

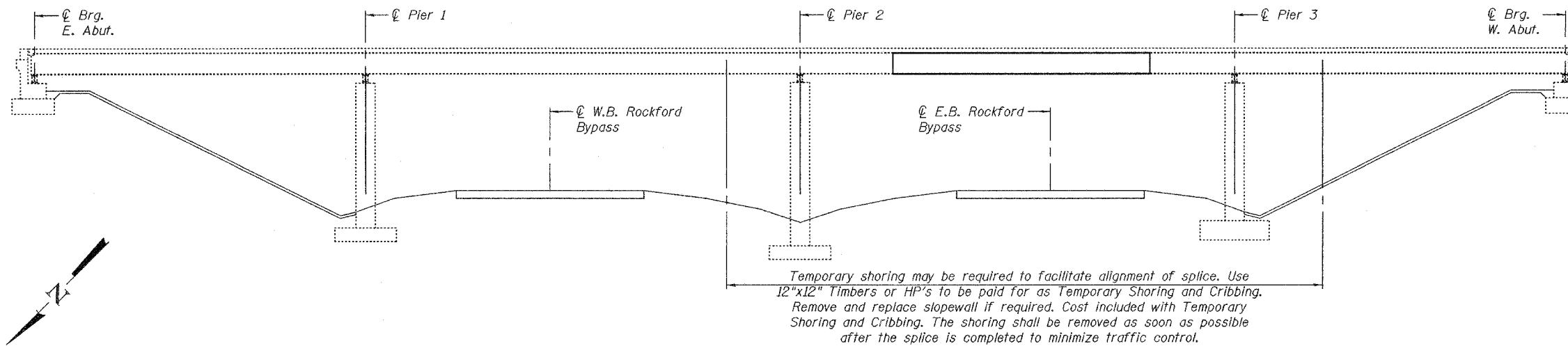
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET	SHEET	SHEET NO. 1 2 SHEETS
		Winnebago	13	12	
FED. ROAD DIST. NO. 7		S.L.LINES		FED. AID PROJECT	

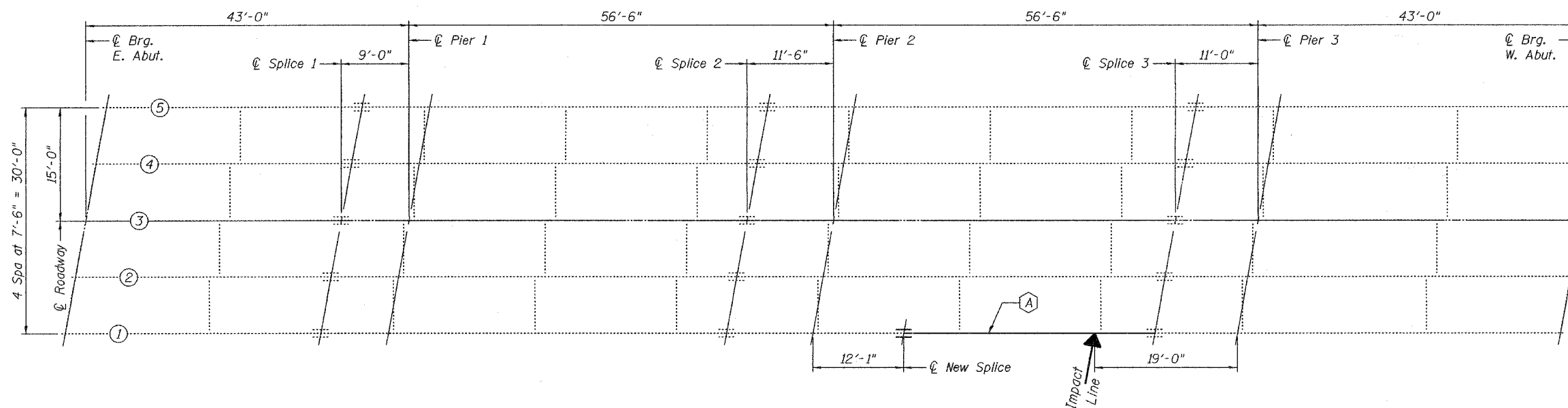
Contract Number: 64D49

**NOTES**

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.  
Fasteners shall be high strength bolts. Flange splice holes shall be  $\frac{15}{16}$ "  $\phi$  for  $\frac{7}{8}$ "  $\phi$  bolts. Web splice holes shall be  $\frac{13}{16}$ "  $\phi$  for  $\frac{3}{4}$ "  $\phi$  bolts.  
The Contractor shall provide support and/or shoring systems for the slab and beam in the area of existing beam removal. See Special Provisions "Temporary Shoring and Cribbing" and "Temporary Slab Support System."  
After the new beam is in its final position and/or beam straightening operations have been completed, the Engineer in the field shall check to see that the top flange is tight against the slab. If not, the Contractor shall inject epoxy between the existing concrete deck and the top flange of the beam. See Special Provision "Epoxy Injection".  
Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.  
The Inorganic Zinc Rich Primer / Acrylic / Acrylic Paint System shall be used for shop and field painting of new structural steel except where otherwise noted. The color of the final finish coat for all interior steel surfaces shall be gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Blue Munsell No. 10B 3/6. See Special Provision "Cleaning and Painting New Metal Structures".  
The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.  
Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.  
Load carrying components designated "NTR" shall conform to the Supplemental Requirements for Notch Toughness, Zone 2.  
Diaphragm connection holes shall be  $\frac{15}{16}$ "  $\phi$  for  $\frac{3}{4}$ "  $\phi$  bolts. Two hardened washers shall be required at diaphragm connections.  
Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.  
Cost of removal and re-installation of all members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included with Furnishing and Erecting Structural Steel.  
Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".  
No field welding is permitted except as specified in the contract documents.



**ELEVATION**



**PLAN**

(A) Remove and Replace W33x130 Section

All existing beams are W33x130  
All interior diaphragms are W16x36  
All end diaphragms are W12x40

**BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	0.8
Concrete Superstructure	Cu. Yd.	0.8
Furnishing and Erecting Structural Steel	Pound	4,860
Temporary Slab Support System	L.S.	1
Structural Steel Removal	Pound	4,430
Temporary Shoring and Cribbing	L.S.	1

**PLAN AND ELEVATION**  
F.A. RT. 194  
WINNEBAGO COUNTY  
SN 101-0094

DESIGNED	<i>[Signature]</i>
CHECKED	<i>[Signature]</i>
DRAWN	<i>[Signature]</i>
CHECKED	SJB AIB

June 21, 2007  
EXAMINED *[Signature]*  
PASSED *[Signature]*  
REPAIR DIVISION UNIT CHIEF  
ENGINEER OF BRIDGES AND STRUCTURES



Expires: November 30, 2008