

ITEM NO. 01A
IDOT LETTING AUGUST 1, 2025

CONSTRUCTION PLANS

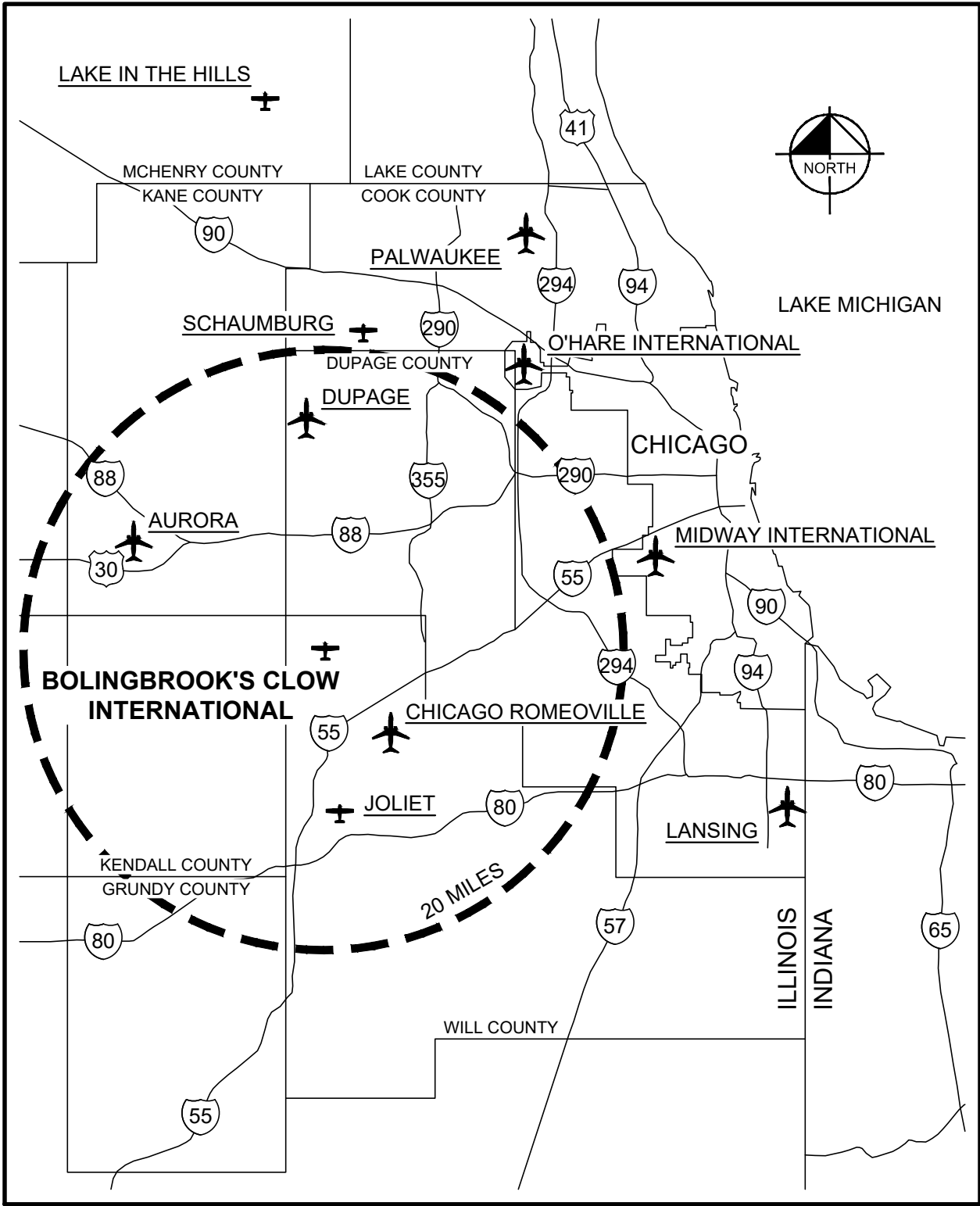
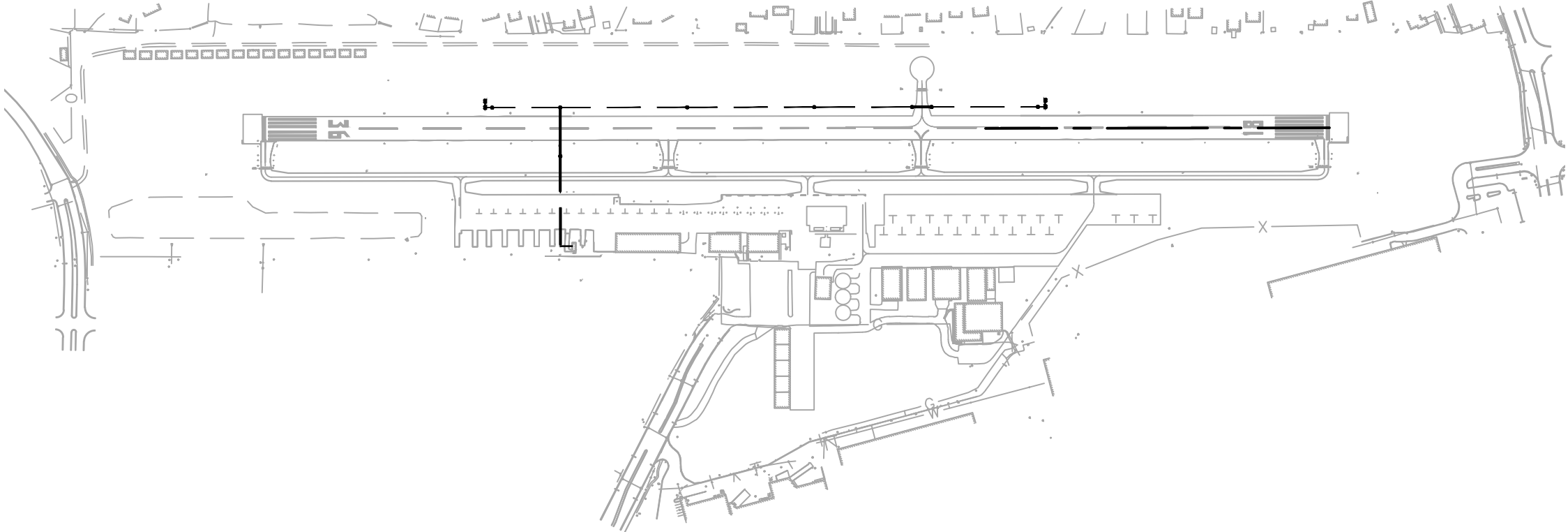
BO008

VILLAGE OF BOLINGBROOK
BOLINGBROOK'S CLOW INTERNATIONAL AIRPORT (1C5)
BOLINGBROOK, WILL COUNTY, ILLINOIS

INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
ILLINOIS PROJECT NO. 1C5-5132

CONTRACT NO. BO008
SBG PROJECT NUMBER: 3-17-SBGP-TBD


ISSUED FOR BID
06/06/2025



LOCATION MAP
NTS




4201 WINFIELD ROAD
SUITE 600
WARRENVILLE, IL 60555
(630) 487-5550



VILLAGE OF BOLINGBROOK
375 WEST BRIARCLIFF ROAD
BOLINGBROOK, ILLINOIS 60440
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Matt Eastman
MATTHEW EASTMAN
DIRECTOR OF COMMUNITY DEVELOPMENT


06/06/2025
DATE




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Joseph D. Bradshaw
SUBMITTED BY:
JOSEPH BRADSHAW, P.E. ELECTRICAL ENGINEER

06/06/2025
DATE






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Luke Burlingame
SUBMITTED BY:
LUKE BURLINGAME, P.E. PROJECT ENGINEER

06/06/2025
DATE





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|---------------------|------------|-------------------|--|
| DATE: 06/06/2025 | | SCALE: NTS | |
| DRAWN BY: LMB | | CHECKED BY: JY | |
| REV. | DATE | DESCRIPTION | |
| 0 | 06/06/2025 | ISSUED FOR BID | |

TITLE:
COVER SHEET
PROJECT:
INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS

| |
|------------------------------|
| CERTIFICATION: |
| DRAWING NO. GC-000 |
| PROJECT NUMBER: 168001006 |

K:\CHS Aviation\168001006-1C5 Install PAPI\Card Plans\168001006-CV Covering Burlington, Luke 6/9/2025 9:30 AM
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| SHEET INDEX | | |
|-------------|-----------|--|
| COUNT | SHEET NO. | SHEET NAME |
| 1 | GC-000 | COVER SHEET |
| 2 | GC-101 | SHEET INDEX, SOQ, LEGEND AND ABBREVIATIONS |
| 3 | GC-102 | ACCESS, SAFETY AND GENERAL NOTES |
| 4 | GC-103 | HORIZONTAL AND VERTICAL CONTROL PLAN |
| 5 | PH-100 | CONSTRUCTION SAFETY AND PHASING PLAN |
| 6 | PH-501 | CONSTRUCTION SAFETY AND PHASING DETAILS |
| 7 | E100 | ELECTRICAL SCOPE OF WORK |
| 8 | E101 | ELECTRICAL LAYOUT |
| 9 | E102 | ELECTRICAL LAYOUT |
| 10 | E103 | ELECTRICAL LAYOUT |
| 11 | E104 | ELECTRICAL LAYOUT |
| 12 | E201 | ELECTRICAL DETAILS |
| 13 | E202 | ELECTRICAL DETAILS |
| 14 | E203 | RUNWAY 18 PAPI SITING |
| 15 | E204 | RUNWAY 36 PAPI SITING |
| 16 | E205 | PAPI FOUNDATION AND INSTALLATION DETAILS |
| 17 | E301 | ELECTRICAL VAULT PLAN |
| 18 | E401 | ELECTRICAL CIRCUIT MAP - BASE BID |
| 19 | E402 | ELECTRICAL CIRCUIT MAP - ALT 1 |
| 20 | E501 | PAPI SITING ELEVATIONS |

| BASE BID | | | | | |
|----------|----------|---|-------------|----------|----------------|
| NO. | ITEM | DESCRIPTION | UNIT | QUANTITY | FIELD QUANTITY |
| 1 | AR108108 | 1/C #8 5 KV CABLE | LINEAR FOOT | 1565.0 | |
| 2 | AR110202 | 2" PVC DUCT, DIRECT BURY | LINEAR FOOT | 270.0 | |
| 3 | AR125615 | ABBREVIATED PAPI (L-881 SYSTEM) | EACH | 1 | |
| 4 | AR150520 | MOBILIZATION | LUMP SUM | 1 | |
| 5 | AR801714 | PROVIDE AND INSTALL NEW H-20 LOAD RATED CONCRETE HANDHOLE | EACH | 3 | |
| 6 | AR801717 | INSTALL NEW 240V, 6.6A, 5KW CONSTANT CURRENT REGULATOR IN VAULT | LUMP SUM | 1 | |
| 7 | AR801718 | 2-2" SCH. 40 PVC DIRECTIONAL BORE | LINEAR FOOT | 440.0 | |

| ALTERNATIVE BID 1 | | | | | |
|-------------------|----------|---|-------------|----------|----------------|
| NO. | ITEM | DESCRIPTION | UNIT | QUANTITY | FIELD QUANTITY |
| 1 | AS108108 | 1/C #8 5 KV CABLE | LINEAR FOOT | 3245.0 | |
| 2 | AS110202 | 2" PVC DUCT, DIRECT BURY | LINEAR FOOT | 1465.0 | |
| 3 | AS125615 | ABBREVIATED PAPI (L-881 SYSTEM) | EACH | 1 | |
| 4 | AS801714 | PROVIDE AND INSTALL NEW H-20 LOAD RATED CONCRETE HANDHOLE | EACH | 5 | |
| 5 | AS801718 | 2-2" SCH. 40 PVC DIRECTIONAL BORE | LINEAR FOOT | 65.0 | |

LEGEND

| | |
|-------------|-------------------------------|
| —ROFA— | RUNWAY OBJECT FREE AREA |
| —RSA— | RUNWAY SAFETY AREA |
| —TOFA— | TAXIWAY OBJECT FREE AREA |
| —TSA— | TAXIWAY SAFETY AREA |
| —ST— | PROPOSED STORM SEWER PIPE |
| —ST— | EXISTING STORM SEWER PIPE |
| —657— | EXISTING CONTOUR |
| —657— | PROPOSED CONTOUR |
| — — — | BITUMINOUS PAVEMENT SAWING |
| — — — | LIMITS OF GRADING |
| —SF— | SILT FENCE |
| —S— | EXISTING SANITARY SEWER LINE |
| —S— | PROPOSED SANITARY SEWER LINE |
| —T— | EXISTING TELEPHONE LINE |
| —WTR— | EXISTING WATER LINE |
| —W— | PROPOSED WATER LINE |
| —X— | EXISTING CHAIN LINK FENCE |
| —O— | EXISTING WROUGHT IRON FENCE |
| — COMM — | EXISTING COMMUNICATIONS CABLE |
| —COM— | PROPOSED COMMUNICATIONS CABLE |
| — — —G— — — | EXISTING NATURAL GAS LINE |
| —GAS— | PROPOSED NATURAL GAS SLEEVE |
| — — —E— — — | EXISTING POWER CONDUIT |
| —ELEC— | PROPOSED ELECTRICAL CONDUIT |
| — — — UD — | PROPOSED UNDERDRAIN |

| | |
|--|--|
| | FOUND SURVEY MONUMENT |
| | SPOT ELEVATION |
| | EXISTING ELECTRICAL HANDHOLE |
| | EXISTING ELECTRICAL MANHOLE |
| | EXISTING ELECTRICAL CABINET |
| | EXISTING ELECTRICAL DUCT MARKER |
| | EXISTING COMMUNICATIONS/TELEPHONE PEDESTAL |
| | EXISTING COMMUNICATIONS/TELEPHONE MANHOLE |
| | EXISTING NATURAL GAS VALVE |
| | EXISTING SANITARY SEWER MANHOLE |
| | EXISTING WATER MANHOLE |
| | EXISTING UTILITY |
| | EXISTING STORM SEWER INLET |
| | PROPOSED STORM SEWER INLET |
| | EXISTING AIRCRAFT TIE DOWN |
| | PROPOSED AIRCRAFT TIE DOWN |
| | EXISTING RETROREFLECTIVE MARKER |
| | PROPOSED RETROREFLECTIVE MARKER |
| | BORING LOCATION |
| | INLET PROTECTION |
| | SURVEY BENCHMARK |
| | CONTROL POINT |
| | EXISTING HYDRANT AND VALVE |
| | PROPOSED WATERMAIN STRUCTURE |

ABBREVIATIONS

| | | | |
|--------|--------------------------|------|------------------------------|
| AC | ADVISORY CIRCULAR | MIN | MINIMUM |
| AOA | AIR OPERATIONS AREA | MH | MANHOLE |
| APPROX | APPROXIMATELY | N | NORTH |
| BM | BENCHMARK | NE | NORTHEAST |
| CB | CATCH BASIN | NO | NUMBER |
| CL | CENTERLINE | NW | NORTHWEST |
| CP | CONTROL POINT | NTS | NOT TO SCALE |
| COMM | COMMUNICATIONS | OC | ON-CENTER |
| CY | CUBIC YARD | OFA | OBJECT FREE AREA |
| E | EAST | PC | POINT OF CURVATURE |
| EA | EACH | POC | POINT ON CURVE |
| EG | EXISTING GROUND | PR | PROPOSED |
| EL | ELEVATION | PT | POINT OF TANGENCY |
| EX | EXISTING | R | RADIUS |
| FAA | FEDERAL AVIATION | RCP | REINFORCED CONCRETE PIPE |
| | ADMINISTRATION | RSA | RUNWAY SAFETY AREA |
| FFE | FINISHED FLOOR ELEVATION | RT | RIGHT |
| FG | FINISHED GRADE | S | SOUTH |
| | FLOWLINE | SE | SOUTHEAST |
| FOD | FOREIGN OBJECT DEBRIS | SOQ | SUMMARY OF QUANTITIES |
| FT | FEET | STA | STATION |
| GAL | GALLON | SW | SOUTHWEST |
| GB | GRADE BREAK | SY | SQUARE YARD |
| IDOT | ILLINOIS DEPARTMENT OF | TOFA | TAXILANE/TAXIWAY OBJECT FREE |
| | TRANSPORTATION | | AREA |
| INV | INVERT | TSA | TAXILANE/TAXIWAY SAFETY AREA |
| LF | LINEAR FOOT | TW | TAXIWAY |
| LP | LOW POINT | TYP | TYPICAL |
| LS | LUMP SUM | UNO | UNLESS NOTED OTHERWISE |
| LT | LEFT | VAR | VARIES |
| MAX | MAXIMUM | W | WEST |



K:\CHS Aviation\168001006-105-Instal-PAPI\CoordPlans\168001006-CC-IndexNotes.dwg, Burlingame, Luke 6/9/2025 3:45 PM
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ACCESS AND SAFETY NOTES:

I. ACCESS AND VEHICLE PARKING:

- A. ACCESS POINT AND HAUL ROUTE FOR ALL CONTRACTOR PERSONNEL, VEHICLES AND EQUIPMENT ARE AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN PERMITS AND COORDINATE OFF SITE HAUL ROUTES WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. HAUL ROUTES ARE TO REMAIN CLEAN AND PASSABLE AT ALL TIMES.
- B. CONTRACTOR SHALL FURNISH AND INSTALL A PROFESSIONALLY PAINTED SIGN MEETING OWNER APPROVAL AT THE CONSTRUCTION ENTRANCE TO DIRECT MATERIAL SUPPLIERS AND EMPLOYEES TO THE CONSTRUCTION SITE. THE SIGN SHALL READ "CONSTRUCTION VEHICLES ONLY - NO VENDORS ALLOWED".
- C. ALL CONTRACTOR VEHICLES AND PERSONNEL SHALL REMAIN WITHIN THE DESIGNATED CONSTRUCTION LIMITS OR HAUL ROUTES. NO CONTRACTOR VEHICLES WILL BE ALLOWED ON OTHER AIRFIELD OPERATIONS AREAS.
- D. THE LIMITS OF CONSTRUCTION, MATERIAL STORAGE AREAS, EQUIPMENT STORAGE AREA, PARKING AREA AND OTHER AREAS DEFINED AS REQUIRED FOR THE CONTRACTOR'S EXCLUSIVE USE DURING CONSTRUCTION SHALL BE MARKED BY THE CONTRACTOR. TEMPORARY BARRICADES, FLAGGING AND FLASHING WARNING LIGHTS WILL BE REQUIRED AT CRITICAL ACCESS POINTS. TYPE OF MARKING AND WARNING DEVICES SHALL BE APPROVED BY THE ENGINEER AND AIRPORT MANAGER.
- E. ALL ACCESS GATES SHALL BE KEPT CLOSED DURING CONTRACTOR'S WORK HOURS. THE CONTRACTOR SHALL CLOSE AND LOCK ACCESS GATE AT THE COMPLETION OF EACH WORK DAY. AIRPORT MANAGER WILL PROVIDE LOCK AND KEYS FOR ACCESS GATE.
- F. AT THE COMPLETION OF EACH WORKDAY OR WHEN NOT IN USE, CONSTRUCTION EQUIPMENT SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA AND/OR DESIGNATED EQUIPMENT STAGING AREAS. CLOSED AREAS OF APRON PAVEMENT MAY BE USED FOR TEMPORARY STAGING OF EQUIPMENT WITH THE APPROVAL OF THE AIRPORT MANAGER.

II. COORDINATION AND COMMUNICATION DURING CONSTRUCTION:

- A. CONTRACTOR SHALL ADVISE AND THEN VERIFY WITH RESIDENT ENGINEER 72 HOURS IN ADVANCE AS TO THE NEED FOR NOTICES TO AIR MISSIONS (NOTAMS). NO RUNWAY, TAXIWAY OR APRON SHALL BE CLOSED WITHOUT APPROVAL FROM THE AIRPORT MANAGER AND THE RESIDENT ENGINEER.
- B. CONTRACTOR SHALL APPOINT A CONSTRUCTION SUPERINTENDENT, SUBJECT TO THE APPROVAL OF THE AIRPORT MANAGER, WHO SHALL BE PRESENT ON THE CONSTRUCTION SITE AT ALL TIMES DURING WORKING HOURS WHILE JOB IS IN PROGRESS. THE SUPERINTENDENT SHALL BE DESIGNATED THE RESPONSIBLE CONTRACTOR'S REPRESENTATIVE WHO SHALL BE AVAILABLE ON A 24-HOUR BASIS.
- C. PRIOR TO CONSTRUCTION IN THE RESPECTIVE WORK AREAS, THE CONTRACTOR SHALL CORDON OFF THE WORK AREA AS SHOWN ON DRAWINGS PH-101 THROUGH PH-103 BY USING LOW PROFILE BARRICADES. CONTRACTOR SHALL MAINTAIN, MOVE AND REMOVE BARRICADES (AS DIRECTED BY THE ENGINEER) TO DELINEATE AREAS CLOSED TO AIRCRAFT TRAFFIC.
- D. PRIOR TO COMMENCEMENT OF CONSTRUCTION, CONTRACTOR SHALL SUBMIT TO AIRPORT MANAGER THE NAME AND TELEPHONE NUMBER OF THE INDIVIDUAL(S) WHO WILL BE ON CALL 24 HOURS PER DAY FOR EMERGENCY MAINTENANCE OF AIRPORT HAZARD LIGHTING & BARRICADES.
- E. THE CONTRACTOR SHALL HAVE ON-SITE AT ALL TIMES DURING WORKING HOURS, AN AIRPORT RADIO TO CONTINUOUSLY MONITOR AIRCRAFT TRAFFIC ON AND AROUND THE AIRPORT. THE COMMON TRAFFIC ADVISORY FREQUENCY IS 122.90. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO MONITOR AND GIVE WAY TO ALL AIRCRAFT TRAFFIC.

III. TRAFFIC CONTROL:

- A. ALL CONSTRUCTION EQUIPMENT AND VEHICLES SHALL BE MARKED WITH COMPANY PAINTED DESIGNS, INSIGNIAS OR OTHER COMPANY MARKINGS, WHICH ARE CLEARLY VISIBLE. CONTRACTOR SHALL PROVIDE EACH VEHICLE WITH A FLAG ON A STAFF ATTACHED TO THE VEHICLE SO THAT THE FLAG WILL BE READILY VISIBLE. THE FLAG SHOULD BE AT LEAST 3 FOOT SQUARE, AND HAVE A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE. ALL VEHICLES ON THE AIRFIELD SHALL BE LIT WITH A BEACON AND FLASHERS.
- B. CONSTRUCTION EQUIPMENT SHALL HAVE AUTOMATIC SIGNALING DEVICES TO SOUND AN ALARM WHEN MOVING IN REVERSE.
- C. ONLY RUBBER TIRED VEHICLES WILL BE ALLOWED ON PAVEMENT.
- D. NO PEDESTRIAN TRAFFIC OUTSIDE THE CONSTRUCTION WORK SITE WILL BE PERMITTED.
- E. ANY DAMAGE TO AIRPORT ROADS, AIRFIELD PAVEMENT, PARKING AREAS, STORAGE AREAS OR TURF AREAS DUE TO CONSTRUCTION EQUIPMENT OR CONSTRUCTION TRAFFIC SHALL BE REPAIRED TO ORIGINAL CONDITION TO THE SATISFACTION OF, AND AT NO EXPENSE TO THE OWNER.
- F. ANY WORKERS AND/OR EQUIPMENT NOT IN COMPLIANCE WITH CONSTRUCTION SAFETY AND PHASING PLAN SHALL IMMEDIATELY BE REMOVED FROM THE WORK AREA AND THEY WILL NOT BE PERMITTED TO WORK ON THE SITE THROUGHOUT THE REMAINDER OF THE PROJECT.

IV. EQUIPMENT AND STOCKPILE HEIGHT:

- A. THE MAXIMUM ALLOWABLE HEIGHT OF CONSTRUCTION EQUIPMENT OPERATING IN THE AOA SHALL BE IN CONFORMANCE WITH THE FAA FORM 7460 DEVELOPED FOR THIS PROJECT. EQUIPMENT MUST NOT PENETRATE THE ROFA OF ANY ACTIVE RUNWAY, OR TOFA OF ANY ACTIVE TAXIWAY, AS DEFINED IN FAA AC 150/5300-13, (CURRENT EDITION). ANY DEVIATIONS FROM THE APPROVED FAA FORM 7460 MUST BE APPROVED BY THE AIRPORT AND RESUBMITTED TO THE FAA. ADDITIONAL FAA SUBMITTAL SHALL HAVE NO IMPACT ON THE CONSTRUCTION SCHEDULE.
- B. CONTRACTOR SHALL NOT STOCKPILE CONSTRUCTION MATERIALS, SPOILS, DEBRIS OR REFUSE IN ANY AREA WHERE SUCH STOCKPILES WOULD PENETRATE APPROACH SURFACES. STOCKPILING OF MATERIAL WITHIN THE SAFETY AREA AND THE OBJECT FREE AREA OF AN ACTIVE RUNWAY OR TAXIWAY IS PROHIBITED.
- C. ALL STOCKPILES ADJACENT TO ACTIVE RUNWAY SAFETY AREAS OR OBSTACLE FREE ZONES SHALL BE IDENTIFIED WITH AN ORANGE & WHITE CONSTRUCTION FLAG AND A 360 DEGREE RED OBSTRUCTION LIGHT PLACED AT THE HIGHEST POINT OF THE STOCKPILE.
- D. STOCKPILED MATERIAL SHALL BE CONTAINED IN A MANNER TO PREVENT MOVEMENT AND/OR DUST RESULTING FROM AIRCRAFT PROPELLER WASH OR WIND CONDITIONS. IN ADDITION, STOCKPILED MATERIAL SHALL HAVE SILT FENCE PLACED AROUND IT TO PREVENT FOD FROM MOVING ONTO THE AIRFIELD PAVEMENTS OR POLLUTING WATERCOURSES.

V. EXCAVATION AND TRENCHES:

- A. ALL BARRICADES LOCATED WITHIN OR ADJACENT TO AREAS OF AIRCRAFT OPERATION SHALL BE LOW PROFILE TYPE AS DETAILED ON SHEET PH-501.
- B. OPEN TRENCHES AND EXCAVATIONS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH ORANGE BARRICADES WITH FLASHING RED LIGHTS ACCEPTABLE TO THE AIRPORT MANAGER.
- C. ALL CONSTRUCTION AREAS THAT POSE A HAZARD TO OPERATING AIRCRAFT SHALL BE PROPERLY MARKED WITH LOW-PROFILE BARRICADES.
- D. LOW PROFILE BARRICADES SHALL BE KEPT OUTSIDE THE LIMITS OF ANY ACTIVE RUNWAY SAFETY AREA OR TAXIWAY OBJECT FREE AREA.

VI. OTHER SAFETY REQUIREMENTS:

- A. CONTRACTOR SHALL MAINTAIN SAFETY PRACTICES THAT CONFORM TO OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) REGULATIONS.
- B. WASTE AND LOOSE MATERIALS CAPABLE OF CAUSING DAMAGE TO AIRCRAFT SHALL NOT BE LEFT WITHIN ACTIVE AIRCRAFT MOVEMENT AREAS. MATERIAL DROPPED WITHIN THESE AREAS SHALL BE CLEANED UP IMMEDIATELY TO THE SATISFACTION OF THE AIRPORT MANAGER OR THE ENGINEER.
- C. ALL TRASH RECEPTACLES SHALL BE PROPERLY COVERED TO PREVENT FOD.
- D. THE CONTRACTOR SHALL CONTROL BLOWING DUST FROM ANY HAUL ROUTE OR WORK AREA REGARDLESS OF SOURCE.
- E. THE CONTRACTOR SHALL BE REQUIRED TO USE A WATER TRUCK AND A SWEEPER DURING ALL CONSTRUCTION ACTIVITIES AND AS DIRECTED BY THE AIRPORT MANAGER. COST FOR THE DUST CONTROL AND SWEEPING WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- F. THE CONTRACTOR MUST ATTEND A MANDATORY SAFETY TRAINING MEETING LED BY THE AIRPORT STAFF PRIOR TO THE START OF CONSTRUCTION.
- G. PRIOR TO THE COMPLETION OF EACH WORK DAY, ALL AIRFIELD PAVEMENT SURFACES WITHIN AND ADJACENT TO THE WORK AREA AND THOSE USED AS HAUL ROUTES SHALL BE SWEEP CLEAN.

VII. CONTRACTOR SAFETY PLAN SUBMITTALS:

- F. CONTRACTOR SHALL FURNISH A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) THAT PROVIDES DETAILED INFORMATION REGARDING COMPLIANCE WITH THE PROJECT'S CONSTRUCTION SAFETY AND PHASING PLAN IN CONJUNCTION WITH HIS/HER PROJECT SCHEDULE. CONTRACTOR SHALL PROVIDE SPCD 10 DAYS PRIOR TO THE PRE-CONSTRUCTION MEETING. INFORMATION SHALL INCLUDE BUT NOT BE LIMITED TO THE FOLLOWING ITEMS:
 - 1. PROPOSED ACCESS POINTS, STAGING AREA AND HAUL ROUTES.
 - 2. ANY TEMPORARY MARKINGS TO BE USED.
 - 3. THE LOCATION AND TYPE OF BARRICADES OR OTHER TRAFFIC CONTROL DEVICES.
 - 4. AREAS TO WHICH CONSTRUCTION OPERATIONS WILL BE LIMITED, ALONG WITH PROPOSED MEASURES TO MINIMIZE THE POSSIBILITY OF CONTRACTOR EQUIPMENT AND/OR WORKERS ACCIDENTALLY ENTERING ANY AOA.
 - 5. ANY REQUESTED TEMPORARY AMENDMENTS TO THE AIRPORT SECURITY PLAN FOR SPECIAL CONDITION OR MODIFICATIONS.
- B. CONTRACTOR SPCD SHALL COMPLY WITH ALL REQUIREMENTS INDICATED IN THESE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS, AND FAA ADVISORY CIRCULAR (AC) 150/5370-2, OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION, (CURRENT EDITION).

GENERAL NOTES:

- 1. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO SUBMIT, OBTAIN AND PAY FOR ALL APPLICABLE PERMITS, FEES AND LICENSES FOR CONSTRUCTION AND EQUIPMENT.
- 2. TRAFFIC MAINTENANCE, INCLUDING BUT NOT LIMITED TO, BARRICADES, TAXIWAY CLOSURE MARKERS, ETC SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING CONSTANT COORDINATION BETWEEN THE SUBCONTRACTORS AND THE ENGINEER. ALL CONSTRUCTION ACTIVITIES PLANNED BY THE CONTRACTOR SHALL BE REVIEWED AND APPROVED BY THE ENGINEER AND OWNER.
- 4. LIMITS OF WORK SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR AND APPROVED BY THE ENGINEER PRIOR TO BEGINNING WORK.
- 5. THERE SHALL BE NO PERMANENT WASTE SITES ON AIRPORT PROPERTY. ANY TEMPORARY WASTE AREA WILL BE APPROVED BY THE ENGINEER AND SHALL BE KEPT IN A NEAT CONDITION. THE OWNER MAY DIRECT THE CONTRACTOR TO REMOVE WASTE IF NOT PROPERLY MAINTAINED.
- 6. PRIOR TO COMPLETION OF THE PROJECT THE CONTRACTOR SHALL RESTORE ALL DAMAGED AREAS (NOT DESIGNATED FOR CONSTRUCTION) TO THEIR PRE-CONSTRUCTION CONDITION.
- 7. LOCATING, FLAGGING, AND PROTECTING ALL EXISTING UTILITIES DURING THE DURATION OF THE CONSTRUCTION PROJECT IS THE RESPONSIBILITY OF THE CONTRACTOR. THE LOCATIONS OF THE EXISTING UTILITIES ON THE CONTRACT DRAWINGS WERE OBTAINED FROM EXISTING RECORDS AND NO GUARANTEE OF ACCURACY OR SUFFICIENCY FOR THIS INFORMATION IS ASSUMED BY THE OWNER OR ENGINEER. ANY DAMAGE TO ANY EXISTING UTILITIES WILL BE REPAIRED OR REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF AND AT NO EXPENSE TO THE AIRPORT MANAGER.
- 8. THE CONSTRUCTION COVERED BY THE PLANS SHALL CONFORM TO ALL APPLICABLE STANDARDS AND SPECIFICATIONS OF THE FAA AND IDOT.
- 9. ALL WORKMANSHIP AND MATERIALS SHALL BE SUBJECT TO INSPECTION AND APPROVAL OF THE ENGINEER.
- 10. CONTRACTOR SHALL EXAMINE THE FINISHED SURFACE, GRADES, TOPSOIL QUALITY, AND TOPSOIL DEPTH PRIOR TO LANDSCAPING. NO WORK SHALL BE CONDUCTED UNTIL UNSATISFACTORY CONDITIONS HAVE BEEN CORRECTED. CONTRACTOR SHALL VERIFY LIMITS OF WORK WITH THE ENGINEER BEFORE STARTING.
- 11. CONTRACTOR SHALL IMPORT AND/OR PROVIDE SALVAGED TOPSOIL MEETING THE REQUIREMENTS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION SPECIFICATIONS.
- 12. THE CONTRACTOR SHALL FURNISH AND PLACE A MINIMUM OF 4.00 INCHES OF TOPSOIL, WITH THE LOWER PORTION BLENDED INTO EXISTING SOILS.
- 13. PROPOSED ELEVATIONS INDICATED ARE FOR TOP OF FINAL GRADE, PAVEMENT, OR STRUCTURE UNLESS INDICATED OTHERWISE.
- 14. ELEVATIONS DENOTED AS "MATCH" ARE INTENDED TO MEET EXISTING GRADE ELEVATIONS. CONTRACTOR SHALL VERIFY ELEVATIONS AT TIE-INS AND MATCH POINTS PRIOR TO BEGINNING CONSTRUCTION AND VERIFY WITH THE ENGINEER.
- 15. ADEQUATE DRAINAGE SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. ANY DRAINAGE FEATURE OR STRUCTURE DISTURBED DURING CONSTRUCTION SHALL BE RESTORED TO EXISTING CONDITIONS OR BETTER SUBJECT TO THE APPROVAL OF THE AIRPORT MANAGER. ANY REWORK OR ADDITIONAL WORK REQUIRED BECAUSE OF THE CONTRACTOR'S FAILURE TO MAINTAIN ADEQUATE DRAINAGE WILL BE AT THE CONTRACTOR'S EXPENSE.
- 16. THE CONTRACTOR SHALL REMOVE STANDING WATER FROM THE PROJECT WORK LIMITS AS NECESSARY TO PROTECT SUBGRADE, SUBBASE, AND/OR BASE COURSE TO NEW PAVEMENT, SURROUNDING PAVEMENT-TO-REMAIN, OR OTHER COMPLETED WORKS.
- 17. CONTRACTOR IS RESPONSIBLE FOR COORDINATING AND PAYING ALL UTILITY CONNECTION FEES AND OBTAINING ALL PERMITS REQUIRED FOR THE UTILITY CONSTRUCTION AND CONNECTIONS. THIS INCLUDES ALL FEES ASSOCIATED WITH CONNECTION CHARGES AND METERS.
- 18. THE CONTRACTOR SHALL NOTE THAT OTHER CONTRACTORS MAY BE WORKING IN THE VICINITY OF THE PROJECT AND MAY REQUIRE ACCESS TO THEIR WORK AREAS. THE CONTRACTOR SHALL NOT DIRECTLY COMMUNICATE WITH THE CONTRACTORS ON OTHER PROJECTS UNLESS AUTHORIZED TO DO SO. COOPERATION OF THE CONTRACTOR WITH ADJACENT PROJECTS IS EXPECTED.



4201 WINFIELD ROAD
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| DATE: 06/06/2025 | | SCALE: NTS | |
| DRAWN BY: LMB | | CHECKED BY: JY | |
| REV. | DATE | DESCRIPTION | |
| 0 | 06/06/2025 | ISSUED FOR BID | |

TITLE:
ACCESS, SAFETY, AND GENERAL NOTES
PROJECT:
INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS

CERTIFICATION:

Luke Burlingame
06/06/2025

DRAWING NO.

GC-102

PROJECT NUMBER:

168001006



LEGEND



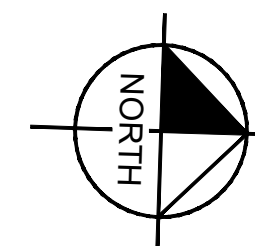
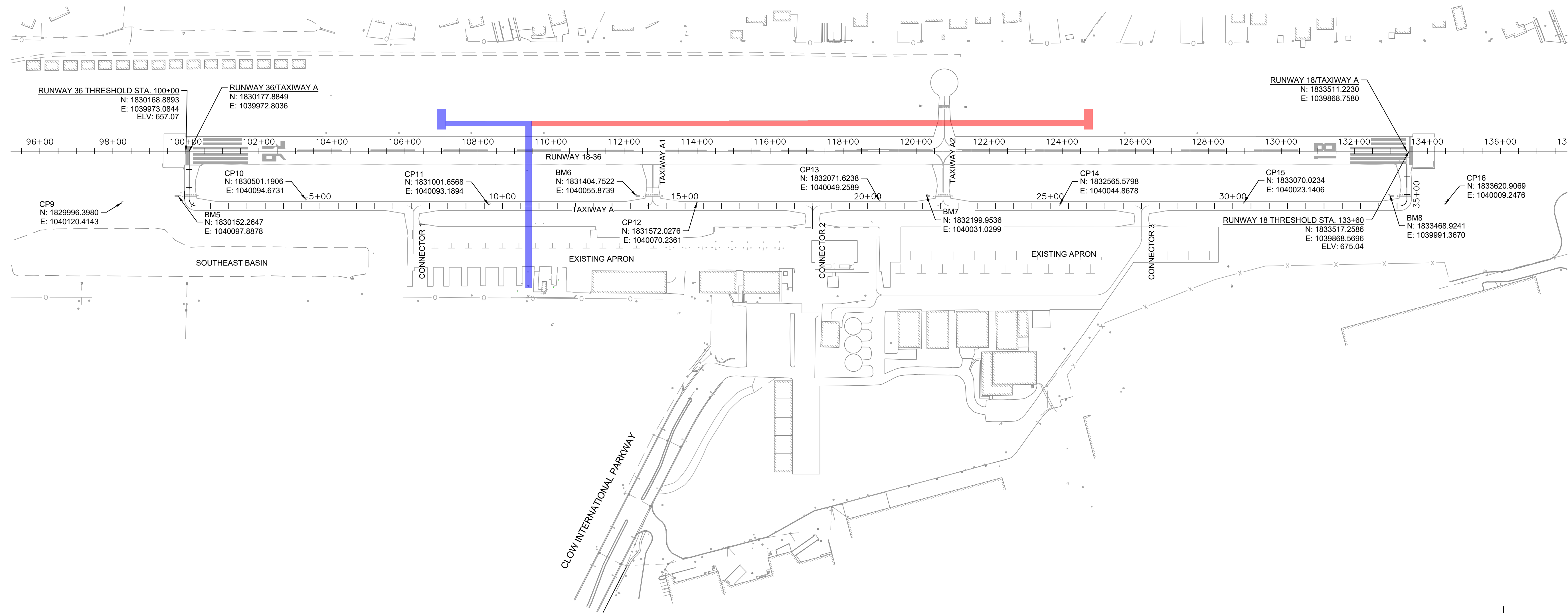
PROJECT AREA - BASE BID

PROJECT AREA - ALTERNATE BID

SURVEY CONTROL POINT OR BENCHMARK

NOTES

1. THE SURVEY WAS BASED ON THE NORTH AMERICAN DATUM OF 1983 (NAD83) HORIZONTALLY AND THE NORTH AMERICAN VERTICAL DATUM (NAV83) VERTICALLY. COORDINATES WERE GENERATED IN ILLINOIS STATE PLANE, EAST ZONE IN UNITS OF U.S. SURVEY FOOT.
2. THE CONTRACTOR IS RESPONSIBLE FOR ALL CONSTRUCTION SURVEYING. ANY DEVIATIONS FROM THE EXISTING GRADES AS SHOWN ON THE PLANS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE ENGINEER THEIR EXISTING GROUND SURVEY PRIOR TO STARTING THE EARTHWORK.



GRAPHIC SCALE IN FEET

0 75 150 300

A horizontal scale bar with four segments. The first segment (0 to 75) is white, the second (75 to 150) is black, the third (150 to 225) is white, and the fourth (225 to 300) is black. The segments are labeled 0, 75, 150, and 300 at the top.

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| REV. | DATE | DESCRIPTION | |
| 0 | 06/06/2025 | ISSUED FOR BID | |

TITLE: HORIZONTAL AND VERTICAL CONTROL PLAN

PROJECT: INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS

CERTIFICATION:



Julie Burlingame 06/06/2025

DRAWING NO.

GC-103

PROJECT NUMBER:

168001006

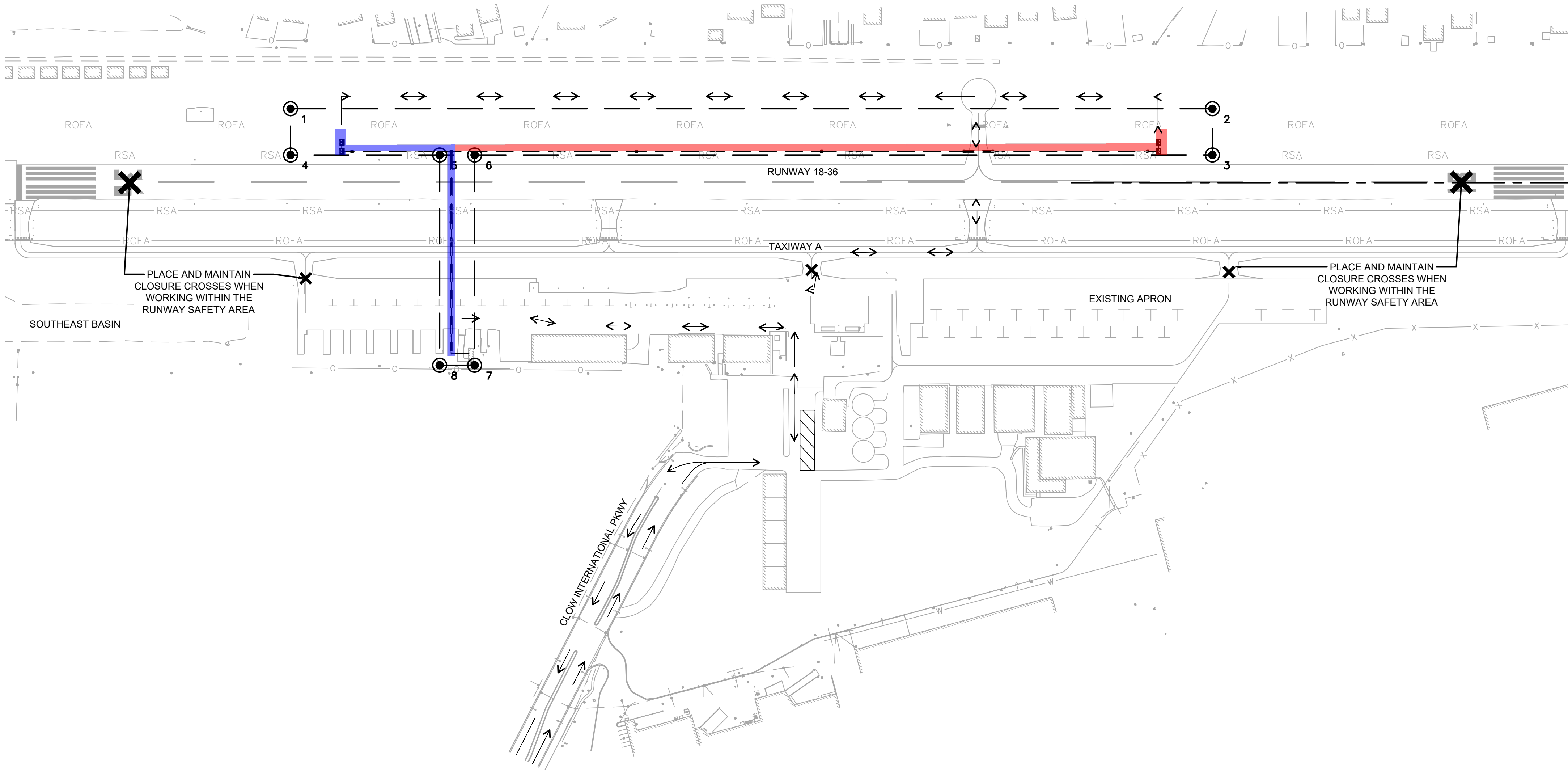
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AIRSPACE CRITICAL LOCATIONS (7460)

| POINT # | DESCRIPTION | NORTHING | EASTING | MAX STRUCTURE HEIGHT | ELEVATION | OVERALL OBJECT HEIGHT | LATITUDE | LONGITUDE | AERONAUTICAL STUDY NUMBER |
|---------|----------------------|------------|------------|----------------------|-----------|-----------------------|-----------------|-----------------|---------------------------|
| 1 | CONTRACTOR EQUIPMENT | 1830754.88 | 1039794.87 | 25' | 651' | 676' | N41° 41' 34.49" | W88° 07' 47.85" | 2025-AGL-8342-NRA |
| 2 | CONTRACTOR EQUIPMENT | 1832744.43 | 1039732.85 | 25' | 665' | 690' | N41° 41' 54.14" | W88° 07' 48.61" | 2025-AGL-8342-NRA |
| 3 | CONTRACTOR EQUIPMENT | 1832747.55 | 1039832.72 | 25' | 668' | 693' | N41° 41' 54.17" | W88° 07' 47.29" | 2025-AGL-8342-NRA |
| 4 | CONTRACTOR EQUIPMENT | 1830757.99 | 1039894.82 | 25' | 659' | 684' | N41° 41' 34.52" | W88° 07' 46.53" | 2025-AGL-8342-NRA |
| 5 | CONTRACTOR EQUIPMENT | 1831079.75 | 1039884.78 | 25' | 660' | 685' | N41° 41' 37.69" | W88° 07' 46.66" | 2025-AGL-8342-NRA |
| 6 | CONTRACTOR EQUIPMENT | 1831155.24 | 1039882.43 | 25' | 660' | 685' | N41° 41' 38.44" | W88° 07' 46.68" | 2025-AGL-8342-NRA |
| 7 | CONTRACTOR EQUIPMENT | 1831169.37 | 1040335.86 | 25' | 659' | 684' | N41° 41' 38.57" | W88° 07' 40.71" | 2025-AGL-8342-NRA |
| 8 | CONTRACTOR EQUIPMENT | 1831093.89 | 1040338.21 | 25' | 658' | 683' | N41° 41' 37.82" | W88° 07' 40.68" | 2025-AGL-8342-NRA |

| PHASE | DURATION (WORKING DAYS)* | AIRFIELD CLOSURES | WORK ELEMENTS** | WORK RESTRICTIONS |
|-------|--------------------------|--|--|--|
| 1 | 20 | 1.TAXIWAY A SOUTH OF TAXIWAY A1 | 1. INSTALLATION OF CABLE IN UNIT DUCT. 2. ELECTRICAL VAULT MODIFICATIONS. 3. INSTALLATION OF NEW L-881 PAPI. | 1. RUNWAY 18-36 SHALL BE CLOSED ANYTIME WORK ENTERS THE RUNWAY SAFETY AREA. 2. ALL EQUIPMENT MUST BE LOWERED WHEN NOT IN USE AND MAY NOT BE LEFT INSIDE OF THE RUNWAY OBJECT FREE AREA. |
| 2 | 10 | 1. RUNWAY 18/36 WHEN WORKING INSIDE THE RUNWAY SAFETY AREA | 1. INSTALLATION OF NEW L-881 PAPI. 2. INSTALLATION OF CABLES, CABLES IN UNIT DUCT, AND ASSOCIATED MATERIALS. | 1. RUNWAY 18-36 SHALL BE CLOSED ANYTIME WORK ENTERS THE RUNWAY SAFETY AREA. 2. ALL EQUIPMENT MUST BE LOWERED WHEN NOT IN USE AND MAY NOT BE LEFT INSIDE OF THE RUNWAY OBJECT FREE AREA. |

* DURATIONS DO NOT INCLUDE TIME REQUIRED FOR THE FAA FLIGHT CHECK
**THESE ARE MAJOR WORK ELEMENTS ONLY, THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL WORK ELEMENTS WITHIN EACH OPERATIONAL PHASE



LEGEND

- PHASE 1 WORK LIMITS (BASE BID)
- PHASE 2 WORK LIMITS (ALTERNATE BID)
- CONTRACTOR STAGING AND STORAGE AREA
- AIRSPACE EVALUATION LIMITS
- AIRSPACE CRITICAL LOCATION
- RSA— RUNWAY SAFETY AREA
- ROFA— RUNWAY OBJECT FREE AREA
- ↔ CONTRACTOR HAUL ROUTE
- ✕ TAXIWAY OR RUNWAY CLOSURE CROSS (SEE DETAIL ON SHEET PH-501)

GENERAL NOTES

- THE CONTRACTOR SHALL PROVIDE A CONSTRUCTION SCHEDULE TO THE AIRPORT AND RESIDENT ENGINEER FOR APPROVAL IN ACCORDANCE WITH THESE PHASING PLANS, SPECIFICATIONS, AND GENERAL PROVISIONS.
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT MANAGER AND RESIDENT ENGINEER A MINIMUM OF 72 HOURS IN ADVANCE OF A RUNWAY CLOSURE
- PRIOR TO IMPLEMENTATION OF THIS PLAN, THE AIRPORT WILL ANALYZE ALL CONSTRUCTION ACTIVITY AND/OR OTHER SPECIALIZED ACTIVITY ON THE AIRPORT AND DETERMINE THE LIMITATIONS TO BE IMPOSED.
- SUBSTANTIAL COMPLETION OF WORK SHALL INCLUDE ALL WORK REQUIRED TO OPEN TAXIWAY, TAXILANE, APRONS AND HANGARS TO OPERATIONAL USE BY AIRCRAFT.
- WORK WITHIN ACTIVE TAXIWAY SAFETY AND/OR OBJECT FREE AREAS SHALL BE PERFORMED IN ACCORDANCE WITH THE PHASING PLANS AND SHALL BE COORDINATED WITH THE AIRPORT.
- THE CONTRACTOR SHALL BE AWARE AND YIELD TO ALL ACTIVE AIRCRAFT OPERATIONS DURING CONSTRUCTION.
- THE CONTRACTOR SHALL ACCESS EACH WORK AREA BY THE HAUL ROUTES SHOWN ON THE PLANS. ANY DEVIATION TO THE HAUL ROUTES MUST BE APPROVED BY THE AIRPORT AND RESIDENT ENGINEER.
- THE CONTRACTOR SHALL MAINTAIN ACCESS TO SERVICE ROADS AND AIRPORT TENANT AREAS AT ALL TIMES.
- THE CONTRACTOR SHALL UTILIZE THE AIRPORT ENTRANCE AT CLOW INTERNATIONAL PARKWAY TO GAIN ACCESS TO THE JOB SITE.
- THE USE OF OFF-ROAD TRUCKS ARE NOT PERMITTED ON THE AIRPORT SERVICE DRIVES OR OTHER ROADWAYS. THE CONTRACTOR CAN ONLY UTILIZE TRUCKING EQUIPMENT SUITABLE FOR OVER-THE-ROAD USE.
- THE CONTRACTOR SHALL LIMIT TRUCK TRAFFIC BACKING UP ON CLOW INTERNATIONAL PARKWAY.
- THE CONTRACTOR SHALL CONDUCT POWER VACUUM CLEANING IMMEDIATELY FOLLOWING ANY ACCESS ONTO OR CROSSING OF THE PAVEMENT BY CONSTRUCTION TRAFFIC.
- NOTICE OF PROPOSED CONSTRUCTION WAS SUBMITTED ON JUNE 09, 2025 TO THE FAA OBSTRUCTION EVALUATION/ AIRPORT AIRSPACE ANALYSIS (OE/AAA).



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TITLE:
CONSTRUCTION SAFETY AND PHASING PLAN
PROJECT:
INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS

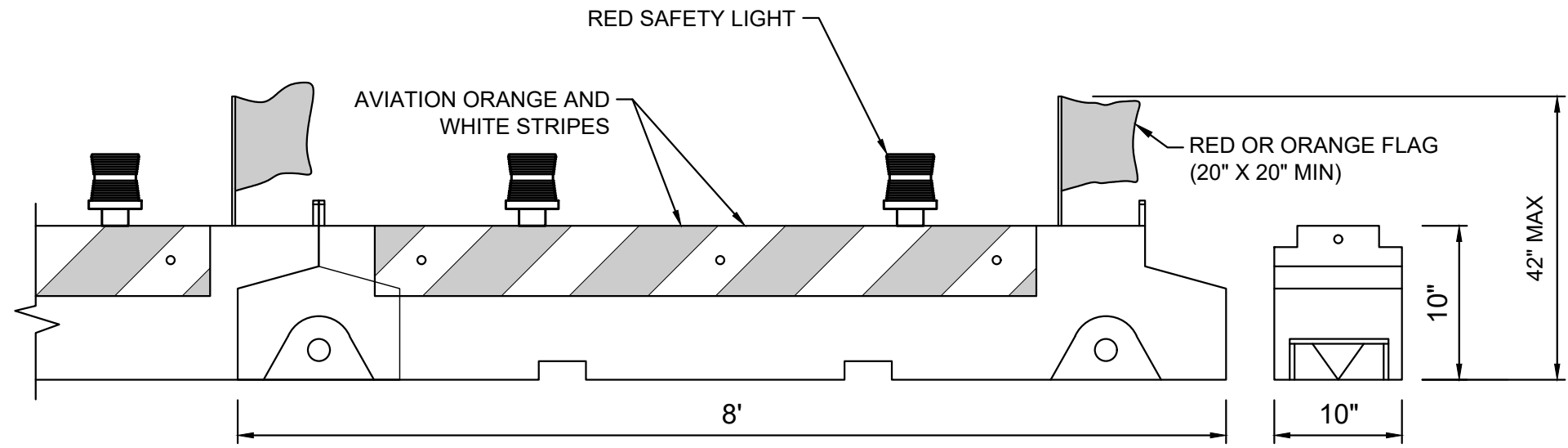
CERTIFICATION:

LUKE M. BURLINGAME
062-076682
OF ILLINOIS
EXP. 11/30/2025

06/06/2025

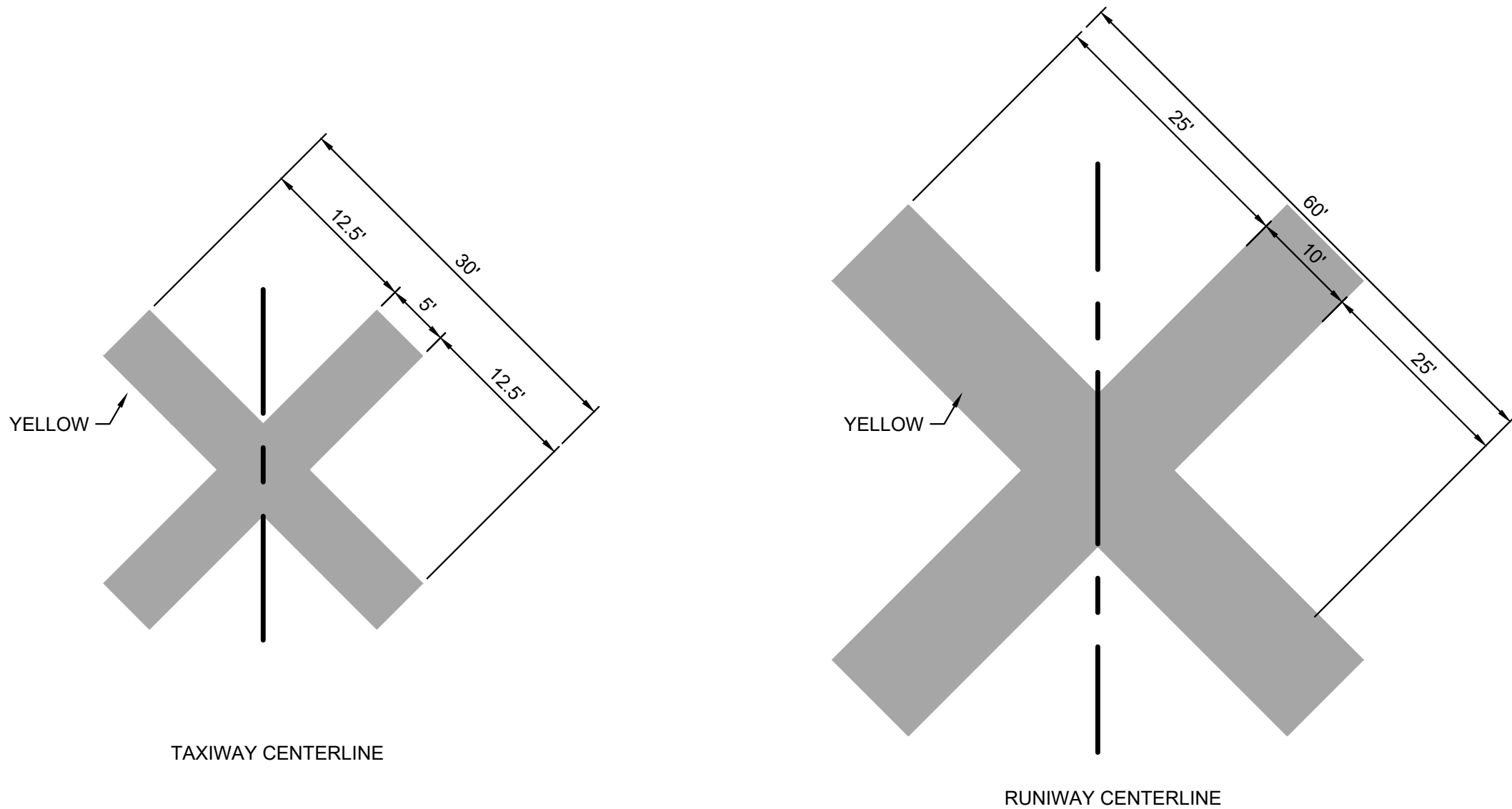
DRAWING NO.
PH-101
PROJECT NUMBER:
168001006

K:\CHS- Aviation\168001006-105-Instal-PAPI\168001006- PH Phasing.dwg, Burlingame, Luke 6/9/2025 11:40 AM
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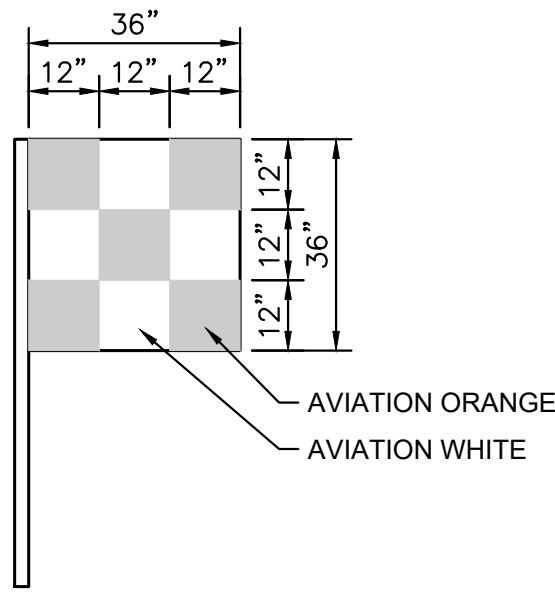
BARRICADE NOTES:

- THE CONTRACTOR SHALL FURNISH AND MAINTAIN PORTABLE LOW PROFILE CONSTRUCTION BARRICADES ON THIS PROJECT. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE BARRICADES IN WORKING ORDER THROUGHOUT THE PROJECT. MAINTENANCE WILL LIKELY INCLUDE, BUT IS NOT LIMITED TO: REPLACEMENT LAMPS, FLAGS, BARRICADES ETC. BARRICADES SHALL BE INSPECTED FOR SATISFACTORY OPERATION EVERY TWO (2) HOURS DURING WORKING HOURS. NO SEPARATE PAYMENT WILL BE MADE FOR SUPPLYING, MAINTAINING AND MOVING BARRICADES BUT SHALL BE CONSIDERED INCIDENTAL TO PAY ITEM AR150520, MOBILIZATION.
- AT PROJECT COMPLETION, ALL BARRICADES SHALL BE REMOVED FROM THE AIRPORT PROPERTY AND SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- LOW PROFILE CONSTRUCTION BARRICADES:
 - SHALL BE MULTI-BARRIER MODEL No. AR-10X96, LO PRO MODEL No. AB96, NAC MODEL No. GD9670, OR APPROVED EQUAL.
 - SHALL HAVE AT LEAST TWO (2) OMNI-DIRECTIONAL LOW INTENSITY FLASHING RED LIGHT ATTACHED TO EACH BARRICADE. LAMPS SHALL ALTERNATE PER FAA AC 70/7460-1 AND IDOT REQUIREMENTS.
 - THE FINAL LOCATION FOR THE BARRICADES SHALL BE ESTABLISHED IN THE FIELD WITH CONCURRENCE FROM THE RESIDENT ENGINEER.
 - SHALL HAVE AT LEAST TWO (2) ORANGE OR RED FLAGS (FOR CLARITY, ORIENTATION IN DETAIL ABOVE SHOWN FOR GENERAL PURPOSE AND DOES NOT REFLECT ACTUAL FLAG PLACEMENT).
 - SHALL BE SECURED TO THE PAVEMENT BY FILLING WITH WATER OR SAND BAGS. SAND BAGS SHALL BE THE SAME COLOR AS THE BARRICADES.
 - BARRICADES SECURED BY WATER OR SAND SHALL BE CHECKED ON A DAILY BASIS FOR LEAKS.
 - SHALL BE POSITIONED END TO END OR INTERLOCKED WITH NO GAP.



NOTES:

- TAXIWAY/RUNWAY CLOSURE MARKERS SHALL BE CONSTRUCTED OF AN EASILY REMOVABLE MATERIAL, SUCH AS PLYWOOD OR FABRIC, AND HELD IN PLACE WITH SAND BAGS PAINTED YELLOW. TAXIWAY/RUNWAY CLOSURE MARKERS SHALL BE YELLOW IN COLOR.
- THE CONTRACTOR SHALL PLACE TAXIWAY/RUNWAY CLOSURE MARKERS ON THE CENTERLINE OF THE TAXIWAY OR RUNWAY TO BE CLOSED AS SHOWN ON PLANS OR AS DIRECTED BY RESIDENT ENGINEER.
- TAXIWAY/RUNWAY CLOSURE MARKERS SHALL BE CONSIDERED INCIDENTAL TO ITEM AR150520, MOBILIZATION.



VEHICLE AND EQUIPMENT FLAG NOTES:

- ALL CONSTRUCTION MECHANIZED EQUIPMENT AUTHORIZED WITHIN THE MOVEMENT AREA OR RELATED SAFETY AREAS SHALL BE MARKED WITH A 3'X3' ORANGE AND WHITE CHECKERED FLAG WITH EACH BOX BEING 1' SQUARE.
- THE FLAG SHALL BE LOCATED ON THE UPPERMOST PORTION OF THE VEHICLE/MOTORIZED EQUIPMENT, OR BE ESCORTED BY A VEHICLE SO EQUIPPED.
- VEHICLE AND EQUIPMENT FLAGS SHALL BE CONSIDERED INCIDENTAL AR150520, MOBILIZATION.

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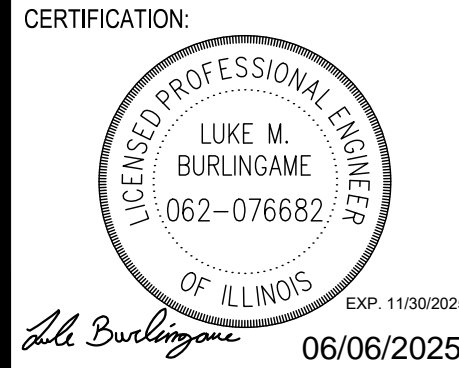
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TITLE:
CONSTRUCTION SAFETY AND PHASING DETAILS

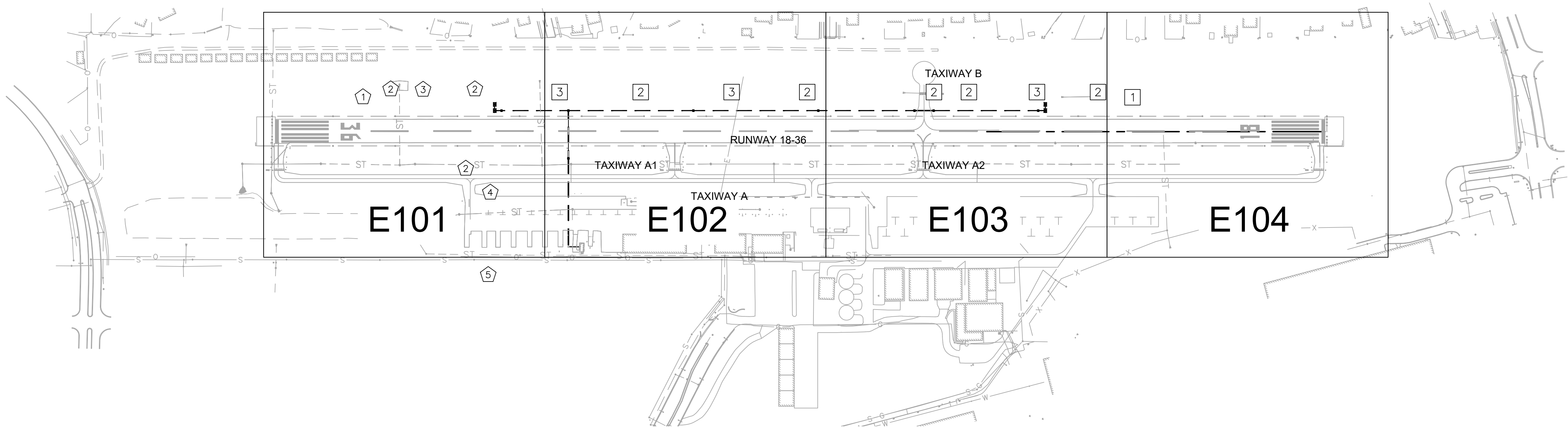
PROJECT:
**INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS**



| |
|------------------------------|
| DRAWING NO. PH-501 |
| PROJECT NUMBER: 168001006 |



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ELECTRICAL SCOPE OF WORK - BASE BID

- 1 INSTALL NEW TWO-BOX LED PAPI ON RUNWAY 36 END.
- 2 PROVIDE AND INSTALL NEW H-20 LOAD RATED CONCRETE HANDHOLE.
- 3 INSTALL NEW 2-2" DIRECT BURIED SCH 40 PVC DUCT BANK.
- 4 DIRECTIONAL BORE 2-2" SCH 80 HDPE CONDUIT FOR PAPI CIRCUIT.
- 5 INSTALL NEW 5KW CONSTANT CURRENT REGULATOR IN EXISTING ELECTRICAL VAULT.

ELECTRICAL SCOPE OF WORK - ALT 1

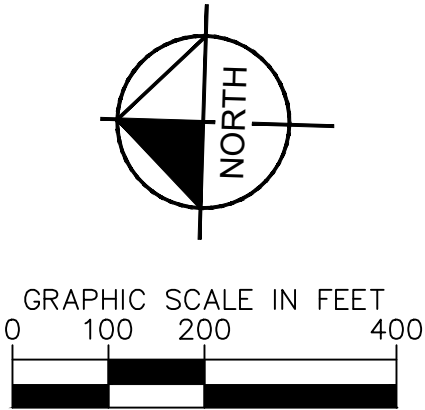
- 1 INSTALL NEW TWO-BOX LED PAPI ON RUNWAY 18 END.
- 2 PROVIDE AND INSTALL NEW H-20 LOAD RATED CONCRETE HANDHOLE.
- 3 INSTALL NEW 2-2" DIRECT BURIED SCH 40 PVC DUCT BANK.

ABBREVIATIONS

| | |
|----------|---|
| ATCT | AIR TRAFFIC CONTROL TOWER |
| C, CNDT | CONDUIT |
| CCR. | CONSTANT CURRENT REGULATOR. |
| C.E. | CONCRETE ENCASED (PER P-610) |
| D.B. | DIRECT EARTH BURIAL, PER P-152 OR P-153 |
| D.I. | DIRECTIONAL BORE/DRILL |
| EX, (E) | EXISTING EQUIPMENT OR FACILITY |
| FAA | FEDERAL AVIATION ADMINISTRATION |
| MFGR | MANUFACTURER |
| MIRL | MEDIUM INTENSITY RUNWAY LIGHTS |
| (N) | NEW EQUIPMENT OR FACILITY |
| OFF | OFFSET ALONG SURVEY DATUM BASELINE. |
| PAPI | PRECISION APPROACH PATH INDICATOR |
| PB | PULL BOX |
| P.O.C. | POINT OF CONNECTION (WITH EXISTING CONDUIT OR DUCT) |
| PVC | POLYVINYLCHLORIDE: CONDUIT MATERIAL |
| (R) | REMOVE EQUIPMENT OR FACILITY |
| REIL | RUNWAY END IDENTIFIER LIGHTS |
| RSA | RUNWAY SAFETY AREA |
| R/W, RWY | RUNWAY |
| STA | DISTANCE ALONG SURVEY DATUM BASE LINE |
| TFMR | TRANSFORMER |
| TSA | TAXIWAY SAFETY AREA |
| T/W, TWY | TAXIWAY |
| TYP. | TYPICAL |
| U.O.N. | UNLESS OTHERWISE NOTED |

SYMBOL LIST

| | |
|-----|--|
| --- | NEW CONDUIT |
| --- | NEW DUCT |
| --- | EXISTING CONDUIT |
| == | DIRECTIONAL BORE |
| ☒ | CONCRETE HANDHOLE |
| 881 | STYLE B L-881 PRECISION APPROACH PATH INDICATOR (PAPI) |



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TITLE:
ELECTRICAL SCOPE OF WORK
PROJECT:
INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS

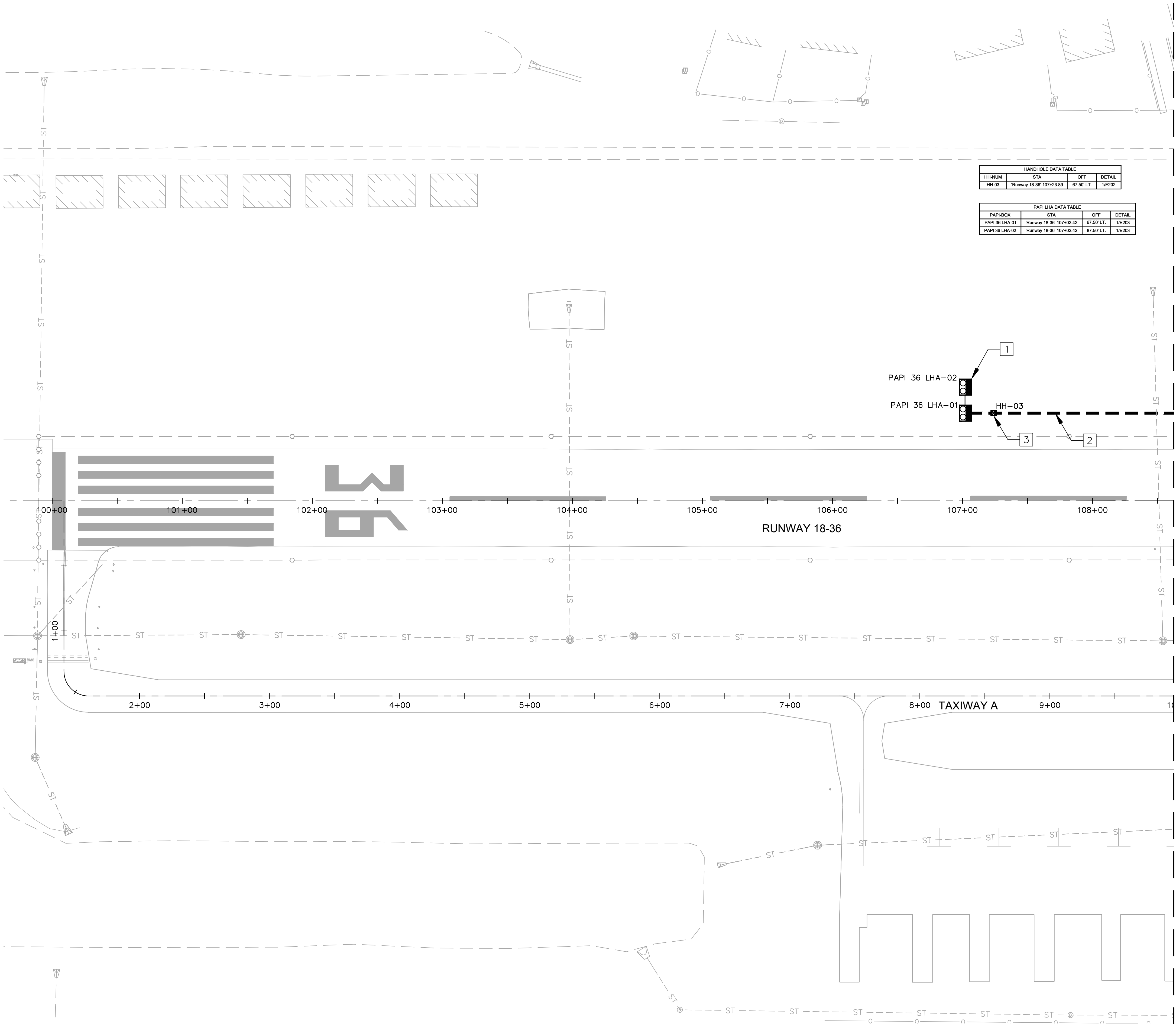
CERTIFICATION:

JOSEPH D. BRADSHAW
062-076052
OF ILLINOIS
EXP. 11/30/2025

06/06/2025

DRAWING NO.
E100
PROJECT NUMBER:
168001006

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LEGEND

- NEW DUCT
- EXISTING CONDUIT
- DIRECTIONAL BORE
- CONCRETE HANDHOLE
- PRECISION APPROACH PATH INDICATOR (PAPI)

CONSTRUCTION NOTES

- INSTALL NEW L-881, STYLE B, CLASS II LED TWO BOX PAPI ON NEW CONCRETE FOUNDATION. SEE SHEET E205 FOR DETAIL.
- INSTALL NEW 2-2" DIRECT BURIED SCH 40 PVC DUCT WITH L-824 5KV AIRFIELD CABLING FOR PAPI CIRCUIT. SEE SHEET E201 FOR DETAIL.
- INSTALL NEW H-20 LOAD RATED CONCRETE HANDHOLE. SEE SHEET E202 FOR DETAIL.

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ELECTRICAL LAYOUT

PROJECT:
INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS

CERTIFICATION:

06/06/2025

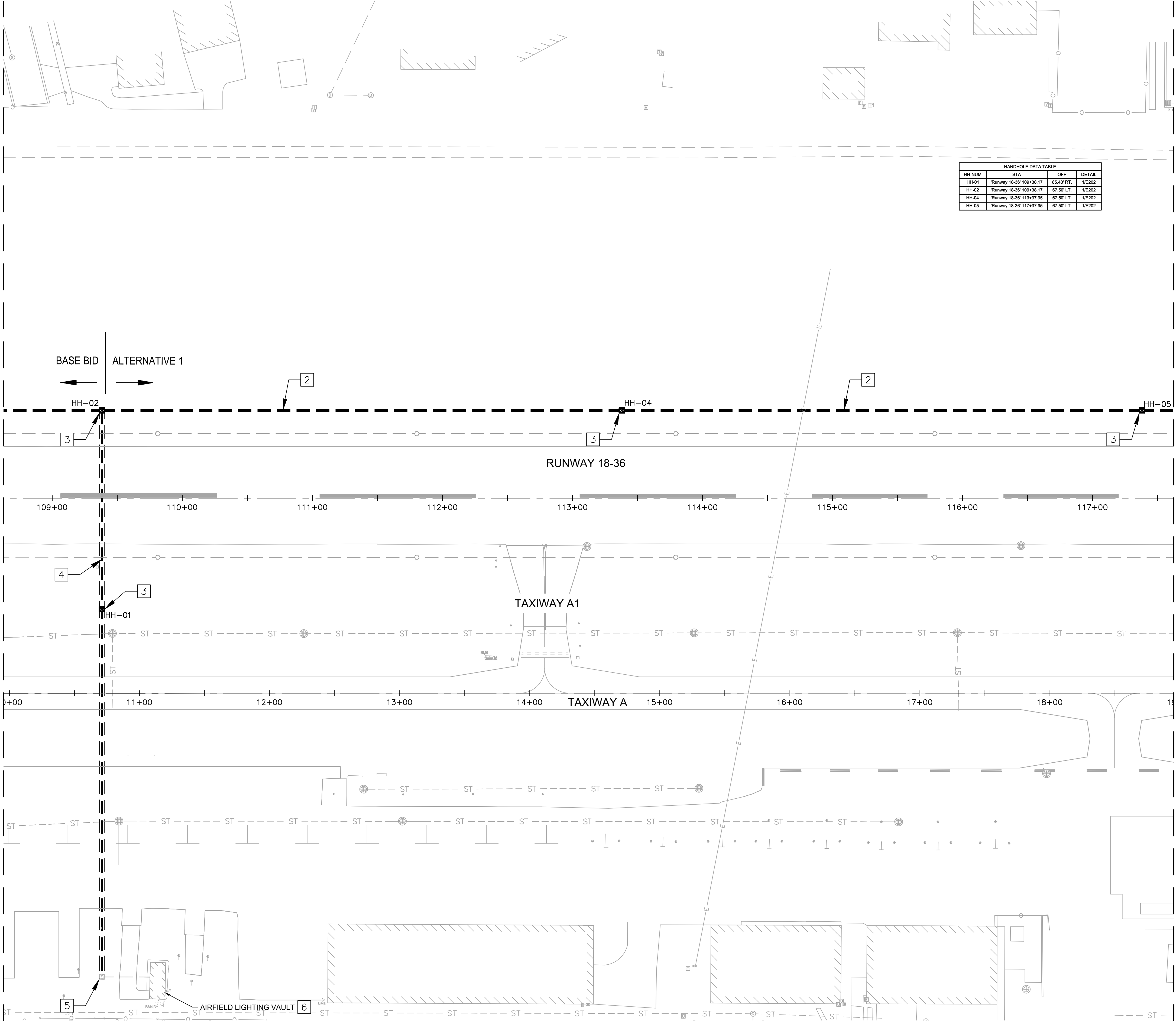
DRAWING NO.
E101

PROJECT NUMBER:
168001006

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MATCH LINE - SEE SHEET E101



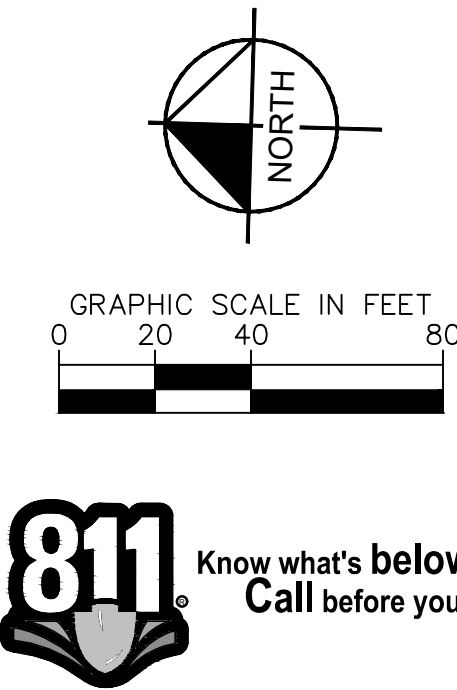
MATCH LINE - SEE SHEET E103

LEGEND

- NEW DUCT
- EXISTING CONDUIT
- DIRECTIONAL BORE
- CONCRETE HANDHOLE
- PRECISION APPROACH PATH INDICATOR (PAPI)

CONSTRUCTION NOTES

- INSTALL NEW 2-2" DIRECT BURIED SCH 40 PVC DUCT WITH L-824 5KV AIRFIELD CABLING FOR PAPI CIRCUIT. SEE SHEET E201 FOR DETAIL.
- INSTALL NEW H-20 LOAD RATED CONCRETE HANDHOLE. SEE SHEET E202 FOR DETAIL.
- INSTALL NEW 2-2" SCH 80 HDPE CONDUIT IN DIRECTIONAL BORE WITH WITH L-824 5KV AIRFIELD CABLING FOR PAPI CIRCUIT. SEE E201 FOR DETAIL.
- INTERCEPT EXISTING HANDHOLE WITH NEW 2-2" CONDUIT AND L-824 5KV AIRFIELD CABLING FOR PAPI CIRCUIT.
- FURNISH AND INSTALL NEW L-828 240V, 6.6A, 5KW CONSTANT CURRENT REGULATOR IN EXISTING AIRFIELD LIGHTING VAULT FOR NEW PAPI CIRCUIT.



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TEL (630) 487-5550

Bolingbrook
a place to grow

| | | | |
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TITLE:
ELECTRICAL LAYOUT

PROJECT:
**INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS**

CERTIFICATION:

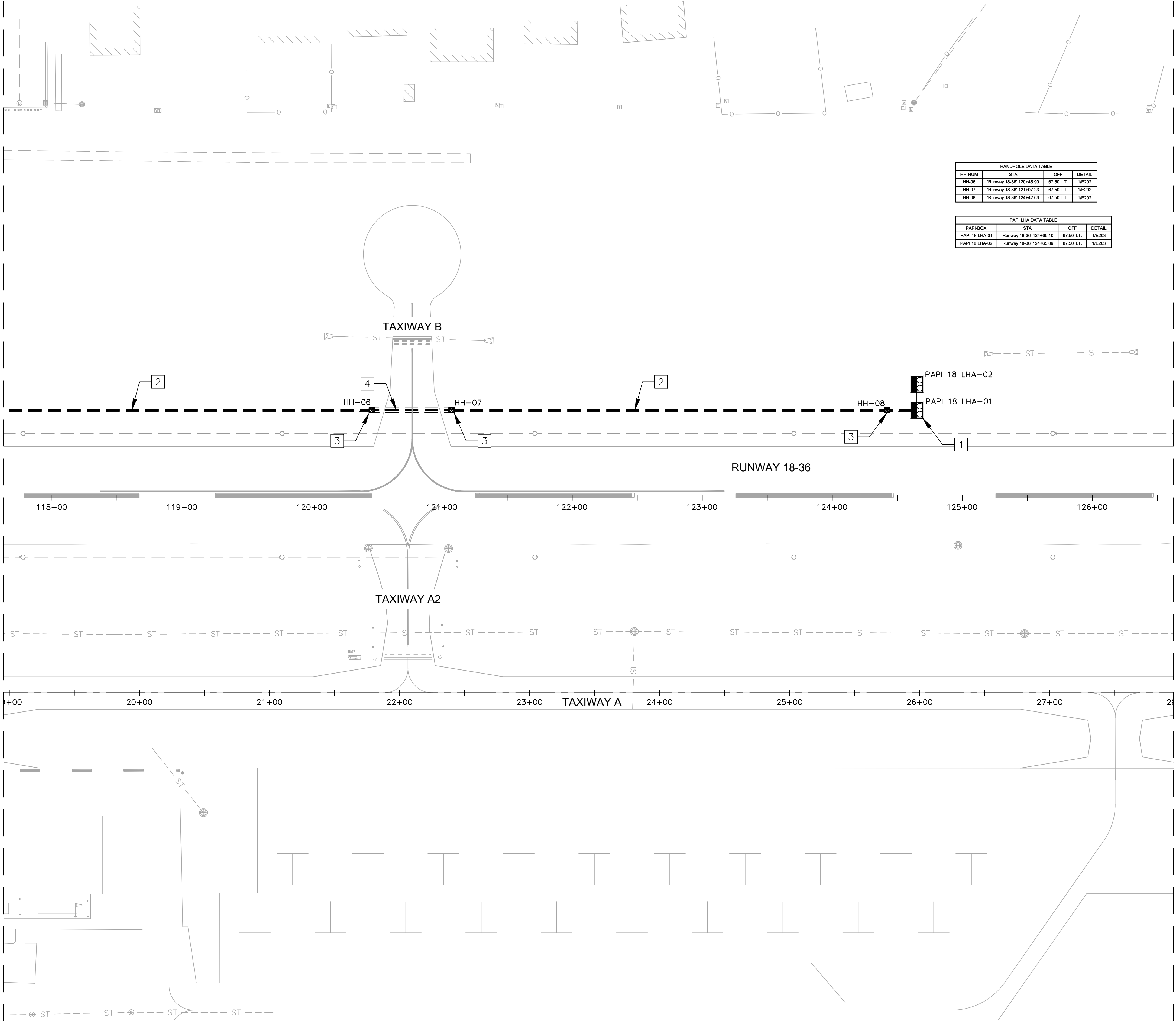
JOSEPH D. BRADSHAW
062-076052
OF ILLINOIS
EXP. 11/30/2025
06/06/2025

DRAWING NO.
E102

PROJECT NUMBER:
168001006

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MATCH LINE - SEE SHEET E102



MATCH LINE - SEE SHEET E104

LEGEND

- NEW DUCT
- EXISTING CONDUIT
- DIRECTIONAL BORE
- CONCRETE HANDHOLE
- PRECISION APPROACH PATH INDICATOR (PAPI)

CONSTRUCTION NOTES

- INSTALL NEW L-881, STYLE B, CLASS II LED TWO BOX PAPI ON NEW CONCRETE FOUNDATION. SEE SHEET E205 FOR DETAIL.
- INSTALL NEW 2-2" DIRECT BURIED SCH 40 PVC DUCT WITH L-824 5KV AIRFIELD CABLING FOR PAPI CIRCUIT. SEE SHEET E201 FOR DETAIL.
- INSTALL NEW H-20 LOAD RATED CONCRETE HANDHOLE. SEE SHEET E202 FOR DETAIL.
- INSTALL NEW 2-2" SCH 80 HDPE CONDUIT IN DIRECTIONAL BORE WITH WITH L-824 5KV AIRFIELD CABLING FOR PAPI CIRCUIT. SEE E201 FOR DETAIL.

Kimley»Horn

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SUITE 600
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TEL (630) 487-5550

Bolingbrook
a place to grow

| | | | |
|-----------|------------|----------------|------------|
| DATE: | 06/06/2025 | SCALE: | 1"= 40'-0" |
| DRAWN BY: | DKP | CHECKED BY: | JDB |
| REV. | DATE | DESCRIPTION | |
| 0 | 06/06/2025 | ISSUED FOR BID | |

TITLE: ELECTRICAL LAYOUT

PROJECT: INSTALL PRECISION APPROACH PATH INDICATOR (PAPI) FOR RUNWAY 18/36 VILLAGE OF BOLINGBROOK, WILL COUNTY, ILLINOIS

CERTIFICATION:

JOSEPH D. BRADSHAW
062-076052
OF ILLINOIS
EXP. 11/30/2025
06/06/2025

DRAWING NO. E103

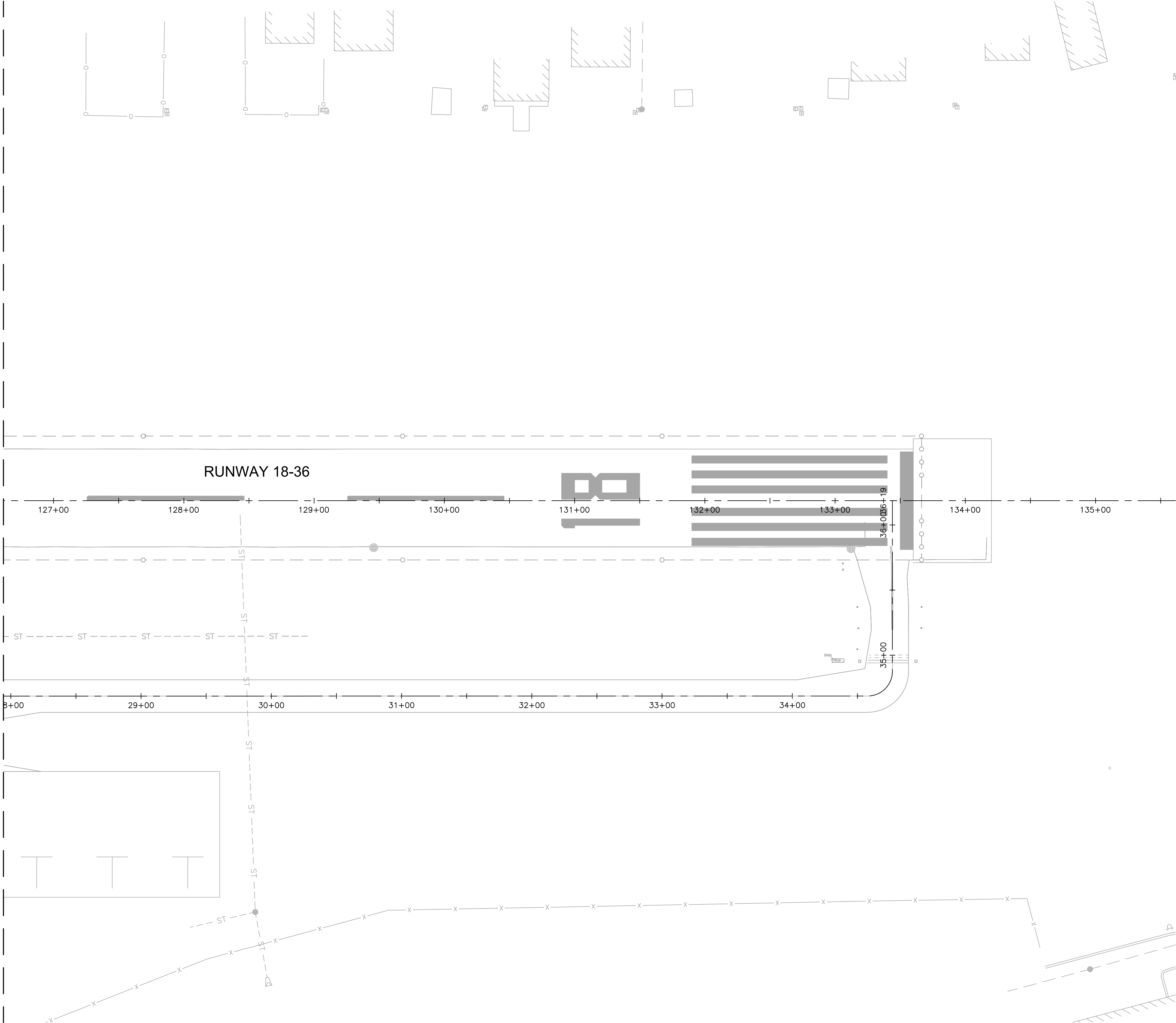
PROJECT NUMBER: 168001006

GRAPHIC SCALE IN FEET
0 20 40 80

811 Know what's below.
Call before you dig.

\\chsf01\my_chs\GIS_Artist\168001006 - 105 Install PAPI\Doc\Plans\168001006-EL.dwg Plot: D:\p... 6/6/2025 8:47 AM
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MATCH LINE - SEE SHEET E103



LEGEND

- NEW DUCT
- EXISTING CONDUIT
- DIRECTIONAL BORE
- CONCRETE HANDHOLE
- PRECISION APPROACH PATH INDICATOR (PAPI)

CONSTRUCTION NOTES

NO CONSTRUCTION THIS SHEET.



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TITLE:
ELECTRICAL LAYOUT

PROJECT:
INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS

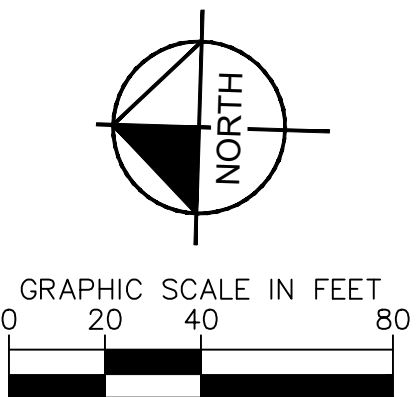
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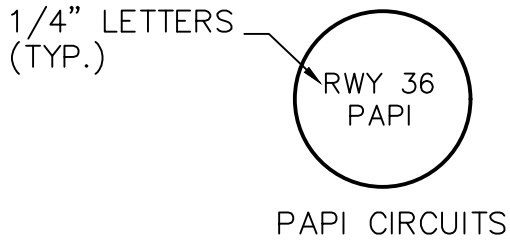
DRAWING NO.

E104

PROJECT NUMBER:
168001006



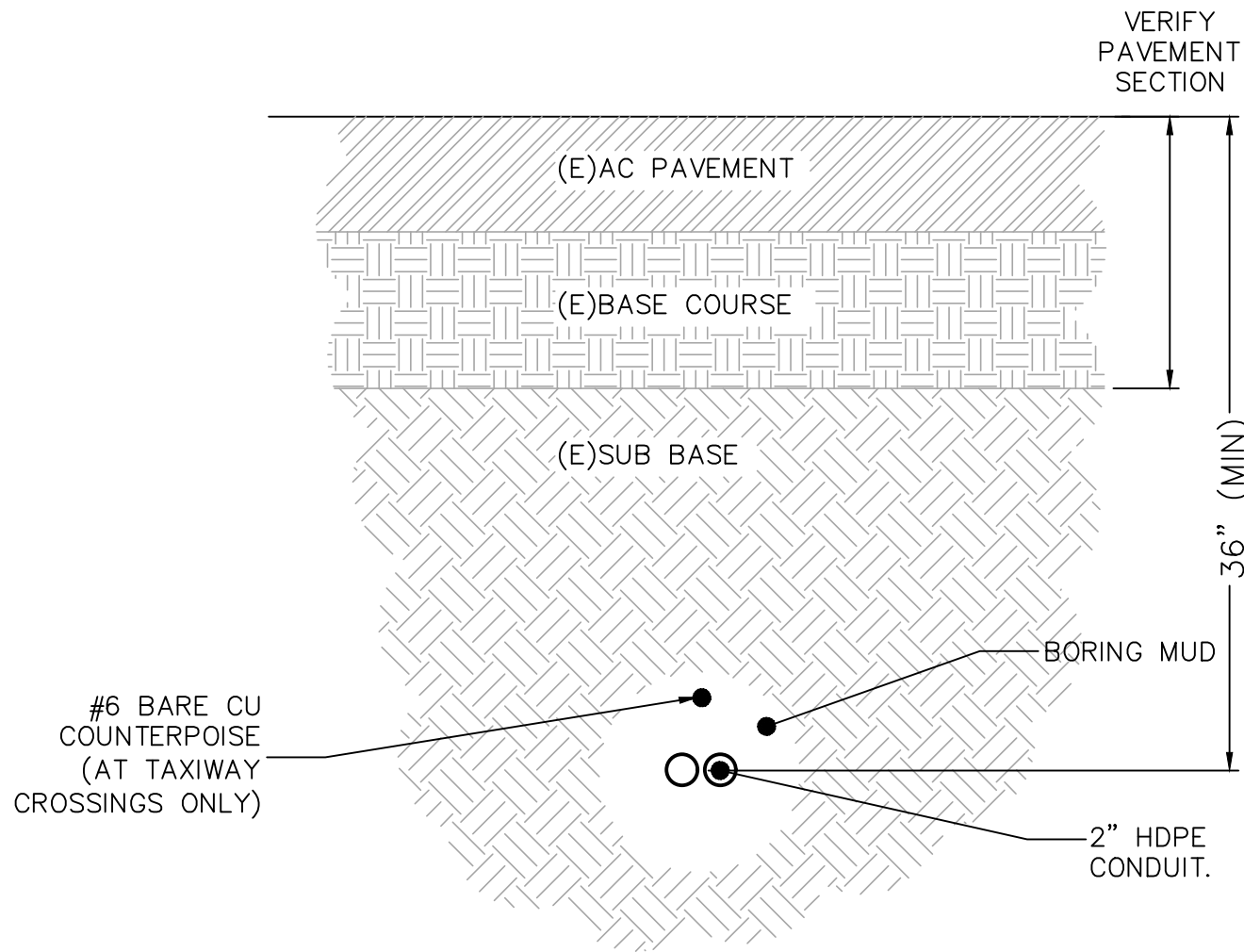
\\chsf601\my_chs\GIS_Actions\168001006-105 Install PAPI\Code\Plans\168001006-EZ.dwg, Paterl, D:\h\g, 6/6/2025 8:48 AM, THIS DRAWING, INCLUDING ALL CONTENTS AND DESIGN INFORMATION, IS THE PROPERTY OF KIMLEY-HORN AND ASSOCIATES, INC. AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, WITHOUT WRITTEN PERMISSION OF KIMLEY-HORN AND ASSOCIATES, INC. ALL RIGHTS ARE RESERVED.



NOTES:

1. BLANK SERVCO BRONZE MARKERS OR EQUIVALENT TO BE EMBEDDED IN FRESH CONCRETE AT TIME OF POUR. LOCATE ON SIDE OF BASE TOWARD R/W OR T/W OR AS DIRECTED BY OWNER. AFTER CONCRETE IS CURED, STAMP WITH FIXTURE AND CIRCUIT IDENTIFICATION AS SHOWN ON PLANS.
2. ALL LIGHTING SYSTEM (SIGNS AND LIGHTS) CABLES ENTERING OR LEAVING OR PASSING THROUGH A MANHOLE, HANDHOLE, PULLBOX, OR LIGHTING FIXTURE BASE SHALL BE IDENTIFIED ON EACH SIDE OF EACH L-823 CONNECTOR KIT WITH AN INDIVIDUAL IDENTIFICATION TAG INDICATING THE LIGHTING CIRCUIT.
3. NO LIGHTING SYSTEM CABLE SHALL BE PULLED "STRAIGHT THROUGH" A HANDHOLE, MANHOLE, JUNCTION BOX OR LIGHT BASE. EACH CABLE SHALL PROVIDED WITH THE SPECIFIED LOOP OF SLACK CABLE AND, UNLESS OTHERWISE NOTED, SHALL BE SPLICED IN EACH MANHOLE, HANDHOLE, JUNCTION BOX AND LIGHT BASE WITH L-823 CONNECTOR KITS AND HEAT-SHRINKABLE SHEATHS AND COLOR CODING. "HOMERUNS" SHALL BE RUN AS CONTINUOUSLY AS POSSIBLE WITHOUT SPLICES IN MANHOLES OR HANDHOLES. "HOMERUNS" ARE THE CIRCUIT PORTION FROM THE LAST HANDHOLE/MANHOLE CONTAINING BRANCHES OF THE CIRCUITS IN SEPARATE DIRECTIONS OF THE FIELD.
4. TAGS SHALL BE NON-CORROSIVE MATERIAL OF TYPE AND SIZE AS SPECIFIED UNDER IL AIRPORT CONSTRUCTION SPECIFICATIONS ITEM 108, CONNECT TO CABLE WITH PLASTIC CABLE TIE.
5. ALL NOMINAL 600V POWER CABLES OR MULTI-PAIR SIGNAL CABLES SHALL BE IDENTIFIED WITH INDIVIDUAL IDENTIFICATION TAGS ONE TIME AS THEY PASS THROUGH EACH HANDHOLE OR MANHOLE AND ON BOTH SIDES OF ANY SPLICES.

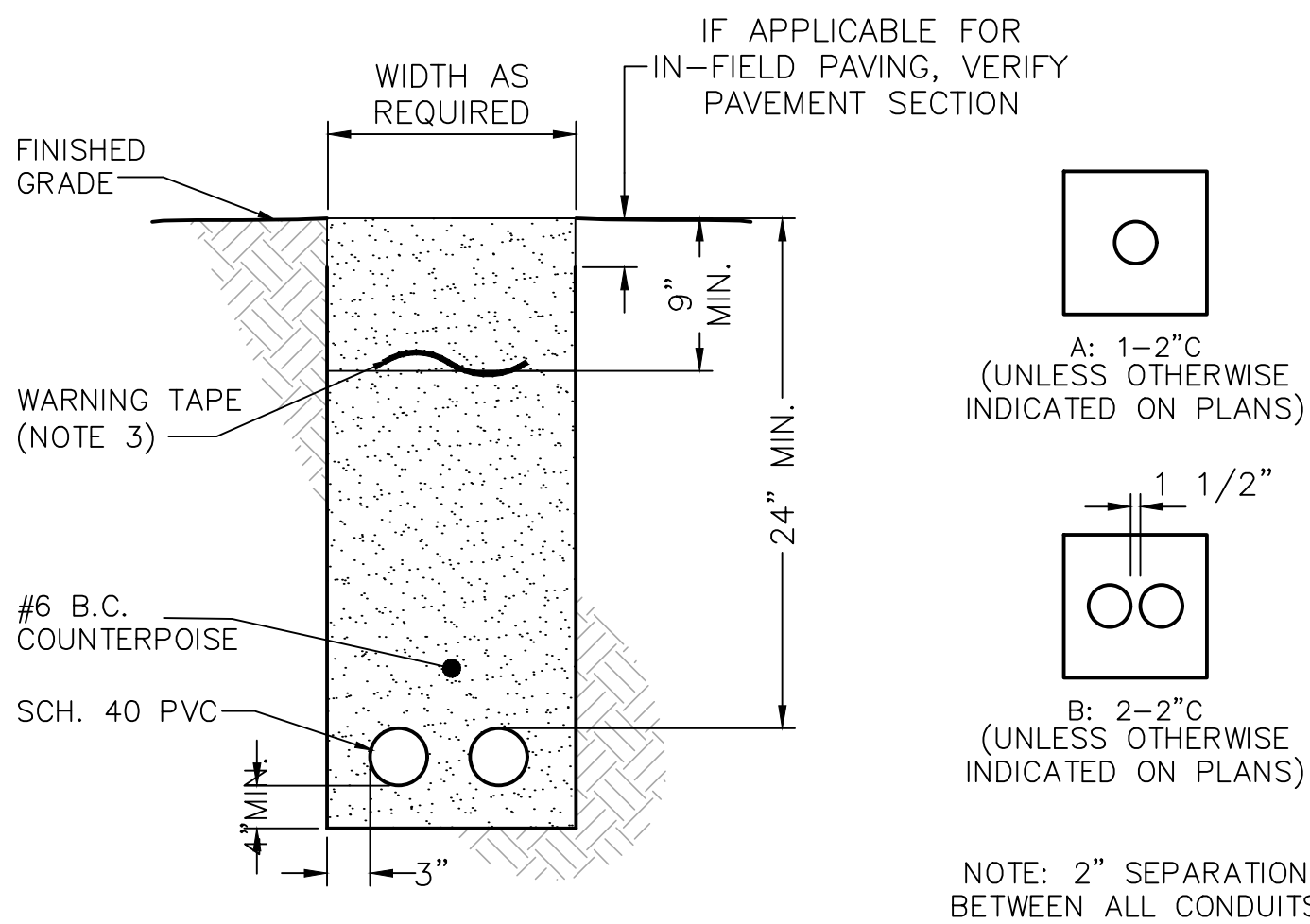
1 POWER AND COMMUNICATION CABLE TAGS
SCALE: NTS



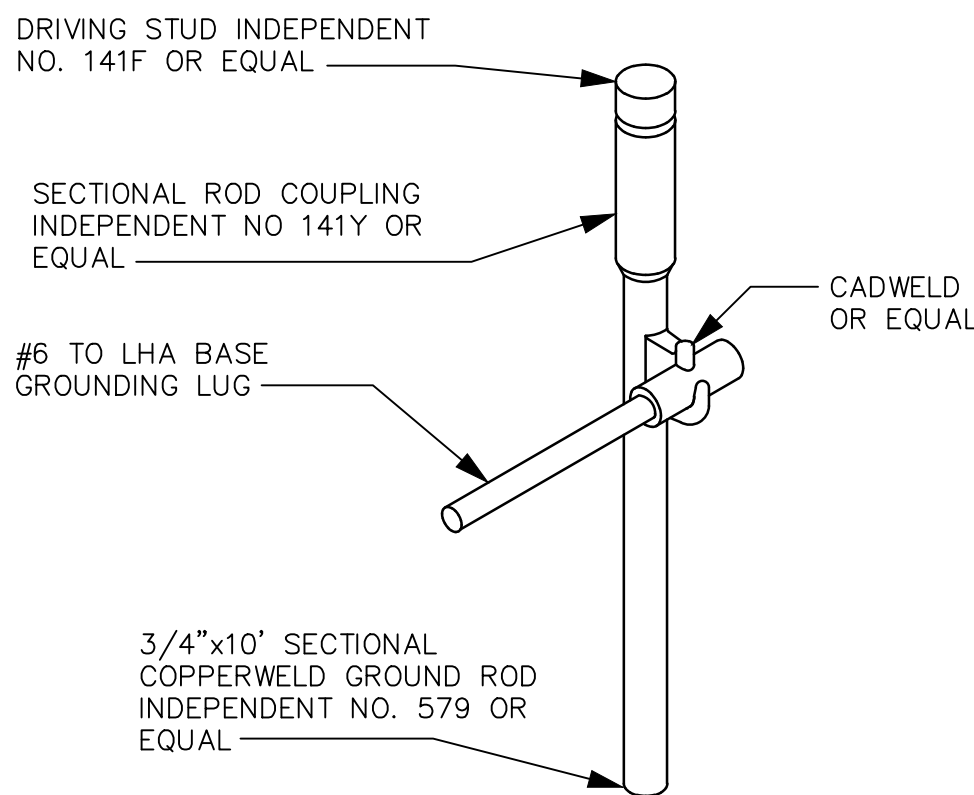
CONDUIT/DUCT & CABLE INSTALLATION NOTES:

1. SEE PLANS FOR REQUIRED DUCT SECTIONS. ALL OF THE SECTIONS SHOWN ON THIS DRAWING MAY NOT BE USED ON THIS PROJECT.
2. PROVIDE PULL ROPES IN ALL (NEW) UNUSED CONDUITS. PLUG UNUSED CONDUIT ENDS IN HANDHOLES OR MANHOLES.
3. WARNING TAPE IS REQUIRED IN ALL AREAS OF THE AIRPORT.
4. UNLESS OTHERWISE NOTED, INSTALL A BARE COPPER (BC) COUNTERPOISE CONDUCTOR (#6 FOR AIRFIELD POWER, #1/0 FOR FAA POWER AND COMM) WITH EACH DUCT ASSEMBLY AND EACH SINGLE CONDUIT. EXOTHERMICALLY WELD TO GROUND RODS PER IL AIRPORT CONSTRUCTION SPECIFICATIONS ITEM 108 AND 110.
5. INSTALL LIGHTING SERIES CIRCUITS AS FOLLOWS:
A. START INSTALLATION IN BOTTOM CONDUITS OF DUCT ARRAY, LEAVING THE UPPER CONDUITS EMPTY.
7. BACKFILL NOTES: REFER TO SPECIFICATION IL AIRPORT CONSTRUCTION SPECIFICATIONS ITEM 110-3.5.
8. REFER TO SPECIFICATION ITEM L-110 FOR CONDUIT/DUCT SPACER SYSTEM.

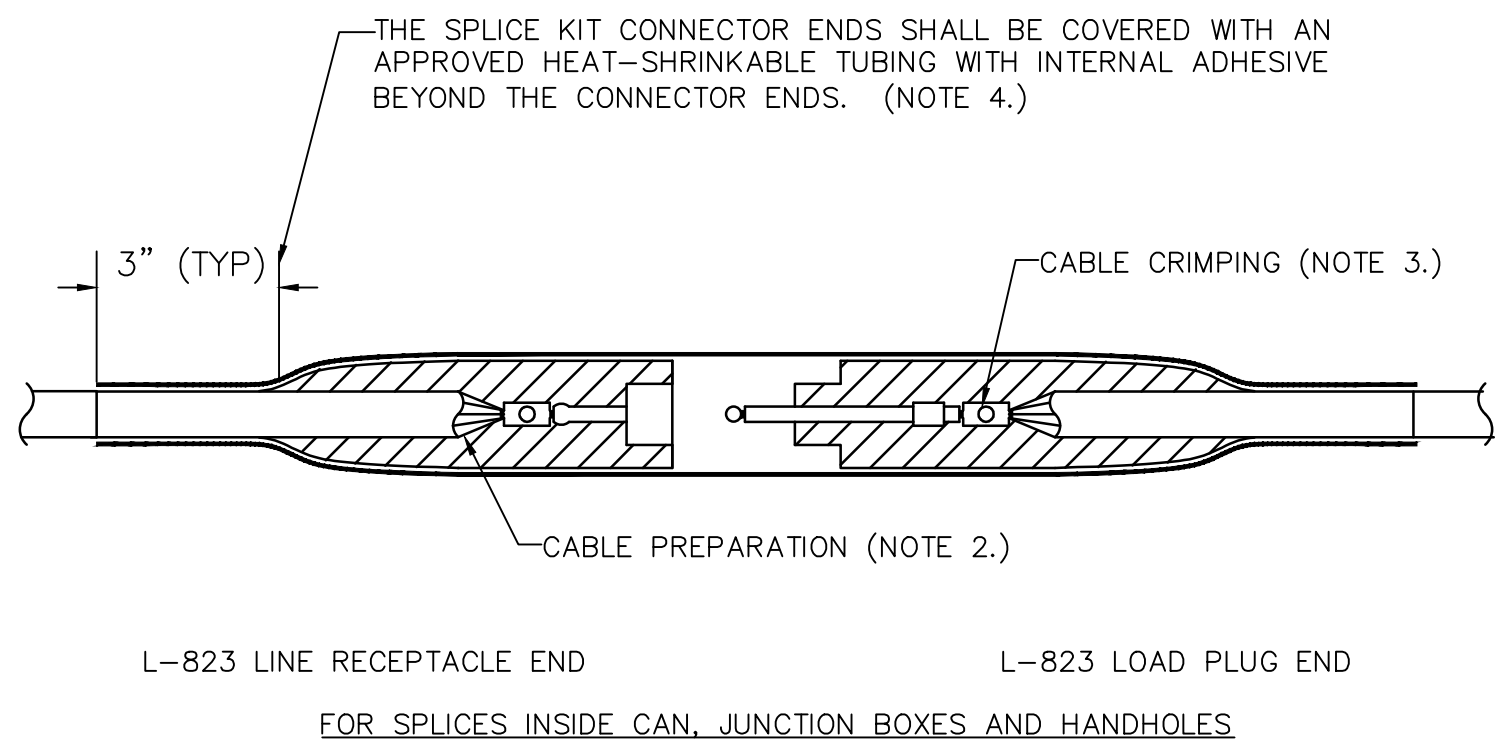
2 DIRECTIONAL BORE DETAIL
SCALE: NTS



3 TYPICAL TRENCH SECTION - DIRECT BURY
SCALE: NTS



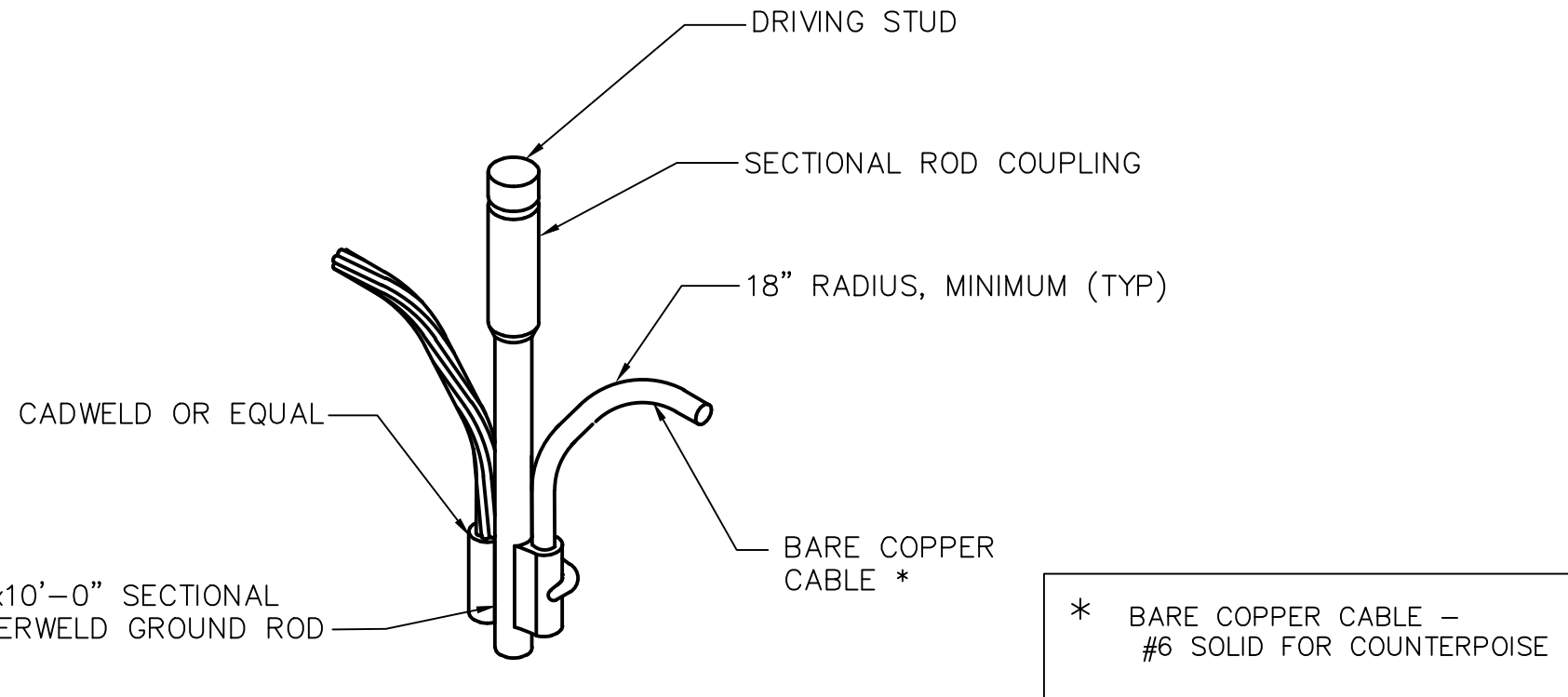
6 GROUNDING SYS. GROUND ROD DETAIL
SCALE: NTS



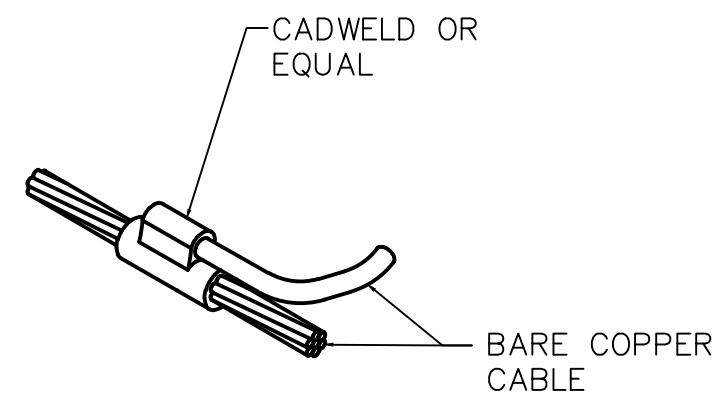
NOTES:

1. PROVIDE MALE & FEMALE L-823 CONNECTORS AS REQUIRED ON EACH CONDUCTOR IN EACH BASE, HANDHOLE OR MANHOLE TO ALLOW ISOLATION OF HOMERUN CIRCUIT.
2. ALL CABLE ENDS SHALL BE PREPARED, IF REQUIRED BY THE MANUFACTURER, WITH THE USE OF A TAPERING TOOL SPECIFICALLY DESIGNED FOR USE WITH L-824 CABLES. KNIVES SHALL NOT BE ALLOWED.
3. PLUG AND RECEPTACLE END FITTINGS SHALL BE CRIMPED ONTO THE CONDUCTOR BY USE OF AN AIRPORT PERSONNEL ACCEPTED HANDHELD RATCHETING TYPE CRIMPING TOOL. EACH CABLE TERMINATION SHALL BE SECURED BY TWO CRIMPS AS PER MANUFACTURER RECOMMENDATIONS.
4. ALL CABLE CONNECTORS SHALL BE MADE WITH L-823 CONNECTOR & WITH A 12" (APPROX.) LENGTH OF HEAT-SHRINKABLE RUBBER SHEATH OVER THE PLUG ASSEMBLY. CABLE CONNECTOR KITS SHALL BE SIZED FOR TYPE B CABLE.
5. AT THE POINT OF CONNECTION WITH THE EXISTING FIELD CIRCUITS, INSTALL NEW L-823 PLUGS ON BOTH THE NEW AND EXISTING CABLES. VERIFY INSULATION TYPES OF BOTH NEW & EXISTING CABLES & COORDINATE WITH TERMINATION KITS TO ASSURE PROPER AND WATERPROOF FIT.
6. THERE SHALL BE NO SPLICES BETWEEN LIGHTS, ONLY IN BASES OR HANDHOLES.

7 TYPICAL 5kV CABLE SPLICE
SCALE: NTS



4 COUNTERPOISE SYS. GROUND ROD DETAIL
SCALE: NTS



5 COUNTERPOISE SYSTEM GROUNDING DETAIL
SCALE: NTS



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TITLE:
ELECTRICAL DETAILS

PROJECT:
INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS

CERTIFICATION:



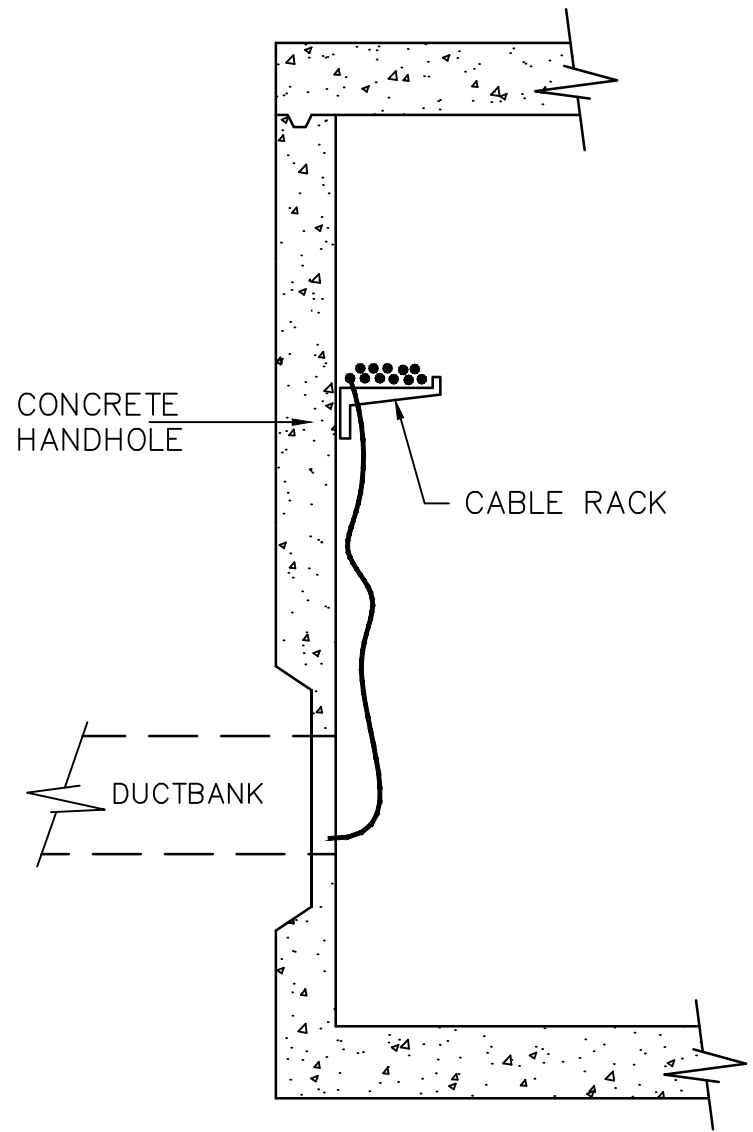
DRAWING NO.

E201

PROJECT NUMBER:

168001006

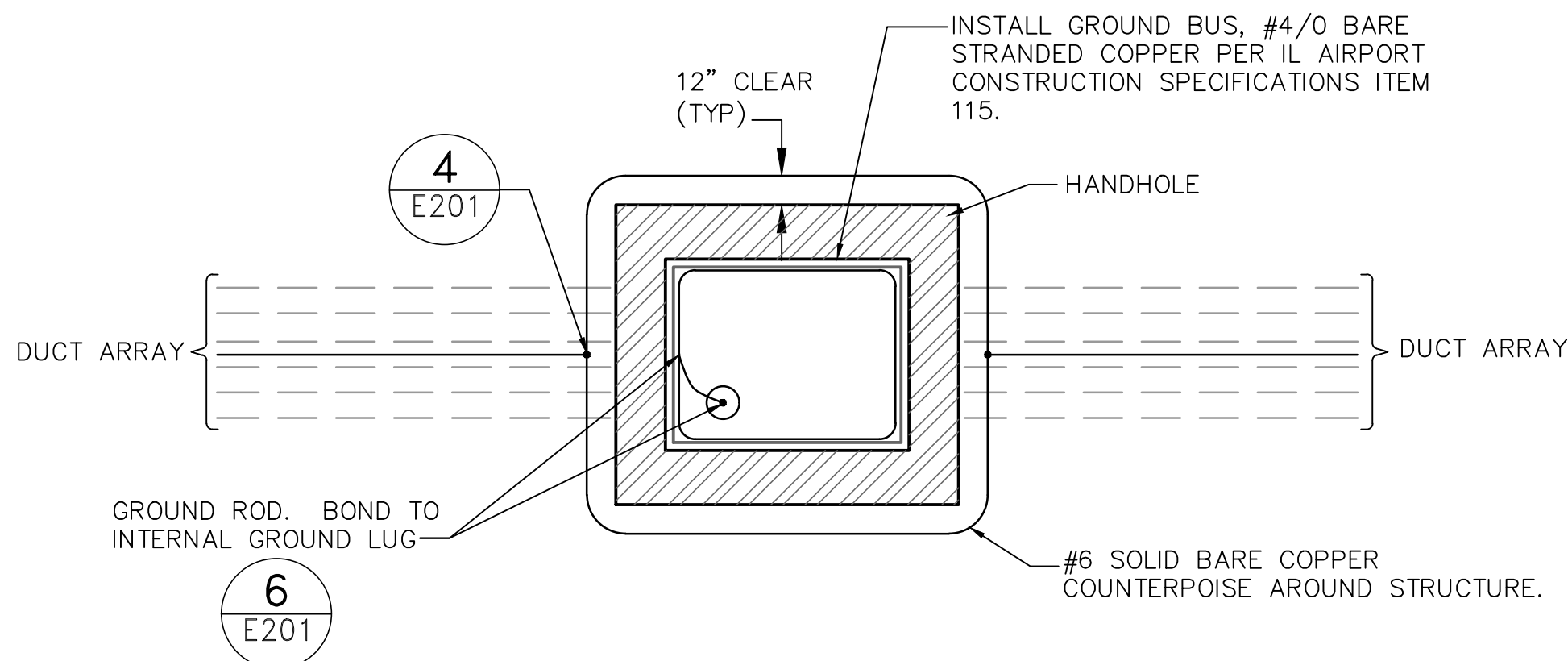
\\chsf601\my_chs\GIS_Artwork\168001006-E2.dwg Plot Date: 6/6/2025 8:48 AM
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1. PROVIDE GALVANIZED STEEL RACK ARMS, BOLTED TO CHANNELS INSET IN HANDHOLE WALLS. PROVIDE MINIMUM (2) ARMS FOR EVERY WALL.
2. CABLE RACKING SHALL BE LIMITED TO THE UPPER HALF OF THE HANDHOLE.
3. PROVIDE CIRCUIT TAGS FOR EVERY L-823 CABLE SPLICE.
4. PROVIDE A MINIMUM OF EIGHT FEET OF CABLE BETWEEN ENTRY POINT AND L-823 SPLICE.
5. RACK CABLES ON SIDE(S) OF ENTRY/EXIT CONDUITS.
6. #2 AWG BARE COPPER PIGTAIL SHALL BOND THE GROUNDING BUS TO METALLIC CABLE RACK PER IL AIRPORT CONSTRUCTION SPECIFICATIONS ITEM 115.
7. DUCT/CONDUIT ARRAY SHALL ENTER HANDHOLE IN LOWER HALF OF THE HANDHOLE.
8. PROVIDE BELL END FOR EVERY CONDUIT IN THE ARRAY.
9. GROUT AROUND/BETWEEN CONDUITS & CONCRETE WALL.

2 CABLE RACKING DETAIL

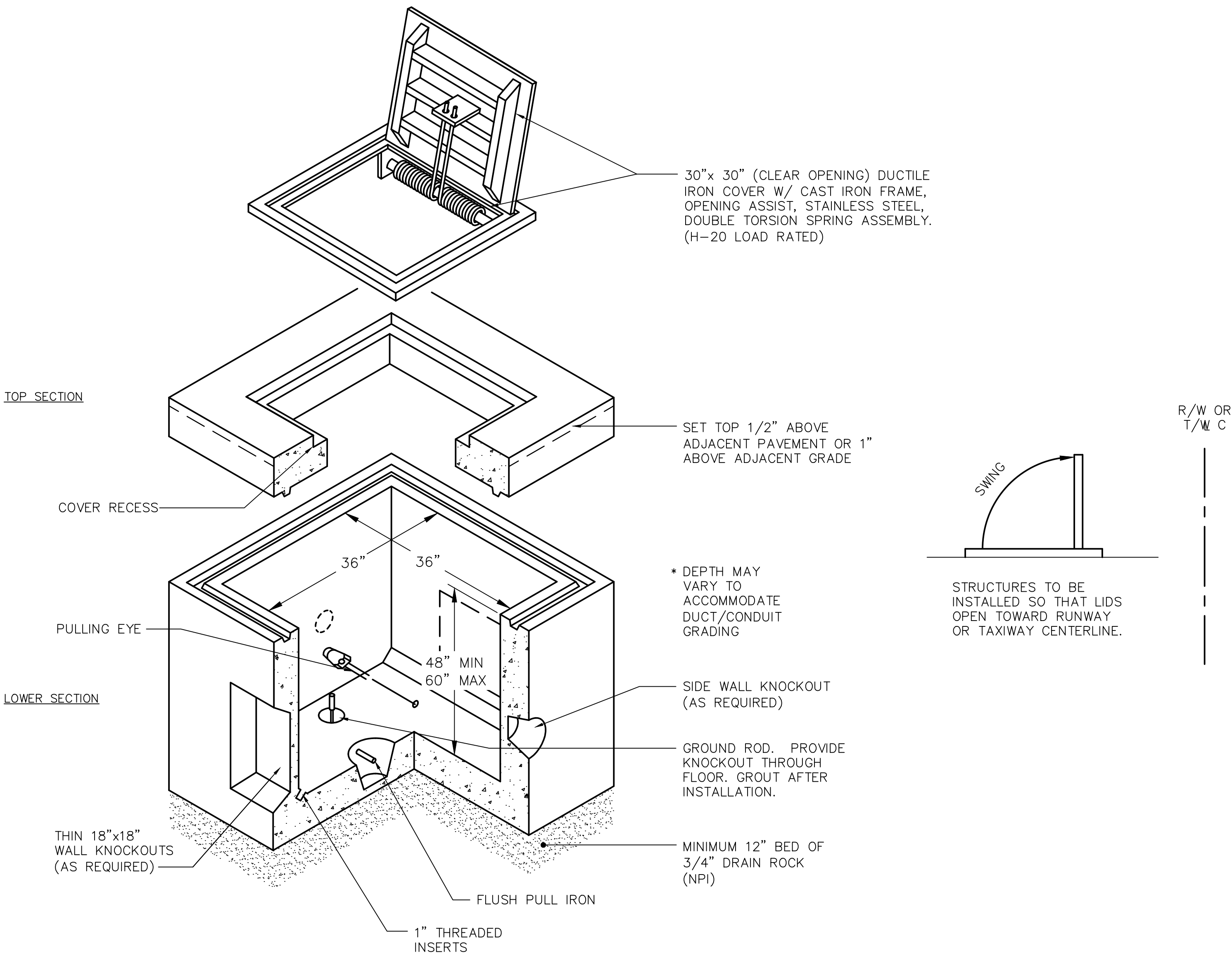
SCALE: NTS



NOTE:
ALL COUNTERPOISE WIRES FOR DUCT BANKS AND CONDUITS TO BE CLAMPED TO COUNTERPOISE LOOP AROUND STRUCTURE

3 HANDHOLE COUNTERPOISE DETAIL

SCALE: NTS



1 H-20 LOAD RATED CONCRETE HANDHOLE

SCALE: NTS

HANDHOLE NOTES:

1. ALL STEEL, OTHER THAN REBAR, TO BE HOT DIPPED GALVANIZED AFTER FABRICATION.
2. PROVIDE ALL HANDHOLES WITH CABLE RACKS AND PULLING EYES PER SPECIFICATIONS.
3. INSTALL HANDHOLES PARALLEL TO ADJACENT RUNWAY OR TAXIWAY CENTERLINE.
4. PROVIDE GROUND ROD FOR EACH HANDHOLE PER SPECIFICATIONS.
5. GROUT ALL CONDUIT PENETRATIONS.
6. PROVIDE MANUFACTURER'S DRAWINGS AND STRUCTURAL CALCULATIONS FOR ALL CONCRETE ELEMENTS OF HANDHOLES AND FABRICATED TRAFFIC LIDS.
7. NOTE: DO NOT CONNECT COUNTERPOISE TO GROUNDING SYSTEM. ROUTE COUNTERPOISE AROUND HANDHOLES. SEE DETAIL 3 THIS SHEET.

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TITLE: ELECTRICAL DETAILS

PROJECT: INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS

CERTIFICATION:



DRAWING NO.

E202

PROJECT NUMBER:

168001006





TITLE: RUNWAY 36 PAPI SITING

PROJECT: INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS

DRAWING NO.

E203

PROJECT NUMBER:

168001006

| R/W | DISTANCE FROM THRESHOLD | D1 DIST. FROM R/W TO C OF LHA #1 (FT) | D2 LIGHT HOUSING ASSEMBLY (LHA) SPA. O.C. (FT.) | R/W CL. ELEV. @ PAPI SITING STA (FT)** | LIGHT CENTER ELEVATION (FT) | AIMING ANGLE BASED ON 3° 00' GLIDE PATH* | | EFFECTIVE VISUAL THRESHOLD CROSSING HEIGHT (TCH) | THRESHOLD R/W CENTERLINE ELEVATION (FT)** | R/W |
|-----|-------------------------|--|--|--|---|--|--------|--|---|-----|
| | | | | | | LHA#01 | LHA#02 | | | |
| 36 | 702.4' | 67'—6" | 20'—0" | 660.2' | 661.2' (NOT TO EXCEED 1' ABOVE RUNWAY CL) | 3° 15' | 2° 45' | 40.0' | 357.0' | 36 |

←
DIRECTION OF
APPROACH





| | | | |
|---------------------|------------|--------------------|--|
| DATE: 06/06/2025 | | SCALE: N/A | |
| DRAWN BY: DKP | | CHECKED BY: JDB | |
| REV. | DATE | DESCRIPTION | |
| 0 | 06/06/2025 | ISSUED FOR BID | |

TITLE: RUNWAY 18 PAPI SITING

PROJECT: INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS

CERTIFICATION:



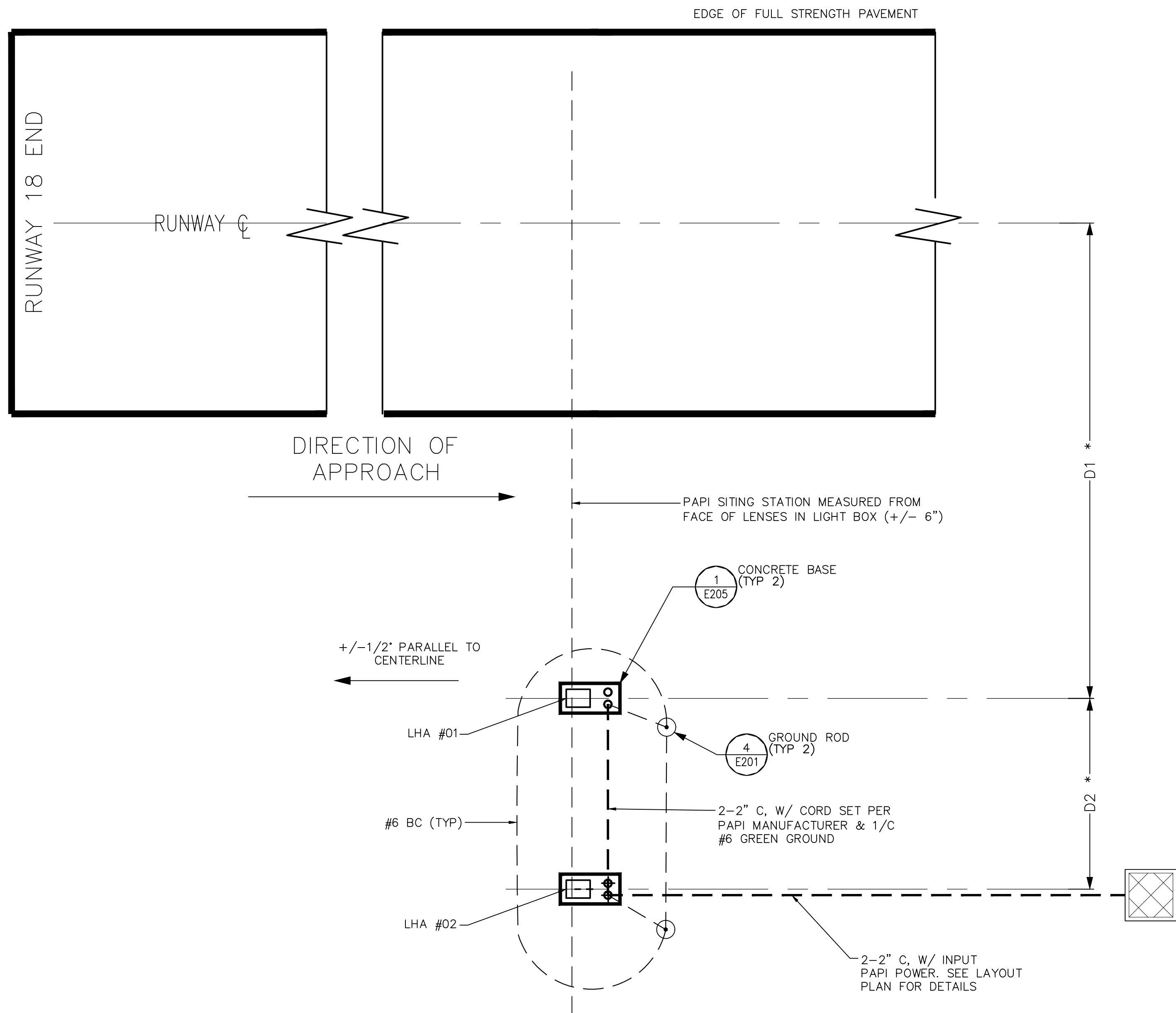
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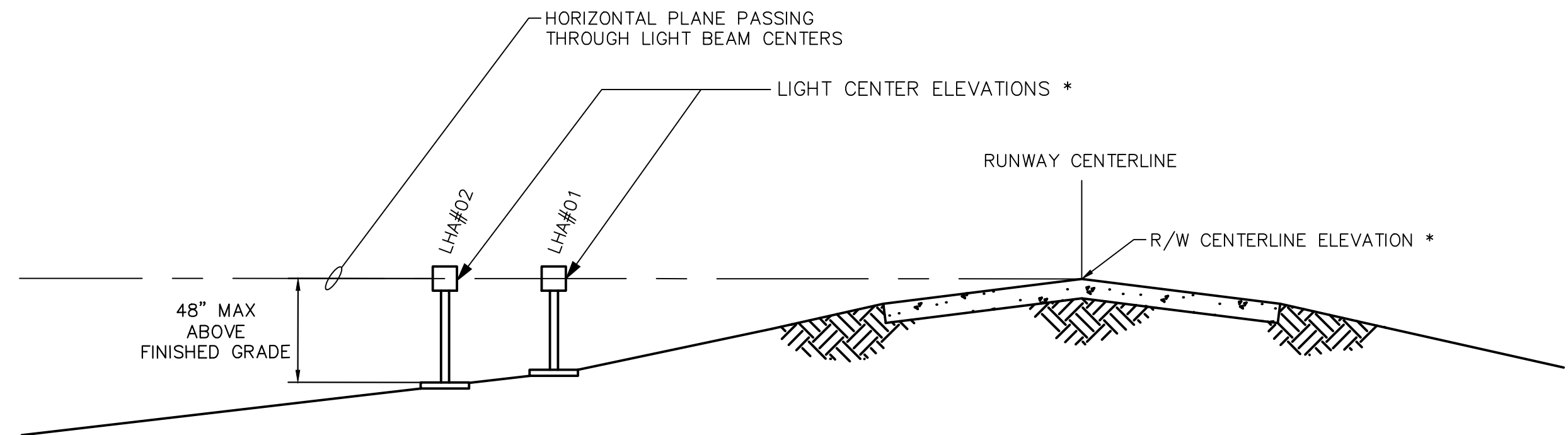
PROJECT NUMBER:
168001006

| PAPI SITING CRITERIA SCHEDULE | | | | | | | | | | |
|-------------------------------|-------------------------|--|--|--|---|--|------------------|--|---|-----|
| R/W | DISTANCE FROM THRESHOLD | D1 DIST. FROM R/W TO C OF LHA #1 (FT) | D2 LIGHT HOUSING ASSEMBLY (LHA) SPA. O.C. (FT.) | R/W CL. ELEV. @ PAPI SITING STA (FT)** | LIGHT CENTER ELEVATION (FT) | AIMING ANGLE BASED ON 3° 00' GLIDE PATH* | | EFFECTIVE VISUAL THRESHOLD CROSSING HEIGHT (TCH) | THRESHOLD R/W CENTERLINE ELEVATION (FT)** | R/W |
| 18 | 894.90' | 67'-6" | 20'-0" | 667.9' | 668.9 (NOT TO EXCEED 1' ABOVE RUNWAY CL) | LHA#01 3° 15' | LHA#02 2° 45' | 40.0' | 674.90' | 18 |

SCHEDULE NOTE:
* COORD W/ AIRPORT FOR AIMING ANGLES PRIOR INSTALLING PLACARD AFTER FLIGHT TESTING.
** CONTRACTOR TO VERIFY ELEVATIONS IN THE FIELD PRIOR TO PAPI INSTALLATION.



1 PAPI SITE PLAN
NOT TO SCALE

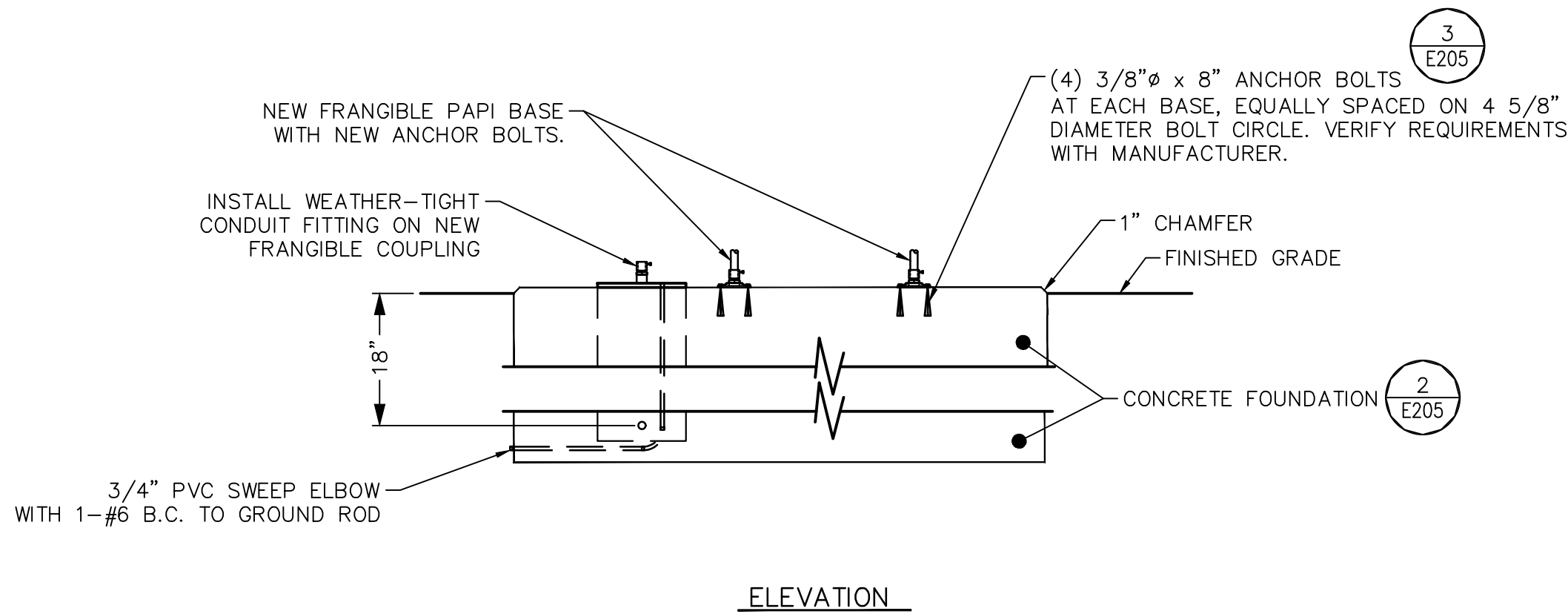
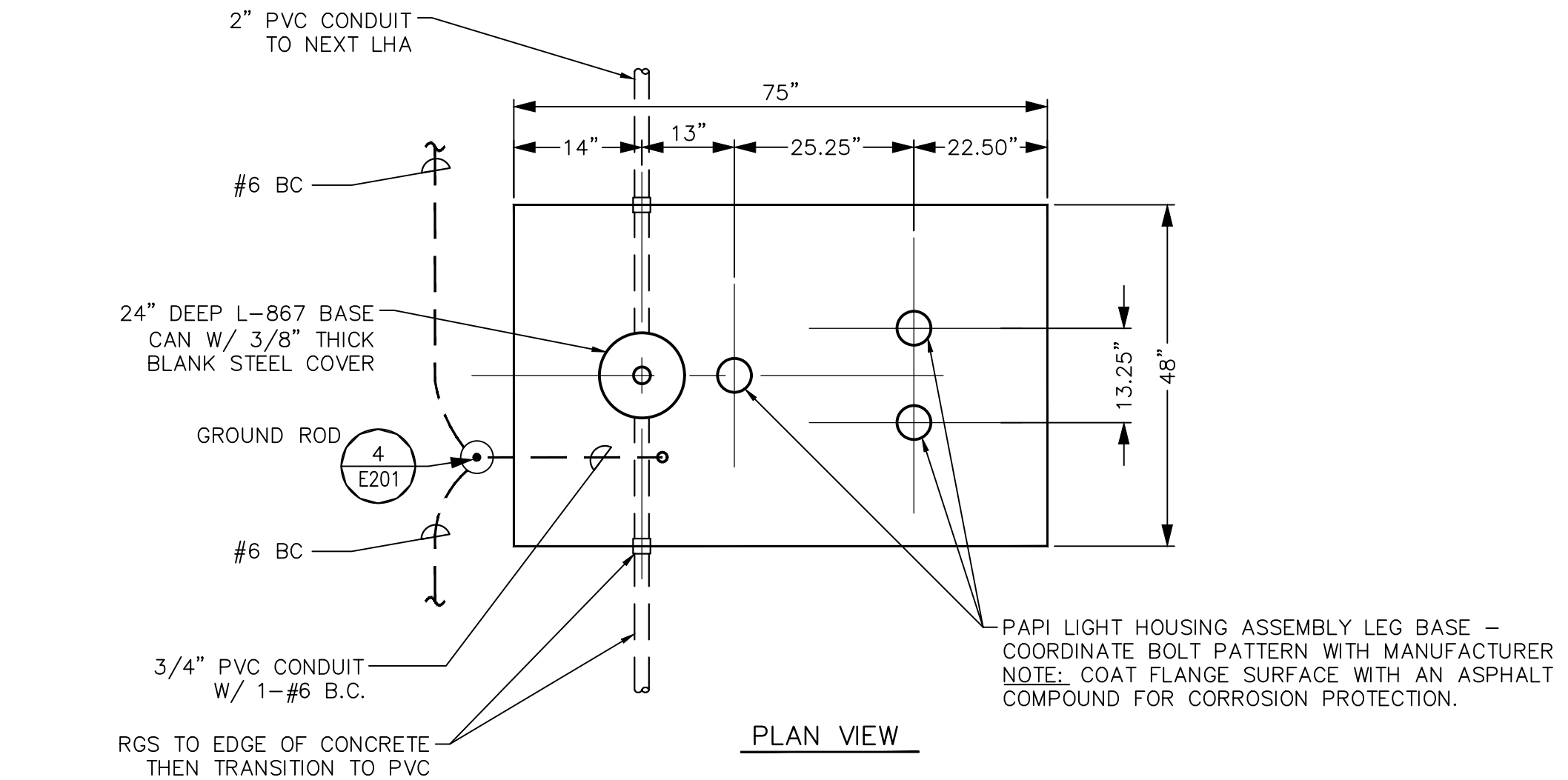


NOTES:
1. SEE PAPI CRITERIA SCHEDULE FOR THESE * DATA.

2 TYPICAL PAPI SECTION
NOT TO SCALE



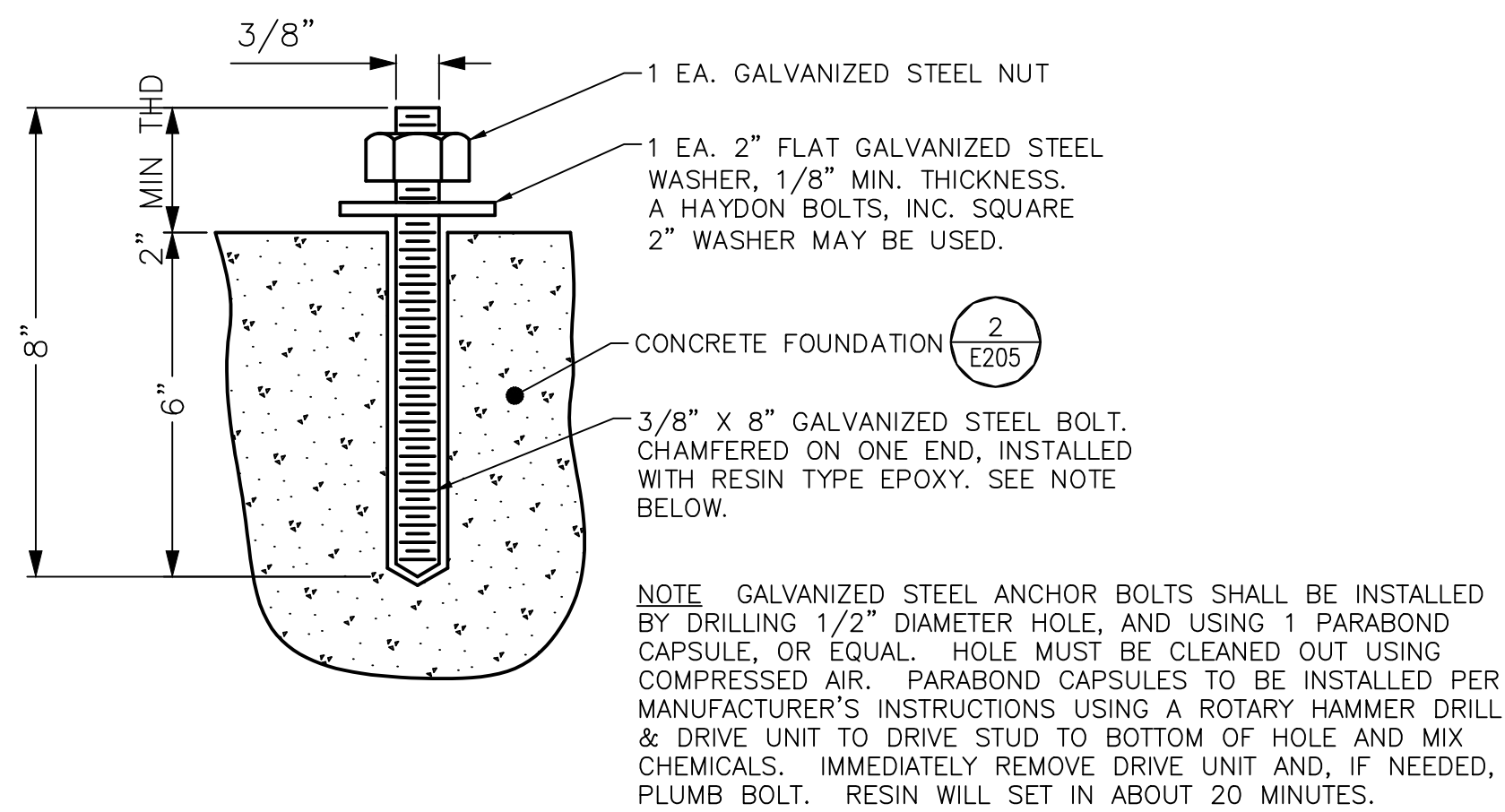
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NOTE: FINISH TOP OF CONCRETE BASE WITH 1/4"/FT. SLOPE, BOTH DIRECTIONS FROM LONGITUDINAL AXES.

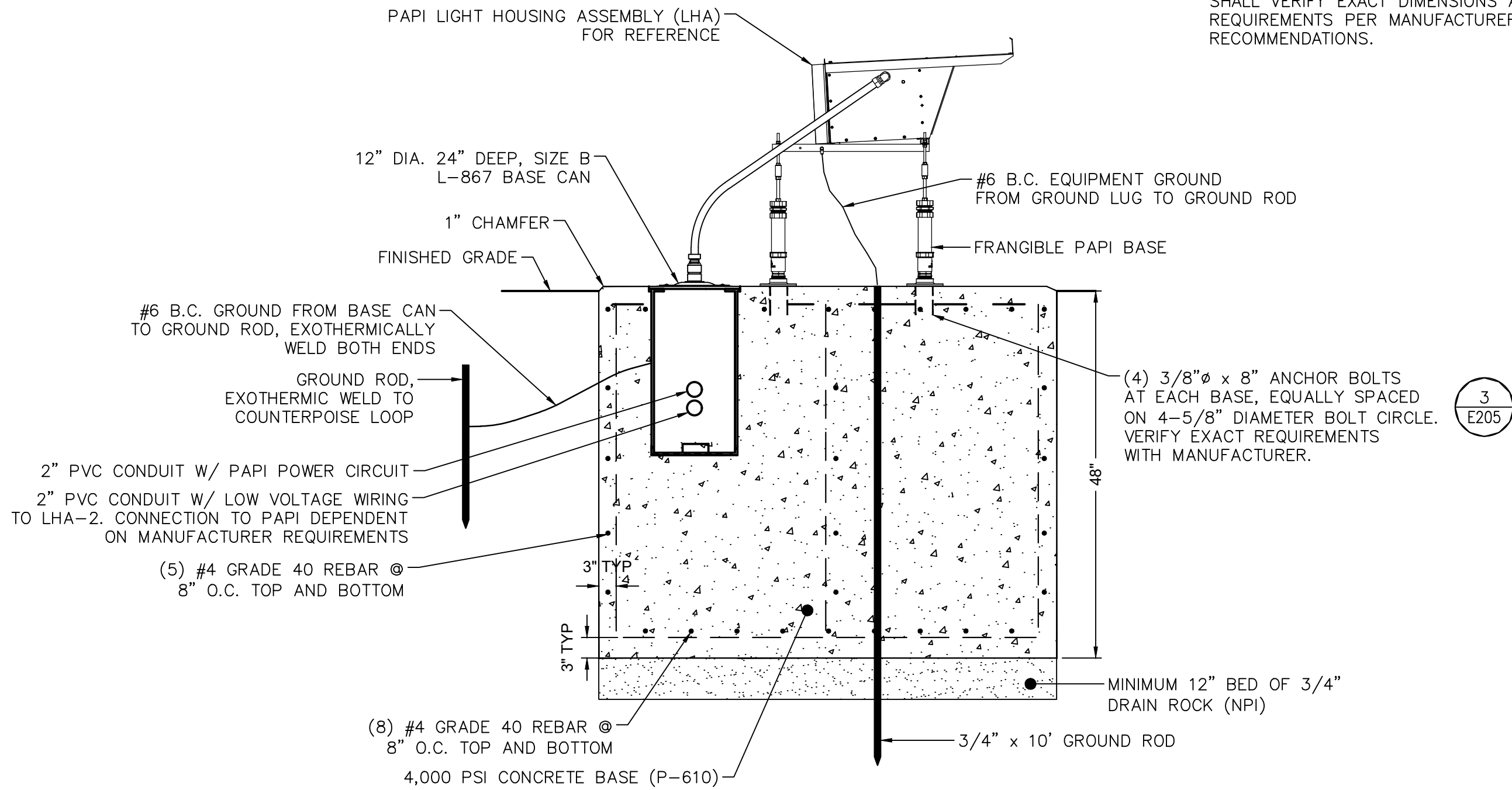
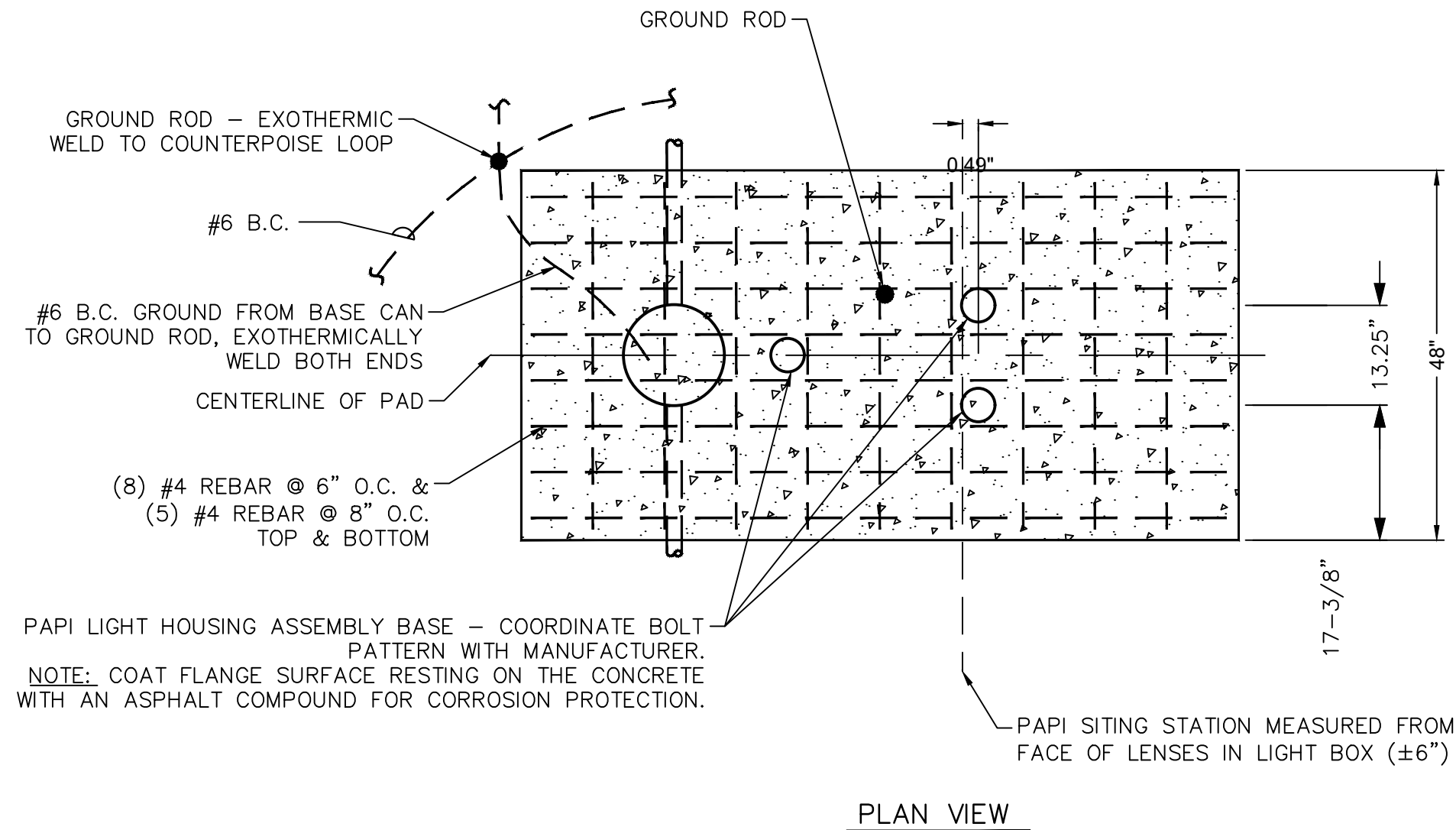
1 PAPI LHA INSTALLATION

SCALE: NTS



3 CHEMICAL ANCHOR BOLT DETAIL

SCALE: NTS



NOTE: FINISH TOP OF CONCRETE BASE WITH 1/4"/FT. SLOPE, BOTH DIRECTIONS FROM LONGITUDINAL AXIS.

2 PAPI LHA CONCRETE FOUNDATION DETAILS

SCALE: NTS



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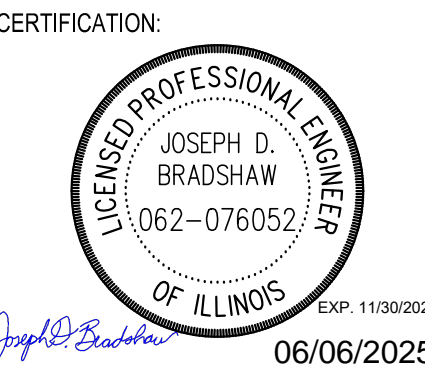
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TITLE: PAPI FOUNDATION AND INSTALLATION DETAILS

PROJECT: INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS



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| DRAWING NO. |
| E205 |
| PROJECT NUMBER: |
| 168001006 |

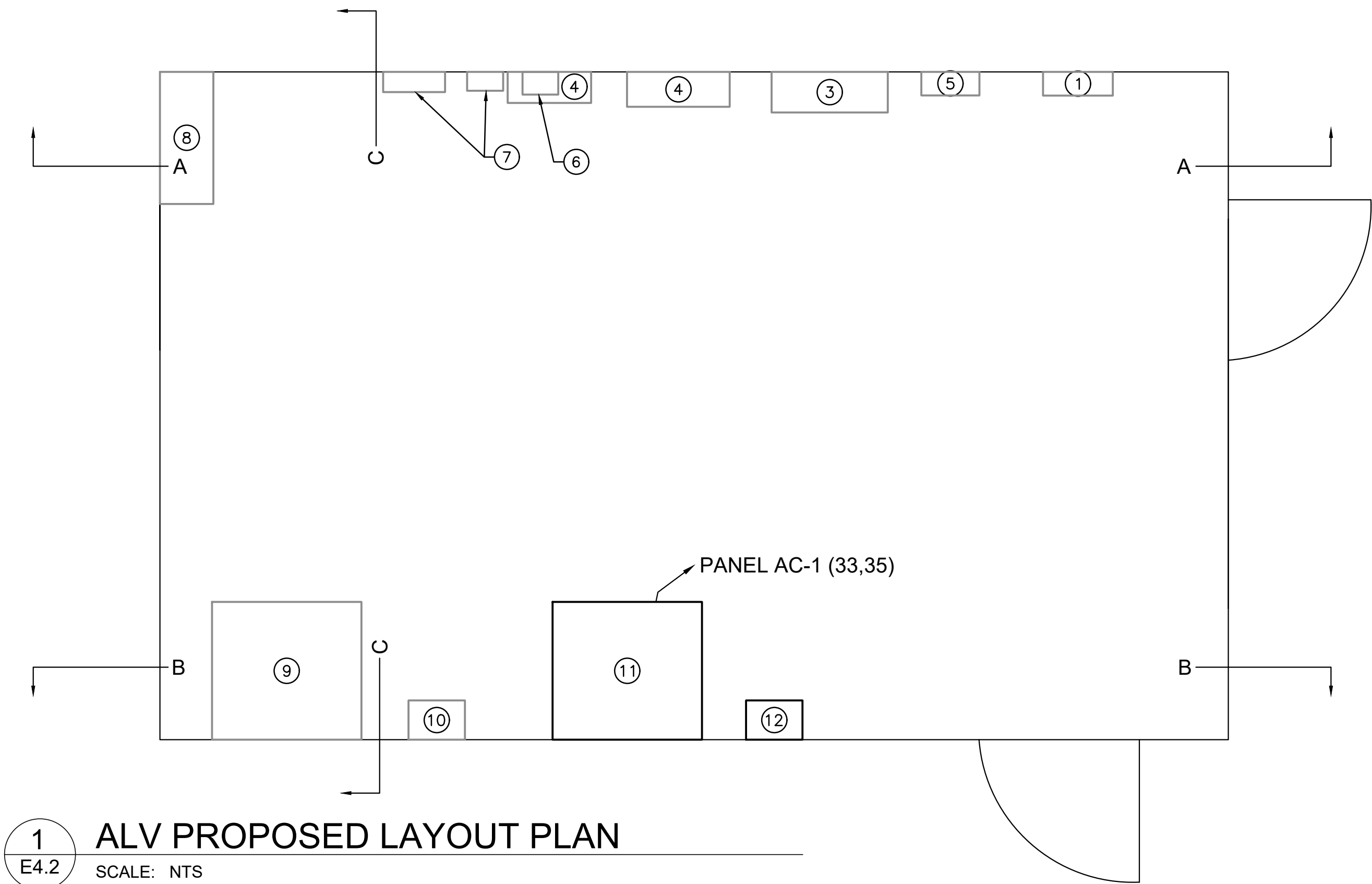
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| EQUIPMENT SCHEDULE | |
|--------------------|---|
| SYM | DESCRIPTION |
| ① | (EX) HEATER 1 |
| ② | (EX) RECEPTACLE |
| ③ | (EX) 200A, 120/240V, 1 PHASE, 3 WIRE PANEL 'AC-1' |
| ④ | (EX) L-821 LIGHTING CONTROL PANEL |
| ⑤ | (EX) PANEL 'LA1' |
| ⑥ | (EX) L-854 PILOT CONTROLLED RADIO |
| ⑦ | (EX) FIRE EQUIPMENT |
| ⑧ | (EX) FAN |
| ⑨ | (EX) L-828 6.6A, 10KW CONSTANT CURRENT REGULATOR |
| ⑩ | (EX) REGULATOR SERIES CIRCUIT CUTOUT |
| ⑪ | (N) L-828 240V, 6.6A, 5KW CONSTANT CURRENT REGULATOR FOR NEW PAPI CIRCUIT |
| ⑫ | (N) REGULATOR SERIES CIRCUIT CUTOUT |

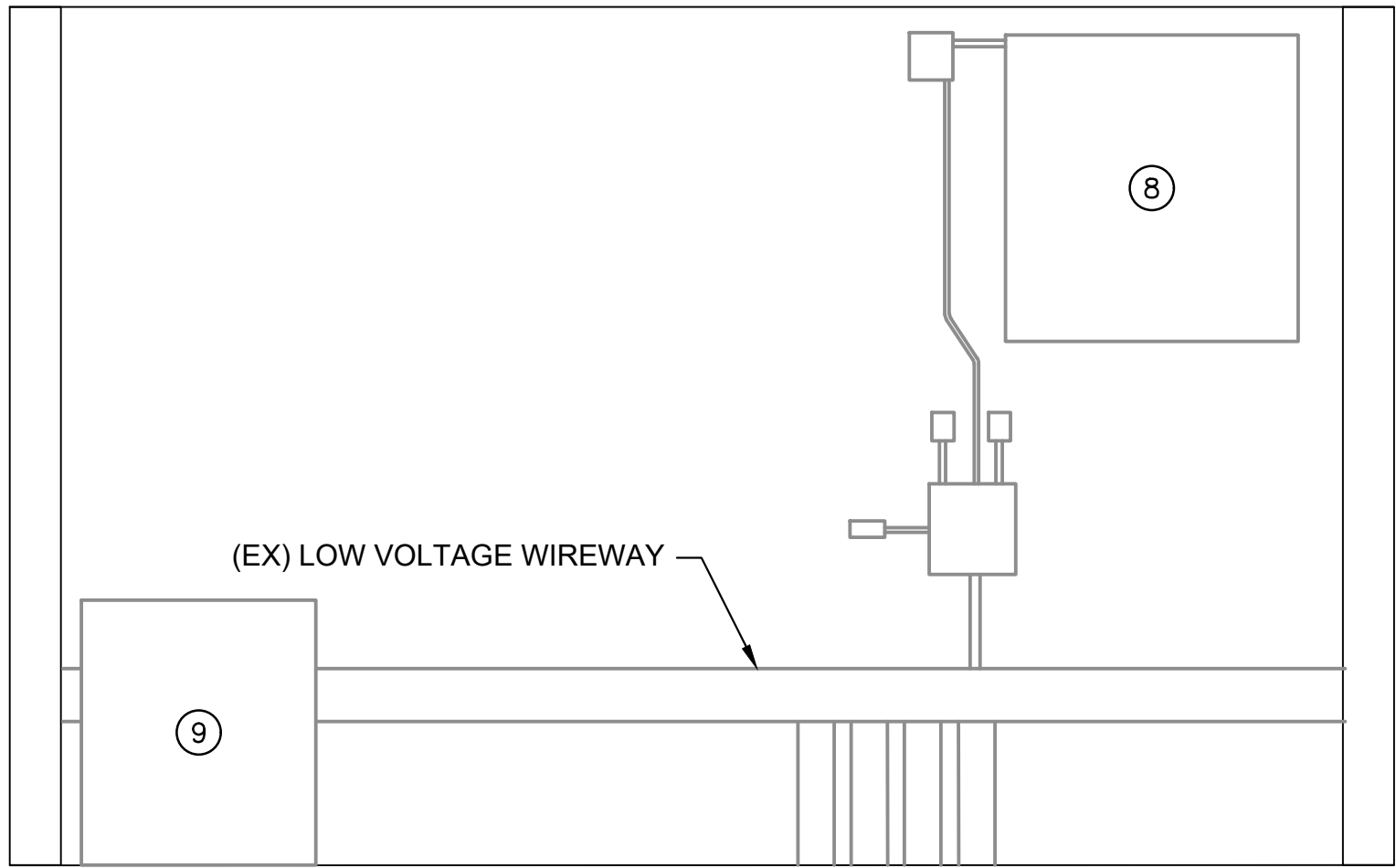
CONTRACTOR TO INSTALL NEW BREAKERS AS REQUIRED IN PANEL 'AC-1' TO ACCOMMODATE NEW CCR.

GENERAL CONSTRUCTION NOTES:

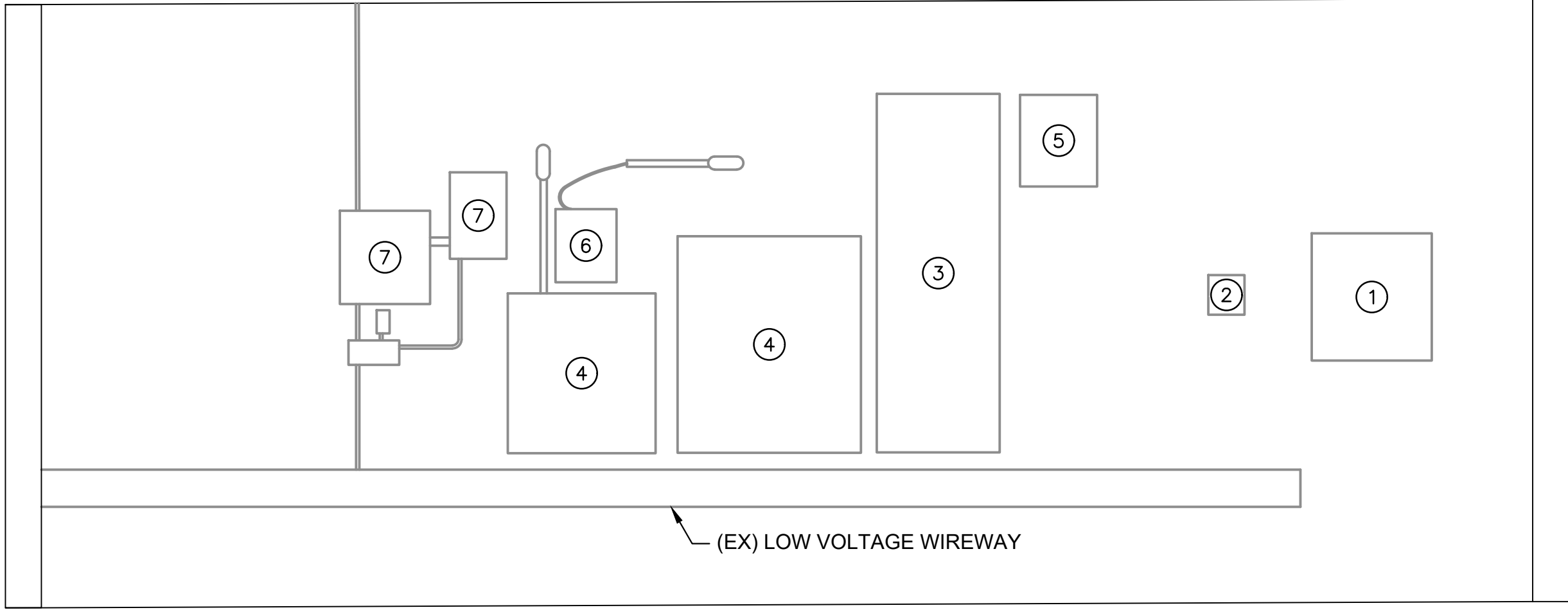
- RE-ESTABLISH ALL CONNECTIONS FOR RELOCATED EQUIPMENT TO ENSURE A COMPLETE AND WORKING SYSTEM.
- ALL NEW EQUIPMENT AND WORK REQUIRED FOR THE INSTALLATION OF THE NEW L-828 REGULATOR, INCLUDING NEW SERIES CIRCUIT CUTOUT, WIRES, BREAKERS, AND REGULATOR TESTING AND COMMISSIONING, IS INCIDENTAL TO THE REGULATOR BID ITEM AR801717.
- LAYOUT NOT TO SCALE. VERIFY ALL DIMENSIONS PRIOR TO EQUIPMENT INSTALLATION.



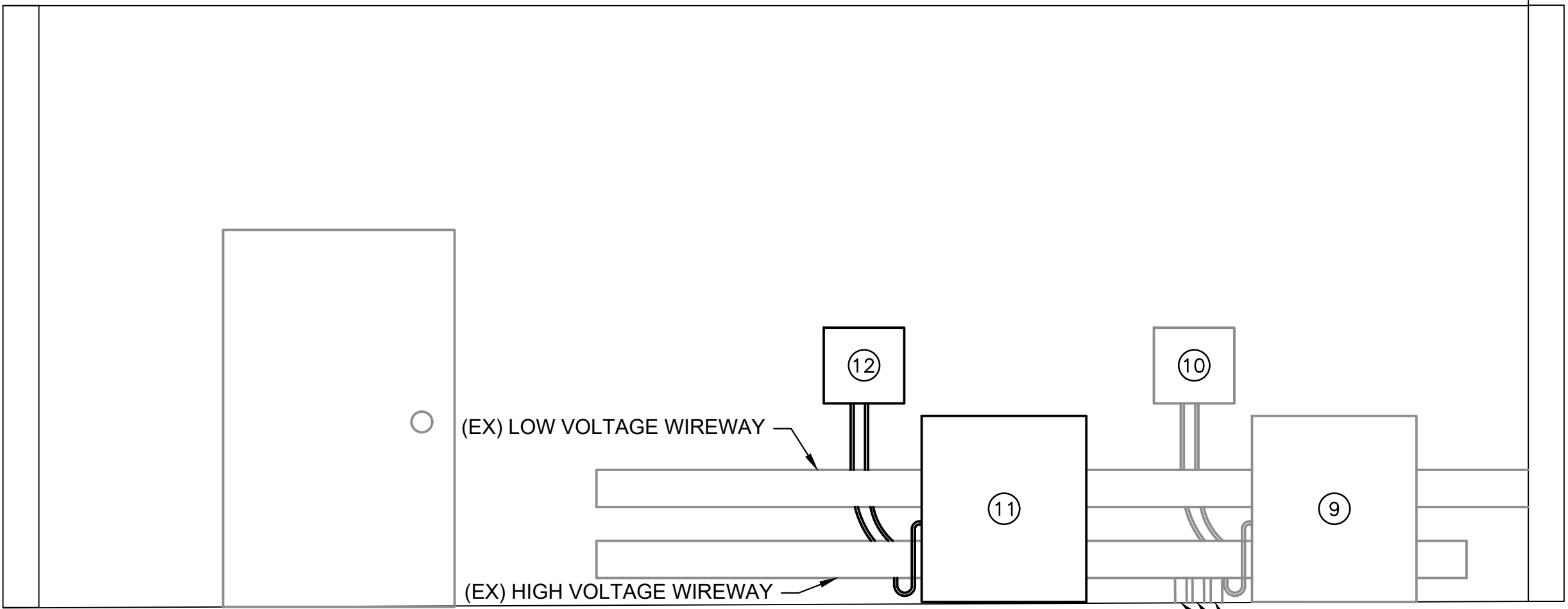
1
E4.2 ALV PROPOSED LAYOUT PLAN
SCALE: NTS



SECTION C-C



SECTION A-A



SECTION B-B

| PANEL NAME: AC-1 | | | | | | | | | | | | | |
|----------------------|------------------------|-----------|------|-----------------|--------|----|--------|---------------------|-------|------|-----------|------------------------|-----|
| LOCATION: ELEC VAULT | | | | VOLTS: 120/240V | | | | AIC RATING: 42K | | | | | |
| SUPPLY FROM: SES | | | | PHASES: 1 | | | | MAINS TYPE: MCB | | | | | |
| MOUNTING: SURFACE | | | | WIRES: 3 | | | | MAINS RATING: 200 A | | | | | |
| ENCLOSURE: NEMA 3R | | | | | | | | MCB RATING: 200 A | | | | | |
| CKT | CIRCUIT DESCRIPTION | LOAD TYPE | TRIP | POLES | A (VA) | | B (VA) | | POLES | TRIP | LOAD TYPE | CIRCUIT DESCRIPTION | CKT |
| 1 | (E) INTERIOR LIGHTS | L | 15 | 1 | EX | EX | | | 2 | 60 | E | (E) LIGHTNING ARRESTOR | 2 |
| 3 | (E) EXTERIOR LIGHTS | L | 15 | 1 | | | EX | EX | | | | | 4 |
| 5 | (E) INTERIOR RECEPT. | R | 20 | 1 | EX | EX | | | 2 | 25 | E | (E) HEATER EH1 | 6 |
| 7 | (E) EXHAUST FAN | E | 20 | 1 | | | EX | EX | | | | | 8 |
| 9 | (E) CONTROL POWER | E | 15 | 1 | EX | EX | | | 2 | 25 | E | (E) HEATER EH2 | 10 |
| 11 | (E) FACP | O | 20 | 1 | | | EX | EX | | | | | 12 |
| 13 | (E) SPARE | | 15 | 1 | EX | EX | | | 2 | 80 | | (E) SPARE | 14 |
| 15 | (E) GFI NORTH EXTERIOR | R | 20 | 1 | | | EX | EX | | | | | 16 |
| 17 | (E) SPARE | | 25 | 1 | EX | | | | | | | | 18 |
| 19 | (E) SPARE | | 30 | 1 | | | | | | | | | 20 |
| 21 | (E) SPARE | | 20 | 2 | | | | | | | | | 22 |
| 23 | | | | | | | | | | | | | 24 |
| 25 | (E) SPARE | | 20 | 2 | | | | | | | | | 26 |
| 27 | | | | | | | | | | | | | 28 |
| 29 | (E) SPARE | | 20 | 2 | | | | | | | | | 30 |
| 31 | | | | | | | | | | | | | 32 |
| 33 | (N) REGULATOR PAPI | E | 40 | 2 | 330 | | | | | | | | 34 |
| E | | 36 | | | | | | | | | | | |
| E | | 38 | | | | | | | | | | | |
| 37 | (E) REGULATOR RUNWAY | | 80 | 2 | EX | EX | | | | | | | 40 |
| 39 | | | | | | | EX | EX | | | | | 42 |
| 41 | | | | | | | | | | | | | 40 |
| TOTAL LOAD: | | | | | 330 VA | | 330 VA | | | | | | |
| TOTAL AMPS: | | | | | 1.4 A | | 1.4 A | | | | | | |

PANEL 'AC-1' PANEL SCHEDULE

Kimley»Horn

4201 WINFIELD ROAD
SUITE 600
WARRENVILLE, IL 60555
TEL (630) 487-5550

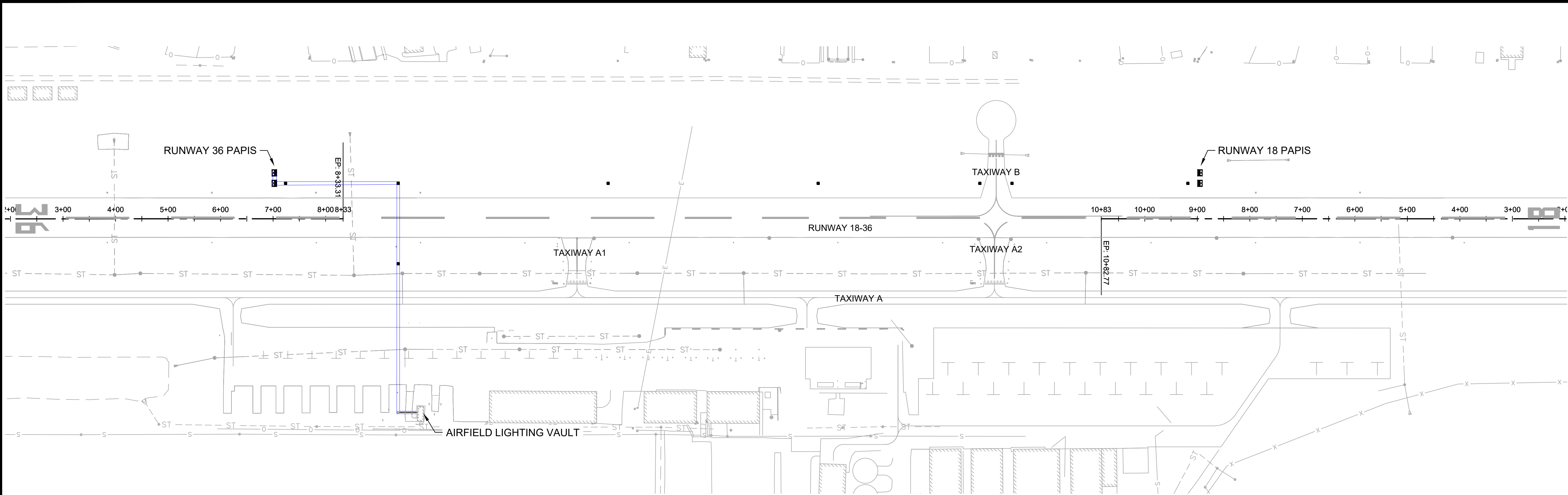


| | | |
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| DATE: 06/06/2025 | | SCALE: N/A |
| DRAWN BY: DKP | | CHECKED BY: JDB |
| REV. | DATE | DESCRIPTION |
| 0 | 06/06/2025 | ISSUED FOR BID |

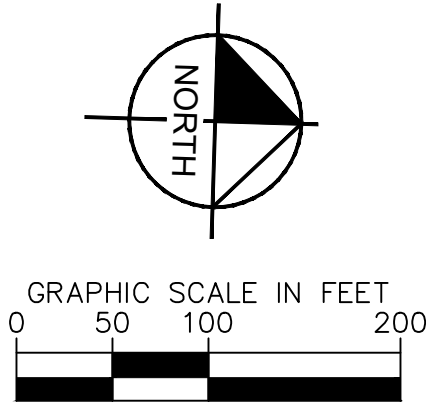
TITLE: ELECTRICAL VAULT PLAN
PROJECT: INSTALL PRECISION APPROACH PATH INDICATOR (PAPI) FOR RUNWAY 18/36 VILLAGE OF BOLINGBROOK, WILL COUNTY, ILLINOIS

| |
|-----------------|
| CERTIFICATION: |
| |
| DRAWING NO. |
| E301 |
| PROJECT NUMBER: |
| 168001006 |

\\chsf01\my_chs\GIS_Artion\168001006_105_Install_PAPI\Code\Plans\168001006_EC.dwg Plot Date: 6/6/2025 8:49 AM
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| DATA TABLE - CIRCUIT 'PAPI 18-36' | | | |
|--|------------|------------|--------------|
| QUANTITY | ITEM DESC. | FT./ITEM * | TOTAL LENGTH |
| 2 | PAPIs | x 10 ' = | 20' |
| 4 | HANDHOLE | x 20 ' = | 80' |
| * SLACK CABLE PER SPECS | | | |
| ITEM TOTAL LENGTH = 100' | | | |
| + PLAN CIRCUIT LENGTH = 1,465' | | | |
| = TOTAL CIRCUIT LENGTH = 1,565' | | | |
| CONDUCTOR TYPE: L-824 5KV AIRFIELD CABLING | | | |
| — INSTALLED IN THIS PROJECT (BASE BID) | | | |



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| | | |
|---------------------|-------------|----------------|
| DATE: 06/06/2025 | SCALE: | |
| DRAWN BY: | CHECKED BY: | |
| REV. | DATE | DESCRIPTION |
| 0 | 06/06/2025 | ISSUED FOR BID |

TITLE:
ELECTRICAL CIRCUIT MAP - BASE BID

PROJECT:
INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS

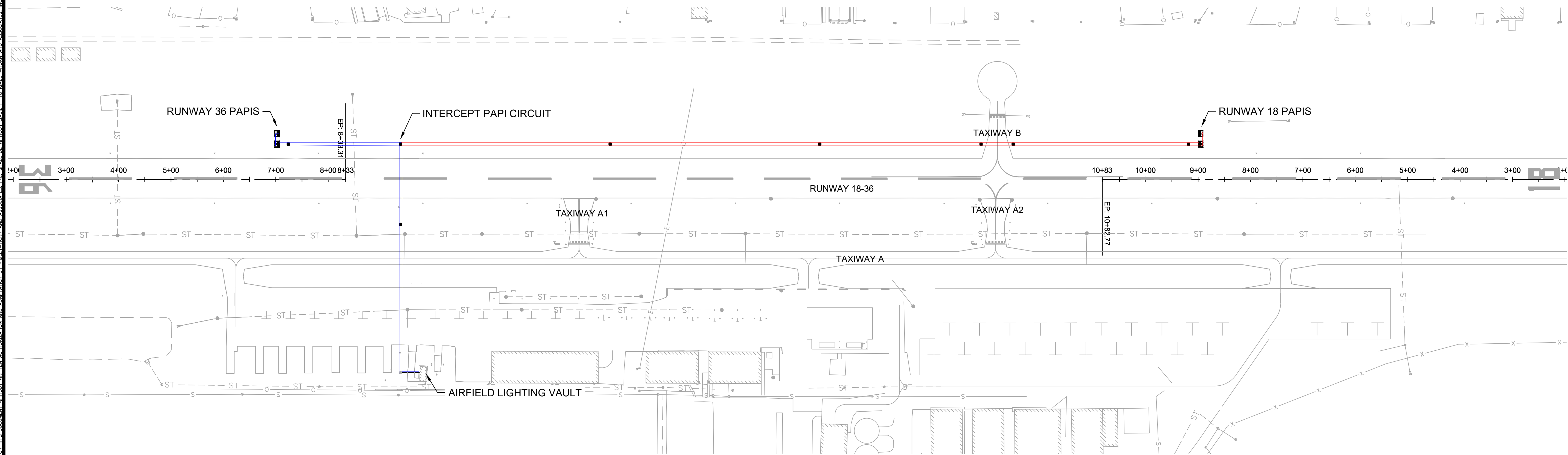
CERTIFICATION:

Joseph D. Bradshaw
06/06/2025

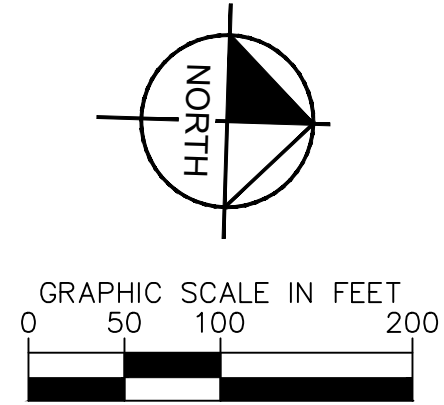
DRAWING NO.
E401

PROJECT NUMBER:
168001006

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| DATA TABLE - CIRCUIT 'PAPI 18-36' | | | |
|--|------------|------------|-----------------|
| QUANTITY | ITEM DESC. | FT./ITEM * | TOTAL LENGTH |
| <u>2</u> | PAPIS | x 10 ' | = <u>20'</u> |
| <u>6</u> | HANDHOLE | x 20 ' | = <u>120'</u> |
| * SLACK CABLE PER SPECS | | | |
| ITEM TOTAL LENGTH | | | = <u>140'</u> |
| + PLAN CIRCUIT LENGTH | | | = <u>3,105'</u> |
| = TOTAL CIRCUIT LENGTH | | | = <u>3,245'</u> |
| CONDUCTOR TYPE: L-824 5KV AIRFIELD CABLING | | | |
| <div style="border-bottom: 1px solid red; width: 100px; display: inline-block;"></div> INSTALLED IN THIS PROJECT (ALT 1) | | | |



811 Know what's below.
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| | | | |
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| DATE: 06/06/2025 | | SCALE: 1"= 100'-0" | |
| DRAWN BY: DKP | | CHECKED BY: JDB | |
| REV. | DATE | DESCRIPTION | |
| 0 | 06/06/2025 | ISSUED FOR BID | |

TITLE: ELECTRICAL CIRCUIT MAP - ALT 1

PROJECT: INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS

CERTIFICATION:



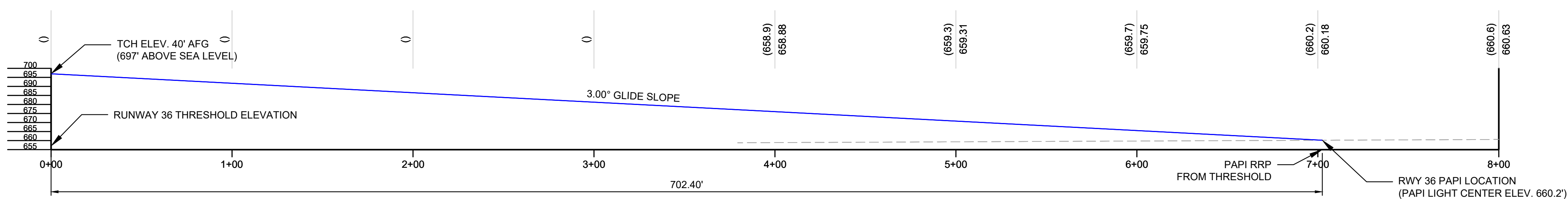
DRAWING NO.

E402

PROJECT NUMBER:
168001006

\\chsf601\my_chs\chis\action\168001006-105 Install PAPI\Coord\Plans\168001006-ELEV.dwg Plot: 6/6/2025 8:50 AM
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RELATIVE ELEVATION
ABOVE RUNWAY
FINISHED GRADE



LEGEND

RUNWAY CENTERLINE FINISHED GRADE PROFILE

VISUAL GLIDESLOPE

EFFECTIVE VISUAL THRESHOLD CROSSING HEIGHT

RUNWAY REFERENCE POINT

TCH

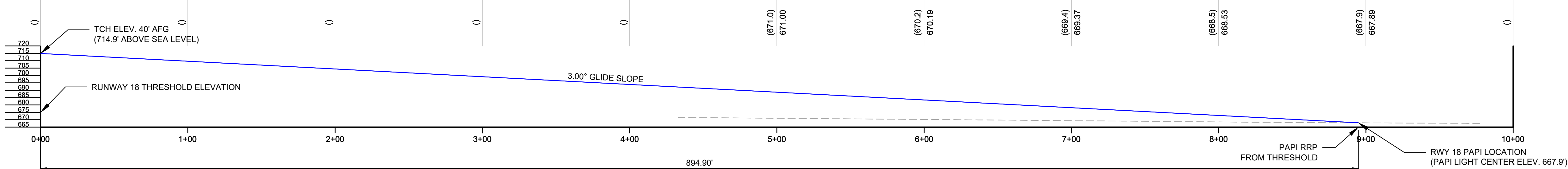
RRP

RUNWAY CENTERLINE SURFACE POINT ELEVATION

1
E501

RUNWAY 36 PAPI SITING ELEVATION

RELATIVE ELEVATION
ABOVE RUNWAY
FINISHED GRADE



2
E501

RUNWAY 18 PAPI SITING ELEVATION

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DATE:
06/06/2025

SCALE:
N/A

DRAWN BY:
DKP

CHECKED BY:
JDB

REV.

DATE

DESCRIPTION

0

06/06/2025

ISSUED FOR BID

TITLE:

PAPI SITING ELEVATIONS

PROJECT:
INSTALL PRECISION APPROACH PATH INDICATOR (PAPI)
FOR RUNWAY 18/36
VILLAGE OF BOLINGBROOK
BOLINGBROOK, WILL COUNTY, ILLINOIS

CERTIFICATION:



DRAWING NO.

E501

PROJECT NUMBER:

168001006

