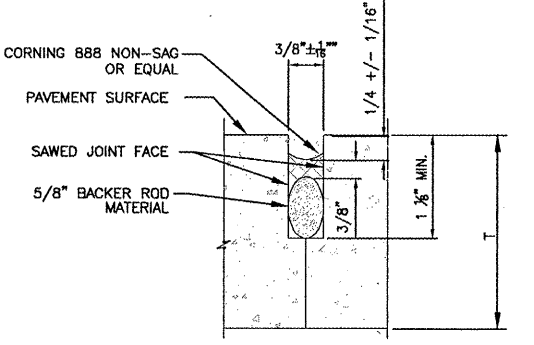
H CONTRACTION JOINT DETAIL
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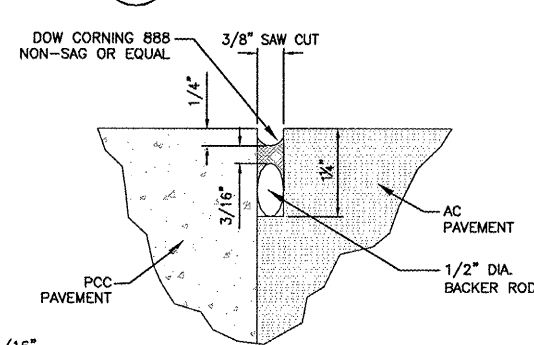
1 EXPANSION JOINT
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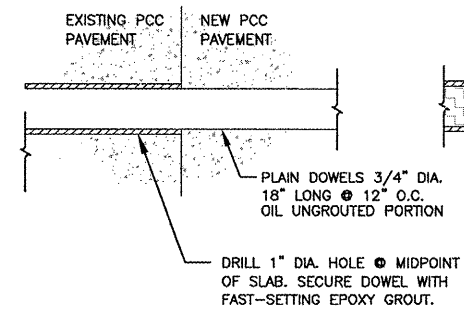
3 CONTRACTION JOINT
NTS



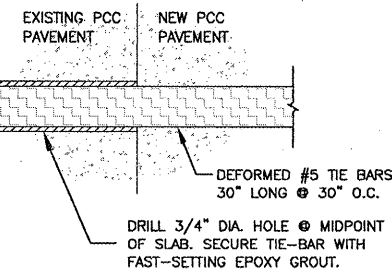
2 CONSTRUCTION JOINT
NTS



4 PCC-AC JOINT SEAL DETAIL
NTS



5 DOWEL DETAIL
NTS



6 TIE-BAR DETAIL
NTS

NOTE: T=14" OR 18" THIS PROJECT

MATERIAL SCHEDULE			
ITEM	SIZE	STANDARD	REMARKS
DOWELS, D & F JOINTS	3/4" INCH DIA x 18" INCHES LONG @ 12" O.C.	ASTM A 615 GRADE 60	EPOXY COATED, INCLUDING ENDS, AASHTO M254
TIE BARS	NO. 5 DEFORMED BARS x 30 INCHES LONG @ 30 INCHES O.C.	ASTM A 615 GRADE 40 OR 60	EPOXY COATED, MAY FIELD BEND GRADE 40
REINFORCED SLABS TYPE R	NO. 5 DEFORMED BAR MATS 16 INCHES O.C. EACH WAY	ASTM A 184 GRADE 60 ASTM A 615 GRADE 60	MESH NOT ALLOWED USE 12 INCH GRID OVERLAP MAY FIELD TIE REBAR EPOXY COATED, GRADE 40
OTHER REINFORCING	AS NOTED	ASTM A 615 GRADE 40 OR 60 UNLESS OTHERWISE NOTED	FIELD BEND GRADE 40 ONLY EPOXY COATED
EXPANSION BOARD	1" WIDTH x DEPTH REQUIRED	ASTM D 1751 OR D 1752	USE REMOVABLE CAP FOR JOINT SEAL
DOWEL EPOXY	AS REQUIRED	ASTM C 881, TYPE IV, GRADE 3	CLASS C FOR INSTALLATION ABOVE 60 F CLASS B FOR INSTALLATION 40 F TO 60 F CLASS A FOR INSTALLATION BELOW 40 F
SILICONE SEALANT	FIELD APPLIED AS REQUIRED	ASTM D 5893	SELF LEVELING USE MANUFACTURERS PRIMER

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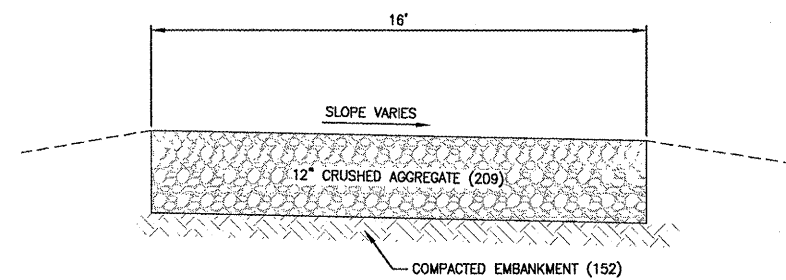
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JOINT DETAILS

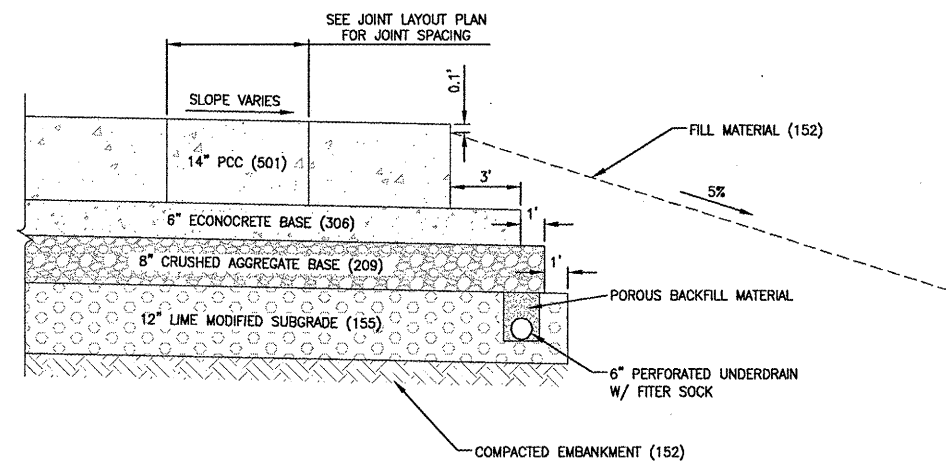
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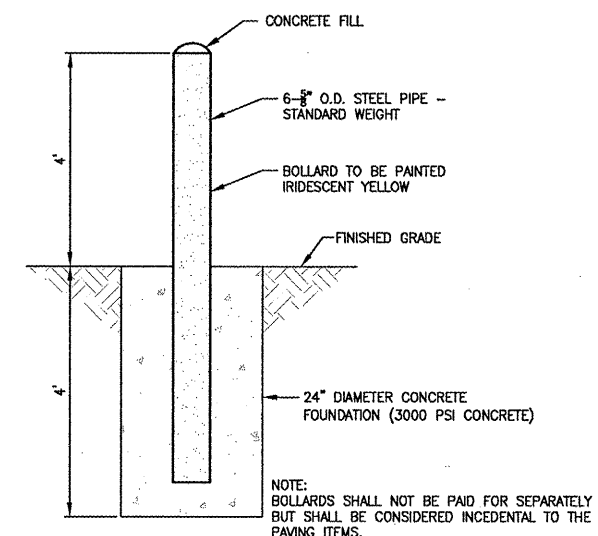
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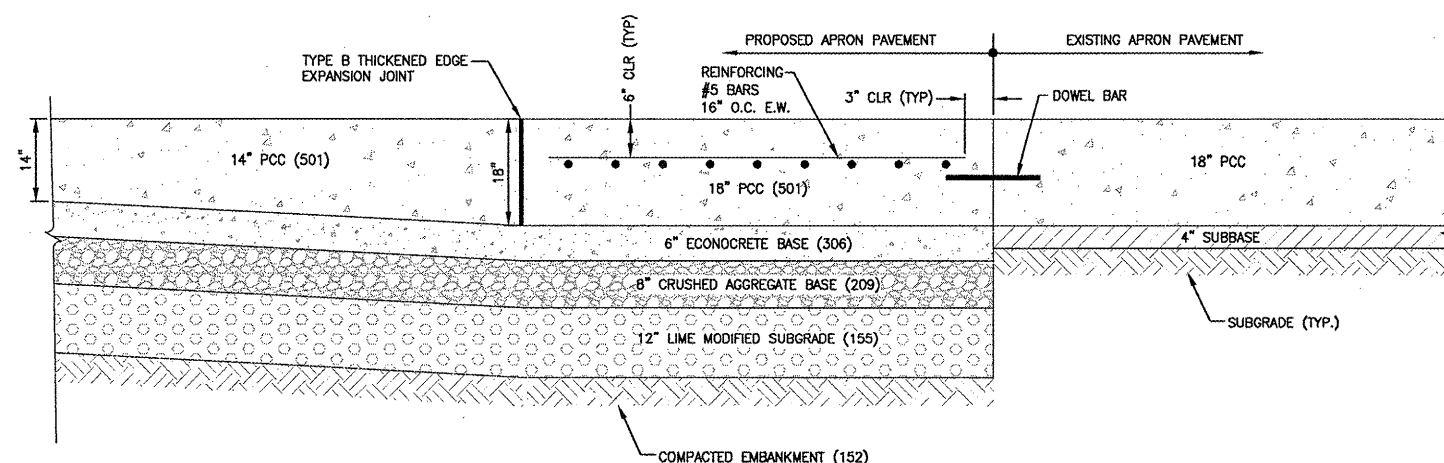
TYPICAL PERIMETER ROAD PAVEMENT SECTION
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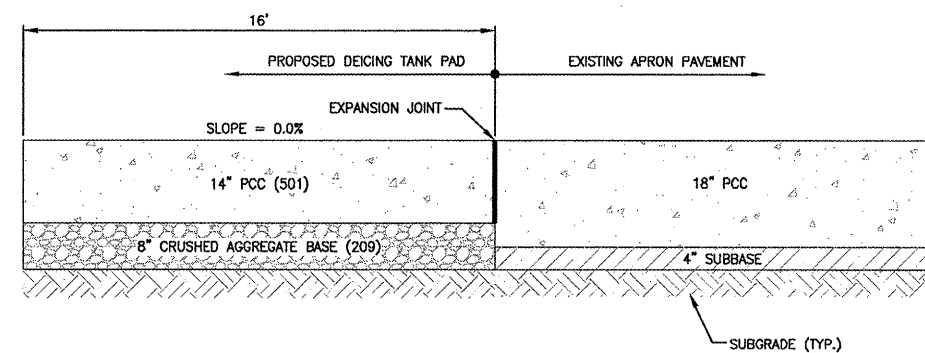
TYPICAL PCC APRON PAVEMENT SECTION
NTS



TYPICAL BOLLARD DETAIL
NTS



JUNCTION DETAIL A-A
NTS



STORAGE TANK PAD SECTION B-B
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SHEET TITLE

TYPICAL
PAVEMENT
SECTIONS
AND DETAILS

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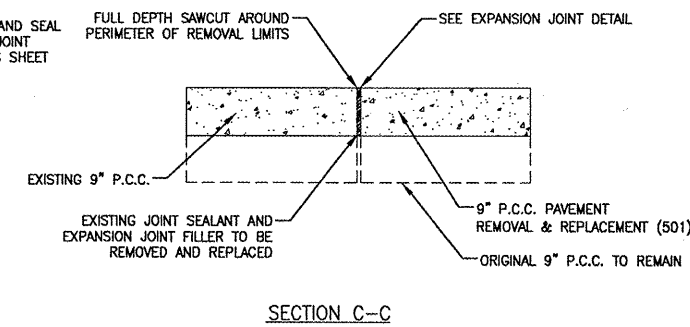
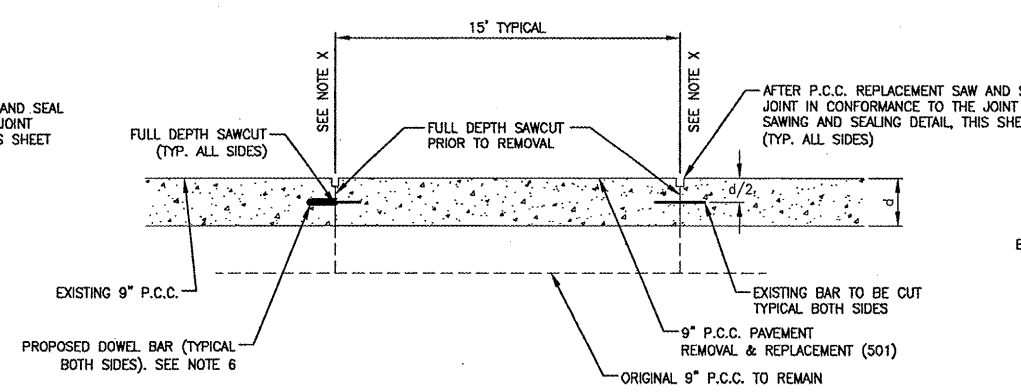
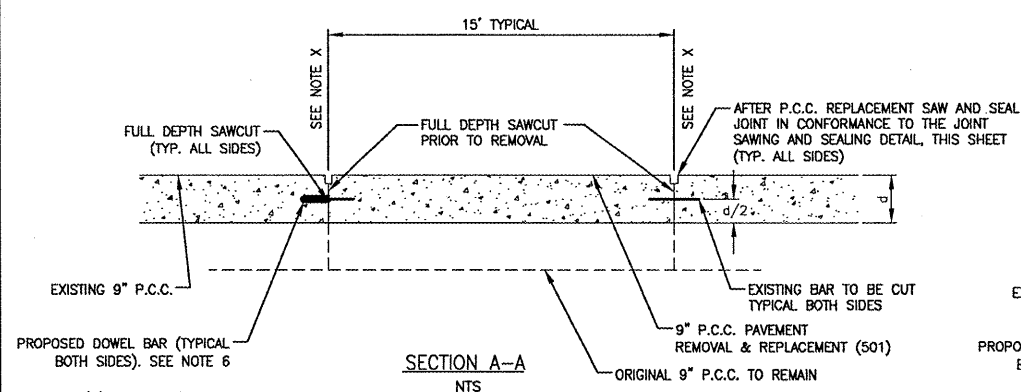
SHEET TITLE

TYPICAL PANEL
REMOVAL AND
REPAIR DETAILS

SHEET NUMBER

18 OF 43

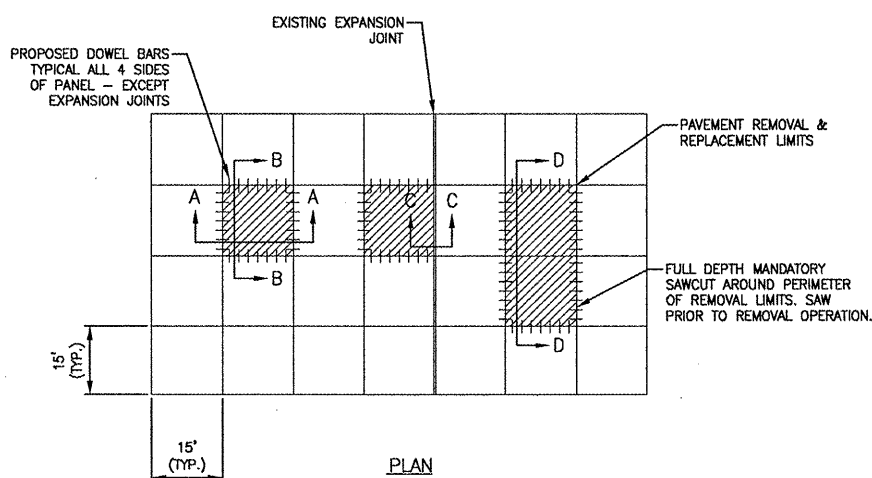
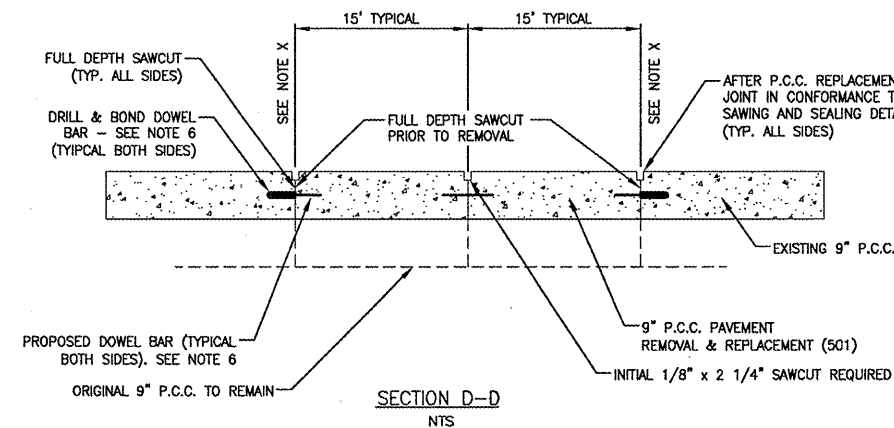
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NOTE 'X':
THE CONTRACTOR SHALL SAWCUT 8" AT THE EXISTING PAVEMENT JOINTS THROUGH EXISTING DOWEL AND TIE BARS. THE CONTRACTOR SHALL DRILL AND BOND NEW DOWEL BARS IN THE EXISTING CONCRETE. THE NEW DOWELS SHALL BE 1" IN DIAMETER, 19" LONG PLACED AT 12" CENTERS. DOWEL BARS ARE REQUIRED REGARDLESS IF THE EXISTING JOINT HAS TIE BARS, DOWEL BARS, NO DOWELS SHALL BE INSTALLED AT EXPANSION JOINTS.

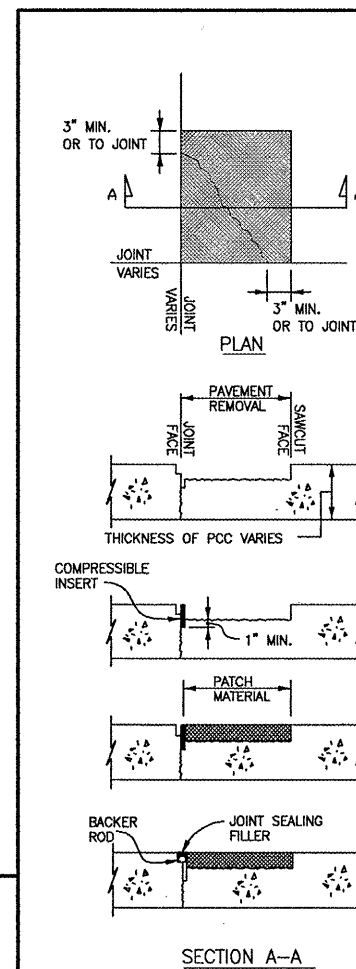
NOTES:

- PANEL REMOVAL AND REPLACEMENT SHALL BE REQUIRED AT THE LOCATIONS SHOWN OR AS DESIGNATED BY THE RESIDENT ENGINEER.
- THE RESIDENT ENGINEER SHALL DETERMINE THE ACTUAL LIMITS OF PANEL REMOVAL AND REPLACEMENT.
- THE CONTRACTOR SHALL USE CARE TO NOT DAMAGE THE UNDERLYING P.C.C. PAVEMENT WHEN REMOVING THE TOP LAYER OF P.C.C. PAVEMENT. ANY DAMAGE CAUSED SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- THE JOINT PATTERN IN THE NEW PAVEMENT REPLACEMENT SHALL MATCH THAT OF THE ORIGINAL PAVEMENTS. JOINTS IN THE ORIGINAL PAVEMENT SHALL BE RE-SAWED AND SEALED IN THE NEW CONCRETE. SAWED JOINT DEPTHS AND DETAILS SHALL CONFORM TO THE JOINT SAWING AND SEALING DETAILS SHOWN ON SHEET 16. THE JOINT AROUND THE PERIMETER OF THE REPLACEMENT SHALL BE SAWED AND SEALED CONFORMING TO THIS SAME DETAIL.
- A FULL DEPTH SAWCUT AROUND THE PERIMETER SHALL BE REQUIRED PRIOR TO PANEL REMOVAL AND REPLACEMENT.
- DRILL HARDENED P.C.C. AND BOND DOWEL BARS IN PLACE USING A POLYESTER RESIN MATERIAL CONFORMING TO PORTLAND CEMENT CONCRETE PAVEMENT SECTION IN THE TECHNICAL SPECIFICATIONS.
- IN THE EVENT THAT THE RESIDENT ENGINEER DETERMINES THAT THE UNDERLYING PCC PAVEMENT IS FRACTURED OR IN A CONDITION SUCH THAT THERE IS THE POTENTIAL FOR REFLECTIVE CRACKING IN THE REPLACEMENT SURFACE PCC LAYER, THE SURFACE PCC LAYER SHALL BE REINFORCED WITH #5 BARS AT 8" O.C. E.W. THIS REINFORCEMENT, IF NECESSARY WILL BE PAID FOR UNDER ITEM AR801704 PCC PANEL REINFORCEMENT (SPECIAL).



PANEL REMOVAL & REPLACEMENT DETAILS

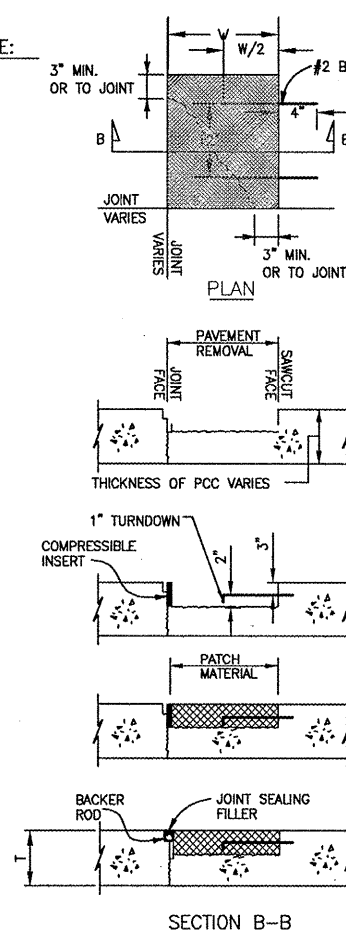
NTS



SHALLOW SPALL REPAIR DETAILS

NTS

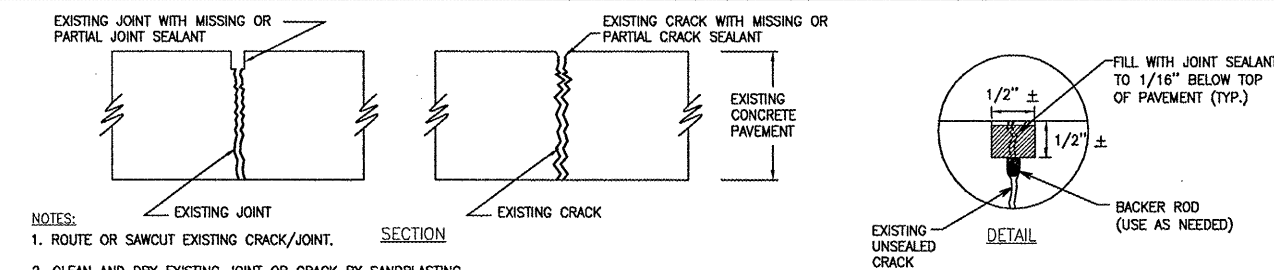
- NOTES:
- SPALL REPAIR PATCH MATERIAL SHALL BE ITEM 610 PCC WITH POLY FIBERS AND 3/4" NOMINAL AGGREGATE SIZE.
 - COAT CAVITY WITH EPOXY BONDING AGENT.
 - MAX DEPTH IS 4".
 - THIS WORK SHALL BE PAID FOR UNDER ITEM AR801702 PART DEPTH PCC PAVEMENT REPAIR, 2"-4".



DEEP SPALL REPAIR DETAILS

NTS

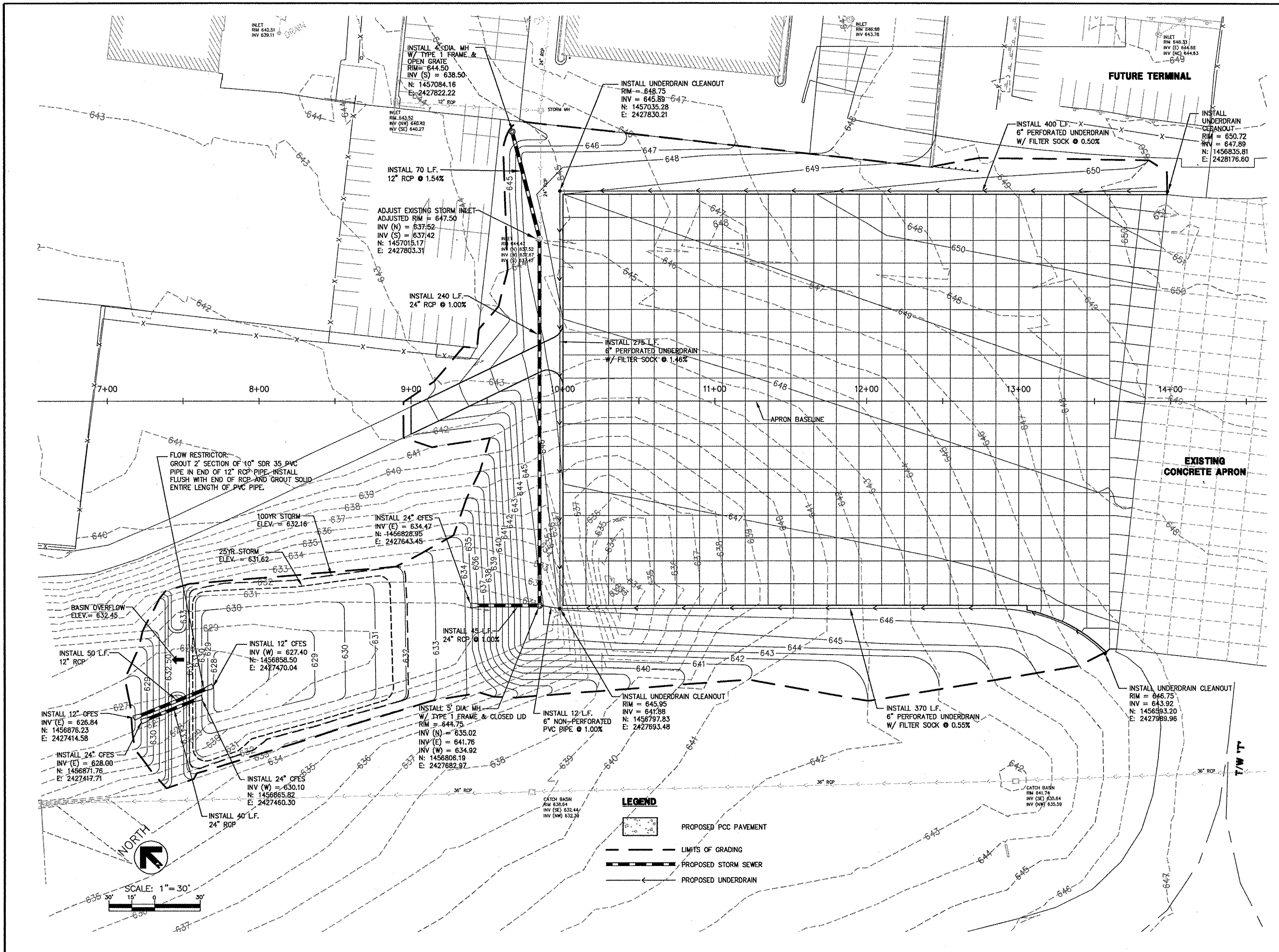
- NOTES:
- SPALL REPAIR PATCH MATERIAL SHALL BE ITEM 610 PCC WITH POLY FIBERS AND 3/4" NOMINAL AGGREGATE SIZE.
 - COAT CAVITY WITH EPOXY BONDING AGENT.
 - MAX DEPTH IS 0.67xT (T=CONCRETE PAVEMENT THICKNESS).
 - DEEPER REPAIR REQUIRES PARTIAL PANEL REPLACEMENT.
 - THIS WORK SHALL BE PAID FOR UNDER ITEM AR801703 PART DEPTH PCC PAVEMENT REPAIR, 4"-6".



CRACK ROUTING AND SEALING DETAILS

NTS

- NOTES:
- ROUTE OR SAWCUT EXISTING CRACK/JOINT.
 - CLEAN AND DRY EXISTING JOINT OR CRACK BY SANDBLASTING AND AIRBLASTING. CRACK OR JOINT SHALL BE DUST-FREE PRIOR TO INSTALLATION OF SEALANT.
 - RESEAL CRACK OR JOINT WITH HOT-POUR JET FUEL RESISTANT SEALANT (ASTM D-1854 OR D-3581) OR DOW CORNING 888 NON-SAG SILICONE JOINT SEALANT.
 - THIS WORK SHALL BE PAID FOR UNDER AR605541 CLEAN AND SEAL CRACKS.



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IL PROJECT: PIA-3812
 AIP PROJECT: 3-17-0080-46

REVISIONS

NO.	DESCRIPTION	DATE

DATE ISSUED: 06/27/08

REVIEWED BY: PCB

DRAWN BY: MJL

DESIGNED BY: NDO

AEP PROJECT NUMBER
 213-6945-000

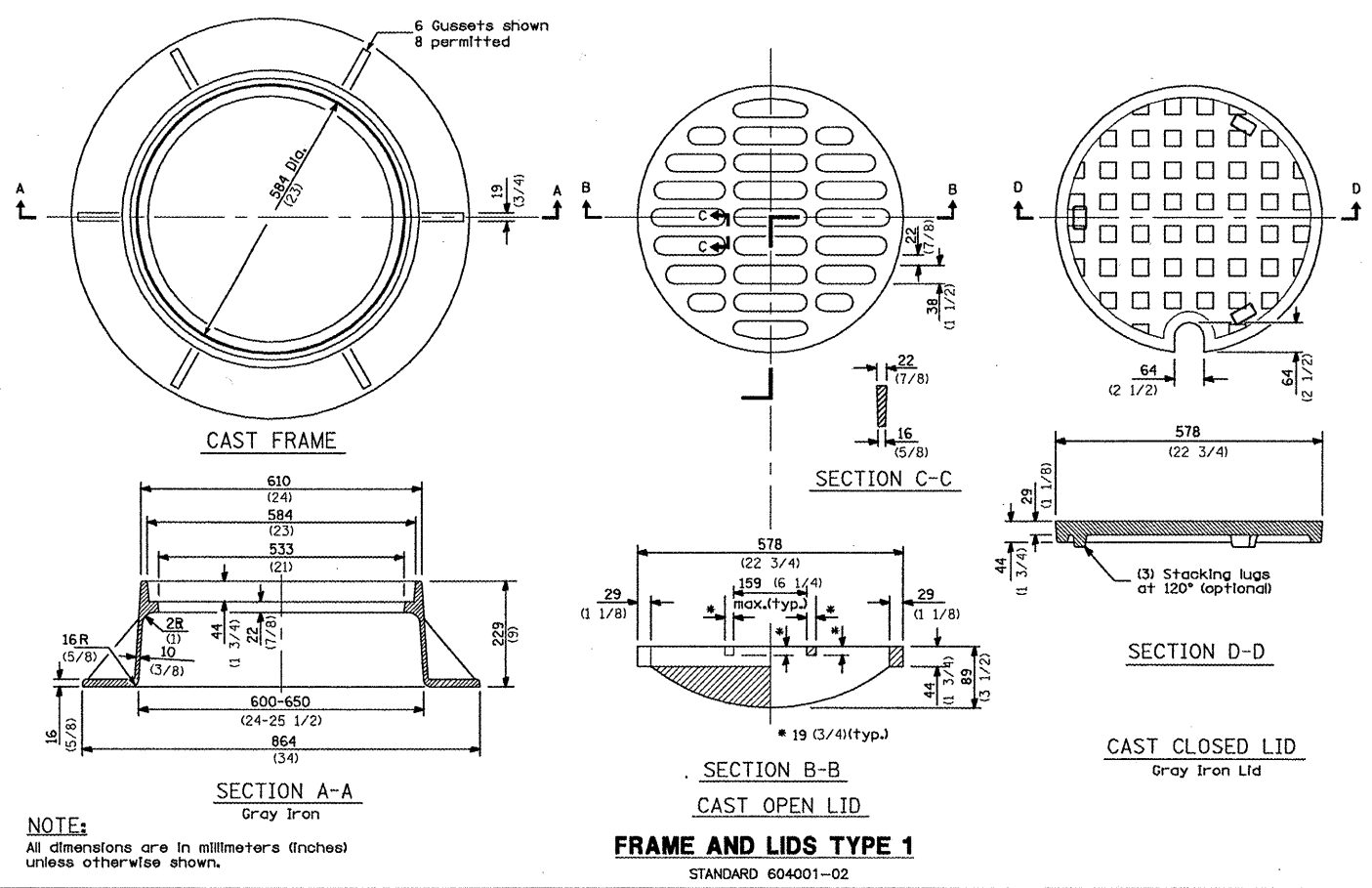
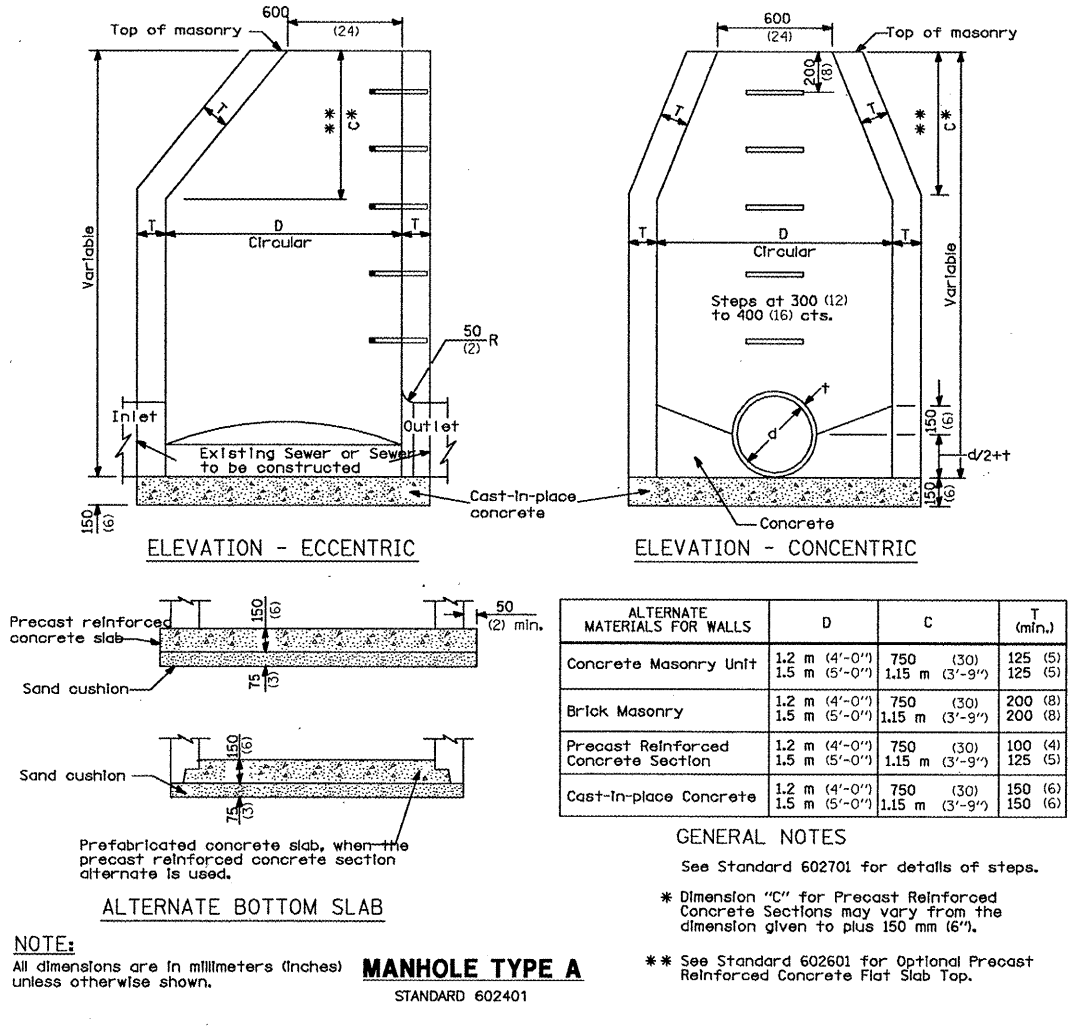
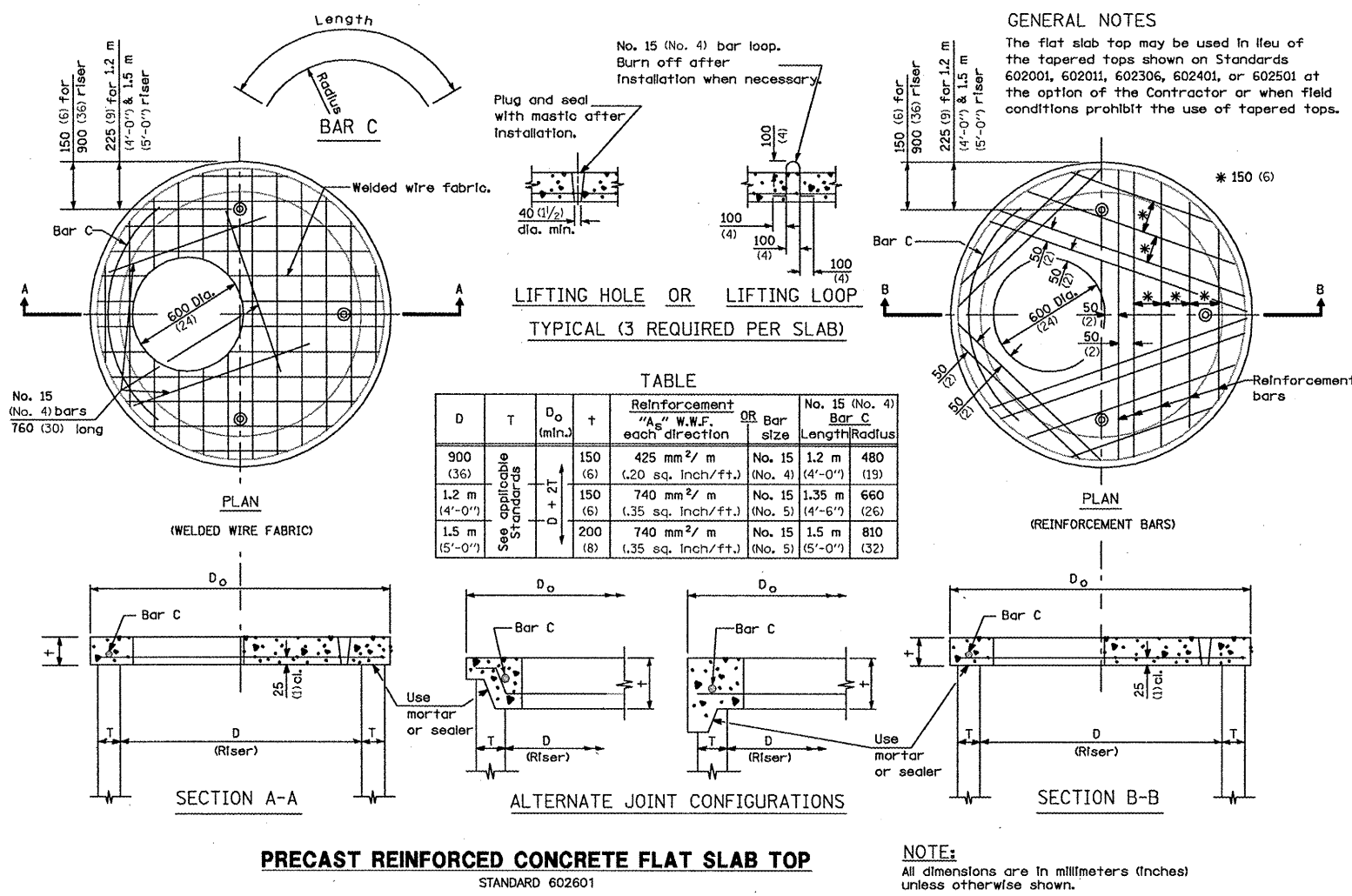
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SHEET TITLE

**GRADING AND
 DRAINAGE PLAN**

SHEET NUMBER
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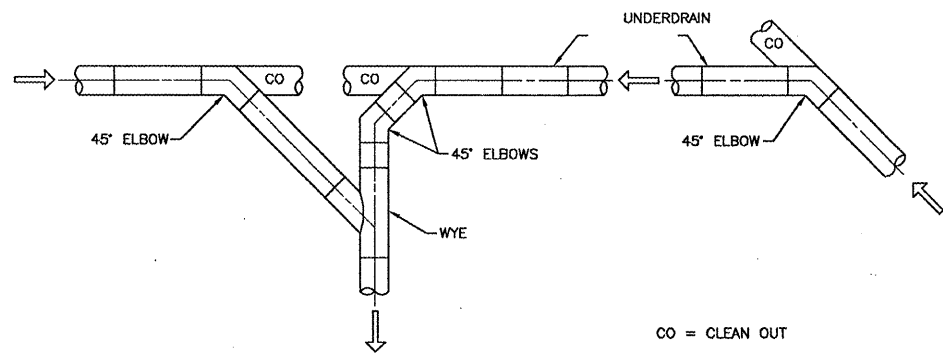
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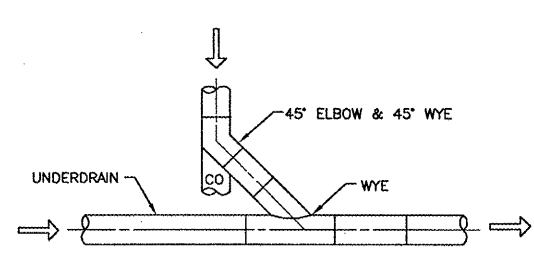
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DETAILS**

**SHEET NUMBER
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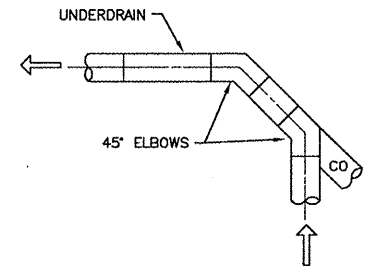
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TYPICAL JUNCTION PLAN
N.T.S.



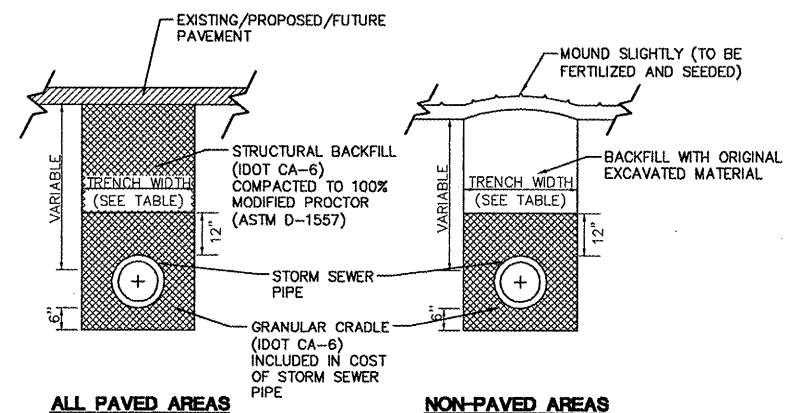
TYPICAL 'T' PLAN
N.T.S.



90 DEGREE BEND PLAN
N.T.S.

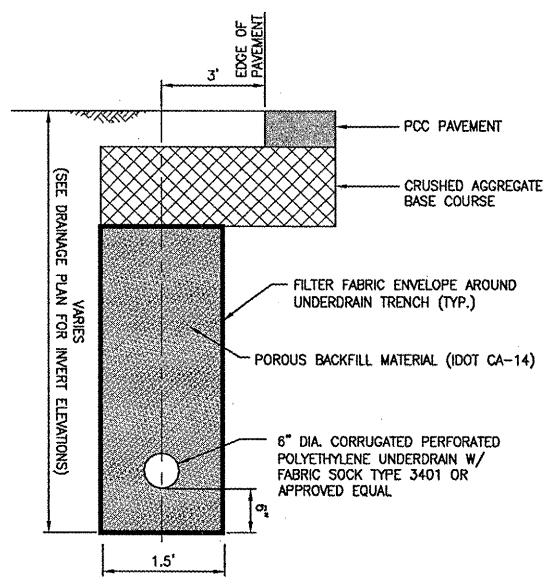
CO = CLEAN OUT

INSIDE DIAMETER OF STORM SEWER (INCHES)	MAXIMUM TRENCH WIDTH
6	3'-7"
8	3'-9"
12	4'-2"
15	4'-6"
18	4'-9"
21	5'-0"
24	5'-4"
27	5'-7"
30	5'-11"
36	6'-6"
42	7'-1"
48	7'-8"
54	8'-3"
60	8'-10"

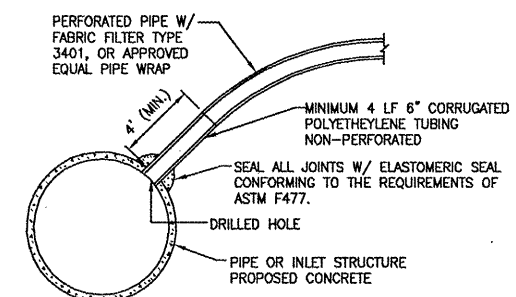


STORM SEWER TRENCH DETAIL
N.T.S.

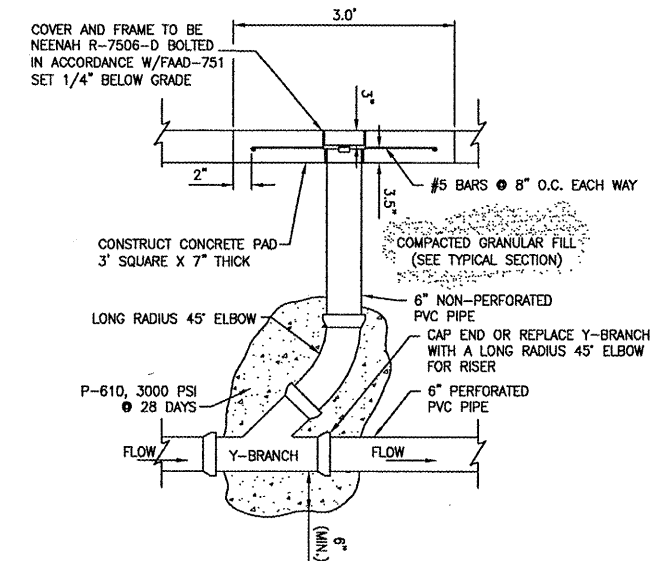
- NOTES:**
1. GRANULAR BACKFILL SHALL BE INSTALLED FIVE FEET BEYOND EDGE OF PROPOSED PAVEMENT.
 2. TRENCH BEDDING AND BACKFILL IS INCIDENTAL TO PIPE INSTALLATION.



TYPICAL TRENCH DETAIL FOR UNDERDRAIN
N.T.S.



UNDERDRAIN CONNECTION TO STORM SEWER
N.T.S.



UNDERDRAIN CLEANOUT DETAIL
N.T.S.

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**DRAINAGE
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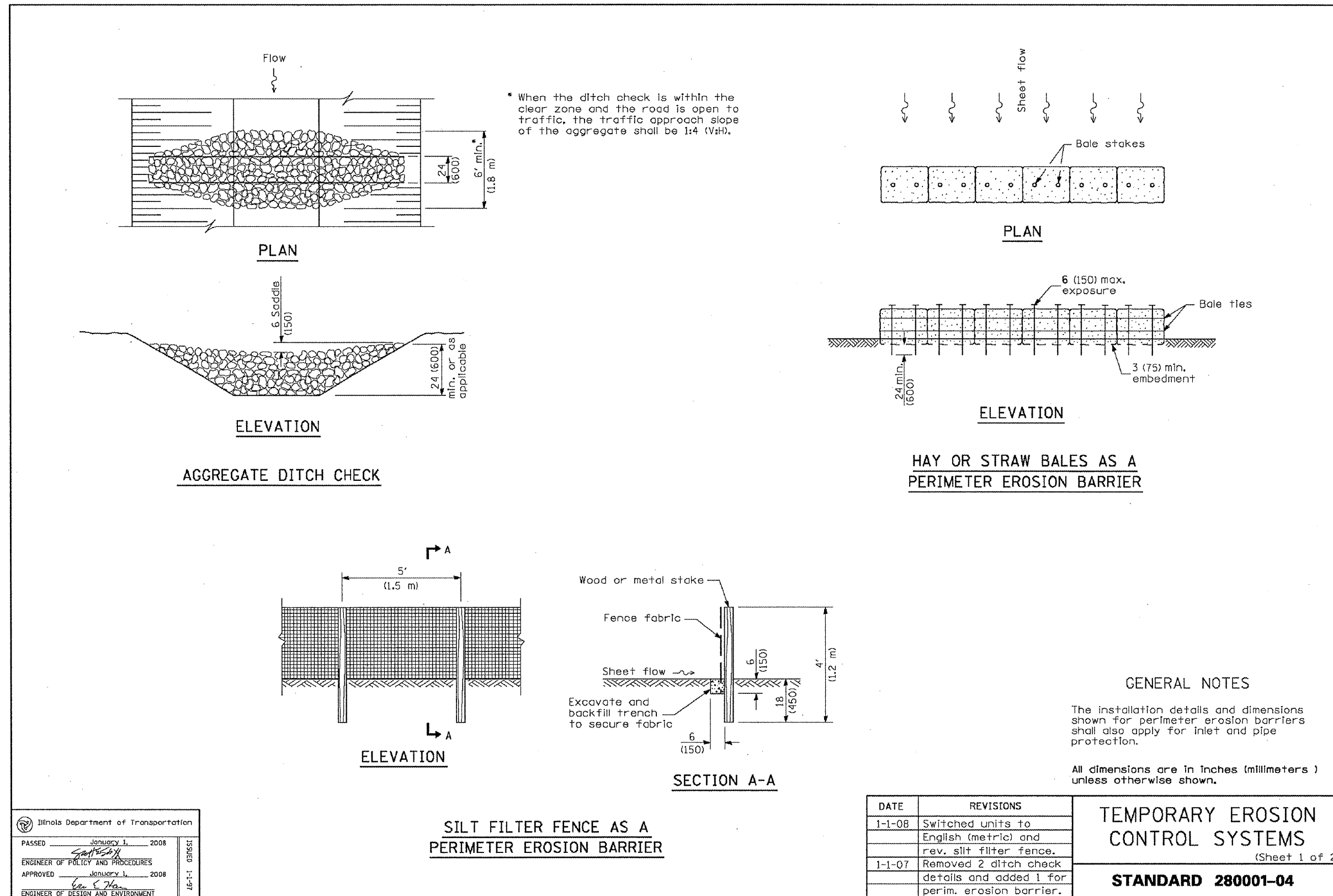
SHEET TITLE

EROSION AND
SEDIMENTATION
CONTROL DETAILS

SHEET NUMBER

24 OF 43

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Illinois Department of Transportation

PASSED January 1, 2008

ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2008

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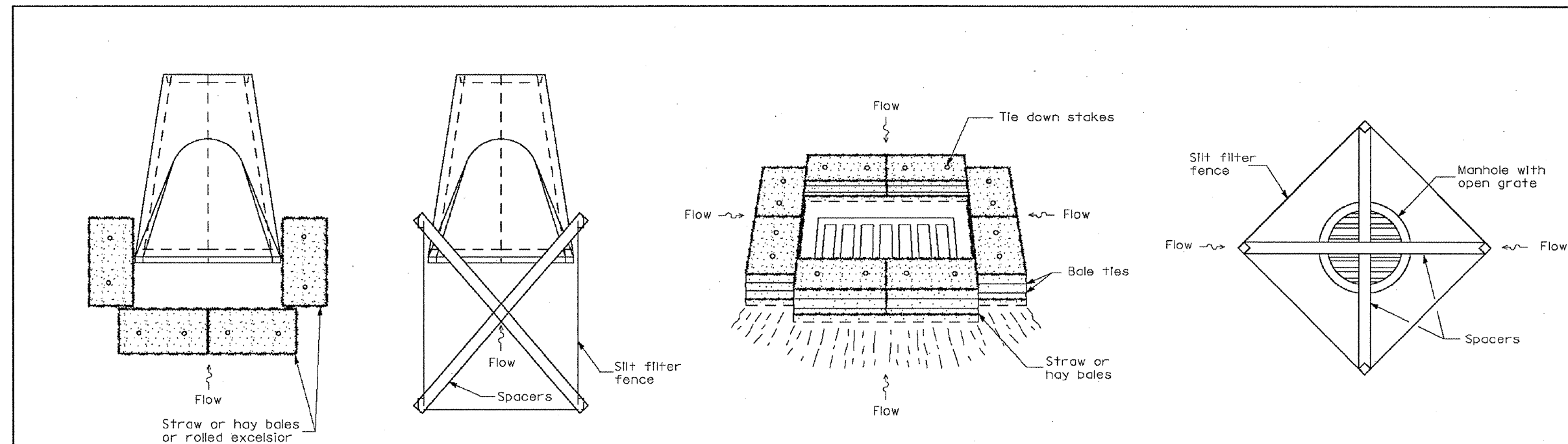
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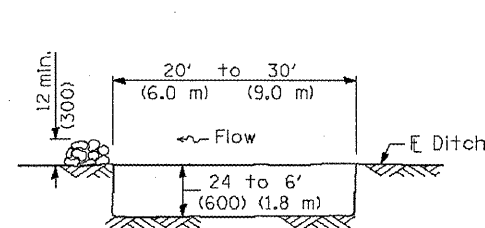
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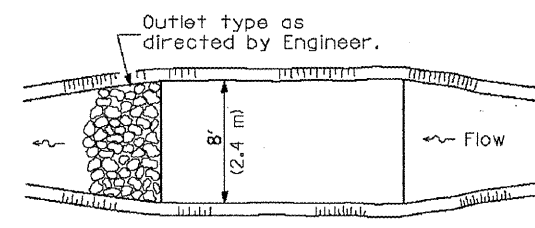
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INLET AND PIPE PROTECTION



ELEVATION

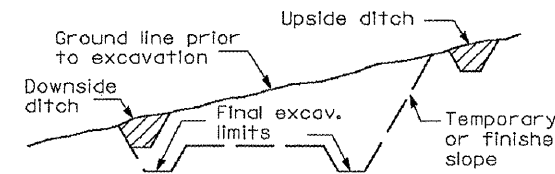


PLAN

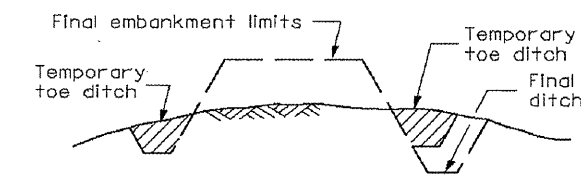
The performance of the basin will improve if put into a series.

The long dimension should be parallel with the direction of the flow. Accumulated silt shall be removed anytime the basins become 75% filled.

SEDIMENT BASIN



TYPICAL CUT CROSS-SECTION



TYPICAL FILL CROSS-SECTION

TEMPORARY DITCHES FOR CUT & FILL SECTIONS

TEMPORARY EROSION
CONTROL SYSTEMS

(Sheet 2 of 2)

STANDARD 280001-04

Illinois Department of Transportation

PASSED January 1, 2008
ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2008
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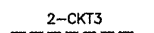
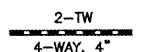

ISSUED 1-1-07

ELECTRICAL GENERAL NOTES:

- THE CONTRACTOR SHALL PROVIDE ALL MATERIALS AND LABOR TO RELOCATE, MODIFY AND INSTALL THE AIRFIELD ELECTRICAL SYSTEMS AS INDICATED ON THE DRAWINGS. ITEMS NOT SHOWN BUT OBVIOUSLY NECESSARY FOR COMPLETION OF THE WORK SHALL BE INCLUDED.
- ITEMS SHOWN IN SCREEN (HALFTONE OR LIGHT) ARE EXISTING OR CIVIL ITEMS. ITEMS SHOWN IN SOLID (BOLD) ARE NEW TO BE INSTALLED UNDER THIS CONTRACT, UNLESS OTHERWISE NOTED.
- THE INSTALLATION SHALL BE IN ACCORDANCE WITH THE NATIONAL ELECTRICAL SAFETY CODE, NATIONAL ELECTRICAL CODE, FEDERAL AVIATION ADMINISTRATION SPECIFICATIONS/ ADVISORY CIRCULARS AND APPLICABLE LOCAL BUILDING CODES.
- THE CONTRACTOR SHALL OBTAIN AND PAY FOR ALL REQUIRED PERMITS, LICENSES, ETC., PRIOR TO COMMENCEMENT OF WORK. THE COST OF PERMITS, LICENSES, ETC., SHALL BE INCIDENTAL TO AND INCLUDED IN THE BID PRICE FOR THE RESPECTIVE PAY ITEMS.
- ALL MATERIALS SCHEDULED FOR REMOVAL SUCH AS EXISTING RUNWAY AND TAXIWAY LIGHTS, ISOLATION TRANSFORMERS, ETC., WHICH ARE DEEMED SALVAGABLE BY THE AIRPORT SHALL BE DELIVERED TO THE LOCATION ON AIRPORT PROPERTY AS INDICATED BY THE AIRPORT. ALL NON-SALVAGABLE MATERIALS REMOVED SUCH AS MANHOLES, HANDHOLES, CONCRETE FOUNDATIONS, CONDUIT, LIGHT BASES, CONDUCTORS, ETC. SHALL BE REMOVED FROM THE SITE BY THE CONTRACTOR AND LEGALLY DISPOSED OF OFF THE SITE BY THE CONTRACTOR.
- EXISTING CONDUIT, DUCTBANK, CIRCUITING AND UTILITY INFORMATION IS BASED ON AIRPORT "AS BUILT" AND "RECORD" DRAWINGS AND SITE VISITS BY THE UTILITY COMPANIES AND THE PROJECT ENGINEER. THE EXISTING UTILITY LOCATIONS SHOWN ON THE PLANS ARE APPROXIMATE AND SHALL NOT BE SCALED FOR EXACT LOCATIONS. NOT ALL UTILITIES MAY BE SHOWN. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT THE APPROPRIATE UTILITY/AGENCY PRIOR TO STARTING WORK, FOR THE LOCATION OF EXISTING UTILITIES, ANY INTERPRETATION OF AN EXISTING SYSTEM OR UTILITY SERVICE SHALL BE COORDINATED AND APPROVED BY THE AUTHORITY, AGENCY OR UTILITY HAVING JURISDICTION. PRIOR TO STARTING WORK, THE CONTRACTOR SHALL CONTACT ALL LOCAL UTILITIES TO ALLOW THEM TIME TO PROPERLY LOCATE ALL UTILITIES.
- THE CONTRACTOR SHALL COORDINATE HIS WORK WITH THE AIRPORT, FEDERAL AVIATION ADMINISTRATION, UTILITY COMPANIES, AND RESIDENT PROJECT REPRESENTATIVE (RPR) PRIOR TO AND DURING CONSTRUCTION TO ENSURE THAT ALL ELECTRICAL CIRCUITS AND FACILITIES HAVE BEEN LOCATED, FLAGGED AND ACCOUNTED FOR AND THAT ALL NECESSARY CIRCUITS HAVE BEEN DETERMINED PRIOR TO INITIATING CONSTRUCTION IN ANY LOCATION.
- IT SHALL BE THE CONTRACTORS' RESPONSIBILITY TO DETERMINE THAT ALL AIRFIELD LIGHTING CIRCUITS, EXCEPT THOSE THAT ARE SERVING CLOSED TAXIWAYS OR RUNWAYS, ARE COMPLETELY OPERATIONAL AT THE END OF EACH WORK SHIFT AND SHALL SO CERTIFY TO THE RESIDENT ENGINEER BEFORE THE END OF EACH SHIFT. **THE CONTRACTOR SHALL NOT LEAVE THE WORK SITE UNTIL CIRCUIT OPERATION HAS BEEN CONFIRMED BY THE RESIDENT ENGINEER.** TEMPORARY CABLE CONNECTIONS SHALL BE MADE IN AIRFIELD LIGHTING CIRCUITS WHEN PERMANENT WIRING CANNOT BE COMPLETED DURING THE WORK SHIFT. ALL AREAS NOT CLOSED FOR CONSTRUCTION (REFER TO PHASING PLAN) SHALL HAVE FULLY OPERABLE AIRFIELD LIGHTING DURING THE HOURS BETWEEN ONE HOUR BEFORE DUSK AND ONE HOUR AFTER DAWN. THE CONTRACTOR SHALL DISCUSS THE PROPOSED WIRING WITH THE RPR AND OBTAIN APPROVAL PRIOR TO COMMENCING WORK IN THAT AREA. ALL ELECTRICAL WIRING SHALL BE COMPLETED AND TESTED ONE (1) HOUR PRIOR TO THE ELECTRICAL MAINTENANCE DEPARTMENT'S END OF SHIFT.
- ALL EXISTING SYSTEMS/UTILITIES TO REMAIN SHALL BE PROTECTED FROM DAMAGE. REPLACEMENT OF ANY DAMAGED EXISTING SYSTEMS/UTILITIES SHALL BE AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGED ELECTRICAL SYSTEMS AND SHALL MAKE REPAIRS IMMEDIATELY, AT THEIR OWN COST, IN ACCORDANCE WITH APPLICABLE SPECIFICATIONS. DAMAGED ELECTRICAL SYSTEMS SHALL BE IMMEDIATELY REPORTED TO THE RPR. THE CONTRACTOR IS URGED TO TAKE EVERY PRECAUTION NECESSARY TO PROTECT ANY AND ALL CABLES FROM DAMAGE OF ANY SORT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KNOWING THE DEPTH OF ANY CABLE IN THE PROXIMITY OF THE CONSTRUCTION AS WELL AS THE HORIZONTAL LOCATION.
- NOT USED.
- ALL EXCAVATION WITHIN 5 FEET OF ANY UNDERGROUND UTILITY SHALL BE PERFORMED BY HAND EXCAVATION METHODS.
- TAXIWAY EDGE LIGHTS SHALL BE INSTALLED 10 FEET FROM THE EDGE OF PAVEMENT OR AS OTHERWISE INDICATED. ALL STRAIGHT SECTIONS OF TAXIWAY EDGE LIGHTS SHALL BE ALIGNED TO DEVELOP A CONTINUOUS "IN-LINE" APPEARANCE OF THE LIGHTS WHEN VIEWED AT GROUND LEVEL FROM ONE END. ALL TAXIWAY EDGE LIGHTS ON STRAIGHT SECTIONS OF THE TAXIWAY SHALL BE LOCATED SUCH THAT A LINE BETWEEN LIGHTS ON OPPOSITE SIDES OF THE TAXIWAY IS PERPENDICULAR TO THE TAXIWAY CENTERLINE.
- CHANGES TO THE LOCATION OF THE TAXIWAY LIGHTS, SHALL BE SUBMITTED BY THE CONTRACTOR TO THE RPR FOR APPROVAL. CONFLICTS THAT MAY OCCUR DUE TO CHANGES IN THE LOCATION OF THE LIGHTS SHALL BE THE FULL RESPONSIBILITY OF THE CONTRACTOR.
- ELECTRICAL DEMOLITION WORK SHALL BE LIMITED TO THE AREAS AND SCHEDULES IDENTIFIED IN THE DEMOLITION PLANS.
- ALL GROUND RODS AND OTHER UNDERGROUND GROUNDING CONNECTIONS SHALL BE CADWELD OR APPROVED EQUIVALENT. CADWELD CONNECTIONS SHALL BE IN ACCORDANCE WITH MANUFACTURERS GUIDELINES. THE PROPOSED COUNTERPOISE SYSTEM SHALL BE CONNECTED WITH THE EXISTING SYSTEM AT ALL CROSSING POINTS.
- CIVIL DATA IS SHOWN ON ELECTRICAL DRAWINGS FOR REFERENCE ONLY. REFER TO CIVIL DRAWINGS FOR DRAINAGE, PAVING DETAILS, PAVEMENT MARKING PLANS, ETC.
- THE CONTRACTOR SHALL UTILIZE A LOCATE SERVICE AS WELL AS HAVE A CABLE TRACER AVAILABLE TO LOCATE THE EXISTING CABLES WHEN HAND DIGGING SHALL BE UNDERTAKEN WITHIN TEN (10) FEET OF ANY KNOWN OR SUSPECTED EXISTING UNDERGROUND CABLES AND UTILITIES WHICH ARE NOT TO BE DISTURBED.
- SHOULD ANY RUNWAY OR TAXIWAY LIGHTING SYSTEM BE INOPERABLE DUE TO CONTRACTOR'S WORK, AND THE CONTRACTOR IS UNABLE TO RESTORE THE SYSTEM BY NIGHTFALL WITH PERMANENT REPAIRS, THE CONTRACTOR SHALL AT HIS OWN EXPENSE TAKE NECESSARY MEASURES TO ENSURE OPERATION OF THE SYSTEM DURING NIGHT HOURS. TEMPORARY WORK SHALL BE SUBJECT TO THE RPR'S APPROVAL. IF THE SYSTEM CANNOT BE RESTORED BY NIGHTFALL, THE CONTRACTOR SHALL INSTALL A TEMPORARY SYSTEM OF BATTERY OPERATED LIGHTS WITH THE APPROPRIATE COLORED LENSES FOR BOTH THRESHOLD AND RUNWAY EDGE LIGHTING TO ENSURE THE RUNWAY OPERATIONS CAN OCCUR. THE COST OF SUCH TEMPORARY LIGHTING SYSTEMS SHALL BE AT THE SOLE COST OF THE CONTRACTOR AND SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN OTHER ITEMS OF WORK.
- ALL MANHOLES, HANDHOLES, ETC., SHALL BE INSTALLED 25 FEET MINIMUM FROM THE EDGE OF THE FULL STRENGTH PAVEMENT UNLESS SHOWN OTHERWISE ON THE PLANS. COORDINATE FINAL LOCATION WITH GRADING AND DRAINAGE PLANS. WHEN EXTENDING EXISTING DUCT TO NEW MANHOLE, HANDHOLE, ETC., LOCATE STRUCTURE IN LINE WITH EXISTING DUCT. WHEN POSSIBLE DO NOT INSTALL STRUCTURES IN DITCHES OR SWALES OR WHERE WATER WILL POND ON TOP OF THE STRUCTURES.
- DEWATERING FOR THE INSTALLATION OF STRUCTURES AND/OR DUCTBANKS IS INCIDENTAL TO THE RESPECTIVE PAY ITEM. THE CONTRACTOR SHALL BE RESPONSIBLE TO PAY FOR AND OBTAIN ANY AND ALL PERMITS REQUIRED FOR DEWATERING.
- THE AIRPORT "LOCK/TAG/TRY" PROCEDURE AND NFPA 70E SHALL BE COMPLIED WITH BY THIS CONTRACTOR
- IF A LIGHT CAN IS INSTALLED INCORRECTLY, THE DUCT/CONDUIT IS PLUGGED/BROKEN, OR THE LIGHT CAN IS DAMAGED, PAVEMENT AROUND THE LIGHT CAN AND THE LIGHT SHALL BE REMOVED AND REPLACED AT THE CONTRACTORS' EXPENSE.

- ALL DUCT LOCATED IN OR UNDER THE PAVEMENT AND WITHIN 5 FEET OF THE EDGE OF THE SHOULDER PAVEMENT SHALL BE CONCRETE ENCASED DUCT. ALL OTHER 2" DUCT SHALL BE DIRECT BURIED.
- THE DIMENSION BETWEEN LIGHTS SHOWN ON A RADIUS IS DEFINED AS THE CHORD LENGTH. LOCATIONS SHOWN ARE TO THE CENTER OF THE LIGHTING FIXTURE.
- PROJECT PAY ITEMS: THE PROJECT PAY ITEMS ARE PROVIDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL WORK TO BE IDENTIFIED WITH A SPECIFIC PAY ITEM IS TO BE CONSIDERED REQUIRED WORK TO COMPLETE THE PROJECT AND IS TO BE SUBSIDIARY TO THE COST OF PROJECT PAY ITEMS PROVIDED.
- EXISTING AIRFIELD SIGNS TO REMAIN PROVIDING DIRECTIONS TO CLOSED AREAS SHALL BE COVERED. ALL AREAS CLOSED TO AIRCRAFT SHALL NOT BE LIGHTED. ADEQUATE LIGHTING IN THE OPINION OF THE RPR SHALL BE PROVIDED TO DELINEATE THE ACTIVE AND CLOSED AREAS OF THE AOA. THE ABOVE ITEMS ARE INCLUDED IN THE L-105 PAY ITEM.
- THIS CONTRACTOR SHALL VERIFY EXISTING CONDITIONS PRIOR TO STARTING WORK.
- THE CONTRACTOR SHALL CONDUCT GROUND RESISTANCE TESTS (MEGGER) ON EACH CIRCUIT AFFECTED BY THIS WORK BEFORE COMMENCING WORK ON THAT CIRCUIT. CONTRACTOR SHALL PREPARE AND FORWARD TO THE RESIDENT ENGINEER A WRITTEN REPORT, BY CIRCUIT, OF THESE RESULTS. THE CONTRACTOR SHALL REPEAT THIS TEST ON EACH AFFECTED CIRCUIT AFTER COMPLETION OF THE WORK. RESULTS OF BOTH TESTS SHALL BE PROVIDED TO THE RESIDENT ENGINEER.
- THE IDENTITY AND ROUTING OF ALL CABLES SHOWN ON THE PLANS SHALL BE VERIFIED IN THE FIELD. ANY DISCREPANCY SHALL BE BROUGHT TO THE ATTENTION OF THE RPR AND RECORDED IN THE AS-BUILT DRAWINGS TO PROVIDE AN ACCURATE RECORD OF CONDITIONS. THE CONTRACTOR SHALL COORDINATE INFORMATION SHOWN ON THE PLAN SHEETS WITH EXISTING RECORD INFORMATION AVAILABLE THROUGH THE AIRPORT MAINTENANCE STAFF. THESE PLANS DO NOT PURPORT TO SHOW ALL EXISTING CABLES AND CONCEALED UTILITIES WHICH WILL REQUIRE STAKE OUT PRIOR TO CONSTRUCTION. CONTRACTOR SHALL VERIFY EXISTING CIRCUIT ROUTING PRIOR TO COMMENCING WORK.
- CONTRACTOR SHALL COORDINATE ALL WORK WITH THE SAFETY, SECURITY AND PHASING PLANS.
- WHENEVER, IN THE CONTRACT DOCUMENTS, THE WORDS "PROVIDE", "FURNISH", "INSTALL", "FURNISH AND INSTALL", OR OTHER WORDS OF LIKE IMPORT ARE USED, IT SHALL BE UNDERSTOOD THAT THE INTENT OF THE CONTRACT DOCUMENTS IS TO PROVIDE FOR THE CONSTRUCTION AND COMPLETION IN EVERY DETAIL OF THE WORK DESCRIBED. IT IS FURTHER INTENDED THAT THE CONTRACTOR SHALL FURNISH ALL LABOR, SUPERVISION, MATERIALS, EQUIPMENT, TOOLS, TRANSPORTATION, SUPPLIES, TESTING AND INCIDENTALS REQUIRED TO COMPLETE THE WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.
- THE CONTRACTOR SHALL COMPLETELY SURVEY AND STAKE OUT EACH AREAS' LIGHTING LAYOUT PRIOR TO STARTING ANY INSTALLATION. SHOULD ANY IRREGULARITIES OCCUR IN THE LIGHTING LAYOUT, THE RPR SHALL BE NOTIFIED IMMEDIATELY. THE BID ITEM PRICE SHALL INCLUDE THE NECESSARY LAYOUT FOR EACH FIXTURE AND THE COST FOR ANY ADDITIONAL ADJUSTMENT OF THE LOCATION OF THE FIXTURES, DUE TO THE EXISTING GEOMETRIC CONDITIONS. THE NEW LIGHTING INSTALLATION SHALL BE COORDINATED WITH AND BLEND INTO THE EXISTING INSTALLATION AS REQUIRED.
- THERE ARE A NUMBER OF AIRPORT, PUBLIC UTILITIES AND FAA LIGHTING, COMMUNICATIONS, UNDERGROUND CABLES AND PIPES TRAVERSING THE AIRFIELD. THE PROJECT ENGINEER HAS MADE EVERY ATTEMPT TO SHOW THE APPROXIMATE LOCATION OF ALL ITEMS. HOWEVER, THE PROJECT ENGINEER IS NOT RESPONSIBLE FOR SHOWING OR LOCATING EVERY ITEM CURRENTLY IN PLACE. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO HAVE EVERY ITEM LOCATED, FLAGGED AND IDENTIFIED PRIOR TO START OF CONSTRUCTION. ANY DAMAGE DONE TO ANY OF THE EXISTING UTILITIES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL REPAIR ANY ITEM DAMAGED, CAUSED BY HIS ACTIONS, WITH NO ADDITIONAL COMPENSATION.
- THE CONTRACTOR SHALL THOROUGHLY INSPECT ALL AIRFIELD LIGHTING, SIGNS, AND MATERIALS UPON ARRIVAL AT THE PROJECT SITE FOR ANY DAMAGE THAT OCCURRED DURING SHIPPING OR FOR MATERIAL DEFECTS.
- ALL WORK SHOWN TO BE DEMOLISHED ON THE DRAWINGS IS BASED ON FIELD OBSERVATION OF THE ACTUAL EXISTING CONDITIONS AND ON EXISTING "AS-BUILT" DRAWINGS OF THE AREAS AFFECTED. THEY ARE THEREFORE CONSIDERED TO BE SCHEMATIC. IT IS THE INTENT OF THE DEMOLITION DRAWINGS THAT ALL EQUIPMENT, DEVICES, FIXTURES, WIRING MATERIALS, SYSTEMS AND APPURTENANCES, ETC. WHICH ARE NO LONGER REQUIRED AS A RESULT OF THE PROJECT BE REMOVED.

ELECTRICAL LEGEND

- | | |
|---|--|
|  | EXISTING L-861T TAXIWAY ELEVATED EDGE LIGHT, ISOLATION TRANSFORMER AND ASSOCIATED EQUIPMENT ON A L-867 BASE CAN, 'R' INDICATES TO BE REMOVED. |
|  | PROPOSED L-861T TAXIWAY ELEVATED EDGE LIGHT, ISOLATION TRANSFORMER AND ASSOCIATED EQUIPMENT ON A L-867 BASE CAN. |
|  | PROPOSED COMMUNICATIONS HANDHOLE |
|  | PROPOSED POWER HANDHOLE |
|  | PROPOSED L-858 SINGLE FACE, INTERNALLY ILLUMINATED, TAXI GUIDANCE SIGN |
|  | PROPOSED 2" PVC CONDUIT, FURNISH AND INSTALL SERIES LIGHTING CIRCUIT CABLES WHERE INDICATED. NUMERAL INDICATES THE NUMBER OF 1/C #8, 5KV SERIES LIGHTING CABLES, TYPE L-824. 'CKT3' INDICATES CIRCUIT DESIGNATION. |
|  | PROPOSED CONCRETE-ENCASED DUCTBANK, QUANTITY AND SIZE OF DUCTS AS INDICATED. FURNISH AND INSTALL SERIES LIGHTING CABLE WHERE INDICATED. NUMERAL INDICATES THE NUMBER OF 1/C #8 5KV SERIES LIGHTING CABLES. 'TW' INDICATES CIRCUIT DESIGNATION. |
|  | PROPOSED DUCTBANK CROSS SECTION. |

ABBREVIATIONS

AOA - AIRCRAFT OPERATIONS AREA
 AWG - AMERICAN WIRE GAUGE
 ¶ - BASELINE
 BSD - BARE SOFT DRAWN SOLID COPPER
 CL - CENTERLINE
 C - CONDUIT
 CONC - CONCRETE
 Ø, DIA - DIAMETER
 DWG - DRAWING
 E - EASTING
 EL/ELEV - ELEVATION
 EOP - EDGE OF PAVEMENT
 ES - EQUAL SPACES
 EX/EXST/EXIST - EXISTING
 FAA - FEDERAL AVIATION ADMINISTRATION
 FOD - FOREIGN OBJECT DEBRIS
 GND - GROUND
 GS - GLIDE SLOPE ANTENNA
 I/C - NUMBER OF CONDUCTORS/CONDUCTOR
 IE/INV - INVERT ELEVATION / INVERT
 ILS - INSTRUMENT LANDING SYSTEM
 kV - KILOVOLT
 LF - LINEAR FEET
 L - LEFT
 MAX - MAXIMUM
 MCB - MAIN CIRCUIT BREAKER
 MIN - MINIMUM
 N - NORTHING
 NAD - NORTH AMERICAN DATUM
 NGVD - NATIONAL GEODETIC VERTICAL DATUM
 NOTAM - NOTICE TO AIRMEN
 NTS - NOT TO SCALE
 OC - ON CENTER
 OFA - OBJECT FREE AREA
 PAPI - PRECISION APPROACH PATH INDICATOR
 PC - POINT OF CURVATURE
 PCC - PORTLAND CEMENT CONCRETE
 PG - PROPOSED GRADE
 PT - POINT OF TANGENCY
 PVC - POLYVINYL CHLORIDE
 RPR - RESIDENT PROJECT REPRESENTATIVE
 R/W - RUNWAY
 RPZ - RUNWAY PROTECTION ZONE
 RSA - RUNWAY SAFETY AREA
 RT - RIGHT
 SCH - SCHEDULE
 SGN - SIGN
 SIDA - SECURITY IDENTIFICATION DISPLAY AREA
 SS - STAINLESS STEEL
 STA - STATION
 STD - STANDARD
 T/A - TAXILANE
 T/W - TAXIWAY
 TBR - TO BE REMOVED
 TDZ - TOUCHDOWN ZONE
 TSA - TAXIWAY SAFETY AREA
 TYP. (TYP) - TYPICAL
 UC - UNDERGROUND ELECTRICAL DUCT
 W - WATER

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IL PROJECT: PIA-3812
 AIP PROJECT: 3-17-0080-46

REVISIONS

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DRAWN BY: RS

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SHEET TITLE
**ELECTRICAL
 LEGEND,
 ABBREVIATIONS
 AND
 NOTES**

SHEET NUMBER

26 OF 43

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SHEET TITLE

**ELEVATED
EDGE LIGHT
AND CABLE
SPlicing
DETAILS**

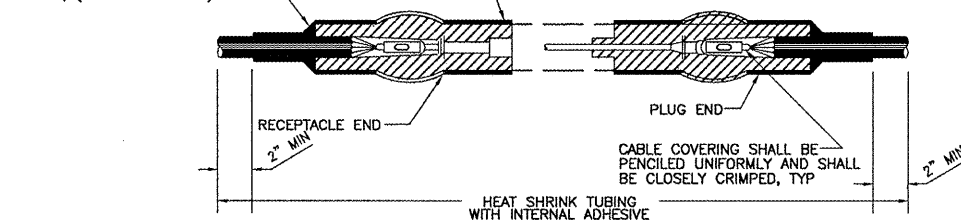
SHEET NUMBER

28 OF 43

**ISSUED FOR
BID DOCUMENTS**

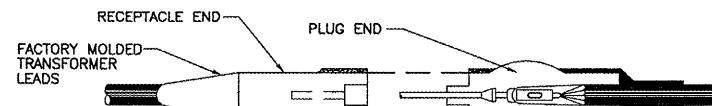
ALL CONNECTIONS SHALL HAVE ONE LAYER, HALF-LAPPED RUBBER OR SYNTHETIC RUBBER INSULATING TAPE COVERED WITH TWO LAYERS OF HALF-LAPPED PLASTIC TAPE TO A DISTANCE OF 1-1/2" FROM JOINT.

ADDITIONAL ADHESIVE COMPOUND FILLER, (TYP. BOTH SIDES)

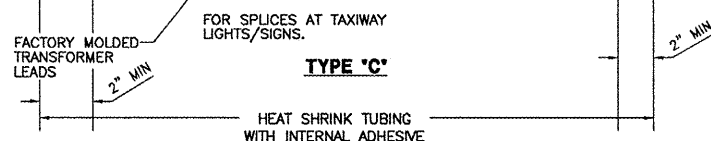


FOR SPLICES AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT. ALL CRIMPING SHALL BE DONE WITH APPROVED TOOL WHICH SHALL NOT RELEASE UNTIL PROPER PRESSURE IS APPLIED.

TYPE 'B'



ALL CONNECTIONS SHALL HAVE ONE LAYER, HALF-LAPPED RUBBER OR SYNTHETIC RUBBER INSULATING TAPE COVERED WITH TWO LAYERS OF HALF-LAPPED PLASTIC TAPE TO A DISTANCE OF 1-1/2" FROM JOINT.



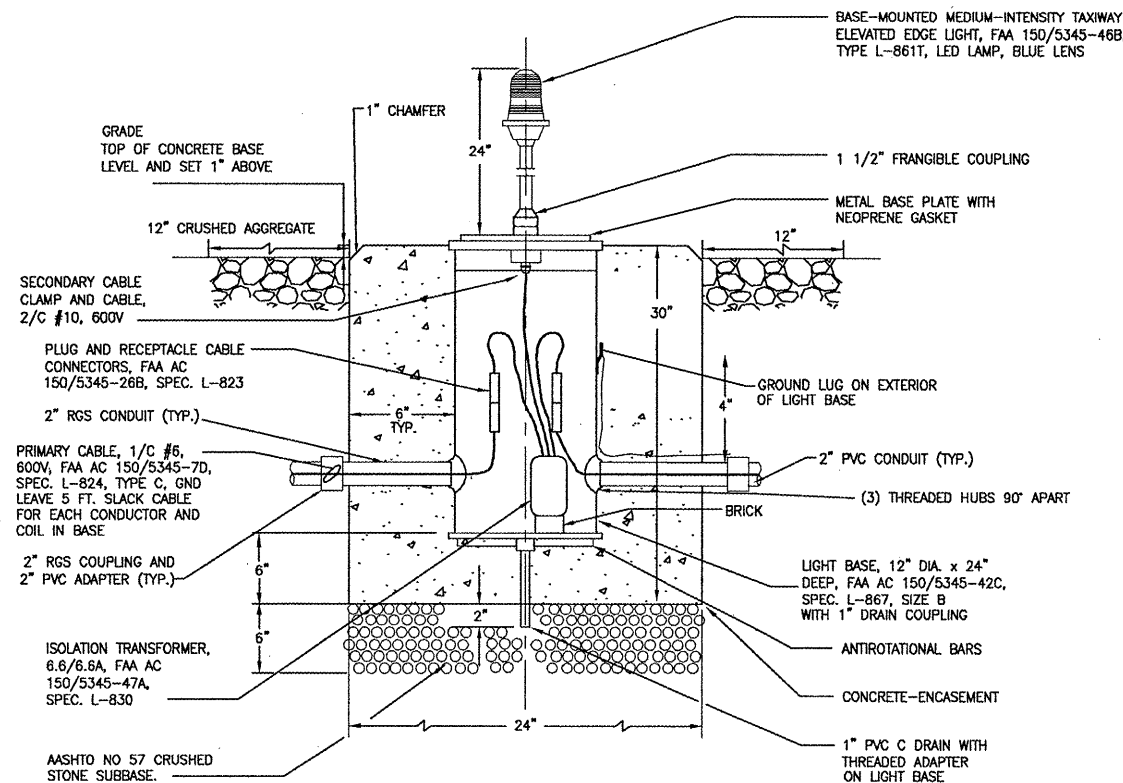
TYPE 'C'

NOTES:

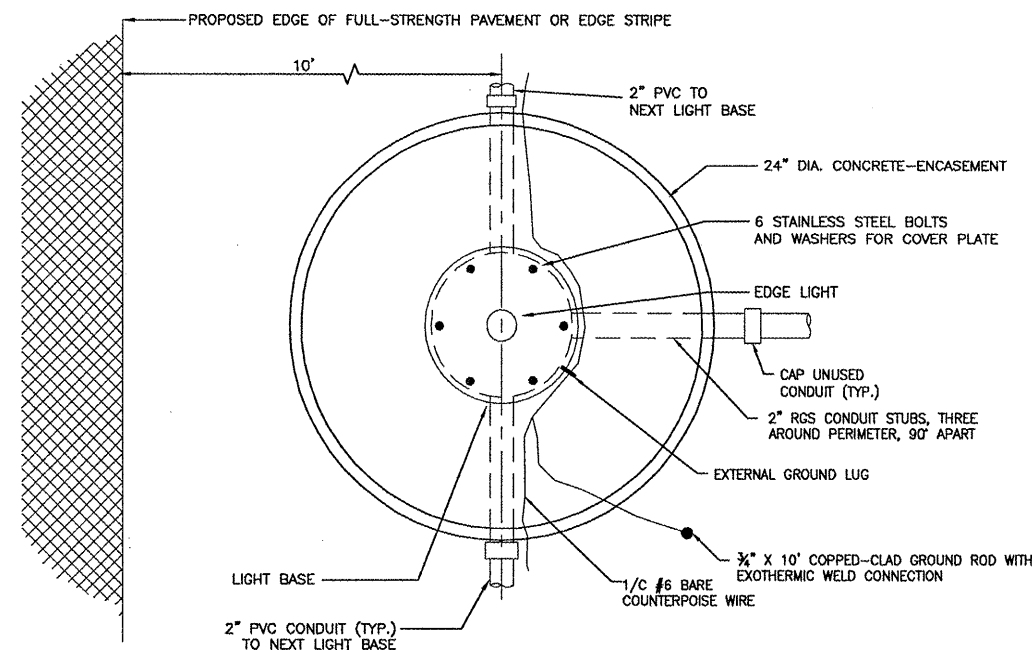
- INSIDE DIAMETER OF CONNECTOR KITS SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- ALL CABLES SHALL BE CLEANED OF ALL OILS, GREASE, DIRT OR OTHER MATERIALS WHICH COULD CONTAMINATE OR ADVERSELY AFFECT THE SPLICE.
- NO CAST SPLICES TO BE USED ON THIS PROJECT WITHOUT WRITTEN AUTHORIZATION OF PROJECT ENGINEER.
- NO SEPARATE PAYMENT FOR SPLICES OR CONNECTORS, COST TO BE INCLUDED IN PRICE FOR CABLE.
- ALL SPLICES SHALL BE PLACED IN JUNCTION CANS AND HANDHOLES.

CABLE SPLICES - TYPE 'B' & 'C'

NTS



ELEVATION



PLAN

TAXIWAY ELEVATED EDGE LIGHT AND BASE IN UNPAVED AREAS

NTS

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IL PROJECT: PIA-3812
AIP PROJECT: 3-17-0080-46

REVISIONS

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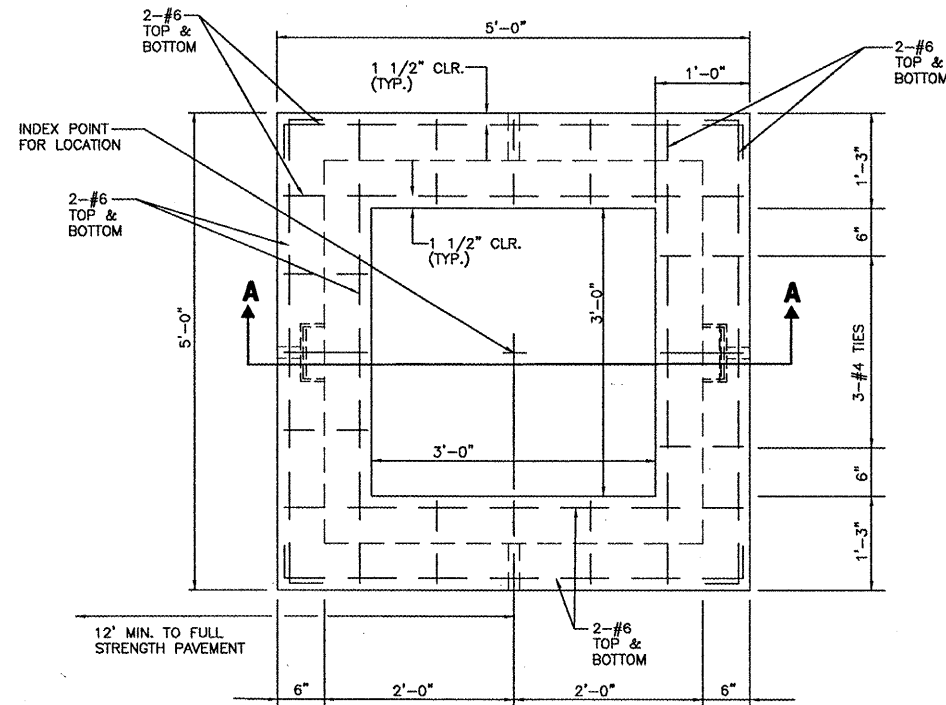
SHEET TITLE

HANDHOLE
DETAILS
(SHEET 1 OF 2)

SHEET NUMBER

29 OF 43

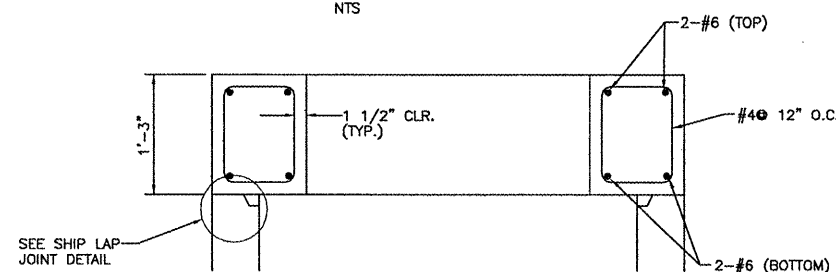
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- NOTES:
1. SHOP DWG. OF HANDHOLE REQUIRED - B727-200 SERIES MAIN GEAR DESIGN LOAD
 2. CONG. TO HAVE 5000 PSI (MIN) COMPRESSIVE STRENGTH @ 28 DAYS.
 3. ALL ENTRANCES MUST BE WATERPROOFED & GROUTED SMOOTH ON THE INSIDE.
 4. ALL HANDHOLES SHALL BE INSTALLED WITH NEW GROUND RODS.
 5. PROPOSED HANDHOLES SHALL BE FAA APPROVED.

PLAN VIEW

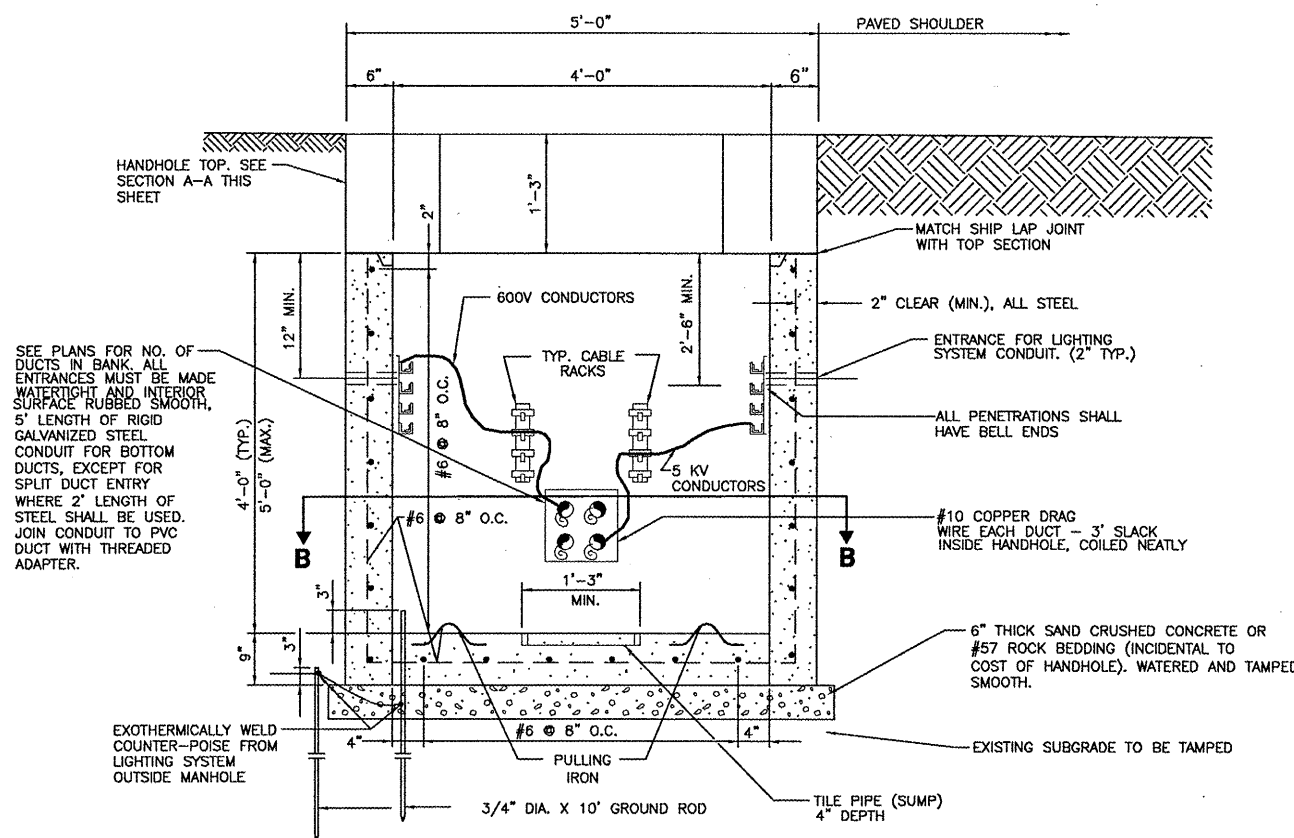
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TOP - SECTION A-A

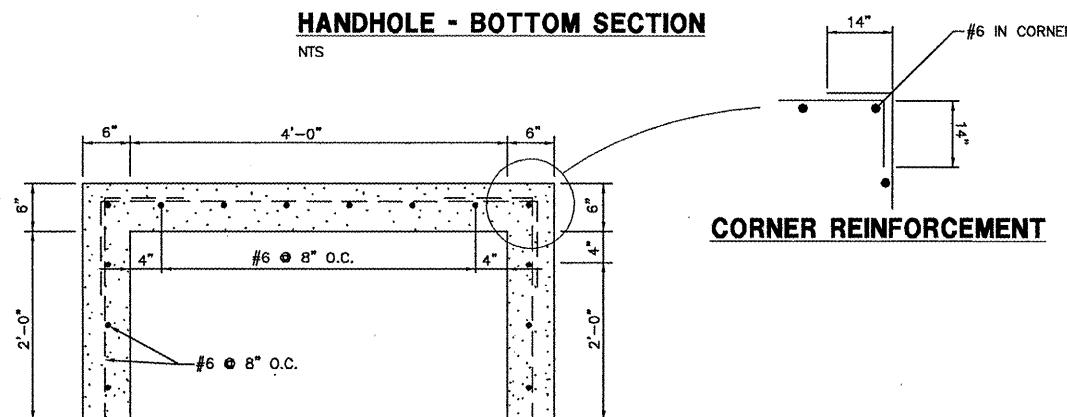
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HANDHOLE DETAIL



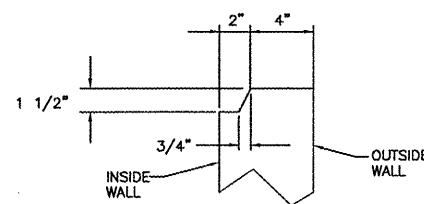
HANDHOLE - BOTTOM SECTION

NTS



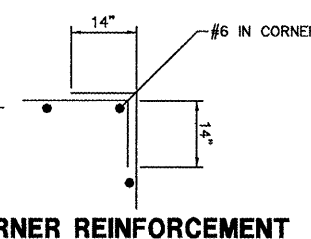
HANDHOLE BOTTOM - SECTION B-B

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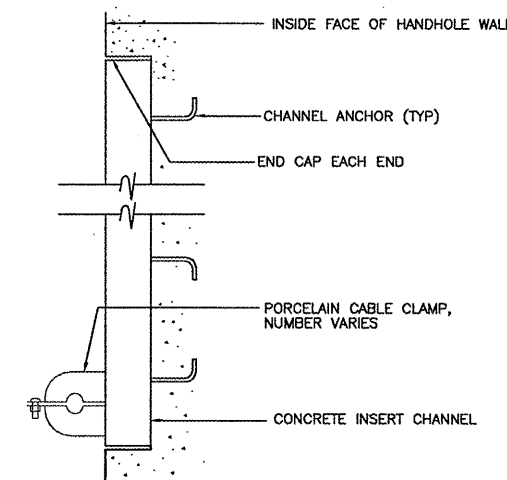


SHIP LAP JOINT DETAIL

NTS



CORNER REINFORCEMENT



CABLE SUPPORT BRACKET

NTS

NOTES:

1. INSTALL IN UNPAVED AREAS.
2. BOND GROUND WIRE TO ALL EXPOSED METAL.
3. BOND COUNTERPOISE WIRE TO GROUND RODS TO MAINTAIN THE COMBINED COUNTERPOISE/GROUND SYSTEM USED FOR GROUNDING EXISTING LIGHTS, EQUIPMENT, ETC.
4. COST OF PULLING IRONS, SPLIT DUCTS WHERE REQUIRED, CABLE SUPPORT RACKS, GROUND RODS AND CONNECTIONS ARE INCIDENTAL TO HANDHOLES AND NO SEPERATE PAYMENT WILL BE MADE.

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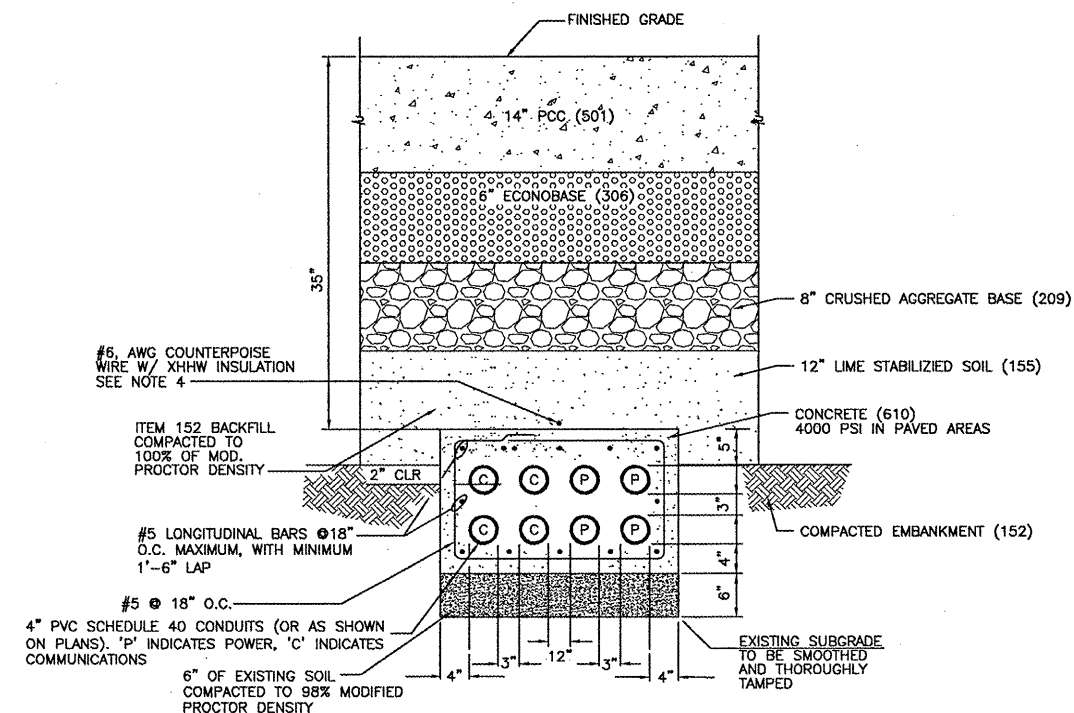
SHEET TITLE

DUCTBANK
AND CONDUIT
DETAILS

SHEET NUMBER

31 OF 43

ISSUED FOR
BID DOCUMENTS



#6 AWG COUNTERPOISE WIRE W/ XHHW INSULATION SEE NOTE 4

ITEM 152 BACKFILL COMPACTED TO 100% OF MOD. PROCTOR DENSITY

#5 LONGITUDINAL BARS @ 18" O.C. MAXIMUM, WITH MINIMUM 1'-6" LAP

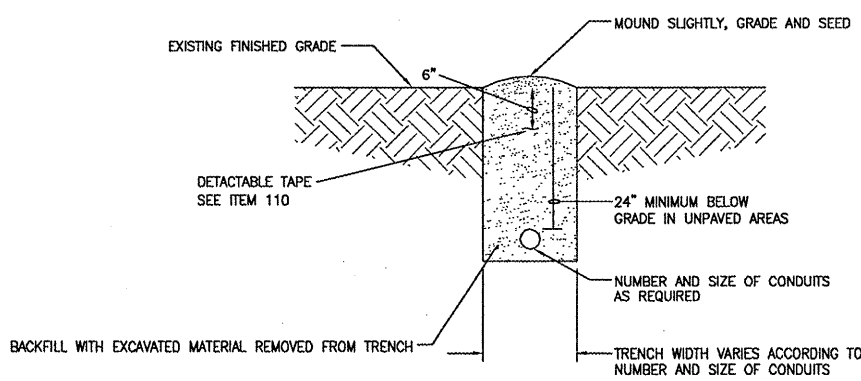
#5 @ 18" O.C. 4" PVC SCHEDULE 40 CONDUITS (OR AS SHOWN ON PLANS). 'P' INDICATES POWER, 'C' INDICATES COMMUNICATIONS

6" OF EXISTING SOIL COMPACTED TO 95% MODIFIED PROCTOR DENSITY

NOTES:

1. COORDINATE TRENCHING AND DUCTBANK WORK WITH EARTHWORK.
2. DUCTBANK INSTALLATION SHALL BE SIMILAR IN TURF AREAS. TRENCH BACKFILL TO BE EXISTING SOIL COMPACTED TO 95% MODIFIED PROCTOR DENSITY IN LIEU OF CONCRETE.
3. DUCTBANK INSTALLATION SHALL BE SIMILAR FOR DUCTBANK IN SHOULDER AREAS.

8-WAY, 4' DUCTBANK
NTS



TYPICAL CONDUIT TRENCHING IN UNPAVED AREAS
NOT TO SCALE

DUCTBANK NOTES:

- 1) THE #6 AWG BSD COPPER COUNTERPOISE WIRE SHALL BE CENTERED ON THE DUCT BANK.
- 2) THE MINIMUM DISTANCE BETWEEN TOP OF CONDUIT IN DUCTBANK AND COUNTERPOISE SHALL BE 3".
- 3) THE CONTRACTOR SHALL INSTALL A NYLON PULL WIRE IN EACH UNUSED DUCT OR CONDUIT INSTALLED AND PLUG OR CAP THE DUCT. THE WIRE SHALL BE SECURELY ATTACHED TO THE PLUG/CAP AT EACH END OF THE DUCT OR CONDUIT. NEW PULL WIRES SHALL BE INSTALLED IN EACH UNUSED DUCT OR CONDUIT IN DUCT BANK EXTENSIONS.
- 4) THE CONTRACTOR SHALL INSTALL A PLASTIC COATED, DETECTABLE MAGNETIC TWO (2) INCH WIDE TAPE EIGHT (8) INCHES BELOW TURF GRADE ABOVE ALL DUCTBANKS OR CONDUITS NOT INSTALLED UNDER AIRFIELD PAVEMENT.
- 5) DUCTBANKS SHALL DRAIN TOWARD THE MANHOLES WITH THE CROWN OF THE DUCT BANK MIDWAY BETWEEN MANHOLES. THE DUCT SLOPE SHALL BE 3" PER 100'.
- 6) ALL CONDUITS, DUCT AND DUCT BANKS SHOWN AS CONCRETE ENCASED SHALL BE ENCASED IN 4000 PSI CONCRETE COMPRESSIVE STRENGTH, WITH NOT LESS THAN 3" OF COVER AT TOP AND SIDES.
- 7) GRADING, SODDING, AREA RESTORATION, AND DEWATERING FOR THE INSTALLATION OF BASE CANS, MANHOLES, DUCT BANKS OR CONDUITS IS INCIDENTAL TO THE RESPECTIVE PAY ITEM. THE CONTRACTOR SHALL BE RESPONSIBLE TO PAY FOR AND OBTAIN ANY AND ALL PERMITS REQUIRED FOR DEWATERING.
- 8) THE ITEM 610 CONCRETE AROUND CONDUITS, DUCTS AND BASE CANS SHALL BE COMPLETELY CONSOLIDATED BY MECHANICAL MEANS AND SHALL BE FREE OF ANY VOIDS.
- 9) ALL DUCTS SHALL BE SECURELY FASTENED IN PLACE DURING CONSTRUCTION AND PROGRESS OF THE WORK AND SHALL BE PLUGGED TO PREVENT SEEPAGE OF GROUT, WATER OR DIRT. ANY DUCT SECTION HAVING A DEFECTIVE JOINT SHALL NOT BE INSTALLED. DUCTS SHALL BE SUPPORTED AND SPACED APART USING APPROVED SPACERS AT INTERVALS NOT TO EXCEED 5 FEET.
- 10) ALL CONSTRUCTION JOINTS IN CONCRETE ENCASED DUCTS SHALL HAVE A MINIMUM OF 4 STEEL DOWELS, EVENLY SPACED AND INSTALLED AT THE JOINT. THE DOWELS SHALL BE #4 DEFORMED STEEL REINFORCING BARS, 24" LONG, WITH 1/2 OF THE LENGTH EMBEDDED IN THE PLASTIC CONCRETE THAT IS CONSTRUCTED INITIALLY.
- 11) ALL LOOSE MATERIAL SHALL BE REMOVED FROM ALL EXCAVATIONS FOR ELECTRICAL EQUIPMENT, RACEWAYS, MANHOLES, PADS, ETC. THE BOTTOM OF THE EXCAVATION SHALL BE COMPACTED TO 95% COMPACTION IN ACCORDANCE WITH ASTM D1557 PRIOR TO THE INSTALLATION OF THE ELECTRICAL ITEM AND BACKFILL.

REVISIONS

NO.	DESCRIPTION	DATE

ADDENDUM 1 5/21/08
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DRAWN BY: JFC
DESIGNED BY: NDO

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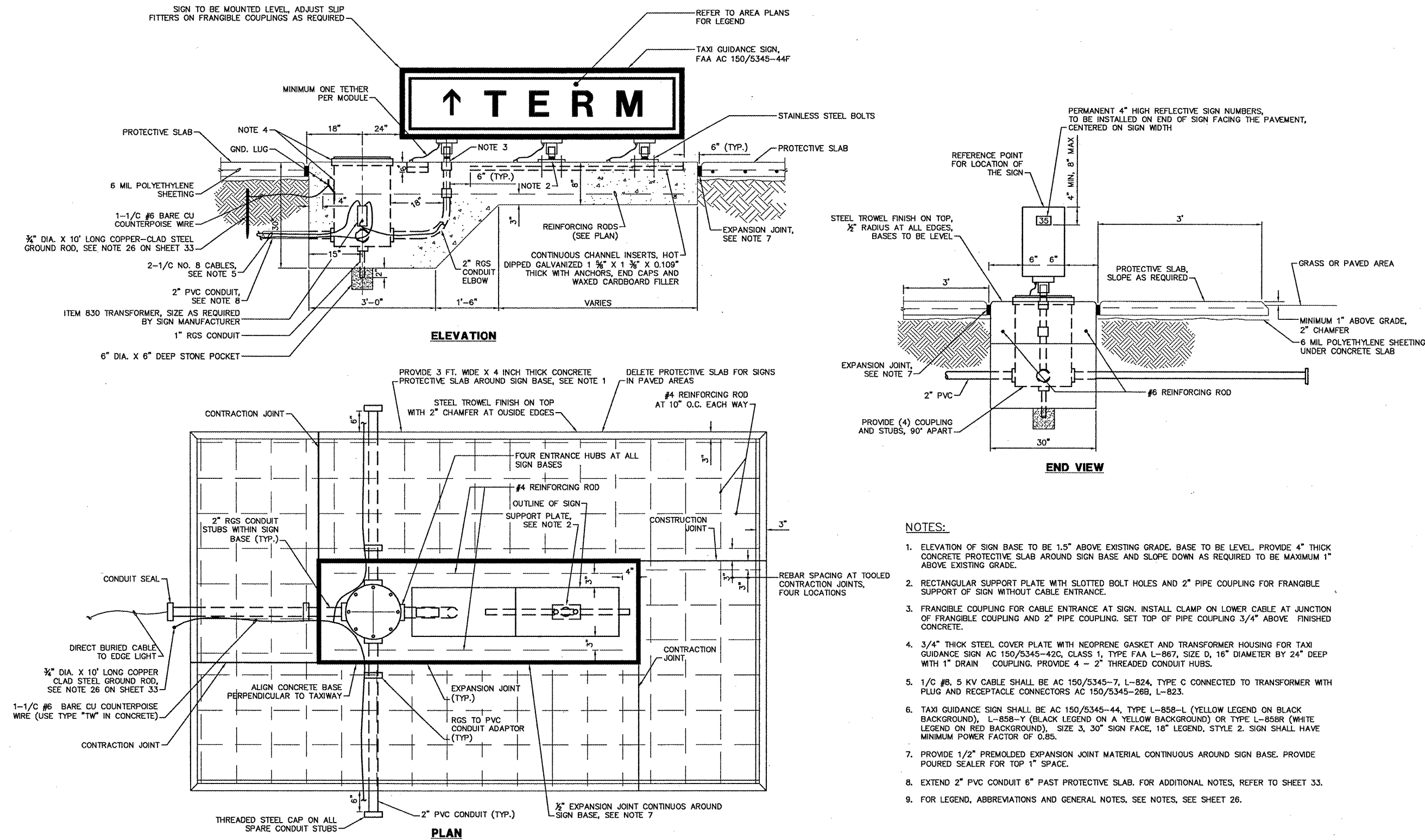
SHEET TITLE

GUIDANCE
SIGNAGE
DETAILS

SHEET NUMBER

32 OF 43

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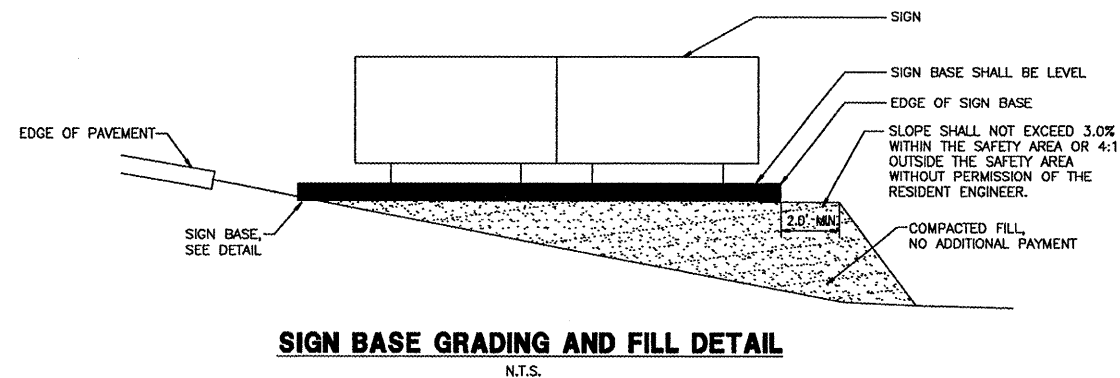


NOTES:

- ELEVATION OF SIGN BASE TO BE 1.5" ABOVE EXISTING GRADE. BASE TO BE LEVEL. PROVIDE 4" THICK CONCRETE PROTECTIVE SLAB AROUND SIGN BASE AND SLOPE DOWN AS REQUIRED TO BE MAXIMUM 1" ABOVE EXISTING GRADE.
- RECTANGULAR SUPPORT PLATE WITH SLOTTED BOLT HOLES AND 2" PIPE COUPLING FOR FRANGIBLE SUPPORT OF SIGN WITHOUT CABLE ENTRANCE.
- FRANGIBLE COUPLING FOR CABLE ENTRANCE AT SIGN. INSTALL CLAMP ON LOWER CABLE AT JUNCTION OF FRANGIBLE COUPLING AND 2" PIPE COUPLING. SET TOP OF PIPE COUPLING 3/4" ABOVE FINISHED CONCRETE.
- 3/4" THICK STEEL COVER PLATE WITH NEOPRENE GASKET AND TRANSFORMER HOUSING FOR TAXI GUIDANCE SIGN AC 150/5345-42C, CLASS 1, TYPE FAA L-867, SIZE D, 16" DIAMETER BY 24" DEEP WITH 1" DRAIN COUPLING. PROVIDE 4 - 2" THREADED CONDUIT HUBS.
- 1/C #8, 5 KV CABLE SHALL BE AC 150/5345-7, L-824, TYPE C CONNECTED TO TRANSFORMER WITH PLUG AND RECEPTACLE CONNECTORS AC 150/5345-26B, L-823.
- TAXI GUIDANCE SIGN SHALL BE AC 150/5345-44, TYPE L-858-L (YELLOW LEGEND ON BLACK BACKGROUND), L-858-Y (BLACK LEGEND ON A YELLOW BACKGROUND) OR TYPE L-858R (WHITE LEGEND ON RED BACKGROUND), SIZE 3, 30" SIGN FACE, 18" LEGEND, STYLE 2. SIGN SHALL HAVE MINIMUM POWER FACTOR OF 0.85.
- PROVIDE 1/2" PREMOLDED EXPANSION JOINT MATERIAL CONTINUOUS AROUND SIGN BASE. PROVIDE POURED SEALER FOR TOP 1" SPACE.
- EXTEND 2" PVC CONDUIT 6" PAST PROTECTIVE SLAB. FOR ADDITIONAL NOTES, REFER TO SHEET 33.
- FOR LEGEND, ABBREVIATIONS AND GENERAL NOTES, SEE NOTES, SEE SHEET 26.

GENERAL SIGN NOTES

- ① ALL SIGNS SHALL BE SIZE 3.
- ② SEE LIGHTING AND SIGNAGE PLAN SHEET 27 FOR SIGN LOCATIONS AND LEGENDS.
- ③ ALL LETTERS AND SPACINGS OF SIGN FACES TO BE PER AC 150/5345-44F, OR LATEST REVISION.
- ④ ALL NEW SIGN MODULES, SIGNS AND ANY MODIFICATIONS TO EXISTING SIGNS SHALL CONFORM TO AC 150/5340-18 AND 150/5345-44, LATEST EDITIONS AND SIGNAGE MARKING SUPPLEMENTS. ALL MATERIALS, SIGN BASE DETAILS, ETC., SHALL BE SUBMITTED TO THE RESIDENT ENGINEER FOR APPROVAL. NEW SIGN PANELS SHALL BE FURNISHED BY THE MANUFACTURER OF THE SIGN IN WHICH THE NEW PANELS ARE TO BE INSTALLED.
- ⑤ THE SIGNS SHALL BE FRANGIBLE, MEETING THE REQUIREMENTS OF AC 150/5345-44, LATEST EDITION.
- ⑥ THE DETAILS SHOWN IN THE PLANS PROVIDE THE MINIMUM REQUIREMENTS FOR SIGN INSTALLATIONS. THE CONTRACTOR SHALL USE STANDARDS APPLICABLE FOR THE PARTICULAR SIGN MANUFACTURER. THE BOLTING PATTERN, METHOD OF ANCHORING, ETC., SHALL BE PER SIGN MANUFACTURER'S RECOMMENDATIONS AND APPROVED BY THE RESIDENT ENGINEER.
- ⑦ THE ACTUAL SIGN DIMENSIONS WILL VARY PER MANUFACTURER. THE BASE SIZE AS SHOWN SHALL BE ADJUSTED TO MATCH THE SIGNS SUBMITTED. THE SUBMITTAL SHALL INCLUDE NEW BASE DIMENSIONS, LAYOUT, ETC.
- ⑧ FOR LOCATION AND ORIENTATION OF SIGNS AND FOUNDATIONS, SEE PLANS. THE LOCATION SHOWN ON THE PLANS IS THE PERPENDICULAR DISTANCE FROM THE DEFINED TAXIWAY EDGE OF FULL STRENGTH PAVEMENT, TO THE NEAR SIDE OF THE SIGN ON THE SIGN'S LONGITUDINAL CENTERLINE (SEE PLAN VIEW). ALL SIGNS SHALL BE INSTALLED WITH FIXTURE I.D. MARKERS.
- ⑨ ALL SIGNS SHALL BE ORIENTED SUCH THAT THE LONGITUDINAL CENTERLINE OF THE SIGN IS PERPENDICULAR TO THE RESPECTIVE TAXIWAY CENTERLINE, UNLESS NOTED OTHERWISE.
- ⑩ CONTRACTOR SHALL PROVIDE NEW CONCRETE FOUNDATION, BASE CAN, LAMPS, GROUND RODS, ISOLATION TRANSFORMERS AND CONNECTIONS FOR ALL RELOCATED OR ADJUSTED GUIDANCE SIGNS.
- ⑪ SITE GRADING AT EACH SIGN IS ANTICIPATED. GRADING AND RESTORATION ARE INCIDENTAL TO SIGN COST.
- ⑫ ALL CONCRETE SHALL COMPLY WITH ITEM 610 SPECIFICATION. ITEM 610 CONCRETE STEEL REINFORCEMENT SHALL BE TYPE ASTM A615 GRADE 60. ALL REINFORCEMENT SHALL HAVE A 2" MINIMUM CONCRETE COVER. REINFORCEMENTS MAY BE ADJUSTED TO MISS INTERFERENCES.
- ⑬ STRUCTURAL BACKFILL, FREE-DRAINING, AS APPROVED BY THE RESIDENT ENGINEER, SHALL BE PLACED IN HORIZONTAL LIFTS NOT TO EXCEED 4" IN LOOSE DEPTH AND COMPACTED TO 95% MAXIMUM DENSITY AT ZERO TO 2% ABOVE OPTIMUM MOISTURE AS DETERMINED BY ASTM D1557. SLOPE SHALL NOT EXCEED 3:1 WITHIN RUNWAY SAFETY AREA. THE RUNWAY SAFETY AREA IS ANY AREA WITHIN 250 FEET OF THE RUNWAY CENTERLINE.
- ⑭ SODDING OF DISTURBED AREAS SHALL BE INCLUDED IN THE INDIVIDUAL SIGN PAY ITEM. EXCAVATION, GRADING, FILL ETC. FOR THE SIGNS IS INCIDENTAL TO THE SIGN PAY ITEM.
- ⑮ ALL AREAS FOR THE SUPPORT PLATES SHALL BE IN THE SAME HORIZONTAL PLANE. WHEN CAST INTEGRALLY WITH THE CONCRETE PAD, THE ANCHOR BOLTS SHALL BE A-36 STEEL, HOT DIP GALVANIZED.
- ⑯ NUMBER AND SPACING OF LEGS SHALL BE AS PER MANUFACTURERS REQUIREMENTS.
- ⑰ GROUNDING SHALL BE PER MANUFACTURERS REQUIREMENTS.
- ⑱ BASE CAN SHALL BE ON SIDE OF SIGN CLOSEST TO THE POWER SOURCE.
- ⑲ APPLY "NEVER SIEZE" OR APPROVED EQUAL TO ALL THREADED BOLTS AND CONNECTIONS.
- ⑳ EXISTING SIGNS TO BE REMOVED SHALL BECOME THE PROPERTY OF THE GREATER PEORIA REGIONAL AIRPORT, UNLESS OTHERWISE NOTED.
- ㉑ SIGNS TO BE PLACED PLUMB AND LEVEL.
- ㉒ FRANGIBLE COUPLING SHALL BE LOCATED 1-1/2" MAX. ABOVE CORNER. MANUFACTURER SHALL PROVIDE AT LEAST A 1/4" DIA. OPENING OR EQUAL FOR DRAINAGE INTO ITEM 867 BASE. COUPLING SHALL NOT BE SUPPLIED WITH WEEP HOLES TO THE OUTSIDE.
- ㉓ NON-METALLIC COMPONENTS IN LIGHT FIXTURE STEMS, FRANGIBLE COUPLING AND BASES SHALL NOT BE PERMITTED.
- ㉔ COLOR CODE PROPOSED WIRING PER AIRPORT REQUIREMENTS. CONTRACTOR SHALL COORDINATE WITH AIRPORT MAINTENANCE PERSONNEL.
- ㉕ CONTRACTOR TO PLACE NEW REPLACEMENT LAMPS FOR ALL LIGHT UNITS INSIDE SIGN AT TIME OF FINAL INSPECTION.
- ㉖ GROUND RODS SHALL BE 3/4 INCH DIAMETER, 10 FEET LONG, COPPER CLAD STEEL SECTIONAL RODS. EACH GROUND ROD SHALL BE INDIVIDUALLY TESTED PRIOR TO CONNECTION TO THE GROUND GRID OR COUNTERPOISE SYSTEM. EACH INDIVIDUAL ROD SHALL HAVE AN EARTH RESISTANCE OF 25 OHMS OR LESS. ADDITIONAL SECTIONS SHALL BE ADDED TO THE ROD TO ACHIEVE THE 25 OHM REQUIREMENT.
- ㉗ THE #6 AWG COPPER BONDING CONDUCTOR AND SIGN TETHER SHALL NOT BE ATTACHED AT THE SAME ANCHOR BOLT. AN APPROVED IRREVERSIBLE CRIMP SHALL BE USED TO CONNECT THE BONDING CONDUCTOR TO THE SIGN FLANGE AND SIGN.
- ㉘ USE #6 AWG COPPER COUNTERPOISE WITH TYPE THHW INSULATION WHEN INSTALLED IN CONCRETE. COUNTERPOISE TO BE CONNECTED TO GROUND ROD AT EACH SIGN.
- ㉙ ALL SIGNS SHALL BE FURNISHED WITH TWO TETHERS PER MODULE. TETHERS SHALL BE 3/16" STAINLESS STEEL AIRCRAFT CABLE WITH A FORMED EYE ON BOTH ENDS. THE TETHER EYE SHALL BE ATTACHED TO THE SIGN AND BASE BY BEING SANDWICHED BETWEEN TWO STAINLESS STEEL FENDER WASHERS, WITH A 1/2" MINIMUM STAINLESS STEEL BOLT. THE TETHER SHALL BE OF SUFFICIENT LENGTH TO HAVE A MINIMUM OF 6" OF SLACK WHEN ATTACHED BETWEEN THE SIGN AND THE SIGN BASE. THE TETHERS AND BONDING CONDUCTORS SHALL BE OF SUFFICIENT LENGTH TO ALLOW THE FRANGIBLE COUPLINGS TO OPERATE WITHOUT RESTRICTIONS AND TO ALLOW THE POWER CABLE TO DISCONNECT IF THE SIGN FALLS OVER.
- ㉚ SIGNS SHALL BE WELL SEALED AGAINST INFILTRATION BY WATER AND DIRT.
- ㉛ EACH SIGN SHALL BE FURNISHED WITH AN ON-OFF TOGGLE SWITCH WITH WEATHERPROOF COVER. THE SWITCH SHALL BE USED BY MAINTENANCE PERSONNEL TO DE-ENERGIZE THE SIGN SO MAINTENANCE WORK CAN BE PERFORMED. THE SWITCH SHALL BE PHYSICALLY AND ELECTRICALLY LOCATED IMMEDIATELY ADJACENT TO THE LOAD SIDE OF THE L-823 DISCONNECT PLUG. WHEN THE SWITCH IS IN THE OFF POSITION, THE SIGN SHALL BE ISOLATED FROM THE SERIES CIRCUIT AND THE SECONDARY OF THE L-830 ISOLATION TRANSFORMER SHALL BE SHORTED. THE WEATHERPROOF COVER SHALL PROVIDE PROTECTION FROM DRIVING RAIN, SNOW AND ICE, AND SHALL HAVE A SPRING OPERATED CLOSING DEVICE. THE WEATHERPROOF COVER SHALL ALSO PROVIDE PHYSICAL PROTECTION FOR THE SWITCH HANDLE.



SIGNAGE SCHEDULE

SIGN	MODULES	SIDE A LEGEND			SIDE B LEGEND			SIDE A COLOR			SIDE B COLOR			NORTHING	EASTING
		1	2	3	1	2	3	1	2	3	1	2	3		
1	1	T→						B/Y			B			1456600.9670	2427935.0618
2	3	↑	TE	RM				B/Y	B/Y	B/Y	B	B	B	1456546.2003	2427945.8149

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AIP PROJECT: 3-17-0080-46

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GENERAL
GUIDANCE SIGN
NOTES & DETAILS

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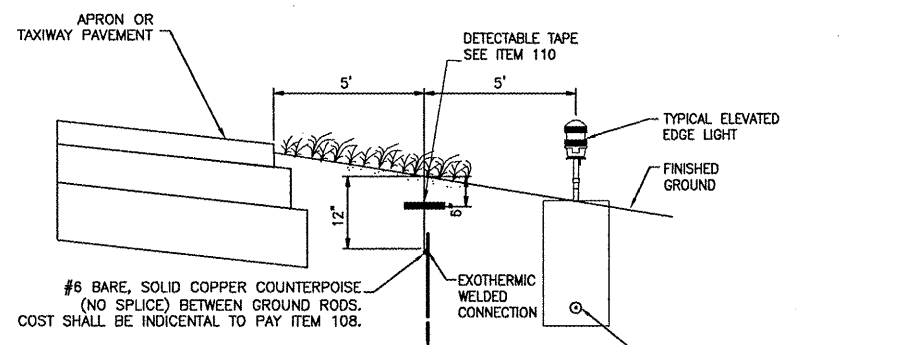
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ELECTRICAL
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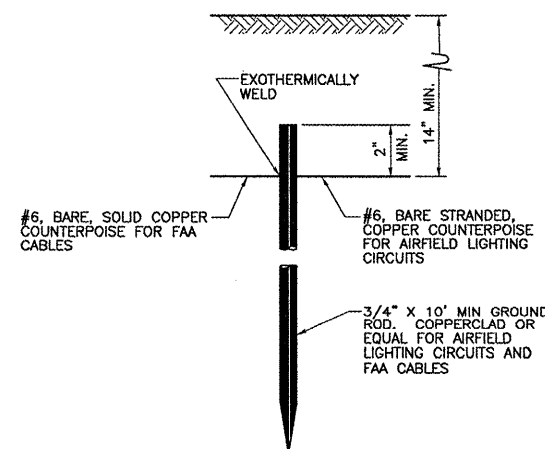


NOTES:

1. INSTALL 3/4" Ø X 10' LONG COPPER CLAD GROUND RODS ALONG COUNTERPOISE SYSTEM AT 300' INTERVALS. CONNECTIONS OF GROUND RODS TO COUNTERPOISE CABLE SHALL BE EXOTHERMIC TYPE (COPPERWELD OR EQUAL)
2. DO NOT CONNECT COUNTERPOISE WIRING TO EDGE LIGHT BASES.
3. THE RESISTANCE TO GROUND OF THE COUNTERPOISE GROUNDING SYSTEM SHALL NOT EXCEED 25 OHMS.
4. COST OF THE GROUND RODS IS INCIDENTAL TO THE ASSOCIATED ITEMS REQUIRING GROUNDING UNLESS OTHERWISE SPECIFIED.

COUNTERPOISE (ALONG PAVEMENT) INSTALLATION DETAIL

N.T.S.

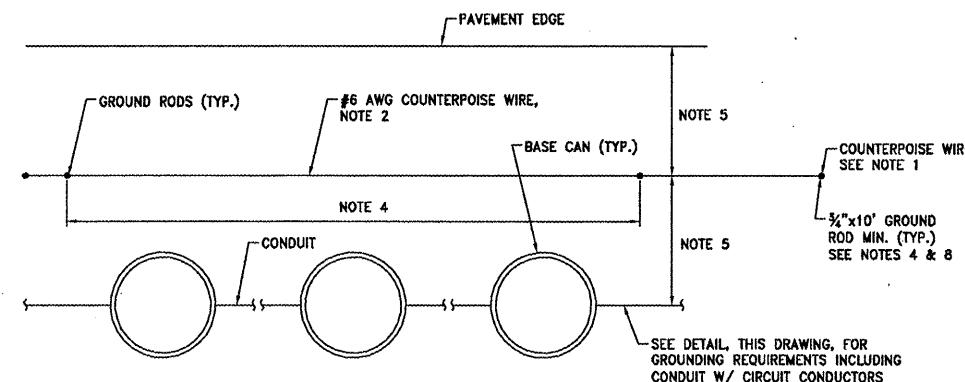


NOTES:

1. THE RESISTANCE TO GROUND OF ANY GROUNDING SYSTEM SHALL NOT EXCEED 10 OHMS.
2. COST OF GROUND RODS AND CONNECTIONS TO COUNTERPOISE IS INCIDENTAL TO ASSOCIATED ITEMS OF WORK REQUIRING GROUNDING AND NO SEPARATE PAYMENT WILL BE MADE.
3. MAXIMUM SPACING OF GROUND RODS FOR EDGE LIGHTING SYSTEMS SHALL NOT EXCEED 200 FEET.
4. ALL GROUND RODS ARE INSTALLED VERTICALLY.
5. EACH ROD TO BE TESTED INDIVIDUALLY TO ENSURE RESISTANCE TO GROUND OF 25 OHMS OR LESS. ADD ADDITIONAL SECTIONS UNTIL THIS REQUIREMENT IS MET.

GROUND ROD DETAIL

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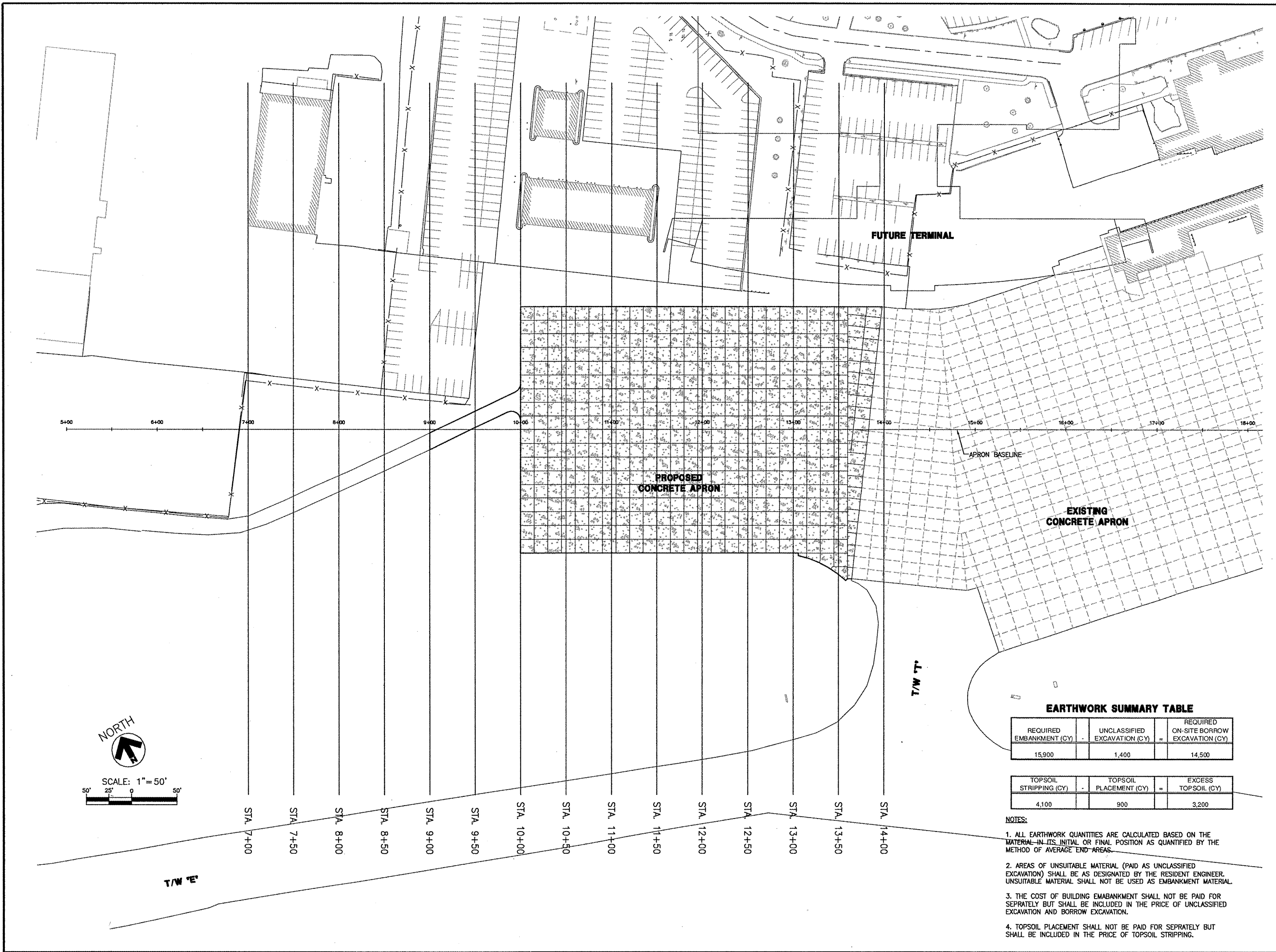


NOTES:

1. CONNECTIONS TO GROUND RODS SHALL BE MADE USING EXOTHERMIC WELDS.
2. COUNTERPOISE WIRE SHALL BE #6 AWG, BARE SOLID COPPER.
3. NOT USED.
4. GROUND RODS FOR AUTHORITY LIGHTING CABLES SHALL BE PROVIDED AT A MAXIMUM SPACING OF 100 FEET. ADDITIONALLY, THE RESISTANCE TO GROUND OF THE COUNTERPOISE SYSTEM SHALL NOT EXCEED 10 OHMS.
5. SEE COUNTERPOISE WIRE INSTALLATION DETAIL FOR COUNTERPOISE WIRE LOCATION, GROUND RODS SHALL BE COPPERCLAD OR EQUAL.
6. WHERE DUCTS CROSS, THE COUNTERPOISE SHALL BE TERMINATED AT GROUND RODS ON EACH SIDE OF A DUCT CROSSING.
7. ALL COUNTERPOISE CONNECTIONS SHALL BE UL LISTED FOR DIRECT EARTH BURIAL AND/OR INSTALLATION IN CONCRETE AS APPLICABLE.
8. PROVIDE GROUND ROD WHERE NEW LIGHTING DUCTS CONNECT TO EXISTING DUCTS, HANDHOLES, MANHOLES, AND/OR LIGHT CANS.
9. COST OF GROUND RODS AND CONNECTIONS TO GROUNDING CONDUCTORS IS INCIDENTAL TO ASSOCIATED ITEMS OF WORK REQUIRING GROUNDING AND NO SEPARATE PAYMENT WILL BE MADE.

PLAN - COUNTERPOISE WIRE FOR PROPOSED EDGE LIGHTS

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**CROSS SECTION
 LAYOUT PLAN**

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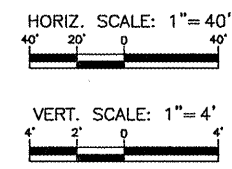
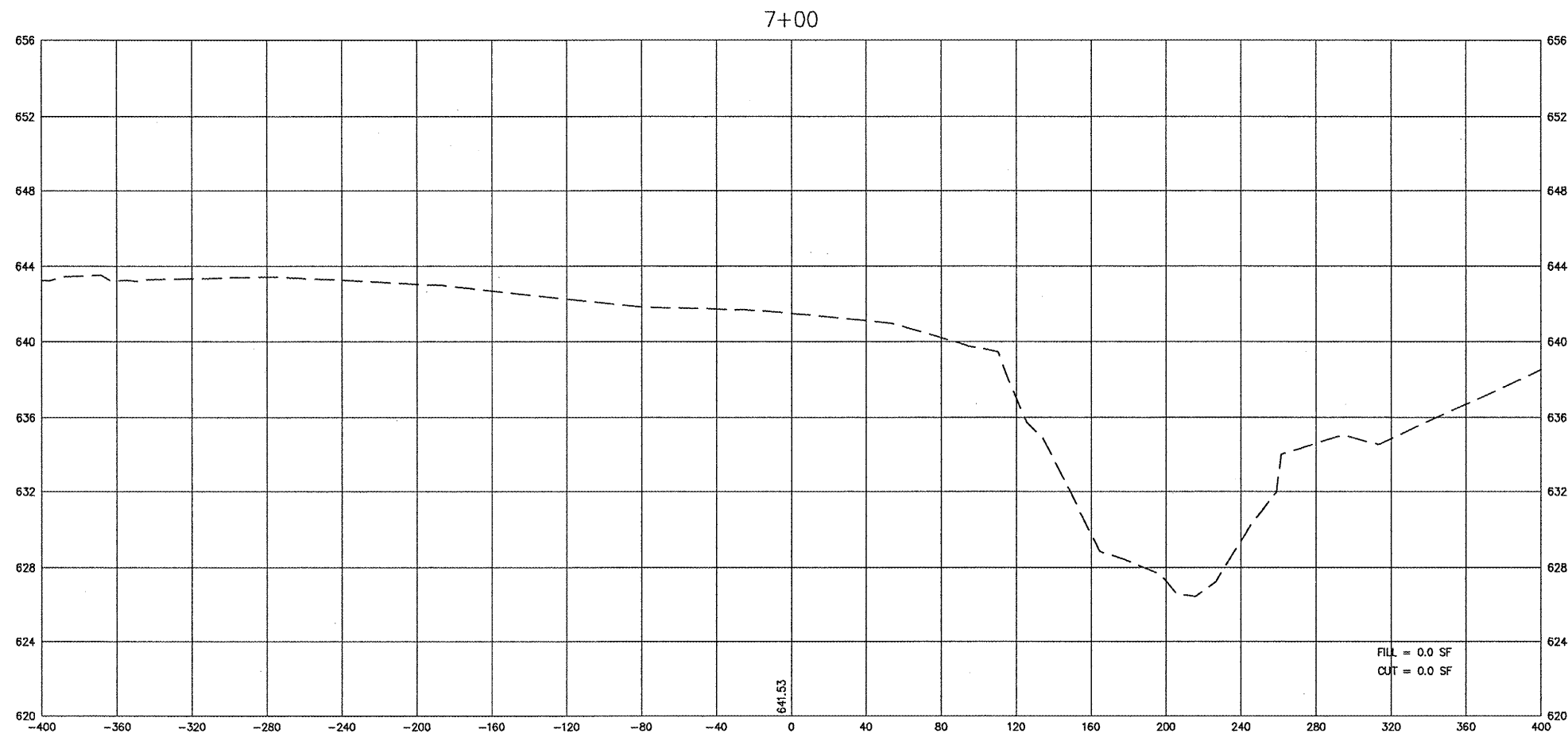
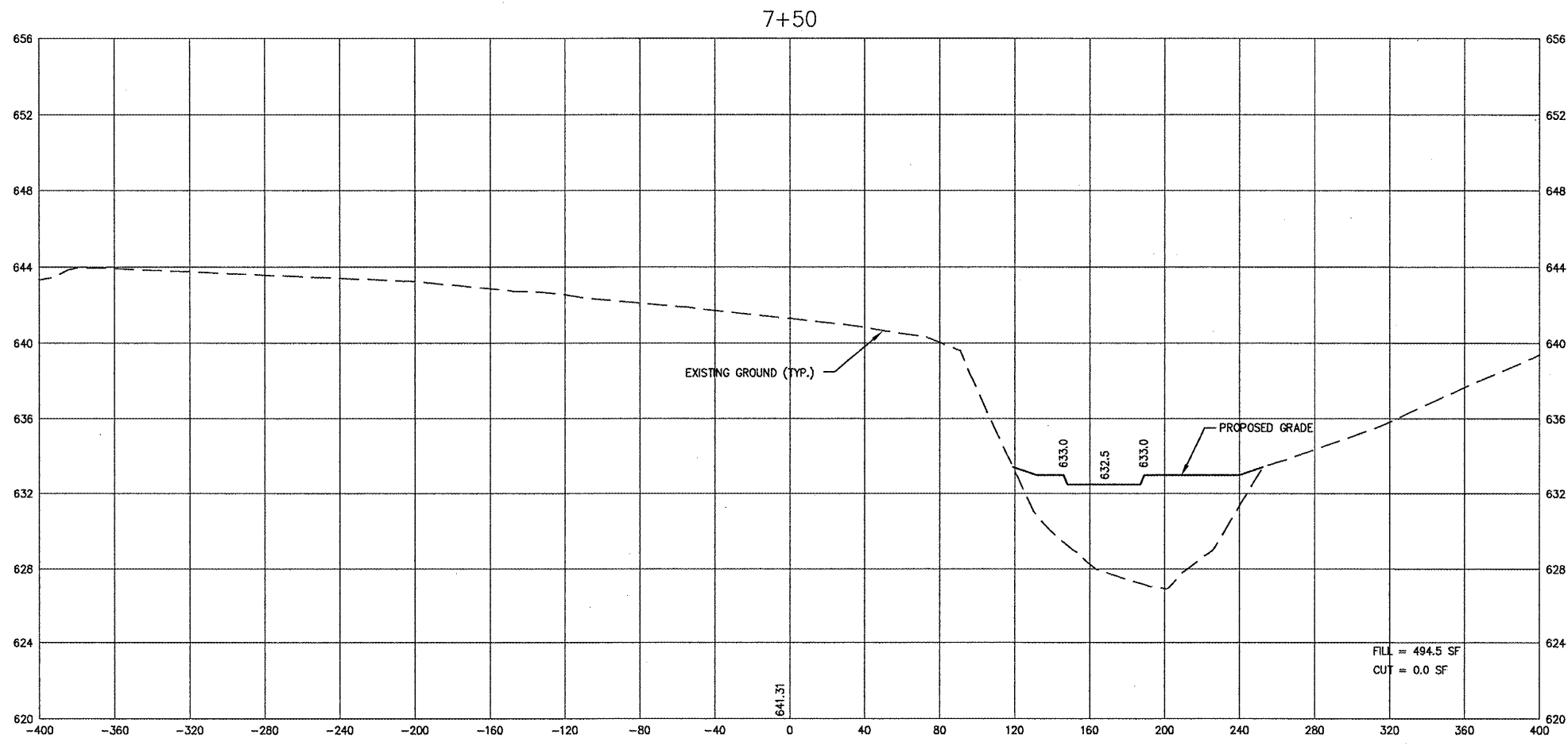
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EARTHWORK SUMMARY TABLE

REQUIRED EMBANKMENT (CY)	UNCLASSIFIED EXCAVATION (CY)	REQUIRED ON-SITE BORROW EXCAVATION (CY)
15,900	1,400	14,500

TOPSOIL STRIPPING (CY)	TOPSOIL PLACEMENT (CY)	EXCESS TOPSOIL (CY)
4,100	900	3,200

- NOTES:**
1. ALL EARTHWORK QUANTITIES ARE CALCULATED BASED ON THE MATERIAL IN ITS INITIAL OR FINAL POSITION AS QUANTIFIED BY THE METHOD OF AVERAGE END-AREAS.
 2. AREAS OF UNSUITABLE MATERIAL (PAID AS UNCLASSIFIED EXCAVATION) SHALL BE AS DESIGNATED BY THE RESIDENT ENGINEER. UNSUITABLE MATERIAL SHALL NOT BE USED AS EMBANKMENT MATERIAL.
 3. THE COST OF BUILDING EMBANKMENT SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE PRICE OF UNCLASSIFIED EXCAVATION AND BORROW EXCAVATION.
 4. TOPSOIL PLACEMENT SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE PRICE OF TOPSOIL STRIPPING.



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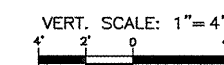
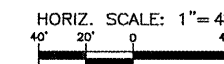
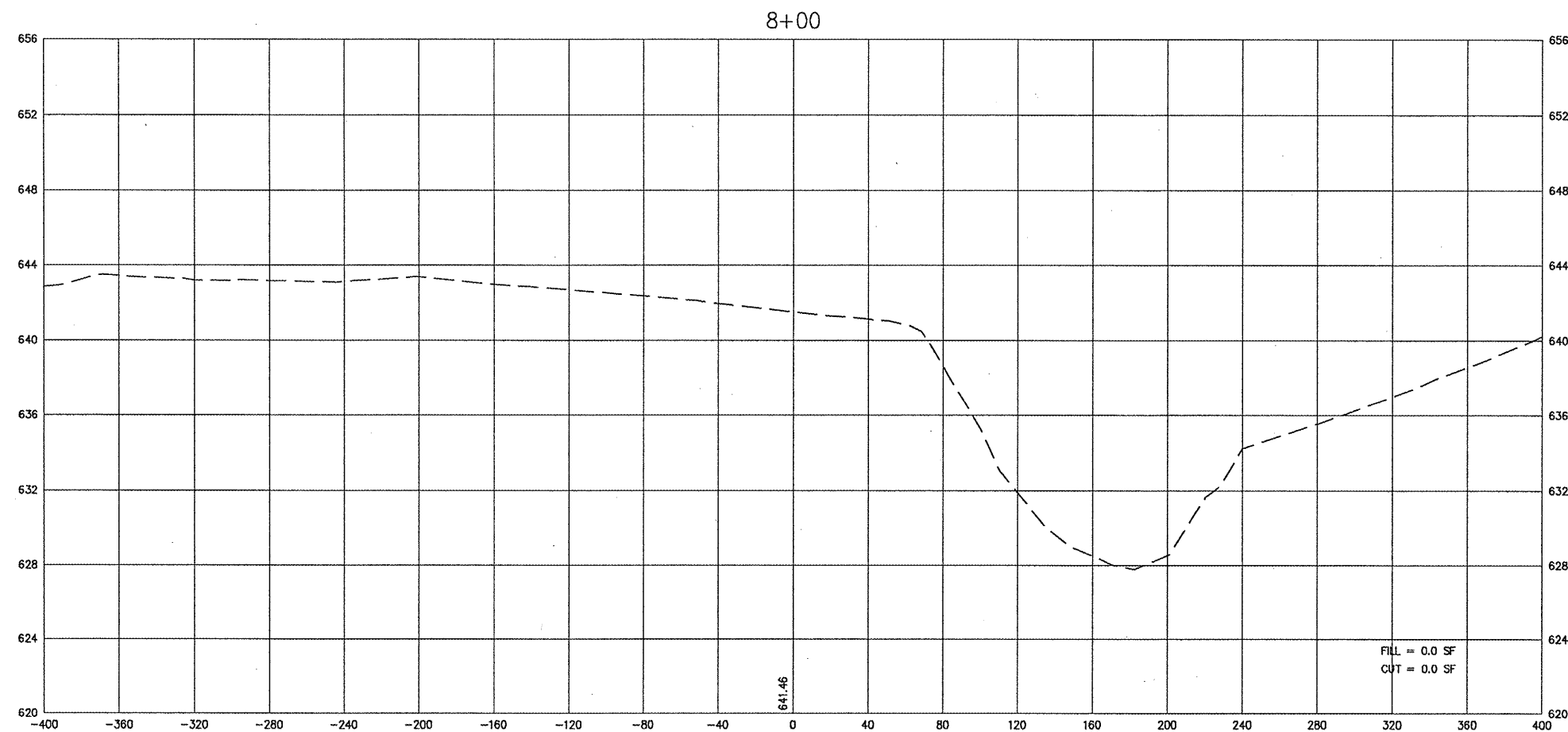
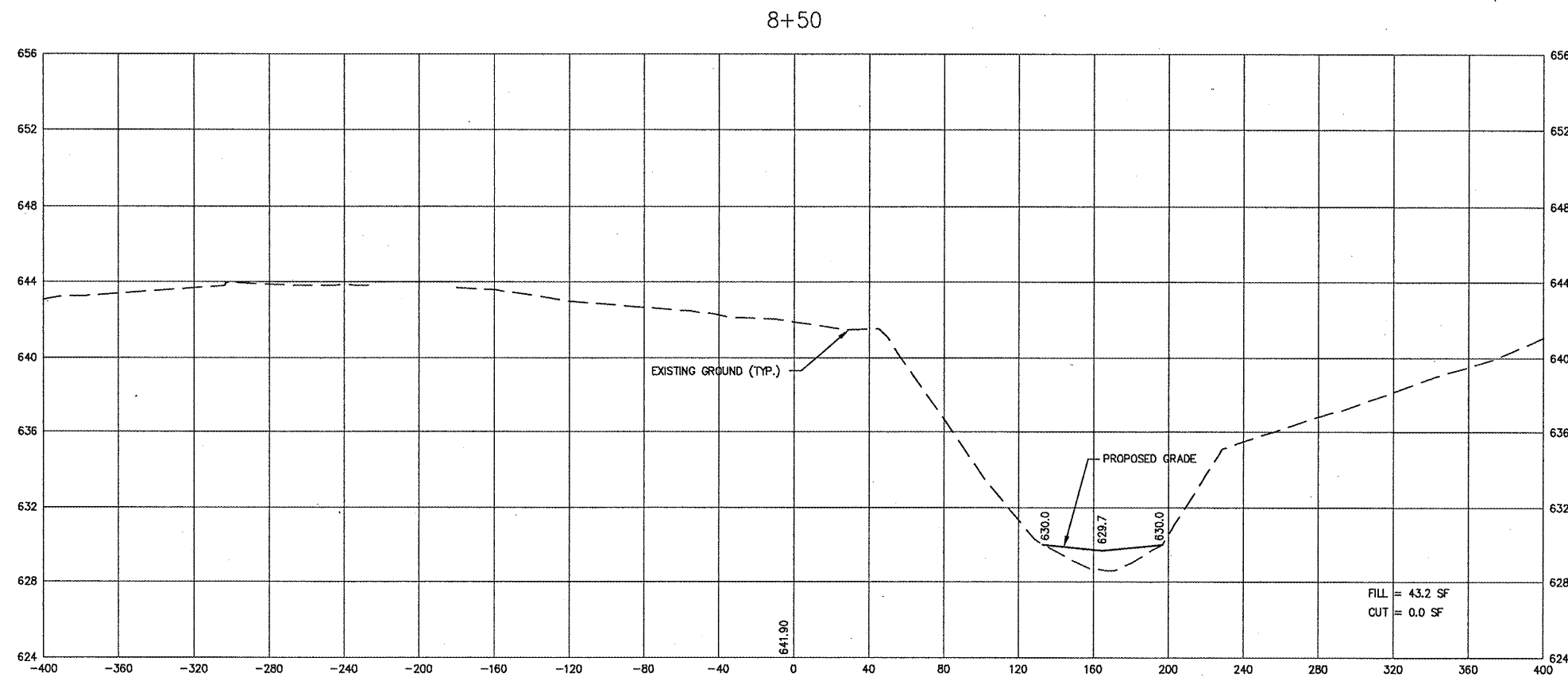
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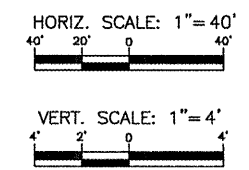
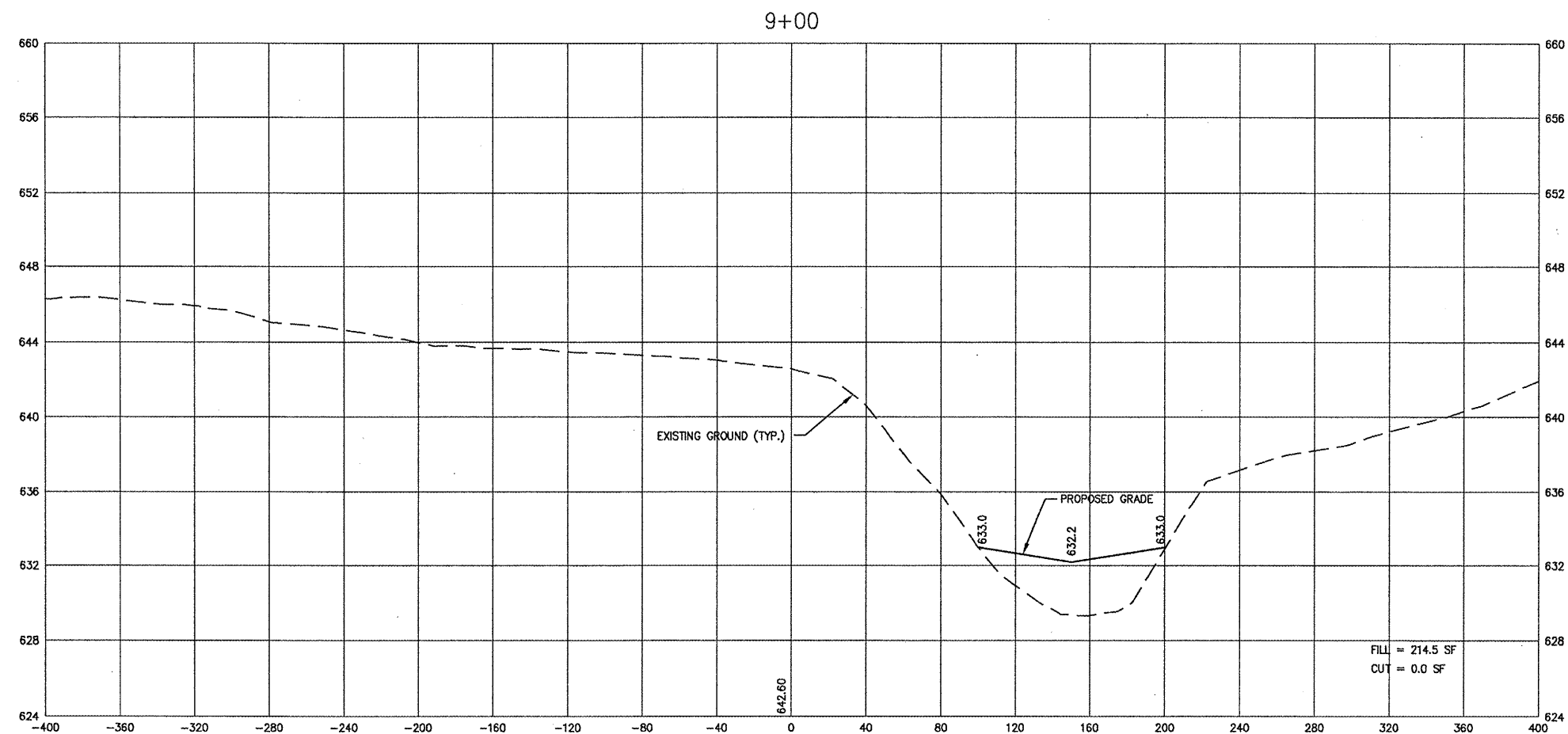
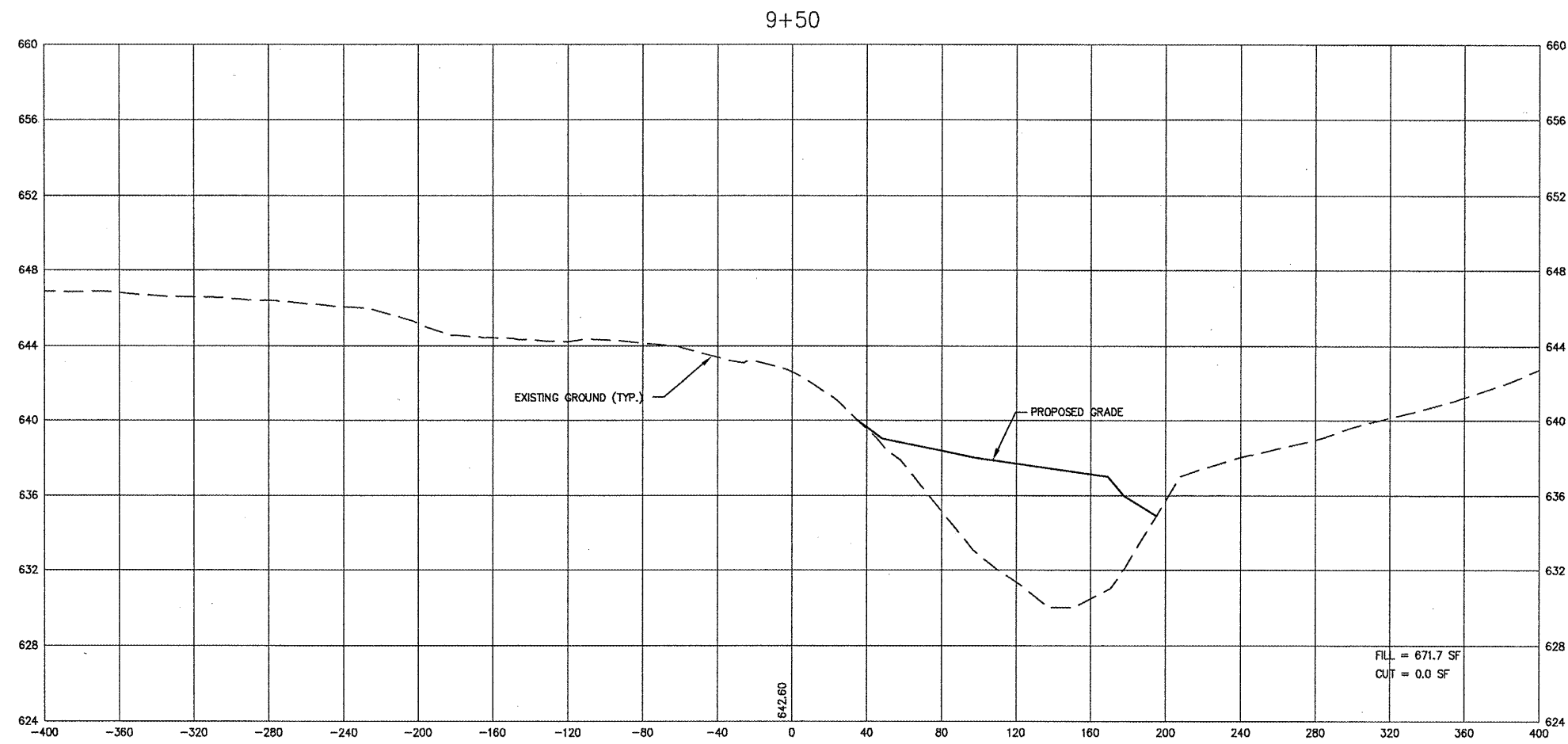
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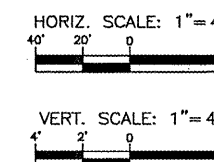
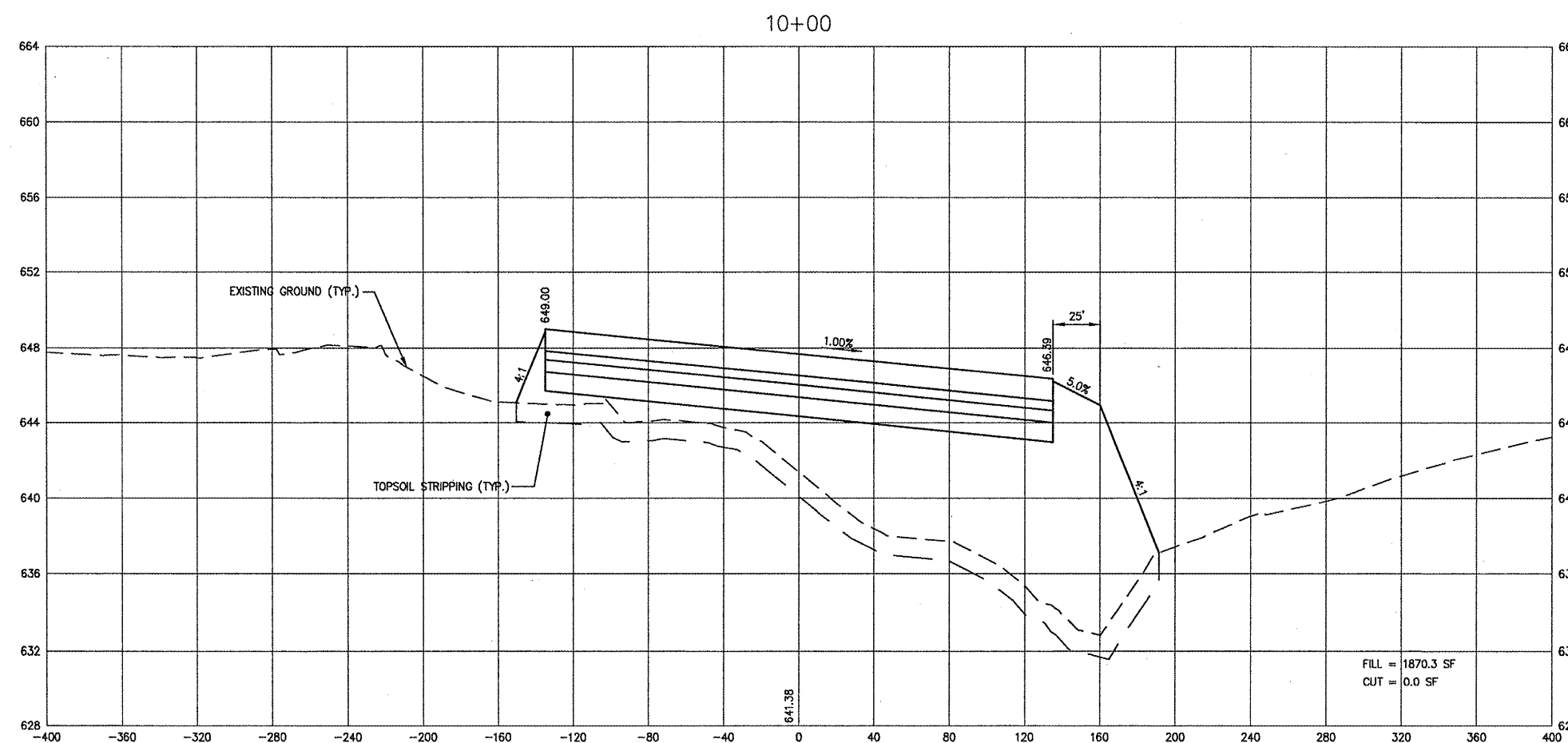
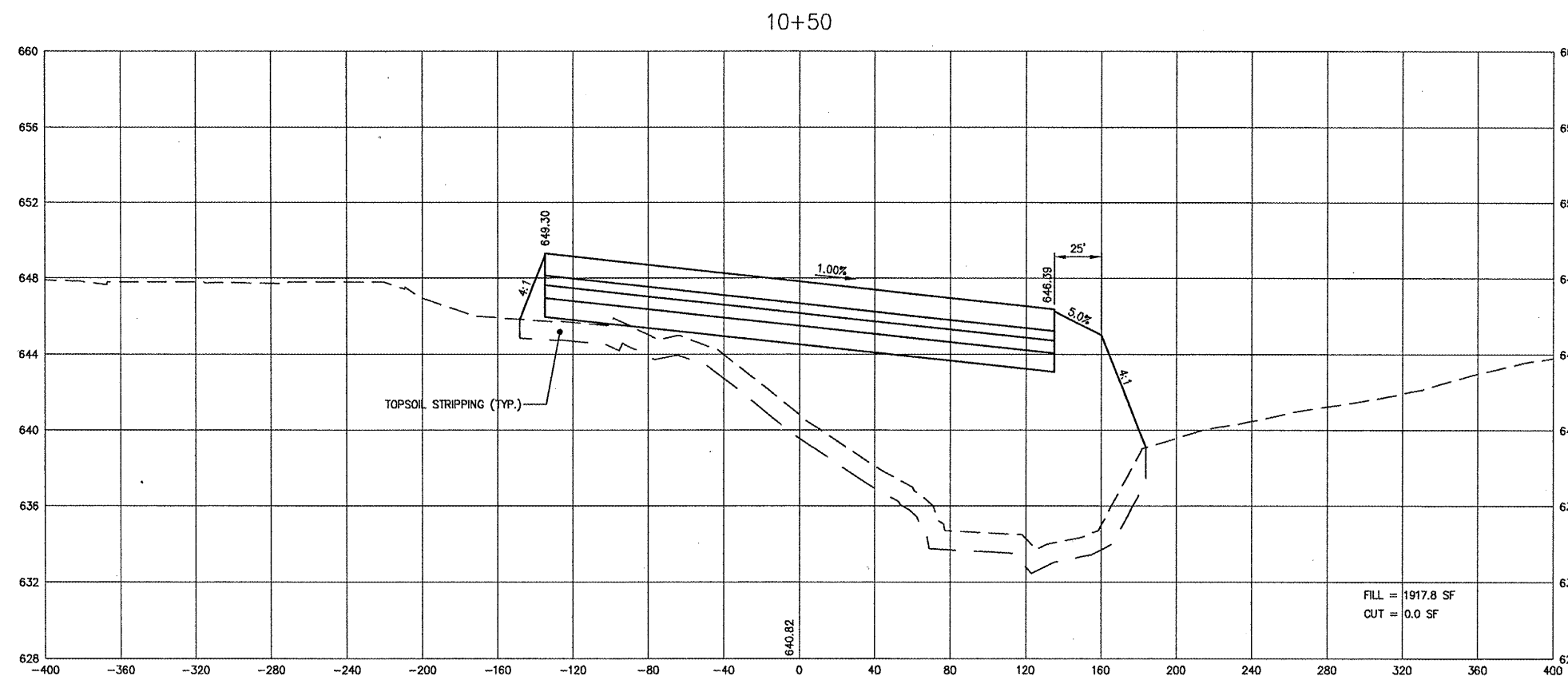
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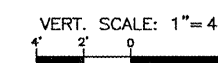
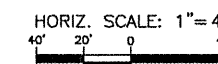
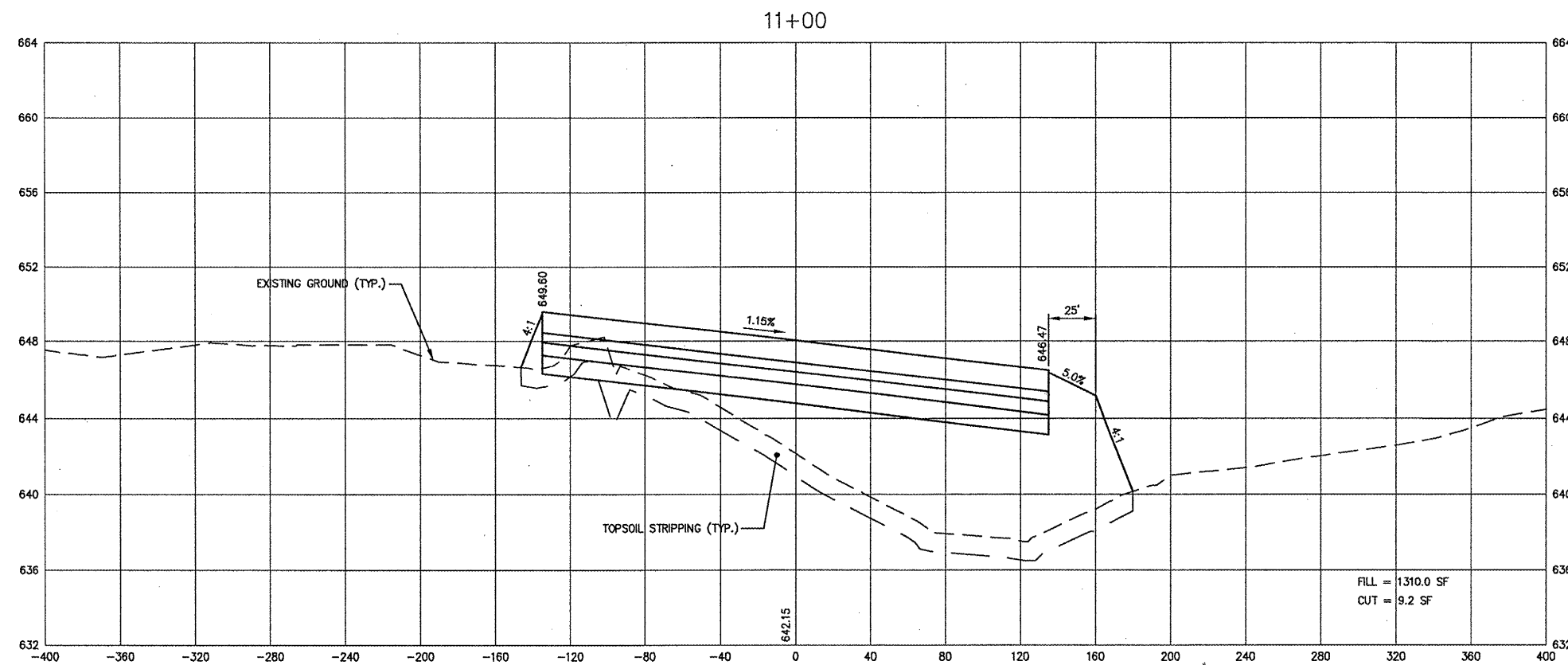
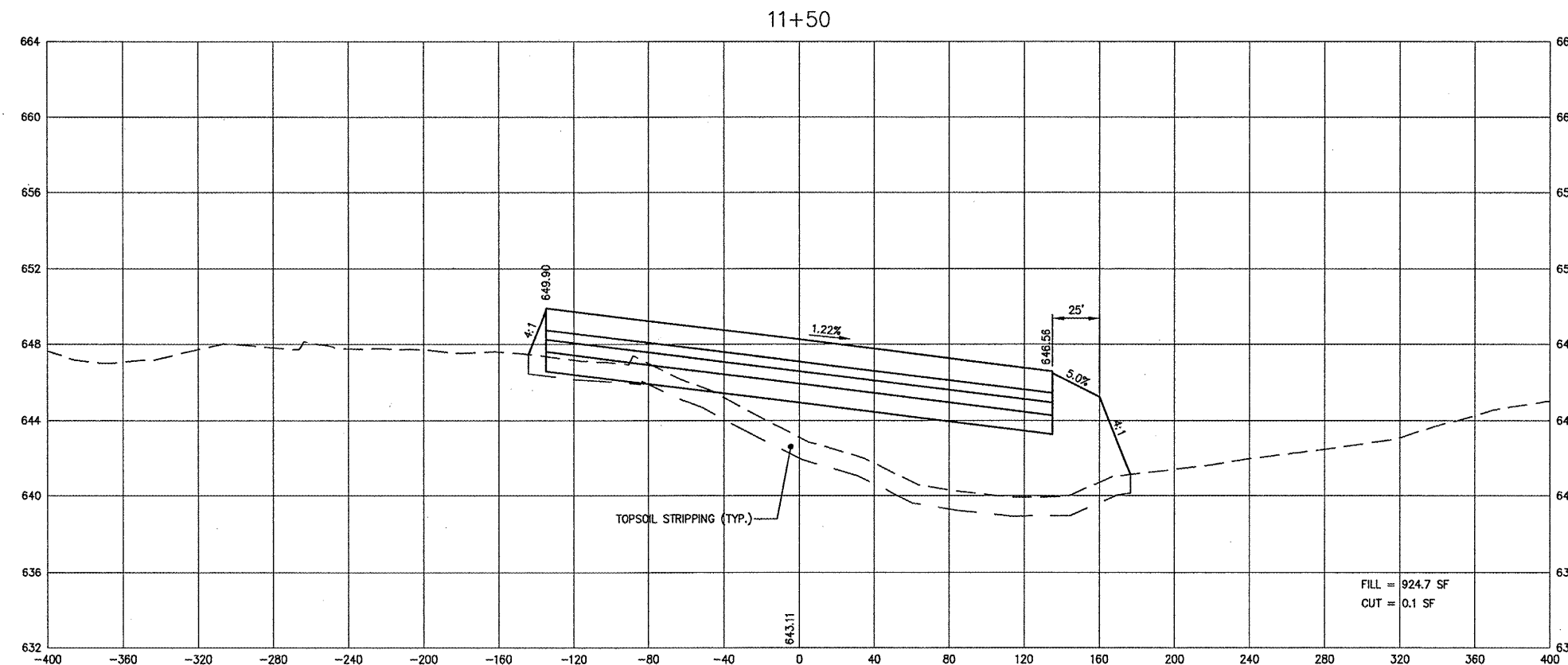
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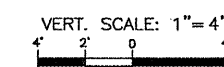
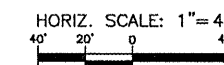
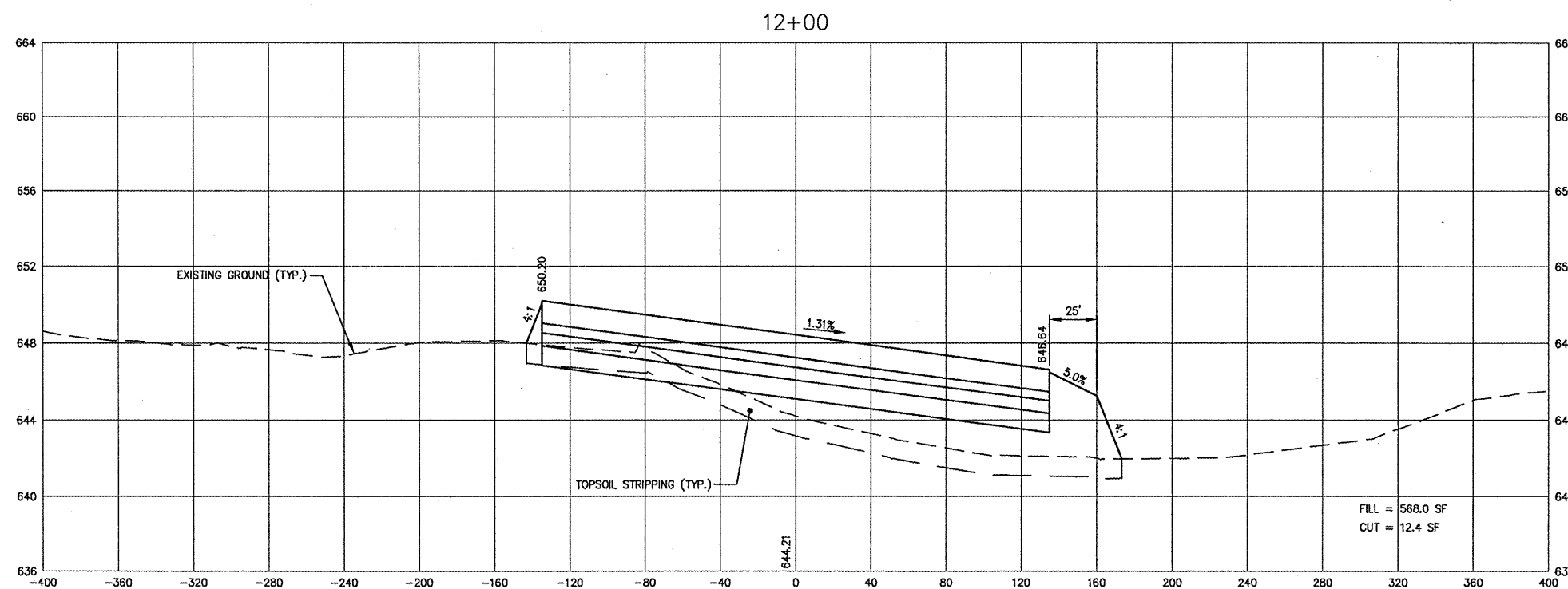
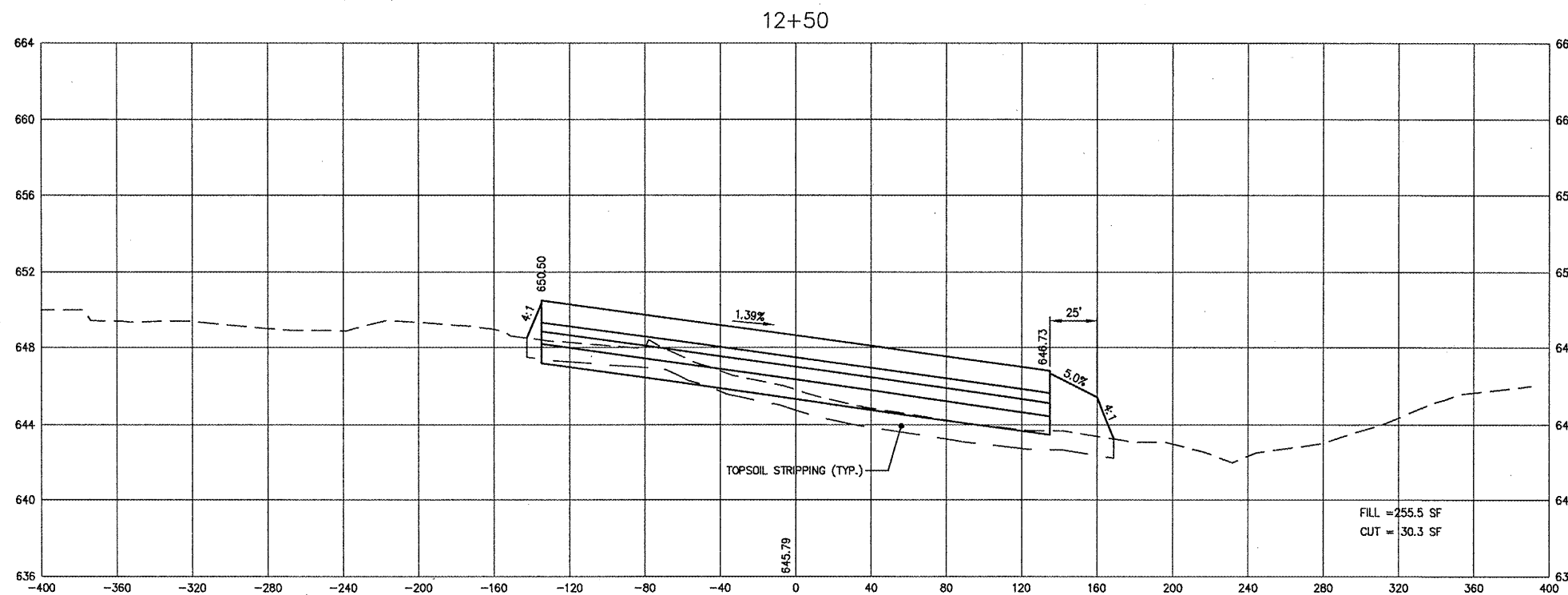
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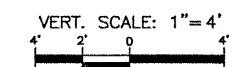
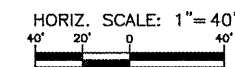
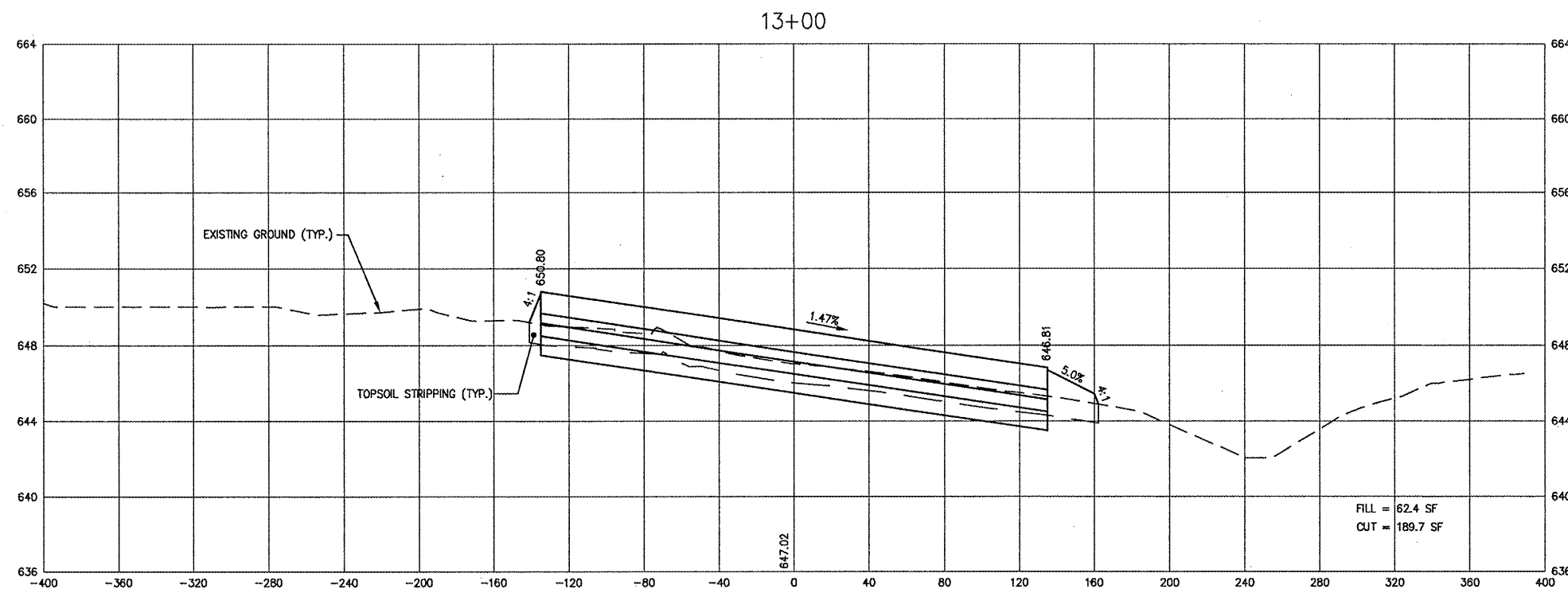
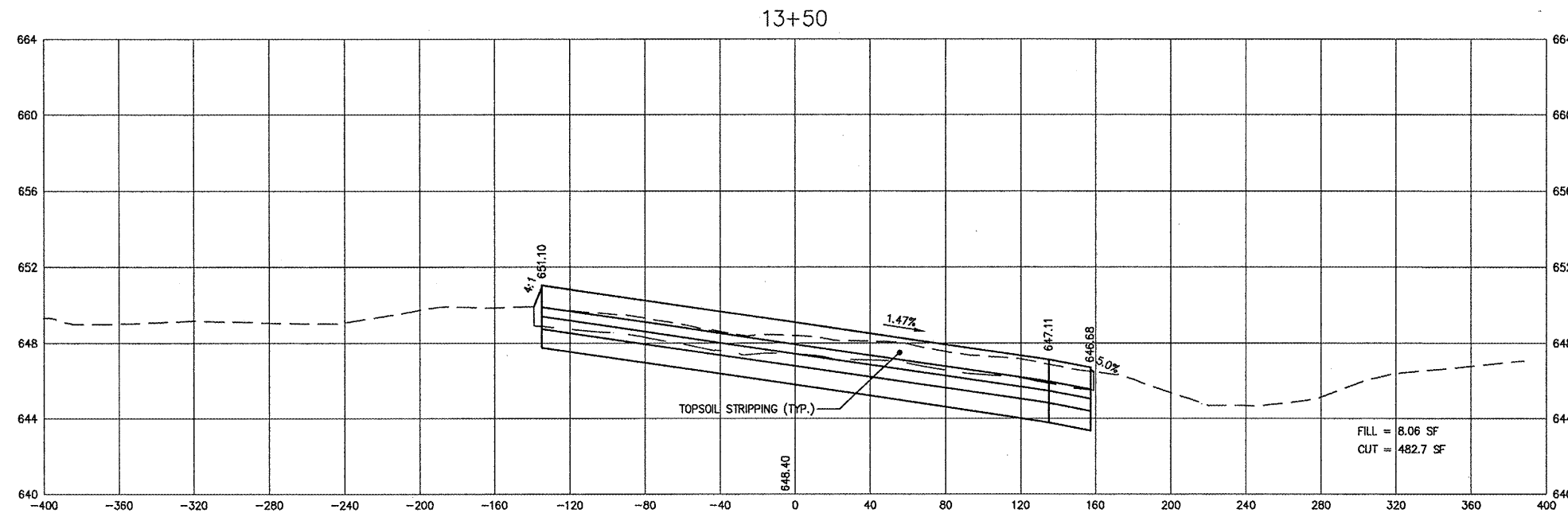
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