LETTING ITEM NO. 03A AUGUST 1, 2025 IDOT LETTING

CITY OF AURORA AURORA, ILLINOIS

CONSTRUCTION PLANS

FOR

AURORA MUNICIPAL AIRPORT

REPLACE AIRFIELD SIGNAGE, WIND CONES, RUNWAY 15 AND 27 REILS, AND AIRPORT ROTATING BEACON; CONSTRUCT RUNWAY GUARD LIGHTS AT RUNWAY 15/33

ILLINOIS PROJECT: ARR-5201

JUNE 13, 2025



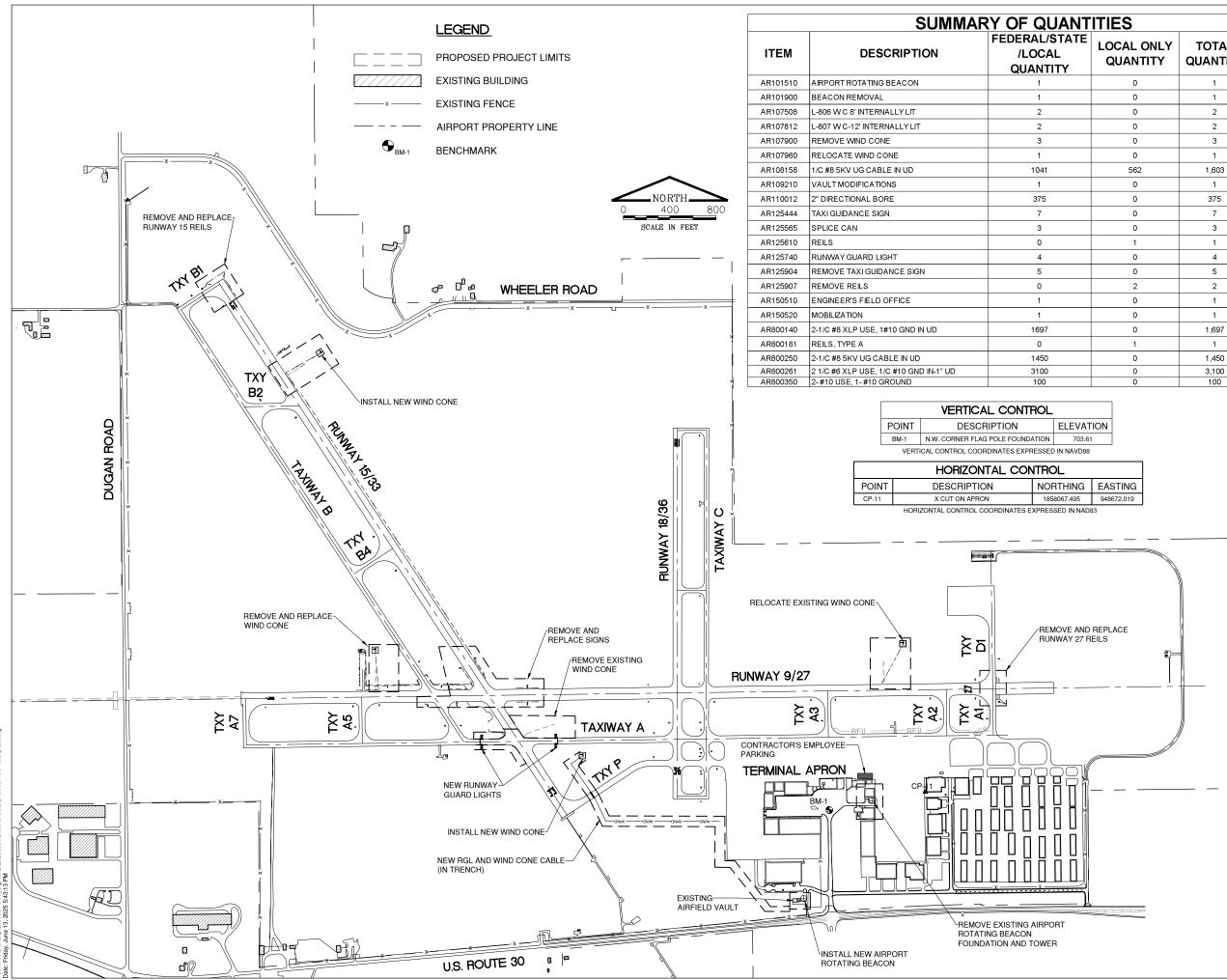
UTILITIES INDICATED ON THE PLANS IS SUFFICIENT OR COMPLETE IT SHALL E DETERMINE ACTUAL LOCATIONS OF A CONNECTIONS TO UNDERGROUND U	J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS www.illinois1call.com MATERIAL OF EXISTING UNDERGROUND S NOT REPRESENTED AS BEING ACCURATE, BE THE CONTRACTOR'S RESPONSIBILITY TO ALL SUCH FACILITIES, INCLUDING SERVICE JTILITIES. PRIOR TO CONSTRUCTION, THE COMPANIES OF HIS OPERATIONAL PLANS.	PROJECT LOCATION		E-W TOLLWAY	PROJECT SITE
ASSISTANCE RELATIVE TO THE LOCAT SCHEDULE OF THE COMPANIES FOR RE IN THE EVENT AN UNEXPECTED UTILIT CONSTRUCTION, THE CONTRACTOR COMPANY OF JURISDICTION AND THE SHALL ALSO BE IMMEDIATELY NOTIFIEL RESTORED TO SERVICE AT ONCE AN	COMPANIES DETAILED INFORMATION AND ION OF THEIR FACILITIES AND THE WORKING MOVAL OR ADJUSTMENT WHERE REQUIRED. YINTERFERENCE IS ENCOUNTERED DURING SHALL IMMEDIATELY NOTIFY THE UTILITY ONE-CALL NOTICE SYSTEM. THE ENGINEER D. ANY SUCH UTILITY OR SERVICES SHALL BE UP PAID FOR BY THE CONTRACTOR AT NO T O T H E C O N T R A C T . ORE EXCAVATING	U.S. ROUTE 30 U.S. ROUTE 30 U.	SUGAR GROVE		
AURORA MU SECTION: 17 AND 18	F 811 NICIPAL AIRPORT COUNTY: KANE		U.S. ROUTE 30	E 31	U.S. ROUTE 30
RANGE: 7 EAST TOWNSHIP: 38 NORTH	U.S. ROUTE 30 SUGAR GROVE TOWNSHIP			U.S. ROU	
		LOCA	TION MAP		SITE PLAN

AU075 TOTAL SHEETS: 26

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TIES					
LOCAL ONLY QUANTITY	TOTAL QUANTITY	UNIT			
0	1	EACH			
0	1	EACH			
0	2	EACH			
0	2	EACH			
0	3	EACH			
0	1	EACH			
562	1,603	FOOT			
0	1	LSUM			
0	375	FOOT			
0	7	EACH			
0	3	EACH			
1	1	PAIR			
0	4	EACH			
0	5	EACH			
2	2	PAIR			
0	1	LSUM			
0	1	LSUM			
0	1,697	FOOT			
1	1	PAIR			
0	1,450	FOOT			
0	3,100	FOOT			
0	100	FOOT			



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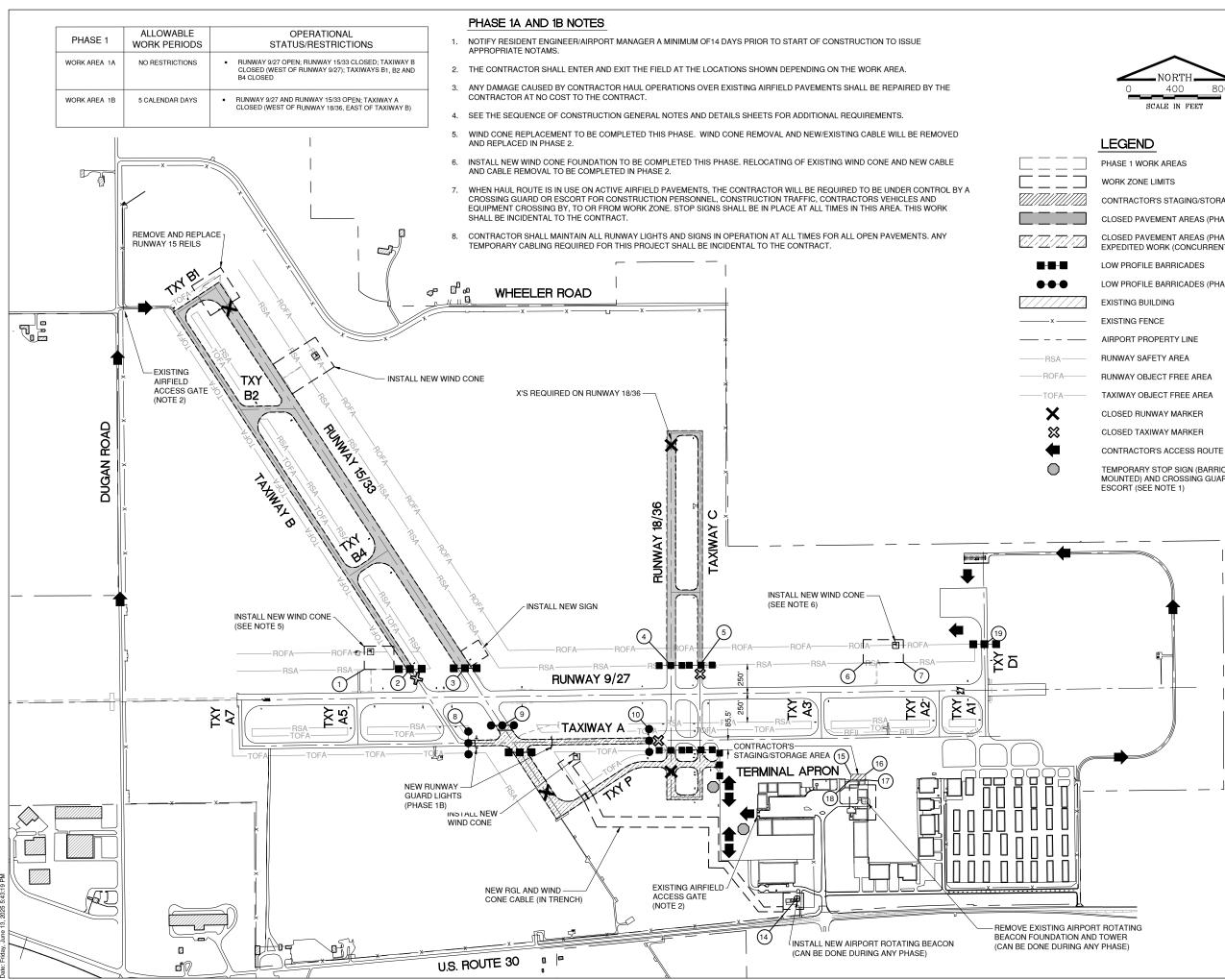
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SITE PLAN AND **PROJECT CONTROL PLAN/** SUMMARY OF QUANTITIES

OF

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- CONTRACTOR'S STAGING/STORAGE AREA
- CLOSED PAVEMENT AREAS (PHASE 1)
- CLOSED PAVEMENT AREAS (PHASE 1B ONLY) EXPEDITED WORK (CONCURRENT TO PHASE 1A)
- LOW PROFILE BARRICADES (PHASE 1B ONLY)

- TEMPORARY STOP SIGN (BARRICADE MOUNTED) AND CROSSING GUARD AND/OR

CONSULTANTS

FINAL

REPLACE AIRFIELD SIGNAGE, WIND CONES, RUNWAY 15 AND 27 RUNWAY END IDENTIFIER LIGHTS, AND AIRPORT **ROTATING BEACON**

JUNE 13, 2025





AURORA MUNICIPAL AIRPORT

MARK	DATE	DESCRIPTION

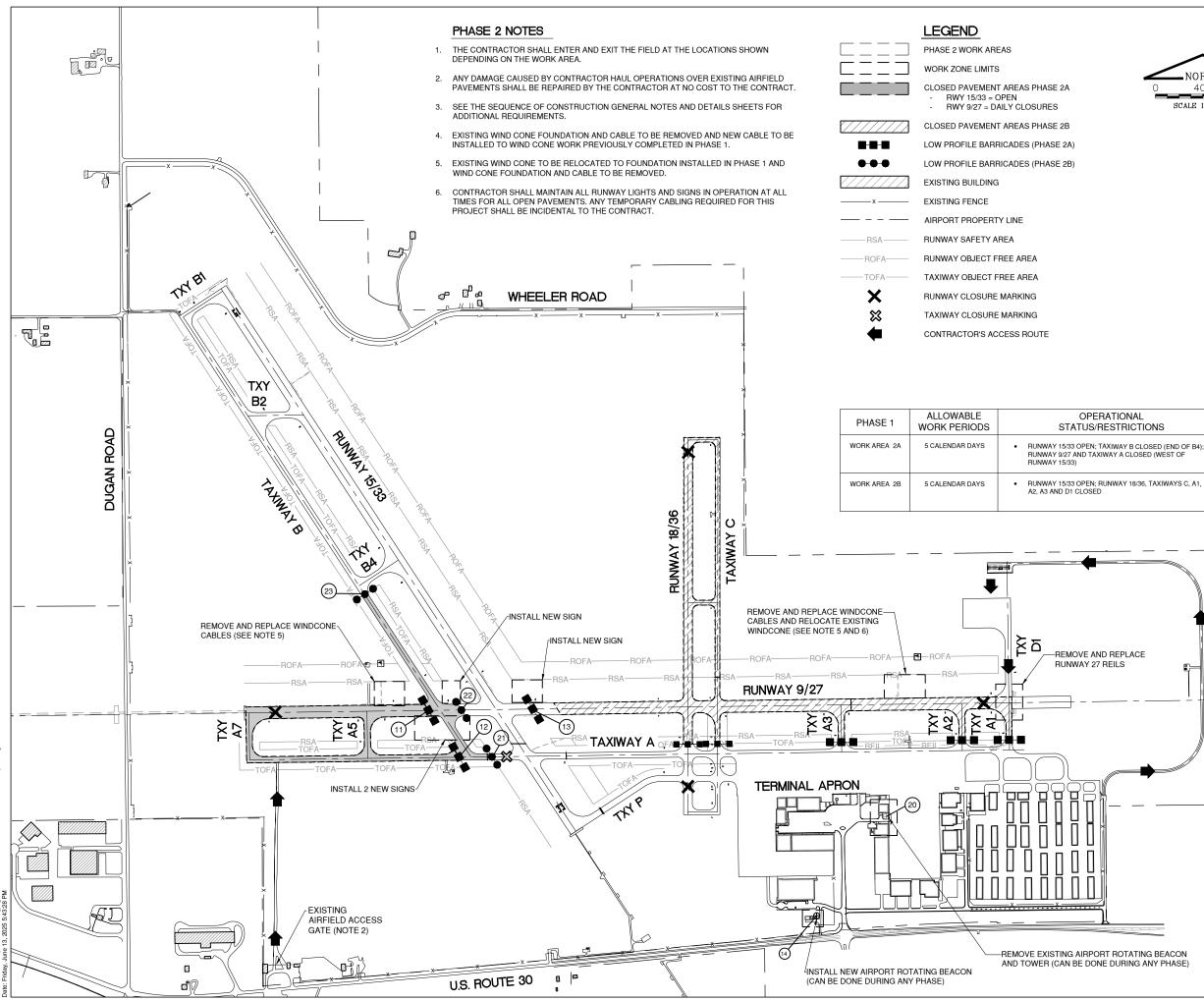
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CMT PROJECT NO:	25007812.00
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DESIGNED BY:	CMJ
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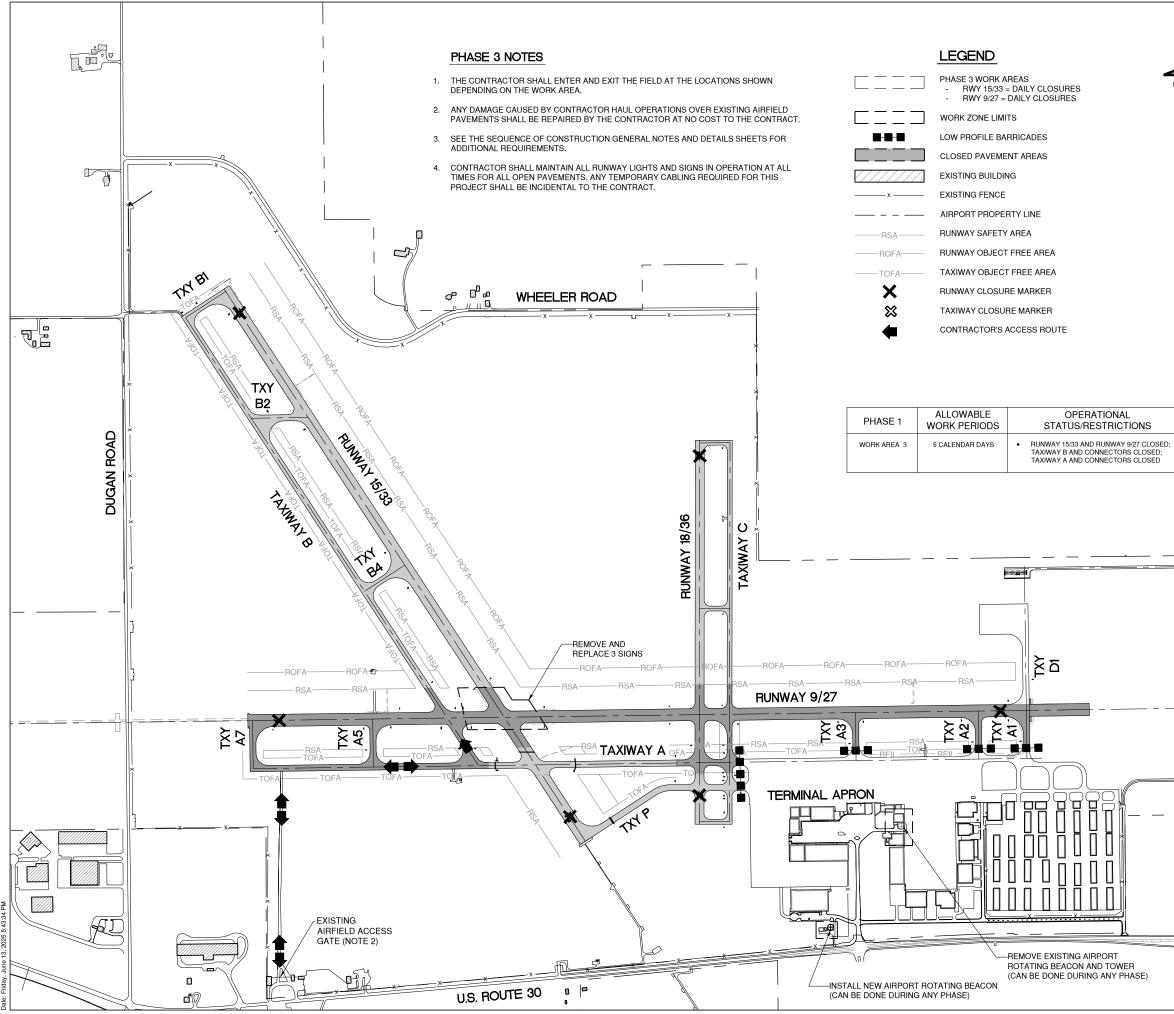
FINAL **REPLACE AIRFIELD** SIGNAGE, WIND CONES, RUNWAY 15 AND 27 RUNWAY END IDENTIFIER LIGHTS, AND AIRPORT **ROTATING BEACON** JUNE 13, 2025 LLINOI CITY OF LIGHTS AURORA MUNICIPAL AIR AURORA MUNICIPAL AIRPORT MARK DATE DESCRIPTION IL PROJECT NO: CMT PROJECT NO: 25007812.00 CAD DWG FILE: DESIGNED BY CMJ JRO DRAWN BY: DKP CHECKED BY APPROVED BY DKP COPYRIGHT SHEET TITLE



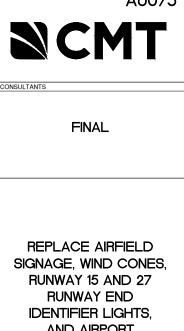
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OF







AND AIRPORT **ROTATING BEACON**

JUNE 13, 2025





AURORA MUNICIPAL AIRPORT

MARK	DATE	DESCRIPTION

IL PROJECT NO:	
CMT PROJECT NO:	25007812.00
CAD DWG FILE:	
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OF

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GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE 1 REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFFTY AND PHASING PLAN (CSPP), FAA AC 150/5370-2, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- 2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2 (LATEST EDITION). NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL З. BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HEB PERSONNEL AND MEETING SAFETY REQUIREMENTS
- A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING. 4 THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING 5. THE CONTRACTOR SHALL SUBMIT THE SPCD FOR APPROVAL
- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED. 6. TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE AIRPORT. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, 7. FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- ALL EXISTING AND PROPOSED FENCE LINES, EXCEPT AS OTHERWISE 8. NOTED, SHALL BE MAINTAINED AND SHALL SERVE AS CONSTRUCTION AROUND THE PERIMETER OF THE PROJECT, ALL EXISTING GATES SHALL BE MAINTAINED , CLOSED AND LOCKED AS DIRECTED BY THE AIRPORT OWNER'S REPRESENTATIVE, SHOULD THE CONTRACTOR CHOOSE TO KEEP A GATE OPEN FOR CONSTRUCTION OPERATIONS, A COMPETENT SECURITY GUARD SHALL MONITOR THE OPEN GATE. ANY COST SHALL NOT BE PAID FOR SEPARATELY, BUT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL 9. AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ONSITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE AIRPORT PAYMENT FOR DUST CONTROL SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION 10. FENCING, BARRICADES, SIGNING, FLAGGER, AIR OPERATIONS AREA (A.O.A) LATH AND RIBBON, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 11. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT. RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE
- ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE 2 CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT OR HIS/HER DESIGNATED REPRESENTATIVE. REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD CIRCUITS IMPACTED BY CONSTRUCTION ACTIVITY.

2. PHASING

- TOTAL CONTRACT TIME SHALL BE 27 CALENDAR DAYS.
- PHASING SHALL BE AS SHOWN ON THE SEQUENCE OF CONSTRUCTION PLAN SHEET

- 3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY
- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND 2. AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. 3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.
- SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.
- 4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)
- THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION SAFETY AND PHASING PLAN SHEETS
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE EXISTING GATE 2. SHOWN. THE ENTRANCE SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLES ACCESS IF APPLICABLE AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.
- SUPERVISORY PERSONNEL SHALL DEMONSTRATE IN THE PRESENCE OF 3. THE AIRPORT MANAGER THAT THEY ARE FAMILIAR WITH AIRPORT RADIO AND AIRPORT DRIVING PROCEDURES IN ORDER TO PERFORM WORK. OTHER CONSTRUCTION PERSONNEL CAN BE WITHIN THE AIRFIELD LIMITS PROVIDED THAT THEY ARE UNDER ESCORT AND IN THE PRESENCE OF AN AUTHORIZED SUPERVISOR. KNOWLEDGE OF THE AIRPORTS PROCEDURES BY THE SUPERVISORY PERSONNEL MUST BE DEMONSTRATED PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR'S STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE
- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, 6. TAXIWAYS AND APBONS OPEN TO AIBCBAFT TRAFFIC SHALL BE LINDER CONTROL BY A FLAGMAN OR ESCORT MONITORING AIRPORT TRAFFIC ON THE RADIO. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN AND AIRPORT RADIO.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE 7. CONTRACTOR FOR HAUL ROADS, STORAGE AREAS AND/OR STAGING AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OB BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK
- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR 8. SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE 2. CONTRACTOR IS NOT WORKING
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES 2. PRIOR TO DRIVING ON AIRFIELD PAVEMENTS

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO 2. AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT
- 3. FOR ANY FOUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
- CONTACTS FOR THIS PROJECT WILL BE DETERMINED AT THE PRECONSTRUCTION MEETING PRIOR TO THE PROJECT START

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX C OF FAA AC 150/5370-2 MAY BE USED TO AID IN THE INSPECTIONS
- THE CONTRACTOR SHALL ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE 1. UNDERGROURD UTILITIES AT CRITICAL POINTS. SEE SECTION 70-16 OF THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY. THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE 2. AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-16 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.
- SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO 3. LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT 4. OWNED UTILITIES AND SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT

12. PENALTIES

NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW

13. SPECIAL CONDITIONS

ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR

14. RUNWAY AND TAXIWAY VISUAL AIDS

RUNWAY OR TAXIWAY CLOSURES (IF REQUIRED) ARE AS DETAILED IN THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET FOR THIS PROJECT, JE ANY BUNWAY OR TAXIWAY CLOSURES ARE REQUESTED. BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2 (LATEST EDITION).

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16. HAZARD MARKING AND LIGHTING

- PROPERTY.

17. PROTECTION

18. OTHER LIMITATIONS ON CONSTRUCTION IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE 1.

- 3.
 - THEY AFFECT FLIGHT SAFETY.
 - CONTROL FREQUENCY.
 - OTHERWISE SPECIFIED.

AIRFIELD LIGHTS AND SIGNS NOTES

- NOT BE ACCEPTABLE.
- 2.

15. MARKING AND SIGNS FOR ACCESS ROUTES

MARKING AND SIGNAGE FOR THE ACCESS ROUTE SHALL BE AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER.

THE CONTRACTOR SHALL FURNISH. ERECT. AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.

ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISOBY CIRCULAR 150/5370-2 AND 150/5210-5 AT ALL TIMES WHILE OPERATING ON AIRPORT

BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION SAFETY AND PHASNG PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL PLACE ALL BARRICADES AND CONSTRUCTION SETBACK LINES ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE, ALL COSTS TO FURNISH, INSTALL, REPOSITION, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

ACCESS TO ACTIVE RUNWAY AND TAXIWAY PAVEMENTS SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON IDOT TYPE II BARRICADES (2 FACH, RIGHT AND LEFT). IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "UNAUTHORIZED ACCESS NOT ALLOWED".

CONTRACTOR PERSONNEL, VEHICLES, FOUIPMENT AND BARBICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS, RUNWAY OBSTACLE FREE ZONE (ROFZ) AND THE RUNWAY SAFETY AREA (RSA) OF ACTIVE RUNWAYS.

PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT

THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS. THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DUBING ALL CONSTRUCTION OPERATION WORKING HOURS.

THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED

WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS, LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED. FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE, LIGHTING SHALL BE APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF

THE CONTRACTOR SHALL SUPPLY AND HAVE IN THEIR POSSESSION AT ALL TIMES AT LEAST ONE AIRPORT RADIO. IN THE EVENT THAT THE AIRPORT MANAGER NEEDS TO CONTACT THE CONTRACTOR DIRECTLY, THE OPERATOR OF SAID BADIO SHALL BE FAMILIAR WITH AIRPORT RADIO PROCEDURES AND TUNED INTO THE GROUND

BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS

CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS INCLUDING LIGHTS/SIGNS ON OTHER SIDE OF BARRICADE LINES WITHIN THE AIRCRAFT MOVEMENT AREA UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL

CONTRACTOR SHALL TURN OFF RUNWAY/TAXIWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY/TAXIWAY CLOSURES. CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY/TAXIWAY, THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY/TAXIWAY CIRCUIT.

AU075

CONSULTANTS

FINAL

REPLACE AIRFIELD SIGNAGE, WIND CONES, RUNWAY 15 AND 27 RUNWAY END **IDENTIFIER LIGHTS** AND AIRPORT **ROTATING BEACON**

JUNE 13, 2025

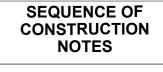




AURORA MUNICIPAL AIRPORT

MARK	DATE	DESCRIPTION

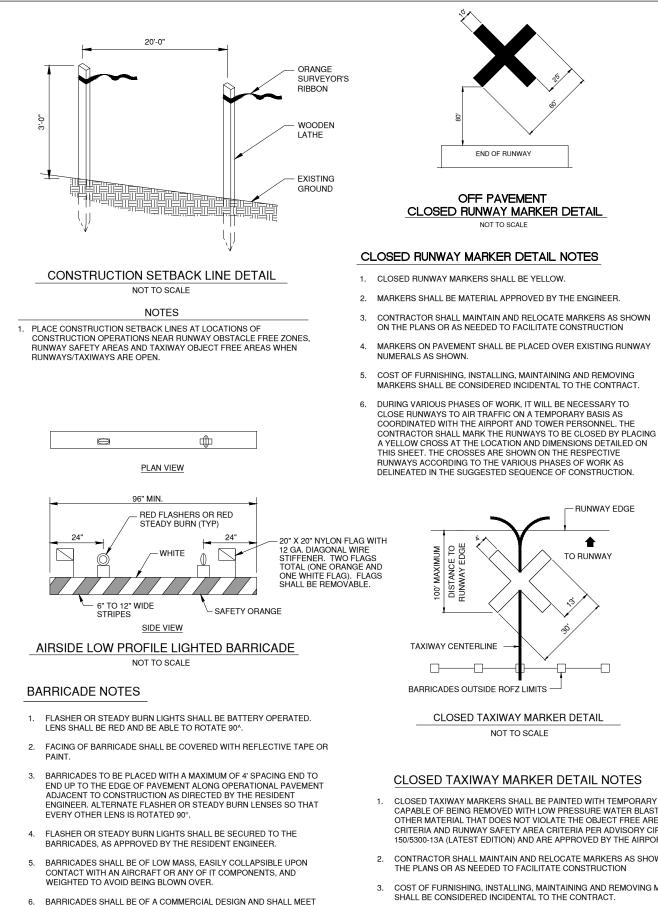
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- 4. PLACE MARKERS OVER TAXIWAY CENTERLINE.
- 5
- CONSECUTIVE DAYS OR MORE.

FAA CRITICAL POINTS TABLE						
					Above	
			Ground	Obstruction	Ground	
Point	Latitude	Longitude	Elevation (ft)	Height (ft)	Elevation (ft)	Critical To
1	N041° 46' 15.20"	W088° 28' 52.93"	710	25	735	9/27
2	N041° 46' 15.24"	W088° 28' 47.67"	710	25	735	9/27
3	N041° 46' 15.31"	W088° 28' 41.41"	711	25	736	9/27
4	N041° 46' 15.58"	W088° 28' 17.81"	715	25	740	9/27
5	N041° 46' 15.61"	W088° 28' 14.49"	715	25	740	9/27
6	N041° 46' 15.82"	W088° 27' 55.82"	716	25	741	9/27
7	N041° 46' 15.87"	W088° 27' 51.23"	716	25	741	9/27
8	N041° 46' 09.89"	W088° 28' 41.01"	704	25	729	9/27
9	N041° 46' 10.42"	W088° 28' 37.12"	709	25	734	9/27
10	N041° 46' 10.10"	W088° 28' 20.31"	715	25	740	9/27
11	N041° 46' 12.78"	W088° 28' 46.86"	710	25	735	15/33
12	N041° 46' 08.87"	W088° 28' 43.42"	707	25	732	15/33
13	N041° 46' 12.94"	W088° 28' 32.44"	711	25	736	15/33
14	N041° 45' 55.63"	W088° 28' 03.35"	701	80	781	15/33
15	N041° 46' 06.28"	W088° 27' 57.25"	705	25	730	9/27
16	N041° 46' 06.28"	W088° 27' 55.53"	705	25	730	9/27
17	N041° 46' 05.80"	W088° 27' 55.53"	705	25	730	9/27
18	N041° 46' 05.80"	W088° 27' 57.25"	705	25	730	9/27
19	N041° 46' 17.43"	W088° 27' 41.93"	702	25	727	9/27
20	N041° 46' 03.95"	W088° 27' 55.57"	701	80	781	9/27
21	N041° 46' 08.92"	W088° 28' 39.65"	701	25	726	15/33
22	N041° 46' 12.83"	W088° 28' 43.16"	701	25	726	15/33
23	N041° 46'22.55"	W088° 28'54.10"	701	25	726	15/33

BE MADE

MAXIMUM ANTICIPATED HEIGHT OF GENERAL CONSTRUCTION EQUIPMENT 25' MAXIMUM ANTICIPATED HEIGHT OF GENERAL CONSTRUCTION EQUIPMENT FOR BEACON INSTALLATION 80'

NOTE - ALL PHASES

ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

EXISTING CRITICAL AIRCRAFT AND REQUIRED SAFETY AREAS					
RUNWAY	15/33				
APPROACH CATEGORY	D				
DESIGN GROUP	Ш				
DESIGN AIRCRAFT	GULFSTREAM 500	GUL			
APPROACH SPEED	160 KNOTS				
WINGSPAN	93.8 FEET				
TAIL HEIGHT	25.8 FEET				
STRENGTH (MGTW)	85,100 LBS.				
AOA @ RUNWAY SAFETY AREA WIDTH (RSA)	500				
RUNWAY OBJECT FREE AREA WIDTH (ROFA)	800				
TAXIWAY SAFETY AREA WIDTH (TSA)	118				
AOA @ TAXIWAY OBJECT FREE AREA WIDTH (TOFA)	171				

AOA = AIRCRAFT OPERATIONS AREA

ON PAVEMENT

NOT TO SCALE

CLOSED RUNWAY MARKER DETAIL

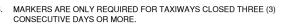
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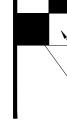
- 6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS
- PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS, RUNWAY OBSTACLE FREE ZONES AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
- ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

DISTANCE TO PISTANCE TO RUNWAY EDGE	
Y CENTERLINE	

- CLOSED TAXIWAY MARKERS SHALL BE PAINTED WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING OR OTHER MATERIAL THAT DOES NOT VIOLATE THE OBJECT FREE AREA CRITERIA AND RUNWAY SAFETY AREA CRITERIA PER ADVISORY CIRCULAR 150/5300-13A (LATEST EDITION) AND ARE APPROVED BY THE AIRPORT
- CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON
- COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS
- MARKERS SHALL BE ADEQUATELY SECURED TO PREVENT MOVEMENT BY PROPELLER WASH, JET BLAST OR OTHER WIND CURRENTS







AU075

CONTRACTOR ACCESS ROUTE NOTES

ALL PAVEMENTS OR TURF AREAS UTILIZED BY THE CONTRACTOR FOR AN ACCESS ROUTE, STAGING, OR STORAGE SHALL BE REPAIRED AND RESTORED TO THEIR ORIGINAL CONDITION TO THE SATISFACTION OF THE AIRPORT. NO ADDITIONAL COMPENSATION TO PROTECT, REPAIR, OR RESTORE THESE AREAS SHALL

DESIGN AIRCRAFT APPROACH CATEGORY: D DESIGN AIRPORT GROUP: III GROUND CONTROL FREQUENCY: 121.70 AIR CONTROL FREQUENCY: 120.60

9/27
D
Ш
FSTREAM 500
160 KNOTS
93.8 FEET
25.8 FEET
85,100 LBS.
500
800
118
171
BB ALP

- WHITE ORANGE

CONSTRUCTION EQUIPMENT AND TRUCK/VEHICLE SIGNAL FLAG

NOT TO SCALE

CONSULTANTS

FINAL

REPLACE AIRFIELD SIGNAGE, WIND CONES, RUNWAY 15 AND 27 RUNWAY END IDENTIFIER LIGHTS, AND AIRPORT **ROTATING BEACON**

JUNE 13, 2025





AURORA MUNICIPAL AIRPORT

MARK	DATE	DESCRIPTION

IL PROJECT NO:	
CMT PROJECT NO:	25007812.00
CAD DWG FILE:	
DESIGNED BY:	CMJ
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
COPYRIGHT:	

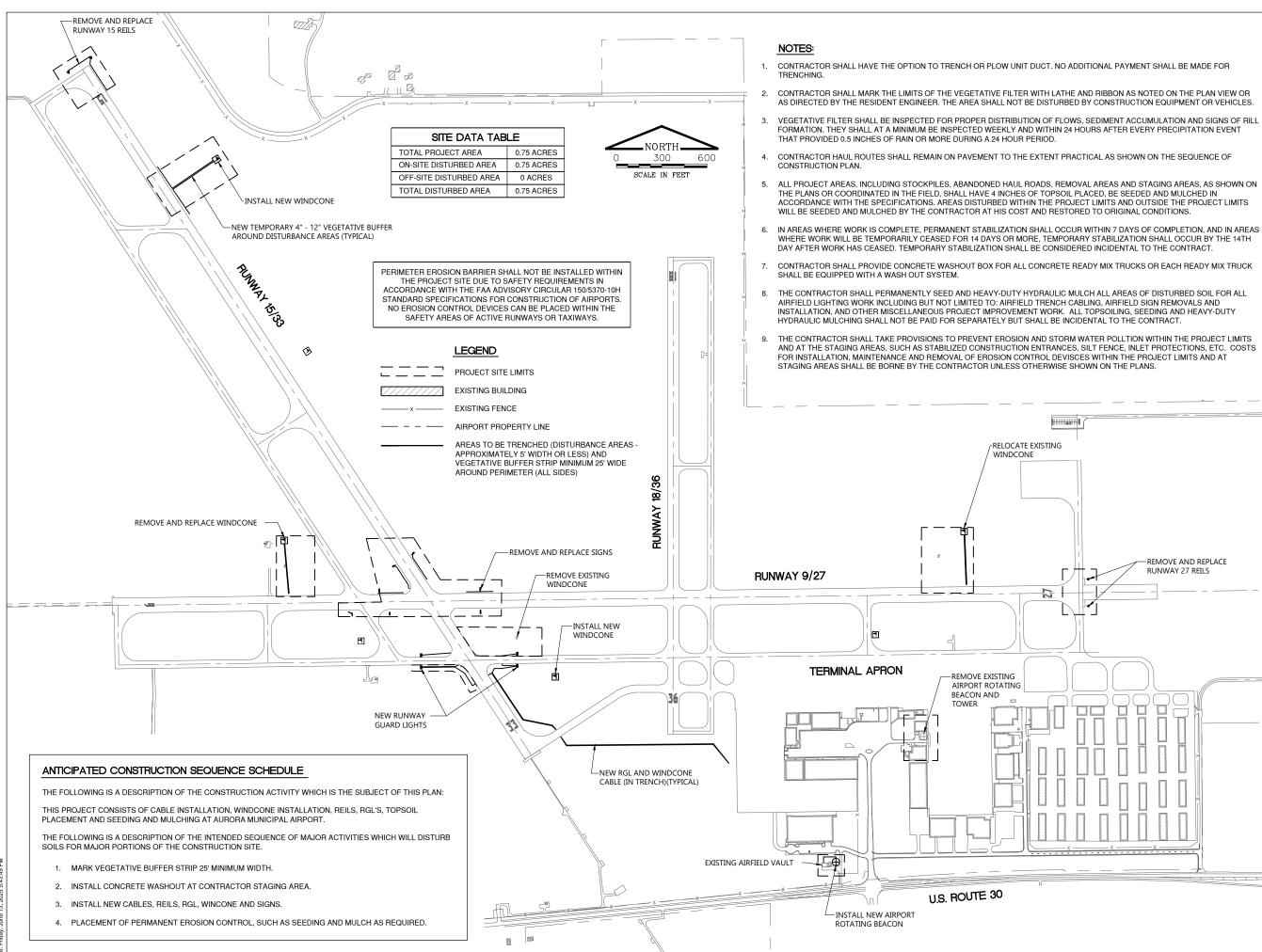
SHEET TITLE

SEQUENCE OF CONSTRUCTION DETAILS

OF

SHEET

7



REMOVE AND REPLACE

RUNWAY 27 REILS

CONSULTANTS

FINAL

REPLACE AIRFIELD SIGNAGE, WIND CONES, RUNWAY 15 AND 27 RUNWAY END IDENTIFIER LIGHTS, AND AIRPORT **ROTATING BEACON**

JUNE 13, 2025





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SHEET TITL

STORM WATER POLLUTION **PREVENTION PLAN**

OF

SHEET

8

STORM WATER POLLUTION PREVENTION NOTES

THE FOLLOWING IS TO ESTABLISHED BEST MANAGEMENT PRACTICES TO BE INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEERAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEEN, THEREFORE MINIMIZING THE MONITOR AT THE PROVIDE HERE IN A TWO PROVIDED HERE IN THE AND THE AND AND THE STORE THE AMOUNT OF A THE STORE OF THE AND THE MONITOR THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE THAT WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES IS LESS THAN ONE (1) ACRE

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE

THE CONSTRUCTION SITE DRAINS INTO STORM SEWERS THAT OUTLET INTO THE WELCH CREEK

SEDIMENTATION AND EROSION CONTROL NOTES

THE RESIDENT ENGINEER WILL PERFORM PERIODIC INSPECTION OF THE SITE TO IDENTIFY POTENTIAL SEDIMENT AND EROSION ISSUES.

ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL, UNLESS OTHERWISE STATED

THE DRAWINGS. SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED WITHIN SEVEN (7) DAYS OF DISTURBANCE

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

- WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION
- 2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED. AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS
- 3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
- B. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. II NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
- 4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLILUTANT IN ACCORDANCE WITH FPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
- 5. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF OFF SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALI BE INCIDENTAL TO THE COST OF THE PROJECT.
- 6. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING

TEMPORABY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

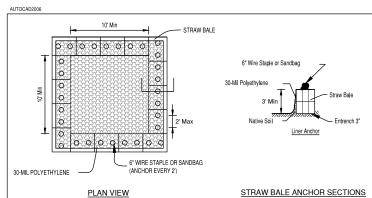
ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED. TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

GRADING AIRFIELD SIGN NOTES:

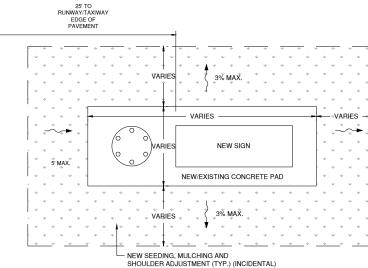
- 1. SLOPES SHOWN ARE FROM FAA STANDARDS AND MAY NOT REFLECT THE ACTUAL GRADES/SLOPES IN THE FIELD
- 2. IT IS ESTIMATED THAT 2 C.Y. OF EMBANKMENT MAY BE REQUIRED TO CONSTRUCT THE SIGN BASE FOUNDATIONS. ALL MATERIAL USED TO CONSTRUCT THE EMBANKMENT SHALL BE APPBOVED BY THE RESIDENT ENGINEER ALL OFFSITE MATERIAL LISED TO CONSTRUCT APPROVED BY THE RESIDENT ENGINEER. ALL OFFSITE MATERIAL OSED TO CONSTRUCT THE EMBANKMENT SHALL BE CATEGORIZED AS CLEAN CONSTRUCTION OR DEMOLITION DEBRIS IN ACCORDANCE WITH SECTION 905-1.2 OF THE SPECIAL PROVISIONS. COSTS TO CONSTRUCT THE EMBANKMENT SHALL BE INCIDENTAL TO THE SIGN PAY ITEM. ESTIMATED QUANTITY OF EMBANKMENT IS FOR INFORMATION ONLY AND NO ADDITIONAL COMPENSATION SHALL BE MADE FOR ANY ADDITIONAL EMBANKMENT NEEDED.
- 3. ACTUAL LOCATION OF THE SIGN WITHIN THE TAXIWAY SAFETY AREA WILL VARY DUE TO PAVEMENT WIDTHS AND VARIANCES IN SIGN FOUNDATION LEN
- 4. DIMENSIONS: BUNWAY 16/34 = 250' RUNWAY 12/30 = 75'
 - RUNWAY 6/24 = 60' ALL TAXIWAYS = 59



- refabricated washout container for approval by the Resident Engineer at no additional cost to the project
 - other incidentals necessary to construct the washout facility will not be measured for payment but will be considered incidental to the concrete pavement pay items

nod_____ TEMPORARY CONCRETE B.JOHNSON _____6/08 WASHOUT FACILITY - STRAW BALE ----

Steel Post Min.



TYPICAL LANDSCAPING FOR NEW SIGN NOT TO SCALE

KANE DUPAGE SWCD SOIL EROSION AND SEDIMENT CONTROL NOTES

UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE CANSTRUCTED ACCORDING TO THE MINIMUM STANDARDS AND SPECIFICATIONS IN THE LATEST VERSION OF THE ILLINOIS URBAN MANUAL.

THE KANE-DUPAGE SOIL AND WATER CONSERVATION DISTRICT (KDSWCD) MUST BE NOTIFIED ONE WEEK PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, ONE WEEK PRIOR TO THE COMMENCEMENT OF LAND DISTURBING ACTIVITIES, AND ONE WEEK PRIOR TO THE FINAL INSPECTION.

A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.

PRIOR TO COMMENCING LAND-DISTURBING ACTIVITIES IN AREAS OTHER THAN INDICATED ON THESE PLANS (INCLUDING BUT NOT LIMITED TO, ADDITIONAL PHASES OF DEVELOPMENT AND OFF-SI BORROW OR WASTE AREAS) A SUPPLEMENTARY EROSION CONTROL PLAN SHALL BE SUBMITTED TO THE OWNER FOR REVIEW BY THE KDSWCD

THE CONTRACTOR IS RESPONSIBLE FOR INSTALLTION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE KDSWCD.

DURING DEWATERING OPERATIONS, WATER WILL BE PUMPED INTO SEDIMENT BASINS OR SILT TRAPS, DEWATERING DIRECTLY INTO FIELD TILES OR STORMWATER STRUCTURES IS PROHIBITED

IT IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO INFORM ANY SUB-CONTRACTOR(S) WHO MAY PERFORM WORK ON THIS PROJECT, OF THE REQUIREMENTS IN IMPLEMENTING AND MAINTAINING THESE EROSION CONTROL PLANS AND THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS SET FORTH BY THE ILLINOIS EPA.

THE MEANS, METHODS, AND LOCATIONS FOR ANY DEWATERING WORK SHOULD BE COORDINATED WITH KDSWCD. ANY DEWATERING OPERATIONS SHALL FOLLOW ILLINOIS URBAN MANUAL PRACTICE STANDARD 813-DEWATERING

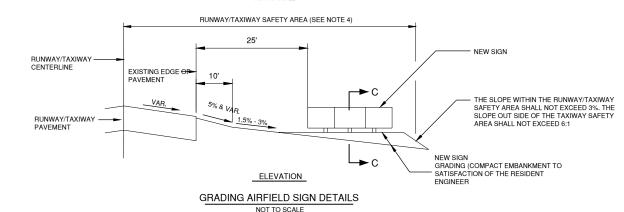
BACKUP BEST MANAGEMENT PRACTICES SHALL BE KEPT ON-SITE IN ORDER TO IMPLEMENT CORRECTIVE ACTIONS FOLLOWING INSTANCES OF NONCOMPLIANCE

TEMPORARY VEGETATIVE FILTER STRIP NOTES

- CONTRACTOR SHALL MARK THE LIMITS OF THE VEGETATIVE FILTER WITH LATHE AND RIBBON AS NOTED ON THE PLAN VIEW OR AS DIRECTED BY THE RESIDENT ENGINEER. THE AREA SHALL NOT BE DISTURBED BY CONSTRUCTION EQUIPMENT OR VEHICLES.
- 2. BUFFFR STRIP SHALL COMMENCE 5' FROM THE GRADING LIMITS AND SHALL BE A MINIMUM OF 25'
- 3. VEGETATIVE FILTER SHALL BE INSPECTED FOR PROPER DISTRIBUTION OF FLOWS, SEDIMENT ACCUMULATION AND SIGNS OF RILL FORMATION. THEY SHALL AT A MINIMUM BE INSPECTED WEEKLY AND WITHIN 24 HOURS AFTER EVERY PRECIPITATION EVENT THAT PROVIDED 0.5 INCHES OF RAIN OR MORE DURING A 24 HOUR PERIOD.
- 4 IF THE BUFFER BECOMES SILT COVERED, CONTAINS BILLS OR IS OTHERWISE BENDERED INFFECTIVE, OTHER PERIMETER SEDIMENT CONTROL MEASURES SHALL BE INSTALLED. ERODED AREAS SHALL BE REPAIRED AND STABILIZED, REPAIR SHALL BE COMPLETED AS SOON AS POSSIBLE WITH CONSIDERATION TO SITE CONDITIONS.
- THE BUFFER VEGITATION SHALL BE MAINTAINED BETWEEN A HEIGHT OF 4-12 INCHES.
- MARKING AND MAINTENANCE OF THE BUFFER AREA WILL NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

STAGING AND PROTECTION NOTES

- AT THE STAGING AREA. THESE DEVICES, INCLUDING INLET PROTECTIONS, SILT FENCE, BALES, DITCH CHECKS, STABILIZED CONSTRUCTION ENTRANCES, ETC., SHALL NOT BE PAID FOR, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT
- 2. AT THE COMPLETION OF WORK, THE STAGING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION AN THE CONTRECTION OF WORK, THE ORANING ATTENDED THE DEFINITE DEFINITION OF WORK SHALL INCLUDE REMOVING ANY AND ALL STORM WATER POLLUTION PREVENTION DEVICES, RESTORING RUTTED AREAS, SEEDING AND MULCHING
- 3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE STAGING AREA(S) IN A MANNER TO PREVENT POLLUTION AND SILTATION OF THE EXISTING STORM SEWER SYSTEM.





SIGN DETAIL

Letters 6" Min. Height

+

NOTES 1. Maintaining temporary concrete washout facilities shall include removing and disposing of hardend concrete and/or slurry and returning the facilities to a functional condition. CONCRETÈ WASHOUT 2. Facility shall be cleaned or reconstructed in a new area once AREA washout becomes two-thirds full. Plywood or Aluminur 48" X 24" Min.

- 3. Each straw bale is to be staked in place using (2) 2"x2"x4' wooden
- 4"x4"x6' Wood Post or 6' The contractor may propose alternate configurations/materials or

5. The construction, maintenance, materials, removals, labor and all

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE STORM WATER POLLUTION PREVENTION DEVICES

ALL EQUIPMENT FUELING AND GREASING SHALL BE COMPLETED AT THE STAGING AREA. SPILLS SHALL BE IMMEDIATELY CONTAINED AND THE AREA CLEANED AT NO ADDITIONAL COST THE CONTRACT.

AU075

NCMT

CONSULTANTS

FINAL

REPLACE AIRFIELD SIGNAGE, WIND CONES, RUNWAY 15 AND 27 RUNWAY END IDENTIFIER LIGHTS, AND AIRPORT **ROTATING BEACON**

JUNE 13, 2025





AURORA MUNICIPAL AIRPORT

MARK DATE DESCRIPTION

IL PROJECT NO:	
CMT PROJECT NO:	25007812.00
CAD DWG FILE:	
DESIGNED BY:	CMJ
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
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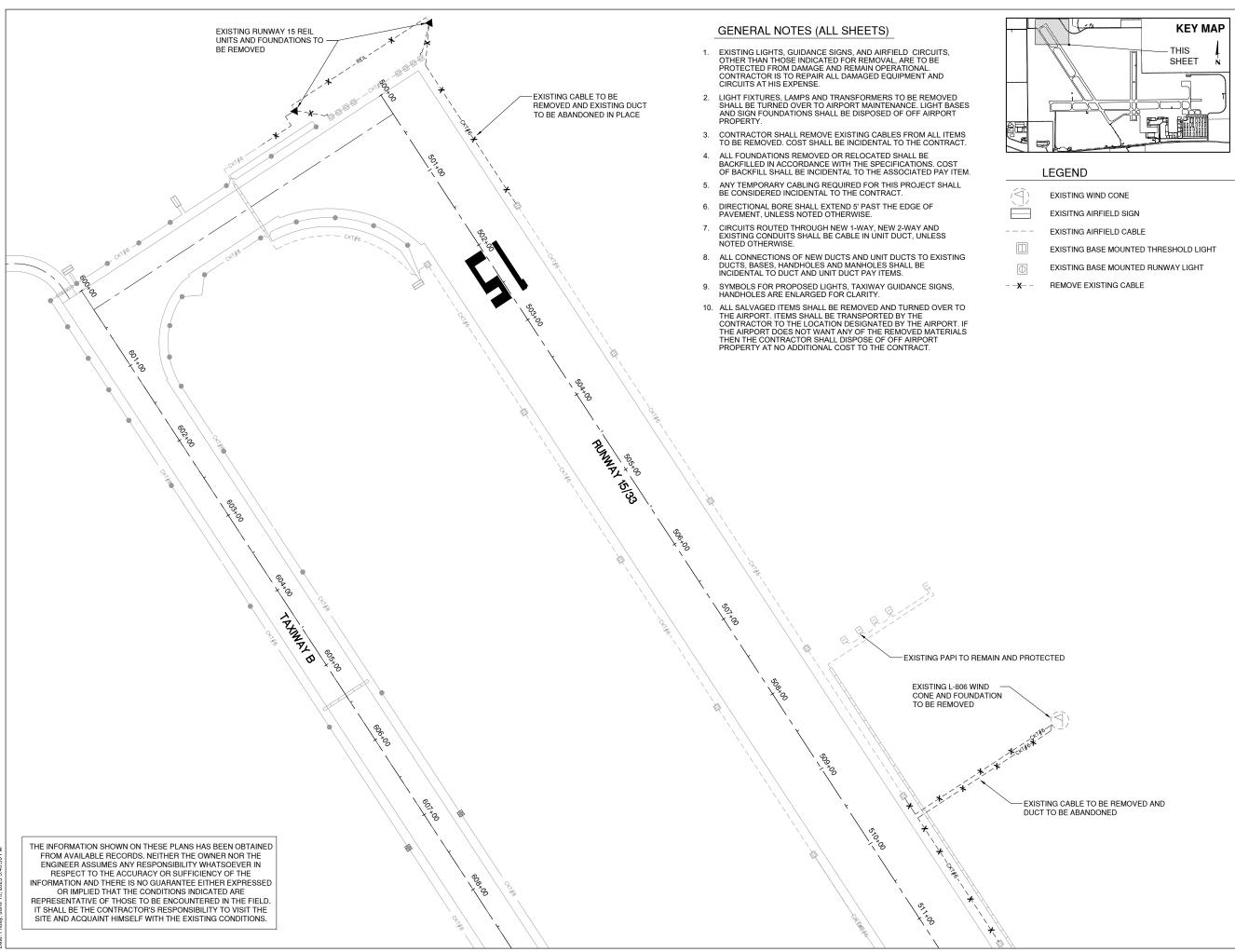
SHEET TITLE

STORMWATER POLLUTION **PREVENTION PLAN - NOTES** AND DETAILS

OF

SHEET

9





CONSULTANTS

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REPLACE AIRFIELD SIGNAGE, WIND CONES, RUNWAY 15 AND 27 RUNWAY END IDENTIFIER LIGHTS, AND AIRPORT **ROTATING BEACON**

JUNE 13, 2025





AURORA MUNICIPAL AIRPORT

MARK	DATE	DESCRIPTION

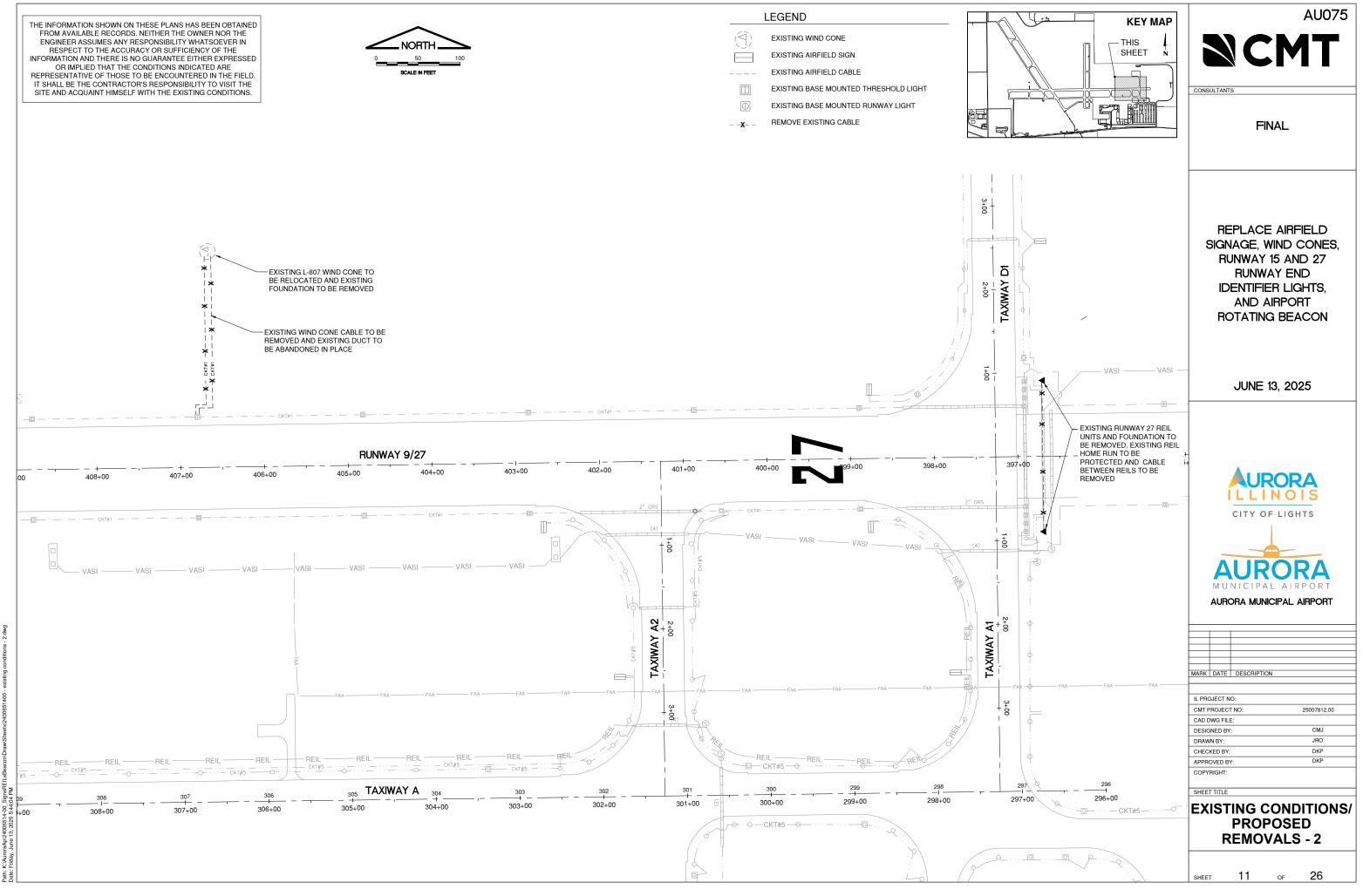
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SHEET TITLE

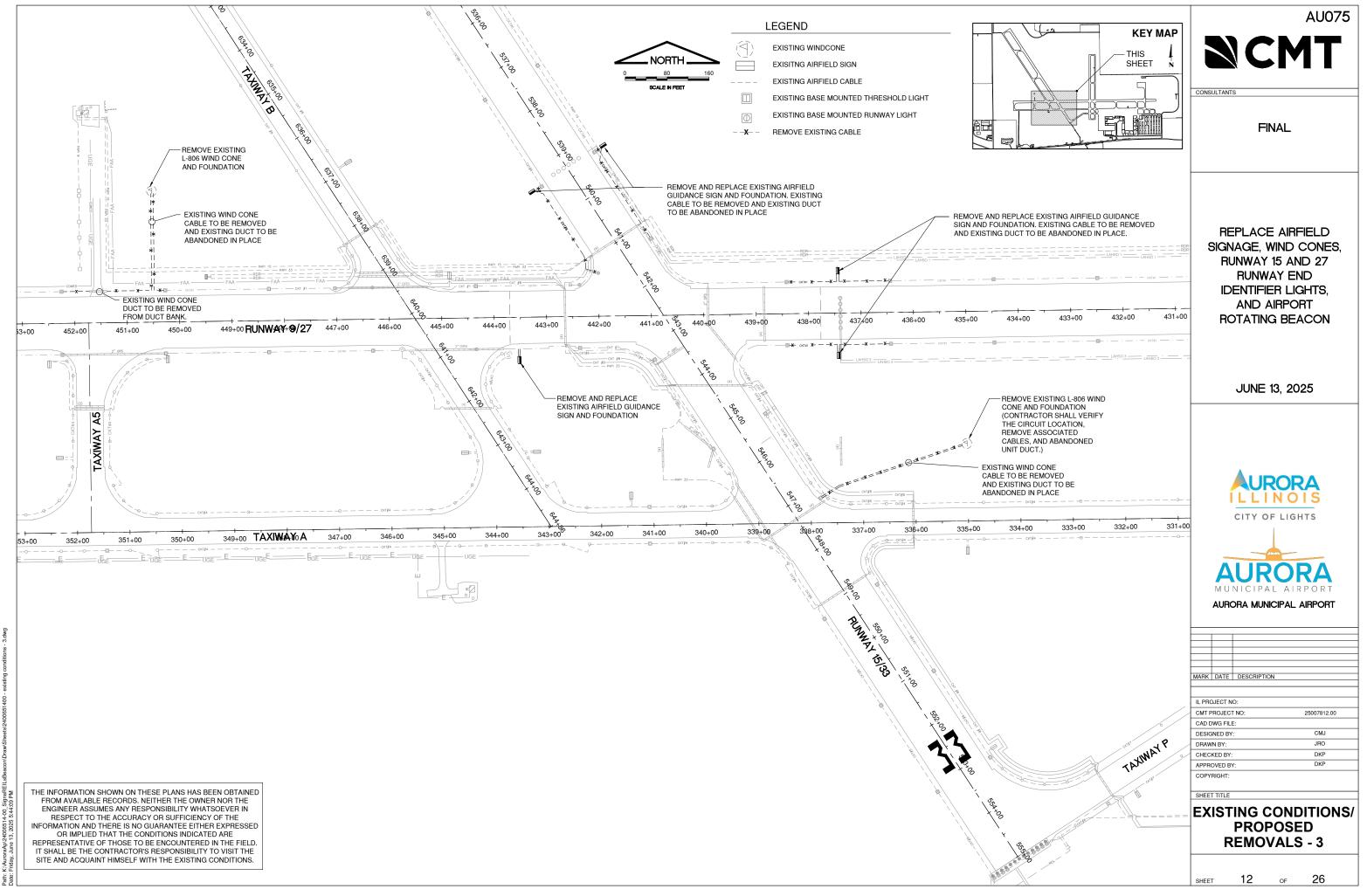
EXISTING CONDITIONS/ PROPOSED **REMOVALS - 1**

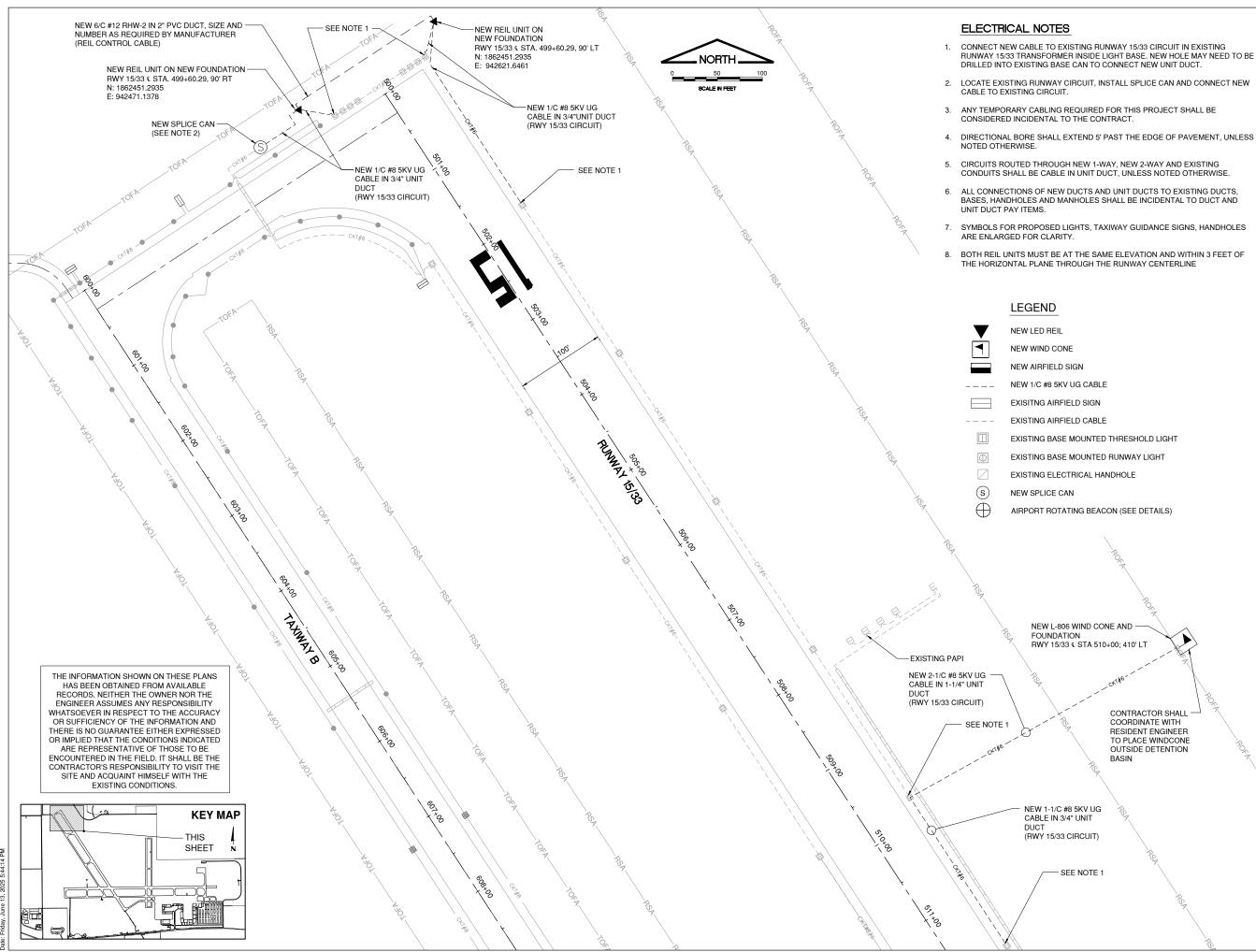
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REPLACE AIRFIELD SIGNAGE, WIND CONES, RUNWAY 15 AND 27 RUNWAY END IDENTIFIER LIGHTS, AND AIRPORT **ROTATING BEACON**

JUNE 13, 2025





AURORA MUNICIPAL AIRPORT

MARK DATE DESCRIPTION

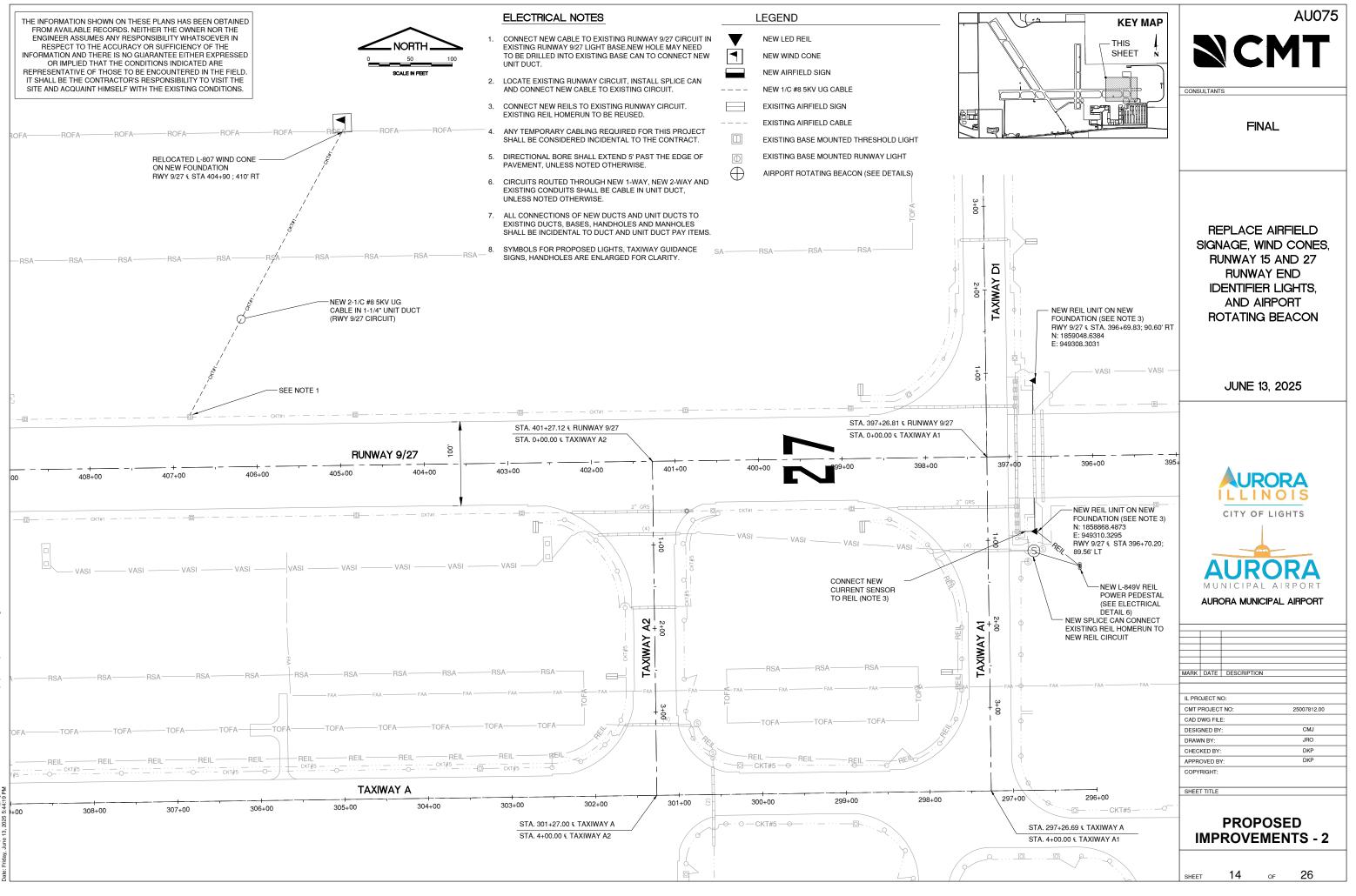
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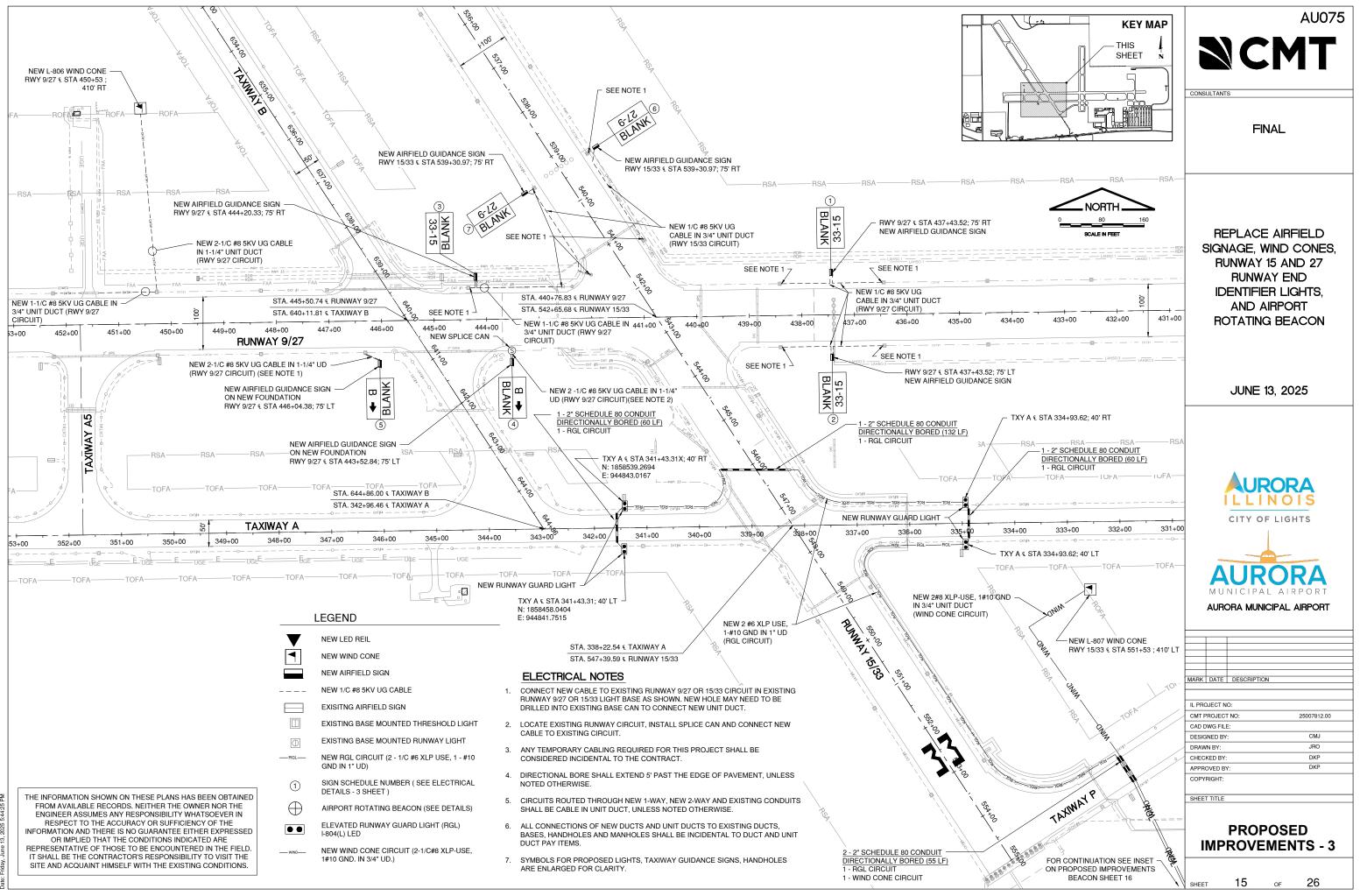
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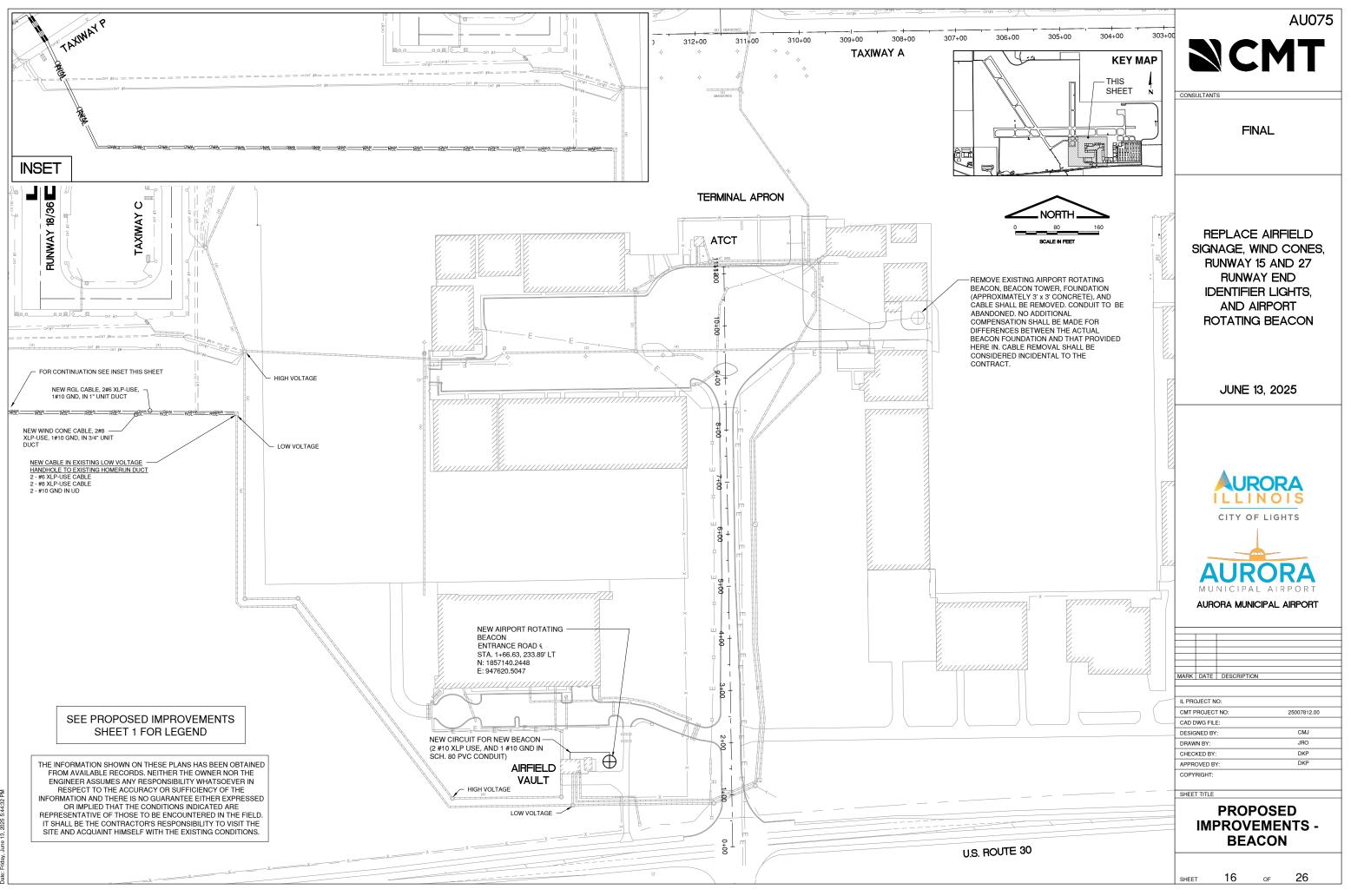
PROPOSED **IMPROVEMENTS - 1**

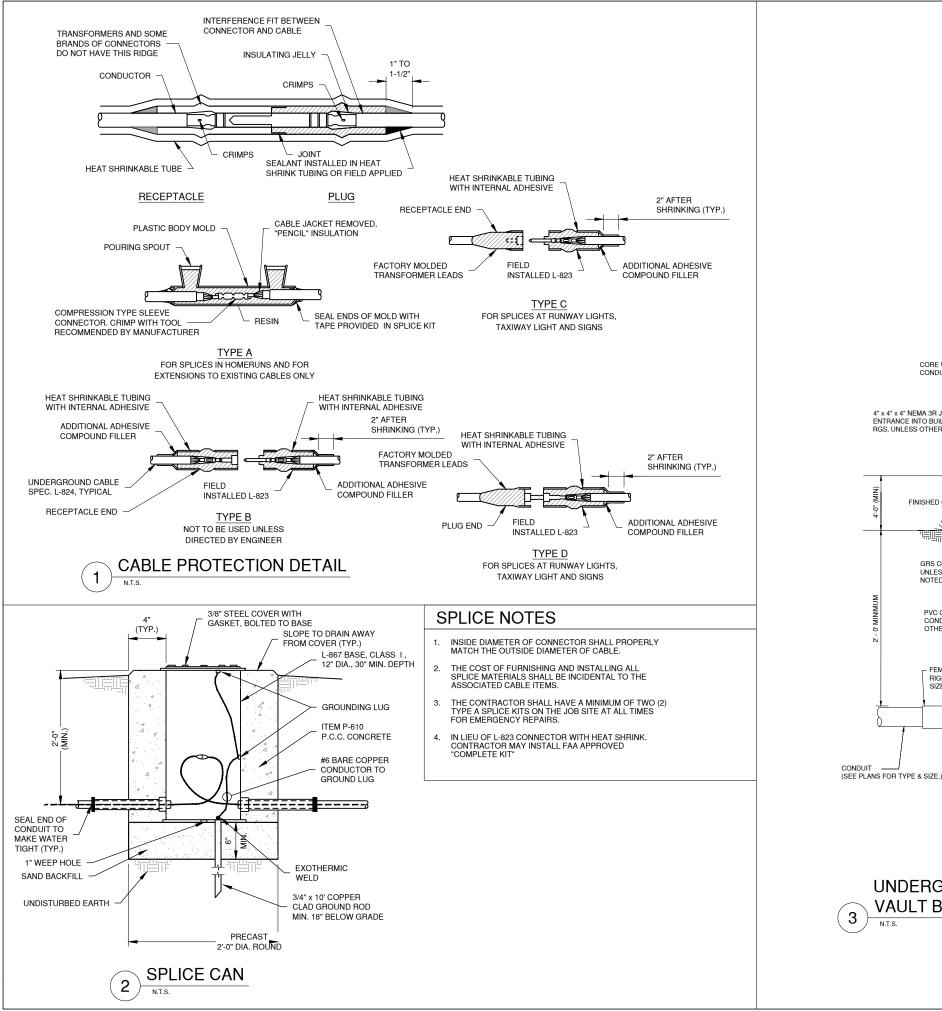
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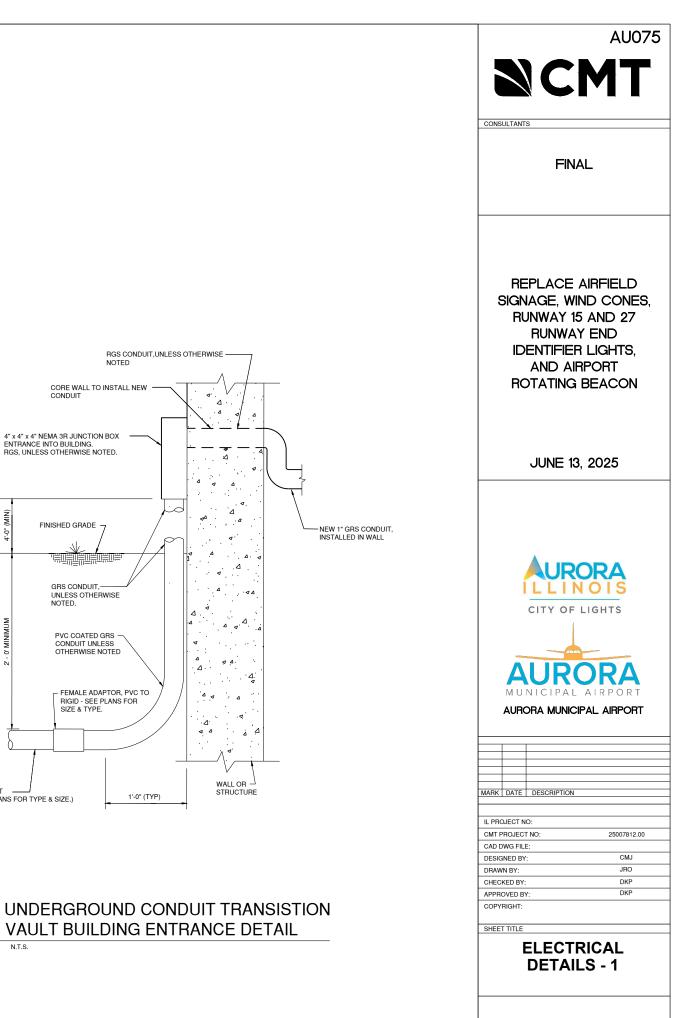
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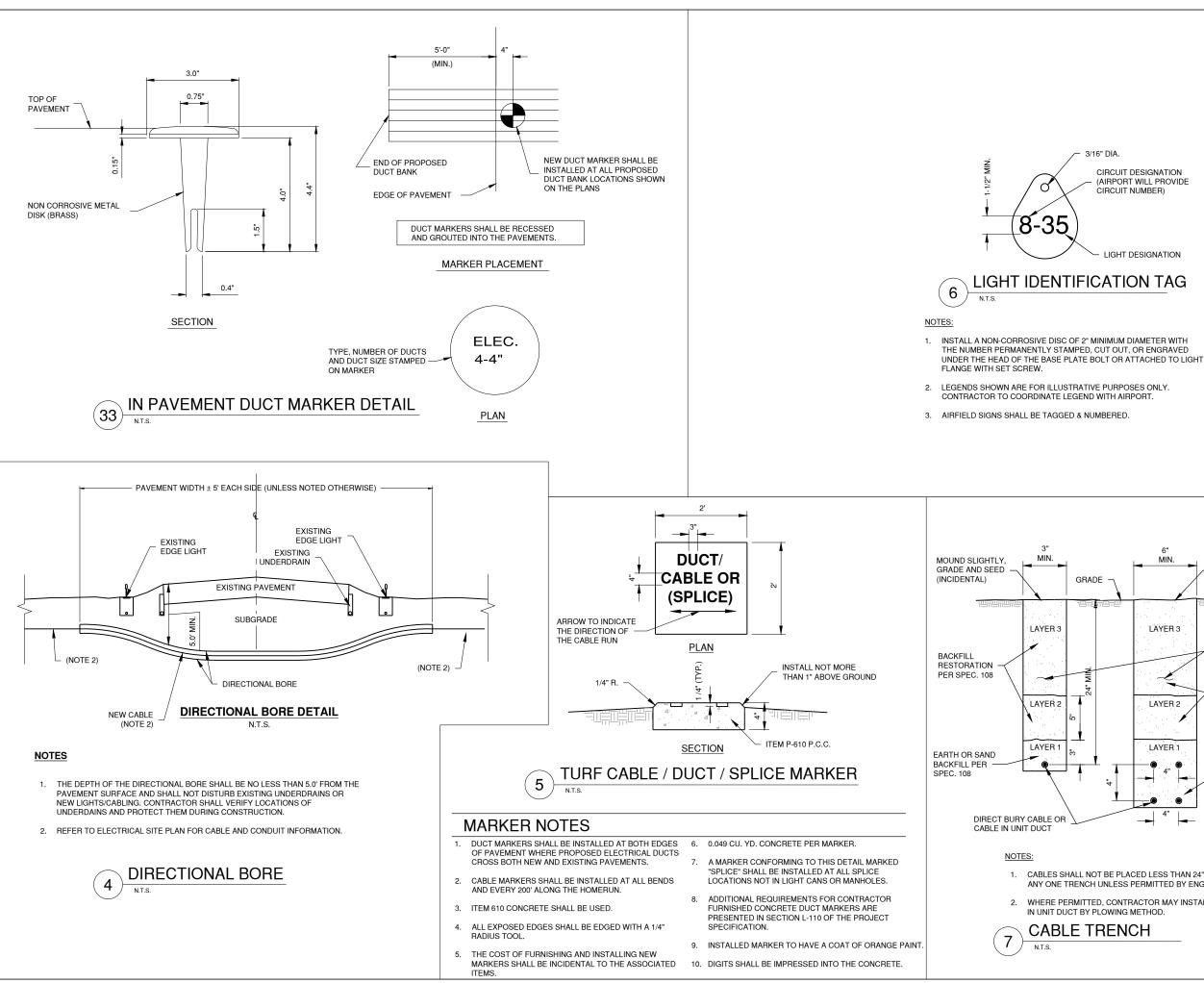
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OF

SHEET

26

06514-00_SignsRE 2025 5:44:39 PM

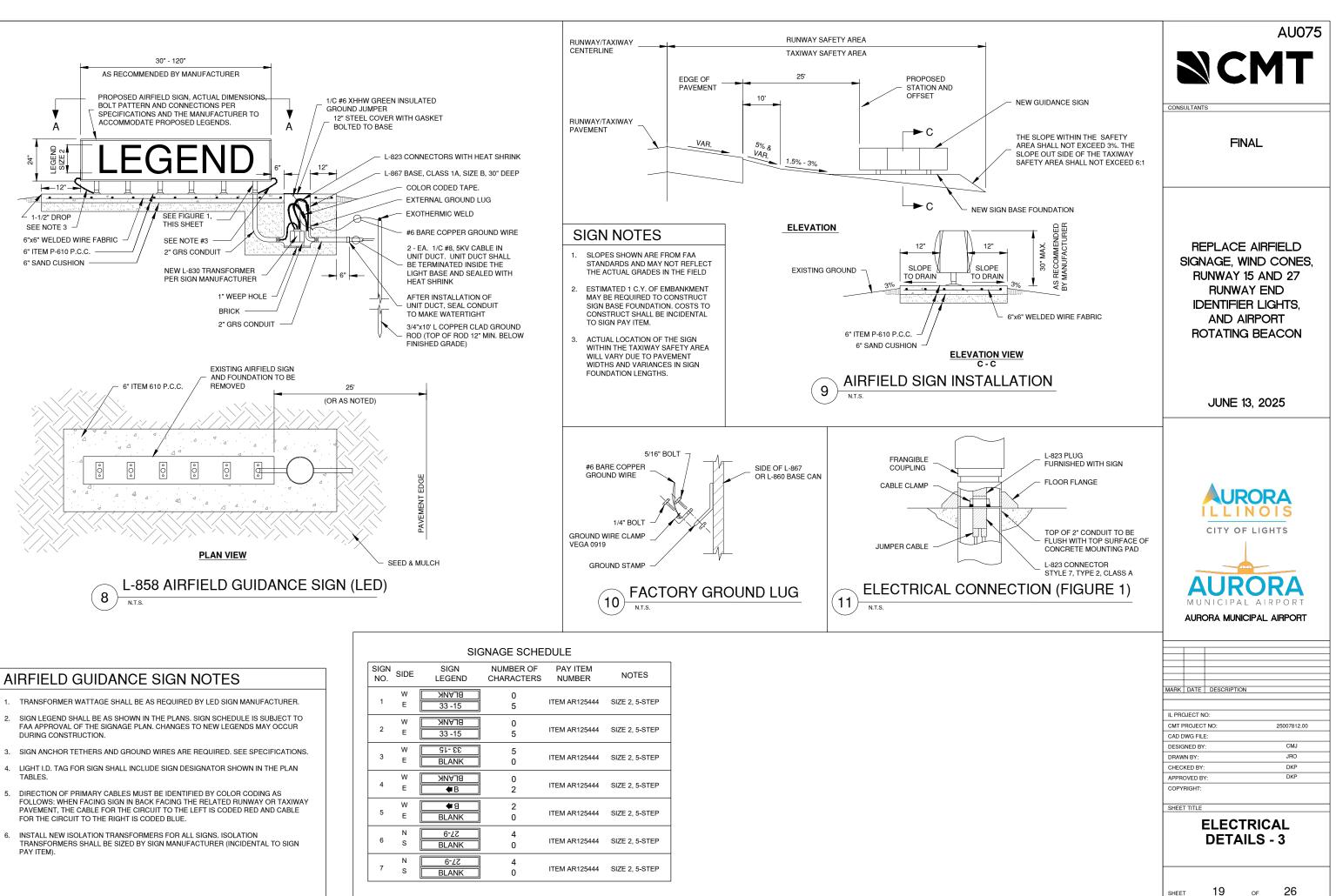


AU075 CONSULTANTS FINAL 3/16" DIA. CIRCUIT DESIGNATION (AIRPORT WILL PROVIDE CIRCUIT NUMBER) LIGHT DESIGNATION REPLACE AIRFIELD SIGNAGE, WIND CONES, RUNWAY 15 AND 27 RUNWAY END IDENTIFIER LIGHTS, AND AIRPORT **ROTATING BEACON** JUNE 13, 2025 6" . I N O MIN. MOUND SLIGHTLY GRADE AND SEED CITY OF LIGHTS (INCIDENTAL) GRADE LAYER 3 RORA CAUTION TAPE MUNICIPAL AIRI AURORA MUNICIPAL AIRPORT BACKFILL RESTORATION PER SPEC. 108 LAYER 2 LAYER 1 MARK DATE DESCRIPTION EARTH OR SAND IL PROJECT NO: BACKELL PER SPEC. 108 CMT PROJECT NO: 25007812.00 CAD DWG FILE: DESIGNED BY CMJ DRAWN BY: JRO DKP CHECKED BY APPROVED BY DKP COPYRIGHT CABLES SHALL NOT BE PLACED LESS THAN 24" DEEP IN SHEET TITLE ANY ONE TRENCH UNLESS PERMITTED BY ENGINEER. **ELECTRICAL** WHERE PERMITTED, CONTRACTOR MAY INSTALL CABLE IN UNIT DUCT BY PLOWING METHOD. **DETAILS - 2** CABLE TRENCH

18

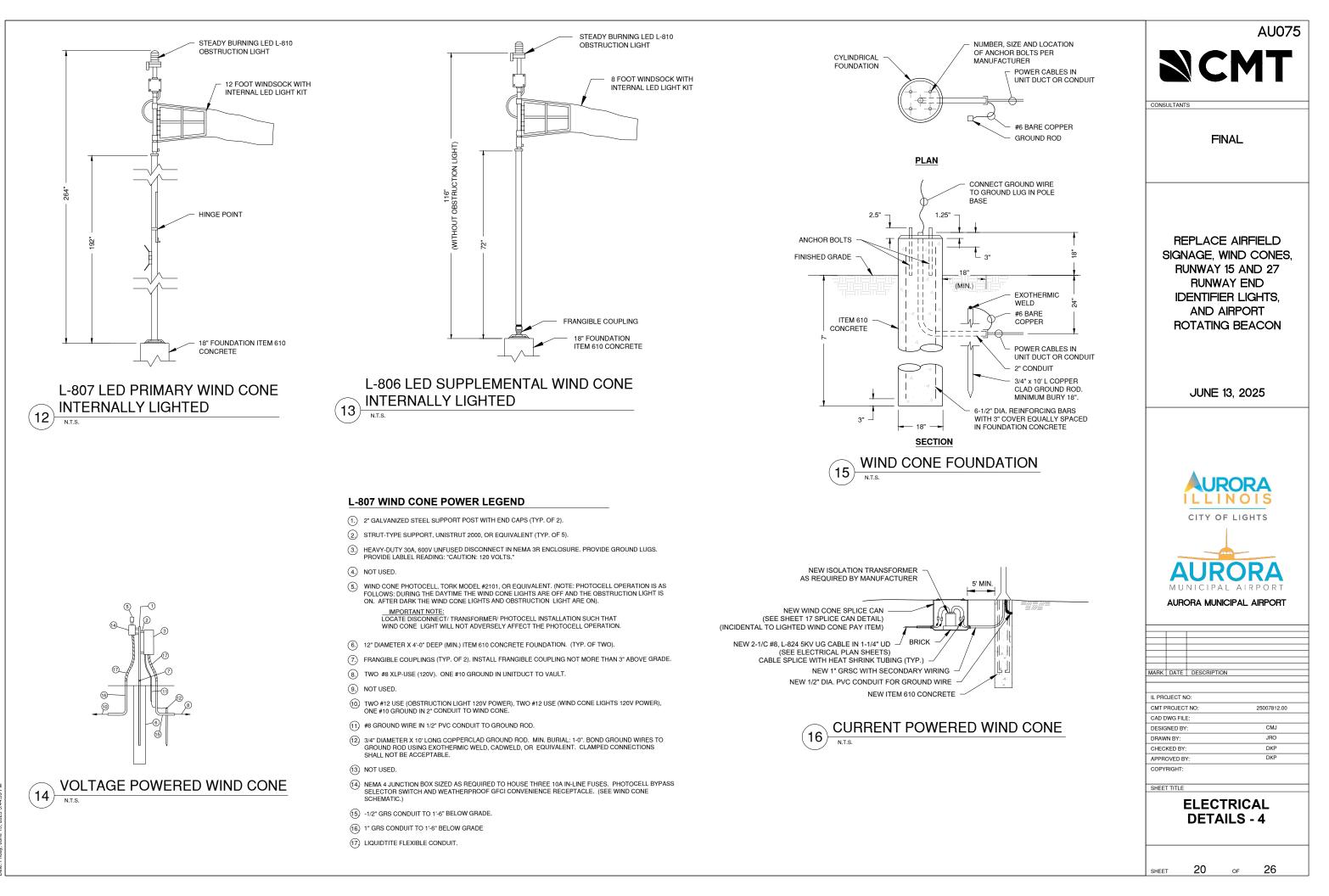
OF

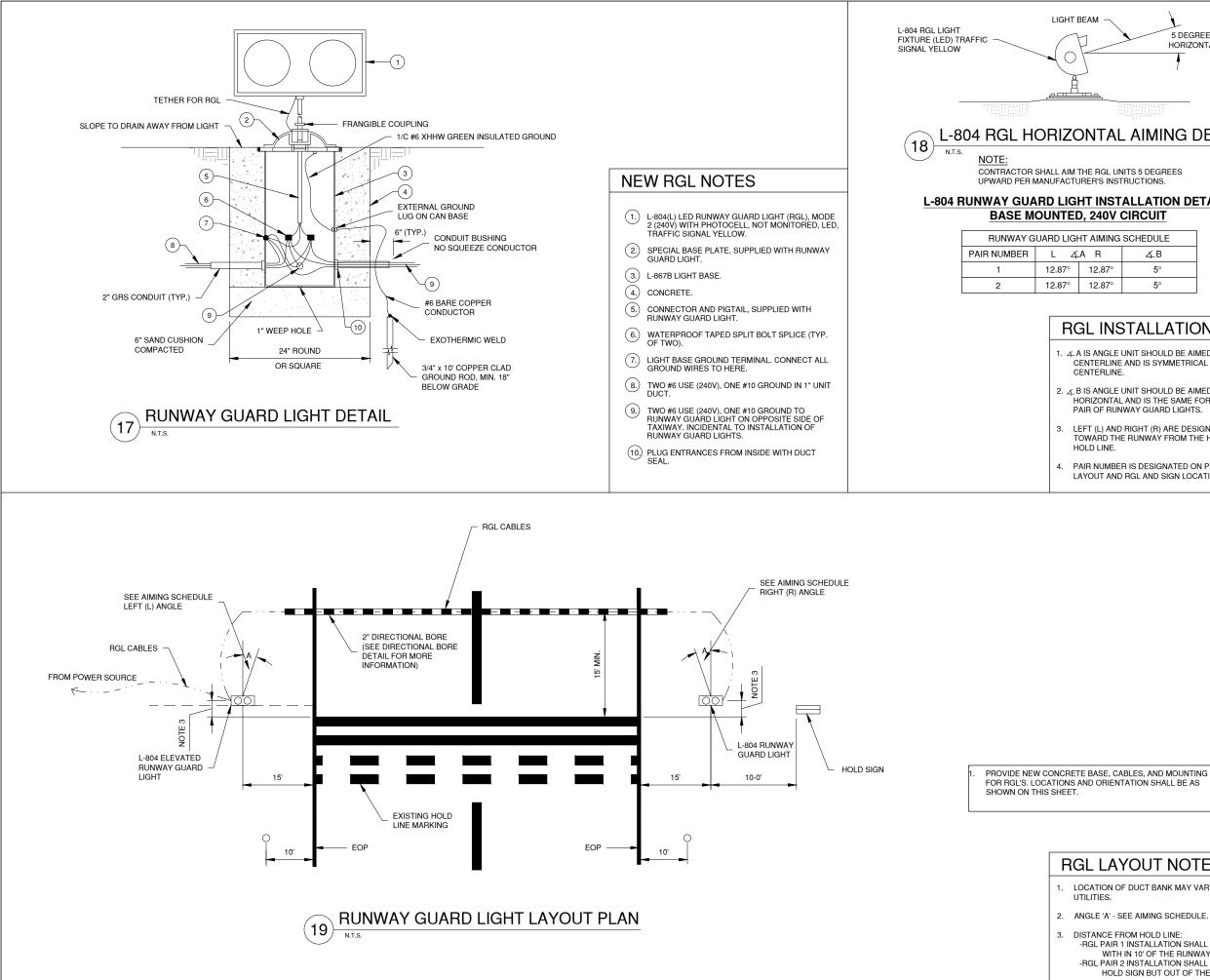
SHEET



Path: Date:

SIDE	SIGN LEGEND	NUMBER OF CHARACTERS	PAY ITEM NUMBER	NOTES
W E	ВГУИК 33 -15	0 5	ITEM AR125444	SIZE 2, 5-STEP
W E	ВГУИК 33 -15	0 5	ITEM AR125444	SIZE 2, 5-STEP
W E	91-88 BLANK	5 0	ITEM AR125444	SIZE 2, 5-STEP
W E	BLANK B	0 2	ITEM AR125444	SIZE 2, 5-STEP
W E	● 9 BLANK	2 0	ITEM AR125444	SIZE 2, 5-STEP
N S	6-22 BLANK	4 0	ITEM AR125444	SIZE 2, 5-STEP
N S	6-22 BLANK	4 0	ITEM AR125444	SIZE 2, 5-STEP
	W E W E W E W E N S	SIDE LEGEND W <u>MN478</u> E 33-15 W <u>MN478</u> E 33-15 W <u>S1-15</u> W <u>S1-15</u> W <u>S1-75</u> W <u>S1-75</u> W <u>S1-75</u> W <u>S1-75</u> W <u>S1-75</u> W <u>S1-75</u> N <u>6-225</u> N <u>6-225</u>	SIDE LEGEND CHARACTERS W XNVTB 0 B 33-15 5 W XNVTB 0 B 33-15 5 W SI-EE 5 BLANK 0 0 W XNVTB 0 W SI-EE 5 BLANK 0 0 N 6-2Z 4 N 6-2Z 4	SIDE LEGEND CHARACTERS NUMBER W XINVTB 0 ITEM AR125444 W SI-5 5 ITEM AR125444 W SI-5 5 ITEM AR125444 W SI-5 0 ITEM AR125444 W MVTB 0 ITEM AR125444 W E 2 ITEM AR125444 N E-2Z 4 ITEM AR125444 N E-2Z 4 ITEM AR125444





5 DEGREES HORIZONTAL	AU075
TAL AIMING DETAIL	FINAL
'S INSTRUCTIONS. INSTALLATION DETAILS 40V CIRCUIT	
MING SCHEDULE R 4.B 1.87° 5° 1.87° 5° 1.87° 5° INSTALLATION NOTES INSTALLATION NOTES INSTALLATION NOTES INGLE UNIT SHOULD BE AIMED TOWARD TAXIWAY ERLINE AND IS SYMMETRICAL ABOUT THE TAXIWAY INGLE UNIT SHOULD BE AIMED UPWARD FROM THE ONTAL AND IS THE SAME FOR EACH UNIT IN EVERY ONTAL AND IS THE SAME FOR EACH UNIT IN EVERY OF RUNWAY GUARD LIGHTS. L) AND RIGHT (R) ARE DESIGNATED BY LOOKING RD THE HOLD SIDE OF THE LINE. LUMBER IS DESIGNATED ON PROPOSED LIGHTING JT AND RGL AND SIGN LOCATIONS SHEETS.	REPLACE AIRFIELD SIGNAGE, WIND CONES, RUNWAY 15 AND 27 RUNWAY END IDENTIFIER LIGHTS, AND AIRPORT ROTATING BEACON JUNE 13, 2025
	CITY OF LIGHTS

MARK DATE DESCRIPTION

SHEET TITLE

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IL PROJECT NO: CMT PROJECT NO: 25007812.00 CAD DWG FILE: DESIGNED BY: CMJ JRO DRAWN BY: DKP CHECKED BY APPROVED BY DKP COPYRIGHT

ELECTRICAL

DETAILS - 5

OF

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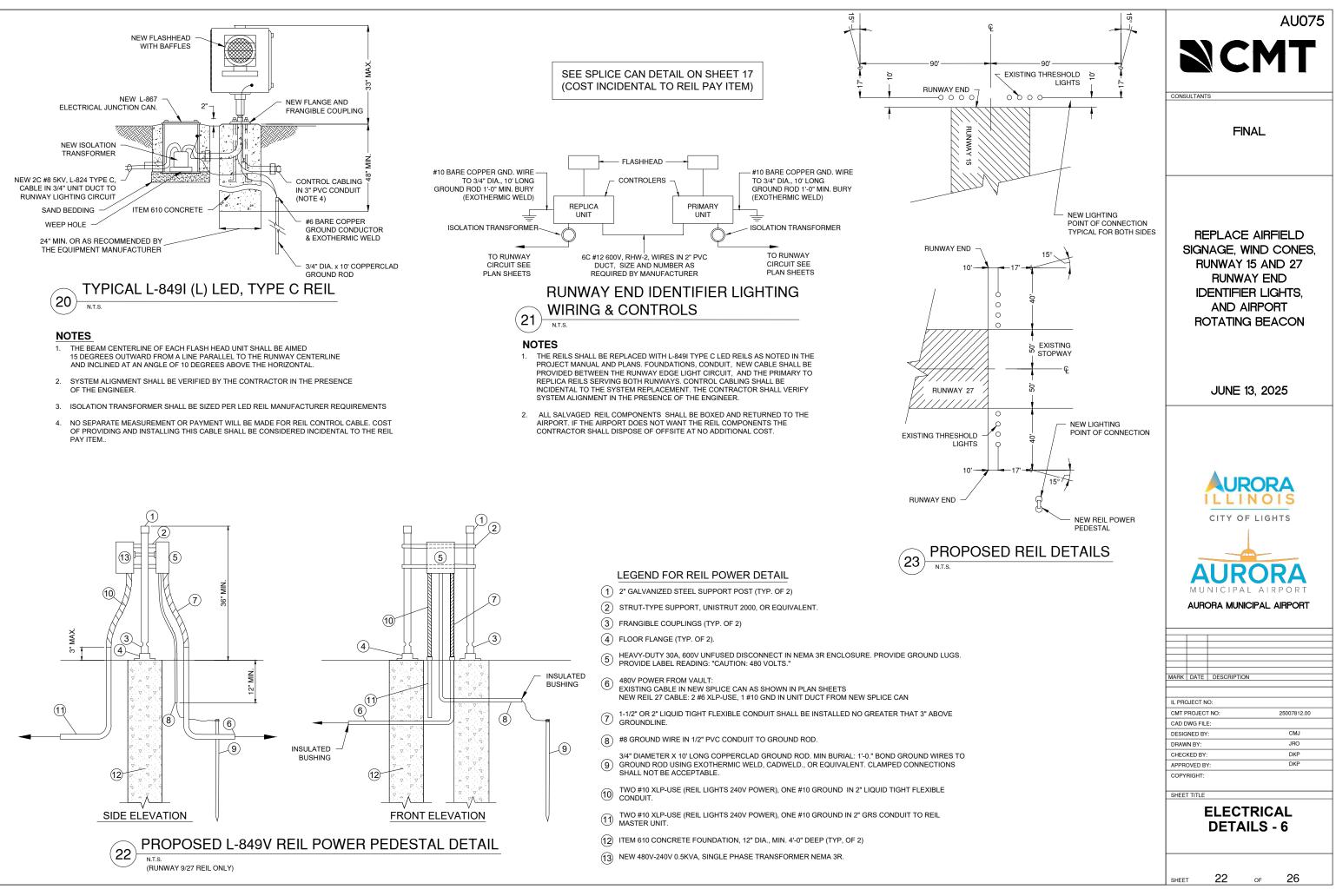
21

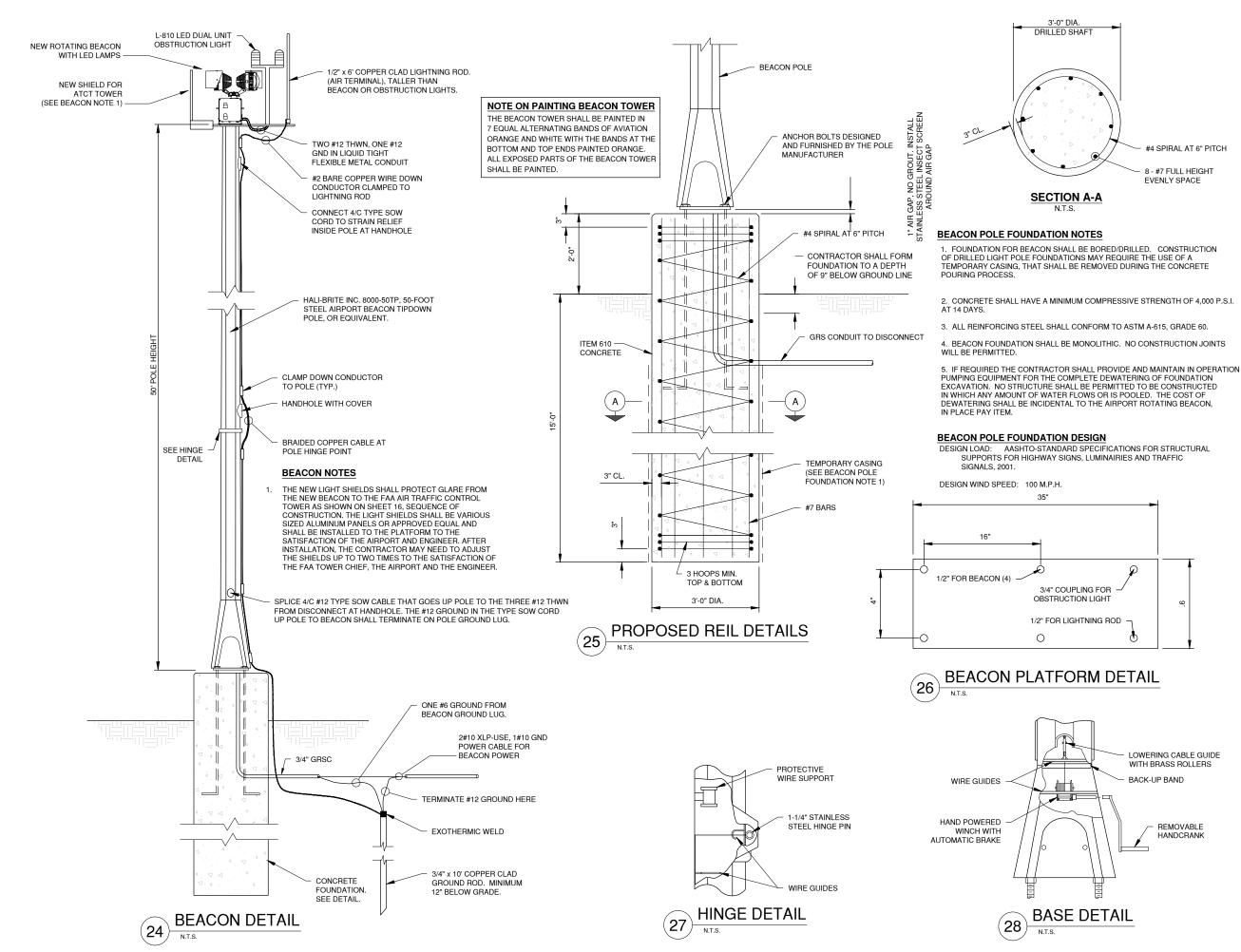
RGL LAYOUT NOTES

1. LOCATION OF DUCT BANK MAY VARY TO AVOID EXISTING

2. ANGLE 'A' - SEE AIMING SCHEDULE.

3. DISTANCE FROM HOLD LINE: -RGL PAIR 1 INSTALLATION SHALL BE OUT OF THE RSA BUT WITH IN 10' OF THE RUNWAY HOLD LINE -RGL PAIR 2 INSTALLATION SHALL BE IN LINE WITH THE HOLD SIGN BUT OUT OF THE RSA.





#4 SPIRAL AT 6" PITCH

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REPLACE AIRFIELD SIGNAGE, WIND CONES, RUNWAY 15 AND 27 RUNWAY END IDENTIFIER LIGHTS, AND AIRPORT **ROTATING BEACON**

JUNE 13, 2025





AURORA MUNICIPAL AIRPORT

MARK	DATE	DESCRIPTION

IL PROJECT NO:	
CMT PROJECT NO:	25007812.00
CAD DWG FILE:	
DESIGNED BY:	CMJ
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
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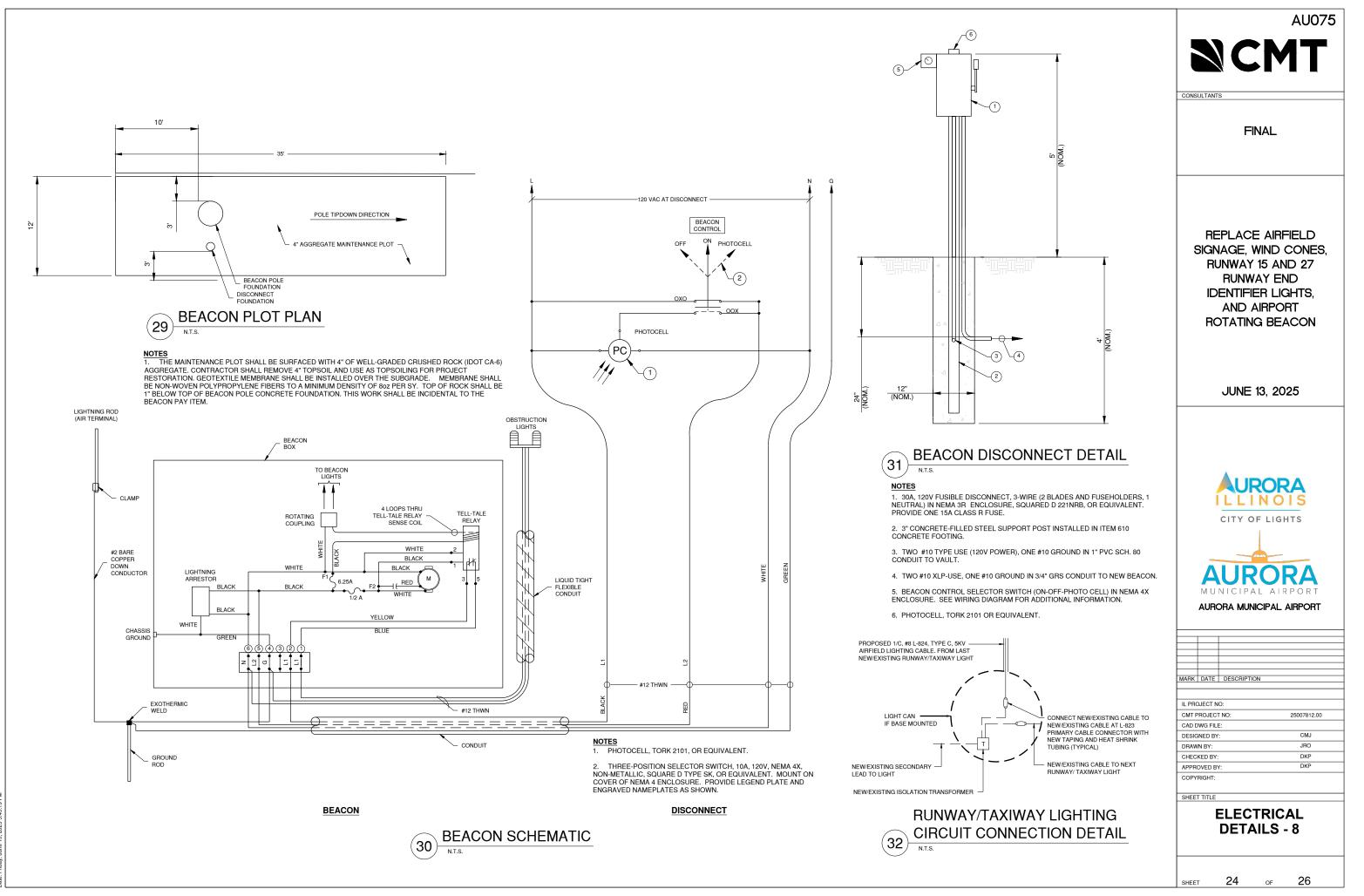
SHEET TITLE

SHEET

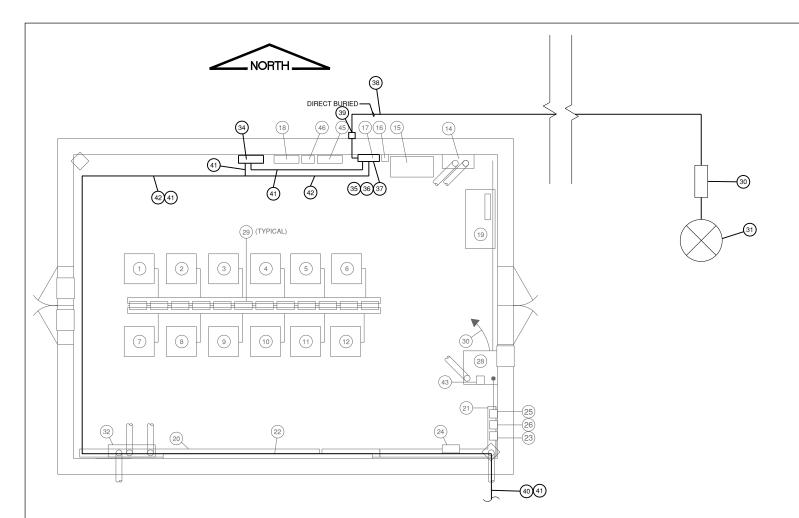


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Path: K:/kurora/pi/24006514-00_SignsRELLsBeacon/Draw/Sheets/2400651400 - electrical details - 8.dw Data: Friday, June 13, 2025 5:45:19 PM



EXISTING ELECTRICAL VAULT PLAN

NOT TO SCALE

NOTES

- 1. EXISTING EQUIPMENT TO REMAIN IS SHOWN FOR INFORMATION ONLY. NEW WORK IS SHOWN IN BOLD.
- 2. INSTALL LAMACOID NAMEPLATES ON ALL EQUIPMENT.

AURORA MUNICIPAL AIRPORT VAULT NOMENCLATURE

- 1. EXISTING 30KW 3-STEP REGULATOR FOR TAXIWAY "A" EAST.
- 2. EXISTING 30KW 3-STEP REGULATOR FOR TAXIWAY "A" WEST.
- 3. EXISTING 30KW 3-STEP REGULATOR FOR TAXIWAY "B".
- 4. EXISTING 20KW 3-STEP REGULATOR FOR TAXIWAY "C".
- 5. EXISTING 15KW 3-STEP REGULATOR FOR TAXIWAY "P"
- 6. EXISTING 30KW 3-STEP REGULATOR FOR TAXIWAY SPARE.
- 7. EXISTING 30KW 5-STEP REGULATOR FOR RUNWAY SPARE.
- 8. EXISTING 30KW 5-STEP REGULATOR FOR RUNWAY 15-33.
- 9. EXISTING 30KW 5-STEP REGULATOR FOR RUNWAY SPARE
- 10. EXISTING 30KW 5-STEP REGULATOR FOR RUNWAY 9-27.
- 11. EXISTING 7.5KW 3-STEP REGULATOR FOR RUNWAY DISTANCE REMAINING 9-27.
- 12. EXISTING 7.5KW 3-STEP REGULATOR FOR RUNWAY DISTANCE REMAINING 9-27
- SPARE. 13. NOT USED.
- 14. EXISTING HIGH VOLTAGE LIGHTING PANEL. 600A, 480/277V, 3 PHASE, 4-WIRE (NOTE 10)
- 15. EXISTING 150KVA 480-120/208Y, 3 PHASE LIGHTING TRANSFORMER.
- 16. EXISTING NORTH AIRFIELD BOOSTER TRANSFORMER / WIND-TEE BOOSTER TRANSFORMER
- 17. EXISTING LOW VOLTAGE LIGHTING PANEL. 225A, 120/208Y, 3 PHASE, 4 WIRE (NOTE 10).
- 18. EXISTING APRON LIGHTING PANEL, 225A, 480/277V, 3-PHASE, 4-WIRE (NOTE 10).
- 19. EXISTING DESK.
- 20. EXISTING 8" BY 8" HIGH VOLTAGE WIREWAY.
- 21. EXISTING 8" BY 8" LOW VOLTAGE WIREWAY.
- 22. EXISTING PLUG CUTOUT, GROUNDING SWITCH AND INDICATOR LIGHT FOR REGULATORS
- 23. EXISTING INTERFACE CONTROL PANEL FOR LOW VOLTAGE AIRFIELD CIRCUITS, BEACON, WINDTEE AND REILS.
- 24. EXISTING CONTACTORS FOR RUNWAY 27 REILS.
- 25. EXISTING L-854 RADIO CONTROLLER. INSTALL NEW ANTENNA AND CABLE
- 26. EXISTING FIBER OPTIC PATCH PANEL.
- 27. NOT USED.
- 28. EXISTING L-890 AIRFIELD LIGHTING CONTROL AND MONITORING SYSTEM (ALCMS)EQUIPMENT RACK WITH TOUCHSCREEN MONITOR AND PRINTER.
- 29. EXISTING INTERFACE CONTROL PANEL AND INSULATION RESISTANCE MEASURING SYSTEM MOUNTED ON UNISTRUT FRAME. (NOTE 9)
- 30. NEW BEACON PEDESTAL WITH DISCONNECT AND PHOTOCELL.
- 31. NEW ROTATING BEACON WITH LED LAMPS. SEE DETAILS
- 32. EXISTING L-823 SPLICE ENCLOSURE. SEE DETAILS.
- 33. EXISTING C.T. CABINET AND UTILITY METER.
- 34. NEW LIGHTING CONTRACTOR FOR RGL CIRCUIT WITH HOA SELECTOR SWITCH.
- 35. NEW 20A/2P CIRCUIT BREAKER FOR RGL CIRCUIT.
- 36. NEW 20A/1P CIRCUIT BREAKER FOR NEW BEACON
- 37. NEW 20A/1P CIRCUIT BREAKER FOR NEW L-807 WIND CONE
- 38. NEW 2#10 XLP-USE, 1#10 GND IN 1" PVC SCH. 80 CONDUIT FOR BEACON.
- 39. NEW NEMA 3R JUNCTION BOX 4" X 4"X 4" FOR BEACON CIRCUIT.
 - 40. NEW CIRCUITS FROM VAULT TO LOW VOLTAGE DUCT BANK -NEW RGL CIRCUIT, 2#6 XLP-USE, 1#10 GND -NEW WIND CONE CIRCUIT, 2#8 XLP-USE, 1#10 GND
 - 41. NEW RGL CIRCUIT, 2#6 XLP-USE, 1#10 GND.
 - 42. NEW WIND CONE CIRCUIT, 2#8 XLP-USE, 1#10 GND.
 - 43. EXISTING FIBER OPTIC CABLE PATCH PANEL (NOTE 8)
- 44. NOT USED.
- 45. EXISTING 200A, 480V, 3-POLE CIRCUIT BREAKER DISCONNECT FOR APRON LIGHTING.
- 46. EXISTING 200A, 3-POLE LIGHTING CONTACTOR WITH H-O-A SELECTOR SWITCH AND PHOTOCELL FOR APRON LIGHTING.

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REPLACE AIRFIELD SIGNAGE, WIND CONES, RUNWAY 15 AND 27 RUNWAY END IDENTIFIER LIGHTS, AND AIRPORT **ROTATING BEACON**

JUNE 13, 2025





AURORA MUNICIPAL AIRPORT

MARK	DATE	DESCRIPTION

IL PROJECT NO:	
CMT PROJECT NO:	25007812.00
CAD DWG FILE:	
DESIGNED BY:	CMJ
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
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						PA	NELBO	DAR	D SO	CHEDU	JLE						
		N: MDP-1 N: Electric E: Square I			BO		NEUT	RALI	BUS I	UND BAR: NO POL 5 RATING: 100% SHORT CIRCUIT RATING E RATED: NO SERIES OR FULLY RATED TVSS & DISCONNECT REQUIRED						65KAIC SERIES	
	PHA:	TS: 277/480V SE: 3 RE: 4				OUNTING: L RATING:		E			MAIN		BUS RATING	BUS	COPPER		
СКТ		BREAKER	LOAD	USAGE	PHASE	E AMPS (U	JSAGE)	PC	OLE	PHAS	E AMPS (U	JSAGE)	USAGE	LOAD	BREAKER		СКТ
10.	LOAD	SIZE	AMPS	FACTOR	А	В	C	N	ю.	А	В	С	FACTOR	AMPS	SIZE		NO.
1					0			1	2	0							2
3	150 KVA TRANSFORMER	225A/3P				0		3	4		0				30A/3P	HEATER EAST SIDE	4
5					0		0	5	6	0		0					6
9	HEATER WEST SIDE	30A/3P			U	0		9	10	U	0				100A/2P	RUNWAY 9-27	8
11	HEATER WEST SIDE					0	0	11	12		0	0					10
13					0			13	-	0					- 60A/3P	TAXIWAY C	14
15	TAXIWAY A WEST	100A/2P			-	0		15		-	0						16
17	SPARE TAXIWAY	100A/2P					0	17	18			0					18
19	SPARE IAAWAT	100A/2P			0			19	20	0					50A/3P	LIFT STATION 1	20
21	RDR 9-27	30A/2P				0		21			0						22
23							0	23				0	0		100A/2P	TAXIWAY B	24
25		200A/3P			0			25		0							26
27	APRON LIGHTING DISCONNECT	200A/3P				0	0	27 29	28 30		0	-			15A/2P	9/27 REILS	28
29 31					0		0	31	30	0		0					30
33	RUNWAY 15/33	100A/2P			0	0		33	-	0	0				- 50A/2P	TAXIWAY P	32
35		000.007					0	35				0	2		1004.05		36
37	SPARE 9/27 RDR	30A/2P		l í	0	1	1	37	38	0					- 100A/3P	SPARE RUNWAY	38
39	TAXIWAY A EAST	100A/2P				0		39	40		0				100A/3P	SPARE RUNWAY	40
41		1000/25					0	41				0			100-0101	SI ARE ROMAT	42
43		1001/0-			0			43		0					-		44
45	BP CORPORATE HANGER	100A/3P				0		45			0				50A/3P	LIFT STATION 2	46
47	050500 5050						0	47	48	-		0					48
	SECTION TOTAL:			L	0	0	0			0	<u>0</u> В	0 C	1		Г	TOTAL USAGE LOAD:	
PHASE TOTAL AMPS:									A 0			1			IOTAL USAGE LUAD.	0 VA	
						FIAO		GME O.	•	A	В	c c			L		0 VA
						РН	ASE TOTA		.	0	0	0	1				

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	PANELBOARD SCHEDULE													PANELBOARD SCHEDULE													
	PANEL DESIGNATION: LV LIGHTING PANEL BOND NEUTRAL AND GROUND BAR: NO POLE: 42 LOCATION: ELECTRICAL ROOM 125.B NEUTRAL BUS RATING: 100% SHORT CIRCUIT RATING: 30KAIC MFR & TYPE: SQUARE D NQ, OR EQUIV. SERVICE ENTRANCE RATED: NO SERIES OR FULLY RATED: SERIES TVSS & DISCONNECT REQUIRED: NO													PANEL DESIGNATION: APRON LIGHTING PANEL BOND NEUTRAL AND GROUND BAR: NO LOCATION: ELECTRICAL ROOM 125.B NEUTRAL BUS RATING: 100 MFR & TYPE: SQUARE D NQ, OR EQUIV. SERVICE ENTRANCE RATED: NO										SHO SERIE DISC			
		VOLTS: PHASE: WIRE:					NG: SURFAC NG: NEMA 1									VOLTS: 277/480V MOUNTING: SURFACE PHASE: 3 ENCL RATING: NEMA 1 WIRE: 4 N				MAIN CIF	BUS R						
-	СКТ		BREAKER	LOAD US	SAGE PHA	SE AMPS	S (USAGE)	POLE	PHAS	E AMPS (L	JSAGE)	USAGE LOAD BREAKER	R	СКТ	СКТ		BREAKER LOAD USAGE		PHASE AMPS (USAGE)		POLE		PHASE AMPS (USA				
	NO.	LOAD		AMPS FA		В	<u> </u>	NO.	A	В		FACTOR AMPS SIZE	LOAD	NO.	NO.	LOAD	SIZE AMPS FACTO		B		NO.	A					
-		RTH AND WEST RECEPTACLES			0			1 2	0				SOUTH AND EAST RECEPTACLES		1	LOAD	SIZE AINIPS FACIO				1 2						
-	3	INTERIOR LIGHTS	20A/1P			0		3 4		0		20A/1P	EXTERIOR LIGHTS	4	3	SOUTH APRON 4-8	40A/3P	0	0		3 4	-	0				
_	5	EXHAUST FAN	20A/1P			-	0	5 6			0	20A/1P	DISTANCE REG CONTROL	6	5	SOUTH APRON 4-6	407/37		U	0	5 6	-		0	4995		
-	7	SPARE	20A/1P		0		-	7 8	0			20A/1P	REILS 9/27 CONTROLS	8	7			0			7 8	12 State Laboration					
Ē	9	BEACON	20A/1P			0		9 10		0		20A/1P	GAS PUMP	10	9	SPARE	50A/3P	0	0		9 10		0	-	-		
_ L_	11	CEILOMETER	20A/1P			-	0	11 12		-	0	20A/1P	APRON LIGHTING CONTROL	12	11	OFARE	504/51		0	0	11 12			0			
ſ	13	L807 WIND CONE	20A/1P		0			13 14	0			20A/1P	SPARE	14	13			0			13 14		-				
4	15 /	EAST GATE	20A/1P	, –		0		15 16		0		20A/1P	ENTRANCE ROAD SIGN	16	15	SPARE	40A/3P	0	0		15 14		0				
	17	0.01.000				0	17 18			0		00405	18	17	017112				0	17 18			0	<u></u>			
_	19 SPARE	SPARE	JUA/2P	30A/2P	30A/2P		0			19 20	0			30A/2P	SPARE	20	19			0			19 20				
	21	SPARE	001/00			0		21 22		0		001/05		22	21	POLES 1,2,3	40A/2P		0		21 22		0				
	23	SPARE	30A/2P				0	23 24			0	30A/2P	SO GATE SO APRON	24	23	SPARE	20A/1P		0	0	23 24	_		0			
	25	NORTH AIRFIELD GATE	30A/2P		0			25 26	0			30A/2P	NO GATE SO APRON	26	25			0			25 26						
_	27	NORTH AIRFIELD GATE	3UA72P			0		27 28		0		30A/2P	NO GATE SO APRON	28		27 SPARE	40A/3P	-	0	-	27 28		0	-	1000		
	29	WEST APRON GATE	30A/2P				0	29 30			0	20A/1P	SPARE	30	29				Ū	0	29 30			0			
	31	WEST APRON GATE	JUAIZP		0			31 32	0			20A/1P	SPARE	32	31			0		t -	31 32	-					
	33	SPARE	20A/1P			0		33 34		0		20A/1P	SPARE	34	33	SPARE	30A/3P		0		33 34	_	0	-			
	35						0	35 36			0	20A/2P	RGLS RWY 33 TXY A	36	35					0	35 36			0			
	37	SPARE	40A/3P		0			37 38	0			204/2P	ROLD RWT 33 TAT A	38	37			0			37 38	The second second	-	Ť			
	39					0		39 40		0		30A/2P/	SPARE	40	39				0		39 40		0				
	41	ALCMS CONTROL PANEL	20A/1P				0	41 42			0	JURIZE/	SFARE	42	41					0	41 42			0			
		SEC	TION TOTAL:		0	0	0		0	0	0						SECTION TOTAL:	0	0	0		0	0	0			
									A	В	С	NOTE 1/	TOTAL USAGE LOAD:								1	A	B	Ċ			
						PHA	ASE TOTAL	AMPS:	0	0	0			0 VA					PHASE	E TOTAL A	MPS:	0	0	0			
									Α	В	C	_										A	В	С			
						F	PHASE TOT	FAL VA:	0	0	0								PH	ASE TOTA	L VA:	0	0	0			
N	IOTES:														NOTES:												
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NOTES

1. NEW CIRCUIT BREAKER AND NEW CIRCUIT

EXISTING PANELBOARD SCHEDULES AND CIRCUIT INFORMATION IS SHOWN FOR INFORMATION ONLY.

3. CONTRACTOR SHALL LOCK-OUT, TAG-OUT CIRCUITS WHEN PERFORMING ANY WORK.

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REPLACE AIRFIELD SIGNAGE, WIND CONES, RUNWAY 15 AND 27 RUNWAY END IDENTIFIER LIGHTS, AND AIRPORT ROTATING BEACON

JUNE 13, 2025



TING (AMPS): 225 BUS: COPPER R: AMP/POLE 200/3

)	USAGE	LOAD	BREAKER		CKT
	FACTOR	AMPS	SIZE		NO.
					2
			30A/3P	SOUTH APPRON 1,2,3	4
					6
					8
			50A/3P	SPARE	10
					12
					14
			40A/3P	SPARE	16
					18
					20
			30A/3P	SPARE	22
					24
					26
			30A/3P	S APRON POLES 9-10-11	28
					30
			15A/2P	4 HEAD PARK LOT LIGHT	32
			10/1/21	4 HEADT ANY EOT EIGHT	34
			20A/3P	BP HANGAR PARKING LOT	36
			20.00	Bi Hritol (China Contest	38
			20A/2P	SPARE	40
			20.021	017112	42
	1			TOTAL USAGE LOAD:	
				0	VA
	1				





AURORA MUNICIPAL AIRPORT

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			-								
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MARK	DATE	DESCRIPTION	_								
			_								
	IL PBQJECT NO:										

IL PROJECT NO:	
CMT PROJECT NO:	25007812.00
CAD DWG FILE:	
DESIGNED BY:	CMJ
DRAWN BY:	JRO
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PANELBOARD SCHEDULE

SHEET