

FAU ROUTE NO. 3597	SECTION 07-00119-00-RS	COUNTY COOK	TOTAL SHEETS 10	SHEET NO. 1
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CONTRACT NO.: 83928

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS**

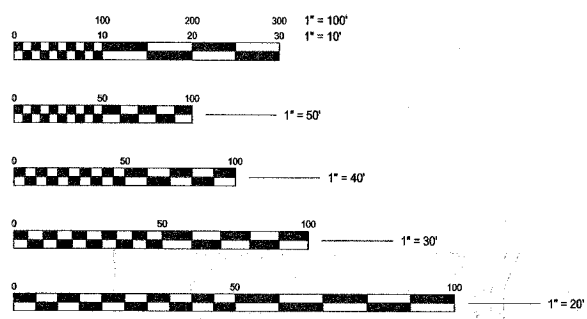
**PLANS FOR PROPOSED
FEDERAL AID PROJECT
FAU 3597 / PARK AVENUE
FROM DIXIE HIGHWAY (FAU 2843) TO 183rd STREET (FAU 1622)
SECTION : 07-00119-00-RS
PROJECT NO.: M-8003 (815)
COOK COUNTY
JOB NO.: C-91-299-07**

INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET, INDEX & LOCATION MAP
2	SUMMARY OF QUANTITIES AND GENERAL NOTES
3	EXISTING TYPICAL SECTIONS
4	PROPOSED TYPICAL SECTIONS
5	PARK AVENUE (FAU ROUTE 3597) EXISTING ROADWAY ROADWAY AND PAVEMENT MARKING PLAN, STA. 0+00- STA. 28+45.52
6	IDOT DISTRICT ONE TYPICAL PAVEMENT MARKING DETAIL
7	IDOT BUTT JOINT & BITUMINOUS TAPER DETAIL
8	FRAME AND LID ADJUSTMENT WITH MILLING
9	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
10	CURB AND GUTTER REMOVAL AND REPLACEMENT

STANDARD NO.

SHEET NO.	TITLE
000001-04	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-04	CURB RAMPS FOR SIDEWALKS
442201-02	CLASS C AND D PATCHES
606001-03	CONCRETE CURB TYPE-B AND COMBINATION CONCRETE CURB & GUTTER
701501-03	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701801-03	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
702001-06	TRAFFIC CONTROL DEVICES

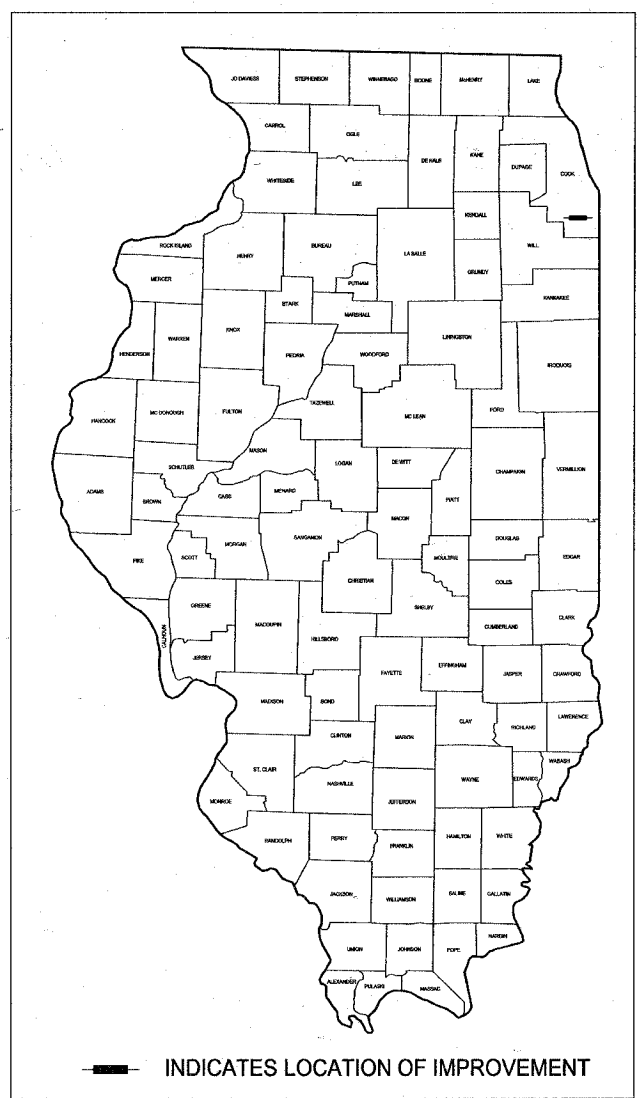


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED SIZE PLANS, THE ABOVE SCALES SHALL BE USED.



CALL JULIE (800) 892-0123
WITH THE FOLLOWING INFORMATION:
COUNTY = COOK
48 HOURS BEFORE YOU DIG
EXCLUDING SAT., SUN. & HOLIDAYS

CONTRACT NO.: 83928

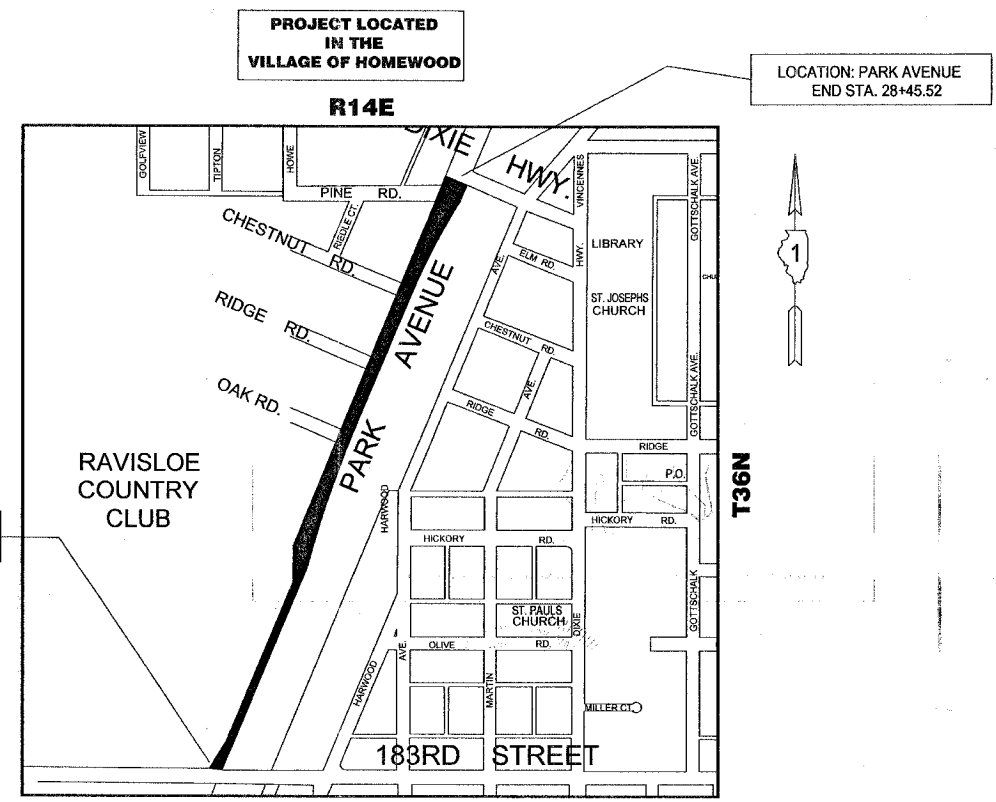


INDICATES LOCATION OF IMPROVEMENT



VILLAGE ENGINEER
* LICENSE EXPIRES 11-30-07 *

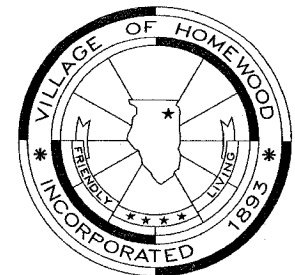
DATE: 4-12-07



LOCATION MAP
NOT TO SCALE

TOTAL NET & GROSS LENGTH OF PROJECT = 2,845 LINEAL FEET (0.538 MILES)

PARK AVENUE DESIGN DESIGNATION
POSTED SPEED: 25 MPH
ADT: 2,808 (2005)



VILLAGE OF HOMEWOOD
Department of Public Works
Engineering Division
17755 S. Ashland Avenue
Homewood, IL 60445
(708) 206-3470

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
APPROVED	20 07 <i>[Signature]</i> VILLAGE PRESIDENT
PASSED	MAY 31 20 07 <i>[Signature]</i> CHRISTOPHER HOLT DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	June 1 20 07 <i>[Signature]</i> DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

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THE STATE OF ILLINOIS

FEDERAL DESIGN ENGINEER: ABIGAIL WILGREEN, (847) 705-4233

GENERAL NOTES

SUMMARY OF QUANTITIES

ITEM NO.	PAY ITEM	UNIT	1000-2A TOTAL
20101300	TREE PRUNING (1 to 10 INCH DIAMETER)	EA	10
21101615	TOPSOIL FURNISH AND PLACE, 4"	SY	183
25000400	NITROGEN FERTILIZER NUTRIENT	LBS	5
25000500	PHOSPHORUS FERTILIZER NUTRIENT	LBS	5
25000600	POTASSIUM FERTILIZER NUTRIENT	LBS	5
25200110	SODDING, SALT TOLERANT	SY	183
25200200	SUPPLEMENTAL WATERING	UNIT	5
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	22
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GAL	1,024
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	430
40603310	HOT-MIX ASPHALT, SURFACE COURSE, MIX "C", N50	TON	860
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SY	237
42400800	DETECTABLE WARNINGS	SF	25
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SY	10,004
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FT	441
44201711	CLASS D PATCHES, TYPE IV, 5"	SY	78
44201747	CLASS D PATCHES, TYPE IV, 8"	SY	236
44300300	AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A	SY	10,241
48100500	AGGREGATE SHOULDERS, TYPE A, 6"	SY	376
60255700	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EA	5
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EA	12
60263010	INLETS TO BE RECONSTRUCTED WITH NEW TYPE 2 FRAME AND GRATE	EA	7
60266600	VALVE BOXES TO BE ADJUSTED	EA	6
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EA	17
67100100	MOBILIZATION	LS	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LS	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LS	1
70300100	SHORT-TERM PAVEMENT MARKING	FT	7,500
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FT	11,492
* 78001130	PAINT PAVEMENT MARKING - LINE 6"	FT	145
* 78001150	PAINT PAVEMENT MARKING - LINE 12"	FT	104
* 78001180	PAINT PAVEMENT MARKING - LINE 24"	FT	40
XX001306	SIDEWALK REMOVAL AND REPLACEMENT	SF	805
XX003435	PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT	SY	28
XX004238	BITUMINOUS DRIVEWAY REMOVAL AND REPLACEMENT	SY	76

* SPECIALTY ITEM

ALL REFERENCES TO THE "VILLAGE" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE VILLAGE OF HOMEWOOD.

THE FINISHED HMA SURFACE SHALL BE CONSTRUCTED 1/4 - INCH ABOVE THE GUTTER FLAG.

THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE VILLAGE AND THE ENGINEER DO NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING THE CONSTRUCTION OPERATION SO AS NOT TO DAMAGE THEM. IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 105.07 OF THE "STANDARD SPECIFICATIONS," THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE "STANDARD SPECIFICATIONS."

THOSE EXISTING TRAFFIC SIGNS WHICH ARE SO DESIGNATED BY THE ENGINEER SHALL BE REMOVED, STORED AND SUBSEQUENTLY RELOCATED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE VILLAGE. IN ADDITION, ANY SIGNS WHICH ARE DAMAGED BEYOND REPAIR BY THE CONTRACTOR AS DETERMINED BY THE ENGINEER SHALL BE REPLACED IN KIND BY THE CONTRACTOR AND TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE VILLAGE.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE PLANS AND THE IDOT STANDARDS FOR TRAFFIC CONTROL AND PROTECTION.

WHERE THE PROPOSED PAVEMENT ABUTS EXISTING PAVEMENT TO REMAIN IN PLACE (BEGIN, END AND LIMITS OF CONSTRUCTION), EXISTING DRIVEWAY PAVEMENT, SIDEWALK TO REMAIN IN PLACE. THE EXISTING PAVEMENT SHALL BE SAW CUT TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING SURFACES. THIS SAW CUT SHALL BE INCLUDED IN THE COST OF THE PAY ITEM "HMA SURFACE REMOVAL - BUTT JOINT".

CURB & GUTTER, DRIVEWAY AND SIDEWALK REMOVAL AND REPLACEMENT SHALL BE LIMITED TO WORKING ON ONE SIDE OF THE STREET IN EACH DIRECTION AT ALL TIMES TO MINIMIZE CONGESTION. NO CURB & GUTTER SHALL BE REMOVED ON FRIDAYS. CONCRETE SHALL HAVE A MINIMUM OF 72 HOURS CURING TIME AND DRIVEWAYS SHALL BE ACCESSIBLE PRIOR TO REMOVING EXISTING CURB ON THE OTHER SIDE OF THE STREET.

ALL FRAME AND GRATES TO BE REPLACED SHALL REMAIN THE PROPERTY OF THE VILLAGE OF HOMEWOOD AND SHALL BE DELIVERED TO THE PUBLIC WORKS BUILDING BY THE CONTRACTOR.

THE CONTRACTOR(S) AND THEIR SUBCONTRACTOR(S) SHALL CONTACT J.U.L.I.E. (800) 892-0123 AND THE VILLAGE OF HOMEWOOD/DEPARTMENT OF PUBLIC WORKS (708) 206-3470 BEFORE DIGGING.

THE CONTRACTOR SHALL NOTIFY RESIDENTS 24 HOURS PRIOR TO BEGINNING WORK SO THAT CARS CAN BE REMOVED FROM DRIVEWAYS. WORK MUST BE PLANNED TO MINIMIZE THE TIME DRIVEWAYS ARE OUT OF SERVICE. WORK SHALL BE SCHEDULED SO THAT DRIVES PULLED ON THE FIRST WORKING DAY ARE FORMED AND POURED BY THE THIRD WORKING DAY. BEYOND THIS 3 DAY TIME IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE AT HIS OWN EXPENSE TEMPORARY AGGREGATE FOR ACCESS TO THE DRIVEWAY. NO CURB IN FRONT OF DRIVE SHALL BE PULLED ON FRIDAYS.

DEBRIS REMOVAL - MATERIALS RESULTING FROM THE VARIOUS CONSTRUCTION OPERATIONS SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGEMENT OF THE VILLAGE, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE VILLAGE WILL REMOVE DEBRIS AND THE CONTRACTOR SHALL BE BILLED ACCORDINGLY.

WATER SUPPLY - THE CONTRACTOR CAN OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A WATERING BAN IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR. WATER IS AVAILABLE AT 17755 S. ASHLAND AVENUE, MUNICIPAL SERVICE CENTER FIRE HYDRANT. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER. THE SUPPLY OF WATER BY THE CONTRACTOR SHALL BE INCLUDED IN THE COST OF SUPPLEMENTAL WATERING.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 40 MM (1 1/2 INCHES) WHERE THE SPEED LIMIT IS 80 KM/HR (45 MPH) OR LESS AND 25 MM (1 INCH) WHERE THE SPEED LIMIT IS GREATER THAN 80 KM/HR (45 MPH). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 75 MM (3 INCHES) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

ALL TREES SHALL BE SAVED. THE CONTRACTOR SHALL ERECT EITHER TEMPORARY FENCING OR TREE TRUNK PROTECTION AS DIRECTED BY THE ENGINEER PER ARTICLE 201.5 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY AND ALL DAMAGE PER ARTICLE 201.07 OF THE STANDARD SPECIFICATIONS. TREE PROTECTION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE WORK.

PROTECTIVE COAT SHALL BE APPLIED TO THE SURFACE OF ALL COMBINATION CONCRETE AND GUTTER AND PORTLAND CEMENT CONCRETE SIDEWALK AND DRIVEWAYS. THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 420.18 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE WORK TO WHICH IT IS APPLIED.

THE QUANTITIES FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, CLASS D PATCHES OF THE TYPE AND SIZE SPECIFIED, SIDEWALK REMOVAL AND REPLACEMENT, PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT AND BITUMINOUS DRIVEWAY REMOVAL AND REPLACEMENT SHALL NOT EXCEED THE QUANTITY LISTED IN THE SUMMARY OF QUANTITIES.

PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL PROVIDE TO THE VILLAGE A VIDEO TAPE COVERING FROM RIGHT-OF-WAY TO RIGHT-OF-WAY OF ALL ROADWAYS TO BE IMPROVED. THIS WORK SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

WHEN REMOVING PAVEMENT, CURB AND GUTTER, SIDEWALK, DRIVEWAY PAVEMENT, PARKING LOT PAVEMENT, AND/OR OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKER WHICH MIGHT DISTURB UNDERGROUND PUBLIC UTILITIES WILL NOT BE PERMITTED.

PRIOR TO THE START OF CONSTRUCTION ACTIVITIES, THE ENGINEER AND CONTRACTOR, JOINTLY, SHALL PERFORM A VISUAL INSPECTION OF VILLAGE DRAINAGE AND UTILITY STRUCTURES TO DETERMINE THE AMOUNT OF EXISTING DEBRIS IN EACH STRUCTURE. THE CONTRACTOR WILL BE REQUIRED TO CLEAN THOSE STRUCTURES WITH DEBRIS THAT WERE CLEAN AT THE BEGINNING OF CONSTRUCTION AT NO ADDITIONAL COST TO THE CONTRACT.

MILLING OF PAVEMENT SHALL BE DONE SO AS TO NOT DAMAGE THE ADJACENT CURB OR STRUCTURES. REMOVAL ADJACENT TO THESE STRUCTURES SHALL BE ACCOMPLISHED TO THE SATISFACTION OF THE ENGINEER AND MAY REQUIRE HANDWORK.



VILLAGE OF HOMEWOOD ENGINEERING DIVISION

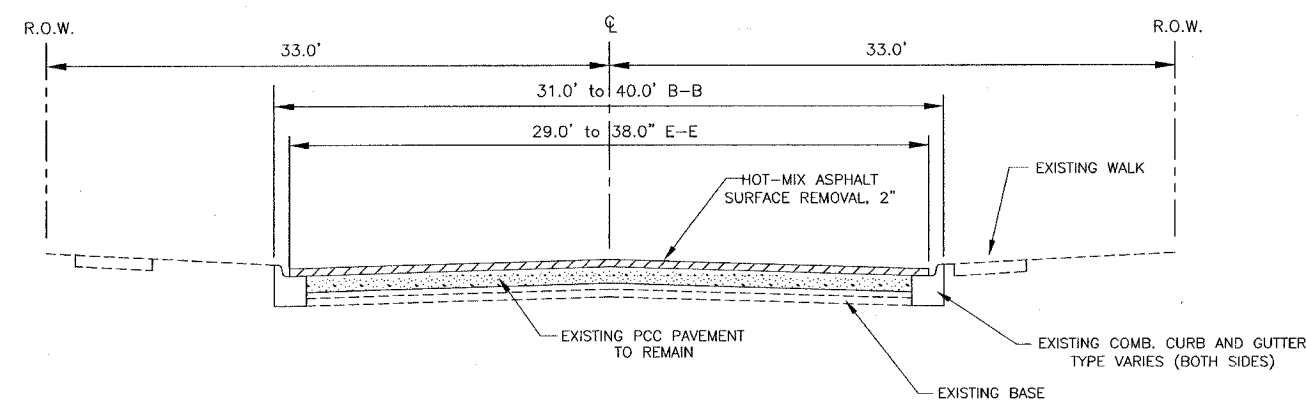
DESIGNED BY: **D.J.A.**
DRAWN BY: **D.J.A.**
CHECKED BY: **M.M.**

REVISIONS			
NO.	DATE	BY	DESCRIPTION
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2	4-11-07	DJA	IDOT REVISIONS

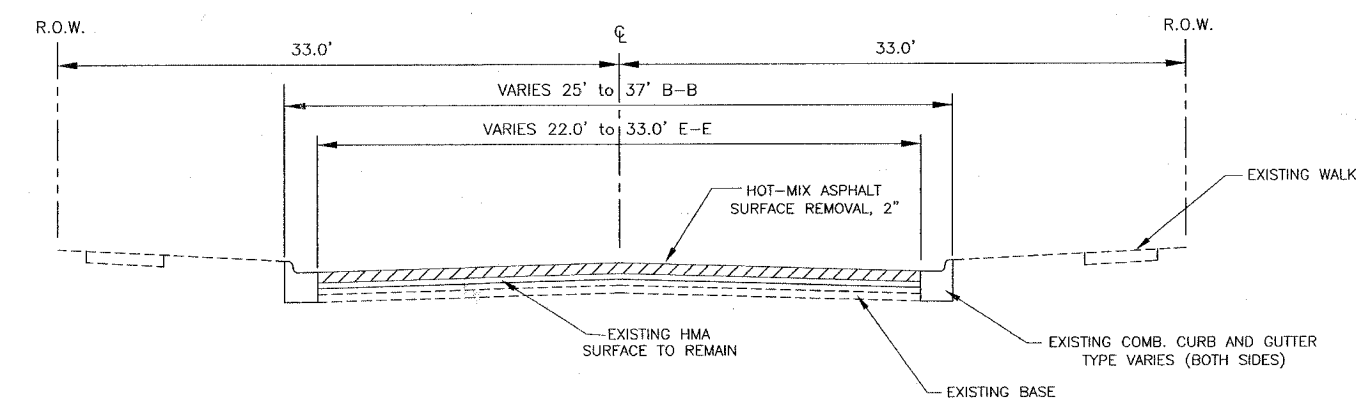
VILLAGE OF HOMEWOOD
PARK AVENUE L.A.P.P. PROJECT

SUMMARY OF QUANTITIES AND GENERAL NOTES

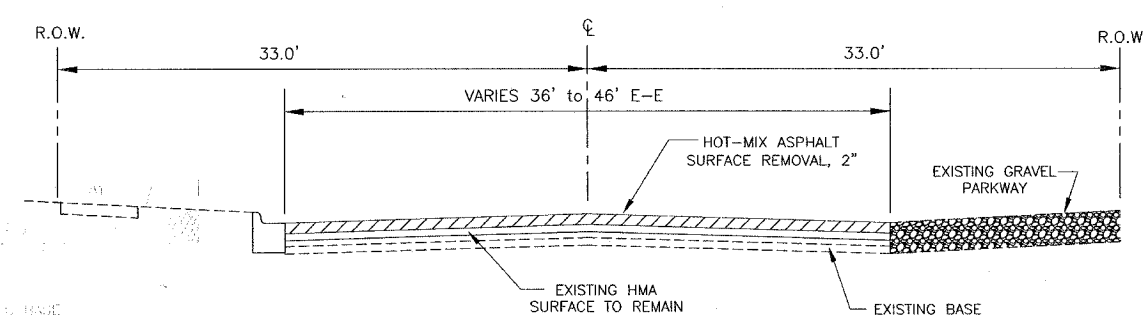
DATE: 04-11-07 SCALE: NOT TO SCALE



EXISTING TYPICAL SECTION
STA. 0+00 to STA. 19+00



EXISTING TYPICAL SECTION
STA. 24+75 to STA. 28+45.52



EXISTING TYPICAL SECTION
STA. 19+00 to STA. 24+75



VILLAGE OF HOMEWOOD
ENGINEERING DIVISION

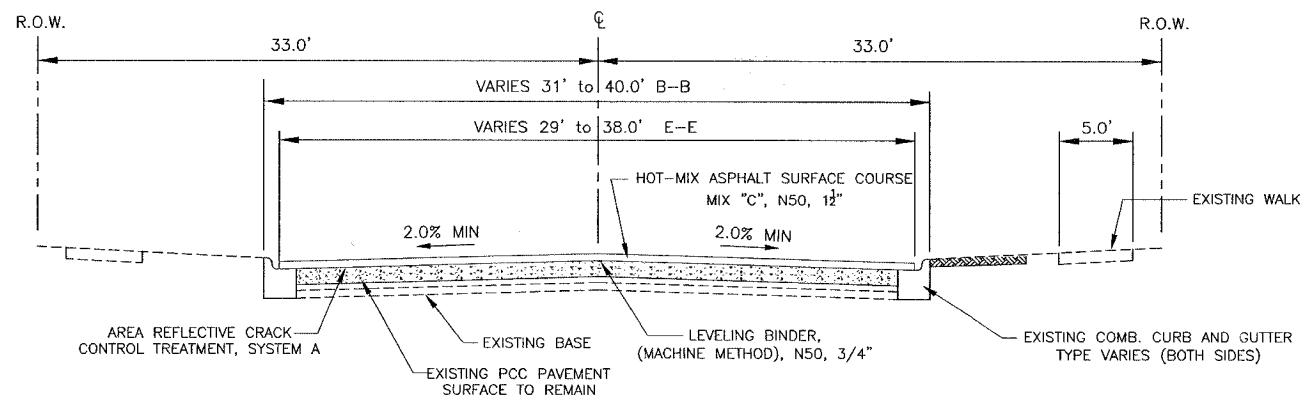
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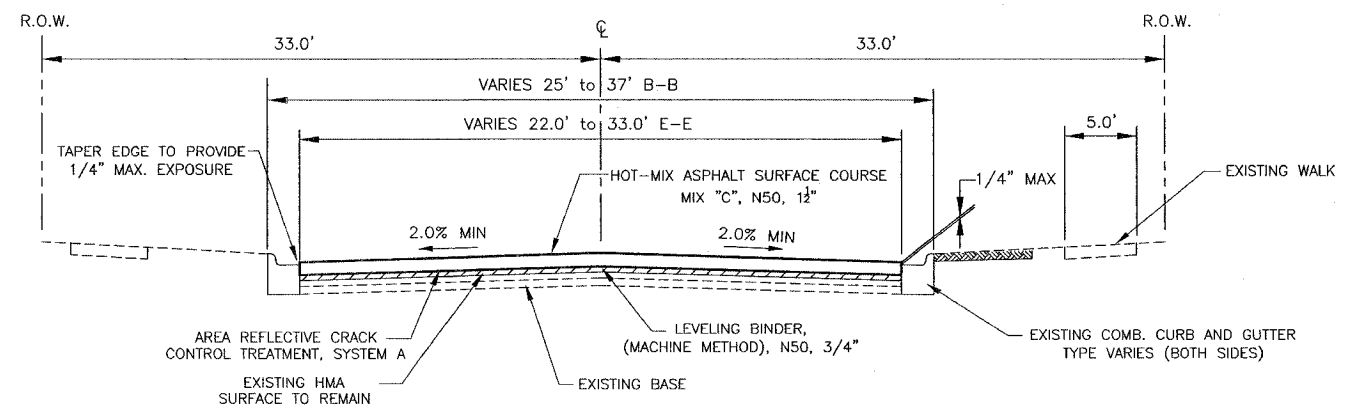
VILLAGE OF HOMEWOOD
PARK AVENUE L.A.P.P. PROJECT
PARK AVENUE (FAU ROUTE 3597)
EXISTING TYPICAL SECTIONS
DATE: 04-11-07 SCALE: NOT TO SCALE

SHEET
3
OF 10

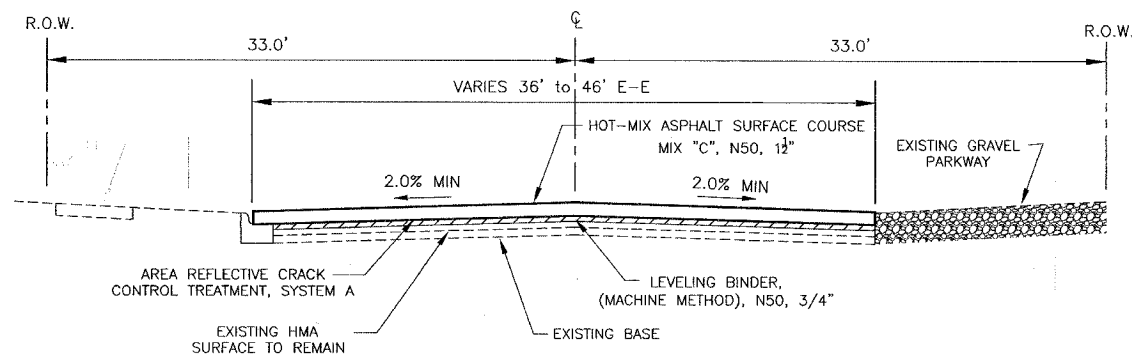
ILLINOIS PROJECT *
CONTRACT NO.: 83928



PROPOSED TYPICAL SECTION
STA. 0+00 to 19+00



PROPOSED TYPICAL SECTION
STA. 24+75 to 28+45.52



PROPOSED TYPICAL SECTION
STA. 19+00 to 24+75

HOT-MIX ASPHALT MIXTURE TABLE

ITEM	AC TYPE	VOIDS
LEVELING BINDER (MACHINE METHOD), N50 (IL-9.5mm)	PG 64-22/58-22*	4% @ 50 GYR
HOT-MIX ASPHALT SURFACE COURSE MIX 'C', N50 (IL-9.5mm)	PG 64-22	4% @ 50 GYR
CLASS D PATCHES, TYPE IV, 5" or 8" HMA BINDER COURSE (IL-19mm)	PG 64-22/58-22*	4% @ 70 GYR
BITUMINOUS DRIVEWAY REMOVAL AND REPLACEMENT HOT-MIX ASPHALT SURFACE COURSE, MIX 'C', N50	PG 64-22	4% @ 50 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.



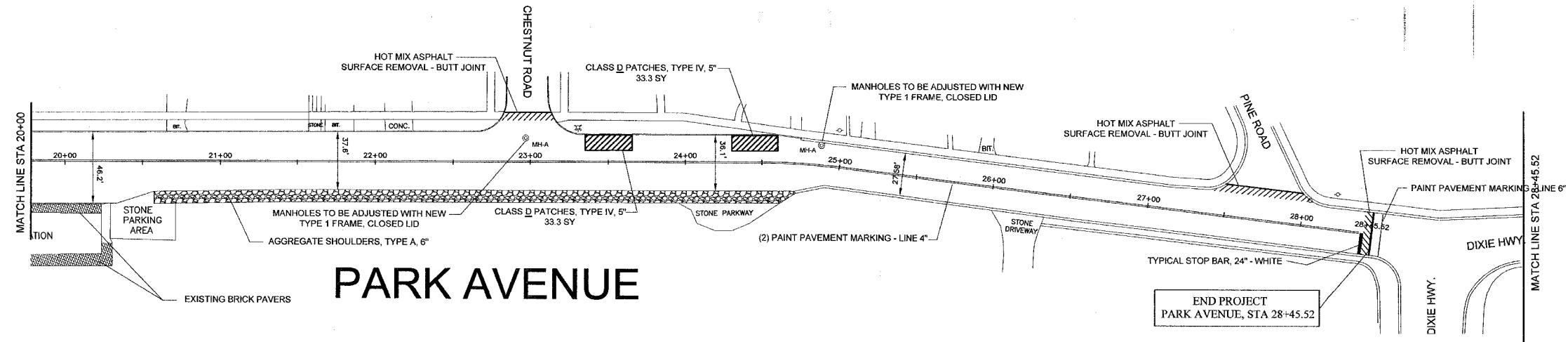
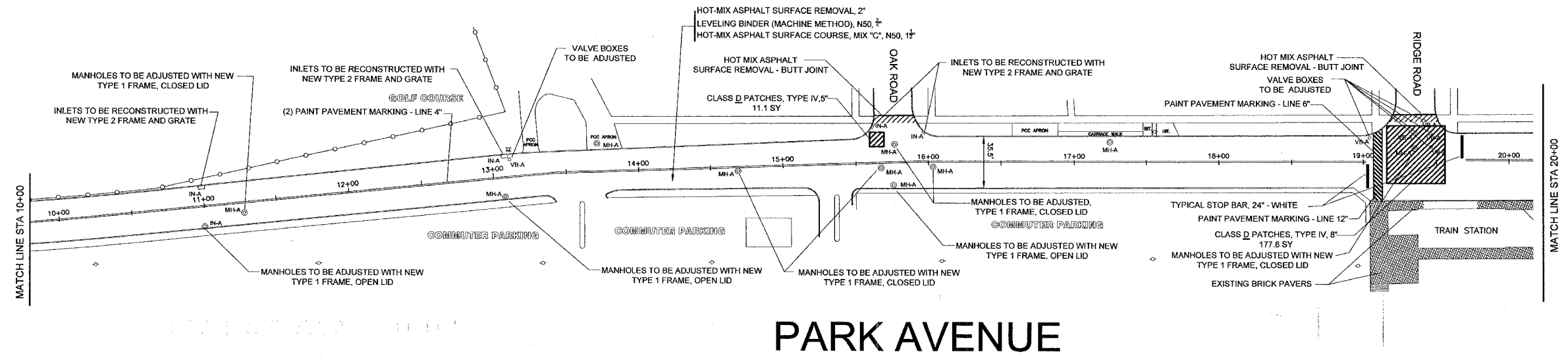
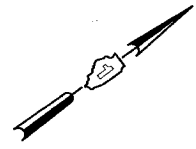
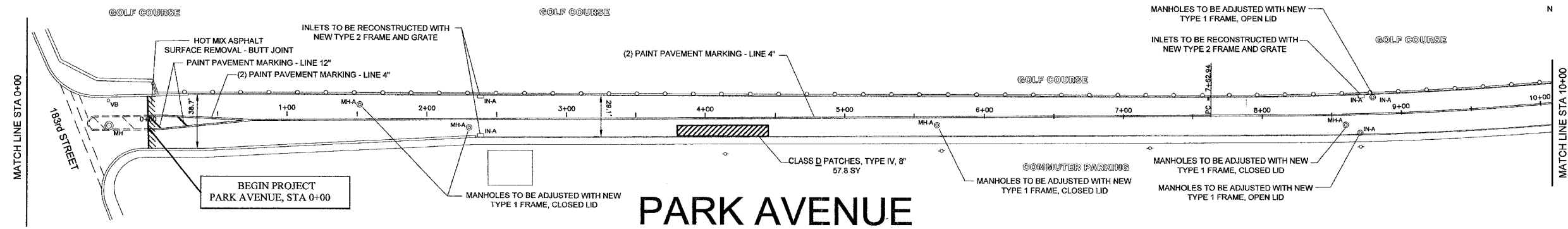
VILLAGE OF HOMEWOOD
ENGINEERING DIVISION

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1	4-04-07	DJA	IDOT REVISIONS	
2	4-11-07	DJA	IDOT REVISIONS	

VILLAGE OF HOMEWOOD
PARK AVENUE L.A.P.P. PROJECT
PARK AVENUE (FAU ROUTE 3597)
PROPOSED TYPICAL SECTIONS
DATE: 04-11-07 SCALE: NOT TO SCALE

SHEET
4
OF 10



**VILLAGE OF HOMEWOOD
ENGINEERING DIVISION**

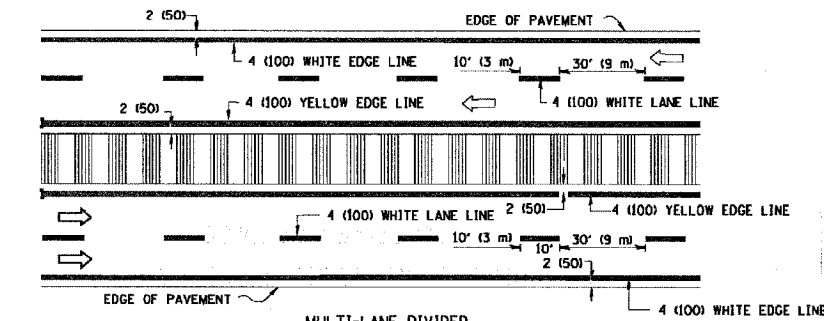
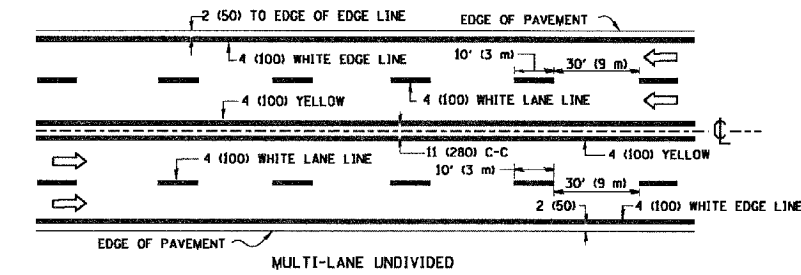
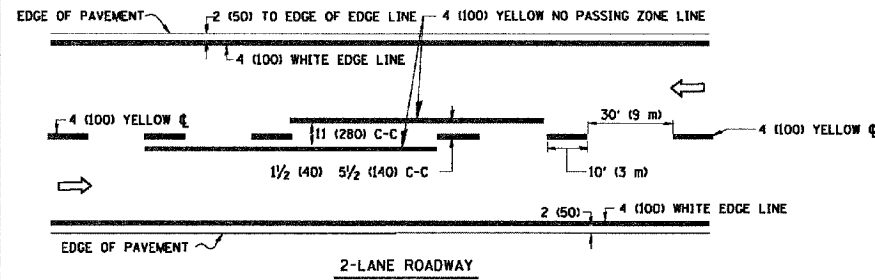
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REVISIONS			
NO.	DATE	BY	DESCRIPTION
1	4-04-07	DJA	IDOT REVISION
2	4-11-07	DJA	IDOT REVISION

VILLAGE OF HOMEWOOD
 PARK AVENUE I.A.P.P. PROJECT
**PARK AVENUE (FAU ROUTE 3597) EXISTING & PROPOSED
 ROADWAY AND PAVEMENT MARKING PLAN
 STA. 0+00 TO STA. 15+00**
 DATE: 04-11-07 SCALE: 1" = 40'

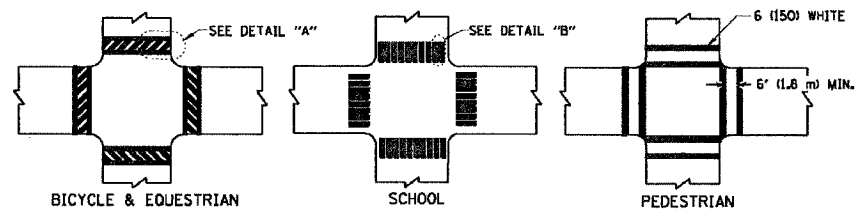
SHEET
5
 OF 10

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3597	07-00119-00-RS	COOK	10	6
STA.		TO STA.		FED. AID PROJECT

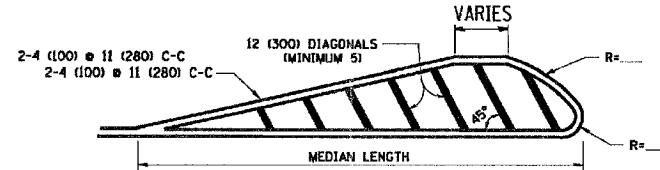
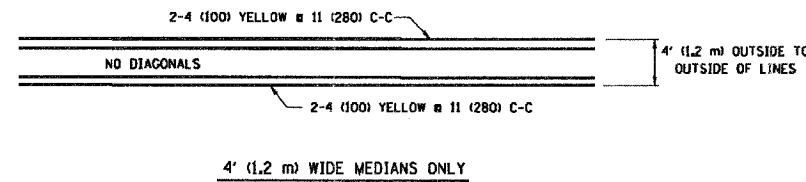


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



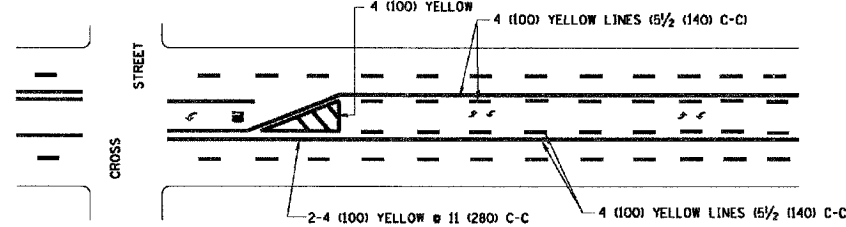
TYPICAL CROSSWALK MARKING



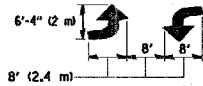
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

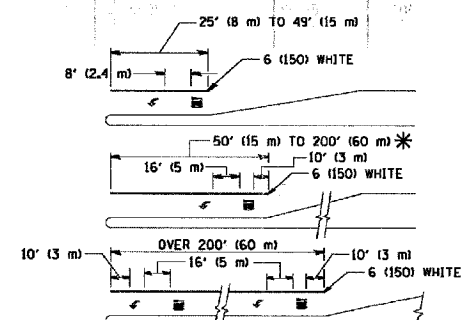


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

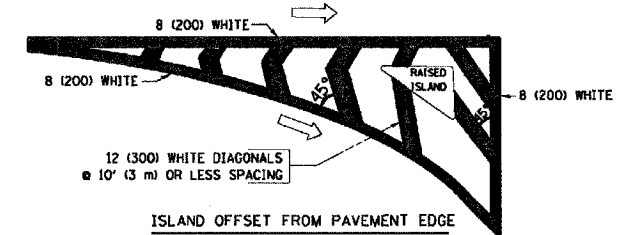


FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED. 1/4 AREA = 35.6 SQ. FT. (1.5 m²) 1/2 AREA = 20.8 SQ. FT. (1.9 m²)

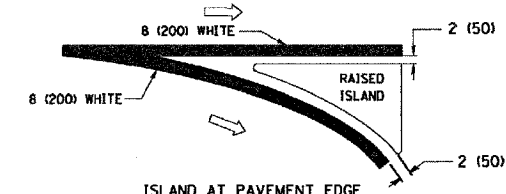
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100)	SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
LANE LINES	5 (125) ON FREEWAYS	SKIP-DASH	WHITE	
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS 8" (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
TWO WAY LEFT TURN MARKING	8" (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN)	2 @ 6 (150)	SOLID	WHITE	NOT LESS THAN 6' (1.8 m) APART
CROSSWALK LINES (BICYCLE & EQUESTRIAN)	12 (300) @ 45°	SOLID	WHITE	2' (600) APART
CROSSWALK LINES (LONGITUDINAL BARS (SCHOOL))	12 (300) @ 90°	SOLID	WHITE	2' (600) APART
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS				
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "R" = 3.6 SQ. FT. (0.33 m ²) EACH "X" = 54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

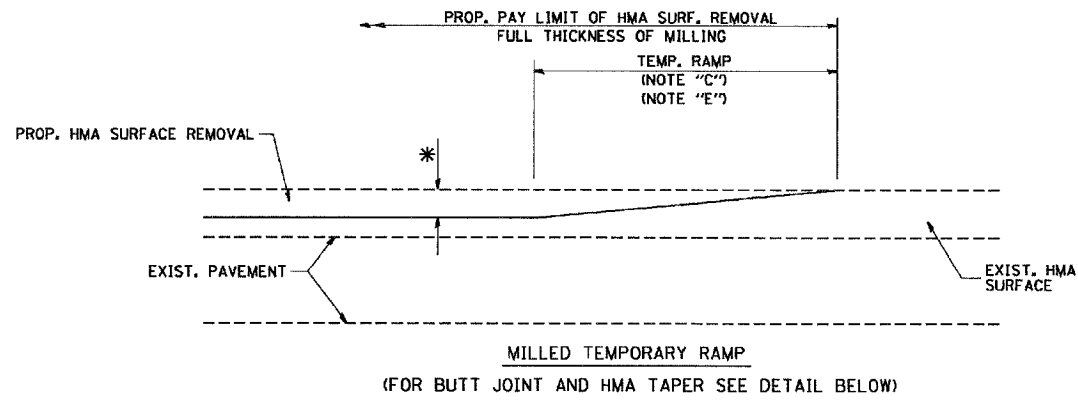
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE
DATE: 1/17/2007

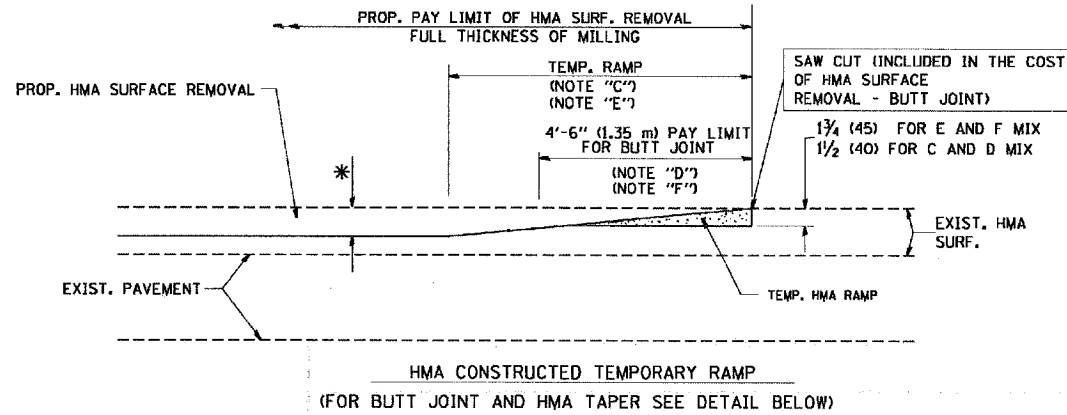
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CHECKED BY

TC-13
REVISION DATE: 01/06/00

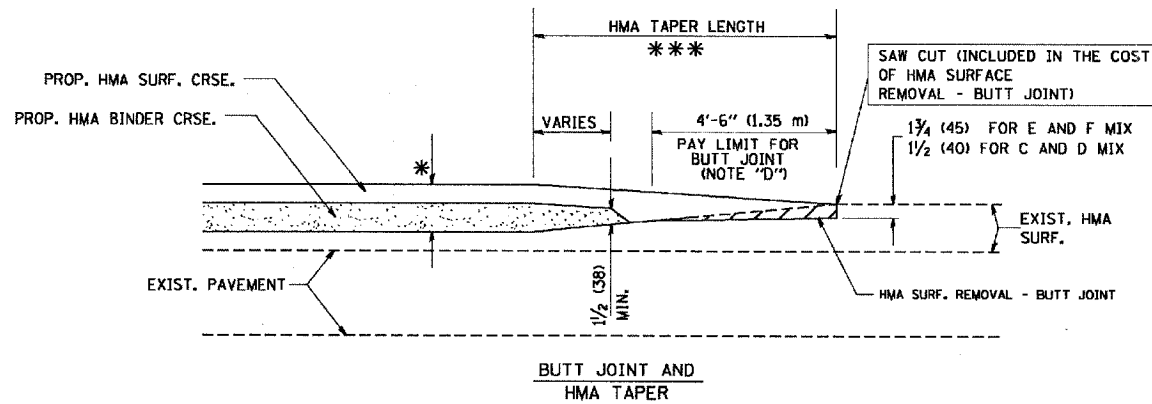
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3597	07-00119-00-RS	COOK	10	7
STA. TO STA.		FED. AID PROJECT		



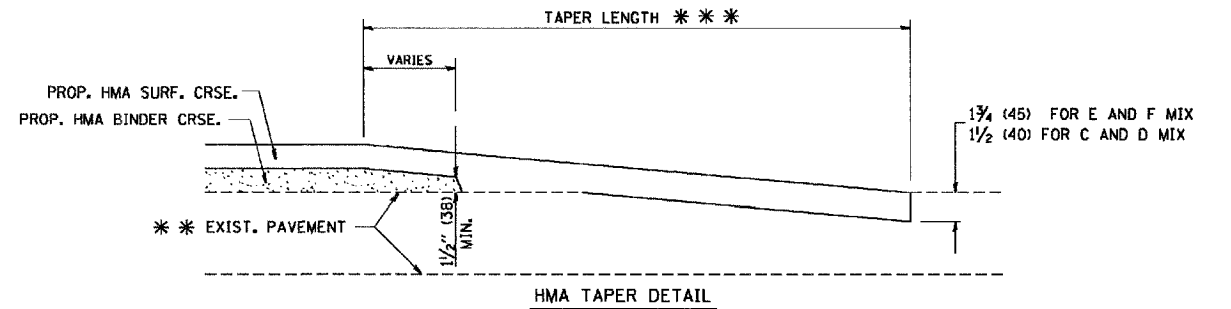
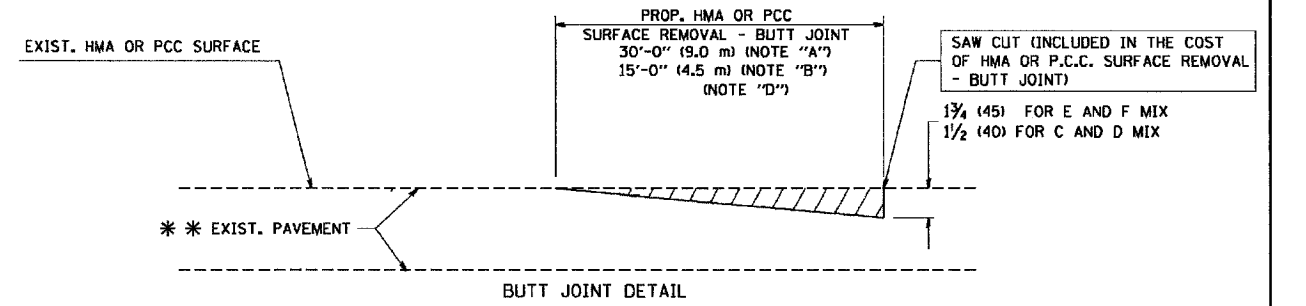
OPTION 1



OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. COMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

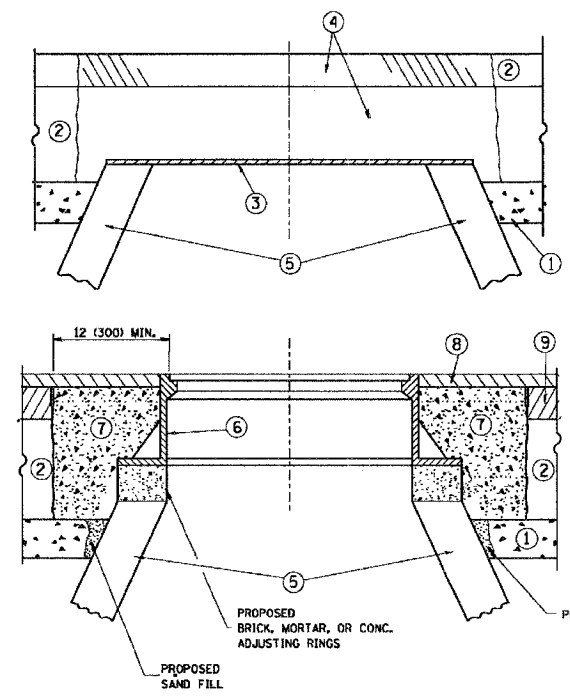
SCALE: VERT. NONE
HORIZ. 1/18/2007

DRAWN BY
CHECKED BY

BD400-05 (VI-BD32)
REVISION DATE: 01/01/07

PLOT DATE = 1/18/2007
FILE NAME = c:\pwworkspace\bd400-05\bd32.dwg
PLOT SCALE = 80.0000 / IN.
USER NAME = dr-106007

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3597	07-00119-00-RS	COOK	10	8
STA.	TO STA.		FED. AID PROJECT	



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS S1 CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL". NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04
R. BORO	01/01/07

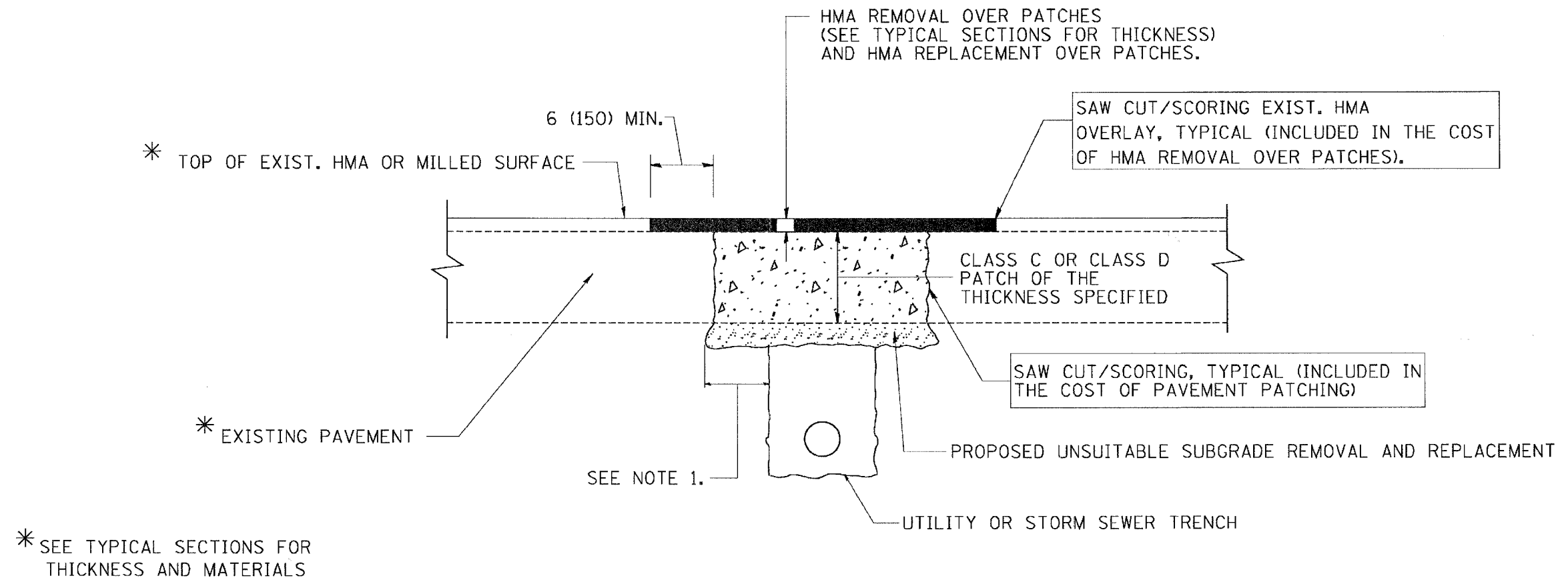
ILLINOIS DEPARTMENT OF TRANSPORTATION

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: VERT. NONE
HORIZ. NONE
PLOT DATE: 1/18/2007

DRAWN BY
CHECKED BY
BD600-03 (BD-B)
REVISION DATE: 01/01/07

PLOT DATE = 1/18/2007
PLOT SCALE = 1/8"=1'-0"
PLOT USER = drw/10/07
PLOT USER NAME = drw/10/07



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

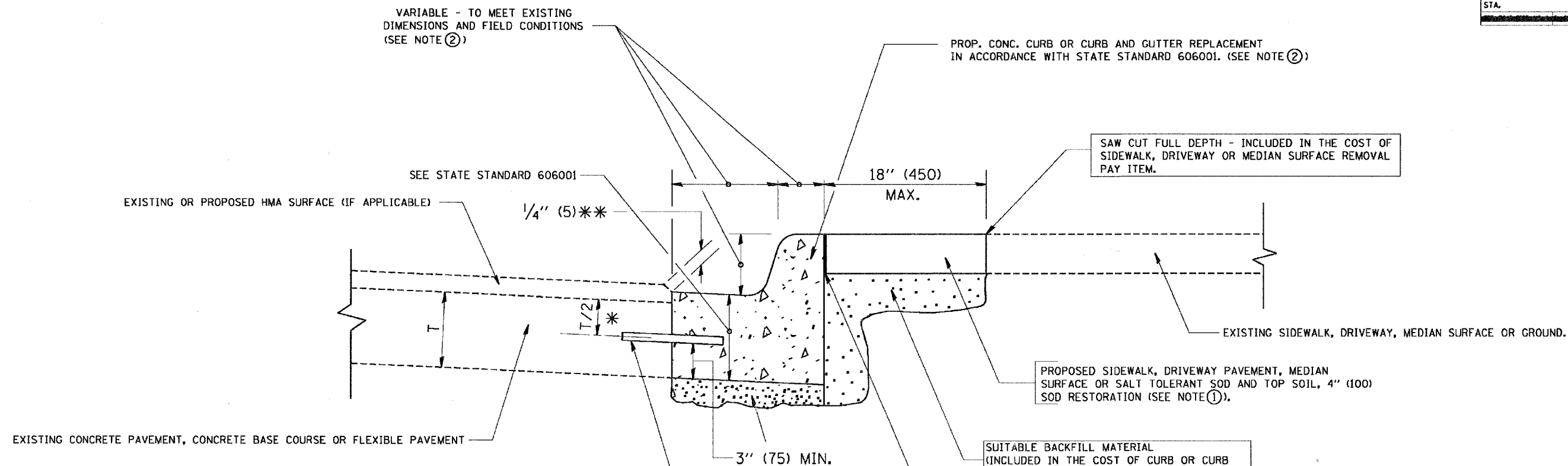
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/14/95
R. SHAH	03/23/95
R. SHAH	04/24/95
A. HOUSEH	03/15/96
A. ABBAS	03/21/97
A. ABBAS	01/20/98
ART ABBAS	04/27/98
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
 SCALE: VERT. NONE
 HORIZ. NONE
 PLOT DATE: 1/18/2007
 DRAWN BY
 CHECKED BY
 BD400-04 (BD-22)
 REVISION DATE: 01/01/07

PLOT DATE = 1/18/2007
 FILE NAME = c:\pavement\mshah\bd400.dgn
 USER = mshah
 CADD = mshah

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3597	07-00119-00-RS	COOK	10	10
STA.		TO STA.		
FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

REVISIONS	
NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: VERT. NONE
HORIZ. NONE
PLOT DATE: 1/18/2007

DRAWN BY

CHECKED BY

BD600-06 (BD-24)

REVISION DATE: 01/01/07