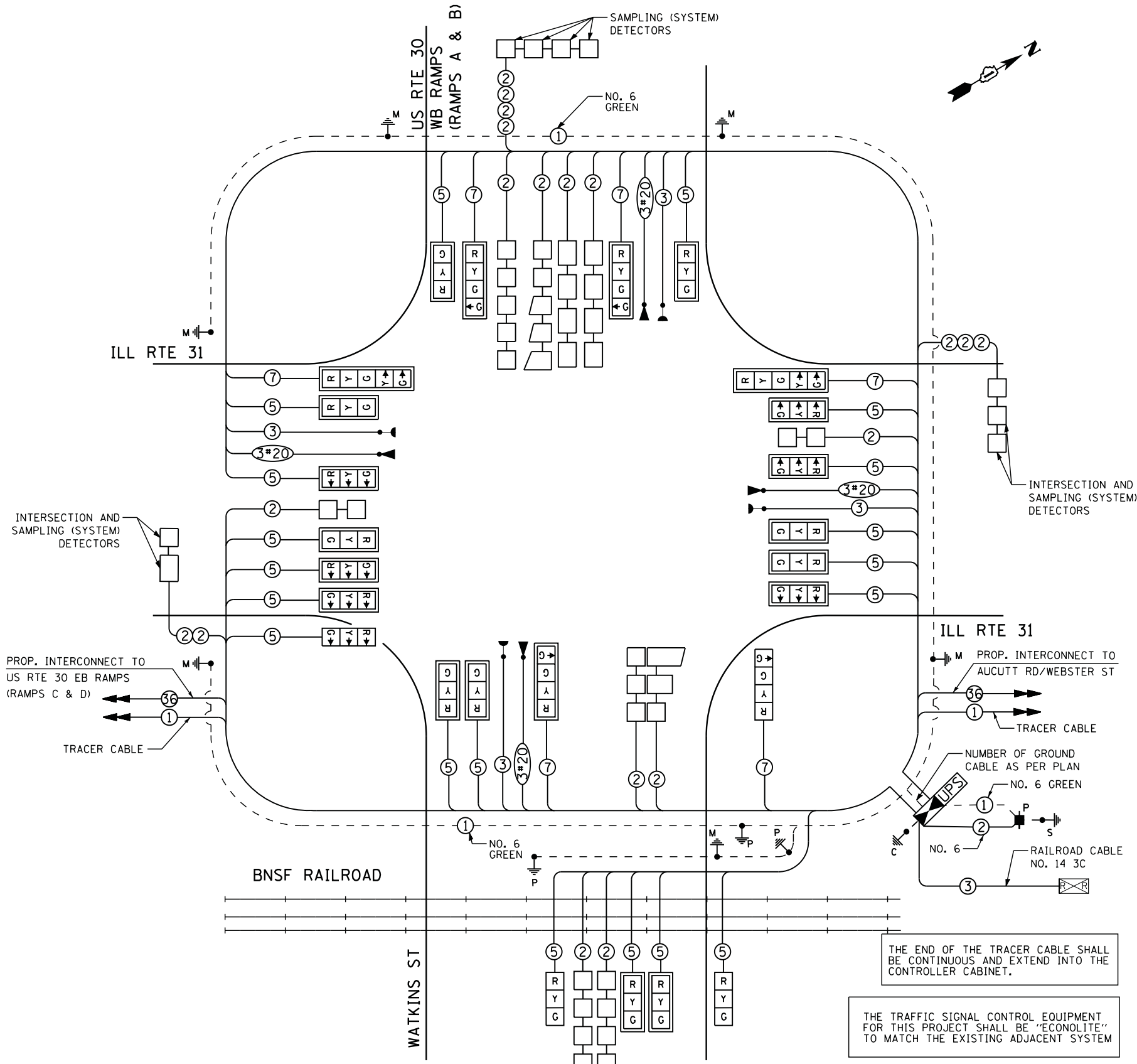


SCHEDULE OF QUANTITIES

QUANTITY	UNIT	ITEM
30	SO FT	SIGN PANEL - TYPE 1
25.0	SO FT	SIGN PANEL - TYPE 2
1	EACH	SERVICE INSTALLATION - POLE MOUNTED
1077	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
50	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.
101	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.
802	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
53	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 6" DIA.
4	EACH	HANDHOLE
7	EACH	HEAVY-DUTY HANDHOLE
2	EACH	DOUBLE HANDHOLE
1	EACH	RAILROAD, FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
1	EACH	TRANSCEIVER - FIBER OPTIC
* 1131	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
4638	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
1743	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
6748	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
64	FOOT	ELECTRIC CABLE IN CONDUIT, RAILROAD, NO. 14 3C
74	FOOT	ELECTRIC CABLE IN CONDUIT, NO. 6 2C
1052	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 9 FT.
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 12 FT.
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 15 FT.
2	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 50 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 58 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 60 FT. AND 34 FT.
12	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
30	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
67	FOOT	CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER
16	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
3	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
1	EACH	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, BRACKET MOUNTED
3	EACH	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, MAST ARM MOUNTED
2	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
21	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
19	EACH	INDUCTIVE LOOP DETECTOR
2048	FOOT	DETECTOR LOOP, TYPE I
* 4	EACH	LIGHT DETECTOR
* 1	EACH	LIGHT DETECTOR AMPLIFIER
* 1131	FOOT	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3C
1	L SUM	RAILROAD PROTECTIVE LIABILITY INSURANCE
1	EACH	UNINTERRUPTABLE POWER SUPPLY SPECIAL
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 66 FT. AND 32 FT.

* 100% VILLAGE OF MONTGOMERY/COUNTRYSIDE FIRE PROTECTION DISTRICT COST



I. D. O. T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE		% OPERATIONS	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	25	135	17	0.50	213
(YELLOW)	25	135	25	0.25	157
(GREEN)	29	135	15	0.25	109
ARROW	4	135	12	0.10	5
PED. SIGNAL	--	90	25	1.00	--
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN		252	25	0.05	--
VIDEO SYSTEM		150	-	1.00	--
TOTAL =					584

ENERGY COSTS- BILLED TO: IDOT - DISTRICT 1
 201 W. CENTER CT.
 SCHAUMBURG, IL 60196-1096

ENERGY SUPPLY - CONTACT LUCY ANDERBERG
 PHONE COMED
 815-724-5691

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM