

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**

**143RD STREET (FAU 1600) AT LEMONT ROAD (FAU 2612)
INTERSECTION RECONSTRUCTION**
PROJECT NO.: M-8003 (562)
SECTION NO.: 04-00003-00-CH
VILLAGE of HOMER GLEN
WILL COUNTY
JOB NO: C-91-136-06

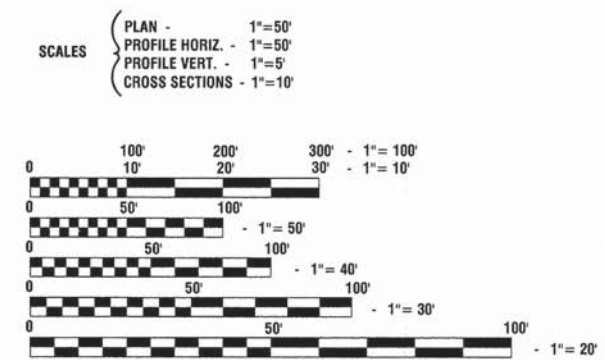
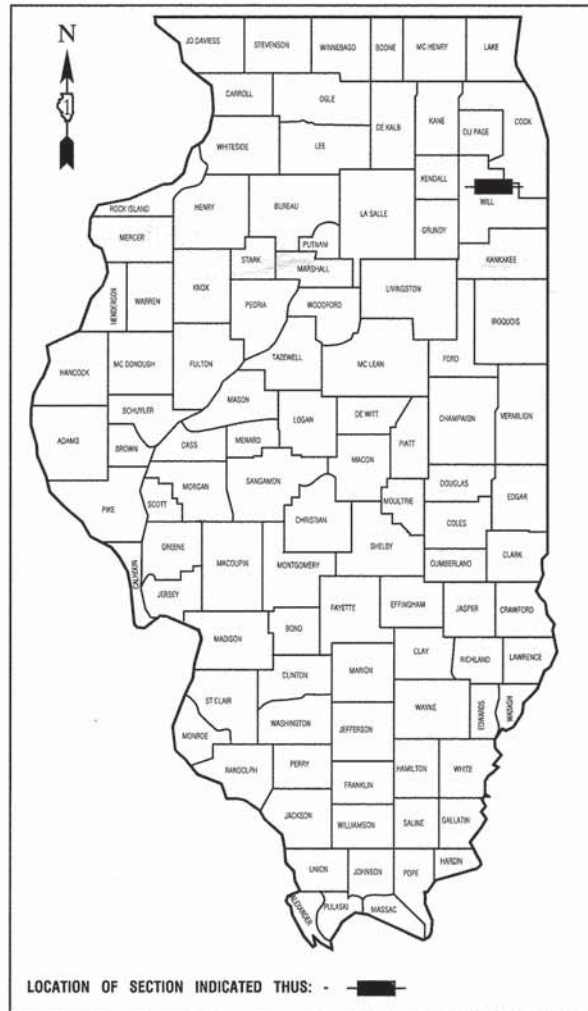
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|-----------------------|----------------|----------|-------------------------------|-----------|
| F. A. U. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 1 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT M-8003 (562) | |

CONTRACT #63147

INDEX OF SHEETS
SEE SHEET NO. 2

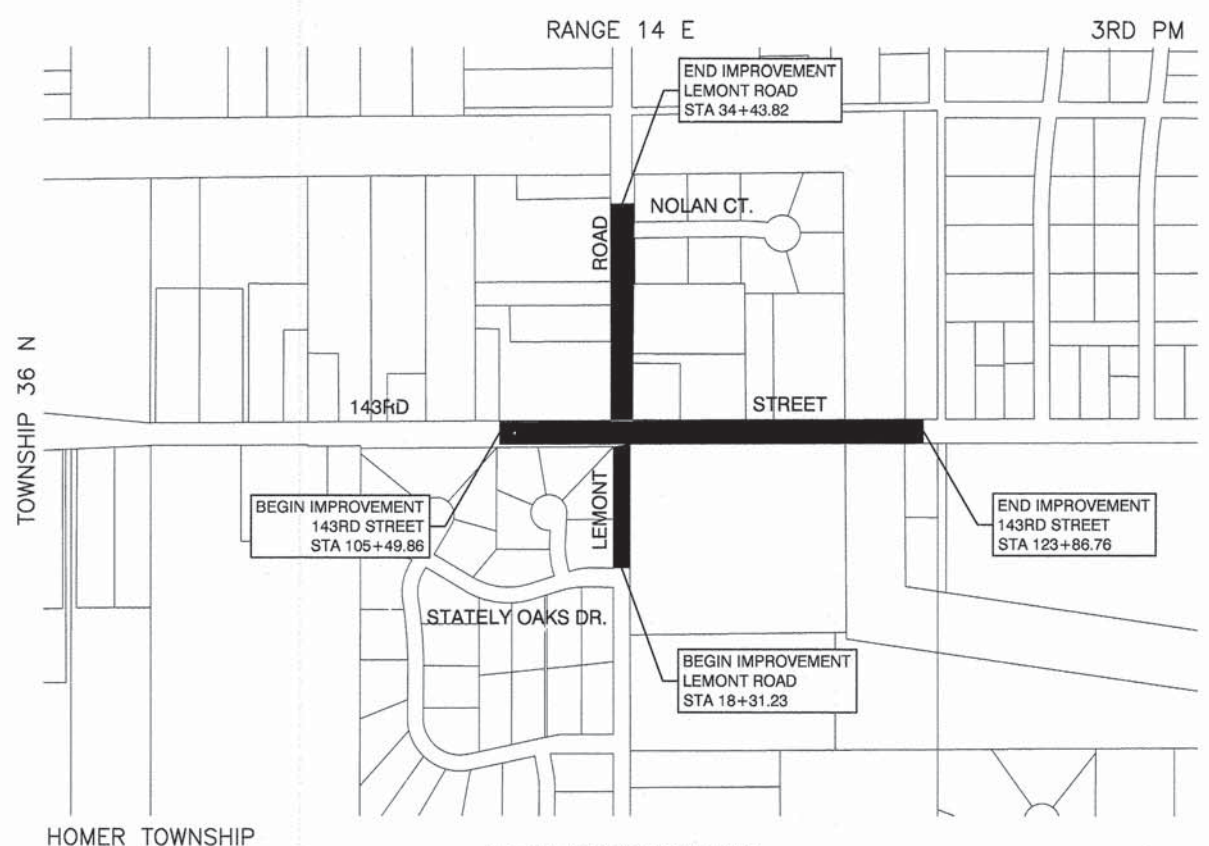
STATE STANDARDS
SEE SHEET NO. 2

| | |
|---|----------|
| DESIGN DESIGNATION - 143RD STREET ADT 19,000 (2030) - ARTERIAL - 2.01 | |
| 2012 ADT - | 15,800 |
| 2030 ADT - | 19,000 |
| POSTED SPEED LIMIT - | 45 mph |
| DESIGN PERIOD - | 20 YEARS |
| DESIGN SPEED LIMIT - | 45 mph |
| STREET CLASSIFICATION - | CLASS I |
| DESIGN DESIGNATION - LEMONT ROAD ADT 17,000 (2030) - COLLECTOR - 1.00 | |
| 2012 ADT - | 8,200 |
| 2030 ADT - | 17,000 |
| POSTED SPEED LIMIT - | 35 mph |
| DESIGN PERIOD - | 20 YEARS |
| DESIGN SPEED LIMIT - | 40 mph |
| STREET CLASSIFICATION - | CLASS II |



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J. U. L. I. E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 or 811



LOCATION MAP

- INDICATES PROPOSED IMPROVEMENT

GROSS LENGTH= 3449.5 FEET = 0.66 MILES
NET LENGTH= 3449.5 FEET =0.66 MILES

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

Approved: *J.P. [Signature]*
5/18/2013
President, Village of Homer Glen

Passed: *MAY 28, 2013*
C. [Signature]
District 1 Engineer of Local Roads & Streets

Released for Bid Based on Limited Review: *May 28 2013*
John [Signature]
Deputy Director of Highways, Region 1 Engineer

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PREPARED BY OR UNDER THE DIRECT SUPERVISION OF:
Harry L. Gilmore, Jr.
5/21/2013

LICENSE EXPIRES: 11/30/13

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. (847) 705-4406, SCHAMBURG, IL.
CONSULTANTS: ROBINSON ENGINEERING, LTD. 708-331-6700
CONTACT ENGINEER: HARRY L. GILMORE, JR.

INDEX OF SHEETS

| | |
|----------|--|
| 1. | TITLE SHEET |
| 2. | INDEX OF SHEETS AND HIGHWAY STANDARDS |
| 3. | GENERAL NOTES |
| 4.-6. | SUMMARY OF QUANTITIES |
| 7.-9. | TYPICAL CROSS SECTIONS |
| 10. | ALIGNMENT AND TIES |
| 11. | DETOUR PLAN |
| 12.-21. | SUGGESTED CONSTRUCTION STAGING |
| 22.-29. | PLAN AND PROFILE |
| 30.-37. | DRAINAGE AND UTILITIES |
| 38. | INTERSECTION GRADING |
| 39.-45. | PAVEMENT MARKING, SIGNING AND LANDSCAPING |
| 46.-47. | EROSION CONTROL (SWPPP) |
| 48.-52. | STREET LIGHTING PLANS |
| 53.-67. | TRAFFIC SIGNAL PLANS |
| 68.-81A. | DISTRICT 1 DETAILS (TC-10, TC-11, TC-13, TC-16, TC-21, TC-22, TC-26) |
| 82.-96. | CROSS SECTIONS |

HIGHWAY STANDARDS

| | |
|-----------|--|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 280001-07 | TEMPORARY EROSION CONTROL SYSTEMS |
| 482001-02 | HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT |
| 542301-03 | PRECAST REINFORCED CONCRETE FLARED END SECTION |
| 602001-02 | CATCH BASIN TYPE A |
| 602011-02 | CATCH BASIN TYPE C |
| 602301-03 | INLET - TYPE A |
| 602401-03 | MANHOLE TYPE A |
| 602601-02 | PRECAST REINFORCED CONCRETE FLAT SLAB TOP |
| 602701-02 | MANHOLE STEPS |
| 604001-03 | FRAME AND LIDS TYPE 1 |
| 604036-02 | GRATE TYPE 8 |
| 604091-02 | FRAME AND GRATE TYPE 24 |
| 606001-05 | CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER |
| 606301-04 | PC CONCRETE ISLANDS AND MEDIANS |
| 630001-10 | STEEL PLATE BEAM GUARDRAIL |
| 630301-06 | SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS |
| 631011-09 | TRAFFIC BARRIER TERMINAL, TYPE 2 |
| 635006-03 | REFLECTOR AND TERMINAL MARKER PLACEMENT |
| 701001-02 | OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15'(4.5m) AWAY |
| 701006-04 | OFF-RD OPERATIONS, 2L, 2W, 15'(4.5m) TO 24" (600m) FROM PAVEMENT EDGE |
| 701101-03 | OFF-RD OPERATIONS, MULTILANE, 15'(4.5m) TO 24" (600m) FROM PAVEMENT EDGE |
| 701201-04 | LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS \geq 45 MPH |
| 701301-04 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS |
| 701306-03 | LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY FOR SPEEDS \geq 45 MPH |
| 701311-03 | LANE CLOSURE, 2L, 2W, MOVING OPERATIONS DAY ONLY |
| 701326-04 | LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS \geq 45 MPH |
| 701421-05 | LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS \geq 45 MPH TO 55 MPH |
| 701426-05 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS \geq 45 MPH |
| 701501-06 | URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED |
| 701701-08 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 701901-02 | TRAFFIC CONTROL DEVICES |
| 720001-01 | SIGN PANEL MOUNTING DETAILS |
| 720006-03 | SIGN PANEL ERECTION DETAILS |
| 720011-01 | METAL POSTS FOR SIGNS, MARKERS & DELINEATORS |
| 728001-01 | TELESCOPING STEEL SIGN SUPPORT |
| 731001-01 | BASE FOR TELESCOPING STEEL SIGN SUPPORT |
| 805001-01 | ELECTRICAL SERVICE INSTALLATION DETAILS |
| 814001-02 | HANDHOLES |
| 814006-02 | DOUBLE HANDHOLES |
| 857001-01 | STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES |
| 862001-01 | UNINTERRUPTIBLE POWER SUPPLY |
| 877001-05 | STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55' |
| 878001-09 | CONCRETE FOUNDATION DETAILS |
| 880001-01 | SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION |
| 880006-01 | TRAFFIC SIGNAL MOUNTING DETAILS |
| 886001-01 | DETECTOR LOOP INSTALLATIONS |
| 886006-01 | TYPICAL LAYOUTS FOR DETECTION LOOPS |
| BLR 24-2 | MAILBOX TURNOUT FOR LOCAL ROADS |

FILE NAME = 04273_02-INDX-01 - 1-01

| | | |
|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISOR -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISION -- |
| PLOT SCALE = | DRAWN -- RG,MED | REVISION -- |
| PLOT DATE = 05-30-13 | CHECKED -- AG | REVISION -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
INDEX OF SHEETS AND STATE STANDARDS

| | | | | |
|--|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 2 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | CONTRACT NO. 63147 | |

SCALE: NONE SHEET NO. 2 OF 96 SHEETS STA. TO STA.

GENERAL NOTES

1. ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE THE LATEST STANDARDS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.
2. ITEMS OF WORK LISTED IN THE SUMMARY OF QUANTITIES WHICH ARE NOT SPECIFICALLY INDICATED IN THE PLANS SHALL BE PERFORMED AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. DRAINAGE STRUCTURE ELEVATIONS: GRADES OF SEWER LINES WERE DETERMINED FROM AVAILABLE PLANS AND SURVEYS. ACCORDINGLY, AS DIRECTED BY THE ENGINEER, THE INVERTS OF THE PROPOSED DRAINAGE WILL BE REVISED TO MEET EXISTING FIELD CONDITIONS.
4. THE TOP OF ALL STRUCTURES SHALL BE FLUSH WITH THE ADJACENT SURFACE OR AT THE INDICATED ELEVATIONS SHOWN ON THE PLANS. ALL RIM ELEVATIONS OF STRUCTURES IN THE PROPOSED CURB LINE ARE GIVEN TO THE EDGE OF PAVEMENT. ALL OTHER RIM ELEVATIONS ARE GIVEN TO THE CENTER OF THE STRUCTURES.
5. HALF TRAPS ARE TO BE OMITTED IN CATCH BASINS EXCEPT WHERE NOTED.
6. FRAME ELEVATIONS ARE GIVEN ONLY TO ASSIST IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE STRUCTURE COST.
7. WHEN, IN THE CONSTRUCTION OPERATION, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR OTHER DRAINAGE STRUCTURES SO THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH DAY BY THE CONTRACTOR AT HIS EXPENSE. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM ALL DIRT AND DEBRIS. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. HE SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS & SEWERS AND DISCHARGE SAME. HE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET, AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY SEWER CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWERS ARE BUILT AND IN SERVICE. THIS WORK SHALL NOT BE PAID FOR DIRECTLY, BUT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
9. THE CAST IRON FRAMES AND COVERS OF FILLED, ABANDONED OR REMOVED MANHOLES, INLETS AND CATCH BASINS OR THOSE FRAMES AND COVERS UPON STRUCTURES RECEIVING NEW FRAMES AND COVERS SHALL BE STOCKPILED WITHIN THE RIGHT-OF-WAY, AS DIRECTED BY THE ENGINEER, AND PICKED UP BY THE COMMUNITY.
10. THE APPROXIMATE LOCATION OF KNOWN PUBLIC UTILITIES ARE SHOWN ON THE PLANS. HOWEVER, THE DEPARTMENT DOES NOT GUARANTEE ITS ACCURACY. PRIOR TO COMMENCING OPERATIONS ON THE PROJECT WHICH MAY IN ANY WAY CREATE THE POSSIBILITY OF INVOLVEMENT WITH EXISTING UTILITIES, THE CONTRACTOR SHALL CONTACT THE FIRM (OR COMMUNITY) INVOLVED. ADJUSTMENT OF ALL PUBLIC UTILITIES WITHIN THE LIMITS OF THIS IMPROVEMENT WILL BE DONE BY THE RESPECTIVE OWNERS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED DUE TO DELAYS OR INCONVENIENCE CAUSED BY THESE ADJUSTMENTS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY LOCATIONS OF UNDERGROUND INSTALLATION BEFORE STARTING CONSTRUCTION OPERATIONS.
11. ALL TRENCHES WITHIN 2 FEET OF PROPOSED PAVEMENT, DRIVEWAYS, AND SIDEWALKS SHALL BE BACKFILLED WITH TRENCH BACKFILL ONLY.
12. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 SHALL BE INSTALLED WITH A THICKNESS EQUAL TO DEPTH OF HMA PAVEMENT WITH A MIN. OF 9".
13. THE CONTRACTOR SHALL PROTECT ALL TREES WITHIN AND ADJACENT TO THE CONSTRUCTION SITE DURING THE CLEARING AND SUBSEQUENT CONSTRUCTION OPERATIONS IN ACCORDANCE WITH SECTION 201 OF THE STANDARD SPECIFICATIONS. THOSE TREES TO BE REMOVED AS SHOWN IN THE PLANS SHALL BE DONE IN ACCORDANCE WITH SECTION 201 AND 202 OF THE STANDARD SPECIFICATIONS.
14. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).
15. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.
16. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON VILLAGE PROPERTY WITHOUT WRITTEN PERMISSION FROM ENGINEER AND THE VILLAGE.

17. ALL STORM SEWERS FOR THIS PROJECT SHALL BE REINFORCED CONCRETE PIPES (RCP) AND BE FURNISHED AND INSTALLED WITH RUBBER GASKETS IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR STORM SEWERS AS SPECIFIED.
18. THE CONTRACTOR SHALL CONTACT THE ASSISTANT WILL COUNTY ENGINEER MR. JEFF RONALDSON (815) 727-8476 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK. THE CONTRACTOR SHALL ALSO CONTACT ROBINSON ENGINEERING (708) 331-6700, THE VILLAGE OF HOMER GLEN DEVELOPMENT SERVICES DIRECTOR MR. MICHAEL SALAMOWICZ (708) 301-0632 AND THE HOMER TOWNSHIP HIGHWAY COMMISSIONER MR. MIKE DEVIVO (708) 301-0246 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
19. ALL HMA PAVING SHALL FOLLOW DESIGNATED DRIVING LANES AS SHOWN IN STRIPING DETAILS. NO LONGITUDINAL PAVING JOINT OR SEAMS ARE ALLOWED WITHIN THE DRIVING LANES. ALL LONGITUDINAL PAVING JOINTS OR SEAMS WILL BE BETWEEN THE DRIVING LANES.
20. AGGREGATE SUBGRADE IMPROVEMENT HAS BEEN PROVIDED FOR LOCATIONS WHERE SOILS TEND TO BE UNSTABLE WHEN WET. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH AGGREGATE SUBGRADE IMPROVEMENT WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER (BY USE OF A CONE PENETROMETER IN CONJUNCTION WITH THE IDOT SUBGRADE STABILITY MANUAL). IF UNSTABLE AND/OR UNSUITABLE MATERIALS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
21. ALL PAVEMENT, CURB AND SIDEWALK REMOVALS SHALL BE MADE BY MEANS OF STRAIGHT SAW CUT JOINT. THE COST FOR SAW CUTTING SHALL BE INCIDENTAL TO THE CONTRACT.
22. CONTRACTOR SHALL BE RESPONSIBLE AT ALL TIMES FOR TRAFFIC CONTROL AND PROTECTION IN ACCORDANCE WITH THE IDOT STANDARD SPECIFICATIONS ADOPTED JANUARY 1, 2012, THE LATEST EDITION OF THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, AND THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS.
23. THE CONTRACTOR SHALL MAKE EVERY ATTEMPT NOT TO DAMAGE EXISTING TREES.
24. 10' TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURB AND GUTTER OR TO TAPER FROM 6" TO 0", UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEM OF WORK SPECIFIED.
25. ALL STORM SEWERS, UNLESS OTHERWISE NOTED, SHALL CONFORM TO THE STATE SPECIFICATIONS FOR REINFORCED CONCRETE CULVERT, STORM DRAIN AND SEWER PIPE A.A.S.H.T.O. DESIGNATION M170 (A.S.T.M. DESIGNATION C76), WITH A MINIMUM OF CLASS III.
26. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS THROUGHOUT THE RECONSTRUCTION LIMITS AT ALL TIMES. IF DRIVEWAY ACCESS MUST BE RESTRICTED, THE CONTRACTOR SHALL NOTIFY THE RESIDENT IN WRITING 24 HOURS IN ADVANCE.
27. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.
28. EROSION CONTROL WORK ITEMS ARE CONSIDERED TO BE HIGH PRIORITY ITEMS ON THIS CONTRACT. THE ENGINEER WILL IMPLEMENT ALL PROVISIONS OF THE SPECIFICATION NECESSARY TO ASSURE THAT EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY WAY. ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES.

29. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
30. ADVANCE NOTICE TO THE TRAVELING PUBLIC SHALL BE INSTALLED 10 DAYS PRIOR TO THE START OF WORK. CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED ON BOTH 143RD STREET AND LEMONT ROAD IN EACH DIRECTION WITH WORDING AS DIRECTED BY THE ENGINEER.
31. NO TREE REMOVAL SHALL OCCUR UNTIL THE CONTRACTOR AND ENGINEER AGREE, ON SITE, ON THE REMOVAL QUANTITIES.
32. AREAS DISTURBED BY CONSTRUCTION SHALL BE KEPT TO A MINIMUM. ALL AREAS DISTURBED UNNECESSARILY SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE.
33. GRANULAR BACKFILL FOR MEDIANS WILL NOT BE PAID FOR SEPARATELY BUT BE INCLUDED IN PAY ITEM FOR P.C.C. MEDIAN SURFACE.
34. UTILITIES INDICATED ON THE PLANS ARE PROVIDED FOR THE CONTRACTORS USE AND ARE BASED UPON INFORMATION AVAILABLE AT THE TIME OF THE ADVERTISEMENT FOR BIDS. THE OWNER AND ENGINEER DO NOT GUARANTEE THE ACCURACY OF UTILITIES INFORMATION.
35. IN AREAS OF PAVEMENT WIDENING ALONG 143rd STREET, THE CONSTRUCTION OF THE BASE COURSE SHALL BE PAID FOR AS HMA BASE COURSE WIDENING, 11-INCH INCLUDING AREAS OF WIDENING GREATER THAN SIX (6) FEET.
36. ALL EXCAVATION, INCLUDING WIDENING SECTIONS, SHALL BE PAID FOR AS EARTH EXCAVATION.
37. THE DEPARTMENT'S DROP OFF POLICY (SAFETY 4-08), UPDATED 8/27/10) MUST BE FOLLOWED FOR ALL STAGES OF THE MOT PLAN. IT IS LOCATED AT: [HTTP://WWW.DOT.IL.GOV/ILLINOISSHP/PDF/20100827WZ_SUPPOLICYSUBPART_K.PDF](http://www.dot.il.gov/ILLINOISSHP/PDF/20100827WZ_SUPPOLICYSUBPART_K.PDF). ADD TEMPORARY BARRIER WALL TO ALL LOCATIONS WHERE THE POLICY WILL BE VIOLATED. THE END OF THE BARRIER WALL MUST BE TAPERED 12:1 AWAY FROM TRAFFIC. IMPACT ATTENUATORS TEST LEVEL 3 ARE ALSO REQUIRED AT THE END OF THE BARRIER WALL THAT IS INSTALLED IN THE DIRECTION OF ONCOMING TRAFFIC. COST OF CONFORMING TO THE POLICY SHALL BE INCLUDED AS PART OF THE TRAFFIC CONTROL AND PROTECTION (SPECIAL) PAY ITEM.

| | | | | | | | | | | | | |
|------------------------------------|----------------------|-----------------|-----------------------------------|---|--|----------------|--------------|-------------|---------|-----------------------|--------------|-------------------------------|
| FILE NAME = 04273_02-NOTE-01 - N01 | USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION GENERAL NOTES | | | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | CHECKED -- | REVISED -- | | 1600 | 04-00003-00-CH | WILL | 96 | 3 | | | |
| | PLOT SCALE = | DRAWN -- RG.MED | REVISED -- | | CONTRACT NO. 63147 | | | | | | | |
| | PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- | | SCALE: NONE | SHEET NO. 3 | OF 96 SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT M-8003 (562) |

| SUMMARY OF QUANTITIES | | | | TOTAL QUANTITY | CONSTRUCTION TYPE CODE | | |
|-----------------------|--|--------|-------|----------------|------------------------|------|--|
| S.I. CODE NO. | PAY ITEM | UNIT | 0003 | | 0021 | 0042 | |
| 20100110 | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | UNIT | 312 | 312 | | | |
| 20100210 | TREE REMOVAL (OVER 15 UNITS DIAMETER) | UNIT | 415 | 415 | | | |
| 20200100 | EARTH EXCAVATION | CU YD | 8085 | 8085 | | | |
| 20201200 | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | CU YD | 400 | 400 | | | |
| 20400800 | FURNISHED EXCAVATION | CU YD | 1515 | 1515 | | | |
| 20800150 | TRENCH BACKFILL | CU YD | 182 | 182 | | | |
| 21001000 | GEOTECHNICAL FABRIC FOR GROUND STABILIZATION | SQ YD | 1200 | 1200 | | | |
| 21101615 | TOPSOIL FURNISH AND PLACE, 4" | SQ YD | 13160 | 13160 | | | |
| 25100630 | EROSION CONTROL BLANKET | SQ YD | 13160 | 13160 | | | |
| 28000250 | TEMPORARY EROSION CONTROL SEEDING | POUND | 540 | 540 | | | |
| 28000305 | TEMPORARY DITCH CHECKS | FOOT | 475 | 475 | | | |
| 28000400 | PERIMETER EROSION BARRIER | FOOT | 4900 | 4900 | | | |
| 28000500 | INLET AND PIPE PROTECTION | EACH | 29 | 29 | | | |
| 28000510 | INLET FILTERS | EACH | 27 | 27 | | | |
| 28100105 | STONE RIPRAP, CLASS A3 | SQ YD | 132 | 132 | | | |
| 30300001 | AGGREGATE SUBGRADE IMPROVEMENT | CU YD | 400 | 400 | | | |
| 30300112 | AGGREGATE SUBGRADE IMPROVEMENT 12" | SQ YD | 20470 | 20470 | | | |
| 31101100 | SUB-BASE GRANULAR MATERIAL, TYPE B | CU YD | 130 | 130 | | | |
| 35501308 | HOT-MIX ASPHALT BASE COURSE, 6" | SQ YD | 502 | 502 | | | |
| 35501316 | HOT-MIX ASPHALT BASE COURSE, 8" | SQ YD | 341 | 341 | | | |
| 35600704 | HOT-MIX ASPHALT BASE COURSE WIDENING, 7" | SQ YD | 100 | 100 | | | |
| 35600720 | HOT-MIX ASPHALT BASE COURSE WIDENING, 11" | SQ YD | 852 | 852 | | | |
| 40201000 | AGGREGATE FOR TEMPORARY ACCESS | TON | 332 | 332 | | | |
| 40600100 | BITUMINOUS MATERIALS (PRIME COAT) | GALLON | 2220 | 2220 | | | |
| 40600635 | LEVELING BINDER (MACHINE METHOD), N70 | TON | 129 | 129 | | | |
| 40600895 | CONSTRUCTING TEST STRIP | EACH | 3 | 3 | | | |

* - INDICATES SPECIALTY ITEMS

| SUMMARY OF QUANTITIES | | | | TOTAL QUANTITY | CONSTRUCTION TYPE CODE | | |
|-----------------------|---|-------|-------|----------------|------------------------|------|--|
| S.I. CODE NO. | PAY ITEM | UNIT | 0003 | | 0021 | 0042 | |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 12 | 12 | | | |
| 40603335 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 | TON | 95 | 95 | | | |
| 40603340 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | TON | 326 | 326 | | | |
| 40701861 | HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9" | SQ YD | 6295 | 6295 | | | |
| 40701941 | HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 13" | SQ YD | 10338 | 10338 | | | |
| 44000100 | PAVEMENT REMOVAL | SQ YD | 13002 | 13002 | | | |
| 44000152 | HOT-MIX ASPHALT SURFACE REMOVAL, 3/4" | SQ YD | 2023 | 2023 | | | |
| 44000200 | DRIVEWAY PAVEMENT REMOVAL | SQ YD | 345 | 345 | | | |
| 44000500 | COMBINATION CURB AND GUTTER REMOVAL | FOOT | 945 | 945 | | | |
| 48101600 | AGGREGATE SHOULDERS, TYPE B 8" | SQ YD | 968 | 968 | | | |
| 50102400 | CONCRETE REMOVAL | CU YD | 2 | 2 | | | |
| 50105220 | PIPE CULVERT REMOVAL | FOOT | 384 | 384 | | | |
| 50800105 | REINFORCEMENT BARS | POUND | 90 | 90 | | | |
| 54001001 | BOX CULVERT END SECTIONS, CULVERT NO.1 | EACH | 2 | 2 | | | |
| 542A0217 | PIPE CULVERTS, CLASS A, TYPE 1 12" | FOOT | 77 | 77 | | | |
| 542A0220 | PIPE CULVERTS, CLASS A, TYPE 1 15" | FOOT | 62 | 62 | | | |
| 54213657 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12" | EACH | 10 | 10 | | | |
| 54213660 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15" | EACH | 9 | 9 | | | |
| 54213669 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24" | EACH | 1 | 1 | | | |
| 54248510 | CONCRETE COLLAR | CU YD | 2 | 2 | | | |
| 550A0050 | STORM SEWERS, CLASS A, TYPE 1 12" | FOOT | 274 | 274 | | | |
| 550A0070 | STORM SEWERS, CLASS A, TYPE 1 15" | FOOT | 40 | 40 | | | |
| 550A0340 | STORM SEWERS, CLASS A, TYPE 2 12" | FOOT | 335 | 335 | | | |
| 550A0360 | STORM SEWERS, CLASS A, TYPE 2 15" | FOOT | 113 | 113 | | | |
| 550A0380 | STORM SEWERS, CLASS A, TYPE 2 18" | FOOT | 230 | 230 | | | |
| 550A0410 | STORM SEWERS, CLASS A, TYPE 2 24" | FOOT | 180 | 180 | | | |

* - INDICATES SPECIALTY ITEMS

FILE NAME = 04273_02-QUAN-01 - 001

| | | |
|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- RG | REVISED -- |
| PLOT DATE = 09-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 4 OF 96 SHEETS STA. TO STA.

| | | | | |
|--------------------------------|---------------------------|----------------|-------------------------------|----------------|
| F.A.U. RTE. 1600 | SECTION 04-00003-00-CH | COUNTY WILL | TOTAL SHEETS 96 | SHEET NO. 4 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | FED. AID PROJECT M-8003 (562) | |

PRINT SUPPORTING SHEETS ON 24" X 36" PLOTTER USING A3 PLOTTER

| SUMMARY OF QUANTITIES | | | TOTAL QUANTITY | CONSTRUCTION TYPE CODE | | |
|-----------------------|--|--------|----------------|------------------------|------|------|
| S.I. CODE NO. | PAY ITEM | UNIT | | 0003 | 0021 | 0042 |
| 55100700 | STORM SEWER REMOVAL 15" | FOOT | 15 | 15 | | |
| 60107600 | PIPE UNDERDRAINS 4" | FOOT | 660 | 660 | | |
| 60200105 | CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID | EACH | 2 | 2 | | |
| 60201340 | CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE | EACH | 10 | 10 | | |
| 60207605 | CATCH BASINS, TYPE C, TYPE 8 GRATE | EACH | 1 | 1 | | |
| 60218400 | MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH | 2 | 2 | | |
| 60221100 | MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH | 2 | 2 | | |
| 60237470 | INLETS, TYPE A, TYPE 24 FRAME AND GRATE | EACH | 11 | 11 | | |
| 60255500 | MANHOLES TO BE ADJUSTED | EACH | 1 | 1 | | |
| 60605000 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 | FOOT | 6100 | 6100 | | |
| 60618300 | CONCRETE MEDIAN SURFACE, 4 INCH | SQ FT | 3113 | 3113 | | |
| 60623745 | CONCRETE MEDIAN TRANSITION | SQ FT | 115 | 115 | | |
| * 63000003 | STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS | FOOT | 175 | | 175 | |
| * 63100045 | TRAFFIC BARRIER TERMINAL, TYPE 2 | EACH | 1 | | 1 | |
| * 63100167 | TRAFFIC BARRIER TERMINAL TYPE 1 (SPECIAL) TANGENT | EACH | 3 | | 3 | |
| * 63500105 | DELINEATORS | EACH | 9 | | 9 | |
| * 66900200 | NON-SPECIAL WASTE DISPOSAL | CU YD | 100 | 100 | | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 12 | 12 | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | | |
| 70106800 | CHANGEABLE MESSAGE SIGN | CAL MO | 20 | 20 | | |
| 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 2980 | 2980 | | |
| 70300210 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS | SQ FT | 256 | 256 | | |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 21940 | 21940 | | |
| 70300280 | TEMPORARY PAVEMENT MARKING - LINE 24" | FOOT | 75 | 75 | | |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 5005 | 5005 | | |
| * 72000100 | SIGN PANEL - TYPE 1 | SQ FT | 56 | 9 | 47 | |
| * 72400100 | REMOVE SIGN PANEL ASSEMBLY - TYPE A | EACH | 2 | 2 | | |

* - INDICATES SPECIALTY ITEMS

| SUMMARY OF QUANTITIES | | | TOTAL QUANTITY | CONSTRUCTION TYPE CODE | | |
|-----------------------|--|-------|----------------|------------------------|------|------|
| S.I. CODE NO. | PAY ITEM | UNIT | | 0003 | 0021 | 0042 |
| * 72400200 | REMOVE SIGN PANEL ASSEMBLY - TYPE B | EACH | 1 | 1 | | |
| * 72400710 | RELOCATE SIGN PANEL - TYPE 1 | SQ FT | 29 | 29 | | |
| * 72400720 | RELOCATE SIGN PANEL - TYPE 2 | SQ FT | 38 | 38 | | |
| * 72800100 | TELESCOPING STEEL SIGN SUPPORT | FOOT | 194 | 194 | | |
| * 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 370 | 370 | | |
| * 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 14050 | 14050 | | |
| * 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 1490 | 1490 | | |
| * 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 930 | 930 | | |
| * 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 180 | 180 | | |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 301 | 301 | | |
| 78300100 | PAVEMENT MARKING REMOVAL | SQ FT | 1375 | 1375 | | |
| 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 8 | 8 | | |
| * 80400100 | ELECTRIC SERVICE INSTALLATION | EACH | 1 | | 1 | |
| * 80400200 | ELECTRIC UTILITY SERVICE CONNECTION | L SUM | 1 | | 1 | |
| * 80500020 | SERVICE INSTALLATION - POLE MOUNTED | EACH | 1 | | 1 | |
| * 81028200 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA. | FOOT | 664 | | 664 | |
| * 81028210 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA. | FOOT | 448 | | 448 | |
| * 81028220 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA. | FOOT | 71 | | 71 | |
| * 81028240 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA. | FOOT | 366 | | 366 | |
| * 81400100 | HANDHOLE | EACH | 5 | | 5 | |
| * 81400200 | HEAVY-DUTY HANDHOLE | EACH | 4 | | 4 | |
| * 81400300 | DOUBLE HANDHOLE | EACH | 1 | | 1 | |
| * 81603090 | UNIT DUCT, 600V, 3-1C NO.4, 1/C NO.6 GROUND, (XLP-TYPE USE), 1 1/4"DIA. POLYETHYLENE | FOOT | 2900 | | 2900 | |
| * 81702440 | ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/C NO. 1/0 | FOOT | 35 | | 35 | |
| * 82102250 | LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 250 WATT | EACH | 16 | | 16 | |
| * 82500360 | LIGHTING CONTROLLER, BASE MOUNTED, 480VOLT, 100AMP | EACH | 1 | | 1 | |

* - INDICATES SPECIALTY ITEMS

FILE NAME = 04273_02-QUAN-01 - Q02

| | | |
|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- 06-22-13 - ADDED ITEM |
| PLOT SCALE = | DRAWN -- RG | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 5 OF 96 SHEETS STA. TO STA.

| | | | | |
|--------------------------------|--------------------------|----------------|--------------------|----------------|
| F.A.U. RTE. 1600 | SECTION 04-0003-00-CH | COUNTY WILL | TOTAL SHEETS 96 | SHEET NO. 5 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | CONTRACT NO. 63147 | |
| FED. AID PROJECT M-8003 (562) | | | | |

| SUMMARY OF QUANTITIES | | | | TOTAL QUANTITY | CONSTRUCTION TYPE CODE | | |
|-----------------------|----------|--|------|----------------|------------------------|------|------|
| S.I. | CODE NO. | PAY ITEM | UNIT | | 0003 | 0021 | 0042 |
| * | 83008400 | LIGHT POLE, ALUMINUM, 40 FT. M.H., 10 FT. MAST ARM | EACH | 4 | | 4 | |
| * | 83008500 | LIGHT POLE, ALUMINUM, 40 FT. M.H., 12 FT. MAST ARM | EACH | 12 | | 12 | |
| * | 83600200 | LIGHT POLE FOUNDATION, 24" DIAMETER | FOOT | 140 | | 140 | |
| * | 83800205 | BREAKAWAY DEVICE, TRANSFORMER BASE, 15 INCH BOLT CIRCLE | EACH | 16 | | 16 | |
| * | 85000205 | MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION (SPECIAL) | EACH | 1 | | 1 | |
| * | 85700200 | FULL-ACTUATED CONTROLLER AND TYPE IV CABINET | EACH | 1 | | 1 | |
| * | 87301225 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C | FOOT | 314 | | 314 | |
| * | 87301245 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | FOOT | 689 | | 689 | |
| * | 87301255 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | FOOT | 3033 | | 3033 | |
| * | 87301295 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 20 3C | FOOT | 314 | | 314 | |
| * | 87301305 | ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR | FOOT | 1806 | | 1806 | |
| * | 87301805 | ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C | FOOT | 113 | | 113 | |
| * | 87301900 | ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C | FOOT | 543 | | 543 | |
| * | 87502500 | TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT. | EACH | 4 | | 4 | |
| * | 87700250 | STEEL MAST ARM ASSEMBLY AND POLE, 42 FT. | EACH | 1 | | 1 | |
| * | 87700280 | STEEL MAST ARM ASSEMBLY AND POLE, 48 FT. | EACH | 2 | | 2 | |
| * | 87700300 | STEEL MAST ARM ASSEMBLY AND POLE, 52 FT. | EACH | 1 | | 1 | |
| * | 87800100 | CONCRETE FOUNDATION, TYPE A | FOOT | 16 | | 16 | |
| * | 87800150 | CONCRETE FOUNDATION, TYPE C | FOOT | 4 | | 4 | |
| * | 87800415 | CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER | FOOT | 60 | | 60 | |
| * | 88030020 | SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED | EACH | 3 | | 3 | |
| * | 88030100 | SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 1 | | 1 | |
| * | 88030110 | SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED | EACH | 7 | | 7 | |
| * | 88030220 | SIGNAL HEAD, LED, 2-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 3 | | 3 | |
| * | 88200210 | TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM | EACH | 10 | | 10 | |
| * | 88500100 | INDUCTIVE LOOP DETECTOR | EACH | 9 | | 9 | |

* - INDICATES SPECIALTY ITEMS

| SUMMARY OF QUANTITIES | | | | TOTAL QUANTITY | CONSTRUCTION TYPE CODE | | |
|-----------------------|----------|--|-------|----------------|------------------------|------|------|
| S.I. | CODE NO. | PAY ITEM | UNIT | | 0003 | 0021 | 0042 |
| * | 88600100 | DETECTOR LOOP, TYPE 1 | FOOT | 897 | | 897 | |
| * | 88700200 | LIGHT DETECTOR | EACH | 2 | | 2 | |
| * | 88700300 | LIGHT DETECTOR AMPLIFIER | EACH | 1 | | 1 | |
| * | 89502380 | REMOVE EXISTING HANDHOLE | EACH | 9 | | 9 | |
| * | 89502500 | REMOVE TEMPORARY TRAFFIC SIGNAL INSTALLATION | EACH | 1 | | 1 | |
| | Z0030850 | TEMPORARY INFORMATION SIGNING | SQ FT | 50 | 50 | | |
| * | Z0033072 | VIDEO VEHICLE DETECTION SYSTEM | EACH | 1 | | 1 | |
| | Z0062456 | TEMPORARY PAVEMENT | SQ YD | 2415 | 2415 | | |
| * | Z0073510 | TEMPORARY TRAFFIC SIGNAL TIMING | EACH | 1 | | 1 | |
| | Z0076600 | TRAINEES | HOURL | 500 | | | 500 |
| | Z0076604 | TRAINEES TRAINING PROGRAM GRADUATE | HOURL | 500 | | | 500 |
| | X0322936 | REMOVE EXISTING FLARED END SECTION | EACH | 5 | 5 | | |
| * | X0326884 | TRAFFIC SIGNAL WOOD POLE, 45 FT, CLASS 5 | EACH | 1 | | 1 | |
| | X2130010 | EXPLORATION TRENCH, SPECIAL | FOOT | 100 | 50 | 50 | |
| | X2501020 | SEEDING, CLASS 2A (SPECIAL) | ACRE | 2.7 | 2.7 | | |
| | X2520650 | SODDING, SALT TOLERANT (SPECIAL) | SQ YD | 160 | 160 | | |
| | X4402020 | CONCRETE MEDIAN SURFACE REMOVAL | SQ FT | 505 | 505 | | |
| | X7010216 | TRAFFIC CONTROL AND PROTECTION, (SPECIAL) | L SUM | 1 | 1 | | |
| * | X8360215 | LIGHT POLE FOUNDATION, 24" DIAMETER, OFFSET | FOOT | 20 | | 20 | |
| * | X8620200 | UNINTERRUPTIBLE POWER SUPPLY, SPECIAL | EACH | 1 | | 1 | |
| | XX000610 | RELOCATE EXISTING MAILBOX | EACH | 3 | 3 | | |
| | XX005690 | STORM SEWER, DUCTILE IRON PIPE, CLASS 52 12" | FOOT | 40 | 40 | | |
| | XX006529 | CONCRETE CURB AND GUTTER END OUTLET SPECIAL | EACH | 4 | 4 | | |
| | XX008668 | PRECAST CONCRETE BOX CULVERT 2' X 2' (SPECIAL) | FOOT | 26 | 26 | | |

* - INDICATES SPECIALTY ITEMS

FILE NAME = 04273_02-QUAN-01 - 003

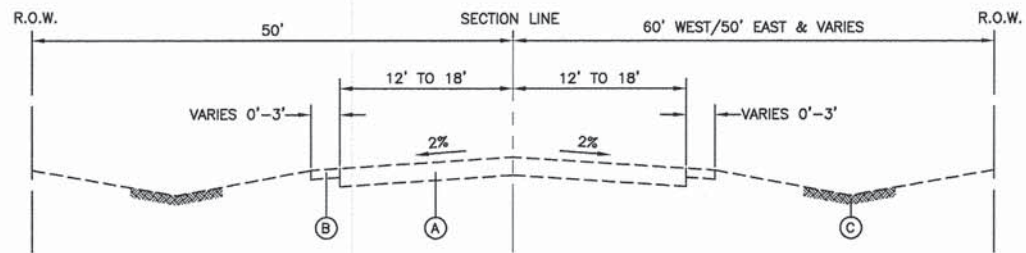
| | | |
|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- RG | REVISED -- |
| PLOT DATE = 08-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

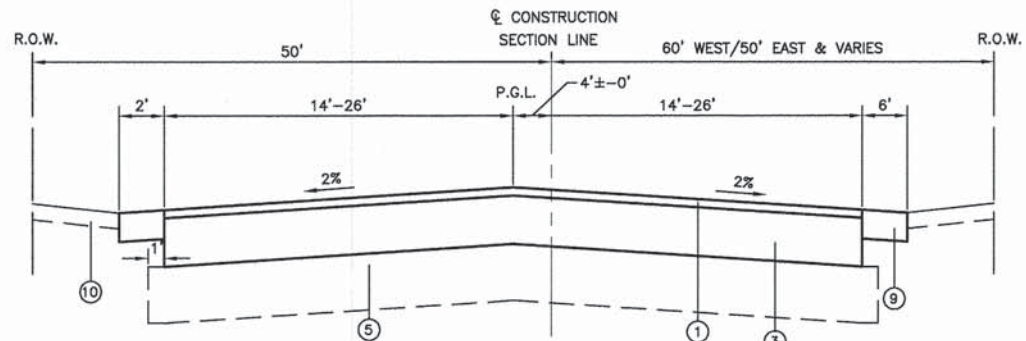
143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 6 OF 96 SHEETS STA. TO STA.

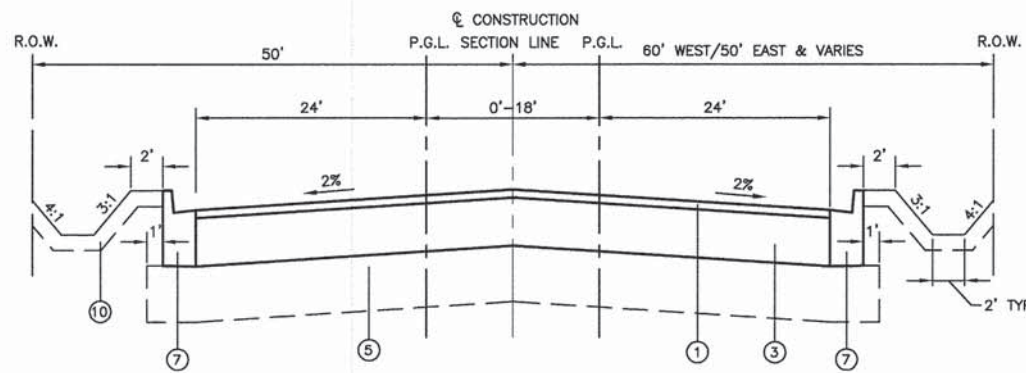
| | | | | |
|--|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 6 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | CONTRACT NO. 63147 | |



EXISTING TYPICAL SECTION
STA 105+49.86 TO STA 123+86.76, 143RD STREET



PROPOSED TYPICAL SECTION
STA 118+54.66 TO STA 119+00, 143RD STREET



PROPOSED TYPICAL SECTION
STA 114+56.66 TO STA 118+54.66, 143RD STREET

EXISTING LEGEND

- (A) EXISTING BITUMINOUS PAVEMENT (11"-17")
- (B) EXISTING AGGREGATE SHOULDER
- (C) EXISTING DITCH (DEPTH VARIES 0'-2')

PROPOSED LEGEND

- (1) HMA SURFACE COURSE, MIX. D, N70, 2"
- (2) HMA BINDER COURSE, IL-19.0, N70, 7" (LEMONT ROAD) (IN 3 LIFTS)
- (3) HMA BINDER COURSE, IL-19.0, N70, 11" (143RD STREET) (IN 5 LIFTS)
- (4) HMA BASE COURSE WIDENING, 7" (IN 3 LIFTS)
- (5) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (6) SUB-BASE GRANULAR MATERIAL, TYPE B, VARIABLE DEPTH
- (7) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- (8) CONCRETE MEDIAN SURFACE, 4"
- (9) AGGREGATE SHOULDER, TYPE B, 8"
- (10) TOPSOIL - 4" AND SEEDING, CLASS 2A
- (11) HMA BASE COURSE WIDENING, 11" (IN 5 LIFTS)
- (12) LEVELING BINDER (MACHINE METHOD), VARIABLE DEPTH (3/4" TO 2 1/4")

NOTE:

ITEMS (1) + (2) AND (1) + (3) SHALL BE PAID FOR AS HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 9" AND 13" RESPECTIVELY.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

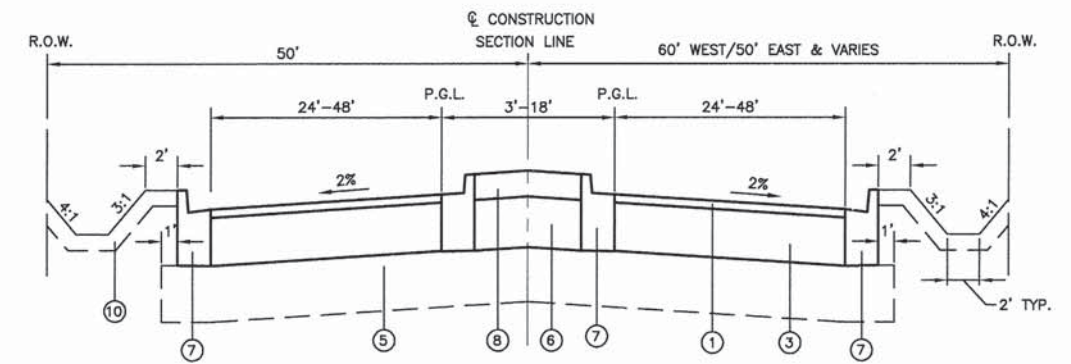
| MIXTURE TYPE | AIR VOIDS@Ndes |
|--|----------------|
| FULL DEPTH PAVEMENT | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5mm); 2" | 4% @ 70 Gyr. |
| HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 7"&11" (3 & 5 LIFTS) | 4% @ 70 Gyr. |
| PAVEMENT WIDENING | |
| HMA BASE COURSE WIDENING (HMA BINDER IL-19mm); 7"&11" (3 & 5 LIFTS) | 4% @ 50 Gyr. |
| LEVELING BINDER (MACHINE METHOD), IL-9.5mm, N70, 3/4" - 2 1/4" | 4% @ 50 Gyr. |
| DRIVEWAYS | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5mm); 2" | 4% @ 50 Gyr. |
| HOT-MIX ASPHALT BASE COURSE, (HMA BINDER IL-19mm); PE-6", CE-8" (2 TO 3 LIFTS) | 4% @ 50 Gyr. |
| TEMPORARY PAVEMENT | |
| HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50; 8" (3 LIFTS) | 4% @ 50 Gyr. |

NOTES:

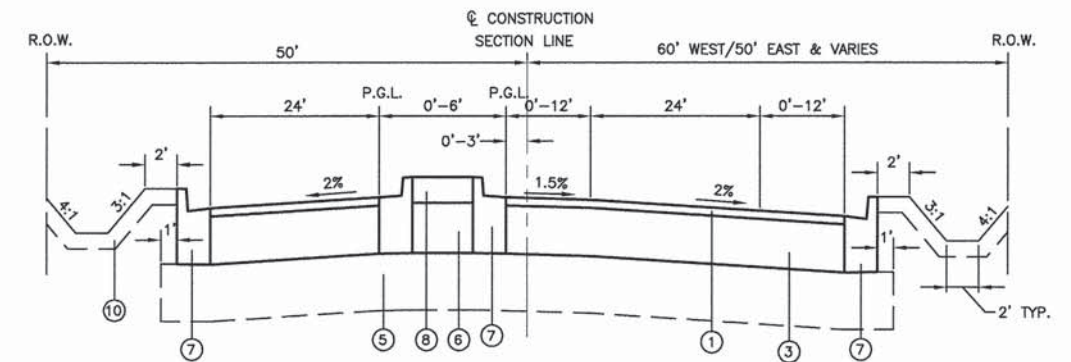
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.
THE 'AC TYPE' FOR POLYMERIZED HMA MIXES SHALL BE 'SBS/SBR PG 76-22' AND FOR NON-POLYMERIZED HMA THE 'AC TYPE' SHALL BE 'PG 640-22' UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.

EARTHWORK QUANTITIES

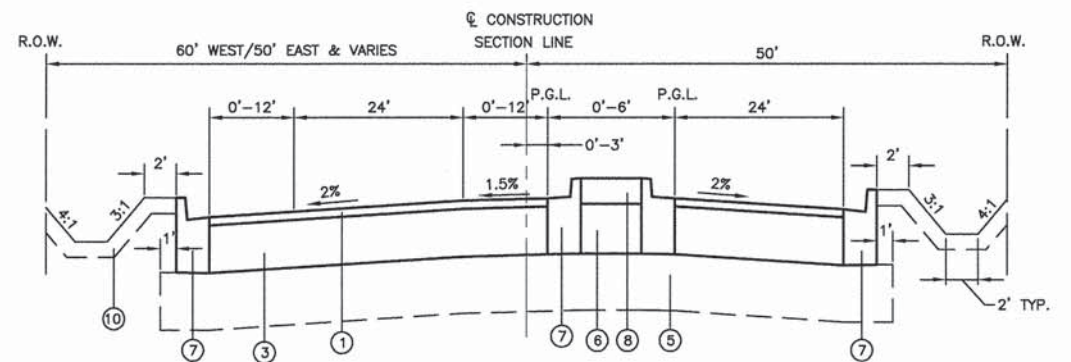
| | |
|-----------------------------|-----------|
| TOTAL CUT | = 8085 CY |
| CUT (STAGE 1) | = 1131 CY |
| CUT (STAGE 2 & 3) | = 6954 CY |
| TOTAL FILL | = 4356 CY |
| CUT TO FILL (25% SHRINKAGE) | = 5435 CY |
| FILL (STAGE 1) | = 1515 CY |
| FILL (STAGE 2 & 3) | = 2841 CY |
| FURNISHED EXCAVATION | = 1515 CY |



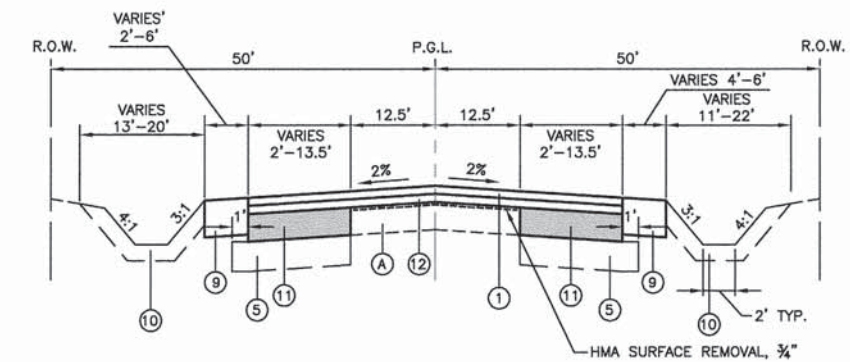
PROPOSED TYPICAL SECTION
STA 105+49.86 TO STA 107+58.66 (W), 143RD STREET
STA 113+49.86 TO STA 114+58.66 (E), 143RD STREET



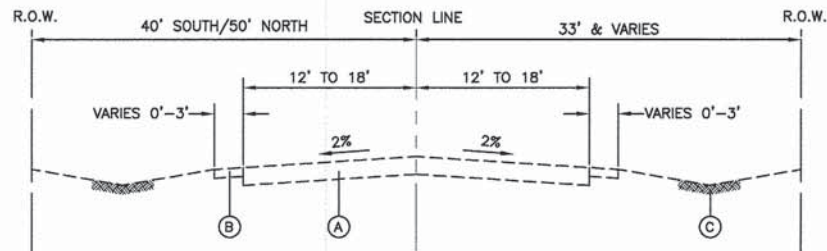
PROPOSED TYPICAL SECTION
STA 107+58.66 TO STA 109+35.89, 143RD STREET



PROPOSED TYPICAL SECTION
STA 110+63.66 TO STA 113+49.66, 143RD STREET



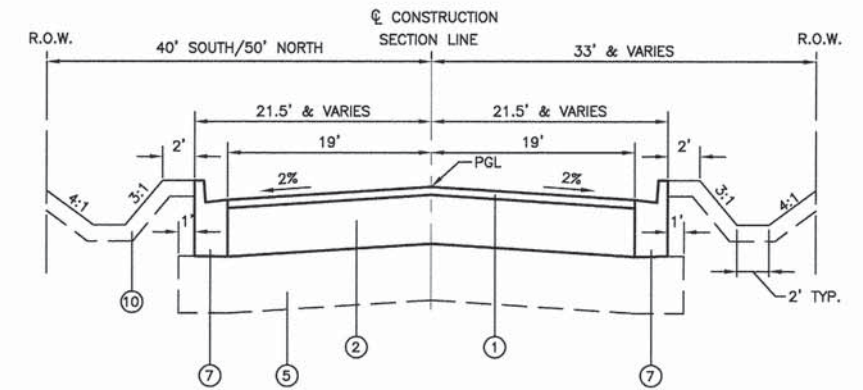
PROPOSED TYPICAL SECTION
STA 119+00 TO STA 123+86.76, 143RD STREET



EXISTING TYPICAL SECTION
STA 18+31.23 TO STA 34+43.82, LEMONT ROAD

EXISTING LEGEND

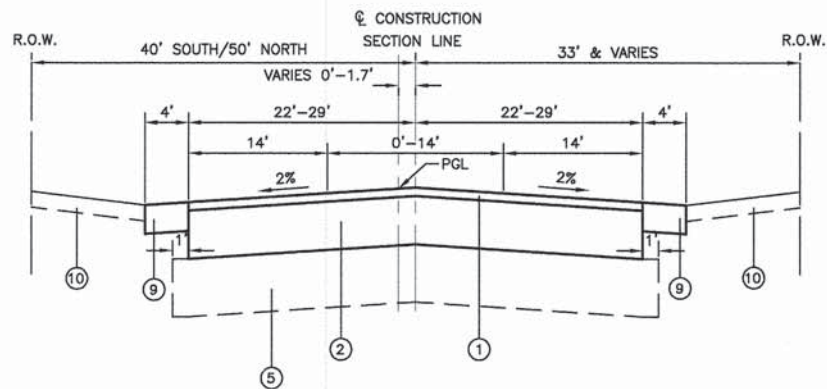
- (A) EXISTING BITUMINOUS PAVEMENT (11"-17")
- (B) EXISTING AGGREGATE SHOULDER
- (C) EXISTING DITCH (DEPTH VARIES 0'-2')



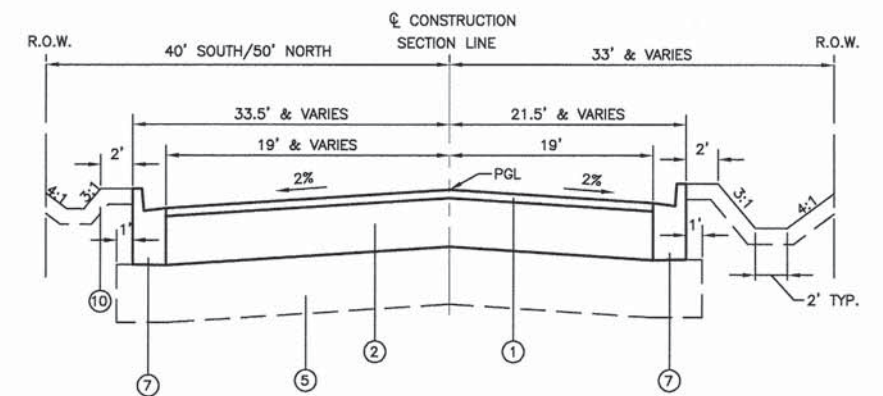
PROPOSED TYPICAL SECTION
STA 21+11.23 TO STA 24+38.70, LEMONT ROAD

PROPOSED LEGEND

- (1) HMA SURFACE COURSE, MIX. D, N70, 2"
- (2) HMA BINDER COURSE, IL-19.0, N70, 7" (LEMONT ROAD) (IN 3 LIFTS)
- (3) HMA BINDER COURSE, IL-19.0, N70, 11" (143RD STREET) (IN 5 LIFTS)
- (4) HMA BASE COURSE WIDENING, 7" (IN 3 LIFTS)
- (5) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (6) SUB-BASE GRANULAR MATERIAL, TYPE B, VARIABLE DEPTH
- (7) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- (8) CONCRETE MEDIAN SURFACE, 4"
- (9) AGGREGATE SHOULDER, TYPE B, 8"
- (10) TOPSOIL - 4" AND SEEDING, CLASS 2A
- (11) HMA BASE COURSE WIDENING, 11" (IN 5 LIFTS)
- (12) LEVELING BINDER (MACHINE METHOD), VARIABLE DEPTH (3/4" TO 2 1/4")



PROPOSED TYPICAL SECTION
STA 18+31.23 TO STA 21+11.23, LEMONT ROAD



PROPOSED TYPICAL SECTION
STA 25+63.82 TO STA 27+13.82, LEMONT ROAD

NOTE:

ITEMS (1) + (2) AND (1) + (3) SHALL BE PAID FOR AS HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 9" AND 13" RESPECTIVELY.

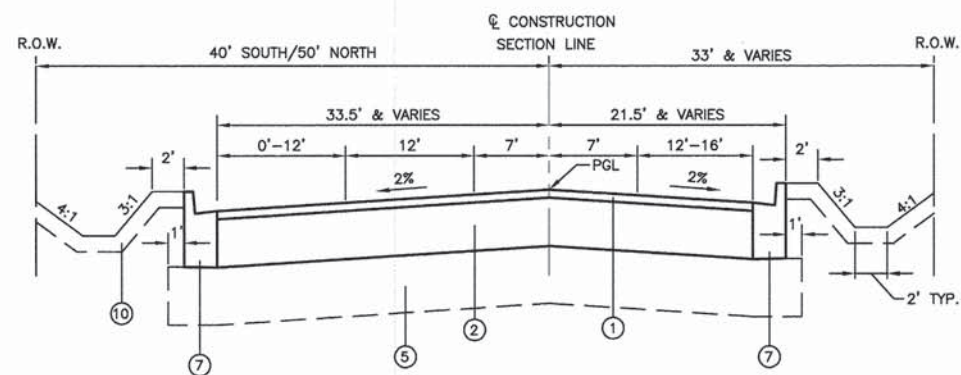
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|------------------------------------|----------------------|-----------------|-----------------------------------|
| FILE NAME = 04273_02-TYPX-01 - P02 | USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | PLOT SCALE = | CHECKED -- | REVISED -- |
| | PLOT DATE = 05-30-13 | DRAWN -- RG | REVISED -- |
| | | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

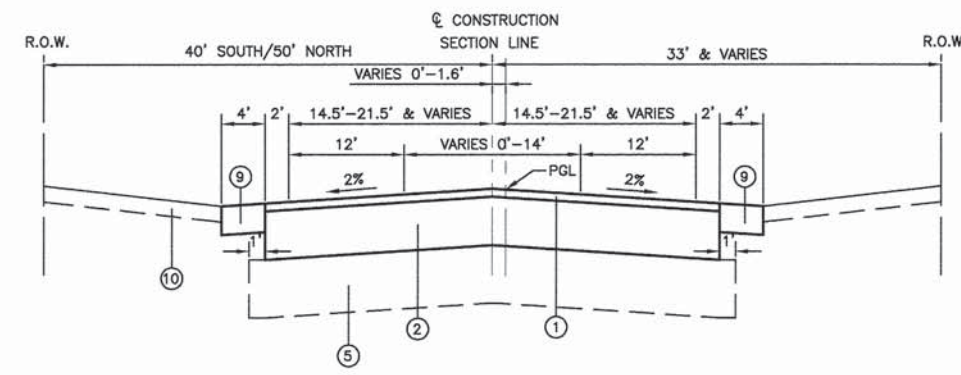
143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
TYPICAL CROSS SECTIONS

SCALE: NONE SHEET NO. 8 OF 96 SHEETS STA. TO STA.

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 8 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



PROPOSED TYPICAL SECTION
STA 27+13.82 TO STA 28+88.82, LEMONT ROAD



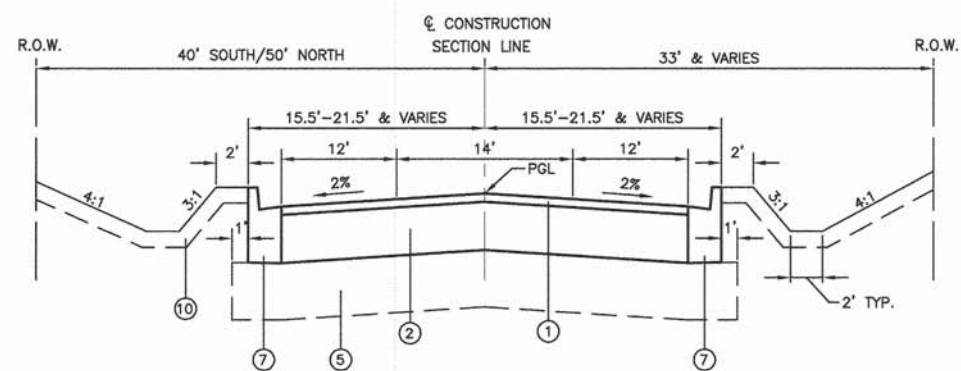
PROPOSED TYPICAL SECTION
STA 31+43.85 TO STA 33+00, LEMONT ROAD

EXISTING LEGEND

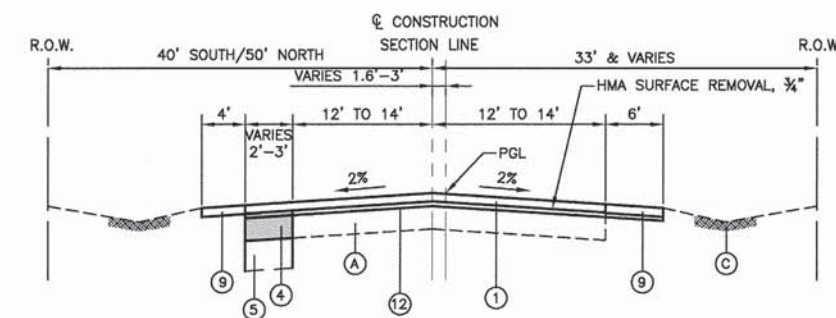
- (A) EXISTING BITUMINOUS PAVEMENT (11"-17")
- (B) EXISTING AGGREGATE SHOULDER
- (C) EXISTING DITCH (DEPTH VARIES 0'-2')

PROPOSED LEGEND

- (1) HMA SURFACE COURSE, MIX. D, N70, 2"
- (2) HMA BINDER COURSE, IL-19.0, N70, 7" (LEMONT ROAD) (IN 3 LIFTS)
- (3) HMA BINDER COURSE, IL-19.0, N70, 11" (143RD STREET) (IN 5 LIFTS)
- (4) HMA BASE COURSE WIDENING, 7" (IN 3 LIFTS)
- (5) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (6) SUB-BASE GRANULAR MATERIAL, TYPE B, VARIABLE DEPTH
- (7) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- (8) CONCRETE MEDIAN SURFACE, 4"
- (9) AGGREGATE SHOULDER, TYPE B, 8"
- (10) TOPSOIL - 4" AND SEEDING, CLASS 2A
- (11) HMA BASE COURSE WIDENING, 11" (IN 5 LIFTS)
- (12) LEVELING BINDER (MACHINE METHOD), VARIABLE DEPTH (3/4" TO 2 1/4")



PROPOSED TYPICAL SECTION
STA 28+88.82 TO STA 31+43.82, LEMONT ROAD



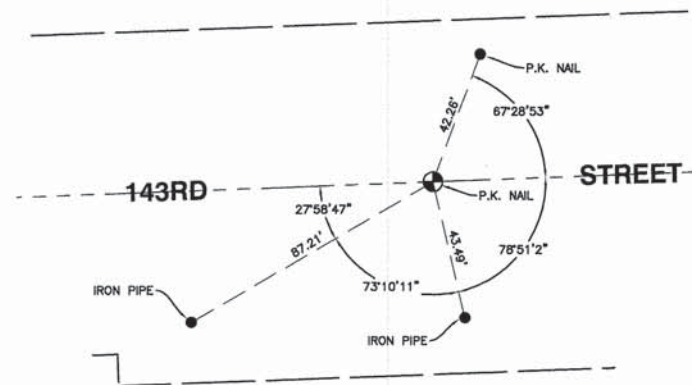
PROPOSED TYPICAL SECTION
STA 33+00 TO STA 34+43.82, LEMONT ROAD

NOTE:

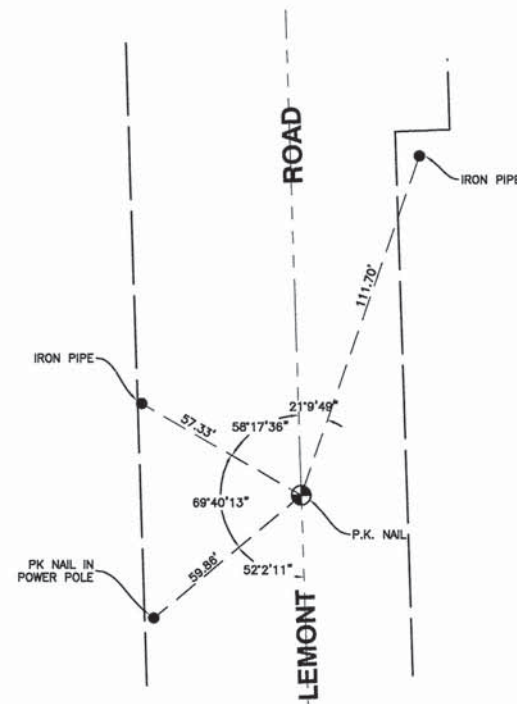
ITEMS (1) + (2) AND (1) + (3) SHALL BE PAID FOR AS HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 9" AND 13" RESPECTIVELY.

| | | |
|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- RG | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

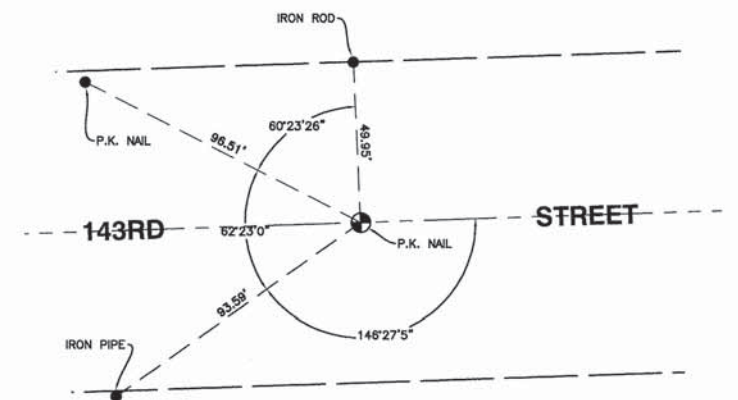
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| F.A.U. RTE. 1600 | SECTION 04-00003-00-CH | COUNTY WILL | TOTAL SHEETS 96 | SHEET NO. 9 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | FED. AID PROJECT M-8003 (562) | |



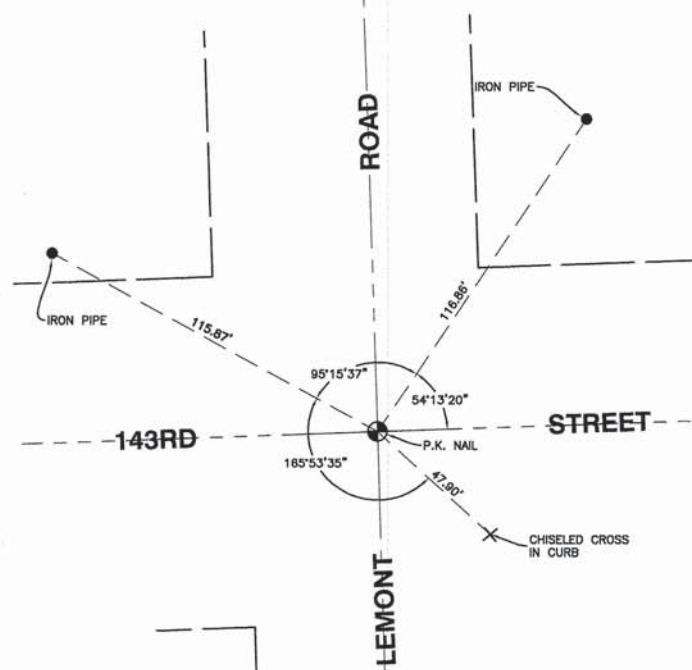
SCALE 1"=30'
STATION 100+00.00
P.K. NAIL



SCALE 1"=30'
STATION 30+00.00
P.K. NAIL



SCALE 1"=30'
STATION 136+16.39
P.K. NAIL



SCALE 1"=30'
STATION 110+00.00
P.K. NAIL



SCALE 1"=30'
STATION -125.04
P.K. NAIL

FILE NAME = 04279_02-ALGN-01 - T01

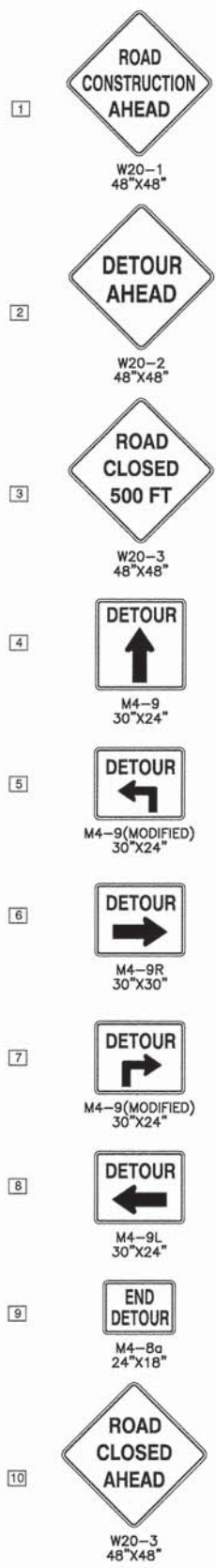
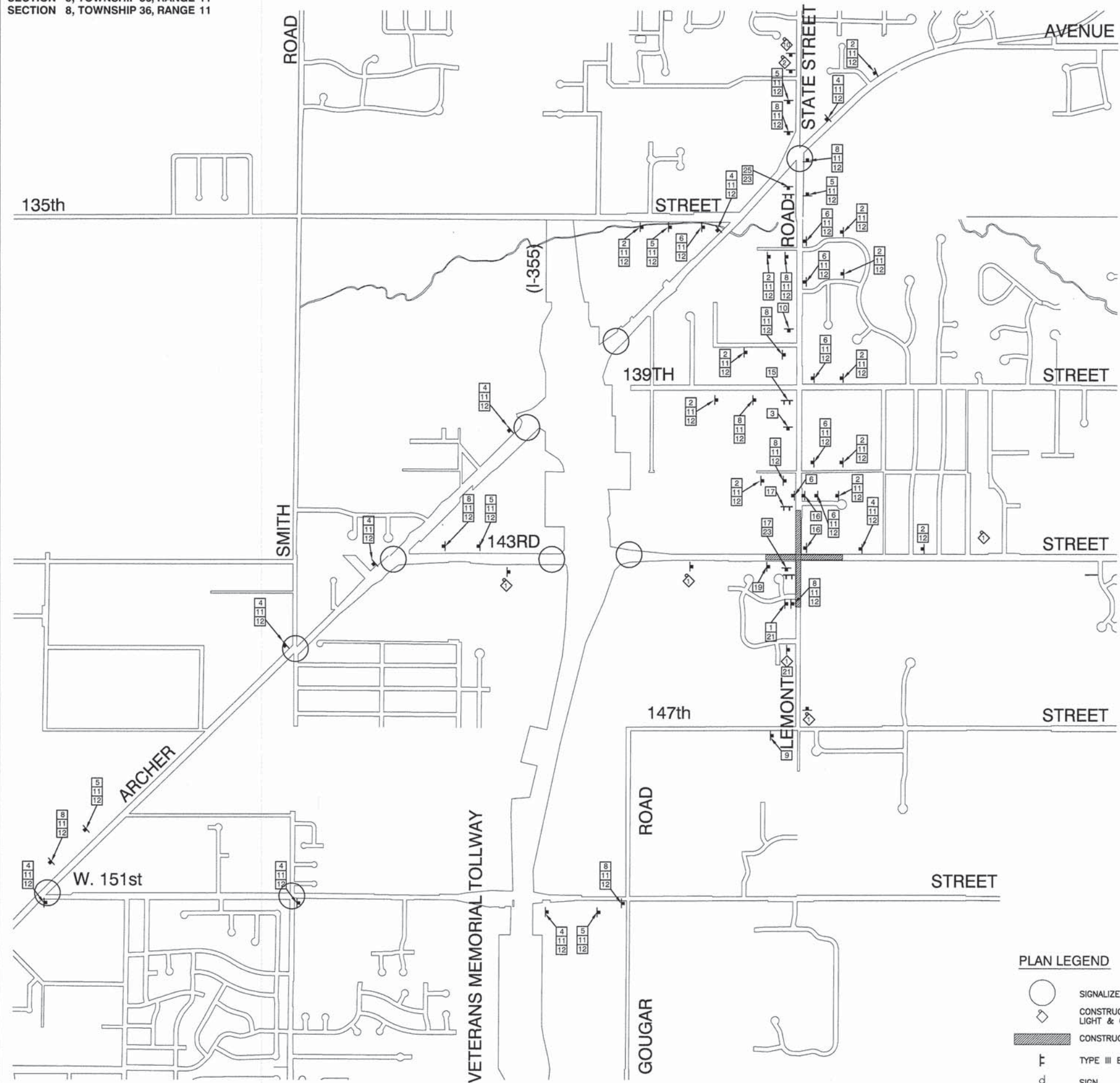
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|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| PLOT SCALE = | CHECKED -- | REVISED -- |
| PLOT DATE = 05-30-13 | DRAWN -- RG | REVISED -- |
| | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

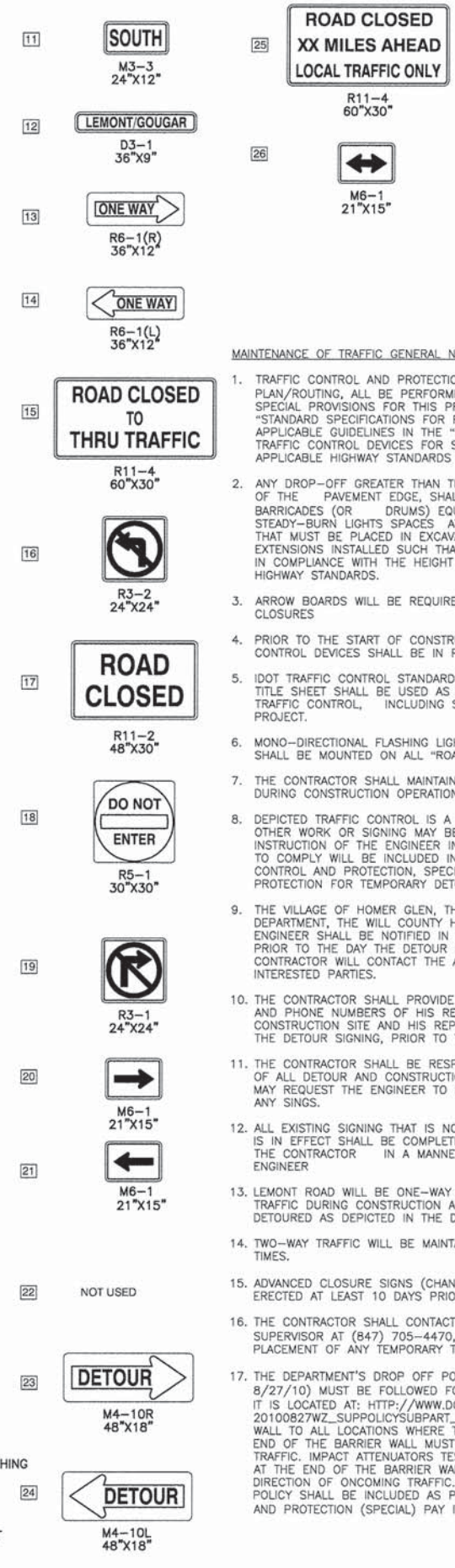
143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
ALIGNMENT AND TIES

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|----------------|--------|--------------|-----------|
| 1600 | 04-00003-00-CH | WILL | 96 | 10 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |

SCALE: 1"=50' SHEET NO. 10 OF 96 SHEETS STA. TO STA.



SIGN LEGEND



MAINTENANCE OF TRAFFIC GENERAL NOTES

1. TRAFFIC CONTROL AND PROTECTION, INCLUDING DETOUR PLAN/ROUTING, ALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISIONS FOR THIS PROJECT, SECTION 701 OF THE IDOT "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", APPLICABLE GUIDELINES IN THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", AND APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL.
2. ANY DROP-OFF GREATER THAN THREE INCHES WITHIN EIGHT FEET OF THE PAVEMENT EDGE, SHALL BE PROTECTED BY TYPE I OR II BARRICADES (OR DRUMS) EQUIPPED WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS SPACES AT 50-FOOT CENTERS. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOP OF THE BARRICADE IS IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF THE IDOT HIGHWAY STANDARDS.
3. ARROW BOARDS WILL BE REQUIRED WHEN IMPLEMENTING ALL LANE CLOSURES
4. PRIOR TO THE START OF CONSTRUCTION, REQUIRED TRAFFIC CONTROL DEVICES SHALL BE IN PLACE.
5. IDOT TRAFFIC CONTROL STANDARDS LISTED BY REFERENCE ON THE TITLE SHEET SHALL BE USED AS GUIDES IN IMPLEMENTING THE TRAFFIC CONTROL, INCLUDING SIGN LOCATIONS, FOR THIS PROJECT.
6. MONO-DIRECTIONAL FLASHING LIGHTS AND ORANGE WARNING FLAGS SHALL BE MOUNTED ON ALL "ROAD CONSTRUCTION AHEAD" SIGNS.
7. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL PROPERTIES DURING CONSTRUCTION OPERATIONS
8. DEPICTED TRAFFIC CONTROL IS A MINIMUM REQUIREMENT, AND OTHER WORK OR SIGNING MAY BE REQUIRED AS PER THE INSTRUCTION OF THE ENGINEER IN CHARGE. THE COST INCURRED TO COMPLY WILL BE INCLUDED IN THE PAY ITEMS "TRAFFIC CONTROL AND PROTECTION, SPECIAL" AND "TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR".
9. THE VILLAGE OF HOMER GLEN, THE HOMER TOWNSHIP HIGHWAY DEPARTMENT, THE WILL COUNTY HIGHWAY DEPARTMENT, AND THE ENGINEER SHALL BE NOTIFIED IN WRITING AT LEAST THREE WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT. THE CONTRACTOR WILL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES.
10. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE NAMES AND PHONE NUMBERS OF HIS REPRESENTATIVES ON THE CONSTRUCTION SITE AND HIS REPRESENTATIVE RESPONSIBLE FOR THE DETOUR SIGNING, PRIOR TO THE START OF WORK.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE FIELD LOCATION OF ALL DETOUR AND CONSTRUCTION SIGNING. THE CONTRACTOR MAY REQUEST THE ENGINEER TO FIELD VERIFY THE POSITIONS OF ANY SIGNS.
12. ALL EXISTING SIGNING THAT IS NOT APPLICABLE WHILE THE DETOUR IS IN EFFECT SHALL BE COMPLETELY COVERED, OR REMOVED, BY THE CONTRACTOR IN A MANNER MEETING THE APPROVAL OF THE ENGINEER
13. LEMONT ROAD WILL BE ONE-WAY NORTHBOUND FOR THROUGH TRAFFIC DURING CONSTRUCTION AND SOUTHBOUND TRAFFIC WILL BE DETOURED AS DEPICTED IN THE DETOUR PLAN.
14. TWO-WAY TRAFFIC WILL BE MAINTAINED ON 143RD STREET AT ALL TIMES.
15. ADVANCED CLOSURE SIGNS (CHANGEABLE MESSAGE SIGNS) SHALL BE ERECTED AT LEAST 10 DAYS PRIOR TO ANY ROAD CLOSURE
16. THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470, 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.
17. THE DEPARTMENT'S DROP OFF POLICY (SAFETY 4--08), UPDATED 8/27/10 MUST BE FOLLOWED FOR ALL STAGES OF THE MOT PLAN. IT IS LOCATED AT: http://www.dot.il.gov/ILLINOISSHP/PDF/20100827WZ_SUPPOLICYSUBPART_K.PDF. ADD TEMPORARY BARRIER WALL TO ALL LOCATIONS WHERE THE POLICY WILL BE VIOLATED. THE END OF THE BARRIER WALL MUST BE TAPERED 12:1 AWAY FROM TRAFFIC. IMPACT ATTENUATORS TEST LEVEL 3 ARE ALSO REQUIRED AT THE END OF THE BARRIER WALL THAT IS INSTALLED IN THE DIRECTION OF ONCOMING TRAFFIC. COST OF CONFORMING TO THE POLICY SHALL BE INCLUDED AS PART OF THE TRAFFIC CONTROL AND PROTECTION (SPECIAL) PAY ITEM.



PLAN LEGEND

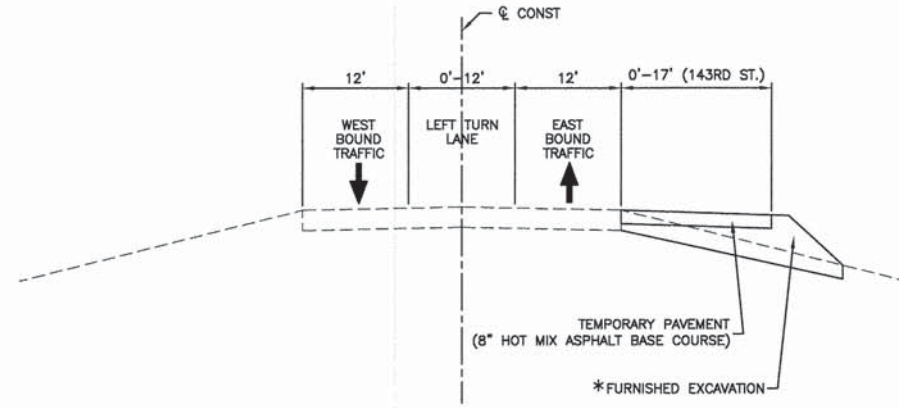
- SIGNALIZED INTERSECTION
- CONSTRUCTION WARNING SIGN WITH AMBER FLASHING LIGHT & ORANGE WARNING FLAG
- CONSTRUCTION AREA DURING DETOUR
- TYPE III BARRICADE WITH STEADY BURNING LIGHT
- SIGN

| | | | |
|------------------------------------|---------------|-----------------|-----------------------------------|
| FILE NAME = 04273_02-DETR-01 - P01 | USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | | |
| PLOT SCALE = | DRAWN -- RG | REVISED -- | |
| PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

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| 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION DETOUR PLAN | | | |
| SCALE: NONE | SHEET NO. 11 OF 96 SHEETS | STA. | TO STA. |

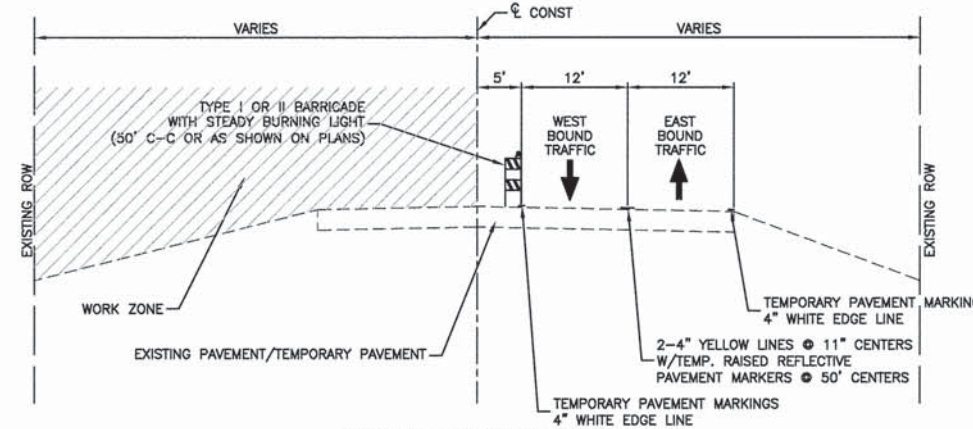
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 11 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



TYPICAL CROSS SECTION

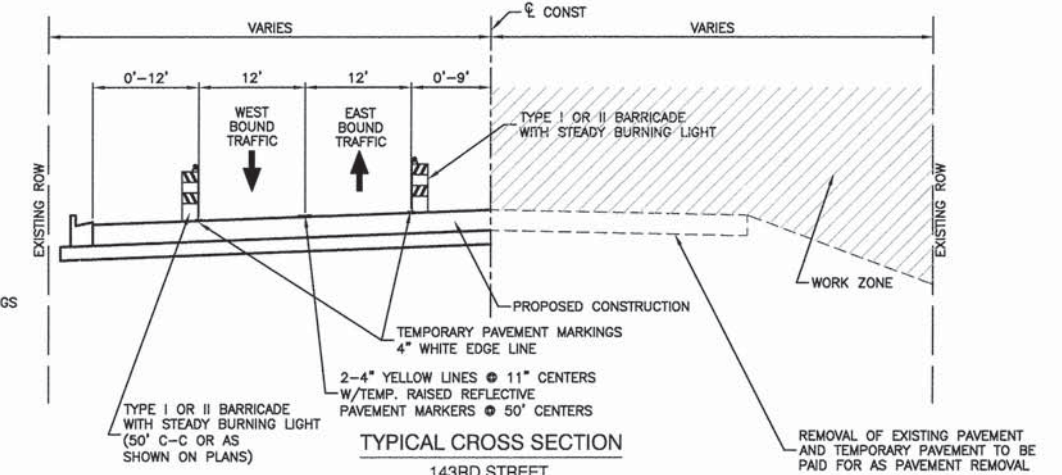
143RD STREET
STAGE 1 - STA 96+34.86 TO STA 123+86.76

* NOTE: PORTION OF FURNISHED EXCAVATION THAT NEED TO BE REMOVED DURING STAGE 3 CONSTRUCTION SHALL BE PAID FOR AS EARTH EXCAVATION.



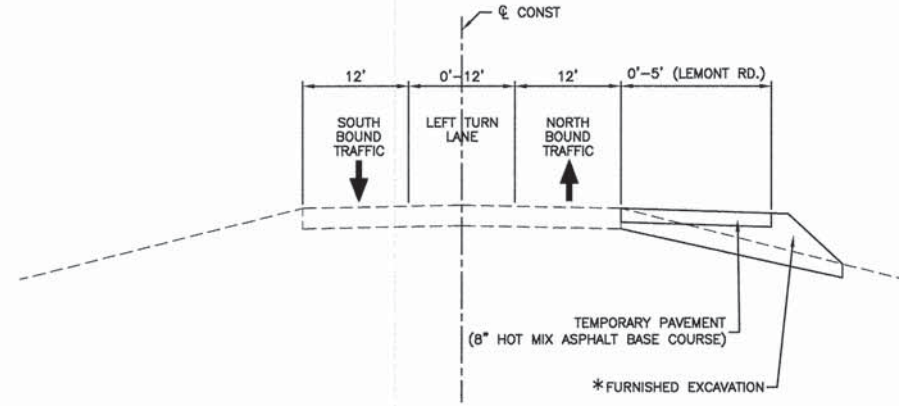
TYPICAL CROSS SECTION

143RD STREET
STAGE 2 - STA 105+49.90 TO STA 123+54.66



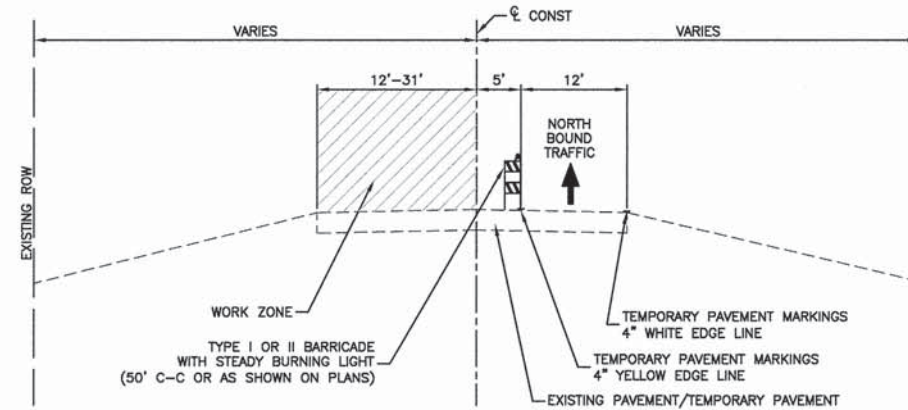
TYPICAL CROSS SECTION

143RD STREET
STAGE 3 - STA 105+49.90 TO STA 123+54.66



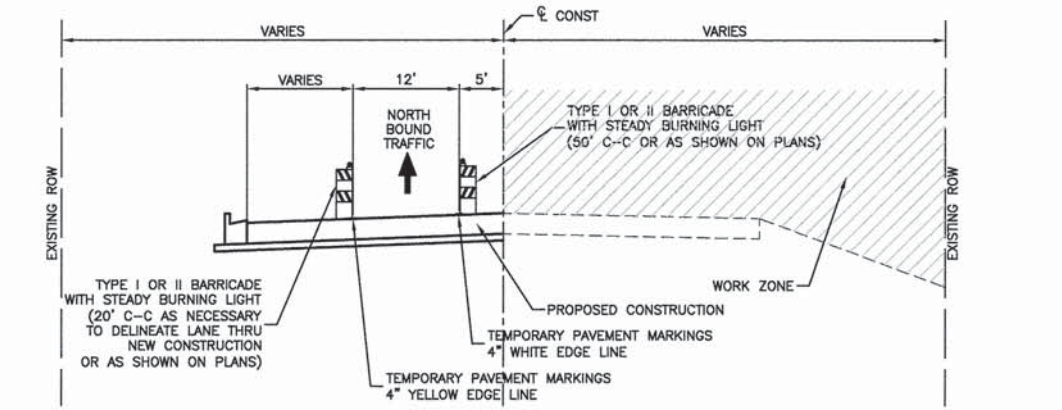
TYPICAL CROSS SECTION

LEMONT ROAD
STAGE 1 - STA 18+31.23 TO STA 34+43.82



TYPICAL CROSS SECTION

LEMONT ROAD
STAGE 2 - STA 18+31.23 TO STA 34+43.82



TYPICAL CROSS SECTION

LEMONT ROAD
STAGE 3 - STA 18+31.23 TO STA 34+43.82

SUGGESTED CONSTRUCTION STAGING NOTES

STAGE 1

- CLEARING.
- TEMPORARY PAVEMENT ALONG SOUTHSIDE OF 143RD STREET, ALONG EASTSIDE OF LEMONT ROAD.
- STORM SEWER WORK - LEMONT ROAD SOUTH OF 143RD - EAST SIDE TO ALLOW DRAINAGE OF NEW INLETS UPON COMPLETION OF STAGE 2.
- STORM SEWER WORK - 143 STREET EAST OF LEMONT, - SOUTH SIDE.
- BOX CULVERT EXTENSIONS.

STAGE 2

- STORM SEWER - 143RD WEST LEG - NORTHSIDE.
- STORM SEWER - LEMONT ROAD - SOUTH LEG - WESTSIDE; NORTH LEG - WEST SIDE.
- CLEARING AND REMOVAL.
- AGGREGATE SUBBASE.
- HMA BINDER.
- TEMPORARY WIDENING FOR TAPER ON 143RD STREET EAST AND WEST.

STAGE 3

- CLEARING AND REMOVAL.
- AGGREGATE SUBBASE.
- HMA BASE & BINDER.
- TRAFFIC SIGNALS.
- ROADWAY LIGHTING.

STAGE 4

- HMA SURFACE ENTIRE PROJECT.
- LANDSCAPING, SIGNING AND STRIPING.

FILE NAME = 04273_02-TCON-01 - T01

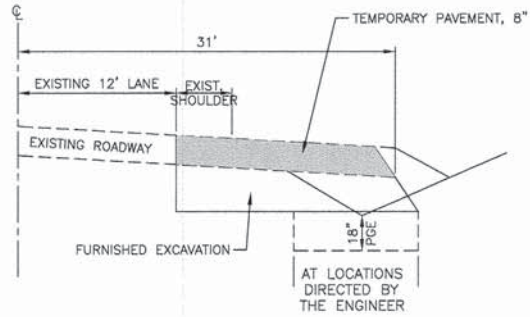
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| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| PLOT SCALE = | CHECKED -- | REVISED -- |
| PLOT DATE = 05-30-13 | DRAWN -- RG | REVISED -- |
| | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

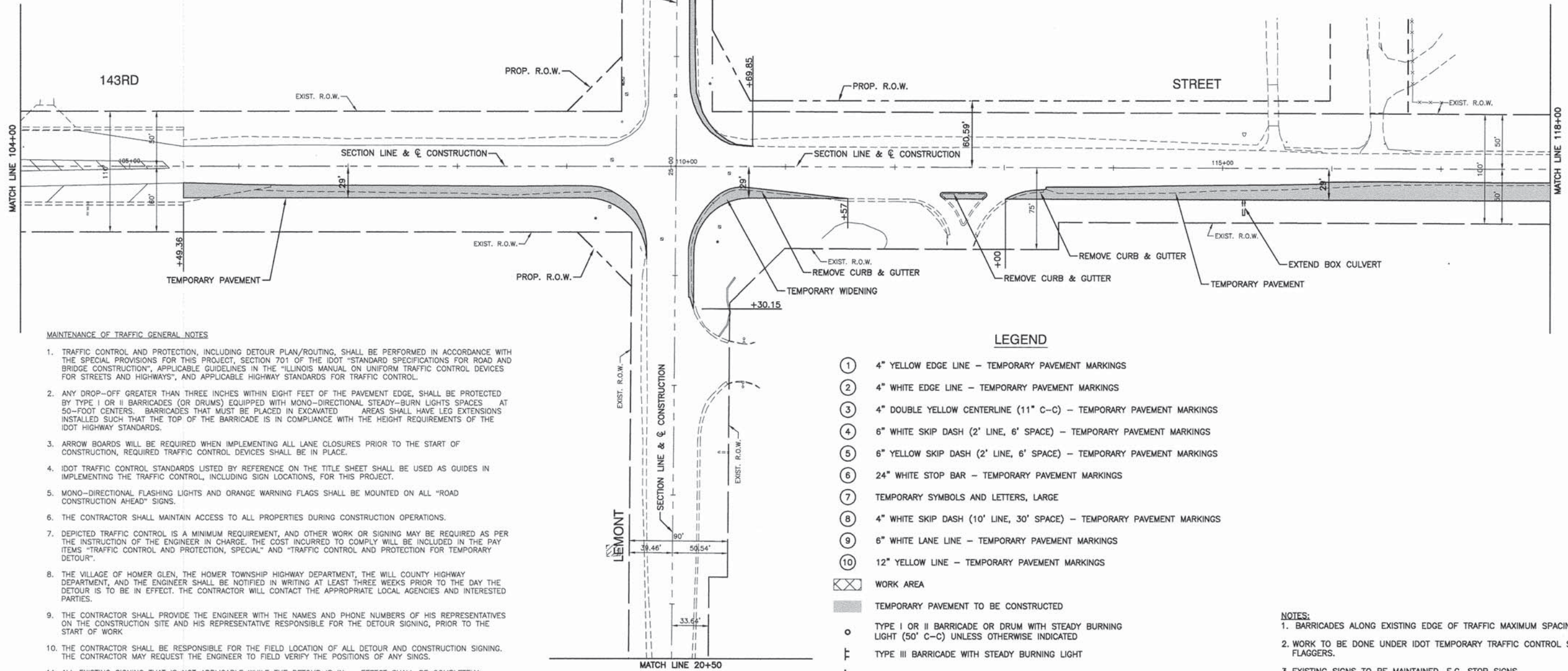
143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
TYPICAL CROSS SECTIONS

SCALE: NONE SHEET NO. 12 OF 96 SHEETS STA. TO STA.

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 12 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



TEMPORARY PAVEMENT DETAIL



MAINTENANCE OF TRAFFIC GENERAL NOTES

1. TRAFFIC CONTROL AND PROTECTION, INCLUDING DETOUR PLAN/ROUTING, SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISIONS FOR THIS PROJECT, SECTION 701 OF THE IDOT "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", APPLICABLE GUIDELINES IN THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", AND APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL.
2. ANY DROP-OFF GREATER THAN THREE INCHES WITHIN EIGHT FEET OF THE PAVEMENT EDGE, SHALL BE PROTECTED BY TYPE I OR II BARRICADES (OR DRUMS) EQUIPPED WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS SPACES AT 50-FOOT CENTERS. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOP OF THE BARRICADE IS IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF THE IDOT HIGHWAY STANDARDS.
3. ARROW BOARDS WILL BE REQUIRED WHEN IMPLEMENTING ALL LANE CLOSURES PRIOR TO THE START OF CONSTRUCTION, REQUIRED TRAFFIC CONTROL DEVICES SHALL BE IN PLACE.
4. IDOT TRAFFIC CONTROL STANDARDS LISTED BY REFERENCE ON THE TITLE SHEET SHALL BE USED AS GUIDES IN IMPLEMENTING THE TRAFFIC CONTROL, INCLUDING SIGN LOCATIONS, FOR THIS PROJECT.
5. MONO-DIRECTIONAL FLASHING LIGHTS AND ORANGE WARNING FLAGS SHALL BE MOUNTED ON ALL "ROAD CONSTRUCTION AHEAD" SIGNS.
6. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL PROPERTIES DURING CONSTRUCTION OPERATIONS.
7. DEPICTED TRAFFIC CONTROL IS A MINIMUM REQUIREMENT, AND OTHER WORK OR SIGNING MAY BE REQUIRED AS PER THE INSTRUCTION OF THE ENGINEER IN CHARGE. THE COST INCURRED TO COMPLY WILL BE INCLUDED IN THE PAY ITEMS "TRAFFIC CONTROL AND PROTECTION, SPECIAL" AND "TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR".
8. THE VILLAGE OF HOMER GLEN, THE HOMER TOWNSHIP HIGHWAY DEPARTMENT, THE WILL COUNTY HIGHWAY DEPARTMENT, AND THE ENGINEER SHALL BE NOTIFIED IN WRITING AT LEAST THREE WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT. THE CONTRACTOR WILL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES.
9. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE NAMES AND PHONE NUMBERS OF HIS REPRESENTATIVES ON THE CONSTRUCTION SITE AND HIS REPRESENTATIVE RESPONSIBLE FOR THE DETOUR SIGNING, PRIOR TO THE START OF WORK.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE FIELD LOCATION OF ALL DETOUR AND CONSTRUCTION SIGNING. THE CONTRACTOR MAY REQUEST THE ENGINEER TO FIELD VERIFY THE POSITIONS OF ANY SIGNS.
11. ALL EXISTING SIGNING THAT IS NOT APPLICABLE WHILE THE DETOUR IS IN EFFECT SHALL BE COMPLETELY COVERED, OR REMOVED, BY THE CONTRACTOR IN A MANNER MEETING THE APPROVAL OF THE ENGINEER.
12. LEMONT ROAD WILL BE ONE-WAY NORTHBOUND FOR THROUGH TRAFFIC DURING CONSTRUCTION AND SOUTHBOUND TRAFFIC WILL BE DETOURED AS DEPICTED IN THE DETOUR PLAN.
13. TWO-WAY TRAFFIC WILL BE MAINTAINED ON 143RD STREET AT ALL TIMES.
14. ADVANCED CLOSURE SIGNS (CHANGEABLE MESSAGE SIGNS) SHALL BE ERECTED AT LEAST 10 DAYS PRIOR TO ANY ROAD CLOSURES.
15. THE CONTRACTOR SHALL NOTIFY THE IDOT ARTERIAL TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470, A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

LEGEND

- ① 4" YELLOW EDGE LINE - TEMPORARY PAVEMENT MARKINGS
- ② 4" WHITE EDGE LINE - TEMPORARY PAVEMENT MARKINGS
- ③ 4" DOUBLE YELLOW CENTERLINE (11" C-C) - TEMPORARY PAVEMENT MARKINGS
- ④ 6" WHITE SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑤ 6" YELLOW SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑥ 24" WHITE STOP BAR - TEMPORARY PAVEMENT MARKINGS
- ⑦ TEMPORARY SYMBOLS AND LETTERS, LARGE
- ⑧ 4" WHITE SKIP DASH (10' LINE, 30' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑨ 6" WHITE LANE LINE - TEMPORARY PAVEMENT MARKINGS
- ⑩ 12" YELLOW LINE - TEMPORARY PAVEMENT MARKINGS
- ⊗ WORK AREA
- TEMPORARY PAVEMENT TO BE CONSTRUCTED
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C) UNLESS OTHERWISE INDICATED
- ⊥ TYPE III BARRICADE WITH STEADY BURNING LIGHT
- ⊢ SIGN
- TYPE F BARRICADE
- CRASH ATTENUATOR
- ➔ TRAFFIC FLOW
- ⊗ MONO-DIRECTIONAL FLASHING BEACON

NOTES:

1. BARRICADES ALONG EXISTING EDGE OF TRAFFIC MAXIMUM SPACING 50' C-C.
2. WORK TO BE DONE UNDER IDOT TEMPORARY TRAFFIC CONTROL STANDARDS WITH FLAGGERS.
3. EXISTING SIGNS TO BE MAINTAINED, E.G. STOP SIGNS.
4. DISTANCES SHOWN FROM C TO EDGE OF TEMPORARY PAVEMENT SHOULD BE MAINTAINED THROUGHOUT UNLESS OTHERWISE INDICATED.

BENCH MARK:

RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95

FILE NAME = 04273_02-TCON-02 - T01

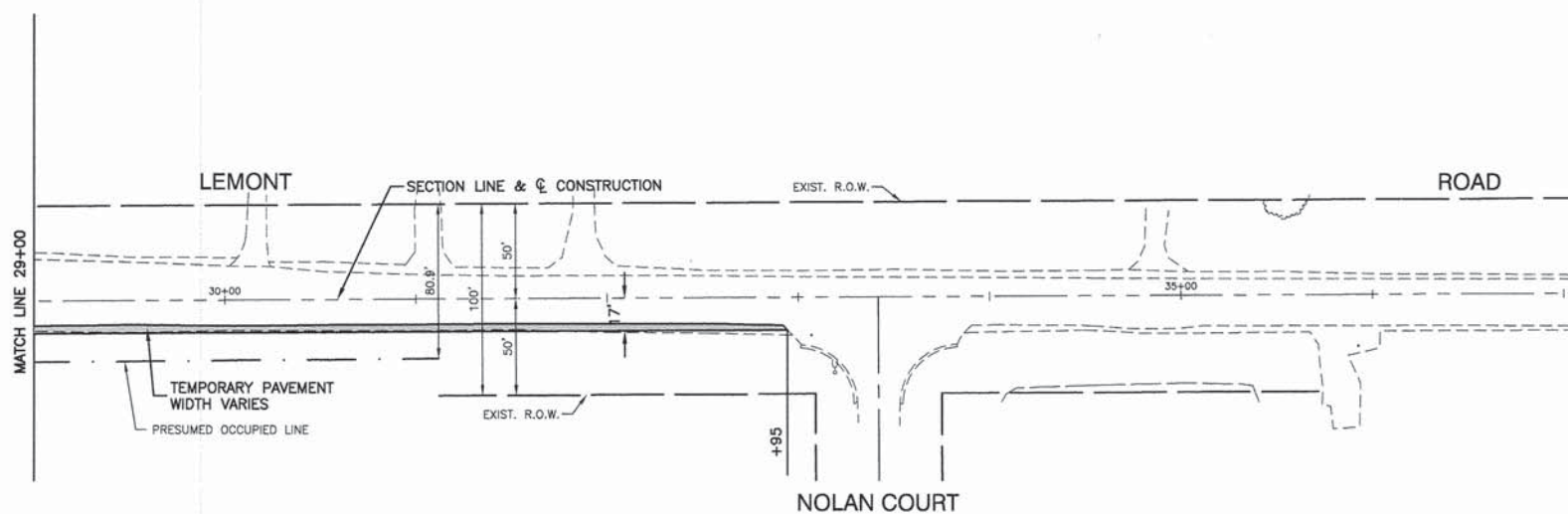
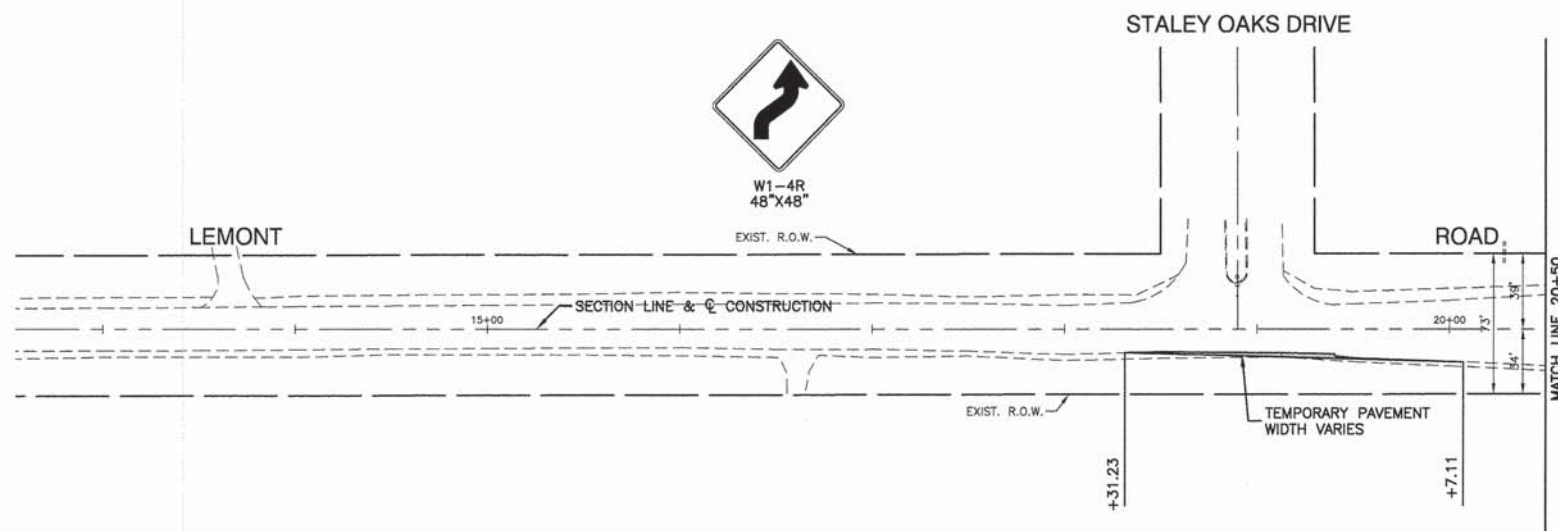
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| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- RG | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING - STAGE 1

SCALE: 1"=50' SHEET NO. 13 OF 96 SHEETS STA. TO STA.

| | | | | |
|--|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 13 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | CONTRACT NO. 63147 | |



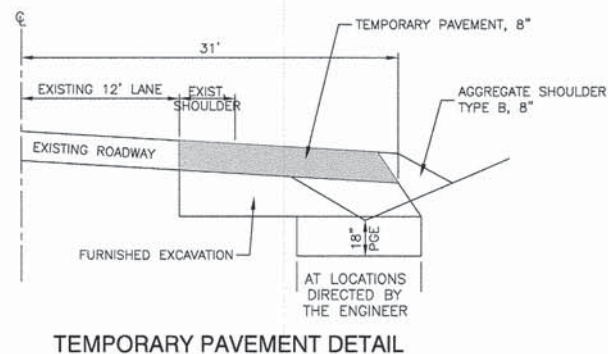
LEGEND

- ① 4" YELLOW EDGE LINE - TEMPORARY PAVEMENT MARKINGS
- ② 4" WHITE EDGE LINE - TEMPORARY PAVEMENT MARKINGS
- ③ 4" DOUBLE YELLOW CENTERLINE (11" C-C) - TEMPORARY PAVEMENT MARKINGS
- ④ 6" WHITE SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑤ 6" YELLOW SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑥ 24" WHITE STOP BAR - TEMPORARY PAVEMENT MARKINGS
- ⑦ TEMPORARY SYMBOLS AND LETTERS, LARGE
- ⑧ 4" WHITE SKIP DASH (10' LINE, 30' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑨ 6" WHITE LANE LINE - TEMPORARY PAVEMENT MARKINGS
- ⑩ 12" YELLOW LINE - TEMPORARY PAVEMENT MARKINGS

- WORK AREA
- TEMPORARY PAVEMENT TO BE CONSTRUCTED
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C) UNLESS OTHERWISE INDICATED
- TYPE III BARRICADE WITH STEADY BURNING LIGHT
- SIGN
- TYPE F BARRICADE
- CRASH ATTENUATOR
- TRAFFIC FLOW
- MONO-DIRECTIONAL FLASHING BEACON

- NOTES:**
- BARRICADES ALONG EXISTING EDGE OF TRAFFIC MAXIMUM SPACING 50' C-C.
 - WORK TO BE DONE UNDER IDOT TEMPORARY TRAFFIC CONTROL STANDARDS WITH FLAGGERS.
 - EXISTING SIGNS TO BE MAINTAINED, E.G. STOP SIGNS.
 - DISTANCES SHOWN FROM $\text{\textcircled{C}}$ TO EDGE OF TEMPORARY PAVEMENT SHOULD BE MAINTAINED THROUGHOUT UNLESS OTHERWISE INDICATED.

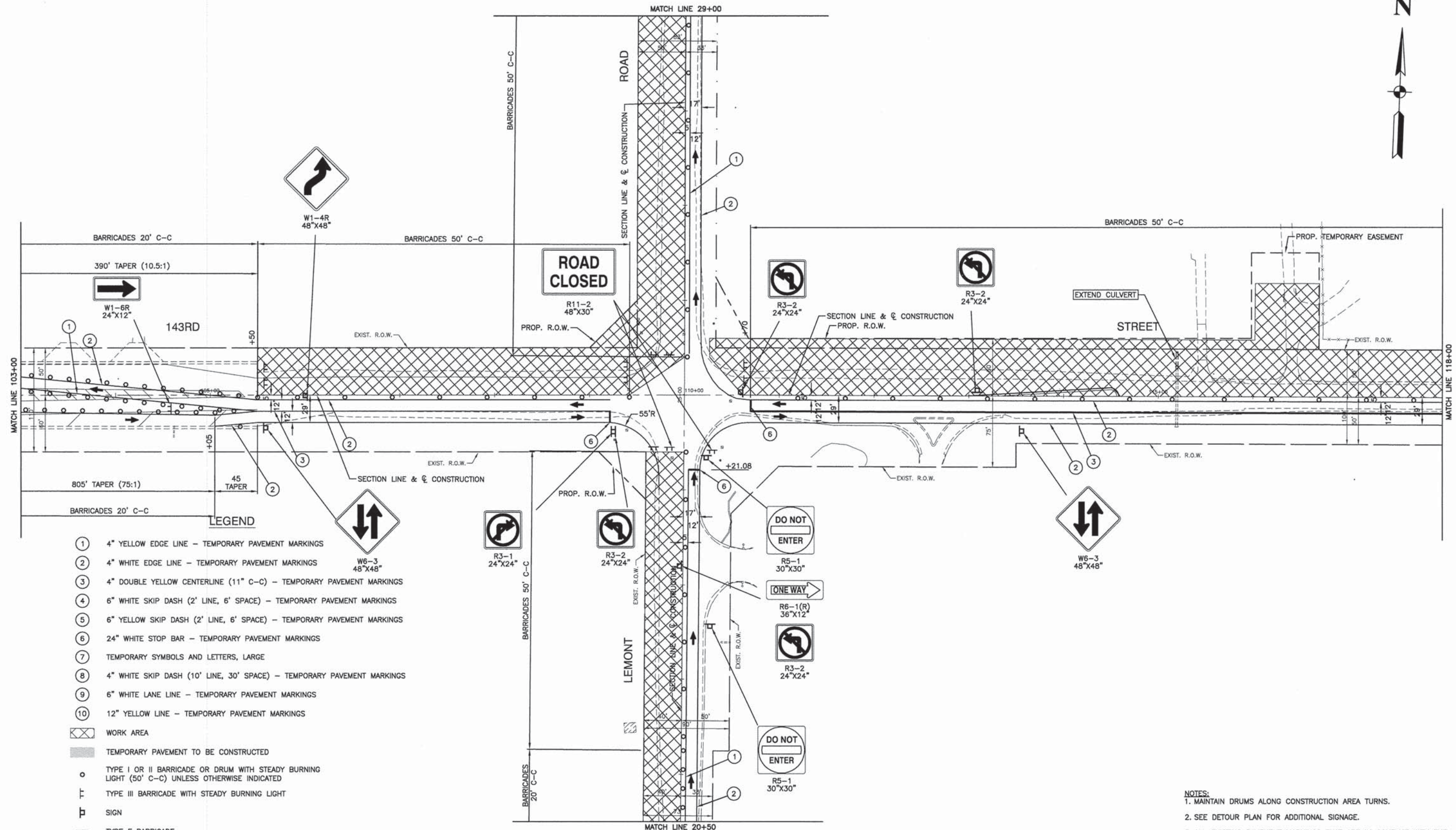
BENCH MARK:
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD.
ELEVATION = 765.95



TEMPORARY PAVEMENT DETAIL

| | | |
|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| PLOT SCALE = | CHECKED -- | REVISED -- |
| PLOT DATE = 05-30-13 | DRAWN -- RG | REVISED -- |
| | CHECKED -- AG | REVISED -- |

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|----------------|--------|--------------------|-----------|
| 1600 | 04-00003-00-CH | WILL | 96 | 15 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (662) | | | CONTRACT NO. 63147 | |



- LEGEND**
- ① 4" YELLOW EDGE LINE - TEMPORARY PAVEMENT MARKINGS
 - ② 4" WHITE EDGE LINE - TEMPORARY PAVEMENT MARKINGS
 - ③ 4" DOUBLE YELLOW CENTERLINE (11" C-C) - TEMPORARY PAVEMENT MARKINGS
 - ④ 6" WHITE SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
 - ⑤ 6" YELLOW SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
 - ⑥ 24" WHITE STOP BAR - TEMPORARY PAVEMENT MARKINGS
 - ⑦ TEMPORARY SYMBOLS AND LETTERS, LARGE
 - ⑧ 4" WHITE SKIP DASH (10' LINE, 30' SPACE) - TEMPORARY PAVEMENT MARKINGS
 - ⑨ 6" WHITE LANE LINE - TEMPORARY PAVEMENT MARKINGS
 - ⑩ 12" YELLOW LINE - TEMPORARY PAVEMENT MARKINGS
 - ⊗ WORK AREA
 - ▨ TEMPORARY PAVEMENT TO BE CONSTRUCTED
 - TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C) UNLESS OTHERWISE INDICATED
 - ⊥ TYPE III BARRICADE WITH STEADY BURNING LIGHT
 - ⊞ SIGN
 - TYPE F BARRICADE
 - CRASH ATTENUATOR
 - ➔ TRAFFIC FLOW
 - ⊗ MONO-DIRECTIONAL FLASHING BEACON

NOTES:

1. MAINTAIN DRUMS ALONG CONSTRUCTION AREA TURNS.
2. SEE DETOUR PLAN FOR ADDITIONAL SIGNAGE.
3. ALL EXISTING PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING PLAN SHALL BE REMOVED PRIOR TO THE IMPLEMENTATION OF THE STAGING PLAN.

BENCH MARK:
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95

FILE NAME = 04273_02-TCOON-02 - T02

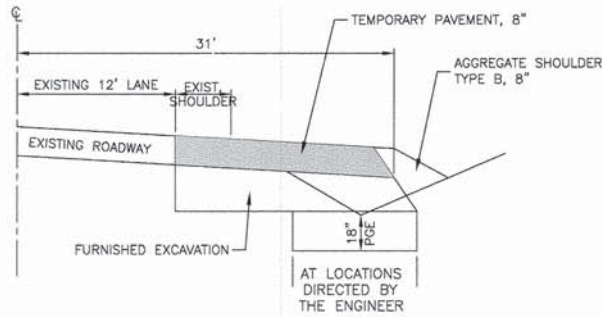
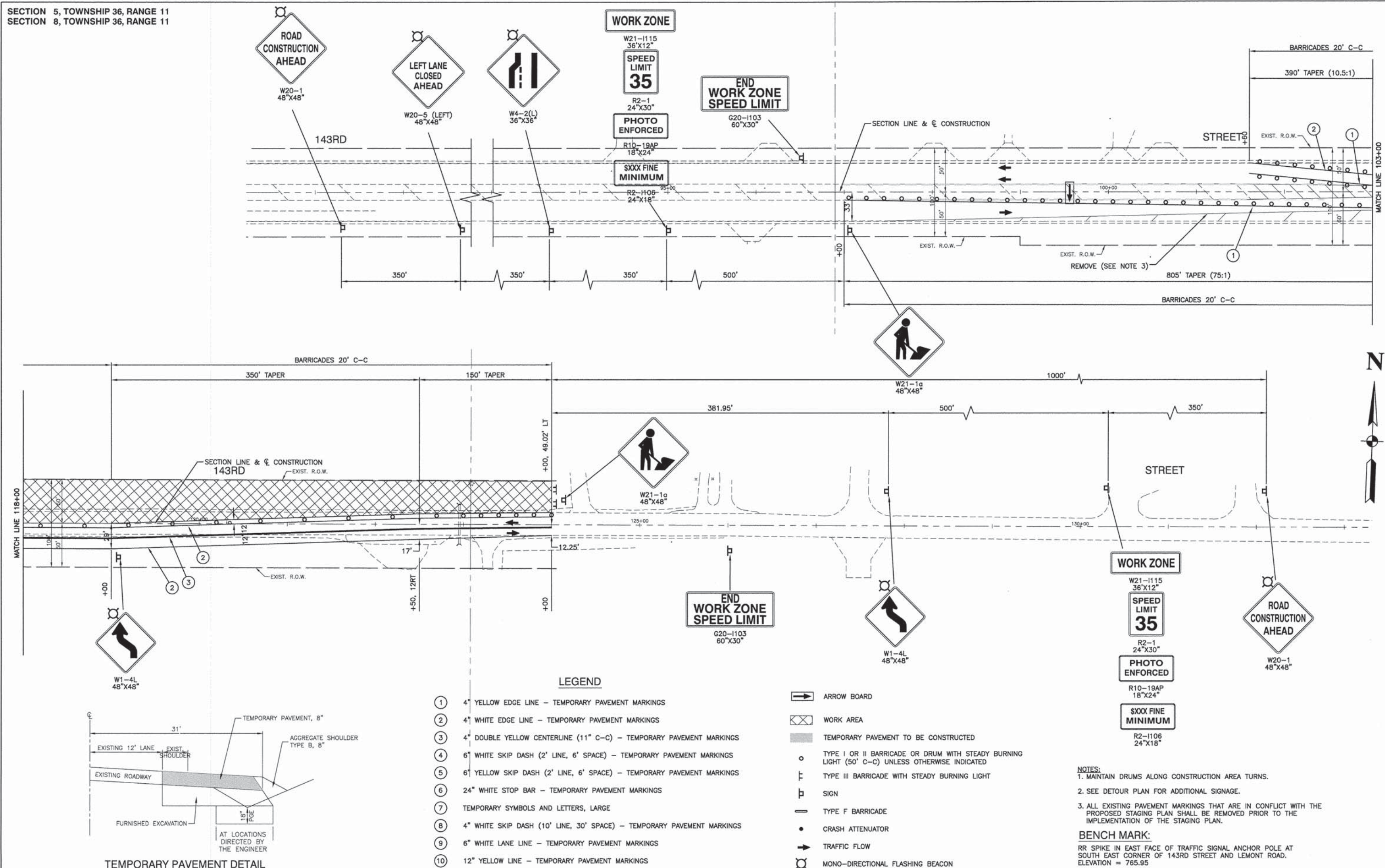
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|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- RG | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING - STAGE 2

SCALE: 1"=50' SHEET NO. 16 OF 96 SHEETS STA. TO STA.

| | | | | |
|--|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 16 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | CONTRACT NO. 63147 | |



TEMPORARY PAVEMENT DETAIL

LEGEND

- ① 4" YELLOW EDGE LINE - TEMPORARY PAVEMENT MARKINGS
- ② 4" WHITE EDGE LINE - TEMPORARY PAVEMENT MARKINGS
- ③ 4" DOUBLE YELLOW CENTERLINE (11" C-C) - TEMPORARY PAVEMENT MARKINGS
- ④ 6" WHITE SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑤ 6" YELLOW SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑥ 24" WHITE STOP BAR - TEMPORARY PAVEMENT MARKINGS
- ⑦ TEMPORARY SYMBOLS AND LETTERS, LARGE
- ⑧ 4" WHITE SKIP DASH (10' LINE, 30' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑨ 6" WHITE LANE LINE - TEMPORARY PAVEMENT MARKINGS
- ⑩ 12" YELLOW LINE - TEMPORARY PAVEMENT MARKINGS

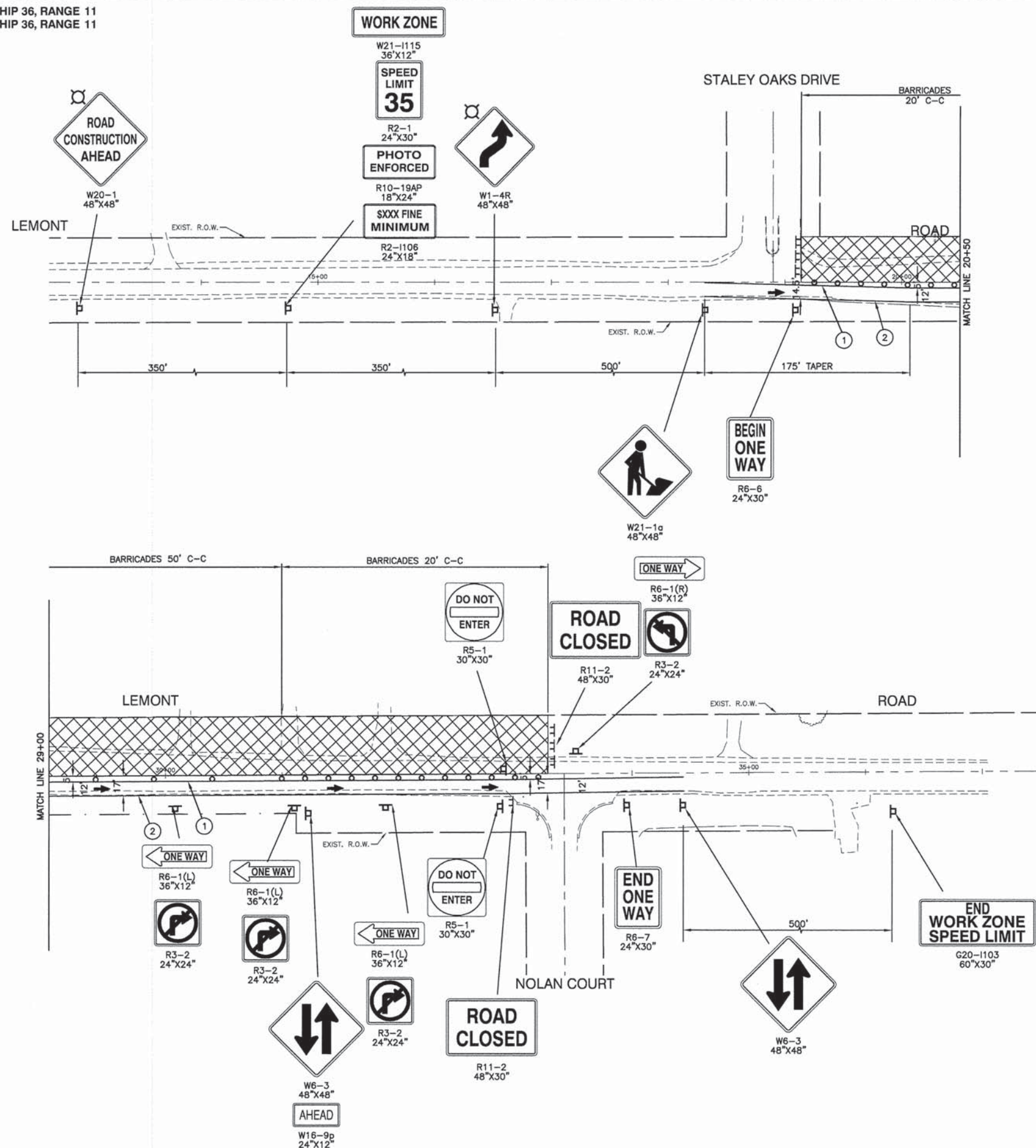
- ➡ ARROW BOARD
- ⊠ WORK AREA
- ▨ TEMPORARY PAVEMENT TO BE CONSTRUCTED
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C) UNLESS OTHERWISE INDICATED
- ⊥ TYPE III BARRICADE WITH STEADY BURNING LIGHT
- ⊞ SIGN
- TYPE F BARRICADE
- CRASH ATTENUATOR
- ➡ TRAFFIC FLOW
- ⊞ MONO-DIRECTIONAL FLASHING BEACON

- NOTES:
1. MAINTAIN DRUMS ALONG CONSTRUCTION AREA TURNS.
 2. SEE DETOUR PLAN FOR ADDITIONAL SIGNAGE.
 3. ALL EXISTING PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING PLAN SHALL BE REMOVED PRIOR TO THE IMPLEMENTATION OF THE STAGING PLAN.

BENCH MARK:
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95

| | | |
|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| PLOT SCALE = | CHECKED -- | REVISED -- |
| PLOT DATE = 05-30-13 | DRAWN -- RG | REVISED -- |
| | CHECKED -- AG | REVISED -- |

| | | | | |
|--|------------------------|-------------|-----------------|--------------|
| F.A.U. RTE. 1600 | SECTION 04-00003-00-CH | COUNTY WILL | TOTAL SHEETS 96 | SHEET NO. 17 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



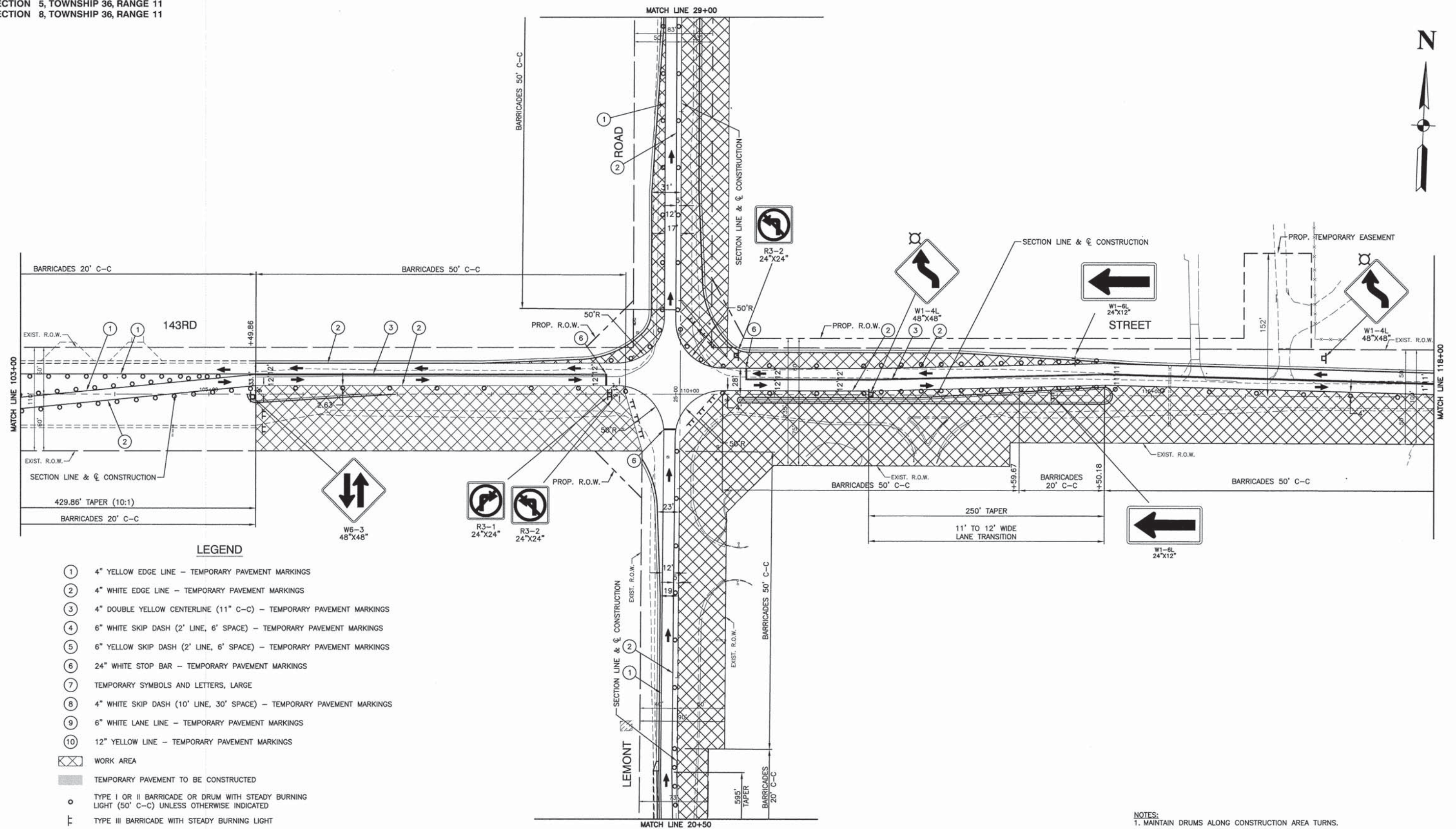
LEGEND

- ① 4" YELLOW EDGE LINE - TEMPORARY PAVEMENT MARKINGS
- ② 4" WHITE EDGE LINE - TEMPORARY PAVEMENT MARKINGS
- ③ 4" DOUBLE YELLOW CENTERLINE (11" C-C) - TEMPORARY PAVEMENT MARKINGS
- ④ 6" WHITE SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑤ 6" YELLOW SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑥ 24" WHITE STOP BAR - TEMPORARY PAVEMENT MARKINGS
- ⑦ TEMPORARY SYMBOLS AND LETTERS, LARGE
- ⑧ 4" WHITE SKIP DASH (10' LINE, 30' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑨ 6" WHITE LANE LINE - TEMPORARY PAVEMENT MARKINGS
- ⑩ 12" YELLOW LINE - TEMPORARY PAVEMENT MARKINGS
- ⊗ WORK AREA
- ▨ TEMPORARY PAVEMENT TO BE CONSTRUCTED
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C) UNLESS OTHERWISE INDICATED
- ⊥ TYPE III BARRICADE WITH STEADY BURNING LIGHT
- ⊞ SIGN
- TYPE F BARRICADE
- CRASH ATTENUATOR
- ➔ TRAFFIC FLOW
- ⊞ MONO-DIRECTIONAL FLASHING BEACON

- NOTES:**
1. MAINTAIN DRUMS ALONG CONSTRUCTION AREA TURNS.
 2. SEE DETOUR PLAN FOR ADDITIONAL SIGNAGE.
 3. ALL EXISTING PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING PLAN SHALL BE REMOVED PRIOR TO THE IMPLEMENTATION OF THE STAGING PLAN.

BENCH MARK:
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95

| | | | | | | | | | | | | |
|--------------------------------------|--------------|-----------------|-----------------------------------|---|---|--------------|--------------------|------------------------|--|-----------------|--------------|--|
| FILE NAME = 04273_02-TCON-02 - T02-3 | USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION SUGGESTED CONSTRUCTION STAGING - STAGE 2 | | F.A.U. RTE. 1600 | SECTION 04-00003-00-CH | COUNTY WILL | TOTAL SHEETS 96 | SHEET NO. 18 | |
| SCALE = 1"=50' | PLOT SCALE = | DRAWN -- RG | REVISED -- | | SHEET NO. 18 OF 96 SHEETS | STA. TO STA. | CONTRACT NO. 63147 | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | |
| PLOT DATE = 05-30-13 | | CHECKED -- AG | REVISED -- | | | | | | | | | |
| | | | | | | | | | | | | |



LEGEND

- ① 4" YELLOW EDGE LINE - TEMPORARY PAVEMENT MARKINGS
- ② 4" WHITE EDGE LINE - TEMPORARY PAVEMENT MARKINGS
- ③ 4" DOUBLE YELLOW CENTERLINE (11" C-C) - TEMPORARY PAVEMENT MARKINGS
- ④ 6" WHITE SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑤ 6" YELLOW SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑥ 24" WHITE STOP BAR - TEMPORARY PAVEMENT MARKINGS
- ⑦ TEMPORARY SYMBOLS AND LETTERS, LARGE
- ⑧ 4" WHITE SKIP DASH (10' LINE, 30' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑨ 6" WHITE LANE LINE - TEMPORARY PAVEMENT MARKINGS
- ⑩ 12" YELLOW LINE - TEMPORARY PAVEMENT MARKINGS
- ⊗ WORK AREA
- ▨ TEMPORARY PAVEMENT TO BE CONSTRUCTED
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C) UNLESS OTHERWISE INDICATED
- ⊥ TYPE III BARRICADE WITH STEADY BURNING LIGHT
- ⊞ SIGN
- TYPE F BARRICADE
- CRASH ATTENUATOR
- ➔ TRAFFIC FLOW
- ⊞ MONO-DIRECTIONAL FLASHING BEACON

- NOTES:**
1. MAINTAIN DRUMS ALONG CONSTRUCTION AREA TURNS.
 2. SEE DETOUR PLAN FOR ADDITIONAL SIGNAGE.
 3. ALL EXISTING PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING PLAN SHALL BE REMOVED PRIOR TO THE IMPLEMENTATION OF THE STAGING PLAN.

BENCH MARK:
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95

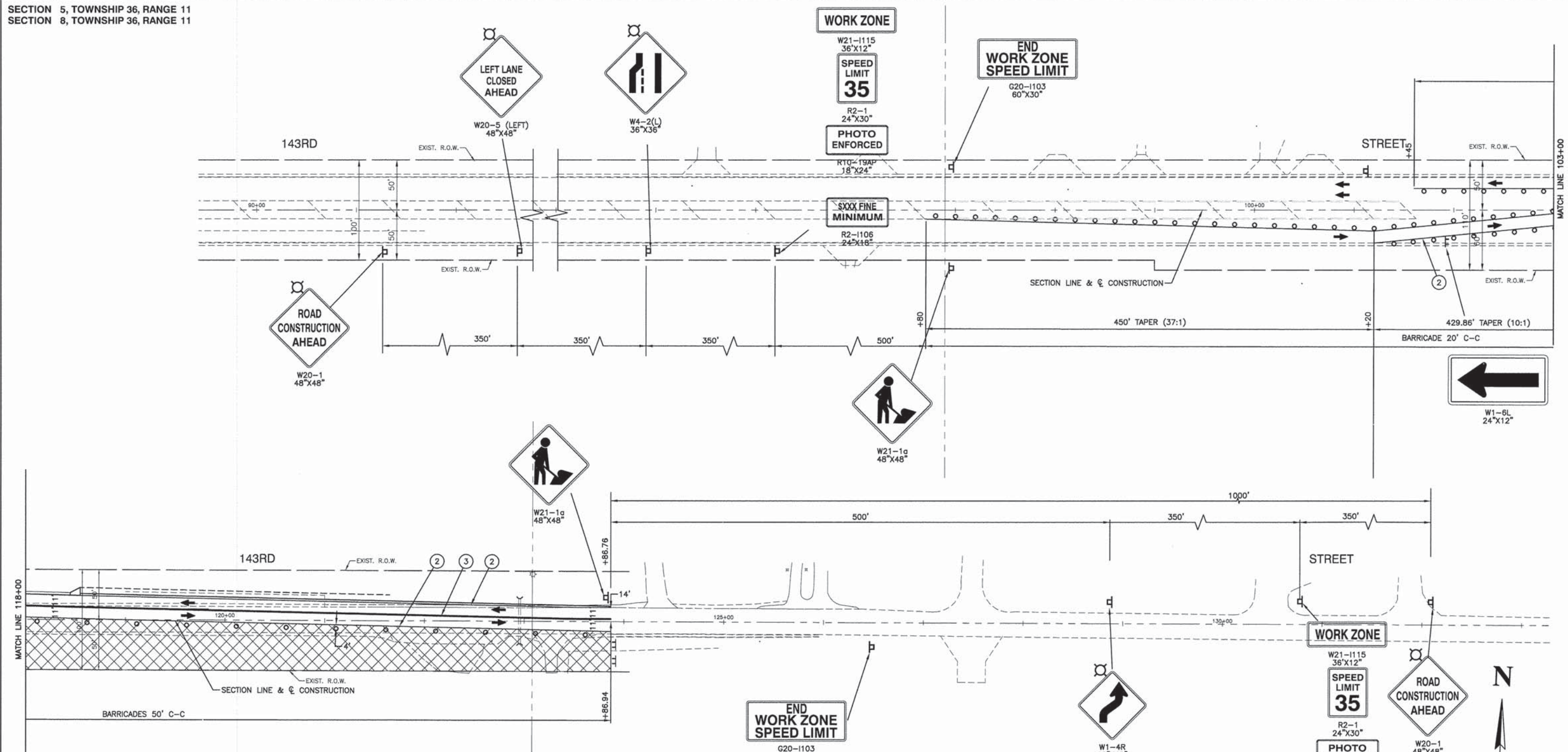
FILE NAME = 04273_02-TCOON-02 - 703

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| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- RG | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING - STAGE 3
SCALE: 1"=50'
SHEET NO. 19 OF 96 SHEETS
STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|----------------|--------|--------------------|-----------|
| 1600 | 04-00003-00-CH | WILL | 96 | 19 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | CONTRACT NO. 63147 | |



LEGEND

- ① 4" YELLOW EDGE LINE - TEMPORARY PAVEMENT MARKINGS
- ② 4" WHITE EDGE LINE - TEMPORARY PAVEMENT MARKINGS
- ③ 4" DOUBLE YELLOW CENTERLINE (11" C-C) - TEMPORARY PAVEMENT MARKINGS
- ④ 6" WHITE SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑤ 6" YELLOW SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑥ 24" WHITE STOP BAR - TEMPORARY PAVEMENT MARKINGS
- ⑦ TEMPORARY SYMBOLS AND LETTERS, LARGE
- ⑧ 4" WHITE SKIP DASH (10' LINE, 30' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑨ 6" WHITE LANE LINE - TEMPORARY PAVEMENT MARKINGS
- ⑩ 12" YELLOW LINE - TEMPORARY PAVEMENT MARKINGS

- WORK AREA
- TEMPORARY PAVEMENT TO BE CONSTRUCTED
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C) UNLESS OTHERWISE INDICATED
- TYPE III BARRICADE WITH STEADY BURNING LIGHT
- SIGN
- TYPE F BARRICADE
- CRASH ATTENUATOR
- TRAFFIC FLOW
- MONO-DIRECTIONAL FLASHING BEACON

NOTES:
1. MAINTAIN DRUMS ALONG CONSTRUCTION AREA TURNS.
2. SEE DETOUR PLAN FOR ADDITIONAL SIGNAGE.
3. ALL EXISTING PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING PLAN SHALL BE REMOVED PRIOR TO THE IMPLEMENTATION OF THE STAGING PLAN.

BENCH MARK:
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95

FILE NAME = 04273_02-TCON-02 - T03-2

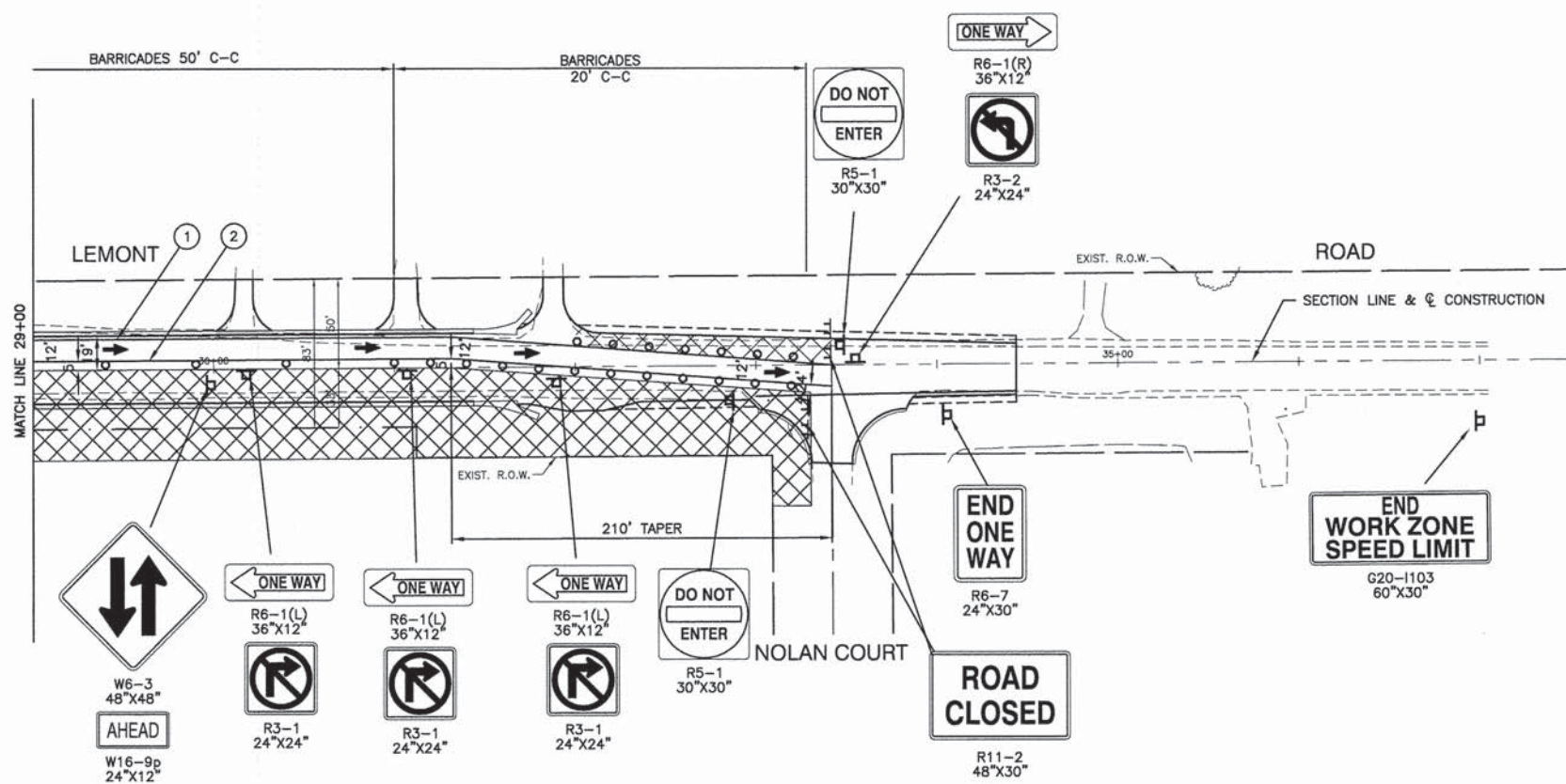
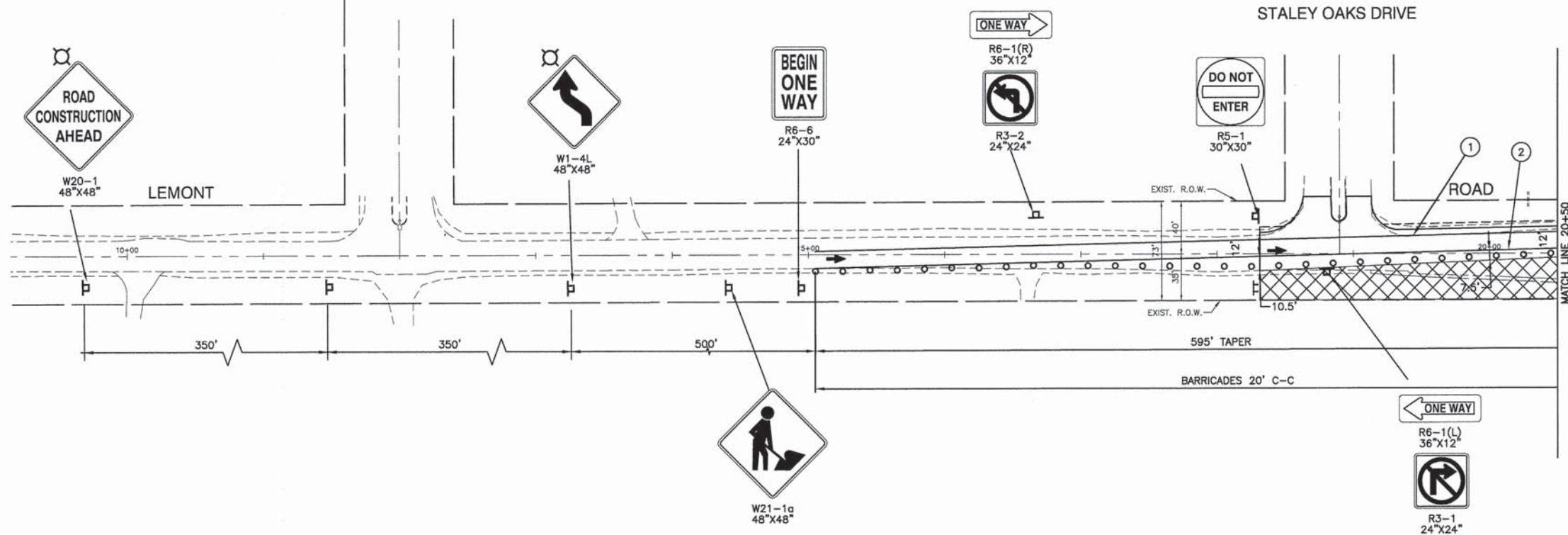
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| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- RG | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING - STAGE 3
SCALE: 1"=50' SHEET NO. 20 OF 96 SHEETS STA. TO STA.

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 20 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |

SECTION 5, TOWNSHIP 36, RANGE 11
SECTION 8, TOWNSHIP 36, RANGE 11



LEGEND

- ① 4" YELLOW EDGE LINE - TEMPORARY PAVEMENT MARKINGS
- ② 4" WHITE EDGE LINE - TEMPORARY PAVEMENT MARKINGS
- ③ 4" DOUBLE YELLOW CENTERLINE (11" C-C) - TEMPORARY PAVEMENT MARKINGS
- ④ 6" WHITE SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑤ 6" YELLOW SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑥ 24" WHITE STOP BAR - TEMPORARY PAVEMENT MARKINGS
- ⑦ TEMPORARY SYMBOLS AND LETTERS, LARGE
- ⑧ 4" WHITE SKIP DASH (10' LINE, 30' SPACE) - TEMPORARY PAVEMENT MARKINGS
- ⑨ 6" WHITE LANE LINE - TEMPORARY PAVEMENT MARKINGS
- ⑩ 12" YELLOW LINE - TEMPORARY PAVEMENT MARKINGS
- ⊗ WORK AREA
- ▨ TEMPORARY PAVEMENT TO BE CONSTRUCTED
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C) UNLESS OTHERWISE INDICATED
- ⊥ TYPE III BARRICADE WITH STEADY BURNING LIGHT
- ⊞ SIGN
- TYPE F BARRICADE
- CRASH ATTENUATOR
- ➔ TRAFFIC FLOW
- ⊞ MONO-DIRECTIONAL FLASHING BEACON

NOTES:
 1. MAINTAIN DRUMS ALONG CONSTRUCTION AREA TURNS.
 2. SEE DETOUR PLAN FOR ADDITIONAL SIGNAGE.
 3. ALL EXISTING PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING PLAN SHALL BE REMOVED PRIOR TO THE IMPLEMENTATION OF THE STAGING PLAN.

BENCH MARK:
 RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95

FILE NAME = 04273_02-TCON-02 - T03-3

| | | |
|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| PLOT SCALE = | CHECKED -- | REVISED -- |
| PLOT DATE = 05-30-13 | DRAWN -- RG | REVISED -- |
| | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

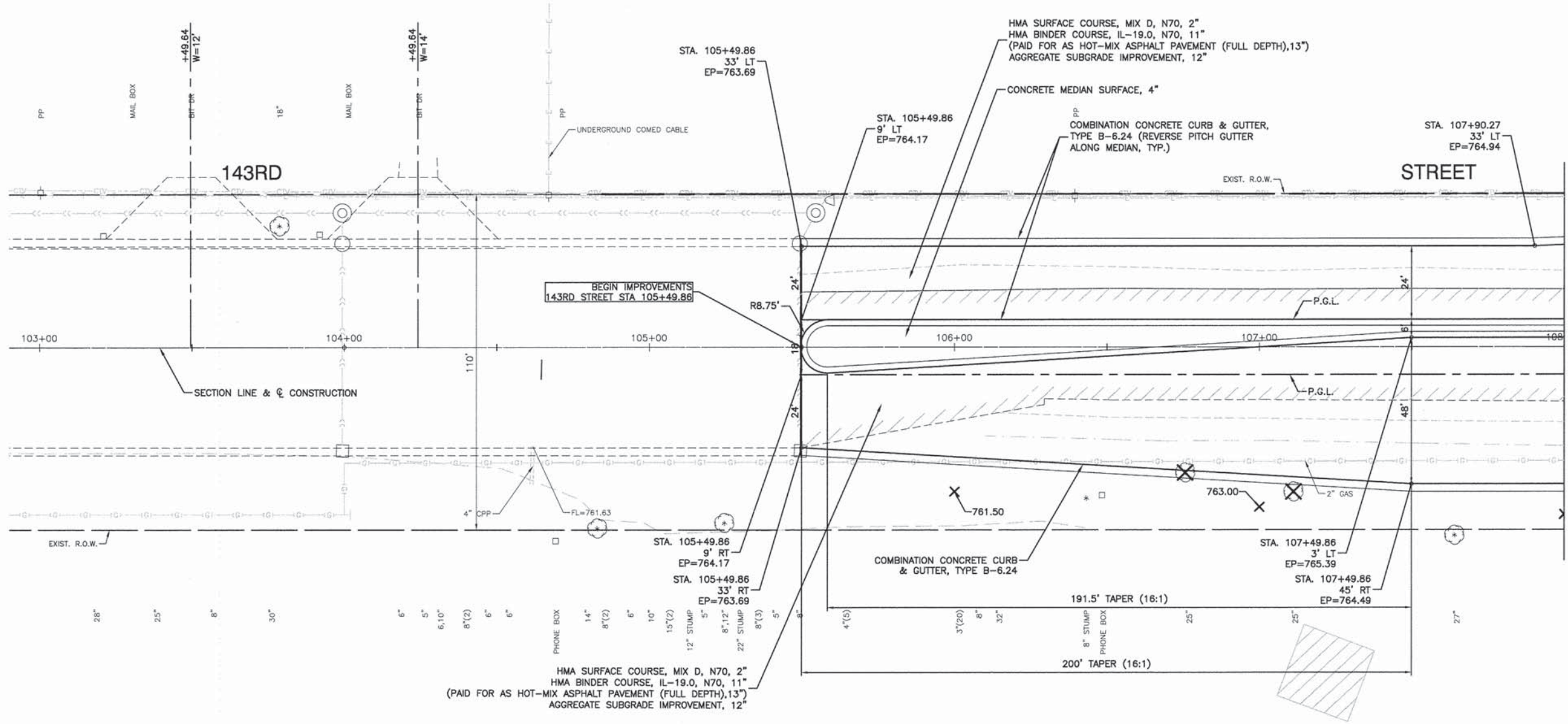
143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING - STAGE 3
SCALE: 1"=50' SHEET NO. 21 OF 96 SHEETS STA. TO STA.

| | | | | |
|-----------------------|----------------|------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 21 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT | M-8003 (562) | |

SECTION 5, TOWNSHIP 36, RANGE 11
SECTION 8, TOWNSHIP 36, RANGE 11



| | |
|--------------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLANNED | |
| ALIGNED | |
| NOTED | |
| RT. OF WAY CHECKED | |
| NO. _____ | |
| NOTE BOOK | |
| NO. _____ | |
| DATE | |
| BY | |
| SURVEYED | |
| PLANNED | |
| ALIGNED | |
| NOTED | |
| RT. OF WAY CHECKED | |
| NO. _____ | |
| NOTE BOOK | |
| NO. _____ | |

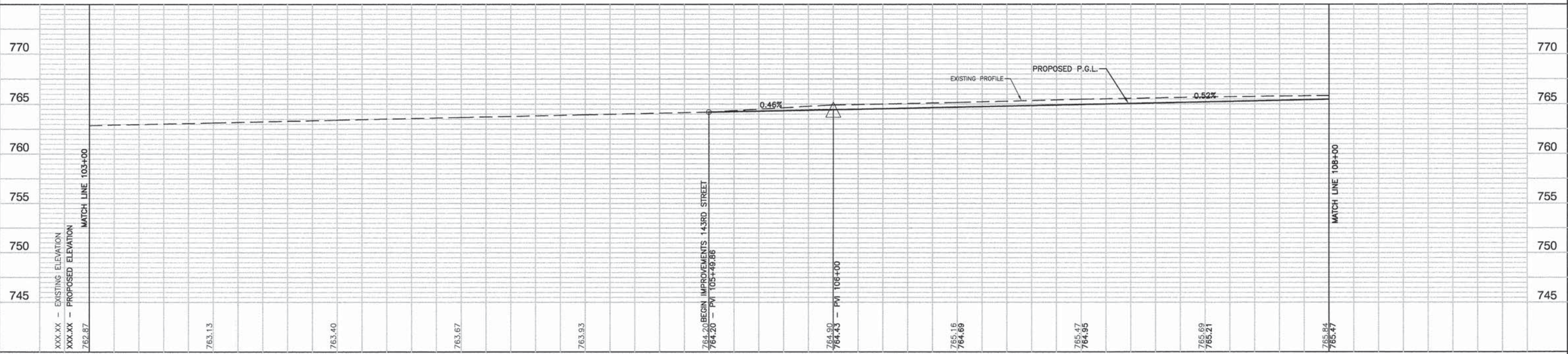


LEGEND

- TREE TO BE REMOVED
- PAVEMENT REMOVAL
- DRIVEWAY REMOVAL
- HMA SURFACE REMOVAL, 3/4"
- COMBINATION CURB & GUTTER REMOVAL
- HMA DRIVEWAY (SEE DISTRICT 1 STANDARD BD-01 OR BD-02)

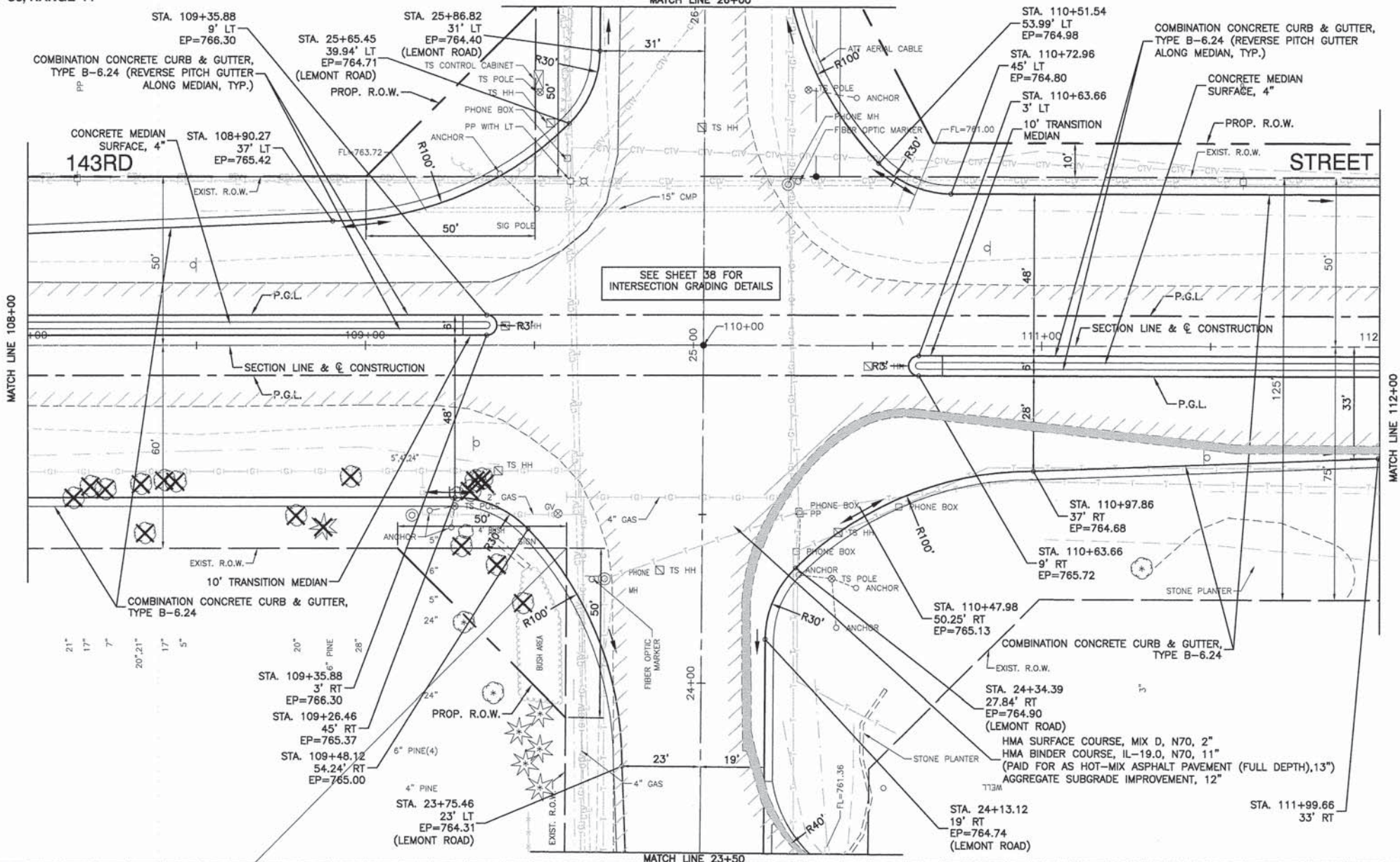
BENCH MARK:
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95

| | |
|------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLANNED | |
| ALIGNED | |
| NOTED | |
| B.M. NOTED | |
| NOTE BOOK | |
| NO. _____ | |
| DATE | |
| BY | |
| SURVEYED | |
| PLANNED | |
| ALIGNED | |
| NOTED | |
| B.M. NOTED | |
| NOTE BOOK | |
| NO. _____ | |



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|--------------------------------------|----------------------|----------------|----------------------------------|---|--|--|---------------------------|-------------------------------|----------------|--------------------|---------------|--|
| FILE NAME = 04273_02-PLPR01 - PLPR03 | USER NAME = | DESIGNED = HLG | REVISED = 01-22-09 - IDOT REVIEW | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION PLAN & PROFILE | | F.A.U. RITE: 1600 | SECTION: 04-00003-00-CH | COUNTY: WILL | TOTAL SHEETS: 96 | SHEET NO.: 22 | |
| | PLOT SCALE = | DRAWN = RG | REVISED = | | SCALE: H 1"=20' V 1"=5' | | SHEET NO. 22 OF 96 SHEETS | STA. 103+00 | TO STA. 108+00 | CONTRACT NO. 63147 | | |
| | PLOT DATE = 05-30-13 | CHECKED = AG | REVISED = | | FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT M-8003 (562) | | | | |
| | | | | | | | | | | | | |

LEMONT ROAD



SEE SHEET 38 FOR INTERSECTION GRADING DETAILS

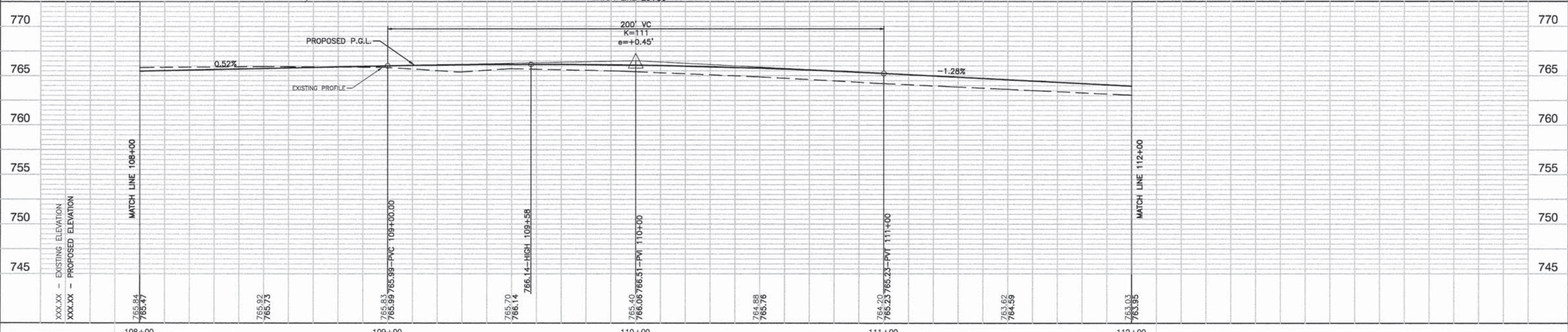


LEGEND

- TREE TO BE REMOVED
- PAVEMENT REMOVAL
- DRIVEWAY REMOVAL
- HMA SURFACE REMOVAL, 3/4"
- COMBINATION CURB & GUTTER REMOVAL
- HMA DRIVEWAY (SEE DISTRICT 1 STANDARD BD-01 OR BD-02)

BENCH MARK:

RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95



| | |
|--------------------|----|
| DATE | BY |
| SURVEYED | |
| PLOTTED | |
| CHECKED | |
| RT. OF WAY CHECKED | |
| ROAD FILE NAME | |
| PLAN | |
| NOTE BOOK NO. | |

| | |
|------------|----|
| DATE | BY |
| SURVEYED | |
| PLOTTED | |
| CHECKED | |
| B.M. NOTED | |
| PROFILE | |
| NOTE BOOK | |

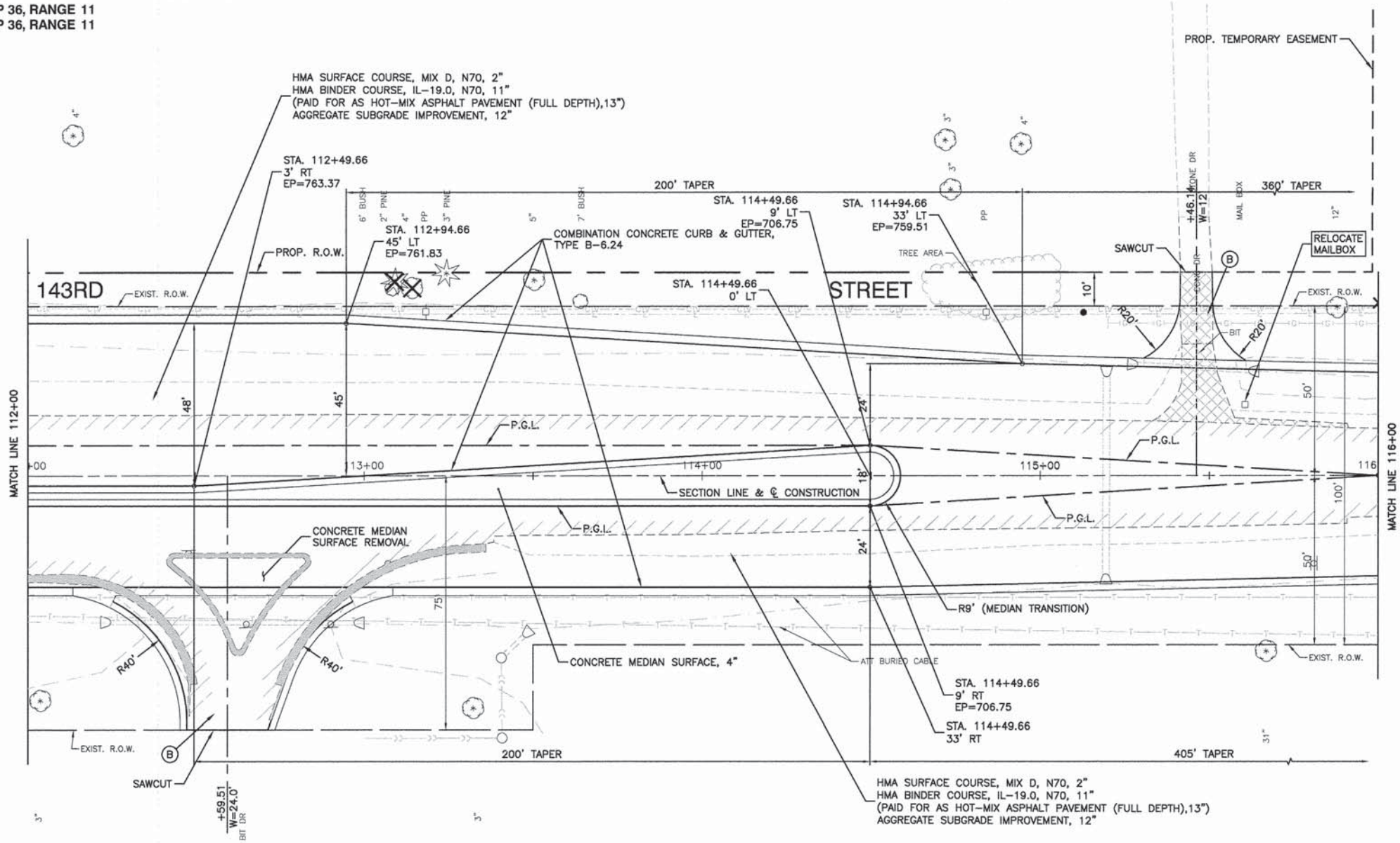
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|---------------------------------------|----------------------|----------------|---------------------------------|---|---|----------------|------------|---------------------------|--------|----------------------------|-----------------------|----------|-------------------------------|
| FILE NAME = 04273_02-PLPR-01 - PLPR04 | USER NAME = | DESIGNED = HLG | REVISD = 01-22-09 - IDOT REVIEW | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION PLAN & PROFILE | | F.A.U. RTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | | |
| | PLOT SCALE = | CHECKED = | REVISD = | | 1600 | 04-00003-00-CH | WILL | 96 | 23 | | | | |
| | PLOT DATE = 05-30-13 | DRAWN = RG | REVISD = | | SCALE: H 1"=20' V 1"=5' | | | SHEET NO. 23 OF 96 SHEETS | | STA. 108+00 TO STA. 112+00 | FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT M-8003 (562) |
| | | CHECKED = AG | REVISD = | | | | | | | | | | |

SECTION 5, TOWNSHIP 36, RANGE 11
SECTION 8, TOWNSHIP 36, RANGE 11



| | | |
|------|--------------------|------|
| PLAN | SURVEYED | DATE |
| | PLOTTED | |
| | CHECKED | |
| | BY | |
| | NO. OF WAY CHECKED | |
| | DATE FILE NAME | |

| | | |
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| PROFILE | SURVEYED | DATE |
| | PLOTTED | |
| | CHECKED | |
| | BY | |
| | B.M. NOTED | |

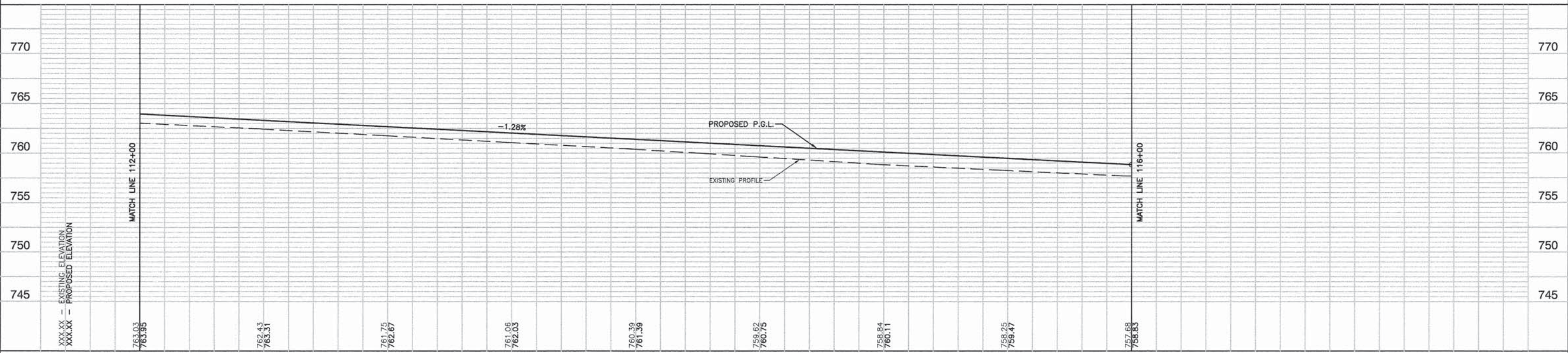


LEGEND

- TREE TO BE REMOVED
- PAVEMENT REMOVAL
- DRIVEWAY REMOVAL
- HMA SURFACE REMOVAL, 3/4"
- COMBINATION CURB & GUTTER REMOVAL
- HMA DRIVEWAY
(SEE DISTRICT 1 STANDARD BD-01 OR BD-02)

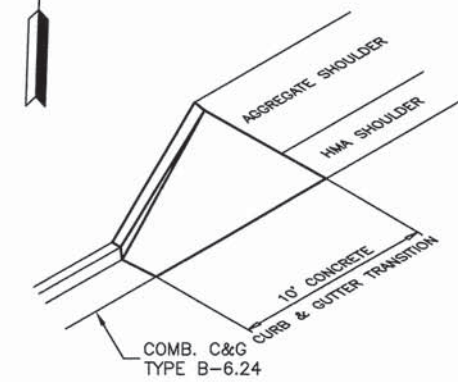
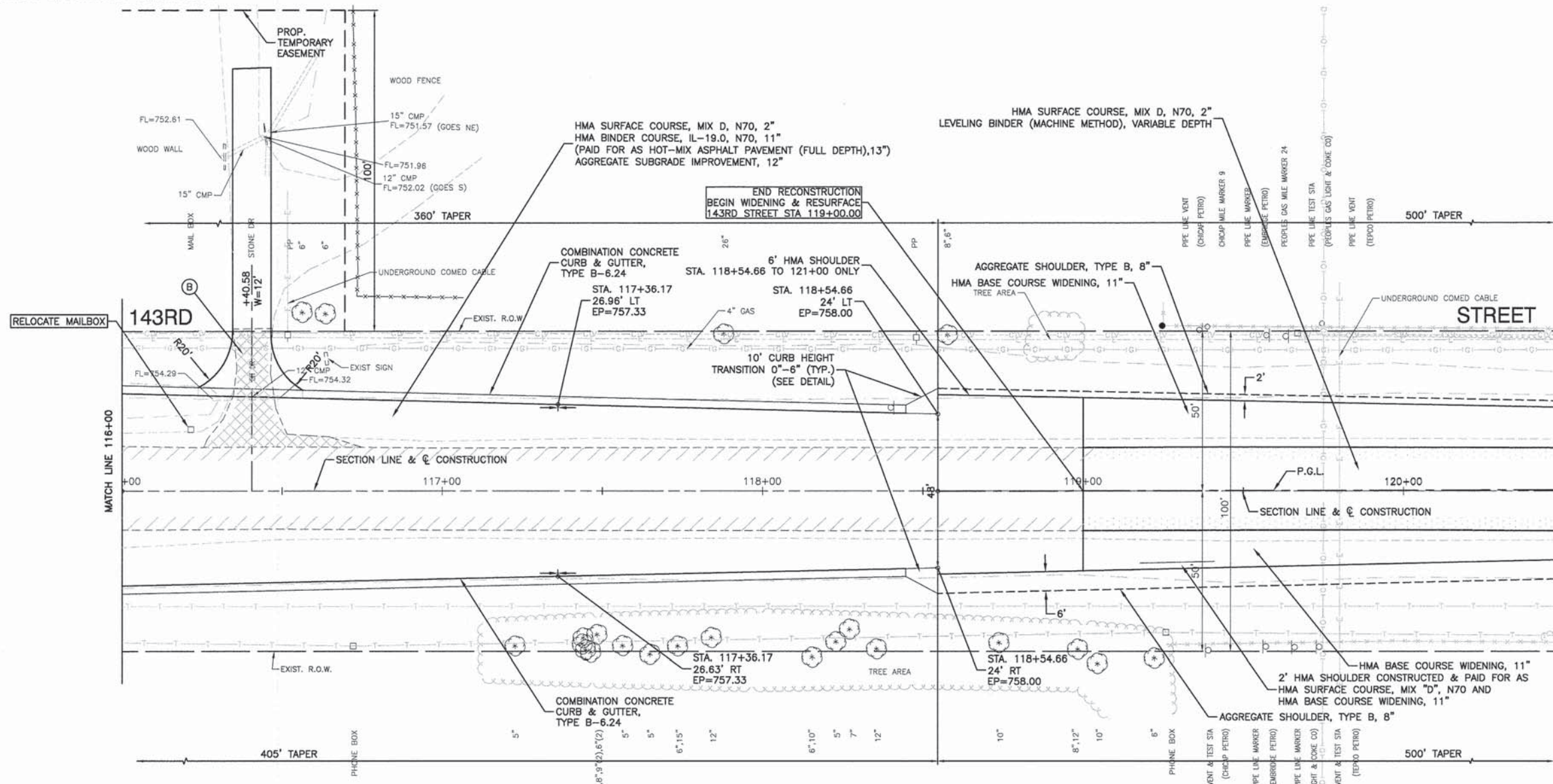
BENCH MARK:

RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95



| | | | | | | | | | | | | |
|---------------------------------------|----------------------|----------------|----------------------------------|---|--|--|--|-------------------------|--------------------------|---------------------------|-------------------|----------------------------|
| FILE NAME = 04273_02-PLPR-01 - PLPR05 | USER NAME = | DESIGNED = HLG | REVISED = 01-22-09 - IDOT REVIEW | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | | 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION PLAN & PROFILE | | F.A.U. RTE. = 1600 | SECTION = 04-00003-00-CH | COUNTY = WILL | TOTAL SHEETS = 96 | SHEET NO. = 24 |
| | PLOT SCALE = | CHECKED = | REVISED = | | | | | SCALE: H 1"=20' V 1"=5' | | SHEET NO. 24 OF 96 SHEETS | | STA. 112+00 TO STA. 116+00 |
| | PLOT DATE = 05-30-13 | CHECKED = AG | REVISED = | | | | | | | | | CONTRACT NO. 63147 |

SECTION 5, TOWNSHIP 36, RANGE 11
SECTION 8, TOWNSHIP 36, RANGE 11



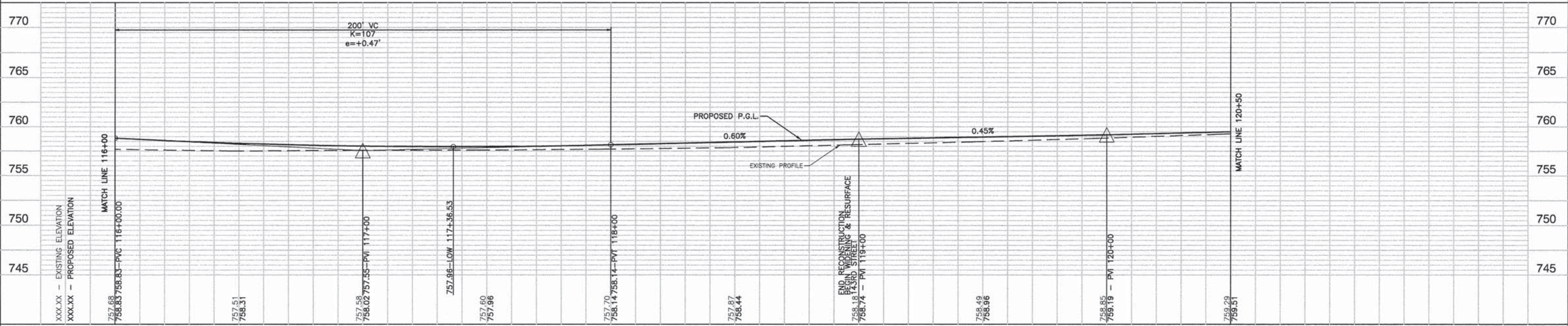
CURB HEIGHT TRANSITION DETAIL

LEGEND

- TREE TO BE REMOVED
- PAVEMENT REMOVAL
- DRIVEWAY REMOVAL
- HMA SURFACE REMOVAL, 3/4"
- COMBINATION CURB & GUTTER REMOVAL
- HMA DRIVEWAY (SEE DISTRICT 1 STANDARD BD-01 OR BD-02)

BENCH MARK:

RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95



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| PLAN | SURVEYED | DATE |
| | PLOTTED | |
| | CHECKED | |
| | BY | |
| | NOTE BOOK | |
| | NO. | |
| | DATE | |

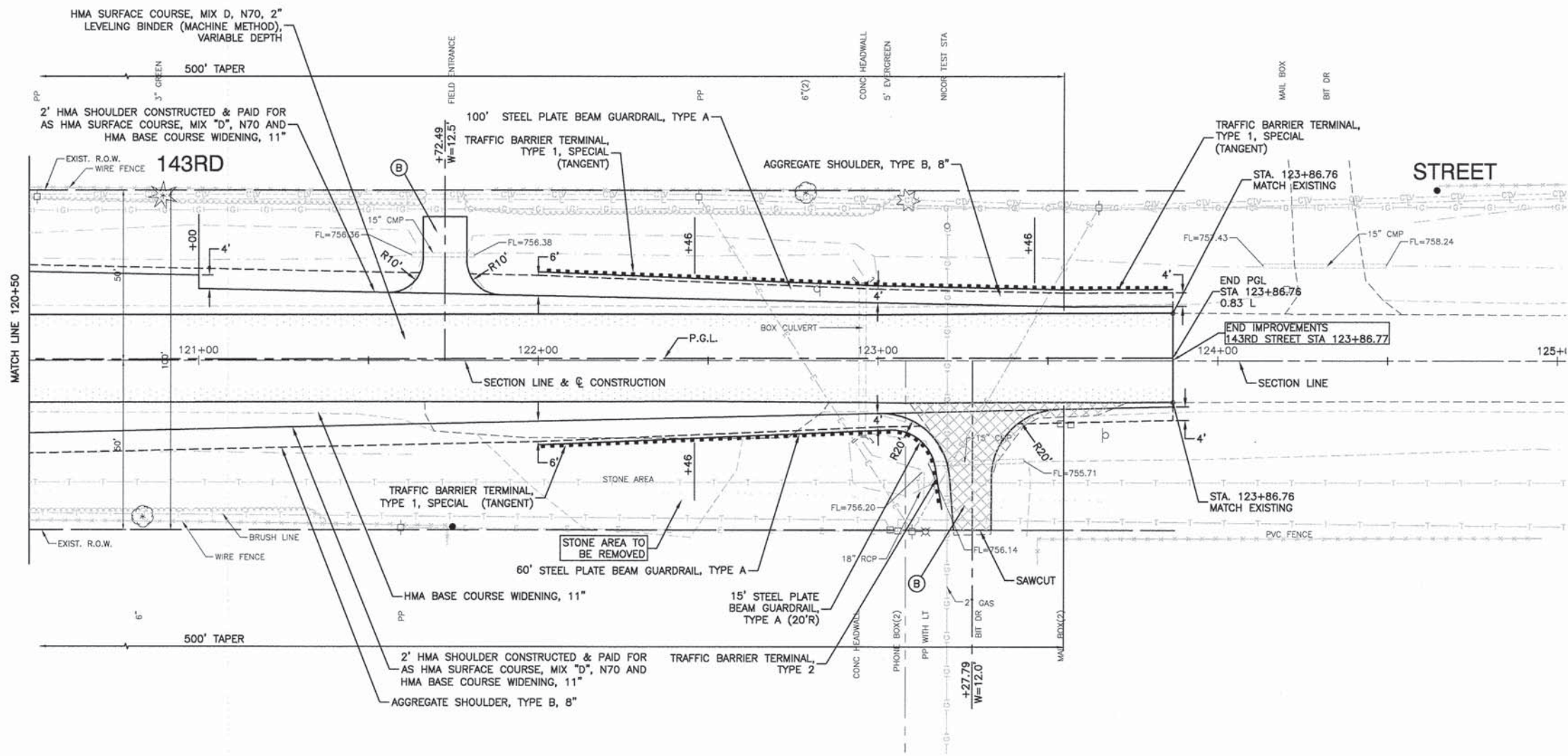
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| PROFILE | SURVEYED | DATE |
| | PLOTTED | |
| | CHECKED | |
| | BY | |
| | NOTE BOOK | |
| | NO. | |
| | DATE | |

SECTION 5, TOWNSHIP 36, RANGE 11
SECTION 8, TOWNSHIP 36, RANGE 11



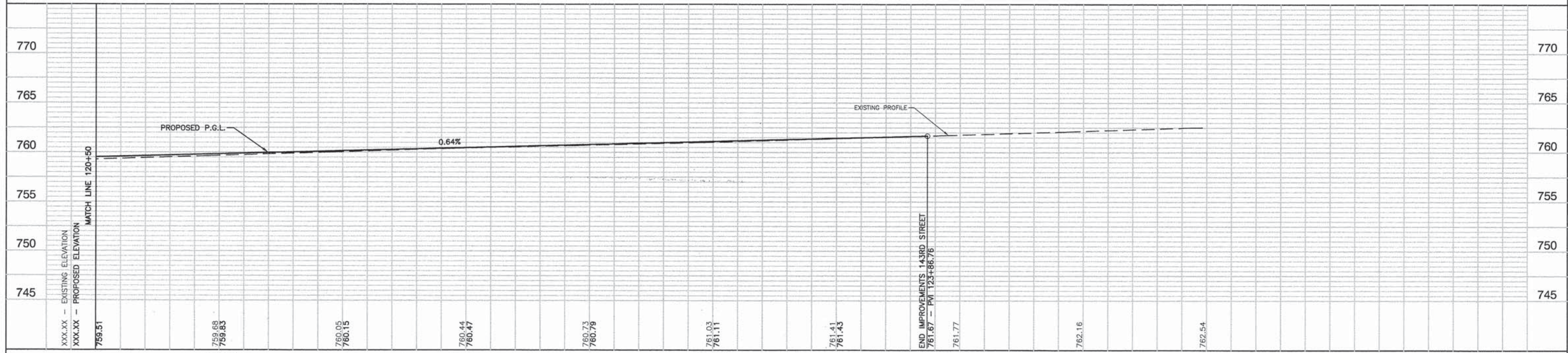
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|--------------------|--|
| DATE | |
| BY | |
| PLAN | |
| SURVEYED | |
| PLOTTED | |
| CHECKED | |
| RT. OF WAY CHECKED | |
| ROAD FILE NAME | |
| NO. | |

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| DATE | |
| BY | |
| PROFILE | |
| SURVEYED | |
| PLOTTED | |
| CHECKED | |
| B.M. NOTED | |
| NO. | |



- LEGEND**
- TREE TO BE REMOVED
 - PAVEMENT REMOVAL
 - DRIVEWAY REMOVAL
 - HMA SURFACE REMOVAL, 3/4"
 - COMBINATION CURB & GUTTER REMOVAL
 - HMA DRIVEWAY (SEE DISTRICT 1 STANDARD BD-01 OR BD-02)

BENCH MARK:
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95



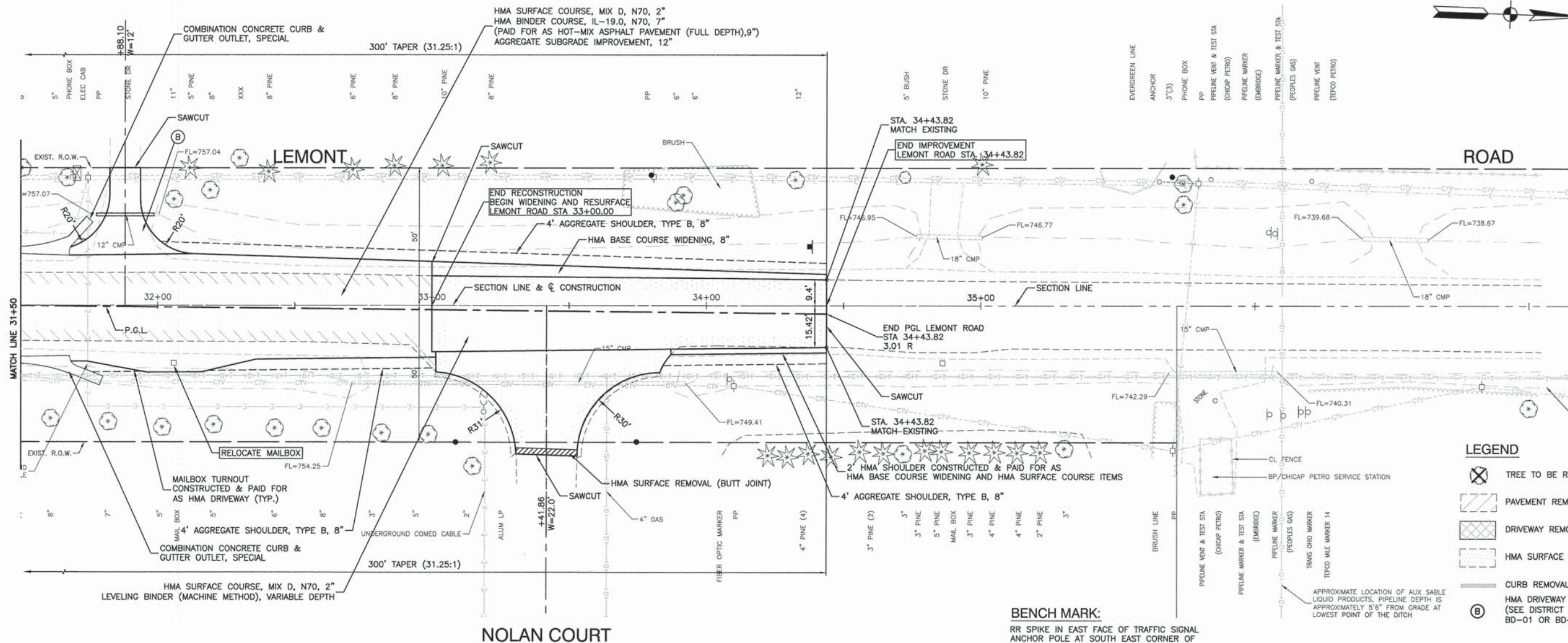
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|---------------------------------------|----------------------|-----------------|-----------------------------------|---|--|--|--|------------------------|-------------|-----------------|--------------|
| FILE NAME = 04273_02-PLPR-01 - PLPR07 | USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION PLAN & PROFILE | | F.A.U. RTE. 1600 | SECTION 04-00003-00-CH | COUNTY WILL | TOTAL SHEETS 96 | SHEET NO. 26 |
| | PLOT SCALE = | DRAWN -- RG | REVISED -- | | SCALE: H 1"=20' V 1"=5' SHEET NO. 26 OF 96 SHEETS STA. 120+50 TO STA. 123+86.76 | | CONTRACT NO. 63147 | | | | |
| | PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |

SECTION 5, TOWNSHIP 36, RANGE 11
SECTION 8, TOWNSHIP 36, RANGE 11

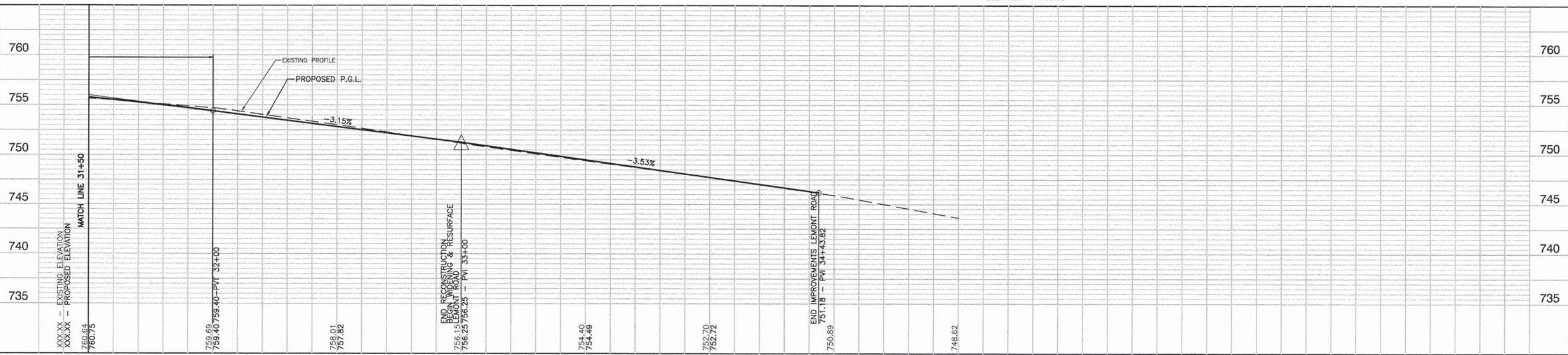


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| DATE | |
| BY | |
| PLAN | |
| NO. | |
| DATE | |
| BY | |
| PROFILE | |
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| DATE | |
| BY | |
| PROFILE | |
| NO. | |



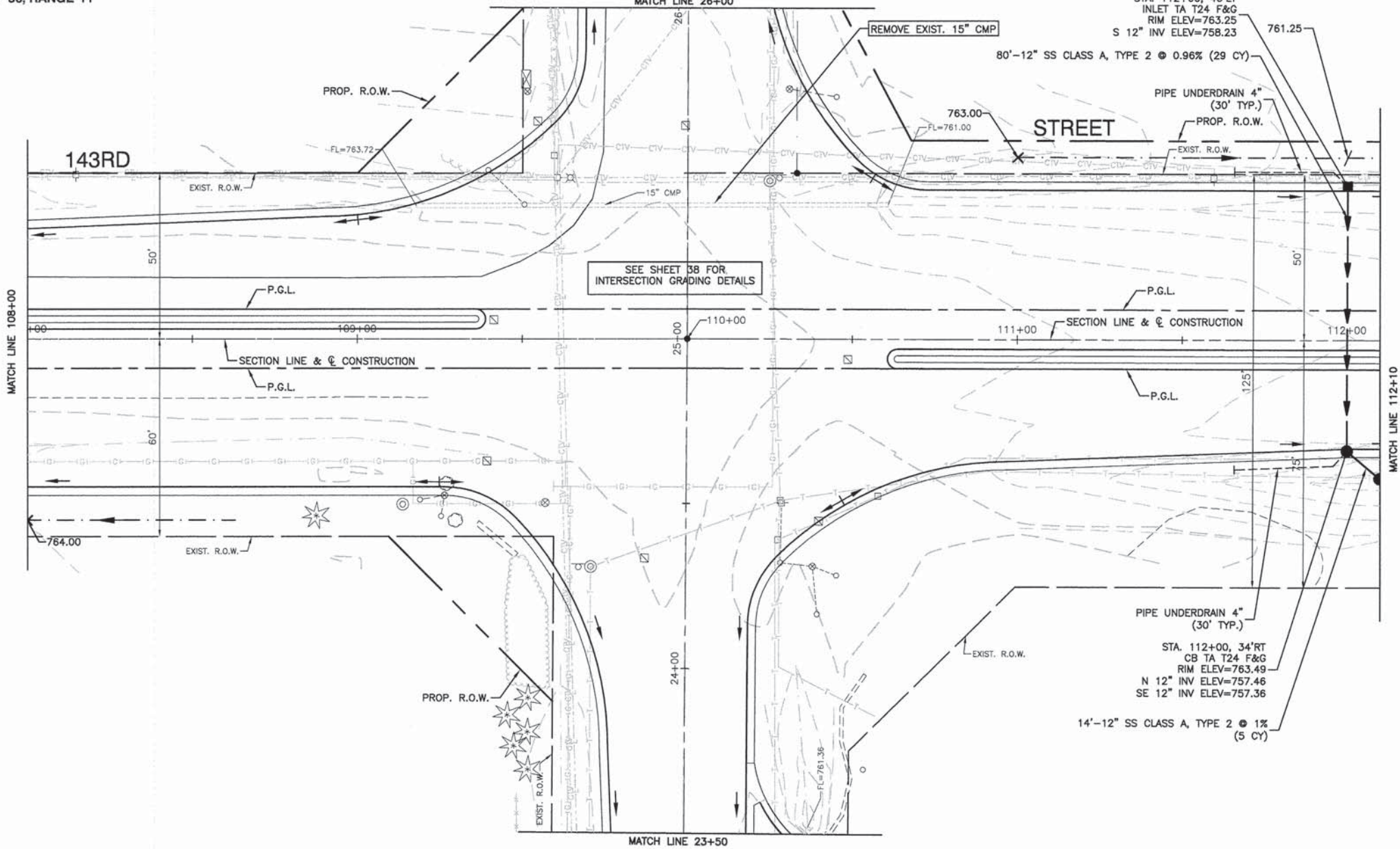
BENCH MARK:
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95



| | | | | | | | | | | | | |
|---------------------------------------|----------------------|----------------|----------------------------------|---|---|---------------------------|-----------------------------|--------------------------|---------------|-------------------|----------------|--|
| FILE NAME = 04273_02-PLPR-01 - PLPR10 | USER NAME = | DESIGNED = HLG | REVISED = 01-22-09 - IDOT REVIEW | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION PLAN & PROFILE | | F.A.U. RTE. = 1600 | SECTION = 04-00003-00-CH | COUNTY = WILL | TOTAL SHEETS = 96 | SHEET NO. = 29 | |
| | PLOT SCALE = | CHECKED = | REVISED = | | SCALE: H 1"=20' V 1"=5' | SHEET NO. 29 OF 96 SHEETS | STA. 31+50 TO STA. 34+43.82 | CONTRACT NO. 63147 | | | | |
| | PLOT DATE = 05-30-13 | CHECKED = AG | REVISED = | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | | | | |
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SECTION 5, TOWNSHIP 36, RANGE 11
SECTION 8, TOWNSHIP 36, RANGE 11

LEMONT ROAD

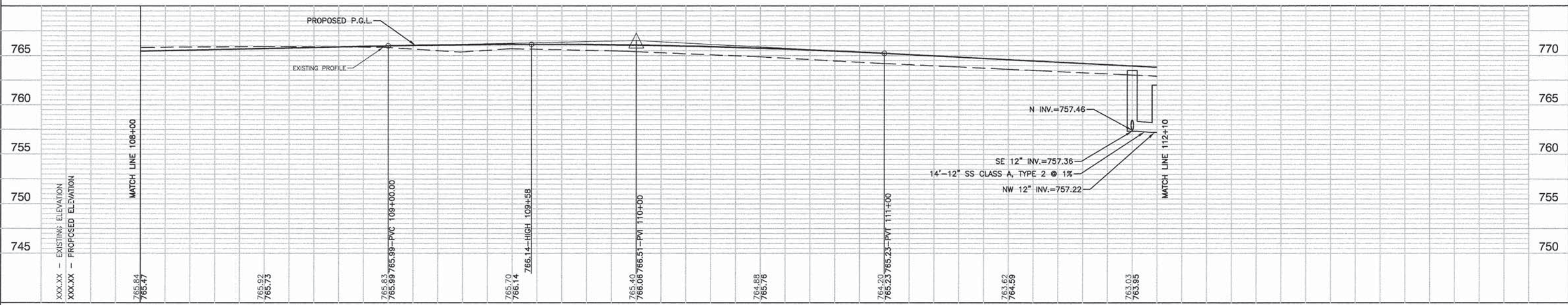


SEE SHEET 38 FOR INTERSECTION GRADING DETAILS

STA. 112+00, 46'LT
INLET TA T24 F&G
RIM ELEV=763.25
S 12" INV ELEV=758.23

PIPE UNDERDRAIN 4"
(30' TYP.)
STA. 112+00, 34'RT
CB TA T24 F&G
RIM ELEV=763.49
N 12" INV ELEV=757.46
SE 12" INV ELEV=757.36
14'-12" SS CLASS A, TYPE 2 @ 1%
(5 CY)

BENCH MARK:
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT
SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD.
ELEVATION = 765.95



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| PLAN | SURVEYED | DATE |
| | PLOTTED | BY |
| | CHECKED | |
| | RT. OF WAY CHECKED | |
| | NO. _____ | DAOD FILE NAME |

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| PROFILE | SURVEYED | DATE |
| | PLOTTED | BY |
| | CHECKED | |
| | B.M. NOTED | |
| | NO. _____ | |

FILE NAME = 04273_02-PLPR-01 - DRAIN04

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|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| PLOT SCALE = | CHECKED -- | REVISED -- |
| PLOT DATE = 05-30-13 | DRAWN -- RG | REVISED -- |
| | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

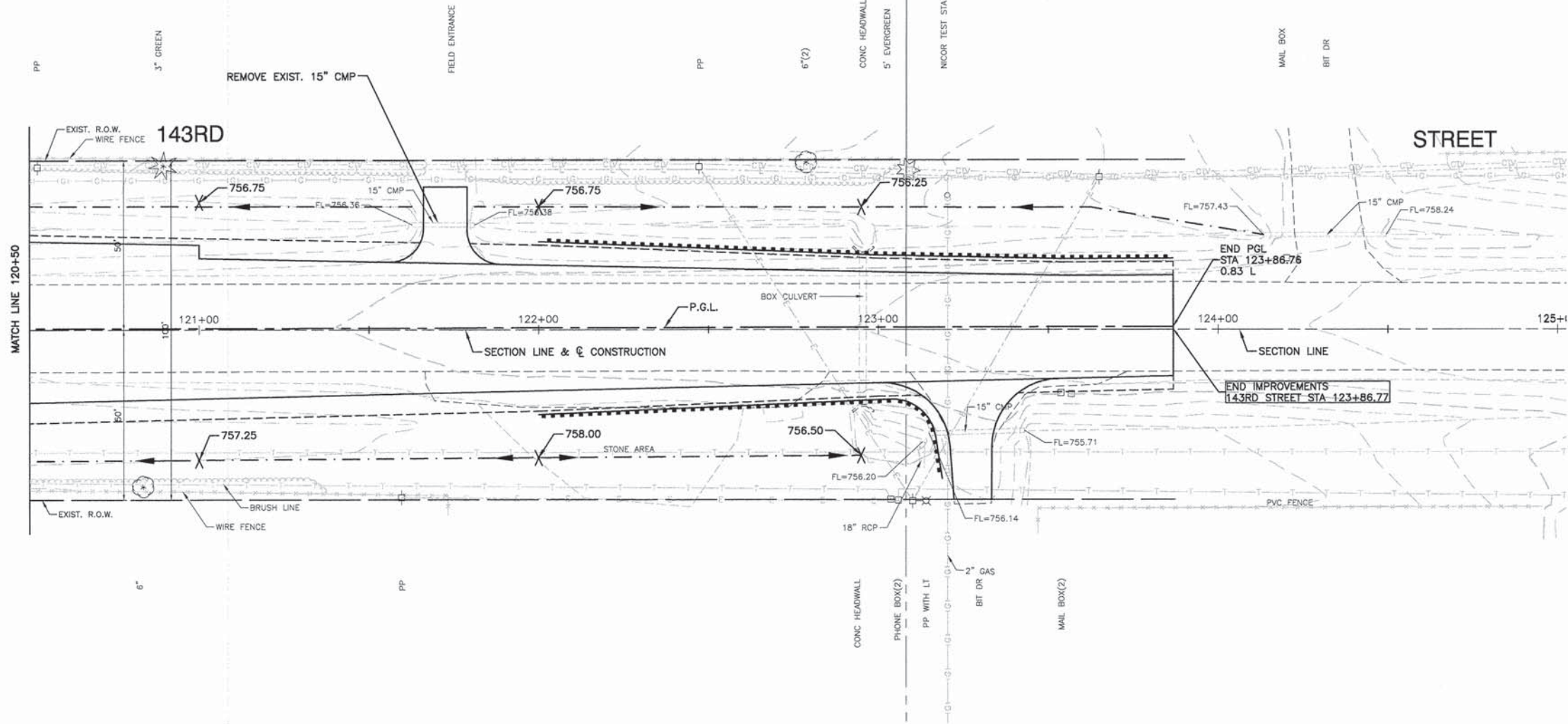
143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
DRAINAGE AND UTILITIES
SCALE: H 1"=20' V 1"=5'
SHEET NO. 31 OF 96 SHEETS
STA. 108+00 TO STA. 112+00

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 31 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |

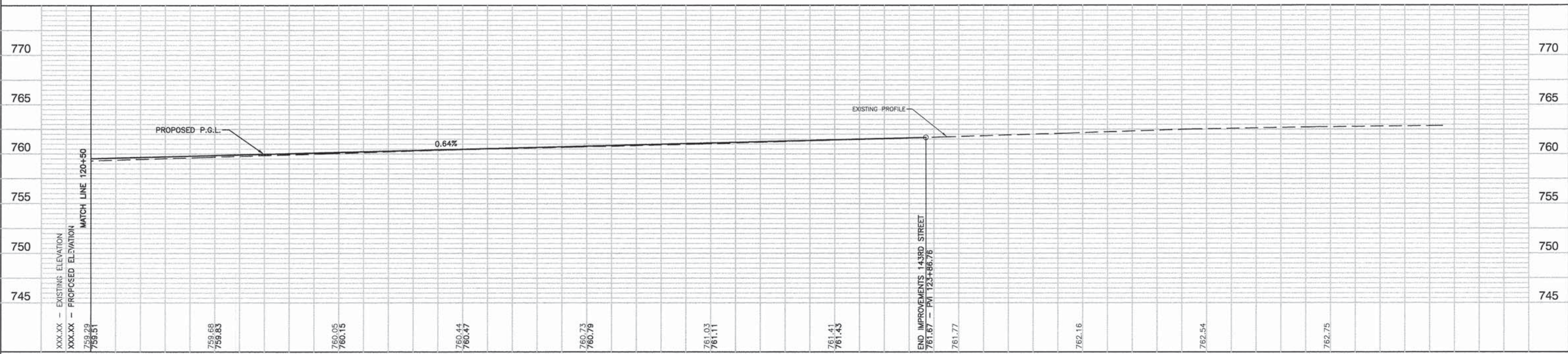


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| PLAN | SURVEYED | DATE |
| | PLOTTED | |
| | CHECKED | |
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| | DATE FILE NAME | |

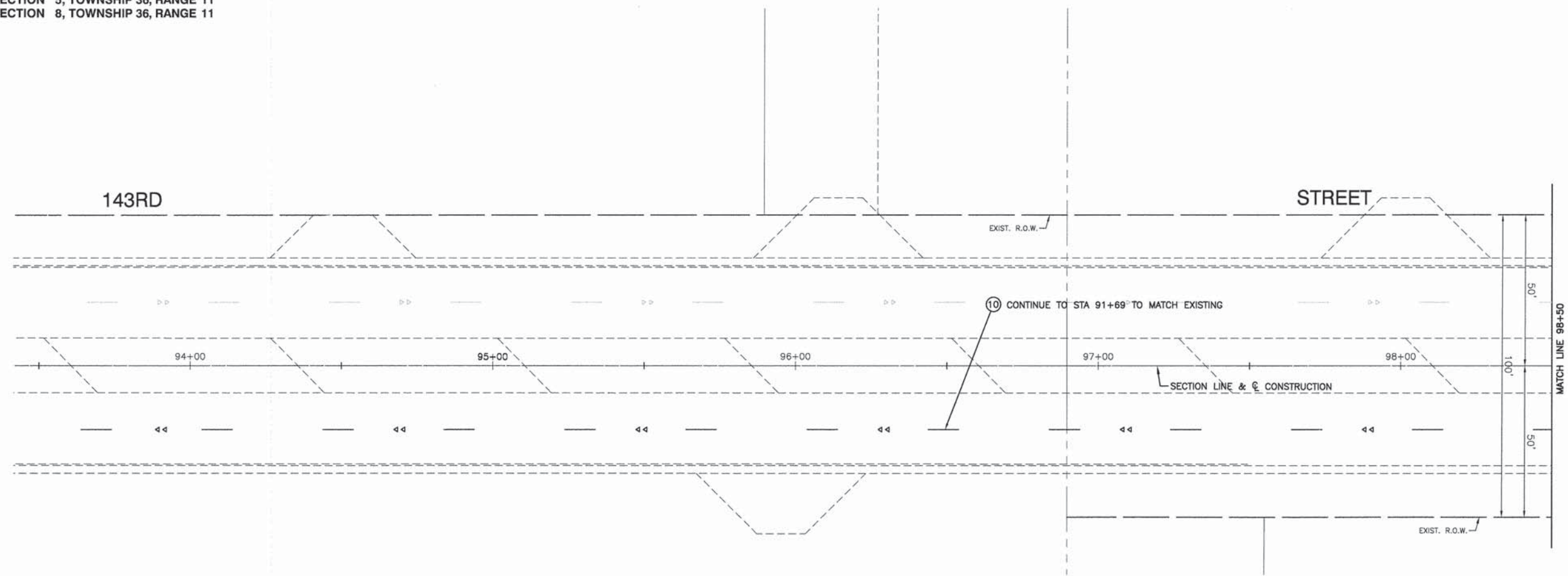
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|---------|--------------------|------|
| PROFILE | SURVEYED | DATE |
| | PLOTTED | |
| | CHECKED | |
| | BY | |
| | NO. OF WAY CHECKED | |
| | DATE FILE NAME | |



BENCH MARK:
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95



| | | | | | | | | | | | | |
|--|----------------------|----------------|----------------------------------|---|--|---|---------------------------|-------------------------------|--------------------------|---------------|-------------------------------|----------------|
| FILE NAME = 04273_02-PLPR-01 - DRAIN07 | USER NAME = | DESIGNED - HLG | REVISOR - 01-22-09 - IDOT REVIEW | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | | 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION DRAINAGE AND UTILITIES | | F.A.U. RTE. = 1600 | SECTION = 04-00003-00-CH | COUNTY = WILL | TOTAL SHEETS = 96 | SHEET NO. = 34 |
| | PLOT SCALE = | DRAWN - RG | REVISOR - | | | SCALE: H 1"=20' V 1"=5' | SHEET NO. 34 OF 96 SHEETS | STA. 120+50 TO STA. 123+86.76 | FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT M-8003 (562) | |
| | PLOT DATE = 05-30-13 | CHECKED - AG | REVISOR - | | | | | | | | | |



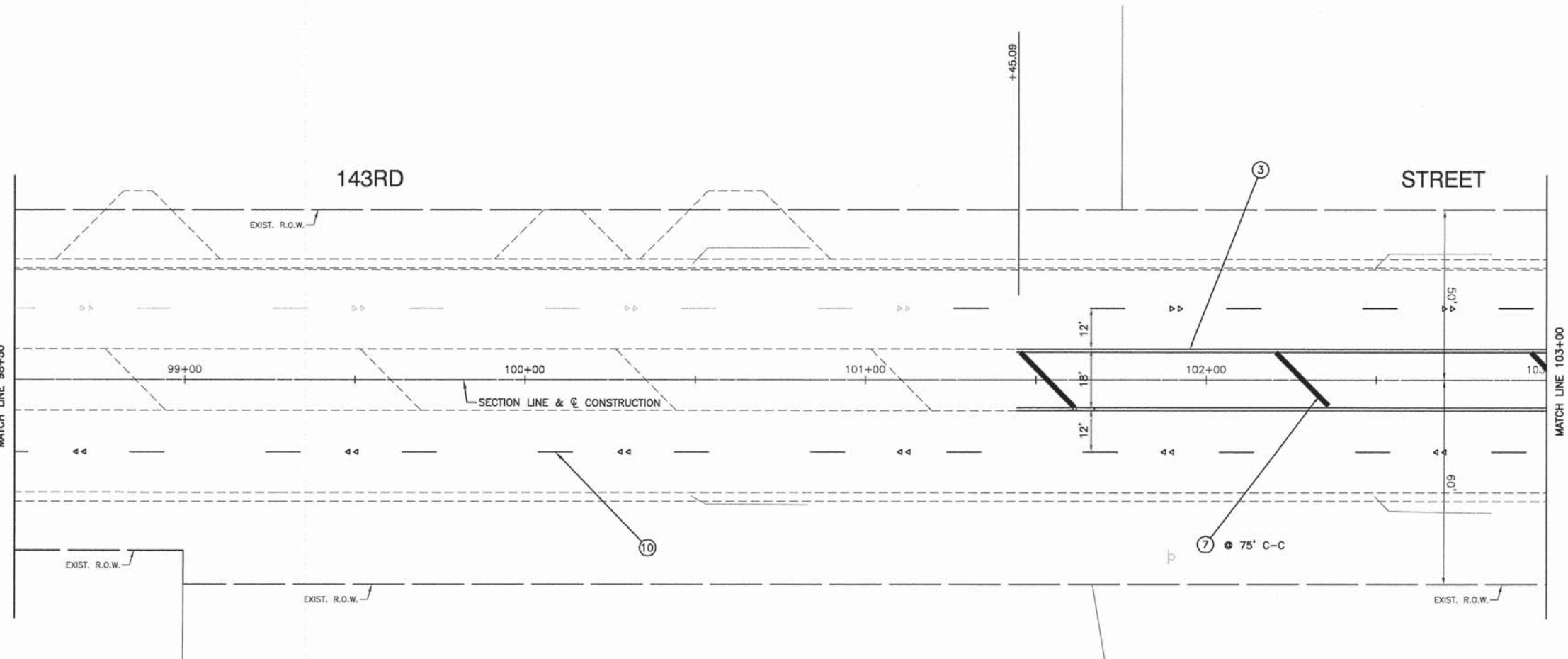
- NOTES:**
1. ALL EXISTING SIGNS THAT CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND STORED OFFSITE. ALL EXISTING SIGNS SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. ANY SIGNS OR POSTS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE COST OF THE CONTRACTOR.
 2. ALL DISTURBED AREAS IN THE ROW SHALL BE RESTORED WITH SEEDING, CLASS 2A.

PAVEMENT MARKING LEGEND

- ① WHITE LETTERS & SYMBOLS
- ② DOUBLE 4" YELLOW CENTERLINE (11" C/C)
- ③ DOUBLE YELLOW MEDIAN OUTLINE (11" C/C)
- ④ 4" YELLOW LINE
- ⑤ 4" WHITE EDGE LINE
- ⑥ 6" WHITE LANE LINE
- ⑦ NOT USED
- ⑧ 12" YELLOW DIAGONAL LINE
- ⑨ 24" WHITE STOP BAR
- ⑩ 12" WHITE LINE (45° ANGLE)
- ⑪ 4" WHITE SKIP - DASH LINE (10' LINE - 30' SPACE)
- ⑫ 4" YELLOW SKIP - DASH LINE (10' LINE - 30' SPACE)
- ▲ ONE-WAY AMBER MARKER 40' C/C UNLESS OTHERWISE INDICATED
- ◀ ONE-WAY CRYSTAL MARKER 80' C/C UNLESS OTHERWISE INDICATED
- ◆ TWO-WAY AMBER MARKER 40' C/C UNLESS OTHERWISE INDICATED
- ⊠ TRAFFIC SIGN

- NOTE:**
- 1.) SEE IDOT STANDARD TC-13 FOR PAVEMENT MARKING DETAILS.
 - 2.) ALL LEGEND ITEMS ARE THERMOPLASTIC PAVEMENT MARKING OF THE WIDTH AND TYPE SPECIFIED.
 - 3.) ALL RELOCATED SIGN PANELS WILL BE INSTALLED WITH NEW TELESCOPING STEEL SIGN SUPPORTS.
 - 4.) RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE OFFSET FOUR (4) INCHES FROM STRIPING IN SKIP-DASH AREAS.

BENCH MARK:
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95



| | | | |
|---------------------------------------|----------------------|-----------------|-----------------------------------|
| FILE NAME = 04273_02-PLPR-01 - PMK001 | USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | | CHECKED -- | REVISED -- |
| | PLOT SCALE = | DRAWN -- RG | REVISED -- |
| | PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

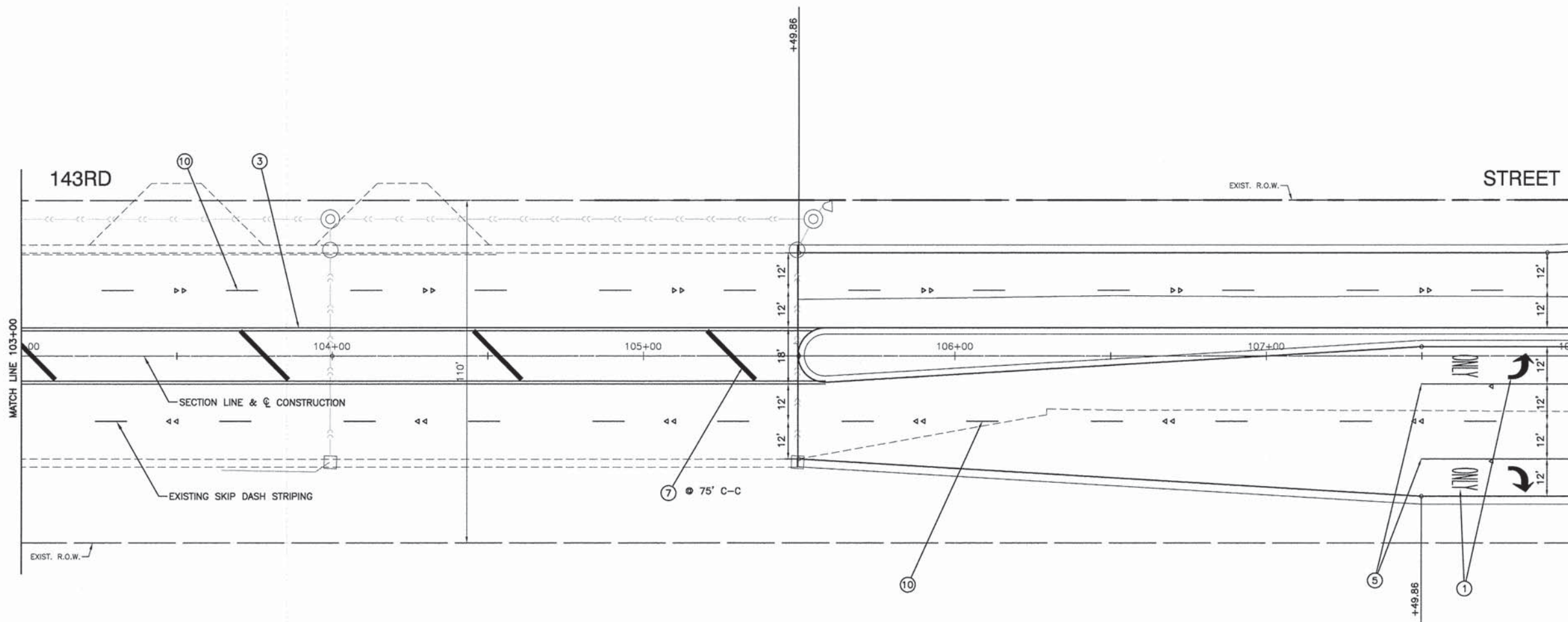
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
PAVEMENT MARKING, SIGNING & LANDSCAPING PLAN

| | | | | |
|--------------------|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 39 |
| CONTRACT NO. 63147 | | | | |

SCALE: 1"=20' SHEET NO. 39 OF 96 SHEETS STA. 96+25 TO STA. 103+00

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562)



NOTES:

1. ALL EXISTING SIGNS THAT CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND STORED OFFSITE. ALL EXISTING SIGNS SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. ANY SIGNS OR POSTS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE COST OF THE CONTRACTOR.
2. ALL DISTURBED AREAS IN THE ROW SHALL BE RESTORED WITH SEEDING, CLASS 2A.

PAVEMENT MARKING LEGEND

- ① WHITE LETTERS & SYMBOLS
- ② DOUBLE 4" YELLOW CENTERLINE (11" C/C)
- ③ DOUBLE YELLOW MEDIAN OUTLINE (11" C/C)
- ④ 4" YELLOW LINE
- ⑤ 4" WHITE EDGE LINE
- ⑥ 6" WHITE LANE LINE
- ⑦ NOT USED
- ⑧ 12" YELLOW DIAGONAL LINE
- ⑨ 24" WHITE STOP BAR
- ⑩ 12" WHITE LINE (45° ANGLE)
- ⑪ 4" WHITE SKIP - DASH LINE (10' LINE - 30' SPACE)
- ⑫ 4" YELLOW SKIP - DASH LINE (10' LINE - 30' SPACE)
- ▲ ONE-WAY AMBER MARKER
40' C/C UNLESS OTHERWISE INDICATED
- ◄ ONE-WAY CRYSTAL MARKER
80' C/C UNLESS OTHERWISE INDICATED
- ◆ TWO-WAY AMBER MARKER
40' C/C UNLESS OTHERWISE INDICATED

NOTE:

- 1.) SEE IDOT STANDARD TC-13 FOR PAVEMENT MARKING DETAILS.
- 2.) ALL LEGEND ITEMS ARE THERMOPLASTIC PAVEMENT MARKING OF THE WIDTH AND TYPE SPECIFIED.
- 3.) ALL RELOCATED SIGN PANELS WILL BE INSTALLED WITH NEW TELESCOPING STEEL SIGN SUPPORTS.
- 4.) RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE OFFSET FOUR (4) INCHES FROM STRIPING IN SKIP-DASH AREAS.

BENCH MARK:

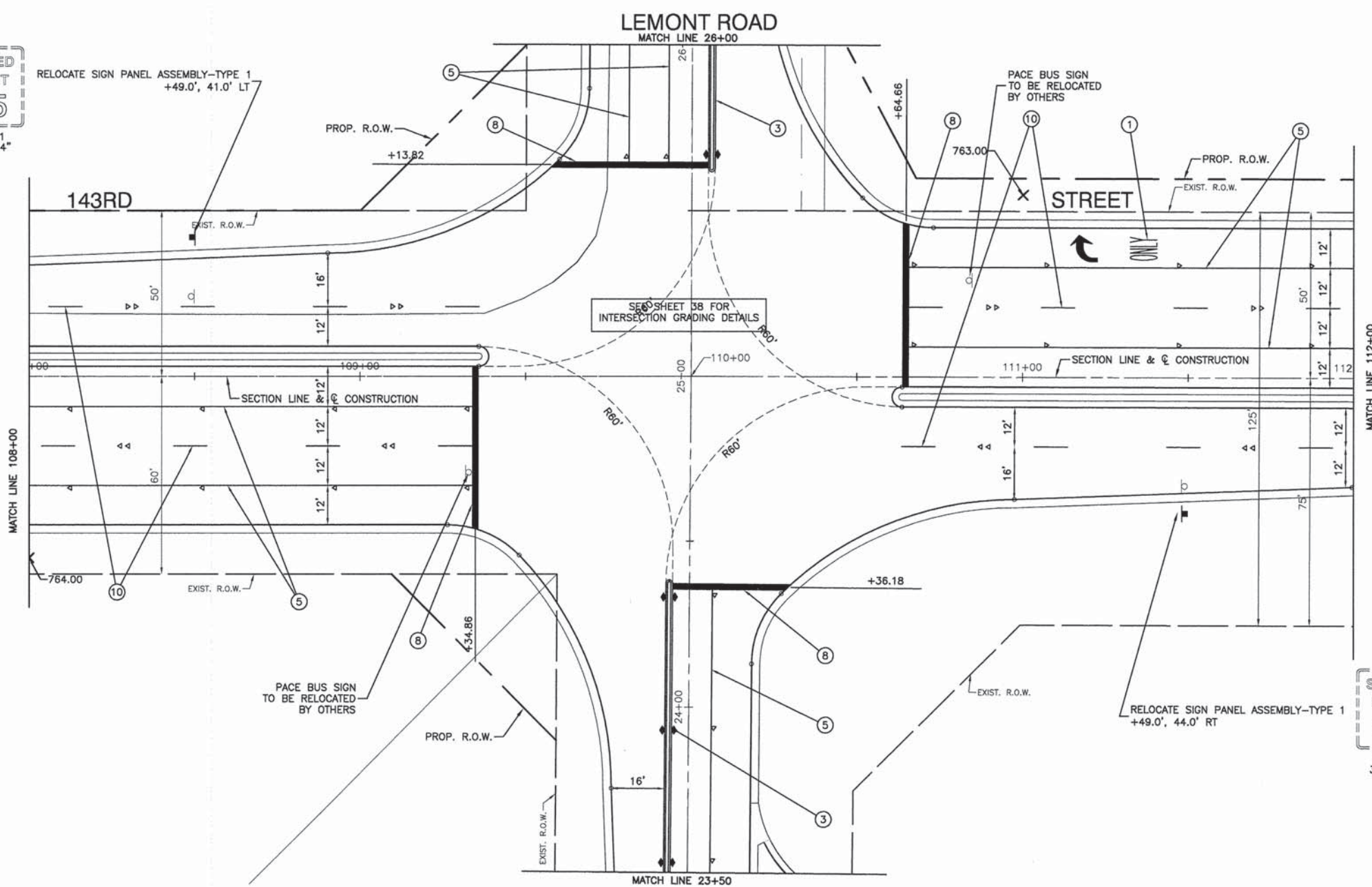
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95

| | | | |
|--------------------------------------|----------------------|-----------------|----------------------------------|
| FILE NAME = 04273_02-PLPR-01 - PMK02 | USER NAME = | DESIGNED -- HLG | REVISD -- 01-22-09 - IDOT REVIEW |
| | | CHECKED -- | REVISD -- |
| | PLOT SCALE = | DRAWN -- RG | REVISD -- |
| | PLOT DATE = 05-30-13 | CHECKED -- AG | REVISD -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | |
|---|---------------------------|----------------------------|--|
| 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION PAVEMENT MARKING, SIGNING & LANDSCAPING PLAN | | | |
| SCALE: 1"=20' | SHEET NO. 40 OF 96 SHEETS | STA. 103+00 TO STA. 108+00 | |

| | | | | |
|--------------------------------|----------------|--------|-------------------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 40 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | FED. AID PROJECT M-8003 (562) | |



NOTES:

1. ALL EXISTING SIGNS THAT CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND STORED OFFSITE. ALL EXISTING SIGNS SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. ANY SIGNS OR POSTS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE COST OF THE CONTRACTOR.
2. ALL DISTURBED AREAS IN THE ROW SHALL BE RESTORED WITH SEEDING, CLASS 2A.

PAVEMENT MARKING LEGEND

- ① WHITE LETTERS & SYMBOLS
- ② DOUBLE 4" YELLOW CENTERLINE (11" C/C)
- ③ DOUBLE YELLOW MEDIAN OUTLINE (11" C/C)
- ④ 4" YELLOW LINE
- ⑤ 4" WHITE EDGE LINE
- ⑥ 6" WHITE LANE LINE
- ⑦ NOT USED
- ⑧ 12" YELLOW DIAGONAL LINE
- ⑨ 24" WHITE STOP BAR
- ⑩ 12" WHITE LINE (45° ANGLE)
- ⑪ 4" WHITE SKIP - DASH LINE (10' LINE - 30' SPACE)
- ⑫ 4" YELLOW SKIP - DASH LINE (10' LINE - 30' SPACE)
- ◀ ONE-WAY AMBER MARKER 40' C/C UNLESS OTHERWISE INDICATED
- ◀ ONE-WAY CRYSTAL MARKER 80' C/C UNLESS OTHERWISE INDICATED
- ◀ TWO-WAY AMBER MARKER 40' C/C UNLESS OTHERWISE INDICATED
- ⊠ TRAFFIC SIGN

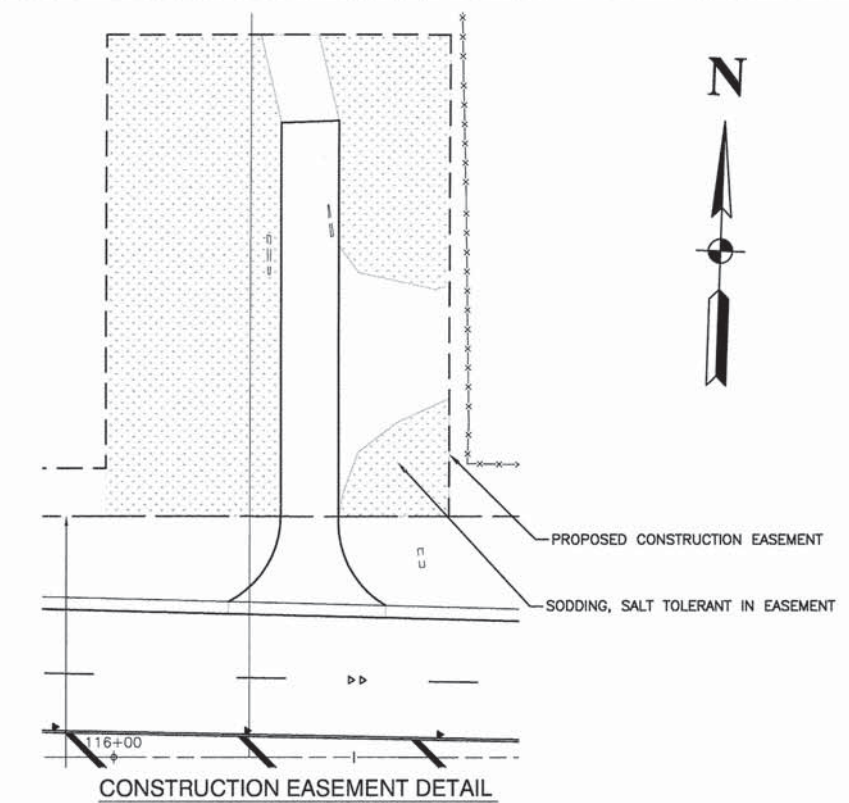
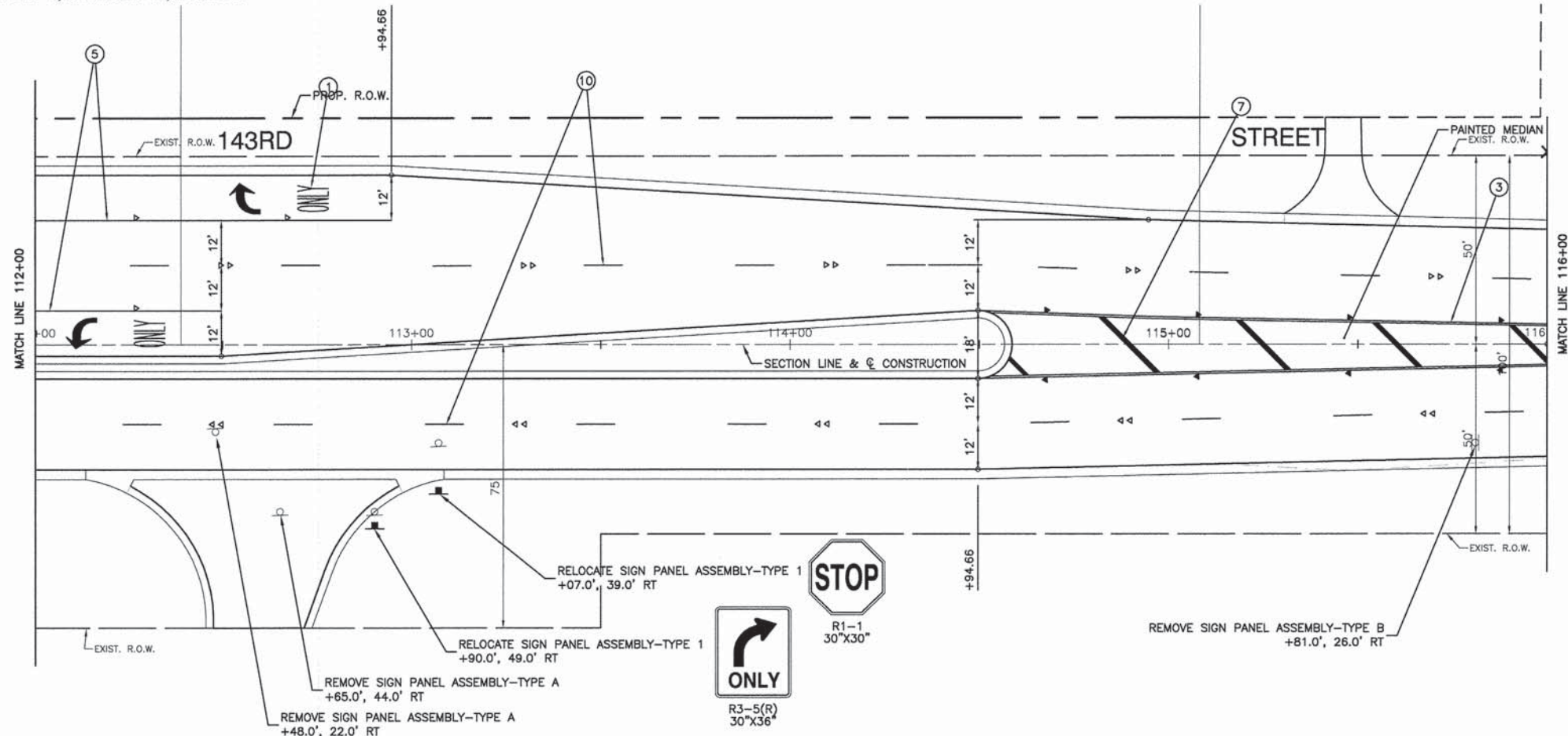
NOTE:

- 1.) SEE IDOT STANDARD TC-13 FOR PAVEMENT MARKING DETAILS.
- 2.) ALL LEGEND ITEMS ARE THERMOPLASTIC PAVEMENT MARKING OF THE WIDTH AND TYPE SPECIFIED.
- 3.) ALL RELOCATED SIGN PANELS WILL BE INSTALLED WITH NEW TELESCOPING STEEL SIGN SUPPORTS.
- 4.) RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE OFFSET FOUR (4) INCHES FROM STRIPING IN SKIP-DASH AREAS.

BENCH MARK:

RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95

| | | | | | | | | | | | |
|---------------------------------------|-------------|----------------------|-----------------------------------|---|--|---------------------------|----------------------------|--|--------------|-----------|--|
| FILE NAME = 04273_02-PLPR-01 - PMKG03 | USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION PAVEMENT MARKING, SIGNING & LANDSCAPING PLAN | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| | | CHECKED -- | REVISED -- | | | 1600 | 04-00003-00-CH | WILL | 96 | 41 | |
| | | PLOT SCALE = | REVISED -- | | | CONTRACT NO. 63147 | | | | | |
| | | PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- | SCALE: 1"=20' | SHEET NO. 41 OF 96 SHEETS | STA. 108+00 TO STA. 112+00 | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | |



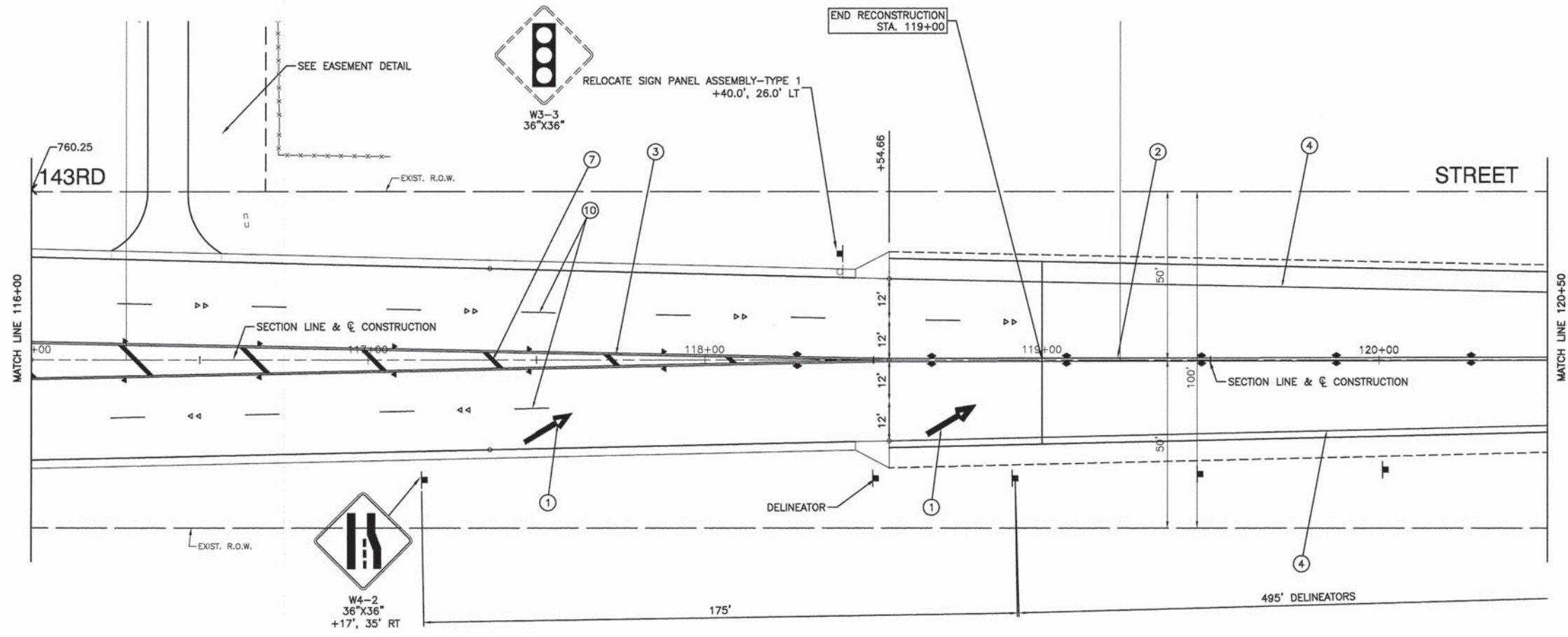
- NOTES:**
1. ALL EXISTING SIGNS THAT CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND STORED OFFSITE. ALL EXISTING SIGNS SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. ANY SIGNS OR POSTS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE COST OF THE CONTRACTOR.
 2. ALL DISTURBED AREAS IN THE ROW SHALL BE RESTORED WITH SEEDING, CLASS 2A.

PAVEMENT MARKING LEGEND

- ① WHITE LETTERS & SYMBOLS
- ② DOUBLE 4" YELLOW CENTERLINE (11" C/C)
- ③ DOUBLE YELLOW MEDIAN OUTLINE (11" C/C)
- ③A 4" YELLOW LINE
- ④ 4" WHITE EDGE LINE
- ⑤ 6" WHITE LANE LINE
- ⑥ NOT USED
- ⑦ 12" YELLOW DIAGONAL LINE
- ⑧ 24" WHITE STOP BAR
- ⑨ 12" WHITE LINE (45° ANGLE)
- ⑩ 4" WHITE SKIP - DASH LINE (10' LINE - 30' SPACE)
- ⑪ 4" YELLOW SKIP - DASH LINE (10' LINE - 30' SPACE)
- ◄ ONE-WAY AMBER MARKER
40' C/C UNLESS OTHERWISE INDICATED
- ◄ ONE-WAY CRYSTAL MARKER
80' C/C UNLESS OTHERWISE INDICATED
- ◄ TWO-WAY AMBER MARKER
40' C/C UNLESS OTHERWISE INDICATED
- ⊣ TRAFFIC SIGN

- NOTE:**
- 1.) SEE IDOT STANDARD TC-13 FOR PAVEMENT MARKING DETAILS.
 - 2.) ALL LEGEND ITEMS ARE THERMOPLASTIC PAVEMENT MARKING OF THE WIDTH AND TYPE SPECIFIED.
 - 3.) ALL RELOCATED SIGN PANELS WILL BE INSTALLED WITH NEW TELESCOPING STEEL SIGN SUPPORTS.
 - 4.) RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE OFFSET FOUR (4) INCHES FROM STRIPING IN SKIP-DASH AREAS.

BENCH MARK:
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95



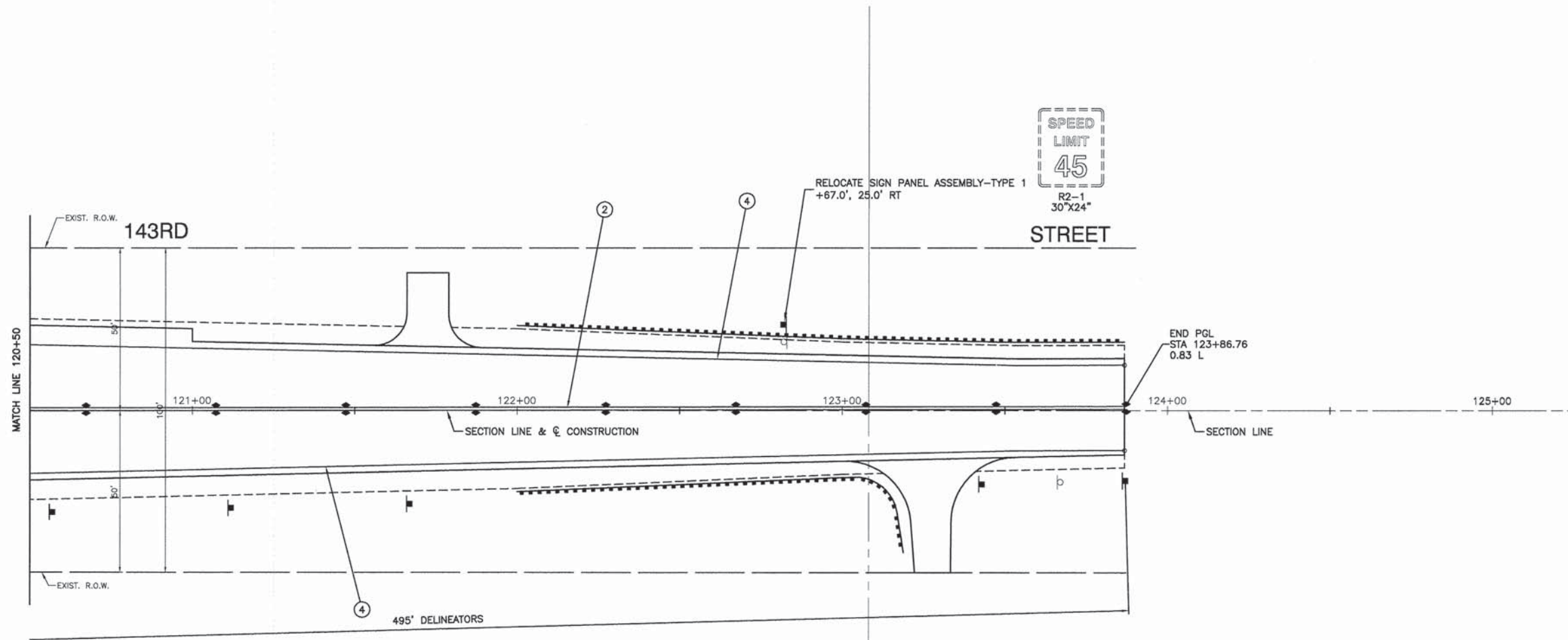
FILE NAME = 04273_02-PLPR-01 - PMK04

| | | |
|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| CHECKED -- | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- RG | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
PAVEMENT MARKING, SIGNING & LANDSCAPING PLAN
SCALE: 1"=20'
SHEET NO. 42 OF 96 SHEETS
STA. 112+00 TO STA. 120+50

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1800 | 04-00003-00-CH | WILL | 96 | 42 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



NOTES:

1. ALL EXISTING SIGNS THAT CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND STORED OFFSITE. ALL EXISTING SIGNS SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. ANY SIGNS OR POSTS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE COST OF THE CONTRACTOR.
2. ALL DISTURBED AREAS IN THE ROW SHALL BE RESTORED WITH SEEDING, CLASS 2A.

PAVEMENT MARKING LEGEND

- ① WHITE LETTERS & SYMBOLS
- ② DOUBLE 4" YELLOW CENTERLINE (11" C/C)
- ③ DOUBLE YELLOW MEDIAN OUTLINE (11" C/C)
- ④ 4" YELLOW LINE
- ⑤ 4" WHITE EDGE LINE
- ⑥ 6" WHITE LANE LINE
- ⑦ NOT USED
- ⑧ 12" YELLOW DIAGONAL LINE
- ⑨ 24" WHITE STOP BAR
- ⑩ 12" WHITE LINE (45° ANGLE)
- ⑪ 4" WHITE SKIP - DASH LINE (10' LINE - 30' SPACE)
- ⑫ 4" YELLOW SKIP - DASH LINE (10' LINE - 30' SPACE)
- ◀ ONE-WAY AMBER MARKER
40' C/C UNLESS OTHERWISE INDICATED
- ◁ ONE-WAY CRYSTAL MARKER
80' C/C UNLESS OTHERWISE INDICATED
- ◆ TWO-WAY AMBER MARKER
40' C/C UNLESS OTHERWISE INDICATED
- ▬ TRAFFIC SIGN

NOTE:

- 1.) SEE IDOT STANDARD TC-13 FOR PAVEMENT MARKING DETAILS.
- 2.) ALL LEGEND ITEMS ARE THERMOPLASTIC PAVEMENT MARKING OF THE WIDTH AND TYPE SPECIFIED.
- 3.) ALL RELOCATED SIGN PANELS WILL BE INSTALLED WITH NEW TELESCOPING STEEL SIGN SUPPORTS.
- 4.) RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE OFFSET FOUR (4) INCHES FROM STRIPING IN SKIP-DASH AREAS.

BENCH MARK:

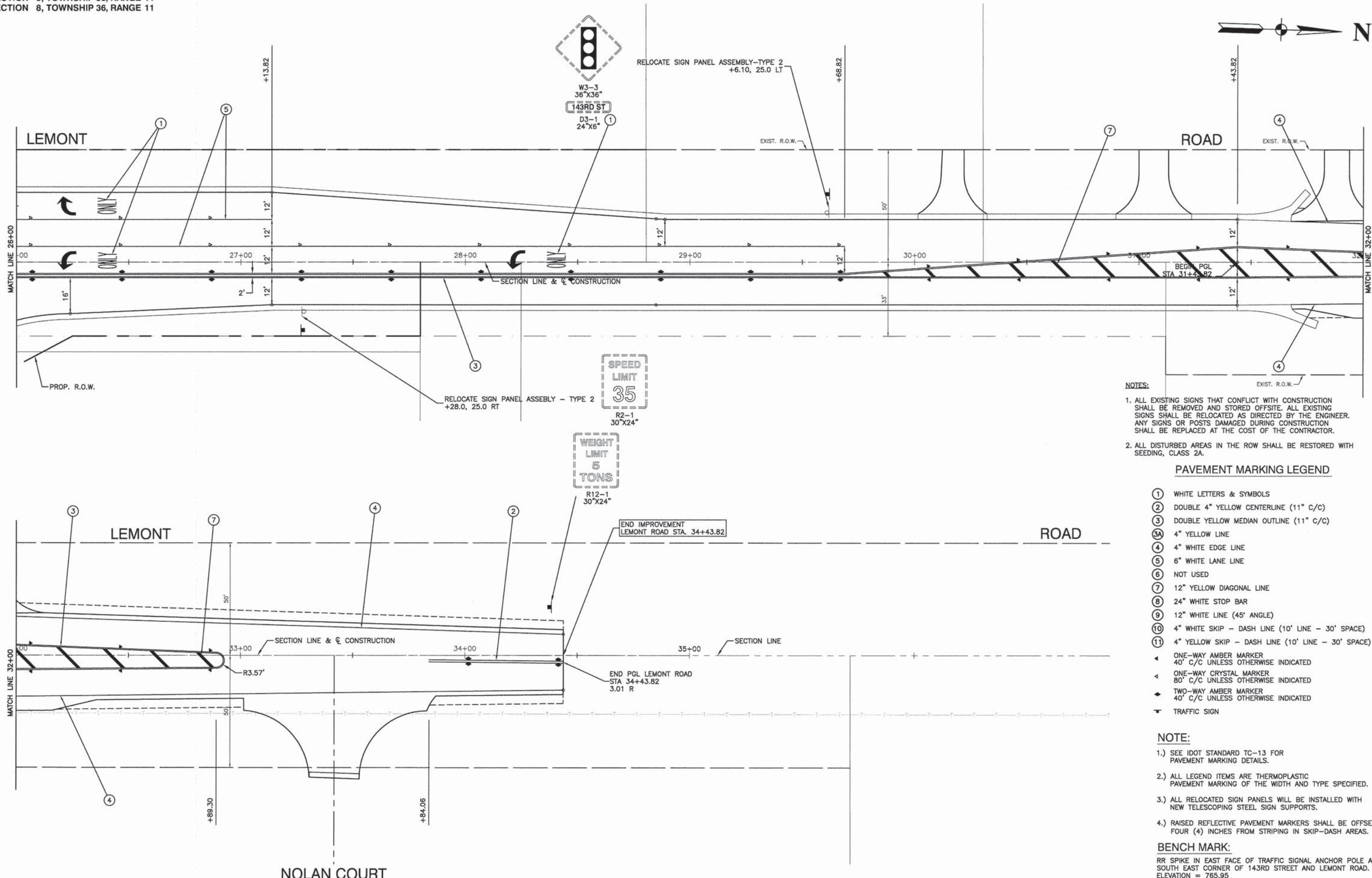
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95

| | | | |
|---------------------------------------|----------------------|-----------------|-----------------------------------|
| FILE NAME = 04273_02-PLPR-01 - PMKG05 | USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | | CHECKED -- | REVISED -- |
| | PLOT SCALE = | DRAWN -- PG | REVISED -- |
| | PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | |
|---|---------------------------|----------------------------|--|
| 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION PAVEMENT MARKING, SIGNING & LANDSCAPING PLAN | | | |
| SCALE: 1"=20' | SHEET NO. 43 OF 96 SHEETS | STA. 120+50 TO STA. 126+36 | |

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 43 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



- NOTES:**
1. ALL EXISTING SIGNS THAT CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND STORED OFFSITE. ALL EXISTING SIGNS SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. ANY SIGNS OR POSTS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE COST OF THE CONTRACTOR.
 2. ALL DISTURBED AREAS IN THE ROW SHALL BE RESTORED WITH SEEDING, CLASS 2A.

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 - ② DOUBLE 4" YELLOW CENTERLINE (11" C/C)
 - ③ DOUBLE YELLOW MEDIAN OUTLINE (11" C/C)
 - ④A 4" YELLOW LINE
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 - ⑤ 6" WHITE LANE LINE
 - ⑥ NOT USED
 - ⑦ 12" YELLOW DIAGONAL LINE
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 - ⑪ 4" YELLOW SKIP - DASH LINE (10' LINE - 30' SPACE)
 - ◀ ONE-WAY AMBER MARKER 40' C/C UNLESS OTHERWISE INDICATED
 - ◀ ONE-WAY CRYSTAL MARKER 80' C/C UNLESS OTHERWISE INDICATED
 - ◀ TWO-WAY AMBER MARKER 40' C/C UNLESS OTHERWISE INDICATED
 - ▶ TRAFFIC SIGN

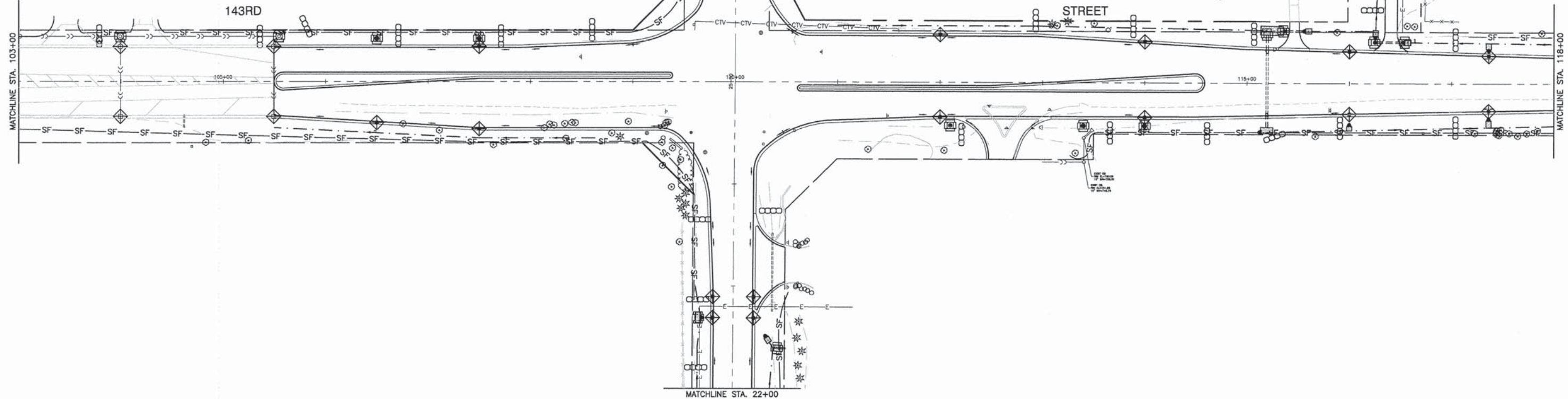
- NOTE:**
- 1.) SEE IDOT STANDARD TC-13 FOR PAVEMENT MARKING DETAILS.
 - 2.) ALL LEGEND ITEMS ARE THERMOPLASTIC PAVEMENT MARKING OF THE WIDTH AND TYPE SPECIFIED.
 - 3.) ALL RELOCATED SIGN PANELS WILL BE INSTALLED WITH NEW TELESCOPING STEEL SIGN SUPPORTS.
 - 4.) RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE OFFSET FOUR (4) INCHES FROM STRIPING IN SKIP-DASH AREAS.

BENCH MARK:
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95

| | | | |
|--------------------------------------|----------------------|-----------------|-----------------------------------|
| FILE NAME = 04273_02-PLPR-01 - PMK07 | USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | | CHECKED -- | REVISED -- |
| | PLOT SCALE = | DRAWN -- RG | REVISED -- |
| | PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | |
|---|---------------------------|-----------------------------|--|
| 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION PAVEMENT MARKING, SIGNING & LANDSCAPING PLAN | | | |
| SCALE: 1"=20' | SHEET NO. 45 OF 96 SHEETS | STA. 26+00 TO STA. 34+43.82 | F.A.U. RTE. 1600 |
| | | | SECTION 04-00003-00-CH |
| | | | COUNTY WILL |
| | | | TOTAL SHEETS 96 |
| | | | SHEET NO. 45 |
| | | | CONTRACT NO. 63147 |
| | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) |



EROSION CONTROL NOTES:

WITHIN 24 HOURS FROM THE TIME SEEDING HAS BEEN PERFORMED, THE SEED AREA SHALL BE GIVEN A COVERING OF MULCH. THE MULCH SHALL CONSIST OF HAND OR MACHINE APPLICATION OF STRAW, MULCH AT RATE OF 2 TON PER ACRE. MULCH SHALL EITHER BE ANCHORED WITH A MECHANICAL STABILIZER OR PARTIALLY COATED WITH EMULSIFIED ASPHALT. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.

ALL EROSION CONTROL PRACTICES SHALL BE INSTALLED PRIOR TO STARTING EACH PHASE OF CONSTRUCTION.

ANY OBSERVED DISRUPTION TO THE EROSION CONTROL PRACTICES SHALL BE IMMEDIATELY REPAIRED BY THE CONTRACTOR.

ANY EXISTING SUBSURFACE DRAINAGE SYSTEM OR FIELD TILES THAT ARE DISTURBED DURING CONSTRUCTION SHALL BE RESTORED.

ANY DUST OR MUD TRACKED ONTO STREETS SHALL BE CLEANED AT THE END OF EACH WORKING DAY.

ALL SOIL EROSION, AND SEDIMENTATION CONTROL PRACTICES SHALL BE IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL.

ALL SOIL SHALL BE STABILIZED WITHIN 7 DAYS OF SOIL DISTURBANCE.

STOCK PILES SHALL BE STABILIZED WITHIN 7 DAYS OF SOIL DISTURBANCE BY MEANS OF TEMPORARY SEEDING.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSPECTION AND MAINTENANCE OF EROSION CONTROL PRACTICES. REQUIRED INSPECTION REPORTS SHALL BE SUBMITTED TO THE WILL/SOUTH COOK SOIL AND WATER CONSERVATION DISTRICT AT 1201 GOUGAR ROAD, NEW LENOX 60451.

ALL EROSION CONTROL PRACTICES SHALL BE INSPECTED WEEKLY AND AFTER ANY RAINFALL GREATER THAN 0.5".

THE SEEDING DATES FOR CLASS 2A SEEDING SHALL BE EARLY SPRING TO JUNE 1, AUGUST 1 TO SEPTEMBER 1, OR NOVEMBER 15 TO MARCH 1. THE SEEDING DATES FOR TEMPORARY SEEDING SHALL BE EARLY SPRING TO SEPTEMBER 30. THE SEEDING DATES FOR BARE EARTH SEEDING AND INTER SEEDING CLASS 4 AND CLASS 4B SHALL BE FROM MAY 15 TO JUNE 30 AND FROM OCTOBER 15 TO DECEMBER 1.

PERMANENT SEEDING SHALL BE FERTILIZED WITH 1000 LBS/ACRE OF 12-12-12 FERTILIZER OR EQUIVALENT.

TEMPORARY SEEDING SHALL BE FERTILIZED WITH 500 LBS/ACRE OF 10-10-10 FERTILIZER OR EQUIVALENT.

STORM WATER POLLUTION PREVENTION NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR HAVING THE SWPPP ON SITE AT ALL TIMES.

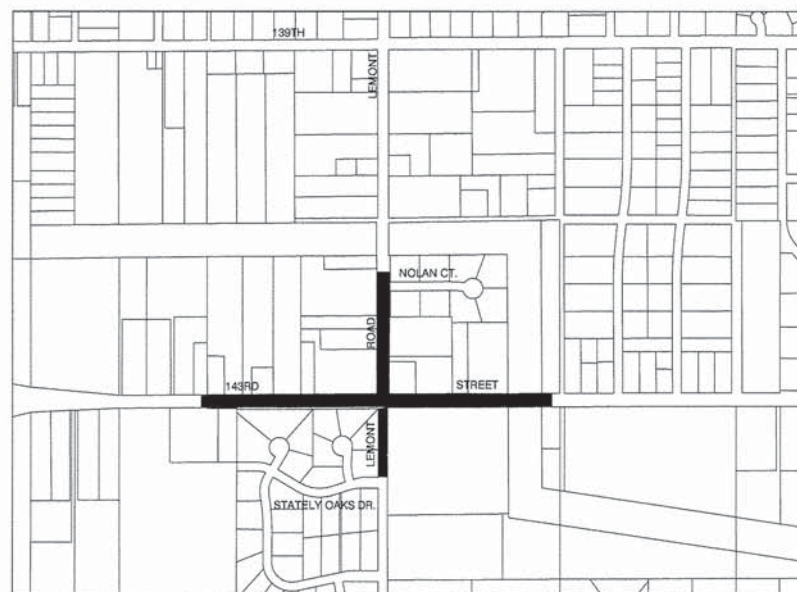
2. THE CONTRACTOR SHALL TAKE THE NECESSARY STEPS TO CONTROL WASTE SUCH AS DISCARDED BUILDING MATERIALS, CONCRETE TRUCK WASHOUT, CHEMICALS, LITTER AND SANITARY WASTE AT THE CONSTRUCTION SITE THAT MAY CAUSE ADVERSE IMPACTS TO WATER QUALITY.

SEEDING MIXTURES SHALL BE AS FOLLOWS

| Class | Seeds | lbs/acre—Pure Live Seed |
|-------|----------------------|-------------------------|
| 2A | Alta Fescue or Ky 31 | 70 |
| | Perennial Ryegrass | 20 |
| | Dawsons Red Fescue | 20 |
| | Scaldia Hard Fescue | 20 |
| | Fulta Salt Grass 1/ | 70 |

TEMPORARY SEEDING

| Seeds | lbs/acre—Pure Live Seed |
|-----------------------|-------------------------|
| Oats | 90 |
| or Cereal Rye | 90 |
| or Wheat | 90 |
| or Perennial Ryegrass | 25 |



LOCATION MAP

SCALE: NOT TO SCALE

| SOIL PROTECTION CHART | | | | | | | | | | | | |
|-----------------------|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|
| STABILIZATION TYPE | JAN | FEB | MAR | APR | MAY | JUN | JUL | AUG | SEP | OCT | NOV | DEC |
| PERMANENT SEEDING | | | A | | | * | * | | | | | |
| DORMANT SEEDING | B | | | | | | | | | | B | |
| TEMPORARY SEEDING | | | C | | | * | D* | | | | | |
| SODDING | | | E** | | | | | | | | | |
| MULCHING | | | | | | | | | | | | |

| | |
|--|-----------------------------------|
| A. KENTUCKY BLUEGRASS 90 LBS/AC MIXED WITH PERENNIAL RYEGRASS 30 LBS/AC | C. SPRING OATS 100 LBS/AC |
| B. KENTUCKY BLUEGRASS 135 LBS/AC MIXED WITH PERENNIAL RYEGRASS 45 LBS/AC + 2 TONS STRAW MULCH/AC | D. WHEAT OR CEREAL RYE 150 LBS/AC |
| E. SOD (NURSERY GROWN KENTUCKY BLUEGRASS) | F. STRAW MULCH 2 TONS/AC |

* IRRIGATION NEEDED DURING JUNE AND JULY
** IRRIGATION NEEDED FOR 2 TO 3 WEEKS AFTER APPLYING SOD

LEGEND

- INLET FILTER
- INLET AND PIPE PROTECTION
- RIP RAP
- PERIMETER EROSION BARRIER
- TEMPORARY DITCH CHECKS (EXCELSIOR LOG)

NOTES:

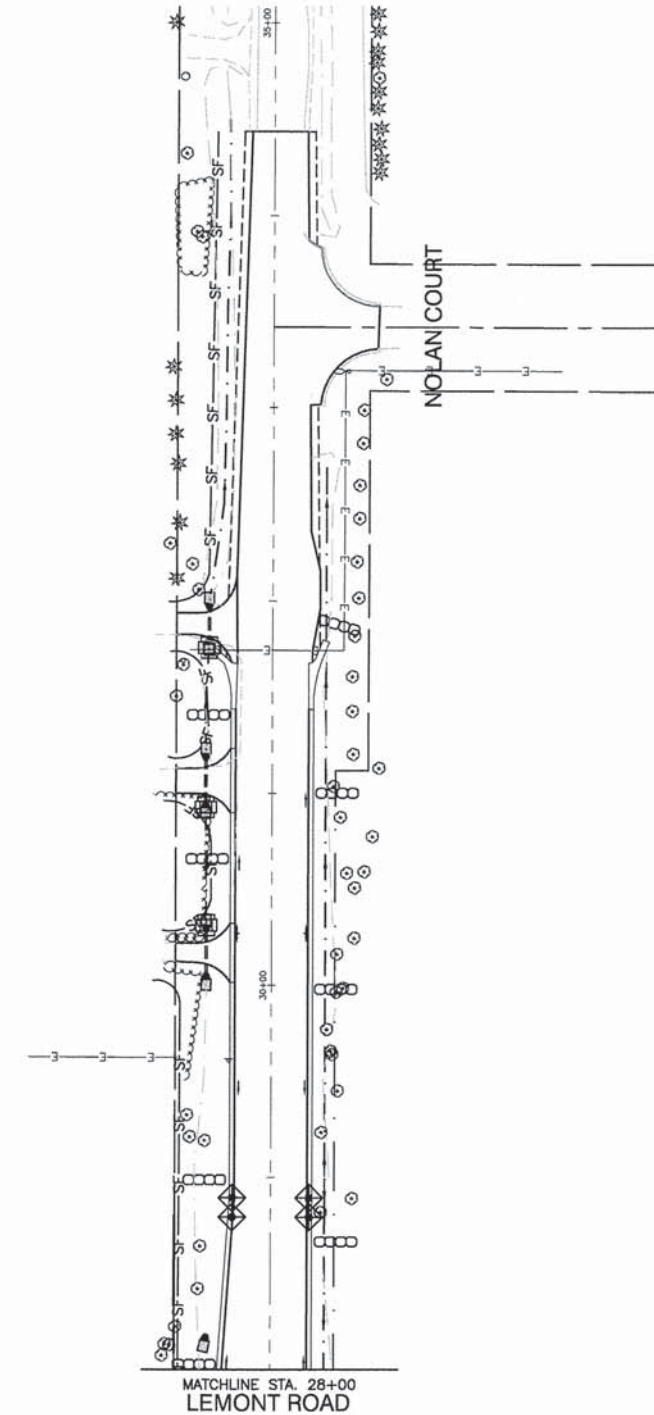
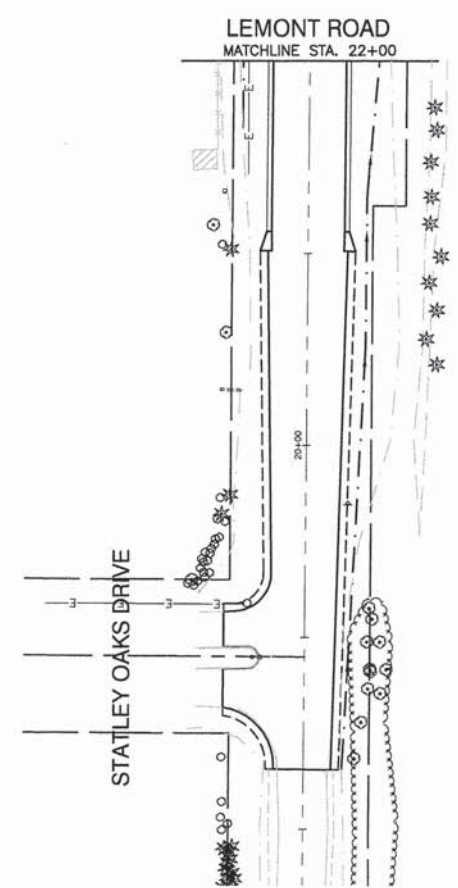
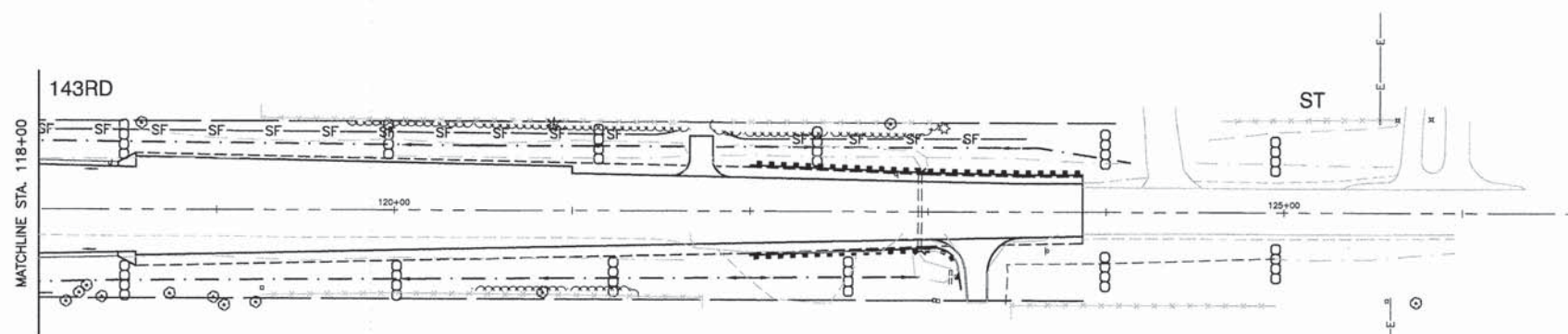
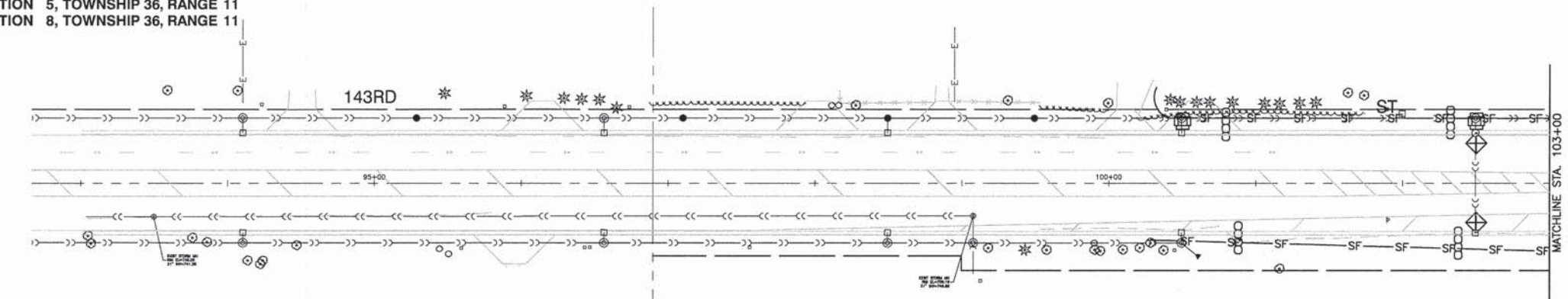
1. EXISTING AND FUTURE DRAINAGE PATTERNS ARE THE SAME.
2. TOTAL AREA OF SOIL DISTURBANCE IS APPROXIMATELY 1.4 ACRES.

BENCH MARK:






RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95

| | | |
|----------------------|----------------|----------------------------------|
| USER NAME = | DESIGNED — HLG | REVISED — 01-22-09 - IDOT REVIEW |
| | CHECKED — | REVISED — |
| PLOT SCALE = | DRAWN — RG | REVISED — |
| PLOT DATE = 05-30-13 | CHECKED — AG | REVISED — |

SECTION 5, TOWNSHIP 36, RANGE 11
SECTION 8, TOWNSHIP 36, RANGE 11



LEGEND

-  INLET FILTER
-  INLET AND PIPE PROTECTION
-  RIP RAP
-  PERIMETER EROSION BARRIER
-  TEMPORARY DITCH CHECKS (EXCELSIOR LOG)

BENCH MARK:

RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD. ELEVATION = 765.95

FILE NAME = 04273_02-SWPP-01 - P02

| | | |
|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- RG | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

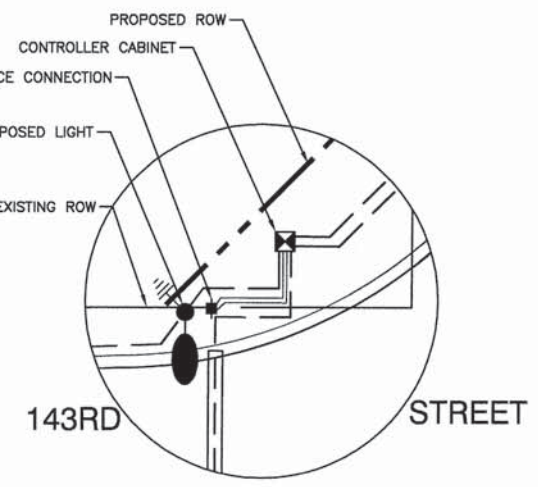
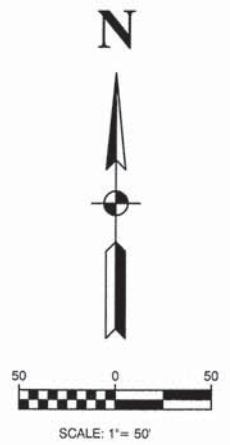
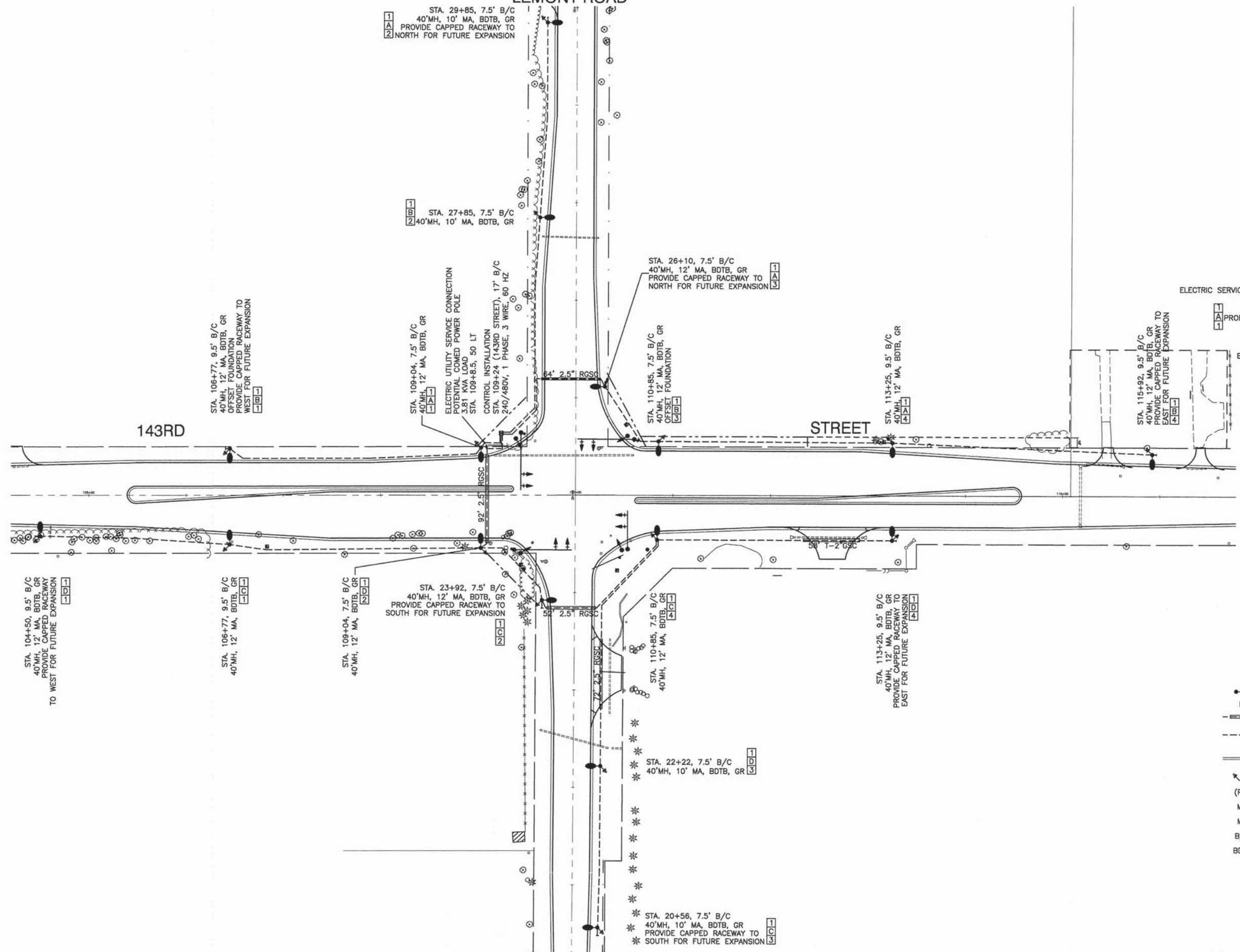
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
STORMWATER POLLUTION PROTECTION PLAN

SCALE: 1"=20'
SHEET NO. 47 OF 96 SHEETS
STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|----------------|--------|--------------------|-----------|
| 1600 | 04-00003-00-CH | WILL | 96 | 47 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | CONTRACT NO. 63147 | |

LEMONT ROAD



ENLARGED PLAN FOR CONTROLLER
SCALE 1"=20'

- LEGEND:**
- PROPOSED STREET LIGHT-250W HPS
 - PROPOSED CONTROLLER
 - UNDERGROUND CONDUIT GALVANIZED STEEL
 - - - UNIT DUCT, 3-1/2", NO.4 AWG AND NO.6 GROUND, 600V IN 1-1/4" DUCT
 - SERVICE WIRE, 3-1/2", NO. 1/0 AWG.
 - ⊗ GR GROUND ROD, 5/8" DIA. x 10'
 - (RO) REMOVAL OF EXISTING STREET LIGHT BY OTHERS
 - MH MOUNTING HEIGHT
 - MA MAST ARM
 - B/C BACK OF CURB
 - BDTB BREAKAWAY DEVICE, TRANSFORMER BASE, 15" BOLT CIRCLE

POLE IDENTIFICATION



FILE NAME = 04273_02-LGHT-01 - LGHT-01

| | | |
|----------------------|---------------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG/DWS | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- RG | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

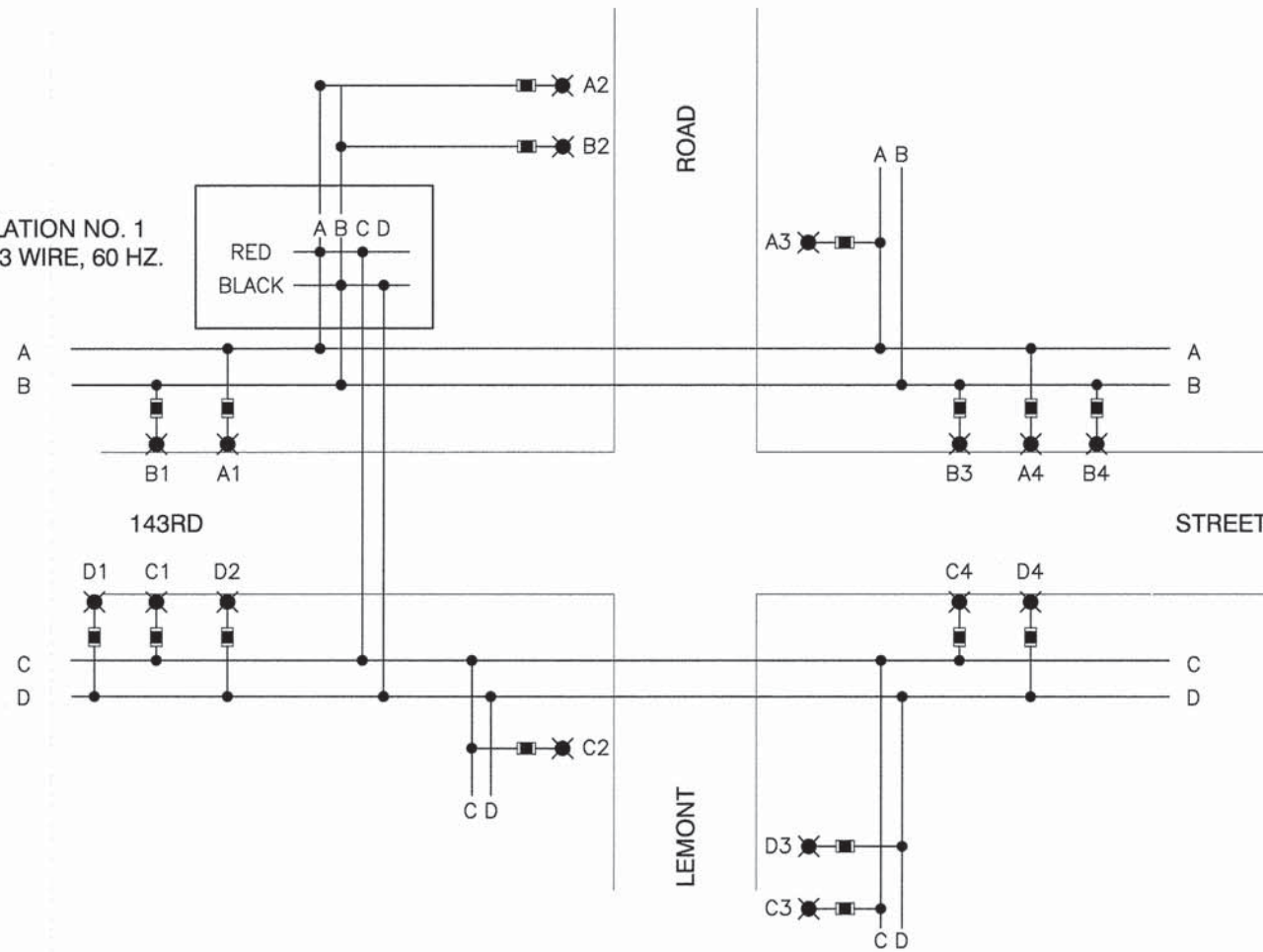
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
STREET LIGHTING PLAN

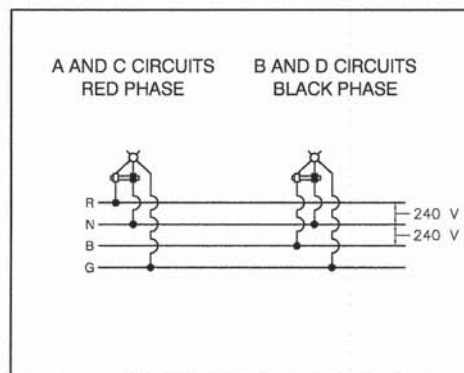
SCALE: 1"=50' SHEET NO. 48 OF 96 SHEETS STA. TO STA.

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 48 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |

CONTROL INSTALLATION NO. 1
240/480V, 1 PHASE, 3 WIRE, 60 HZ.



TYPICAL POLE WIRING



LOAD TABULATIONS CONTROLLER #1

| CIRCUIT | WATTS | AMPS @ 240V | |
|---------------------------|--------------|--------------|--------------|
| | | RED PHASE | BLACK PHASE |
| A | 1,220 | 5.20 | |
| B | 1,220 | | 5.20 |
| C | 1,220 | 5.20 | |
| D | 1,220 | | 5.20 |
| CAB TOTAL | 4,880 | 10.40 | 10.40 |
| TOTAL AMPS @ 240 V | | 20.80 | |

INDIVIDUAL LUMINAIRE LOAD DATA

| RATED WATTS | INPUT VOLTS | MAX INPUT AMPS | INPUT WATTS |
|-------------|-------------|----------------|-------------|
| 250 | 240 | 1.3 | 305 |

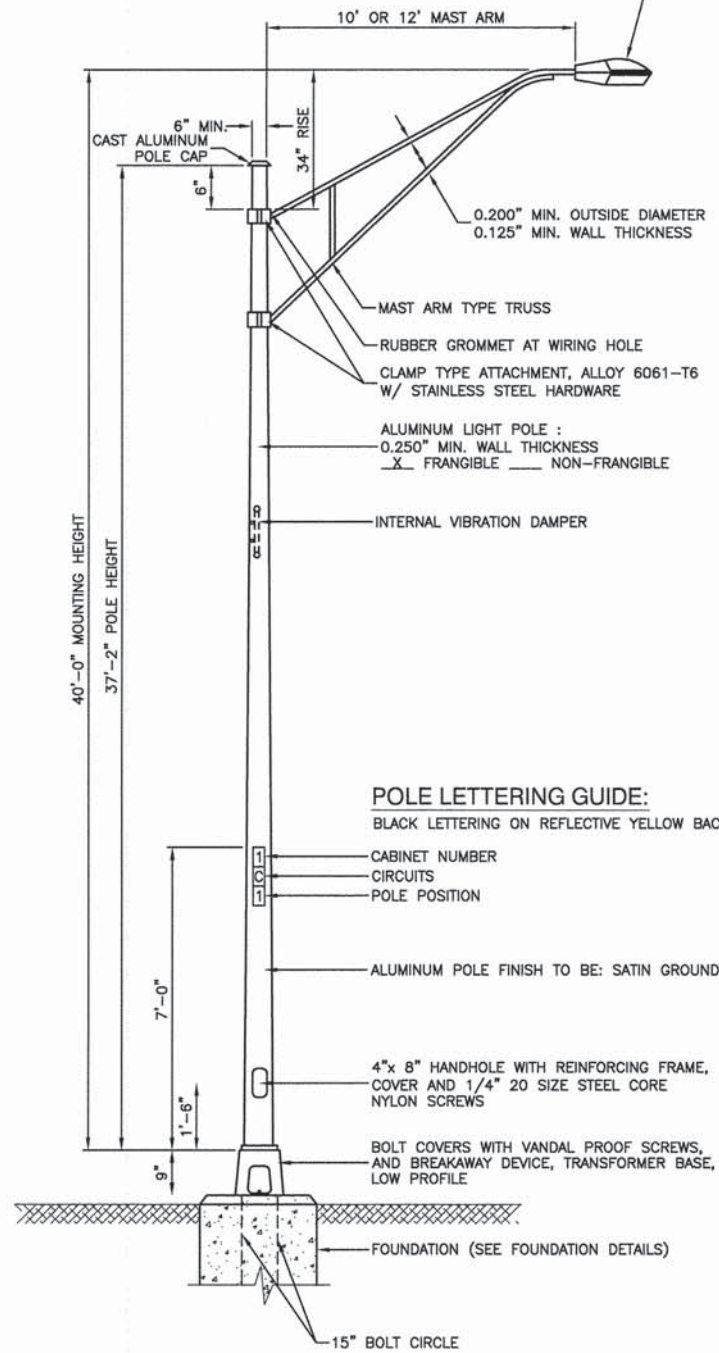
LEGEND

- LUMINAIRE, 250W HPS, 240V
- FUSE, 4.0 AMP
- LUMINAIRE CIRCUIT
- CONNECTION

NOTES

- CONTRACTOR SHALL PLACE SIX COPIES, 11X17 OR LARGER, OF THIS SHEET IN A WATER RESISTANT FOLDER INSIDE THE CONTROL CABINET.

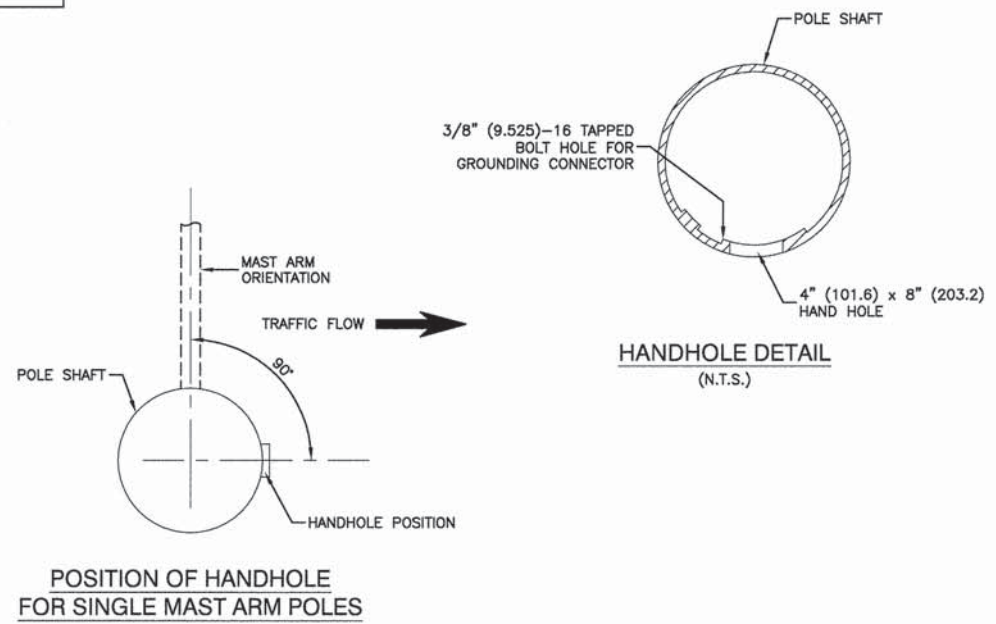
LUMINAIRE:
 250 WATT HIGH PRESSURE SODIUM VAPOR LAMP
 240 VOLT BALLAST
I.E.S. TYPE: MC III LIGHT DISTRIBUTION,
 FULL CUTOFF
LENS TYPE: FLAT
INITIAL LAMP LUMENS: 28,000
LAMP LIFE: 24,000 HOURS



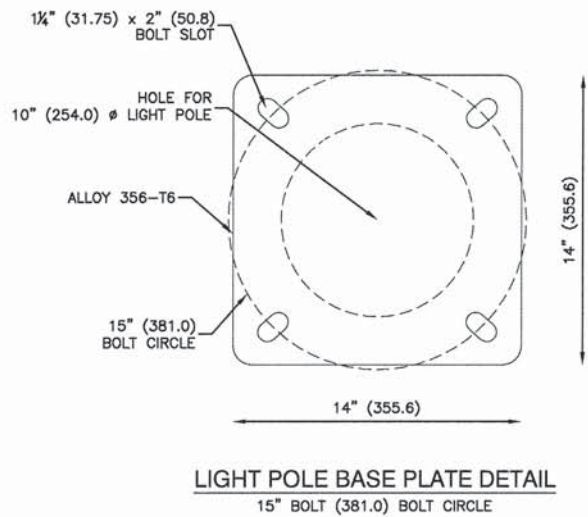
POLE LETTERING GUIDE:
 BLACK LETTERING ON REFLECTIVE YELLOW BACKGROUND

- NOTES:**
1. THE LIGHTING UNITS SHALL MEET AASHTO DESIGN CRITERIA. DESIGN FOR 90 M.P.H. WIND WITH 30% GUST AND 75 POUND LUMINAIRE HAVING AN E.P.A. OF 1.6 SQ. FT. AND PROPER ICE LOADING.
 2. ALUMINUM ALLOY 6063-T6 SHALL BE USED.
 3. LIGHT POLE AND ASSOCIATED EQUIPMENT TO BE U.L. LISTED

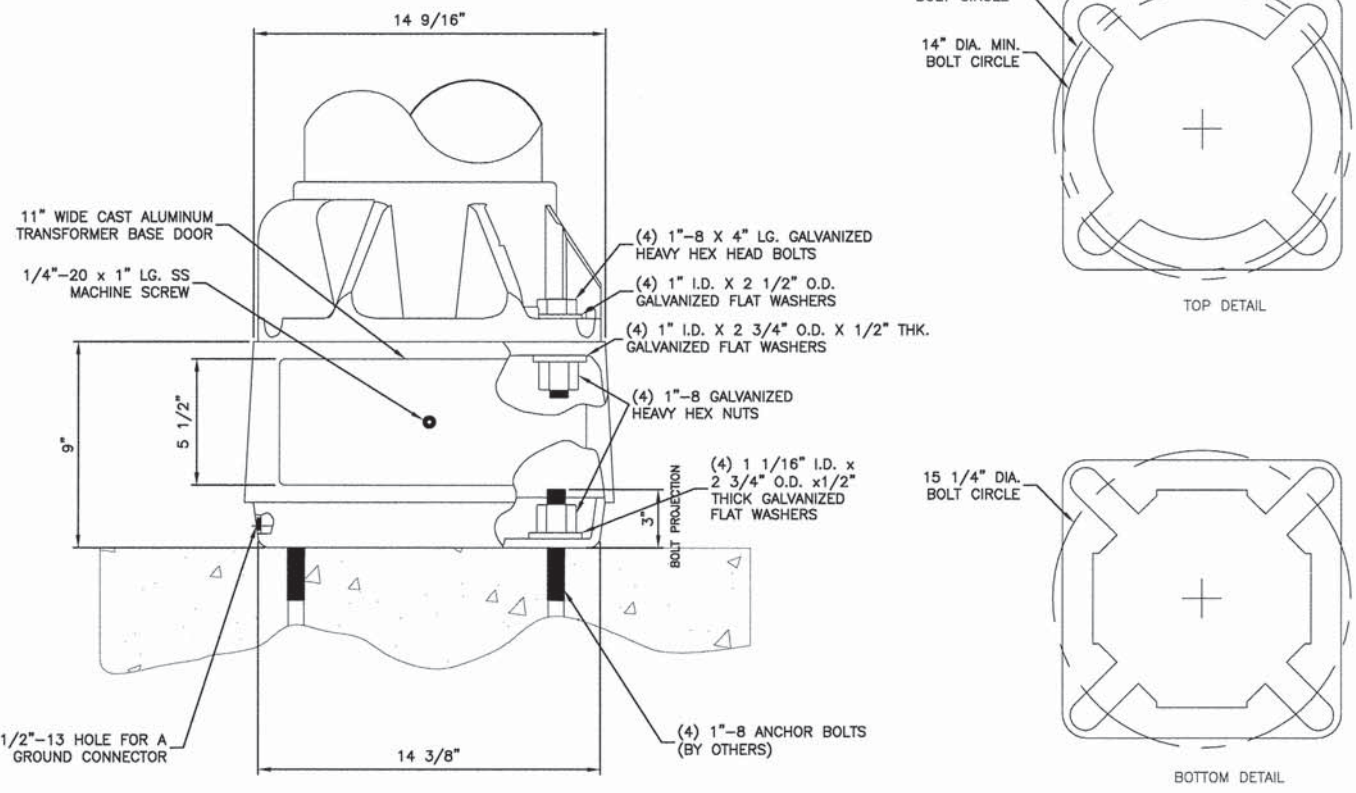
TYPICAL POLE INSTALLATION



POSITION OF HANDHOLE FOR SINGLE MAST ARM POLES



LIGHT POLE BASE PLATE DETAIL
 15" BOLT (381.0) BOLT CIRCLE



TRANSFORMER BASE DETAIL, 15" BOLT CIRCLE

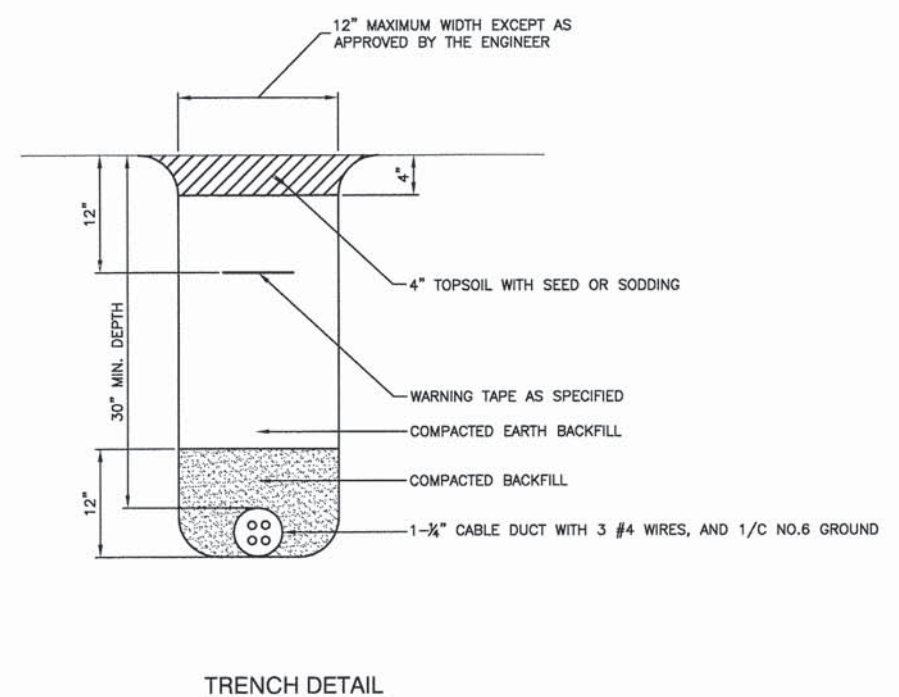
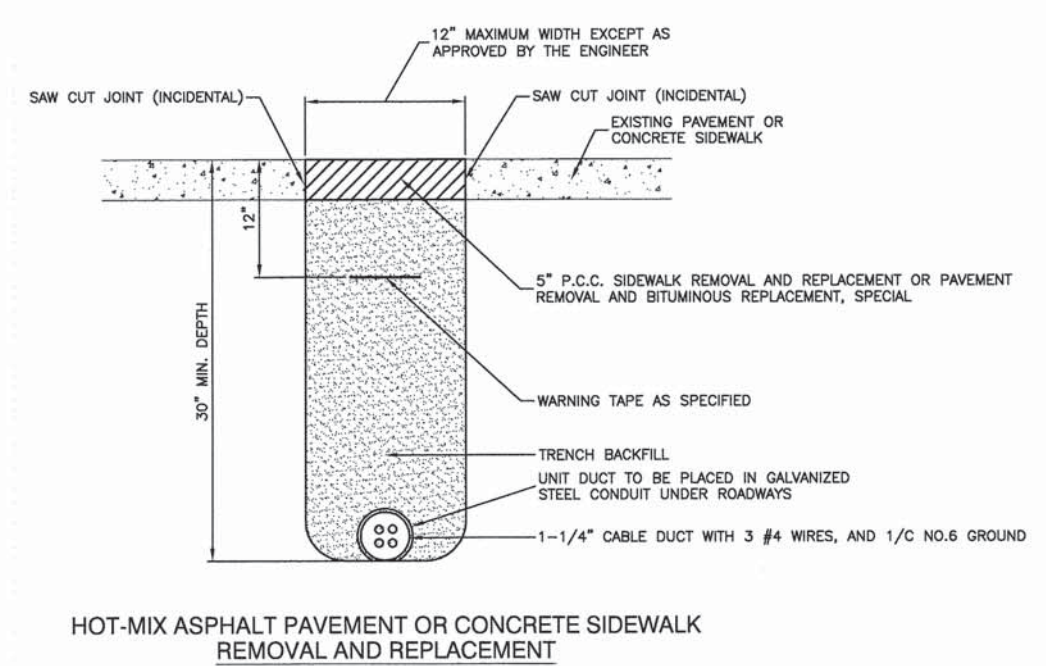
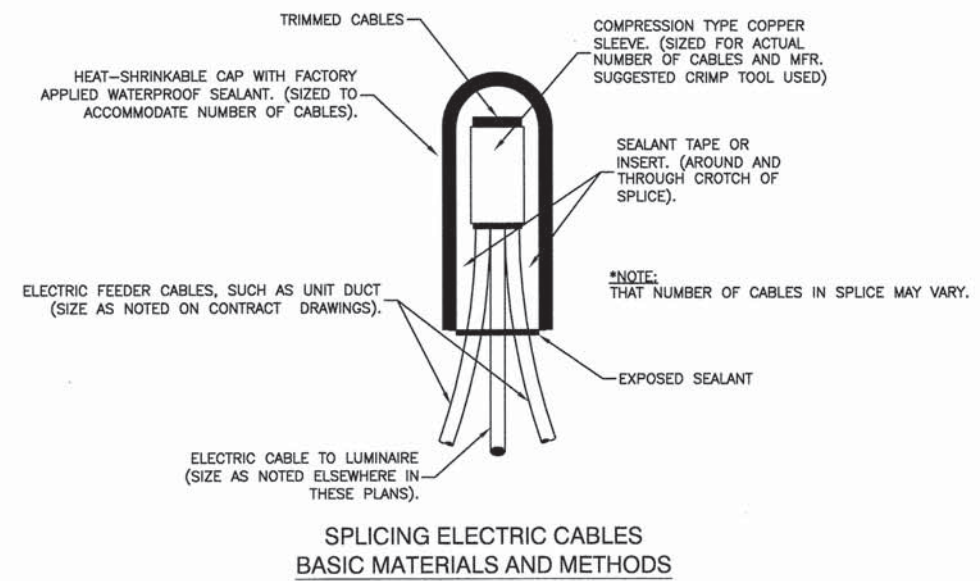
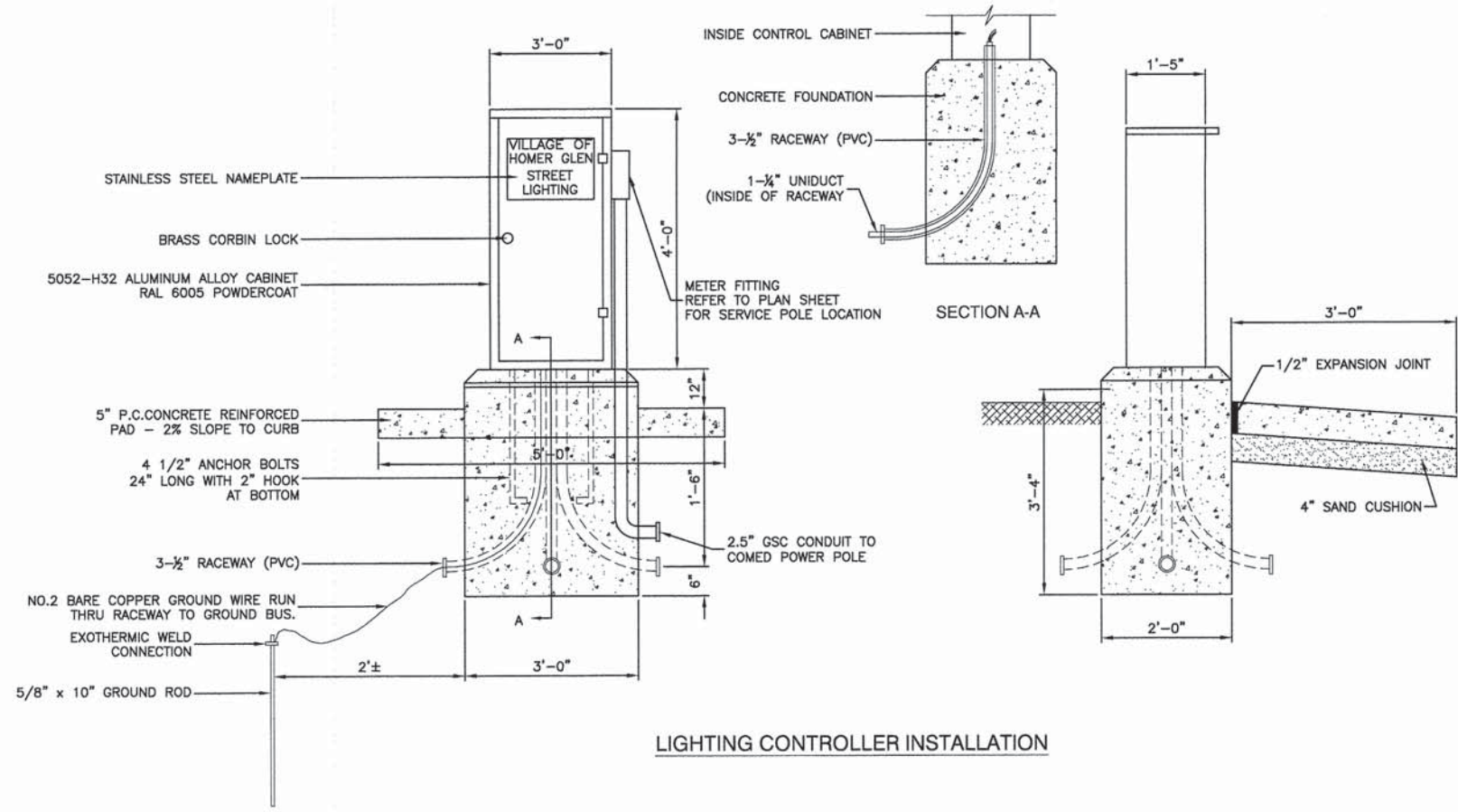
- NOTES:**
1. BREAKAWAY DEVICE TO BE AASHTO APPROVED.
 2. BREAKAWAY DEVICE AND POLE SHALL BE LEVELED AS ONE UNIT USING LEVELING NUTS IF REQUIRED

| | | | |
|-------------------------------------|----------------------|---------------------|-----------------------------------|
| FILE NAME = 04273_02-LIGHT-03 - P01 | USER NAME = | DESIGNED -- HLG/DWS | REVISED -- 01-22-09 - IDOT REVIEW |
| | | CHECKED -- | REVISED -- |
| | PLOT SCALE = | DRAWN -- RG | REVISED -- |
| | PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

| | | | | | | |
|--|---------------------------|------------------|------------------------|--|-----------------|--------------|
| 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION STREET LIGHTING DETAILS | | F.A.U. RTE. 1600 | SECTION 04-00003-00-CH | COUNTY WILL | TOTAL SHEETS 96 | SHEET NO. 50 |
| SCALE: NONE | SHEET NO. 50 OF 96 SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | |

| | | | | |
|--|------------------------|-------------|-----------------|--------------|
| F.A.U. RTE. 1600 | SECTION 04-00003-00-CH | COUNTY WILL | TOTAL SHEETS 96 | SHEET NO. 50 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



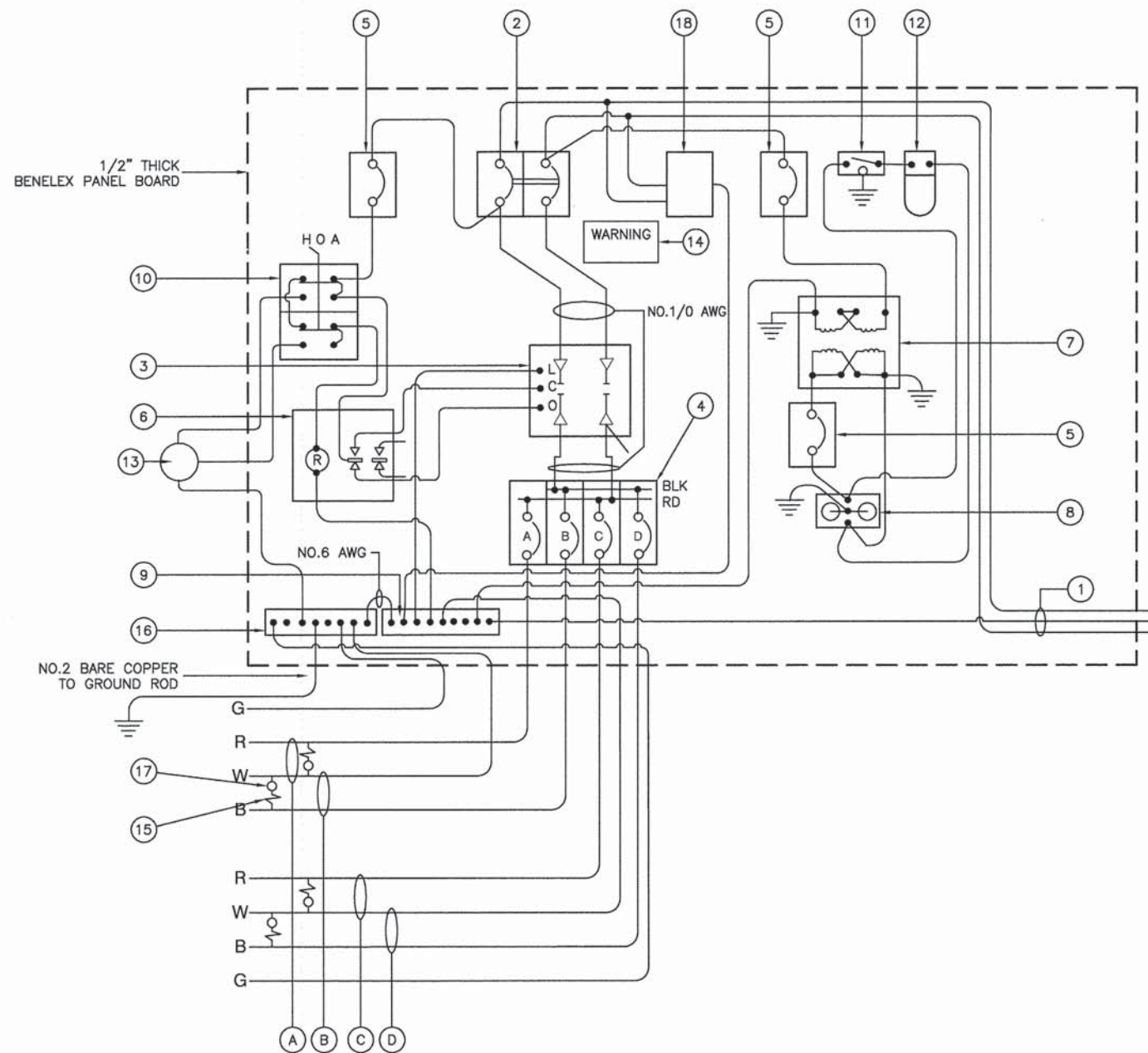
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|------------------------------------|----------------------|---------------------|-----------------------------------|
| FILE NAME = 04273_02-LGHT-03 - P02 | USER NAME = | DESIGNED -- HLG/DWS | REVISED -- 01-22-09 - IDOT REVIEW |
| | | CHECKED -- | REVISED -- |
| | PLOT SCALE = | DRAWN -- RG | REVISED -- |
| | PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
STREET LIGHTING DETAILS

SCALE: NONE SHEET NO. 51 OF 96 SHEETS STA. TO STA.

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 51 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



CONTROLLER WIRING DIAGRAM
CONTROLLER #1

CONTROLLER WIRING DIAGRAM LEGEND

- ① 3-1/C, NO. 1/0 600V SERVICE WIRE IN 2" DIA RIGID GSC FOR 240/480 VOLT, 1Ø, 3 WIRE, 60HZ. SERVICE.
 - ② (1) 100 AMP MAIN CIRCUIT BREAKER, 2 POLE, 600 VOLT, 100 AMP BASE, NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA - 22 KA AT 480 V.
 - ③ (1) 100 AMP CONTACTOR SWITCH, ELECTRICALLY OPERATED, MECHANICALLY HELD, 2 POLE, 600 VOLT
 - ④ (4) 30 AMP CIRCUIT BREAKER, 1 POLE, 240 VOLT, 100 AMP BASE, NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA - 22 KA AT 240 VOLTS.
 - ⑤ (3) 20 AMP CONTROL CIRCUIT-CIRCUIT BREAKER, 1 POLE, 120 VOLT, 100 AMP BASE, NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA 22 KA AT 240 V.
 - ⑥ (1) 20 AMP, 1 POLE DOUBLE THROW, 240 VOLT RELAY
 - ⑦ (1) 1.5 KVA, SINGLE PHASE, ENCAPSULATED TRANSFORMER 240 X 480/120 X 240 VOLT, 60HZ.
 - ⑧ (1) 20 AMP, 120 VOLT DUPLEX GFCI RECEPTACLE MOUNTED IN BOX.
 - ⑨ NEUTRAL BUS BAR, 1/4"x1"x12" LONG MOUNTED ON PANEL WITH LUGS.
 - ⑩ 3 POSITION SELECTOR SWITCH, 240V, 30 AMP.
 - ⑪ SWITCH FOR LIGHTING FIXTURE MOUNTED TO DOOR, 20 AMP, SPST.
 - ⑫ WEATHER-PROOF INCANDESCENT LIGHTING FIXTURE WITH 100 WATT, 120 V LAMP.
 - ⑬ PHOTOCCELL MOUNTED TO CABINET OVERHANG, 240 V, 1000 VA.
 - ⑭ WARNING PLATE TO READ: WARNING, MAINTENANCE CIRCUIT IS LIVE WHEN MAIN BREAKER IS SWITCHED OFF.
 - ⑮ IN-LINE FUSEHOLDER WITH FUSE AS NOTED IN FUSE TABLE
 - ⑯ GROUND BUS BAR 1/4"x1"x12" MINIMUM LENGTH MOUNTED ON PANEL WITH LUGS.
 - ⑰ LUMINAIRE
 - ⑱ SURGE ARRESTOR, 3,405 JOULES @ 240 VOLTS.
- (A) CIRCUIT (RED)
 - (B) CIRCUIT (BLACK)
 - (C) CIRCUIT (RED)
 - (D) CIRCUIT (BLACK)

| NOMINAL WATTAGE | FUSE SIZE |
|-----------------|-----------|
| 250W | 4.0 AMP |

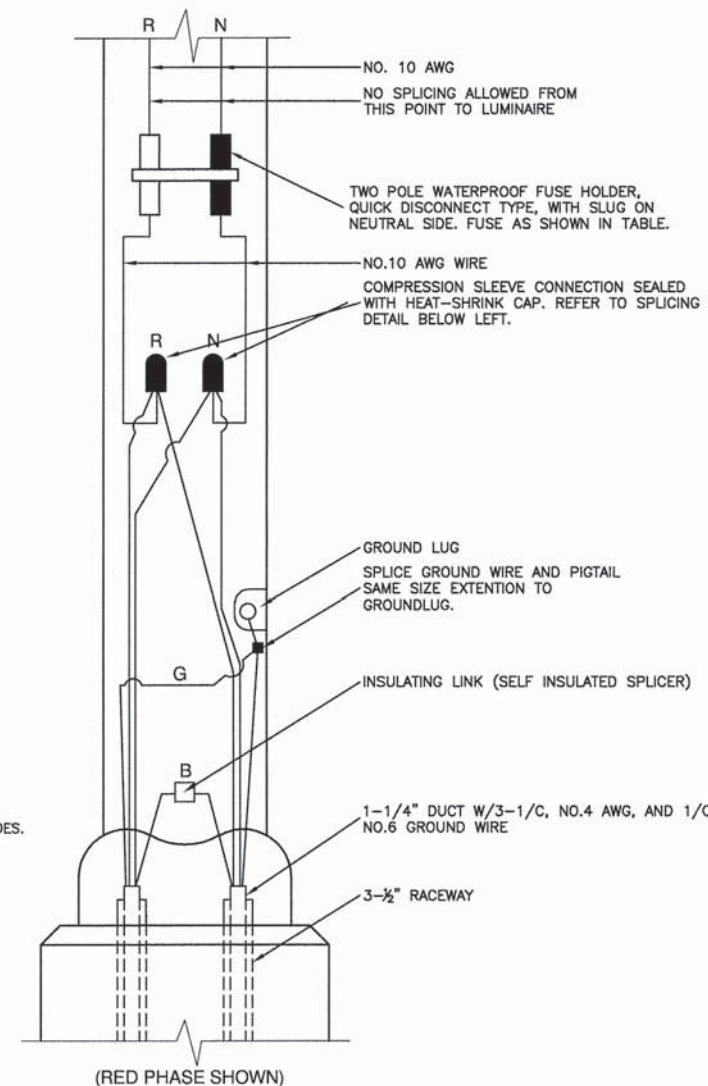
LUMINAIRE FUSE SIZE TABLE

GENERAL NOTES FOR CONTROL CABINET

- ENTIRE CONTROL CABINET SHALL BE GROUNDED.
- ALL WIRING SHALL BE TAGGED WITH SELF-STICKING WIRE MARKERS.
- GROUND BUS TO BE COLOR CODED GREEN, NEUTRAL BUS WHITE, AND BONDED TO CABINET ENCLOSURE, BY LISTED PRESSURE CONNECTORS OR LISTED CLAMPS.
- ALL INTERNAL CONTROLLER WIRING TO BE NO.12 AWG UNLESS OTHERWISE SPECIFIED.
- CABINET WIRING INSULATION TO BE TYPE XHHW OR APPROVED EQUAL.
- THE CONTROLLER SHALL BE NEMA 3R, BE SUITABLE FOR USE AS SERVICE ENTRANCE RATED, AND CONSTRUCTED IN ACCORDANCE WITH UL STD. 508. UL LABEL TO BE VISIBLE FROM OUTSIDE CONTROL CABINET.

LIGHTING GENERAL NOTES

- ALL WORK TO CONFORM TO THE MOST RECENT NATIONAL ELECTRICAL CODE AND ANY APPLICABLE LOCAL CODES.
- CONTRACTOR TO VERIFY LOCATION OF ALL UNDERGROUND UTILITIES BEFORE TRENCHING OR AUGERING.
- BEFORE INSTALLING STANDARDS NEAR OVERHEAD FACILITIES CALL ComEd FOR APPROVAL OF LOCATION.
- FOR LOCATION OF EXISTING UNDERGROUND ELECTRICAL CABLE CALL ComEd.
- CONTRACTOR SHALL TAKE ALL NECESSARY MEASURES TO RESTORE ANY SPECIALIZED LANDSCAPING, (i.e. DECORATIVE ROCKS, SHRUBS, PLANTS, ECT.) OR SHALL REPLACE IT, THE COST OF WHICH SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- CARE IS TO BE TAKEN AS NOT TO DAMAGE ANY OF THE EXISTING TRAFFIC SIGNAL CONDUIT, MAGNETIC DETECTORS AND EQUIPMENT. IF ANY OF THE TRAFFIC SIGNAL CONDUIT AND/OR EQUIPMENT IS DAMAGED, THE CONTRACTOR SHALL REPAIR AND/OR REPLACE THE CONDUIT AND/OR EQUIPMENT AT NO COST TO THE CITY OR STATE.
- THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE REQUIREMENT FOR BURIED WARNING TAPE, SPECIFIED AS PART OF "TRENCH AND BACK FILL FOR ELECTRICAL WORK". THE INSTALLATION OF THE TAPE SHALL BE INSPECTED AND APPROVED BY THE RESIDENT ENGINEER PRIOR TO BACKFILLING OR DURING PLOWING OPERATIONS, AS APPLICABLE.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR THE ESTABLISHMENT OF FINISHED GRADE. THE RESIDENT ENGINEER MAY ASSIST THE CONTRACTOR, AS APPLICABLE, BUT THE RESPONSIBILITY FOR COORDINATING THE FINISHED GRADE ELEVATION WITH THE TOP OF THE FOUNDATION HEIGHTS AND THE LIGHT SHALL REMAIN WITH THE CONTRACTOR.
- NO POLES SHALL BE ERECTED UNTIL THE RESPECTIVE FOUNDATIONS HAVE CURED, AS APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE REQUIREMENTS FOR WIRE MARKERS AND SHALL TAG ALL WIRE ACCORDINGLY.
- EQUIPMENT GROUND CONDUCTORS SHALL BE SPLICED AND BONDED AT EACH LIGHT POLE OR OTHER PIECE OF EQUIPMENT.
- UNDERGROUND SPLICES OF LIGHTING CONDUCTORS WILL NOT BE ALLOWED EXCEPT AT LIGHT POLE BASE.
- CONDUITS AND UNIT DUCTS MUST BE POSITIONED IN THE FIELD TO AVOID CONFLICT WITH TREE, BUSHES, DRAINS AND OTHER UTILITIES.



POLE HANDHOLE WIRING DIAGRAM
(TYPICAL FOR SINGLE LUMINAIRE INSTALLATION)

NOTE:
ALLOW 36" LOOP OF CABLES TO INSURE SUFFICIENT SLACK FOR WITHDRAWAL OF THE CONNECTORS OUTSIDE OF THE POLE HANDHOLE.

FILE NAME = 04273_02-LGHT-03 - P03

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| USER NAME = | DESIGNED -- HLG/DWS | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- RG | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
STREET LIGHTING DETAILS

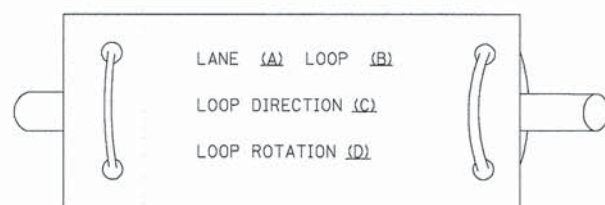
SCALE: NONE SHEET NO. 52 OF 96 SHEETS STA. TO STA.

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|-----------------------|----------------|------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 52 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT | M-8003 (562) | |

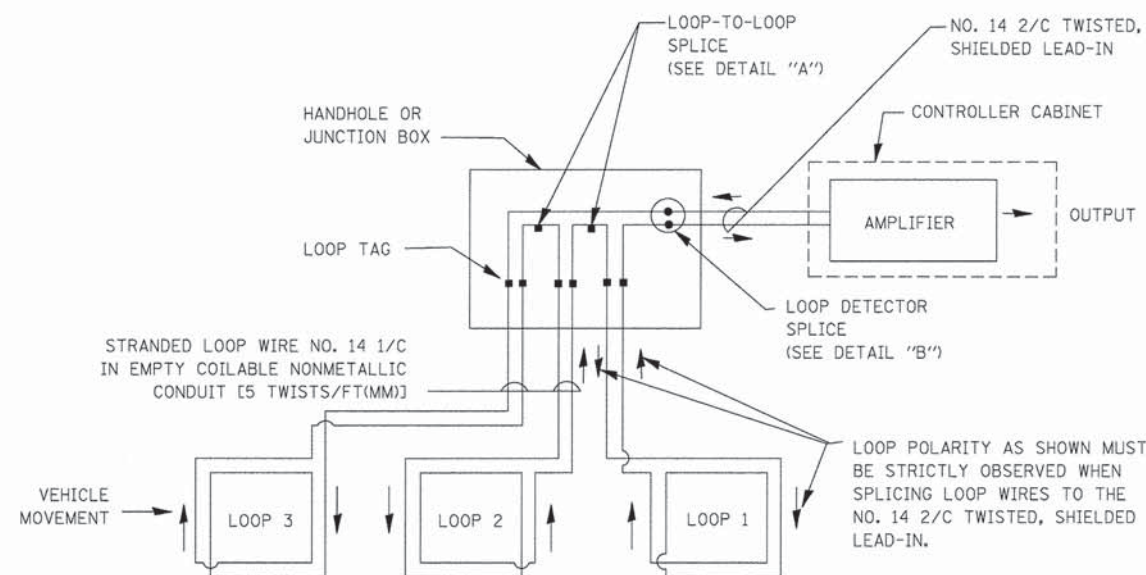
LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

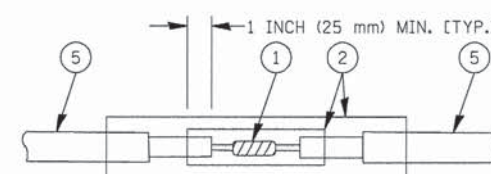


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

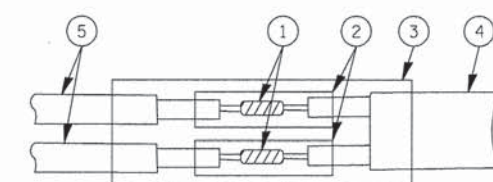


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

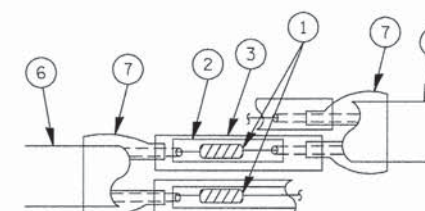


DETAIL "A"
LOOP-TO-LOOP SPLICE

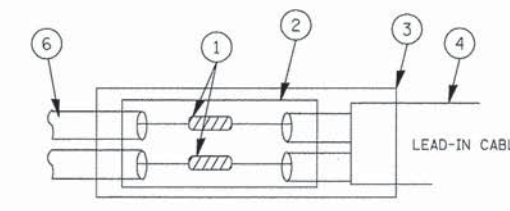


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



PRE-FORMED LOOP

DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- PREFORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

| | | | |
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| ct:\pv\work\PWIDOT\BAUERDL\d2128315\ts05.dgn | | DRAWN - BCK | REVISED - |
| | | CHECKED - DAD | REVISED - |
| | | DATE - 10-28-09 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

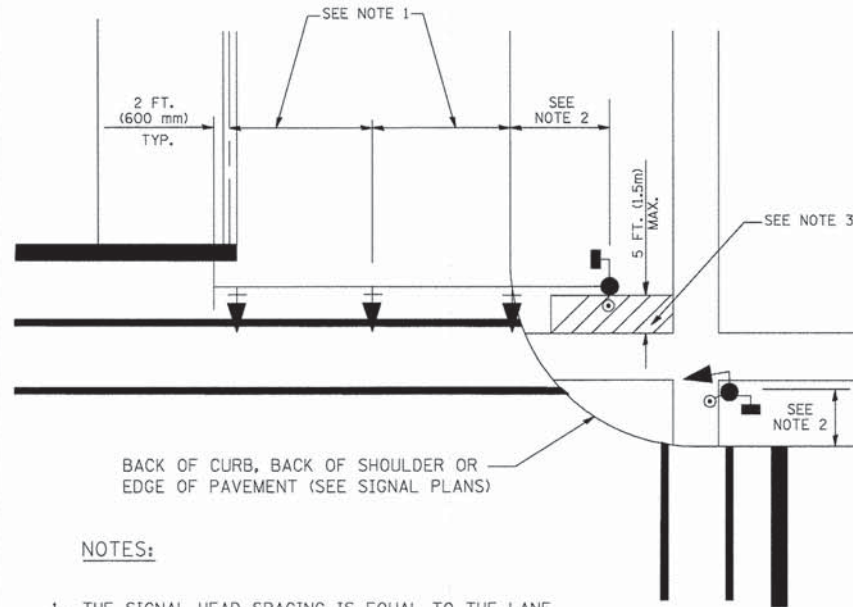
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET NO. 1 OF 6 SHEETS STA. TO STA.

| | | | | |
|--|---------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-0003-00-CH | WILL | 96 | 53 |
| TS-05 | | | CONTRACT NO. 63147 | |
| FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-8003 (562) | | | | |

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

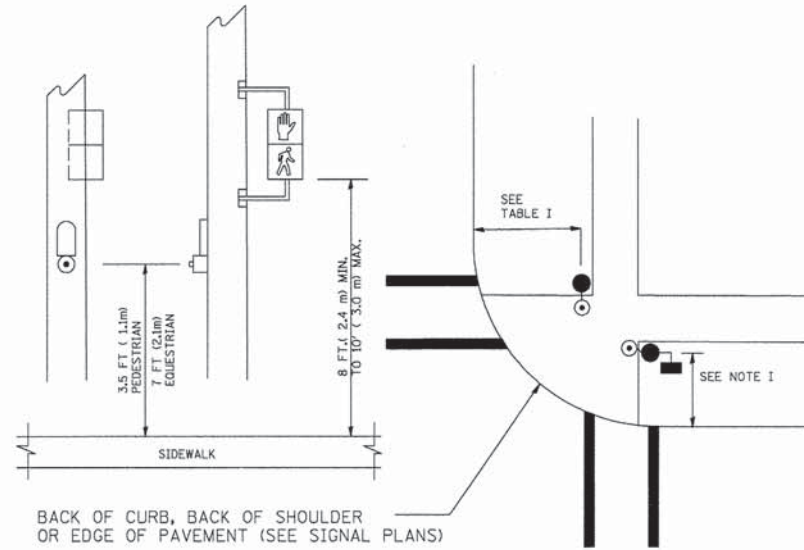
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

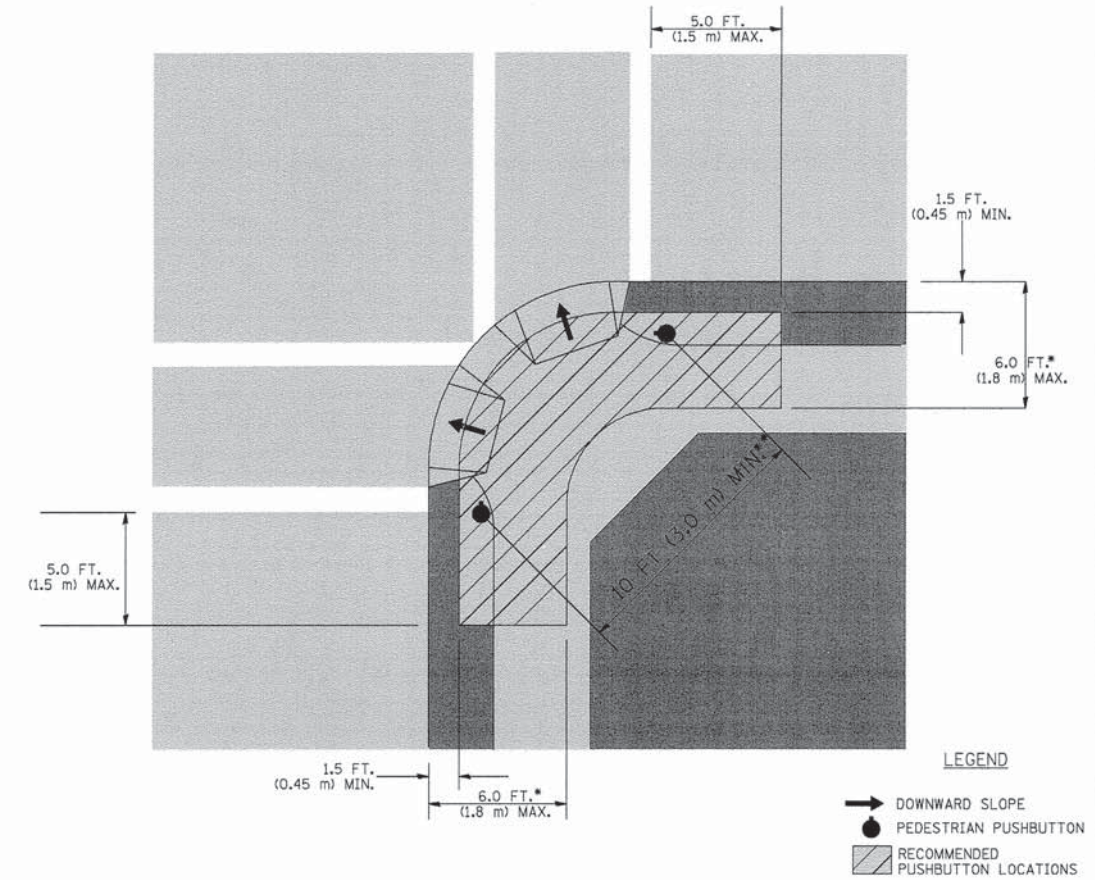
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

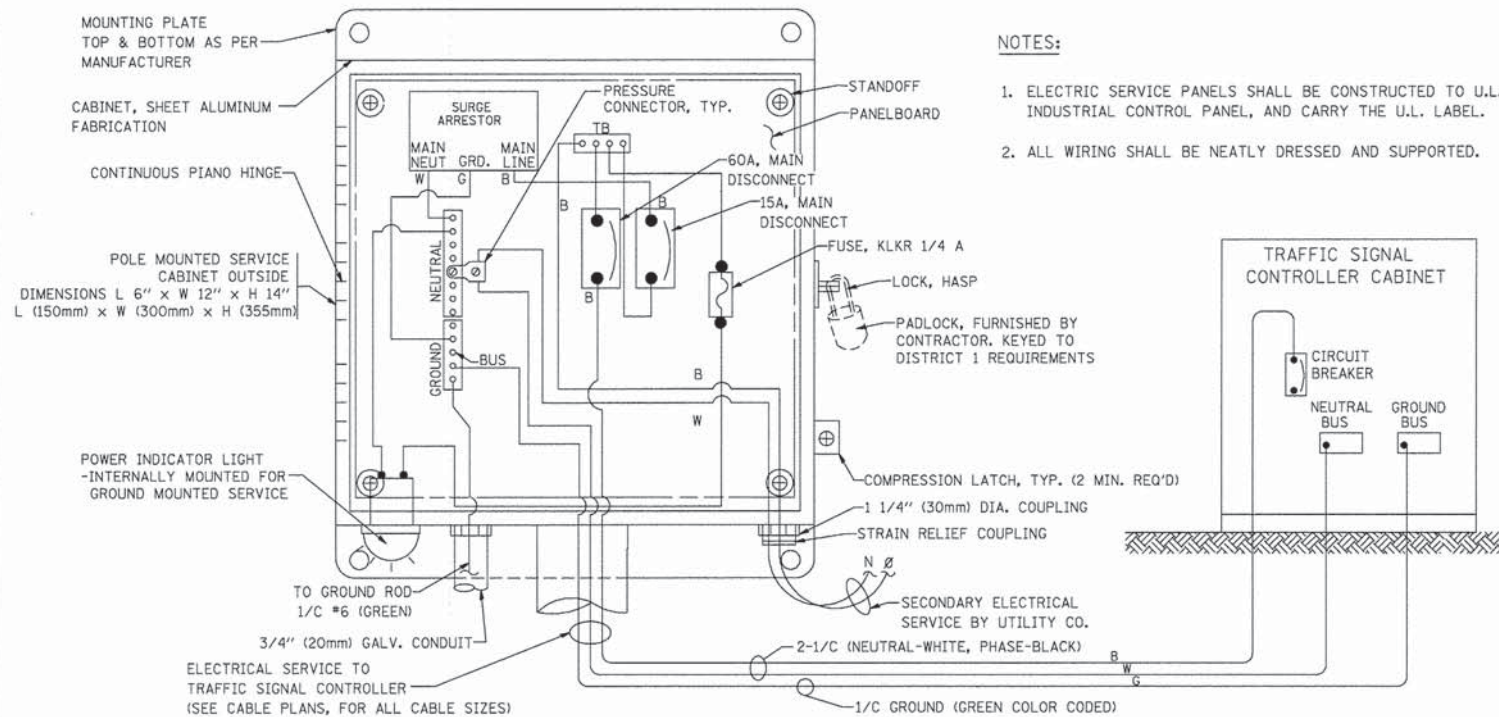
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

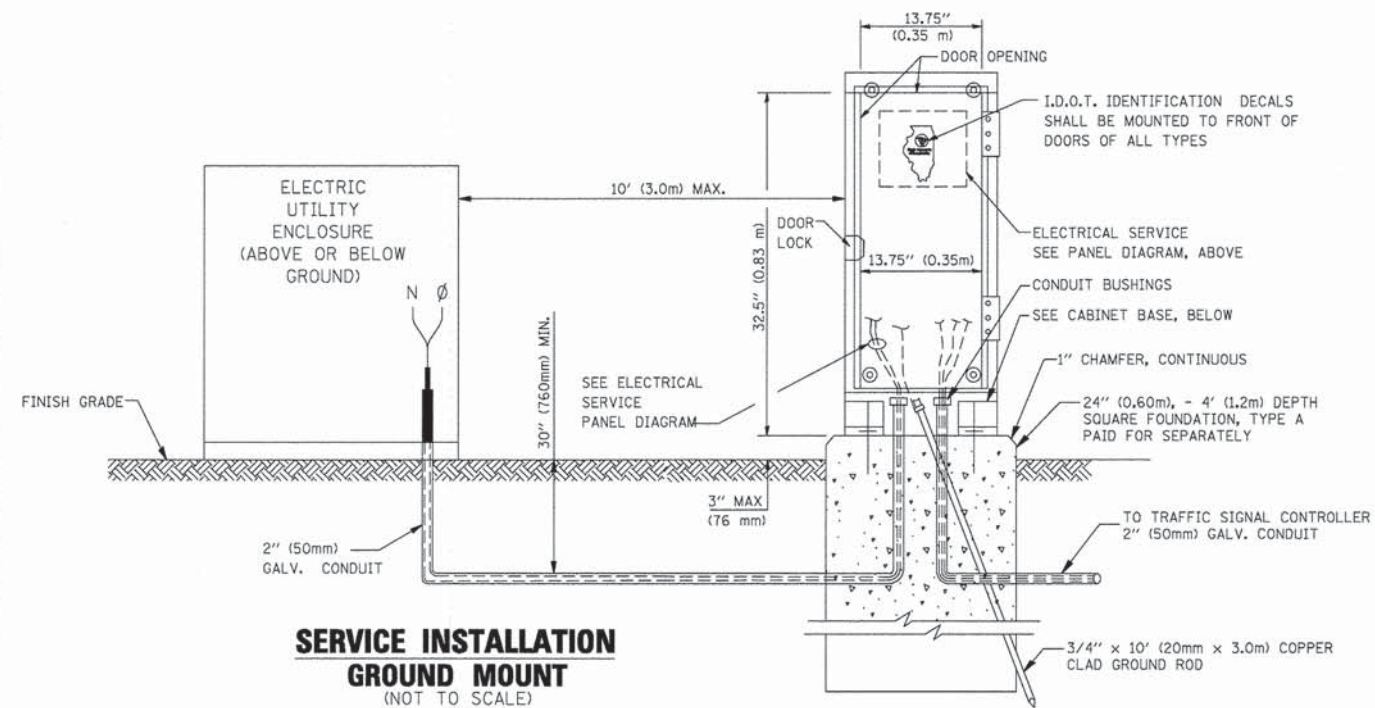
| TRAFFIC SIGNAL EQUIPMENT | COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION) | SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION) |
|------------------------------------|---|---|
| TRAFFIC SIGNAL MAST ARM POLE | 6 FT (1.8m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| TRAFFIC SIGNAL POST | 4 FT (1.2m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| PEDESTRIAN SIGNAL POST | 4 FT (1.2m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| PEDESTRIAN PUSHBUTTON POST | 4 FT (1.2m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| TEMPORARY WOOD POLE | 6 FT (1.8m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| CONTROLLER CABINET | 6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2 | SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3. |
| SERVICE INSTALLATION, GROUND MOUNT | 6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2 | SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3. |

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

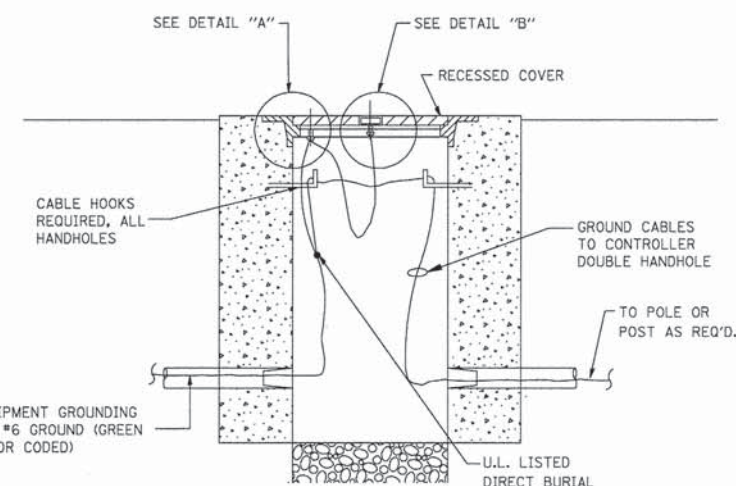
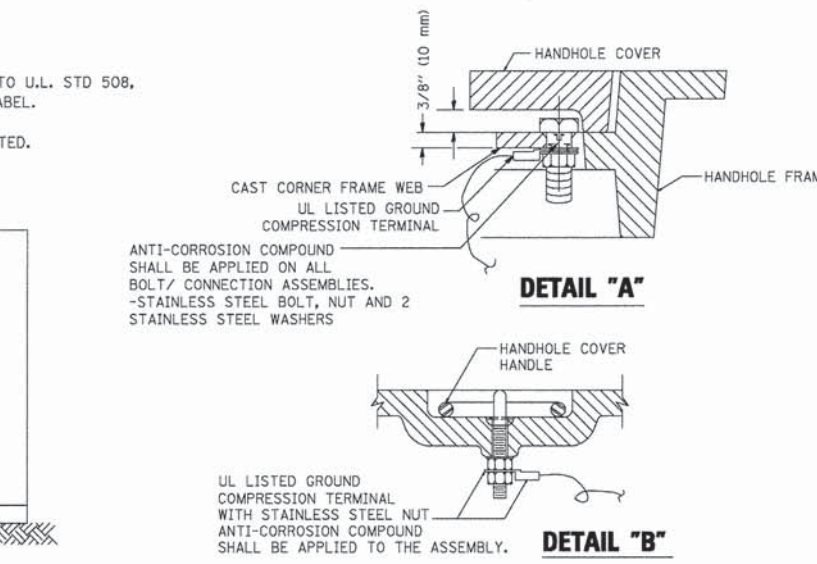
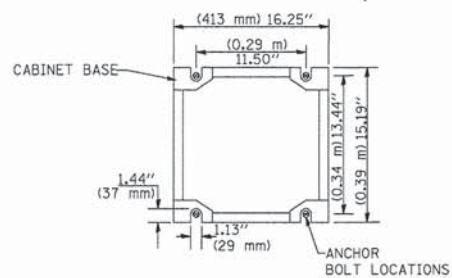


**ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)**

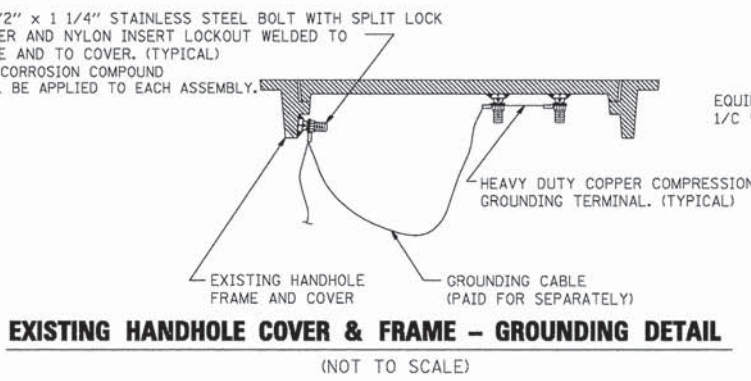


**SERVICE INSTALLATION
GROUND MOUNT
(NOT TO SCALE)**

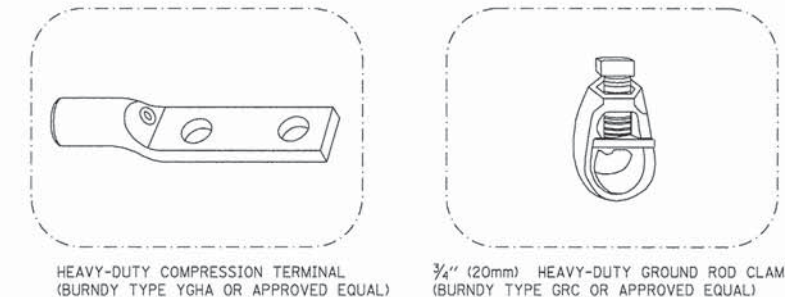
**CABINET – BASE BOLT PATTERN
(NOT TO SCALE)**



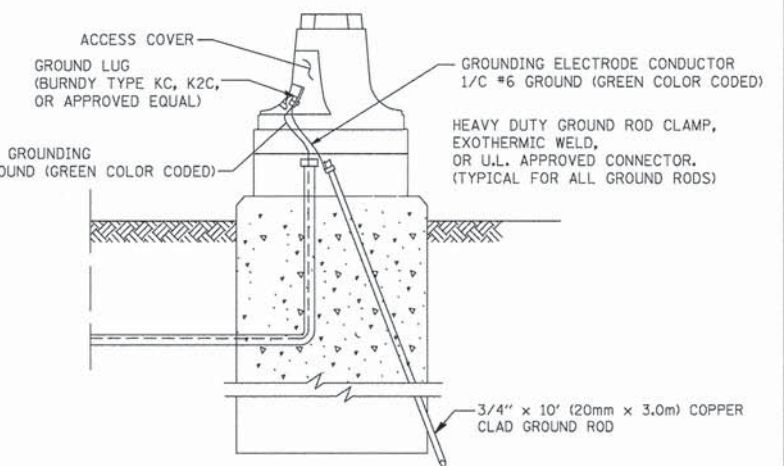
**HANDHOLE COVER & FRAME – GROUNDING DETAIL
(NOT TO SCALE)**



**EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL
(NOT TO SCALE)**



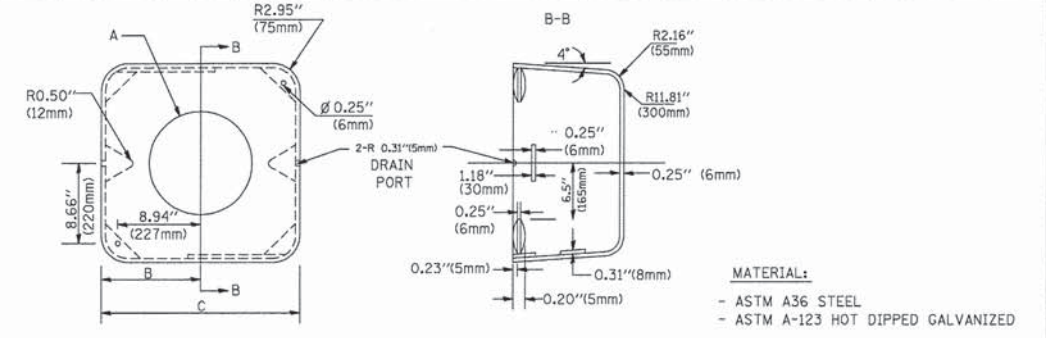
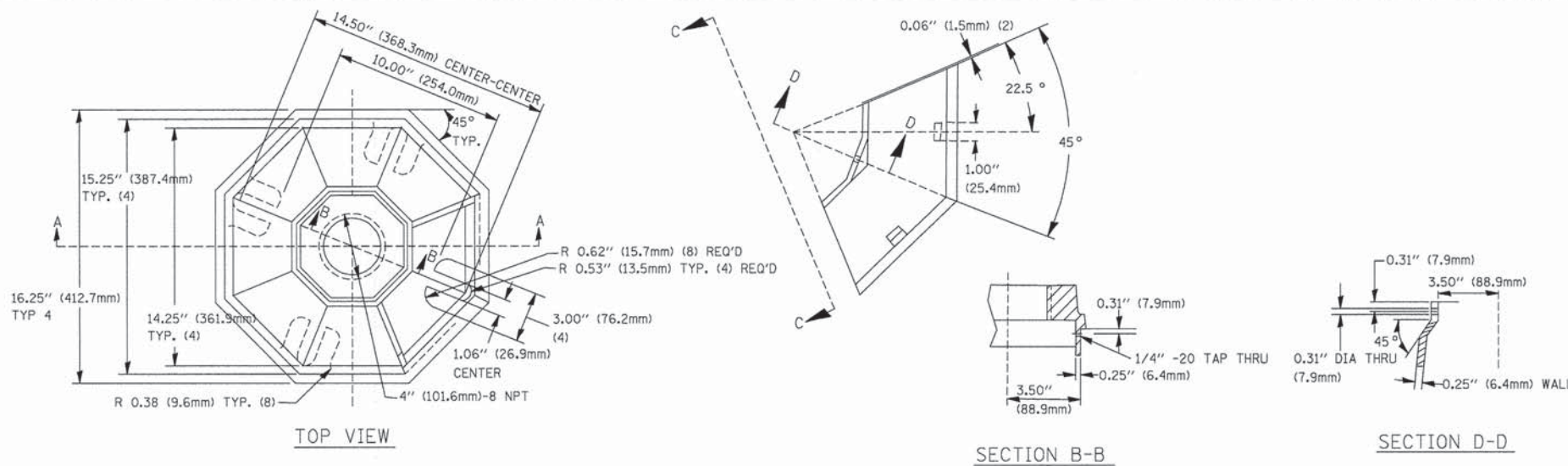
- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



**MAST ARM POLE / POST-GROUNDING DETAIL
(NOT TO SCALE)**

NOTES:
GROUNDING SYSTEM

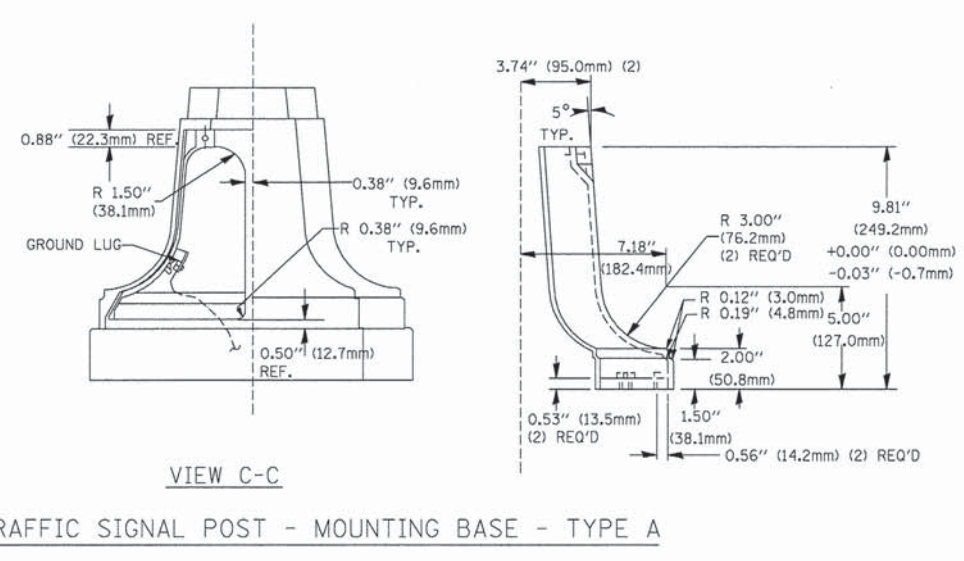
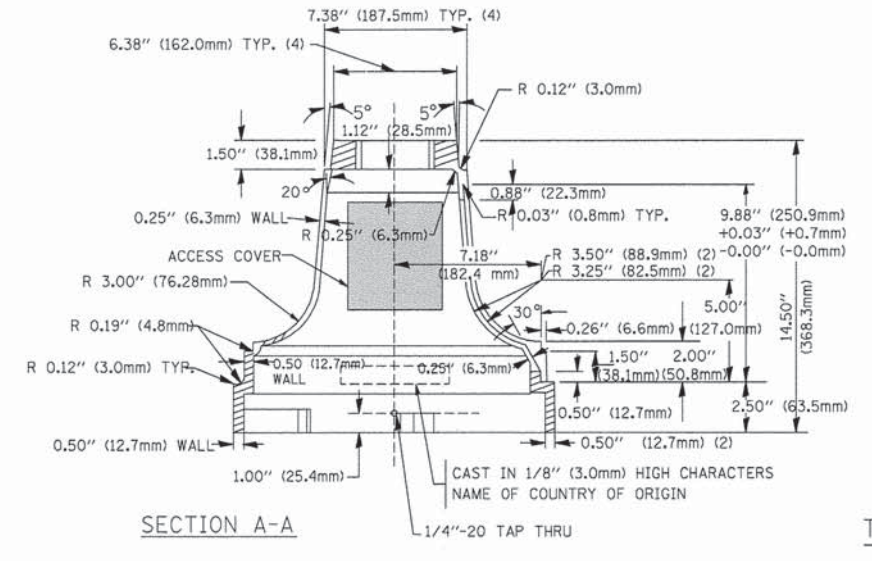
- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



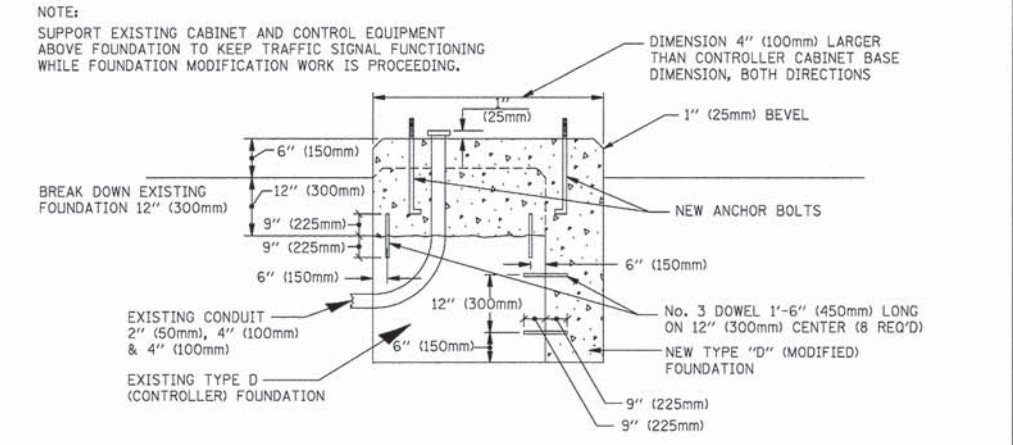
| A | B | C | HEIGHT | WEIGHT |
|-----------|-----------------|----------------|----------------------------|-----------------|
| VARIABLES | 9.5\" (241mm) | 19\" (483mm) | 7\" (178mm) - 12\" (300mm) | 53 lbs (24kg) |
| VARIABLES | 10.75\" (273mm) | 21.5\" (546mm) | 7\" (178mm) - 12\" (300mm) | 68 lbs (31 kg) |
| VARIABLES | 13.0\" (330mm) | 26\" (660mm) | 7\" (178mm) - 12\" (300mm) | 81 lbs (37 kg) |
| VARIABLES | 18.5\" (470mm) | 37\" (940mm) | 7\" (178mm) - 12\" (300mm) | 126 lbs (57 kg) |

NOTES:

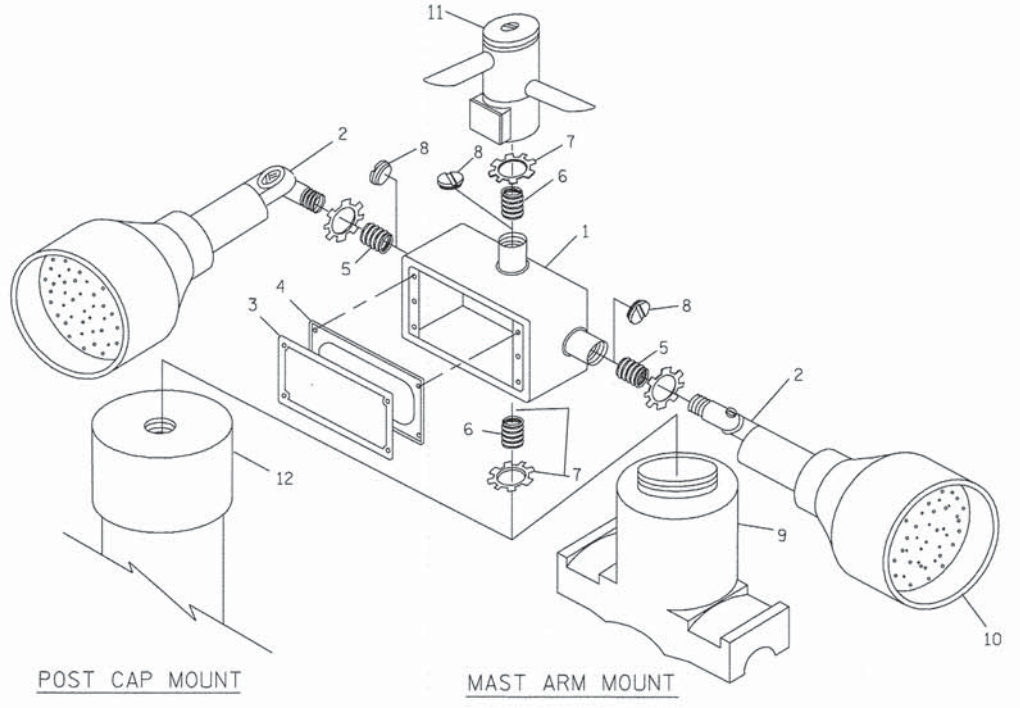
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
- THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
- THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



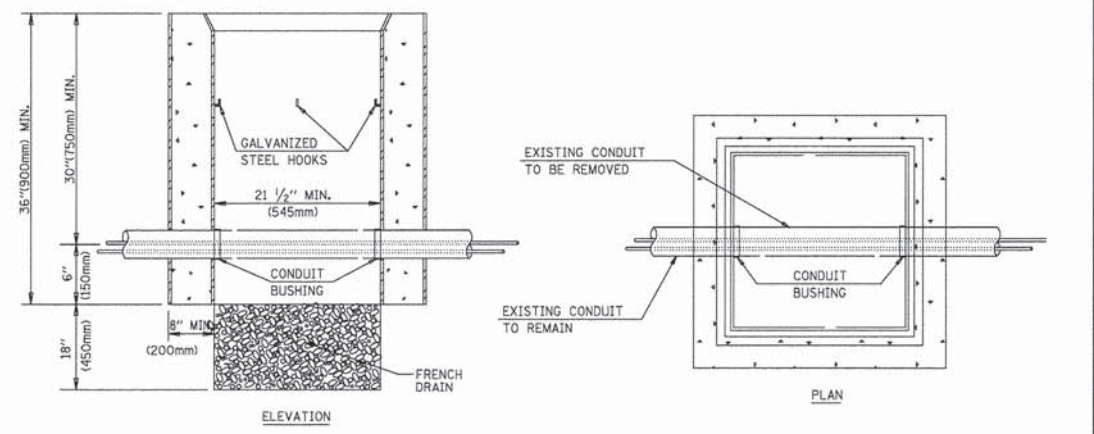
MODIFY EXISTING TYPE "D" FOUNDATION



| ITEM NO. | IDENTIFICATION |
|----------|--|
| 1 | OUTLET BOX- GALV. 21 CU. IN. (0.000344 CU-M) |
| 2 | LAMP HOLDER AND COVER |
| 3 | OUTLET BOX COVER |
| 4 | RUBBER COVER GASKET |
| 5 | REDUCING BUSHING |
| 6 | 3/4\" (19 mm) CLOSE NIPPLE |
| 7 | 3/4\" (19 mm) LOCKNUT |
| 8 | 3/4\" (19 mm) HOLE PLUG |
| 9 | SADDLE BRACKET - GALV. |
| 10 | 6 WATT PAR 38 LED FLOOD LAMP |
| 11 | DETECTOR UNIT |
| 12 | POST CAP (18 FT. (5.4 m) POST MIN.) |

NOTES:

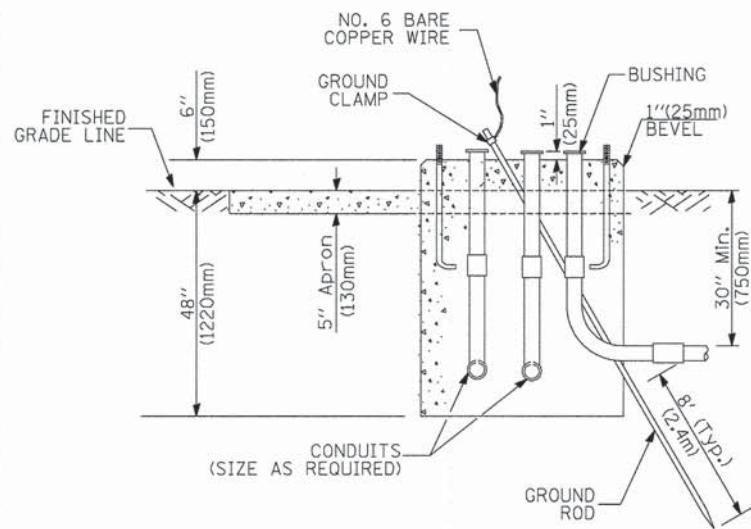
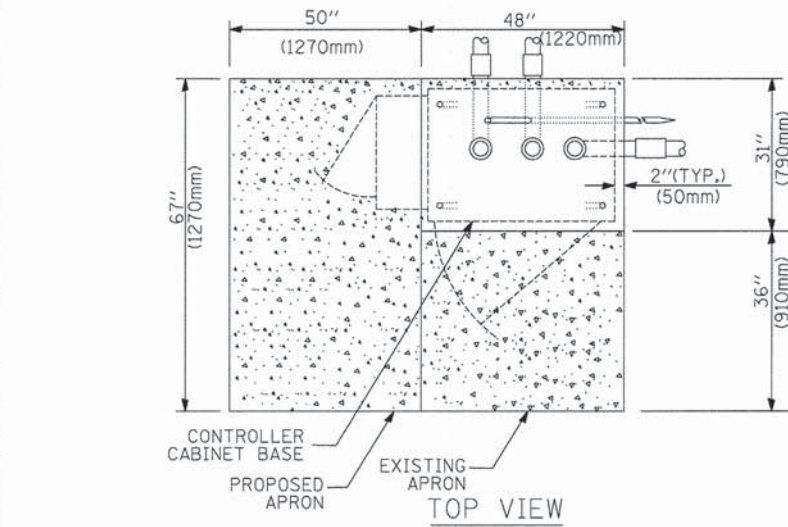
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



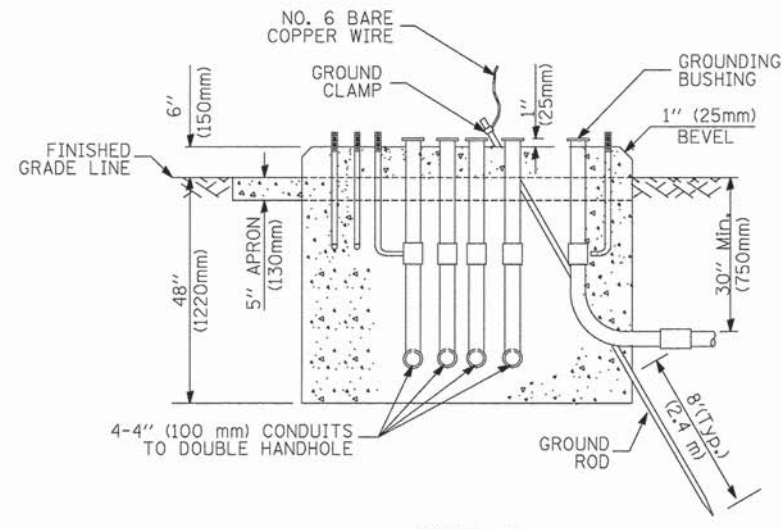
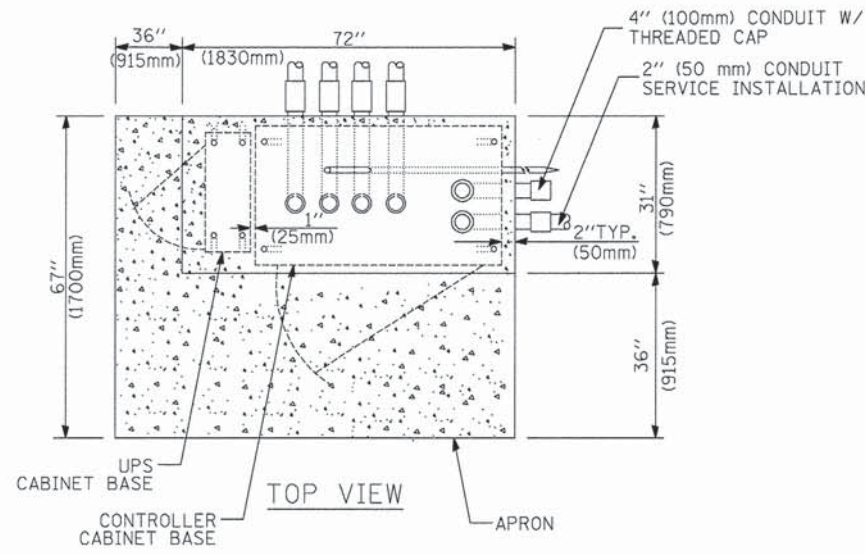
NOTES:

- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
- REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

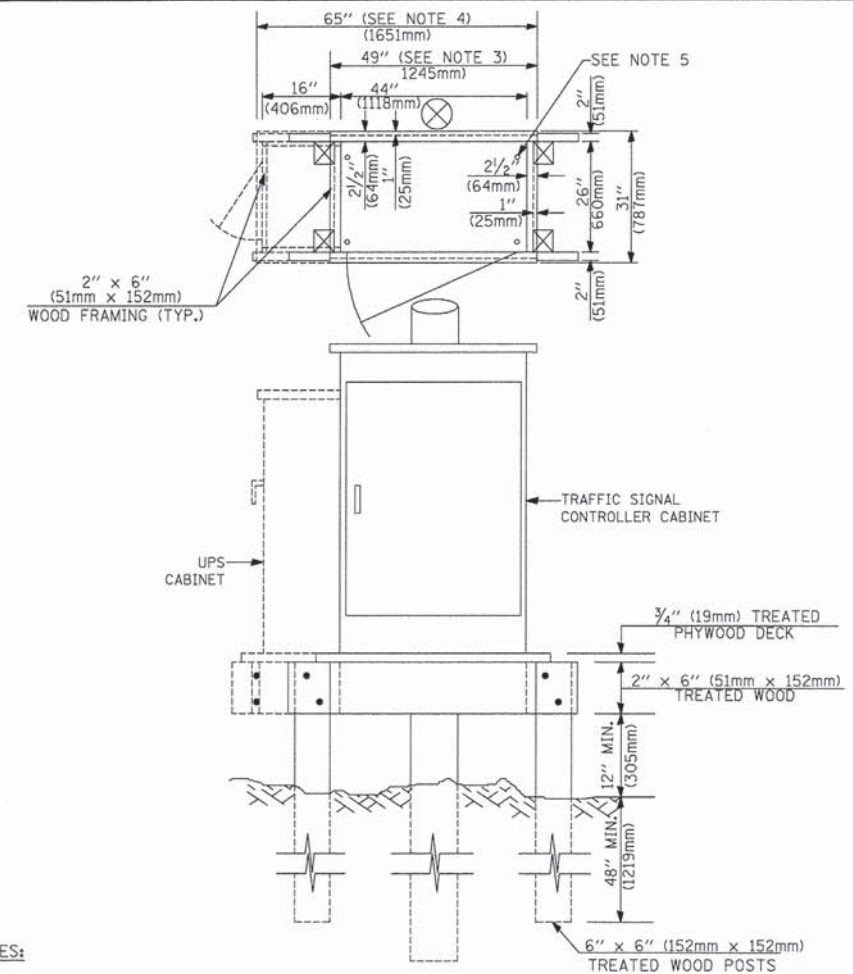
HANDHOLE TO INTERCEPT EXISTING CONDUIT



**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**



**TYPE C
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

| CABLE SLACK LENGTH | FEET | METER |
|---|------|-------|
| HANDHOLE | 6.5 | 2.0 |
| DOUBLE HANDHOLE | 13.0 | 4.0 |
| SIGNAL POST | 2.0 | 0.6 |
| MAST ARM | 2.0 | 0.6 |
| CONTROLLER CABINET | 1.5 | 0.5 |
| FIBER OPTIC AT CABINET | 13.0 | 4.0 |
| ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION) | 1.5 | 0.5 |
| GROUND CABLE (SIGNAL POST, MAST ARM, CABINET) | 1.5 | 0.5 |
| GROUND CABLE (BETWEEN FRAME AND COVER) | 5.0 | 1.6 |

CABLE SLACK

| VERTICAL CABLE LENGTH | FEET | METER |
|---|--------|-------|
| MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM) | 20.0+L | 6.0+L |
| BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE) | 13.0 | 4.0 |
| PEDESTRIAN PUSH BUTTON | 6.0 | 2.0 |
| SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP | 13.5 | 4.1 |
| SERVICE INSTALLATION POLE MOUNT TO GROUND | 13.5 | 4.1 |
| SERVICE INSTALLATION GROUND MOUNT | 6.0 | 2.0 |
| FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT) | 3.0 | 1.0 |

VERTICAL CABLE LENGTH

| FOUNDATION | DEPTH |
|---|--------------|
| TYPE A - Signal Post | 4'-0" (1.2m) |
| TYPE C - CONTROLLER W/ UPS | 4'-0" (1.2m) |
| TYPE D - CONTROLLER | 4'-0" (1.2m) |
| SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE | 4'-0" (1.2m) |

DEPTH OF FOUNDATION

| Mast Arm Length | ① Foundation Depth | Foundation Diameter | Spiral Diameter | Quantity of Rebars | Size of Rebars |
|--|--------------------|---------------------|-----------------|--------------------|----------------|
| Less than 30' (9.1 m) | 10'-0" (3.0 m) | 30" (750mm) | 24" (600mm) | 8 | 6(19) |
| Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m) | 13'-6" (4.1 m) | 30" (750mm) | 24" (600mm) | 8 | 6(19) |
| Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m) | 11'-0" (3.4 m) | 36" (900mm) | 30" (750mm) | 12 | 7(22) |
| Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m) | 13'-0" (4.0 m) | 36" (900mm) | 30" (750mm) | 12 | 7(22) |
| Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m) | 15'-0" (4.6 m) | 36" (900mm) | 30" (750mm) | 12 | 7(22) |
| Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m) | 21'-0" (6.4 m) | 42" (1060mm) | 36" (900mm) | 16 | 8(25) |
| Greater than or equal to 75' (22.9 m) | 25'-0" (7.6 m) | 42" (1060mm) | 36" (900mm) | 16 | 8(25) |

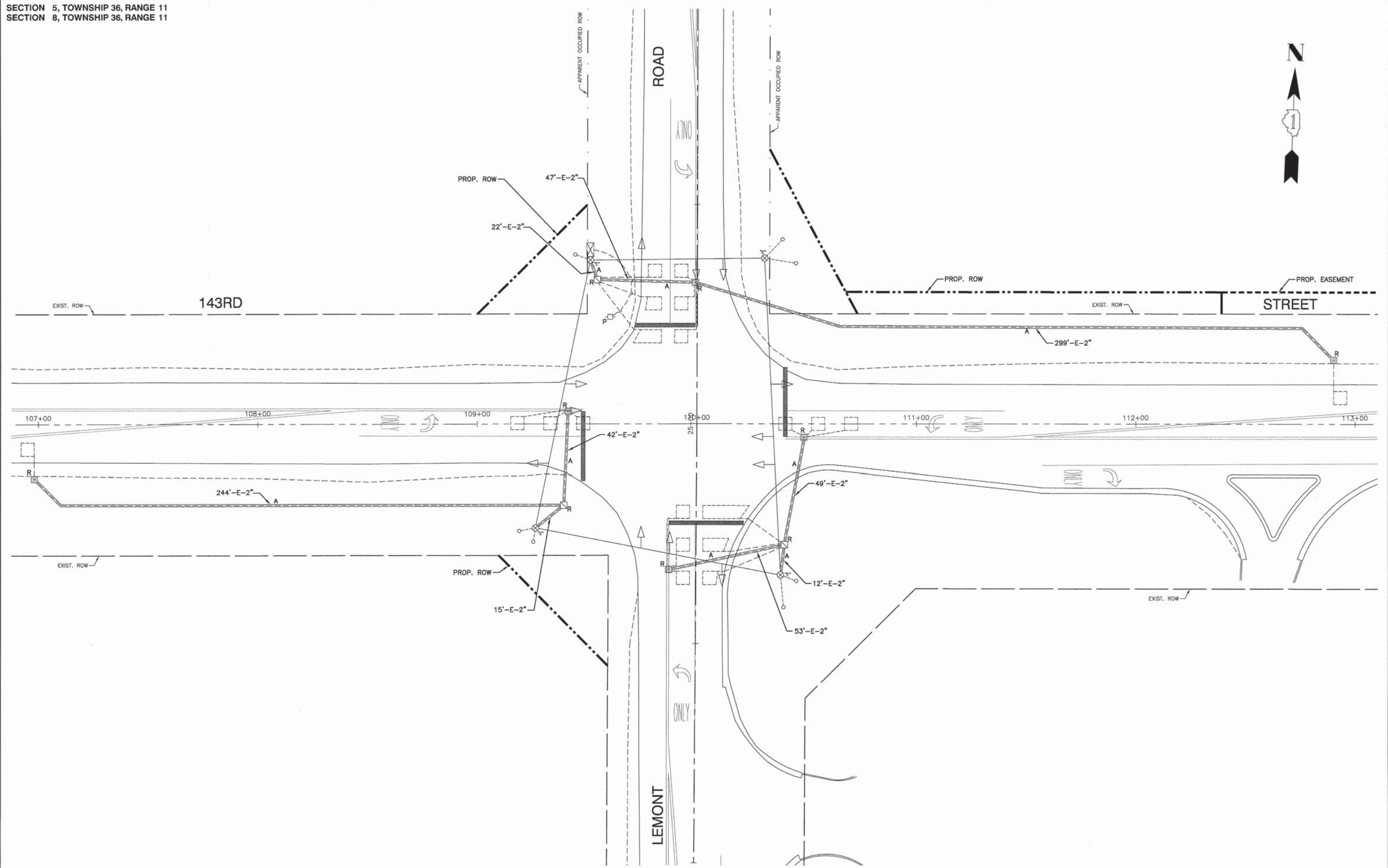
NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

TRAFFIC SIGNAL LEGEND

| ITEM | REMOVAL | EXISTING | PROPOSED | ITEM | REMOVAL | EXISTING | PROPOSED | ITEM | REMOVAL | EXISTING | PROPOSED |
|---|---------|----------|----------|---|---------|----------|----------|--|---------|----------|----------|
| CONTROLLER CABINET | | | | EMERGENCY VEHICLE LIGHT DETECTOR | | | | ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE | | | |
| RAILROAD CONTROL CABINET | | | | CONFIRMATION BEACON | | | | COAXIAL CABLE | | | |
| COMMUNICATIONS CABINET | | | | HANDHOLE | | | | VENDOR CABLE FOR CAMERA | | | |
| MASTER CONTROLLER | | | | HEAVY DUTY HANDHOLE | | | | COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED | | | |
| MASTER MASTER CONTROLLER | | | | DOUBLE HANDHOLE | | | | FIBER OPTIC CABLE NO. 62.5/125, MM12F | | | |
| UNINTERRUPTIBLE POWER SUPPLY | | | | JUNCTION BOX | | | | FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F | | | |
| SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT | | | | GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P) | | | | FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F | | | |
| TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT | | | | TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE | | | | FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS) | | | |
| STEEL MAST ARM ASSEMBLY AND POLE | | | | COMMON TRENCH | | | | GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE | | | |
| ALUMINUM MAST ARM ASSEMBLY AND POLE | | | | COILABLE NONMETALLIC CONDUIT (EMPTY) | | | | CONTROLLER CABINET AND FOUNDATION TO BE REMOVED | | | |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE | | | | SYSTEM ITEM | | | | STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED | | | |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA | | | | INTERSECTION ITEM | | | | ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED | | | |
| SIGNAL POST | | | | REMOVE ITEM | | | | STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED | | | |
| TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM | | | | RELOCATE ITEM | | | | SIGNAL POST AND FOUNDATION TO BE REMOVED | | | |
| GUY WIRE | | | | ABANDON ITEM | | | | INTERSECTION & SAMPLING (SYSTEM) DETECTOR | | | |
| SIGNAL HEAD | | | | 12" (300mm) TRAFFIC SIGNAL SECTION | | | | SAMPLING (SYSTEM) DETECTOR | | | |
| SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE) | | | | 12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE | | | | EXISTING INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR | | | |
| SIGNAL HEAD WITH BACKPLATE | | | | SIGNAL FACE | | | | EXISTING PREFORMED INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR | | | |
| SIGNAL HEAD OPTICALLY PROGRAMMED | | | | SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD | | | | PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR | | | |
| FLASHER INSTALLATION (S DENOTES SOLAR POWER) | | | | 12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL | | | | PREFORMED SAMPLING (SYSTEM) DETECTOR | | | |
| PEDESTRIAN SIGNAL HEAD | | | | 12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED | | | | RAILROAD SYMBOLS | | | |
| PEDESTRIAN PUSHBUTTON DETECTOR | | | | 12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID | | | | RAILROAD CONTROL CABINET | | | |
| ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR | | | | PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER | | | | RAILROAD CANTILEVER MAST ARM | | | |
| ILLUMINATED SIGN "NO LEFT TURN" | | | | RADIO INTERCONNECT | | | | FLASHING SIGNAL | | | |
| ILLUMINATED SIGN "NO RIGHT TURN" | | | | RADIO REPEATER | | | | CROSSING GATE | | | |
| DETECTOR LOOP, TYPE I | | | | DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED | | | | CROSSBUCK | | | |
| PREFORMED DETECTOR LOOP | | | | GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN) | | | | | | | |
| MICROWAVE VEHICLE SENSOR | | | | | | | | | | | |
| VIDEO DETECTION CAMERA | | | | | | | | | | | |
| VIDEO DETECTION ZONE | | | | | | | | | | | |
| PAN, TILT, ZOOM CAMERA | | | | | | | | | | | |
| WIRELESS DETECTOR SENSOR | | | | | | | | | | | |
| WIRELESS ACCESS POINT | | | | | | | | | | | |



| | | | |
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| | | CHECKED -- | REVISED -- |
| | PLOT SCALE = | DRAWN -- JRA | REVISED -- |
| | PLOT DATE = 05-30-13 | CHECKED -- BDB | REVISED -- |

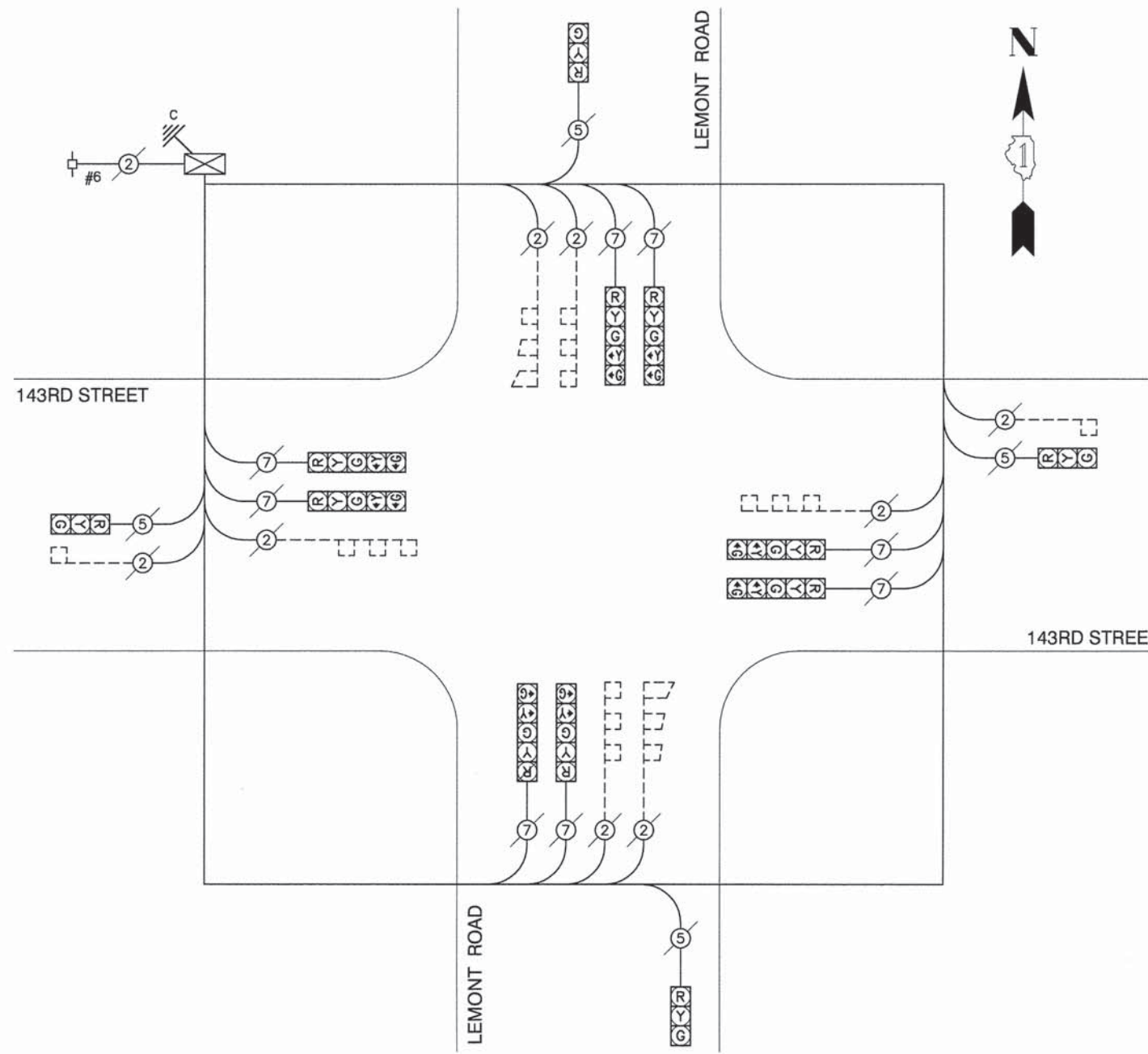
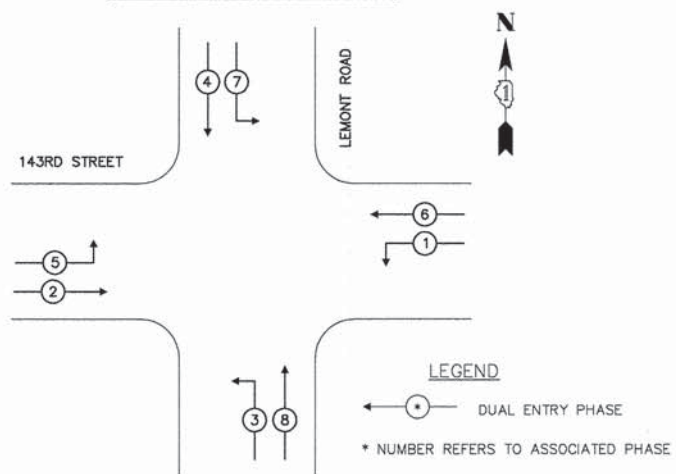
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
EXISTING TEMPORARY TRAFFIC SIGNAL PLAN

SCALE: 1"=20' SHEET NO. 59 OF 96 SHEETS STA. TO STA.

| | | | | |
|--------------------------------|----------------|--------|-------------------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL. | 96 | 59 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | FED. AID PROJECT M-8003 (562) | |

EXISTING
CONTROLLER SEQUENCE



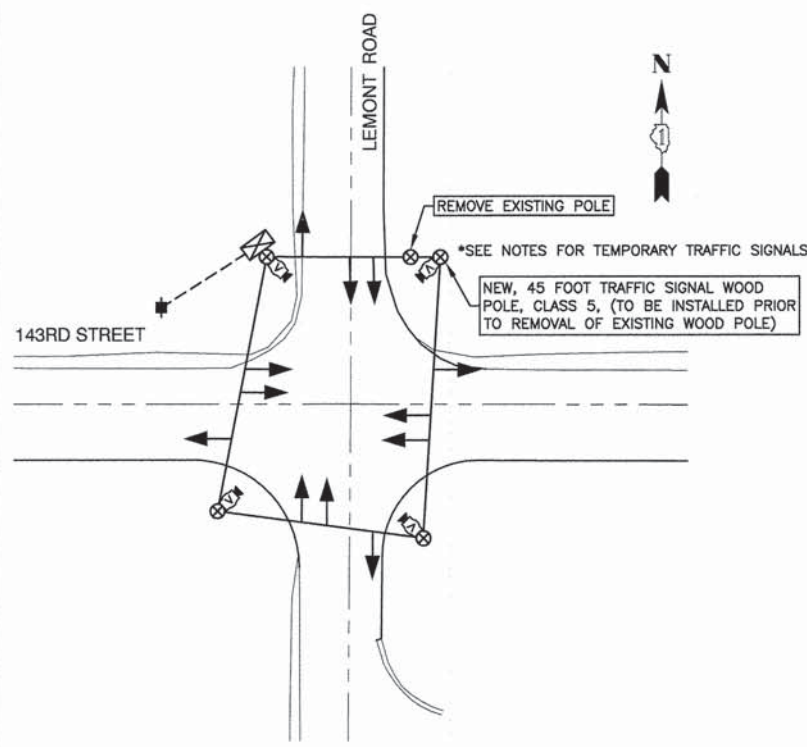
EXISTING TEMPORARY
CABLE PLAN

| | | | |
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| | | CHECKED -- | REVISED -- |
| | PLOT SCALE = | DRAWN -- JRA | REVISED -- |
| | PLOT DATE = 05-30-13 | CHECKED -- BOB | REVISED -- |

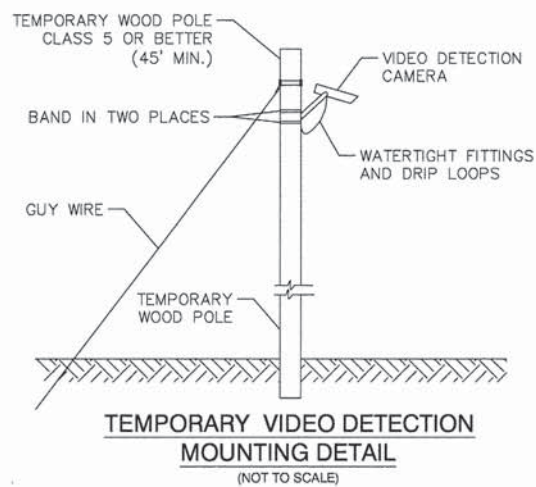
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | |
|---|--|
| 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION EXISTING TEMPORARY TRAFFIC SIGNAL CABLE PLAN | |
| SCALE: 1"=50' | SHEET NO. 60 OF 96 SHEETS STA. TO STA. |

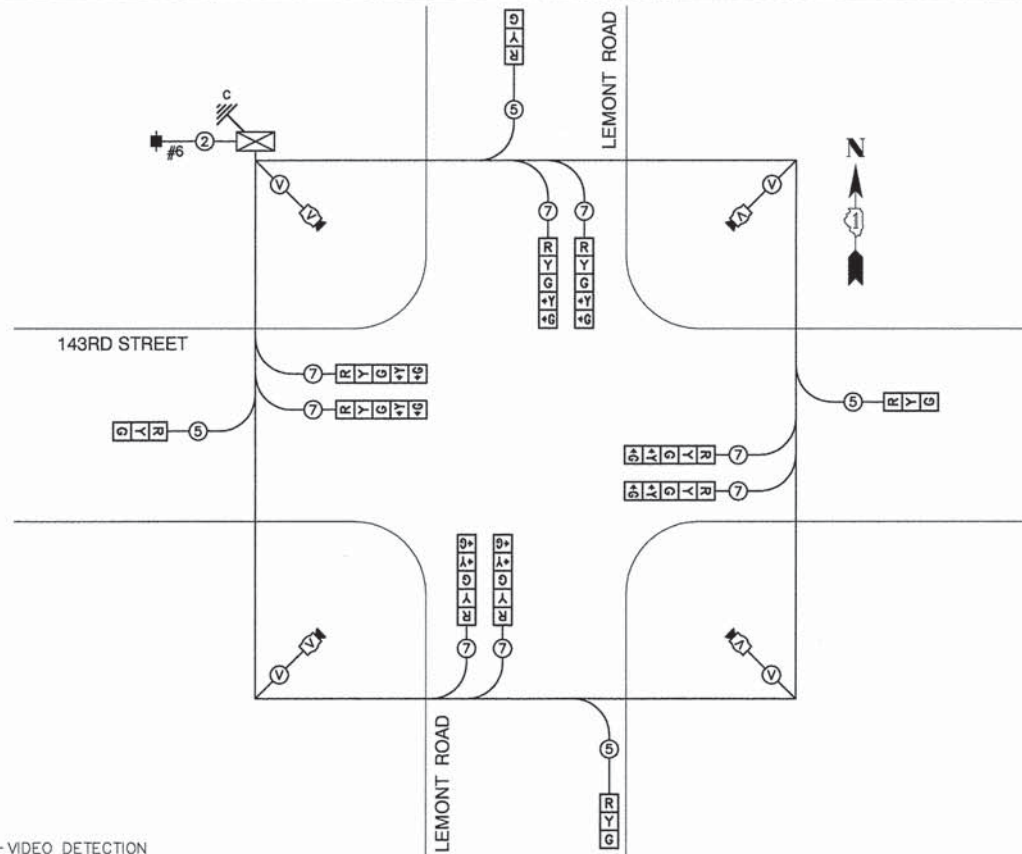
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| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT M-8003 (562) | |



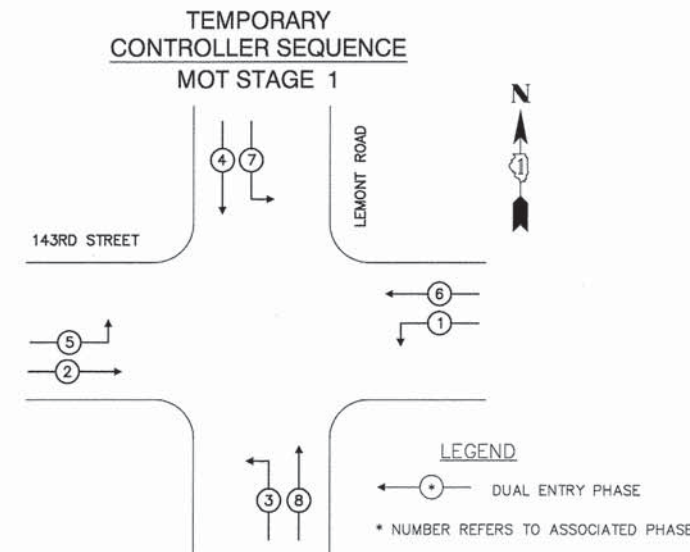
MOT STAGE 1



TEMPORARY VIDEO DETECTION MOUNTING DETAIL (NOT TO SCALE)



CABLE PLAN MOT STAGE 1 (NOT TO SCALE)

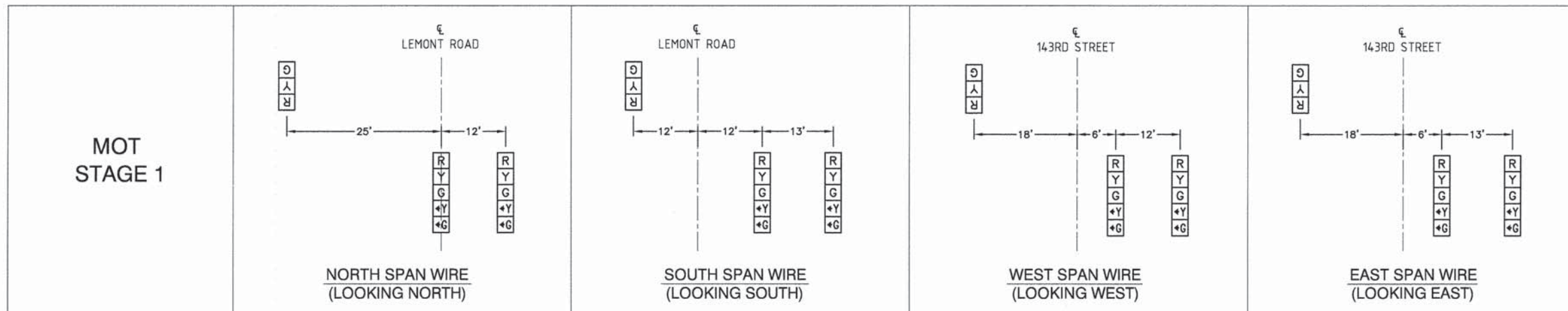


LEGEND
 ← ● → DUAL ENTRY PHASE
 * NUMBER REFERS TO ASSOCIATED PHASE

NOTES FOR TEMPORARY TRAFFIC SIGNALS

- PRIOR TO STAGE 1, CONTRACTOR SHALL INSTALL A NEW, 45 FOOT TRAFFIC SIGNAL WOOD POLE, CLASS 5, ON THE NORTHEAST CORNER OF 143RD STREET AND LEMONT ROAD INTERSECTION AND RELOCATE SIGNALS. ADDITIONAL SPAN WIRE, TETHER WIRE AND SIGNAL CABLE WILL NOT BE REQUIRED DUE TO INCREASED WIRE QUANTITIES PROVIDED FROM PREVIOUS TEMPORARY SIGNAL INSTALLATION'S ANTICIPATION OF FUTURE POLE RELOCATION. THE CONTRACTOR SHALL REMOVE EXISTING WOOD SIGNAL POLE AFTER THE RELOCATION HAS BEEN COMPLETED. THIS WORK AND ALL WORK PERTAINING TO THE "TEMPORARY TRAFFIC SIGNAL-MOT PLAN" SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION (SPECIAL).
- ALL APPROACHES SHALL HAVE VEHICULAR DETECTION PROVIDED BY A VIDEO VEHICLE DETECTION SYSTEM. THE CONTRACTOR SHALL INSTALL WIRE AND ADJUST THE ALIGNMENT OF THE VIDEO VEHICLE DETECTION SYSTEM IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATIONS AND REQUIREMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE ALIGNMENT OF THE VIDEO VEHICLE DETECTION SYSTEM FOR ALL CONSTRUCTION STAGING CHANGES AND FOR MAINTAINING PROPER ALIGNMENT THROUGHOUT THE PROJECT. A REPRESENTATIVE OF THE APPROVED CONTROL EQUIPMENT VENDOR SHALL BE PRESENT AND ASSIST THE CONTRACTOR IN SETTING UP AND MAINTAINING THE VIDEO VEHICLE DETECTION SYSTEM.
- ALL TRAFFIC SIGNAL HEADS NOT BEING USED SHALL BE COVERED AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL ALSO BE REQUIRED TO RELOCATE SIGNAL HEADS DURING STAGING OPERATIONS TO THE LOCATIONS SHOWN BELOW.

| SCHEDULE OF QUANTITIES | | |
|---|------|------|
| ITEM | UNIT | QUAN |
| MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION (SPECIAL) | EACH | 1 |
| TRAFFIC SIGNAL WOOD POLE, 45 FT, CLASS 5 | EACH | 1 |
| VIDEO VEHICLE DETECTION SYSTEM | EACH | 1 |



FILE NAME = 04273_02-SGNL-02 - DTL01

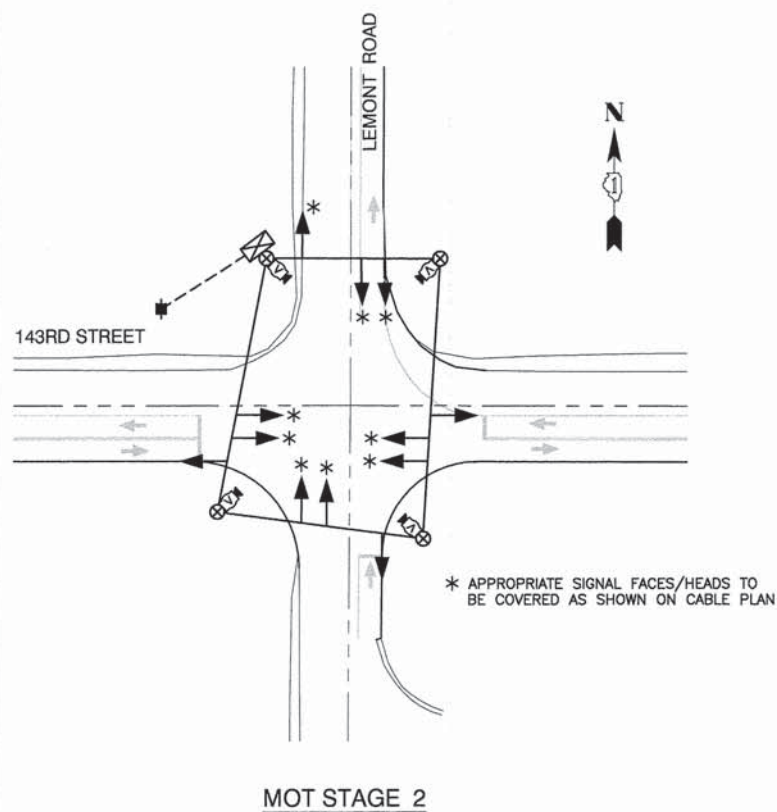
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| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- JRA | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- BDB | REVISED -- |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

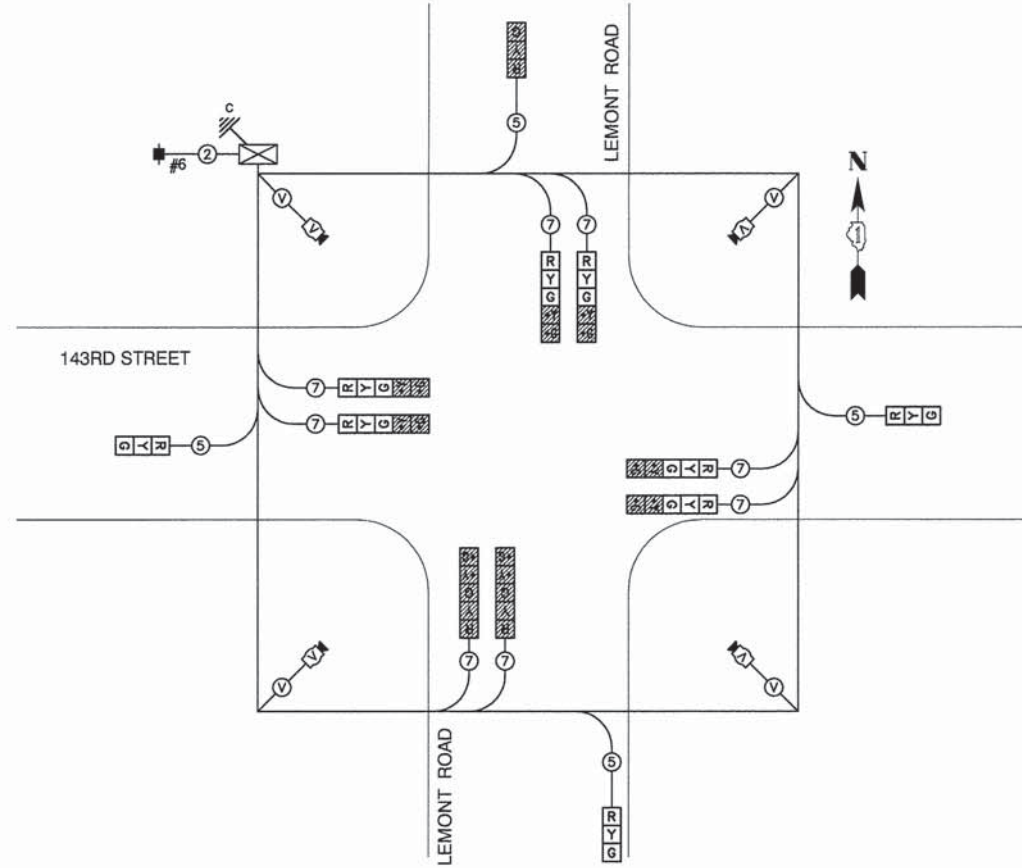
143RD STREET AND LEMONT ROAD
 INTERSECTION RECONSTRUCTION
 TEMPORARY TRAFFIC SIGNAL - MOT PLAN

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 61 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |

SCALE: NONE SHEET NO. 61 OF 96 SHEETS STA. TO STA.



MOT STAGE 2



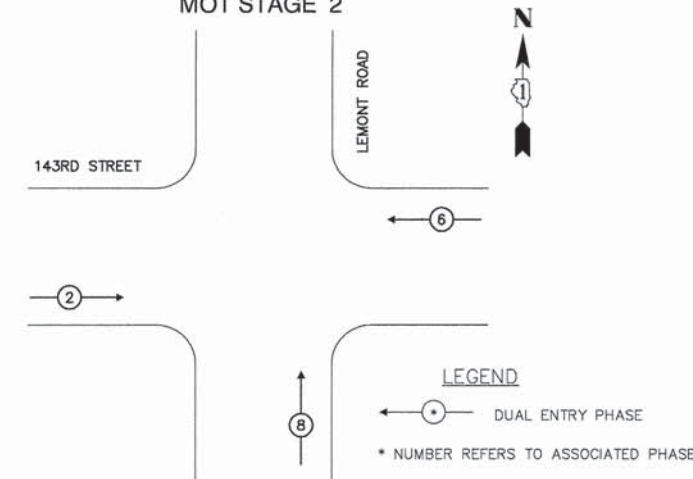
CABLE PLAN MOT STAGE 2

NOT TO SCALE

LEGEND

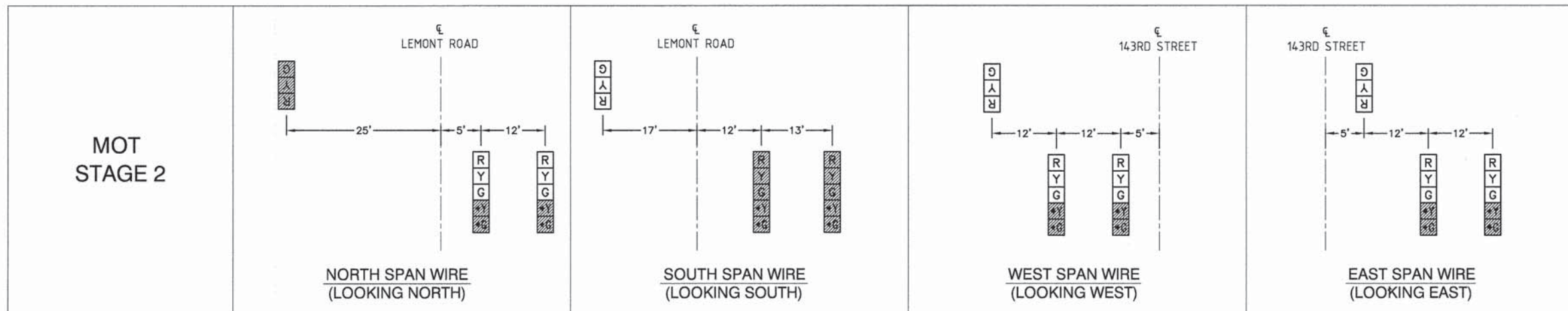
TEMPORARY TRAFFIC SIGNAL SECTION, 12" TO BE COVERED DURING MOT STAGE

TEMPORARY CONTROLLER SEQUENCE MOT STAGE 2



NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL APPROACHES SHALL HAVE VEHICULAR DETECTION PROVIDED BY A VIDEO VEHICLE DETECTION SYSTEM. THE CONTRACTOR SHALL INSTALL WIRE AND ADJUST THE ALIGNMENT OF THE VIDEO VEHICLE DETECTION SYSTEM IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATIONS AND REQUIREMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE ALIGNMENT OF THE VIDEO VEHICLE DETECTION SYSTEM FOR ALL CONSTRUCTION STAGING CHANGES AND FOR MAINTAINING PROPER ALIGNMENT THROUGHOUT THE PROJECT. A REPRESENTATIVE OF THE APPROVED CONTROL EQUIPMENT VENDOR SHALL BE PRESENT AND ASSIST THE CONTRACTOR IN SETTING UP AND MAINTAINING THE VIDEO VEHICLE DETECTION SYSTEM.
2. ALL TRAFFIC SIGNAL HEADS NOT BEING USED SHALL BE COVERED AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL ALSO BE REQUIRED TO RELOCATE SIGNAL HEADS DURING STAGING OPERATIONS TO THE LOCATIONS SHOWN BELOW.



FILE NAME = 04273_02-SGNL-02 - DTL02

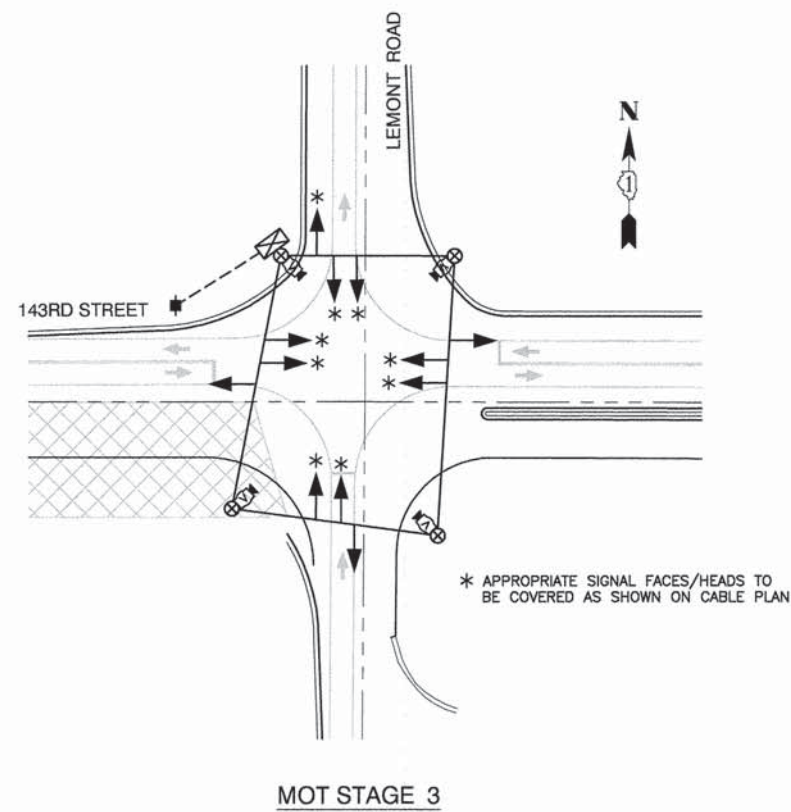
| | | |
|----------------------|---------------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG/DWS | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- JRA | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- BDB | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

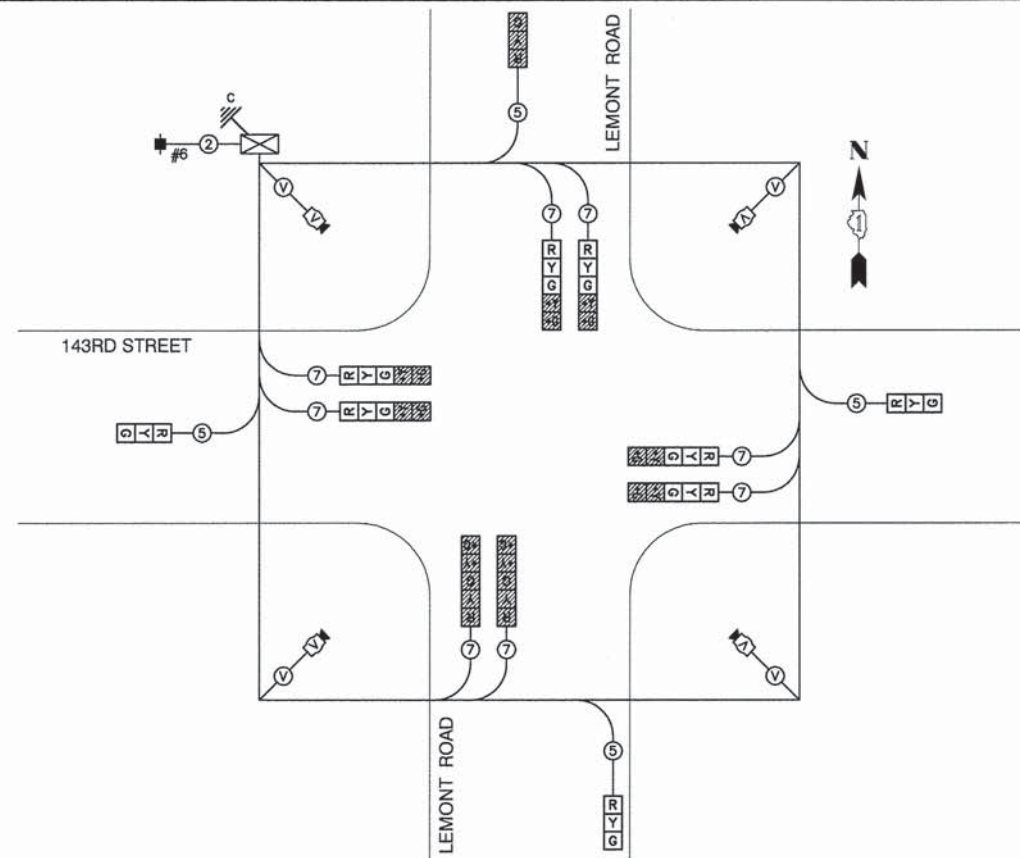
143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
TEMPORARY TRAFFIC SIGNAL - MOT PLAN

SCALE: NONE SHEET NO. 62 OF 96 SHEETS STA. TO STA.

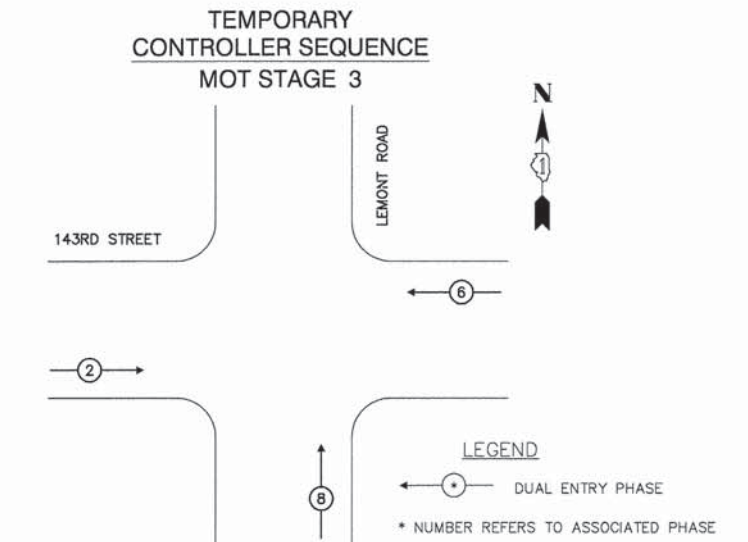
| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 62 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



MOT STAGE 3

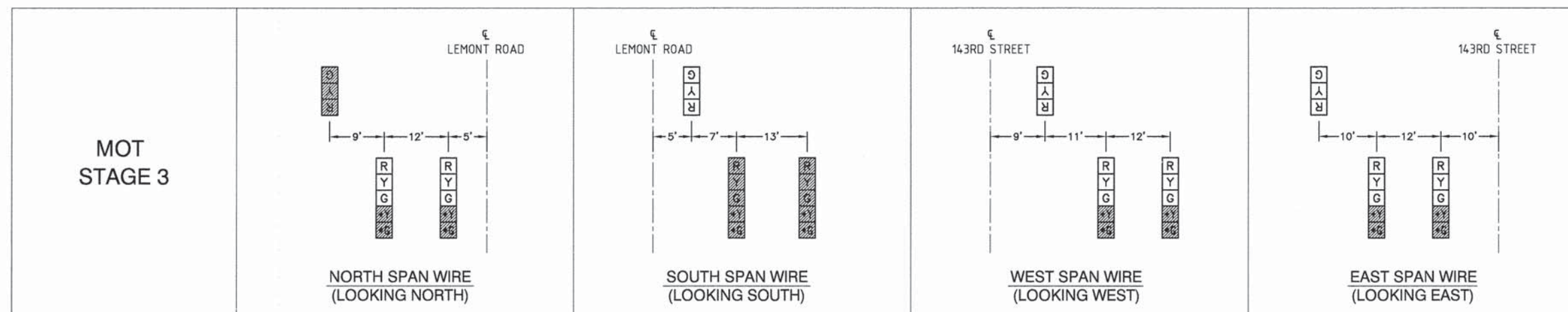


CABLE PLAN MOT STAGE 3
NOT TO SCALE



NOTES FOR TEMPORARY TRAFFIC SIGNALS

- ALL APPROACHES SHALL HAVE VEHICULAR DETECTION PROVIDED BY A VIDEO VEHICLE DETECTION SYSTEM. THE CONTRACTOR SHALL INSTALL WIRE AND ADJUST THE ALIGNMENT OF THE VIDEO VEHICLE DETECTION SYSTEM IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATIONS AND REQUIREMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE ALIGNMENT OF THE VIDEO VEHICLE DETECTION SYSTEM FOR ALL CONSTRUCTION STAGING CHANGES AND FOR MAINTAINING PROPER ALIGNMENT THROUGHOUT THE PROJECT. A REPRESENTATIVE OF THE APPROVED CONTROL EQUIPMENT VENDOR SHALL BE PRESENT AND ASSIST THE CONTRACTOR IN SETTING UP AND MAINTAINING THE VIDEO VEHICLE DETECTION SYSTEM.
- ALL TRAFFIC SIGNAL HEADS NOT BEING USED SHALL BE COVERED AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL ALSO BE REQUIRED TO RELOCATE SIGNAL HEADS DURING STAGING OPERATIONS TO THE LOCATIONS SHOWN BELOW.



FILE NAME = 04273_02-SGNL-02 - DTL03

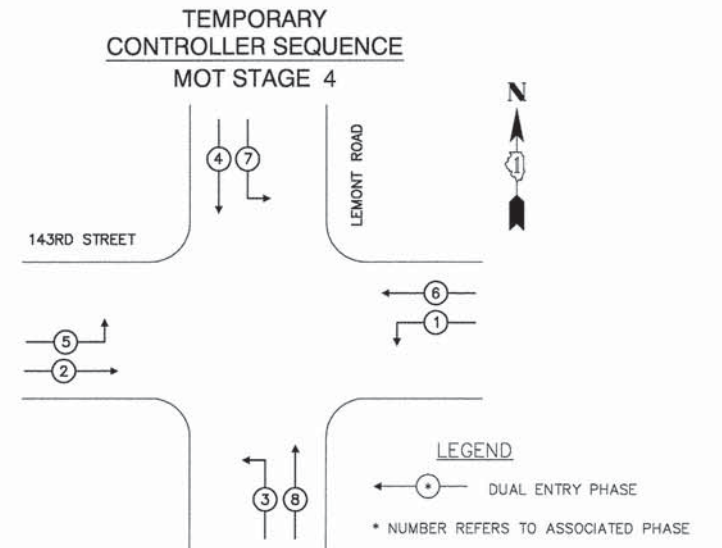
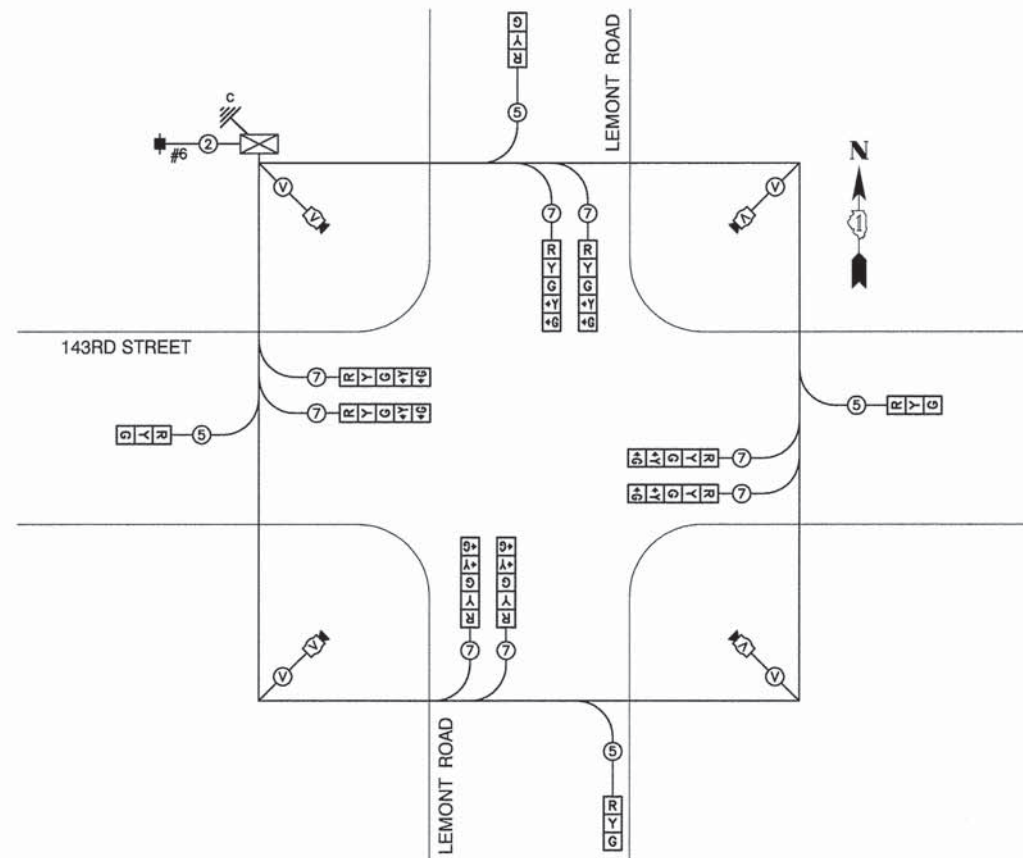
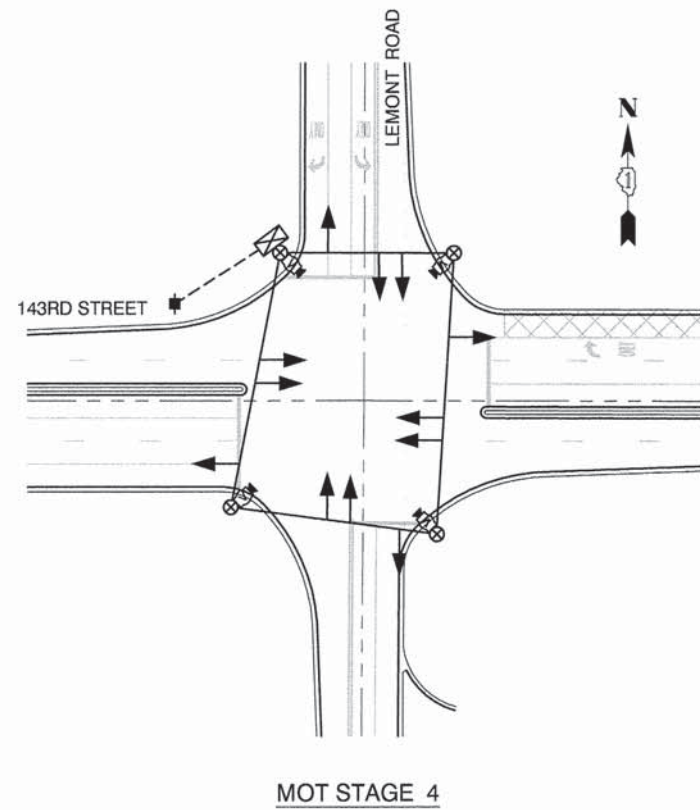
| | | |
|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- JRA | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- BDB | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
TEMPORARY TRAFFIC SIGNAL - MOT PLAN

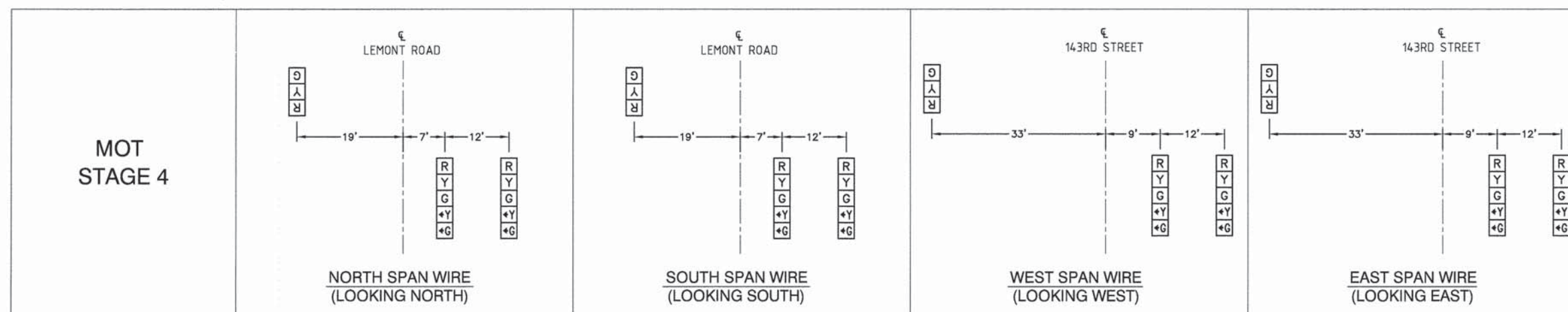
SCALE: NONE SHEET NO. 63 OF 96 SHEETS STA. TO STA.

| | | | | |
|-----------------------|----------------|------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 63 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT | M-8003 (562) | |



NOTES FOR TEMPORARY TRAFFIC SIGNALS

- ALL APPROACHES SHALL HAVE VEHICULAR DETECTION PROVIDED BY A VIDEO VEHICLE DETECTION SYSTEM. THE CONTRACTOR SHALL INSTALL WIRE AND ADJUST THE ALIGNMENT OF THE VIDEO VEHICLE DETECTION SYSTEM IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATIONS AND REQUIREMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE ALIGNMENT OF THE VIDEO VEHICLE DETECTION SYSTEM FOR ALL CONSTRUCTION STAGING CHANGES AND FOR MAINTAINING PROPER ALIGNMENT THROUGHOUT THE PROJECT. A REPRESENTATIVE OF THE APPROVED CONTROL EQUIPMENT VENDOR SHALL BE PRESENT AND ASSIST THE CONTRACTOR IN SETTING UP AND MAINTAINING THE VIDEO VEHICLE DETECTION SYSTEM.
- ALL TRAFFIC SIGNAL HEADS NOT BEING USED SHALL BE COVERED AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL ALSO BE REQUIRED TO RELOCATE SIGNAL HEADS DURING STAGING OPERATIONS TO THE LOCATIONS SHOWN BELOW.



FILE NAME = 04273_02-SGNL-02 - DTL04

| | | |
|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| PLOT SCALE = | CHECKED -- | REVISED -- |
| PLOT DATE = 05-30-13 | DRAWN -- JRA | REVISED -- |
| | CHECKED -- BDB | REVISED -- |

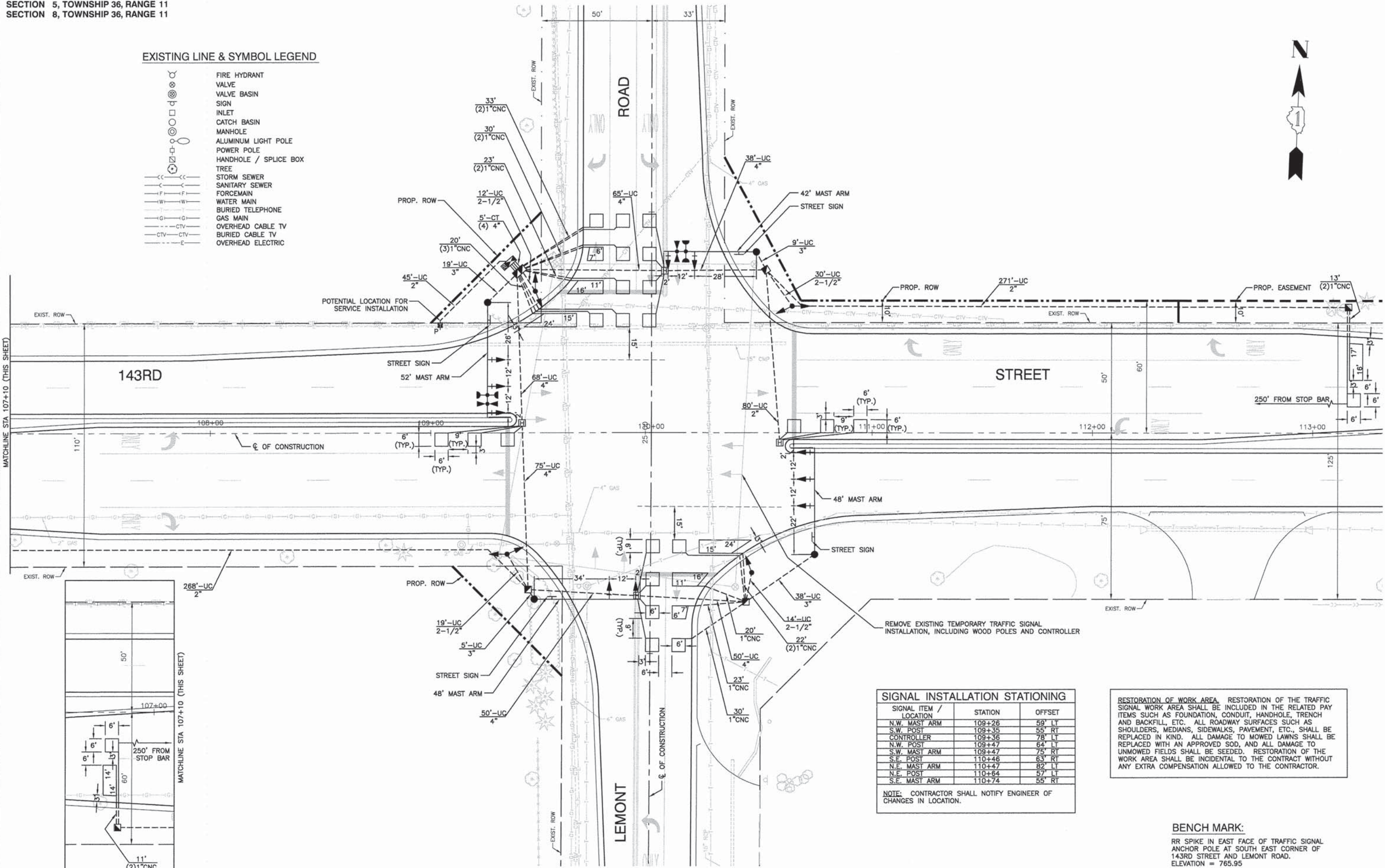
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | |
|--|---------------------------|
| 143RD STREET AND LEMONT ROAD INTERSECTION RECONSTRUCTION TEMPORARY TRAFFIC SIGNAL - MOT PLAN | |
| SCALE: NONE | SHEET NO. 64 OF 96 SHEETS |
| STA. TO STA. | |

| | | | | |
|--------------------------------|------------------------|-------------|-------------------------------|--------------|
| F.A.U. RTE. 1600 | SECTION 04-00003-00-CH | COUNTY WILL | TOTAL SHEETS 96 | SHEET NO. 64 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | FED. AID PROJECT M-8003 (562) | |

EXISTING LINE & SYMBOL LEGEND

- FIRE HYDRANT
- VALVE
- VALVE BASIN
- SIGN
- INLET
- CATCH BASIN
- MANHOLE
- ALUMINUM LIGHT POLE
- POWER POLE
- HANDHOLE / SPLICE BOX
- TREE
- STORM SEWER
- SANITARY SEWER
- FORCE MAIN
- WATER MAIN
- BURIED TELEPHONE
- GAS MAIN
- OVERHEAD CABLE TV
- BURIED CABLE TV
- OVERHEAD ELECTRIC



SIGNAL INSTALLATION STATIONING

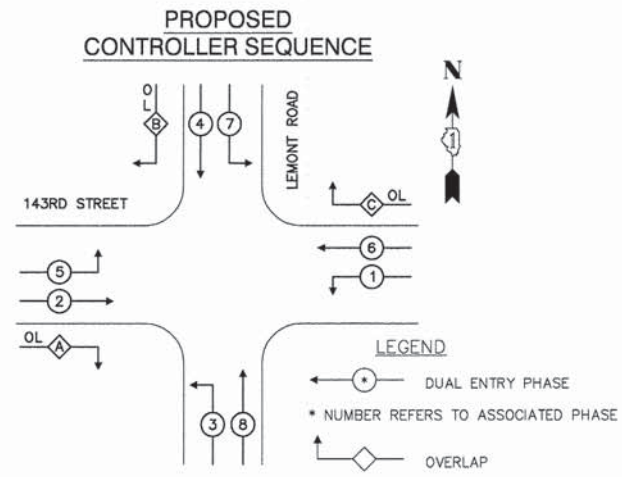
| SIGNAL ITEM / LOCATION | STATION | OFFSET |
|------------------------|---------|--------|
| N.W. MAST ARM | 109+26 | 59' LT |
| S.W. POST | 109+35 | 55' RT |
| CONTROLLER | 109+36 | 78' LT |
| N.W. POST | 109+47 | 64' LT |
| S.W. MAST ARM | 109+47 | 75' RT |
| S.E. POST | 110+46 | 63' RT |
| N.E. MAST ARM | 110+47 | 82' LT |
| N.E. POST | 110+64 | 57' LT |
| S.E. MAST ARM | 110+74 | 55' RT |

NOTE: CONTRACTOR SHALL NOTIFY ENGINEER OF CHANGES IN LOCATION.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEMS SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC., SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED. RESTORATION OF THE WORK AREA SHALL BE INCIDENTAL TO THE CONTRACT WITHOUT ANY EXTRA COMPENSATION ALLOWED TO THE CONTRACTOR.

BENCH MARK:
RR SPIKE IN EAST FACE OF TRAFFIC SIGNAL ANCHOR POLE AT SOUTH EAST CORNER OF 143RD STREET AND LEMONT ROAD.
ELEVATION = 765.95

| | | |
|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| PLOT SCALE = | CHECKED -- RG | REVISED -- 05-01-13 - IDOT REVIEW |
| PLOT DATE = 05-30-13 | DRAWN -- AG | REVISED -- |
| | CHECKED -- AG | REVISED -- |

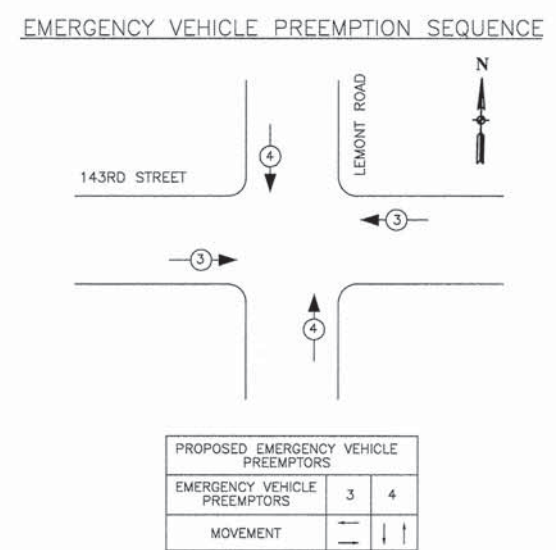
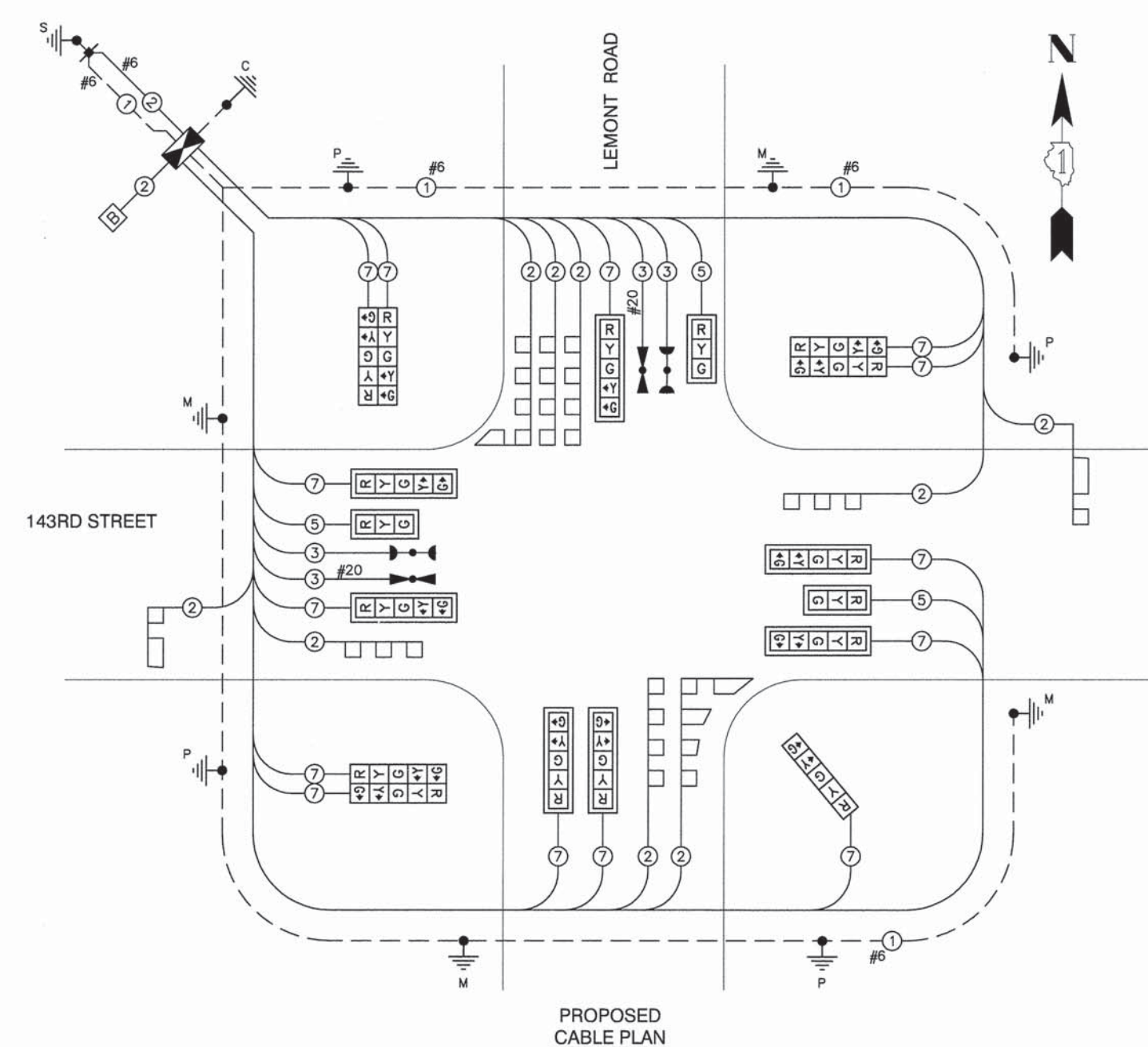


PHASE DESIGNATION DIAGRAM

| OVERLAP LETTER | PERMISSIVE PHASE | PROTECTED PHASE |
|----------------|------------------|-----------------|
| A | = 2 | + 3 |
| B | = 4 | + 5 |
| C | = 6 | + 7 |

SCHEDULE OF QUANTITIES

| ITEM | UNIT | QUAN |
|--|-------|------|
| SIGN PANEL - TYPE 1 | SQ FT | 47 |
| SERVICE INSTALLATION - POLE MOUNTED | EACH | 1 |
| UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA. | FOOT | 664 |
| UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA. | FOOT | 75 |
| UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA. | FOOT | 71 |
| UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA. | FOOT | 366 |
| HANDHOLE | EACH | 5 |
| HEAVY-DUTY HANDHOLE | EACH | 4 |
| DOUBLE HANDHOLE | EACH | 1 |
| FULL-ACTUATED CONTROLLER AND TYPE IV CABINET | EACH | 1 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C | FOOT | 314 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | FOOT | 689 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | FOOT | 3033 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 20 3C | FOOT | 314 |
| ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR | FOOT | 1806 |
| ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C | FOOT | 113 |
| ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C | FOOT | 543 |
| TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT. | EACH | 4 |
| STEEL MAST ARM ASSEMBLY AND POLE, 42 FT. | EACH | 1 |
| STEEL MAST ARM ASSEMBLY AND POLE, 48 FT. | EACH | 2 |
| STEEL MAST ARM ASSEMBLY AND POLE, 52 FT. | EACH | 1 |
| CONCRETE FOUNDATION, TYPE A | FOOT | 16 |
| CONCRETE FOUNDATION, TYPE C | FOOT | 4 |
| CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER | FOOT | 60 |
| SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED | EACH | 3 |
| SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 1 |
| SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED | EACH | 7 |
| SIGNAL HEAD, LED, 2-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 3 |
| TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM | EACH | 10 |
| INDUCTIVE LOOP DETECTOR | EACH | 9 |
| DETECTOR LOOP, TYPE 1 | FOOT | 897 |
| LIGHT DETECTOR | EACH | 2 |
| LIGHT DETECTOR AMPLIFIER | EACH | 1 |
| REMOVE EXISTING HANDHOLE | EACH | 9 |
| REMOVE TEMPORARY TRAFFIC SIGNAL INSTALLATION | EACH | 1 |
| TEMPORARY TRAFFIC SIGNAL TIMING | EACH | 1 |
| UNINTERRUPTIBLE POWER SUPPLY, SPECIAL | EACH | 1 |

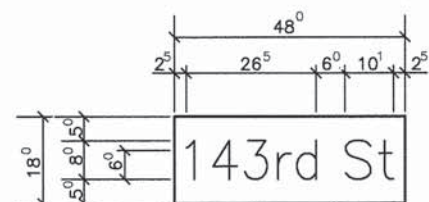


- ### NOTES:
- THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE ECONOLITE AS DESCRIBED IN THE SPECIAL PROVISIONS.
 - ALL INDICATIONS SHALL BE LED.
 - STOP BARS ON 143RD STREET SHALL NOT BE INSTALLED UNTIL TRAFFIC SIGNAL IS OPERATIONAL.
 - THE GROUNDING SYSTEM SHALL INCLUDE GROUND RODS AND CONNECTION IN HANDHOLES. ALL GROUND ROD CONNECTIONS SHALL BE AN IRREVERSIBLE COMPRESSION GROUND TAP INSTALLED WITH A HYDRAULIC 12 TON PRESS TOOL OR EQUAL.
 - ALL NEW GROUND RODS SHALL BE 3/4" X 10' LONG COPPER CLAD. THE COST SHALL BE INCIDENTAL TO THE COST OF INSTALLATION.
 - EQUIPMENT GROUND CONDUCTOR (GREEN COLOR CODED) SPLICE TO FRAME AND COVER IS REQUIRED FOR ALL HANDHOLES OR DOUBLE HANDHOLES THAT CARRY SIGNAL CABLES AND SERVICE CABLES.

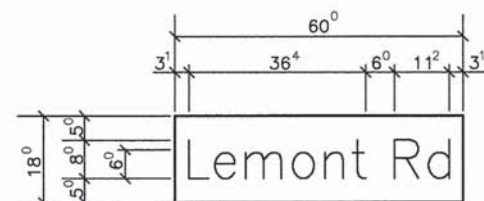
RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEMS SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC., SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEED. RESTORATION OF THE WORK AREA SHALL BE INCIDENTAL TO THE CONTRACT WITHOUT ANY EXTRA COMPENSATION ALLOWED TO THE CONTRACTOR.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS

| TYPE | NO. LAMPS | WATTAGE INCAND. | LED | % OPERATION | TOTAL WATTAGE |
|---|-----------|-----------------|-----|-------------|---------------|
| SIGNAL (RED) | 17 | 135 | 17 | 0.50 | 145 |
| SIGNAL (YELLOW) | 17 | 135 | 25 | 0.25 | 108 |
| SIGNAL (GREEN) | 17 | 135 | 15 | 0.25 | 84 |
| ARROW | 28 | 135 | 12 | 0.10 | 34 |
| PED. SIGNAL | - | 90 | 25 | 1.00 | - |
| CONTROLLER | 1 | 100 | 100 | 1.00 | 100 |
| ILLUM. SIGN | - | 84 | - | 0.05 | - |
| LIGHTS | - | 250 | - | 0.25 | - |
| FLASHER | - | - | - | 0.50 | - |
| ENERGY COSTS TO: | | | | | TOTAL = 449 |
| VILLAGE OF HOMER GLEN | | | | | |
| 14933 S. FOUNDERS CROSSING | | | | | |
| HOMER GLEN, IL 60491 | | | | | |
| ENERGY SUPPLY CONTACT: COM-ED BUSINESS DEPARTMENT | | | | | |
| PHONE: 866-639-3532 | | | | | |
| COMPANY: COMMONWEALTH EDISON COMPANY | | | | | |



6 SQ FT EA
2 REQ'D
SERIES "D"



7.5 SQ FT EA
2 REQ'D
SERIES "D"

GENERAL NOTES

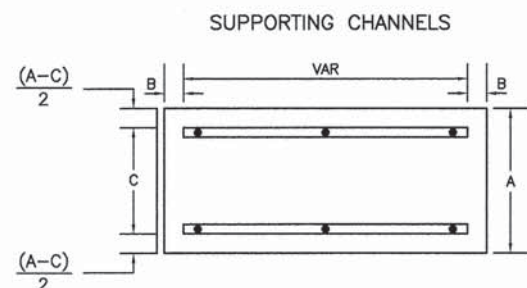
- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADING CALLED FOR, AS APPLICABLE, PLUS A 2'-6" BY 6'-0" SIGN PANEL MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINARIES AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 MPH WIND VELOCITY.
- ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND.
- THE SIGN LENGTH SHOULD BE INCREASED IN 6 INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 6'-0".
- ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADII SHALL BE 2-1/4".
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS, LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:

- A.K.T. CORPORATION - AMERICAN FABRICATION CO.
SCHAUMBURG, IL CHICAGO HEIGHTS, IL
- TUCKER COMPANY, INC. - WESTERN TRAFFIC CONTROL INC.
WAUWATOSE, WI CICERO, IL

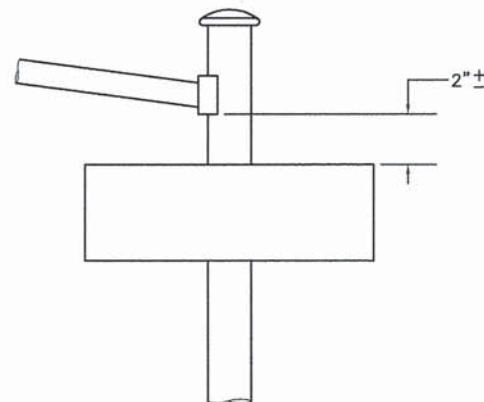
PARTS LISTINGS:

- SIGN CHANNEL PART #HPN053 (MED. CHANNEL)
- SIGN SCREWS 1/4" X 14 X 1" H.W.H. #3
SELF TAPPING WITH NEOPRENE WASHER
- BRACKETS PART #HPN034 (UNIVERSAL)
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

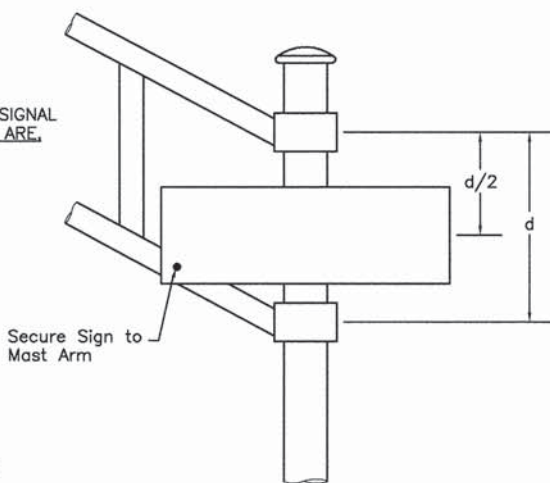
OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BRACKET OF THE ABOVE PRODUCT.



| A | B | C |
|-----|----|-----|
| 18" | 2" | 12" |
| 30" | 2" | 22" |



SINGLE ARM



DUAL ARM

| SERIES | SECOND LETTER | | | | | | | | | | | | | | | | | |
|---------|---------------|----|----|----|-----------|----|----|----|-----|----|----|----|-----|----|-----|----|-----|---|
| | a c d e | | | | b h i k l | | | | f w | | j | | s t | | v y | | x z | |
| | C | D | C | D | C | D | C | D | C | D | C | D | C | D | C | D | C | D |
| A W X | 12 | 14 | 14 | 15 | 12 | 14 | 06 | 10 | 11 | 14 | 06 | 10 | 11 | 12 | 12 | 14 | | |
| B | 14 | 15 | 20 | 21 | 14 | 15 | 11 | 12 | 14 | 15 | 12 | 14 | 12 | 14 | 16 | 17 | | |
| C E G | 14 | 15 | 20 | 21 | 12 | 14 | 06 | 10 | 12 | 14 | 12 | 14 | 14 | 15 | 14 | 15 | | |
| D O Q R | 14 | 15 | 20 | 21 | 14 | 15 | 06 | 10 | 12 | 14 | 12 | 14 | 14 | 15 | 14 | 15 | | |
| F | 05 | 06 | 14 | 15 | 06 | 10 | 05 | 06 | 06 | 10 | 06 | 10 | 06 | 10 | 11 | 12 | | |
| H I M N | 20 | 21 | 22 | 24 | 20 | 21 | 14 | 15 | 16 | 17 | 16 | 17 | 20 | 21 | 20 | 21 | | |
| J U | 20 | 21 | 20 | 21 | 16 | 17 | 14 | 15 | 16 | 17 | 16 | 17 | 16 | 17 | 20 | 21 | | |
| K L | 11 | 12 | 16 | 17 | 11 | 12 | 05 | 06 | 11 | 12 | 11 | 12 | 11 | 12 | 12 | 14 | | |
| P | 12 | 14 | 14 | 15 | 12 | 14 | 05 | 06 | 11 | 12 | 11 | 12 | 12 | 14 | 12 | 14 | | |
| S | 12 | 14 | 16 | 17 | 12 | 14 | 06 | 10 | 12 | 14 | 12 | 14 | 12 | 14 | 12 | 14 | | |
| T | 11 | 12 | 16 | 17 | 06 | 10 | 06 | 10 | 11 | 12 | 11 | 12 | 11 | 12 | 12 | 14 | | |
| V | 06 | 10 | 14 | 15 | 11 | 12 | 06 | 10 | 12 | 14 | 12 | 14 | 12 | 14 | 12 | 14 | | |
| Y | 05 | 06 | 14 | 15 | 06 | 10 | 05 | 06 | 05 | 07 | 05 | 06 | 06 | 10 | 11 | 12 | | |
| Z | 16 | 17 | 22 | 24 | 16 | 17 | 12 | 14 | 16 | 17 | 16 | 17 | 16 | 17 | 20 | 21 | | |

Lower Case To Lower Case
Spacing Chart 6 inch Series C & D

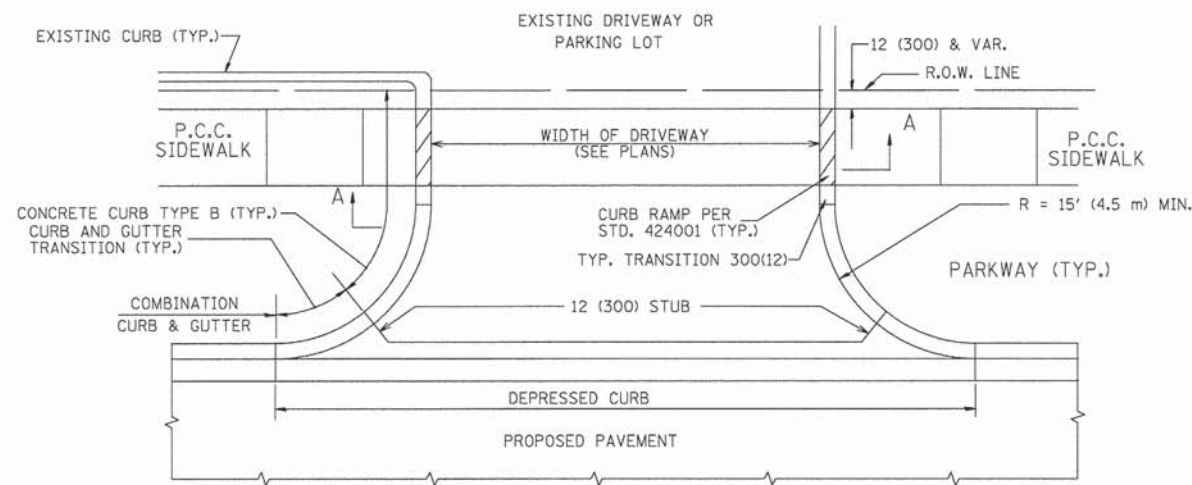
| SERIES | SECOND LETTER | | | | | | | | | | | | | | | | | |
|-------------|---------------|----|----|----|-------------|----|----|----|-----|----|----|----|-----|----|-----|----|-----|---|
| | a d h g i j | | | | b f k o p s | | | | c e | | r | | t z | | v y | | w x | |
| | C | D | C | D | C | D | C | D | C | D | C | D | C | D | C | D | C | D |
| a d h g i j | 16 | 17 | 22 | 24 | 16 | 17 | 12 | 14 | 14 | 15 | 14 | 15 | 16 | 17 | 16 | 17 | | |
| b f k o p s | 12 | 14 | 16 | 17 | 11 | 12 | 05 | 06 | 11 | 12 | 11 | 12 | 12 | 14 | 12 | 14 | | |
| c e | 12 | 14 | 16 | 17 | 12 | 14 | 06 | 10 | 12 | 14 | 12 | 14 | 12 | 14 | 12 | 14 | | |
| r | 06 | 10 | 12 | 14 | 06 | 10 | 03 | 03 | 05 | 06 | 05 | 06 | 06 | 10 | 06 | 10 | | |
| t z | 12 | 14 | 16 | 17 | 12 | 14 | 06 | 10 | 11 | 12 | 11 | 12 | 12 | 14 | 12 | 14 | | |
| v y | 11 | 12 | 14 | 15 | 11 | 12 | 05 | 06 | 06 | 10 | 06 | 10 | 11 | 12 | 11 | 12 | | |
| w x | 11 | 12 | 14 | 15 | 11 | 12 | 05 | 06 | 11 | 12 | 11 | 12 | 11 | 12 | 12 | 14 | | |
| | 12 | 14 | 16 | 17 | 11 | 12 | 05 | 06 | 11 | 12 | 11 | 12 | 11 | 12 | 12 | 14 | | |

Number To Number
Spacing Chart 8 Inch Series "C & D"

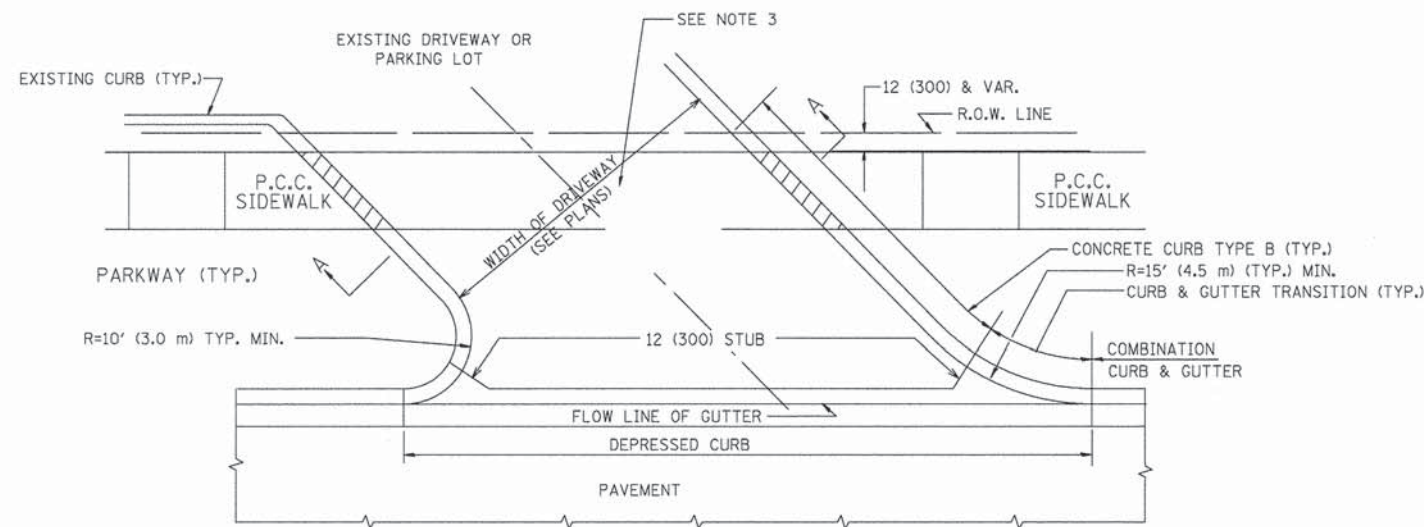
| SERIES | SECOND LETTER | | | | | | | | | | | | | | | | | | | |
|--------|---------------|----|----|----|----|----|----|----|----|----|----|----|----|----|----|----|----|----|----|----|
| | 0 | | 1 | | 2 | | 3 | | 4 | | 5 | | 6 | | 7 | | 8 | | 9 | |
| | C | D | C | D | C | D | C | D | C | D | C | D | C | D | C | D | C | D | C | D |
| 0 9 | 16 | 17 | 16 | 17 | 14 | 15 | 12 | 14 | 14 | 15 | 14 | 15 | 16 | 17 | 12 | 14 | 16 | 17 | 16 | 17 |
| 1 | 20 | 21 | 20 | 21 | 20 | 21 | 16 | 17 | 14 | 15 | 20 | 21 | 20 | 21 | 14 | 15 | 20 | 21 | 20 | 21 |
| 2 3 4 | 14 | 15 | 14 | 15 | 14 | 15 | 12 | 14 | 12 | 14 | 14 | 15 | 14 | 15 | 11 | 12 | 16 | 17 | 14 | 15 |
| 5 | 14 | 15 | 14 | 15 | 14 | 15 | 11 | 12 | 11 | 12 | 14 | 15 | 14 | 15 | 11 | 12 | 14 | 15 | 14 | 15 |
| 6 | 16 | 17 | 14 | 15 | 14 | 15 | 12 | 14 | 14 | 15 | 14 | 15 | 11 | 12 | 14 | 15 | 14 | 15 | 14 | 15 |
| 7 | 12 | 14 | 12 | 14 | 14 | 15 | 12 | 15 | 05 | 06 | 12 | 14 | 14 | 15 | 11 | 12 | 14 | 15 | 12 | 14 |
| 8 | 16 | 17 | 16 | 17 | 14 | 15 | 12 | 15 | 12 | 14 | 14 | 15 | 16 | 17 | 12 | 14 | 16 | 17 | 14 | 15 |

| LETTERS | UPPER AND LOWER CASE LETTER WIDTHS | | | | | | |
|---------|------------------------------------|----------------|---------------------------|----------------|---------|---------------------------|----------------|
| | 6 INCH UPPER CASE LETTERS | | 8 INCH UPPER CASE LETTERS | | LETTERS | 6 INCH LOWER CASE LETTERS | |
| | SERIES | | SERIES | | | SERIES | |
| | C | D | C | D | | C | D |
| A | 3 ⁶ | 5 ⁰ | 5 ⁰ | 6 ⁵ | a | 3 ⁵ | 4 ² |
| B | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | b | 3 ⁵ | 4 ² |
| C | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | c | 3 ⁵ | 4 ¹ |
| D | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | d | 3 ⁵ | 4 ² |
| E | 3 ⁰ | 3 ⁵ | 4 ⁰ | 4 ⁷ | e | 3 ⁵ | 4 ² |
| F | 3 ⁰ | 3 ⁵ | 4 ⁰ | 4 ⁷ | f | 2 ³ | 2 ⁶ |
| G | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | g | 3 ⁵ | 4 ² |
| H | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | h | 3 ⁵ | 4 ² |
| I | 0 ⁷ | 0 ⁷ | 1 ¹ | 1 ² | i | 1 ¹ | 1 ¹ |
| J | 3 ⁰ | 3 ⁶ | 4 ⁰ | 5 ⁰ | j | 2 ⁰ | 2 ² |
| K | 3 ² | 4 ¹ | 4 ³ | 5 ⁴ | k | 3 ⁵ | 4 ² |
| L | 3 ⁰ | 3 ⁵ | 4 ⁰ | 4 ⁷ | l | 1 ¹ | 1 ¹ |
| M | 3 ⁷ | 4 ⁵ | 5 ¹ | 6 ¹ | m | 6 ⁰ | 7 ⁰ |
| N | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | n | 3 ⁵ | 4 ² |
| O | 3 ⁴ | 4 ² | 4 ⁵ | 5 ⁵ | o | 3 ⁶ | 4 ³ |
| P | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | p | 3 ⁵ | 4 ² |
| Q | 3 ⁴ | 4 ² | 4 ⁵ | 5 ⁵ | q | 3 ⁵ | 4 ² |
| R | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | r | 2 ⁶ | 3 ² |
| S | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | s | 3 ⁶ | 4 ² |
| T | 3 ⁰ | 3 ⁵ | 4 ⁰ | 4 ⁷ | t | 2 ⁷ | 3 ² |
| U | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | u | 3 ⁵ | 4 ² |
| V | 3 ⁵ | 4 ⁴ | 4 ⁷ | 6 ⁰ | v | 4 ² | 4 ⁷ |
| W | 4 ⁴ | 5 ² | 6 ⁰ | 7 ⁰ | w | 5 ⁵ | 6 ⁴ |
| X | 3 ⁴ | 4 ⁰ | 4 ⁵ | 5 ³ | x | 4 ⁴ | 5 ¹ |
| Y | 3 ⁶ | 5 ⁰ | 5 ⁰ | 6 ⁶ | y | 4 ⁶ | 5 ³ |
| Z | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | z | 3 ⁶ | 4 ³ |

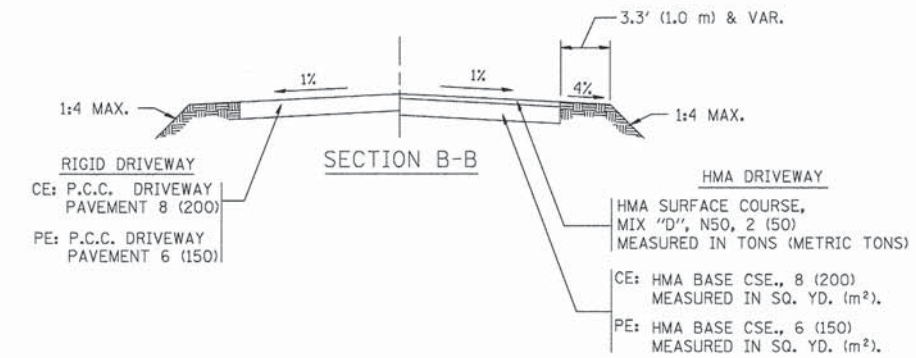
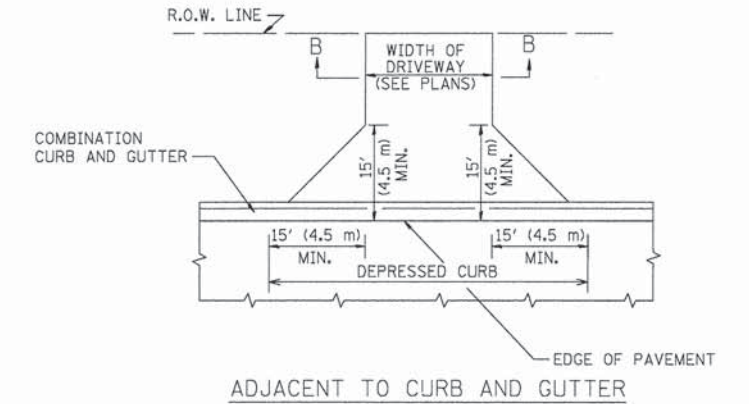
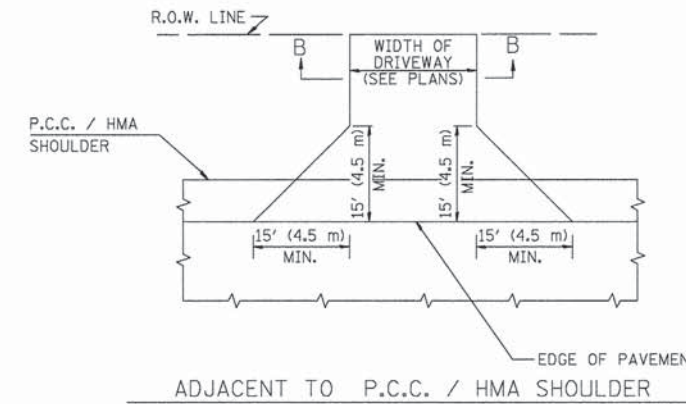
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|--------|----------------|----------------|----------------|----------------|
| | C | D | C | D |
| 1 | 1 ² | 1 ⁴ | 1 ⁵ | 2 ⁰ |
| 2 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 3 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 4 | 3 ⁵ | 4 ³ | 4 ⁷ | 5 ⁷ |
| 5 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 6 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 7 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 8 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 9 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 0 | 3 ⁴ | 4 ² | 4 ⁵ | 5 ⁵ |



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

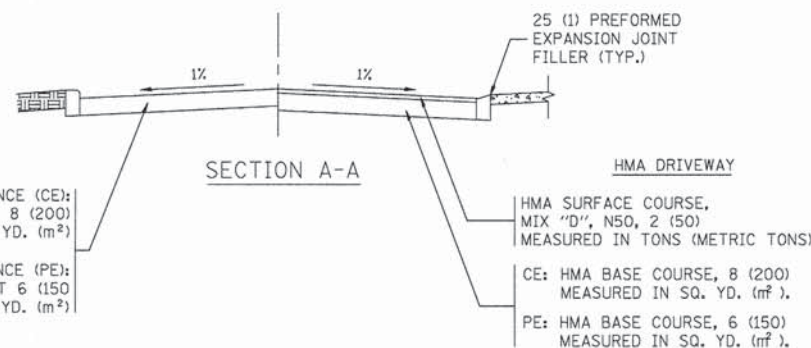
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



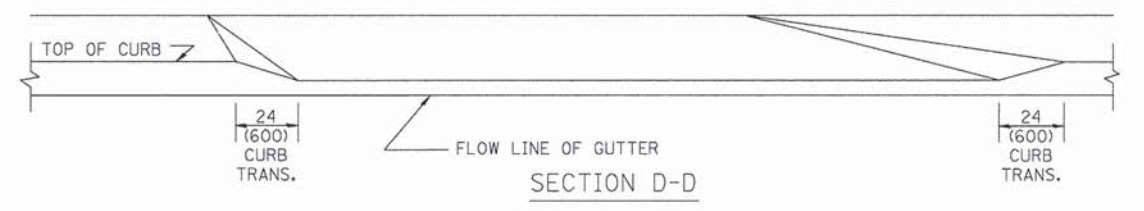
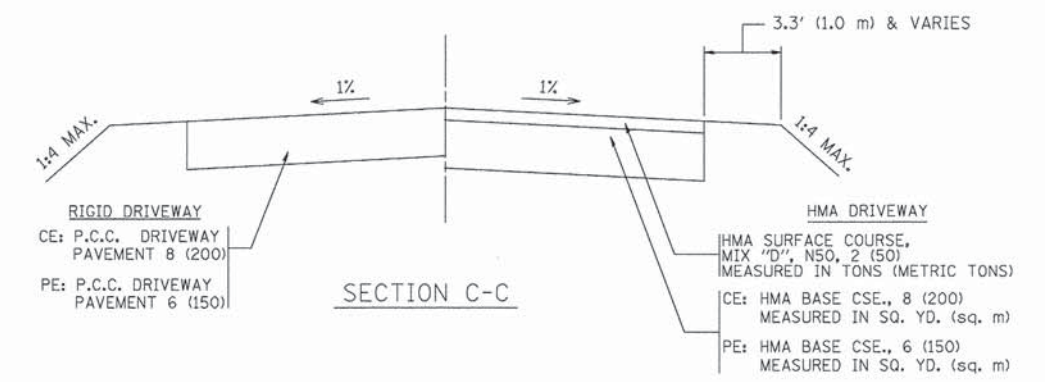
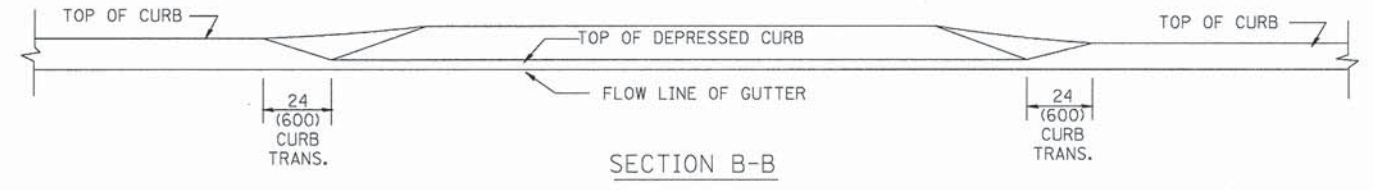
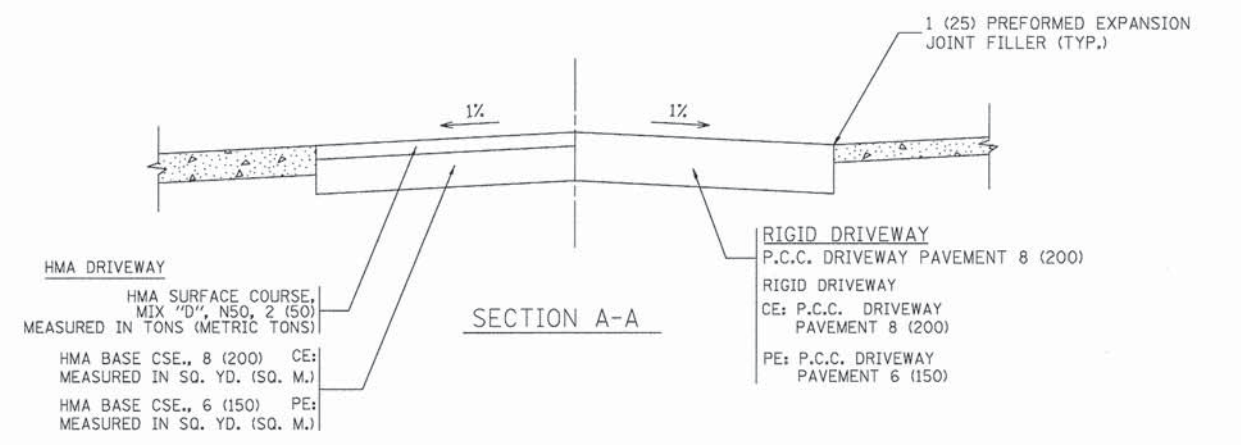
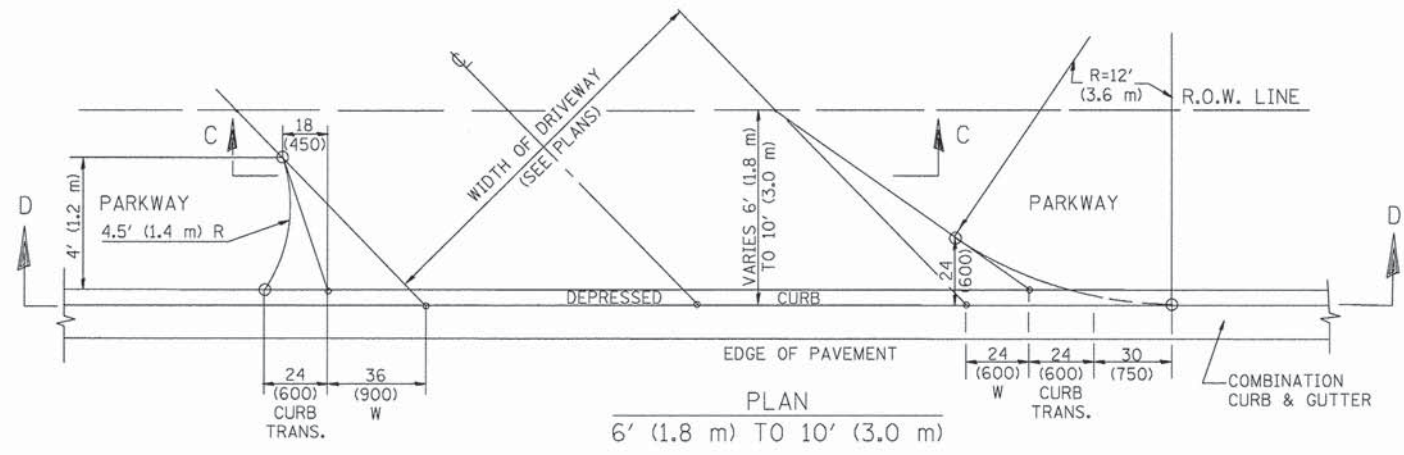
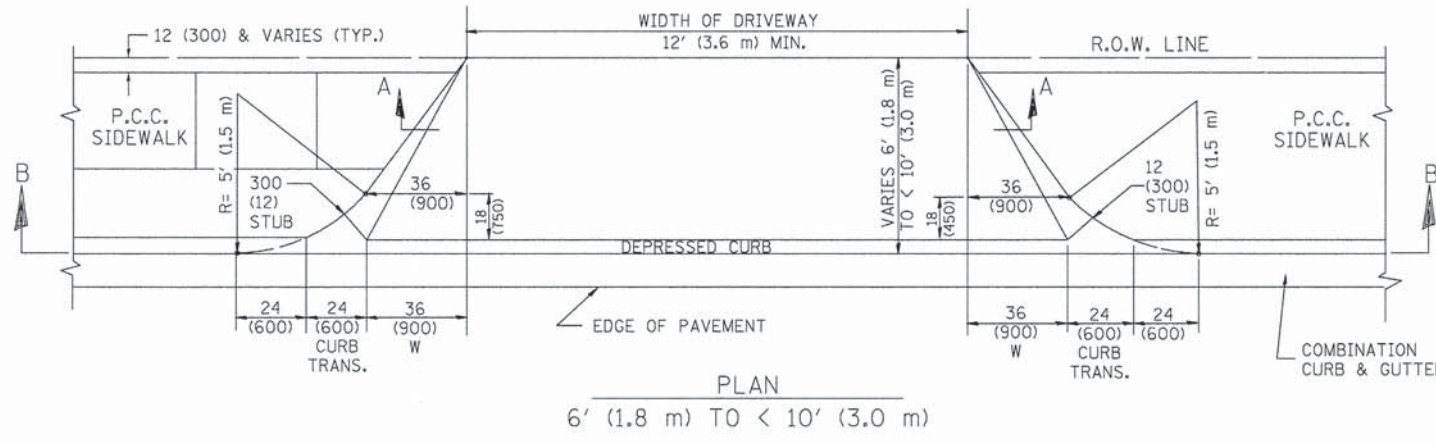
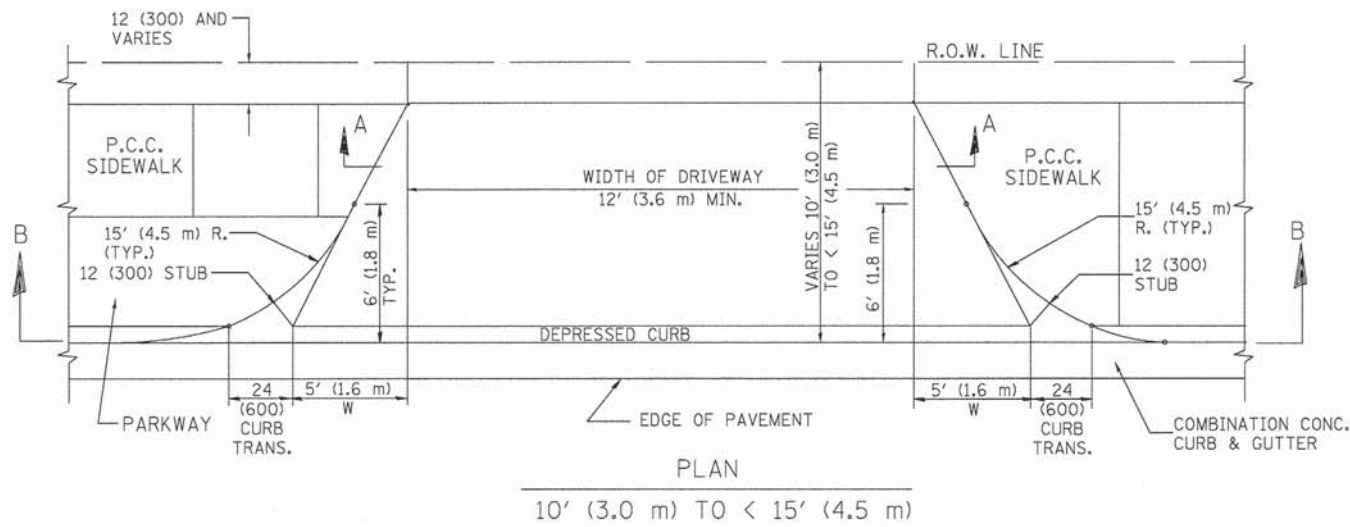
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| | PLOT SCALE = 50.0000' / 1" = | CHECKED - | REVISED - R. BORO 06-11-08 |
| | PLOT DATE = 9/6/2011 | DATE - 11-04-95 | REVISED - R. BORO 09-06-11 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.
AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|--|----------------|--------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 68 |
| BD0156-07 (BD-01) | | CONTRACT NO. 63147 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

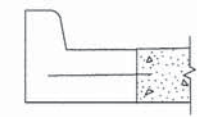
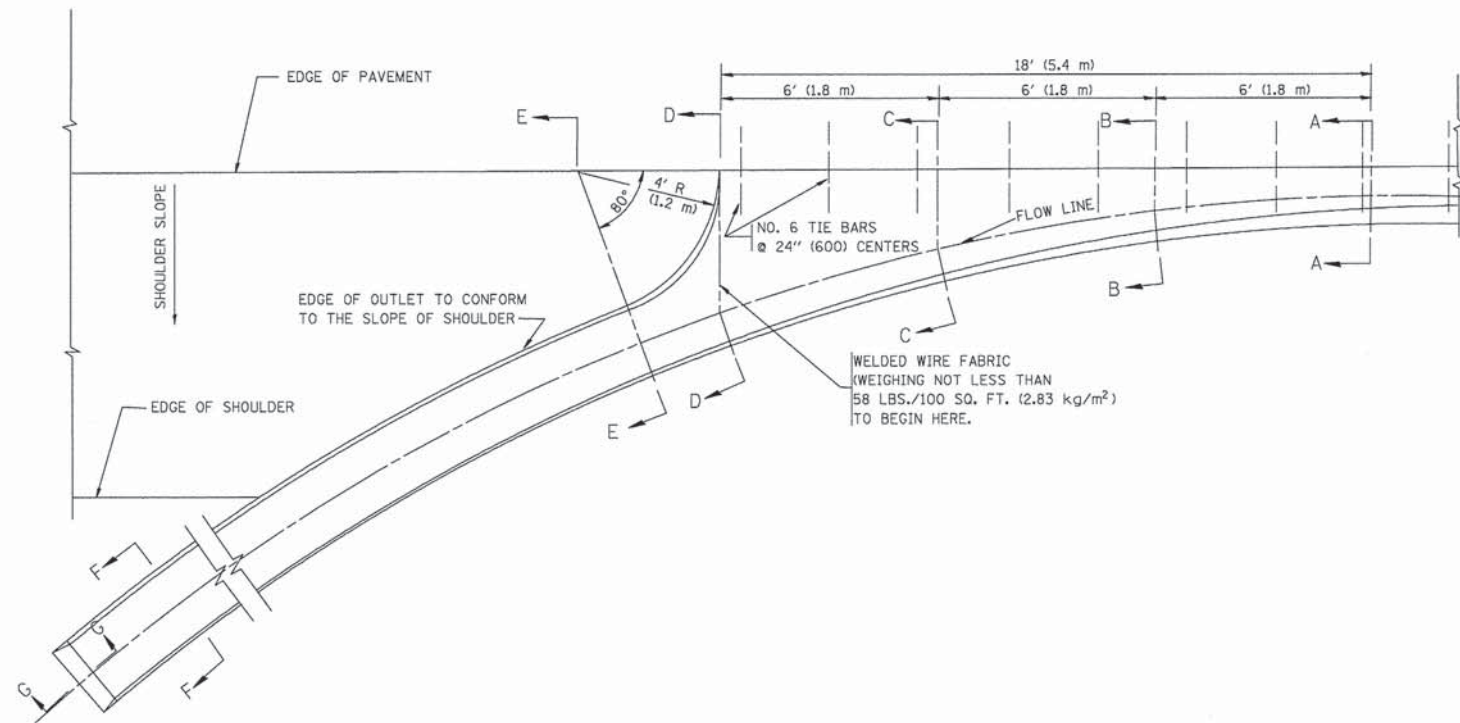
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

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| ct:\pv_work\pdxdot\lryse\d0108315\bd02.dgn | | DRAWN - | REVISED - P. LOFLEUR 04-15-03 |
| | PLOT SCALE = 58.0000' / 1" | CHECKED - | REVISED - R. BORO 01-01-07 |
| | PLOT DATE = 10/28/2011 | DATE - 11-06-95 | REVISED - R. BORO 09-06-11 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

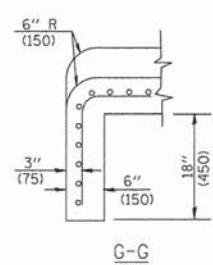
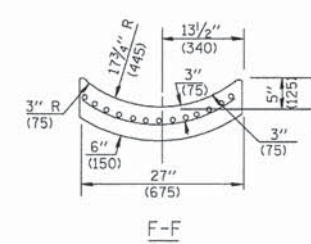
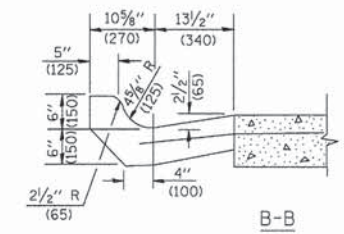
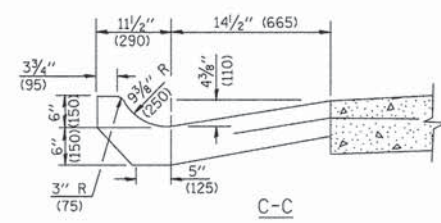
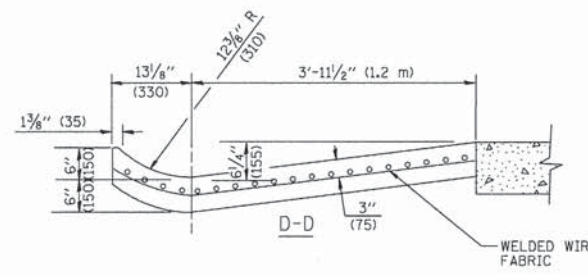
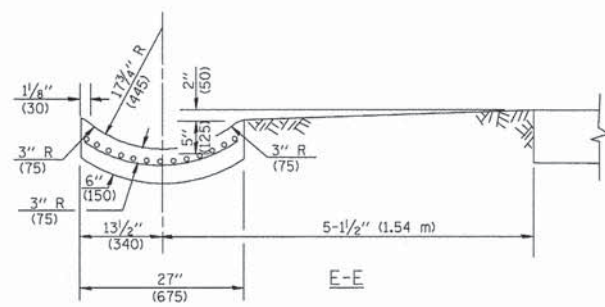
| DRIVEWAY DETAILS | | | |
|---|-------------------------|------|---------|
| DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m) | | | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|----------------|--------|--------------------|-----------|
| 1600 | 04-00003-00-CH | WILL | 96 | 69 |
| BD400-02 (BD-02) | | | CONTRACT NO. 63147 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



A-A *

* DIMENSIONS OF THE CURB & GUTTER AT SECTION A-A ARE SHOWN ON STATE STANDARD 606001. FOR DETAILS OF OUTLET FOR CONCRETE CURB & GUTTER, TYPE B-6.24 (B-15.60) SEE STATE STANDARD 606006.



GENERAL NOTES

GUTTER OUTLET SHALL BE TIED TO THE PAVEMENT IN ACCORDANCE WITH DETAILS FOR LONGITUDINAL CONSTRUCTION JOINT SHOWN ON STANDARD 420001.

TIE BARS SHALL BE NO. 20 (NO.6) AT 24\" (600) CENTERS UNLESS OTHERWISE SHOWN.

IF THE AVERAGE GRADE OF PAVEMENT FOR THE DISTANCE FROM SECTION A-A TO D-D EXCEEDS 2%, THIS DISTANCE SHALL BE INCREASED 6\" (1.8 m) FOR EACH 1% INCREASE IN GRADE.

QUANTITIES

FOR SECTION A-A TO E-E AND CURTAIN WALL =
 1.25 CU. YDS. (0.96 m³) CLASS SI CONCRETE (OUTLET) FOR 9\" (225) PAV'T.
 1.27 CU. YDS. (0.96 m³) CLASS SI CONCRETE (OUTLET) FOR 10\" (250) PAV'T.
 FOR SECTION F-F =
 0.045 CU. YDS. (0.03 m³) CLASS SI CONCRETE PER FT. (m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\datastd\22x34\bd03.dgn

USER NAME = geglianobt

DESIGNED - M. DE YONG
 DRAWN -

REVISED - R. SHAH 09-09-94
 REVISED - R. SHAH 10-25-94

PLOT SCALE = 50.0000' / IN.

CHECKED -

REVISED - E. GOMEZ 12-21-00

PLOT DATE = 1/4/2008

DATE = 08-04-86

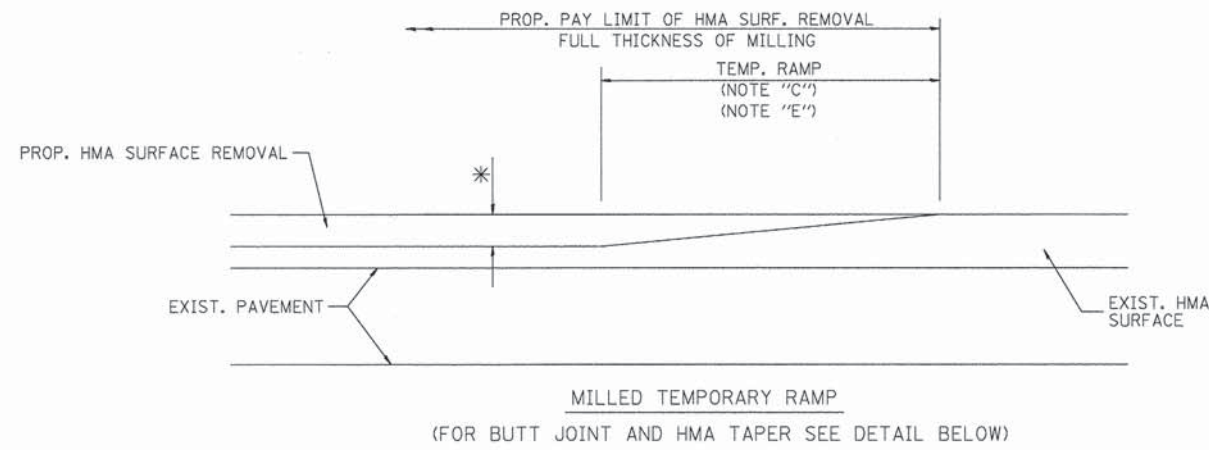
REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

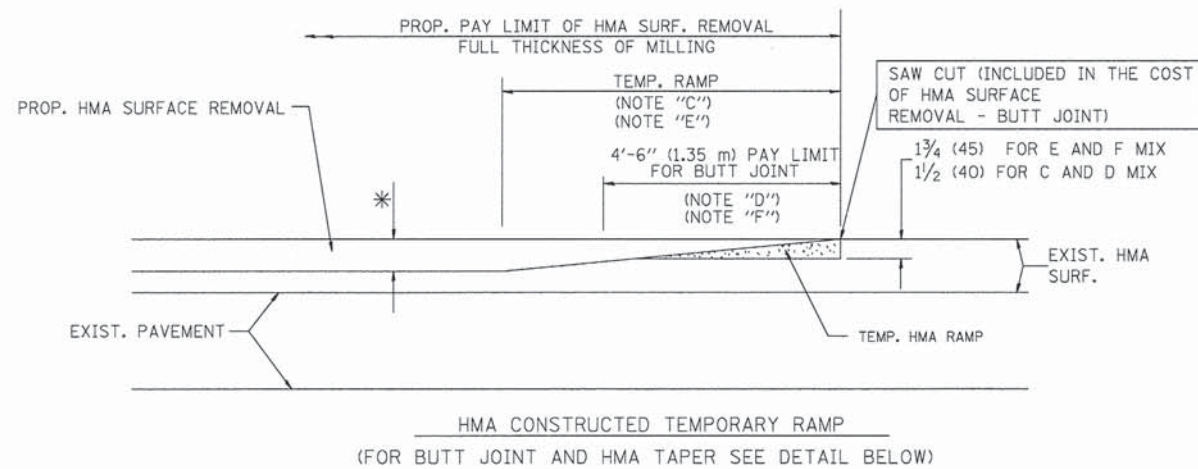
**OUTLET FOR CONCRETE
 CURB AND GUTTER**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|----------------|--------------------|--------------|-----------|
| 1600 | 04-00003-00-CH | WILL | 96 | 70 |
| BD600-01 (BD-03) | | CONTRACT NO. 63147 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |

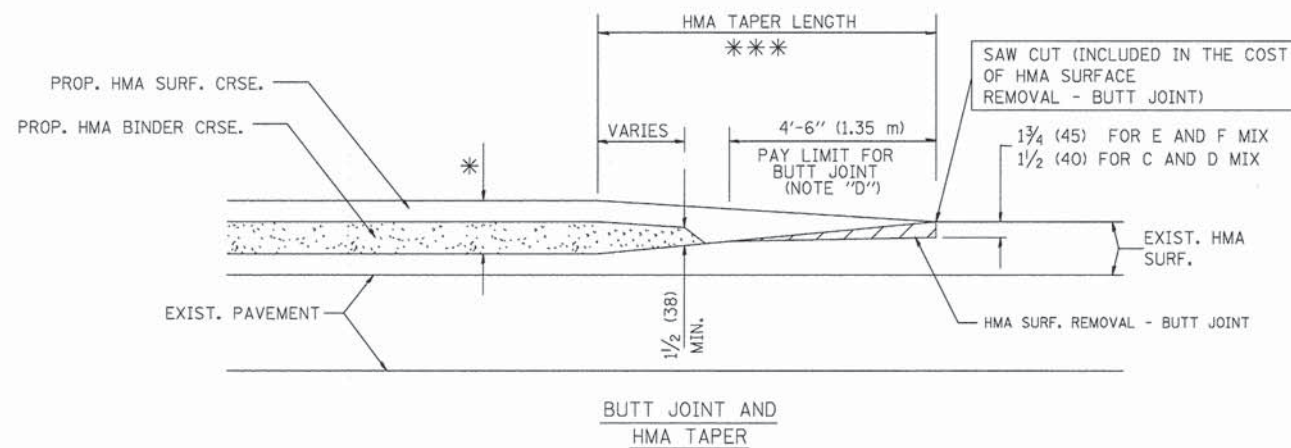


OPTION 1

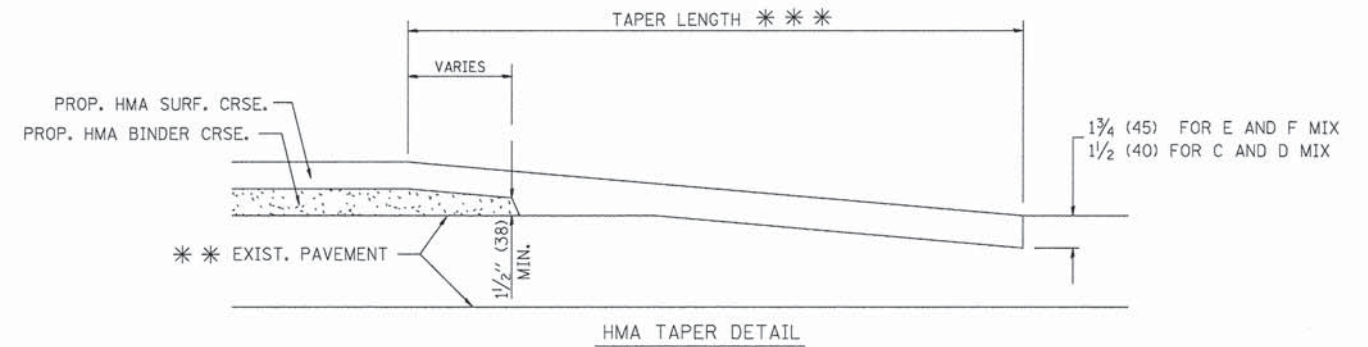
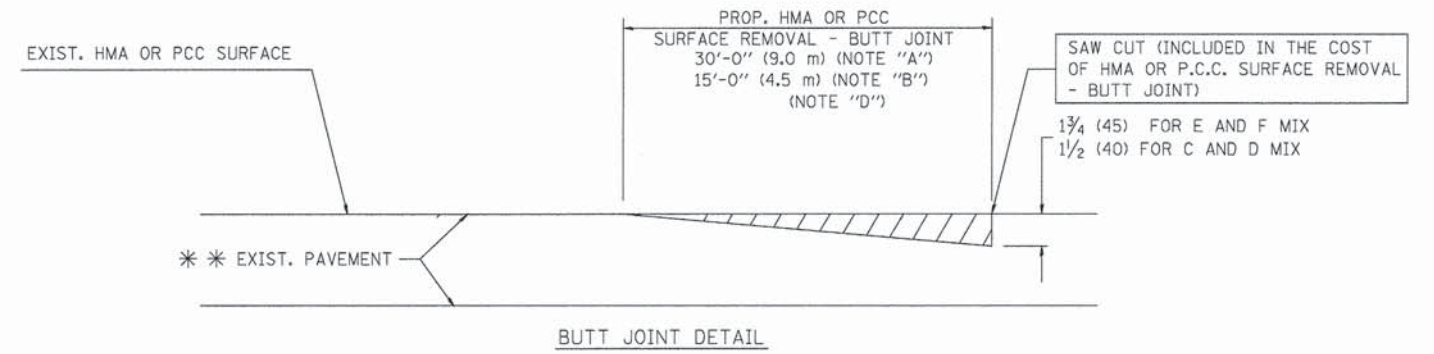


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\distatd\22x34\bd32.dgn

USER NAME = goglianobt

DESIGNED - M. DE YONG

REVISED - R. SHAH 10-25-94

PLOT SCALE = 50.0000' / IN.

DRAWN -

REVISED - A. ABBAS 03-21-97

PLOT DATE = 1/4/2008

CHECKED -

REVISED - M. GOMEZ 04-06-01

DATE - 06-13-90

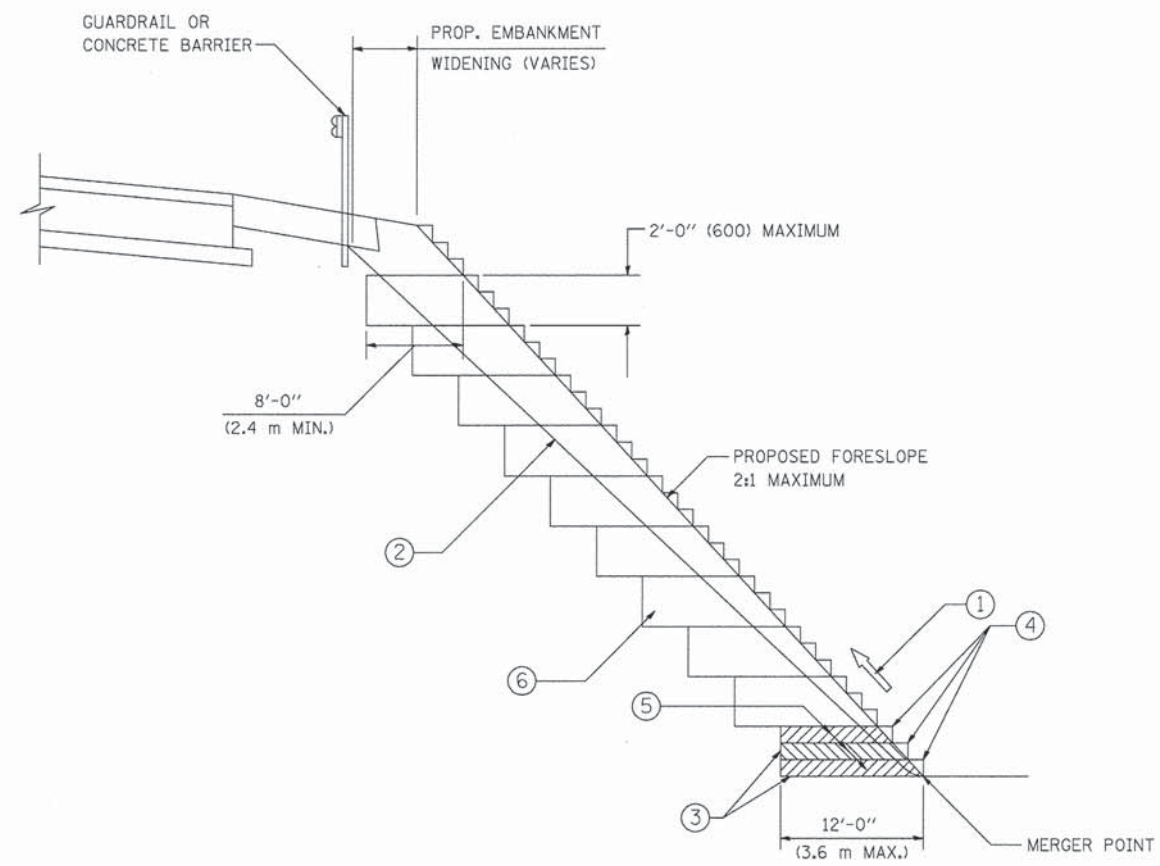
REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|----------------|--------|--------------------|-----------|
| 1600 | 04-00003-00-CH | WILL | 96 | 71 |
| BD400-05 BD32 | | | CONTRACT NO. 63147 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



TYPICAL BENCHING DETAIL
FOR EMBANKMENT

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

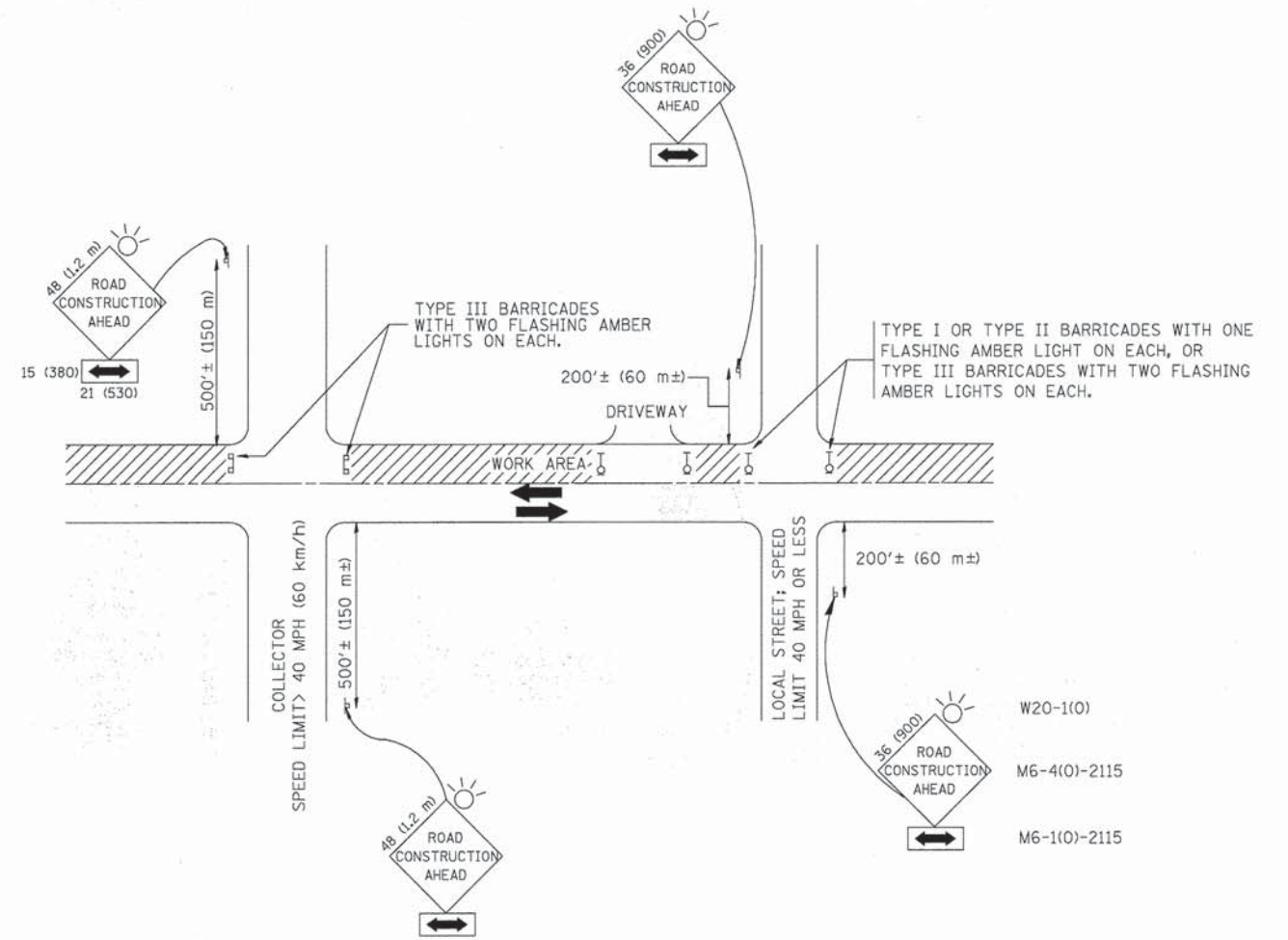
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| | | DRAWN - CADD | REVISED - |
| | | CHECKED - S.E.B. | REVISED - |
| | | DATE - 06-16-04 | REVISED - |
| | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | |
|--|-------------------------|------|---------|
| BENCHING DETAIL FOR EMBANKMENT WIDENING | | | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

| | | | | |
|--|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 72 |
| BD-51 | | | CONTRACT NO. 63147 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

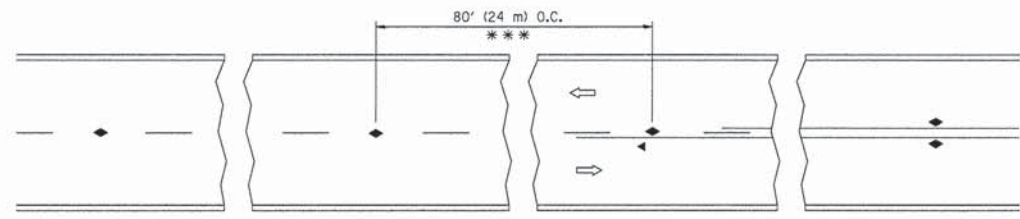
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|--|------------------------------|-------------------|----------------------------------|
| FILE NAME = W:\distatd\22x34\to18.dgn | USER NAME = gaglianobt | DESIGNED - LHA | REVISED - J. OBERLE 10-18-95 |
| | | DRAWN - | REVISED - A. HOUSEH 03-06-96 |
| | PLOT SCALE = 50.000 / IN. | CHECKED - | REVISED - A. HOUSEH 10-15-96 |
| | PLOT DATE = 1/4/2008 | DATE - 06-89 | REVISED - T. RAMMACH 01-06-00 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

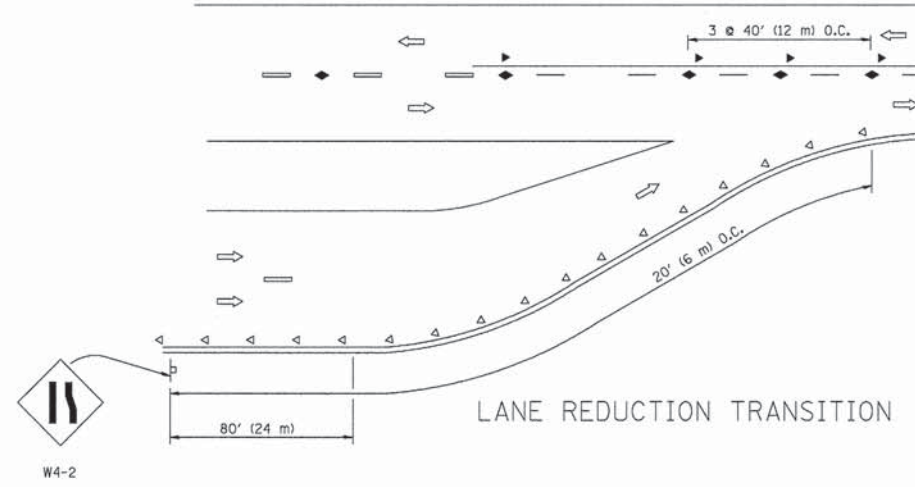
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|----------------|--------|--------------------|-----------|
| 1600 | 04-00003-00-CH | WILL | 96 | 73 |
| TC-10 | | | CONTRACT NO. 63147 | |
| FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-8003 (562) | | | | |

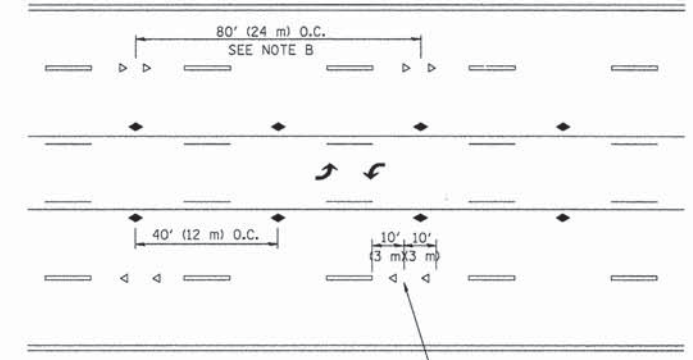


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

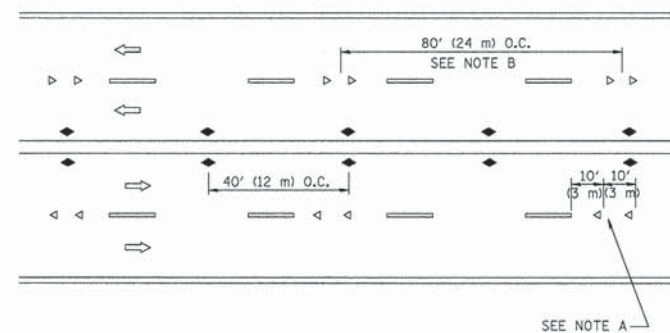
TWO-LANE/TWO-WAY



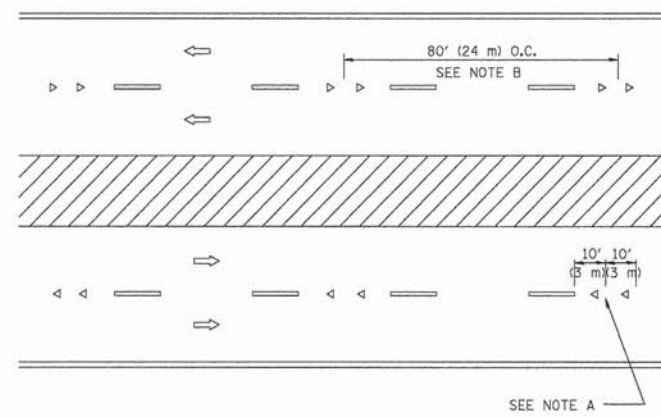
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

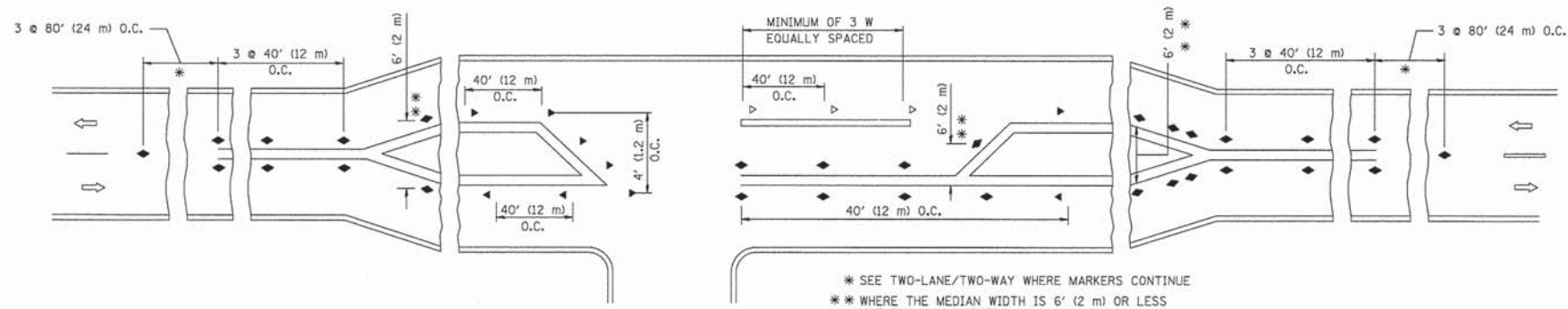
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

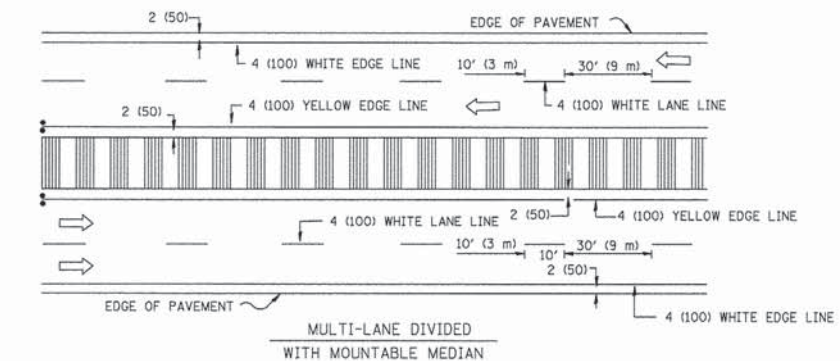
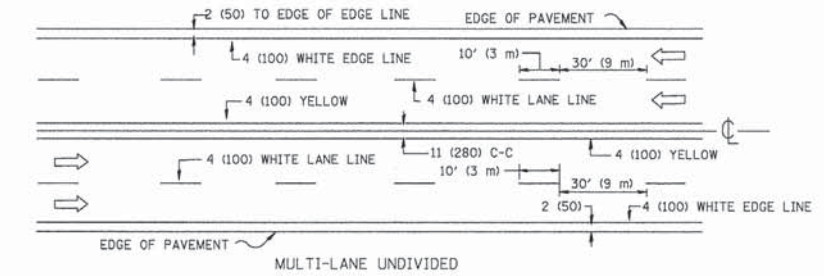
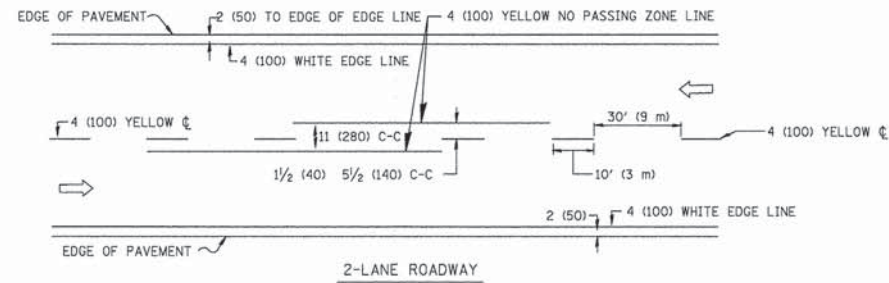
All dimensions are in inches (millimeters) unless otherwise shown.

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|---|---------------------------|------------|---------------------------------|
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| | | DRAWN - | REVISED - T. RAMMACHER 03-12-99 |
| | PLOT SCALE = 50.000' / 1" | CHECKED - | REVISED - T. RAMMACHER 01-06-00 |
| | PLOT DATE = 3/5/2013 | DATE - | REVISED - C. JUCIUS 09-09-09 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

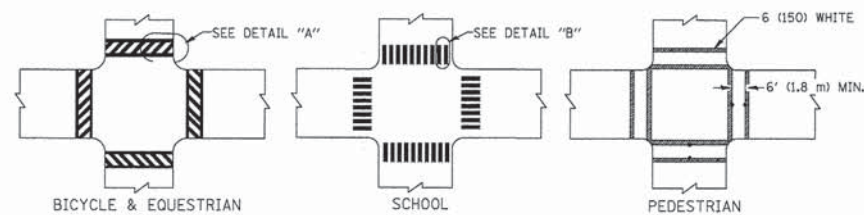
| TYPICAL APPLICATIONS | |
|--|-------------------------|
| RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS |
| STA. | TO STA. |

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|----------------|--------|--------------------|-----------|
| 1600 | 04-00003-00-CH | WILL | 96 | 74 |
| TC-11 | | | CONTRACT NO. 63147 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |

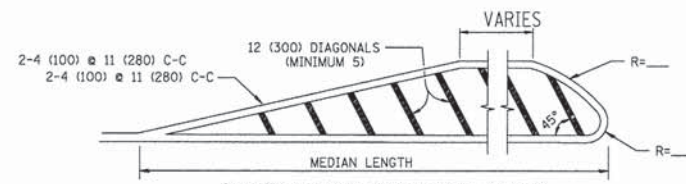
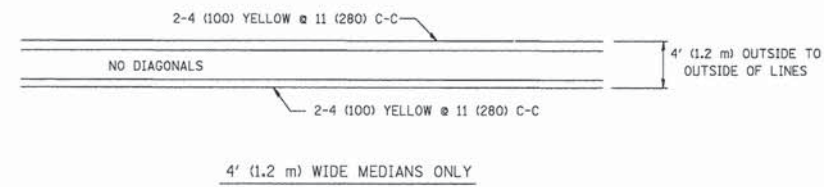


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

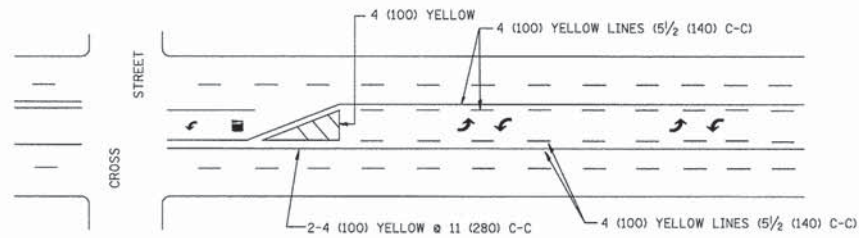


TYPICAL CROSSWALK MARKING

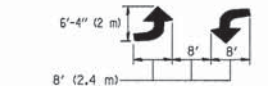


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

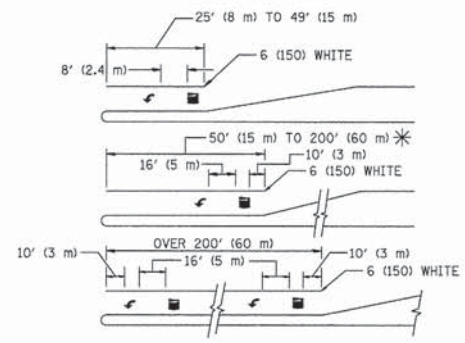


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

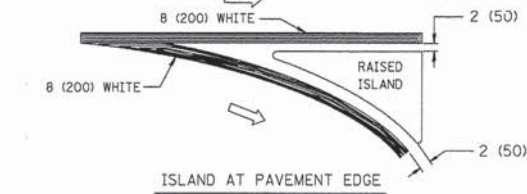
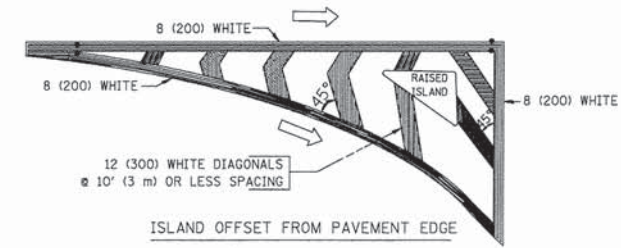
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

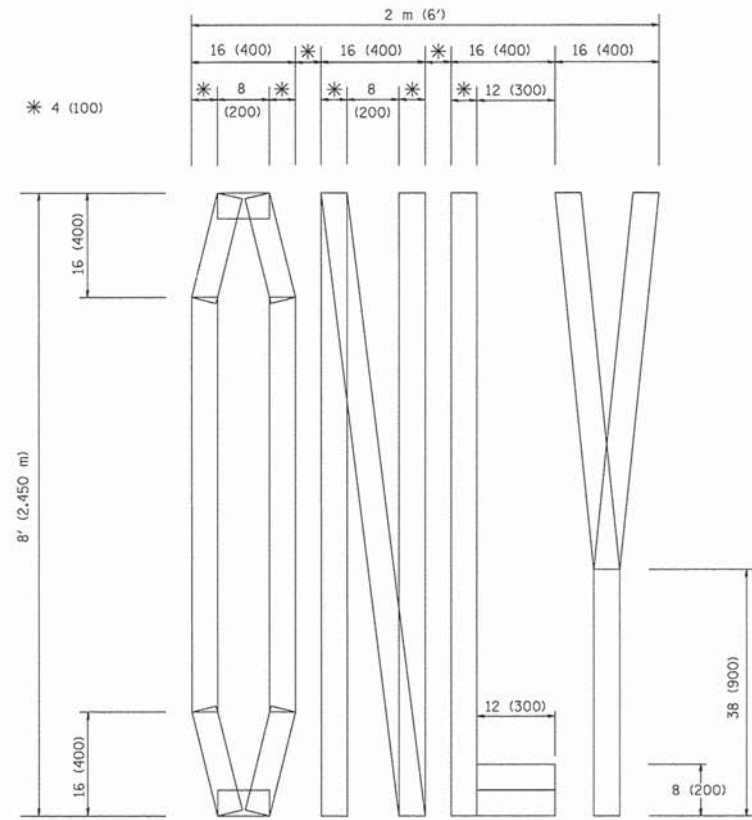


TYPICAL ISLAND MARKING

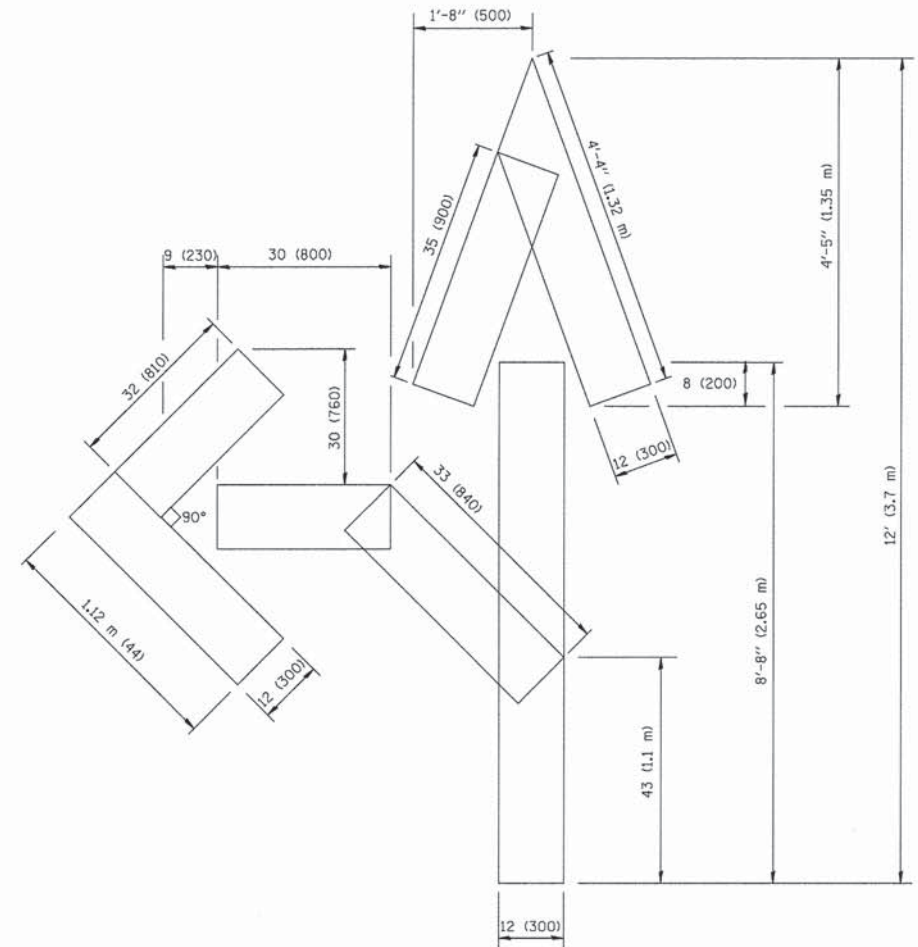
| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|--|------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS | 4 (100) 2 @ 4 (100) | SOLID SOLID | YELLOW YELLOW | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²) |
| SHOULDER DIAGONALS | 12 (300) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

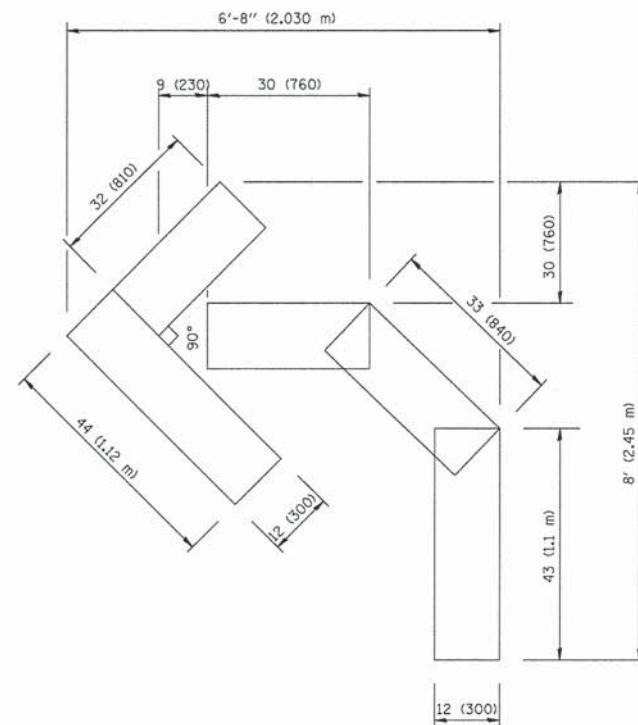
All dimensions are in inches (millimeters) unless otherwise shown.



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in Inches (millimeters) unless otherwise shown.

| | | | |
|--|-----------------------|--------------------------------|--|
| FILE NAME = W:\distatd\22x34\to16.dgn | USER NAME = gaglianob | DESIGNED - DRAWN - | REVISED -T. RAMMACHER 06-05-96 REVISED -T. RAMMACHER 11-04-97 |
| PLLOT SCALE = 58.0000 "/ IN. | CHECKED - | REVISED -T. RAMMACHER 03-02-98 | |
| PLLOT DATE = 1/4/2008 | DATE - 09-18-94 | REVISED -E. GOMEZ 08-28-00 | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING LETTERS AND SYMBOLS
FOR TRAFFIC STAGING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|----------------|--------|--------------------|-----------|
| 1600 | 04-00003-00-CH | WILL | 96 | 76 |
| TC-16 | | | CONTRACT NO. 63147 | |
| FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-8003 (562) | | | | |

ROUTE MARKERS

FOR U.S. ROUTES
M1-40-2424

FOR ILLINOIS ROUTES
M1-50-2424

R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

ARROWS SIGNS

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-3-2115

CARDINAL DIRECTION & DETOUR SIGNS

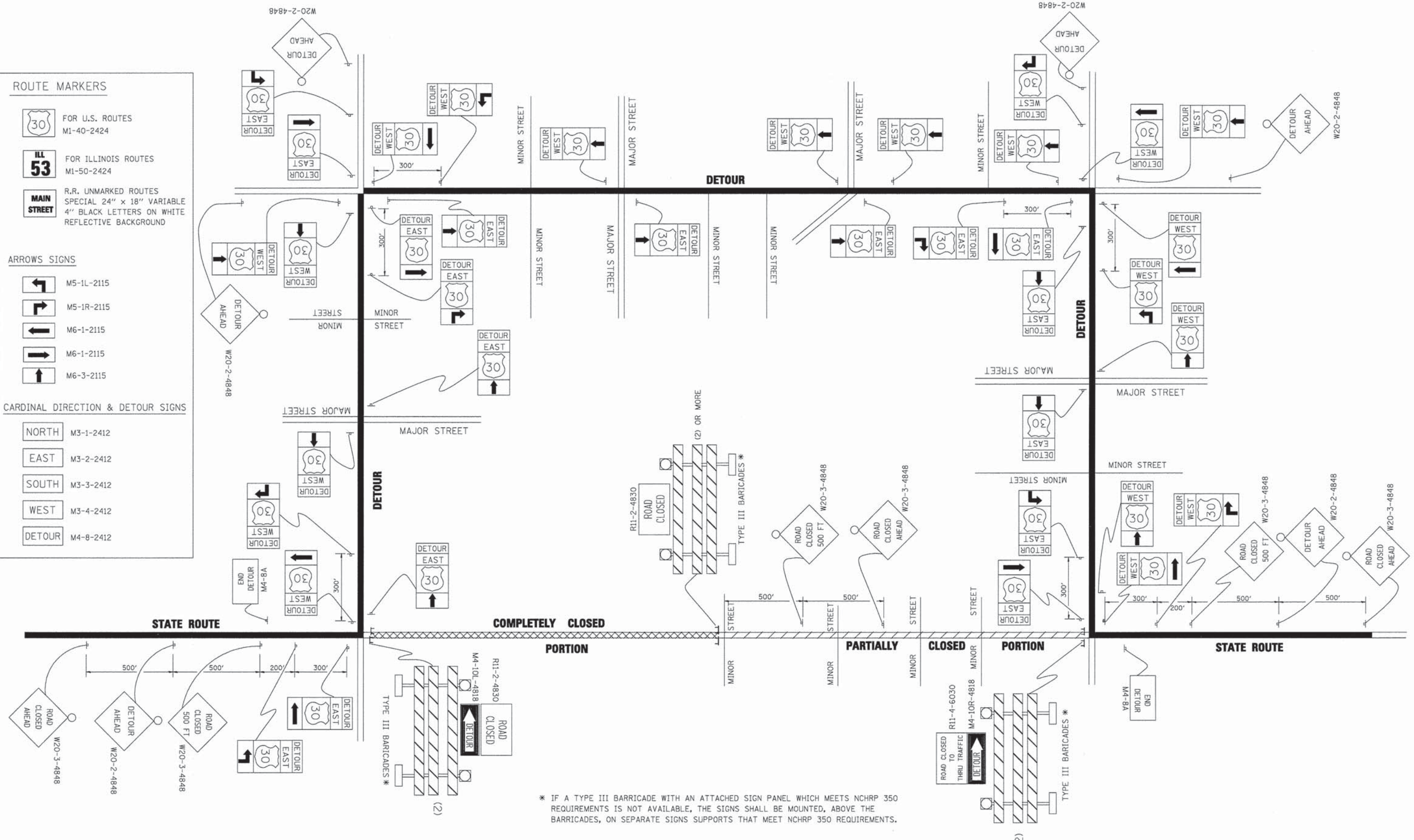
NORTH M3-1-2412

EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

DETOUR M4-8-2412



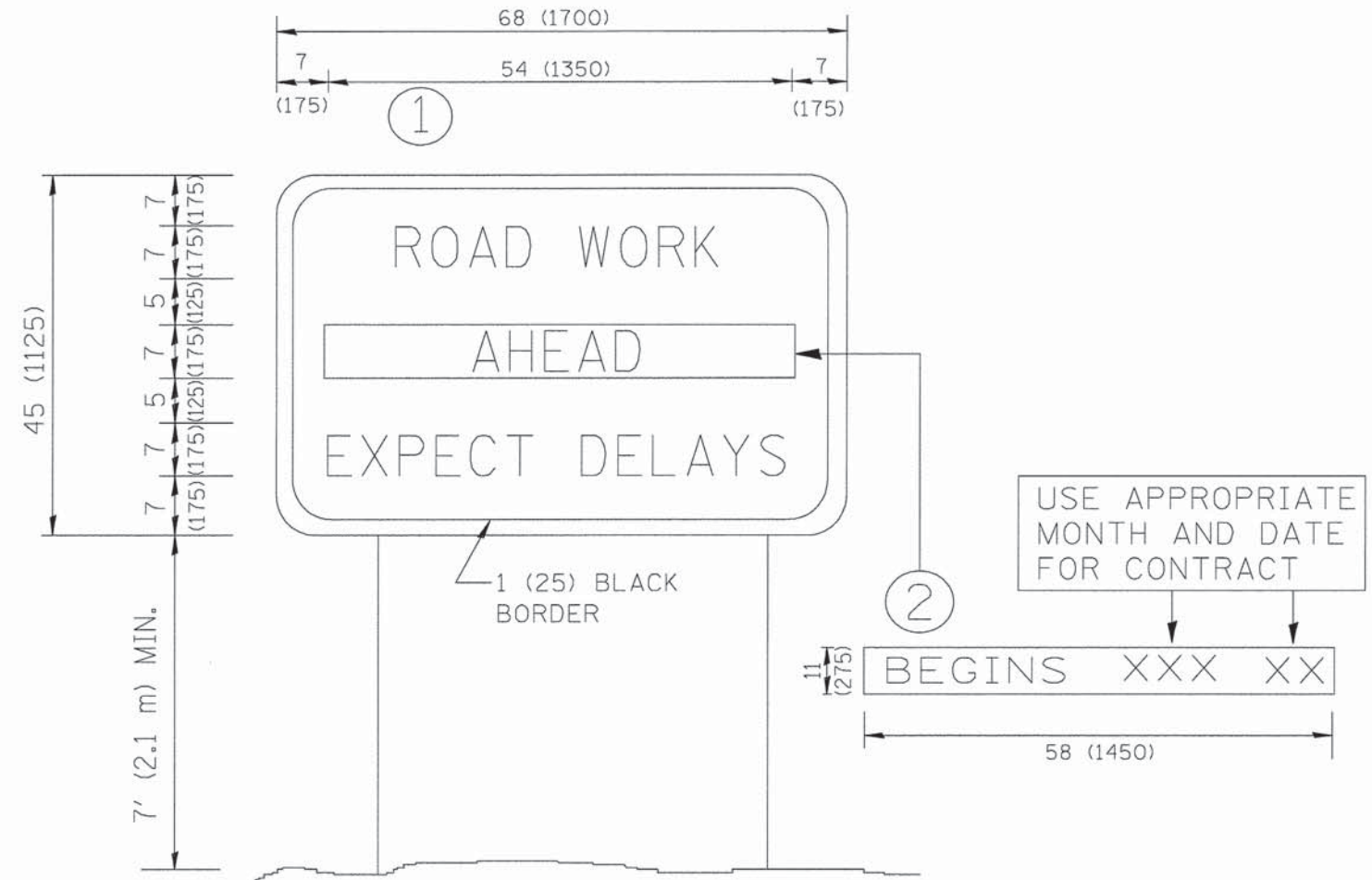
* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

| | | | |
|---|------------------------|------------|----------------------------|
| FILE NAME = | USER NAME = drivakoagn | DESIGNED - | REVISED - 10-18-02 |
| ci:\pv_work\VP\I00T\DRIVAKOSGN\0108315\1421.dgn | | DRAWN - | REVISED - R. BORO 09-14-09 |
| | | CHECKED - | REVISED - |
| | | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | |
|--|-------------------------|
| DETOUR SIGNING FOR CLOSING STATE HIGHWAYS | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS |
| STA. | TO STA. |

| | | | | |
|--|----------------|--------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 77 |
| TC-21 | | CONTRACT NO. 63147 | | |
| FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-8003 (562) | | | | |



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | |
|---------------------------------------|-----------------------------|-----------------------|--|
| FILE NAME = W:\data\22x34\to22.dgn | USER NAME = geglianobt | DESIGNED - DRAWN - | REVISED - R. MIRS 09-15-97 REVISED - R. MIRS 12-11-97 |
| | PLOT SCALE = 50,000 ' / IN. | CHECKED - | REVISED - T. RAMMACHER 02-02-99 |
| | PLOT DATE = 1/4/2008 | DATE - | REVISED - C. JUCIUS 01-31-07 |

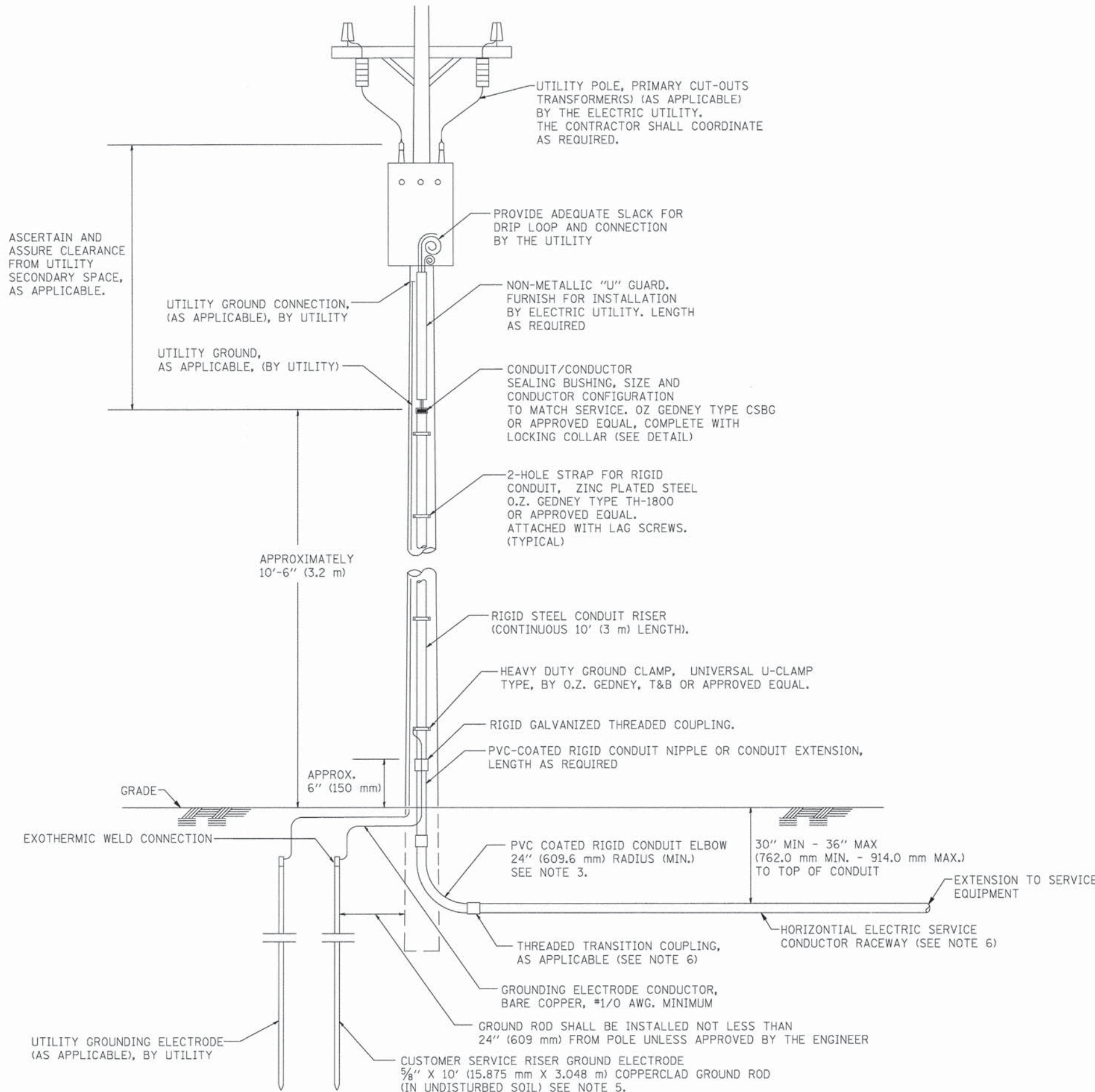
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|----------------|--------|--------------------|-----------|
| 1600 | 04-00003-00-CH | WILL | 96 | 78 |
| TC-22 | | | CONTRACT NO. 63147 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |

ASCERTAIN AND ASSURE CLEARANCE FROM UTILITY SECONDARY SPACE, AS APPLICABLE.



UTILITY POLE, PRIMARY CUT-OUTS TRANSFORMER(S) (AS APPLICABLE) BY THE ELECTRIC UTILITY. THE CONTRACTOR SHALL COORDINATE AS REQUIRED.

PROVIDE ADEQUATE SLACK FOR DRIP LOOP AND CONNECTION BY THE UTILITY

UTILITY GROUND CONNECTION, (AS APPLICABLE), BY UTILITY

UTILITY GROUND, AS APPLICABLE, (BY UTILITY)

NON-METALLIC "U" GUARD. FURNISH FOR INSTALLATION BY ELECTRIC UTILITY. LENGTH AS REQUIRED

CONDUIT/CONDUCTOR SEALING BUSHING, SIZE AND CONDUCTOR CONFIGURATION TO MATCH SERVICE. OZ GEDNEY TYPE CSBG OR APPROVED EQUAL, COMPLETE WITH LOCKING COLLAR (SEE DETAIL)

2-HOLE STRAP FOR RIGID CONDUIT, ZINC PLATED STEEL O.Z. GEDNEY TYPE TH-1800 OR APPROVED EQUAL. ATTACHED WITH LAG SCREWS. (TYPICAL)

APPROXIMATELY 10'-6" (3.2 m)

RIGID STEEL CONDUIT RISER (CONTINUOUS 10' (3 m) LENGTH).

HEAVY DUTY GROUND CLAMP, UNIVERSAL U-CLAMP TYPE, BY O.Z. GEDNEY, T&B OR APPROVED EQUAL.

RIGID GALVANIZED THREADED COUPLING.

PVC-COATED RIGID CONDUIT NIPPLE OR CONDUIT EXTENSION, LENGTH AS REQUIRED

APPROX. 6" (150 mm)

GRADE

EXOTHERMIC WELD CONNECTION

PVC COATED RIGID CONDUIT ELBOW 24" (609.6 mm) RADIUS (MIN.) SEE NOTE 3.

30" MIN - 36" MAX (762.0 mm MIN. - 914.0 mm MAX.) TO TOP OF CONDUIT

EXTENSION TO SERVICE EQUIPMENT

THREADED TRANSITION COUPLING, AS APPLICABLE (SEE NOTE 6)

HORIZONTAL ELECTRIC SERVICE CONDUCTOR RACEWAY (SEE NOTE 6)

GROUNDING ELECTRODE CONDUCTOR, BARE COPPER, #1/0 AWG. MINIMUM

GROUND ROD SHALL BE INSTALLED NOT LESS THAN 24" (609 mm) FROM POLE UNLESS APPROVED BY THE ENGINEER

UTILITY GROUNDING ELECTRODE (AS APPLICABLE), BY UTILITY

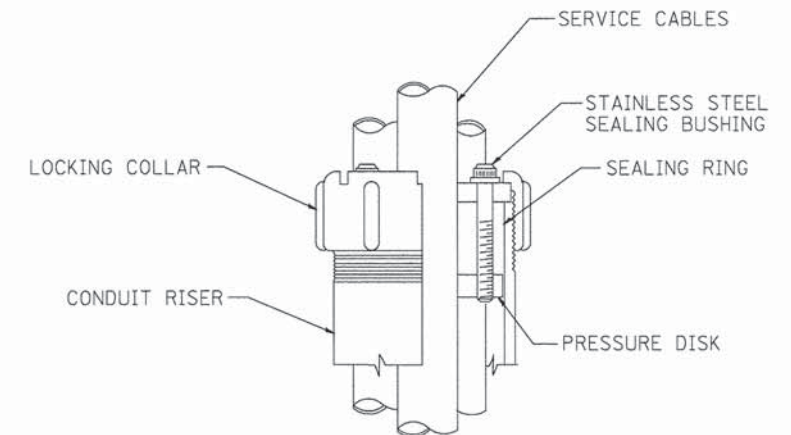
CUSTOMER SERVICE RISER GROUND ELECTRODE 5/8" X 10' (15.875 mm X 3.048 m) COPPERCLAD GROUND ROD (IN UNDISTURBED SOIL) SEE NOTE 5.

APPLICATION

THIS DETAIL APPLIES FOR LOW VOLTAGE ELECTRIC SERVICE (660 V OR LESS) FROM AN OVERHEAD UTILITY SUPPLY TO SEPERATLY-MOUNTED SERVICE EQUIPMENT.

NOTES

- SERVICE VOLTAGE SHALL BE AS INDICATED ELSEWHERE IN THE DRAWINGS.
- UNLESS OTHERWISE INDICATED, ITEMS AND WORK SHALL BE INCLUDED AND PAID AS PART OF THE ELECTRIC UTILITY SERVICE INSTALLATION PAY ITEM.
- CONDUIT AND CONNECTOR DIAMETER SHALL MATCH THE DIAMETER OF THE SERVICE CONDUCTOR RACEWAY AS INDICATED ON THE PLANS.
- PVC COATED RACEWAYS AND ACCESSORIES SHALL BE CAREFULLY INSTALLED WITH MFR RECOMMENDED TOOLS AND PROCEDURES TO AVOID DAMAGE. ANY DAMAGE SHALL BE REPAIRED WITH COMPATIBLE PVC TOUCH-UP MATERIAL TO THE SATISFACTION OF THE ENGINEER OR THE DAMAGED MATERIAL SHALL BE REPLACED AT NO ADDITIONAL COST.
- THE CONTRACTOR SHALL OBTAIN INSPECTION AND APPROVAL BY THE ENGINEER OF SERVICE RISER GROUND ELECTRODE, RISER ELBOW, NIPPLE AND CONNECTION TO SERVICE CONDUCTOR RACEWAY EXTENSION BEFORE BACKFILL AND SHALL ALSO OBTAIN INSPECTION OF SERVICE RISER AND SEALING BUSHING BEFORE UTILITY "U" GUARD INSTALLATION AND SERVICE CONNECTION.
- THE HORIZONTAL ELECTRIC SERVICE CONDUCTOR RACEWAY SHALL BE AS INDICATED AND SHALL BE MEASURED SEPARATELY FOR PAYMENT. WHEN THE RACEWAY IS PVC-COATED RIGID GALVANIZED STEEL, THE COUPLING SHALL BE THE SAME. WHEN THE RACEWAY IS PVC CONDUIT (IN CONCRETE), THE COUPLING SHALL BE A METALIC TO NON METALIC ADAPTER. WHEN THE RACEWAY IS ENCASED IN CONCRETE, THE CONCRETE SHALL EXTEND TO COVER THE COUPLING.
- PLANS AND DETAILS INDICATE THE GENERAL NATURE AND REQUIREMENTS. THEY DO NOT SHOW EVERY ACCESSORY AND ATTACHMENT, AND THEY DO NOT RELIEVE THE CONTRACTOR OF THE REQUIREMENTS OF THE SPECIFICATIONS AND SPECIAL PROVISIONS TO ASCERTAIN UTILITY REQUIREMENTS AND TO COORDINATE ACCORDINGLY, FURNISHING ALL ITEMS AND WORK NOT PROVIDED BY THE UTILITY, BUT NECESSARY FOR A COMPLETE SERVICE INSTALLATION IS REQUIRED AND SHALL BE INCLUDED IN THE ELECTRIC UTILITY SERVICE INSTALLATION PAY ITEM.

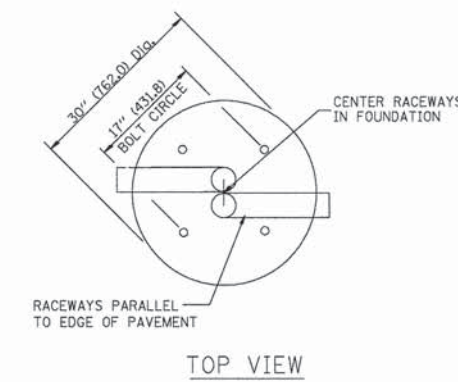
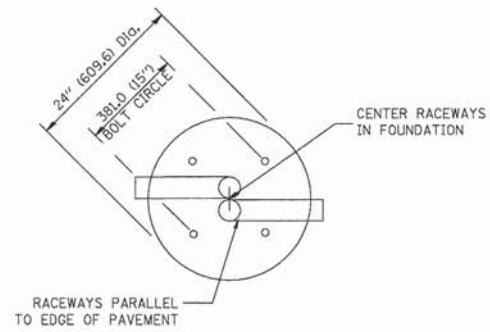


SEALING BUSHING DETAIL

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| PLOT SCALE = 5/8" = 1'-0" | CHECKED - MEA | REVISED - | REVISED - | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | BE-220 | | CONTRACT NO. 63147 |
| PLOT DATE = 3/6/2013 | DATE - | REVISED - | REVISED - | | FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-8003 (562) | | | | | | |
| | | | | | | | | | | | |

LIGHT POLE FOUNDATION DEPTH TABLE
40 FT. (12.192 m) TO 47.5 FT. (14.478 m) MOUNTING HEIGHT

| SOIL CONDITIONS | DESIGN DEPTH "D" OF FOUNDATION | |
|---|--------------------------------|--------------------|
| | SINGLE ARM POLE | TWIN ARM POLE |
| SOFT CLAY Q _u = 0.375 TON/SQ. FT. | 13'-0" (3.96 m) | 15'-0" (4.57 m) |
| MEDIUM CLAY Q _u = 0.75 TON/SQ.FT | 9'-6" (2.93 m) | 10'-9" (3.23 m) |
| STIFF CLAY Q _u = 1.50 TON/SQ. FT. | 7'-0" (2.13 m) | 8'-0" (2.44 m) |
| LOOSE SAND φ = 34° | 9'-0" (2.74 m) | 10'-0" (3.05 m) |
| MEDIUM SAND φ = 37.5° | 8'-3" (2.52 m) | 9'-0" (2.74 m) |
| DENSE SAND φ = 40° | 7'-9" (2.36 m) | 9'-0" (2.74 m) |

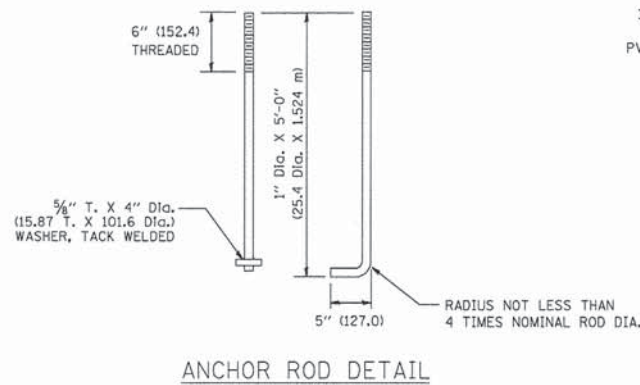
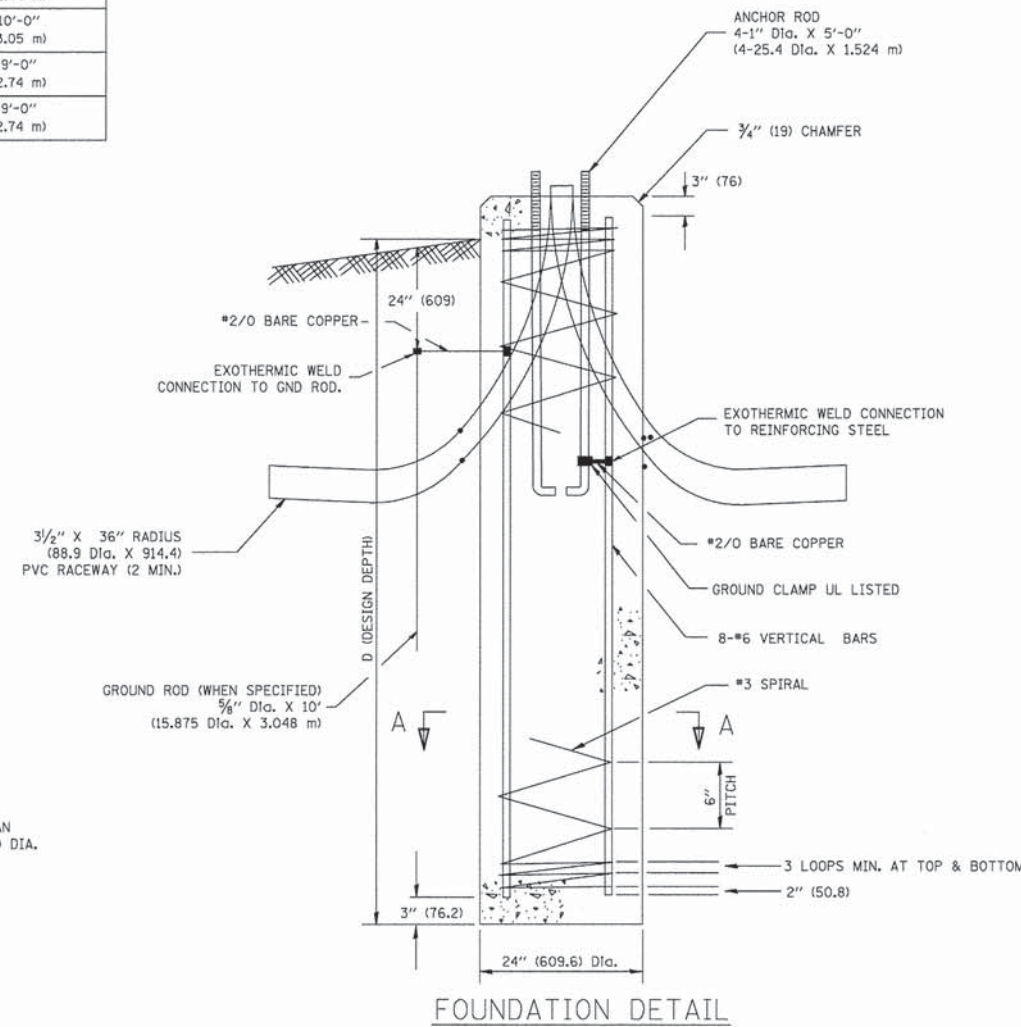


TOP VIEW

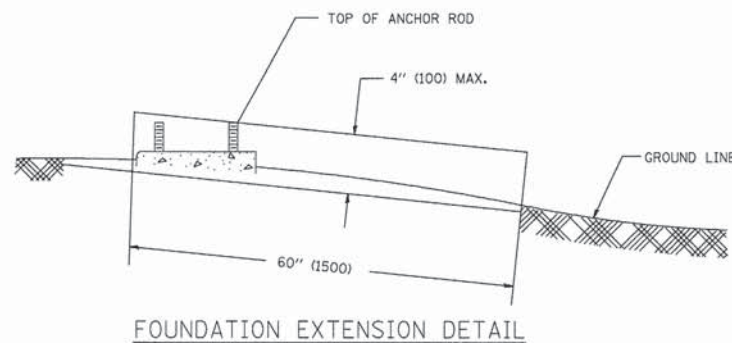
TOP VIEW

NOTES

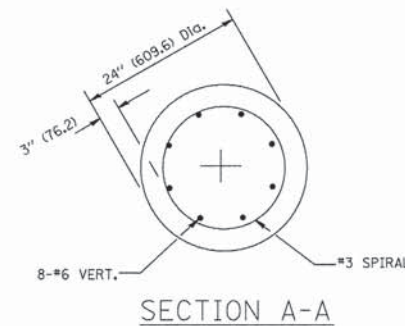
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 100MM (4 IN.) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 1/4-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS SI. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UM(6 MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.



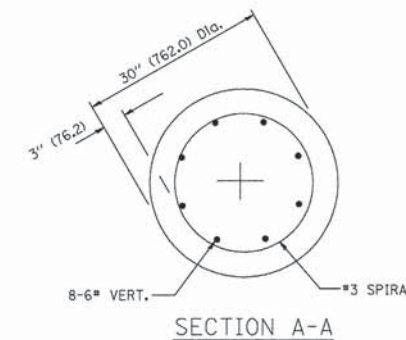
ANCHOR ROD DETAIL



FOUNDATION EXTENSION DETAIL



SECTION A-A



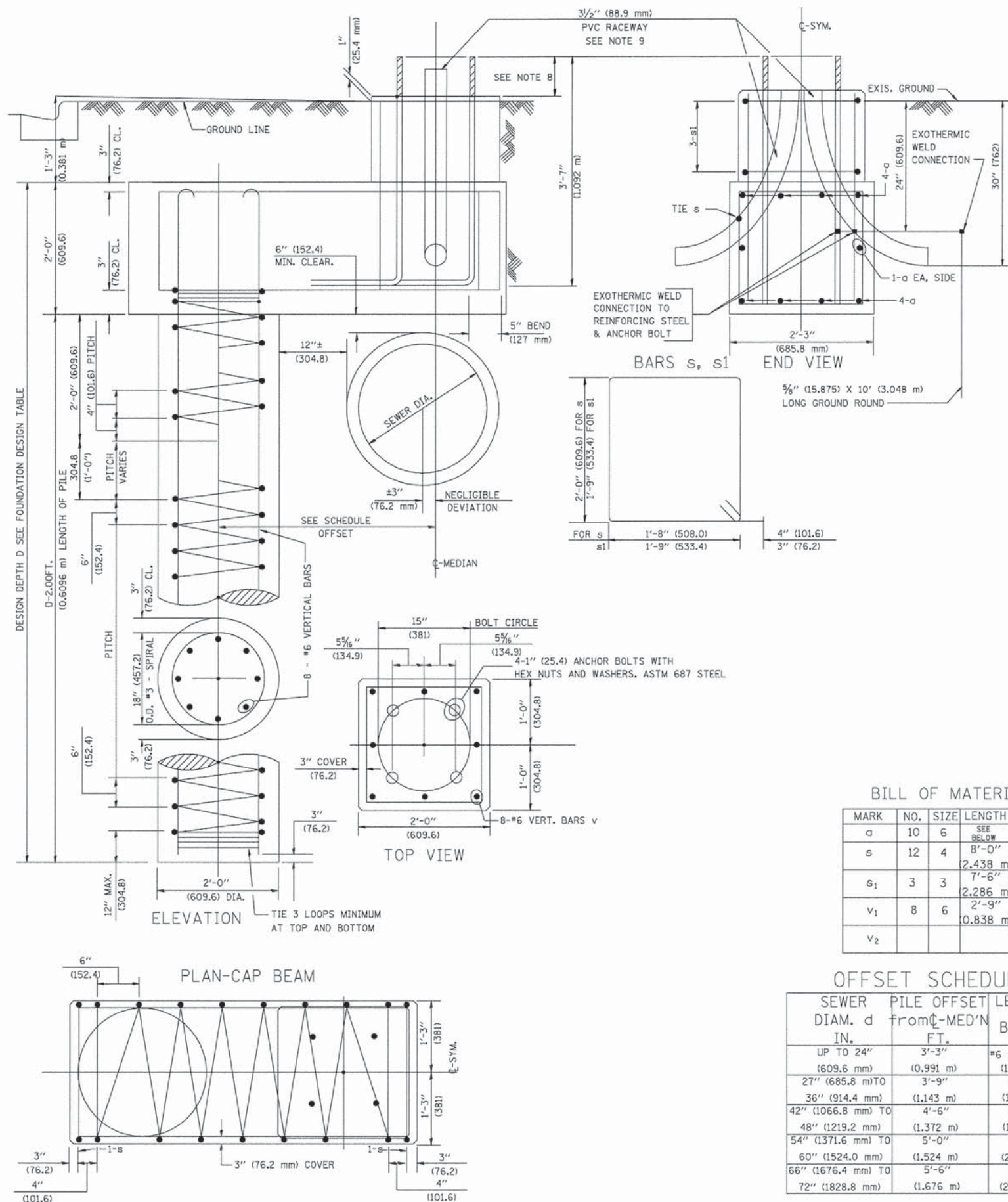
SECTION A-A

FOUNDATION DESIGN TABLE

| TYPE OF SOIL | DESIGN DEPTH OF FOUNDATION | | REINFORCEMENT IN FOUNDATION | | | |
|-------------------------|----------------------------|---------------------|-----------------------------|-----------------------|--------------------------|-----------------------|
| | SINGLE ARM D | TWIN ARM D | SINGLE ARM | | TWIN ARM | |
| | | | VERT BARS | SPIRAL | VERT BARS | SPIRAL |
| SOFT CLAY | 13'-0" (3.962 m) | 15'-0" (4.572 m) | 8-#6X12'-6" (3.810 m) | #3X122' (37.186 m) | 8-#6X14'-3" (4.343 m) | #3X141' (42.977 m) |
| MEDIUM CLAY | 9'-6" (2.896 m) | 10'-9" (3.277 m) | 8-#6X9'-0" (2.743 m) | #3X90' (27.432 m) | 8-#6X10'-0" (3.048 m) | #3X100' (30.480 m) |
| STIFF CLAY | 7'-0" (2.134 m) | 8'-0" (2.438 m) | 8-#6X6'-6" (1.981 m) | #3X66' (20.112 m) | 8-#6X7'-6" (2.286 m) | #3X76' (23.165 m) |
| LOOSE SAND | 9'-0" (2.743 m) | 10'-0" (3.048 m) | 8-#6X8'-6" (2.591 m) | #3X85' (25.908 m) | 8-#6X9'-6" (2.896 m) | #3X94' (28.651 m) |
| MEDIUM SAND | 8'-3" (2.515 m) | 9'-0" (2.743 m) | 8-#6X8'-0" (2.438 m) | #3X78' (23.774 m) | 8-#6X8'-6" (2.591 m) | #3X85' (25.908 m) |
| DENSE SAND | 7'-9" (2.362 m) | 9'-0" (2.743 m) | 8-#6X7'-6" (2.286 m) | #3X73' (22.250 m) | 8-#6X8'-6" (2.591 m) | #3X85' (25.908 m) |
| ROCK OR SOLIDIFIED SLAG | 5'-0" (1.524 m) | 5'-0" (1.524 m) | NONE | NONE | NONE | NONE |

NOTES

- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ENGINEER SHALL DETERMINE THE CLASS OF SOIL DURING EXCAVATION AND SELECT THE DESIGN DEPTH OF FOUNDATION FROM THE DESIGN TABLE.
- EXCAVATION OF THE POLE FOUNDATION SHALL BE MADE WITH AN AUGER, 24" (609.6 mm) OR 30" (762.0 mm) IN DIAMETER.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR BOLTS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED IN THE FORM.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- THE CONTRACTOR SHALL COORDINATE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF FOUNDATION WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS. IF LIGHT POLE IS MOUNTED WITHOUT BREAKAWAY DEVICE, ANCHOR BOLTS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE TOP OF THE FOUNDATION. THE CONTRACTOR SHALL CONFIRM ANCHOR BOLT EXTENTION WITH ENGINEER.
- RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.
- THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE LIGHT IS ERECTED.



BILL OF MATERIAL

| MARK | NO. | SIZE | LENGTH | SHAPE |
|----------------|-----|------|--------------------|-------|
| a | 10 | 6 | SEE BELOW | — |
| s | 12 | 4 | 8'-0" (2.438 m) | □ |
| s ₁ | 3 | 3 | 7'-6" (2.286 m) | □ |
| v ₁ | 8 | 6 | 2'-9" (0.838 m) | — |
| v ₂ | | | | |

OFFSET SCHEDULE

| SEWER DIAM. d IN. | PILE OFFSET FROM C-MED'N FT. | LENGTH OF BAR a FT. |
|----------------------|------------------------------|----------------------|
| UP TO 24" (609.6 mm) | 3'-3" (0.991 m) | #6 x 5'-3" (1.600 m) |
| 27" (685.8 mm) TO | 3'-9" (1.143 m) | 5'-9" (1.753 m) |
| 36" (914.4 mm) TO | 4'-6" (1.372 m) | 6'-6" (1.981 m) |
| 42" (1066.8 mm) TO | 5'-0" (1.524 m) | 7'-0" (2.134 m) |
| 48" (1219.2 mm) TO | 5'-6" (1.676 m) | 7'-6" (2.286 m) |
| 54" (1371.6 mm) TO | 6'-0" (1.828 m) | 8'-0" (2.438 m) |
| 60" (1524.0 mm) TO | 6'-6" (1.981 m) | 8'-6" (2.591 m) |
| 66" (1676.4 mm) TO | 7'-0" (2.134 m) | 9'-0" (2.743 m) |
| 72" (1828.8 mm) TO | 7'-6" (2.286 m) | 9'-6" (2.896 m) |

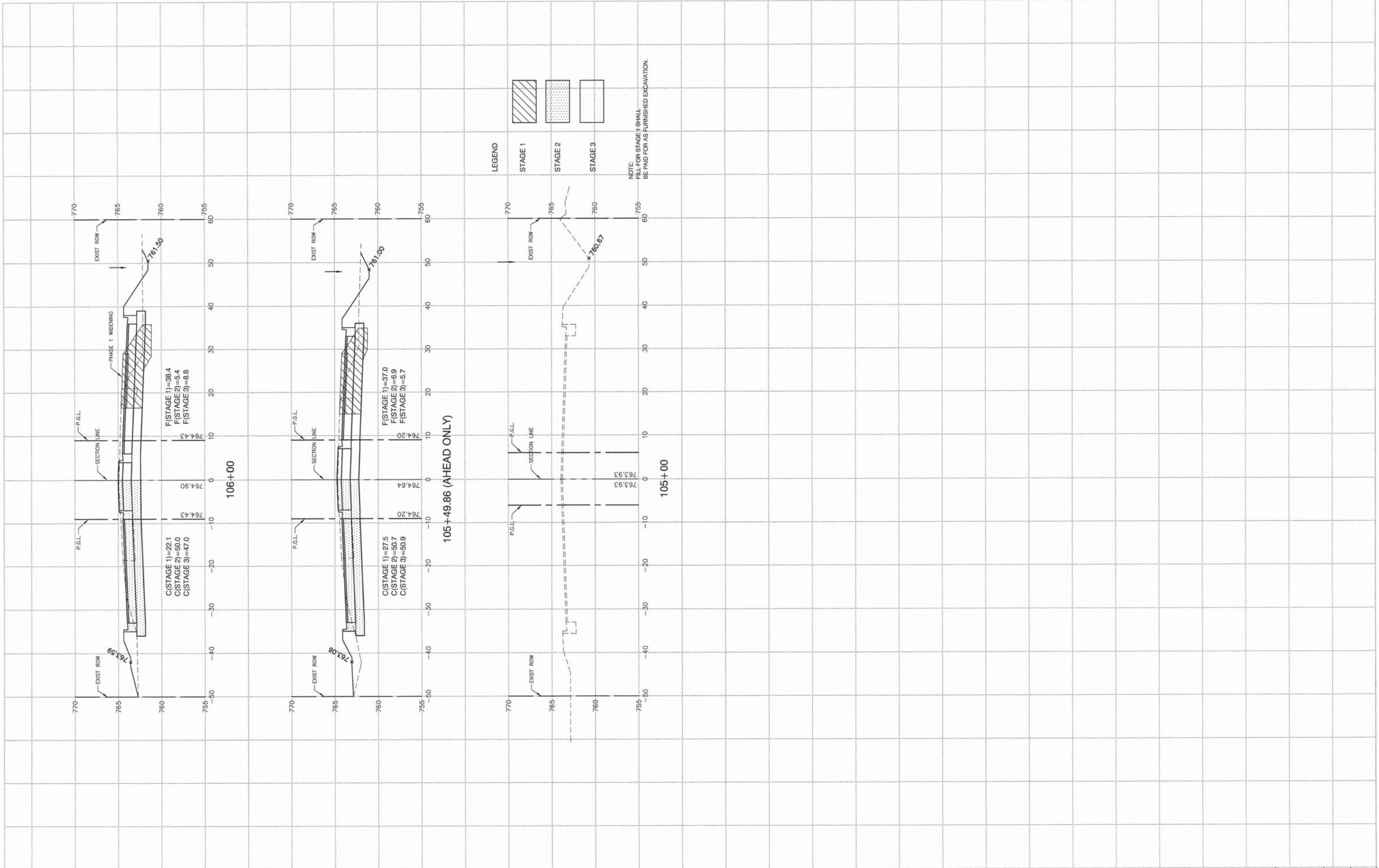
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| | | DRAWN - | REVISED - |
| | PLOT SCALE = 5/8" = 1'-0" | CHECKED - | REVISED - |
| | PLOT DATE = 3/6/2013 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LIGHT POLE FOUNDATION OFFSET
40" (1219.2 mm) TO 47 1/2" (1447.8 mm) M.H.
15" (381 mm) BOLT CIRCLE

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|--|------------------------|--------------------|-----------------|---------------|
| F.A.U. RTE. 1600 | SECTION 04-00003-00-CH | COUNTY WILL | TOTAL SHEETS 96 | SHEET NO. 81A |
| BE-310 | | CONTRACT NO. 63147 | | |
| ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



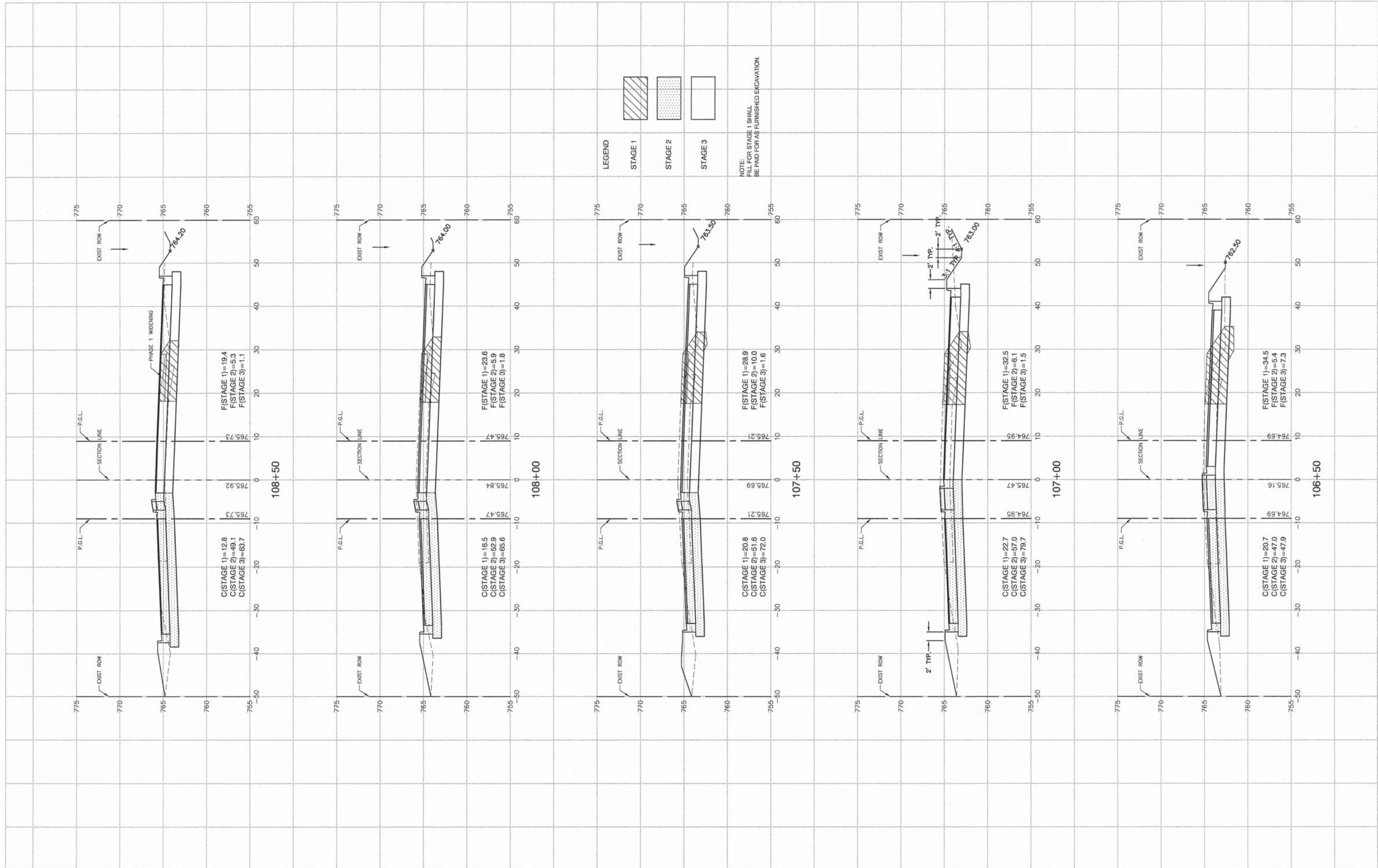
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| CHECKED | | REVISED | |
| DRAWN | RG | REVISED | |
| CHECKED | AG | REVISED | |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
 INTERSECTION RECONSTRUCTION
 CROSS SECTIONS
 SCALE: H 1"=10' V 1"=5'
 SHEET NO. 82 OF 96 SHEETS
 STA. 103+50 TO STA. 106+00

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 82 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



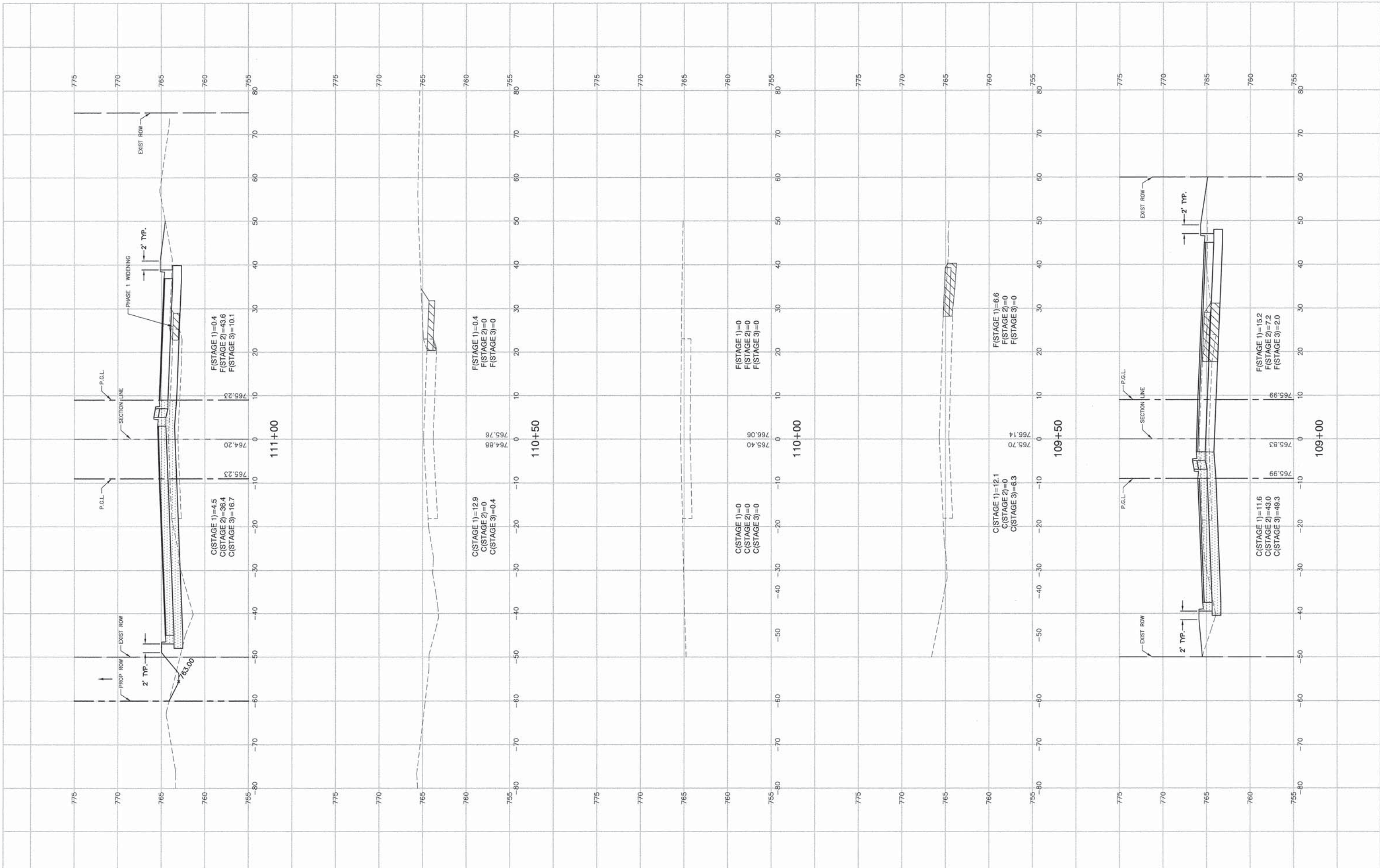
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| | PLOT DATE = 05-30-13 | CHECKED = AG | REVISED = |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
CROSS SECTIONS**

SCALE: H 1"=10' V 1"=5' SHEET NO. 83 OF 96 SHEETS STA. 106+50 TO STA. 108+50

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL. | 96 | 83 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



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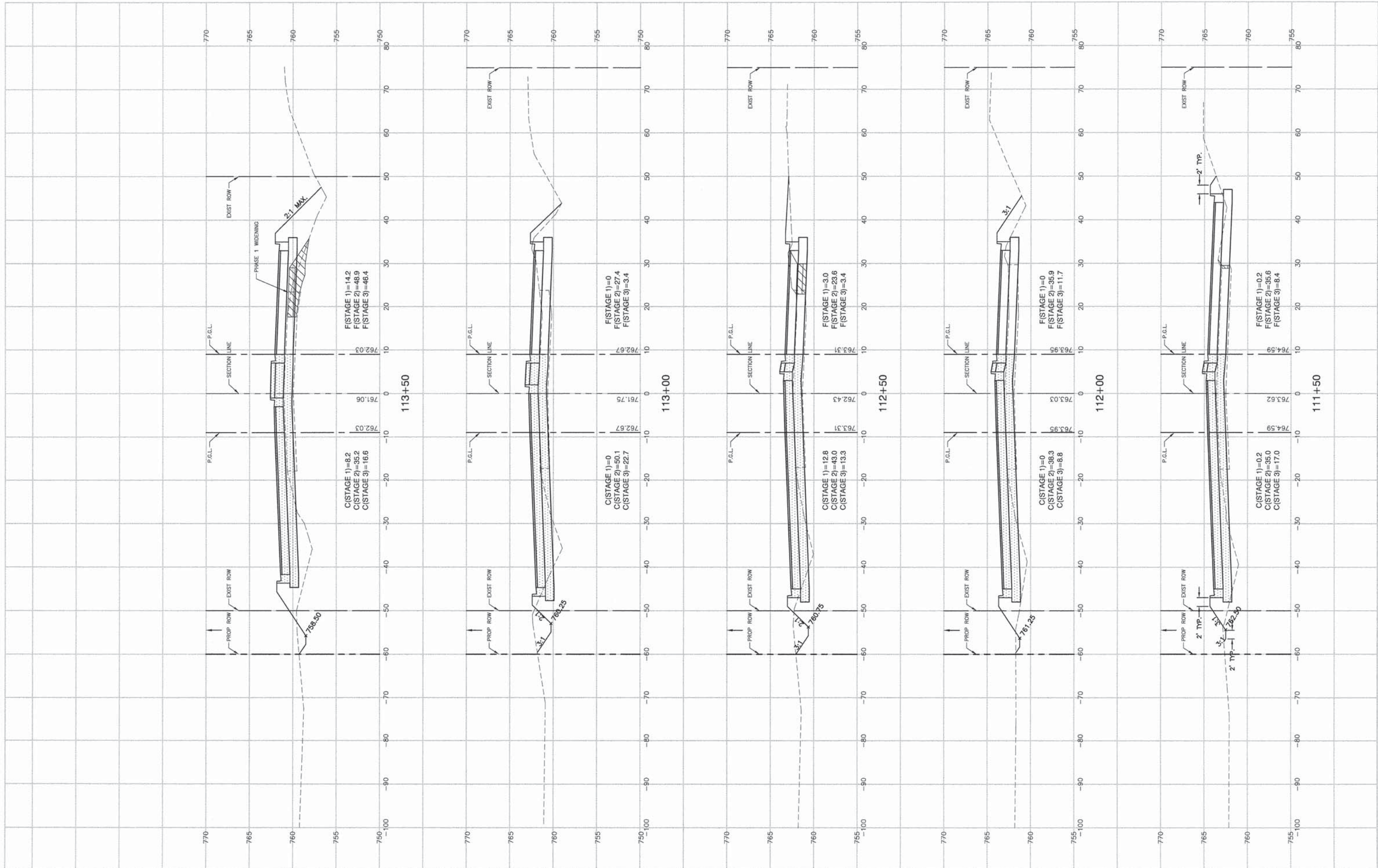
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
CROSS SECTIONS

SCALE: H 1"=10' V 1"=5' SHEET NO. 84 OF 96 SHEETS STA. 109+00 TO STA. 111+00

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RITE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 84 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



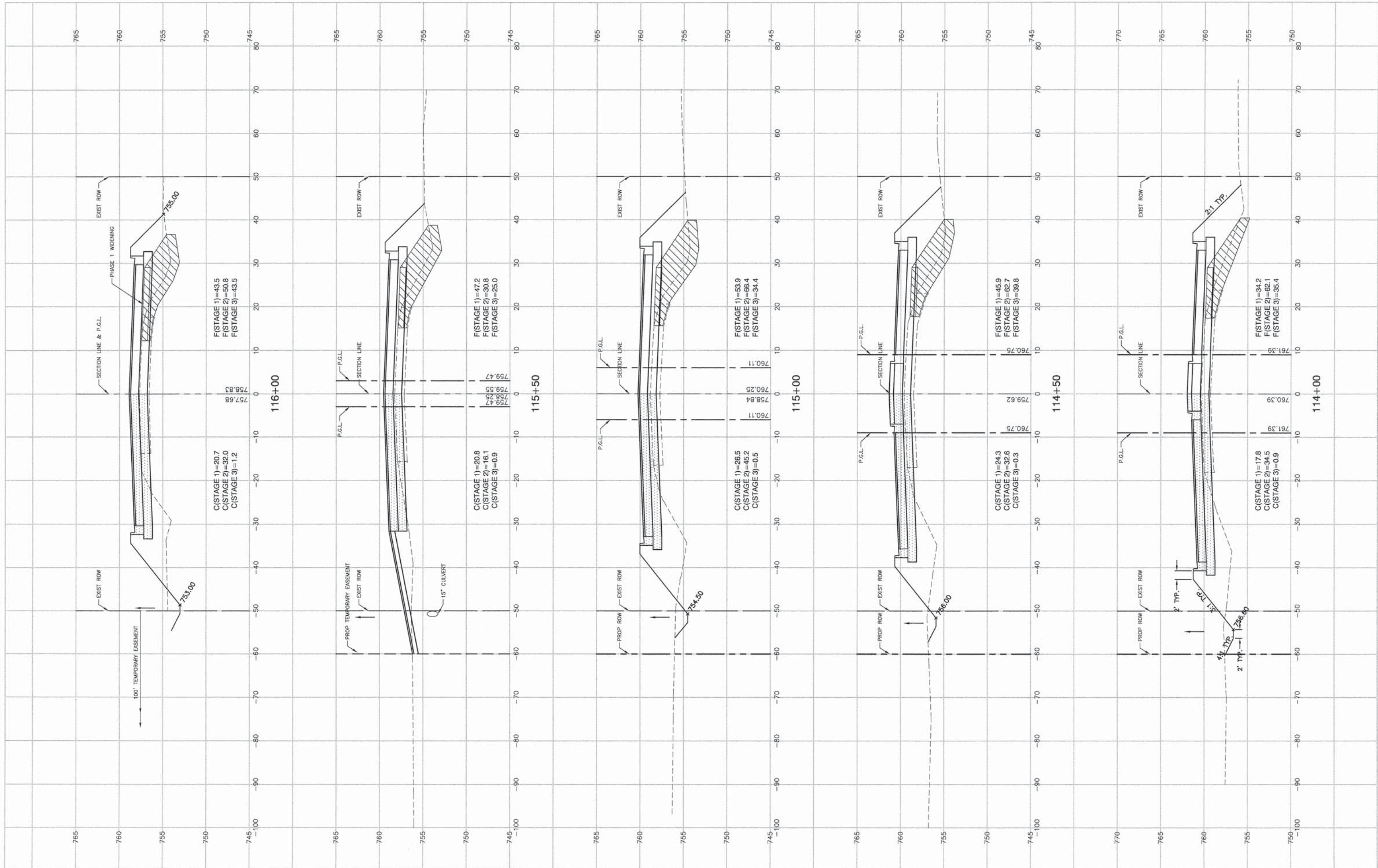
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| | PLOT DATE = 05-30-13 | | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
CROSS SECTIONS**

SCALE: H 1"=10' V 1"=5' SHEET NO. 85 OF 96 SHEETS STA. 111+50 TO STA. 113+50

| | | | | |
|--------------------------------|------------------------|-------------|--------------------|--------------|
| F.A.U. RTE. 1600 | SECTION 04-00003-00-CH | COUNTY WILL | TOTAL SHEETS 96 | SHEET NO. 85 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | CONTRACT NO. 63147 | |
| FED. AID PROJECT M-8003 (562) | | | | |



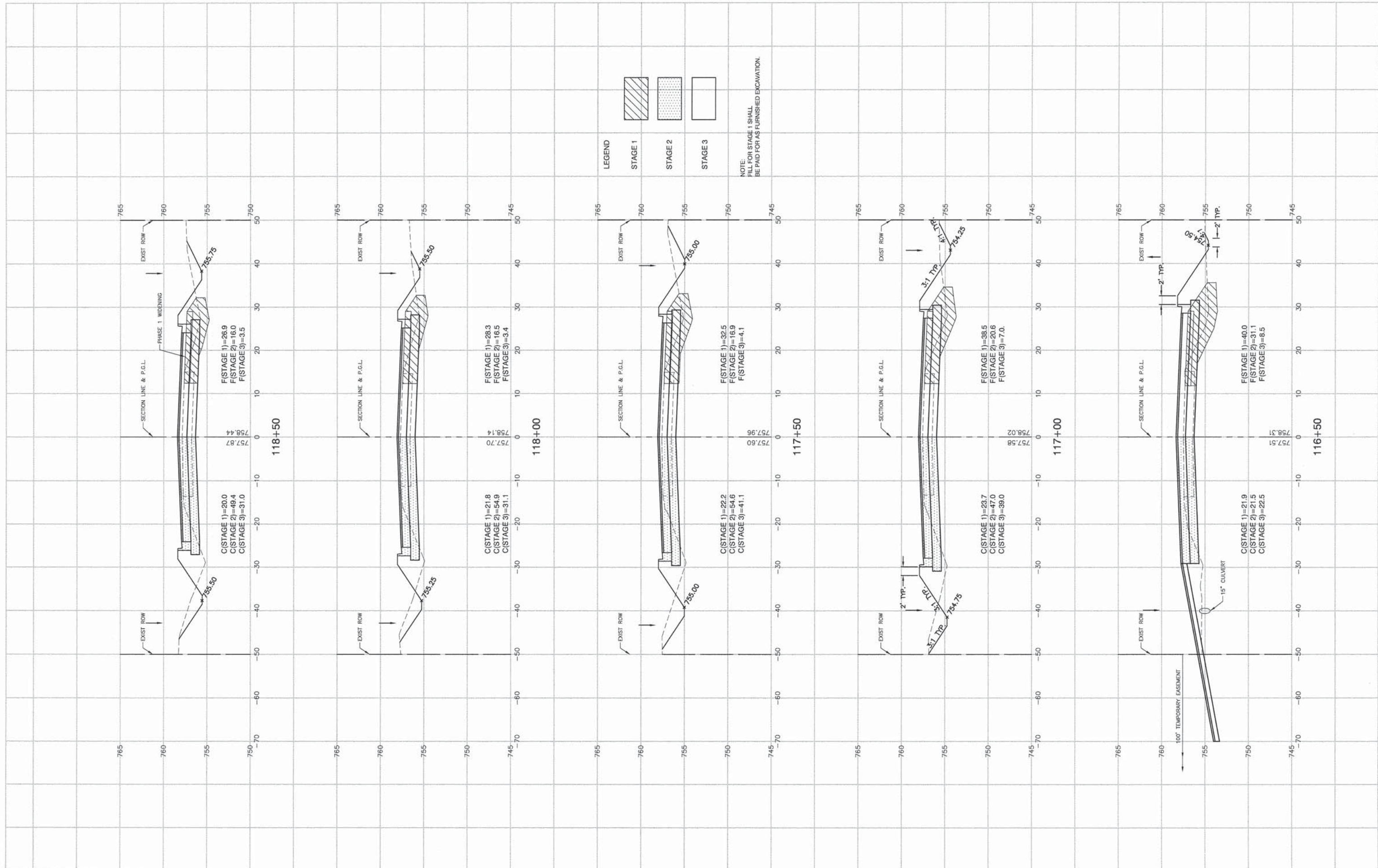
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| CHECKED | | REVISED | |
| DRAWN | RG | REVISED | |
| CHECKED | AG | REVISED | |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
 INTERSECTION RECONSTRUCTION
 CROSS SECTIONS
 SCALE: H 1"=10' V 1"=5'
 SHEET NO. 86 OF 96 SHEETS
 STA. 114+00 TO STA. 116+00

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 86 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



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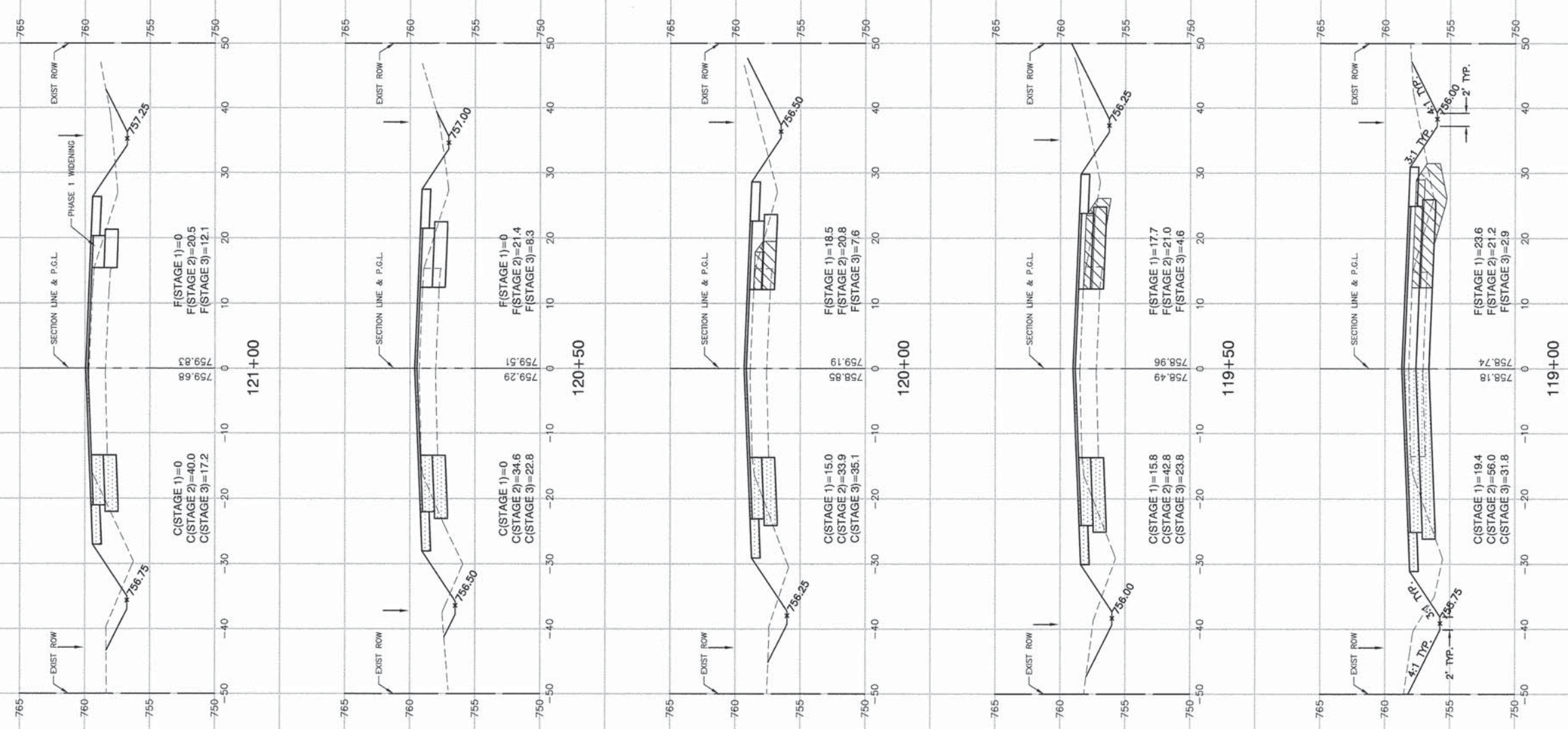
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| CHECKED -- | REVISED -- |
| DRAWN -- RG | REVISED -- |
| CHECKED -- AG | REVISED -- |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**143RD STREET AND LEMONT ROAD
 INTERSECTION RECONSTRUCTION
 CROSS SECTIONS**

SCALE: H 1"=10' V 1"=5' SHEET NO. 87 OF 96 SHEETS STA. 116+50 TO STA. 118+50

| | | | | |
|--------------------------------|------------------------|--------------|-------------------------------|--------------|
| F.A.U. RTE. 1600 | SECTION 04-00003-00-CH | COUNTY WILL. | TOTAL SHEETS 96 | SHEET NO. 87 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | FED. AID PROJECT M-8003 (562) | |
| CONTRACT NO. 63147 | | | | |



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

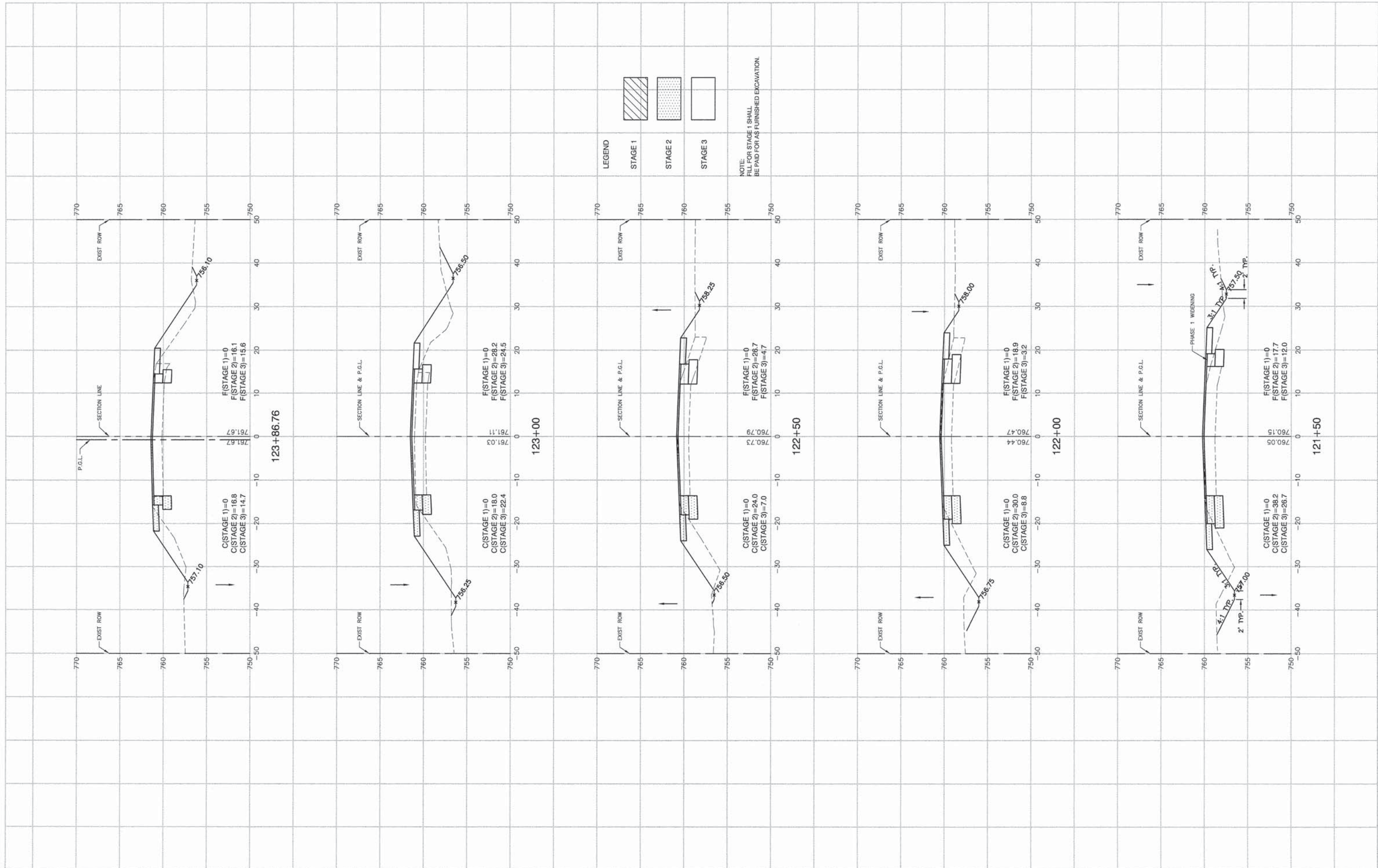
143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
CROSS SECTIONS

SCALE: H 1"=10' V 1"=5'

SHEET NO. 88 OF 96 SHEETS

STA. 119+00 TO STA. 121+00

| | | | | |
|-----------------------|------------------------|--------------------|-------------------------------|--------------|
| F.A.U. RTE. 1600 | SECTION 04-00003-00-CH | COUNTY WILL | TOTAL SHEETS 96 | SHEET NO. 88 |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT M-8003 (562) | |
| | | CONTRACT NO. 63147 | | |



NOTE: FILL FOR STAGE 1 SHALL BE PAID FOR AS FURNISHED EXCAVATION.

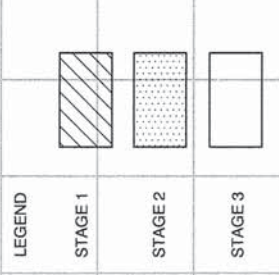
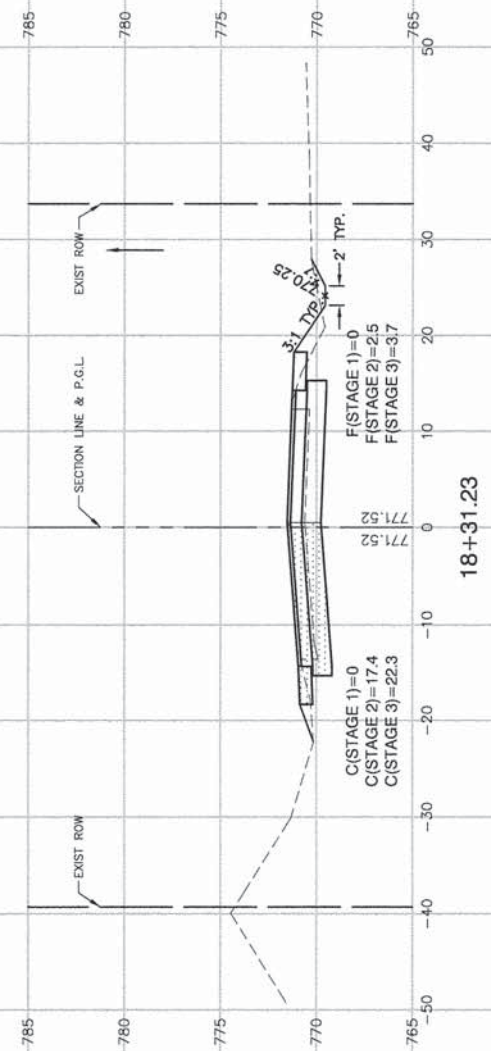
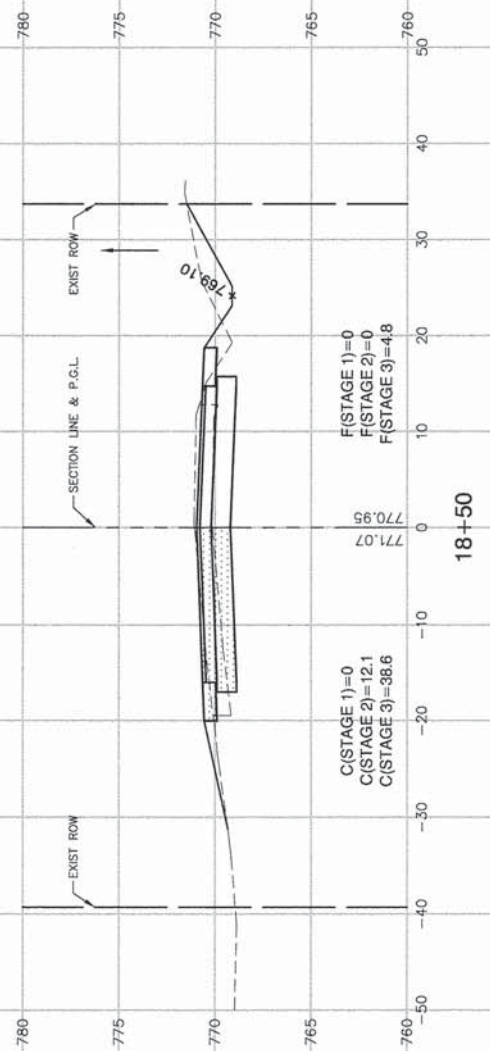
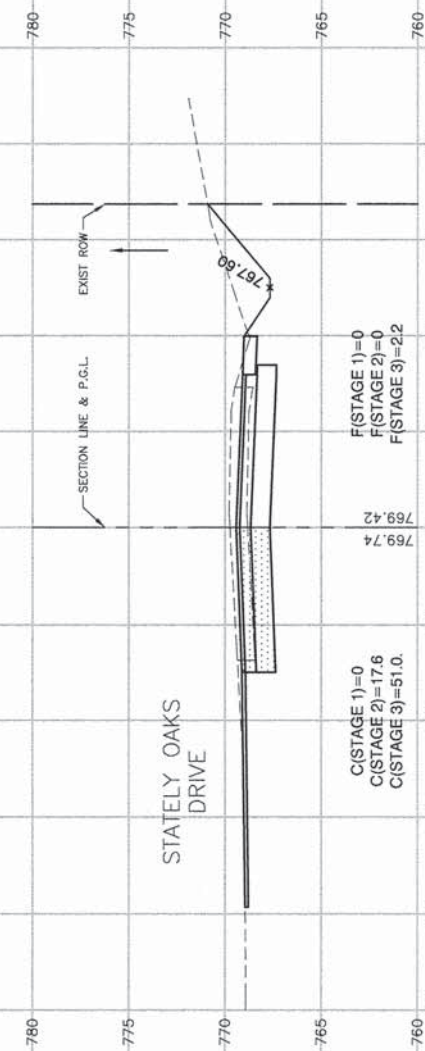
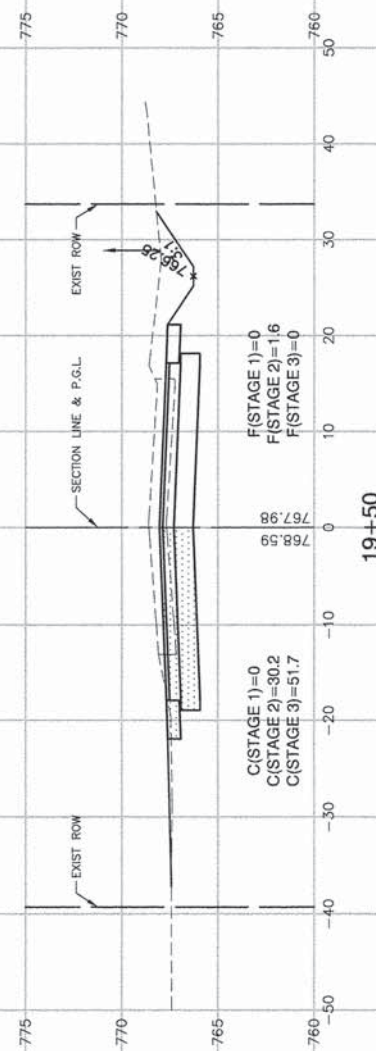
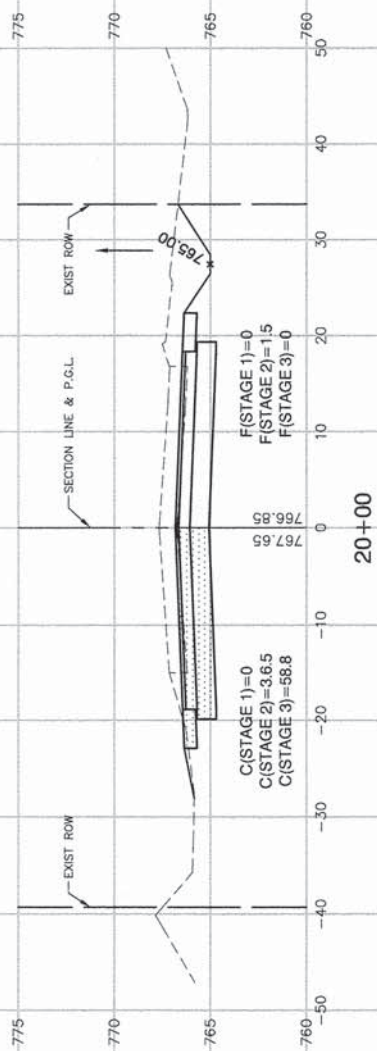
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| | | CHECKED = | REVISED = |
| | PLOT SCALE = | DRAWN = RG | REVISED = |
| | PLOT DATE = 05-30-13 | CHECKED = AG | REVISED = |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
CROSS SECTIONS

SCALE: H 1"=10' V 1"=5' SHEET NO. 89 OF 96 SHEETS STA. 121+50 TO STA. 123+54.66

| | | | | |
|--------------------------------|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL. | 96 | 89 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | CONTRACT NO. 63147 | |
| FED. AID PROJECT M-8003 (562) | | | | |



NOTE:
FILL FOR STAGE 1 SHALL
BE PAID FOR AS FURNISHED EXCAVATION.

FILE NAME = 04275_02-XSCT-01 -X13

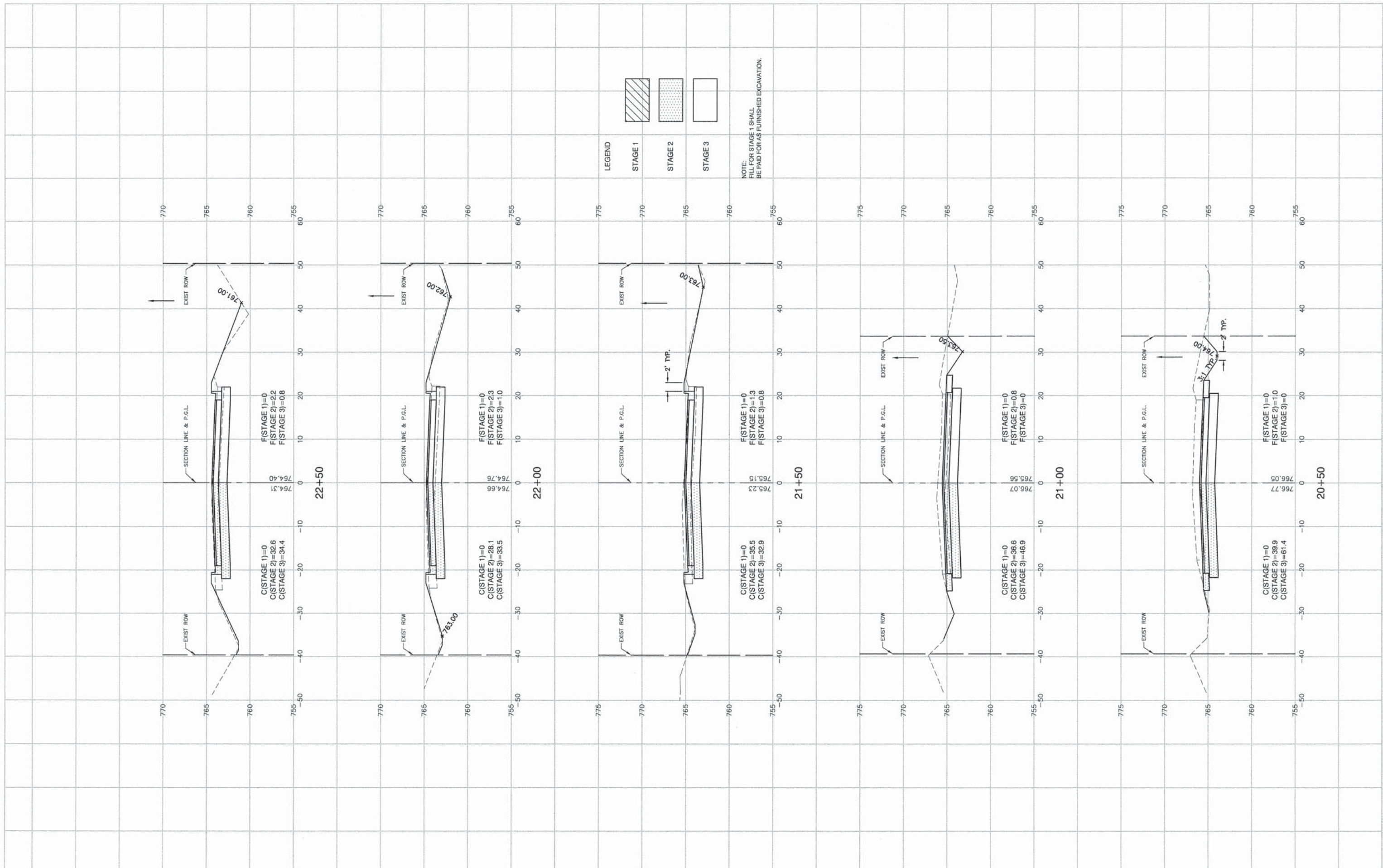
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| USER NAME = | DESIGNED -- HLG | REVISD -- 01-22-09 - IDOT REVIEW |
| PLOT SCALE = | CHECKED -- | REVISD -- |
| PLOT DATE = 05-30-13 | DRAWN -- RG | REVISD -- |
| | CHECKED -- AG | REVISD -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
CROSS SECTIONS

SCALE: H 1"=10' V 1"=5' SHEET NO. 90 OF 96 SHEETS STA. 18+31.23 TO STA. 20+00

| | | | | |
|--------------------------------|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 90 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | CONTRACT NO. 63147 | |
| FED. AID PROJECT M-8003 (562) | | | | |



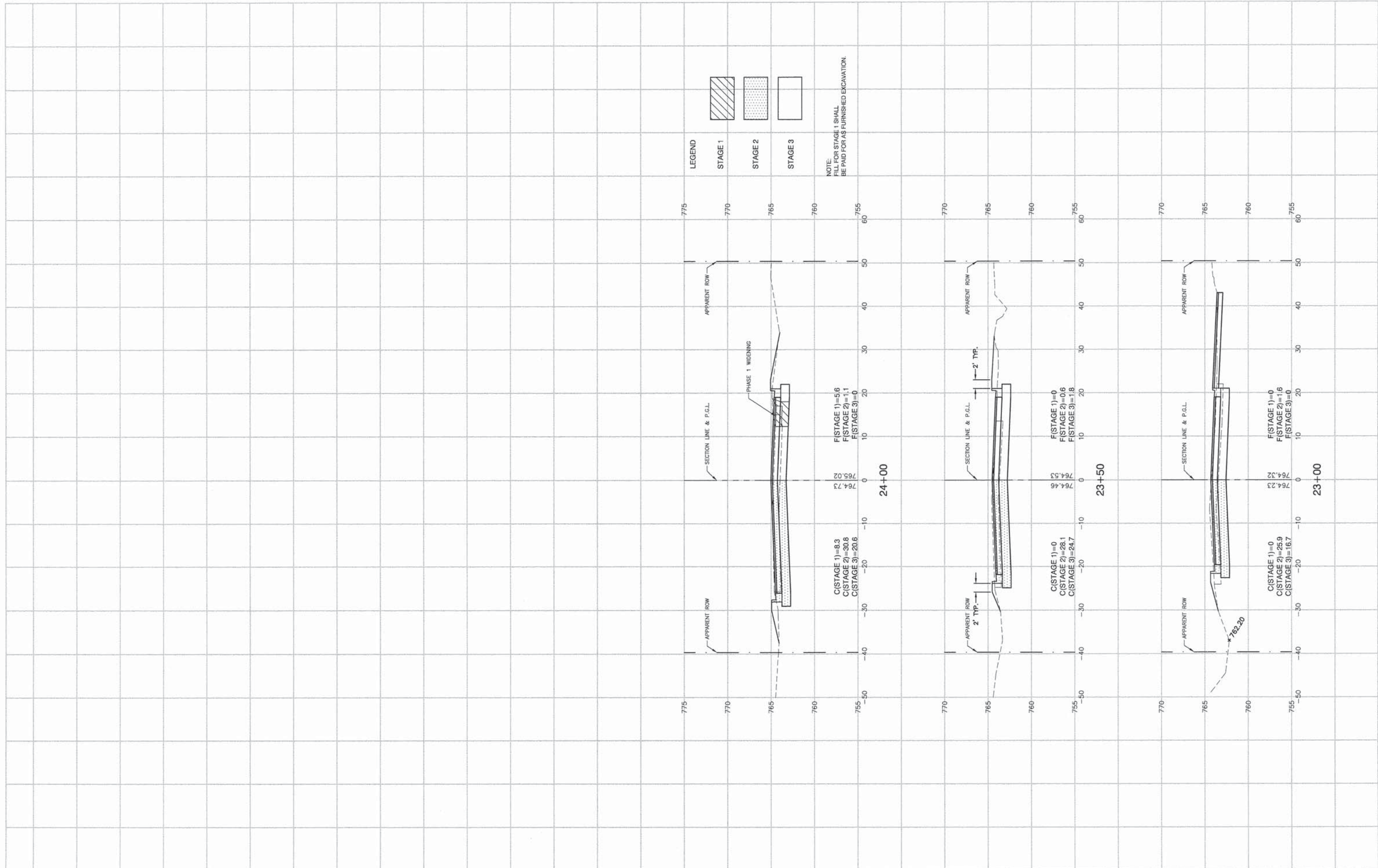
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| FILE NAME = 04273_02-XSCT-01 -X14 | USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | | CHECKED -- | REVISED -- |
| | PLOT SCALE = | DRAWN -- RG | REVISED -- |
| | PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
CROSS SECTIONS**

SCALE: H 1"=10' V 1"=5' SHEET NO. 91 OF 96 SHEETS STA. 20+50 TO STA. 22+50

| | | | | |
|--------------------------------|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 91 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | CONTRACT NO. 63147 | |
| FED. AID PROJECT M-8003 (562) | | | | |



FILE NAME = 04273_02-XSCT-01 - X15
 USER NAME =
 PLOT SCALE =
 PLOT DATE = 05-30-13

| | |
|-----------------|-----------------------------------|
| DESIGNED -- HLG | REVISOR -- 01-22-09 - IDOT REVIEW |
| CHECKED -- | REVISOR -- |
| DRAWN -- RG | REVISOR -- |
| CHECKED -- AG | REVISOR -- |

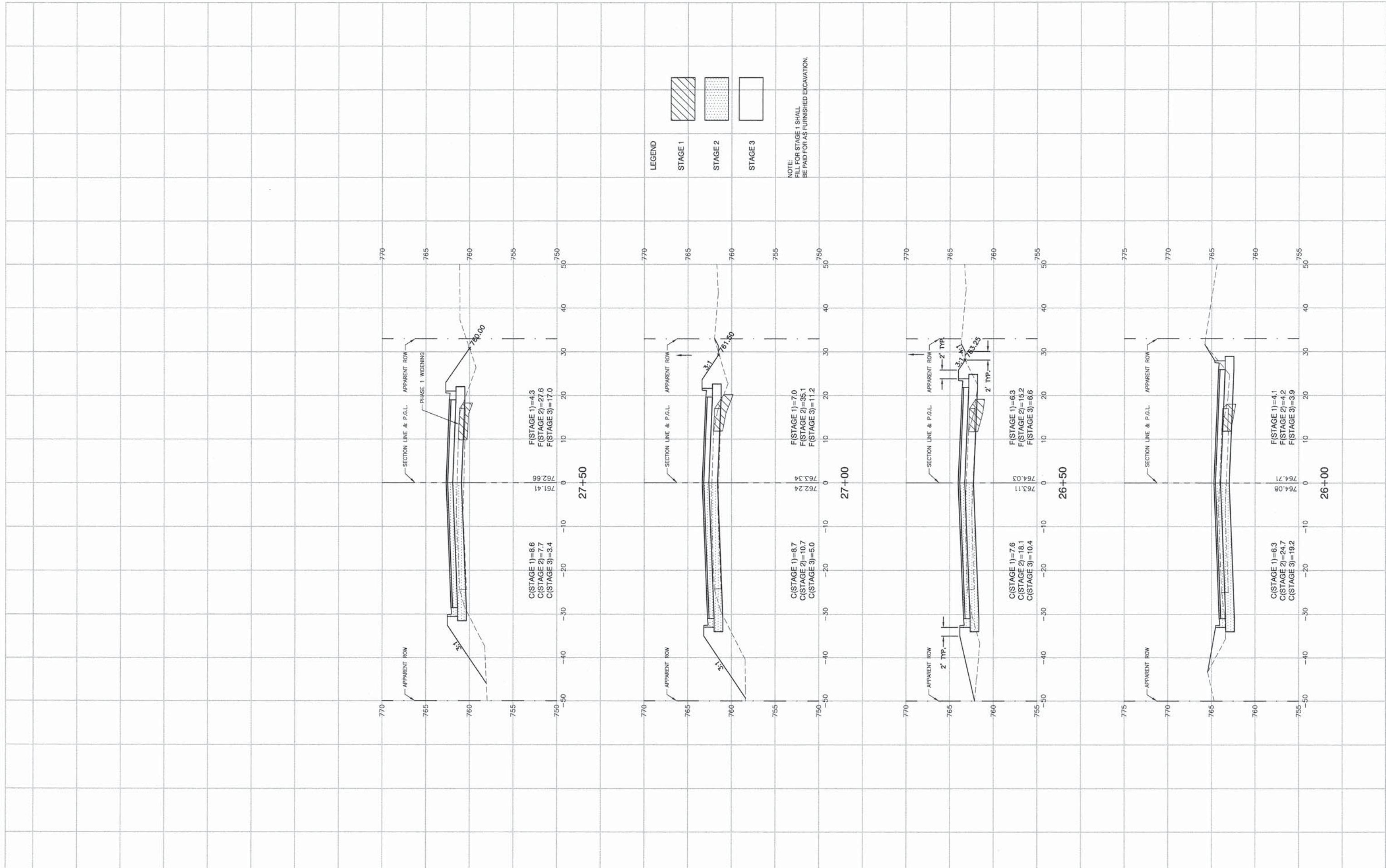
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**143RD STREET AND LEMONT ROAD
 INTERSECTION RECONSTRUCTION
 CROSS SECTIONS**

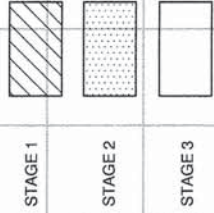
SCALE: H 1"=10' V 1"=5'
 SHEET NO. 92 OF 96 SHEETS
 STA. 23+00 TO STA. 24+00

| | | | | |
|--------------------------------|------------------------|-------------|-------------------------------|--------------|
| F.A.U. RTE. 1600 | SECTION 04-00003-00-CH | COUNTY WILL | TOTAL SHEETS 96 | SHEET NO. 92 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | FED. AID PROJECT M-8003 (562) | |

CONTRACT NO. 63147



LEGEND



NOTE:
BILL FOR STAGE 1 SHALL
BE PAID FOR AS FURNISHED EXCAVATION.

FILE NAME = 04273_02-XSCT-01 - X16

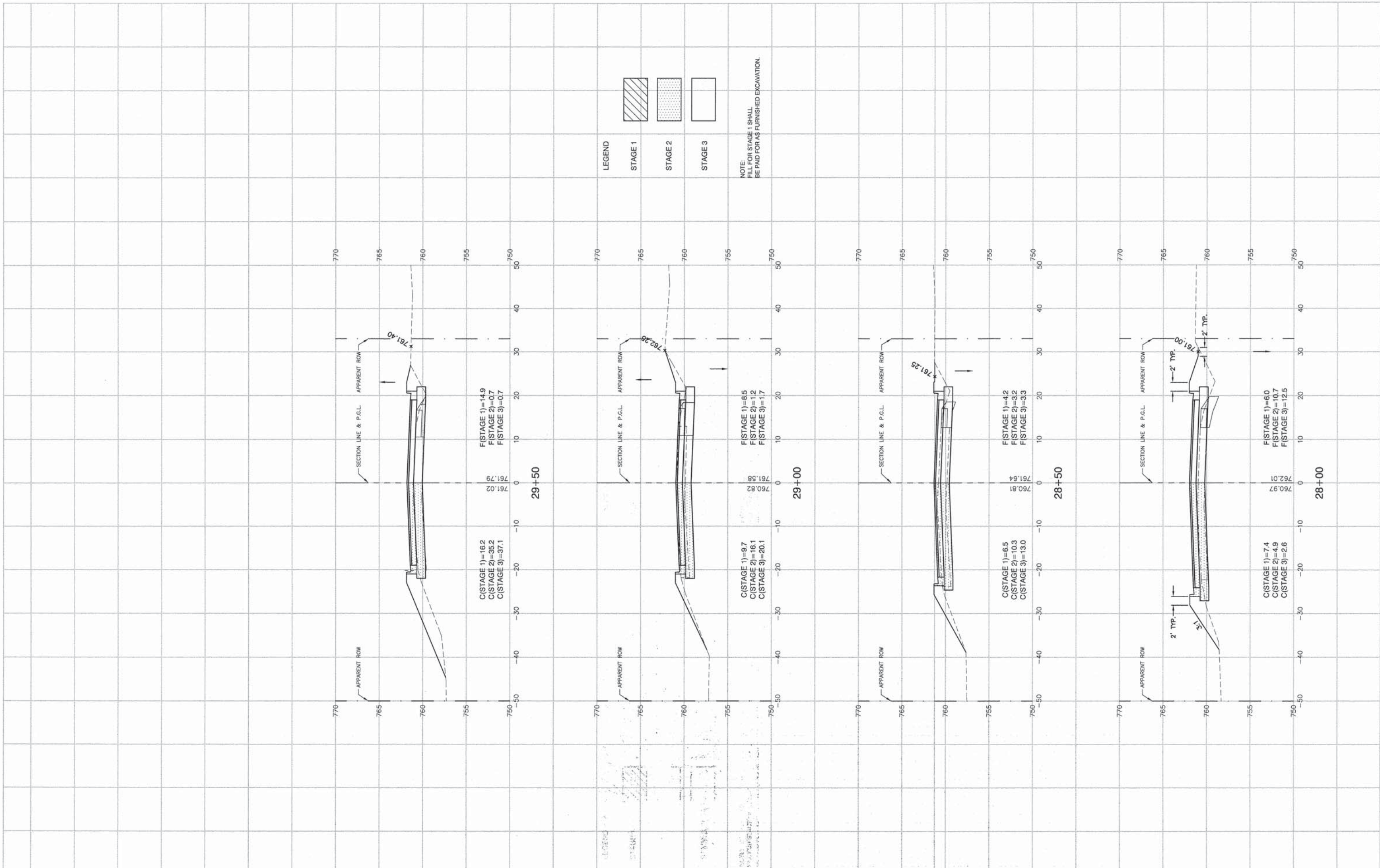
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|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- RG | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
CROSS SECTIONS

SCALE: H 1"=10' V 1"=5' SHEET NO. 93 OF 96 SHEETS STA. 26+00 TO STA. 27+50

| | | | | |
|--------------------------------|----------------|--------|-------------------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 93 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | FED. AID PROJECT M-8003 (562) | |



FILE NAME = 04273_02XSCT-01 - X17

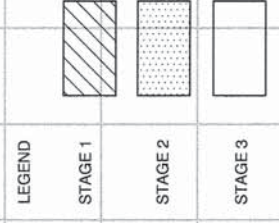
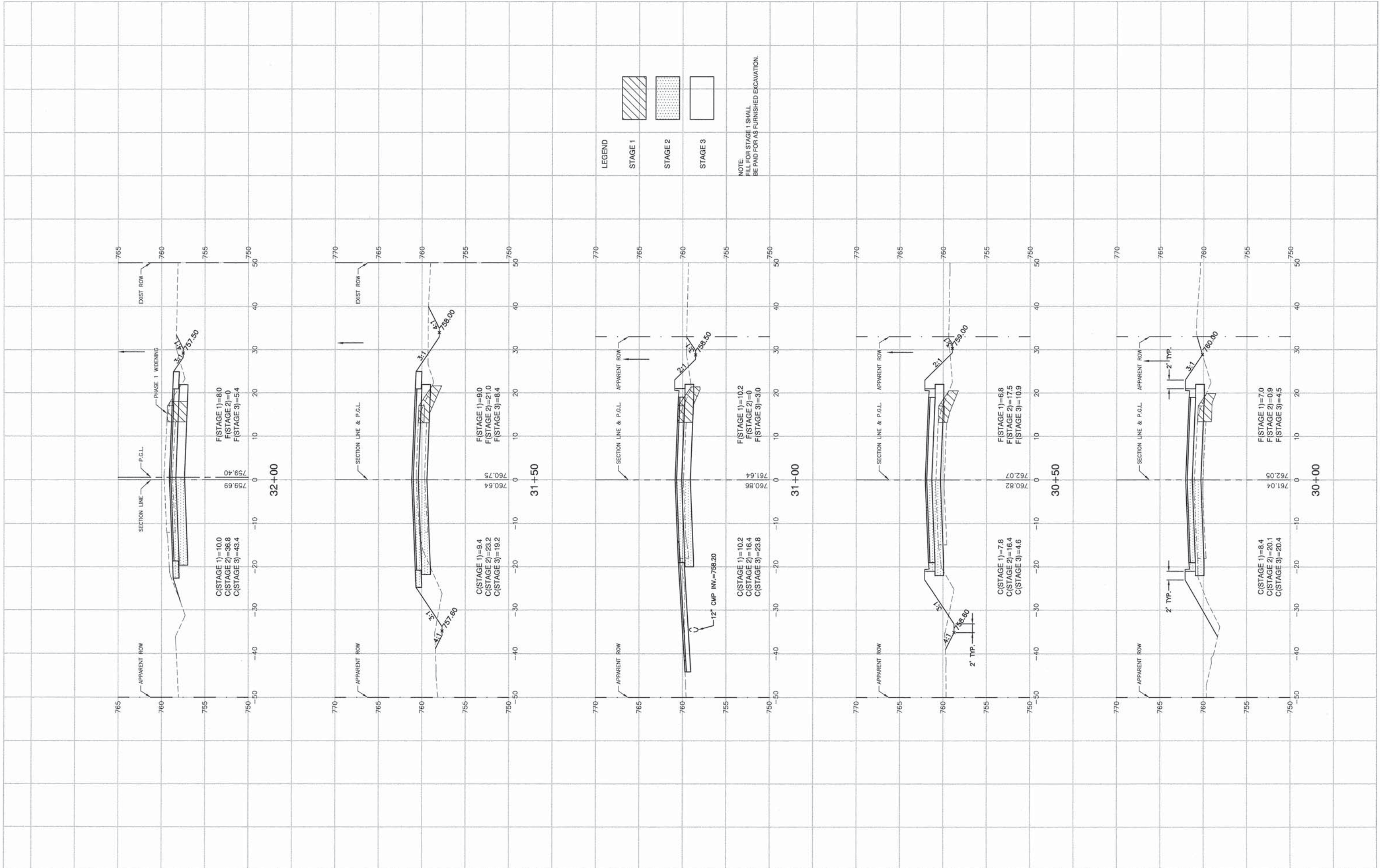
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|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- RG | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
CROSS SECTIONS

SCALE: H 1"=10' V 1"=5' SHEET NO. 94 OF 96 SHEETS STA. 28+00 TO STA. 29+50

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 94 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |



NOTE:
FILL FOR STAGE 1 SHALL
BE PAID FOR AS FURNISHED EXCAVATION.

FILE NAME = 04273_02XSCT-01 - X18

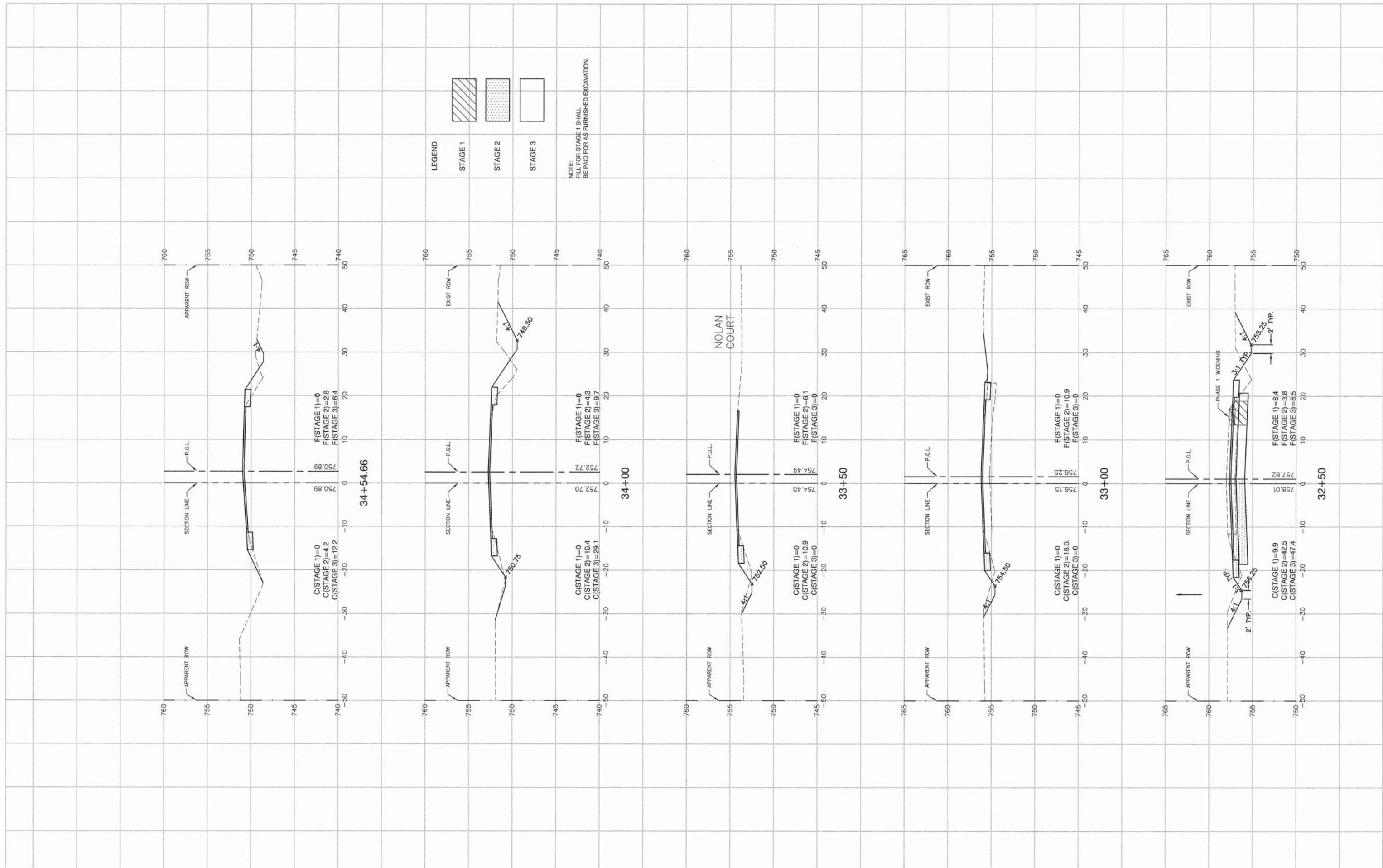
| | | |
|----------------------|-----------------|-----------------------------------|
| USER NAME = | DESIGNED -- HLG | REVISED -- 01-22-09 - IDOT REVIEW |
| | CHECKED -- | REVISED -- |
| PLOT SCALE = | DRAWN -- RG | REVISED -- |
| PLOT DATE = 05-30-13 | CHECKED -- AG | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
INTERSECTION RECONSTRUCTION
CROSS SECTIONS

SCALE: H 1"=10' V 1"=5' SHEET NO. 95 OF 96 SHEETS STA. 30+00 TO STA. 32+00

| | | | | |
|--|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 95 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | CONTRACT NO. 63147 | |



FILE NAME = 04273_02XSCT-01 - X19
 USER NAME =
 PLOT SCALE =
 PLOT DATE = 05-30-13

| | |
|----------------|----------------------------------|
| DESIGNED = HLG | REVISOR = 01-22-09 - IDOT REVIEW |
| CHECKED = | REVISOR = |
| DRAWN = RG | REVISOR = |
| CHECKED = AG | REVISOR = |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

143RD STREET AND LEMONT ROAD
 INTERSECTION RECONSTRUCTION
 CROSS SECTIONS
 SCALE: H 1"=10' V 1"=5'
 SHEET NO. 96 OF 96 SHEETS
 STA. 32+50 TO STA. 34+54.66

| | | | | |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1600 | 04-00003-00-CH | WILL | 96 | 96 |
| CONTRACT NO. 63147 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-8003 (562) | | | | |