

LETTING ITEM NO. 10A
 LETTING DATE: AUGUST 1, 2014

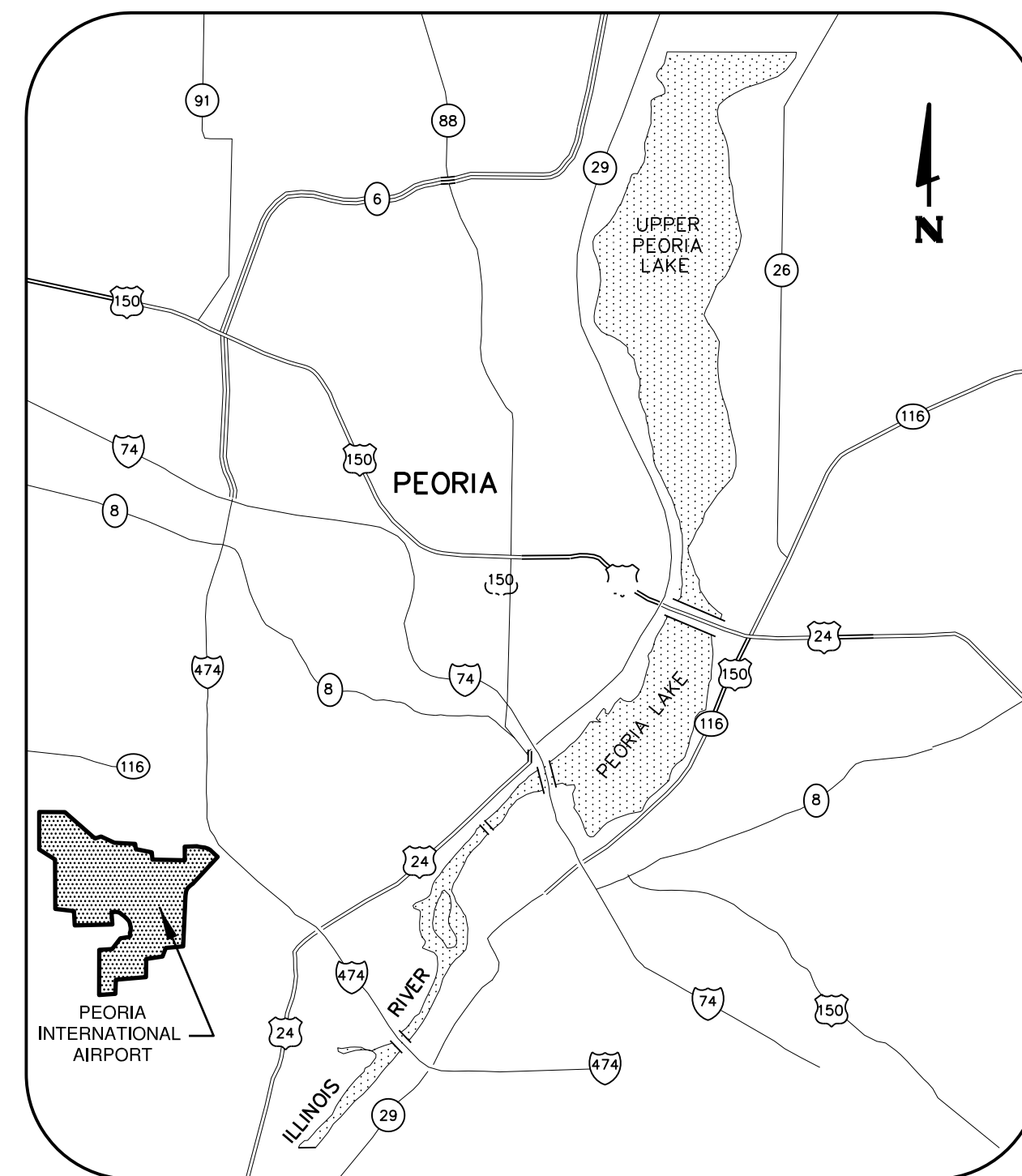
TOTAL SHEETS: 67
 PE098

CONSTRUCTION PLANS FOR GENERAL WAYNE A. DOWNING PEORIA INTERNATIONAL AIRPORT PEORIA, ILLINOIS

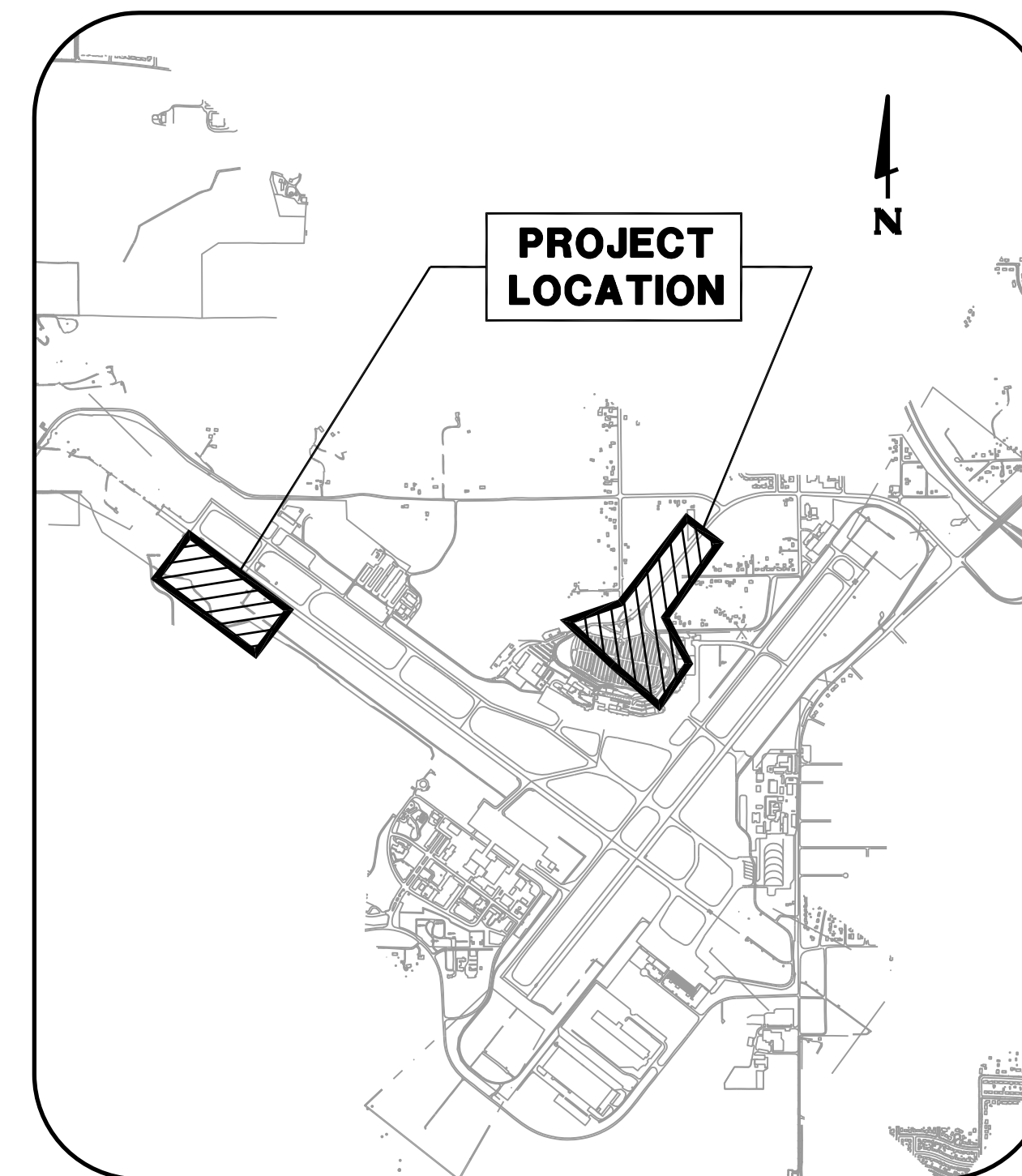
IL PROJ. NO. PIA-4415
 AIP PROJ. NO. 3-17-0080-XX

DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2

JULY 11, 2014



LOCATION MAP



SITE PLAN

811 Know what's below.
 Call before you dig. COMMON GROUND ALLIANCE
 www.call811.com or
 Phone: 811

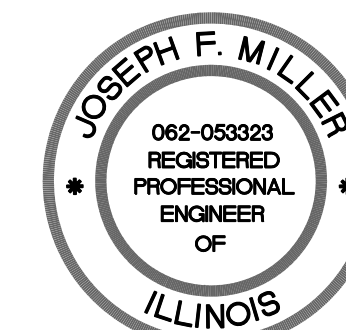
THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE
 RESULTS IN THE RELEASE OF NATURAL GAS.

MAXIMUM EQUIPMENT HEIGHT = 25'
 GROUND FREQUENCY 121.85

GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

TOWNSHIP: 8 NORTH
 RANGE: 7 EAST
 COUNTY: PEORIA
 TOWNSHIP: LIMESTONE



DATE 07/11/2014 RENEWAL DATE 11/30/2015
Joseph F. Miller

GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

APPROVED *Joseph F. Miller*
 DATE 07/11/2014

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SUBMITTED BY *John S. Johnson*
 DATE 07/11/2014

CMT JOB NUMBER: 13061-05

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 FILE: 01 - Cover.dwg
 UPDATE BY: Joe Miller
 PLOT DATE: 7/10/2014 1:17 PM

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SUMMARY OF QUANTITIES

Item No.	Item Description	Unit	ESTIMATED QUANTITY
AR150520	MOBILIZATION	LS	1
AR150530	TRAFFIC MAINTENANCE	LS	1
AR151450	CLEARING AND GRUBBING	ACRE	4
AR152410	UNCLASSIFIED EXCAVATION	CY	6,300
AR152512	SUBGRADE REPAIR	CY	51
AR156511	DITCH CHECK	EA	12
AR156515	STRAW WATTLE	LF	1,540
AR156520	INLET PROTECTION	EA	21
AR156544	RIPRAP-GRADATION NO. 4	SY	562
AR156545	RIPRAP-GRADATION NO. 5	SY	167
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	734
AR209600	GEOTEXTILE FABRIC	SY	76
AR401610	BITUMINOUS SURFACE COURSE	TON	1,665
AR401650	BITUMINOUS PAVEMENT MILLING	SY	4,472
AR401655	BUTT JOINT CONSTRUCTION	SY	2,236
AR401910	REMOVE & REPLACE BIT. PAVEMENT	SY	253
AR403610	BITUMINOUS BASE COURSE	TON	3,939
AR403620	BITUMINOUS BASE COURSE, LEVELING	TON	525
AR403630	BITUMINOUS BASE TEST SECTION	EA	1
AR501120	RUBBLIZE PAVEMENT	SY	12,928
AR501606	6" PCC SIDEWALK	SF	1,021
AR501690	PCC SIDEWALK REMOVAL	SF	949
AR501900	REMOVE PCC PAVEMENT	SY	182
AR603510	BITUMINOUS TACK COAT	GAL	2,851
AR620510	PAVEMENT MARKING	SF	3,211
AR701412	12" RCP, CLASS III	LF	10
AR701436	36" RCP, CLASS III	LF	98
AR751601	CATCH BASIN TYPE A	EA	2
AR751940	ADJUST INLET	EA	12
AR752412	PRECAST REINFORCED CONC. FES 12"	EA	2
AR752436	PRECAST REINFORCED CONC. FES 36"	EA	1
AR754410	COMB CONCRETE CURB & GUTTER	LF	8,785
AR754904	REMOVE COMB CURB & GUTTER	LF	8,912
AR800249	CONSTRUCT TEMPORARY ENTRANCE	LS	1
AR801500	RETAINING WALL	SFF	2,640
AR901510	SEEDING	ACRE	4
AR905520	TOPSOILING (FROM OFF SITE)	CY	1,487
AR908514	LIGHT-DUTY HYDRAULIC MULCH	ACRE	2
AR908520	EXCELSIOR BLANKET	SY	5,731
AR910200	ROADWAY SIGN	EA	4
AR910420	BOLLARD	EA	10

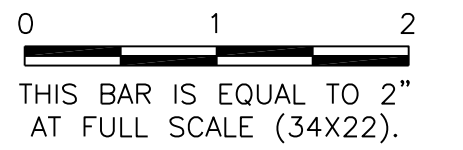
GENERAL NOTES:

- THE CONTRACTOR IS RESPONSIBLE FOR ALL UTILITY COORDINATION SHOWN OR NOT SHOWN. THE LOCATIONS, SIZE, MATERIAL, DESCRIPTION OR TYPE OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS ARE APPROXIMATE LOCATIONS BASED ON IN PART, INFORMATION SUPPLIED BY OTHERS AND IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. THE CONTRACTOR SHALL HAVE ALL UTILITIES FIELD LOCATED AS NECESSARY PRIOR TO BEGINNING WORK. THE SUPPORT, PROTECTION AND RESTORATION OF ALL UTILITIES AND APPURTENANCES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT PUBLIC AND PRIVATE PROPERTY INCLUDING UTILITIES AND STRUCTURES TO REMAIN. IF AT ANY TIME, THE CONTRACTOR DAMAGES OR DESTROYS ANY OTHER PUBLIC OR PRIVATE PROPERTY, THE CONTRACTOR SHALL, AT HIS OWN EXPENSE, RESTORE SUCH PROPERTY TO A CONDITION EQUAL TO THAT OF EXISTING BEFORE SUCH DAMAGE. PUBLIC ROADWAYS, PRIVATE ENTRANCES, AND OTHER FACILITIES SHALL REMAIN ACCESSIBLE AND FREE OF CONSTRUCTION DEBRIS. ANY MATERIAL TO BE STAGED OR STOCKPILED SHALL BE COORDINATED WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER.
- EXISTING CURB & GUTTER TYPES MAY VARY. CONTRACTOR SHALL INSPECT CURB PRIOR TO BEGINNING WORK TO ASSURE PROPER FIT AND TRANSITION.
- ALL DISTURBED TURF AREAS WITHIN PROJECT LIMITS SHALL BE SEEDED & STABILIZED. ANY AREAS DISTURBED BY THE CONTRACTOR OUTSIDE THE LIMITS SHOWN SHALL BE SEEDED & STABILIZED AT THE CONTRACTOR'S EXPENSE.
- ALL EXISTING SIGNAGE TO REMAIN UNLESS NOTED OTHERWISE. IF EXISTING ROADWAY SIGNS INTERFERE WITH THE INSTALLATION OF THE WORK FOR THIS PROJECT, SUCH SIGNS AND POSTS SHALL BE REMOVED AND REPLACED AFTER CONSTRUCTION. THE COST OF ANY REMOVAL, STORING, AND REPLACEMENT OF EXISTING SIGNS WILL NOT BE PAID FOR DIRECTLY. CONTRACTOR SHALL PROTECT ALL EXISTING MONUMENT SIGNAGE FROM DISRUPTION AND DAMAGE DURING CONSTRUCTION.
- ACCESS TO ALL ENTRANCES SHALL BE MAINTAINED AT ALL TIMES UNLESS NOTED OTHERWISE ON THE PLANS.
- ALL COMPACTION CONTROL TESTS SHALL CONFORM TO ASTM D1557 (MODIFIED PROCTOR) FOR AIRCRAFT (HEAVY EQUIPMENT) WEIGHTS OF 60,000 LBS OR MORE.

PE098

REVISIONS

NUMBER	BY	DATE



GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS

DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
SUMMARY OF QUANTITIES & GENERAL NOTES

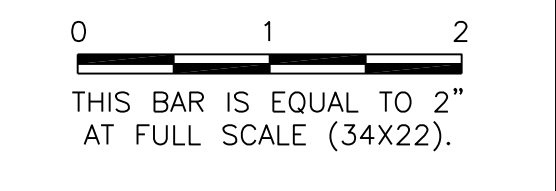
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PEORIA INTERNATIONAL AIRPORT

DESIGN BY:	JFM
DRAWN BY:	JFM
CHECKED BY:	JMW
APPROVED BY:	CET
DATE:	JULY 11, 2014
JOB No:	13061-05
IL PROJ. NO. PIA-4415	
AIP PROJ. NO. 3-17-0080-XX	
SHEET	02 OF 67 SHEETS


PE098

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PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
SITE PLAN - PHASE 1

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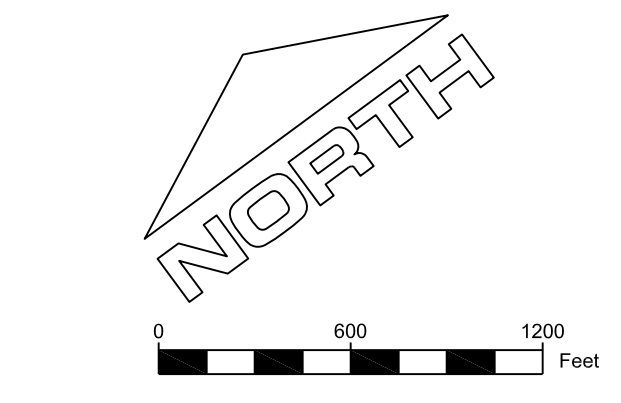
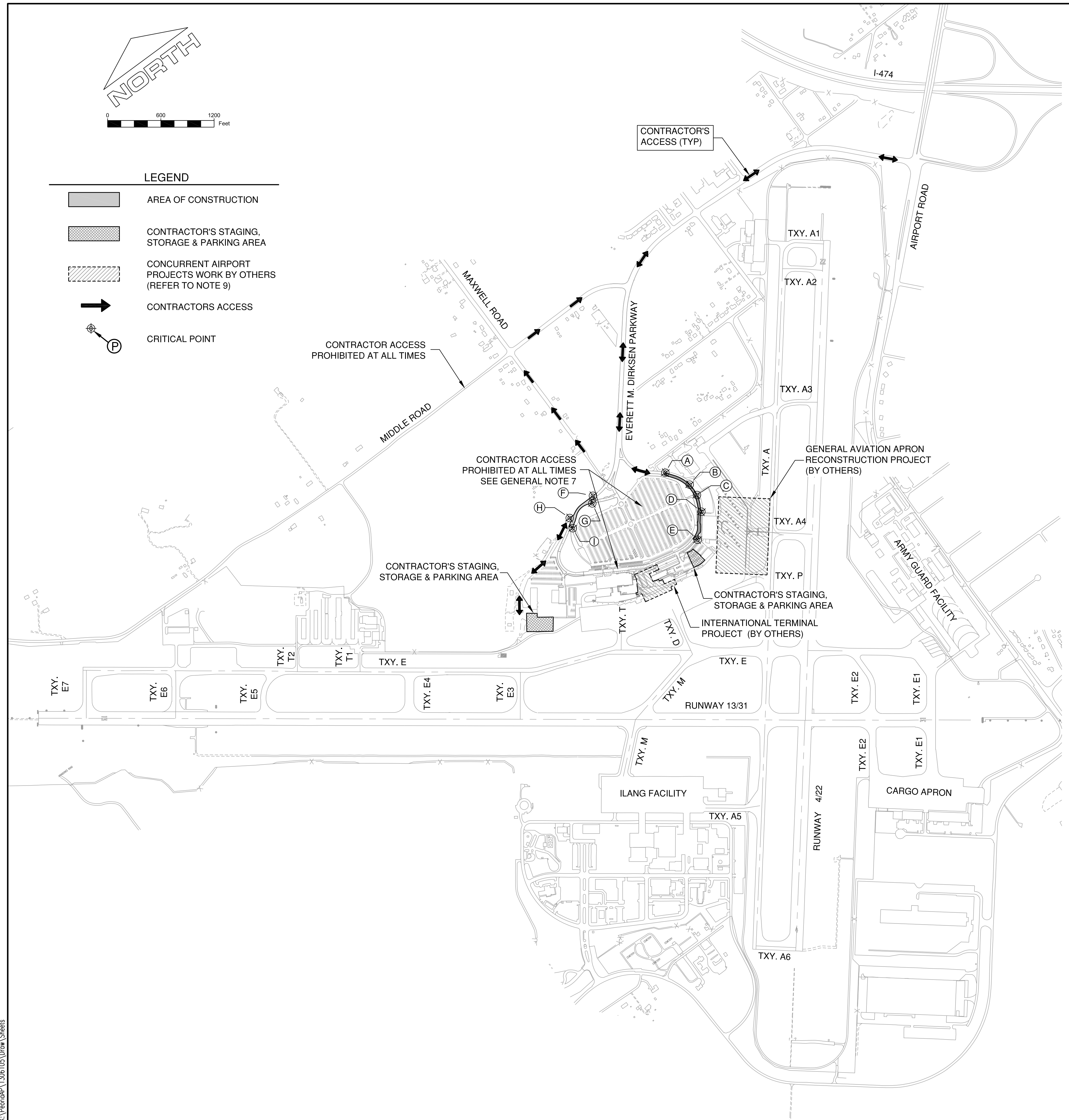
GENERAL NOTES

1. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS, AND STOCKPILES SHALL BE 25 FEET ABOVE GROUND ELEVATION AND SHALL BE STORED A MINIMUM OF 10 FEET FROM THE AIRPORT FENCE.
2. EXCESS EARTH, BROKEN ASPHALT, AND CONCRETE SHALL BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
3. ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
4. ALL DISTURBED AREAS WITHIN THE PROJECT LIMITS SHALL BE SEEDED AND STABILIZED. ANY AREAS DISTURBED BY THE CONTRACTOR OUTSIDE THE LIMITS SHOWN SHALL BE SEEDED & STABILIZED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
5. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE RESIDENT ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
6. SEE CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS SHEET FOR NOTES REGARDING THE CONTRACTOR'S ACCESS AND CONSTRUCTION ACTIVITY PLANS FOR DETAILED PHASING REQUIREMENTS.
7. CONTRACTOR SHALL BE EXPLICITLY RESTRICTED FROM USING THE ROAD IN FRONT OF THE MAIN TERMINAL BUILDING (PCC PORTION) AND THE MAIN TERMINAL PARKING LOT FOR CONSTRUCTION ACCESS OF ANY KIND UNLESS OTHERWISE NOTED IN THE PLANS. THE CONTRACTOR SHALL BE REQUIRED TO STAGE CONSTRUCTION AROUND THE USE OF THESE AREAS AT ALL TIMES. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR'S VEHICLES OR EQUIPMENT BE ALLOWED TO USE THESE AREAS.
8. THERE SHALL NOT BE ANY CONSTRUCTION TRAFFIC ON THE AIRFIELD SIDE OF THE FENCE.
9. THERE ARE TWO (2) CONCURRENT PEORIA INTERNATIONAL PROJECTS UNDER CONSTRUCTION DURING THE PROPOSED REHABILITATE TERMINAL ENTRANCE ROAD PROJECT, PHASE 1. THE CONTRACTOR SHALL BE REQUIRED TO COORDINATE WITH ADJACENT WORK AT ALL TIMES.
10. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT PUBLIC AND PRIVATE PROPERTY INCLUDING UTILITIES AND STRUCTURES TO REMAIN. IF AT ANY TIME, THE CONTRACTOR DAMAGES OR DESTROYS ANY OTHER PUBLIC OR PRIVATE PROPERTY, THE CONTRACTOR SHALL, AT HIS OWN EXPENSE, RESTORE SUCH PROPERTY TO A CONDITION EQUAL TO THAT OF EXISTING BEFORE SUCH DAMAGE. PUBLIC ROADWAYS, PRIVATE ENTRANCES, AND OTHER FACILITIES SHALL REMAIN ACCESSIBLE AND FREE OF CONSTRUCTION DEBRIS. ANY MATERIAL TO BE STAGED OR STOCKPILED SHALL BE COORDINATED WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER.
11. IF EXISTING ROADWAY SIGNS INTERFERE WITH THE INSTALLATION OF THE WORK FOR THIS PROJECT, SUCH SIGNS AND POSTS SHALL BE REMOVED AND REPLACED AFTER CONSTRUCTION. THE COST OF REMOVING AND STORING EXISTING SIGNS WILL NOT BE PAID FOR DIRECTLY. THE COST SHALL BE INCLUDED IN OTHER PAY ITEMS.
12. ACCESS TO ALL ENTRANCES SHALL BE MAINTAINED AT ALL TIMES UNLESS NOTED OTHERWISE ON THE PLANS.

CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	ELEVATION
A	N40° 40' 05.96"	W89° 41' 14.90"	646.90
B	N40° 40' 03.28"	W89° 41' 13.06"	646.83
C	N40° 40' 01.88"	W89° 41' 13.10"	645.80
D	N40° 40' 00.09"	W89° 41' 14.03"	646.97
E	N40° 39' 57.83"	W89° 41' 16.90"	649.65
F	N40° 40' 08.68"	W89° 41' 25.41"	616.96
G	N40° 40' 08.03"	W89° 41' 26.14"	618.16
H	N40° 40' 08.22"	W89° 41' 30.07"	631.98
I	N40° 40' 07.10"	W89° 41' 30.59"	636.05

MAXIMUM EQUIPMENT HEIGHT 25 FEET



LEGEND

- AREA OF CONSTRUCTION
- CONTRACTOR'S STAGING, STORAGE & PARKING AREA
- CONCURRENT AIRPORT PROJECTS WORK BY OTHERS (REFER TO NOTE 9)
- CONTRACTORS ACCESS
- P CRITICAL POINT

PE098

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
CONSTRUCTION SAFETY & PHASING PLAN NOTES

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GENERAL WAYNE A. DOWNING
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IL PROJ. NO. PIA-4415 AIP PROJ. NO. 3-17-0080-XX	
SHEET 04 OF 67 SHEETS	

17. OTHER LIMITATIONS ON CONSTRUCTION

- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
- CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.

AIRPORT OPERATOR
 DOUG PALMER - DIRECTOR OF OPERATIONS (309) 303-0994

AIRPORT MAINTENANCE
 GREG HUSER - MAINTENANCE SUPERVISOR (309) 303-1005

ENGINEER
 CHUCK TAYLOR, P.E. - PROJECT ENGINEER (309) 637-1890
 CMT - RESIDENT ENGINEER (309) 637-1890

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL ATTEND AN INSPECTION OF EACH STAGE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

- ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- NO RUNWAY OR TAXIWAY CLOSURES ARE REQUIRED FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2F.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

15. MARKING AND SIGNS FOR ACCESS ROUTES

- BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THIS SHEET AND THE CONSTRUCTION ACTIVITY PLAN SHEET.

16. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
- THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN.
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F (OR LATEST), AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS AIRFIELD OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL, THE TRAVELLING PUBLIC, AND MEETING OSHA REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PROJECT STAGING

- TOTAL CONTRACT TIME SHALL BE 56 CALENDAR DAYS.
- STAGING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.

STAGING NOTES:

- ALL STAGES SHALL CONSIST OF REMOVAL AND REPLACEMENT OF THE EXISTING CURB AND GUTTER, MILLING OF THE EXISTING BITUMINOUS PAVEMENT, PATCHING, AND INSTALLATION OF NEW BITUMINOUS PAVEMENT AND STRIPING.
- ALL WORK SHALL BE COMPLETED IN PREVIOUS STAGE PRIOR TO STARTING WORK IN THE NEXT STAGE UNLESS OTHERWISE PERMITTED BY THE AIRPORT.
- THE WORK STAGING PLAN PRESENTED IS NOT INTENDED TO RESTRICT THE CONTRACTOR TO THIS SPECIFIC STAGING. THE CONTRACTOR MAY SUBMIT THEIR OWN STAGING SCHEDULE FOR REVIEW AND WRITTEN APPROVAL.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

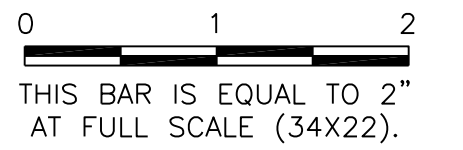
4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

- THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

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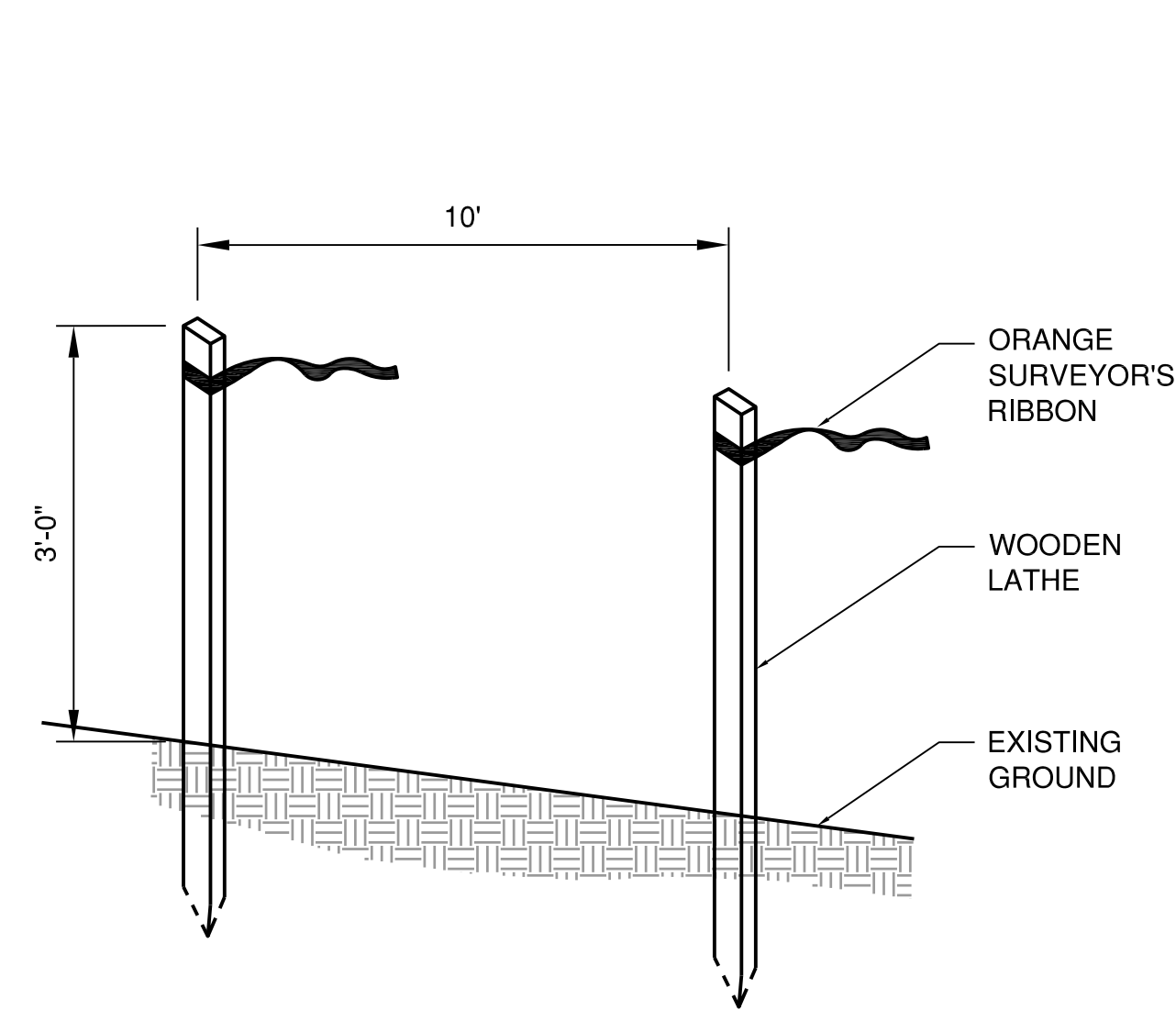
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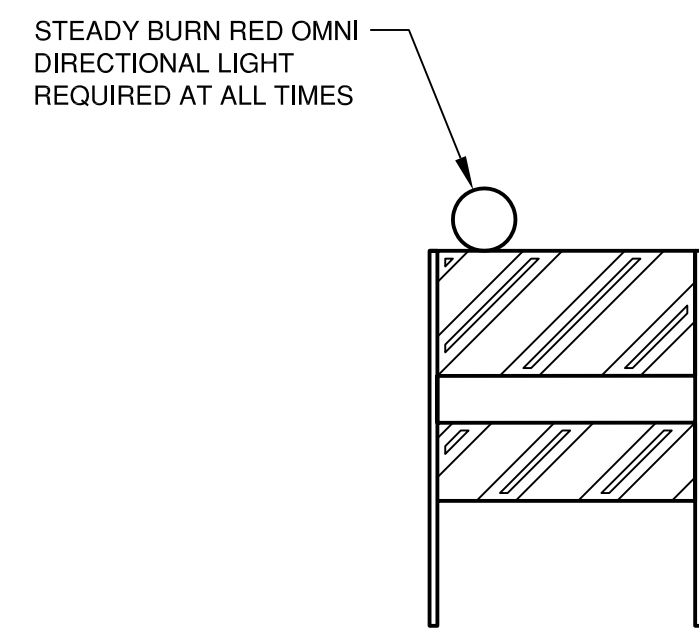


CONSTRUCTION ACTIVITY PLAN GENERAL NOTES

- THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK SO AS TO NOT INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS.
- THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS.
- IT SHALL BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECT ARE THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS.
- WHEN CONFLICTS ARISE, RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT THROUGH THE RESIDENT ENGINEER IN THE BEST INTEREST OF THE AIRPORT AND THE PROJECT.
- DELAYS, CHANGES IN SCHEDULING OR THE EXPEDITION OF WORK UNDER THIS CONTRACT TO PROVIDE FOR THE TIMELY PROSECUTION OF THE PROJECT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- CONTRACTOR'S STAGING, STORAGE, AND PARKING WILL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS.
- THE CONTRACTOR SHALL PLACE ALL BARRICADES, CONSTRUCTION SETBACK LINES, AND EROSION CONTROL ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING PAVEMENTS WHICH ARE USED BY AIRPORT CUSTOMERS ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.
 - THE CONTRACTOR SHALL EVALUATE AND PRESERVE THE CONDITION OF THE EXISTING AIRPORT ROADS AND ALL OTHER ACCESS ROUTES UTILIZED BY THE CONTRACTOR FOR ACCESS TO THE SITE. THE CONTRACTOR SHALL DOCUMENT THE CONDITION OF THE EXISTING PAVEMENT IN THE PRESENCE OF THE AIRPORT AND RESIDENT ENGINEER PRIOR TO THE INITIATION OF CONSTRUCTION. AT THE COMPLETION OF CONSTRUCTION, ANY PAVEMENT USED BY THE CONTRACTOR THAT IS SAID TO BE DAMAGED BY HIS OPERATIONS SHALL BE REPAIRED TO IT'S ORIGINAL CONDITION AND/OR TO THE SATISFACTION AT THE CONTRACTOR'S EXPENSE.



CONSTRUCTION SETBACK LINE DETAIL
N.T.S.



FLASHER BARRICADE DETAIL-IDOT TYPE 1
N.T.S.

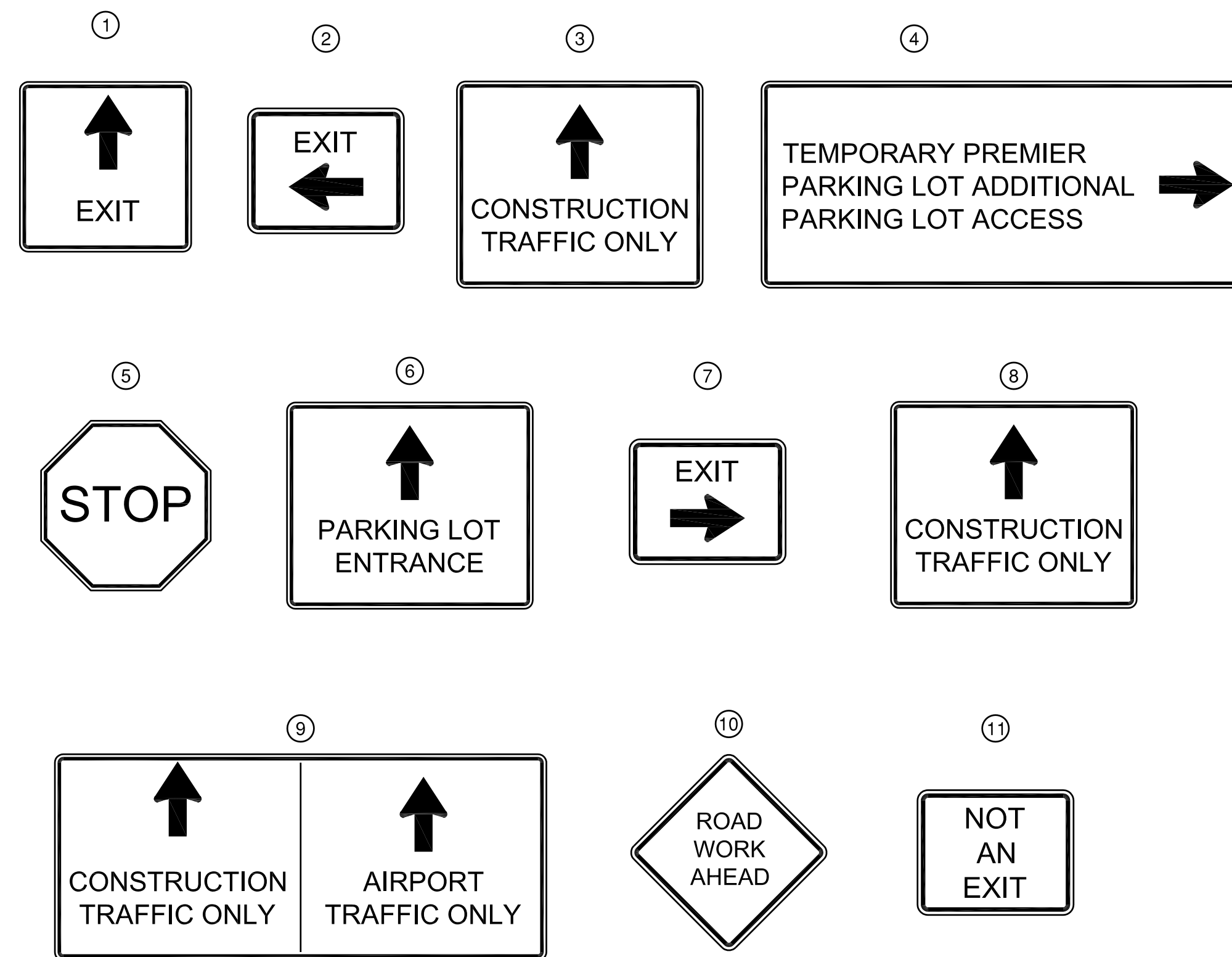
FLASHER BARRICADE NOTES

- FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
- SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- INSTALL AT 10' INTERVALS OR REQUIRED BY APPLICABLE IDOT STANDARD.

TEMPORARY CONSTRUCTION SIGNAGE NOTES

- ALL SIGNAGE SHALL BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JANUARY 1, 2012, THE DETAILS IN THESE PLANS AND THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- ALL SIGNS SHALL BE PROVIDED, PLACED AND MAINTAINED BY THE CONTRACTOR.
- THE TRAFFIC CONTROL STANDARDS PROVIDED IN THESE PLANS MAY REQUIRE MODIFICATION AT TIMES. THE AIRPORT SHALL APPROVE ANY MODIFICATIONS PROPOSED BY THE CONTRACTOR.
- ALL EXISTING SIGNAGE THAT IS NOT APPLICABLE TO THE TRAFFIC CONTROL IN PLACE SHALL BE REMOVED OR COMPLETELY COVERED.
- ALL WORK ZONE SIGNS ARE REQUIRED TO MEET, AT A MINIMUM, TYPE B REFLECTIVITY REQUIREMENTS OF TABLE 1091-2 IN ARTICLE 1091.02 OF THE IDOT STANDARD SPECIFICATIONS.

NOTE: REFER TO CONSTRUCTION ACTIVITY PLANS FOR NUMBERED LOCATIONS



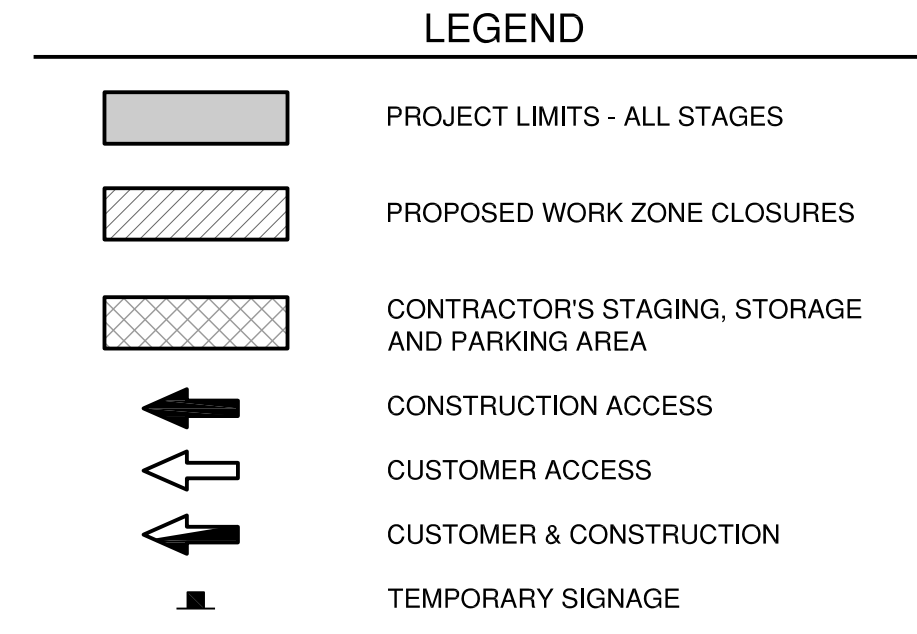
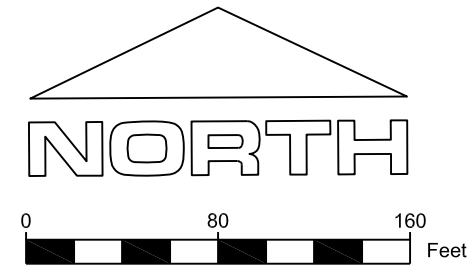
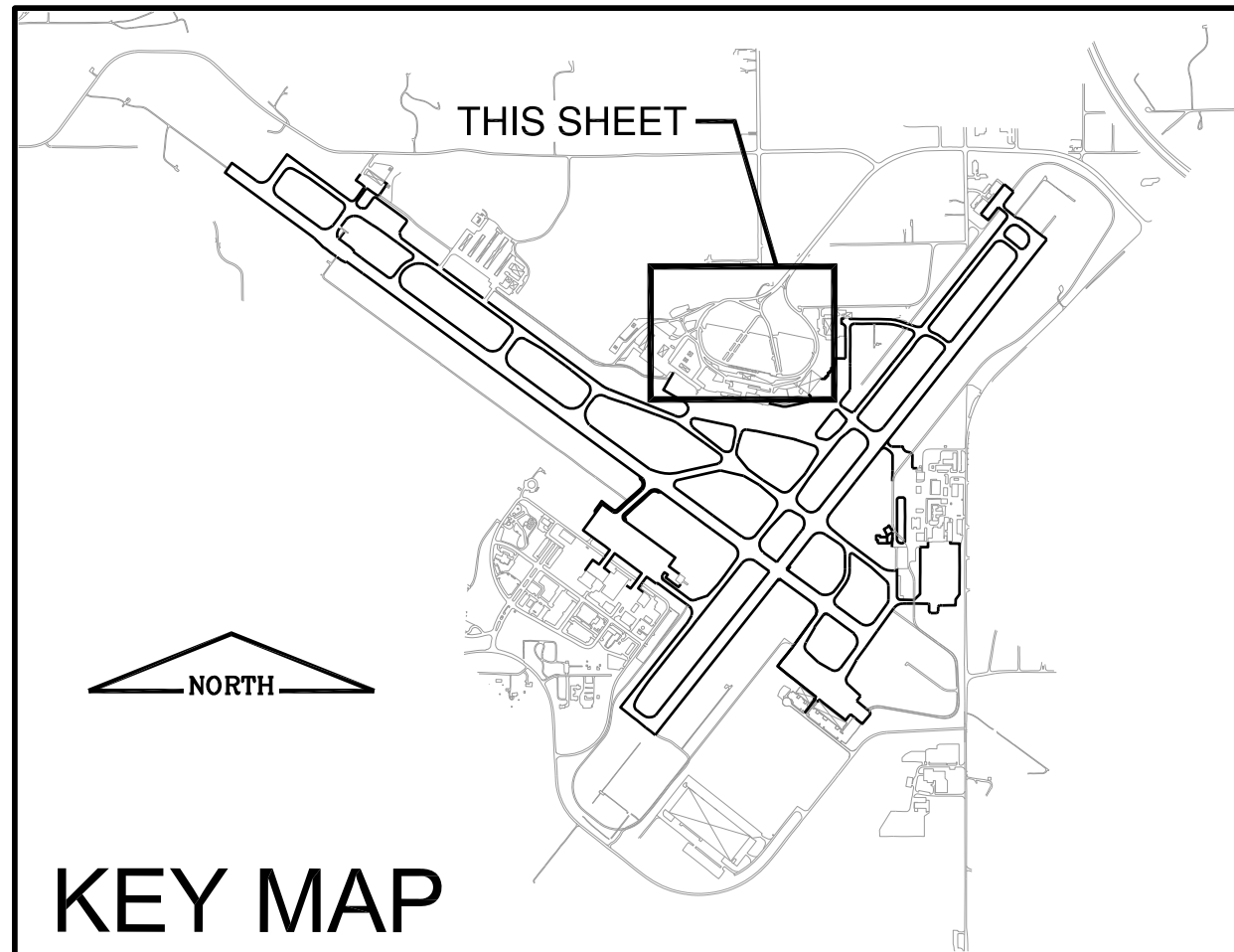
TEMPORARY CONSTRUCTION SIGNAGE
N.T.S.

GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS

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JOB No:	13061-05

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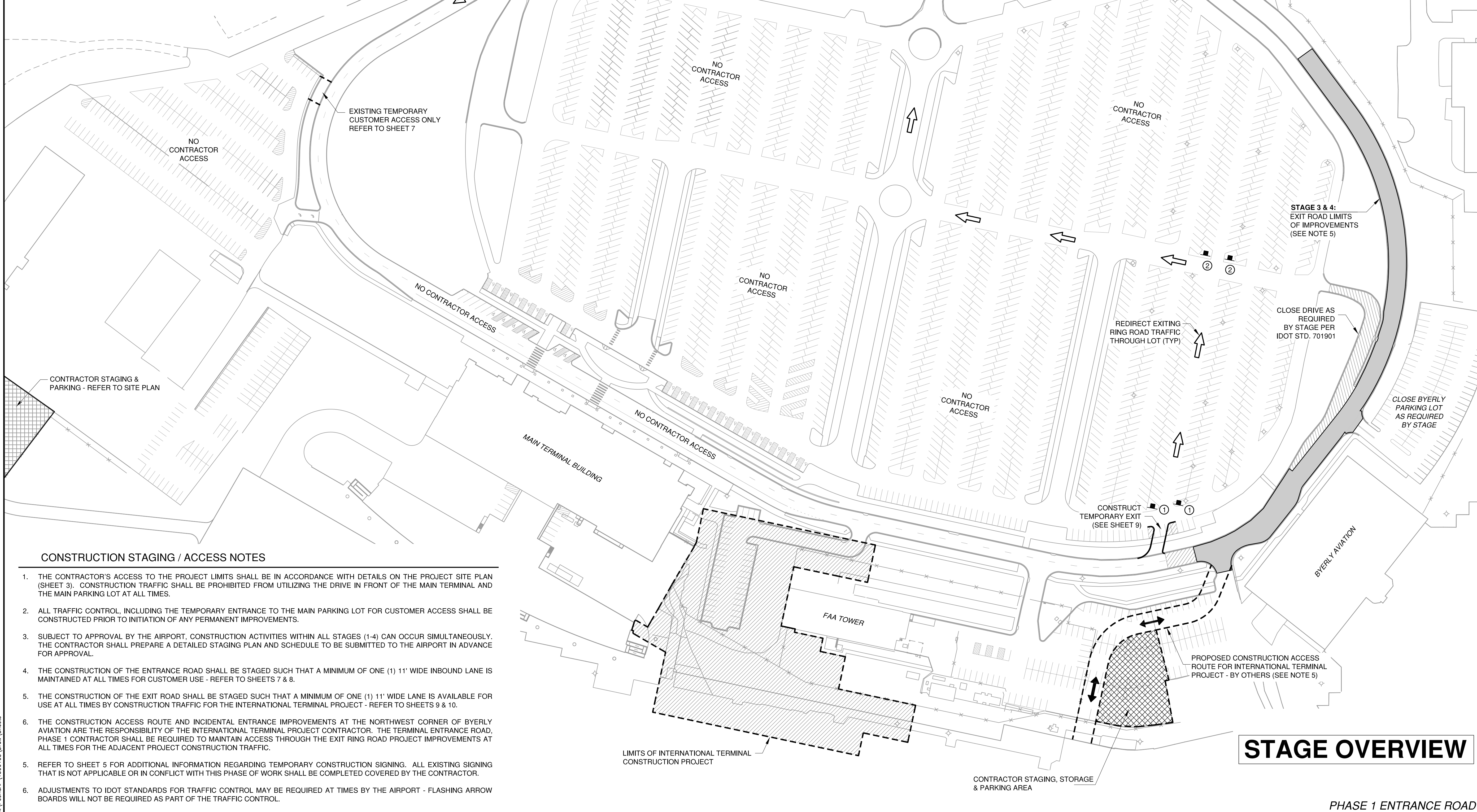
KEYMAP
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 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

KEY MAP



- CONSTRUCTION STAGING / ACCESS NOTES**
1. THE CONTRACTOR'S ACCESS TO THE PROJECT LIMITS SHALL BE IN ACCORDANCE WITH DETAILS ON THE PROJECT SITE PLAN (SHEET 3). CONSTRUCTION TRAFFIC SHALL BE PROHIBITED FROM UTILIZING THE DRIVE IN FRONT OF THE MAIN TERMINAL AND THE MAIN PARKING LOT AT ALL TIMES.
 2. ALL TRAFFIC CONTROL, INCLUDING THE TEMPORARY ENTRANCE TO THE MAIN PARKING LOT FOR CUSTOMER ACCESS SHALL BE CONSTRUCTED PRIOR TO INITIATION OF ANY PERMANENT IMPROVEMENTS.
 3. SUBJECT TO APPROVAL BY THE AIRPORT, CONSTRUCTION ACTIVITIES WITHIN ALL STAGES (1-4) CAN OCCUR SIMULTANEOUSLY. THE CONTRACTOR SHALL PREPARE A DETAILED STAGING PLAN AND SCHEDULE TO BE SUBMITTED TO THE AIRPORT IN ADVANCE FOR APPROVAL.
 4. THE CONSTRUCTION OF THE ENTRANCE ROAD SHALL BE STAGED SUCH THAT A MINIMUM OF ONE (1) 11' WIDE INBOUND LANE IS MAINTAINED AT ALL TIMES FOR CUSTOMER USE - REFER TO SHEETS 7 & 8.
 5. THE CONSTRUCTION OF THE EXIT ROAD SHALL BE STAGED SUCH THAT A MINIMUM OF ONE (1) 11' WIDE LANE IS AVAILABLE FOR USE AT ALL TIMES BY CONSTRUCTION TRAFFIC FOR THE INTERNATIONAL TERMINAL PROJECT - REFER TO SHEETS 9 & 10.
 6. THE CONSTRUCTION ACCESS ROUTE AND INCIDENTAL ENTRANCE IMPROVEMENTS AT THE NORTHWEST CORNER OF BYERLY AVIATION ARE THE RESPONSIBILITY OF THE INTERNATIONAL TERMINAL PROJECT CONTRACTOR. THE TERMINAL ENTRANCE ROAD, PHASE 1 CONTRACTOR SHALL BE REQUIRED TO MAINTAIN ACCESS THROUGH THE EXIT RING ROAD PROJECT IMPROVEMENTS AT ALL TIMES FOR THE ADJACENT PROJECT CONSTRUCTION TRAFFIC.
 5. REFER TO SHEET 5 FOR ADDITIONAL INFORMATION REGARDING TEMPORARY CONSTRUCTION SIGNING. ALL EXISTING SIGNING THAT IS NOT APPLICABLE OR IN CONFLICT WITH THIS PHASE OF WORK SHALL BE COMPLETED COVERED BY THE CONTRACTOR.
 6. ADJUSTMENTS TO IDOT STANDARDS FOR TRAFFIC CONTROL MAY BE REQUIRED AT TIMES BY THE AIRPORT - FLASHING ARROW BOARDS WILL NOT BE REQUIRED AS PART OF THE TRAFFIC CONTROL.

STAGE OVERVIEW

GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS

DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2

CONSTRUCTION ACTIVITY PLAN 1

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 APPROVED BY: CET

DATE: JULY 11, 2014
 JOB No: 13061-05

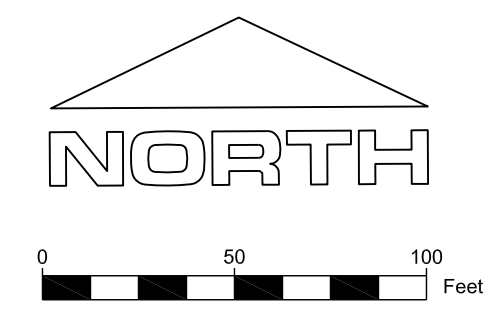
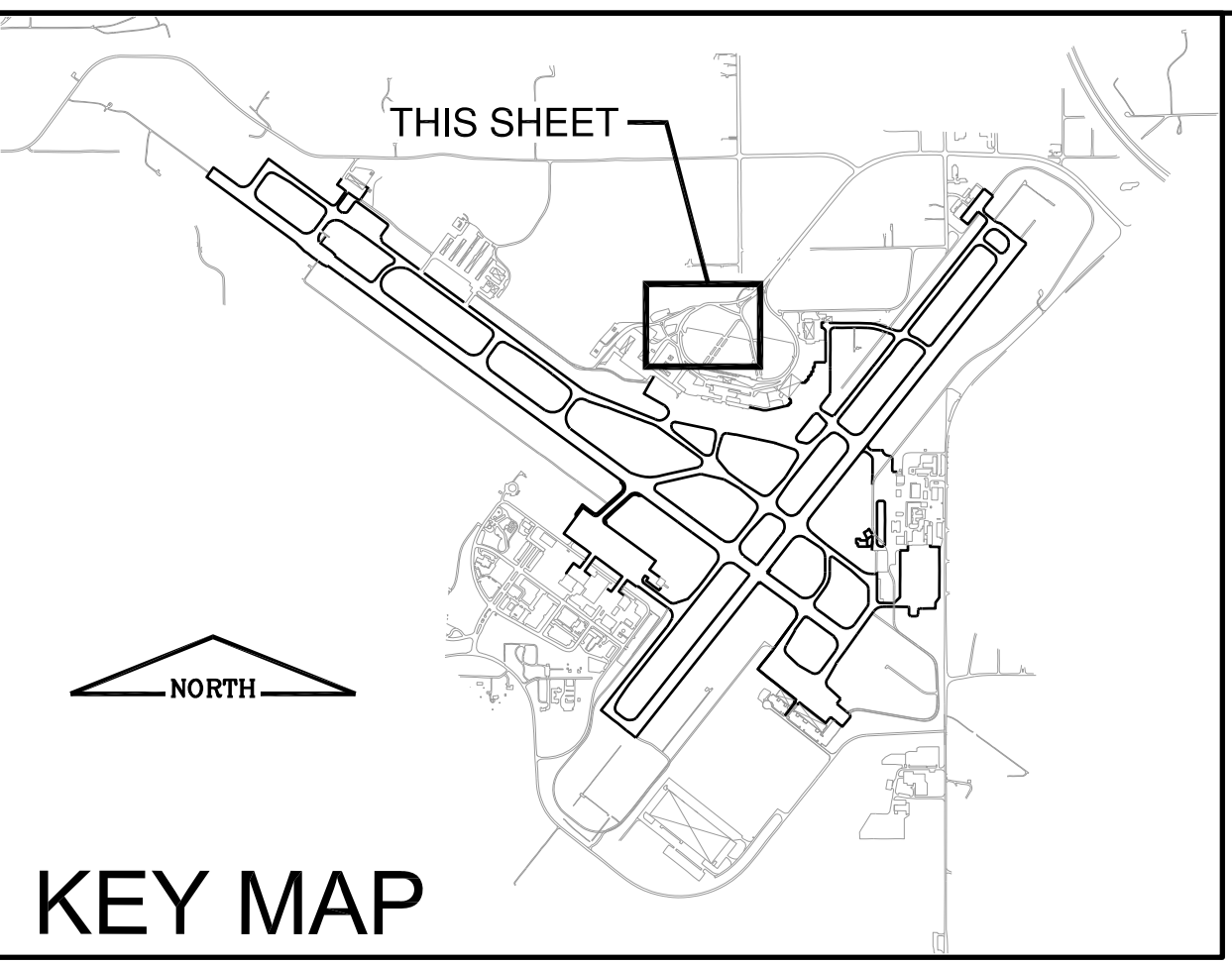
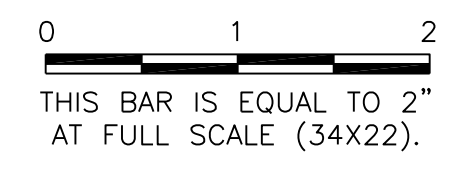
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SHEET 06 OF 67 SHEETS

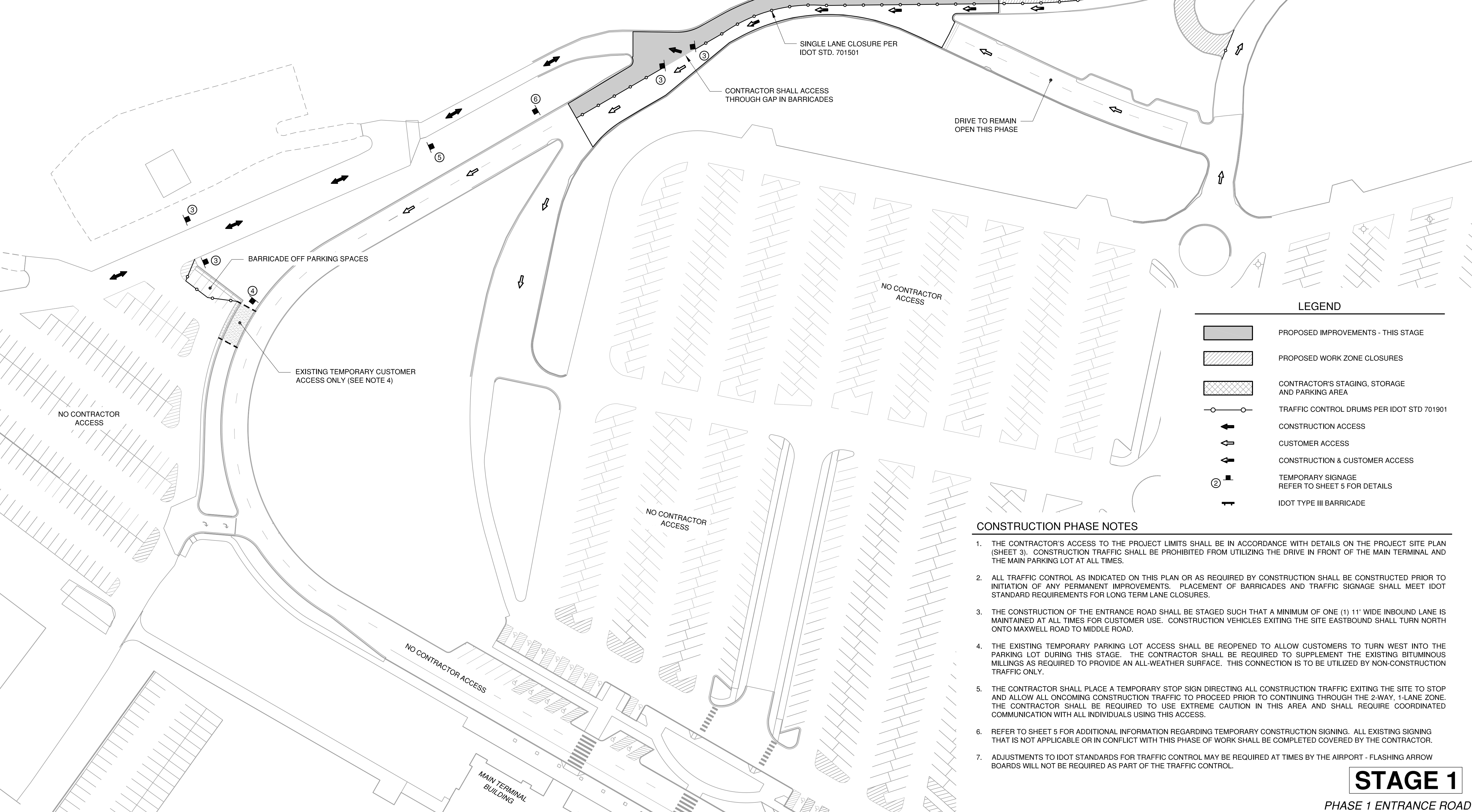
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KEY MAP



LEGEND

	PROPOSED IMPROVEMENTS - THIS STAGE
	PROPOSED WORK ZONE CLOSURES
	CONTRACTOR'S STAGING, STORAGE AND PARKING AREA
	TRAFFIC CONTROL DRUMS PER IDOT STD 701901
	CONSTRUCTION ACCESS
	CUSTOMER ACCESS
	CONSTRUCTION & CUSTOMER ACCESS
	TEMPORARY SIGNAGE REFER TO SHEET 5 FOR DETAILS
	IDOT TYPE III BARRICADE

CONSTRUCTION PHASE NOTES

1. THE CONTRACTOR'S ACCESS TO THE PROJECT LIMITS SHALL BE IN ACCORDANCE WITH DETAILS ON THE PROJECT SITE PLAN (SHEET 3). CONSTRUCTION TRAFFIC SHALL BE PROHIBITED FROM UTILIZING THE DRIVE IN FRONT OF THE MAIN TERMINAL AND THE MAIN PARKING LOT AT ALL TIMES.
2. ALL TRAFFIC CONTROL AS INDICATED ON THIS PLAN OR AS REQUIRED BY CONSTRUCTION SHALL BE CONSTRUCTED PRIOR TO INITIATION OF ANY PERMANENT IMPROVEMENTS. PLACEMENT OF BARRICADES AND TRAFFIC SIGNAGE SHALL MEET IDOT STANDARD REQUIREMENTS FOR LONG TERM LANE CLOSURES.
3. THE CONSTRUCTION OF THE ENTRANCE ROAD SHALL BE STAGED SUCH THAT A MINIMUM OF ONE (1) 11' WIDE INBOUND LANE IS MAINTAINED AT ALL TIMES FOR CUSTOMER USE. CONSTRUCTION VEHICLES EXITING THE SITE EASTBOUND SHALL TURN NORTH ONTO MAXWELL ROAD TO MIDDLE ROAD.
4. THE EXISTING TEMPORARY PARKING LOT ACCESS SHALL BE REOPENED TO ALLOW CUSTOMERS TO TURN WEST INTO THE PARKING LOT DURING THIS STAGE. THE CONTRACTOR SHALL BE REQUIRED TO SUPPLEMENT THE EXISTING BITUMINOUS MILLINGS AS REQUIRED TO PROVIDE AN ALL-WEATHER SURFACE. THIS CONNECTION IS TO BE UTILIZED BY NON-CONSTRUCTION TRAFFIC ONLY.
5. THE CONTRACTOR SHALL PLACE A TEMPORARY STOP SIGN DIRECTING ALL CONSTRUCTION TRAFFIC EXITING THE SITE TO STOP AND ALLOW ALL ONCOMING CONSTRUCTION TRAFFIC TO PROCEED PRIOR TO CONTINUING THROUGH THE 2-WAY, 1-LANE ZONE. THE CONTRACTOR SHALL BE REQUIRED TO USE EXTREME CAUTION IN THIS AREA AND SHALL REQUIRE COORDINATED COMMUNICATION WITH ALL INDIVIDUALS USING THIS ACCESS.
6. REFER TO SHEET 5 FOR ADDITIONAL INFORMATION REGARDING TEMPORARY CONSTRUCTION SIGNING. ALL EXISTING SIGNING THAT IS NOT APPLICABLE OR IN CONFLICT WITH THIS PHASE OF WORK SHALL BE COMPLETED COVERED BY THE CONTRACTOR.
7. ADJUSTMENTS TO IDOT STANDARDS FOR TRAFFIC CONTROL MAY BE REQUIRED AT TIMES BY THE AIRPORT - FLASHING ARROW BOARDS WILL NOT BE REQUIRED AS PART OF THE TRAFFIC CONTROL.

STAGE 1

PHASE 1 ENTRANCE ROAD

GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS

DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2

CONSTRUCTION ACTIVITY PLAN 2

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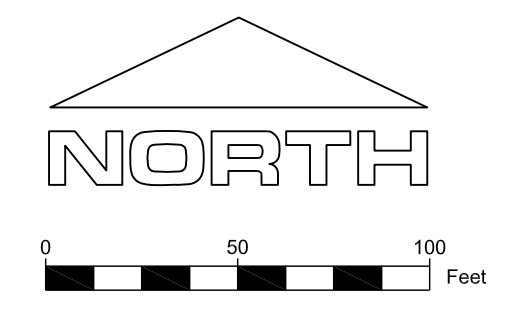
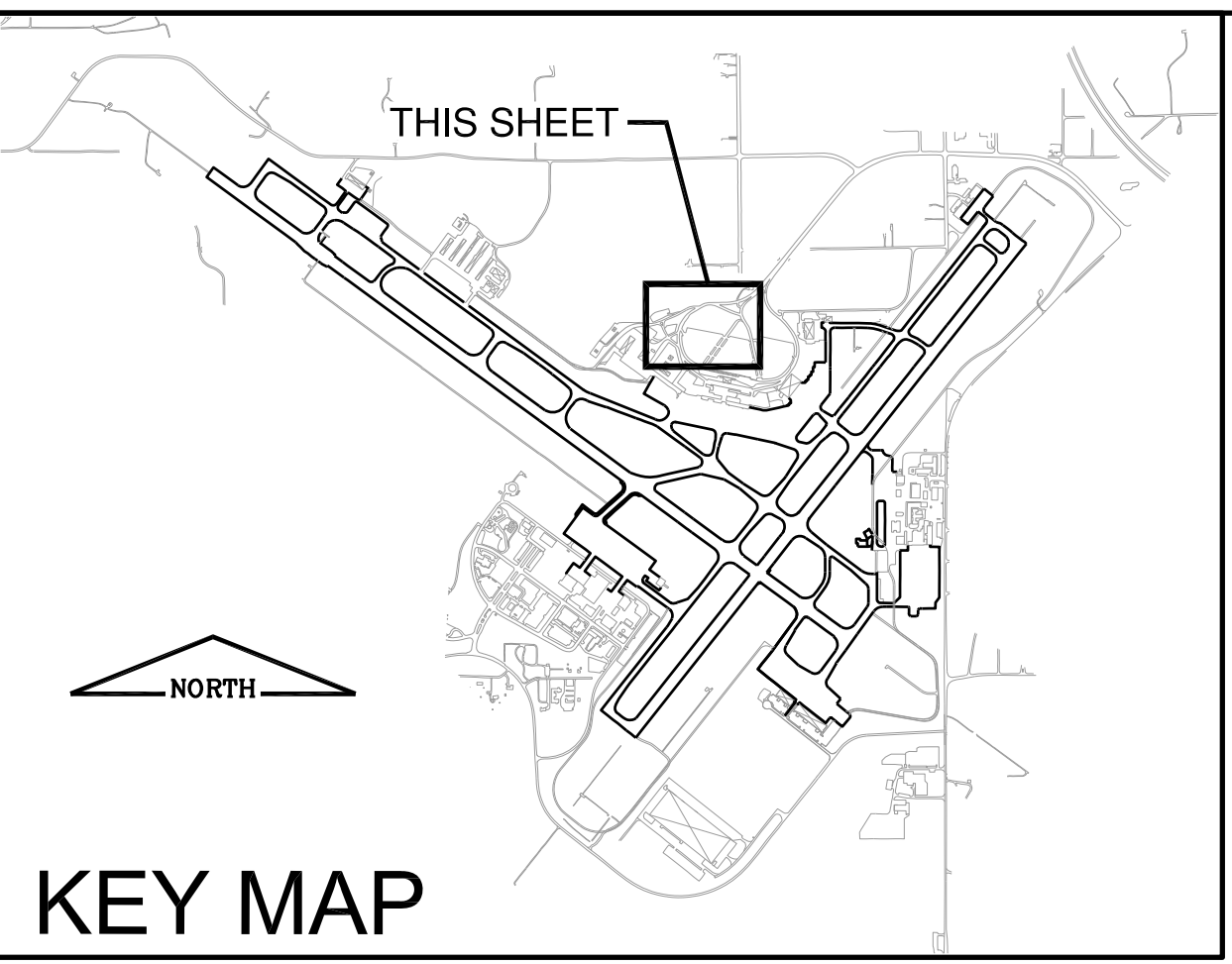
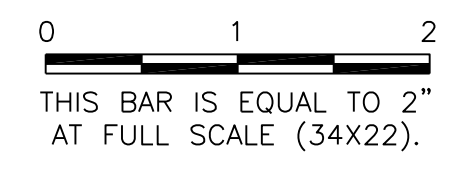
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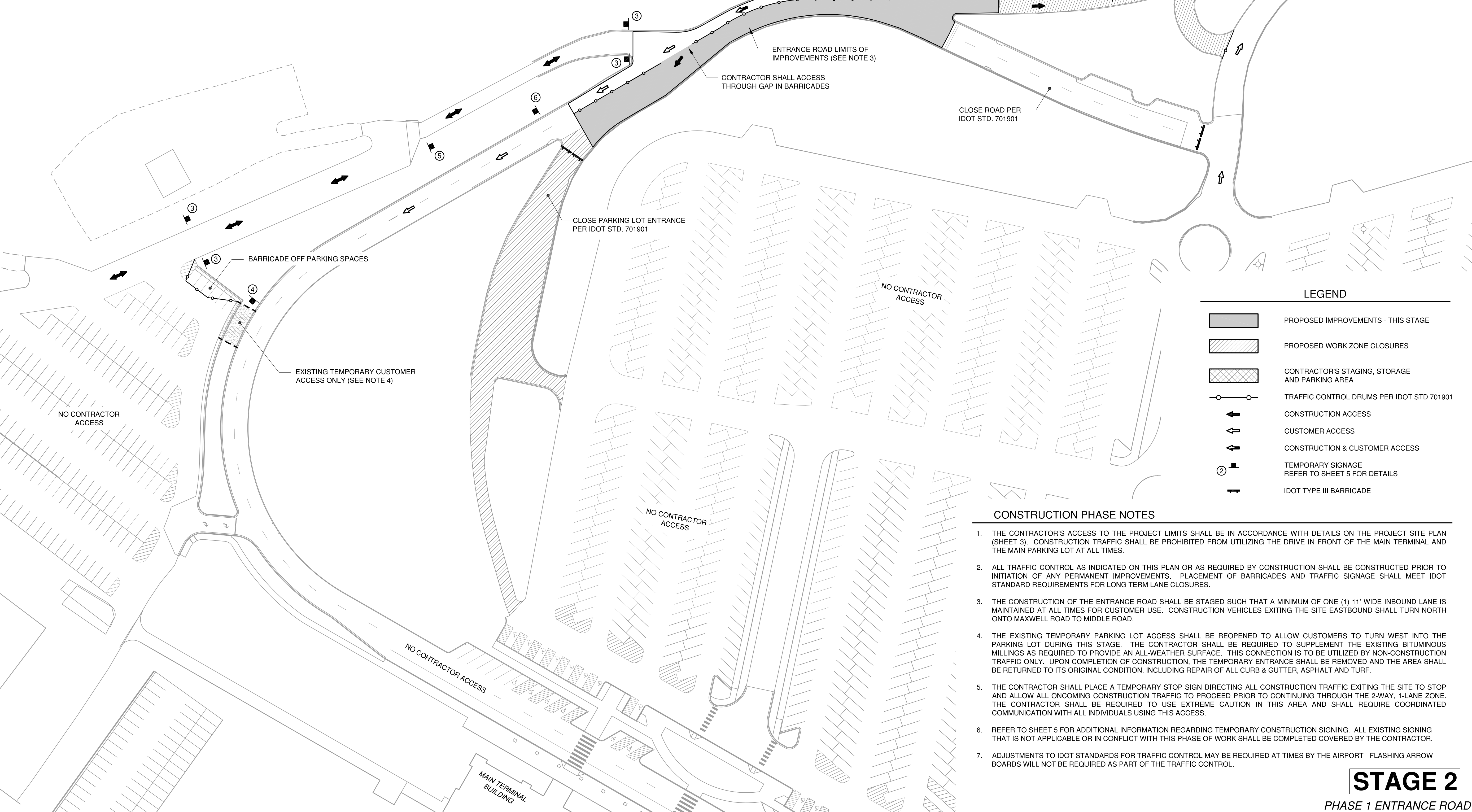
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JOB No:	13061-05
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AIP PROJ. NO. 3-17-0080-XX	
SHEET 07 OF 67 SHEETS	

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KEY MAP



LEGEND

	PROPOSED IMPROVEMENTS - THIS STAGE
	PROPOSED WORK ZONE CLOSURES
	CONTRACTOR'S STAGING, STORAGE AND PARKING AREA
	TRAFFIC CONTROL DRUMS PER IDOT STD 701901
	CONSTRUCTION ACCESS
	CUSTOMER ACCESS
	CONSTRUCTION & CUSTOMER ACCESS
	TEMPORARY SIGNAGE REFER TO SHEET 5 FOR DETAILS
	IDOT TYPE III BARRICADE

CONSTRUCTION PHASE NOTES

1. THE CONTRACTOR'S ACCESS TO THE PROJECT LIMITS SHALL BE IN ACCORDANCE WITH DETAILS ON THE PROJECT SITE PLAN (SHEET 3). CONSTRUCTION TRAFFIC SHALL BE PROHIBITED FROM UTILIZING THE DRIVE IN FRONT OF THE MAIN TERMINAL AND THE MAIN PARKING LOT AT ALL TIMES.
2. ALL TRAFFIC CONTROL AS INDICATED ON THIS PLAN OR AS REQUIRED BY CONSTRUCTION SHALL BE CONSTRUCTED PRIOR TO INITIATION OF ANY PERMANENT IMPROVEMENTS. PLACEMENT OF BARRICADES AND TRAFFIC SIGNAGE SHALL MEET IDOT STANDARD REQUIREMENTS FOR LONG TERM LANE CLOSURES.
3. THE CONSTRUCTION OF THE ENTRANCE ROAD SHALL BE STAGED SUCH THAT A MINIMUM OF ONE (1) 11' WIDE INBOUND LANE IS MAINTAINED AT ALL TIMES FOR CUSTOMER USE. CONSTRUCTION VEHICLES EXITING THE SITE EASTBOUND SHALL TURN NORTH ONTO MAXWELL ROAD TO MIDDLE ROAD.
4. THE EXISTING TEMPORARY PARKING LOT ACCESS SHALL BE REOPENED TO ALLOW CUSTOMERS TO TURN WEST INTO THE PARKING LOT DURING THIS STAGE. THE CONTRACTOR SHALL BE REQUIRED TO SUPPLEMENT THE EXISTING BITUMINOUS MILLINGS AS REQUIRED TO PROVIDE AN ALL-WEATHER SURFACE. THIS CONNECTION IS TO BE UTILIZED BY NON-CONSTRUCTION TRAFFIC ONLY. UPON COMPLETION OF CONSTRUCTION, THE TEMPORARY ENTRANCE SHALL BE REMOVED AND THE AREA SHALL BE RETURNED TO ITS ORIGINAL CONDITION, INCLUDING REPAIR OF ALL CURB & GUTTER, ASPHALT AND TURF.
5. THE CONTRACTOR SHALL PLACE A TEMPORARY STOP SIGN DIRECTING ALL CONSTRUCTION TRAFFIC EXITING THE SITE TO STOP AND ALLOW ALL ONGOING CONSTRUCTION TRAFFIC TO PROCEED PRIOR TO CONTINUING THROUGH THE 2-WAY, 1-LANE ZONE. THE CONTRACTOR SHALL BE REQUIRED TO USE EXTREME CAUTION IN THIS AREA AND SHALL REQUIRE COORDINATED COMMUNICATION WITH ALL INDIVIDUALS USING THIS ACCESS.
6. REFER TO SHEET 5 FOR ADDITIONAL INFORMATION REGARDING TEMPORARY CONSTRUCTION SIGNING. ALL EXISTING SIGNING THAT IS NOT APPLICABLE OR IN CONFLICT WITH THIS PHASE OF WORK SHALL BE COMPLETED COVERED BY THE CONTRACTOR.
7. ADJUSTMENTS TO IDOT STANDARDS FOR TRAFFIC CONTROL MAY BE REQUIRED AT TIMES BY THE AIRPORT - FLASHING ARROW BOARDS WILL NOT BE REQUIRED AS PART OF THE TRAFFIC CONTROL.

STAGE 2

PHASE 1 ENTRANCE ROAD

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
 REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
 CONSTRUCTION ACTIVITY PLAN 3**

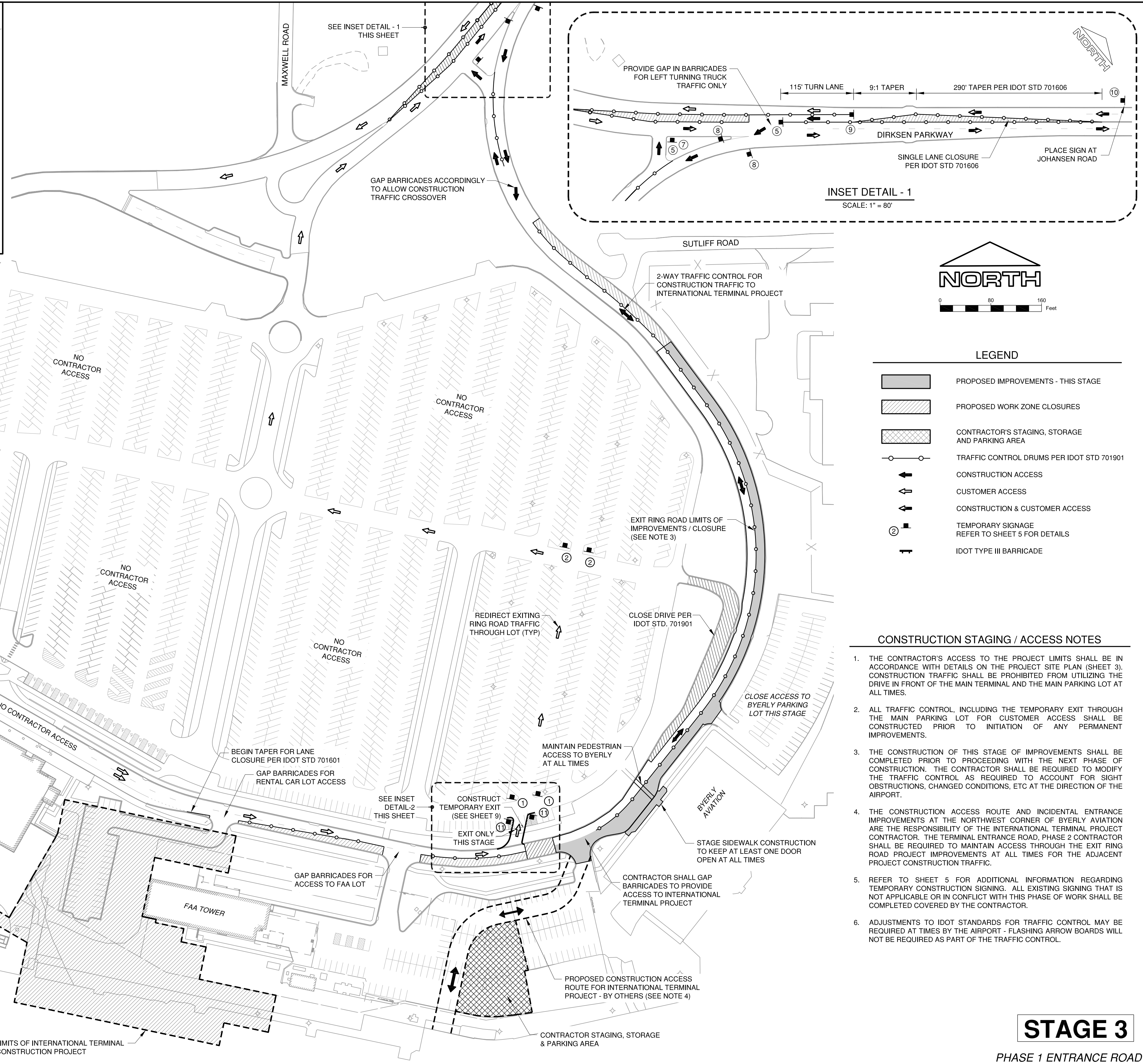
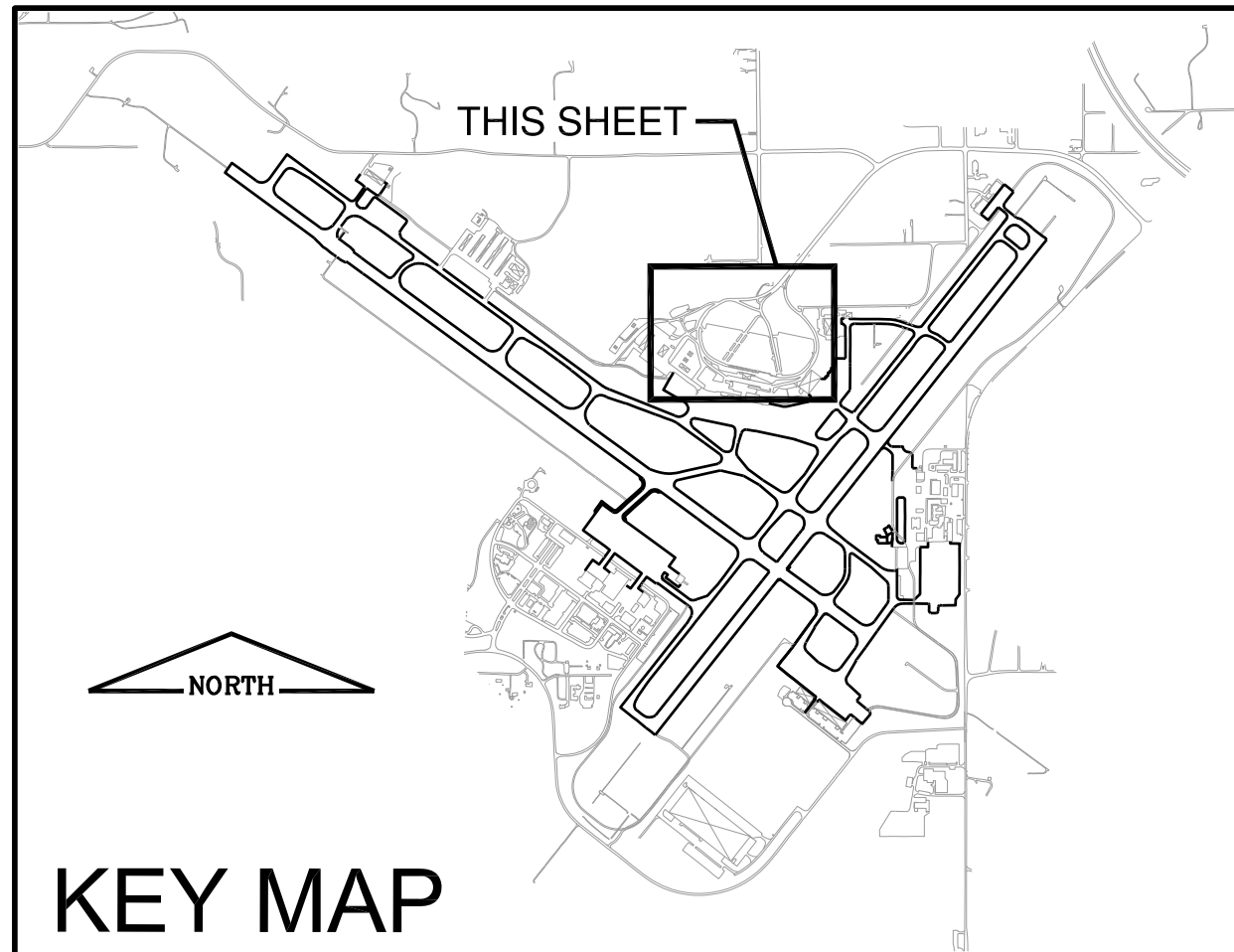
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**GENERAL WAYNE A. DOWNING
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SHEET 08 OF 67 SHEETS	

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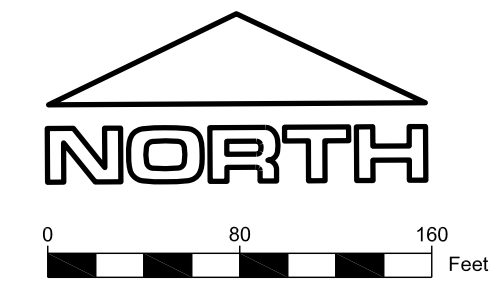
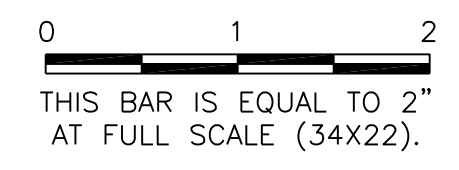


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 PLOT DATE: 7/10/2014 10:41 AM

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LEGEND

	PROPOSED IMPROVEMENTS - THIS STAGE
	PROPOSED WORK ZONE CLOSURES
	CONTRACTOR'S STAGING, STORAGE AND PARKING AREA
	TRAFFIC CONTROL DRUMS PER IDOT STD 701901
	CONSTRUCTION ACCESS
	CUSTOMER ACCESS
	CONSTRUCTION & CUSTOMER ACCESS
	TEMPORARY SIGNAGE REFER TO SHEET 5 FOR DETAILS
	IDOT TYPE III BARRICADE

CONSTRUCTION STAGING / ACCESS NOTES

1. THE CONTRACTOR'S ACCESS TO THE PROJECT LIMITS SHALL BE IN ACCORDANCE WITH DETAILS ON THE PROJECT SITE PLAN (SHEET 3). CONSTRUCTION TRAFFIC SHALL BE PROHIBITED FROM UTILIZING THE DRIVE IN FRONT OF THE MAIN TERMINAL AND THE MAIN PARKING LOT AT ALL TIMES.
2. ALL TRAFFIC CONTROL, INCLUDING THE TEMPORARY EXIT THROUGH THE MAIN PARKING LOT FOR CUSTOMER ACCESS SHALL BE CONSTRUCTED PRIOR TO INITIATION OF ANY PERMANENT IMPROVEMENTS.
3. THE CONSTRUCTION OF THIS STAGE OF IMPROVEMENTS SHALL BE COMPLETED PRIOR TO PROCEEDING WITH THE NEXT PHASE OF CONSTRUCTION. THE CONTRACTOR SHALL BE REQUIRED TO MODIFY THE TRAFFIC CONTROL AS REQUIRED TO ACCOUNT FOR SIGHT OBSTRUCTIONS, CHANGED CONDITIONS, ETC AT THE DIRECTION OF THE AIRPORT.
4. THE CONSTRUCTION ACCESS ROUTE AND INCIDENTAL ENTRANCE IMPROVEMENTS AT THE NORTHWEST CORNER OF BYERLY AVIATION ARE THE RESPONSIBILITY OF THE INTERNATIONAL TERMINAL PROJECT CONTRACTOR. THE TERMINAL ENTRANCE ROAD, PHASE 2 CONTRACTOR SHALL BE REQUIRED TO MAINTAIN ACCESS THROUGH THE EXIT RING ROAD PROJECT IMPROVEMENTS AT ALL TIMES FOR THE ADJACENT PROJECT CONSTRUCTION TRAFFIC.
5. REFER TO SHEET 5 FOR ADDITIONAL INFORMATION REGARDING TEMPORARY CONSTRUCTION SIGNING. ALL EXISTING SIGNING THAT IS NOT APPLICABLE OR IN CONFLICT WITH THIS PHASE OF WORK SHALL BE COMPLETED COVERED BY THE CONTRACTOR.
6. ADJUSTMENTS TO IDOT STANDARDS FOR TRAFFIC CONTROL MAY BE REQUIRED AT TIMES BY THE AIRPORT - FLASHING ARROW BOARDS WILL NOT BE REQUIRED AS PART OF THE TRAFFIC CONTROL.

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 PEORIA, ILLINOIS

DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2

CONSTRUCTION ACTIVITY PLAN 4

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JOB No:	13061-05

IL PROJ. NO. PIA-4415
 AIP PROJ. NO. 3-17-0080-XX
 SHEET 09 OF 67 SHEETS

STAGE 3

PHASE 1 ENTRANCE ROAD

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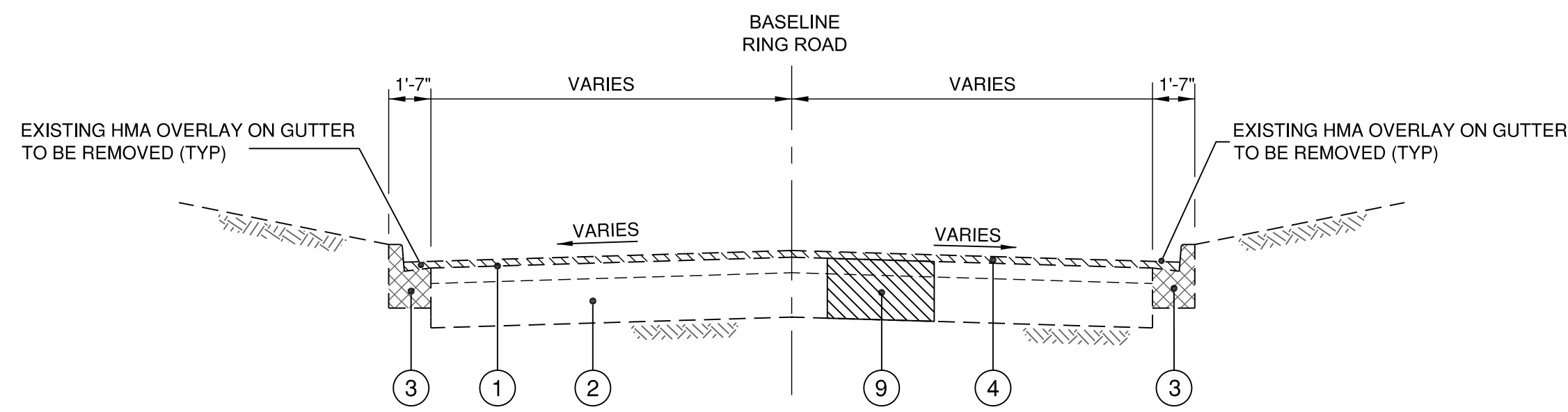
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TYPICAL SECTIONS

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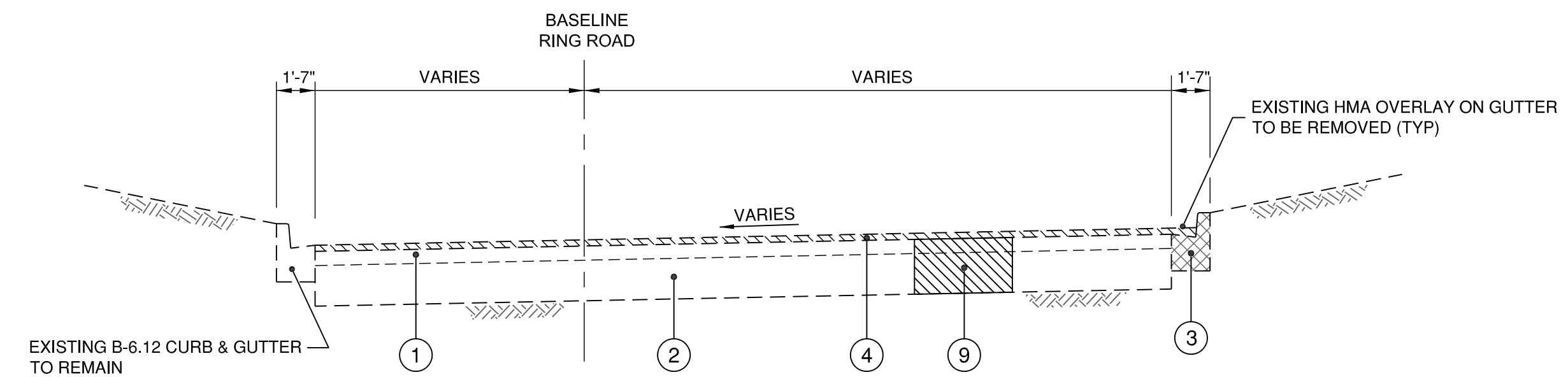
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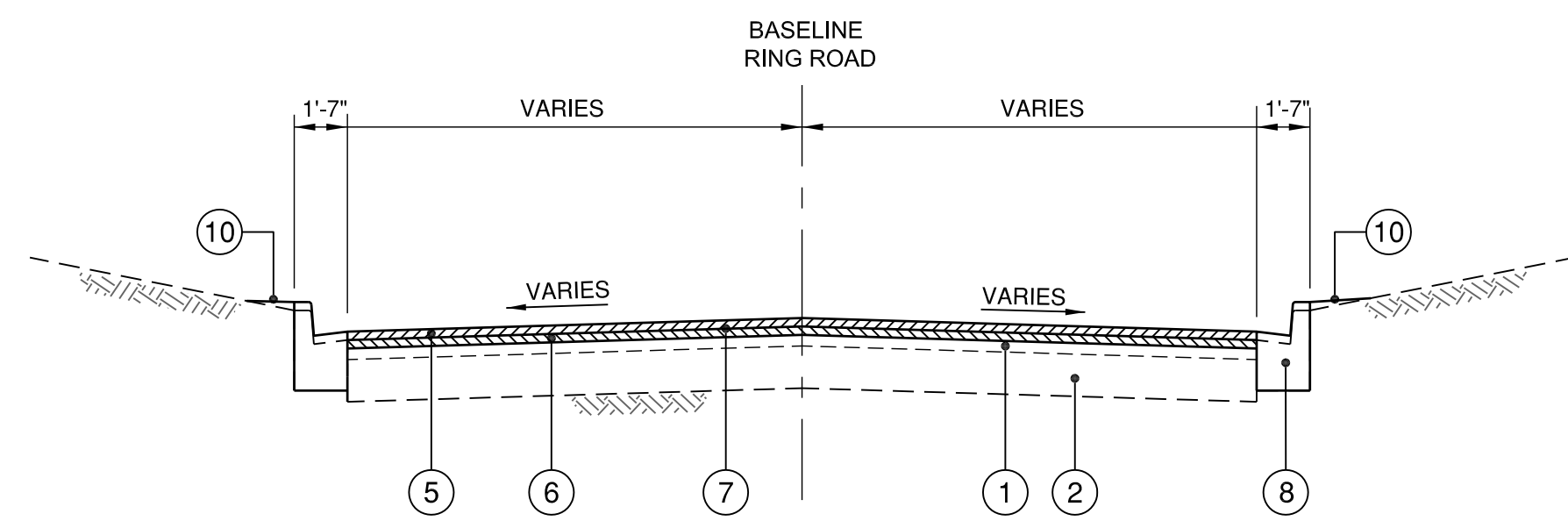
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AIP PROJ. NO.	3-17-0080-XX



EXISTING SECTION & REMOVALS



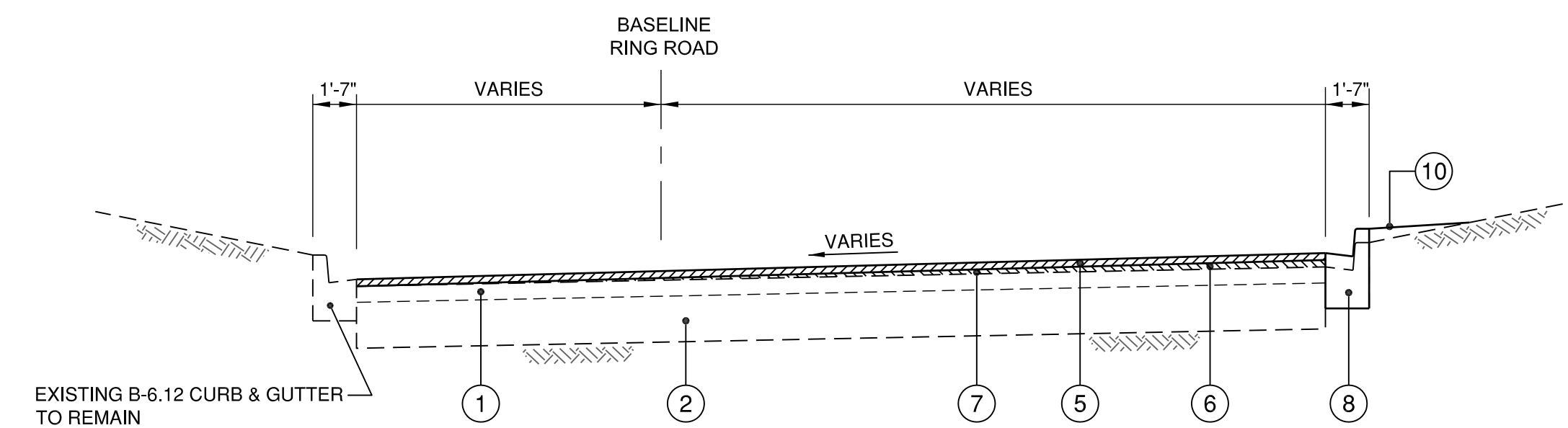
EXISTING SECTION & REMOVALS



PROPOSED IMPROVEMENTS

TYPICAL SECTION A-A : RING ROAD

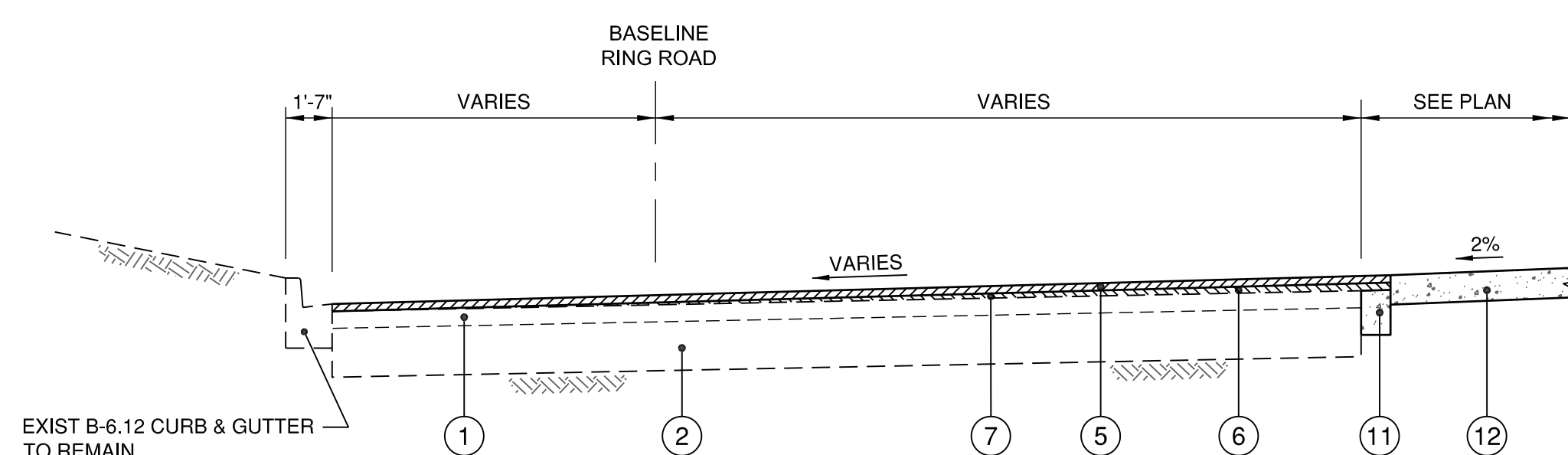
SCALE 1:2 (H:V)



PROPOSED IMPROVEMENTS

TYPICAL SECTION B-B : RING ROAD

SCALE 1:2 (H:V)



PROPOSED IMPROVEMENTS
 (EXISTING CONDITIONS SIMILAR TO SECTION B-B)

TYPICAL SECTION C-C : RING ROAD

SCALE 1:2 (H:V)

GENERAL NOTES:

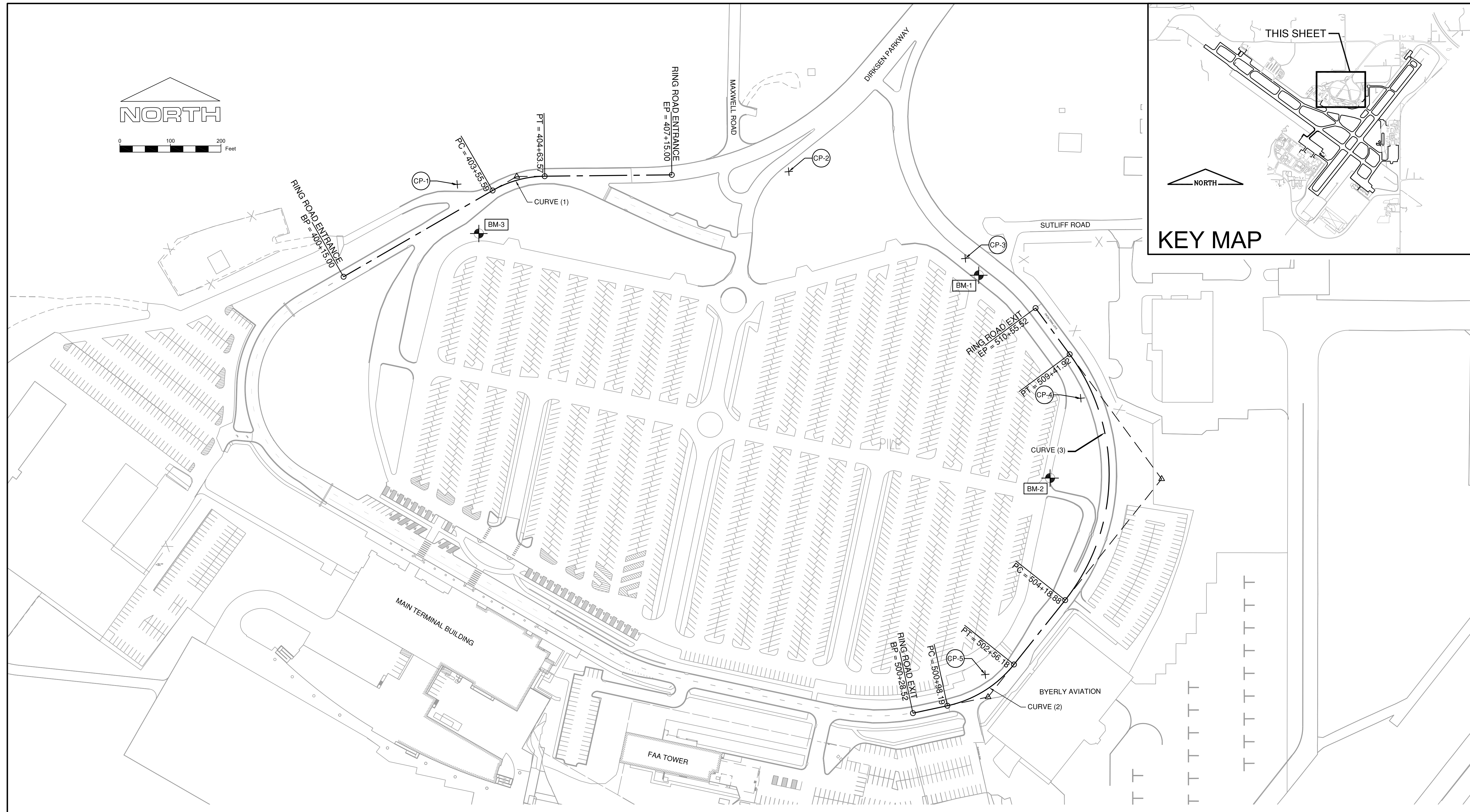
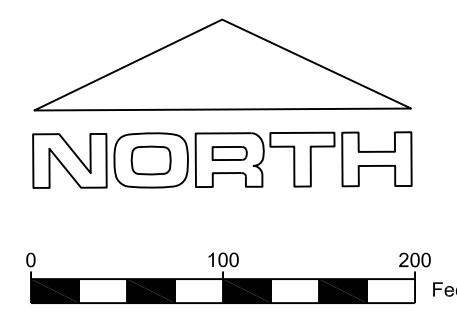
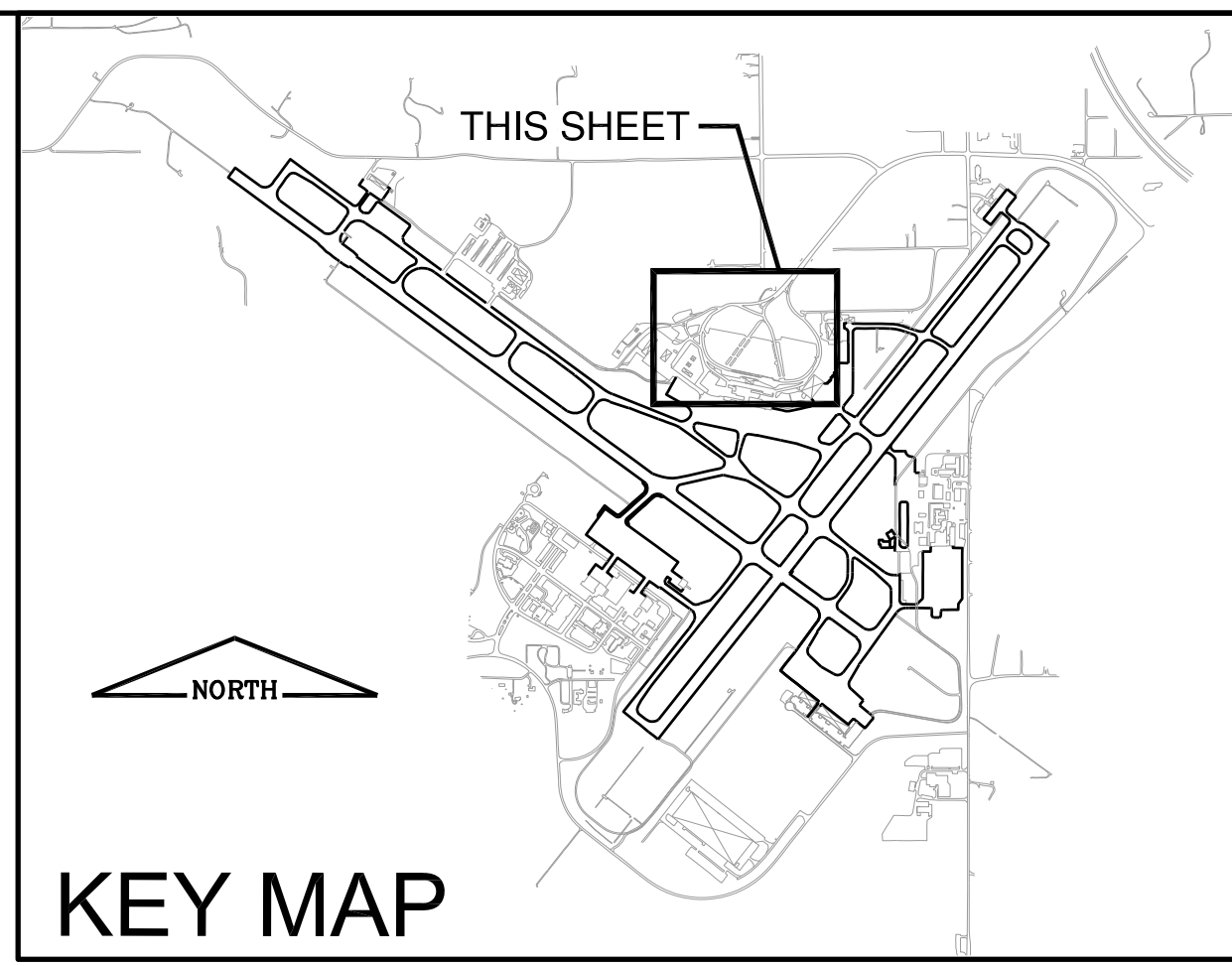
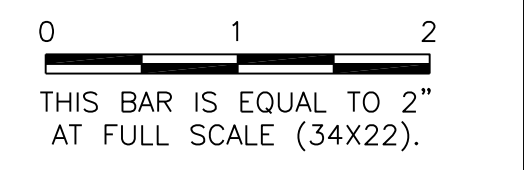
- ALL MATERIALS SHALL MEET OR EXCEED THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS, LATEST EDITION.
- CONTRACTOR SHALL PROVIDE A BITUMINOUS TACK COAT TO VERTICAL FACE OF GUTTER FLAG WHERE IT CONTACTS ASPHALT PAVING. APPLY BITUMINOUS TACK COAT AT 0.2 GAL/SY ON MILLED SURFACE AND 0.1 GAL/SY ON GUTTER AND BETWEEN ASPHALT LIFTS.
- TYPES AND LOCATIONS OF PAVEMENT JOINTS SHALL CONFORM TO PROJECT SPECIFICATIONS AND CONSTRUCTION DETAILS.
- PAVEMENT PATCHING WILL BE REQUIRED IN AREAS DESIGNATED BY THE RESIDENT ENGINEER. POTENTIAL SUBGRADE REPAIR MAY BE REQUIRED IN PATCHING AREAS. SEE DETAIL A4/SHEET 19.
- ALL PROPOSED CURB & GUTTER SHALL SMOOTHLY TRANSITION FROM EXISTING CURB & GUTTER GRADES. SEE PAVEMENT STAKING PLANS.
- REFER TO PAVEMENT STAKING PLANS FOR REQUIRED GRADES.

KEYNOTES:

- EXISTING HMA SURFACE/BINDER (VARIES 4.5" TO 5.75")
- EXISTING AGGREGATE BASE COURSE (VARIES 9.5" TO 17.5")
- EXISTING COMB. CURB & GUTTER (APPARENT TYPE B-6.12) TO BE REMOVED
- BITUMINOUS PAVEMENT MILLING (1.5")
- PROPOSED HMA SURFACE COURSE, MIX 'D' N70, 1.5"
- BITUMINOUS BASE COURSE, LEVELING
- BITUMINOUS TACK COAT (MILLED SURFACE AND BETWEEN HMA LAYERS)
- PROPOSED COMB. CONCRETE CURB & GUTTER, TYPE B-6.12 (TYP) (GUTTER DEPTH SHALL MATCH ELEVATION OF EXISTING GUTTER BOTTOM)
- PAVEMENT PATCH - SEE NOTE 4
- TOPSOIL WEDGE WITH SEEDING & MULCH
- CONCRETE FILLER WEDGE - PAID FOR AS COMB. CURB & GUTTER
- 6" PCC SIDEWALK - SEE PLAN

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CONTROL POINTS
DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2

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 APPROVED BY: CET
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 JOB No: 13061-05
 IL PROJ. NO. PIA-4415
 AIP PROJ. NO. 3-17-0080-XX
 SHEET 12 OF 67 SHEETS

HORIZONTAL CONTROL

POINT	OBJECT	NORTHING	EASTING
CP-1	IRON PIN	1457927.647	2428355.391
CP-2	IRON PIN	1457952.852	2429010.966
CP-3	IRON PIN	1457781.181	2429360.147
CP-4	IRON PIN	1457504.504	2429588.460
CP-5	IRON PIN	1456957.732	2492399.604

VERTICAL CONTROL

POINT	DESCRIPTION	ELEVATION
BM-1	HYDRANT-BOLT IN MUELLER, NE OF BUS SHELTER H-2 & ACROSS FROM SUTLIFF RD, DEADEND	645.05
BM-2	HYDRANT-BOLT IN MUELLER, NE QUAD OF 3 WAY STOP/ENTRANCE TO LONG TERM PARKING.	650.50
BM-3	HYDRANT-BOLT IN MUELLER @ N. END OF "E-1" PARKING & BETWEEN LP'S 6-30 & 6-31	636.08

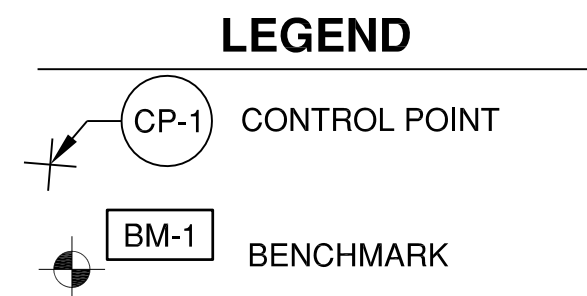
CURVE DATA (1)
 PI STA 404+09.58
 $\Delta = 29^\circ 27' 38''$
 $D = 27^\circ 17' 01''$
 $T = 55.21'$
 $R = 210.00'$
 $L = 107.979'$
 $E = 7.14'$
 PC STA 403+55.59
 PT STA 404+63.57

CURVE DATA (2)
 PI STA 501+77.19
 $\Delta = 39^\circ 21' 22''$
 $D = 24^\circ 54' 40''$
 $T = 82.25'$
 $R = 230.00'$
 $L = 157.985'$
 $E = 14.27'$
 PC STA 500+98.19
 PT STA 502+56.18

CURVE DATA (3)
 PI STA 506+80.40
 $\Delta = 74^\circ 55' 10''$
 $D = 14^\circ 19' 26''$
 $T = 306.48'$
 $R = 400.00'$
 $L = 523.036'$
 $E = 103.92'$
 PC STA 504+18.88
 PT STA 509+41.92

ALIGNMENT CONTROL

ALIGNMENT	POINT	NORTHING	EASTING
RING ROAD ENTRANCE	BP	1457744.914	2428130.548
	PC	1457915.551	2428425.307
	PI	1457943.212	2428473.089
	PT	1457943.796	2428528.297
	EP	1457946.456	2428779.717
RING ROAD EXIT	BP	1456881.378	2429256.638
	PC	1456896.024	2429324.752
	PI	1456913.315	2429405.167
	PT	1456977.678	2429456.380
	EP	1457104.993	2429557.683
	PI	1457344.820	2429748.512
	PT	1457591.474	2429566.593
	EP	1457682.901	2429499.162



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**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
 REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2**

EXISTING CONDITIONS AND REMOVALS


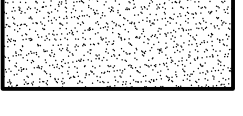

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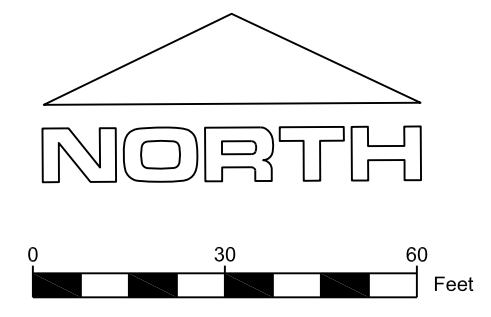
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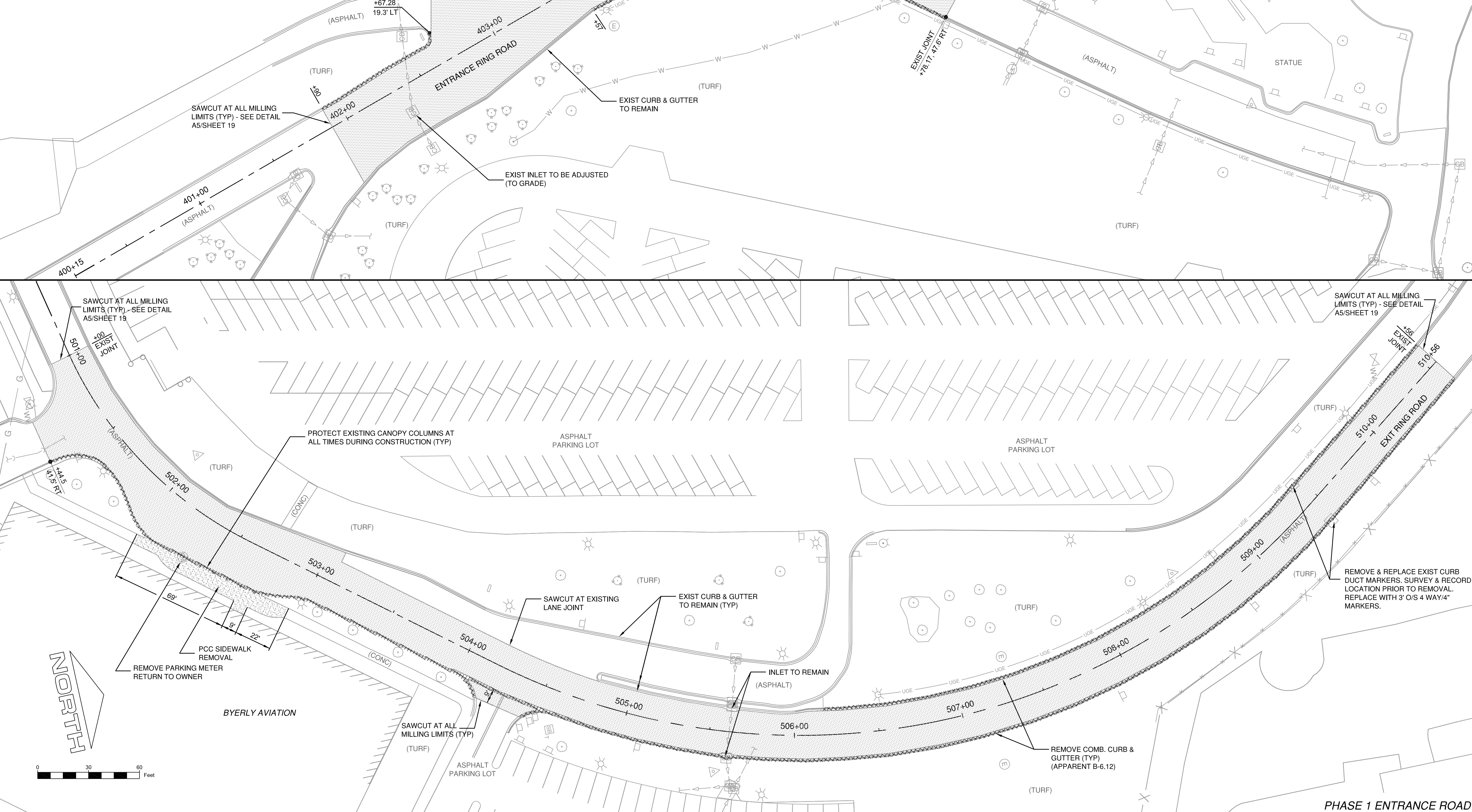
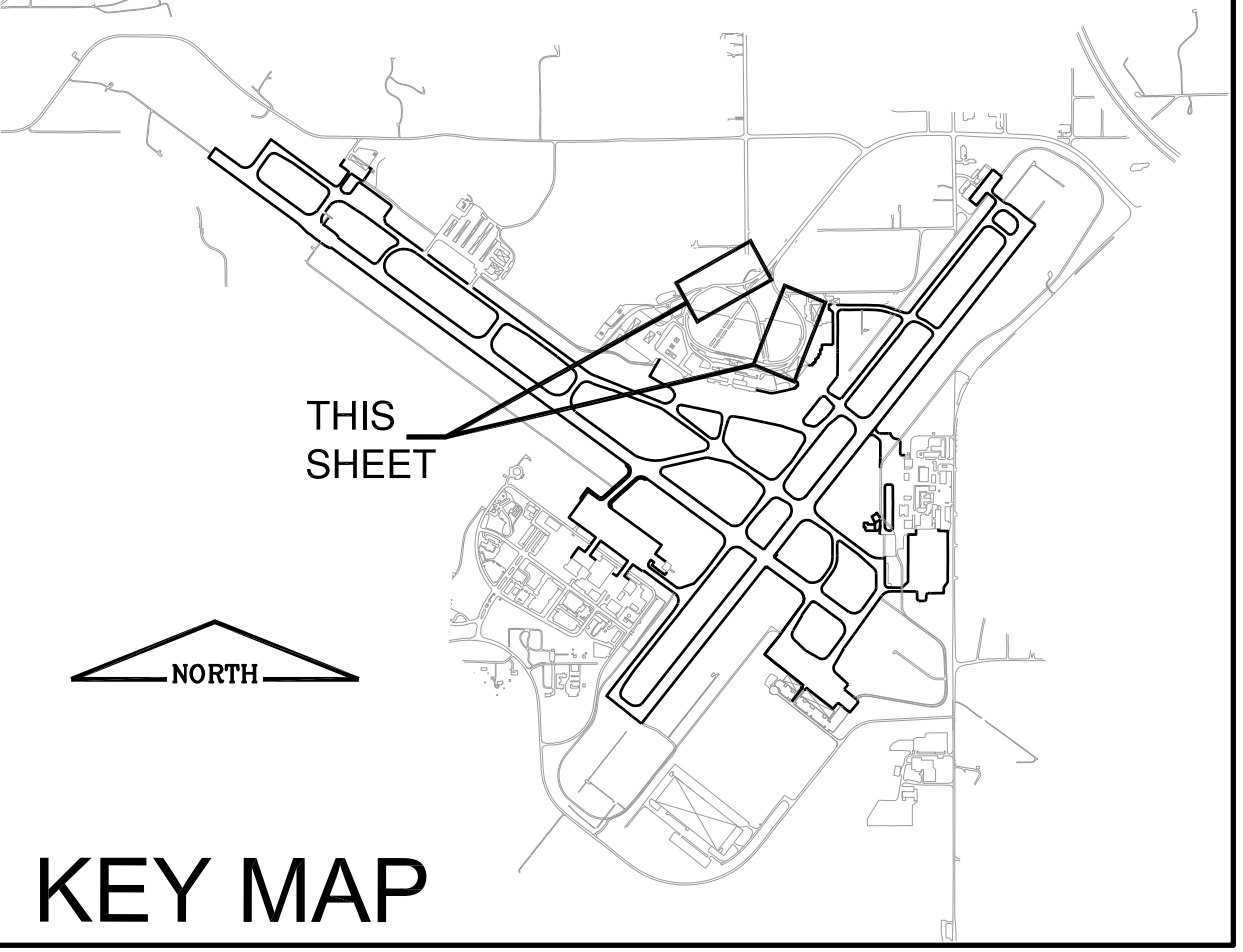
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SHEET 13 OF 67 SHEETS	

REMOVAL LEGEND

-  LIMITS OF BITUMINOUS PAVEMENT MILLING (1.5")
-  PCC SIDEWALK REMOVAL
-  REMOVE COMB. CURB & GUTTER



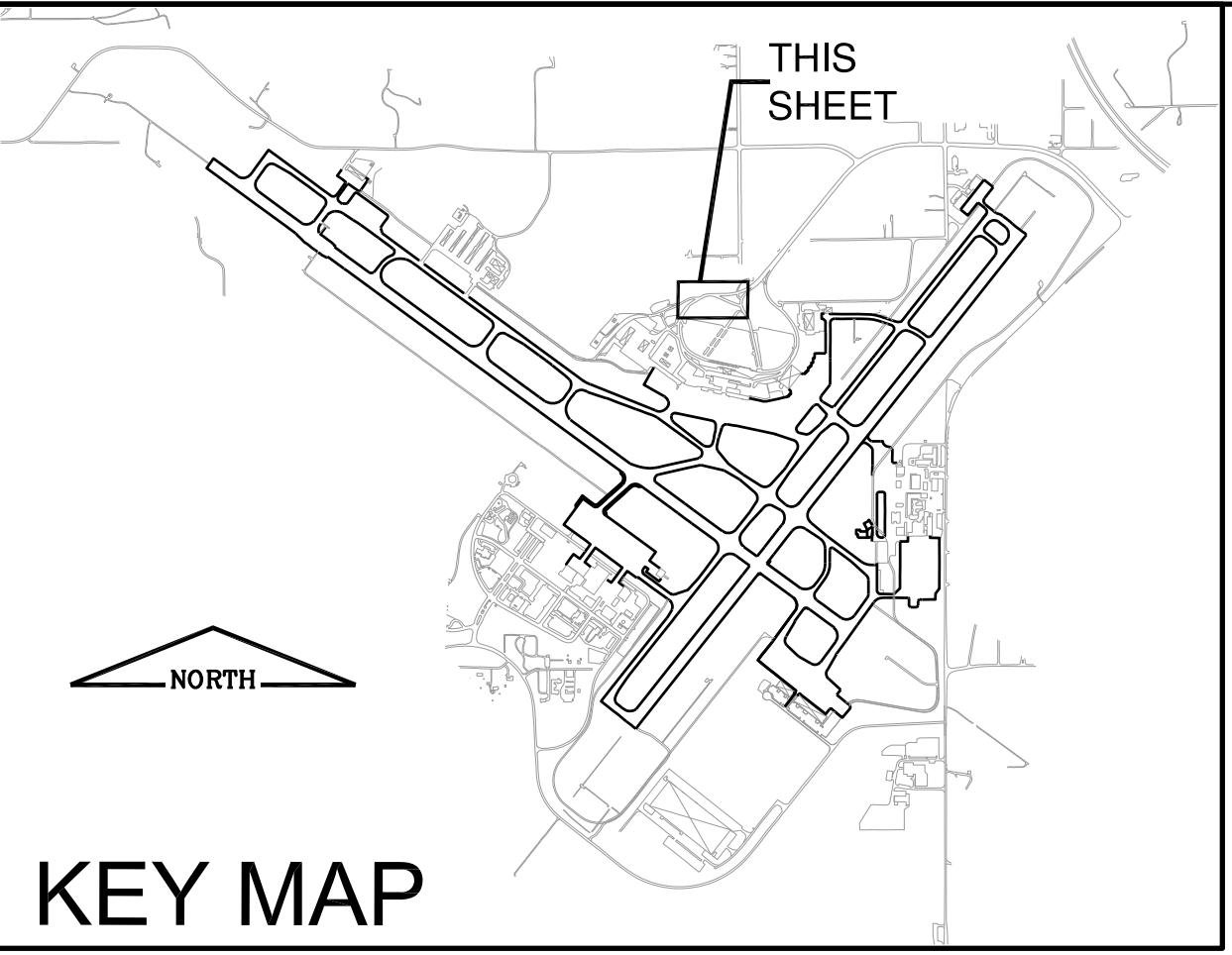
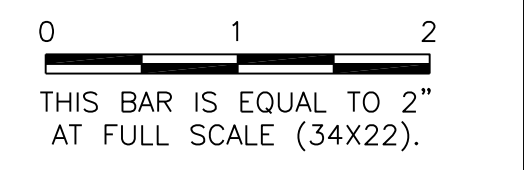
KEY MAP



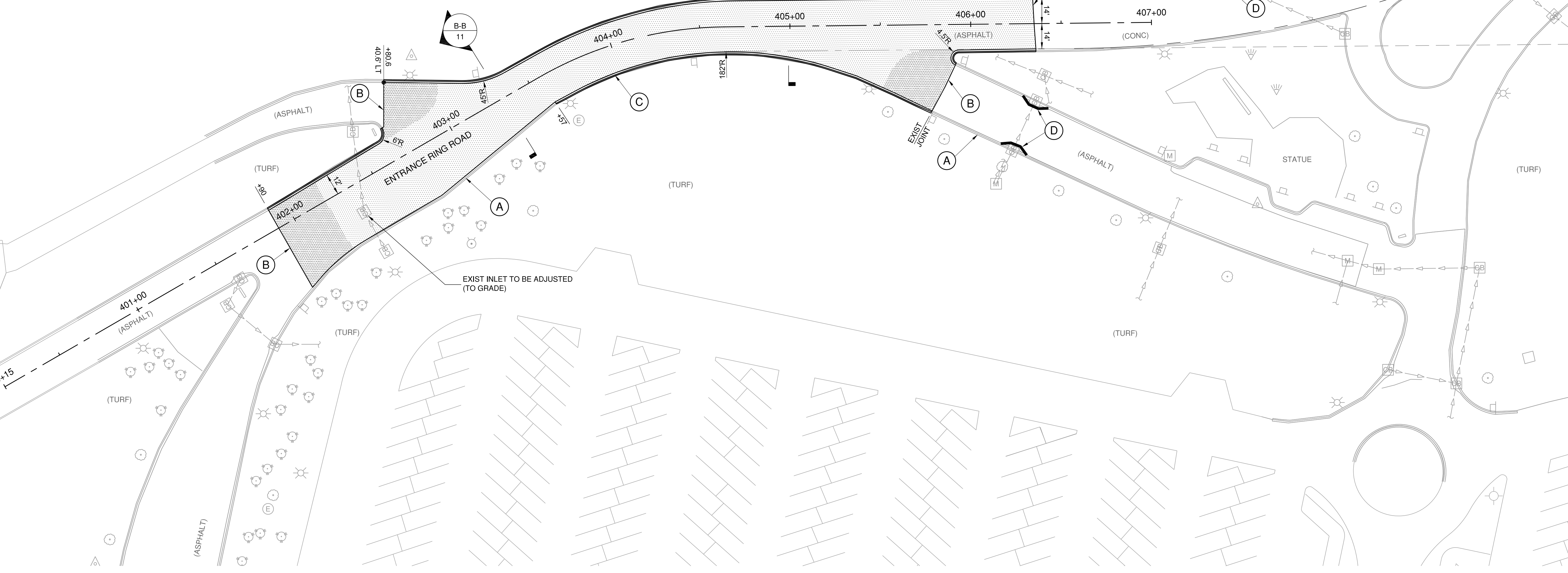
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KEY MAP



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**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
 REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2**

PROPOSED IMPROVEMENT PLAN 1

LEGEND

	LIMITS OF BITUMINOUS PAVEMENT MILLING & OVERLAY
--	---

- KEYNOTES:**
- (A) EXISTING CURB & GUTTER TO REMAIN, TYPE B-6.12 (APPARENT)
 - (B) LIMITS OF BITUMINOUS SURFACE REMOVAL & OVERLAY: SAWCUT FULL DEPTH OF OVERLAY FOR CLEAN EDGE, TEMP RAMP REQUIRED AT PHASE 2 LIMITS - SEE BUTT JOINT DETAIL A5/SHEET 19.
 - (C) PROPOSED COMB. CONCRETE CURB & GUTTER, TYPE B-6.12
 - (D) INSTALL AND MAINTAIN INLET PROTECTION DURING CONSTRUCTION SEE DETAIL A3/SHEET 19

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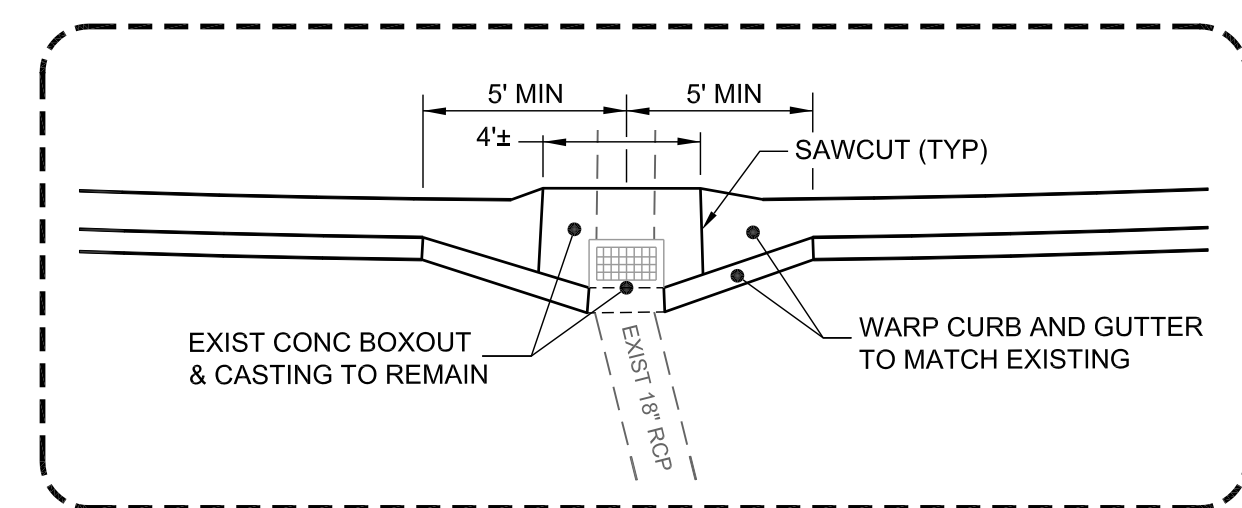
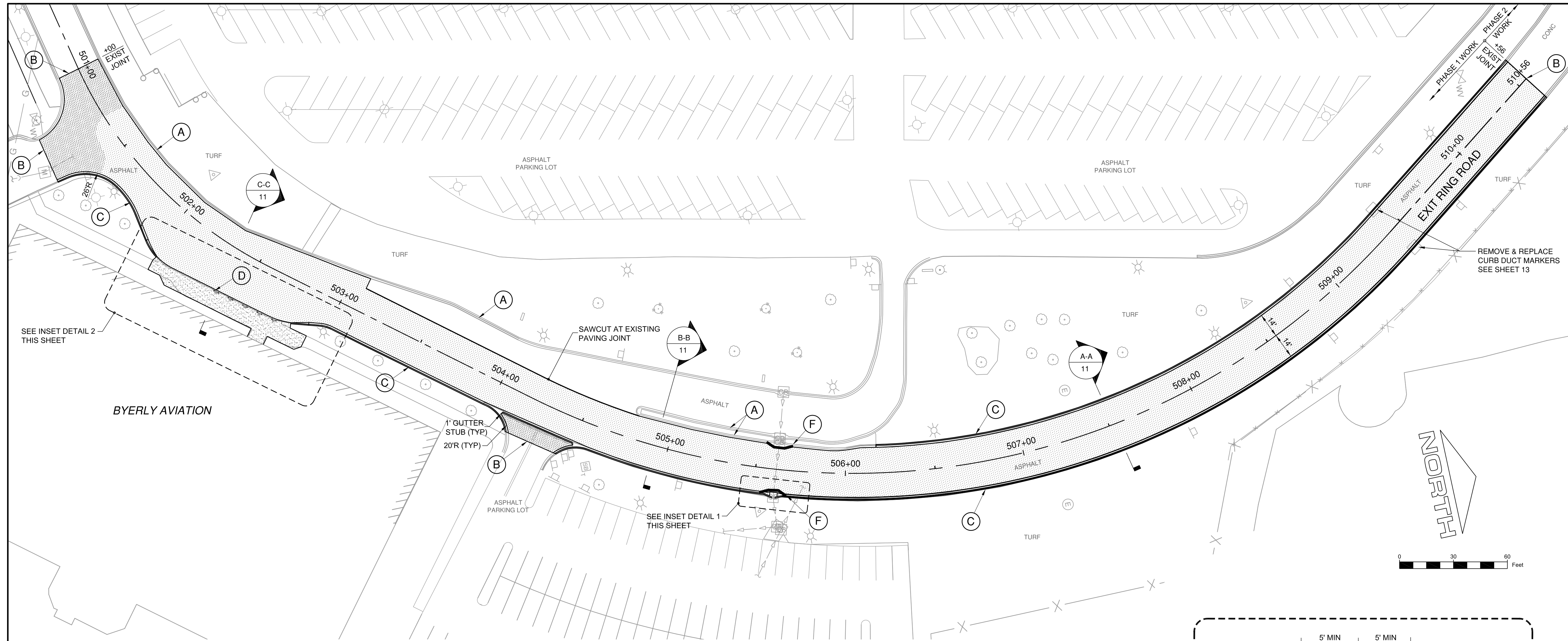
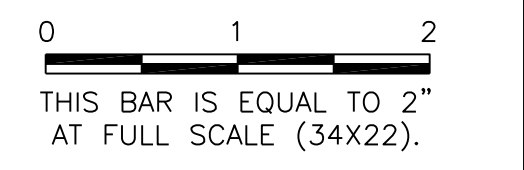
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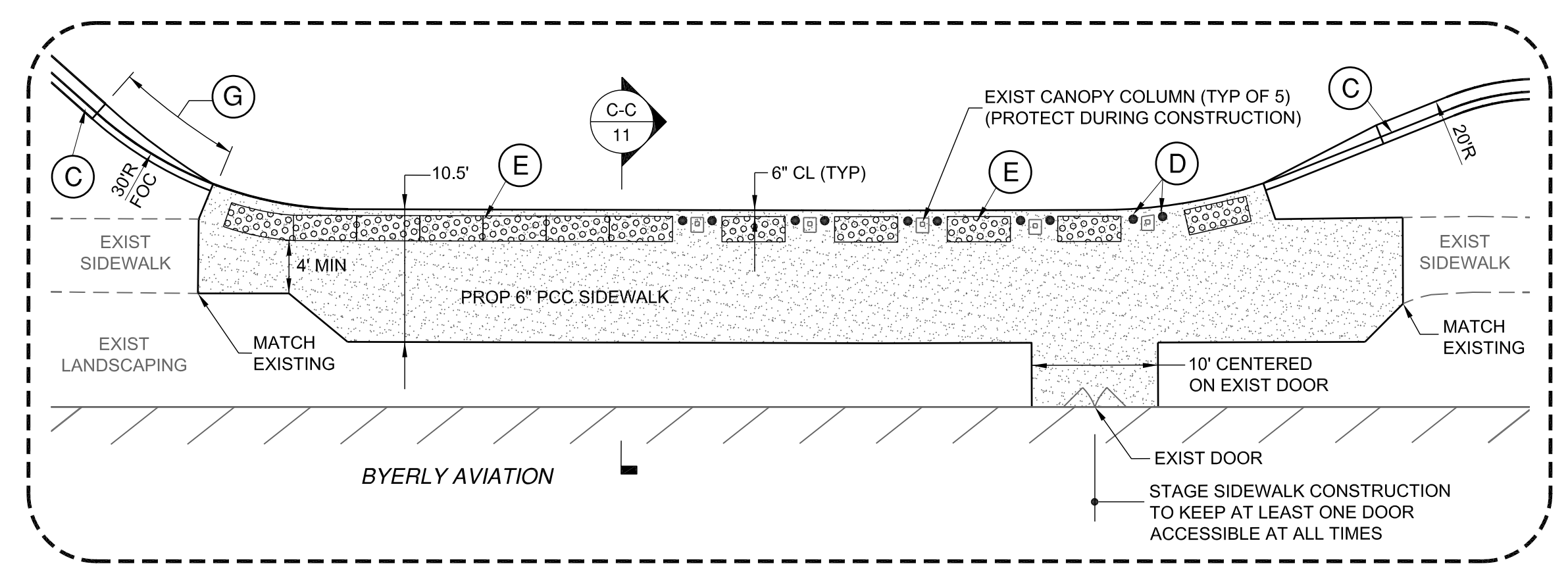
INSET DETAIL 1
SCALE: 1" = 5'

SITE PLAN KEYNOTES:

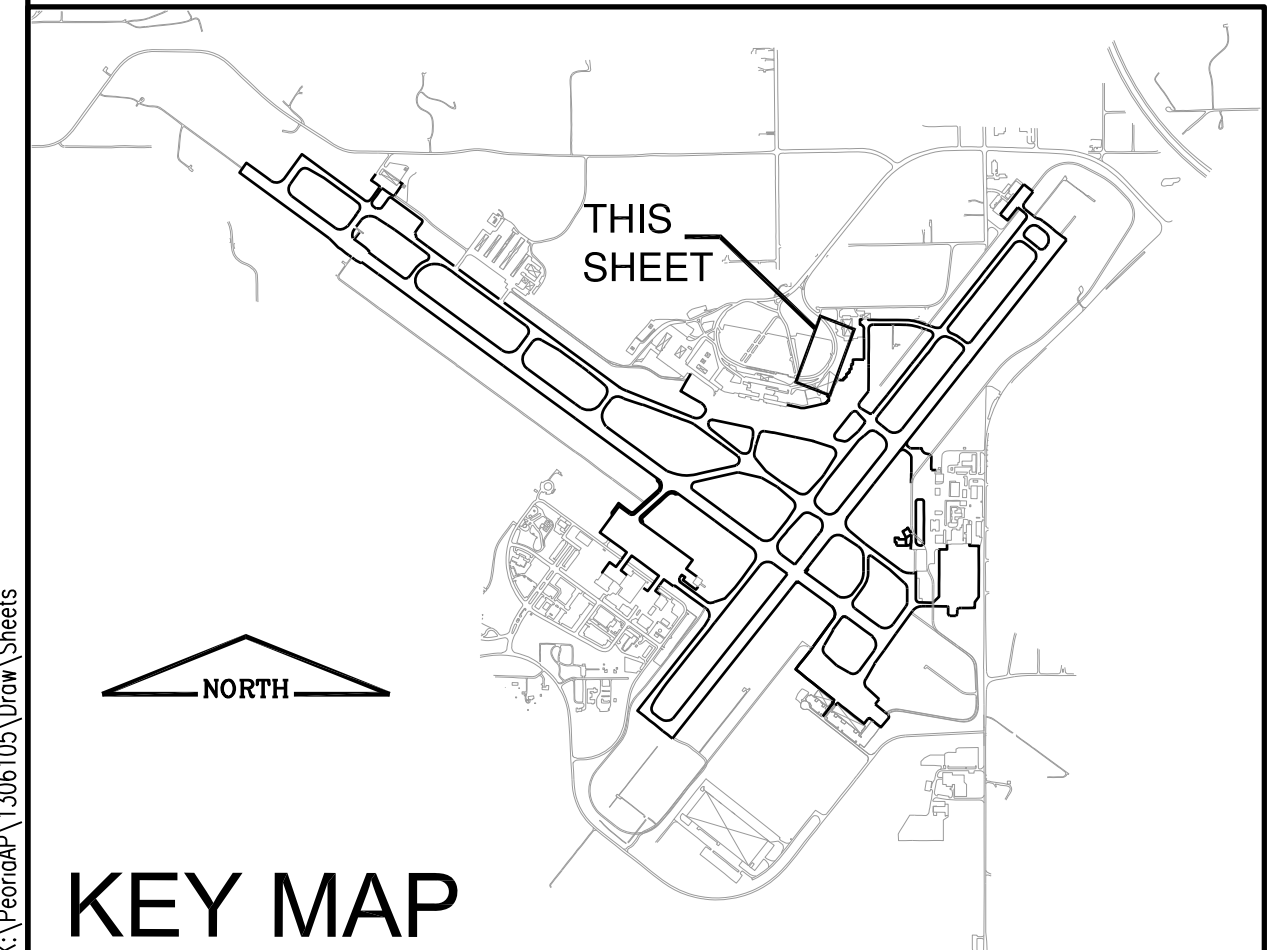
- (A) EXISTING CURB & GUTTER TO REMAIN, APPARENT TYPE B-6.12
- (B) LIMITS OF BITUMINOUS SURFACE REMOVAL & OVERLAY: SAWCUT FULL DEPTH OF OVERLAY FOR CLEAN EDGE, TEMP RAMP REQUIRED AT PHASE 2 LIMITS - SEE BUTT JOINT DETAIL A5/C-19
- (C) PROPOSED COMB. CONCRETE CURB & GUTTER, TYPE B-6.12
- (D) 6" PIPE BOLLARD - TYPICAL OF (2) AT EACH CANOPY COLUMN PROTECT EXISTING CANOPY COLUMN AND FOOTING DURING CONSTRUCTION - SEE DETAIL A2/SHEET 19
- (E) CAST-IN-PLACE ADA DETECTABLE WARNING AREA PER ADA SOLUTIONS, INC. (OR APPROVED EQUAL) BRICK RED COLOR. INSTALL PER MANUFACTURER'S RECOMMENDATIONS.
- (F) INSTALL AND MAINTAIN INLET PROTECTION DURING CONSTRUCTION SEE DETAIL A3/SHEET 19
- (G) 10' CURB & GUTTER TRANSITION - TYPICAL EACH SIDE. TRANSITION CURB FROM 6" TO 0" AND TRANSITION GUTTER FROM 12" TO 0".

SITE PLAN LEGEND

- LIMITS OF BITUMINOUS PAVEMENT MILLING & OVERLAY
- PCC SIDEWALK SEE DETAIL A1/SHEET 19



INSET DETAIL 2
SCALE: 1" = 10'



KEY MAP

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PEORIA, ILLINOIS
DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
PROPOSED IMPROVEMENT PLAN 2

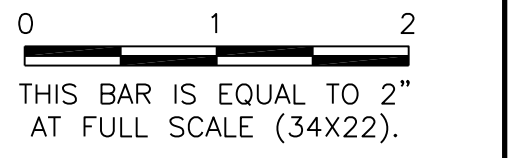
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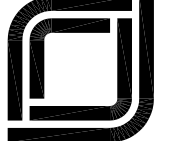
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**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
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 PAVEMENT STAKING PLAN 1**

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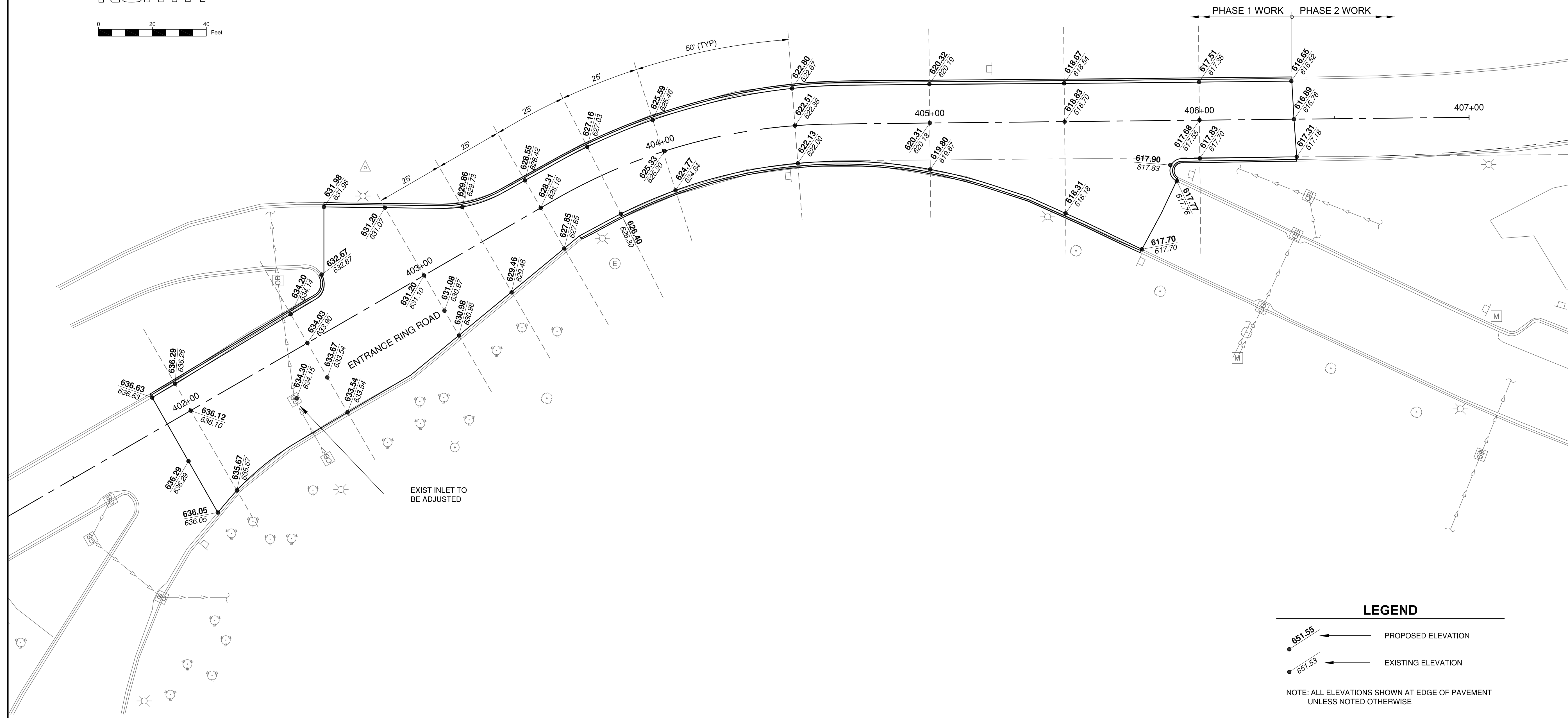
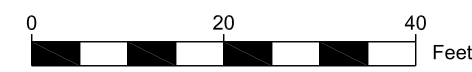


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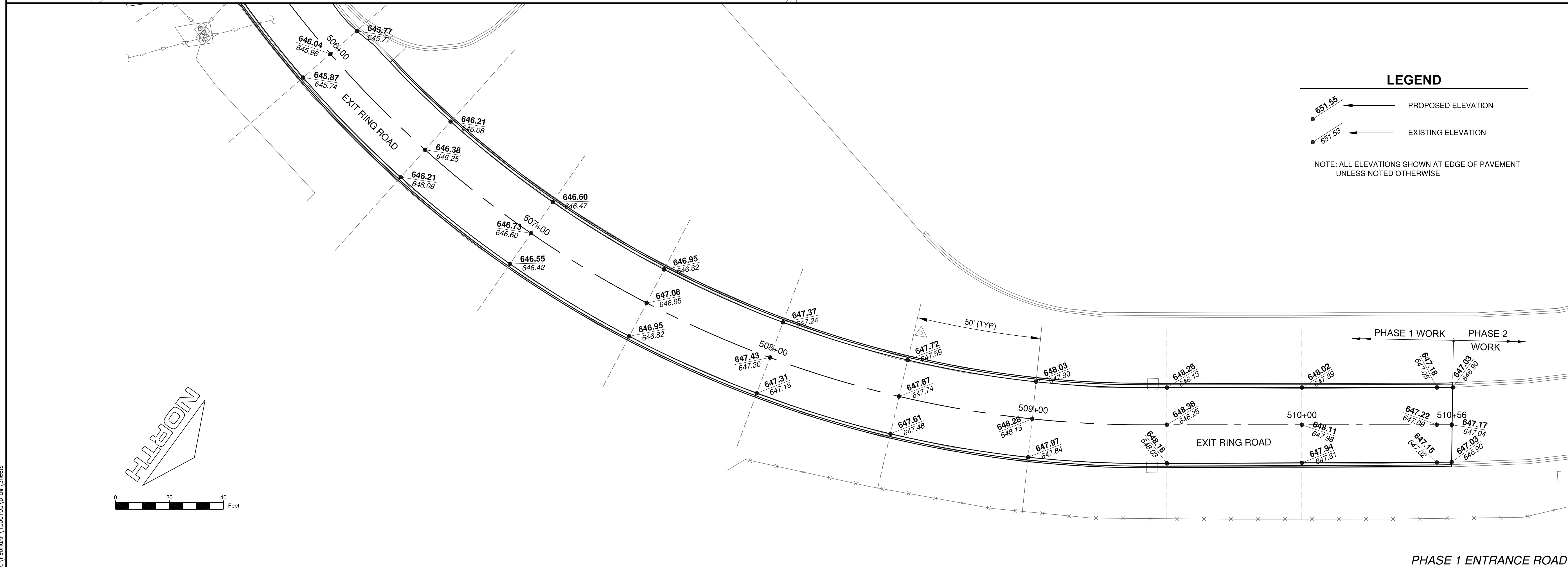
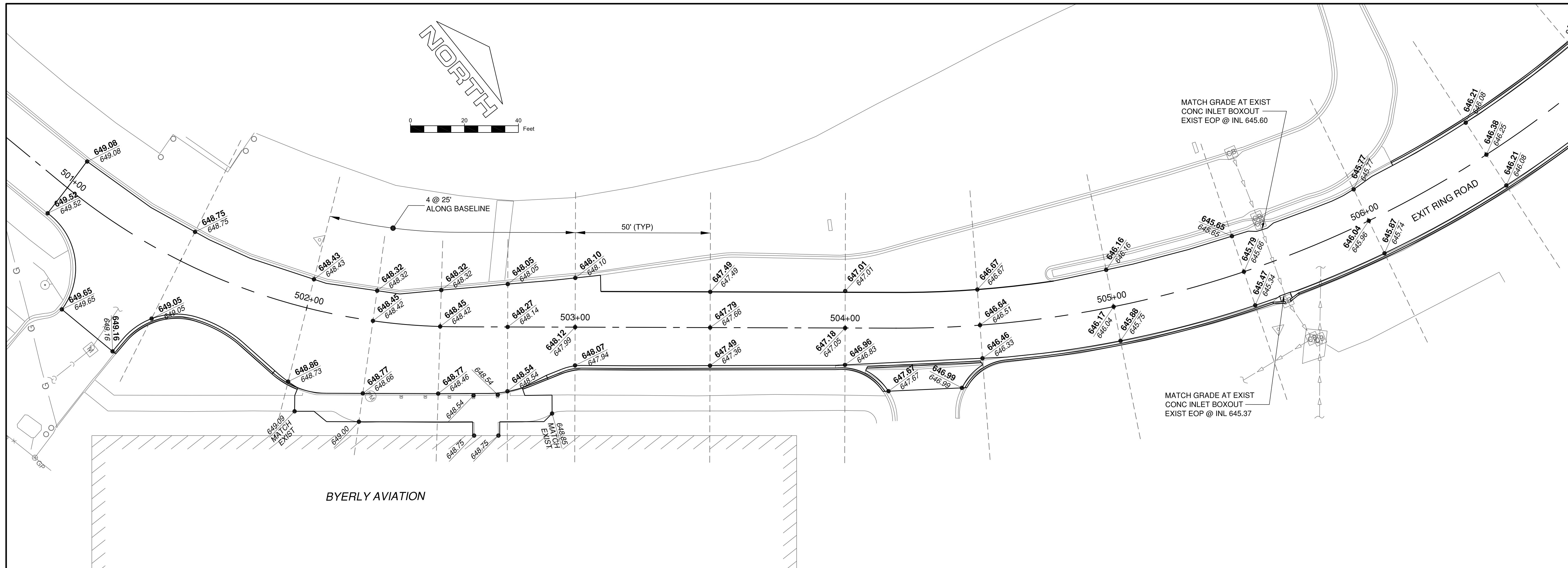
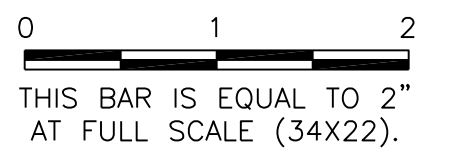
LEGEND

- 651.55 ← PROPOSED ELEVATION
- 651.53 ← EXISTING ELEVATION

NOTE: ALL ELEVATIONS SHOWN AT EDGE OF PAVEMENT UNLESS NOTED OTHERWISE

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LEGEND

PROPOSED ELEVATION
 EXISTING ELEVATION

NOTE: ALL ELEVATIONS SHOWN AT EDGE OF PAVEMENT UNLESS NOTED OTHERWISE

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DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2

PAVEMENT STAKING PLAN 2

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**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
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PAVEMENT MARKING PLAN

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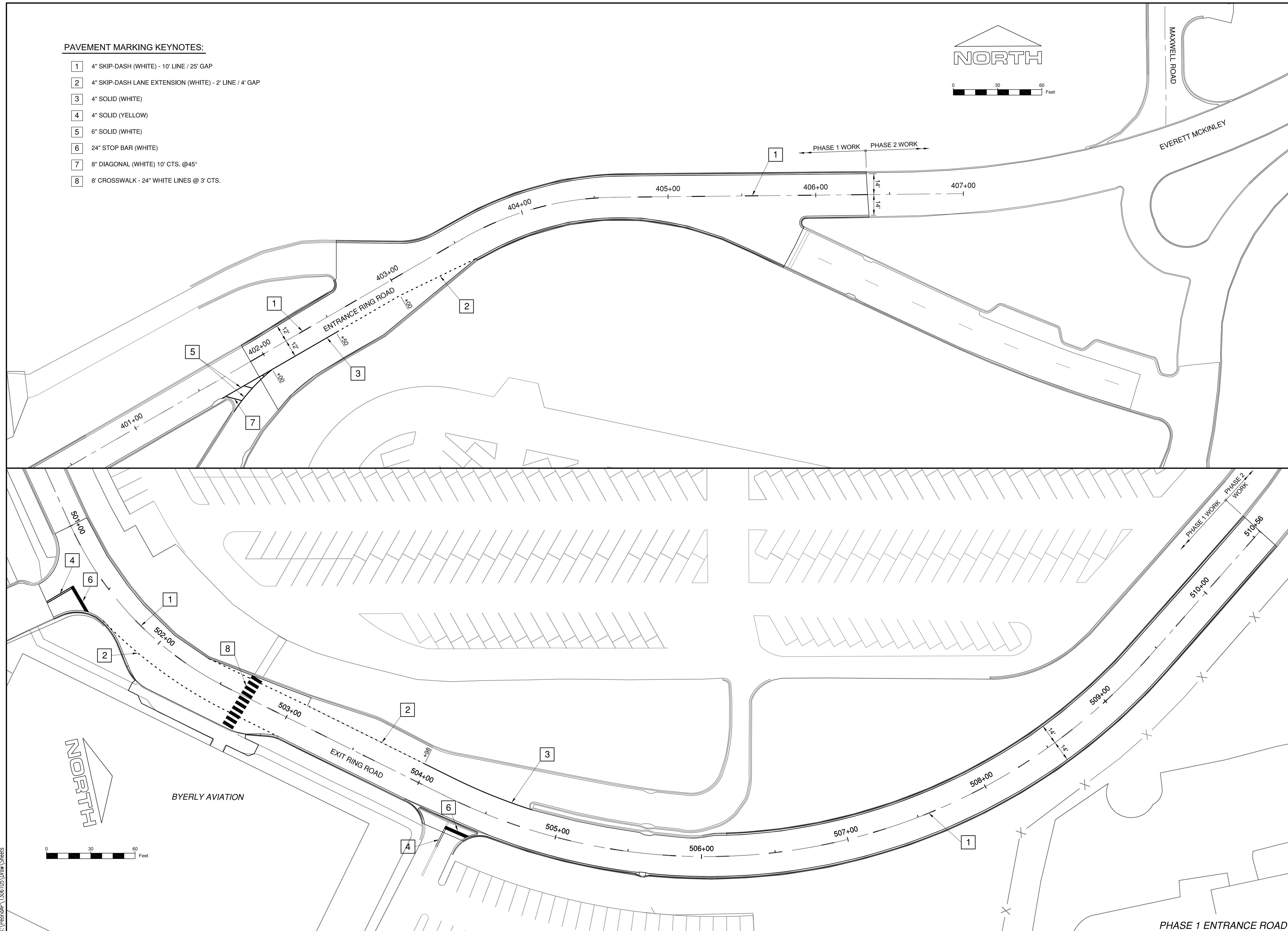
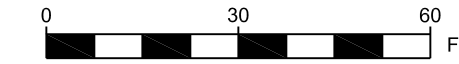
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AIP PROJ. NO.	3-17-0080-XX
SHEET	18 OF 67 SHEETS

PAVEMENT MARKING KEYNOTES:

- 1 4" SKIP-DASH (WHITE) - 10' LINE / 25' GAP
- 2 4" SKIP-DASH LANE EXTENSION (WHITE) - 2' LINE / 4' GAP
- 3 4" SOLID (WHITE)
- 4 4" SOLID (YELLOW)
- 5 6" SOLID (WHITE)
- 6 24" STOP BAR (WHITE)
- 7 8" DIAGONAL (WHITE) 10' CTS. @45°
- 8 8" CROSSWALK - 24" WHITE LINES @ 3' CTS.



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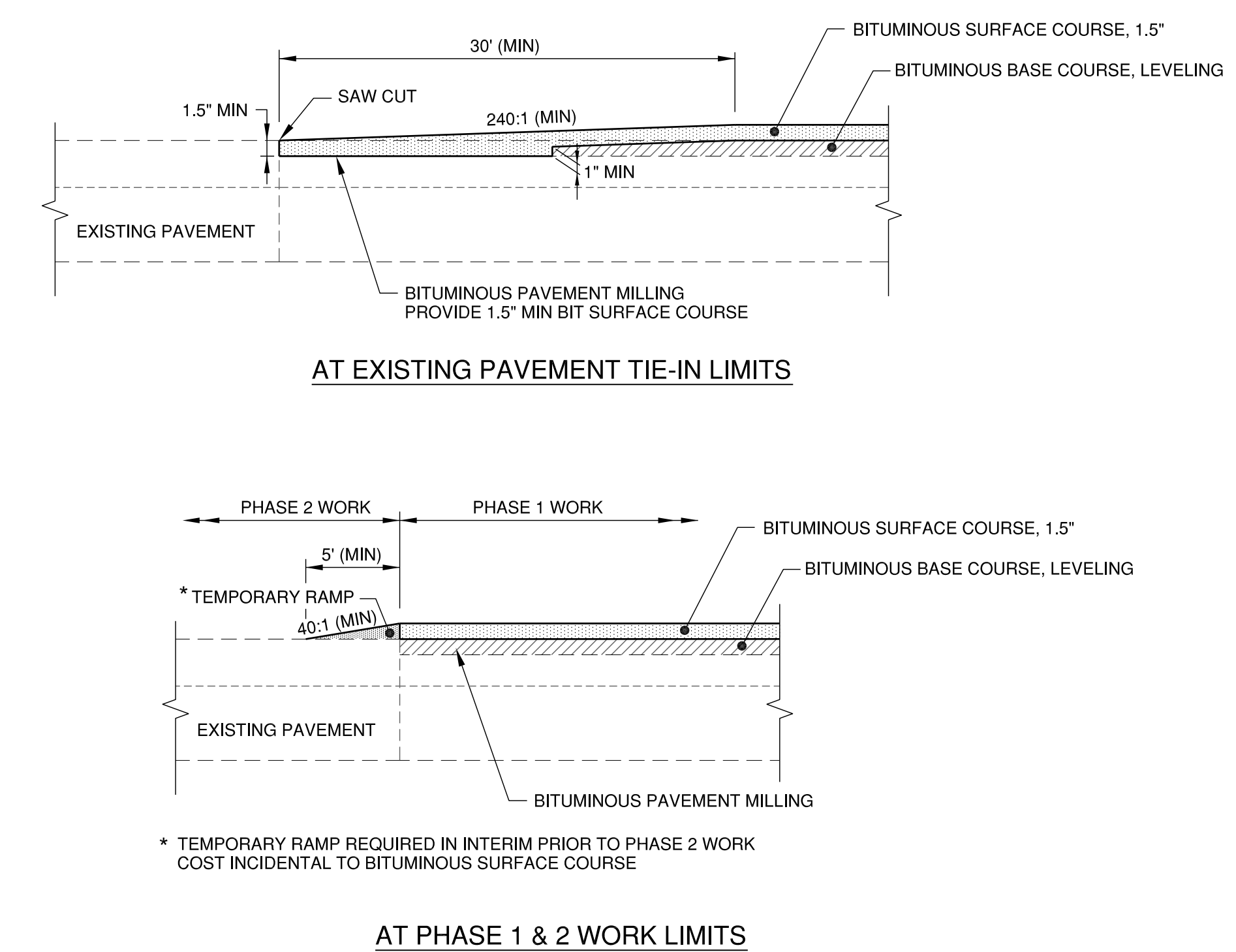
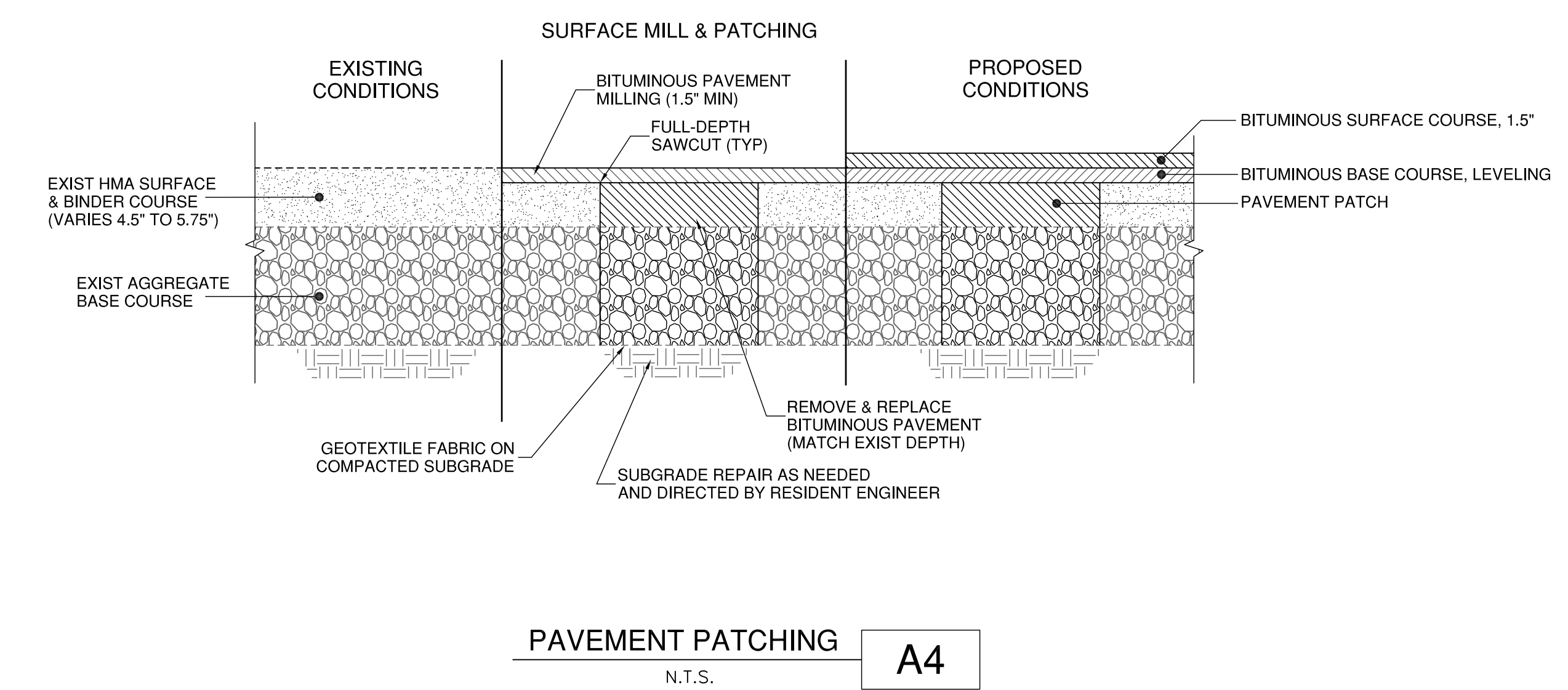
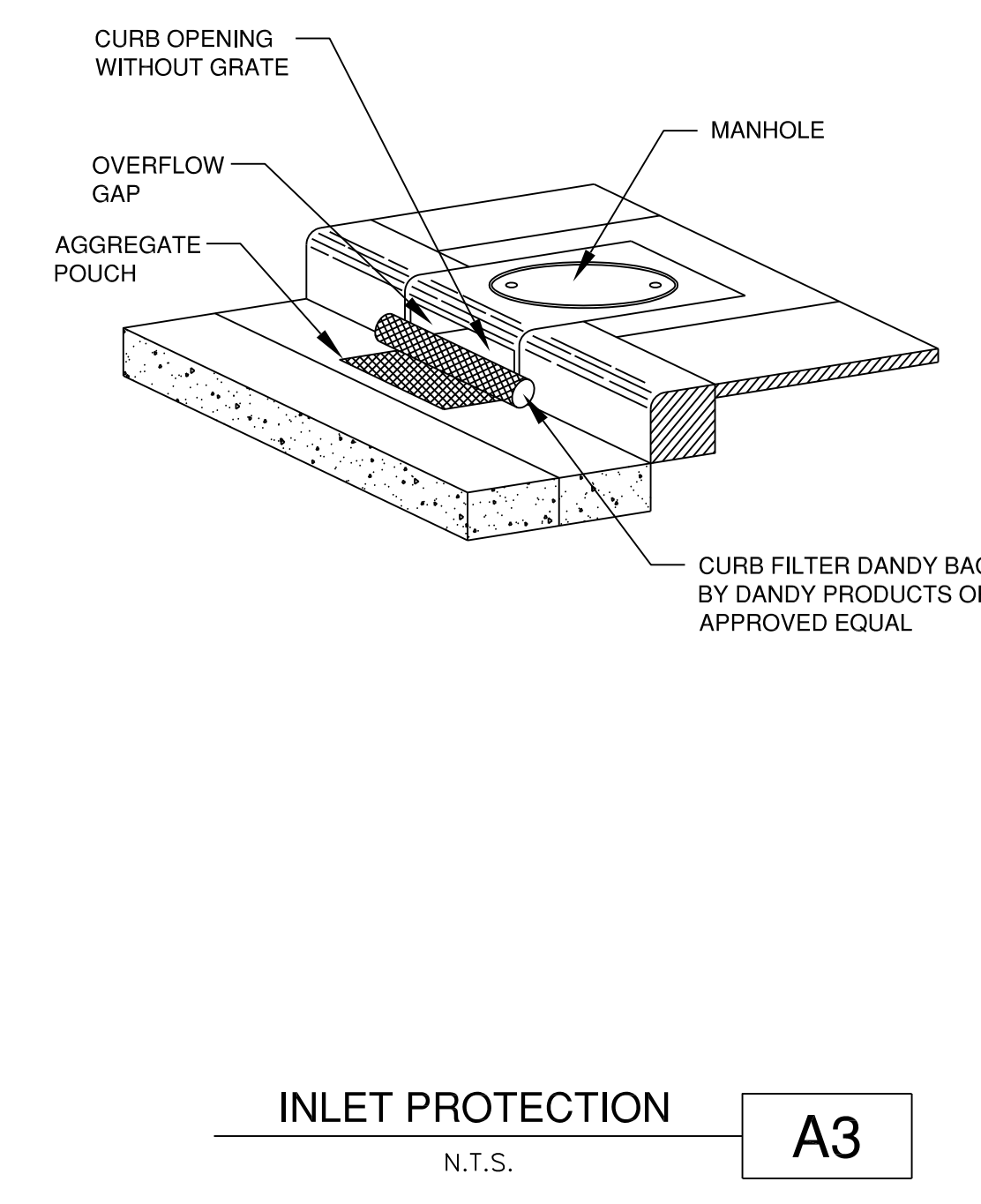
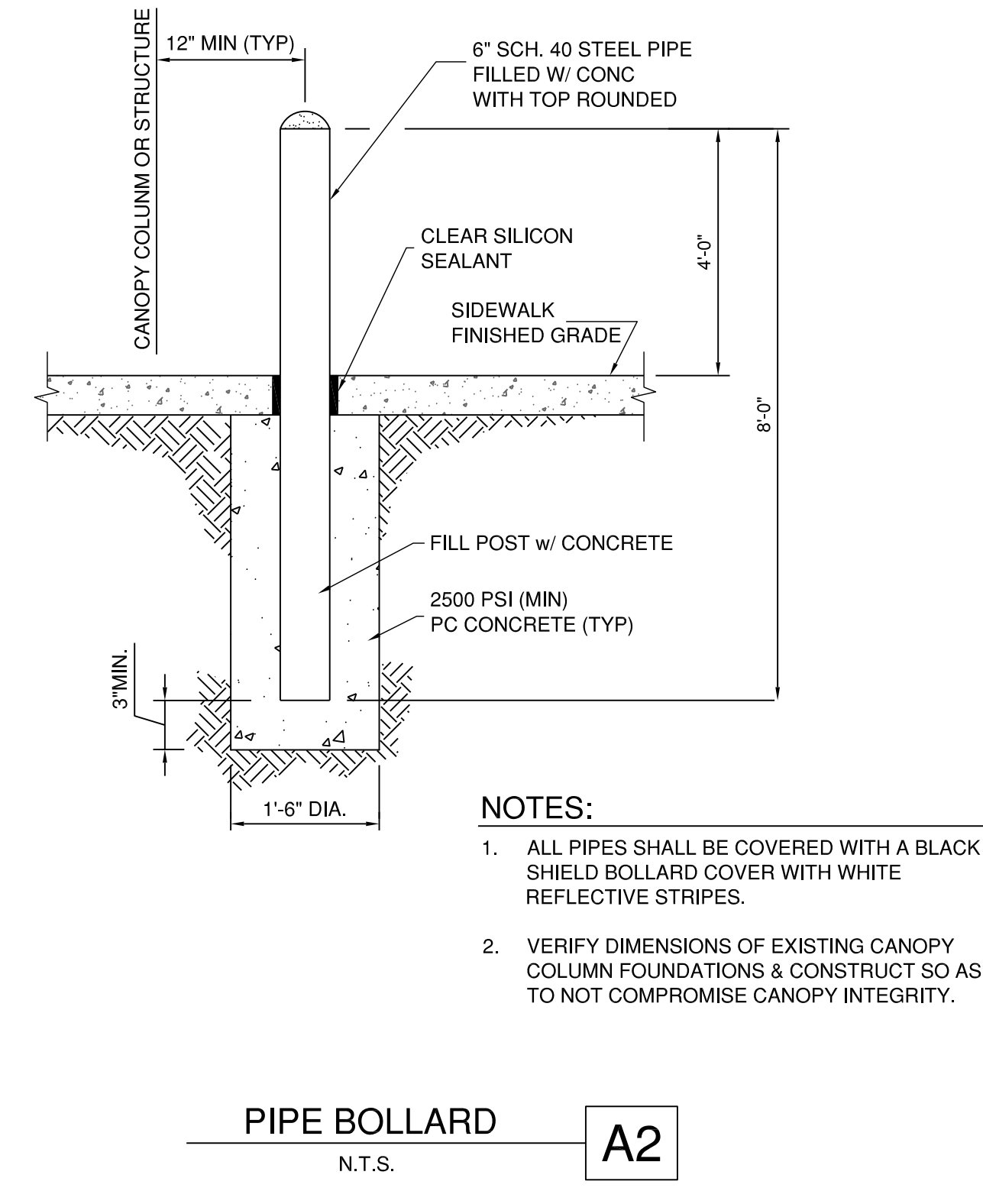
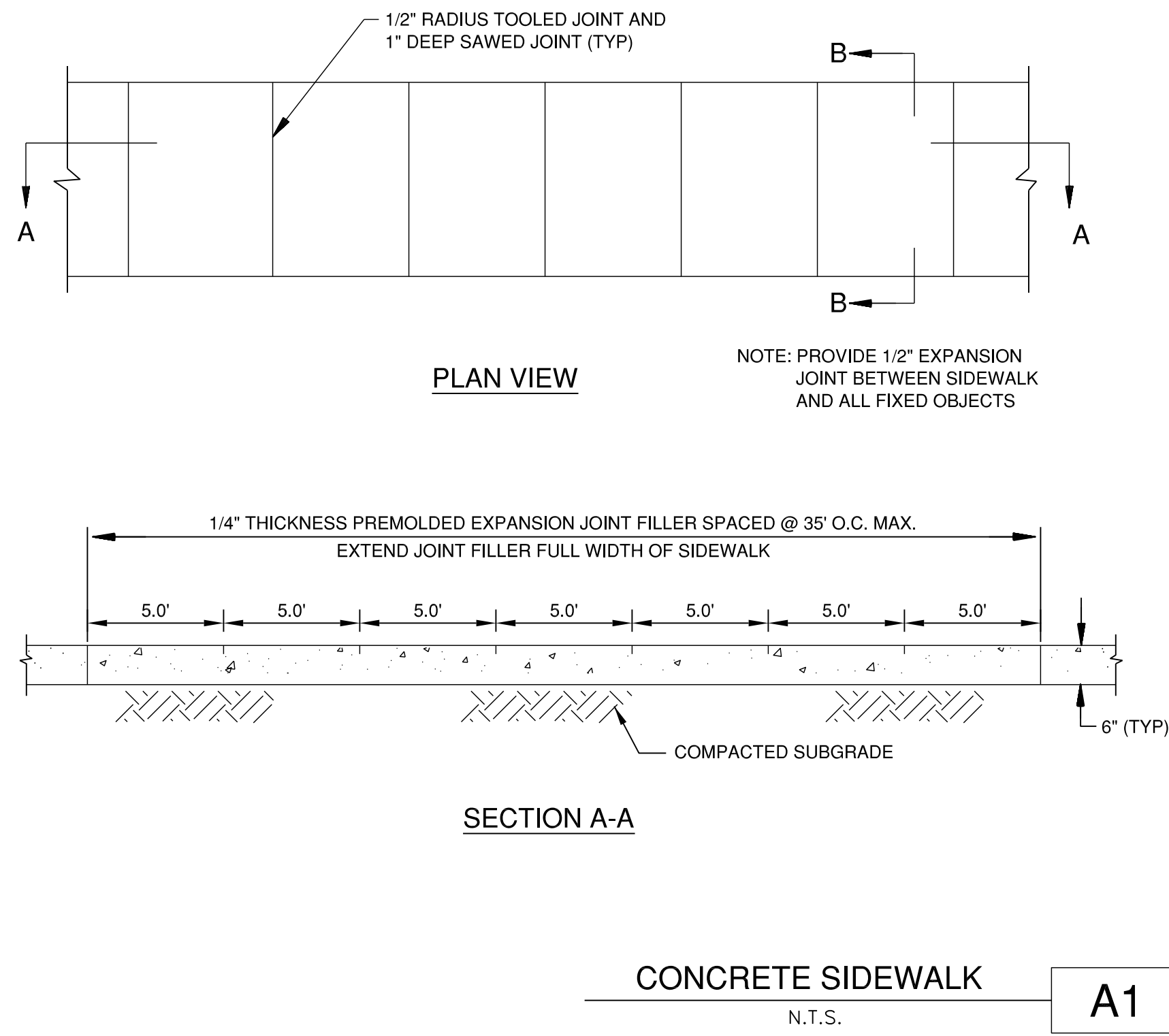
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GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
CONSTRUCTION DETAILS
DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2

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GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT

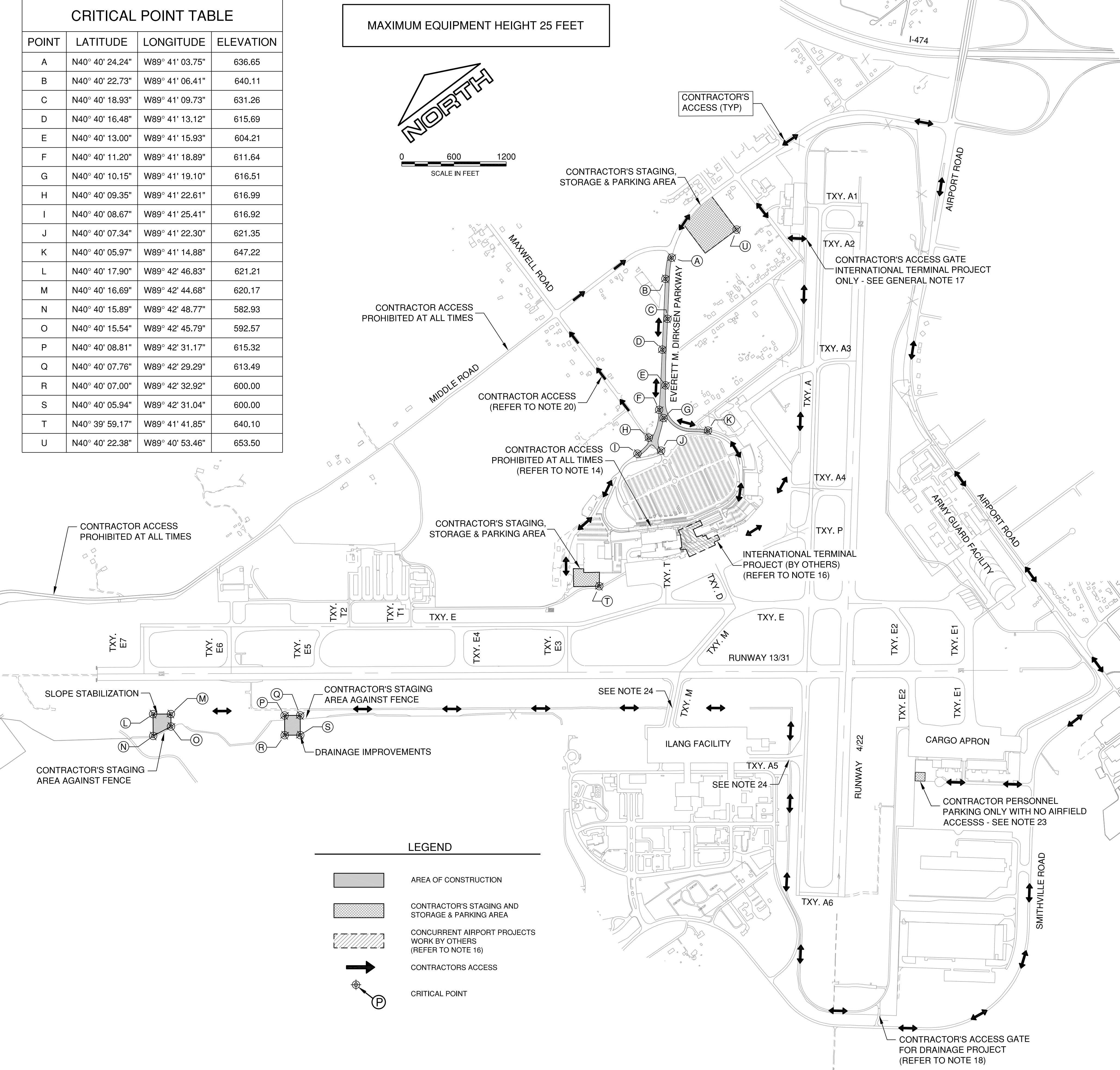
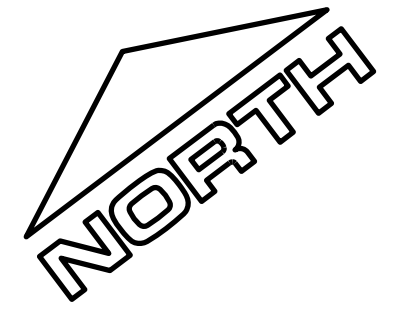
DESIGN BY:	JFM
DRAWN BY:	JFM
CHECKED BY:	SKS
APPROVED BY:	CET
DATE:	JULY 11, 2014
JOB No:	13061-05
IL PROJ. NO.	PIA-4415
AIP PROJ. NO.	3-17-0080-XX



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CRITICAL POINT TABLE			
POINT	LATITUDE	LONGITUDE	ELEVATION
A	N40° 40' 24.24"	W89° 41' 03.75"	636.65
B	N40° 40' 22.73"	W89° 41' 06.41"	640.11
C	N40° 40' 18.93"	W89° 41' 09.73"	631.26
D	N40° 40' 16.48"	W89° 41' 13.12"	615.69
E	N40° 40' 13.00"	W89° 41' 15.93"	604.21
F	N40° 40' 11.20"	W89° 41' 18.89"	611.64
G	N40° 40' 10.15"	W89° 41' 19.10"	616.51
H	N40° 40' 09.35"	W89° 41' 22.61"	616.99
I	N40° 40' 08.67"	W89° 41' 25.41"	616.92
J	N40° 40' 07.34"	W89° 41' 22.30"	621.35
K	N40° 40' 05.97"	W89° 41' 14.88"	647.22
L	N40° 40' 17.90"	W89° 42' 46.83"	621.21
M	N40° 40' 16.69"	W89° 42' 44.68"	620.17
N	N40° 40' 15.89"	W89° 42' 48.77"	582.93
O	N40° 40' 15.54"	W89° 42' 45.79"	592.57
P	N40° 40' 08.81"	W89° 42' 31.17"	615.32
Q	N40° 40' 07.76"	W89° 42' 29.29"	613.49
R	N40° 40' 07.00"	W89° 42' 32.92"	600.00
S	N40° 40' 05.94"	W89° 42' 31.04"	600.00
T	N40° 39' 59.17"	W89° 41' 41.85"	640.10
U	N40° 40' 22.38"	W89° 40' 53.46"	653.50

MAXIMUM EQUIPMENT HEIGHT 25 FEET



LEGEND	
	AREA OF CONSTRUCTION
	CONTRACTOR'S STAGING AND STORAGE & PARKING AREA
	CONCURRENT AIRPORT PROJECTS WORK BY OTHERS (REFER TO NOTE 16)
	CONTRACTORS ACCESS
	CRITICAL POINT

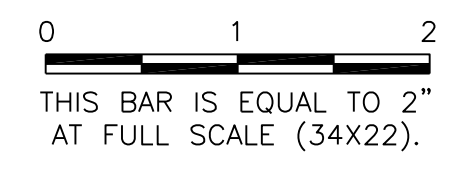
GENERAL NOTES

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS, AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF AN ESCORT OR A FLAGMAN IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED MAAP/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT OPERATIONS DIRECTOR.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS, AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
- EXCESS EARTH, BROKEN ASPHALT, AND CONCRETE SHALL BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 160' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
- THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC.
- ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- SEE CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS SHEET FOR NOTES REGARDING THE CONTRACTOR'S ACCESS.
- CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE LIGHTED, MARKED, AND FLAGGED PER SECTION 70-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITIES PRIOR TO COMPLETING EXCAVATION OR TRENCHING ITEMS. ANY UTILITIES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED IMMEDIATELY BY THE CONTRACTOR AT HIS/HER OWN COST.
- CONTRACTOR SHALL BE EXPLICITLY RESTRICTED FROM USING THE RING ROAD IN FRONT OF THE MAIN TERMINAL BUILDING (PCC PORTION) OR THE MAIN TERMINAL PARKING LOT FOR CONSTRUCTION ACCESS OF ANY KIND. THE CONTRACTOR SHALL BE REQUIRED TO STAGE CONSTRUCTION AROUND THE USE OF THESE AREAS AT ALL TIMES. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR'S VEHICLES OR EQUIPMENT BE ALLOWED TO USE THESE AREAS.
- THERE SHALL NOT BE ANY CONSTRUCTION TRAFFIC ON THE AIRFIELD SIDE OF THE FENCE DURING THE REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 2 PORTION OF THE PROJECT.
- THERE IS ONE (1) KNOWN CONCURRENT PEORIA INTERNATIONAL PROJECT UNDER CONSTRUCTION DURING THE PROPOSED REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 2 PROJECT. THE CONTRACTOR SHALL BE REQUIRED TO COORDINATE WITH ADJACENT WORK AT ALL TIMES.
- CONTRACTOR'S ACCESS THROUGH GATE NEAR TAXIWAY A2 IS SHOWN FOR REFERENCE ONLY. THIS ACCESS POINT WILL BE USED BY SEMI TRAILERS FOR THE INTERNATIONAL TERMINAL PROJECT ONLY DURING CRITICAL PHASES OF THE REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 2 PORTION OF THE PROJECT. THERE SHALL BE NO CONSTRUCTION ACCESS THROUGH THIS GATE AND NO CONSTRUCTION TRAFFIC ON THE AIRFIELD SIDE OF THE FENCE DURING THE REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 2 PORTION OF THE PROJECT.
- CONTRACTOR'S ACCESS THROUGH GATE AT SOUTH END OF AIRFIELD OFF SMITHVILLE ROAD SHALL BE UTILIZED FOR THE DRAINAGE IMPROVEMENTS / SLOPE STABILIZATION PORTION OF THE PROJECT ONLY. ACCESS TO THIS GATE SHALL BE FROM SMITHVILLE ROAD BY WAY OF AIRPORT ROAD ONLY.
- PRIOR TO BEGINNING WORK ON THE AIRSIDE, THE CONTRACTOR SHALL COORDINATE ALL CONSTRUCTION ACTIVITIES WITH THE ILLINOIS AIR NATIONAL GUARD (ILANG). THE CONTRACTOR SHALL COMPLY TO ALL ADDITIONAL SECURITY MEASURES IMPLEMENTED BY THE ILANG AND SHALL CONTINUOUSLY UPDATE THEM ON THE CONSTRUCTION PROGRESS SCHEDULE. REFER TO SPECIAL PROVISIONS FOR A CONTACT AT THE ILANG.
- CONTRACTOR ACCESS ON MAXWELL ROAD SHALL BE LIMITED TO EMPTY (UNLOADED) TRUCKS AND LIGHT DUTY CONSTRUCTION VEHICLES ONLY, UNLESS REQUIRED BY PHASE. CONTRACTOR SHALL REFER TO CONSTRUCTION ACTIVITY PLANS FOR ADDITIONAL DETAILS.
- IF EXISTING ROADWAY SIGNS INTERFERE WITH THE INSTALLATION OF WORK FOR THIS PROJECT, SUCH SIGNS AND POSTS SHALL BE REMOVED AND REPLACED AFTER CONSTRUCTION. THE COST OF REMOVING AND STORING EXISTING SIGNS WILL NOT BE PAID FOR DIRECTLY. THE COSTS SHALL BE INCLUDED IN OTHER PAY ITEMS.
- ACCESS TO ALL ENTRANCES SHALL BE MAINTAINED AT ALL TIMES UNLESS NOTED OTHERWISE ON THE PLANS.
- CONTRACTOR SHALL PARK PERSONAL VEHICLES AT THE LOCATION INDICATED ON THE PLANS. WORKERS SHALL THEN CARPOOL TO THE PROJECT SITE THROUGH THE CONTRACTOR'S ACCESS GATE TO MINIMIZE THE NUMBER OF CONTRACTOR VEHICLES IN THE AOA.
- CONTRACTOR SHALL PHASE / STAGE WORK TO LIMIT ACCESS ACROSS ACTIVE TAXIWAYS. ALL CONSTRUCTION EQUIPMENT AND PERSONNEL OPERATING ACROSS THE TAXIWAY SHALL BE IN CONTINUOUS RADIO CONTACT WITH AND OPERATING UNDER THE DIRECTION OF THE OFA. ALL WORK ACROSS THESE LIMITS SHALL BE EXPEDITED AND THE CONTRACTOR SHALL BE PREPARED TO IMMEDIATELY EXIT THE OFA WHEN DIRECTED BY THE ATCT. THE PAVEMENT SURFACES SHALL BE THOROUGHLY CLEANED AFTER EACH CROSSING AS REQUIRED BY CONSTRUCTION ACTIVITIES.

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 PLOT DATE: 7/10/2014 11:43 AM

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NUMBER	BY	DATE



GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
SITE PLAN - PHASE 2

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GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

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CHECKED BY:	SKS
APPROVED BY:	CET
DATE:	JULY 11, 2014
JOB No:	13061-05

IL PROJ. NO. PIA-4415
 AIP PROJ. NO. 3-17-0080-XX
 SHEET 20 OF 67 SHEETS

PHASE 2 ENTRANCE ROAD & DRAINAGE/STABILIZATION IMPROVEMENTS

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GENERAL

1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS AIRFIELD OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL, THE TRAVELING PUBLIC AND MEETING OSHA REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE STORM WATER POLLUTION PREVENTION PROGRAM (SWPPP) CERTIFICATION STATEMENT.
6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

1. TOTAL CONTRACT TIME SHALL BE 135 CALENDAR DAYS.
 2. PHASING FOR THE DRAINAGE IMPROVEMENTS / SLOPE STABILIZATION PORTION OF THE PROJECT SHALL BE INDEPENDENT OF THE REQUIRED PHASING FOR THE REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 2 PORTION OF THE PROJECT. THE WORK CAN OCCUR SIMULTANEOUSLY.
 3. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEET.
- PHASES 1 THROUGH 5 NOTES**
1. ALL WORK SHALL BE COMPLETED IN PREVIOUS PHASE PRIOR TO STARTING WORK IN THE NEXT PHASE UNLESS OTHERWISE PERMITTED BY THE AIRPORT.
 2. THE WORK PHASING PLAN PRESENTED IS NOT INTENDED TO RESTRICT THE CONTRACTOR TO THIS SPECIFIC PHASING. THE CONTRACTOR MAY SUBMIT THEIR OWN PHASING SCHEDULE FOR REVIEW AND WRITTEN APPROVAL.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

1. THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN.
3. CERTAIN CONTRACTOR EMPLOYEES SHALL OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET CERTAIN BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTAIN CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
4. ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATIONS AREA (AOA) SHALL ALSO ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT. ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
5. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR.
6. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS.
7. THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN
8. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
9. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
10. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
11. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
12. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
13. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
14. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
15. THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

1. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTICE TO AIRMEN (NOTAMS) MAY BE ISSUED BY THE AIRPORT.
3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
5. CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.

AIRPORT OPERATOR DOUG PALMER - DIRECTOR OF OPERATIONS	(309) 303-0994
AIRPORT MAINTENANCE GREG HUSER - MAINTENANCE SUPERVISOR	(309) 303-1005
ENGINEER CHUCK TAYLOR, P.E. - PROJECT ENGINEER	(309) 637-1890
CMT - RESIDENT ENGINEER	(309) 637-1890

10. INSPECTION REQUIREMENTS

1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL ATTEND A FINAL INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. PENALTIES

1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

1. NO RUNWAY OR TAXIWAY CLOSURES ARE REQUIRED FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2F.
2. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

15. MARKING AND SIGNS FOR ACCESS ROUTES

1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THIS SHEET AND THE CONSTRUCTION ACTIVITY PLAN SHEET.

16. HAZARD MARKING AND LIGHTING

1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR APPLICABLE IDOT STANDARD.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
5. THE AIRPORT WILL SUPPLY UP TO TWO LIGHTED RUNWAY CLOSURE MARKERS FOR USE DURING THE PROJECT. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTENANCE OF THE RUNWAY CLOSURE MARKERS INCLUDING FUEL, OIL CHANGES AND REPLACEMENT OF THE LIGHTS. IF ADDITIONAL LIGHTED RUNWAY CLOSURE MARKERS ARE NEEDED, THE CONTRACTOR SHALL SUPPLY THE ADDITIONAL LIGHTED RUNWAY CLOSURE MARKERS. COST SHALL BE INCIDENTAL TO THE CONTRACT. UPON COMPLETION OF THE PROJECT, THE CONTRACTOR WILL RETAIN POSSESSION OF ANY ADDITIONAL LIGHTED RUNWAY CLOSURE MARKERS SUPPLIED BY THE CONTRACTOR. THE LIGHTED RUNWAY CLOSURE MARKERS SUPPLIED BY THE AIRPORT SHALL REMAIN PROPERTY OF THE AIRPORT AND SHALL BE RETURNED IN LIKE CONDITION WITH AN ALLOWANCE FOR NORMAL WEAR AND TEAR AS DETERMINED BY THE RESIDENT ENGINEER. ANY DAMAGED TO THE LIGHTED RUNWAY CLOSURE MARKERS SHALL BE REPLACED BY THE CONTRACTOR WITH NEW LIGHTED RUNWAY CLOSURE MARKERS OF SIMILAR TYPE AND DESIGN AT NO ADDITIONAL COST TO THE CONTRACT.

17. PROTECTION

1. ALL WORK REQUIRED INSIDE OF THE RUNWAY 4-22 OR 13-31 SAFETY AREAS, WHICH EXTENDS 250' FROM THE RUNWAY CENTERLINE, WILL REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY SAFETY AREA, WHICH EXTENDS 107' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

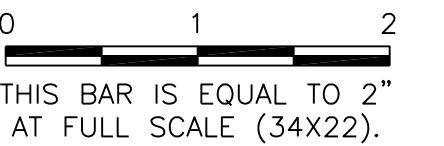
18. OTHER LIMITATIONS ON CONSTRUCTION

1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

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REVISIONS

NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS**

**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2**

CONSTRUCTION SAFETY AND PHASING PLAN NOTES

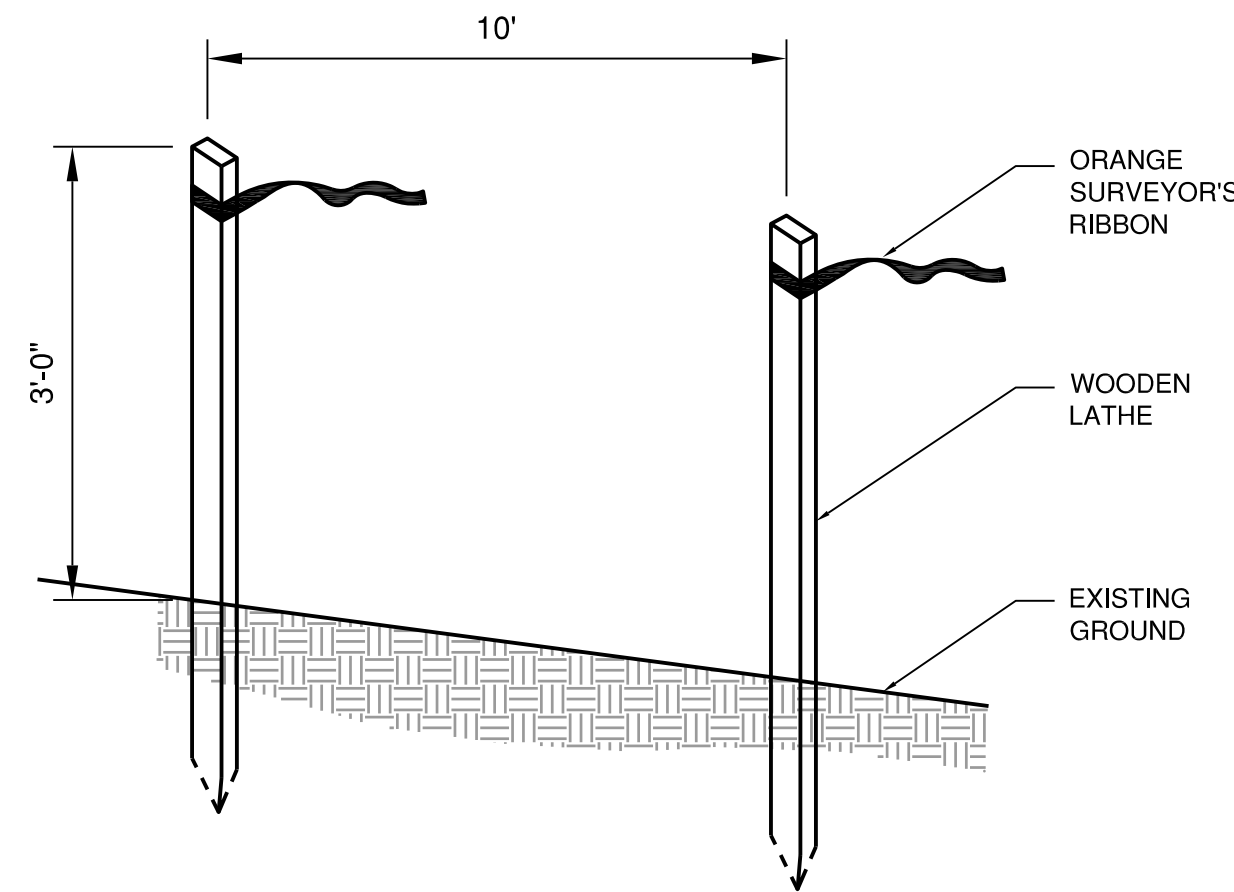
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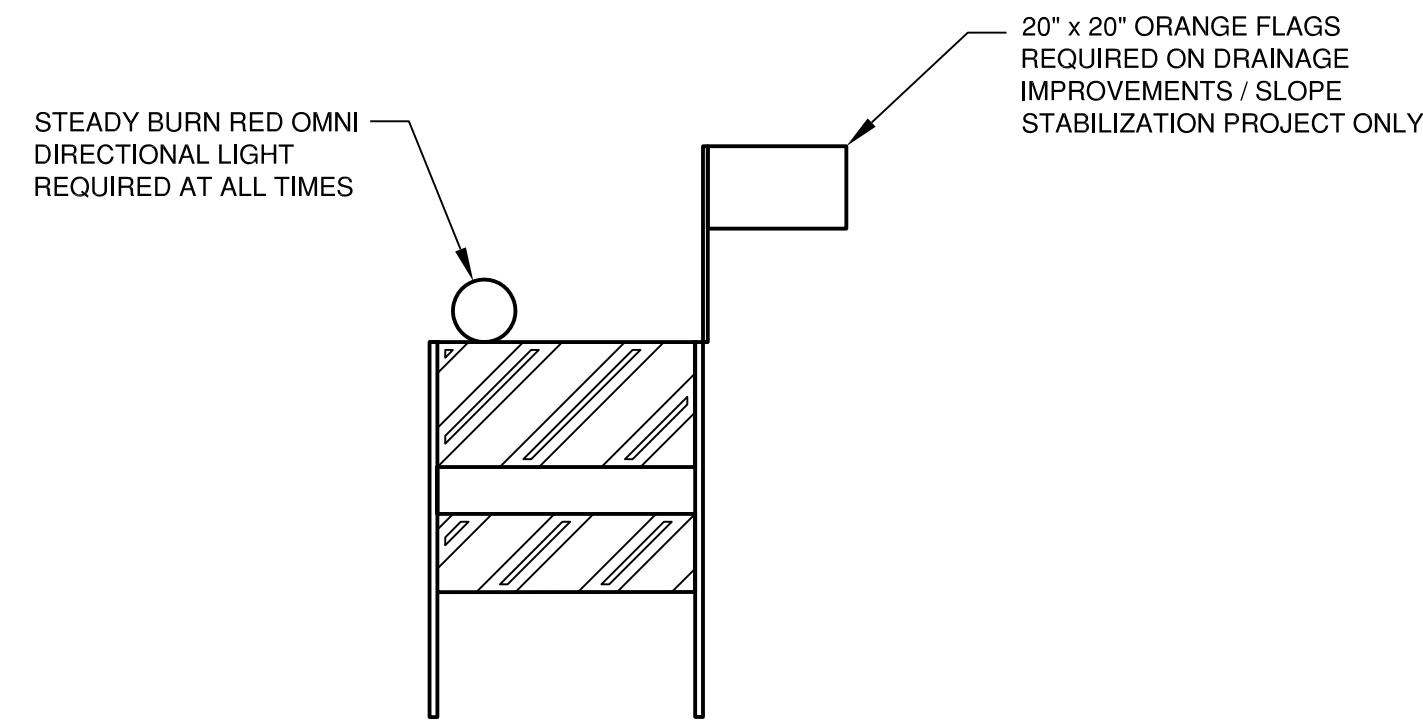
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CONSTRUCTION SETBACK LINE DETAIL
 N.T.S.



FLASHER BARRICADE DETAIL - IDOT TYPE 1
 N.T.S.

FLASHER BARRICADE NOTES

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. INSTALL AT 10' INTERVALS OR REQUIRED BY APPLICABLE IDOT STANDARD.

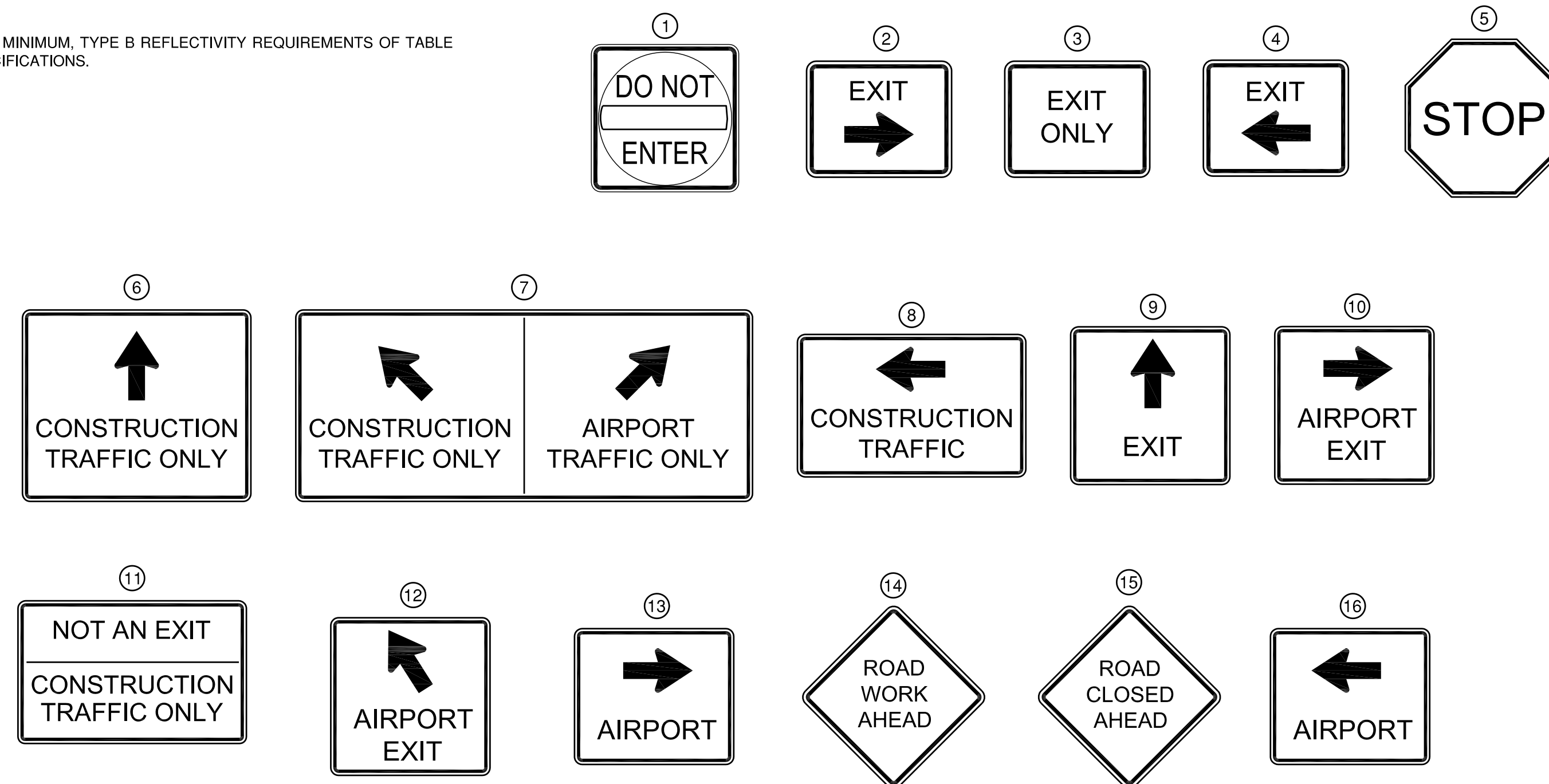
CONSTRUCTION ACTIVITY PLAN GENERAL NOTES

1. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK SO AS TO NOT INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS.
2. THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS.
3. IT SHALL BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECT ARE THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS.
4. WHEN CONFLICTS ARISE, RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT THROUGH THE RESIDENT ENGINEER IN THE BEST INTEREST OF THE AIRPORT AND THE PROJECT.
5. DELAYS, CHANGES IN SCHEDULING OR THE EXPEDITION OF WORK UNDER THIS CONTRACT TO PROVIDE FOR THE TIMELY PROSECUTION OF THE PROJECT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
6. VEHICLE ACCESS ON PERIMETER ROADS SHALL BE MAINTAINED THROUGH OUT THE PROJECT.
7. THE CONTRACTOR SHALL BE AWARE THAT DAILY OPERATION OF THE AIRPORT WILL CONTINUE THROUGHOUT THIS PROJECT. AS A RESULT, DAILY COORDINATION WILL BE NECESSARY TO LIMIT DISRUPTION TO AIRPORT/TENTANT AND CONTRACTOR OPERATIONS.
8. CONTRACTOR'S STAGING, STORAGE, AND PARKING WILL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS.
9. THE CONTRACTOR SHALL PLACE ALL BARRICADES, CONSTRUCTION SETBACK LINES, AND EROSION CONTROL ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
10. CONSTRUCTION PHASING IS OF CRITICAL IMPORTANCE TO THE AIRPORT FOR THIS PROJECT. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE ALLOWED TO CLOSE BOTH RUNWAYS AT THE SAME TIME.
11. CONSTRUCTION RELATED ITEMS REQUIRING THE CLOSURE OF RUNWAYS AND/OR TAXIWAYS SHALL REQUIRE A MINIMUM OF 72 HOURS NOTICE TO COORDINATION WITH THE AIRPORT AND FAA. NO EXTENSION TO CONTRACT TIME WILL BE GIVEN FOR DELAYS CAUSED BY LACK OF ADEQUATE COORDINATION.
12. CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A. THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - B. THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE AIRPORT OPERATIONS BY THE CONTRACTOR.
 - C. THE CONTRACTOR SHALL USE AN EXISTING GATE(S), OR NEW GATES AS CALLED OUT IN THE PLANS, FOR ACCESS TO THE AIRFIELD.
 - D. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - E. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - F. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
 - G. THE CONTRACTOR SHALL CLOSE AND SECURE THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - H. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - I. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
 - J. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE USED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.
 - K. THE CONTRACTOR SHALL EVALUATE THE PRESERVE THE CONDITION OF THE EXISTING AIRPORT PERIMETER ROAD AND ALL OTHER ACCESS ROUTES UTILIZED BY THE CONTRACTOR FOR ACCESS TO THE SITE. THE CONTRACTOR SHALL DOCUMENT THE CONDITION OF THE EXISTING PAVEMENT IN THE PRESENCE OF THE AIRPORT AND RESIDENT ENGINEER PRIOR TO THE INITIATION OF CONSTRUCTION. AT THE COMPLETION OF CONSTRUCTION, ANY PAVEMENT USED BY THE CONTRACTOR THAT IS SAID TO BE DAMAGED BY HIS OPERATIONS SHALL BE REPAIRED TO IT'S ORIGINAL CONDITION AND/OR TO THE SATISFACTION AT THE CONTRACTOR'S EXPENSE.

TEMPORARY CONSTRUCTION SIGNAGE NOTES

1. ALL SIGNAGE SHALL BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JANUARY 1, 2012, THE DETAILS IN THESE PLANS AND THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
2. ALL SIGNS SHALL BE PROVIDED, PLACED AND MAINTAINED BY THE CONTRACTOR.
3. THE TRAFFIC CONTROL STANDARDS PROVIDED IN THESE PLANS MAY REQUIRE MODIFICATION AT TIMES. THE AIRPORT SHALL APPROVE ANY MODIFICATIONS PROPOSED BY THE CONTRACTOR.
4. ALL EXISTING SIGNAGE THAT IS NOT APPLICABLE TO THE TRAFFIC CONTROL IN PLACE SHALL BE REMOVED OR COMPLETELY COVERED.
5. ALL WORK ZONE SIGNS ARE REQUIRED TO MEET, AT A MINIMUM, TYPE B REFLECTIVITY REQUIREMENTS OF TABLE 1091-2 IN ARTICLE 1091.02 OF THE IDOT STANDARD SPECIFICATIONS.

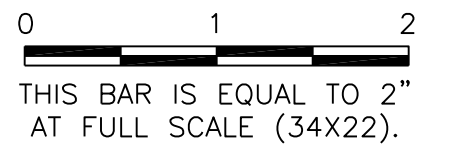
NOTE: REFER TO CONSTRUCTION ACTIVITY PLANS FOR NUMBERED LOCATIONS



TEMPORARY CONSTRUCTION SIGNAGE
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PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
CONSTRUCTION ACTIVITY PLAN NOTES & DETAILS

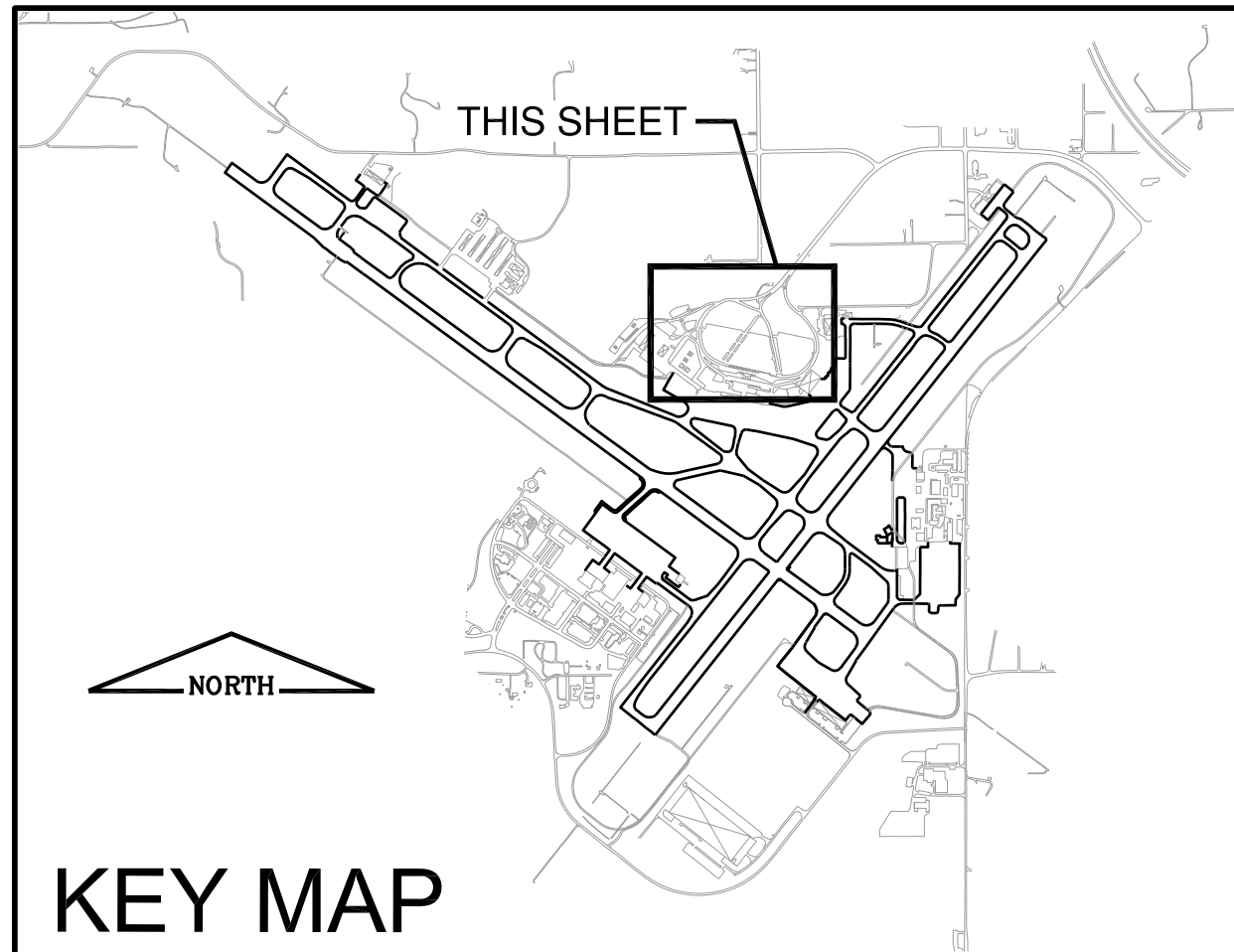
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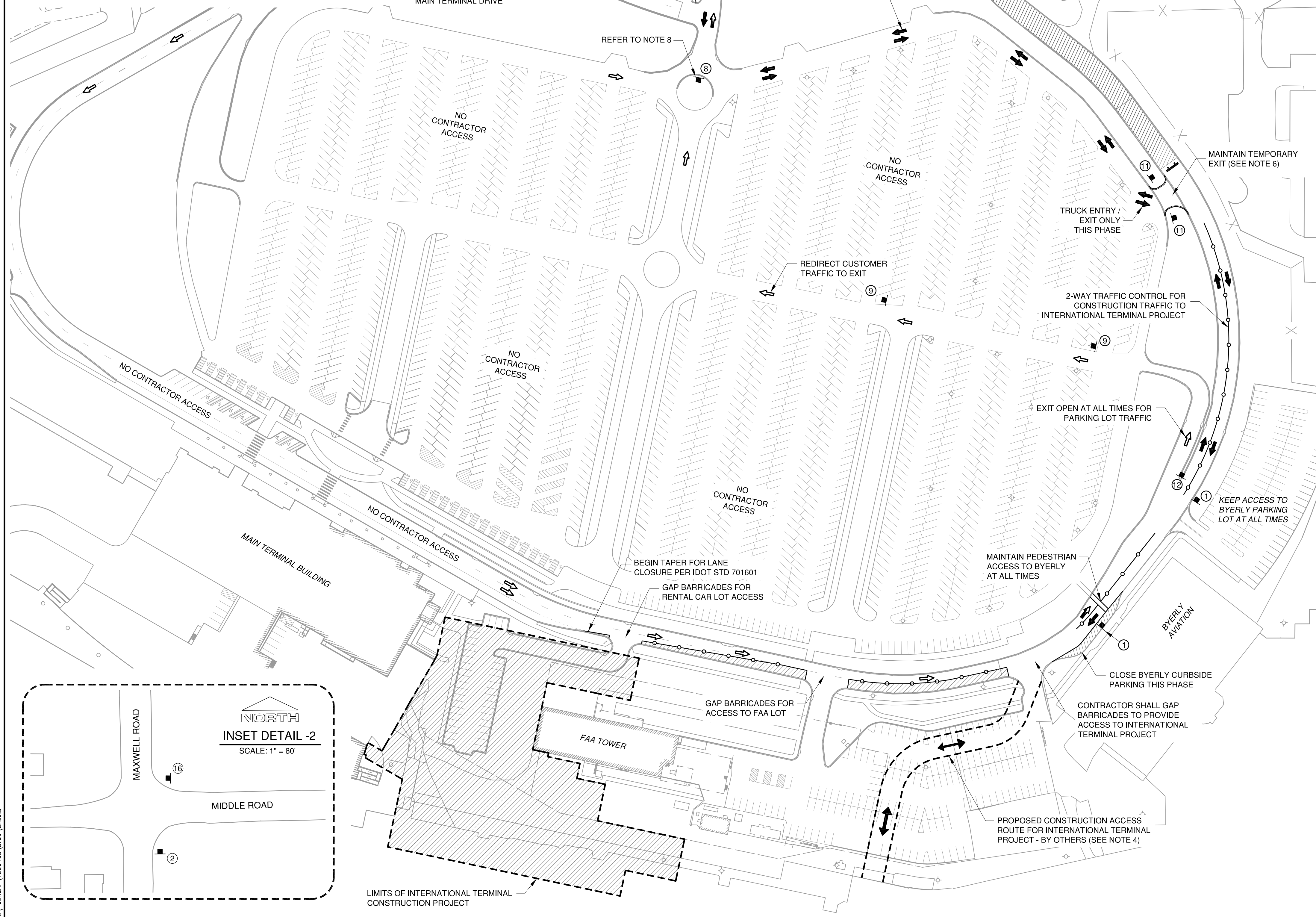
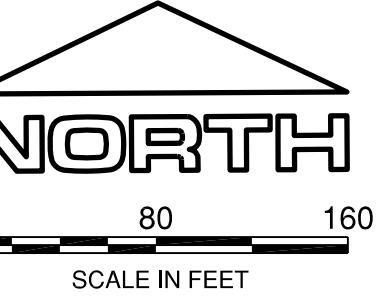
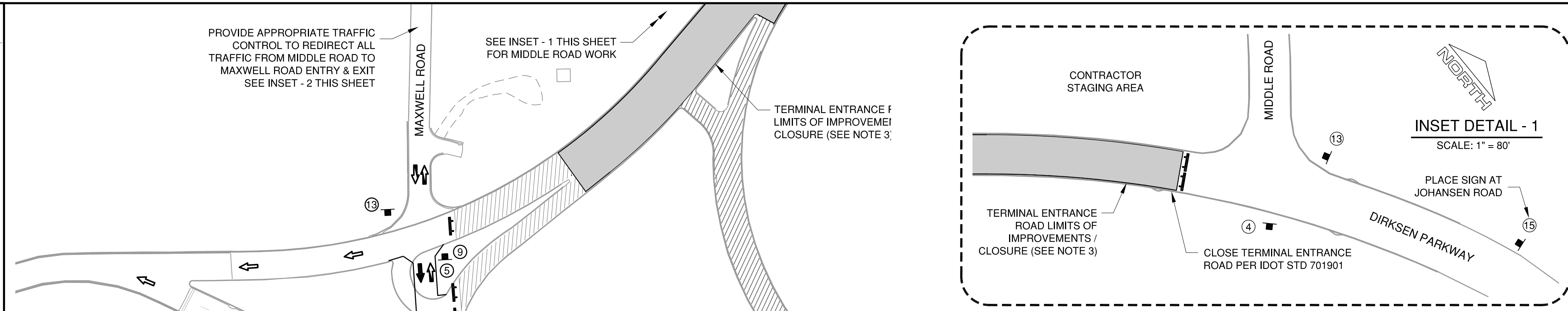
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KEY MAP



LEGEND

	PROPOSED IMPROVEMENTS - THIS PHASE
	PROPOSED WORK ZONE CLOSURES
	CONTRACTOR'S STAGING, STORAGE AND PARKING AREA
	TRAFFIC CONTROL DRUMS PER IDOT STD 701901
	CONSTRUCTION ACCESS
	CUSTOMER ACCESS
	CONSTRUCTION & CUSTOMER ACCESS
	TEMPORARY SIGNAGE REFER TO SHEET 22 FOR DETAILS
	IDOT TYPE III BARRICADE

CONSTRUCTION STAGING / ACCESS NOTES

1. THE CONTRACTOR'S ACCESS TO THE PROJECT LIMITS SHALL BE IN ACCORDANCE WITH DETAILS ON THE PROJECT SITE PLAN (SHEET 20). CONSTRUCTION TRAFFIC SHALL BE PROHIBITED FROM UTILIZING THE DRIVE IN FRONT OF THE MAIN TERMINAL AND THE MAIN PARKING LOT AT ALL TIMES.
2. ALL TRAFFIC CONTROL, INCLUDING THE TEMPORARY EXIT TO THE MAIN PARKING LOT FOR CUSTOMER ACCESS SHALL BE INSTALLED PRIOR TO INITIATION OF ANY PERMANENT IMPROVEMENTS.
3. THE CONSTRUCTION OF THIS PHASE OF IMPROVEMENTS SHALL BE COMPLETED PRIOR TO PROCEEDING WITH THE NEXT PHASE OF CONSTRUCTION. THE CONTRACTOR SHALL BE REQUIRED TO MODIFY THE TRAFFIC CONTROL AS REQUIRED TO ACCOUNT FOR SIGHT OBSTRUCTIONS, CHANGED CONDITIONS, ETC AT THE DIRECTION OF THE AIRPORT.
4. THE CONSTRUCTION ACCESS ROUTE AND INCIDENTAL ENTRANCE IMPROVEMENTS AT THE NORTHWEST CORNER OF BYERLY AVIATION ARE THE RESPONSIBILITY OF THE INTERNATIONAL TERMINAL PROJECT CONTRACTOR. THE TERMINAL ENTRANCE ROAD, PHASE 2 CONTRACTOR SHALL BE REQUIRED TO MAINTAIN ACCESS THROUGH THE EXIT RING ROAD PROJECT IMPROVEMENTS AT ALL TIMES FOR THE ADJACENT PROJECT CONSTRUCTION TRAFFIC.
5. REFER TO SHEET 5 FOR ADDITIONAL INFORMATION REGARDING TEMPORARY CONSTRUCTION SIGNING. ALL EXISTING SIGNING THAT IS NOT APPLICABLE OR IN CONFLICT WITH THIS PHASE OF WORK SHALL BE COMPLETED COVERED BY THE CONTRACTOR.
6. THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE TEMPORARY PARKING LOT EXIT CONSTRUCTED DURING PREVIOUS PHASE.
7. ADJUSTMENTS TO IDOT STANDARDS FOR TRAFFIC CONTROL MAY BE REQUIRED AT TIMES BY THE AIRPORT - FLASHING ARROW BOARDS WILL NOT BE REQUIRED AS PART OF THE TRAFFIC CONTROL.
8. AT TIMES DURING CONSTRUCTION, THE CONTRACTOR MAY BE REQUIRED TO PROVIDE AN ESCORT OR FLAGGER FOR CONSTRUCTION TRAFFIC SHORTCUTTING THE TRAFFIC CIRCLE IN THE WRONG DIRECTION TO PROTECT CUSTOMER SAFETY.

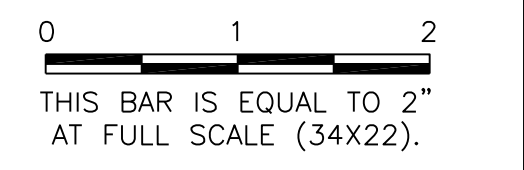
STAGE 3

FILE: 25-ConstActivityPlan3.dwg
 UPDATE BY: Joe Miller
 PLOT DATE: 7/10/2014 10:58 AM

PIA Base Existing
 KEYMAP
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GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
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CONSTRUCTION ACTIVITY PLAN 3
DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2

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SHEET 25 OF 67 SHEETS	

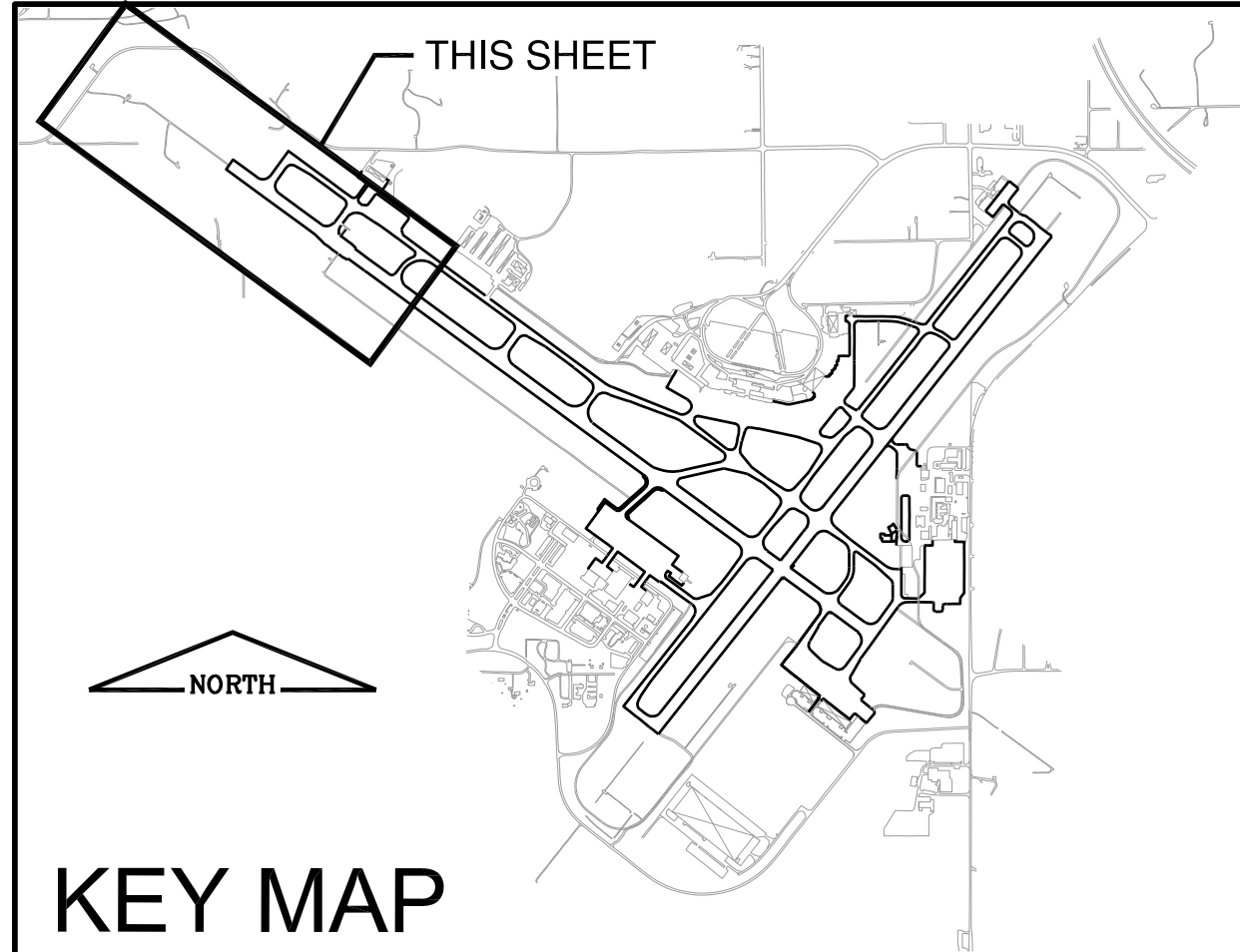
PHASE 2 ENTRANCE ROAD

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 AT FULL SCALE (34X22).






CONSTRUCTION STAGING / ACCESS NOTES

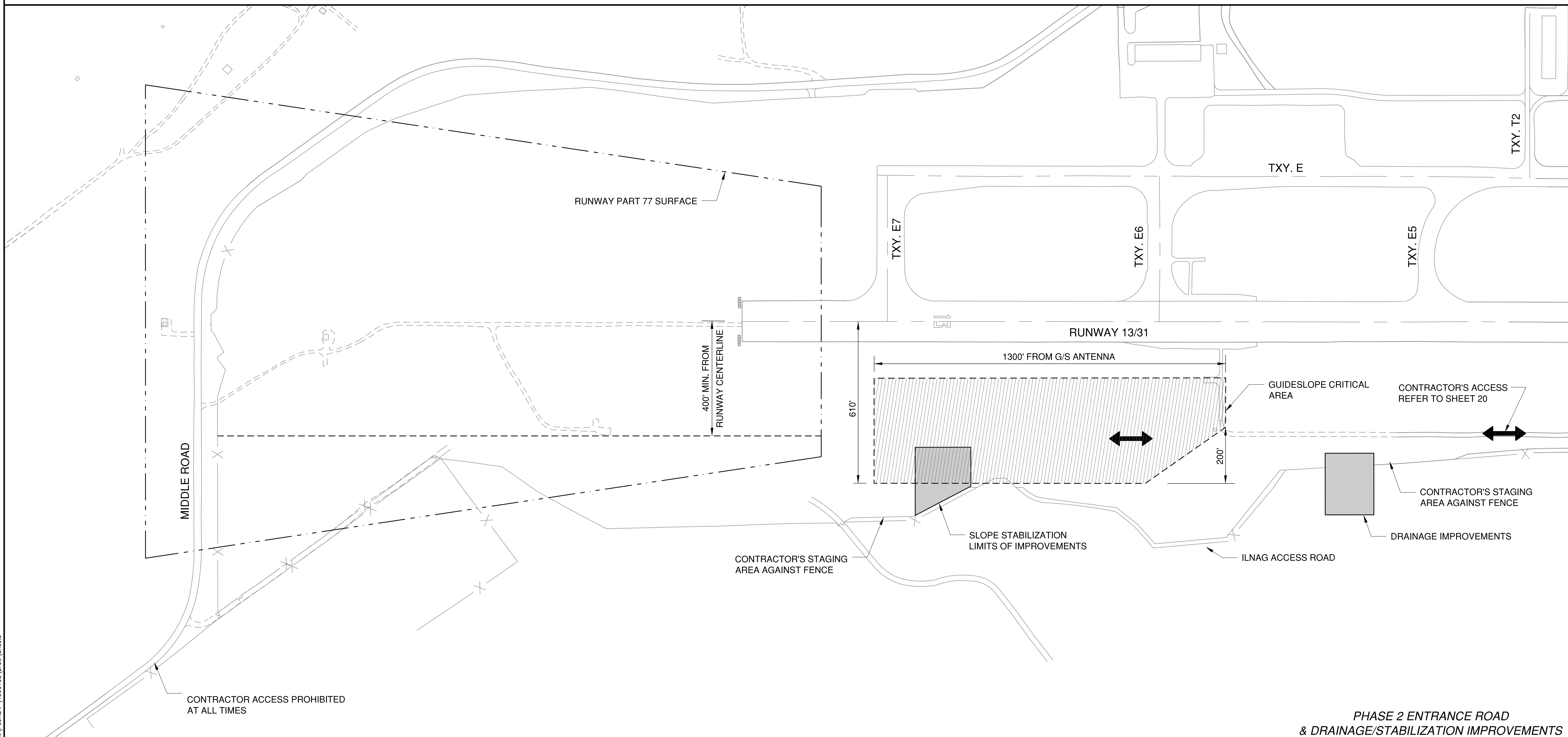
1. THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR AND RESIDENT ENGINEER SEVEN (7) CALENDAR DAYS PRIOR TO THE INITIATION OF THIS PHASE.
2. THE CONTRACTOR WILL BE ALLOWED ACCESS TO THE FIELD AT THE LOCATION SHOWN. EQUIPMENT AND MATERIALS WILL NOT BE ALLOWED ACCESS AIRFIELD PAVEMENTS.
3. NO WORK IS TO BE COMPLETED WITHIN 400' OF THE RUNWAY CENTERLINE.
4. ANY WORK TO BE COMPLETED WITHIN THE GLIDESLOPE CRITICAL AREA SHALL BE COORDINATED WITH THE AIRPORT AND THE FAA. THE CONTRACTOR SHALL GIVE THE RESIDENT ENGINEER 5 DAYS NOTIFICATION PRIOR TO COMPLETING ANY WORK IN THIS AREA.
5. PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COORDINATE ALL CONSTRUCTION ACTIVITIES WITH ILLINOIS AIR NATIONAL GUARD (ILNAG). THE CONTRACTOR SHALL COMPLY TO ALL ADDITIONAL SECURITY MEASURES IMPLEMENTED BY THE ILNAG AND SHALL CONTINUOUSLY UPDATE THEM OF THE CONSTRUCTION PROGRESS SCHEDULE. REFER TO THE SPECIAL PROVISIONS FOR A CONTACT AT THE ILNAG.
6. THE CONTRACTOR'S MAXIMUM EQUIPMENT HEIGHT FOR THE CONTRACTOR'S ACCESS AND CONTRACTOR'S STAGING AND STORAGE AREAS SHALL BE 25' MAXIMUM.
7. THE CONSTRUCTION OF THIS PHASE OF IMPROVEMENTS SHALL BE INDEPENDENT TO THE TERMINAL ENTRANCE ROAD IMPROVEMENTS.
8. COST OF TEMPORARY EDGE LIGHTING AND CABLING IN ORDER TO MAINTAIN AIRFIELD CIRCUITS WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



0 200 400
 SCALE IN FEET

LEGEND

-  AREA OF CONSTRUCTION
-  CONTRACTOR'S STAGING AND STORAGE & PARKING AREA
-  CONTRACTORS ACCESS



GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS

DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2

CONSTRUCTION ACTIVITY PLAN 5

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SHEET 27 OF 67 SHEETS	

PHASE 2 ENTRANCE ROAD & DRAINAGE/STABILIZATION IMPROVEMENTS

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CONTRACTOR ACCESS PROHIBITED AT ALL TIMES

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 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**GENERAL WAYNE A. DOWNING
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 PEORIA, ILLINOIS**

**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
 REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2**

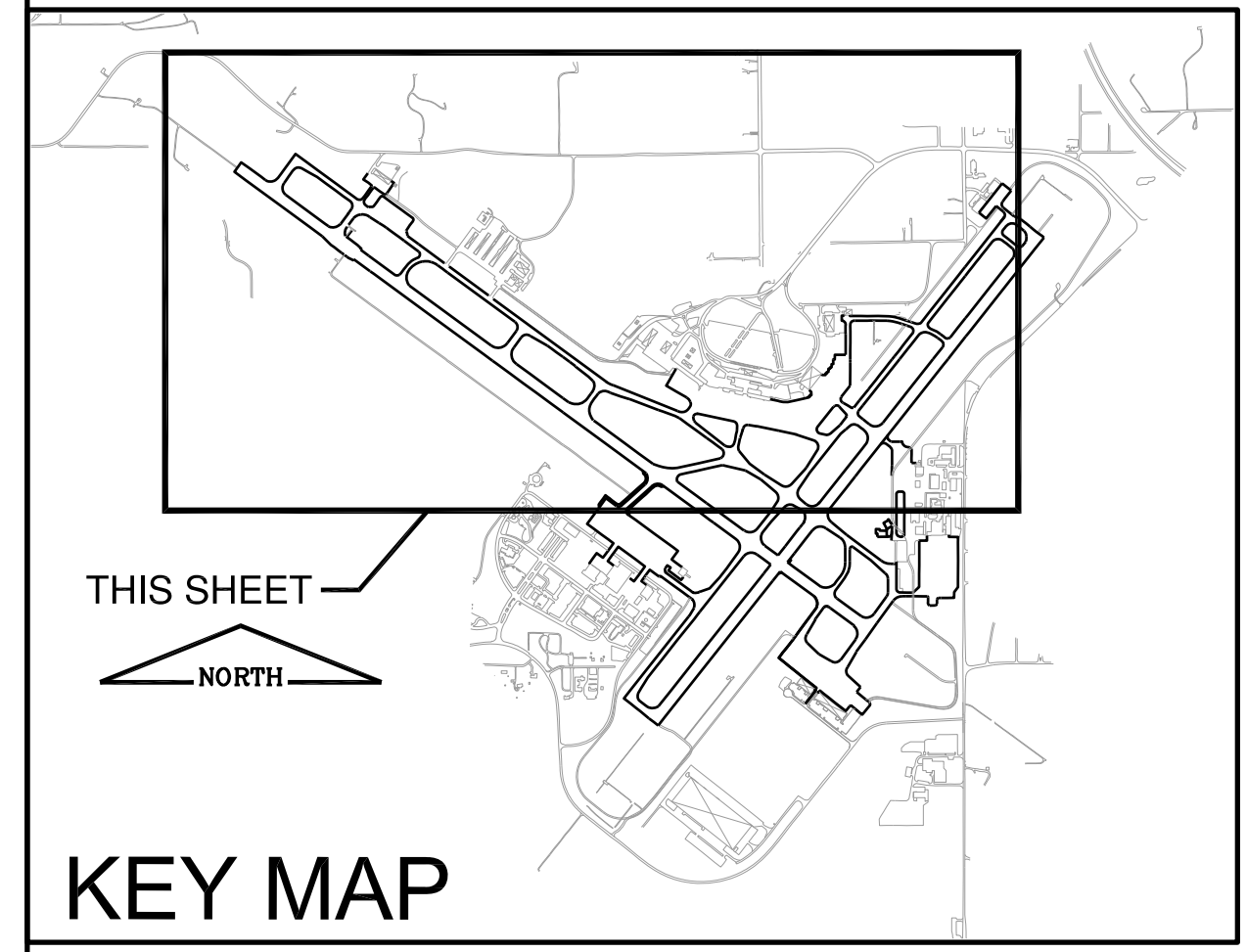
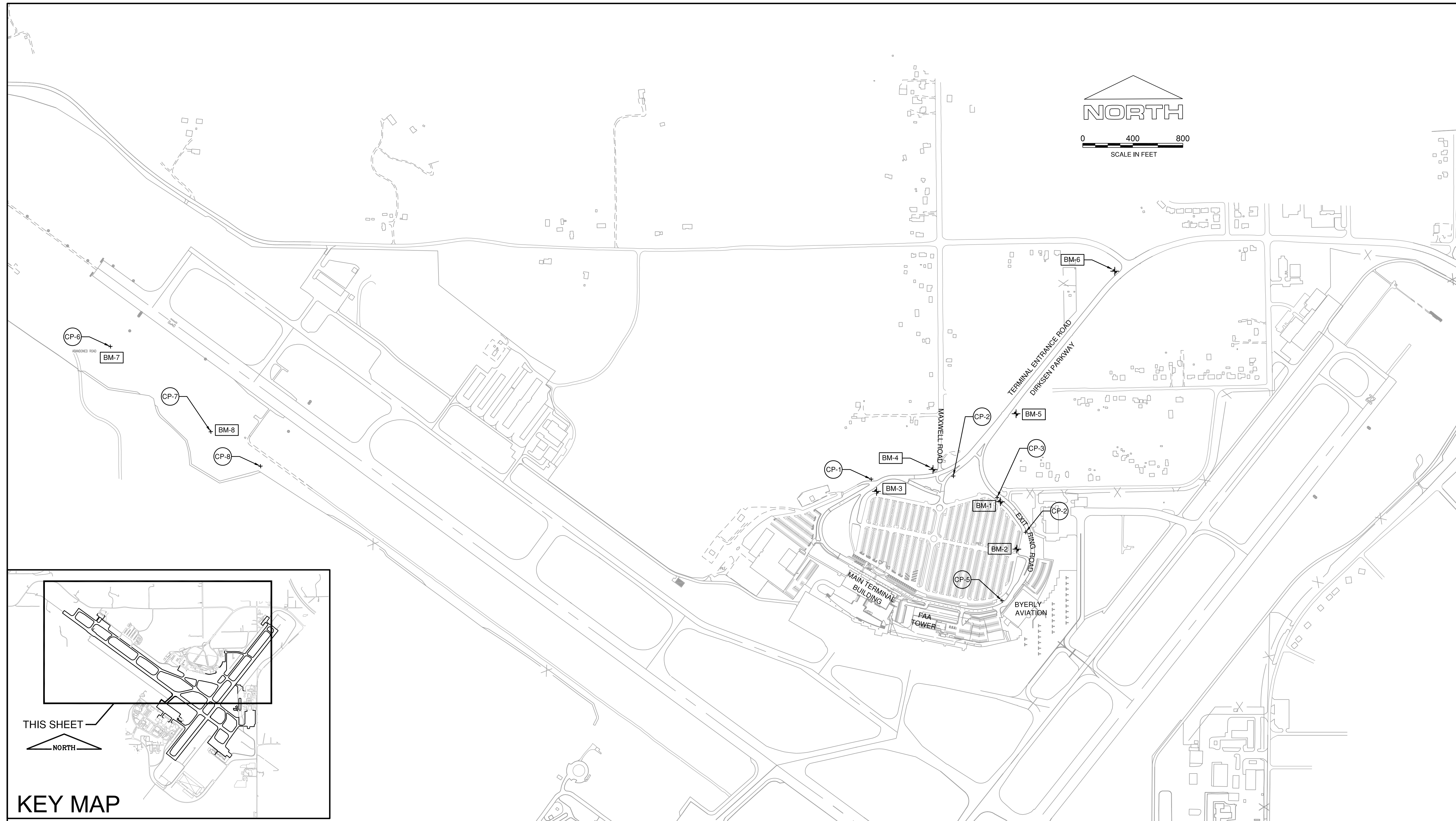
CONTROL POINTS

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AIP PROJ. NO. 3-17-0080-XX	
SHEET 28 OF 67 SHEETS	



VERTICAL CONTROL		
POINT	DESCRIPTION	ELEVATION
BM-1	HYDRANT-BOLT IN MUELLER, NORTHEAST OF BUS SHELTER H-2 & ACROSS FROM SUTLIFF ROAD, DEADEND	645.05
BM-2	HYDRANT-BOLT IN MUELLER, NORTH EAST QUAD OF 3 WAY STOP/ENTRANCE TO LONG TERM PARKING.	650.50
BM-3	HYDRANT-BOLT IN MUELLER @ NORTH END OF "E-1" PARKING & BETWEEN LIGHT POLE'S 6-30 & 6-31	636.08
BM-4	HYDRANT-BOLT IN MUELLER, NORTHWEST CORNER OF MAXWELL ROAD & AIRPORT LOOP ROAD	618.98
BM-5	CHISELED "I" ON NORTH EAST CORNER OF BOX CULVERT ON SOUTH EAST SIDE OF EVERETT MCKINLEY DIRKSEN PARKWAY	600.57
BM-6	CHISELED "I" ON EAST SIDE OF LIGHT POLE BASE. LOCATED ON WEST SIDE OF EVERETT MCKINLEY DIRKSEN PARKWAY. FIRST LIGHTPOLE SOUTH OF MIDDLE & EVERETT MCKINLEY DIRKSEN PARKWAY	641.05
BM-7	IRON PIN CONTROL POINT	632.13
BM-8	IRON PIN CONTROL POINT	616.98

HORIZONTAL CONTROL			
POINT	OBJECT	NORTHING	EASTING
CP-1	IRON PIN	1457927.647	2428355.391
CP-2	IRON PIN	1457952.852	2429010.966
CP-3	IRON PIN	1457781.181	2429360.147
CP-4	IRON PIN	1457504.504	2429588.460
CP-5	IRON PIN	1456957.732	2492399.604
CP-6	IRON PIN	1458997.828	2422278.671
CP-7	IRON PIN	1458307.000	2423080.339
CP-8	IRON PIN	1458031.603	2423477.733

LEGEND

CP-1 CONTROL POINT

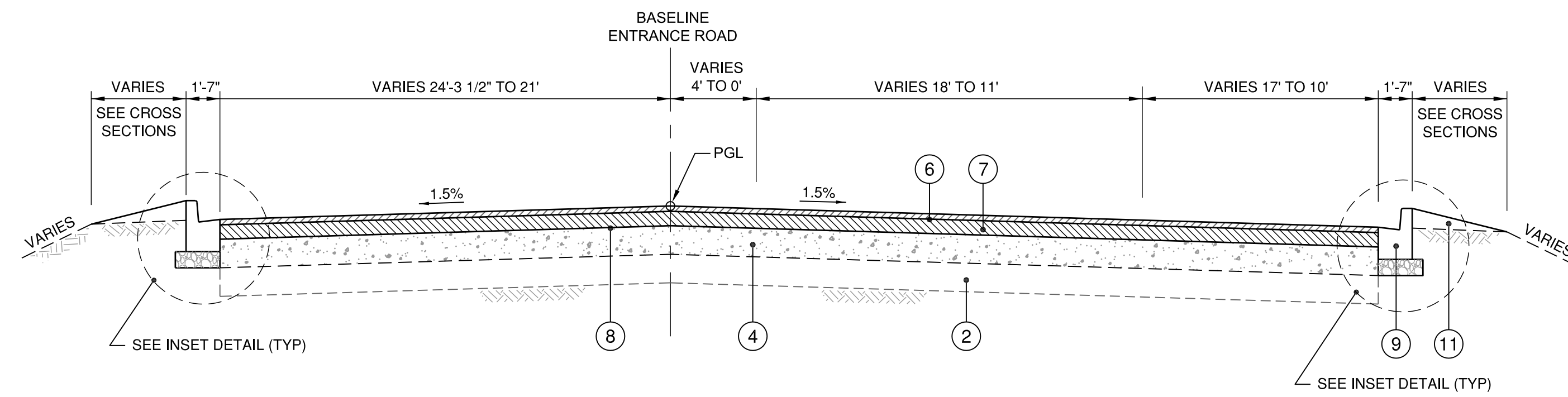
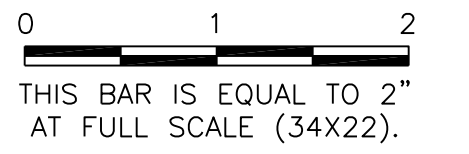
BM-1 BENCHMARK

**PHASE 2 ENTRANCE ROAD
 & DRAINAGE/STABILIZATION IMPROVEMENTS**

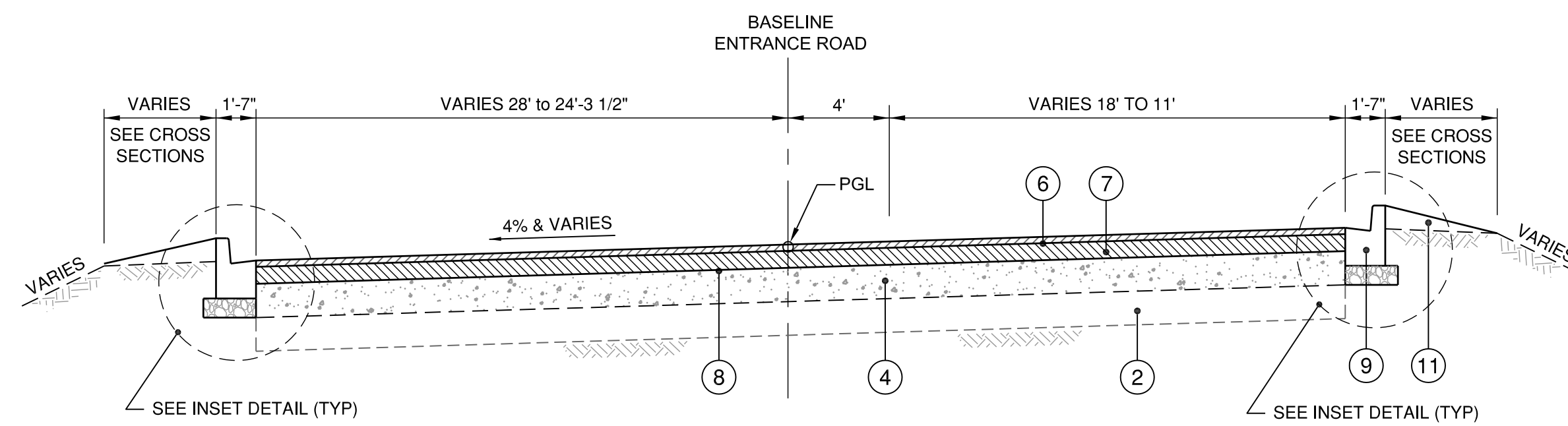
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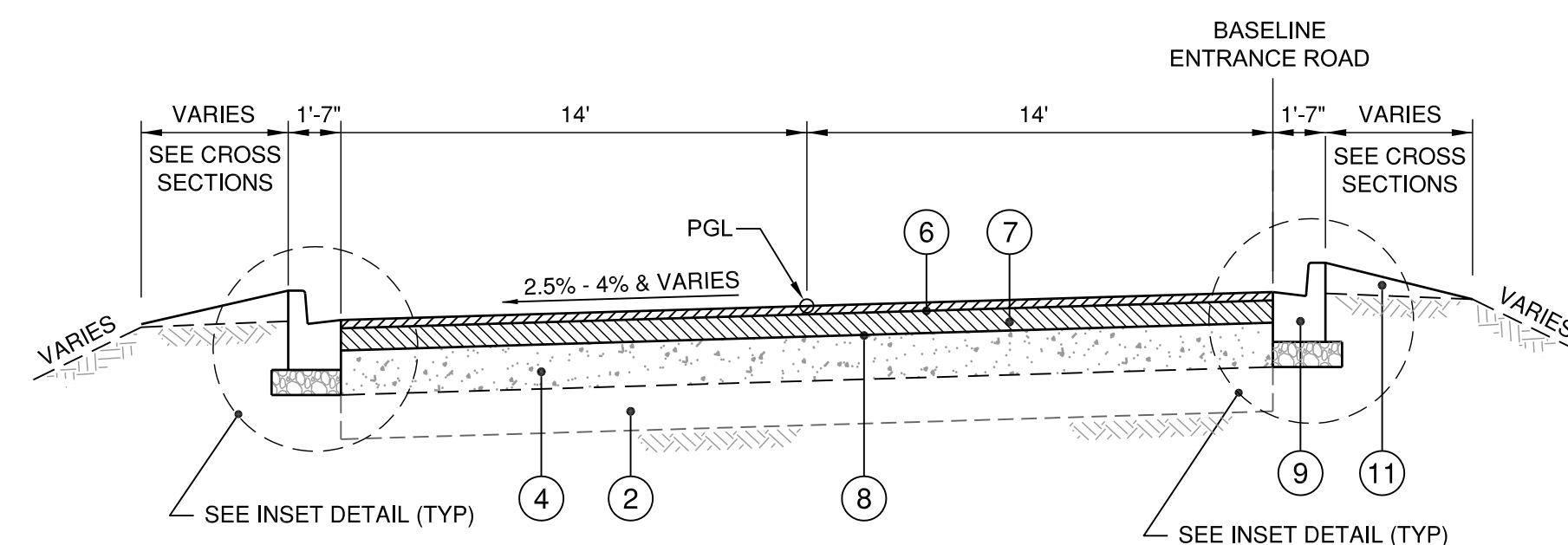
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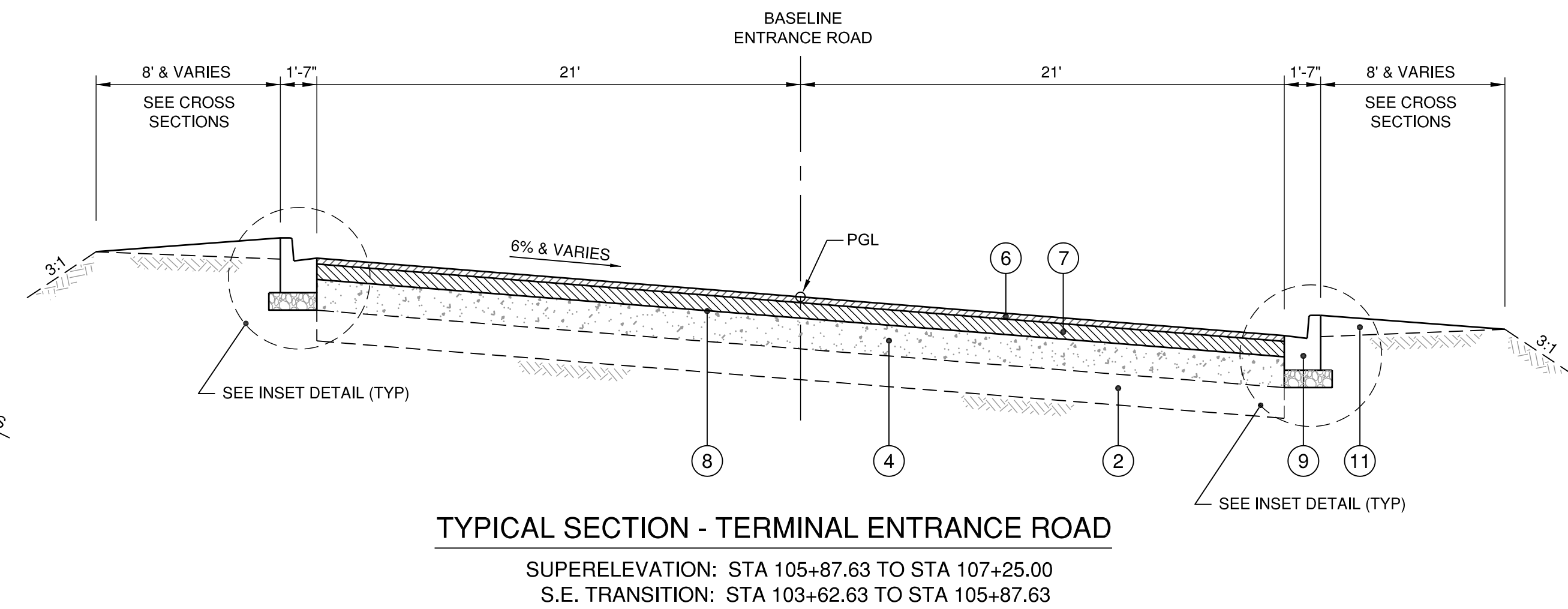
TYPICAL SECTION - TERMINAL ENTRANCE ROAD
 TANGENT: STA 90+47.10 TO STA 94+87.23



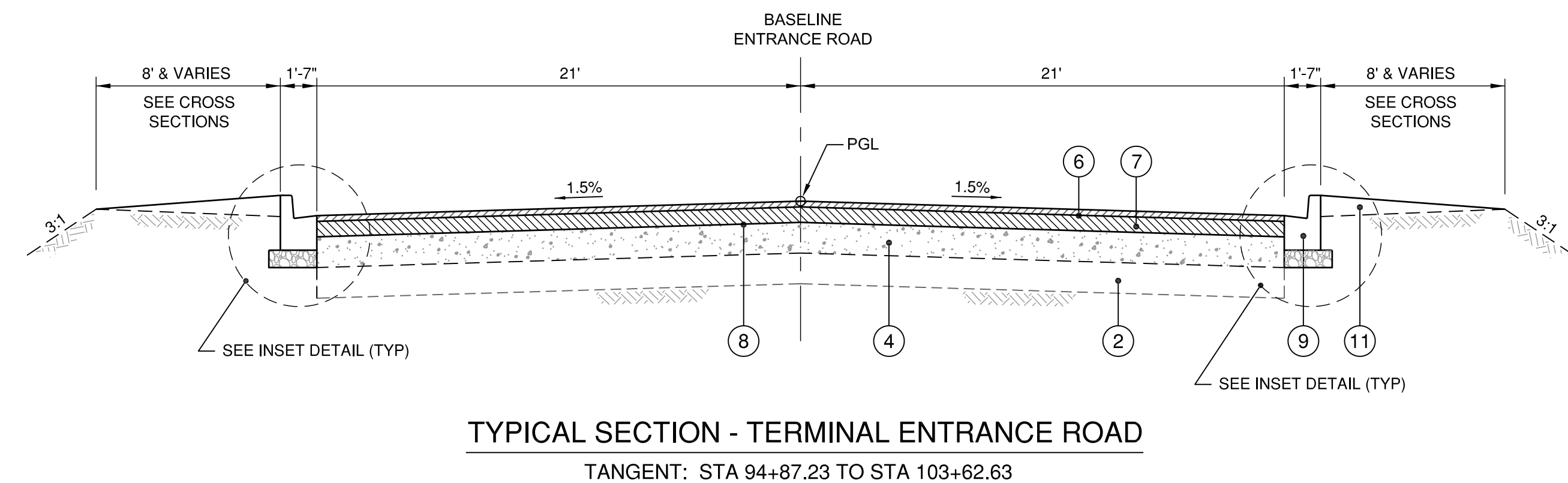
TYPICAL SECTION - TERMINAL ENTRANCE ROAD
 TANGENT: STA 90+38.67 TO STA 90+47.10
 S.E. TRANSITION: STA 88+78.67 TO STA 90+38.67
 SUPERELEVATION: STA 87+68.43 TO STA 88+78.67



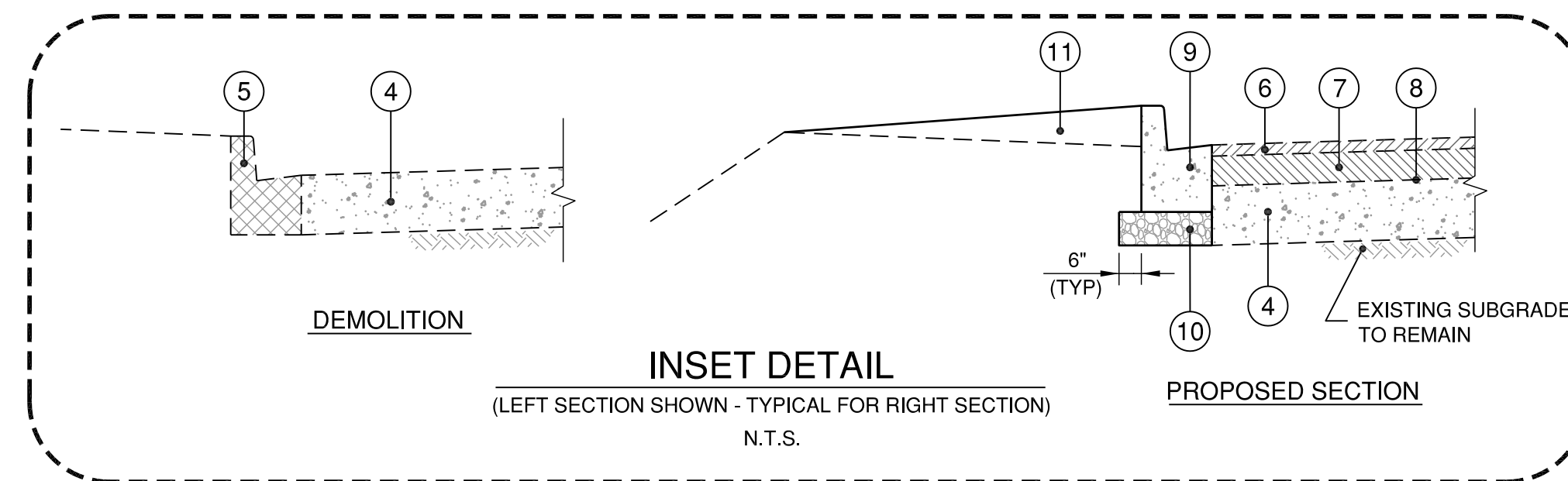
TYPICAL SECTION - TERMINAL ENTRANCE ROAD
 SUPERELEVATION: STA 86+00.00 TO STA 87+68.43
 S.E. TRANSITION: STA 85+00.00 TO STA 86+00.00
 TANGENT: STA 83+47.17 TO STA 85+00.00



TYPICAL SECTION - TERMINAL ENTRANCE ROAD
 SUPERELEVATION: STA 105+87.63 TO STA 107+25.00
 S.E. TRANSITION: STA 103+62.63 TO STA 105+87.63



TYPICAL SECTION - TERMINAL ENTRANCE ROAD
 TANGENT: STA 94+87.23 TO STA 103+62.63



GENERAL NOTES:

1. TYPICAL SECTIONS SHOWN AT 1:2 (H:V) SCALE UNLESS NOTED OTHERWISE.
2. ALL MATERIALS SHALL MEET OR EXCEED THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS, LATEST EDITION.
3. REFER TO SITEWORK SPECIFICATIONS FOR SUBGRADE PREPARATION.
4. CONTRACTOR SHALL PROVIDE A PRIME COAT AND TACK COAT TO GUTTER FLAG WHERE IT CONTACTS ASPHALT PAVING. APPLY TACK COAT AT 0.05 GAL/SY BETWEEN ASPHALT LIFTS.
5. TYPES AND LOCATIONS OF PAVEMENT JOINTS SHALL CONFORM TO SITEWORK SPECIFICATIONS AND CONSTRUCTION DETAILS.

KEYNOTES:

- 1 EXISTING 8" NON-REINFORCED CONCRETE PAVEMENT
- 2 EXISTING LIME-MODIFIED SUBGRADE
- 3 EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- 4 RUBBLIZED 8" CONCRETE PAVEMENT (REFER TO SPECIAL PROVISIONS)
- 5 CURB & GUTTER REMOVAL (MONOLITHIC WHERE APPLICABLE)
- 6 PROPOSED HMA SURFACE COURSE, MIX 'D' N70, 1.5"
- 7 PROPOSED HMA BINDER COURSE, IL-19 N70, 4"
- 8 BITUMINOUS BASE COURSE, LEVELING (AS NEEDED FOR GRADE)
- 9 PROPOSED COMB. CONCRETE CURB & GUTTER, TYPE B-6.12
- 10 PROPOSED AGGREGATE BASE COURSE, TYPE B, 4.5" (NOMINAL)
- 11 TOPSOIL WEDGE WITH SEEDING CLASS 2, MULCH METHOD 2
- 12 OFFSITE TOPSOIL, 4"
- 13 PROPOSED HMA BINDER COURSE, IL-19 N70, 9.5"

GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
TYPICAL SECTIONS 1

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DRAWN BY:	JFM
CHECKED BY:	SKS
APPROVED BY:	CET
DATE:	JULY 11, 2014
JOB No:	13061-05
IL PROJ. NO. PIA-4415	
AIP PROJ. NO. 3-17-0080-XX	

PE098

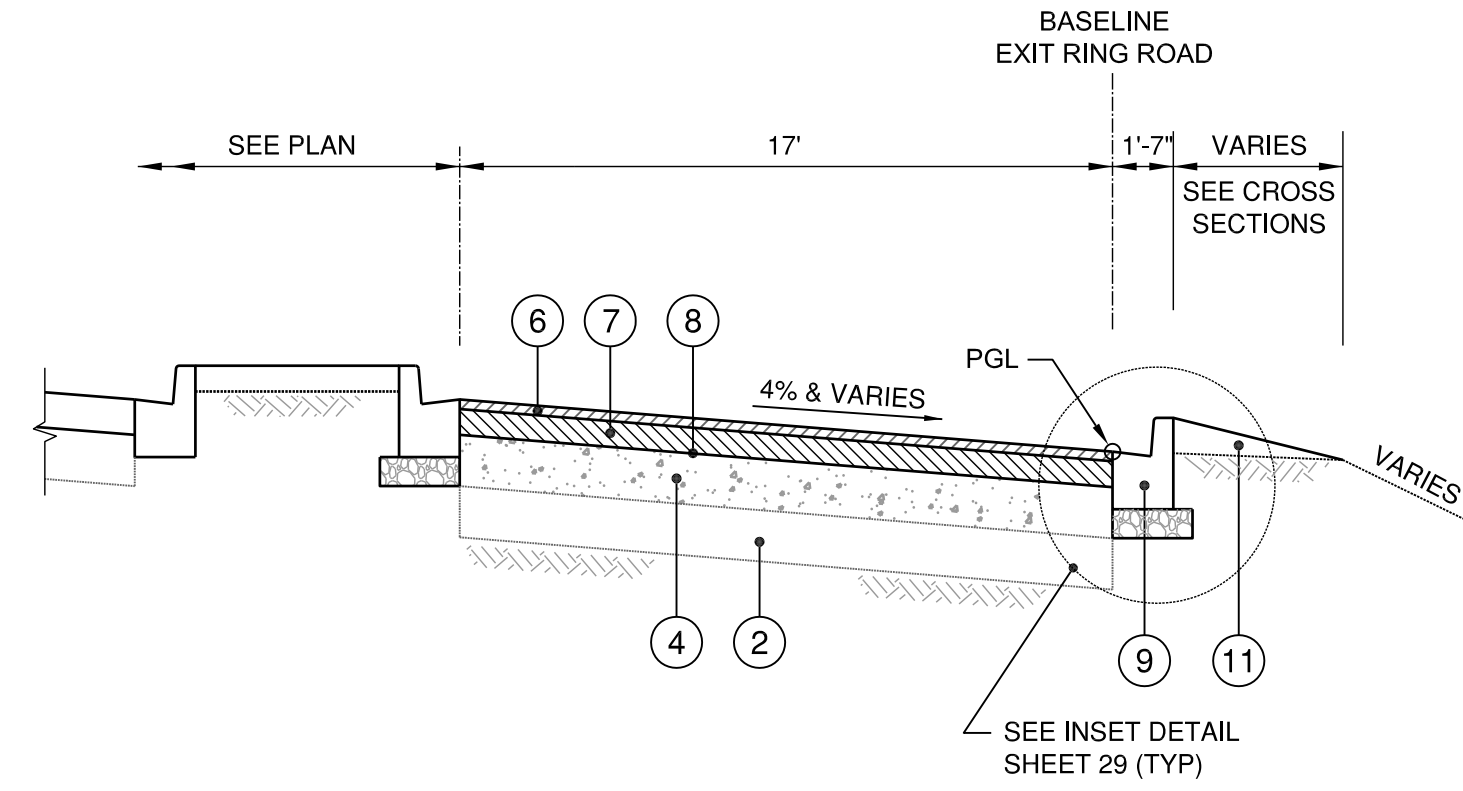
REVISIONS		
NUMBER	BY	DATE

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 AT FULL SCALE (34X22).

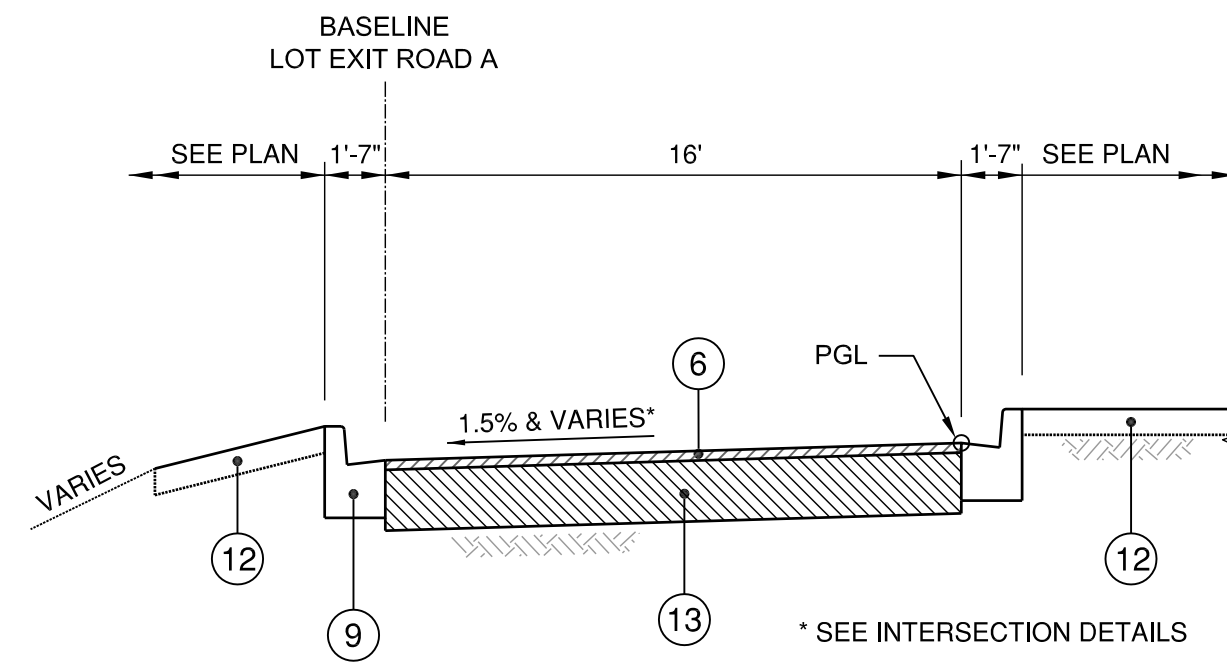
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DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
TYPICAL SECTIONS 2

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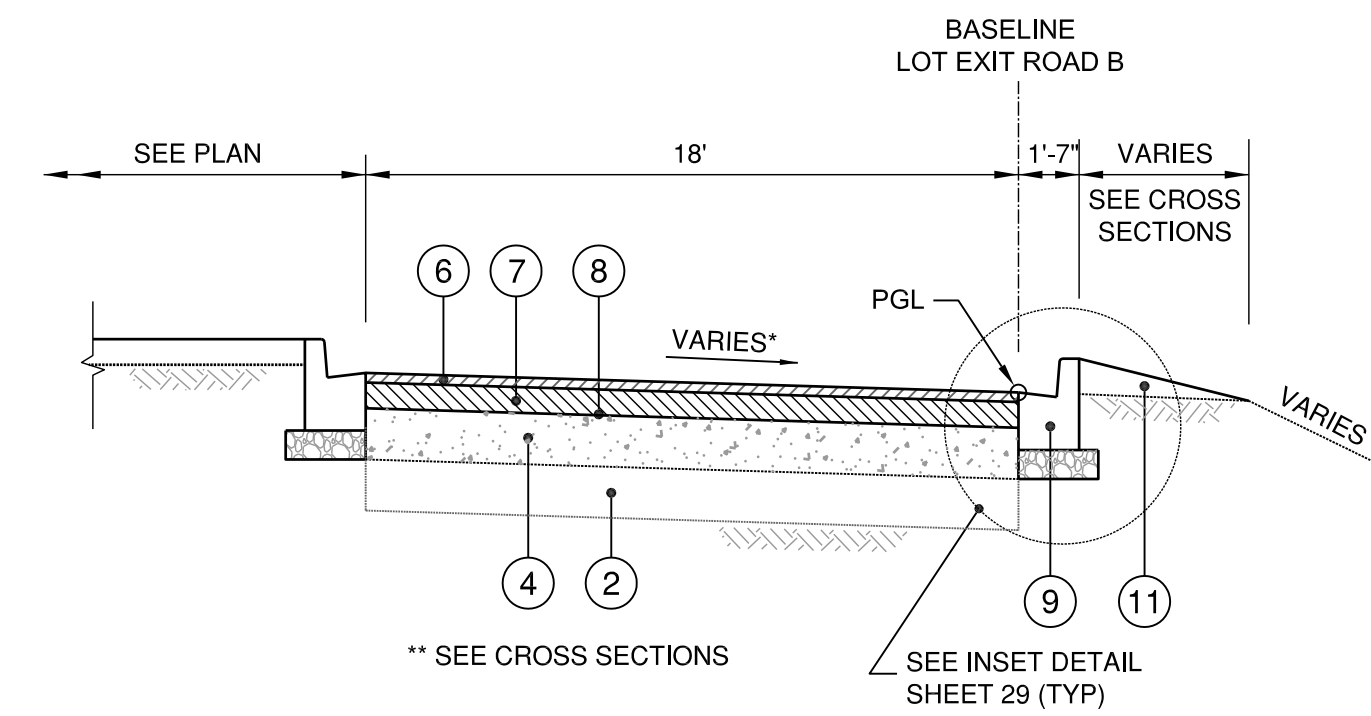
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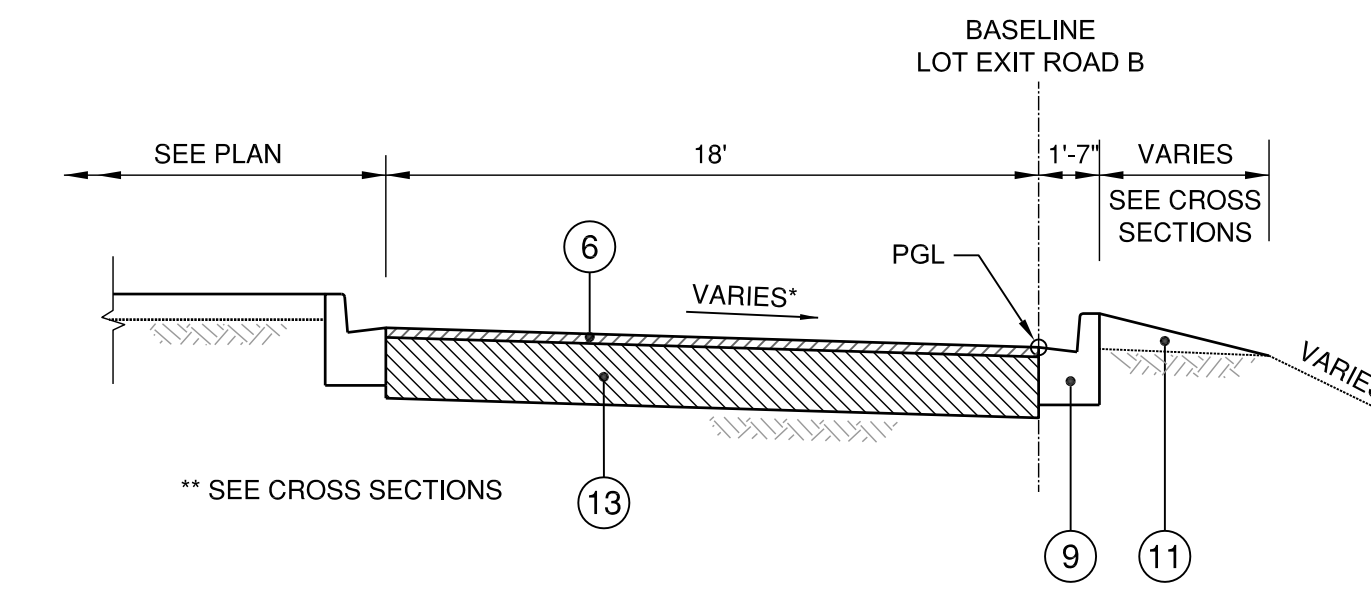
TYPICAL SECTION - EXIT RING ROAD
 STA 189+00.32 TO STA 190+44.00



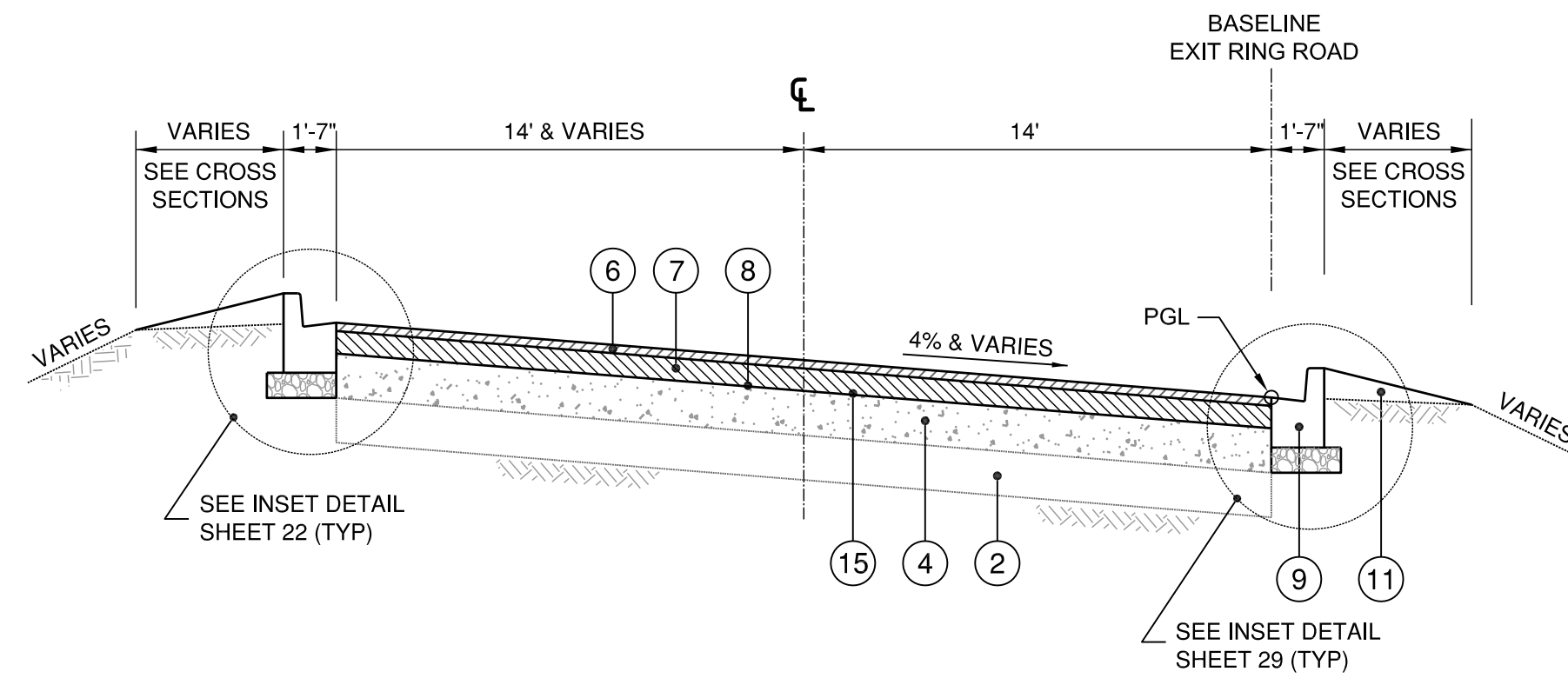
TYPICAL SECTION - LOT EXIT ROAD A
 STA 500+00.00 TO STA 501+32.10



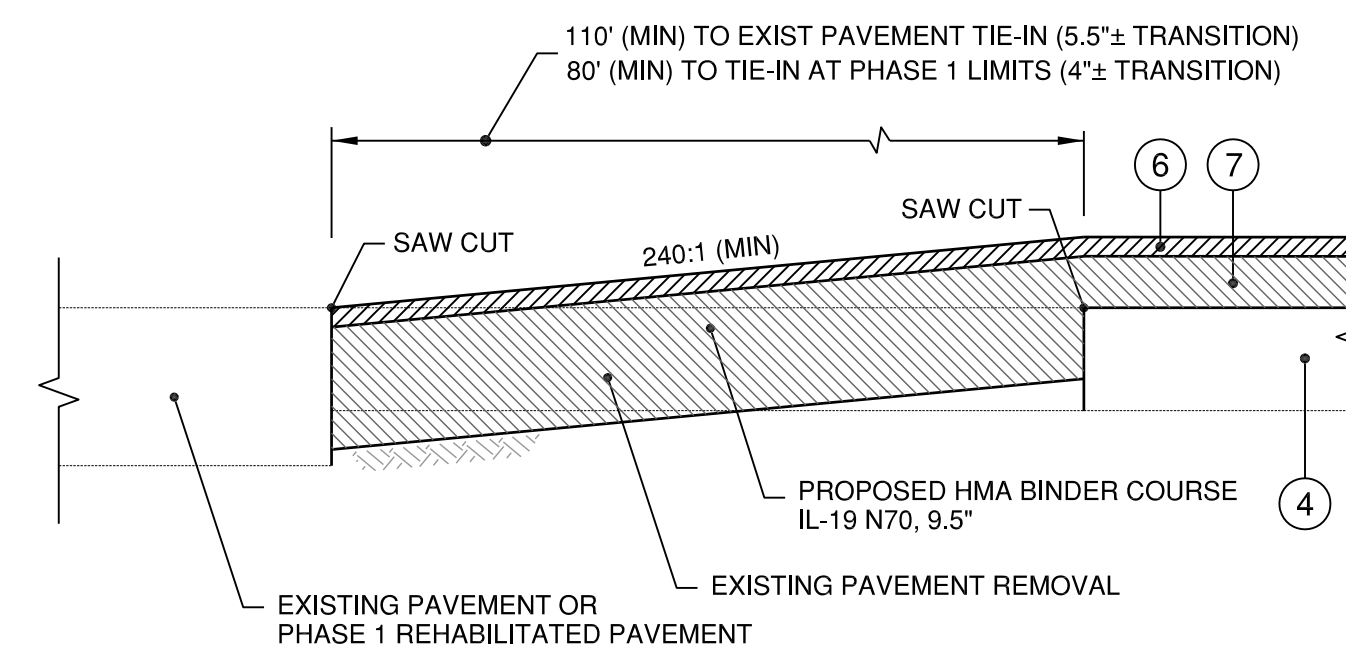
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 STA 285+85.31 TO STA 287+60.42



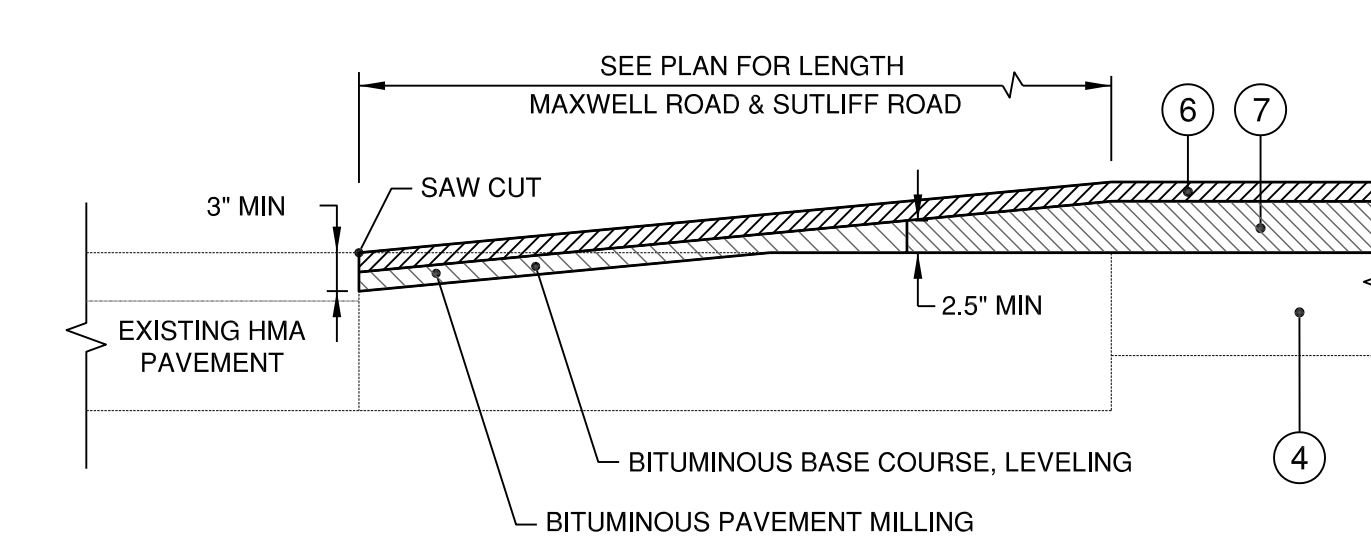
TYPICAL SECTION - LOT EXIT ROAD B
 STA 284+75.31 TO STA 285+85.31



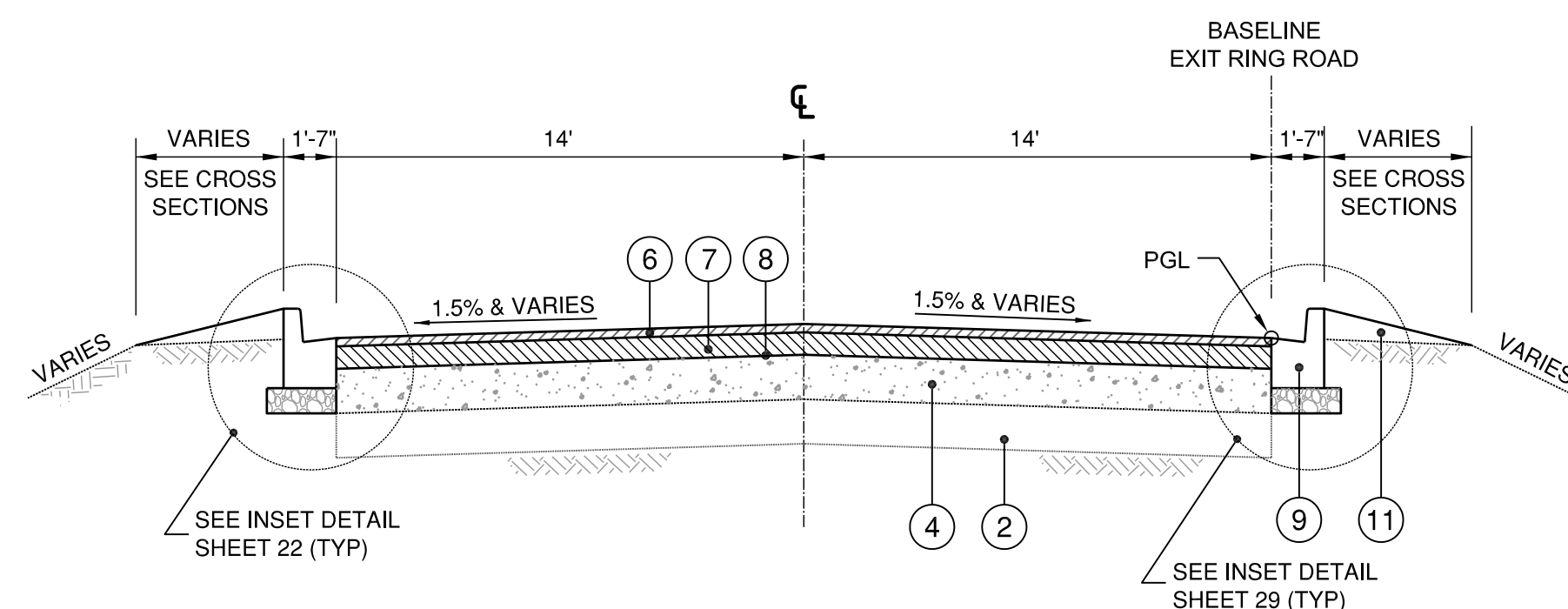
TYPICAL SECTION - EXIT RING ROAD
 STA 188+47.51 TO STA 189+00.32



BUTT-JOINT DETAIL (CONC PAVEMENT)
 N.T.S.



HMA SURFACE REMOVAL BUTT-JOINT DETAIL (ENTRANCE PAVEMENT)
 N.T.S.



TYPICAL SECTION - EXIT RING ROAD
 STA 183+71.94 TO STA 188+47.51

GENERAL NOTES:

1. TYPICAL SECTIONS SHOWN AT 1:2 (H:V) SCALE UNLESS NOTED OTHERWISE.
2. ALL MATERIALS SHALL MEET OR EXCEED THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS, LATEST EDITION.
3. REFER TO SITWORK SPECIFICATIONS FOR SUBGRADE PREPARATION.
4. CONTRACTOR SHALL PROVIDE A TACK COAT TO GUTTER FLAG WHERE IT CONTACTS ASPHALT PAVING AND BETWEEN BITUMINOUS LIFTS. APPLY TACK COAT AT 0.05 GAL/SY.
5. TYPES AND LOCATIONS OF PAVEMENT JOINTS SHALL CONFORM TO SITWORK SPECIFICATIONS AND CONSTRUCTION DETAILS.

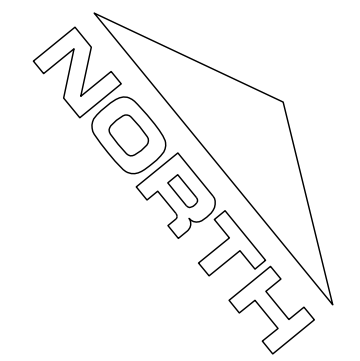
KEYNOTES:

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13. PROPOSED HMA BINDER COURSE, IL-19 N70, 9.5"

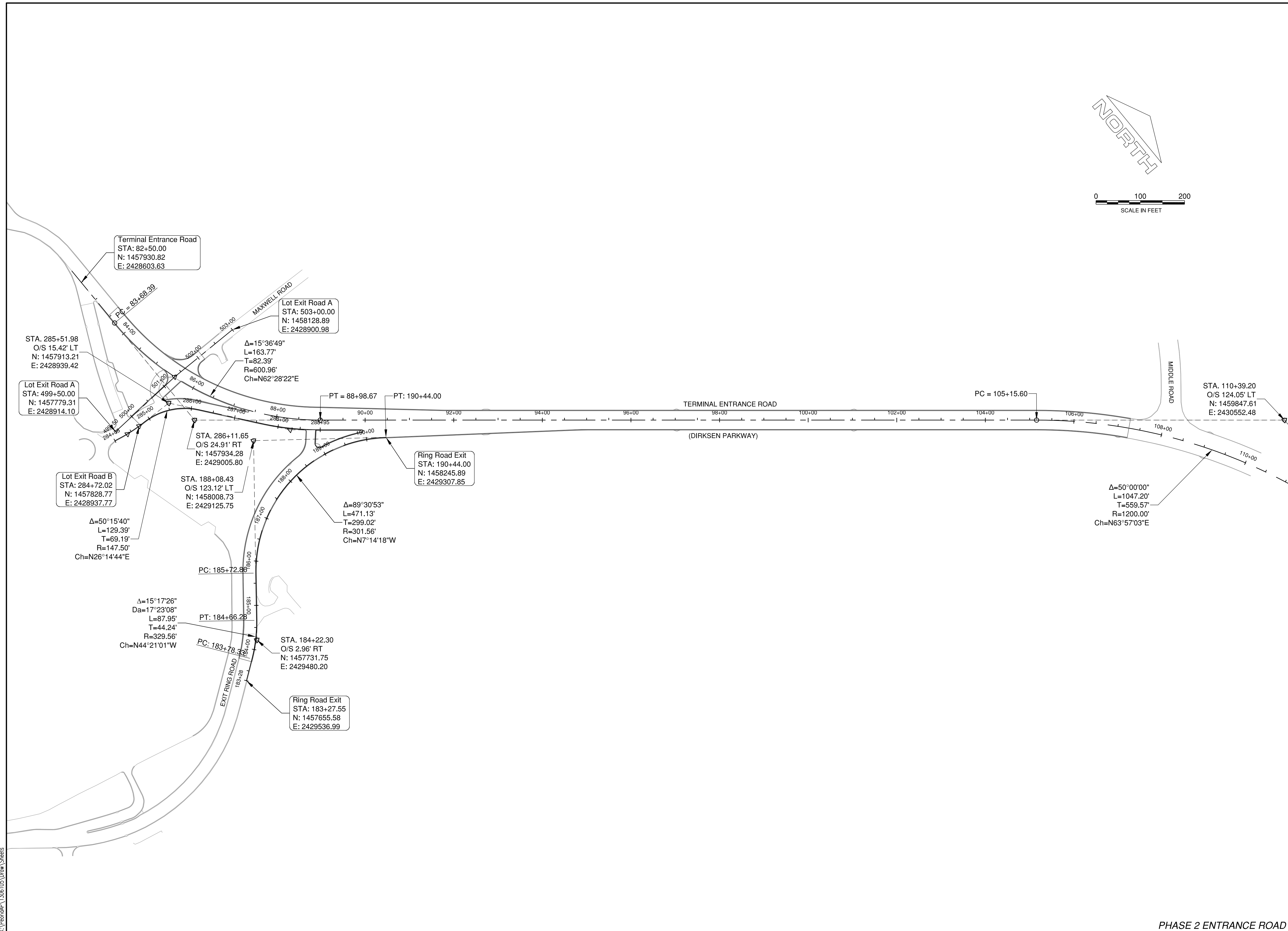
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 PEORIA, ILLINOIS**

**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
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DRAWN BY:	JMW
CHECKED BY:	JFM
APPROVED BY:	CET
DATE:	JULY 11, 2014
JOB No:	13061-05
IL PROJ. NO.	PIA-4415
AIP PROJ. NO.	3-17-0080-XX

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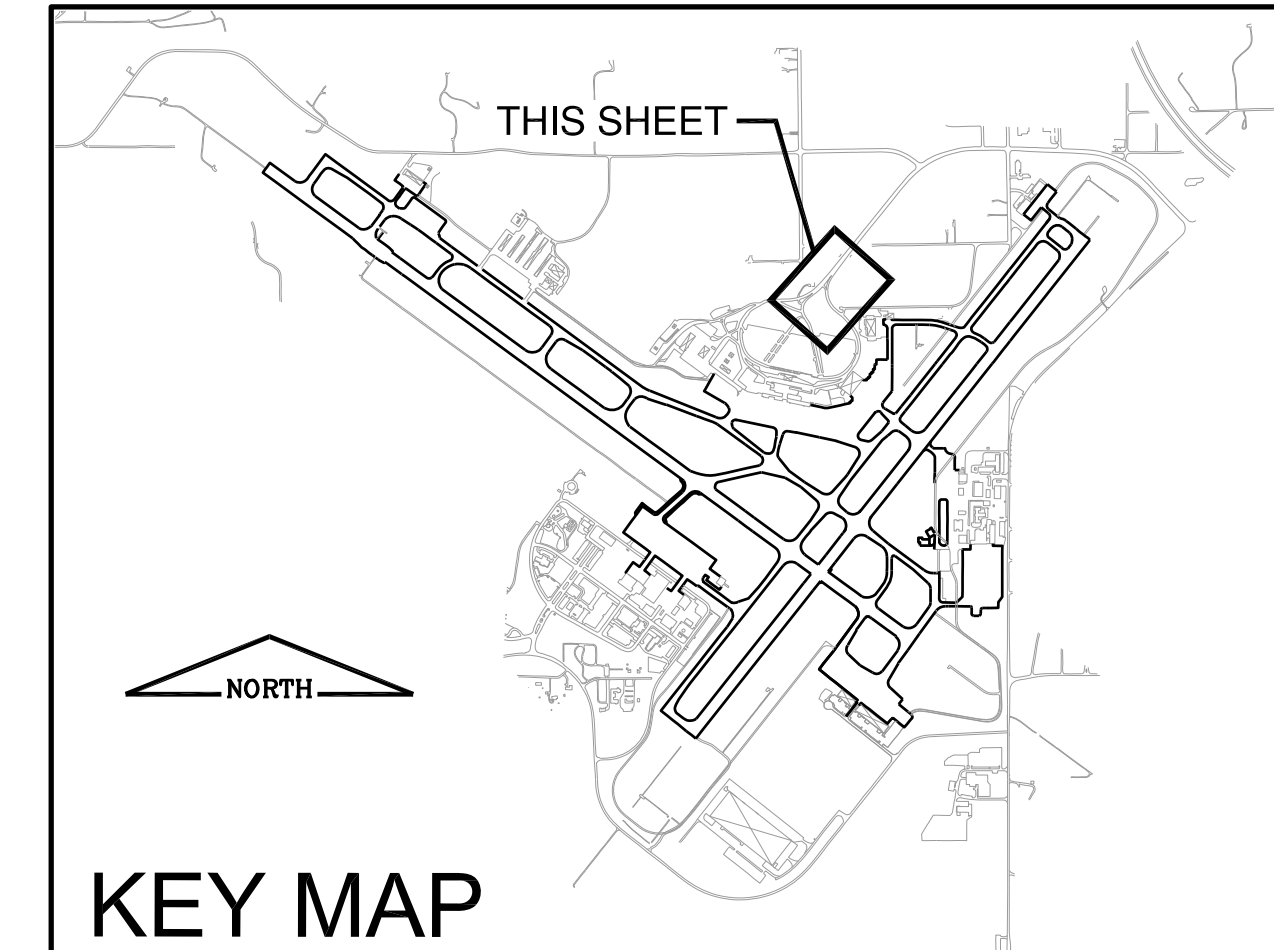
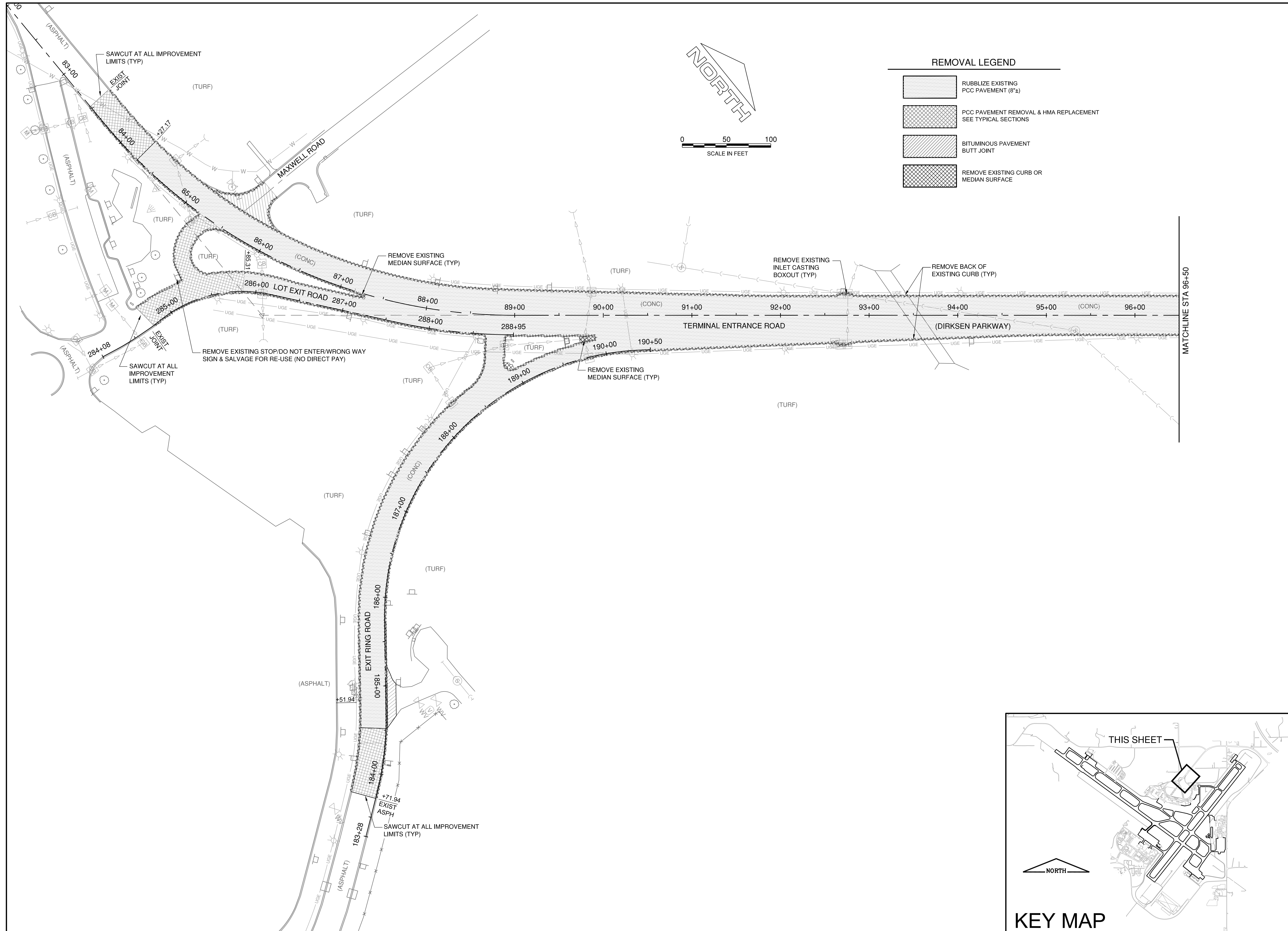
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 EXISTING CONDITIONS AND REMOVALS 1**

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IL PROJ. NO.	PIA-4415
AIP PROJ. NO.	3-17-0080-XX
SHEET	32 OF 67 SHEETS



REMOVAL LEGEND

- RUBBLIZE EXISTING PCC PAVEMENT (8"±)
- PCC PAVEMENT REMOVAL & HMA REPLACEMENT SEE TYPICAL SECTIONS
- BITUMINOUS PAVEMENT BUTT JOINT
- REMOVE EXISTING CURB OR MEDIAN SURFACE

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**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
 REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
 EXISTING CONDITIONS AND REMOVALS 2**

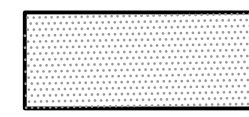
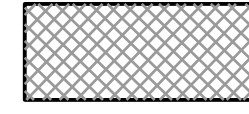
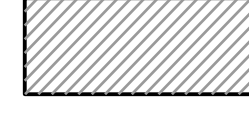
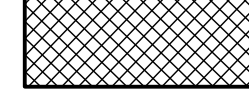
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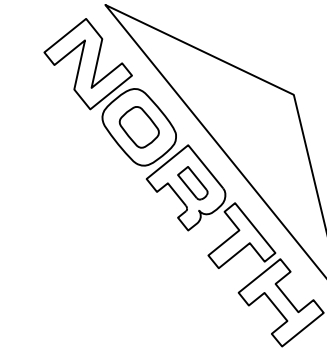
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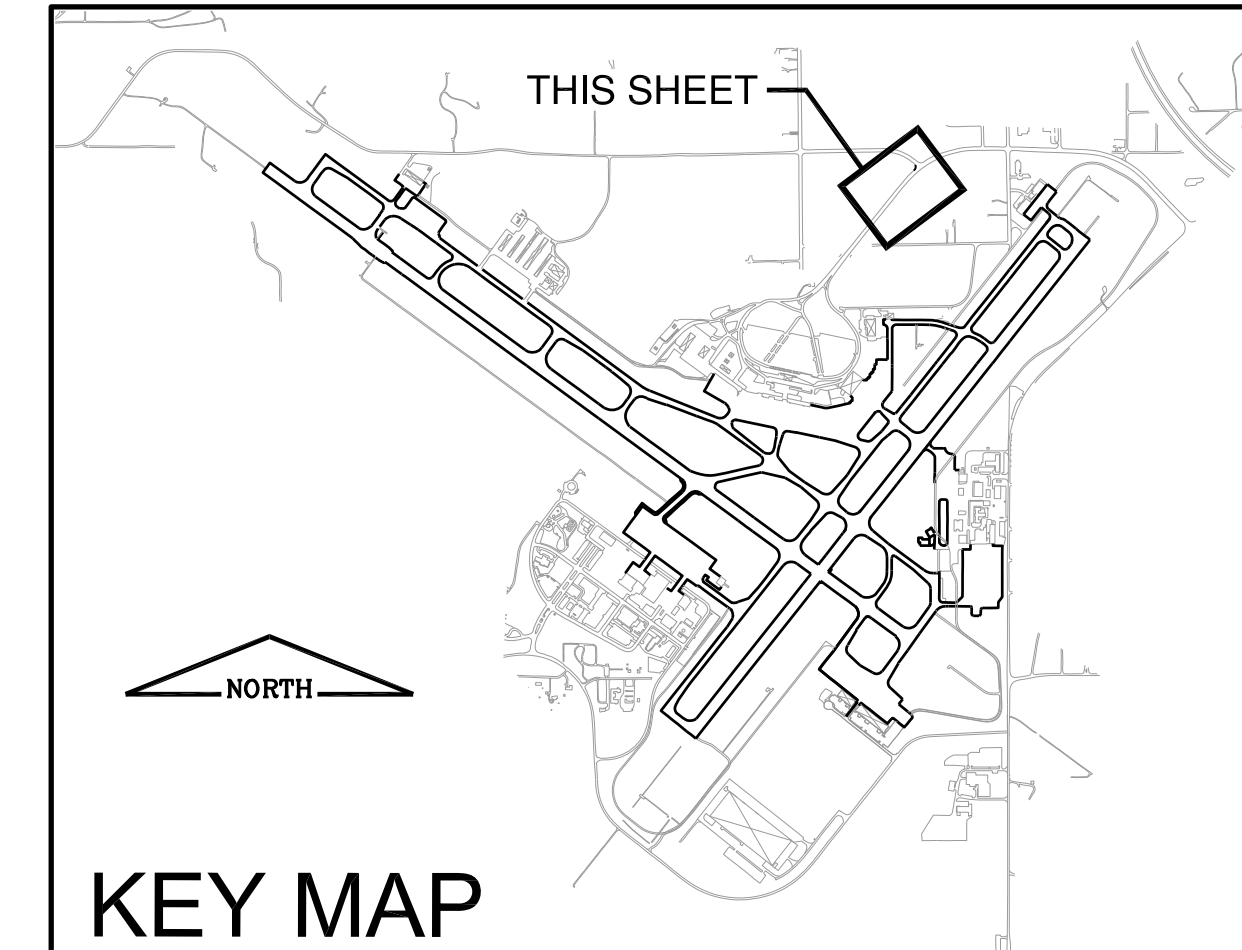
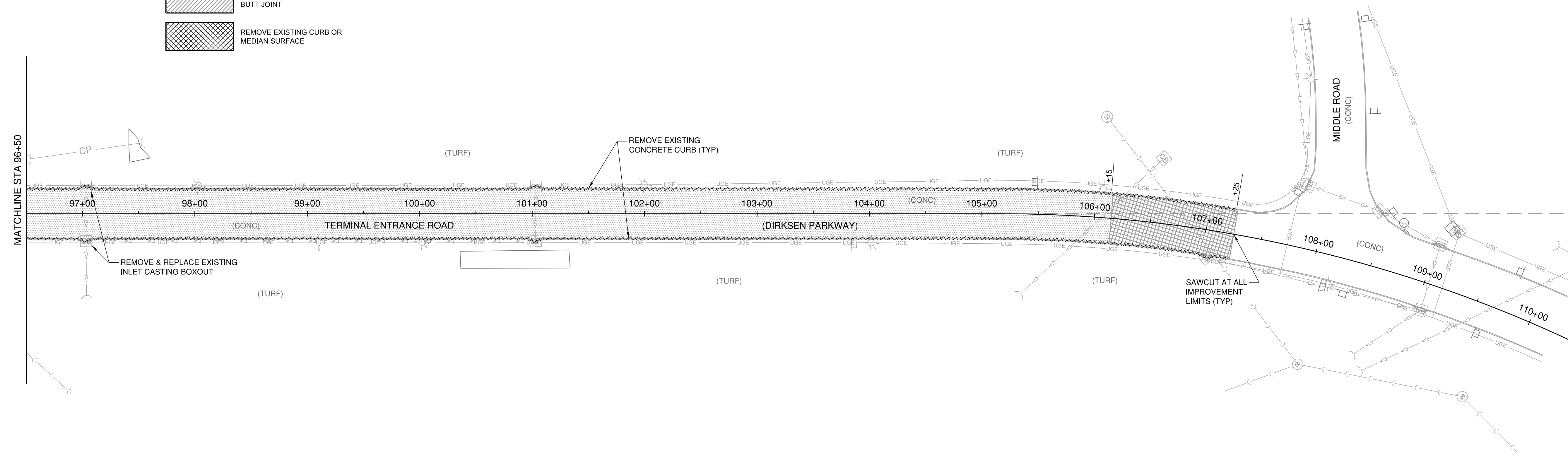
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JOB No:	13061-05
IL PROJ. NO. PIA-4415	
AIP PROJ. NO. 3-17-0080-XX	
SHEET 33 OF 67 SHEETS	

REMOVAL LEGEND

-  RUBBLIZE EXISTING PCC PAVEMENT (8'±)
-  PCC PAVEMENT REMOVAL & HMA REPLACEMENT SEE TYPICAL SECTIONS
-  BITUMINOUS PAVEMENT BUTT JOINT
-  REMOVE EXISTING CURB OR MEDIAN SURFACE

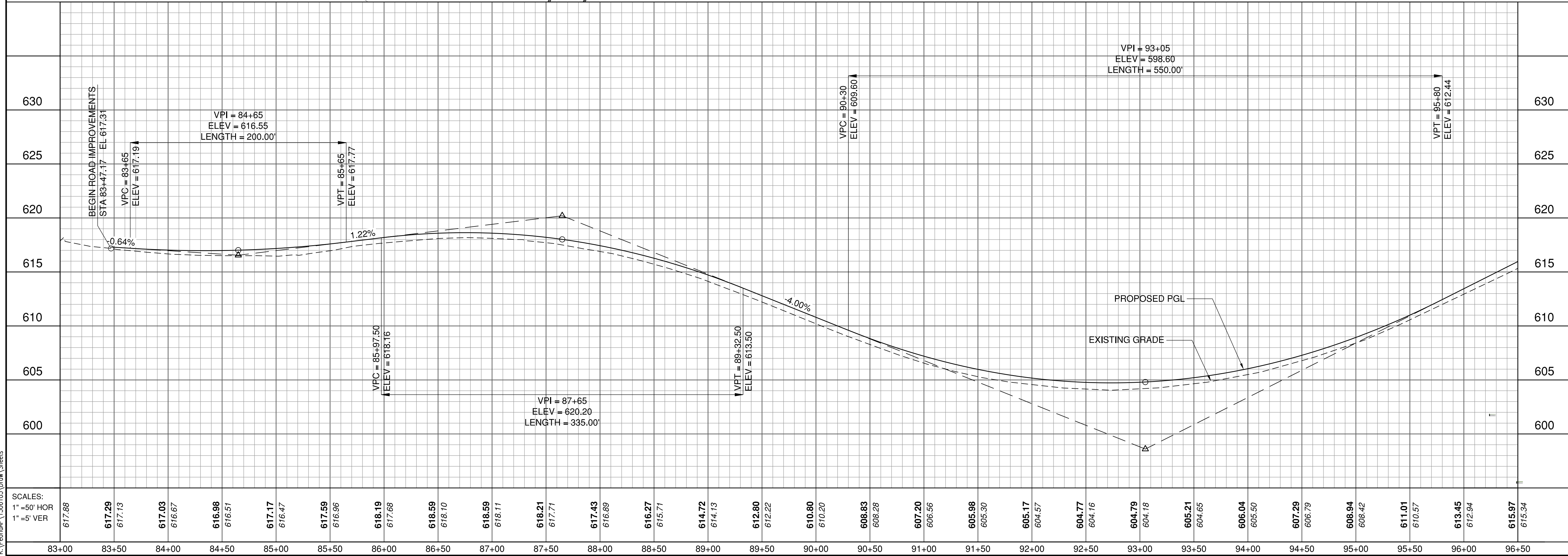
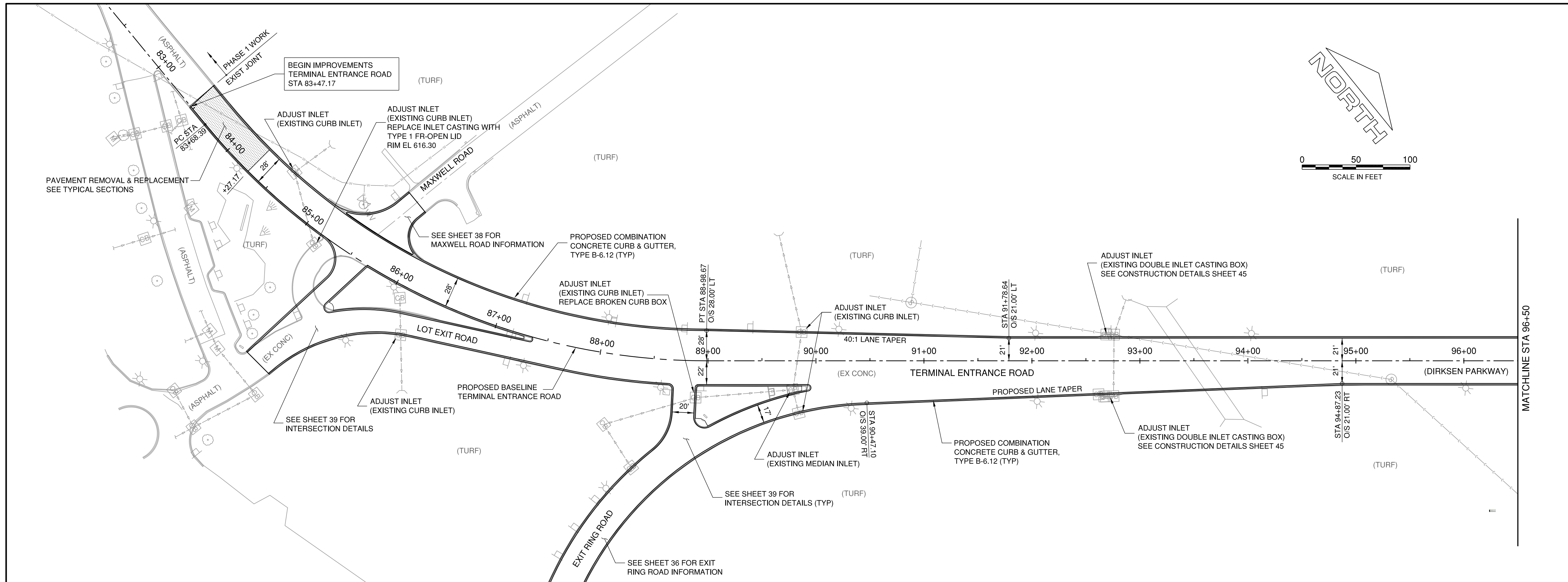
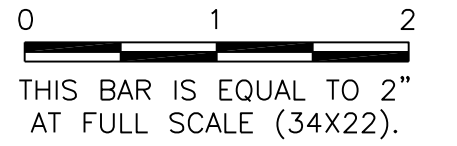


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 PEORIA INTERNATIONAL AIRPORT
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**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
 REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2**

PLAN AND PROFILE - ENTRANCE ROAD 1

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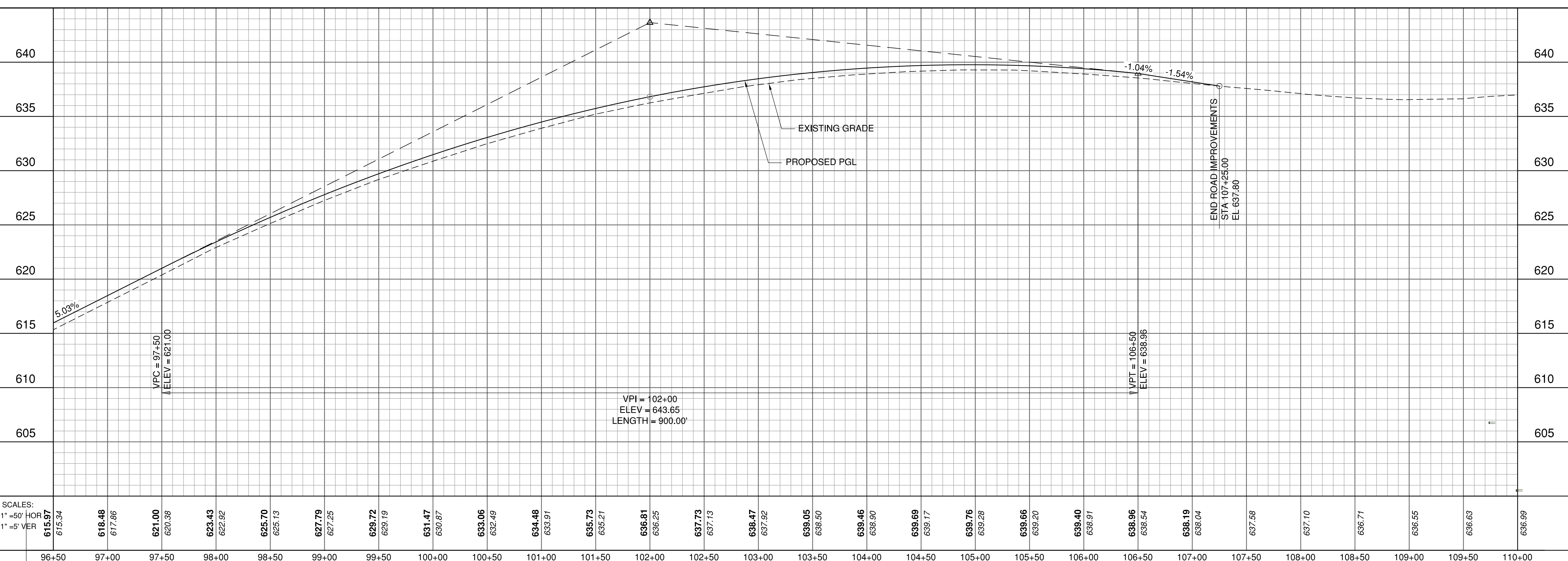
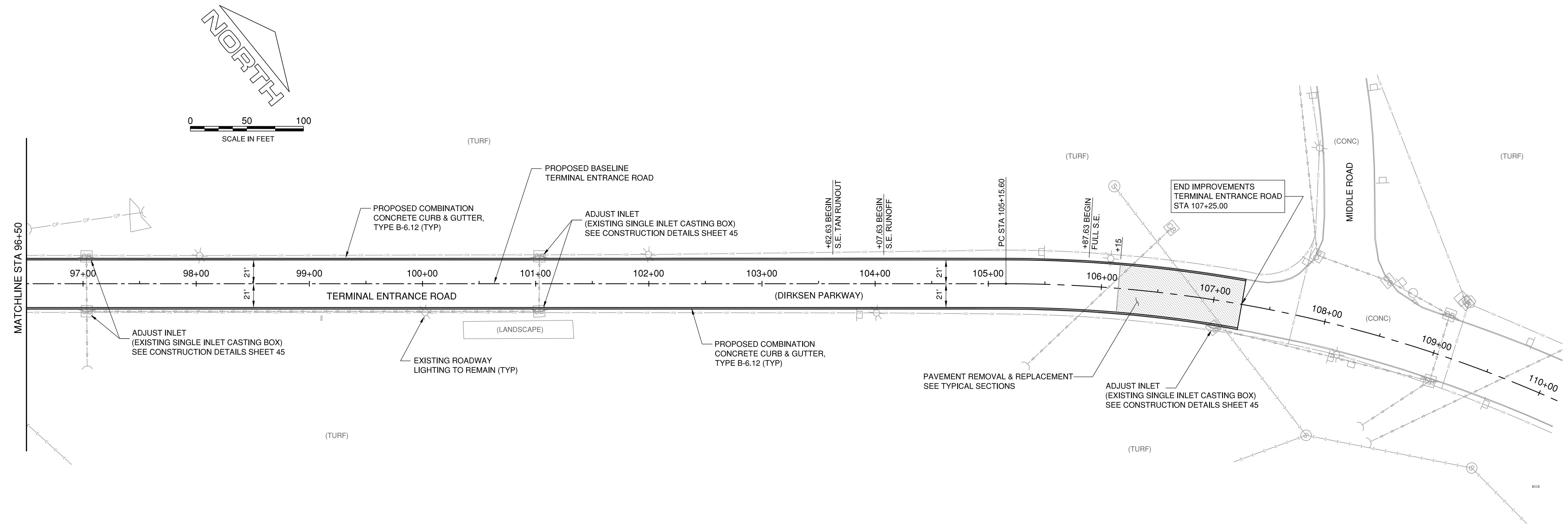
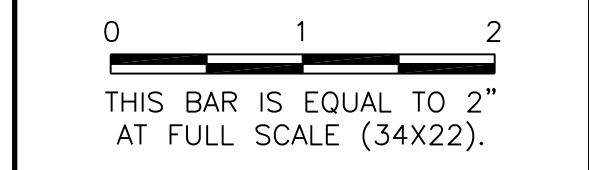
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SHEET	34 OF 67 SHEETS

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STATION	ELEVATION
96+50	615.97
97+00	617.86
97+50	621.00
98+00	623.43
98+50	625.70
99+00	627.79
99+50	629.72
100+00	631.47
100+50	633.06
101+00	634.48
101+50	635.73
102+00	636.81
102+50	637.73
103+00	638.47
103+50	639.05
104+00	639.46
104+50	639.69
105+00	639.76
105+50	639.66
106+00	639.40
106+50	638.96
107+00	638.19
107+50	637.58
108+00	637.10
108+50	636.71
109+00	636.55
109+50	636.63
110+00	636.99

GENERAL WAYNE A. DOWNING
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PEORIA, ILLINOIS

DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2

PLAN AND PROFILE - ENTRANCE ROAD 2

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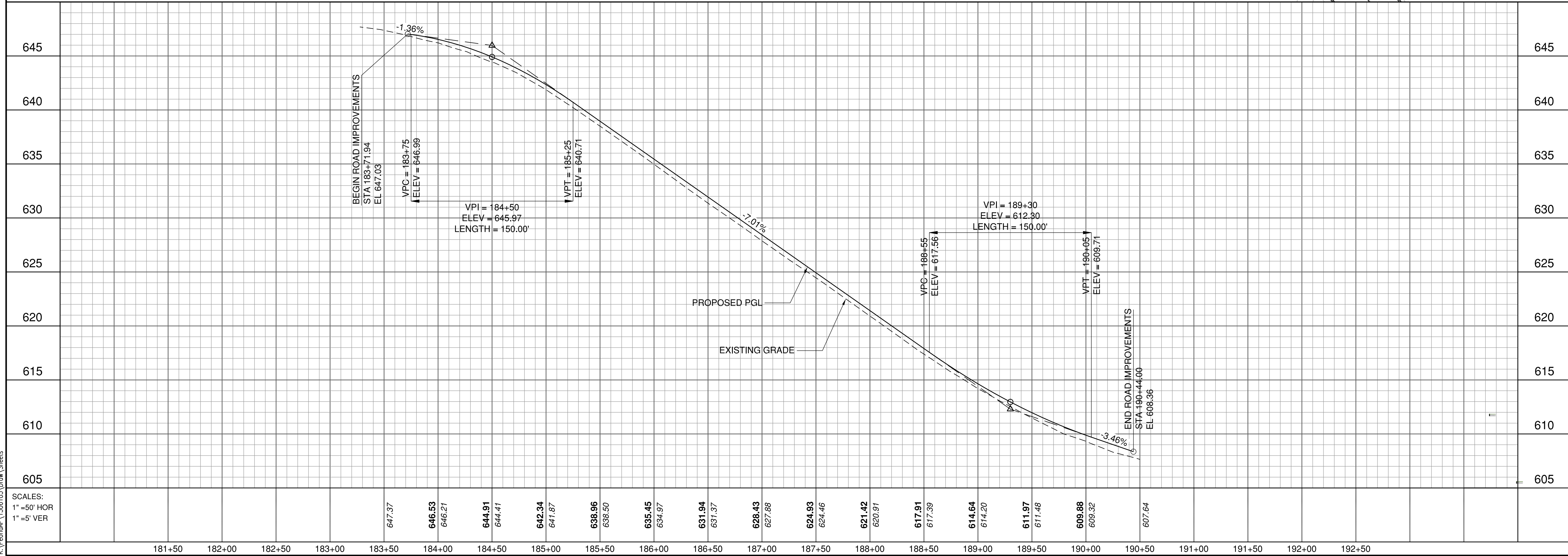
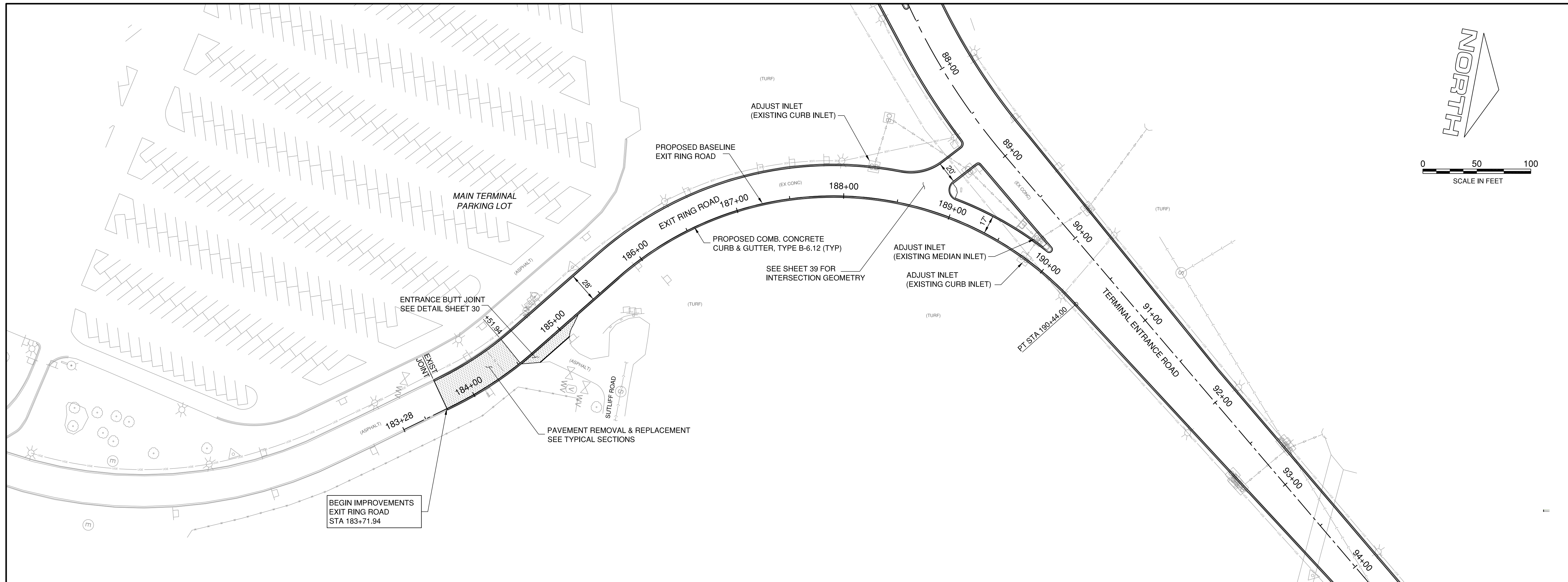
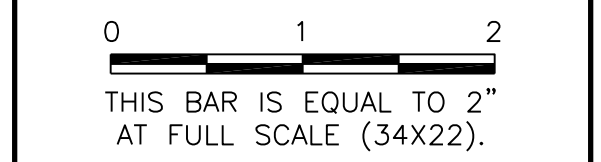
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SCALES:
 1" = 50' HOR
 1" = 5' VER

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 PEORIA, ILLINOIS**

**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
 REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2**

PLAN AND PROFILE - EXIT RING ROAD

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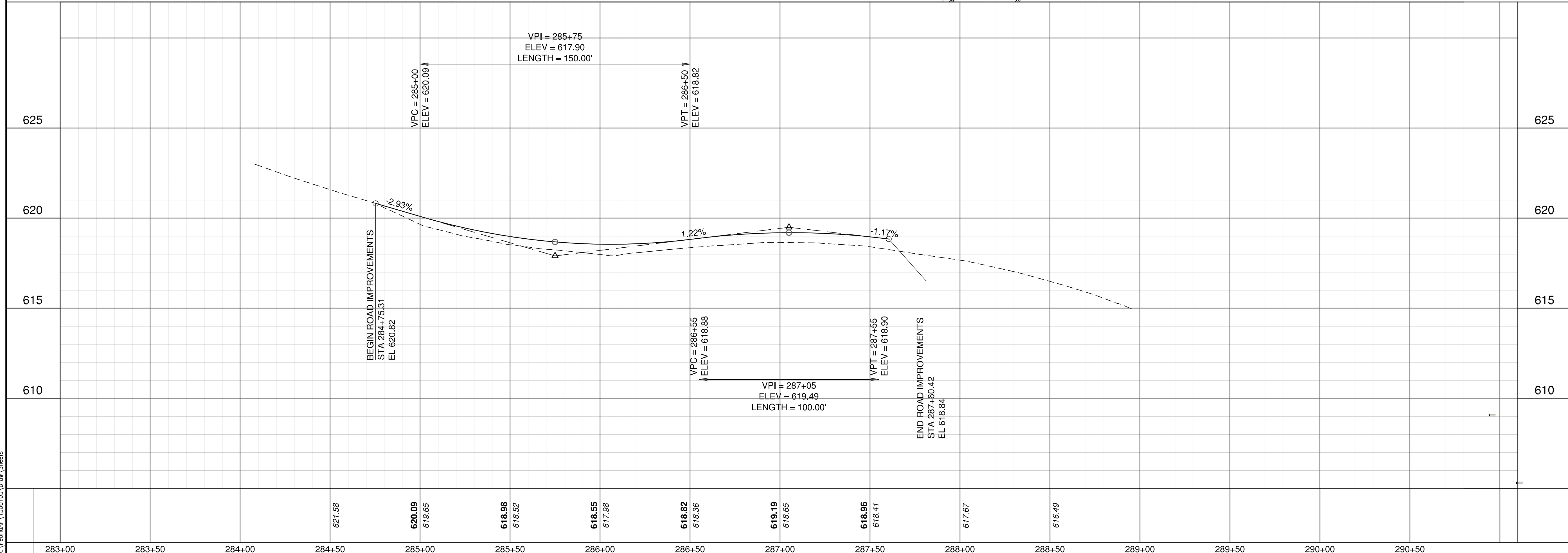
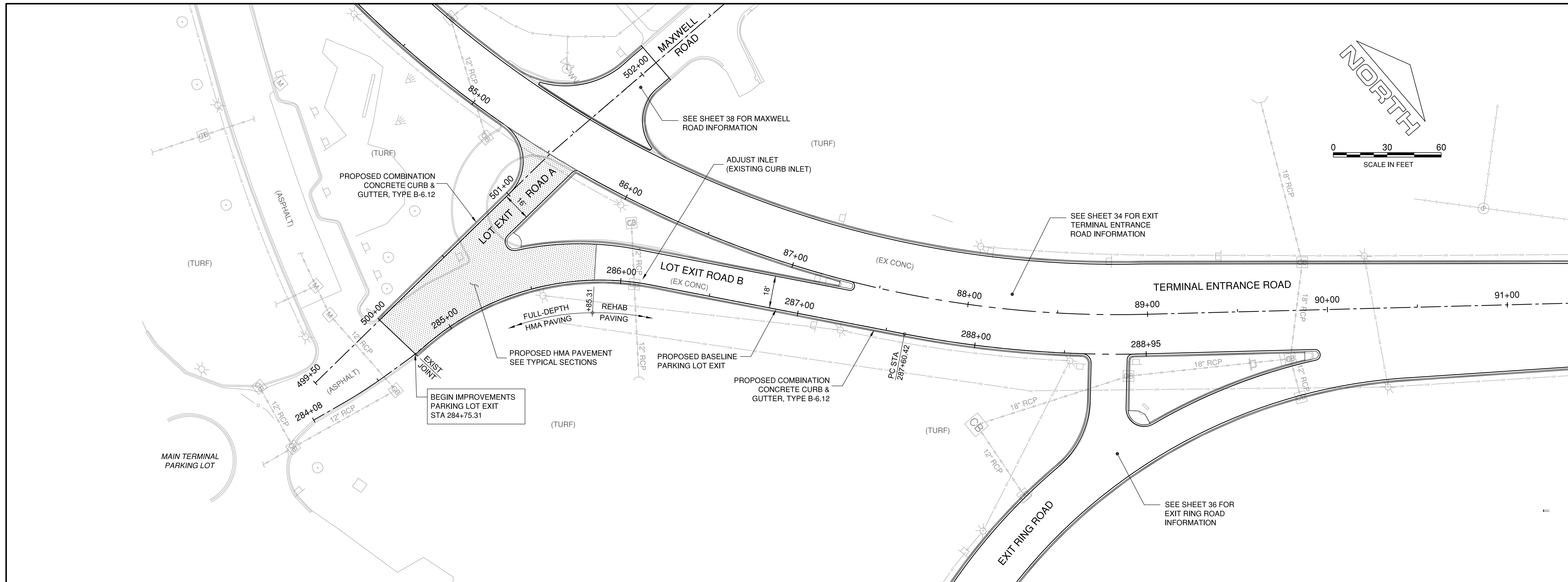
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GENERAL WAYNE A. DOWNING
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PEORIA, ILLINOIS

DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2

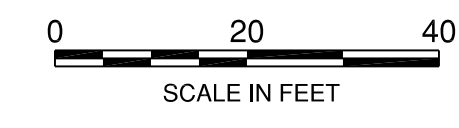
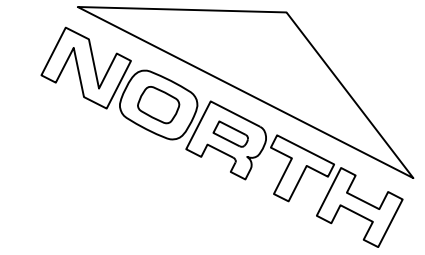
PLAN AND PROFILE - PARKING LOT EXIT B

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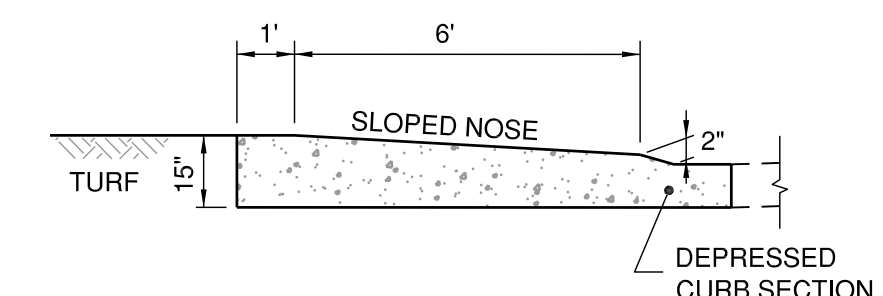
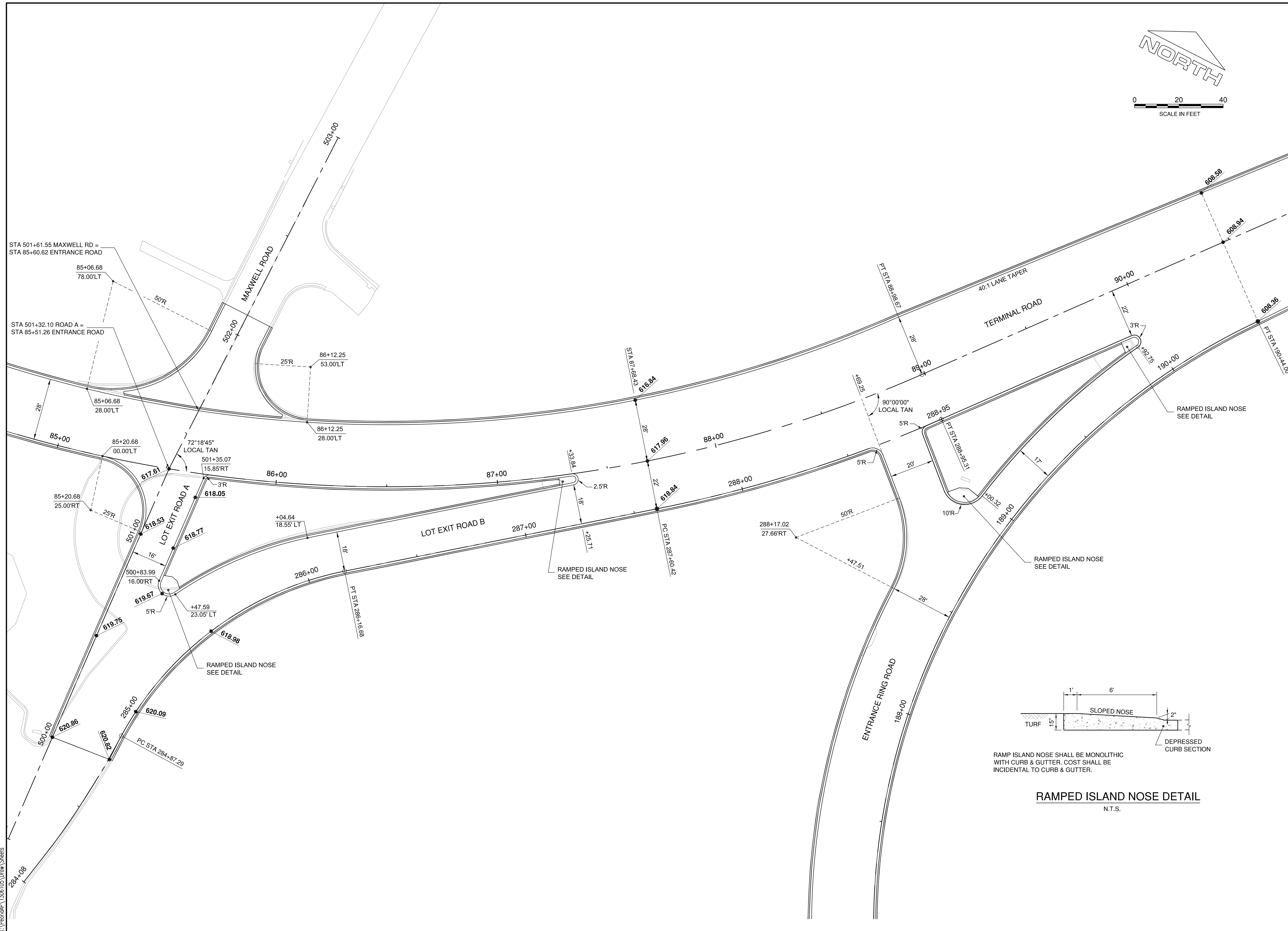
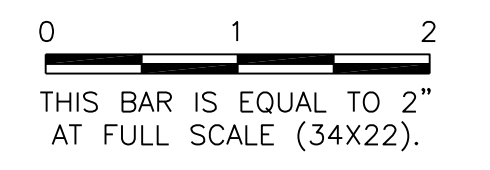
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RAMP ISLAND NOSE SHALL BE MONOLITHIC WITH CURB & GUTTER. COST SHALL BE INCIDENTAL TO CURB & GUTTER.

RAMPED ISLAND NOSE DETAIL
 N.T.S.

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
 REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2**

INTERSECTION DETAIL

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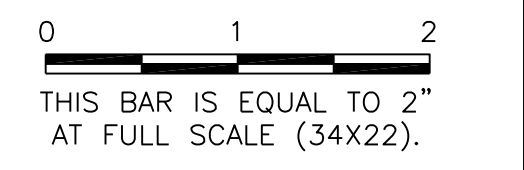
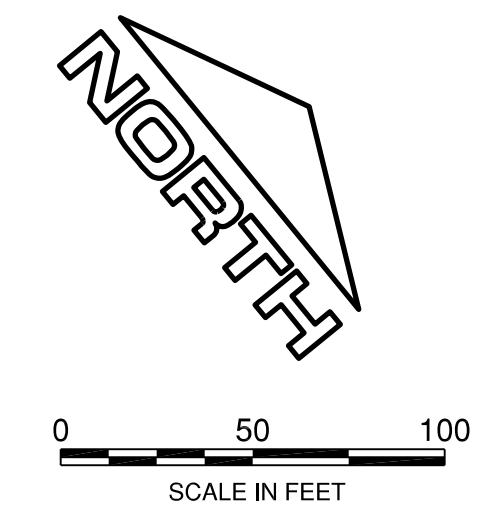
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SHEET	39 OF 67 SHEETS

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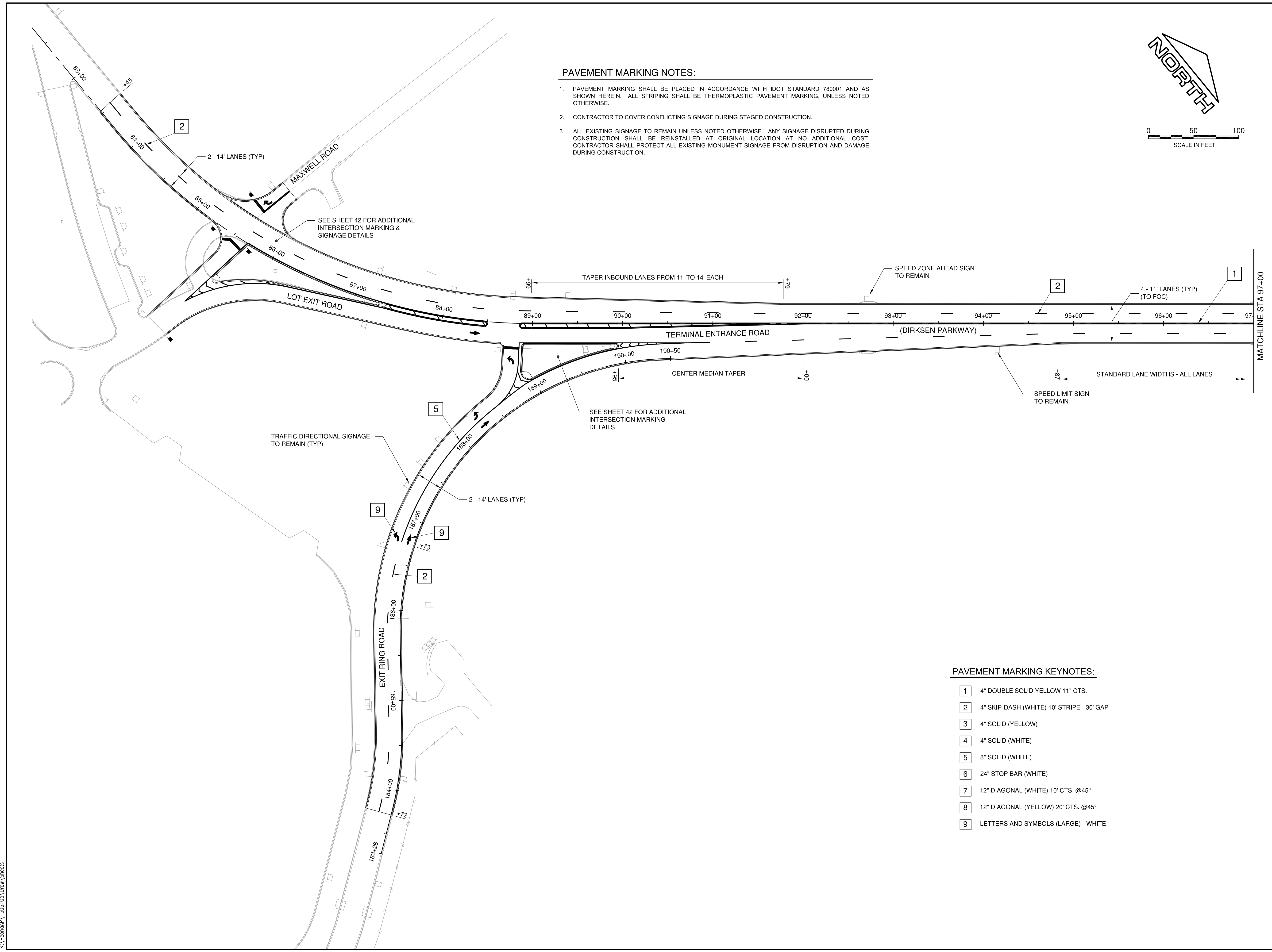
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PAVEMENT MARKING NOTES:

1. PAVEMENT MARKING SHALL BE PLACED IN ACCORDANCE WITH IDOT STANDARD 780001 AND AS SHOWN HEREIN. ALL STRIPING SHALL BE THERMOPLASTIC PAVEMENT MARKING, UNLESS NOTED OTHERWISE.
2. CONTRACTOR TO COVER CONFLICTING SIGNAGE DURING STAGED CONSTRUCTION.
3. ALL EXISTING SIGNAGE TO REMAIN UNLESS NOTED OTHERWISE. ANY SIGNAGE DISRUPTED DURING CONSTRUCTION SHALL BE REINSTALLED AT ORIGINAL LOCATION AT NO ADDITIONAL COST. CONTRACTOR SHALL PROTECT ALL EXISTING MONUMENT SIGNAGE FROM DISRUPTION AND DAMAGE DURING CONSTRUCTION.



PAVEMENT MARKING KEYNOTES:

- 1 4" DOUBLE SOLID YELLOW 11" CTS.
- 2 4" SKIP-DASH (WHITE) 10' STRIPE - 30' GAP
- 3 4" SOLID (YELLOW)
- 4 4" SOLID (WHITE)
- 5 8" SOLID (WHITE)
- 6 24" STOP BAR (WHITE)
- 7 12" DIAGONAL (WHITE) 10' CTS. @45°
- 8 12" DIAGONAL (YELLOW) 20' CTS. @45°
- 9 LETTERS AND SYMBOLS (LARGE) - WHITE

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
 REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
 PAVEMENT MARKING & SIGNAGE PLAN 1**

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DRAWN BY:	CJS
CHECKED BY:	JFM
APPROVED BY:	CET
DATE:	JULY 11, 2014
JOB No:	13061-05
IL PROJ. NO.	PIA-4415
AIP PROJ. NO.	3-17-0080-XX
SHEET	40 OF 67 SHEETS

PE098

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**GENERAL WAYNE A. DOWNING
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 PEORIA, ILLINOIS**

**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
 REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2**

PAVEMENT MARKING & SIGNAGE PLAN 2

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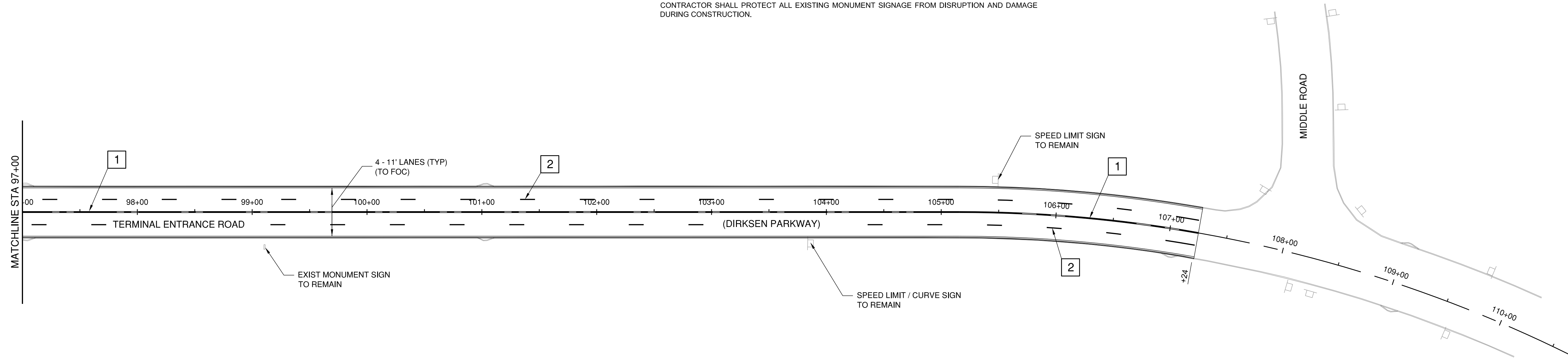
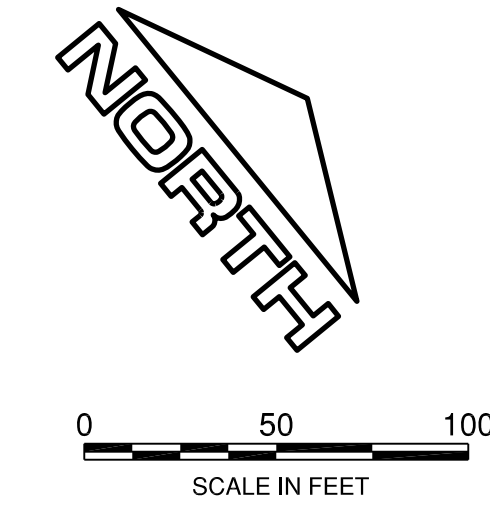
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AIP PROJ. NO.	3-17-0080-XX
SHEET	41 OF 67 SHEETS

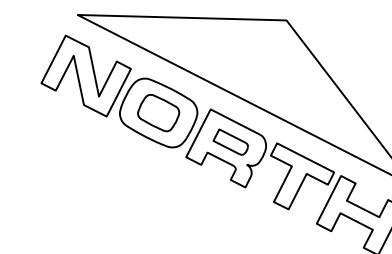
PAVEMENT MARKING NOTES:

1. PAVEMENT MARKING SHALL BE PLACED IN ACCORDANCE WITH IDOT STANDARD 780001 AND AS SHOWN HEREIN. ALL STRIPING SHALL BE THERMOPLASTIC PAVEMENT MARKING, UNLESS NOTED OTHERWISE.
2. CONTRACTOR TO COVER CONFLICTING SIGNAGE DURING STAGED CONSTRUCTION.
3. ALL EXISTING SIGNAGE TO REMAIN UNLESS NOTED OTHERWISE. ANY SIGNAGE DISRUPTED DURING CONSTRUCTION SHALL BE REINSTALLED AT ORIGINAL LOCATION AT NO ADDITIONAL COST. CONTRACTOR SHALL PROTECT ALL EXISTING MONUMENT SIGNAGE FROM DISRUPTION AND DAMAGE DURING CONSTRUCTION.



PAVEMENT MARKING KEYNOTES:

- | | |
|---|---|
| 1 | 4" DOUBLE SOLID YELLOW 11" CTS. |
| 2 | 4" SKIP-DASH (WHITE) 10' STRIPE - 30' GAP |
| 3 | 4" SOLID (YELLOW) |
| 4 | 4" SOLID (WHITE) |
| 5 | 8" SOLID (WHITE) |
| 6 | 24" STOP BAR (WHITE) |
| 7 | 12" DIAGONAL (WHITE) 10' CTS. @45° |
| 8 | 12" DIAGONAL (YELLOW) 20' CTS. @45° |
| 9 | LETTERS AND SYMBOLS (LARGE) - WHITE |

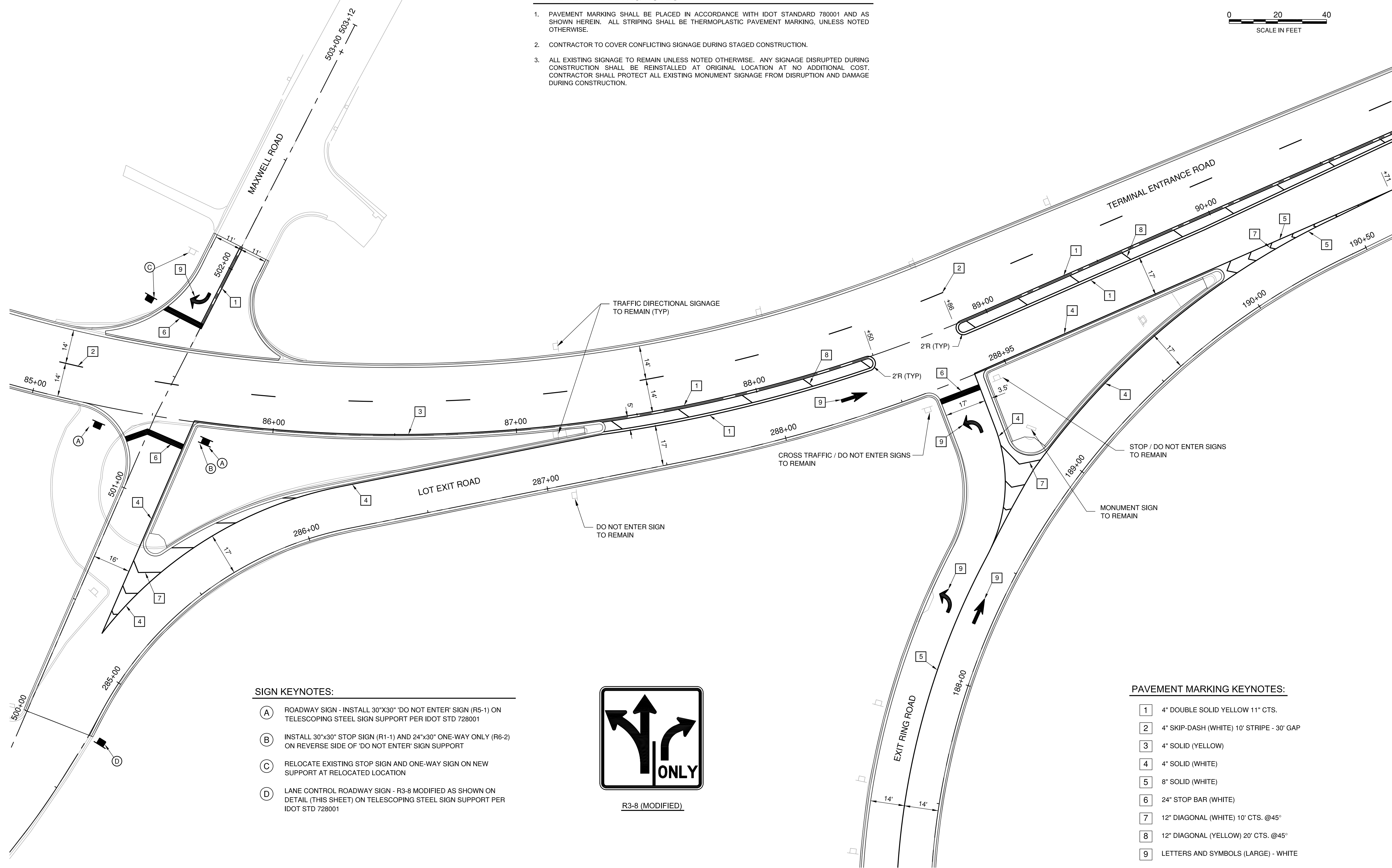
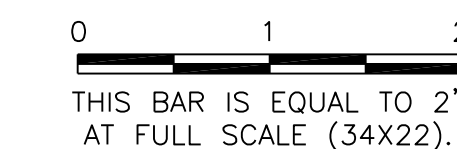


PAVEMENT MARKING NOTES:

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SIGN KEYNOTES:

- (A) ROADWAY SIGN - INSTALL 30"x30" "DO NOT ENTER" SIGN (R5-1) ON TELESCOPING STEEL SIGN SUPPORT PER IDOT STD 728001
- (B) INSTALL 30"x30" STOP SIGN (R1-1) AND 24"x30" ONE-WAY ONLY (R6-2) ON REVERSE SIDE OF "DO NOT ENTER" SIGN SUPPORT
- (C) RELOCATE EXISTING STOP SIGN AND ONE-WAY SIGN ON NEW SUPPORT AT RELOCATED LOCATION
- (D) LANE CONTROL ROADWAY SIGN - R3-8 MODIFIED AS SHOWN ON DETAIL (THIS SHEET) ON TELESCOPING STEEL SIGN SUPPORT PER IDOT STD 728001



R3-8 (MODIFIED)

PAVEMENT MARKING KEYNOTES:

- 1 4" DOUBLE SOLID YELLOW 11" CTS.
- 2 4" SKIP-DASH (WHITE) 10' STRIPE - 30' GAP
- 3 4" SOLID (YELLOW)
- 4 4" SOLID (WHITE)
- 5 8" SOLID (WHITE)
- 6 24" STOP BAR (WHITE)
- 7 12" DIAGONAL (WHITE) 10' CTS. @45°
- 8 12" DIAGONAL (YELLOW) 20' CTS. @45°
- 9 LETTERS AND SYMBOLS (LARGE) - WHITE

GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
PAVEMENT MARKING & SIGNAGE PLAN 3

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SHEET 42 OF 67 SHEETS	

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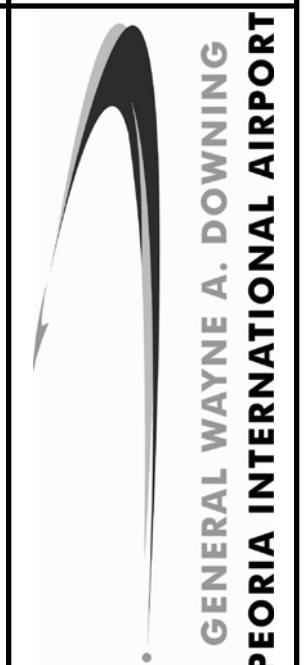
NUMBER	BY	DATE

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 AT FULL SCALE (34X22).

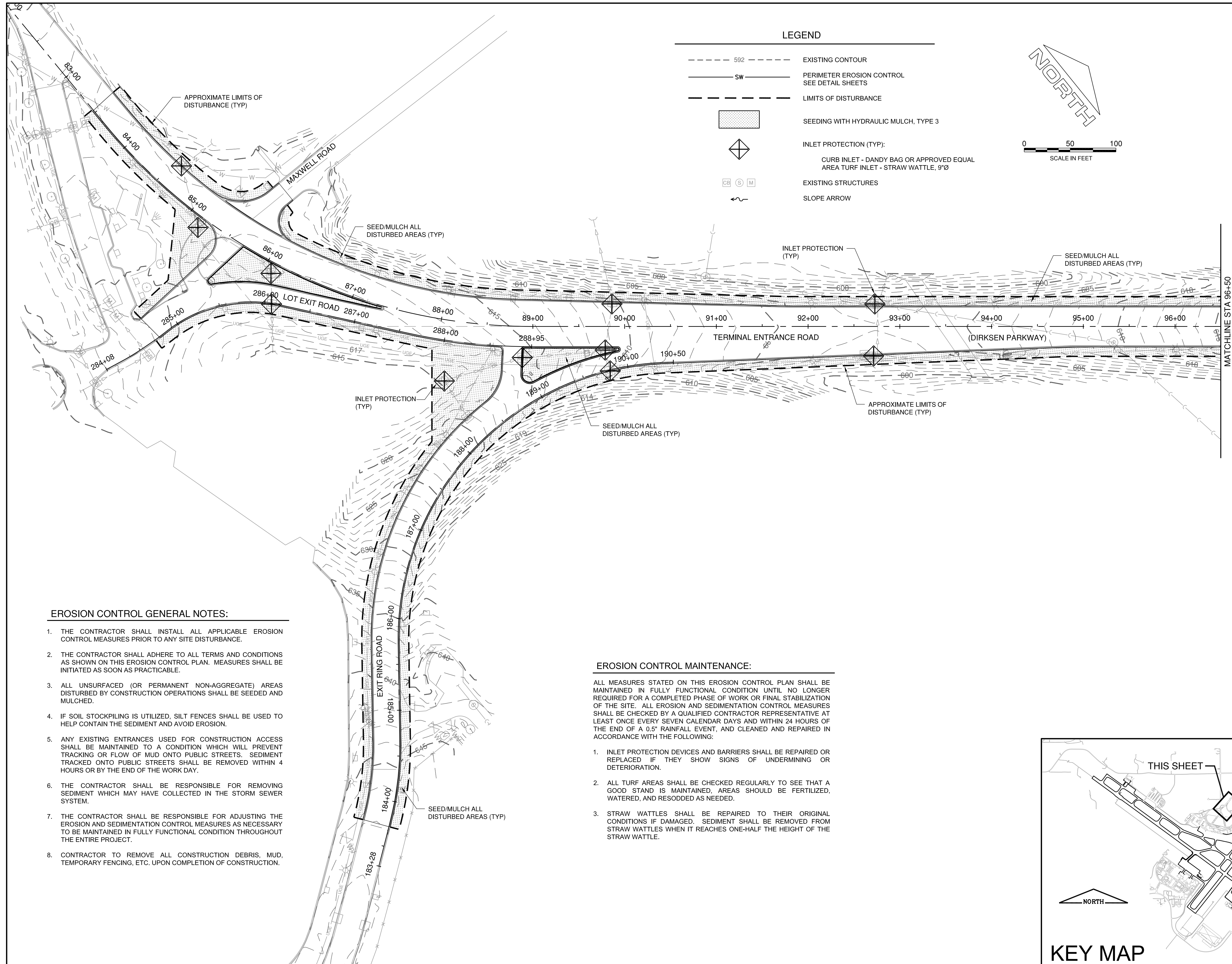
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DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
 REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
 EROSION & SEDIMENT CONTROL PLAN 1

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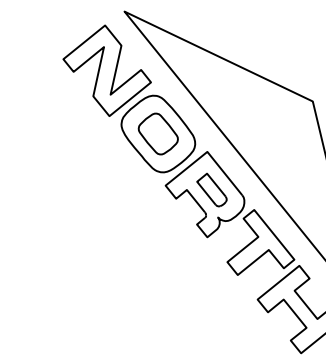


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IL PROJ. NO. PIA-4415	
AIP PROJ. NO. 3-17-0080-XX	
SHEET 43 OF 67 SHEETS	



LEGEND

- 592 --- EXISTING CONTOUR
- SW --- PERIMETER EROSION CONTROL SEE DETAIL SHEETS
- --- LIMITS OF DISTURBANCE
- [Pattern] SEEDING WITH HYDRAULIC MULCH, TYPE 3
- [Symbol] INLET PROTECTION (TYP):
 CURB INLET - DANDY BAG OR APPROVED EQUAL AREA TURF INLET - STRAW WATTLE, 9"Ø
- [Symbol] EXISTING STRUCTURES
- [Symbol] SLOPE ARROW



0 50 100
 SCALE IN FEET

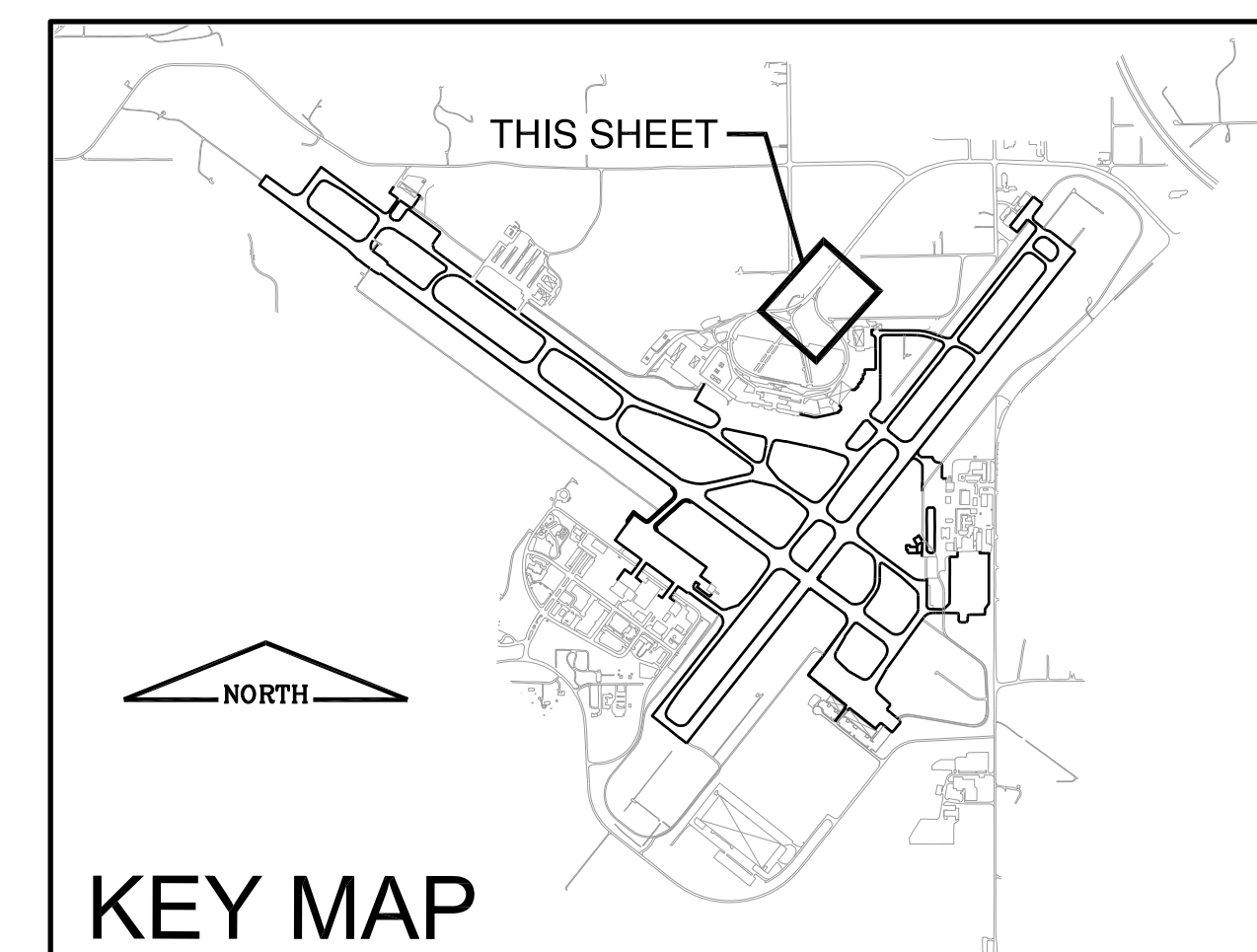
EROSION CONTROL GENERAL NOTES:

1. THE CONTRACTOR SHALL INSTALL ALL APPLICABLE EROSION CONTROL MEASURES PRIOR TO ANY SITE DISTURBANCE.
2. THE CONTRACTOR SHALL ADHERE TO ALL TERMS AND CONDITIONS AS SHOWN ON THIS EROSION CONTROL PLAN. MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE.
3. ALL UNSURFACED (OR PERMANENT NON-AGGREGATE) AREAS DISTURBED BY CONSTRUCTION OPERATIONS SHALL BE SEEDED AND MULCHED.
4. IF SOIL STOCKPILING IS UTILIZED, SILT FENCES SHALL BE USED TO HELP CONTAIN THE SEDIMENT AND AVOID EROSION.
5. ANY EXISTING ENTRANCES USED FOR CONSTRUCTION ACCESS SHALL BE MAINTAINED TO A CONDITION WHICH WILL PREVENT TRACKING OR FLOW OF MUD ONTO PUBLIC STREETS. SEDIMENT TRACKED ONTO PUBLIC STREETS SHALL BE REMOVED WITHIN 4 HOURS OR BY THE END OF THE WORK DAY.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING SEDIMENT WHICH MAY HAVE COLLECTED IN THE STORM SEWER SYSTEM.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE EROSION AND SEDIMENTATION CONTROL MEASURES AS NECESSARY TO BE MAINTAINED IN FULLY FUNCTIONAL CONDITION THROUGHOUT THE ENTIRE PROJECT.
8. CONTRACTOR TO REMOVE ALL CONSTRUCTION DEBRIS, MUD, TEMPORARY FENCING, ETC. UPON COMPLETION OF CONSTRUCTION.

EROSION CONTROL MAINTENANCE:

ALL MEASURES STATED ON THIS EROSION CONTROL PLAN SHALL BE MAINTAINED IN FULLY FUNCTIONAL CONDITION UNTIL NO LONGER REQUIRED FOR A COMPLETED PHASE OF WORK OR FINAL STABILIZATION OF THE SITE. ALL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE CHECKED BY A QUALIFIED CONTRACTOR REPRESENTATIVE AT LEAST ONCE EVERY SEVEN CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A 0.5" RAINFALL EVENT, AND CLEANED AND REPAIRED IN ACCORDANCE WITH THE FOLLOWING:

1. INLET PROTECTION DEVICES AND BARRIERS SHALL BE REPAIRED OR REPLACED IF THEY SHOW SIGNS OF UNDERMINING OR DETERIORATION.
2. ALL TURF AREAS SHALL BE CHECKED REGULARLY TO SEE THAT A GOOD STAND IS MAINTAINED, AREAS SHOULD BE FERTILIZED, WATERED, AND RESODED AS NEEDED.
3. STRAW WATTLES SHALL BE REPAIRED TO THEIR ORIGINAL CONDITIONS IF DAMAGED. SEDIMENT SHALL BE REMOVED FROM STRAW WATTLES WHEN IT REACHES ONE-HALF THE HEIGHT OF THE STRAW WATTLE.



KEY MAP

PE098

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS

DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2

EROSION & SEDIMENT CONTROL PLAN 2

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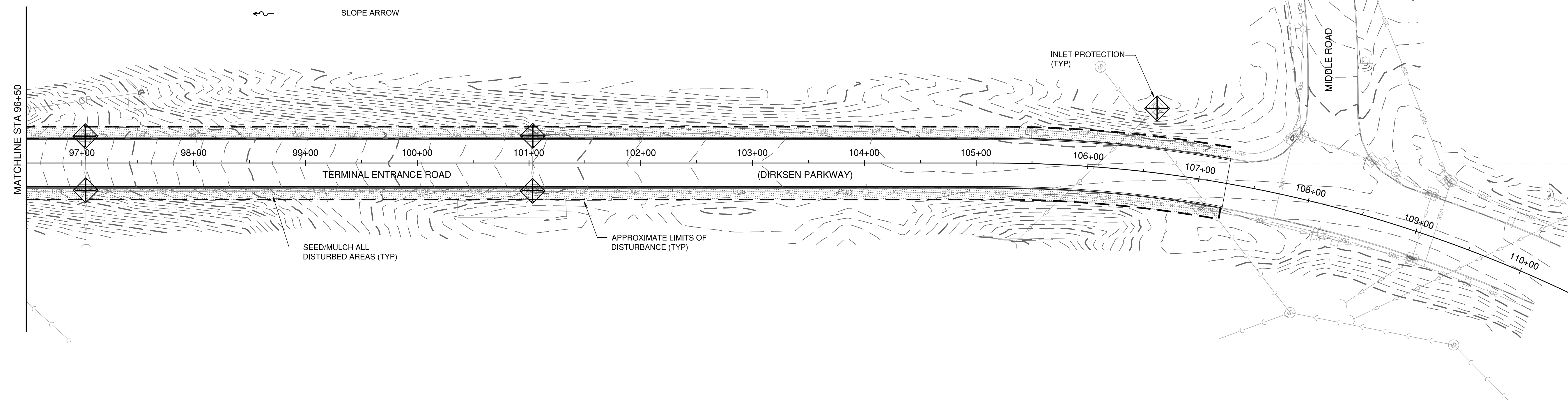
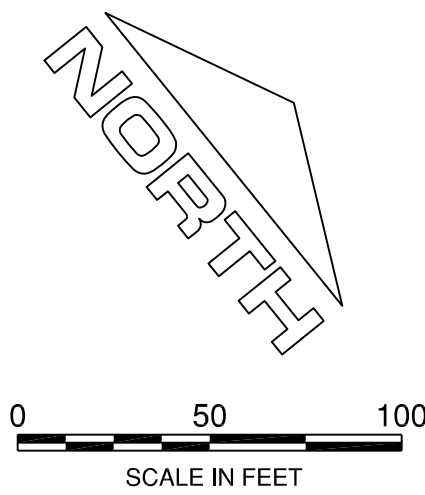
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CHECKED BY:	JFM
APPROVED BY:	CET
DATE:	JULY 11, 2014
JOB No:	13061-05
IL PROJ. NO. PIA-4415	
AIP PROJ. NO. 3-17-0080-XX	
SHEET 44 OF 67 SHEETS	

LEGEND

- 592 --- EXISTING CONTOUR
- SW — PERIMETER EROSION CONTROL
SEE DETAIL SHEETS
- --- LIMITS OF DISTURBANCE
- [Hatched Box] SEEDING WITH HYDRAULIC MULCH, TYPE 3
- [Diamond with X] INLET PROTECTION (TYP):
CURB INLET - DANDY BAG OR APPROVED EQUAL
AREA TURF INLET - STRAW WATTLE, 9'Ø
- [CB S M] EXISTING STRUCTURES
- [Arrow] SLOPE ARROW



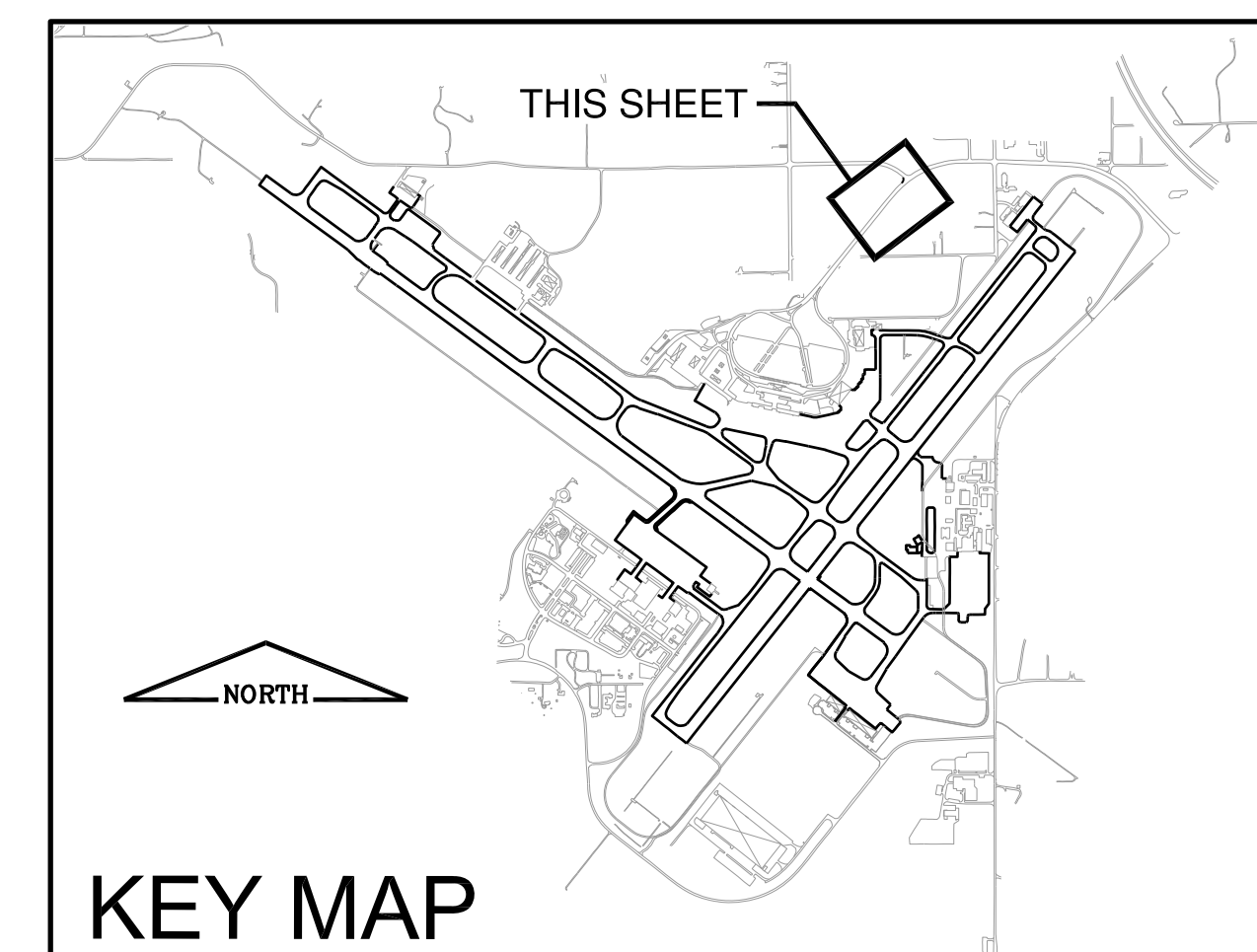
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KEY MAP

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NUMBER	BY	DATE

0 1 2
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 AT FULL SCALE (34X22).

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
 REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2**

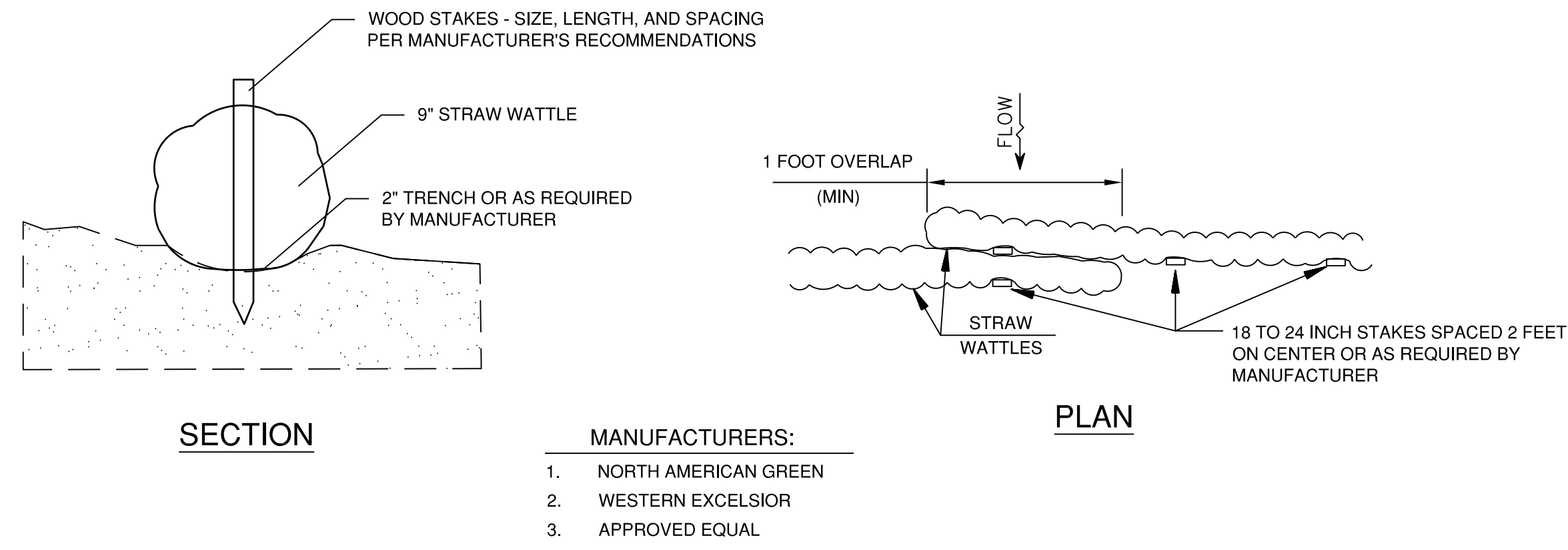
CONSTRUCTION DETAILS

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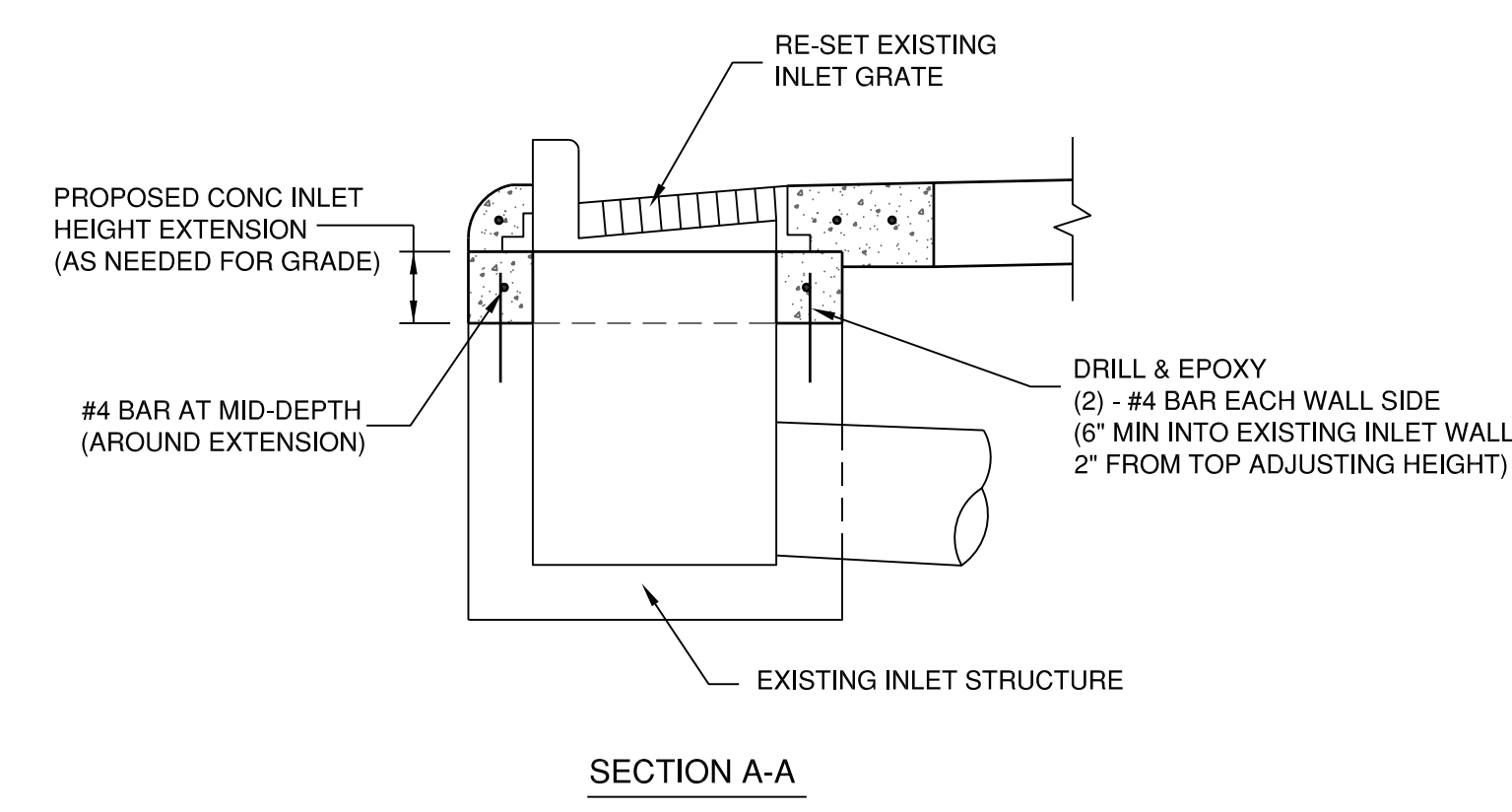
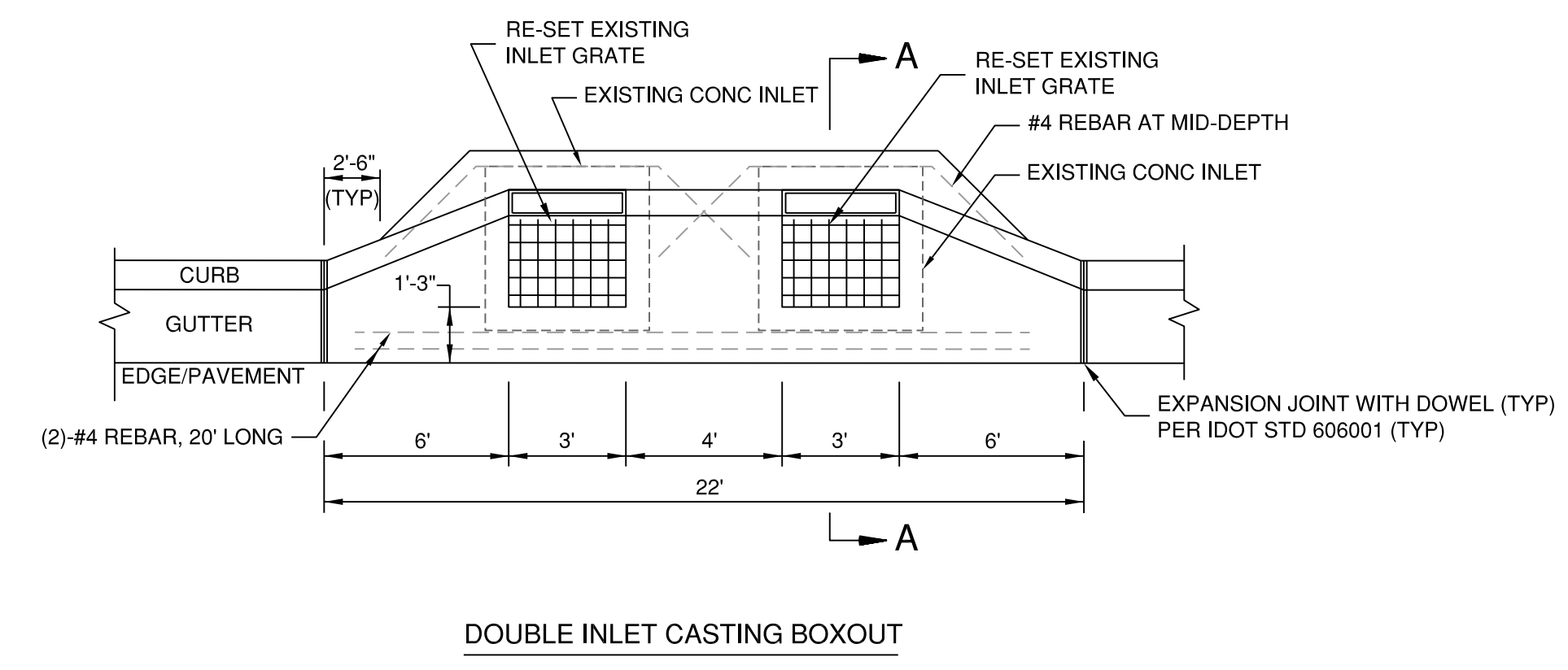
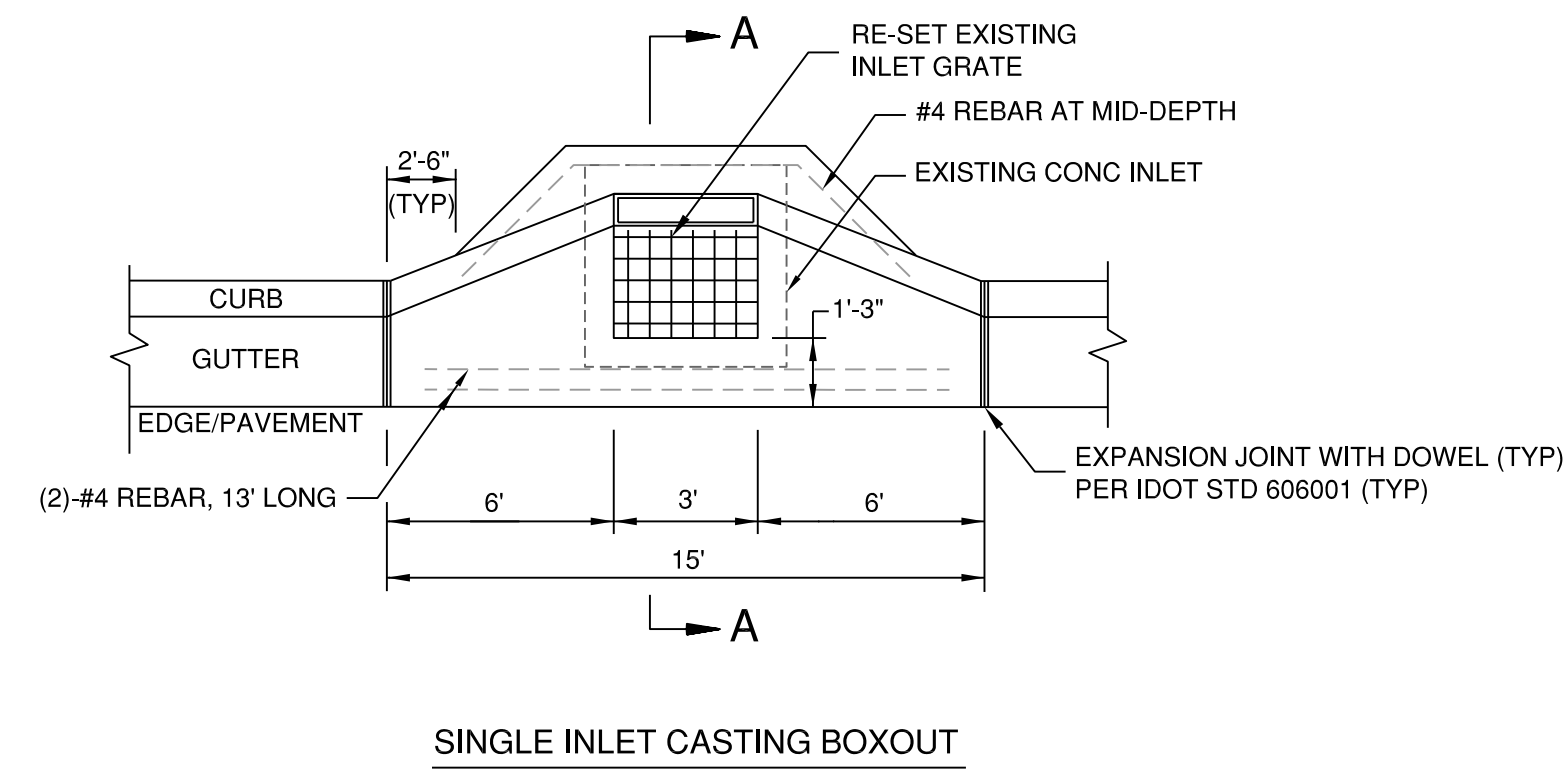
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DATE:	JULY 11, 2014
JOB No:	13061-05
IL PROJ. NO.	PIA-4415
AIP PROJ. NO.	3-17-0080-XX
SHEET	45 OF 67 SHEETS

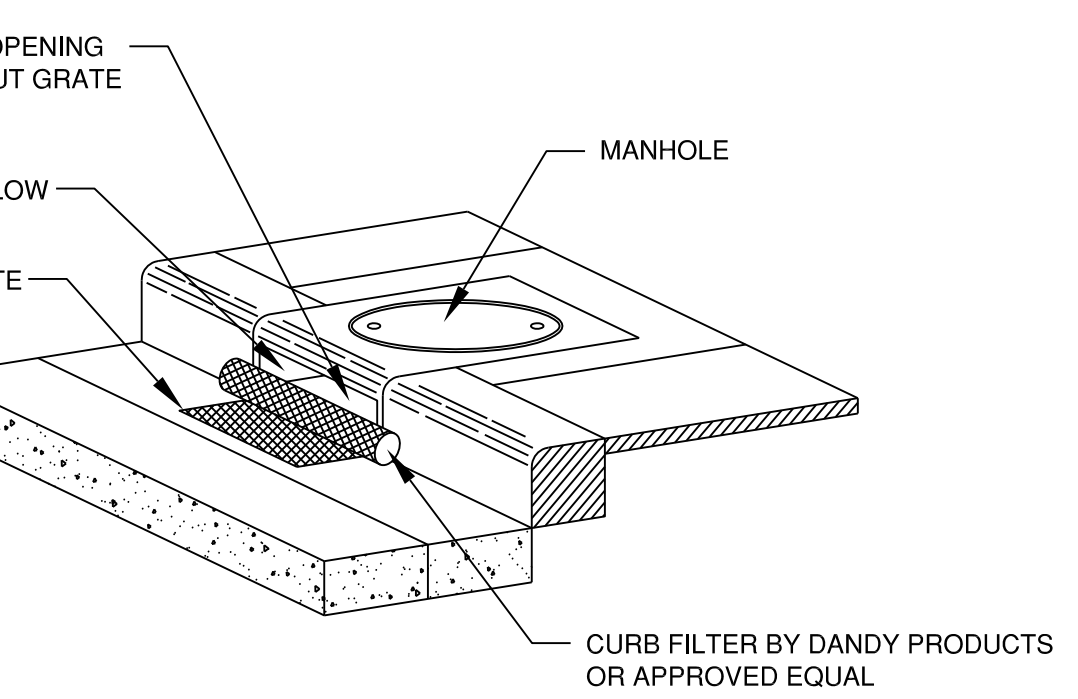
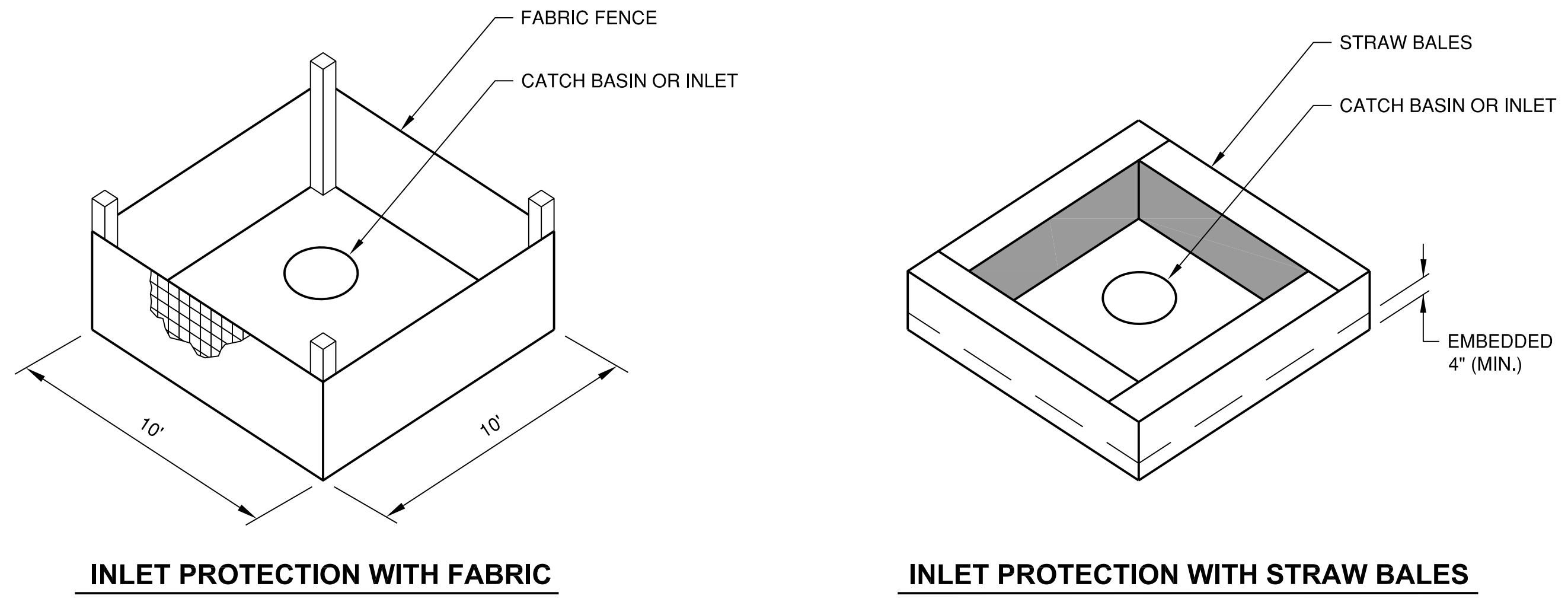


STRAW WATTLE DETAILS
 N.T.S.



- NOTES:**
1. CONTRACTOR SHALL SUBMIT SHOP DRAWING FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION OF INLET BOXOUT.
 2. THE COST OF THE INLET CASTING BOXOUT INCLUDING ALL CURB & GUTTER CONCRETE, INLET ADJUSTMENT CONCRETE, REBAR, JOINTS, ETC. SHALL BE INCLUDED IN THE UNIT PRICE FOR ADJUST INLET.

INLET CASTING BOXOUT DETAILS
 N.T.S.

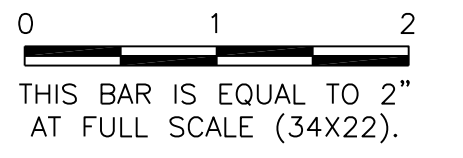


INLET PROTECTION DETAILS
 N.T.S.

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**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
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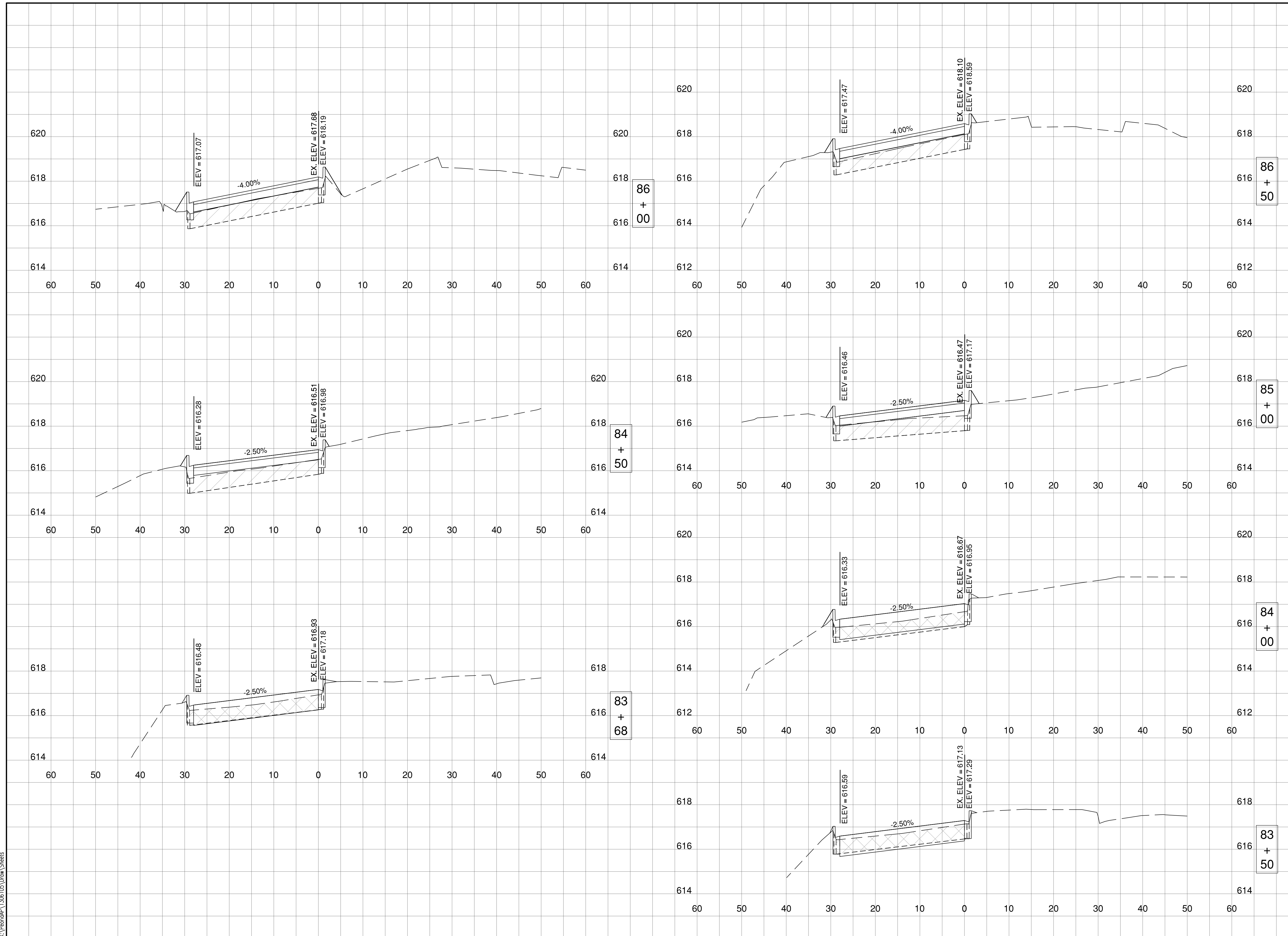
CROSS SECTIONS - TERMINAL ENTRANCE ROAD

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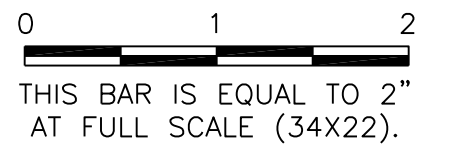
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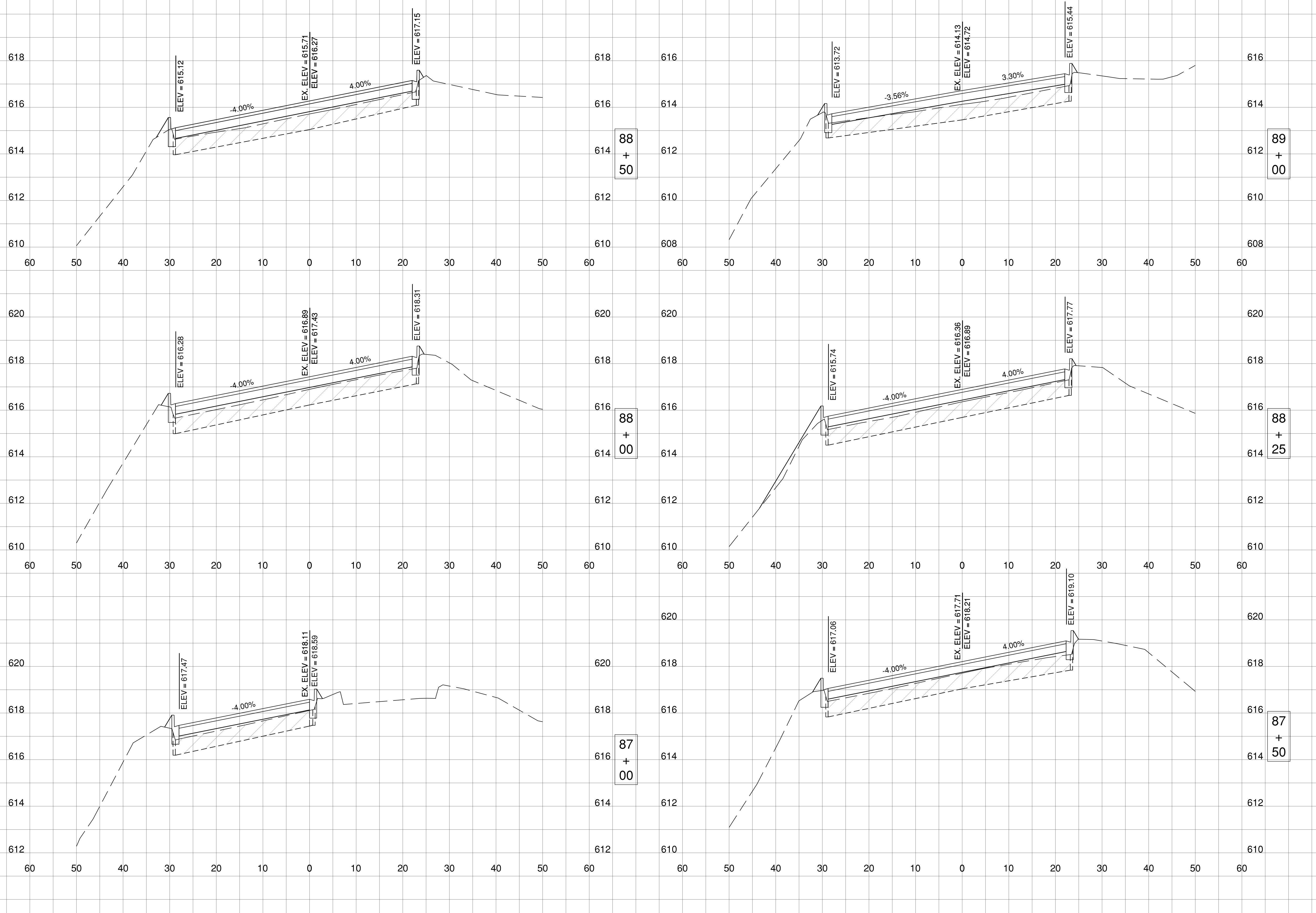
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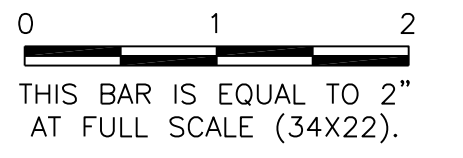
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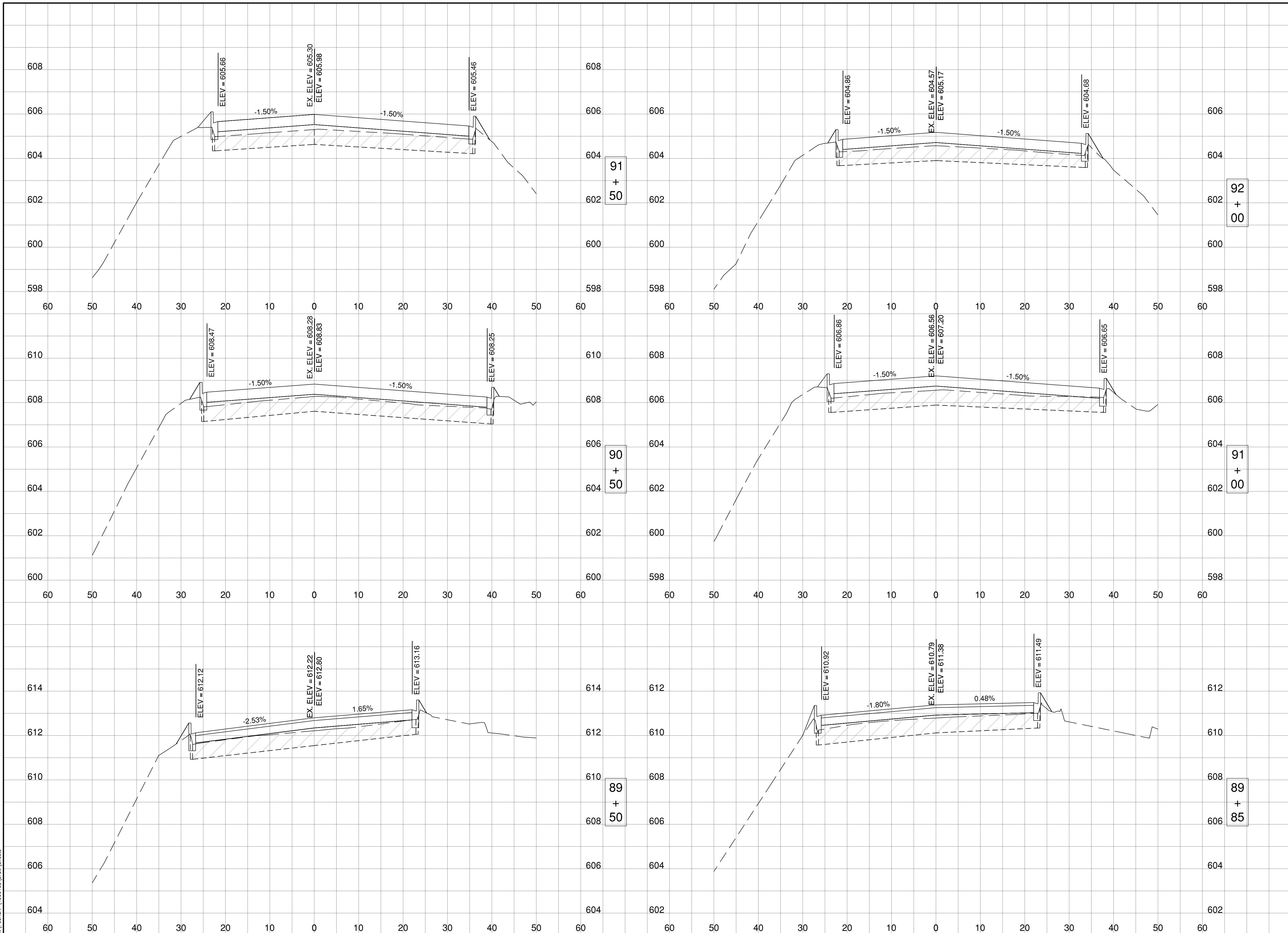
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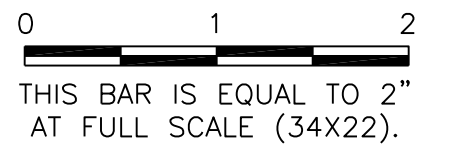
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SHEET 48 OF 67 SHEETS	



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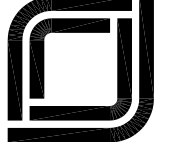


**GENERAL WAYNE A. DOWNING
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**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
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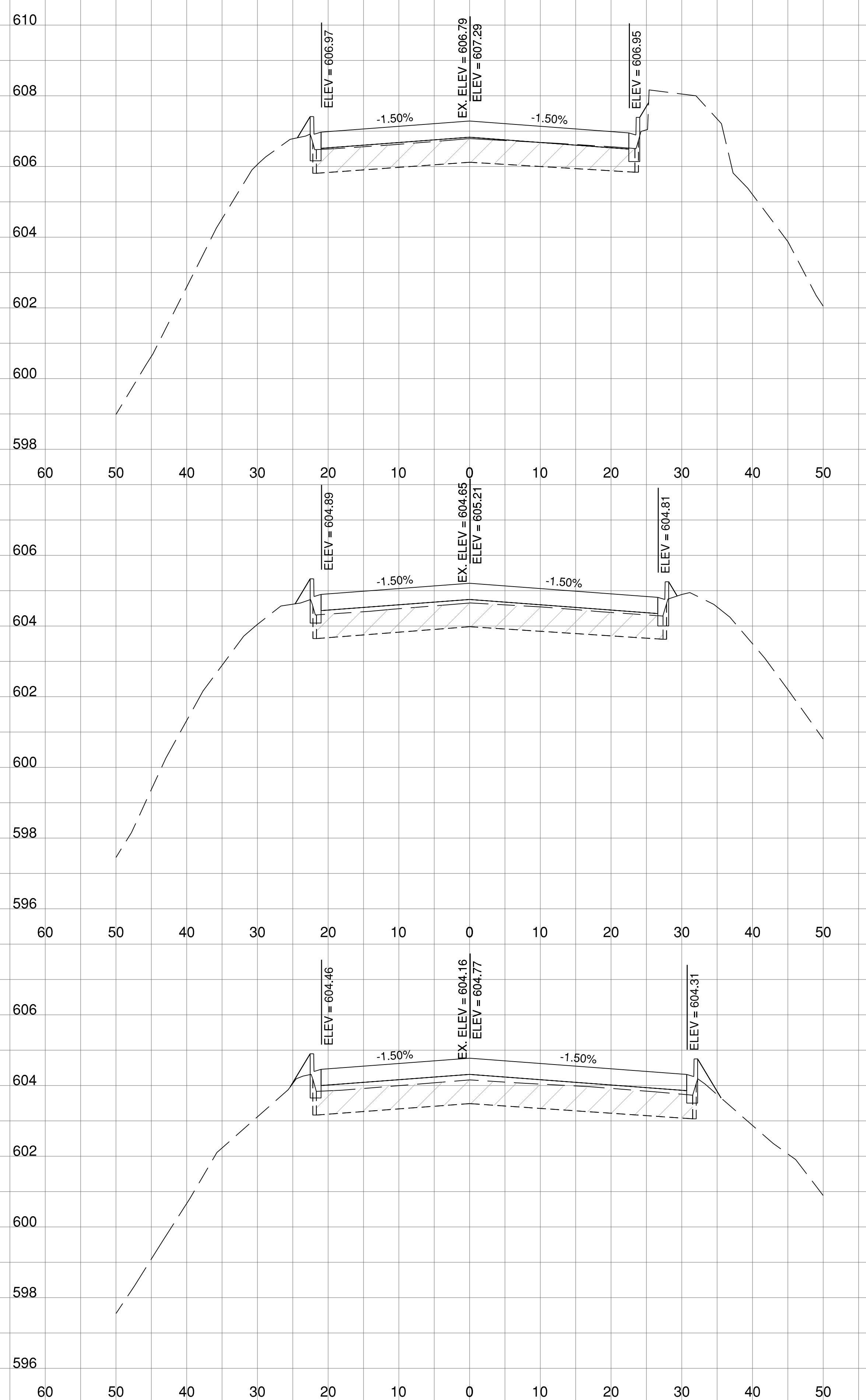
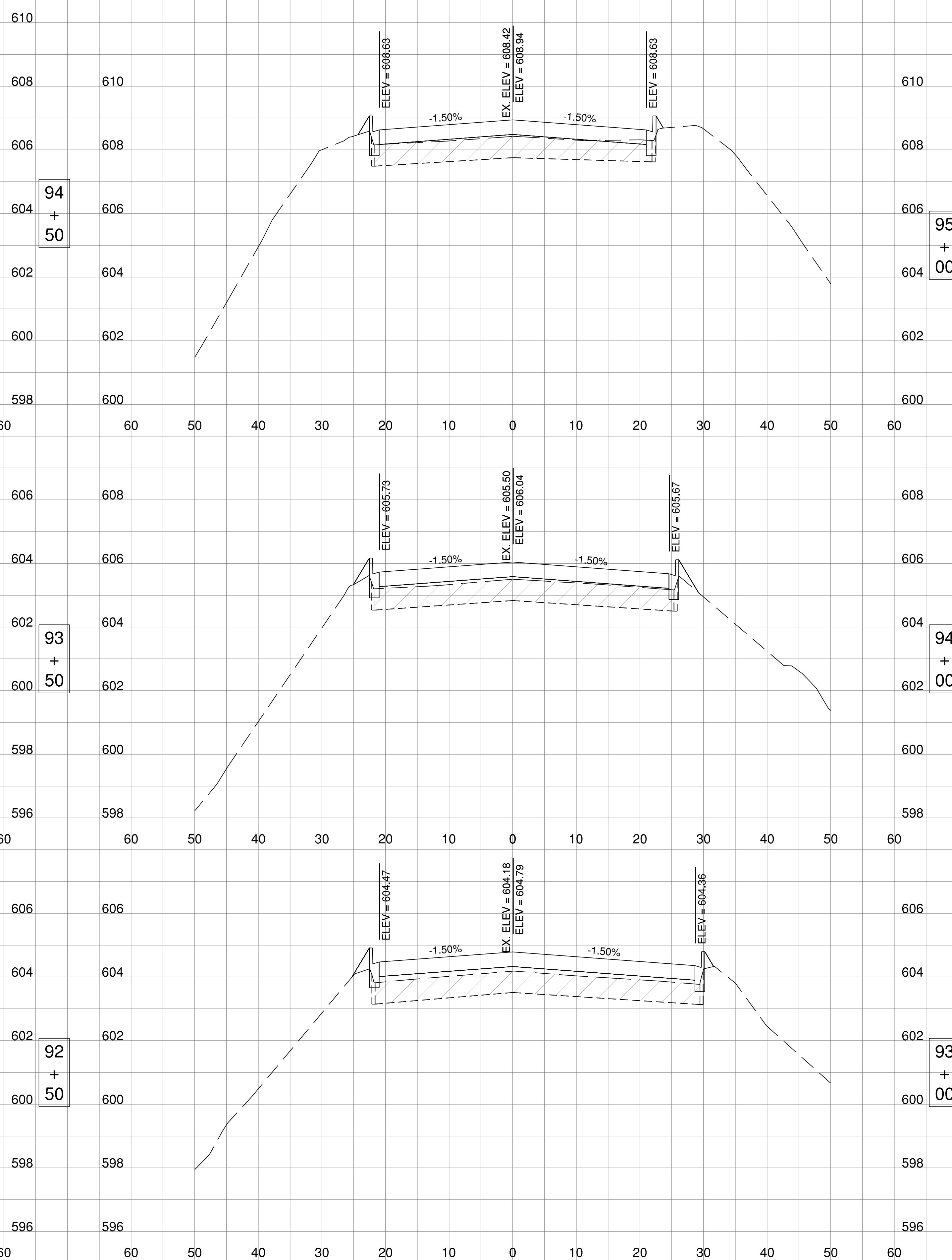


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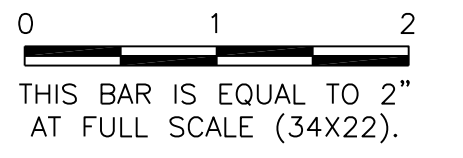
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AIP PROJ. NO.	3-17-0080-XX
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 PEORIA, ILLINOIS**

**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
 REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2**

CROSS SECTIONS - TERMINAL ENTRANCE ROAD

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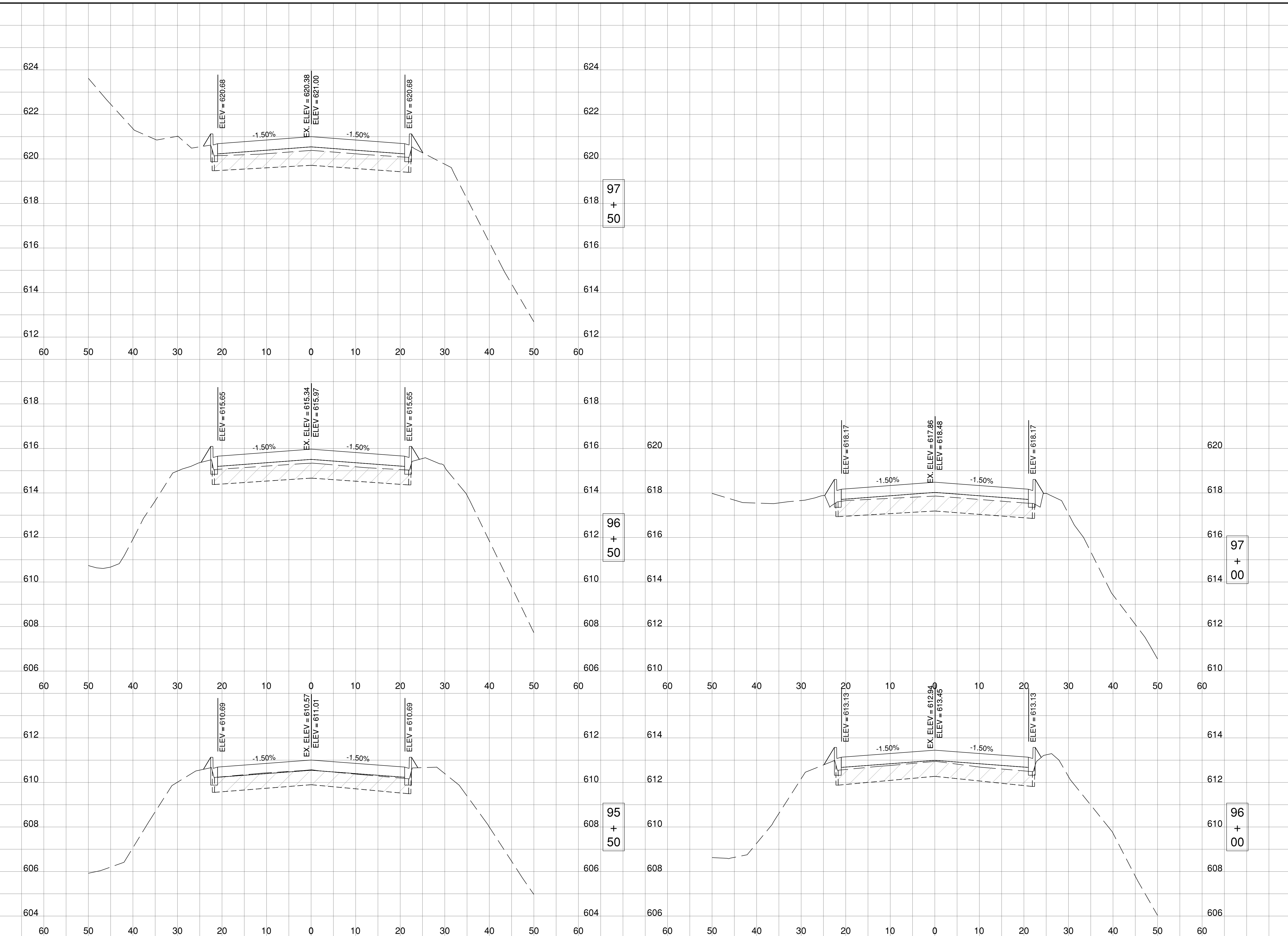
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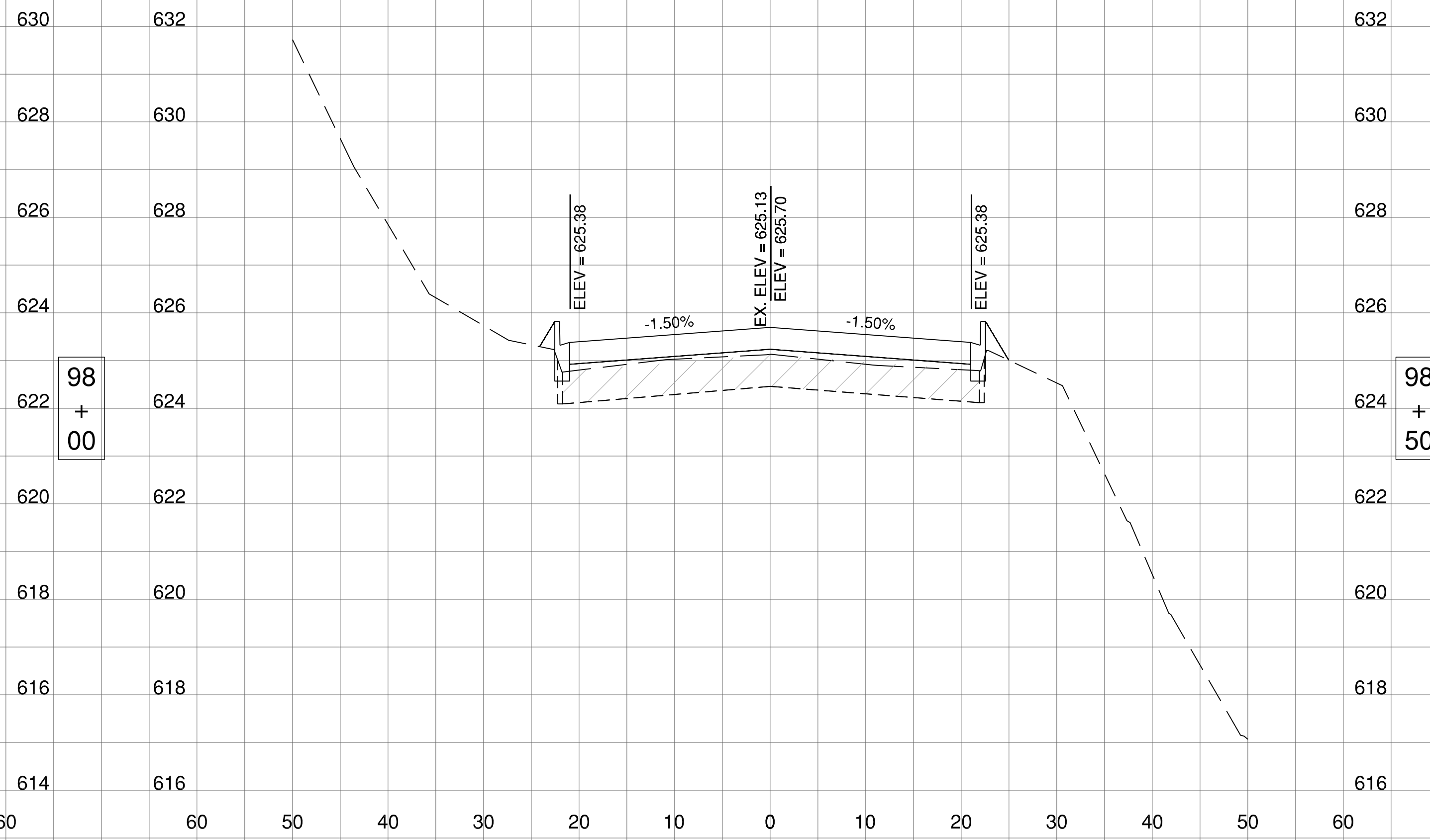
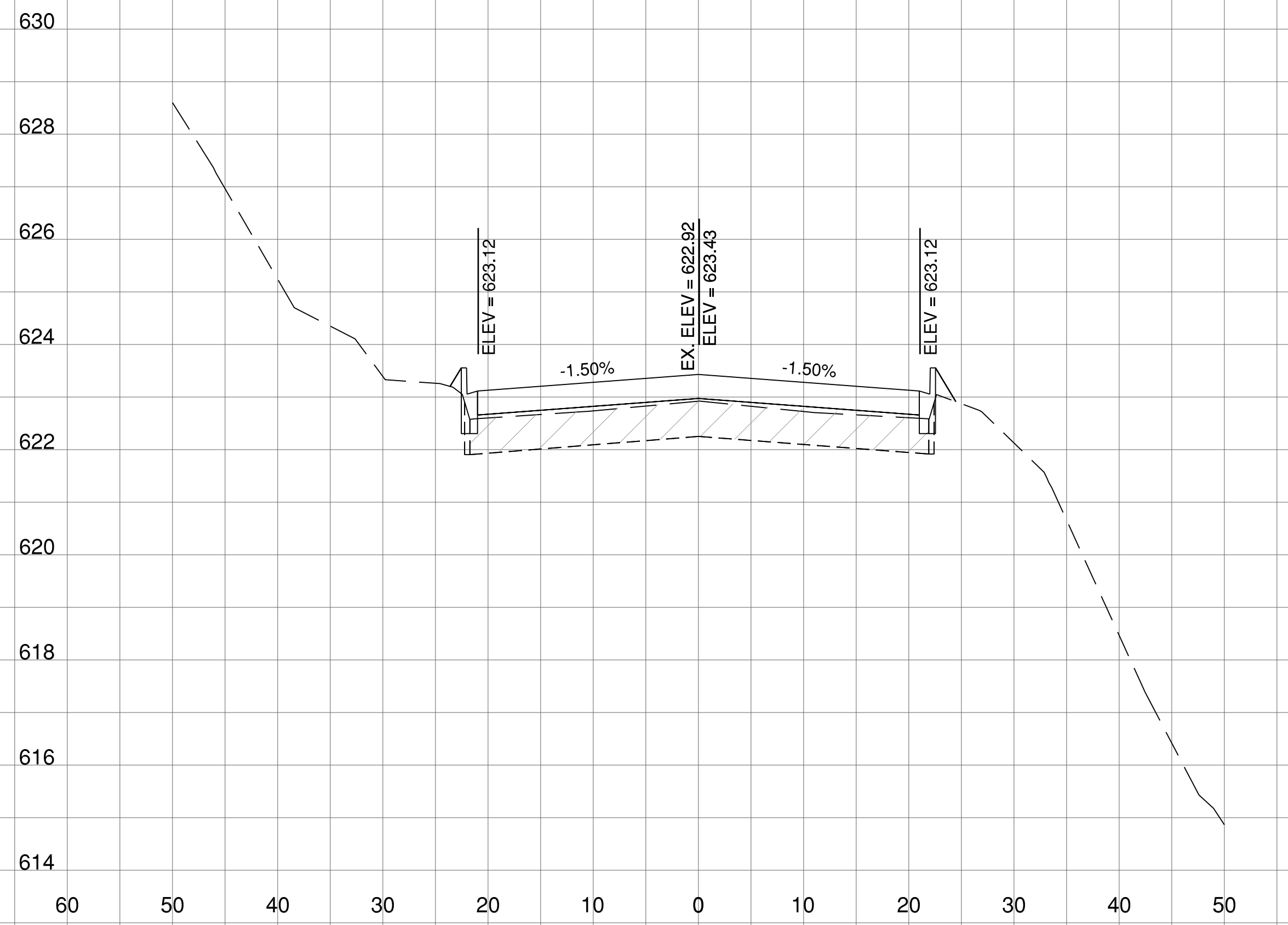
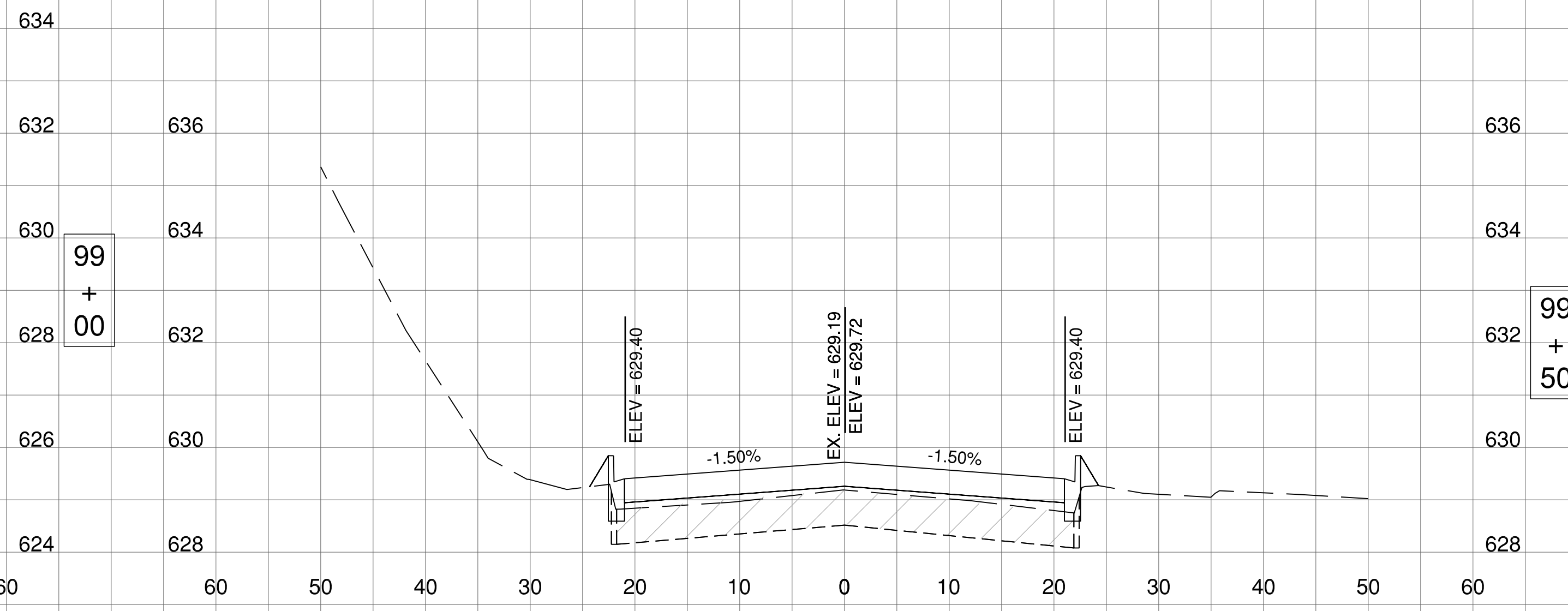
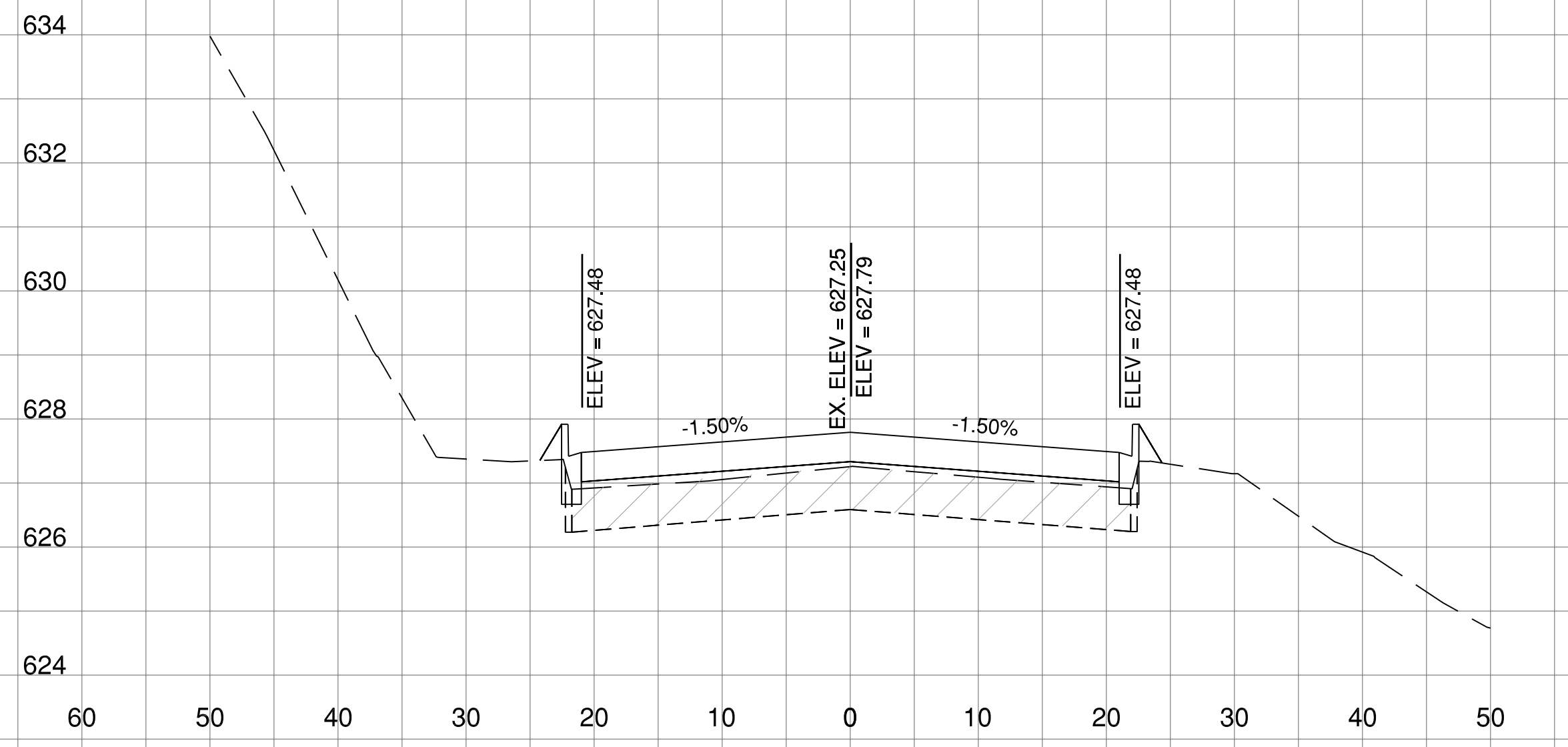
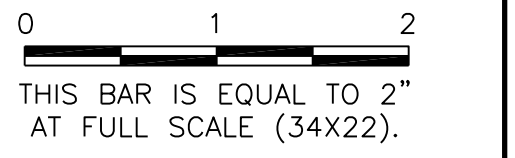
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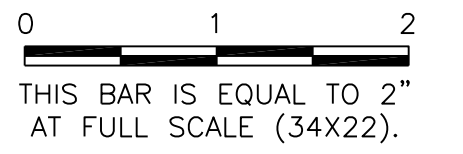
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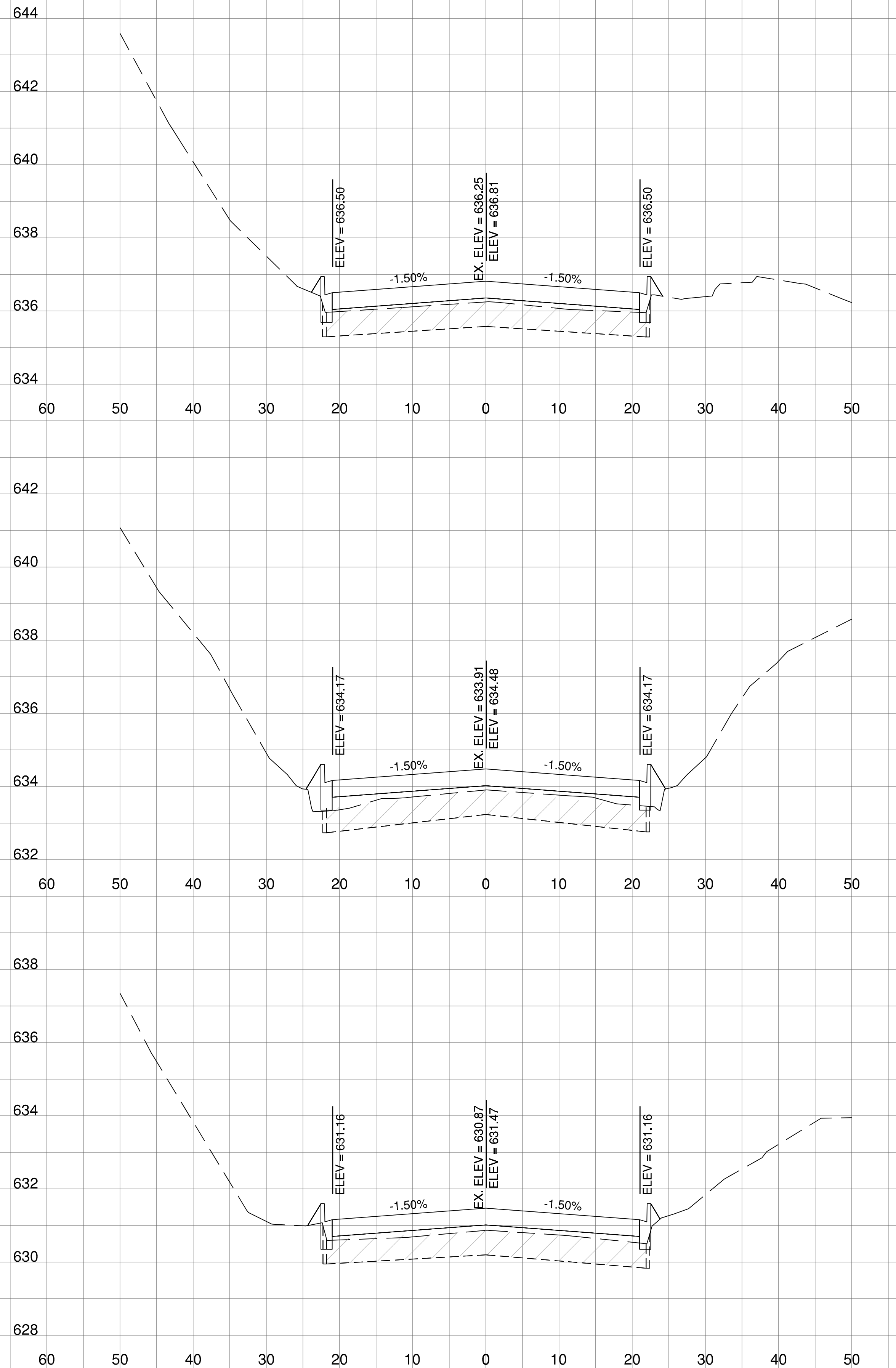
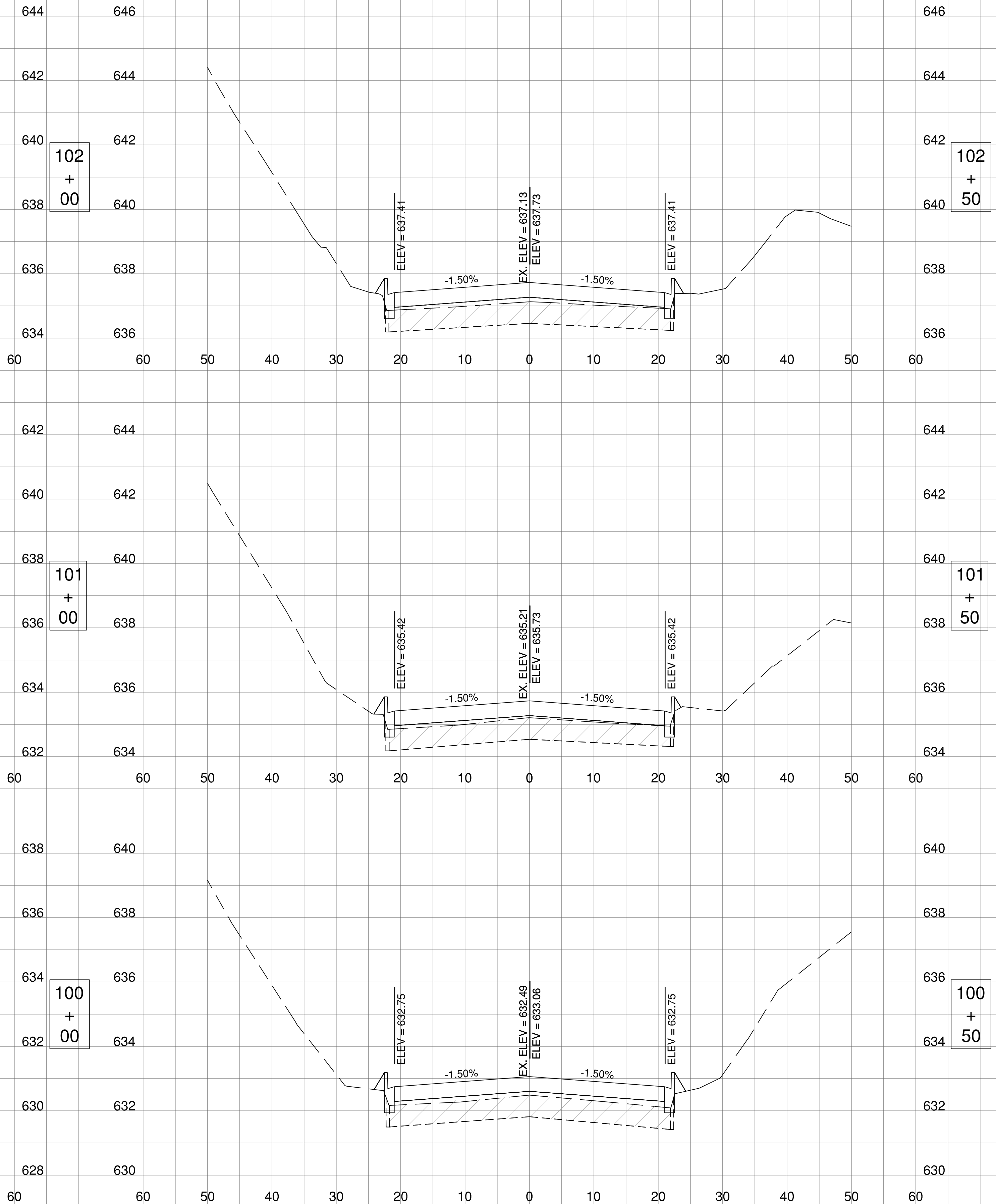


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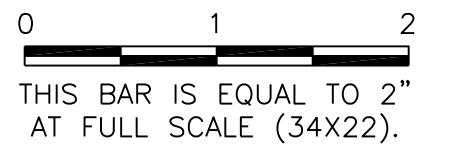
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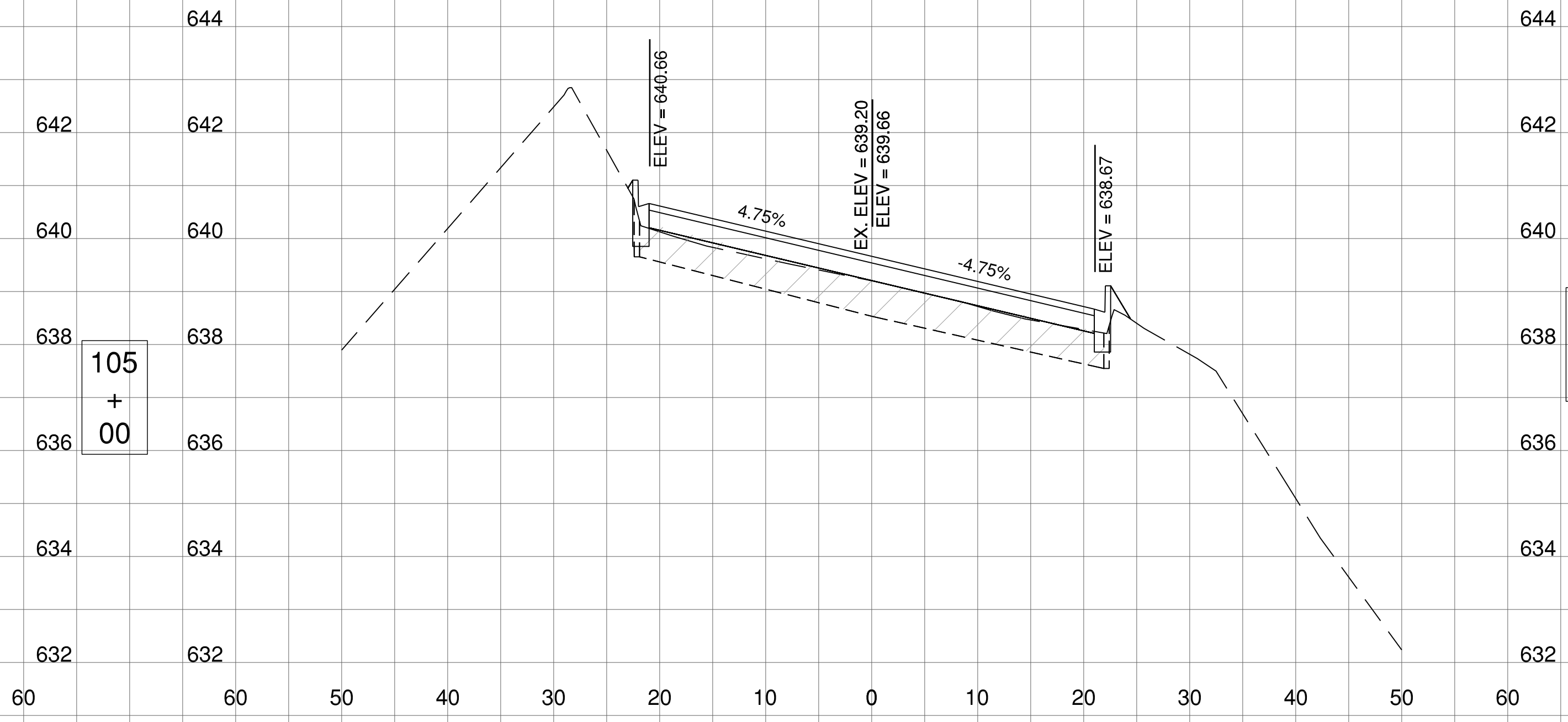
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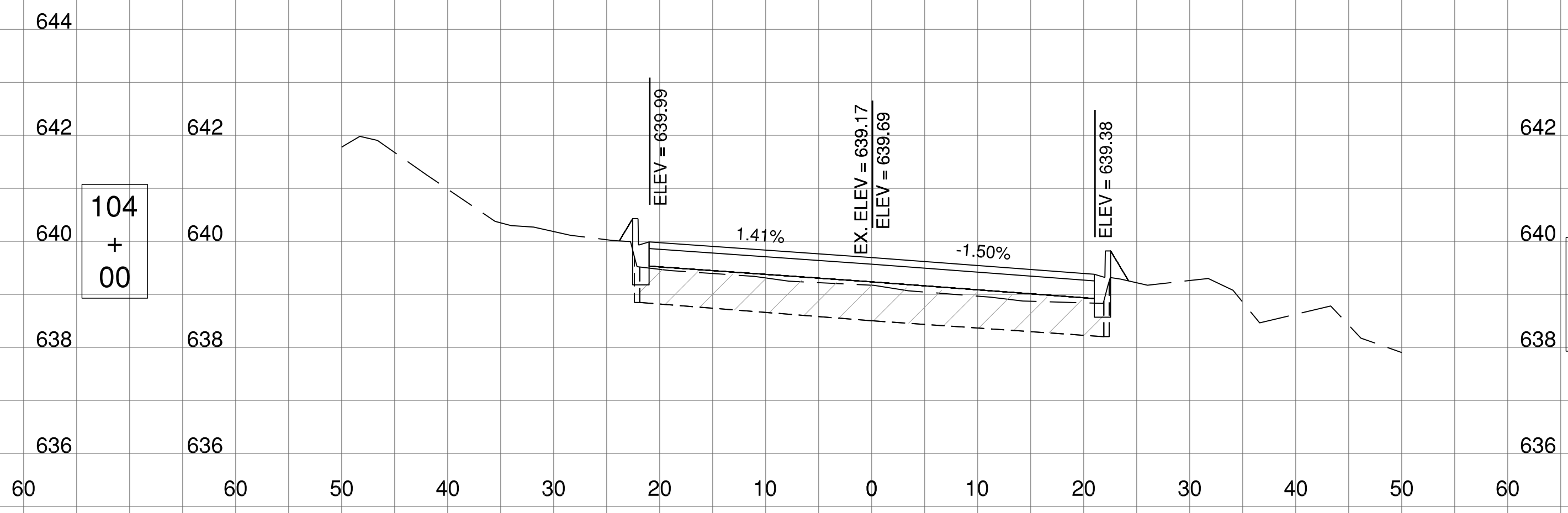
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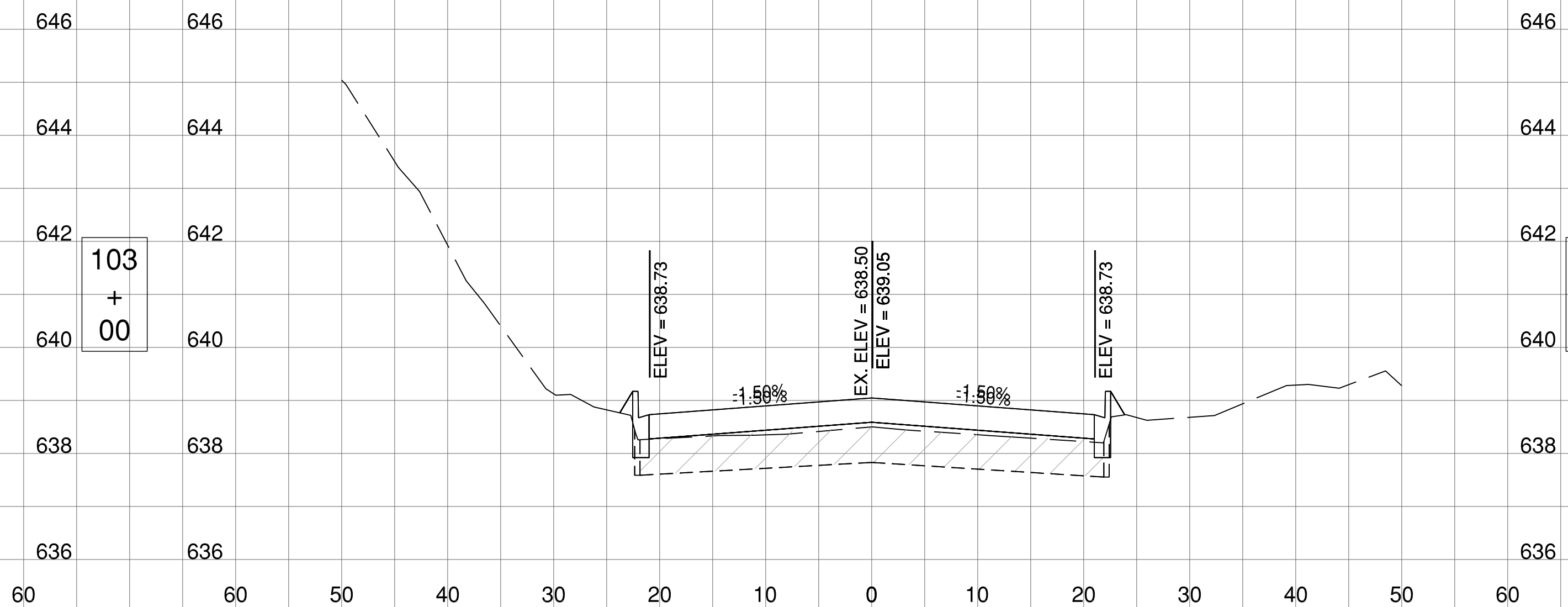
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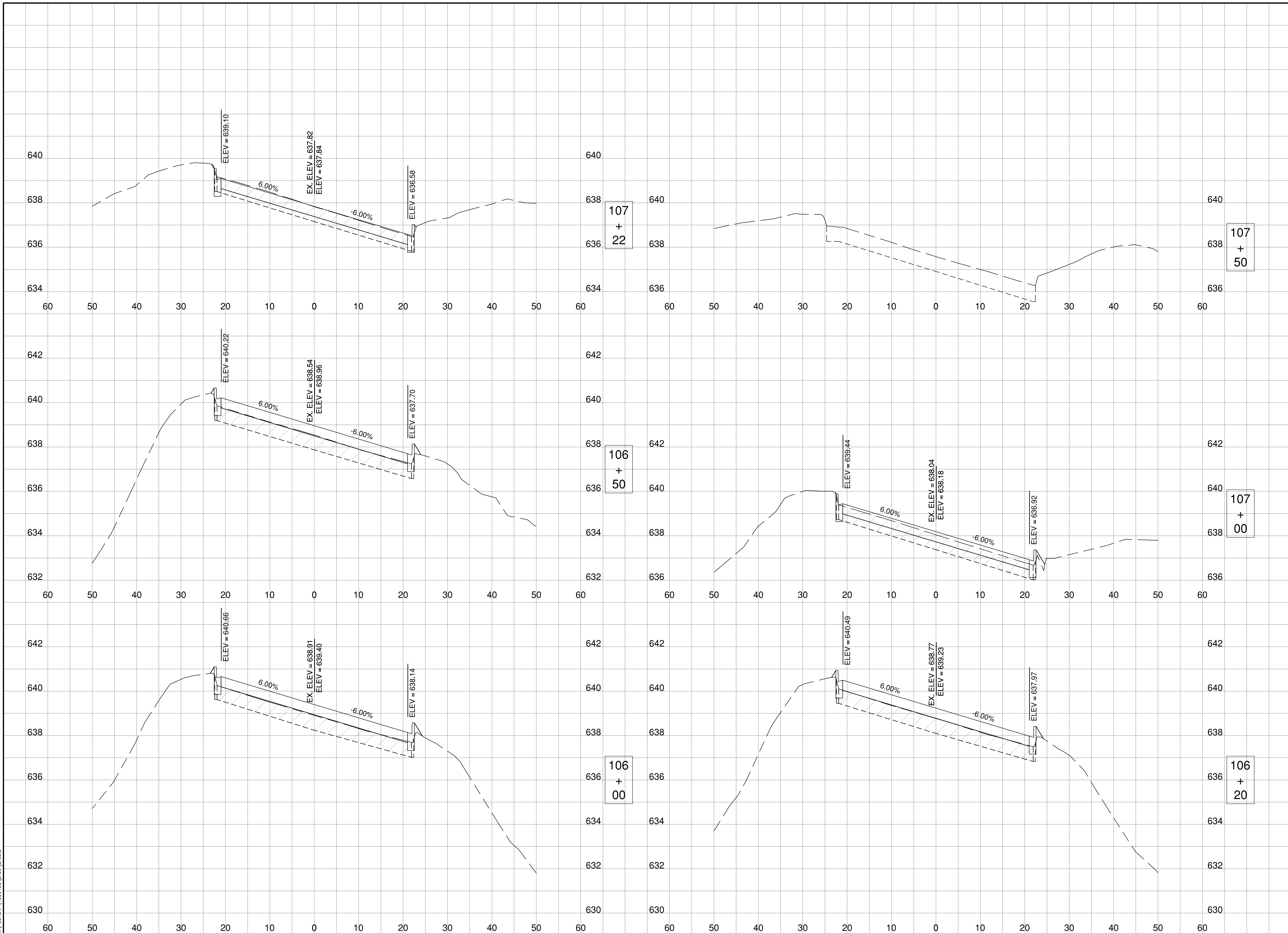
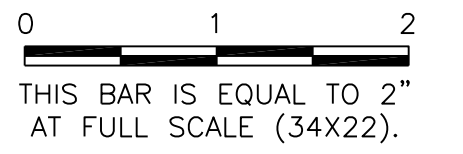
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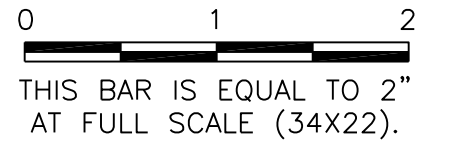
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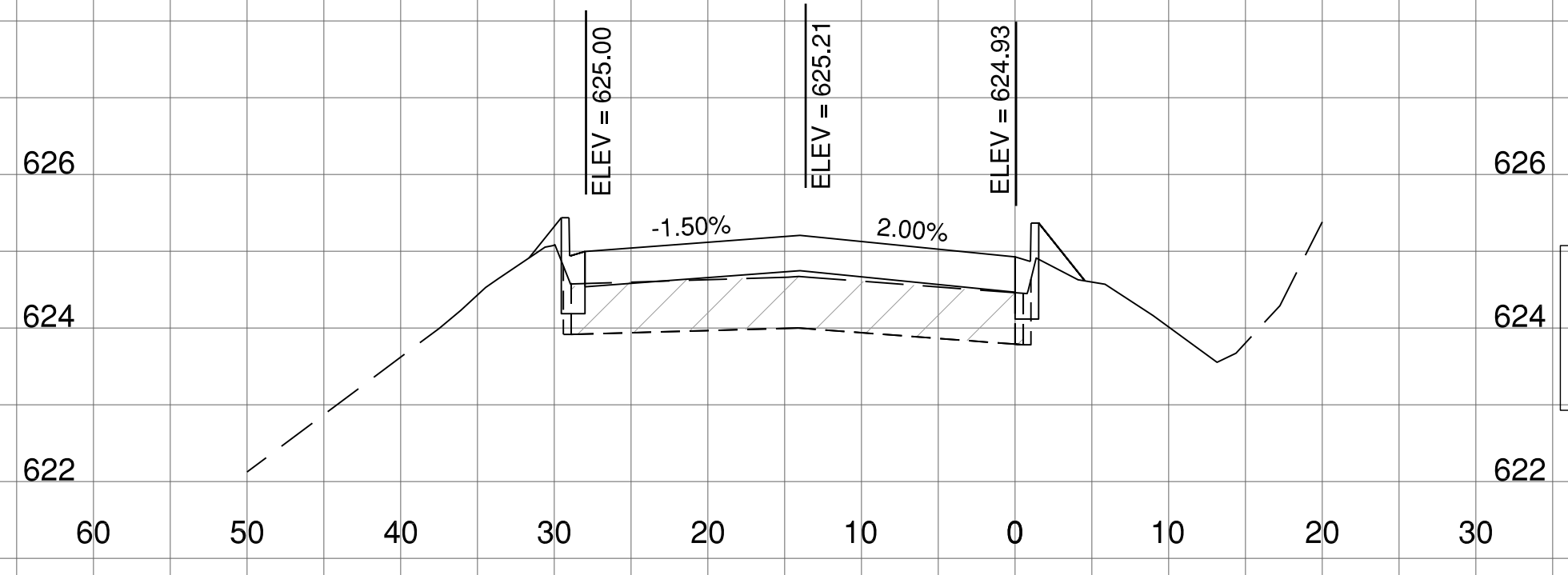
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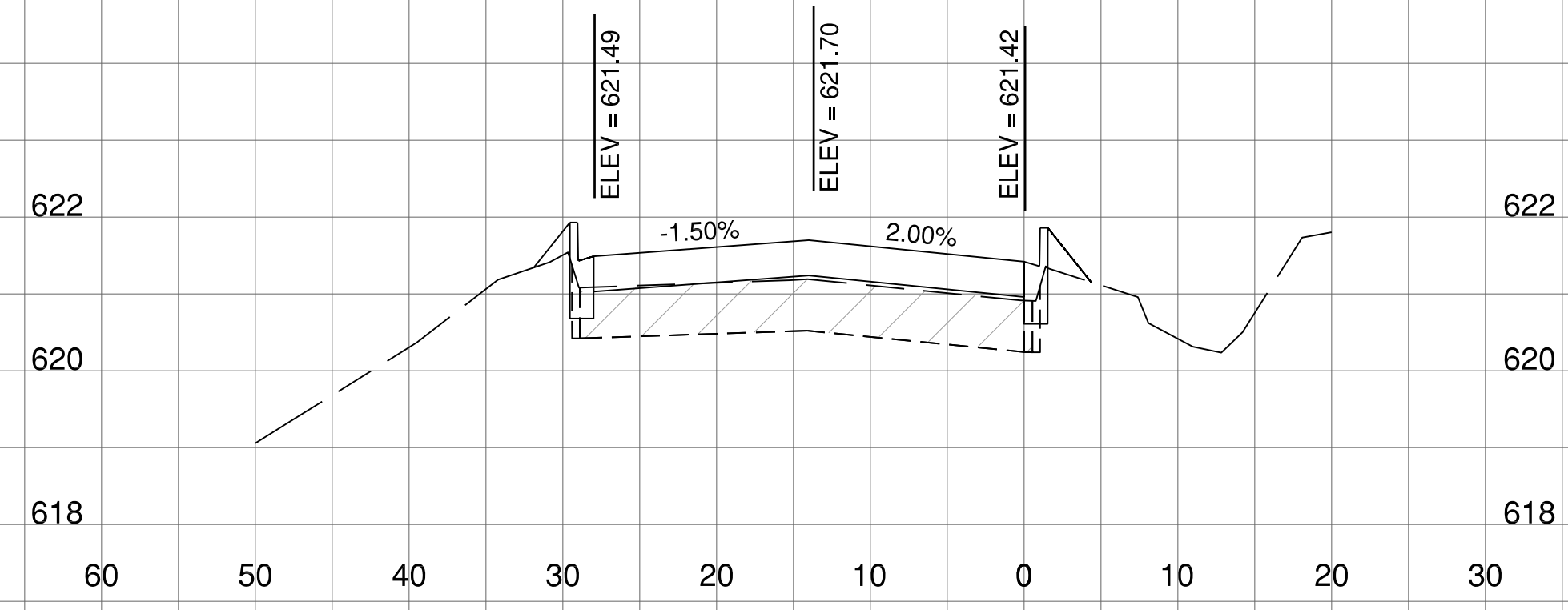
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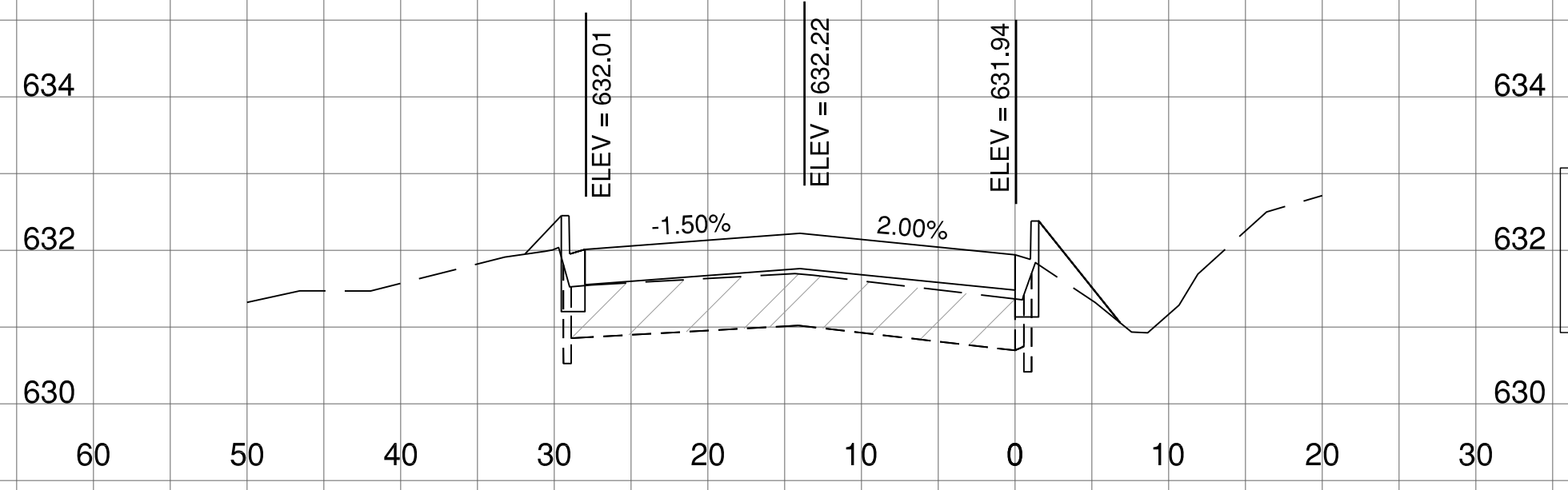
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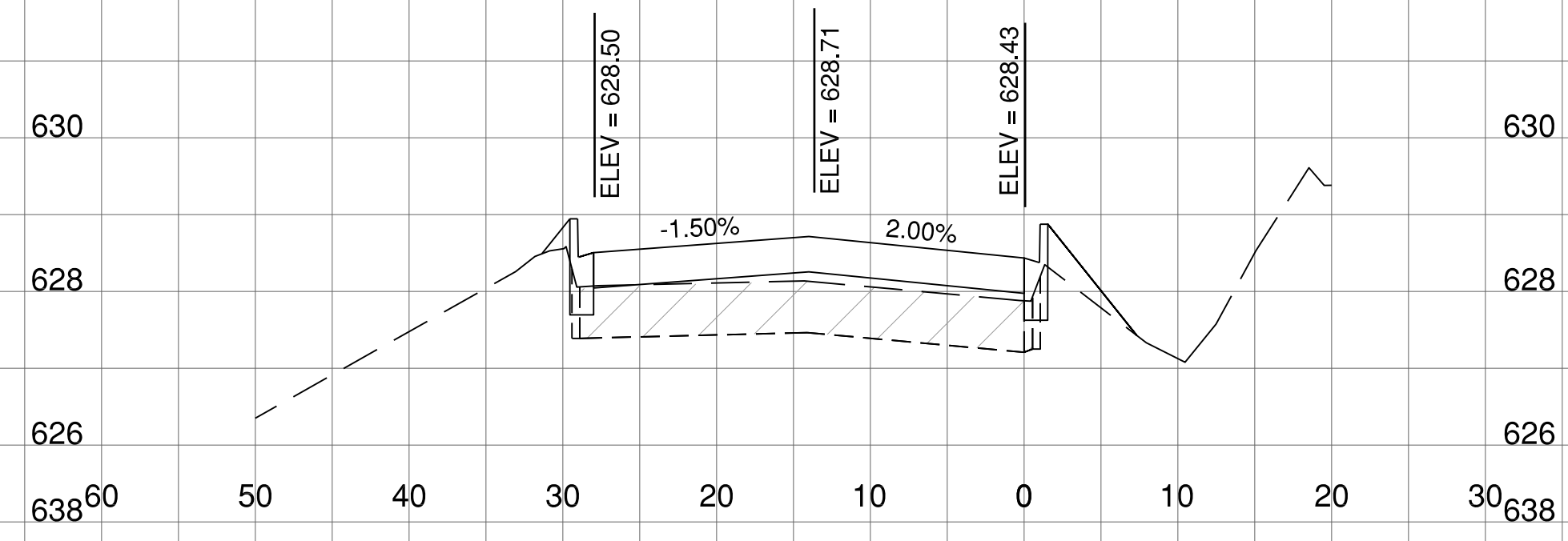
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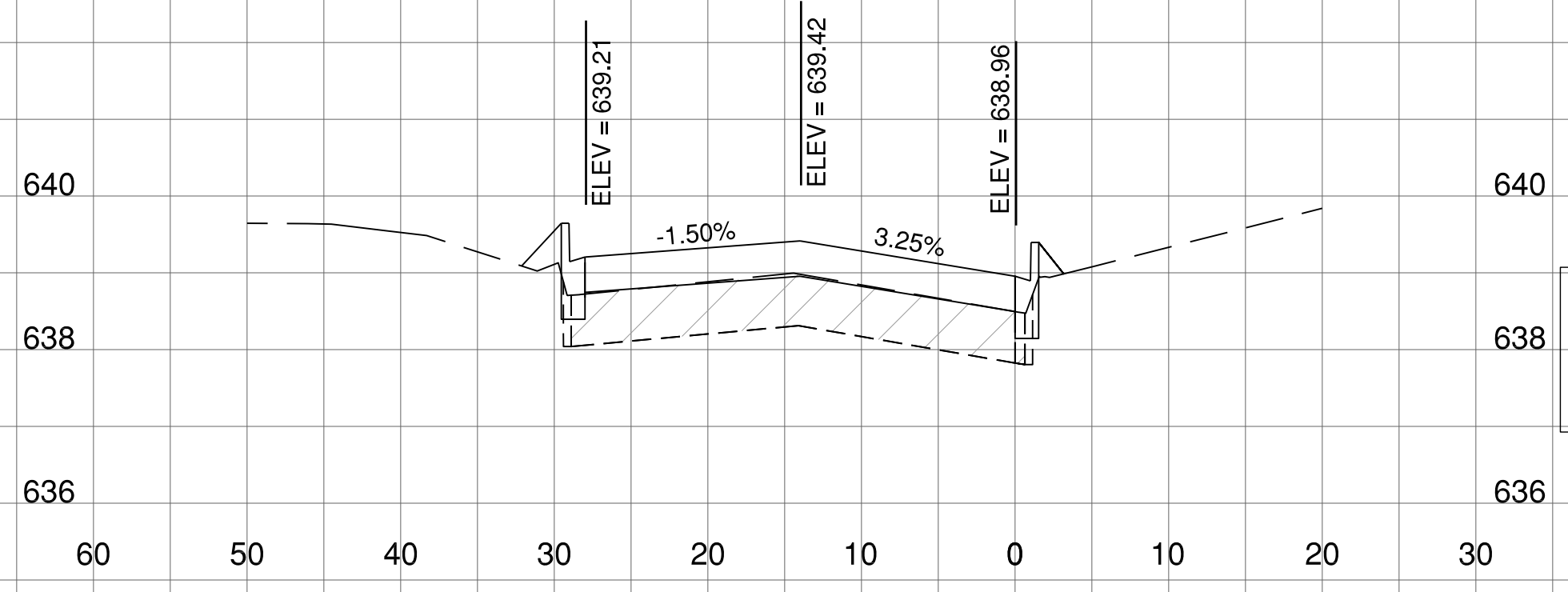
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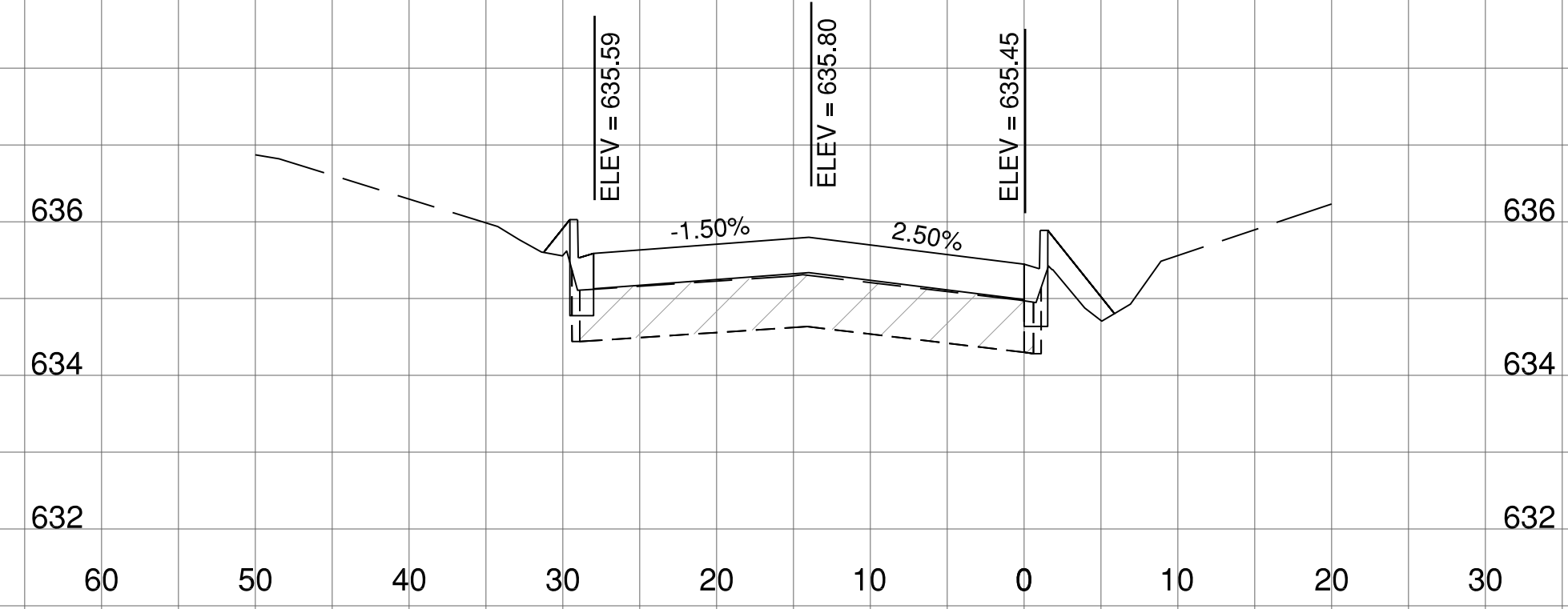
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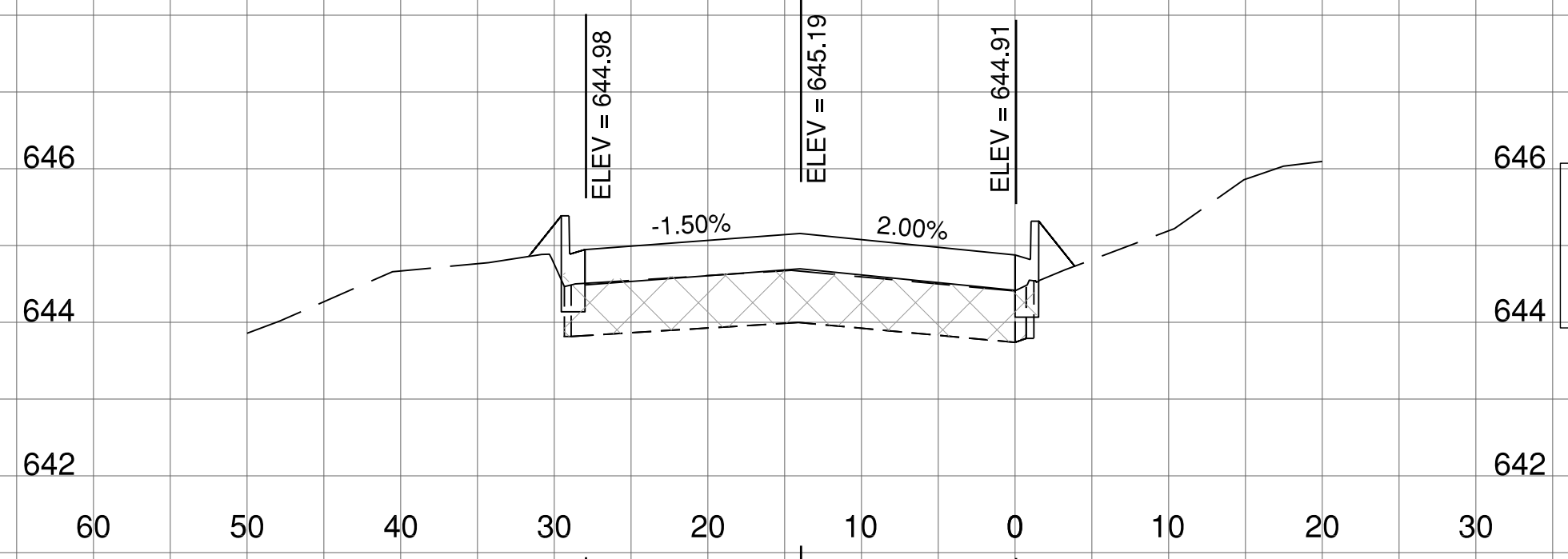
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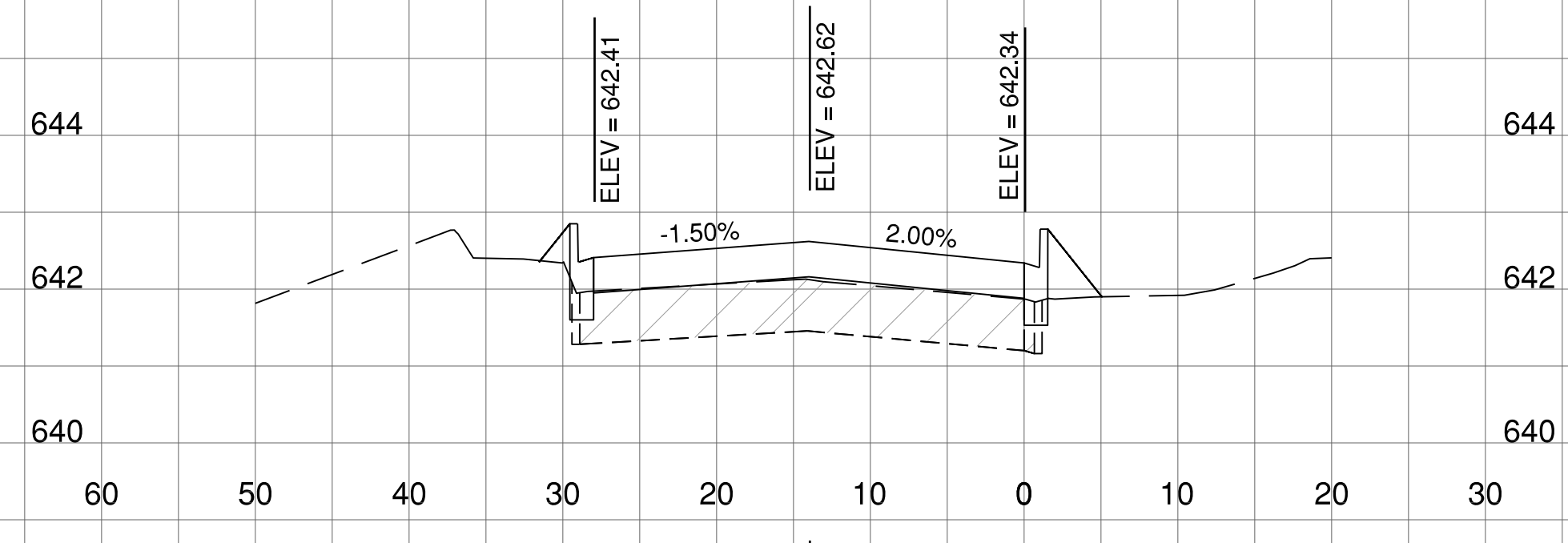
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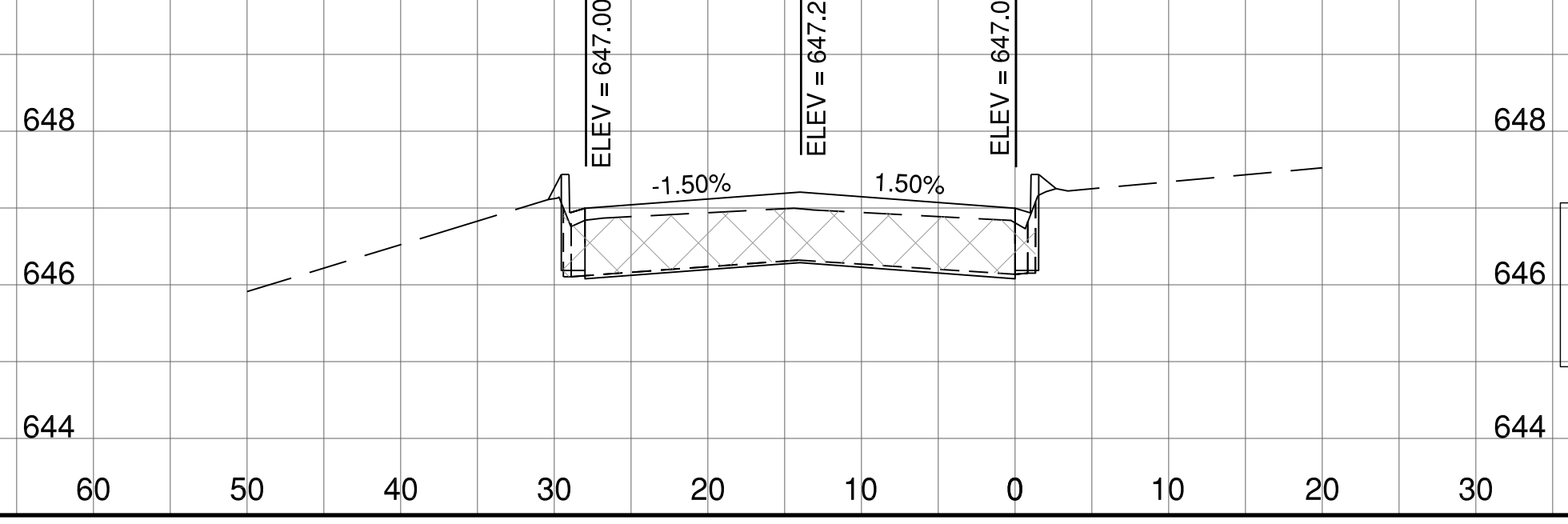
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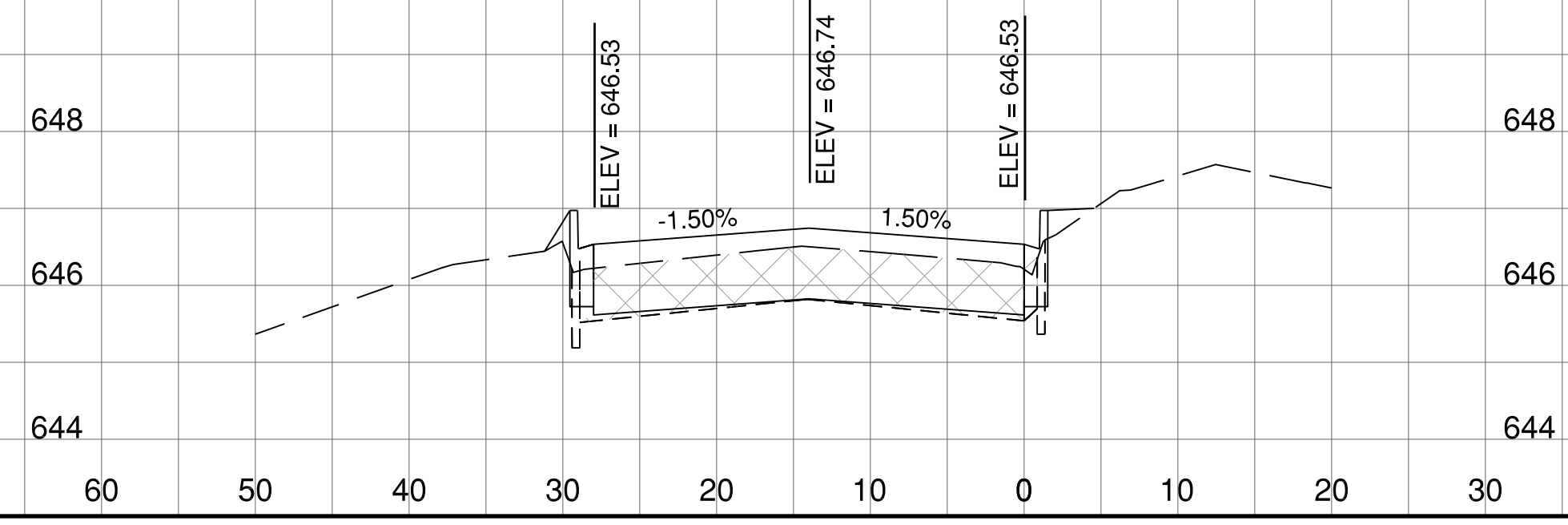
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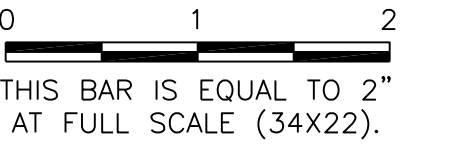
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**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
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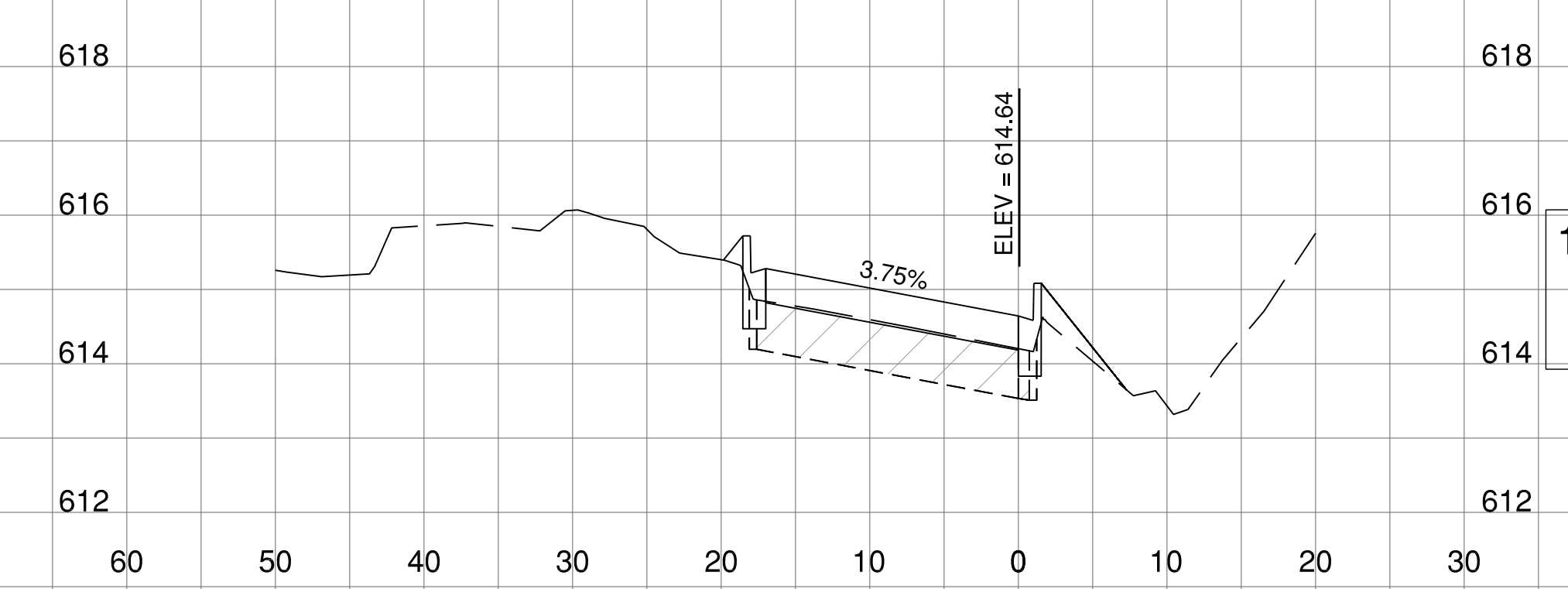
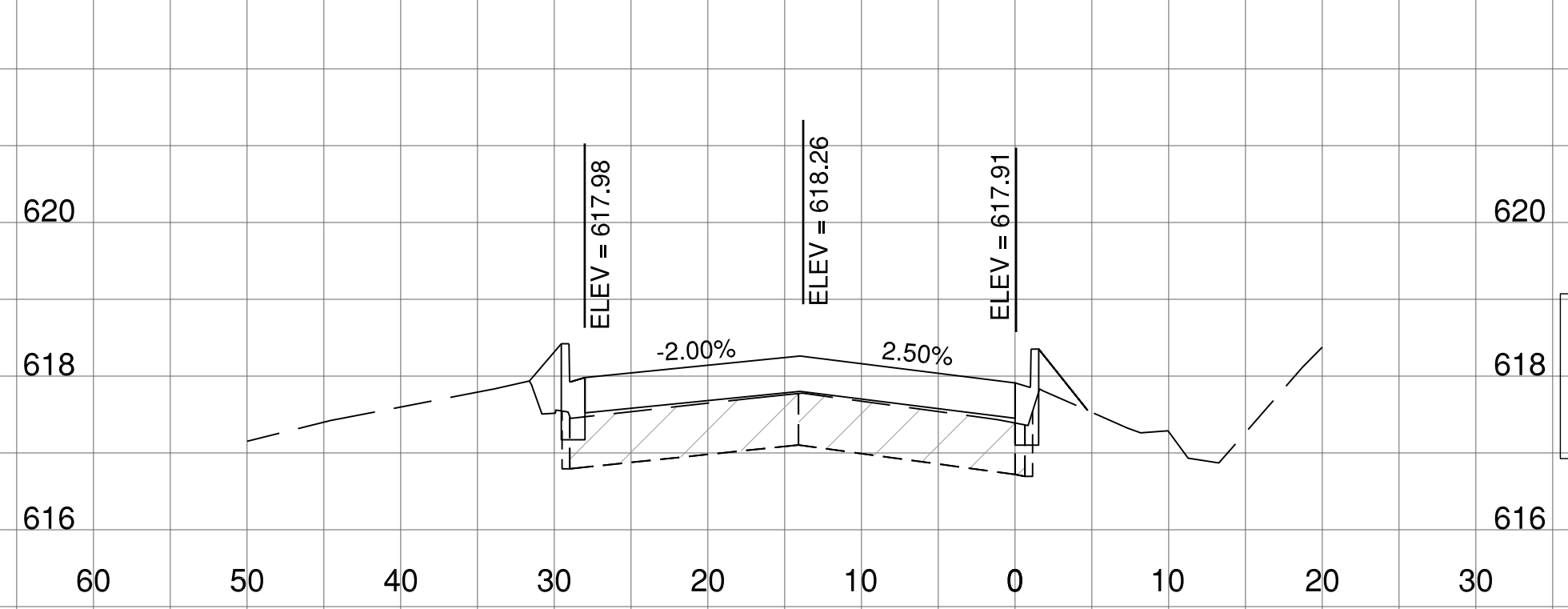
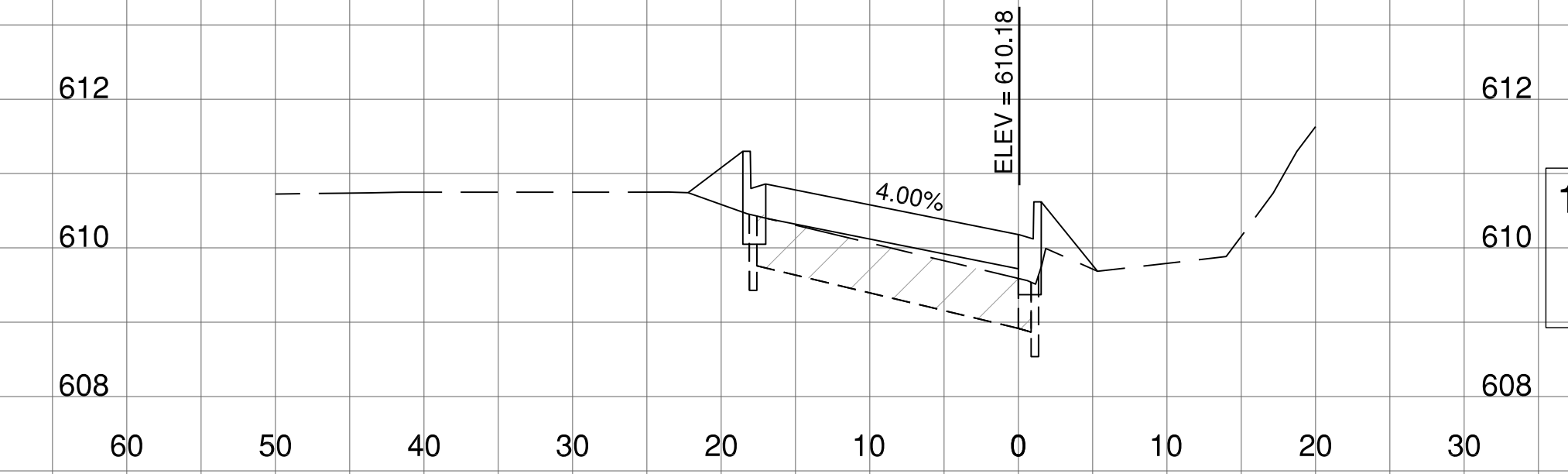
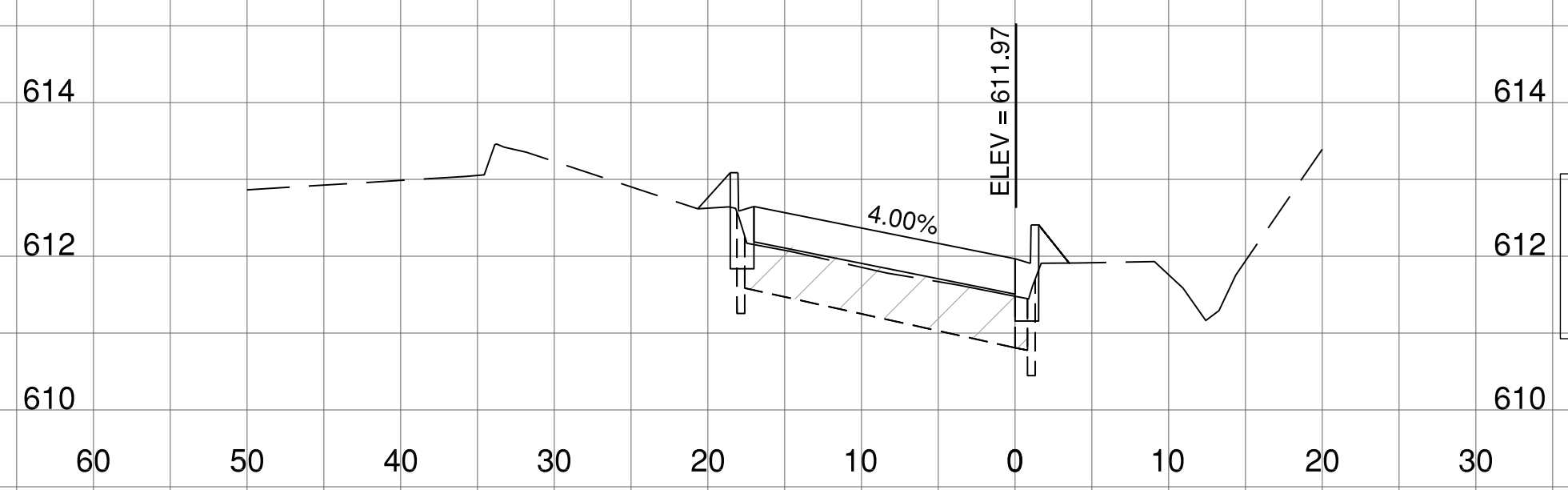


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JOB No:	13061-05
IL PROJ. NO. PIA-4415 AIP PROJ. NO. 3-17-0080-XX	
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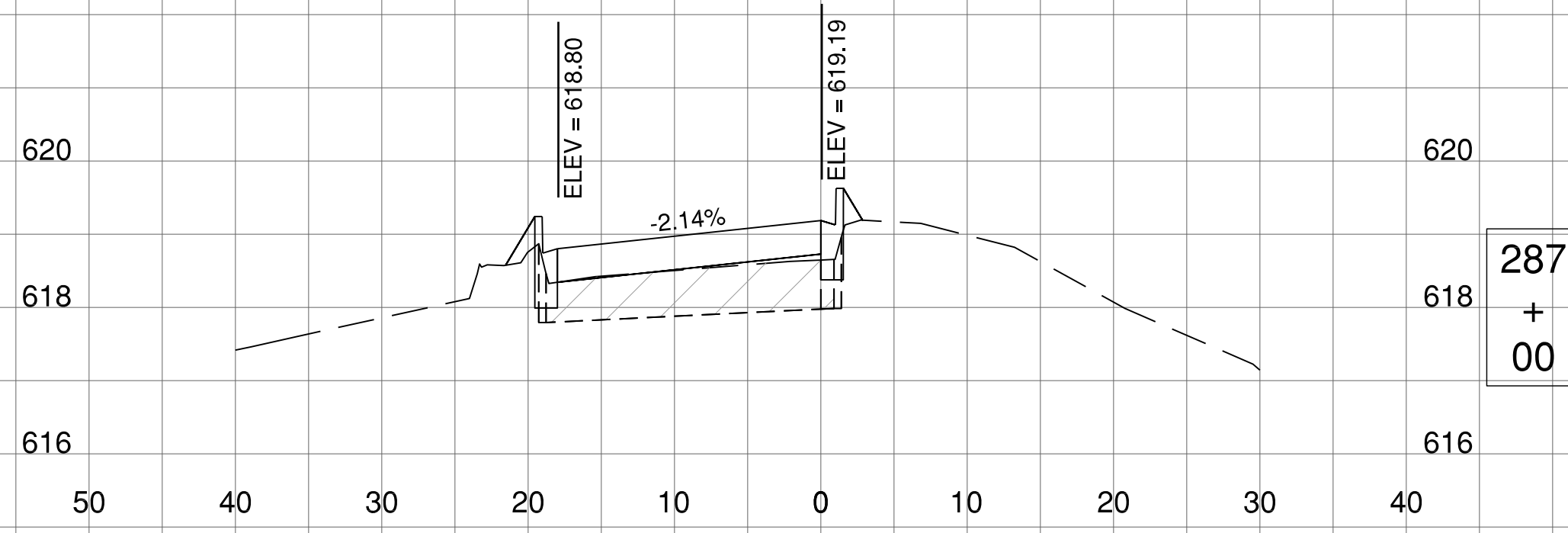
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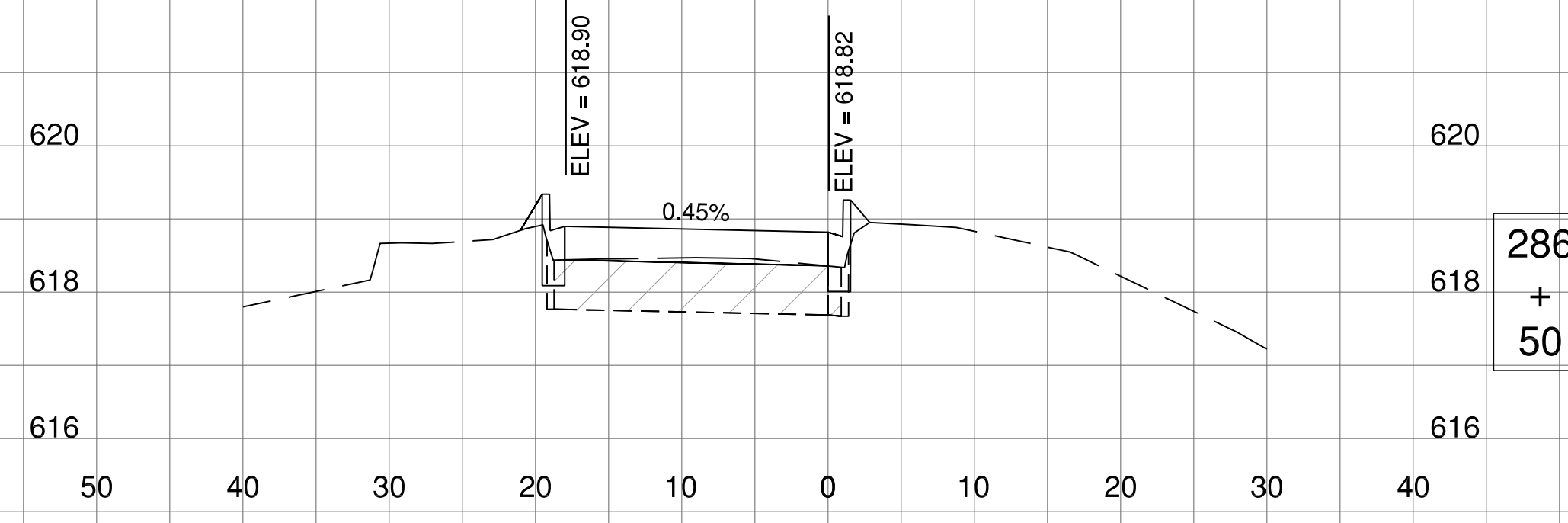
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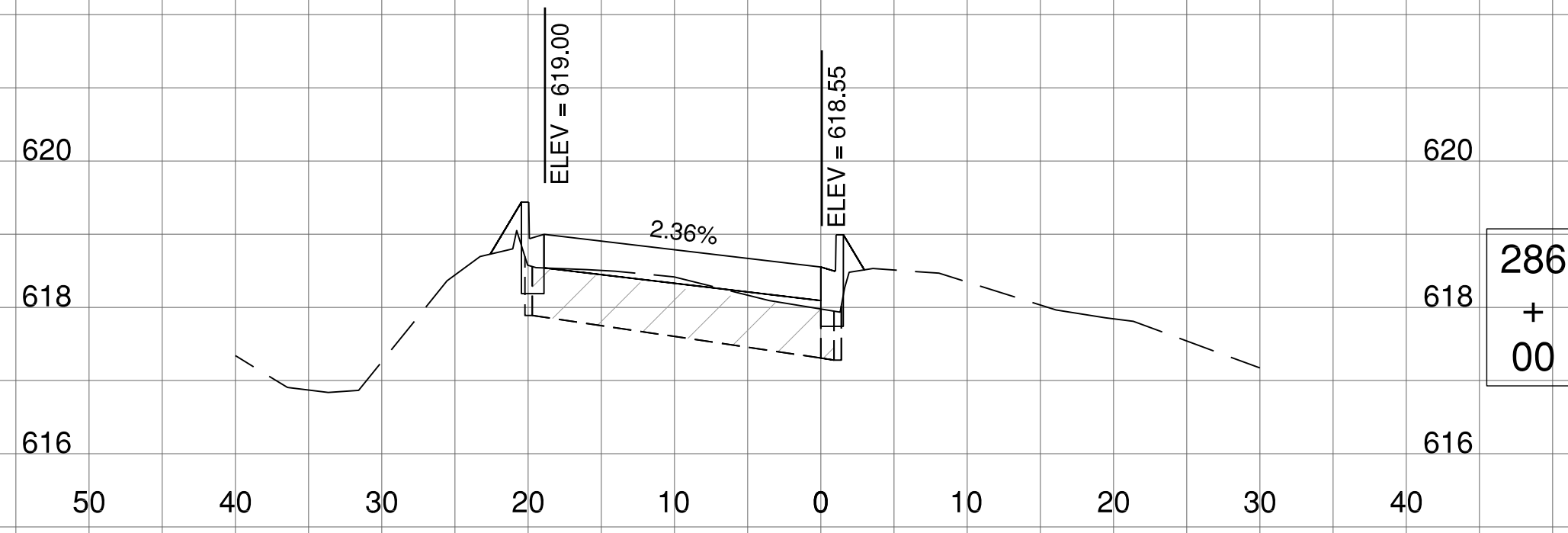
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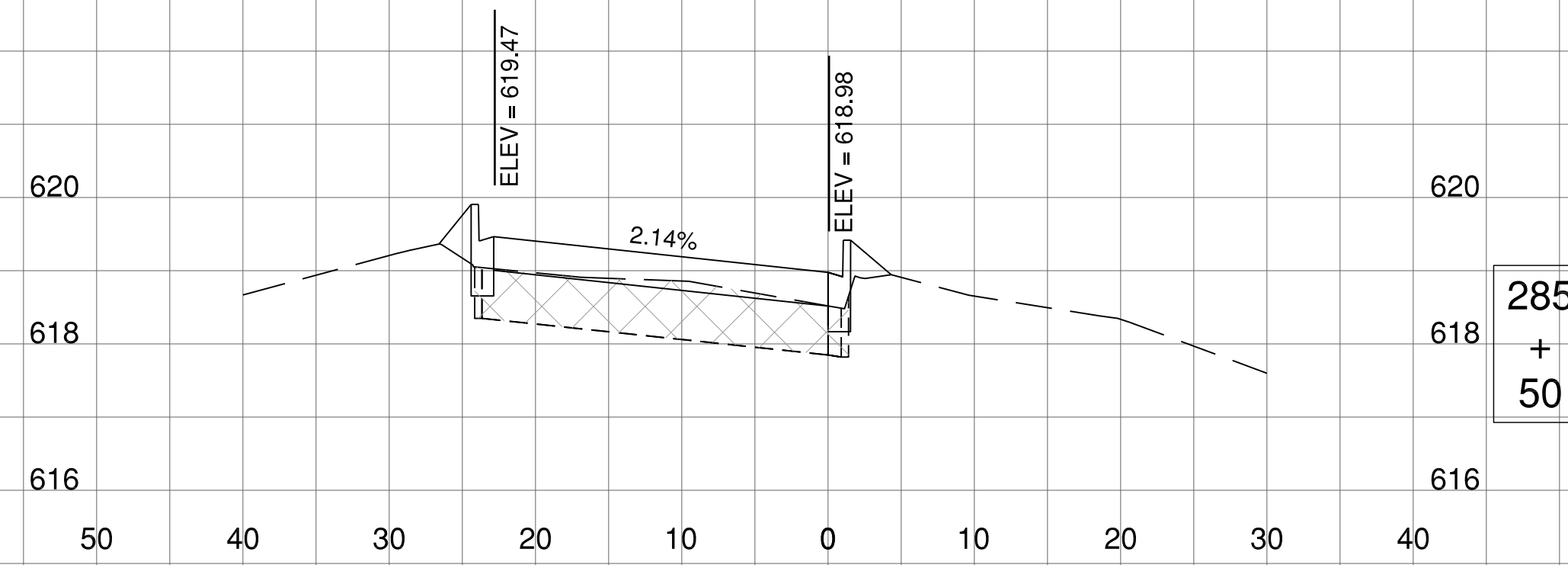
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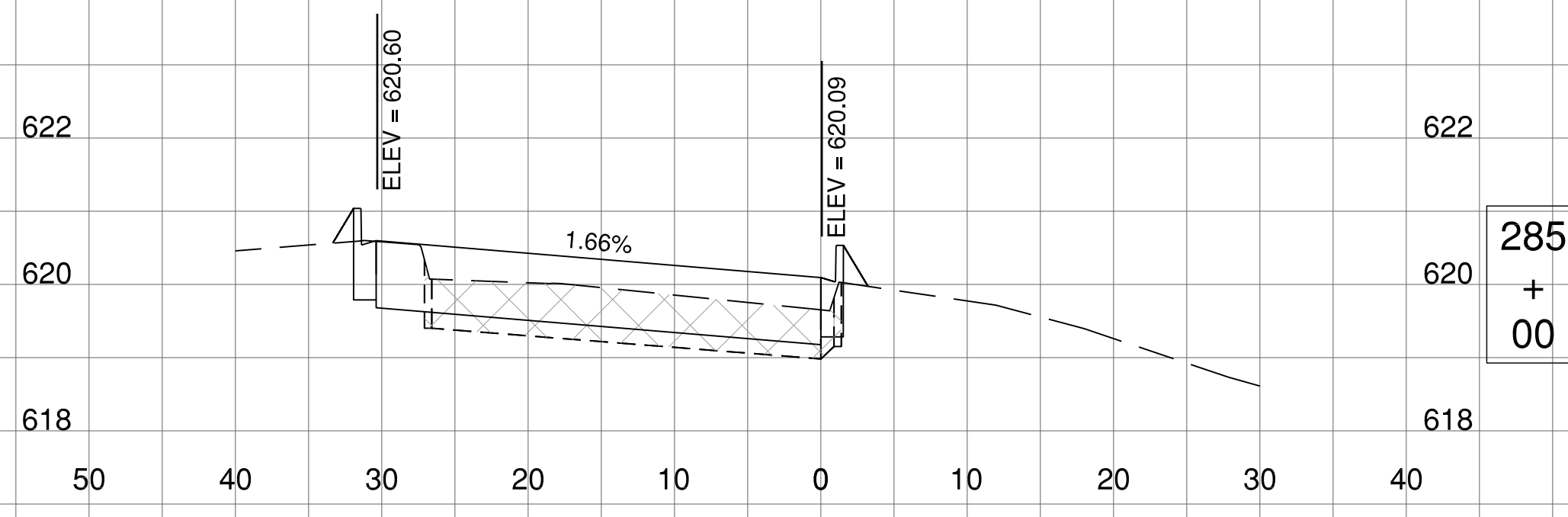
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**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
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CROSS SECTIONS - LOT EXIT ROAD B

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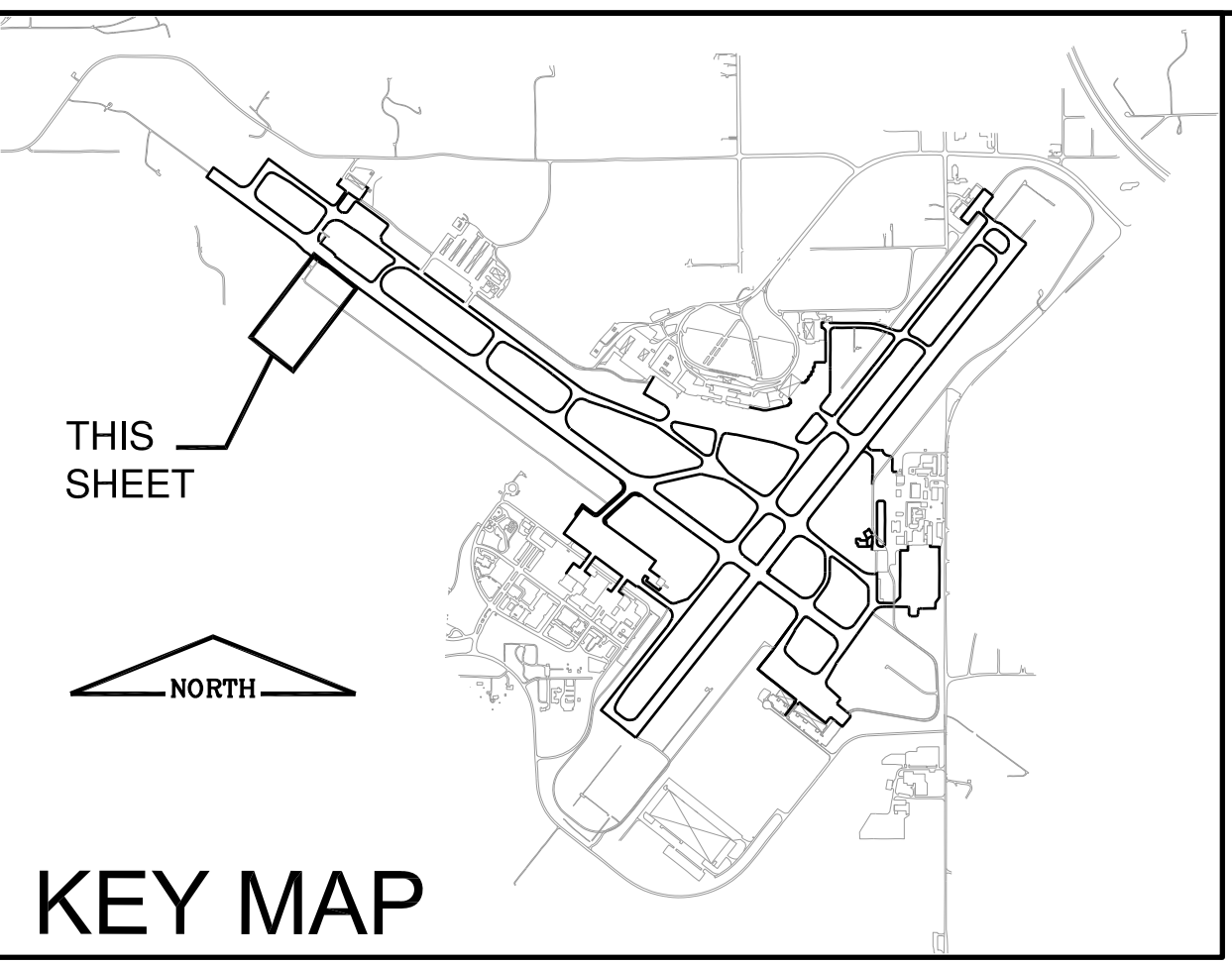
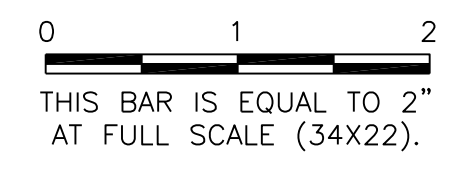
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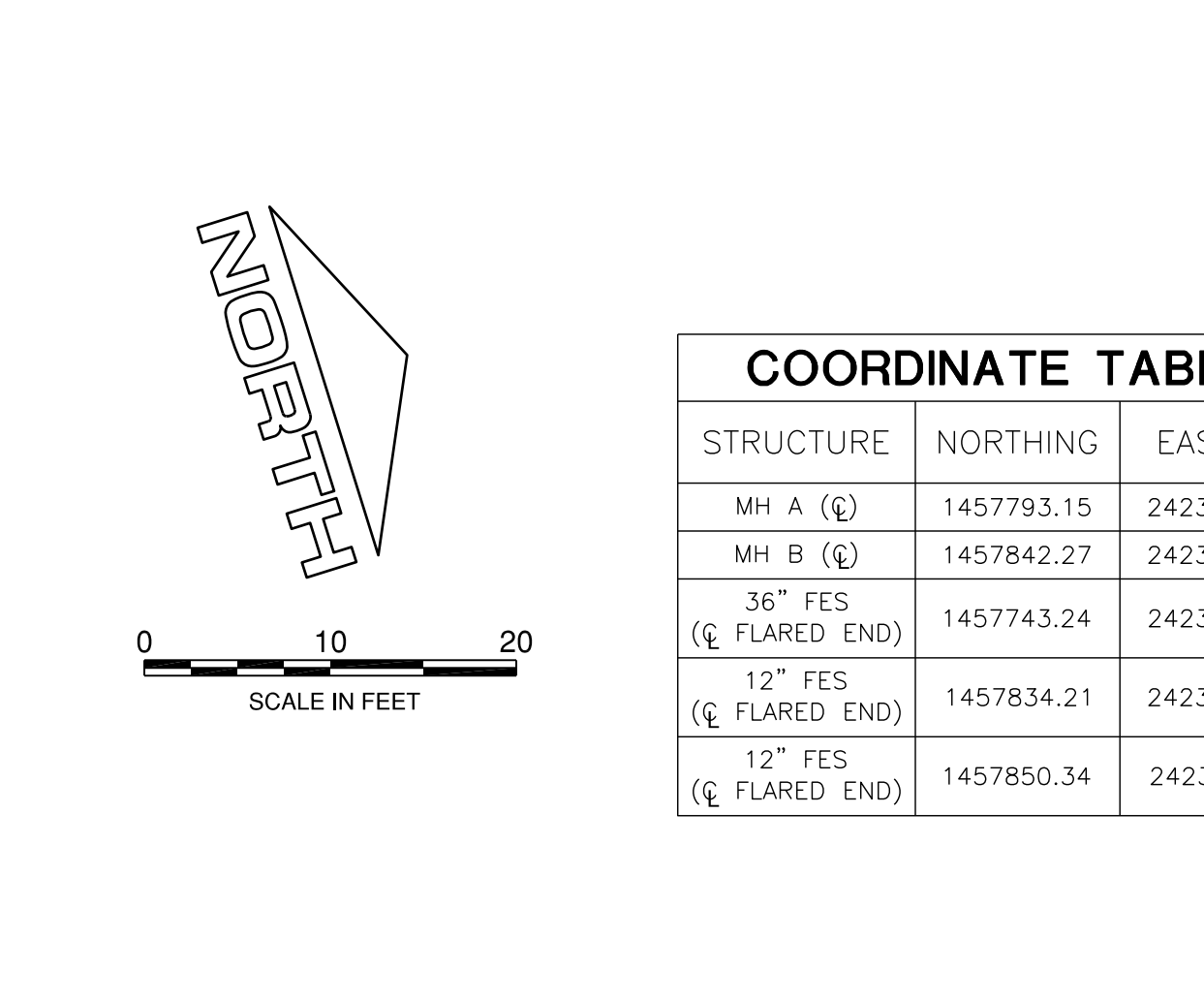
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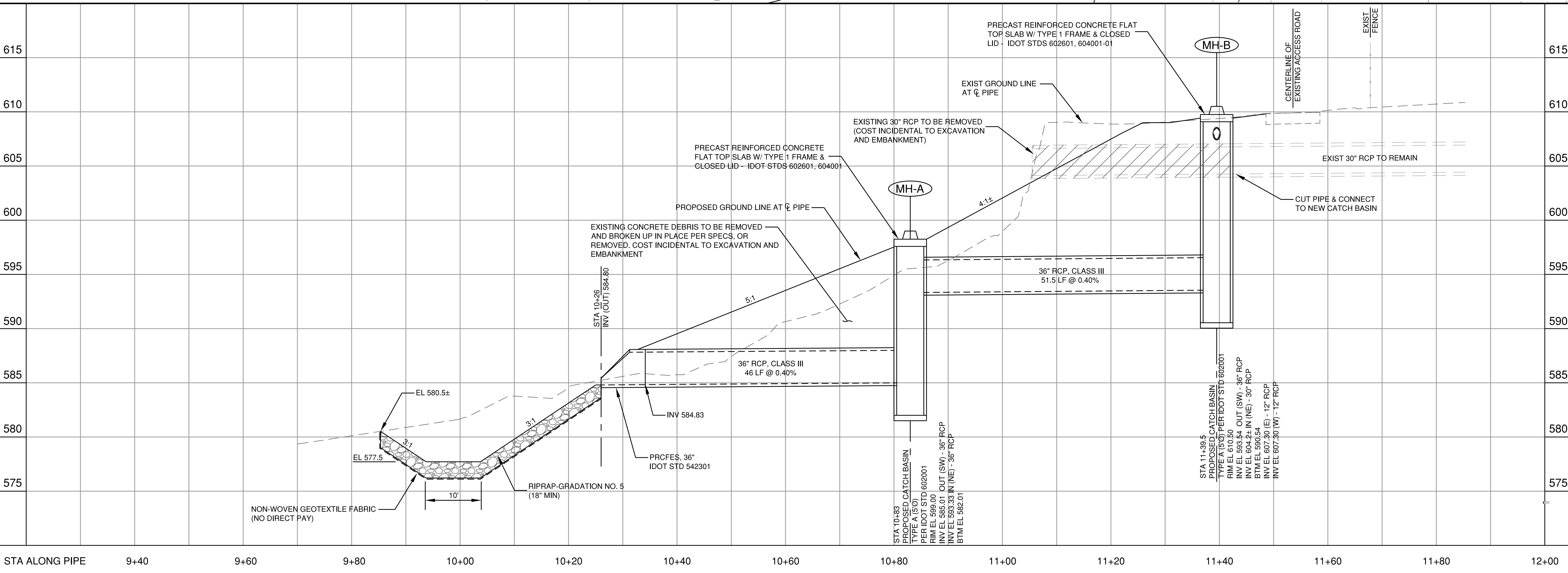
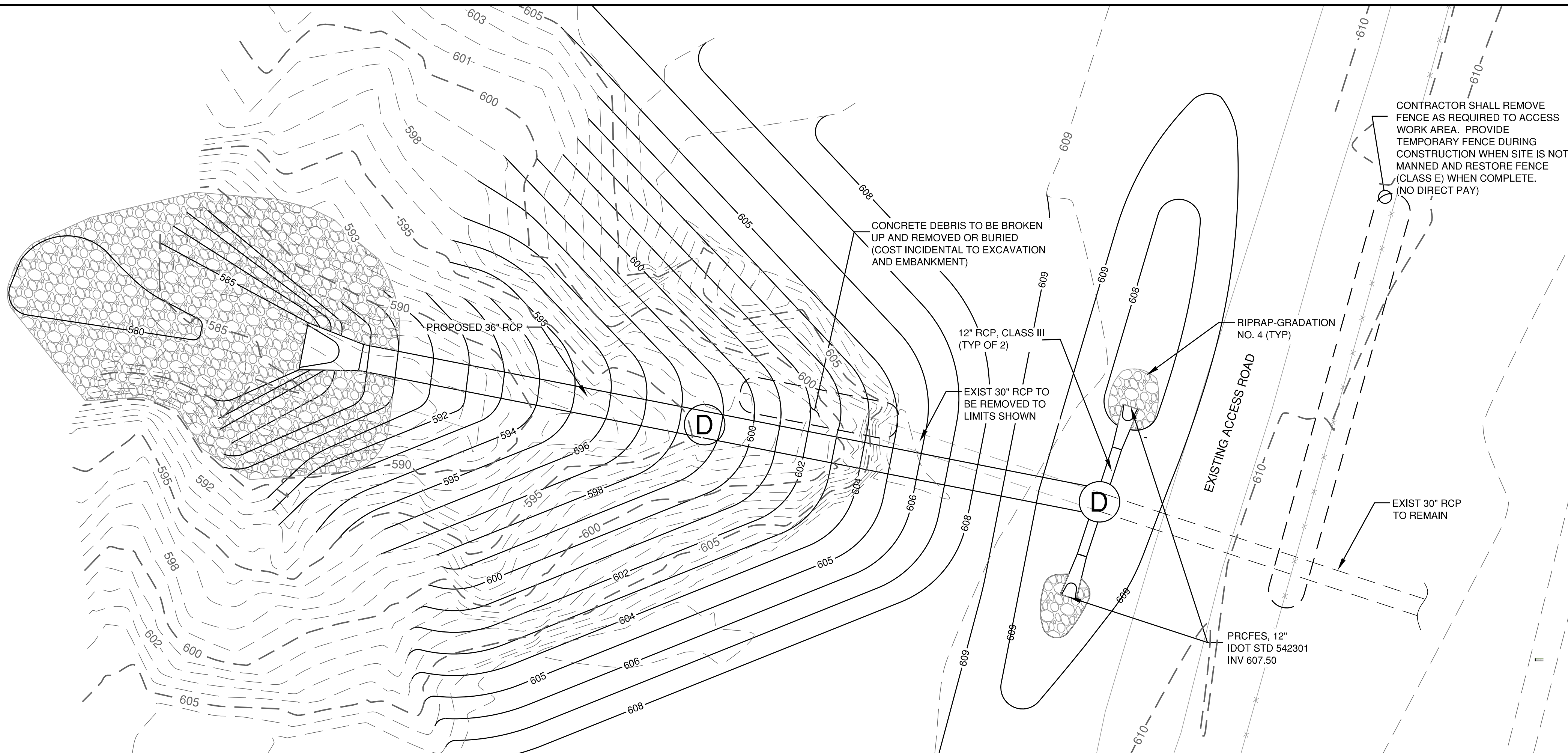
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KEY MAP



COORDINATE TABLE		
STRUCTURE	NORTHING	EASTING
MH A (C)	1457793.15	2423619.37
MH B (C)	1457842.27	2423645.92
36" FES (C FLARED END)	1457743.24	2423592.39
12" FES (C FLARED END)	1457834.21	2423657.22
12" FES (C FLARED END)	1457850.34	2423634.61



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PLAN AND PROFILE - DRAINAGE OUTFALL

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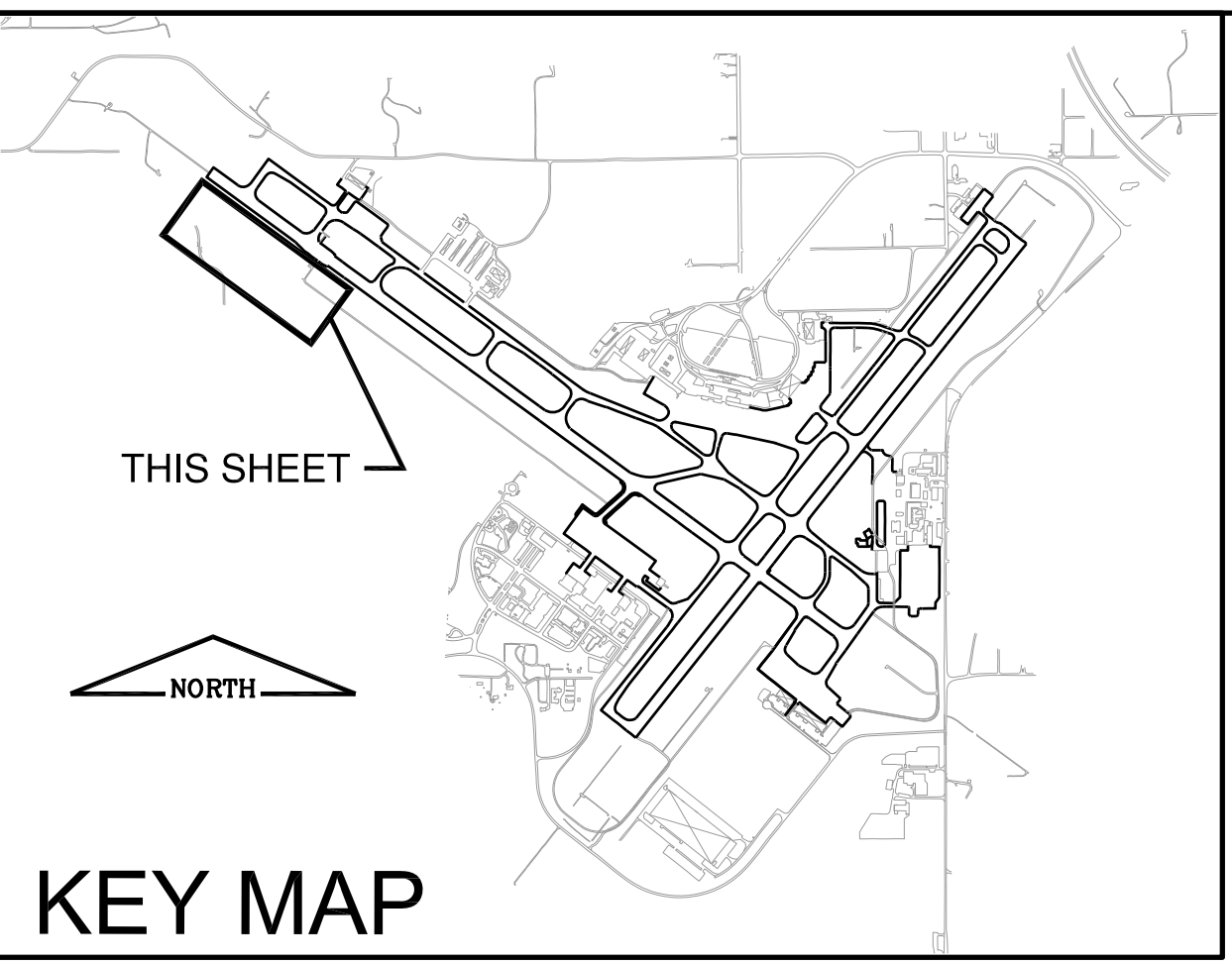
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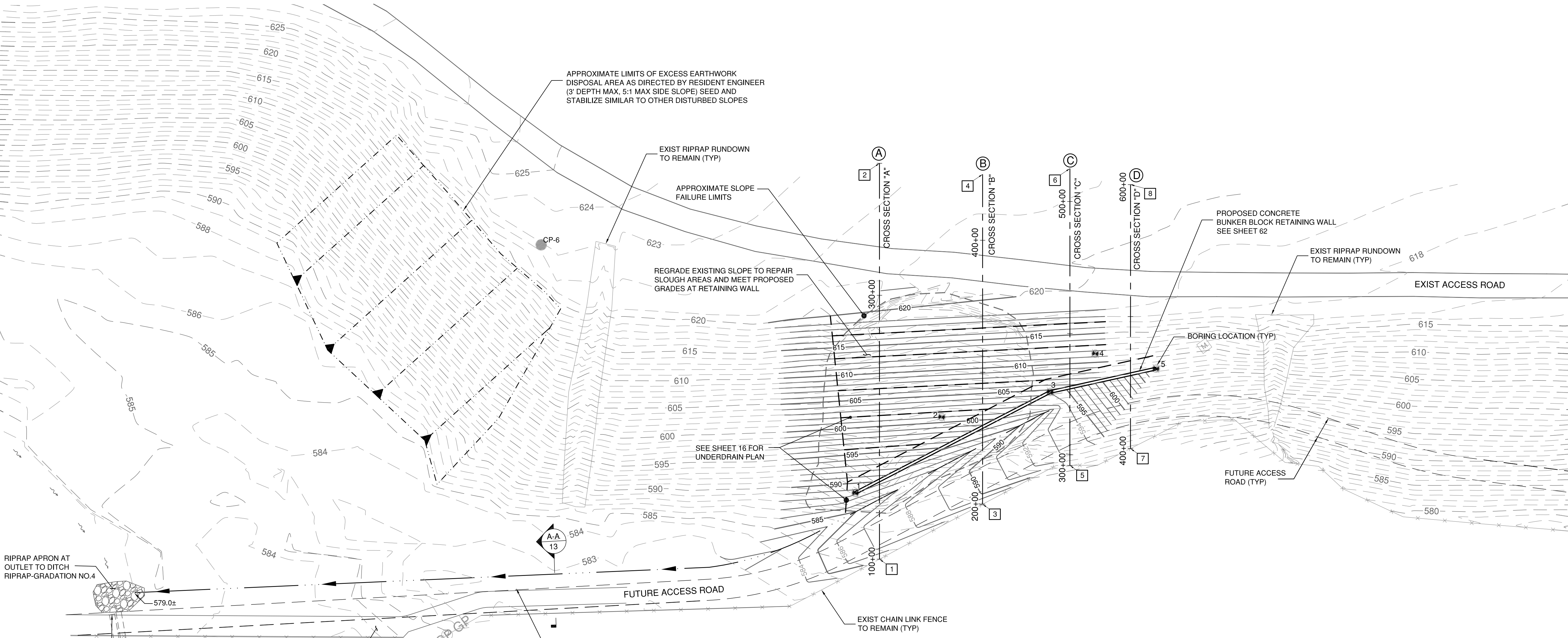
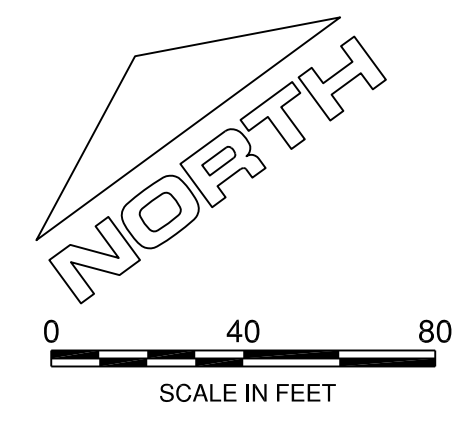
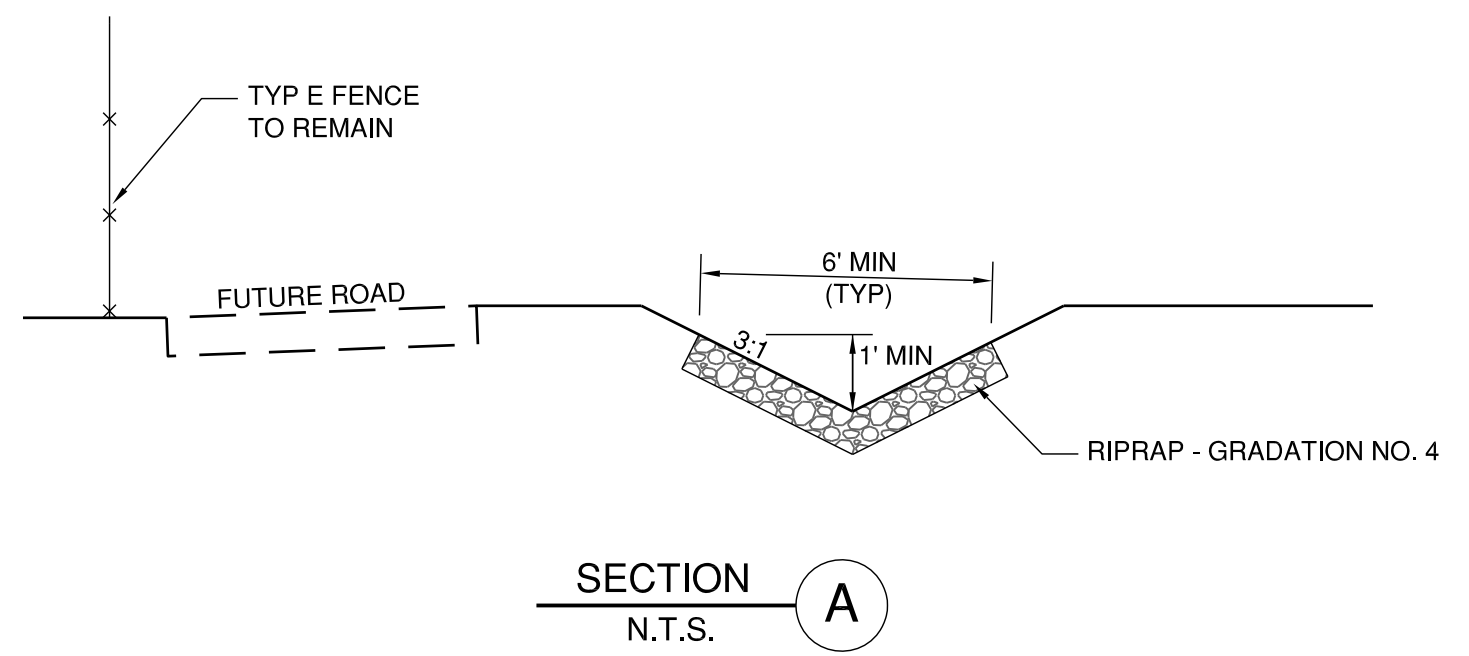
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 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



RUNWAY 13-31



XS CONTROL POINTS			
POINT	XS STA	NORTHING	EASTING
1	100+00	1458644.21	2422344.34
2	300+00	1458886.10	2422521.79
3	200+00	1458631.61	2422431.97
4	450+00	1458833.19	2422579.85
5	300+00	1458616.20	2422502.67
6	525+00	1458787.61	2422635.76
7	400+00	1458599.36	2422547.25
8	600+00	1458760.62	2422665.55

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SLOPE STABILIZATION AREA PLAN

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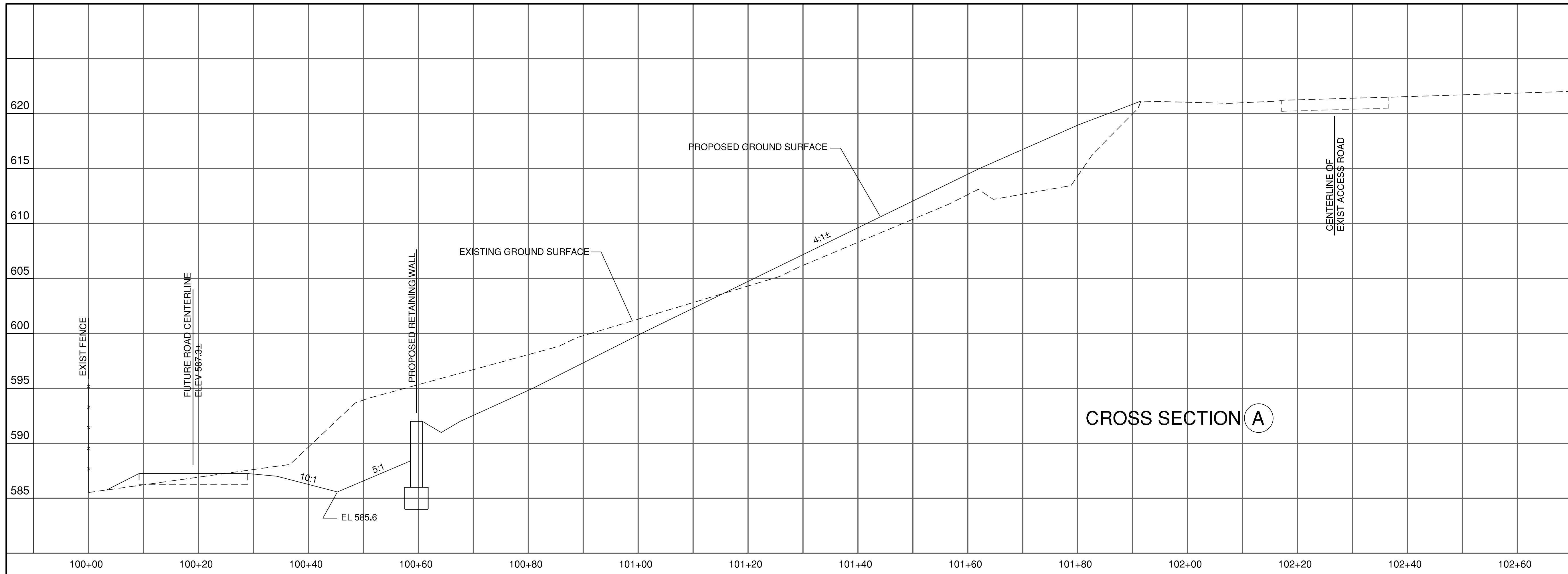
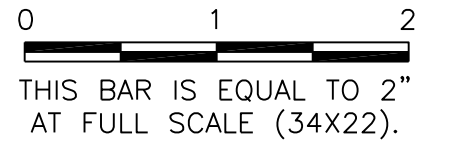
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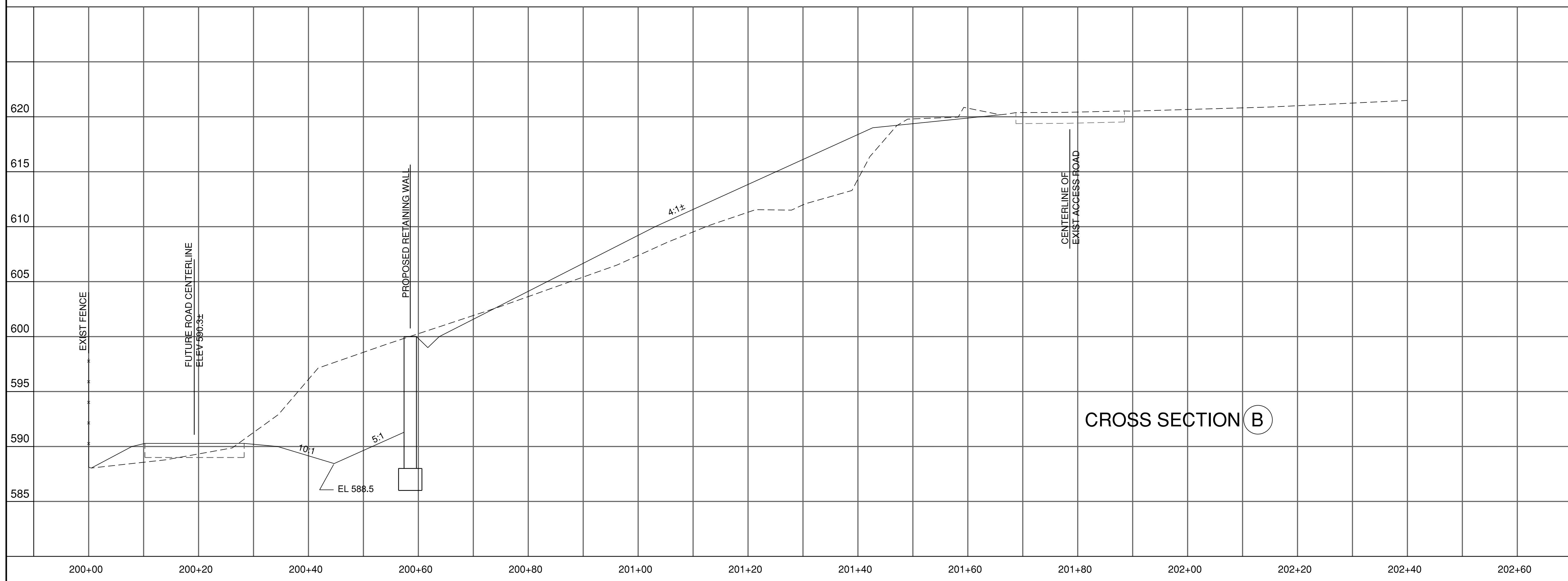
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CROSS SECTION (A)



CROSS SECTION (B)

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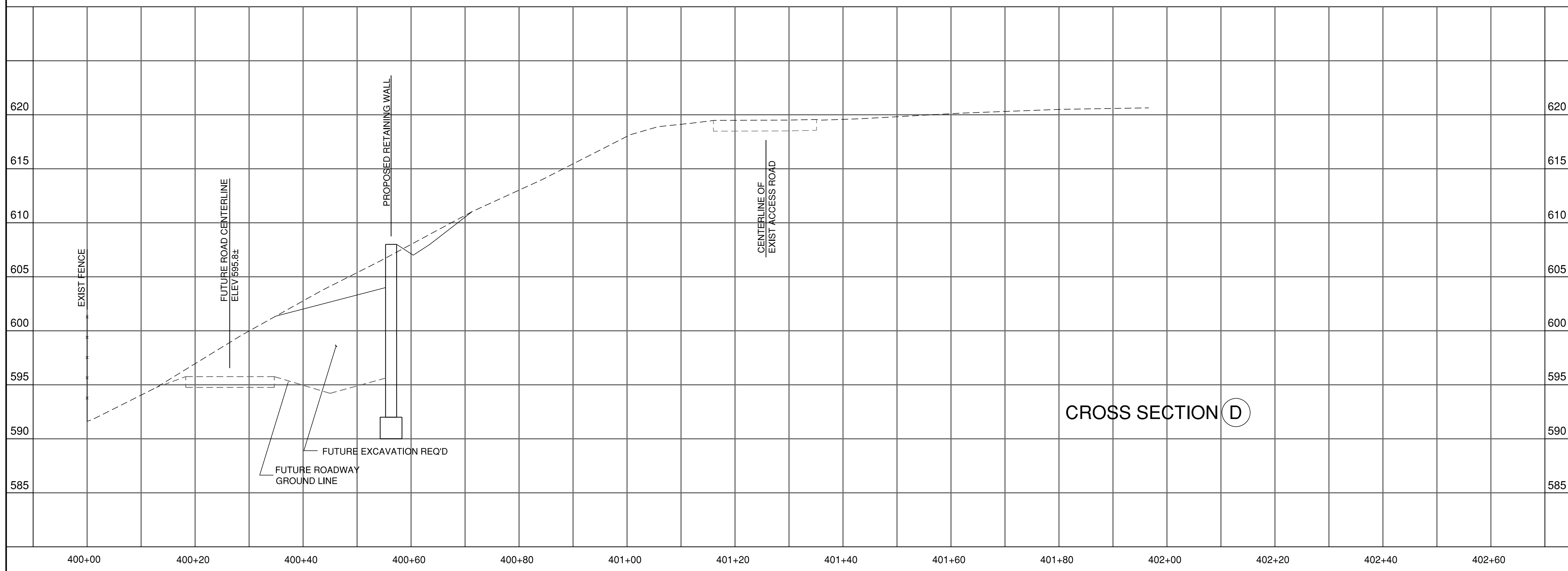
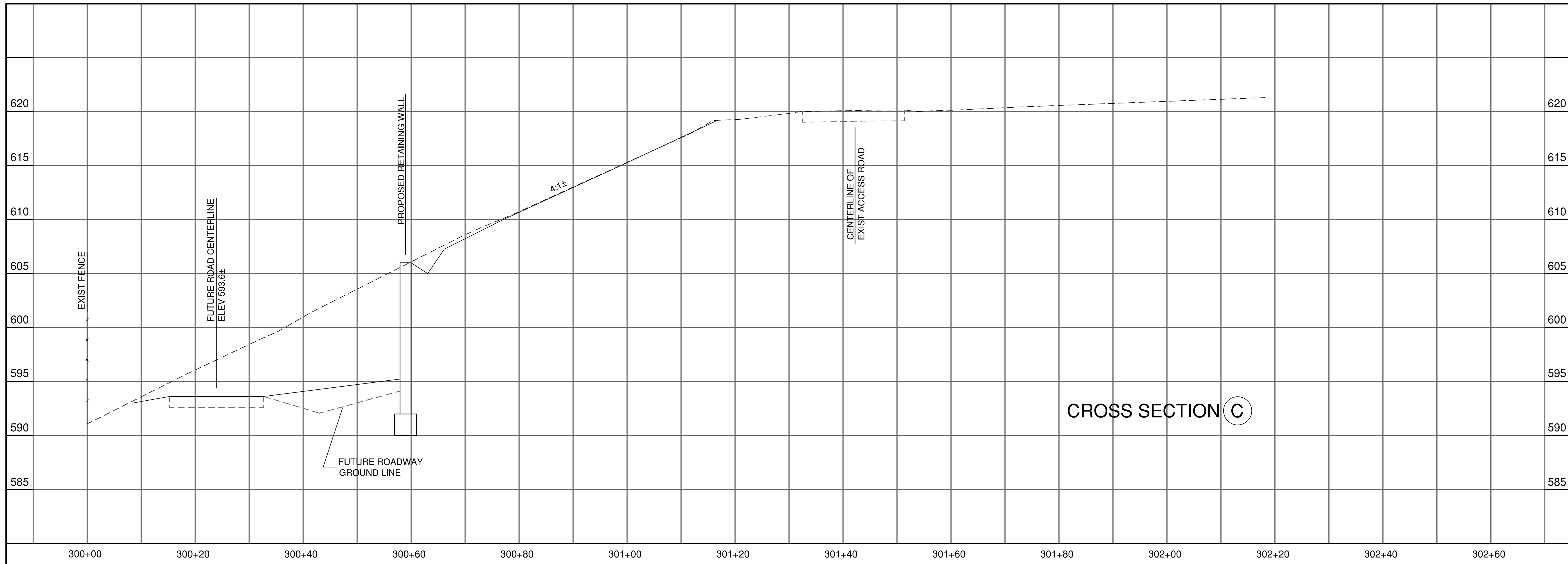
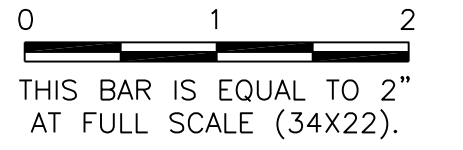
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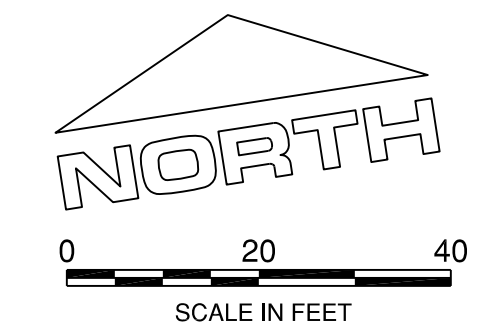
**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
 REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2**

CROSS SECTIONS - STABILIZATION AREA

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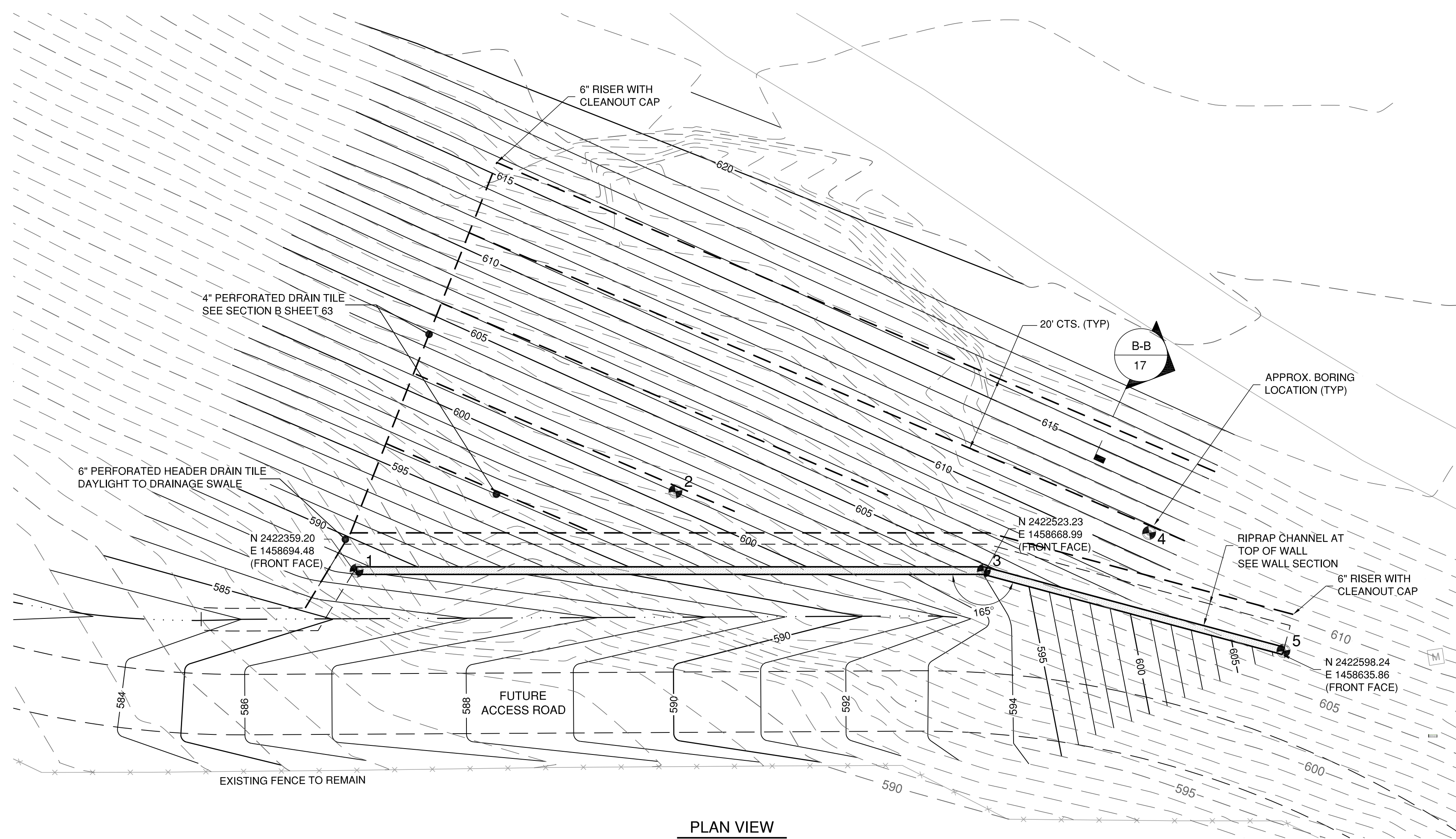
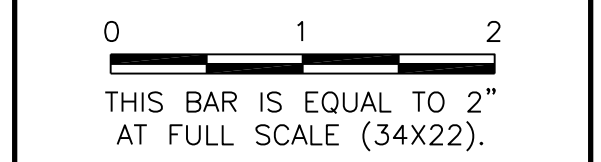
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DATE:	JULY 11, 2014
JOB No:	13061-05
IL PROJ. NO. PIA-4415 AIP PROJ. NO. 3-17-0080-XX	
SHEET 61 OF 67 SHEETS	



PE098

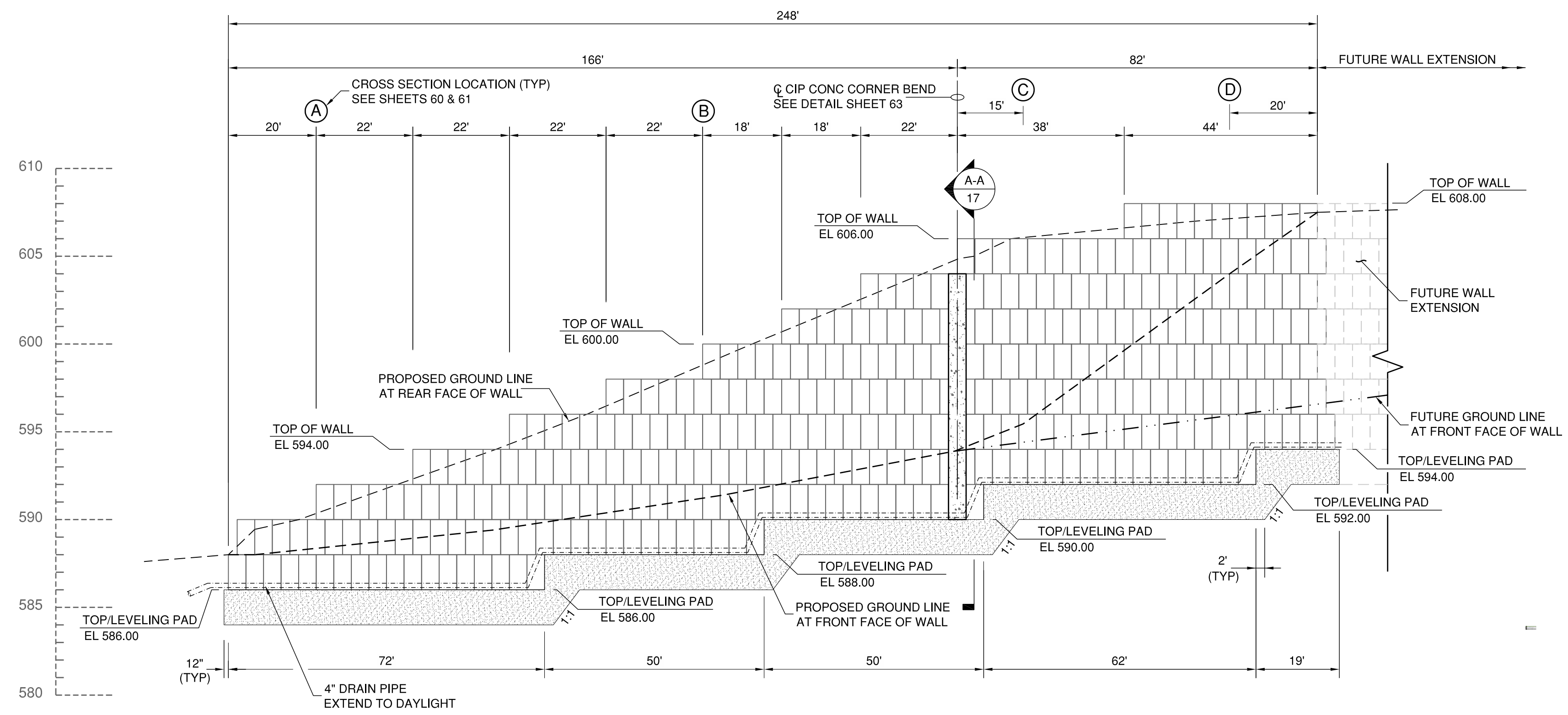
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PLAN VIEW
 SCALE: 1" = 20'

GENERAL NOTES

- SEE SHEET 28 AND 59 FOR HORIZONTAL & VERTICAL CONTROL INFORMATION.
- CONTRACTOR TO VERIFY BEARING CAPACITY OF SOIL BENEATH LEVELING PAD. SOIL SHALL HAVE A MINIMUM BEARING CAPACITY OF 1000 PSF. NOTIFY ENGINEER IN ADVANCE TO BE PRESENT DURING TEST.
- CONTRACTOR RESPONSIBLE FOR ANY DEWATERING OF EXCAVATIONS TO PERFORM THE WORK PER THE PLANS AND SPECIFICATIONS. NO ADDITIONAL COMPENSATION FOR DEWATERING ALLOWED.



ELEVATION VIEW
 HORZ: 1" = 20'
 VERT: 1" = 5'

**GENERAL WAYNE A. DOWNING
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 PEORIA, ILLINOIS**

**DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION &
 REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
 RETAINING WALL DETAILS 1**

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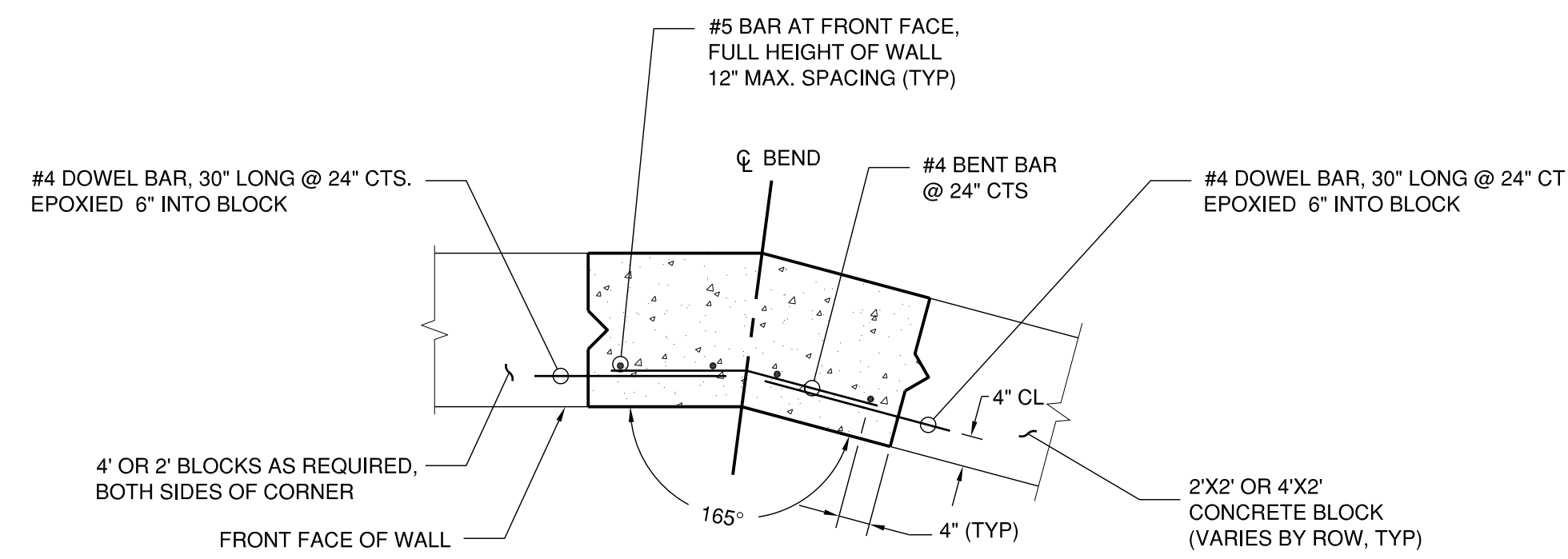
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SHEET	62 OF 67 SHEETS

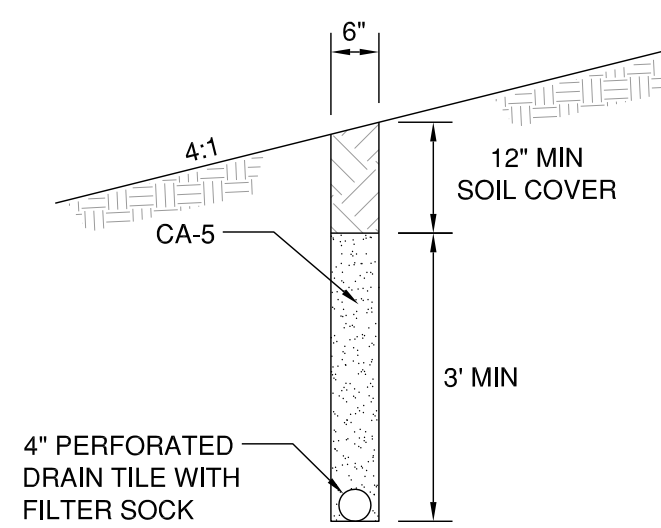
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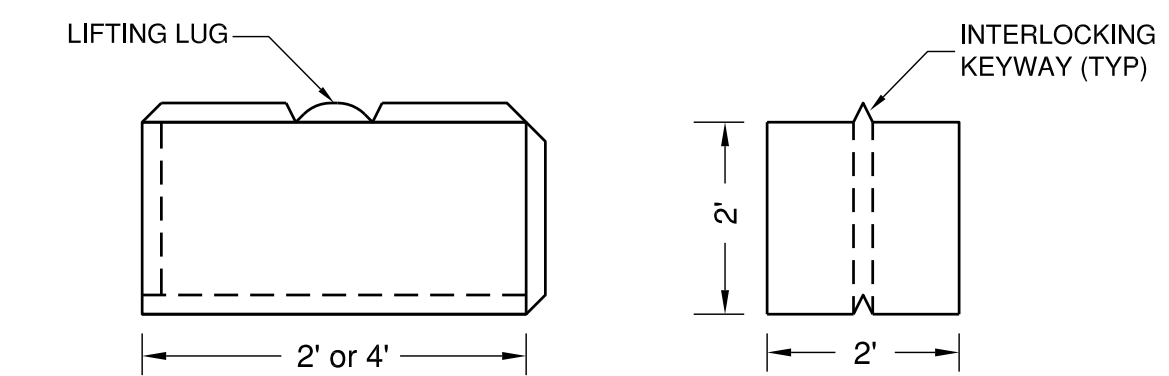
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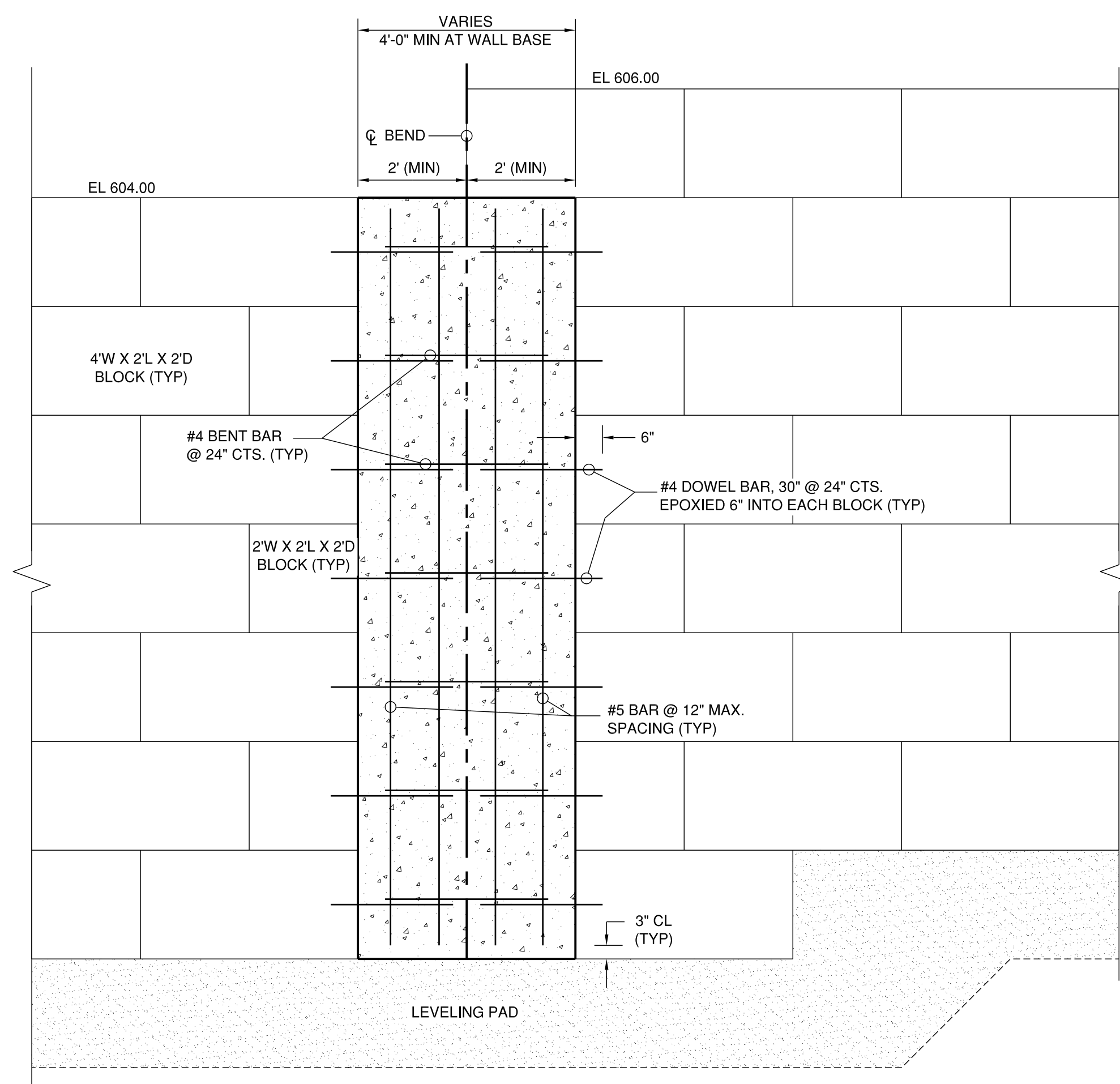
PLAN VIEW
 CAST-IN-PLACE CONCRETE WALL CORNER
 N.T.S.



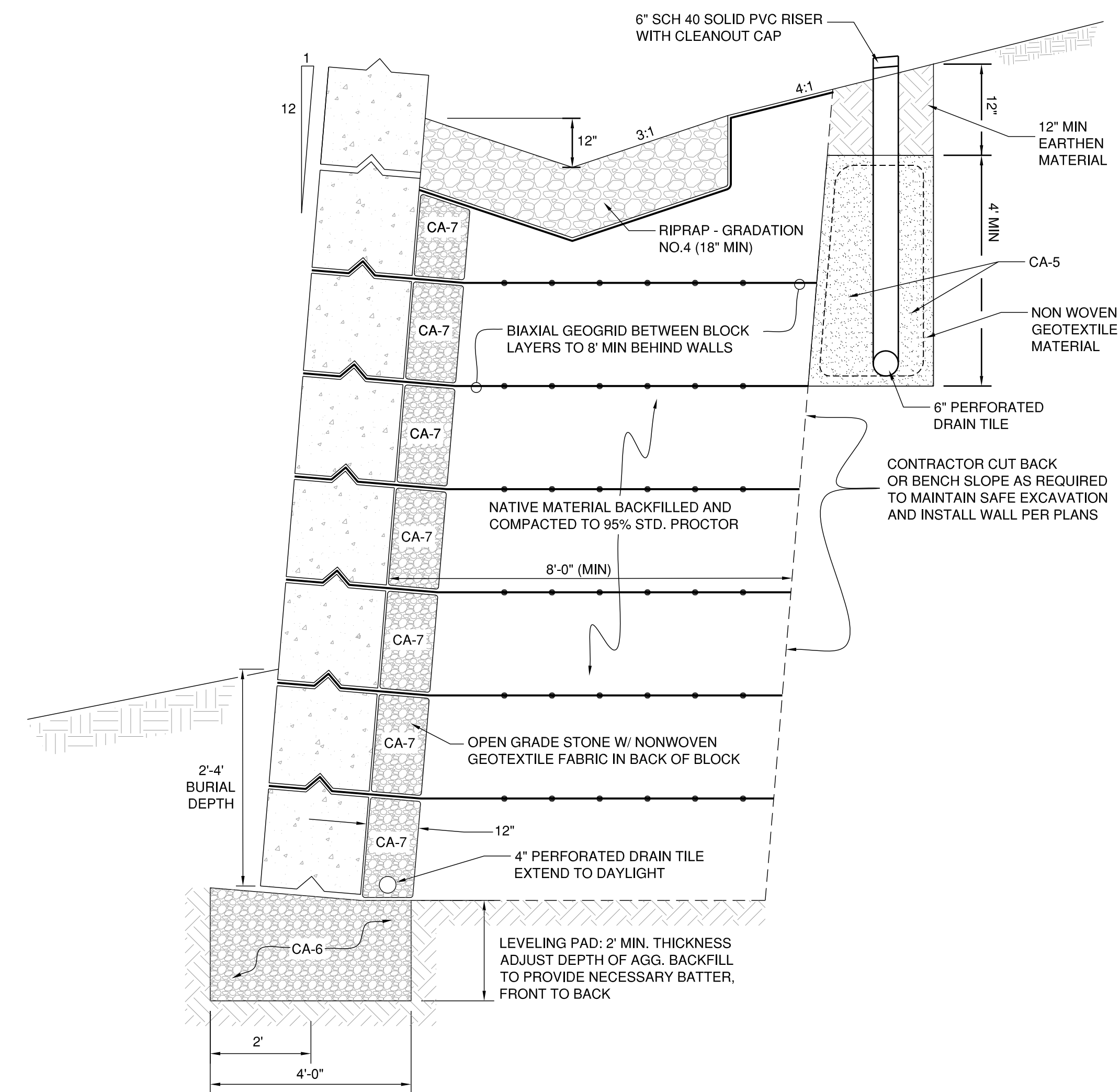
DRAIN TILE SECTION B
 SCALE: 1" = 2'



BUNKER BLOCK DETAIL
 (TYP FOR 2' & 4' WIDE BLOCKS)
 N.T.S.



ELEVATION VIEW
 CAST-IN-PLACE CONCRETE WALL CORNER
 N.T.S.



WALL SECTION A
 SCALE: 1" = 2'

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DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
RETAINING WALL DETAILS 2

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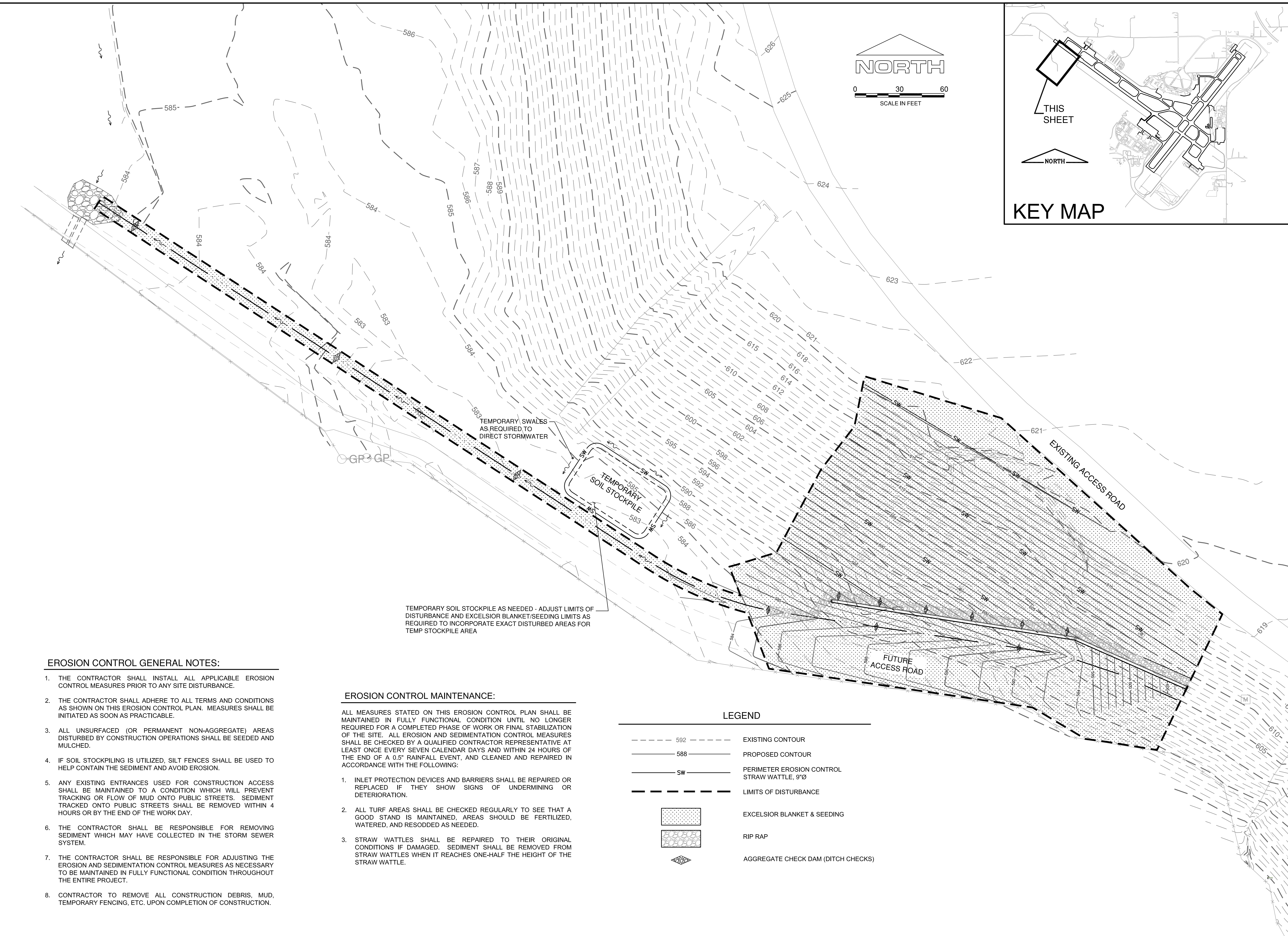
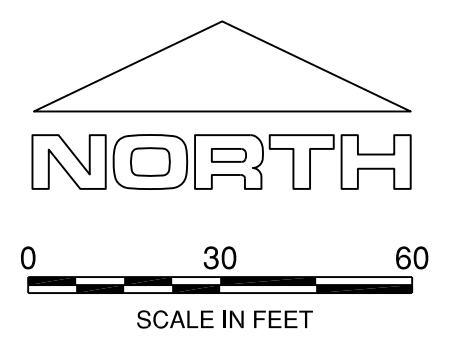
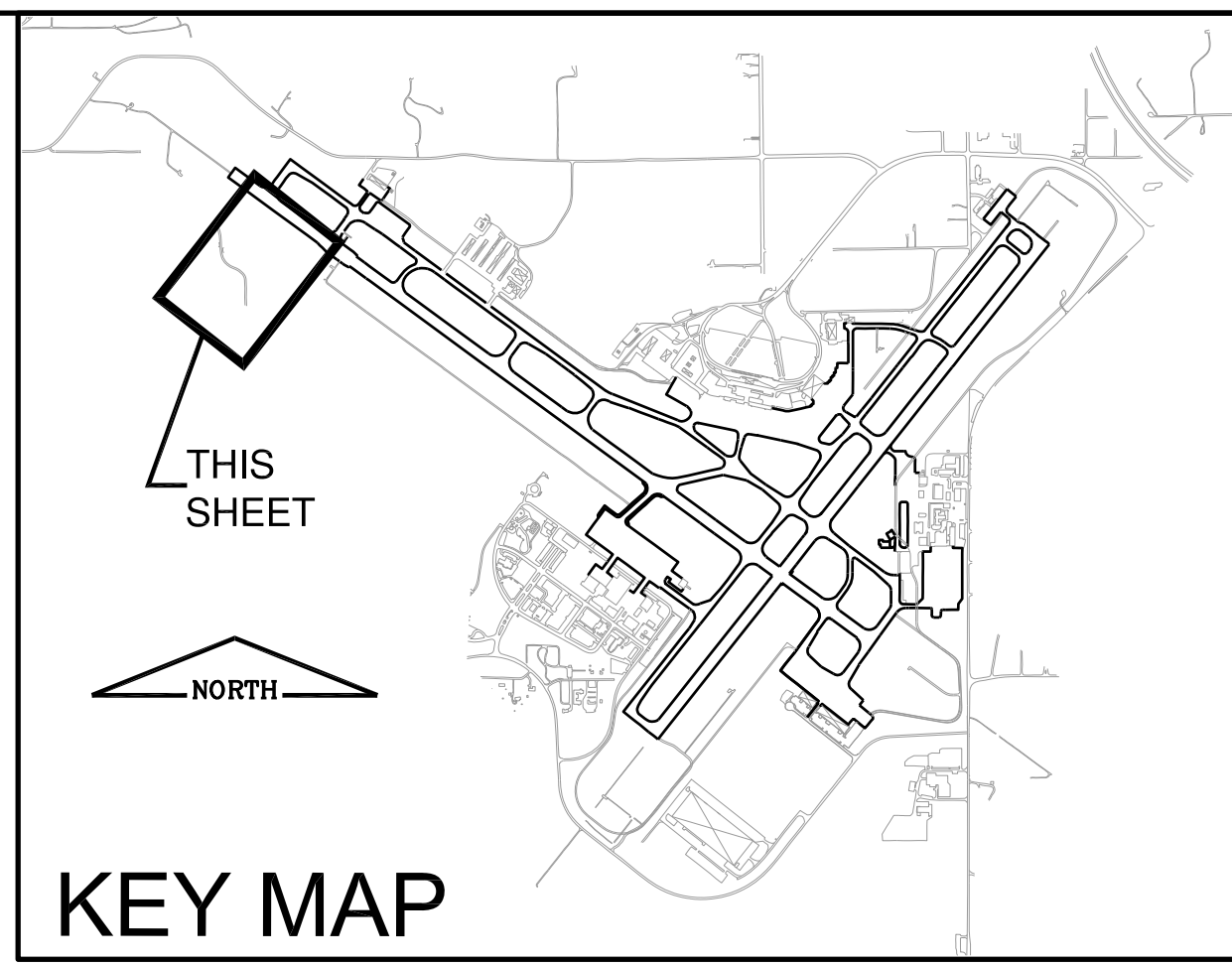
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SHEET 63 OF 67 SHEETS	

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



EROSION CONTROL GENERAL NOTES:

1. THE CONTRACTOR SHALL INSTALL ALL APPLICABLE EROSION CONTROL MEASURES PRIOR TO ANY SITE DISTURBANCE.
2. THE CONTRACTOR SHALL ADHERE TO ALL TERMS AND CONDITIONS AS SHOWN ON THIS EROSION CONTROL PLAN. MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE.
3. ALL UNSURFACED (OR PERMANENT NON-AGGREGATE) AREAS DISTURBED BY CONSTRUCTION OPERATIONS SHALL BE SEEDED AND MULCHED.
4. IF SOIL STOCKPILING IS UTILIZED, SILT FENCES SHALL BE USED TO HELP CONTAIN THE SEDIMENT AND AVOID EROSION.
5. ANY EXISTING ENTRANCES USED FOR CONSTRUCTION ACCESS SHALL BE MAINTAINED TO A CONDITION WHICH WILL PREVENT TRACKING OR FLOW OF MUD ONTO PUBLIC STREETS. SEDIMENT TRACKED ONTO PUBLIC STREETS SHALL BE REMOVED WITHIN 4 HOURS OR BY THE END OF THE WORK DAY.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING SEDIMENT WHICH MAY HAVE COLLECTED IN THE STORM SEWER SYSTEM.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE EROSION AND SEDIMENTATION CONTROL MEASURES AS NECESSARY TO BE MAINTAINED IN FULLY FUNCTIONAL CONDITION THROUGHOUT THE ENTIRE PROJECT.
8. CONTRACTOR TO REMOVE ALL CONSTRUCTION DEBRIS, MUD, TEMPORARY FENCING, ETC. UPON COMPLETION OF CONSTRUCTION.

EROSION CONTROL MAINTENANCE:

- ALL MEASURES STATED ON THIS EROSION CONTROL PLAN SHALL BE MAINTAINED IN FULLY FUNCTIONAL CONDITION UNTIL NO LONGER REQUIRED FOR A COMPLETED PHASE OF WORK OR FINAL STABILIZATION OF THE SITE. ALL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE CHECKED BY A QUALIFIED CONTRACTOR REPRESENTATIVE AT LEAST ONCE EVERY SEVEN CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A 0.5" RAINFALL EVENT, AND CLEANED AND REPAIRED IN ACCORDANCE WITH THE FOLLOWING:
1. INLET PROTECTION DEVICES AND BARRIERS SHALL BE REPAIRED OR REPLACED IF THEY SHOW SIGNS OF UNDERMINING OR DETERIORATION.
 2. ALL TURF AREAS SHALL BE CHECKED REGULARLY TO SEE THAT A GOOD STAND IS MAINTAINED. AREAS SHOULD BE FERTILIZED, WATERED, AND RESODDED AS NEEDED.
 3. STRAW WATTLES SHALL BE REPAIRED TO THEIR ORIGINAL CONDITIONS IF DAMAGED. SEDIMENT SHALL BE REMOVED FROM STRAW WATTLES WHEN IT REACHES ONE-HALF THE HEIGHT OF THE STRAW WATTLE.

LEGEND

	592	EXISTING CONTOUR
	588	PROPOSED CONTOUR
	SW	PERIMETER EROSION CONTROL STRAW WATTLE, 9"Ø
		LIMITS OF DISTURBANCE
		EXCELSIOR BLANKET & SEEDING
		RIP RAP
		AGGREGATE CHECK DAM (DITCH CHECKS)

GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS

DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2

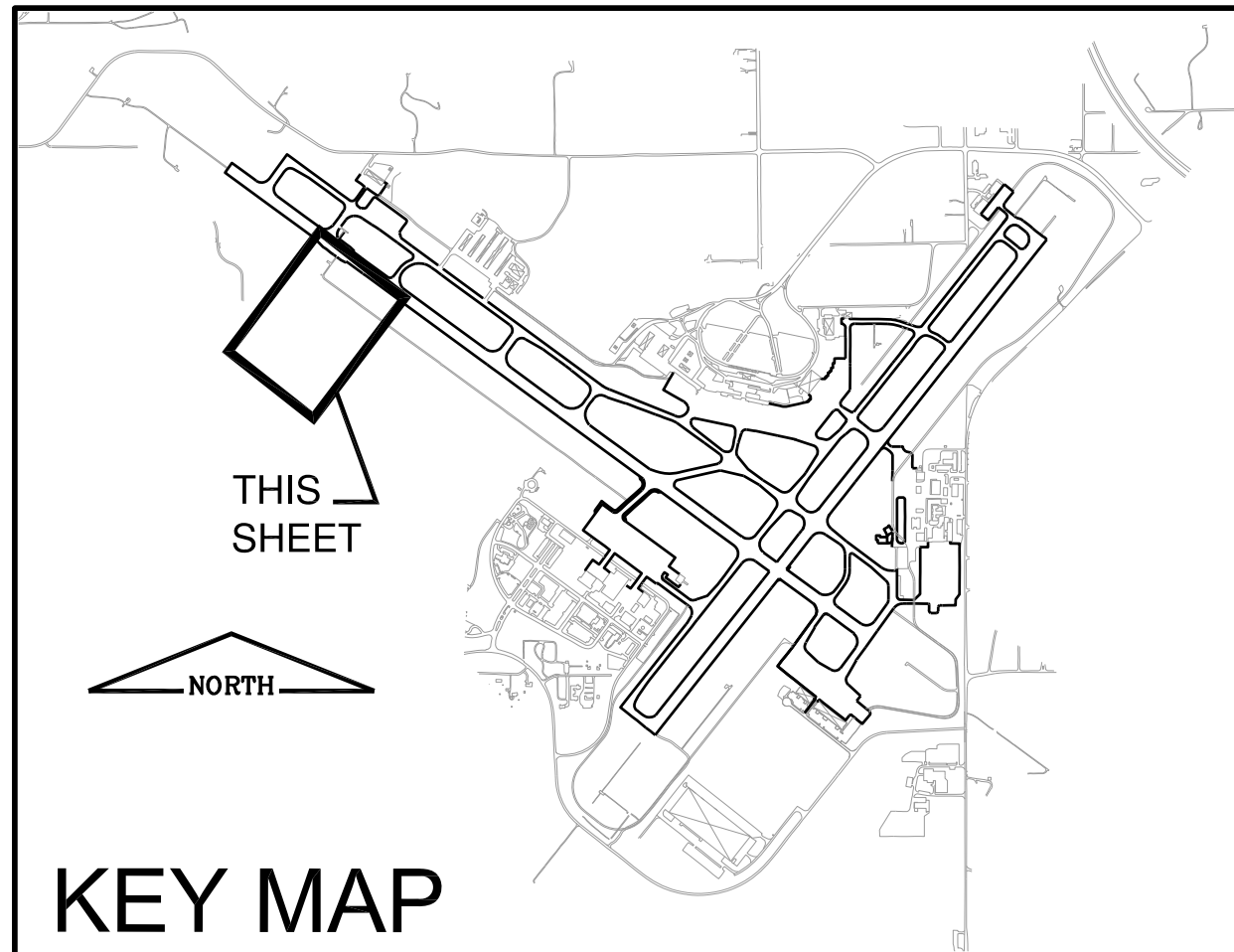
EROSION & SEDIMENT CONTROL PLAN 1

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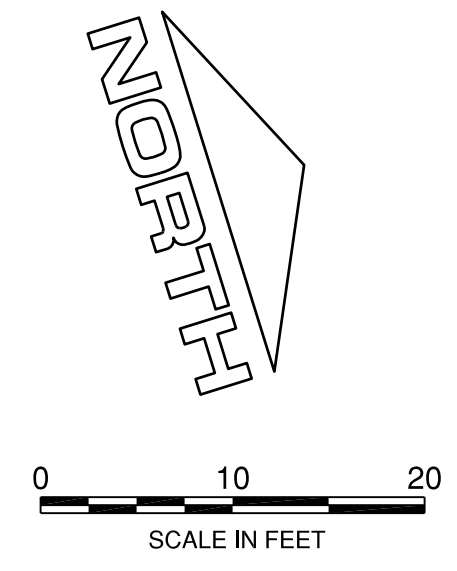
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AIP PROJ. NO. 3-17-0080-XX	
SHEET 64 OF 67 SHEETS	



KEY MAP

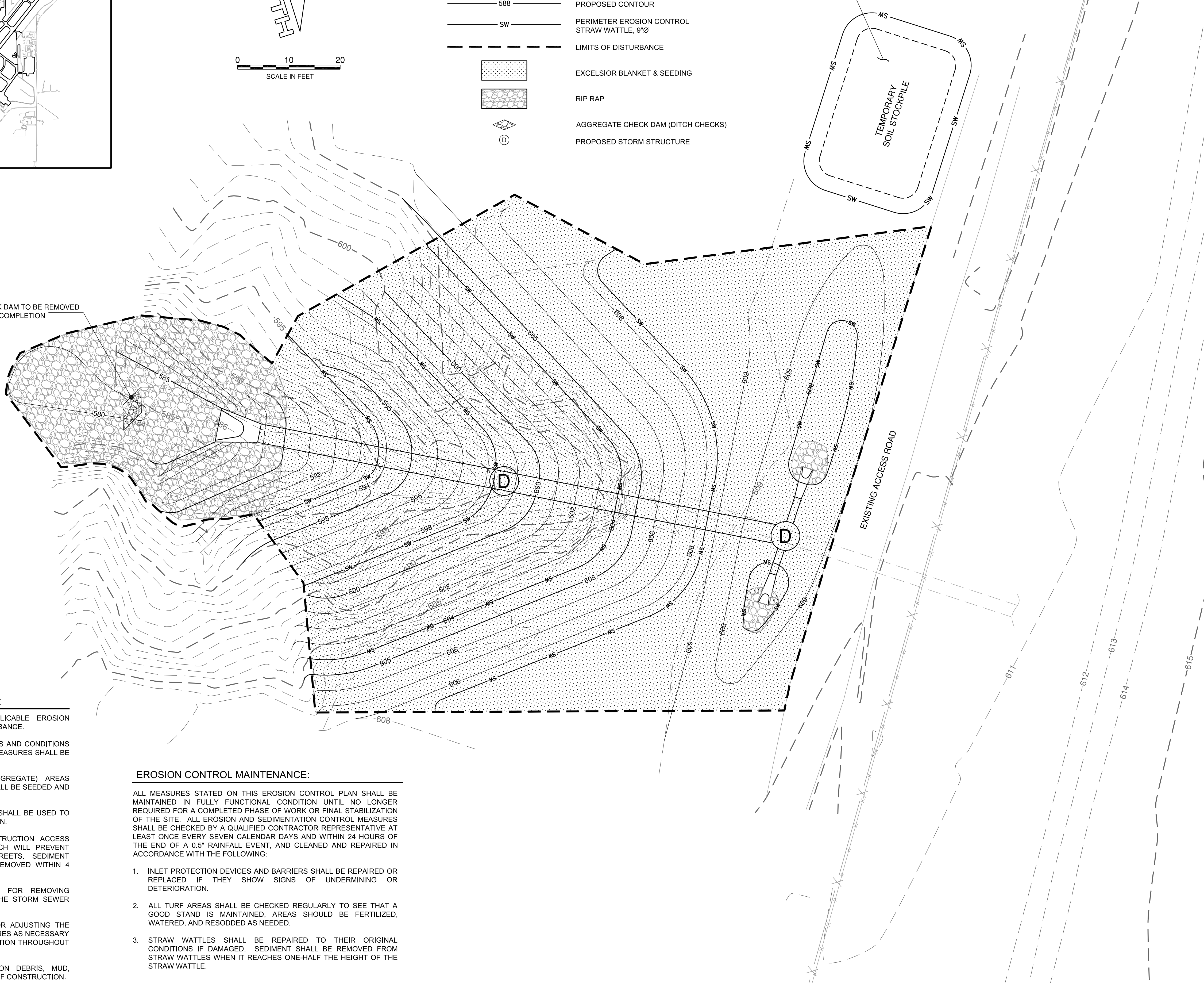


LEGEND

---	592	EXISTING CONTOUR
---	588	PROPOSED CONTOUR
---	SW	PERIMETER EROSION CONTROL STRAW WATTLE, 9"Ø
---		LIMITS OF DISTURBANCE
[Pattern]		EXCELSIOR BLANKET & SEEDING
[Pattern]		RIP RAP
[Symbol]		AGGREGATE CHECK DAM (DITCH CHECKS)
[Symbol]		PROPOSED STORM STRUCTURE

TEMPORARY SOIL STOCKPILE AS NEEDED - ADJUST LIMITS OF DISTURBANCE AND EXCELSIOR BLANKET/SEEDING LIMITS AS REQUIRED TO INCORPORATE EXACT DISTURBED AREAS FOR TEMP STOCKPILE AREA

CHECK DAM TO BE REMOVED UPON COMPLETION



EROSION CONTROL GENERAL NOTES:

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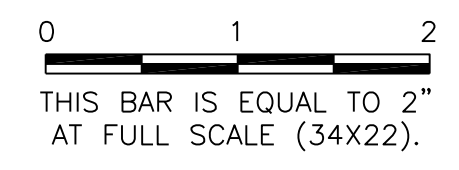
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 UPDATE BY: Joe Miller
 PLOT DATE: 7/10/2014 11:32 AM
 130610500-V-VF3D
 C-SPGM-13061
 KEYMAP
 PIA Base Existing

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PEORIA, ILLINOIS

DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2

EROSION & SEDIMENT CONTROL PLAN 2

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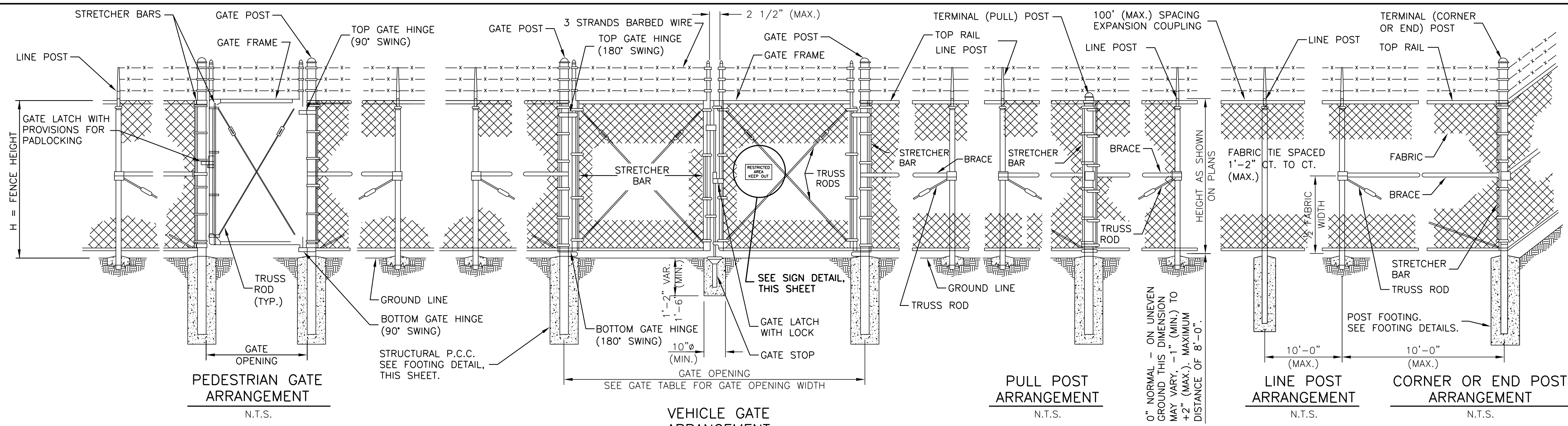
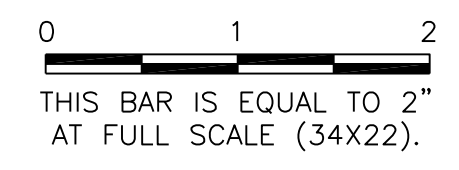
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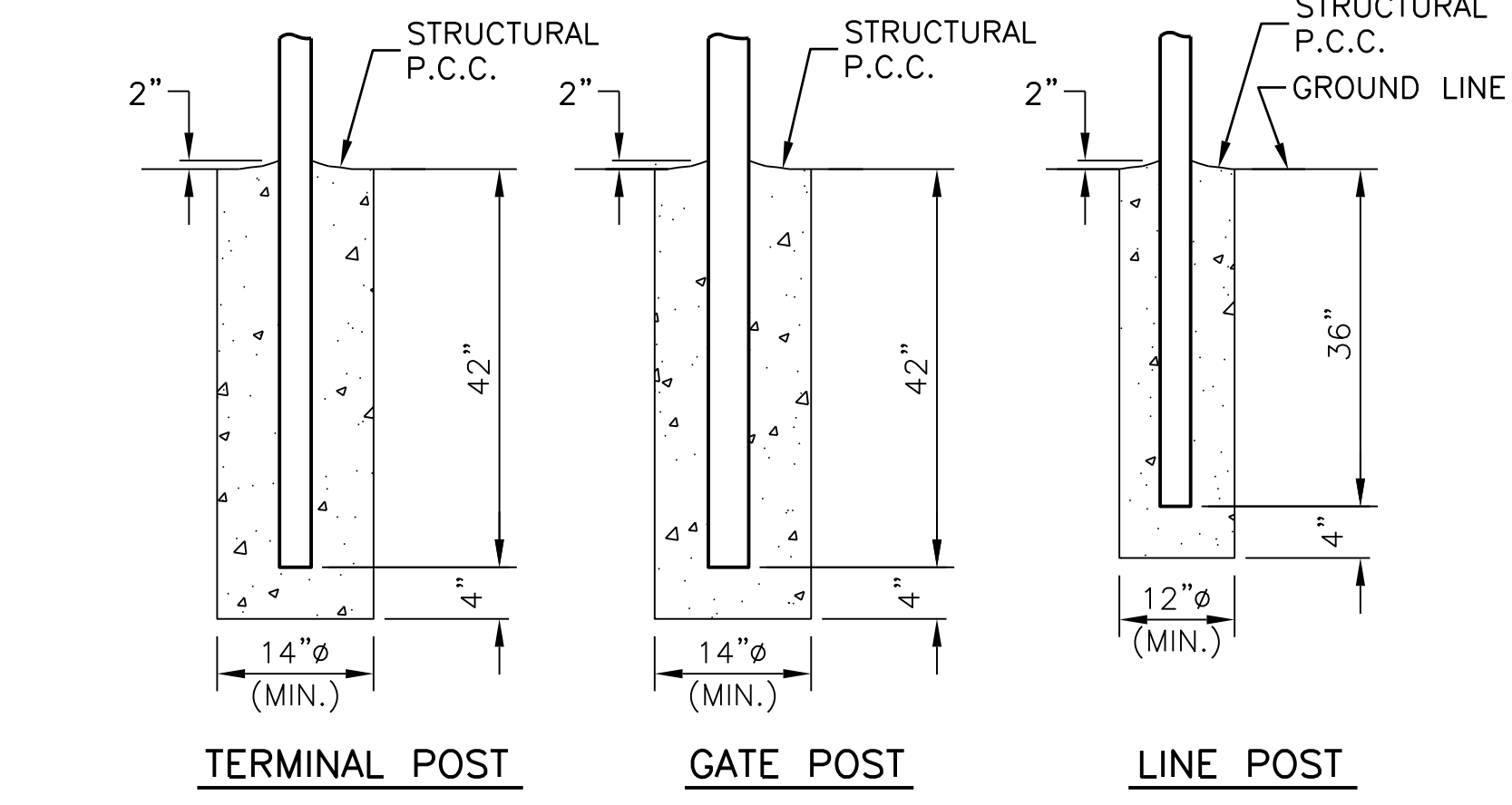
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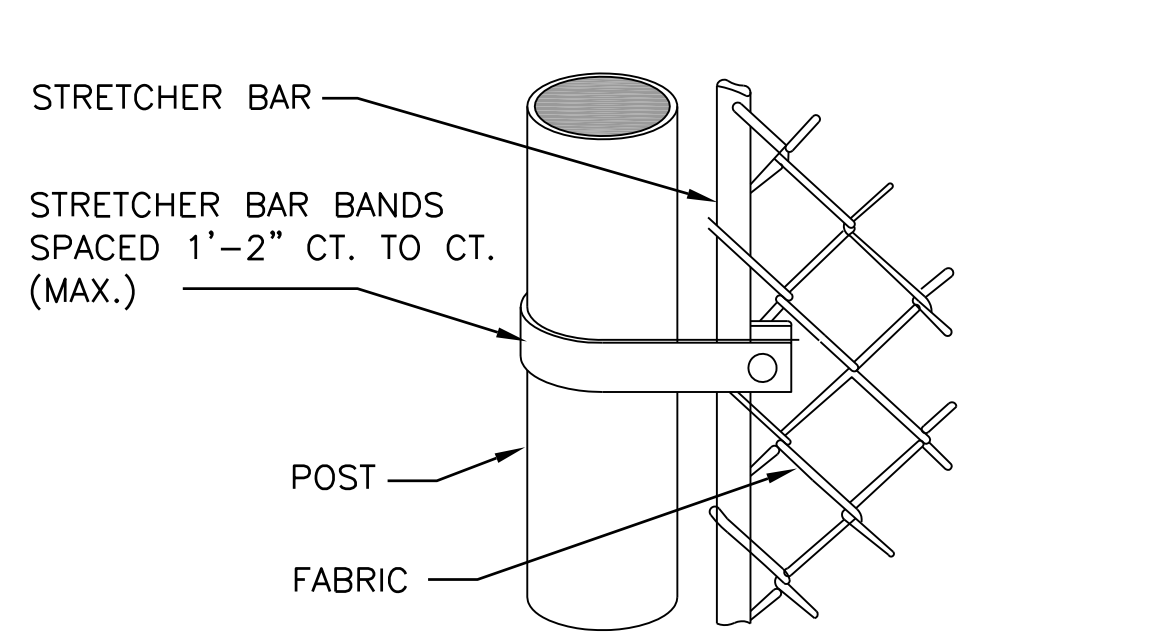


PEDESTRIAN GATE ARRANGEMENT N.T.S.
VEHICLE GATE ARRANGEMENT N.T.S.
PULL POST ARRANGEMENT N.T.S.
LINE POST ARRANGEMENT N.T.S.
CORNER OR END POST ARRANGEMENT N.T.S.

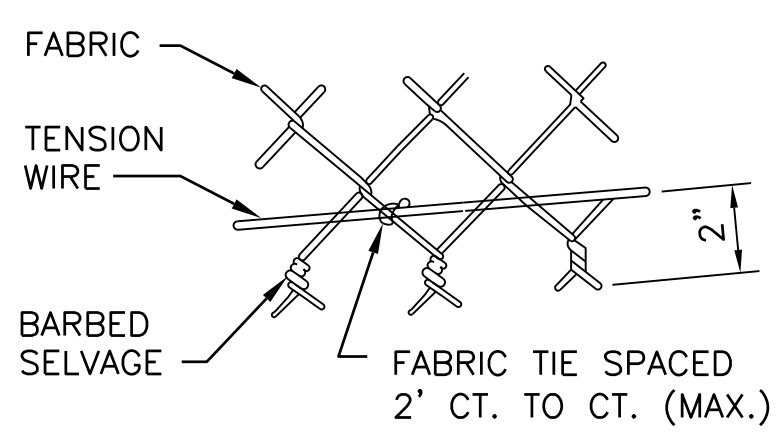
PULL POSTS SHALL BE PLACED AT LOCATIONS DETERMINED BY THE ENGINEER. THEY SHALL BE PLACED AT 660 FT. INTERVALS BETWEEN POSTS TO WHICH THE ENDS OF THE FABRIC ARE CLAMPED OR MIDWAY BETWEEN SUCH POSTS WHEN THE DISTANCE IS LESS THAN 1320 FT. AND GREATER THAN 660 FT.



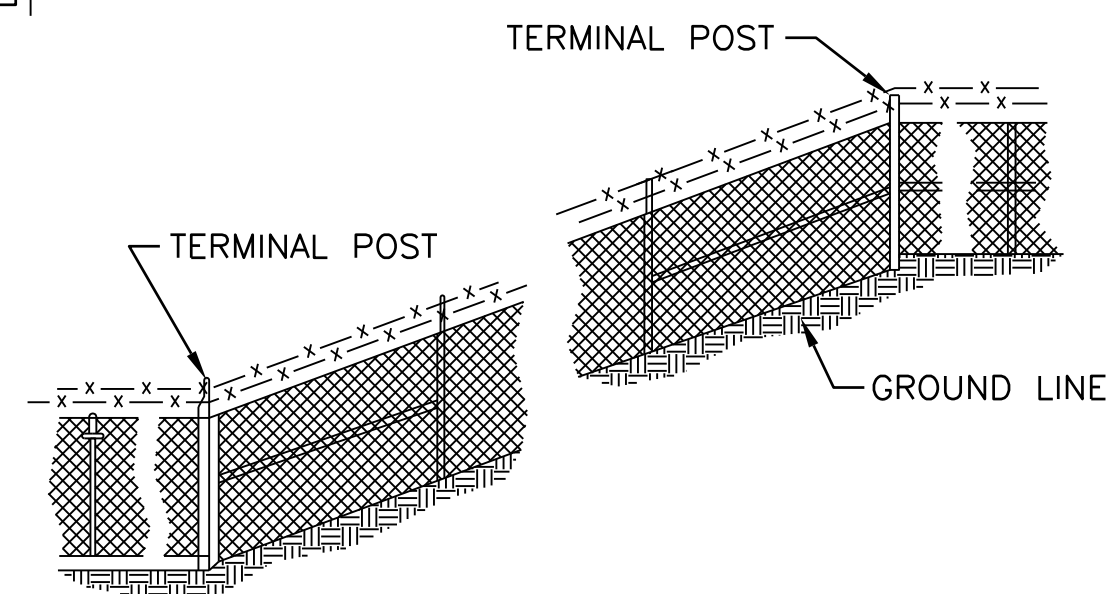
TERMINAL POST N.T.S.
GATE POST N.T.S.
LINE POST N.T.S.
FOOTING DETAILS N.T.S.



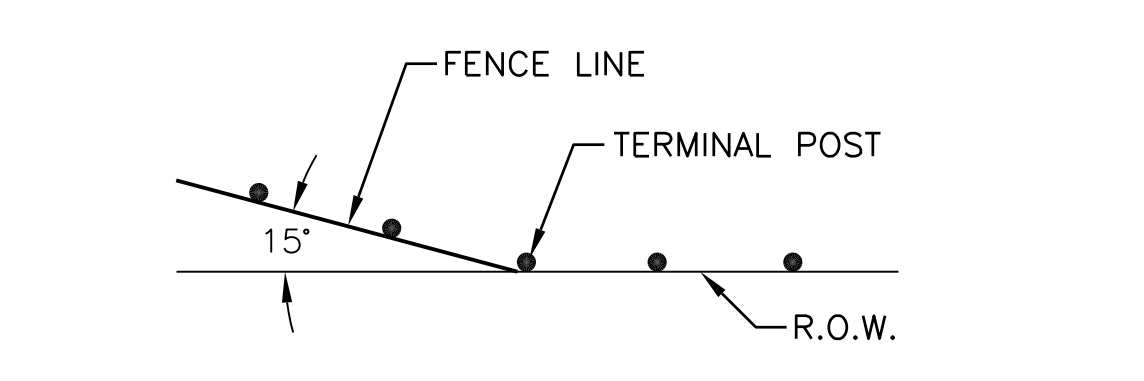
FASTENING STRETCHER BAR TO POST N.T.S.



TYING FABRIC TO TENSION WIRE N.T.S.

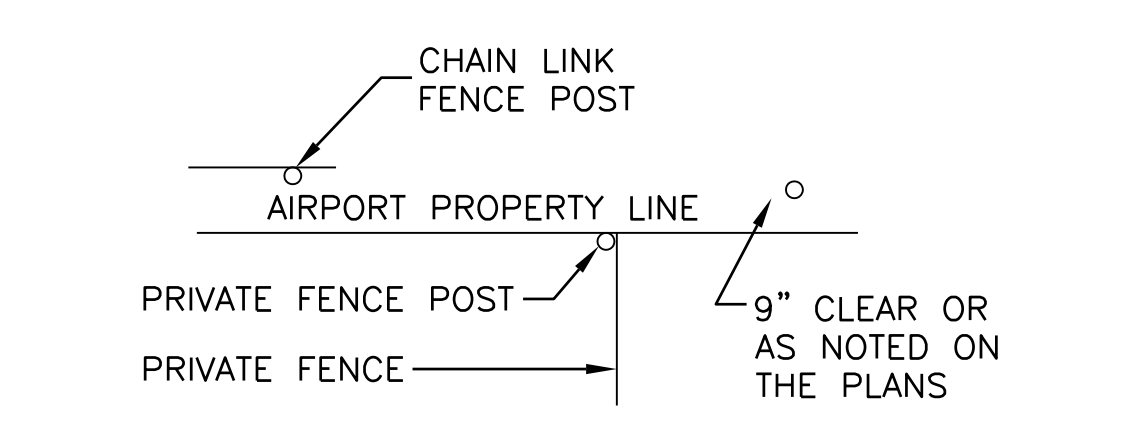


FENCE INSTALLATION ON SLOPES N.T.S.

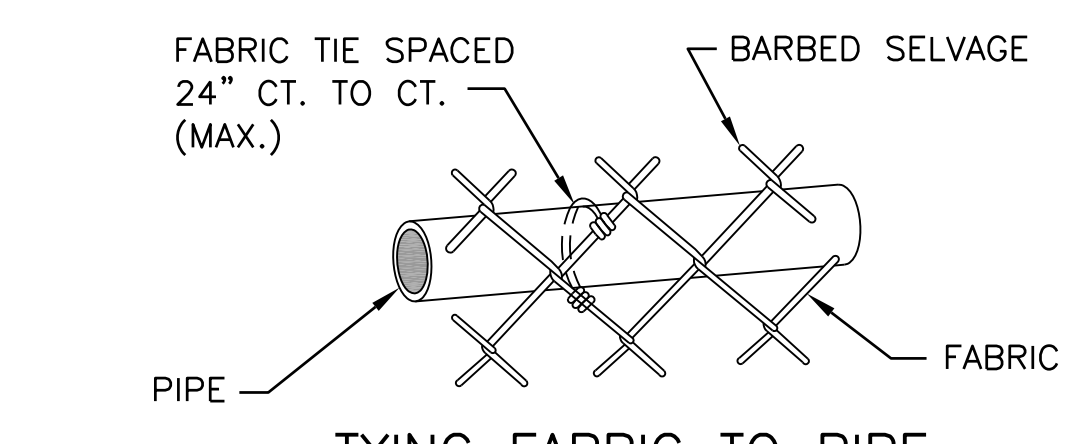


WHERE FENCE LINE HAS A CHANGE IN DIRECTION OF 15° OR MORE, A TERMINAL POST SHALL BE PLACED AS SHOWN ABOVE. WHERE ANGLE IS LESS THAN 15° AND EXISTING CONDITIONS REQUIRE A TERMINAL POST, THEY SHALL BE PLACED AS DIRECTED BY THE ENGINEER.

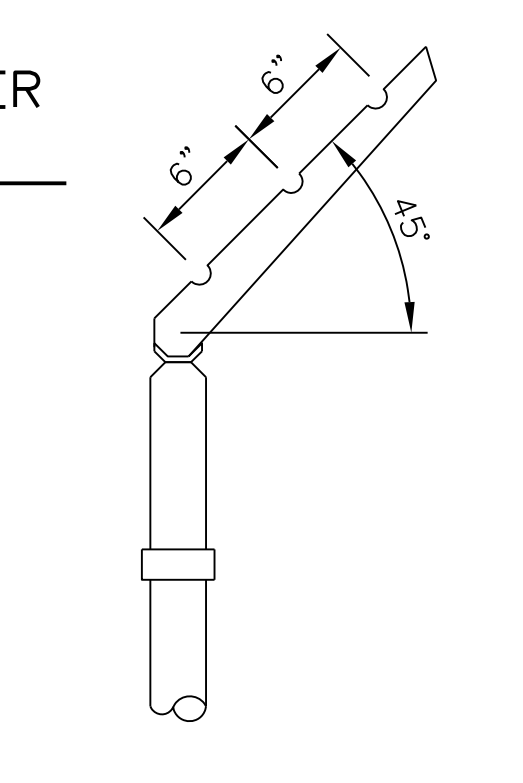
INSTALLATION AT CORNERS N.T.S.



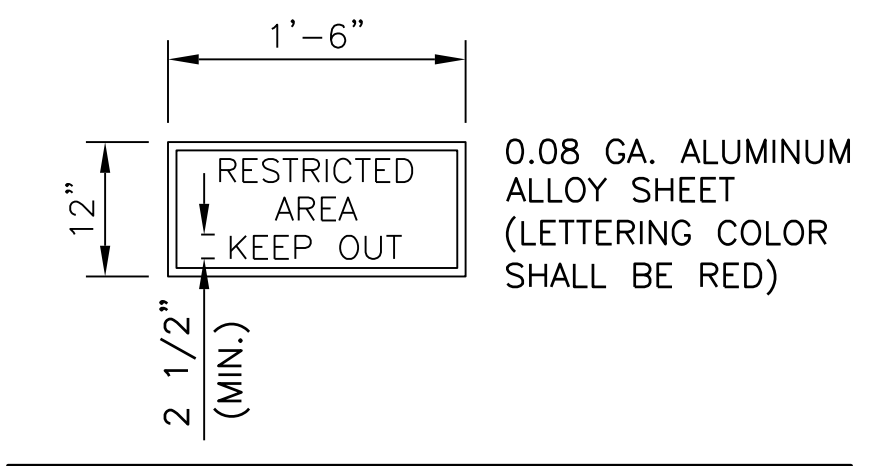
PRIVATE FENCE TERMINAL N.T.S.



TYING FABRIC TO PIPE N.T.S.



BARBED WIRE ARM ON LINE POST N.T.S.



EACH GATE SHALL REQUIRE TWO SIGNS. EVERY 100' OF FENCE SHALL REQUIRE ONE SIGN. EVERY STRAIGHT SECTION OF FENCE SHALL REQUIRE MINIMUM ONE SIGN.

SIGN DETAIL N.T.S.

NOTES

1. CONTINUOUS FENCE SHALL BE GROUNDED AT INTERVALS NOT EXCEEDING 500 FT. EXCEPT THERE SHALL BE A GROUND NOT EXCEEDING 100 FT. FROM GATE IN EACH SECTION OF THE FENCE ADJACENT TO THE GATE.
2. FENCE UNDER POWER LINES SHALL BE GROUNDED BY 3 GROUNDS, ONE DIRECTLY UNDER THE CROSSING AND ONE ON EACH SIDE 25 TO 50 FT. AWAY. A SINGLE GROUND SHALL BE LOCATED DIRECTLY UNDER EACH TELEPHONE WIRE OR CABLE CROSSINGS.
3. THE COUNTERPOISE SHALL BE USED ONLY WHERE IT IS IMPOSSIBLE TO DRIVE A GROUND ROD BECAUSE OF IMPERVIOUS EARTH STRUCTURES.
4. THE GROUND WIRE SHALL BE CONNECTED TO THE FABRIC AND THE GROUND ROD BY A MECHANICAL CLAMP OF CAST BRONZE BODY AND BRONZE OR STAINLESS STEEL BOLTS AND WASHERS. WHEN A TENSION WIRE IS REQUIRED, THE BOTTOM CONNECTION OF THE GROUND WIRE SHALL BE MADE TO THE TENSION WIRE.
5. ALL PROPOSED CLASS E FENCE SHALL MEET THE REQUIREMENTS OF IDOT STANDARD 664001-01.

POST TYPE	FENCE POST SHAPE, SIZE AND WEIGHT					
	FENCE POST MATERIAL			ROLL FORMED C SECTION		
	DIAMETER	WEIGHT	DIAMETER	WEIGHT	SIZE	WEIGHT
CORNER, END, PULL	4.0" O.D.	9.11 LB/FT	4.0" O.D.	6.57 LB/LF	3.25" X 2.5"	4.50 LB/FT
LINE POST	2.875" O.D.	5.79 LB/FT	2.875" O.D.	4.64 LB/LF	3.25" X 2.5"	4.50 LB/FT
TOP RAIL	1.66" O.D.	2.77 LB/FT	1.66" O.D.	1.84 LB/LF	1.625" X 1.25"	1.35 LB/FT
GATE POST	6.25" O.D.	18.97 LB/FT	N/A	N/A	N/A	N/A

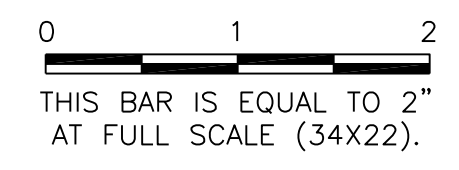
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DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2
FENCE DETAILS

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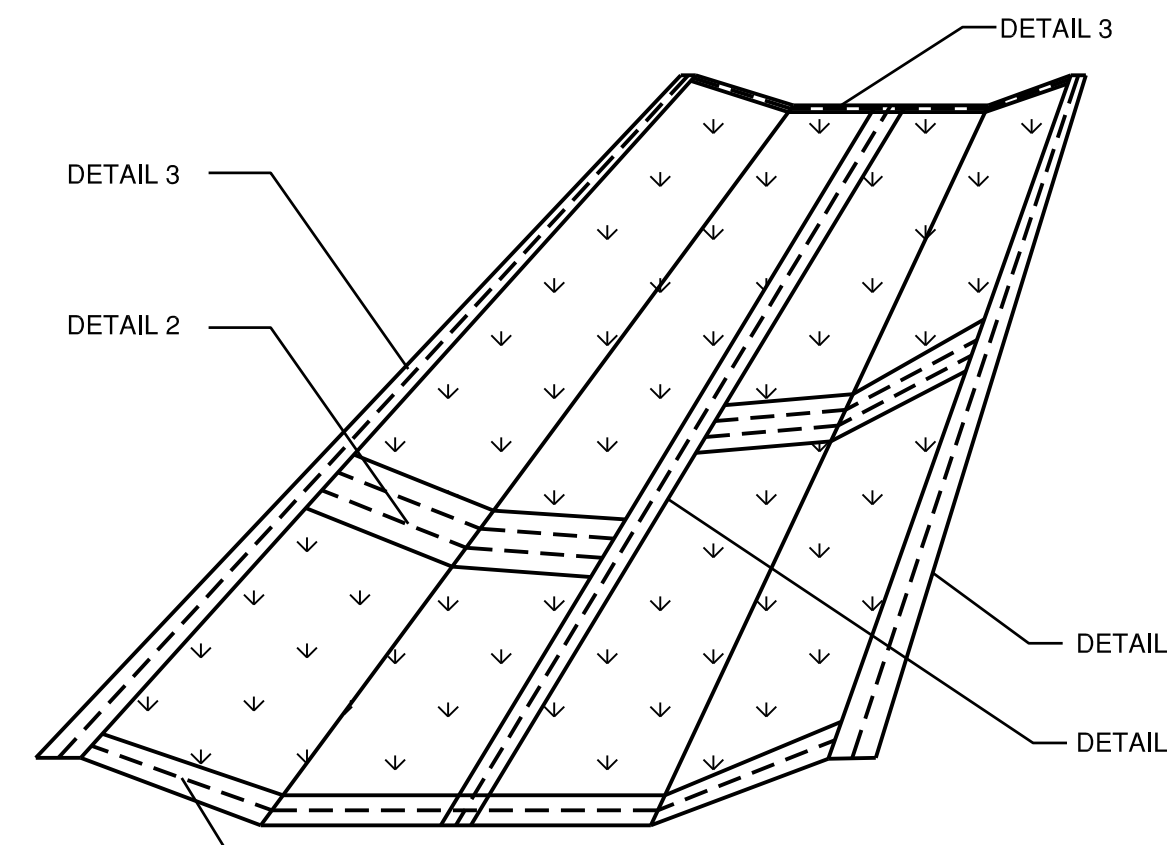
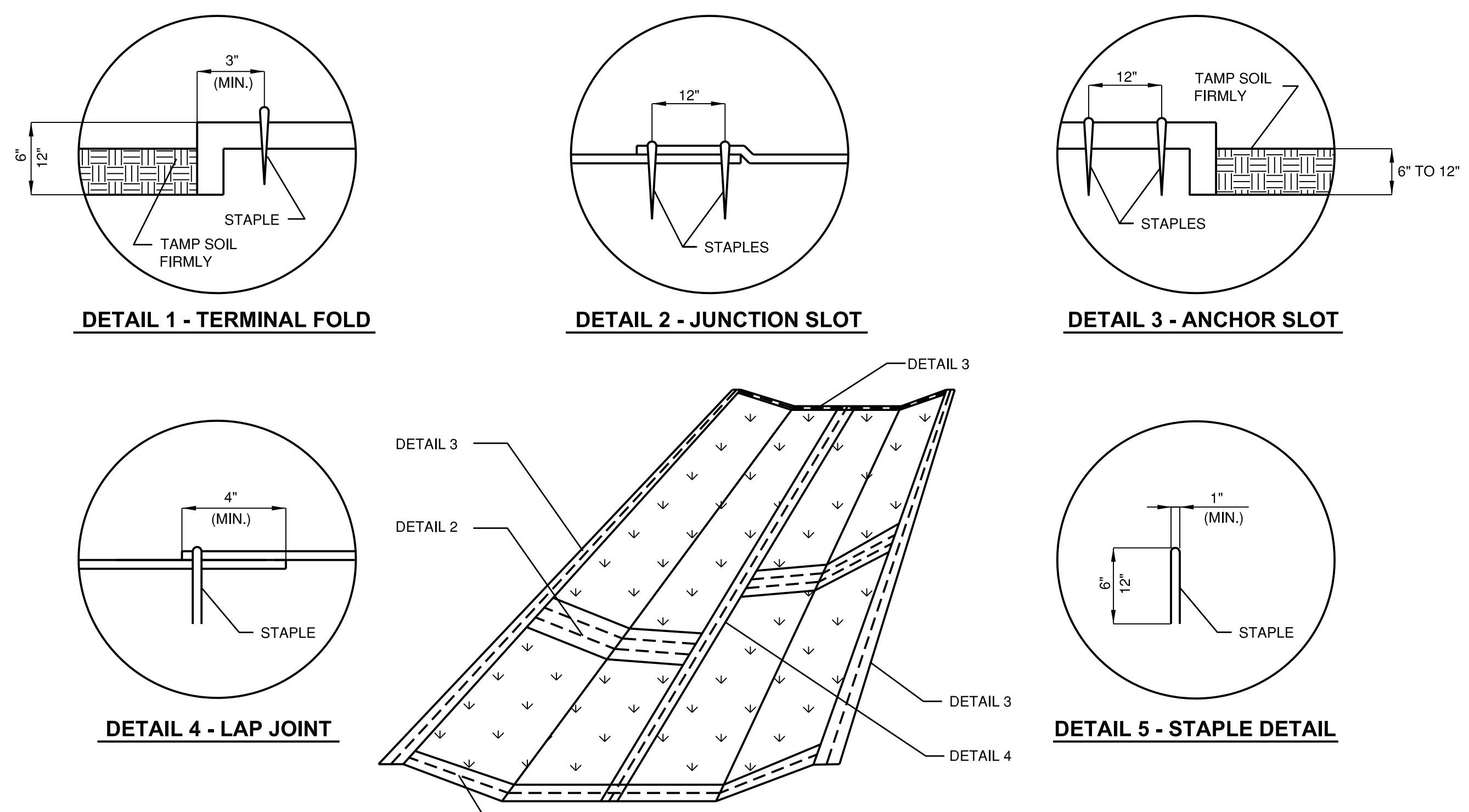
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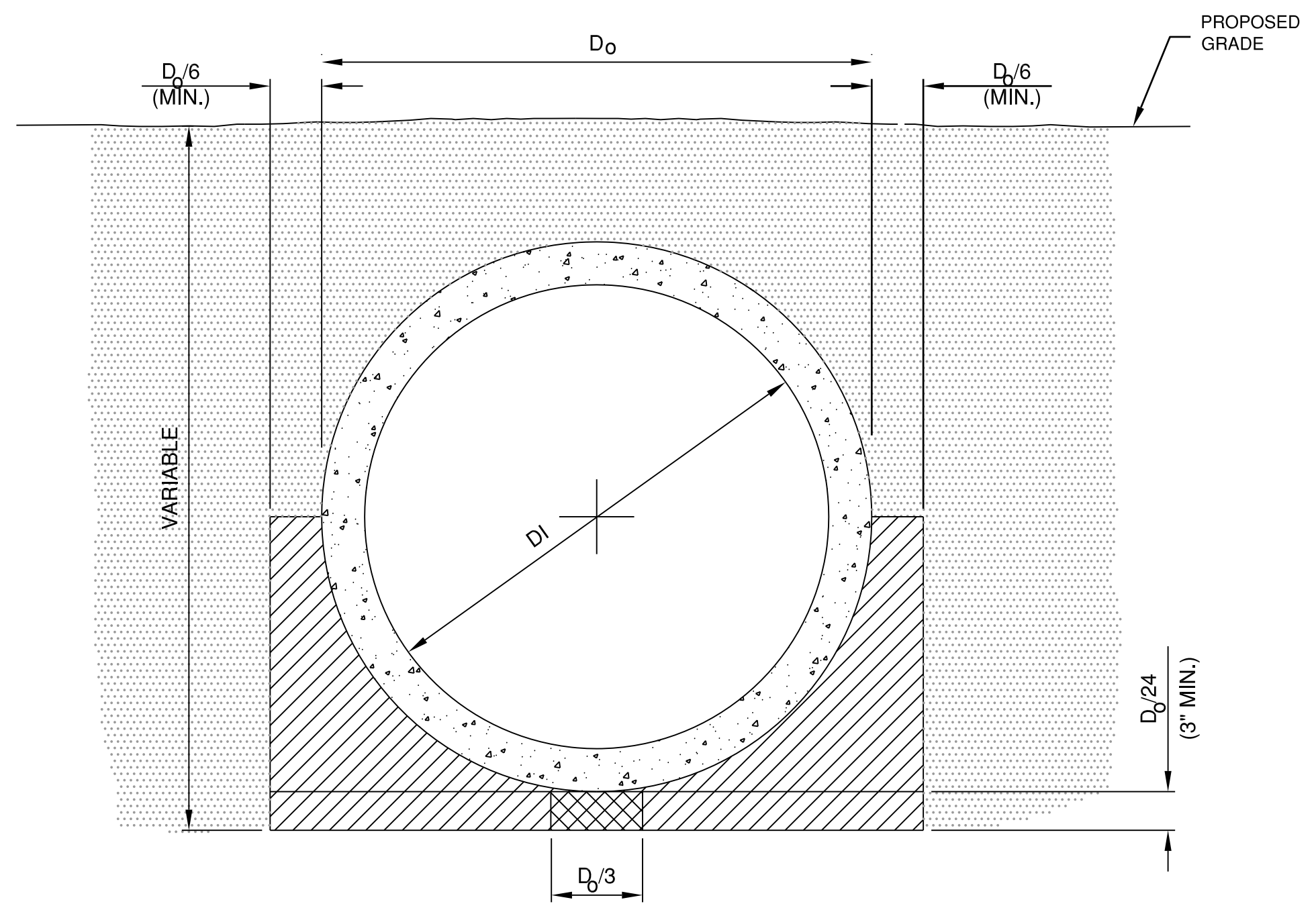


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CONSTRUCTION DETAILS
DRAINAGE IMPROVEMENTS/SLOPE STABILIZATION & REHABILITATE TERMINAL ENTRANCE ROAD, PHASE 1 & 2



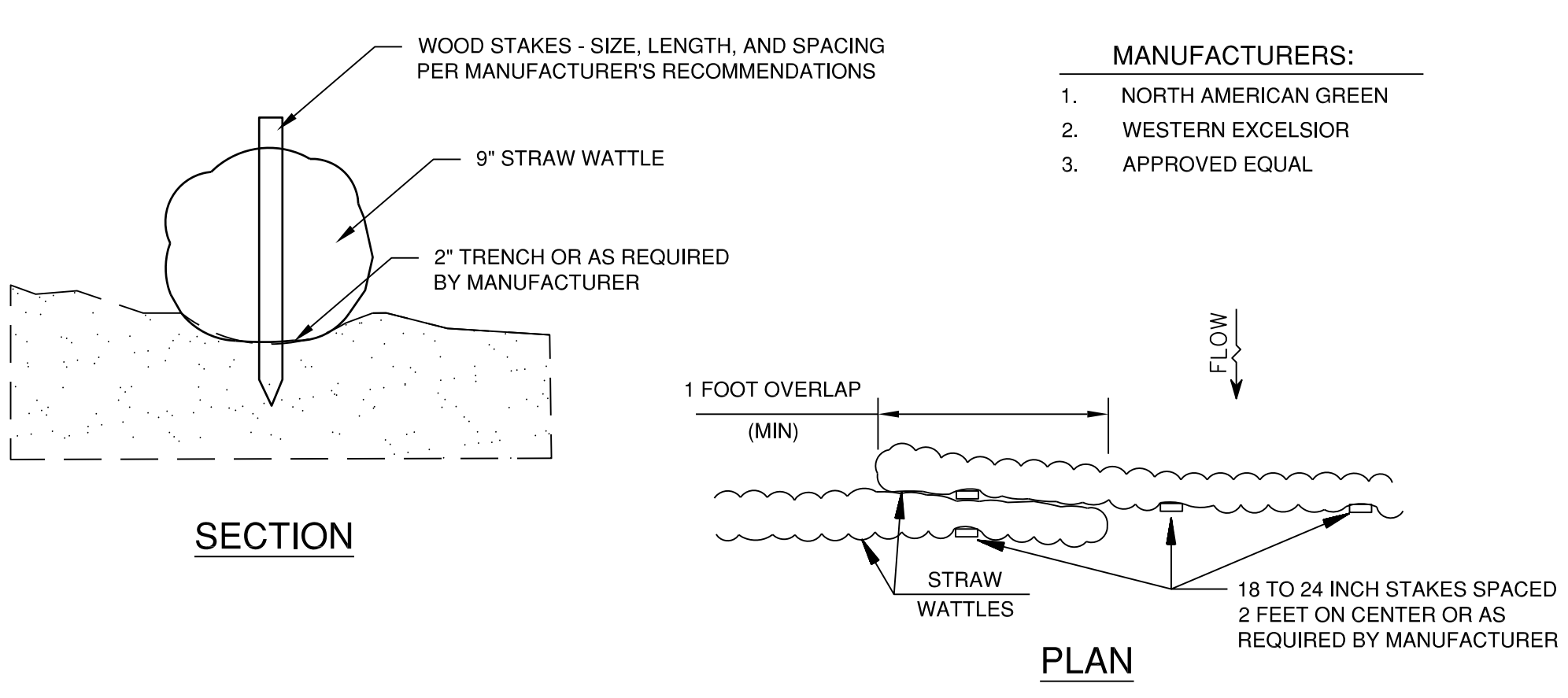
- EXCELSIOR BLANKET NOTES**
1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
 2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
 3. ALL TERMINALS ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.

EXCELSIOR BLANKET DETAILS
 N.T.S.

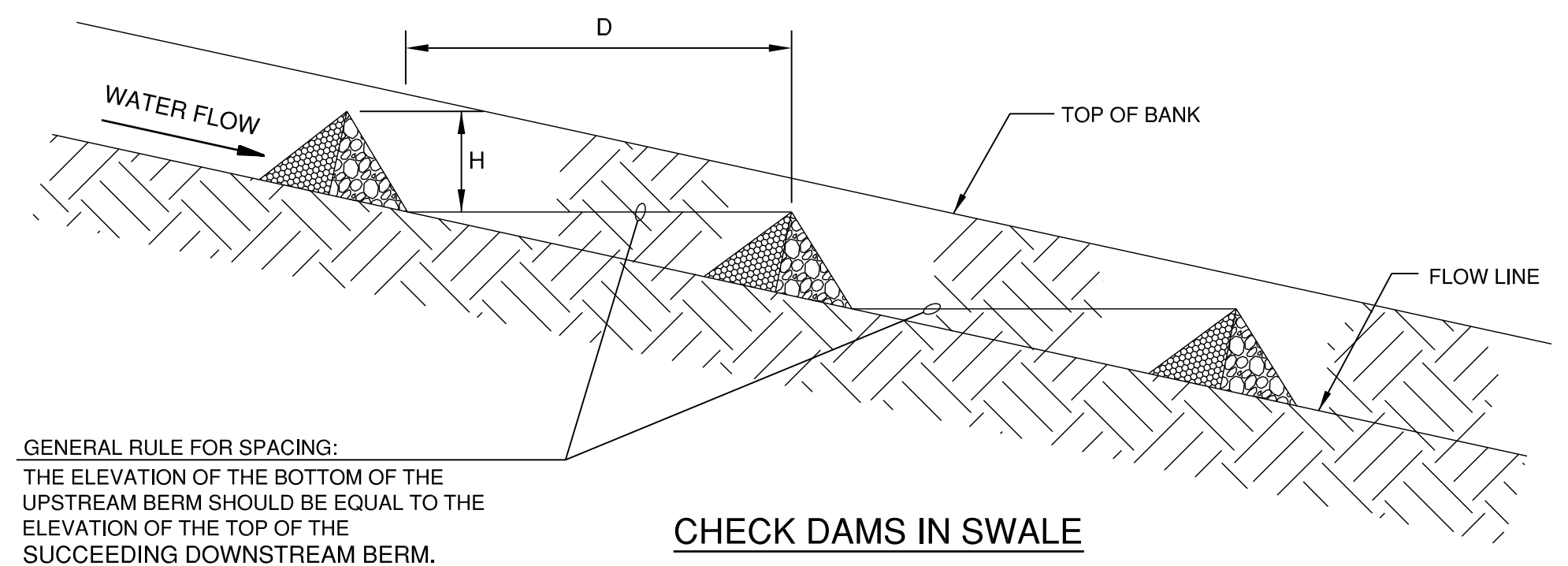


- TRENCH INSTALLATION LEGEND**
- DRAINAGE CONDUIT MATERIAL-CONCRETE
 - MIDDLE BEDDING LOOSELY PLACED UNCOMPACTED BEDDING
 - HAUNCH AND OUTER BEDDING COMPACTION- TO ENGINEER'S SATISFACTION OR 95% STANDARD PROCTOR
 - LOWER SIDE AND OVERFILL COMPACTION- SAME AS EMBANKMENT REQUIREMENTS
- TRENCH INSTALLATION NOTES**
1. BEDDING SHOWN IS IN ACCORDANCE WITH "STANDARD EMBANKMENT INSTALLATIONS" STANDARD INSTALLATION & BEDDING FACTORS FOR THE INDIRECT DESIGN METHOD (DESIGN DATA 40), AMERICAN CONCRETE PIPE ASSOCIATION.
 2. BACKFILL TO EXTEND 3' BEYOND EDGES OF PROPOSED PAVEMENT.
- D_o PIPE OUTSIDE DIAMETER
 D_i PIPE INSIDE DIAMETER

STANDARD TRENCH INSTALLATION
NON-PAVED AREA
 N.T.S.



STRAW WATTLE DETAILS
 N.T.S.



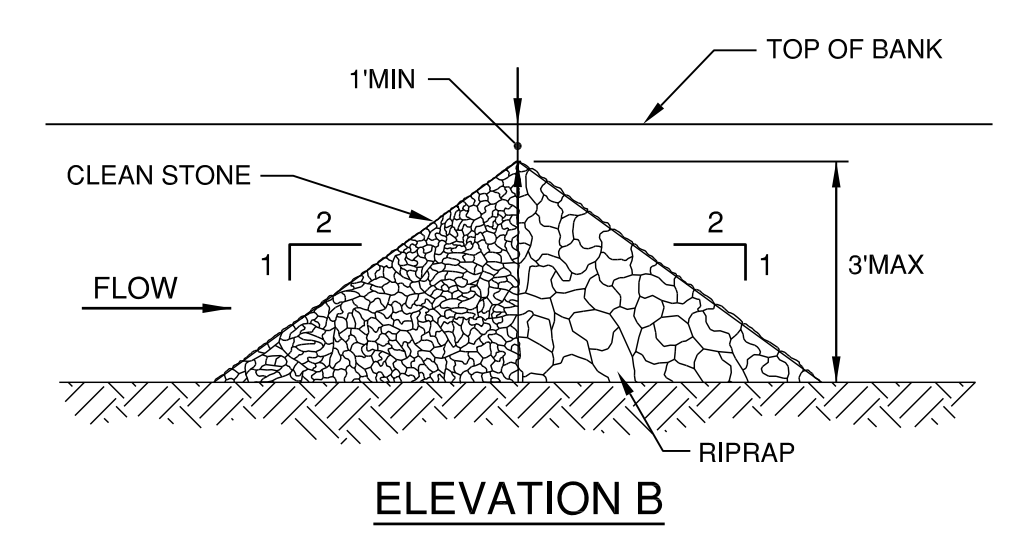
GENERAL RULE FOR SPACING:
 THE ELEVATION OF THE BOTTOM OF THE UPSTREAM BERM SHOULD BE EQUAL TO THE ELEVATION OF THE TOP OF THE SUCCEEDING DOWNSTREAM BERM.

CHECK DAMS IN SWALE

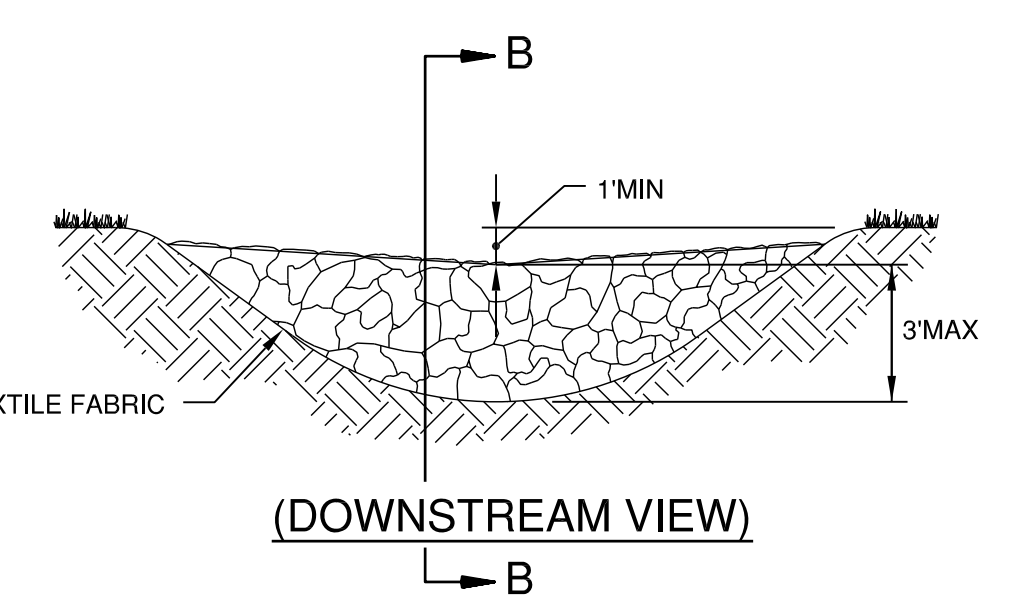
- MAINTENANCE NOTES:**
1. INSPECT CHECK AFTER EVERY STORM EVENT AND REPAIR ANY DAMAGE IMMEDIATELY.
 2. REMOVE SEDIMENT ACCUMULATED BEHIND EACH DAM AS NEEDED TO MAINTAIN CAPACITY, AND TO PREVENT LARGE FLOWS FROM DISPLACING SEDIMENT.
 3. ADD ROCK TO THE DAMS AS NEEDED TO MAINTAIN DESIGN HEIGHT AND CROSS SECTION.
 4. UPON STABILIZATION, REMOVE ROCK CHECK DAM.

- NOTES:**
1. CLEAN STONE SHALL MEET IDOT CA-1, CA-2, CA-3, OR CA-4 GRADATION.
 2. RIPRAP SHALL MEET IDOT RR-3 OR RR-4 GRADATION.

ROCK CHECK DAM DETAILS
 N.T.S.



ELEVATION B



(DOWNSTREAM VIEW)

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GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

DESIGN BY:	A00
DRAWN BY:	KN
CHECKED BY:	JFM
APPROVED BY:	CET
DATE:	JULY 11, 2014
JOB No:	13061-05
IL PROJ. NO.	PIA-4415
AIP PROJ. NO.	3-17-0080-XX

SHEET 67 OF 67 SHEETS