

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2638	17-00158-00-RS	DUPAGE	25	1
		ILLINOIS	CONTRACT NO. 61E93	

FOR INDEX OF SHEETS, SEE SHEET 2
FOR LIST OF STATE STANDARDS, SEE SHEET 2

08-03-2018 LETTING ITEM 012

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**PLANS FOR PROPOSED
FEDERAL-AID HIGHWAY**

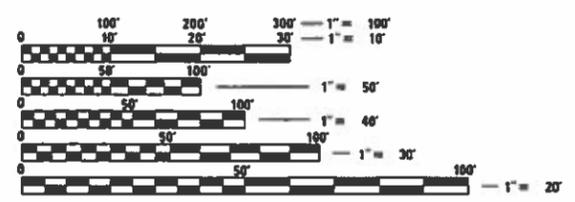
FAU ROUTE 2638 (WESTMORE-MEYERS ROAD)
IL ROUTE 38 (ROOSEVELT ROAD) TO ST CHARLES ROAD
RESURFACING, SIDEWALKS, PAVEMENT MARKING
SECTION 17-00158-00-RS
PROJECT HF60 (663)
VILLAGE OF LOMBARD
DuPAGE COUNTY
C-91-242-17



PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E., SCHAUMBURG, IL

TRAFFIC DATA

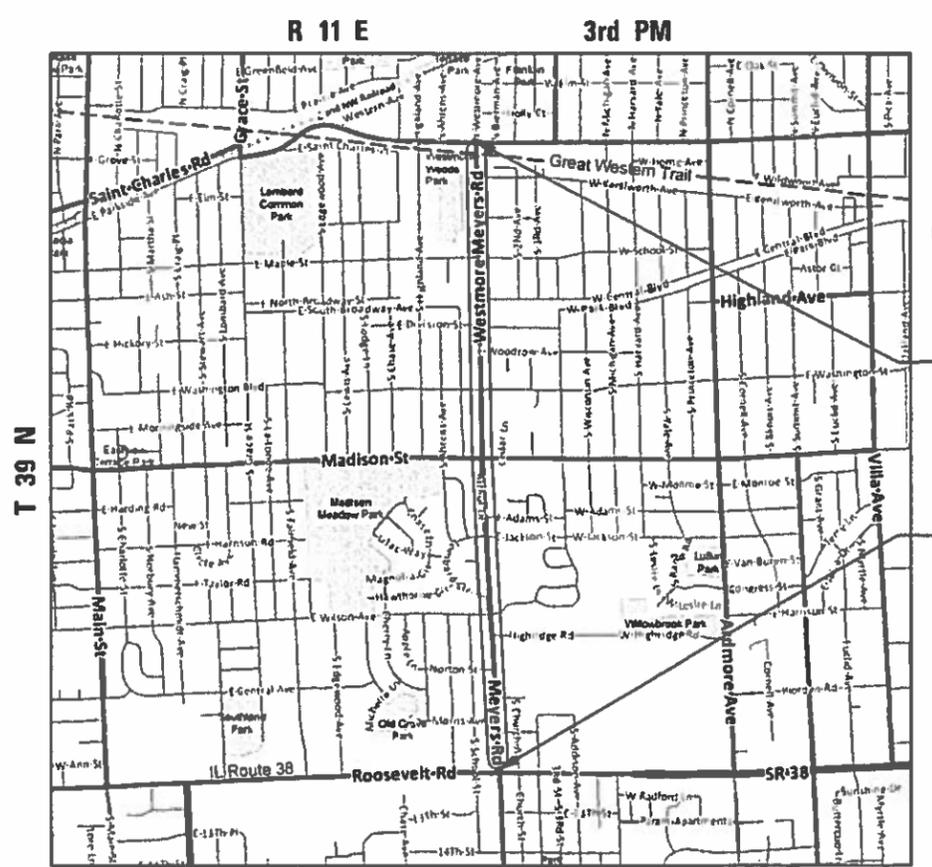
WESTMORE-MEYERS ROAD (FAU 2638)
MINOR ARTERIAL
TRAFFIC = 13,100 ADT (2016)
POSTED SPEED = 30 MPH
DESIGN SPEED = 30 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

CALL JULIE 811
WITH THE FOLLOWING:
COUNTY DUPAGE
CITY-TOWNSHIP LOMBARD/YORK
SECTION & RANGE T39N-R11E, Sec E09
48 HOURS BEFORE YOU DIG.
EXCLUDING SAT., SUN. & HOLIDAYS

MEADE ELECTRIC CO. DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR
LOCATES 100T ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES 773-267-7872.



YORK TOWNSHIP
LOCATION MAP
GROSS LENGTH = 10,568 FT. = 2.00 MILE
NET LENGTH = 10,568 FT. = 2.00 MILE



END IMPROVEMENTS
SAINT CHARLES ROAD
STA 206+37.00

BEGIN IMPROVEMENTS
ROOSEVELT ROAD
STA 100+69.00

4-19
Mark B. Thomas
MARK B. THOMAS
ILLINOIS REGISTRATION No. 062-05728
EXPIRATION DATE: 1/30/19

CH CHRISTOPHER B. BURKE ENGINEERING, LTD.
9575 W. Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500
PROFESSIONAL DESIGN FIRM NO. 184-001175
EXPIRATION DATE: 04/30/19

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED: [Signature] 4-19-18
VILLAGE OF LOMBARD
VILLAGE ENGINEER

PASSED: MAY 22, 2018
[Signature]
DISTRICT ENGINEER OF LOCAL ROADS AND STREETS

RELEASED FOR BID
BASED ON LIMITED
REVIEW
MAY 22 2018
[Signature]
REGIONAL ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

CONTRACT NO. 61E93

INDEX OF SHEETS

PAGE	TITLE
1	COVER SHEET
2	GENERAL NOTES, INDEX OF SHEETS, INDEX OF STANDARDS
3-4	SUMMARY OF QUANTITIES
5	TYPICAL SECTIONS
6	ALIGNMENT, TIES AND BENCHMARK PLAN
7-10	ROADWAY PLANS
11-14	PATH CROSSING DETAILS
15-16	CONSTRUCTION DETAILS
17	DETECTOR LOOP REPLACEMENT PLAN
18-25	DISTRICT 1 DETAILS

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-10	PERPENDICULAR CURB RAMPS
424016-04	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-04	DEPRESSED CORNER FOR SIDEWALKS
424026-02	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
604001-04	FRAME AND LIDS TYPE 1
604051-04	FRAME AND GRATE TYPE 11
606001-07	CONC. CURB TYPE B AND COMB. CONC CURB AND GUTTER
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS <= 40 MPH
701602-09	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-07	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS
728001-01	TELESCOPING STEEL SIGN SUPPORT
729001-01	APPLICATIONS OF TYPES A AND B METAL POSTS (FOR SIGNS & MARKERS)
731001-01	BASE FOR TELESCOPING STEEL SIGN SUPPORT
780001-05	TYPICAL PAVEMENT MARKINGS

GENERAL NOTES

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2018; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (MUTCD); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JUNE 2014 SEVENTH EDITION; THE "ILLINOIS URBAN MANUAL " AND THE "ILLINOIS URBAN MANUAL FIELD MANUAL FOR INSPECTION OF EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES"; THE "AMERICANS WITH DISABILITIES ACT OF 1990 ACCESSIBILITY GUIDELINES"; THE "DRAFT" REHABILITATION ACT OF 1973 (SECTION 504); THE "DETAILS" IN THE PLANS; AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT HIGHWAY STANDARD.

CODES OF THE IEPA TITLE 35, AND O.S.H.A. SHALL BE ADHERED TO FOR THE CONSTRUCTION OF THIS PROJECT. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE ADHERENCE TO THESE (NOT THE VILLAGE OR CHRISTOPHER B. BURKE ENGINEERING).

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

UTILITIES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE VILLAGE.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES (48 HOURS NOTIFICATION IS REQUIRED.)

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS.

ANY EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. CONTACT WATER DEPARTMENT FOR THEM TO TURN VALVES OR OPERATE HYDRANTS. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE ENGINEER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT.

MISCELLANEOUS

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

AGGREGATE BASE REPAIR, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL AND REPLACEMENT, AND STRUCTURES TO BE ADJUSTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES.

CURB AND GUTTER TO BE REMOVED AT DRIVEWAYS SHALL BE REPLACED WITH DEPRESSED CURB AND GUTTER.

WITH THE EXCEPTION OF ROOSEVELT ROAD (IL ROUTE 38), ALL TRAFFIC SIGNALS WITHIN THE PROJECT LIMITS USE VIDEO DETECTION.

PAVING

HOT-MIX ASPHALT BINDER COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY CURED AND BACKFILLED TO THE SATISFACTION OF THE ENGINEER.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACE, BINDER, OR BASE UPON WHICH THE HOT-MIX ASPHALT MATERIALS ARE PLACED. THE THICKNESSES SHOWN ON THE PLANS ARE THE MINIMUM ACCEPTABLE THICKNESSES.

LANDSCAPING

RESTORATION WORK WILL BE PAID FOR UP TO TWO FEET ADJACENT TO RECONSTRUCTED CURB AND GUTTER OR SIDEWALK. AREAS OUTSIDE THE TWO FOOT ZONE THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR SHALL BE RESTORED BY THE CONTRACTOR AT HIS/HER OWN EXPENSE.

CONTRACTOR SHALL BE CAUTIOUS NOT TO DISTURB ANY PARKWAY TREES DURING CONSTRUCTION. ANY TREES THAT ARE DAMAGED OR DISTURBED DURING THE CONSTRUCTION SHALL BE REPLACED IN KIND BY THE CONTRACTOR AT HIS/HER OWN EXPENSE. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

FILE NAME =	USER NAME = mthomas	DESIGNED MBT	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	VILLAGE OF LOMBARD WESTMORE-MEYERS ROAD GENERAL NOTES	FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\Lombard\170012.00002\Civil\GEN.170012.01.sht		DRAWN MBT	REVISED -			2638	17-00158-00-RS	DuPAGE	25	2	
Default	PLOT SCALE = 48'	CHECKED JGS	REVISED -			CONTRACT NO. 61E93					
	PLOT DATE = 5/24/2018	DATE -	REVISED -			SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY
20200100	EARTH EXCAVATION	CU YD	70
21101600	TOPSOIL FURNISH AND PLACE, VARIABLE DEPTH	SQ YD	228
25000210	SEEDING, CLASS 2A	ACRE	0.25
25100630	EROSION CONTROL BLANKET	SQ YD	228
28000510	INLET FILTERS	EACH	112
31101180	SUBBASE GRANULAR MATERIAL, TYPE B 2"	SQ YD	592
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	37848
40600990	TEMPORARY RAMP	SQ YD	600
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	7277
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	6469
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	25
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	70
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	5849
42400800	DETECTABLE WARNINGS	SQ FT	210
44000100	PAVEMENT REMOVAL	SQ YD	113
44000166	HOT-MIX ASPHALT SURFACE REMOVAL, 4 1/4"	SQ YD	56070
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	120
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	830
44000600	SIDEWALK REMOVAL	SQ FT	3669
44201325	CLASS C PATCHES, TYPE I, 8 INCH	SQ YD	54
44201329	CLASS C PATCHES, TYPE II, 8 INCH	SQ YD	66
44201333	CLASS C PATCHES, TYPE III, 8 INCH	SQ YD	50
44201335	CLASS C PATCHES, TYPE IV, 8 INCH	SQ YD	50
44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	150
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	150
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	150
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	150
60250200	CATCH BASINS TO BE ADJUSTED	EACH	10
60260100	INLETS TO BE ADJUSTED	EACH	10

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY
60262700	INLETS TO BE RECONSTRUCTED	EACH	2
60266600	VALVE BOXES TO BE ADJUSTED	EACH	2
60404800	FRAMES AND GRATES, TYPE 11	EACH	5
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	5
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	5
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	610
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	100
60605900	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.12	FOOT	120
67100100	MOBILIZATION	L SUM	1
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1
70102634	TRAFFIC CONTROL AND PROTECTION, STANDARD 701611	L SUM	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
70300100	SHORT TERM PAVEMENT MARKING	FOOT	6500
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	2200
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	584
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	47260
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	8580
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	788
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	1326
Δ 72000100	SIGN PANEL - TYPE 1	SQ FT	24
Δ 72400310	REMOVE SIGN PANEL - TYPE 1	SQ FT	25
Δ 72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	2
Δ 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	16
Δ 73100100	BASE FOR TELESCOPING STEEL SIGN SUPPORT	EACH	2
Δ 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	312
Δ 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	23630
Δ 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	4286

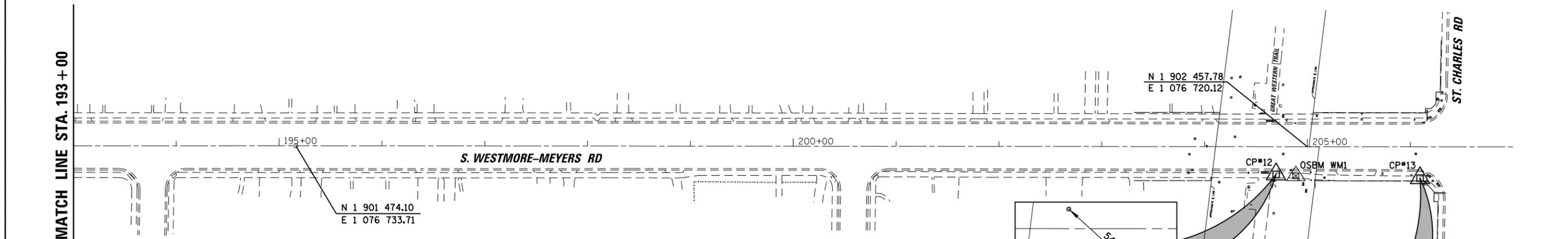
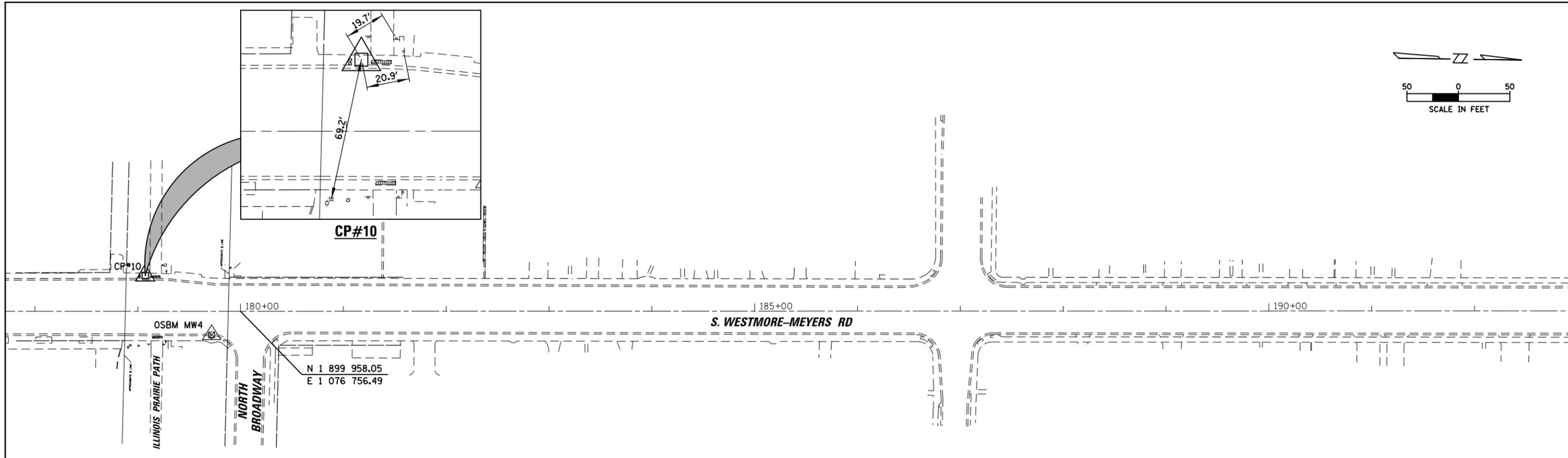
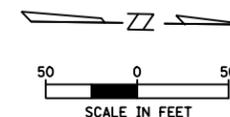
Δ INDICATES SPECIALTY ITEM

CONSTRUCTION CODE 0005

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY
Δ 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	98
Δ 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	394
Δ 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	890
Δ 83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	11
Δ 84200804	REMOVAL OF POLE FOUNDATION	EACH	1
Δ 84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	1
Δ 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	12
Δ 88600600	DETECTOR LOOP REPLACEMENT	FOOT	296
Δ 89502376	REBUILD EXISTING HANDHOLE	EACH	3
X0323455	ADJUST MONITORING WELLS	EACH	1
X0326806	WASHOUT BASIN	L SUM	1
Δ X0326899	SOLAR-POWERED FLASHING BEACON ASSEMBLY (COMPLETE)	EACH	2
Δ X5091725	BICYCLE RAILING, SPECIAL	FOOT	286
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	75
Δ X8130130	JUNCTION BOX TO BE ADJUSTED	EACH	2
Z0004522	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6"	SQ YD	25
Z0013798	CONSTRUCTION LAYOUT	L SUM	1
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	52
Δ Z0033028	MAINTENANCE OF LIGHTING SYSTEM	CAL MO	3

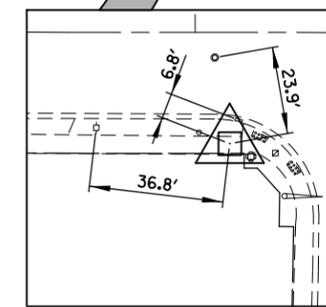
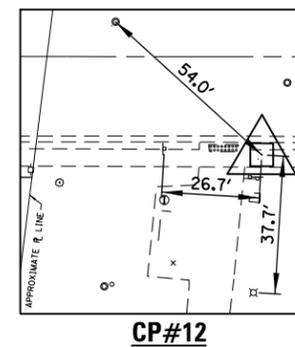
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Default	PLOT SCALE = 48'	CHECKED JGS	REVISED -			SCALE: NONE	SHEET 1 OF 2 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		
	PLOT DATE = 5/24/2018	DATE -	REVISED -			CONTRACT NO. 61E93						



ELEVATION BENCHMARKS DATUM: NAVD'88 (GPS OBSERVED)		
NO.	DESCRIPTION	ELEV.
LOMBARD 1-004	VILLAGE OF LOMBARD BENCHMARK 1-004 STAINLESS STEEL DISK WITH DATUM POINT AND NO STAMPING, 200' SOUTH OF ST. CHARLES ROAD AND 375' WEST OF WESTMORE-MEYERS ROAD	712.93
OSBM WM1	RAILROAD SPIKE IN WEST FACE OF FIRST POWER POLE NORTH OF THE GREAT WESTERN TRAIL ON THE EAST SIDE OF WESTMORE-MEYERS ROAD	717.61
OSBM WM2	RAILROAD SPIKE IN WEST FACE OF POWER POLE WITH LIGHT IN FRONT OF HOUSE #117 WESTMORE-MEYERS ROAD	709.36
OSBM WM3	NORTHWEST CORNER OF CONCRETE BASE FOR TRAFFIC SIGNAL WITH MAST AT THE NORTHEAST CORNER OF MAPLE & WESTMORE-MEYERS ROAD	708.96
OSBM WM4	SQUARE CUT ON WEST SIDE OF CONCRETE BASE FOR LIGHT POLE AT THE SOUTHEAST CORNER OF WESTMORE-MEYERS ROAD & NORTH BROADWAY	713.92

HORIZONTAL CONTROL POINTS					
POINT NO.	NORTHING (Y)	EASTING (X)	DESCRIPTION	STATION	OFFSET
10	1 899 864.83	1 076 723.16	CP-XCUT	179+07.30	34.73' LT
12	1 902 427.73	1 076 747.30	CP-XCUT	204+69.58	26.76' RT
13	1 902 567.72	1 076 748.94	CP-XCUT	206+09.53	30.33' RT



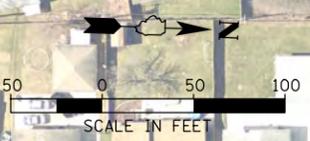
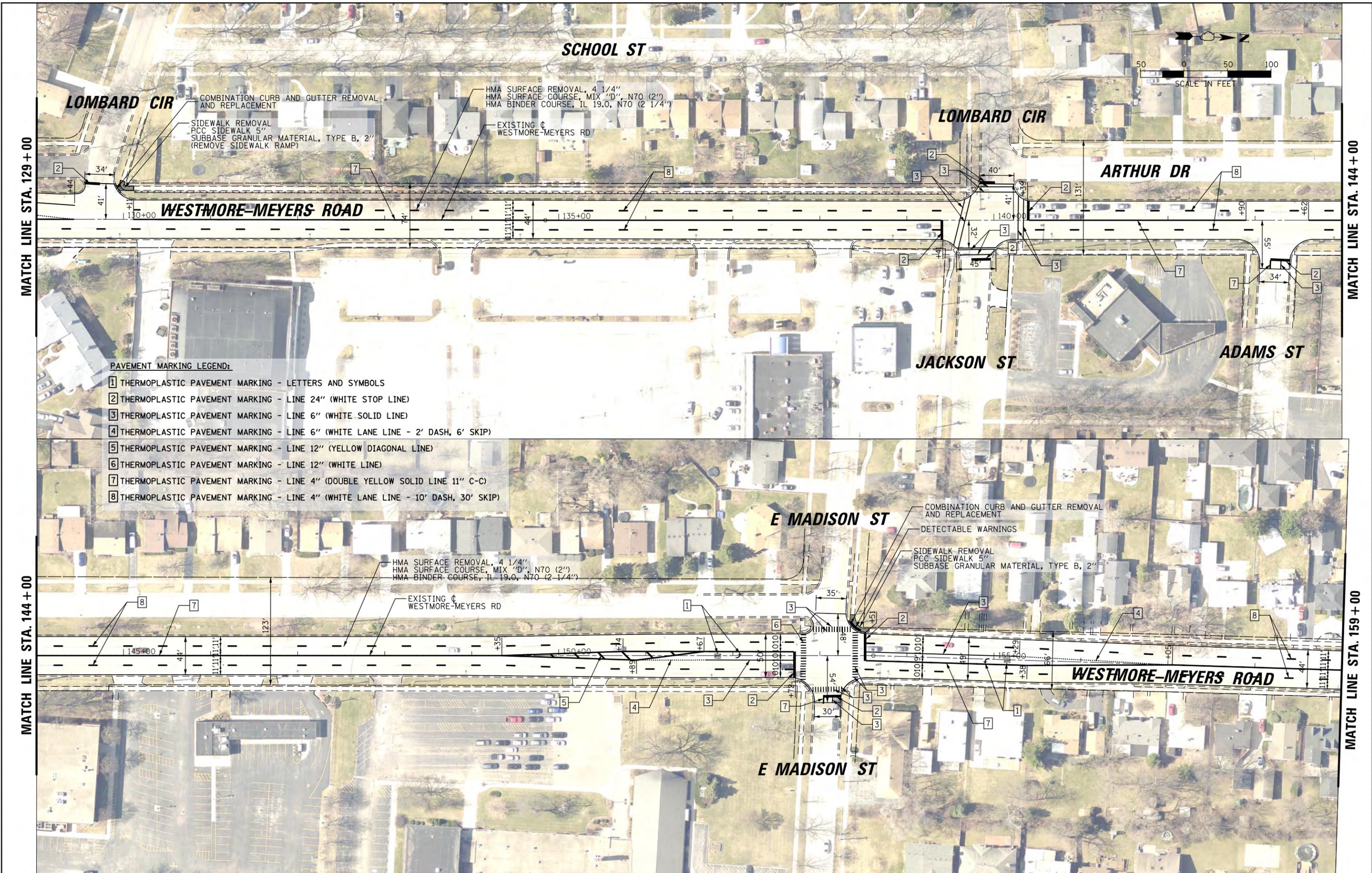
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Default	PLOT SCALE = 100'	CHECKED JGS	REVISED -
	PLOT DATE = 5/24/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

VILLAGE OF LOMBARD WESTMORE-MEYERS ROAD ALIGNMENT, TIES AND BENCHMARK PLAN			
SCALE: 1" = 50'	SHEET 1 OF 1 SHEETS	STA. 178+00.00 TO STA. 207+00.00	

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2638	17-00158-00-RS	DuPAGE	25	6
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MATCH LINE STA. 193 + 00



- PAVEMENT MARKING LEGEND:**
- 1 THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
 - 2 THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE STOP LINE)
 - 3 THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE SOLID LINE)
 - 4 THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)
 - 5 THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONAL LINE)
 - 6 THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE LINE)
 - 7 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW SOLID LINE 11" C-C)
 - 8 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP)

FILE NAME = N:\Lombard\170012.00002\Civil\PLN.170012.02.sht
 USER NAME = mthomas
 PLOT SCALE = 100'
 PLOT DATE = 5/24/2018

DESIGNED MBT
 DRAWN MBT
 CHECKED JGS
 DATE -

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

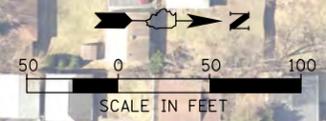
VILLAGE OF LOMBARD
 WESTMORE-MEYERS ROAD
 ROADWAY PLAN
 SCALE: 1" = 50'

FAU RTE. 2638
 SECTION 17-00158-00-RS
 COUNTY DuPAGE
 TOTAL SHEETS 25
 SHEET NO. 8
 CONTRACT NO. 61E93
 STA. 129+00.00 TO STA. 159+00.00

ILLINOIS FED. AID PROJECT

PAVEMENT MARKING LEGEND:

- 1 THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 2 THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE STOP LINE)
- 3 THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE SOLID LINE)
- 4 THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)
- 5 THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONAL LINE)
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- 7 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW SOLID LINE 11" C-C)
- 8 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP)

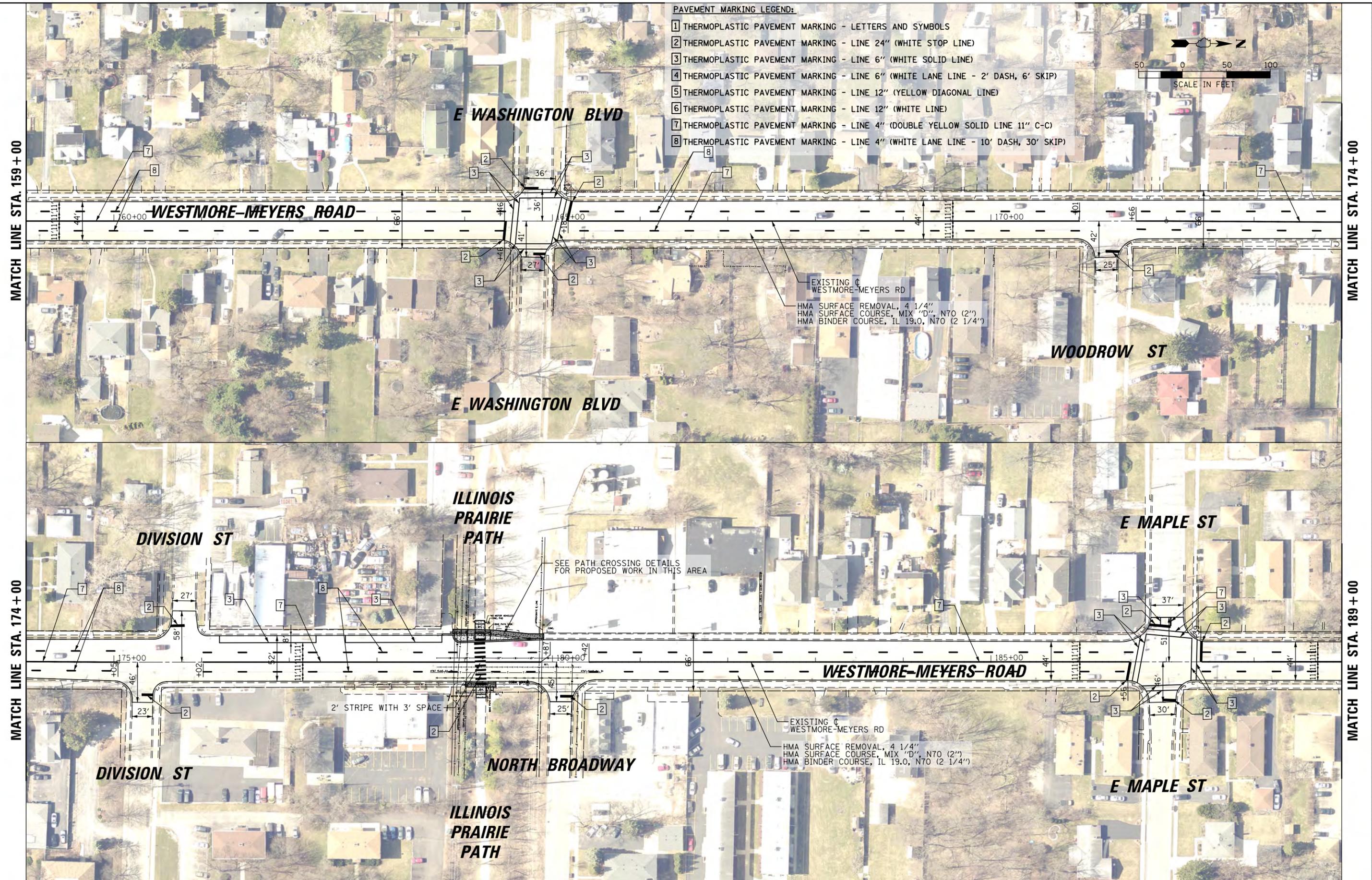


MATCH LINE STA. 159+00

MATCH LINE STA. 174+00

MATCH LINE STA. 174+00

MATCH LINE STA. 189+00



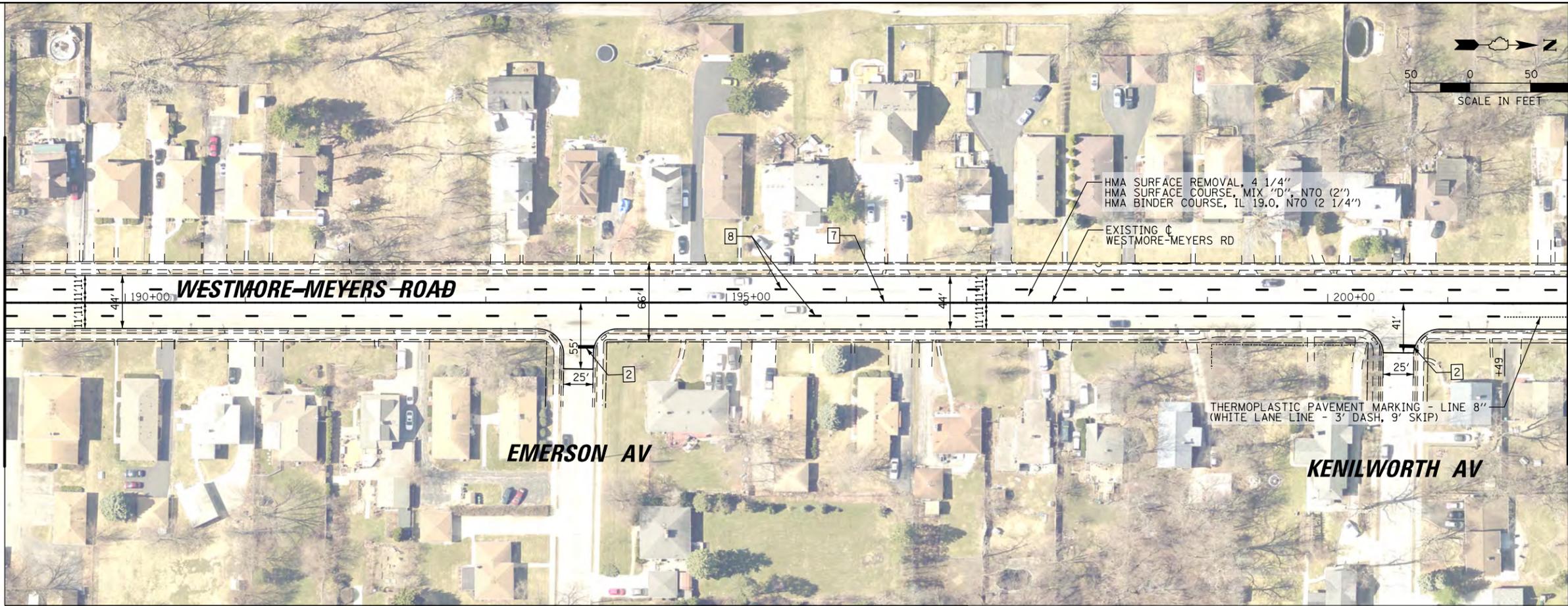
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF LOMBARD
WESTMORE-MEYERS ROAD
ROADWAY PLAN**

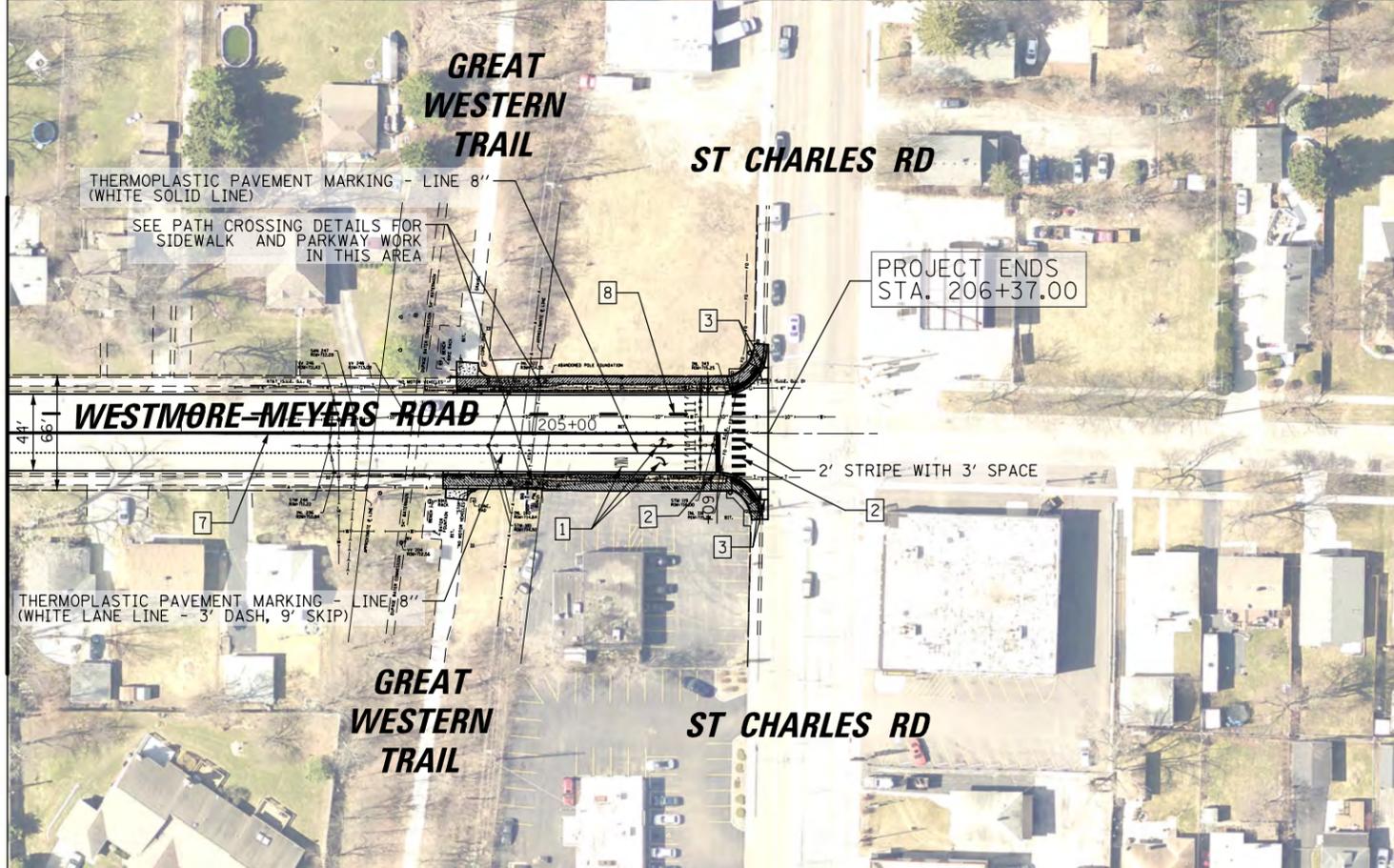
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FAU R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2638	17-00158-00-RS	DuPAGE	25	9
CONTRACT NO. 61E93			ILLINOIS FED. AID PROJECT	



MATCH LINE STA. 189 + 00

MATCH LINE STA. 202 + 00



PAVEMENT MARKING LEGEND:

- ① THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- ② THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE STOP LINE)
- ③ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE SOLID LINE)
- ④ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)
- ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONAL LINE)
- ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE LINE)
- ⑦ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW SOLID LINE 11" C-C)
- ⑧ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP)

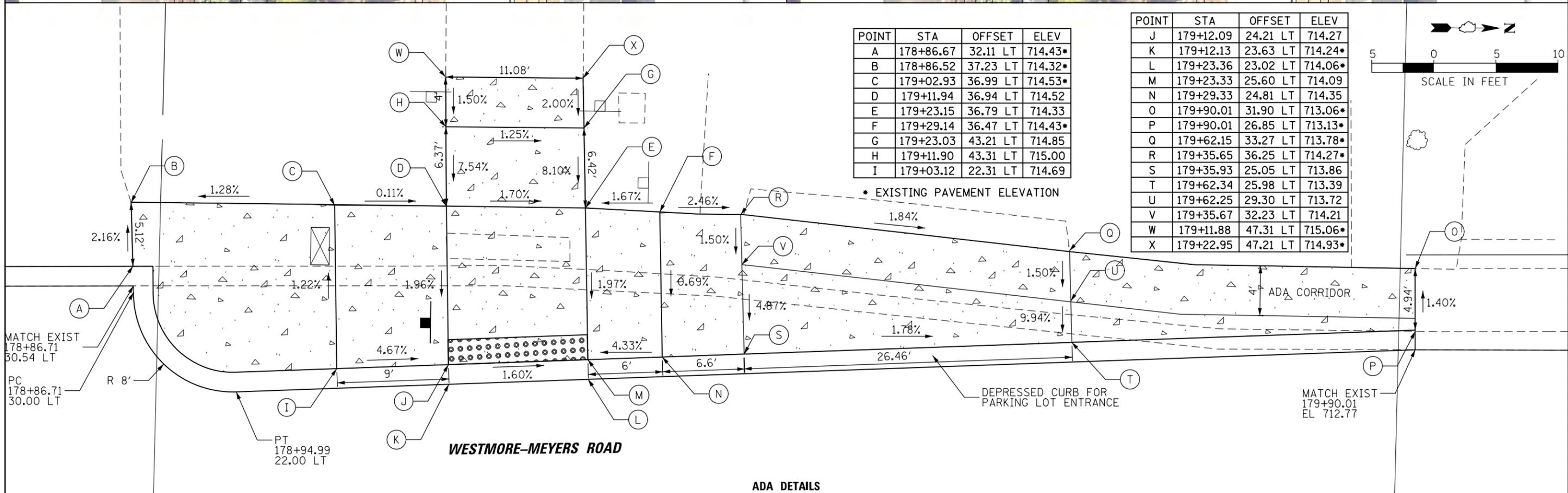
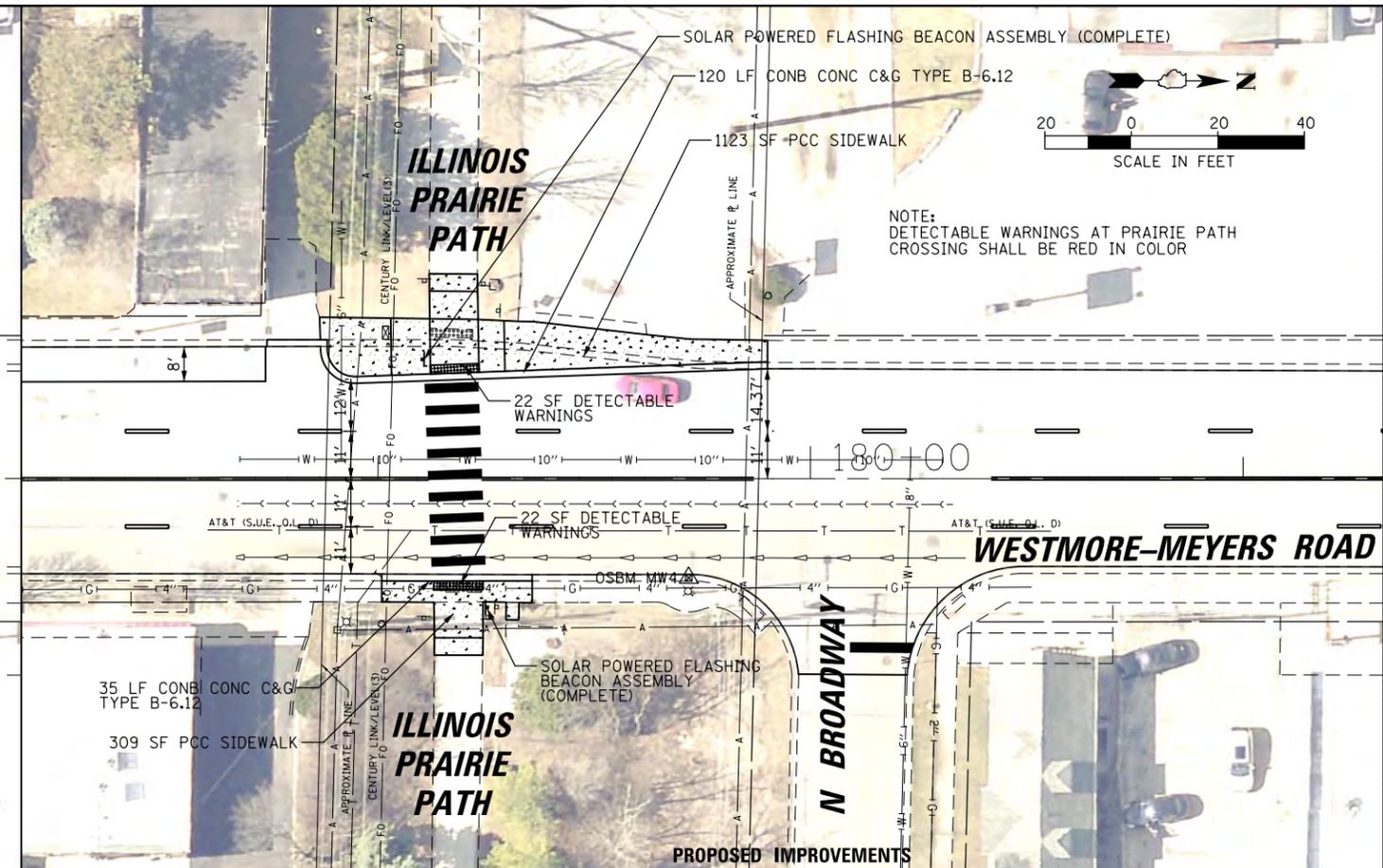
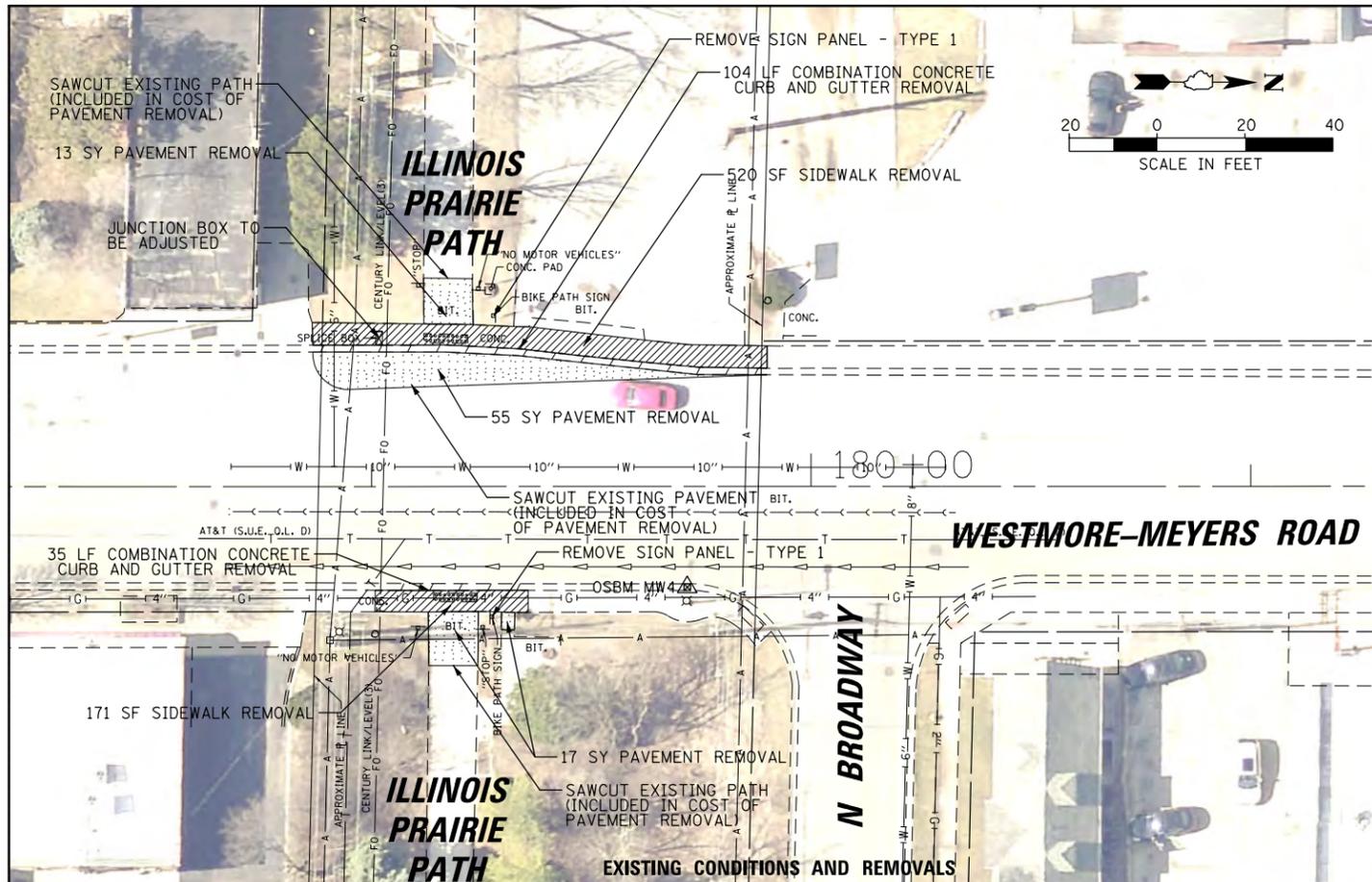
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		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

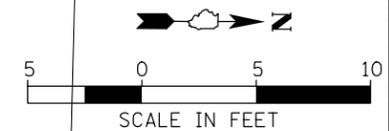
**VILLAGE OF LOMBARD
WESTMORE-MEYERS ROAD
ROADWAY PLAN**

SCALE: 1" = 50' SHEET 4 OF 4 SHEETS STA. 189+00.00 TO STA. 207+00.00

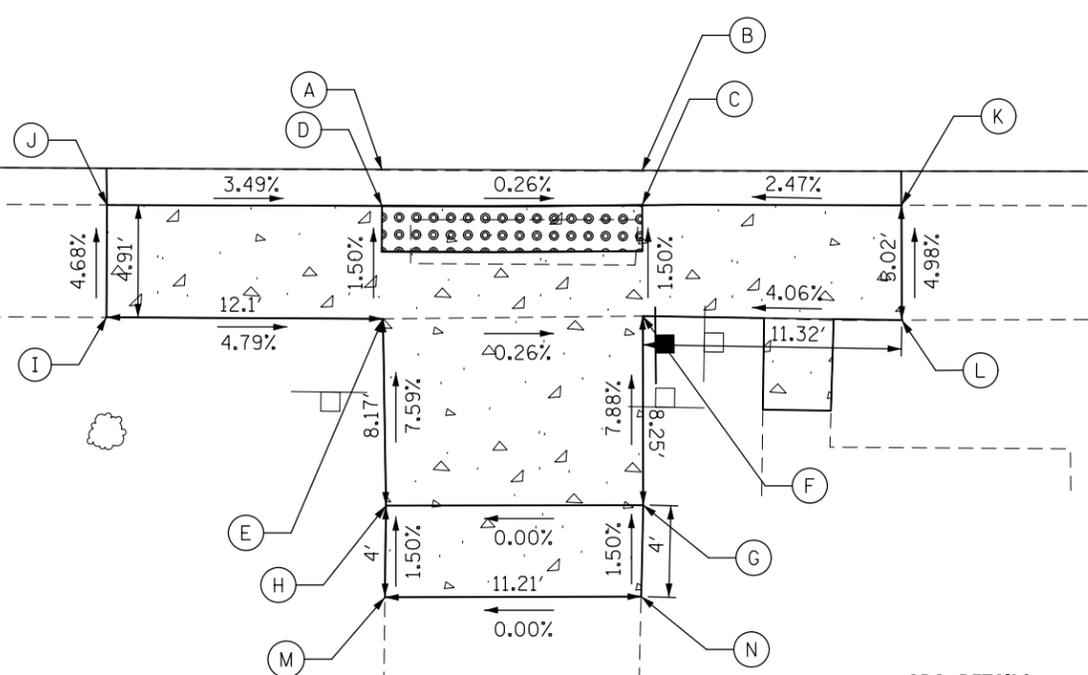
FAU RTE. 2638	SECTION 17-00158-00-RS	COUNTY DuPAGE	TOTAL SHEETS 25	SHEET NO. 10
CONTRACT NO. 61E93				ILLINOIS FED. AID PROJECT



FILE NAME = N:\Lombard\170012.00002\Civil\DET_XING.170012.01.sht	USER NAME = mthomas	DESIGNED MBT	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	VILLAGE OF LOMBARD WESTMORE-MEYERS ROAD PATH CROSSING DETAILS	FAU RTE. 2638	SECTION 17-00158-00-RS	COUNTY DuPAGE	TOTAL SHEETS 25	SHEET NO. 11
Default	PLOT SCALE = 48"	CHECKED JGS	REVISED -			SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.	CONTRACT NO. 61E93			
	PLOT DATE = 5/24/2018	DATE -	REVISED -			ILLINOIS FED. AID PROJECT				



WESTMORE-MEYERS ROAD



POINT	STA	OFFSET	ELEV
A	179+12.87	22.01 RT	714.01*
B	179+24.25	22.05 RT	713.98*
C	179+24.26	23.55 RT	714.00
D	179+12.86	23.58 RT	714.03
E	179+12.90	28.50 RT	714.10
F	179+24.28	28.39 RT	714.07
G	179+24.30	36.64 RT	714.72
H	179+13.03	36.67 RT	714.72
I	179+00.80	28.46 RT	714.68*
J	179+00.83	23.53 RT	714.45*
K	179+35.60	23.55 RT	714.28*
L	179+35.62	28.57 RT	714.53*
M	179+12.99	40.67 RT	714.78*
N	179+24.20	40.64 RT	714.78*

ADA DETAILS

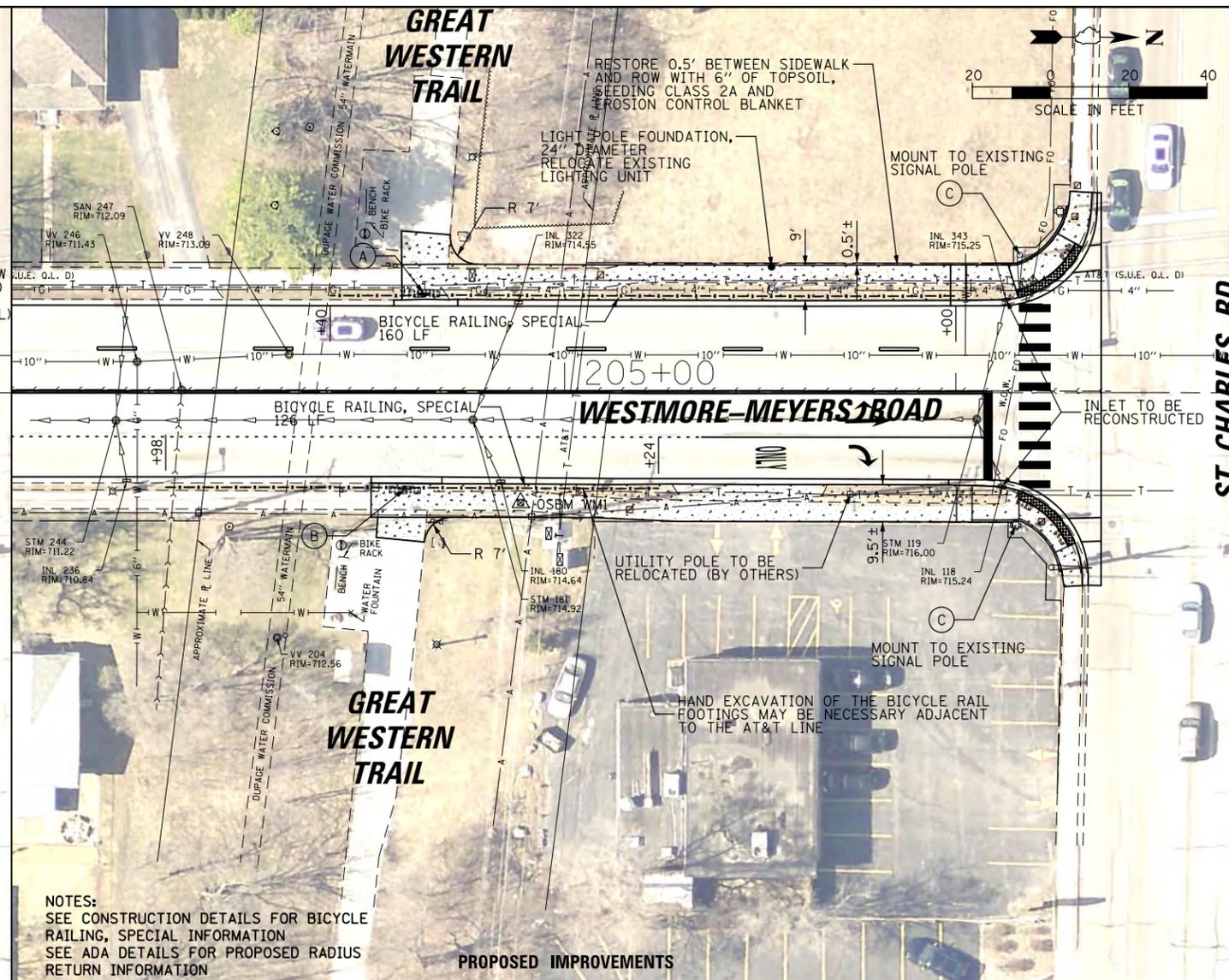
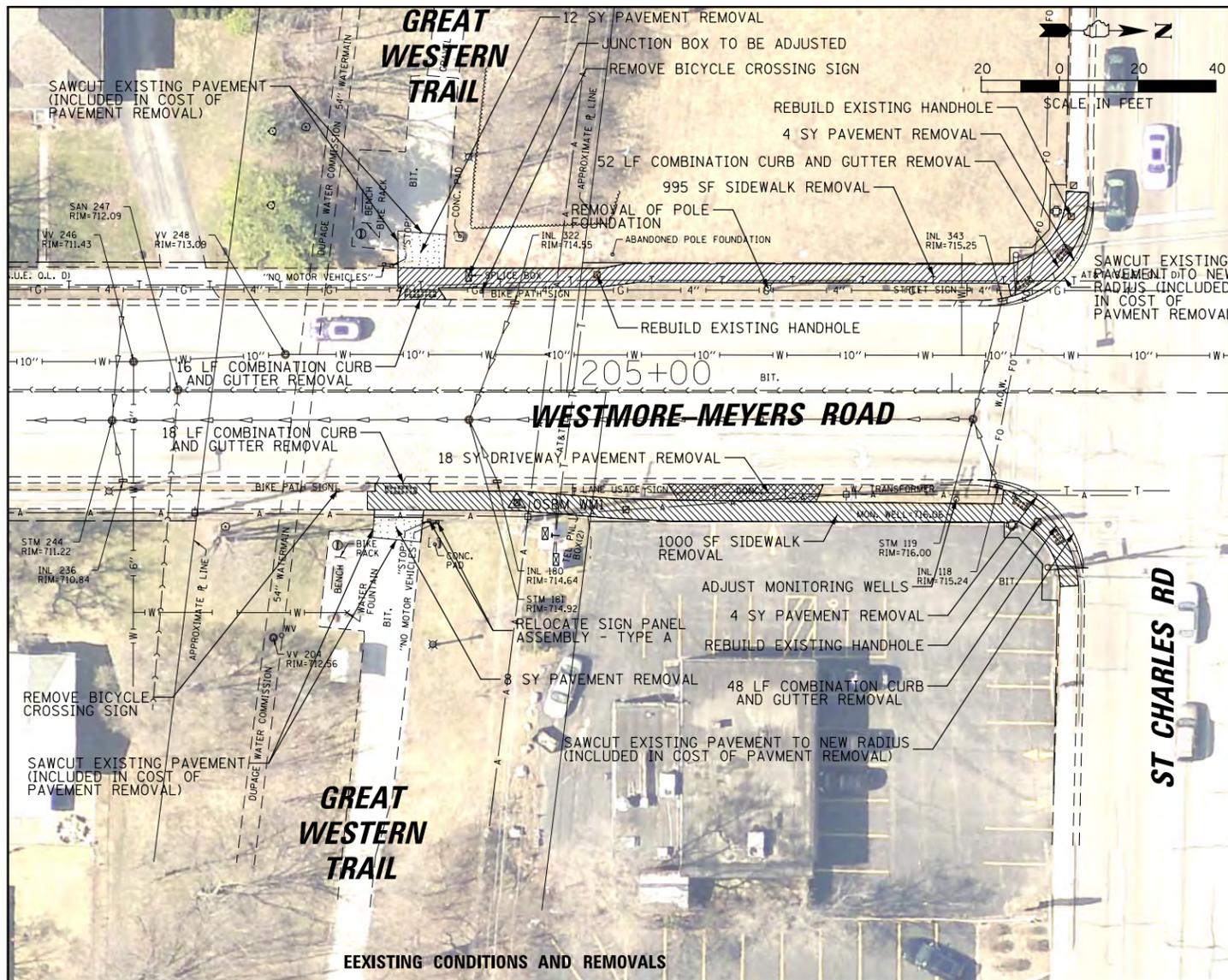
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	PLOT DATE = 5/24/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

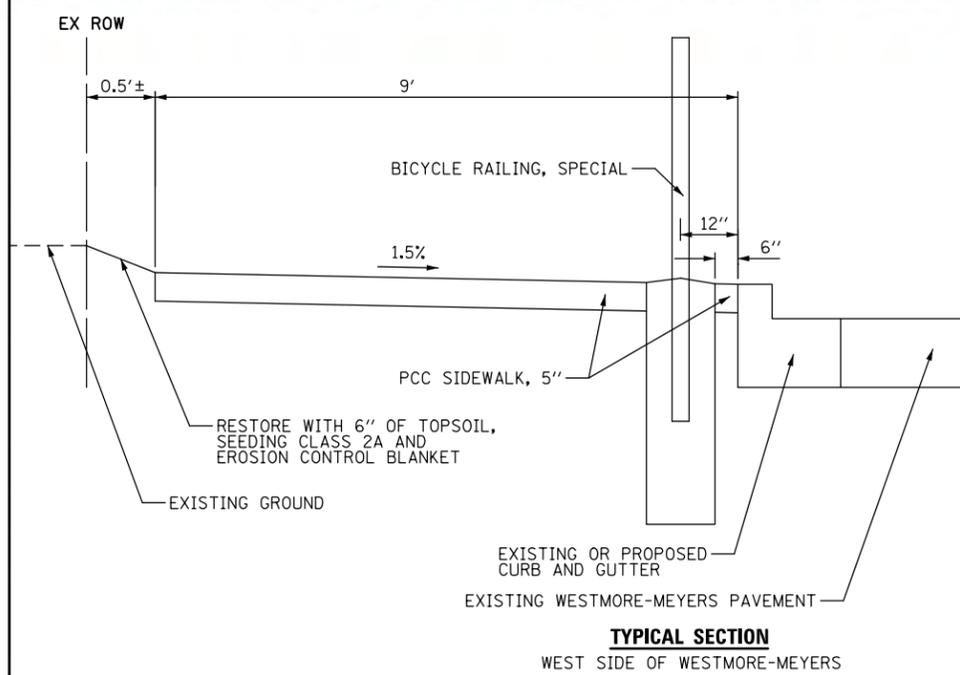
**VILLAGE OF LOMBARD
WESTMORE-MEYERS ROAD
PATH CROSSING DETAILS**

SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.

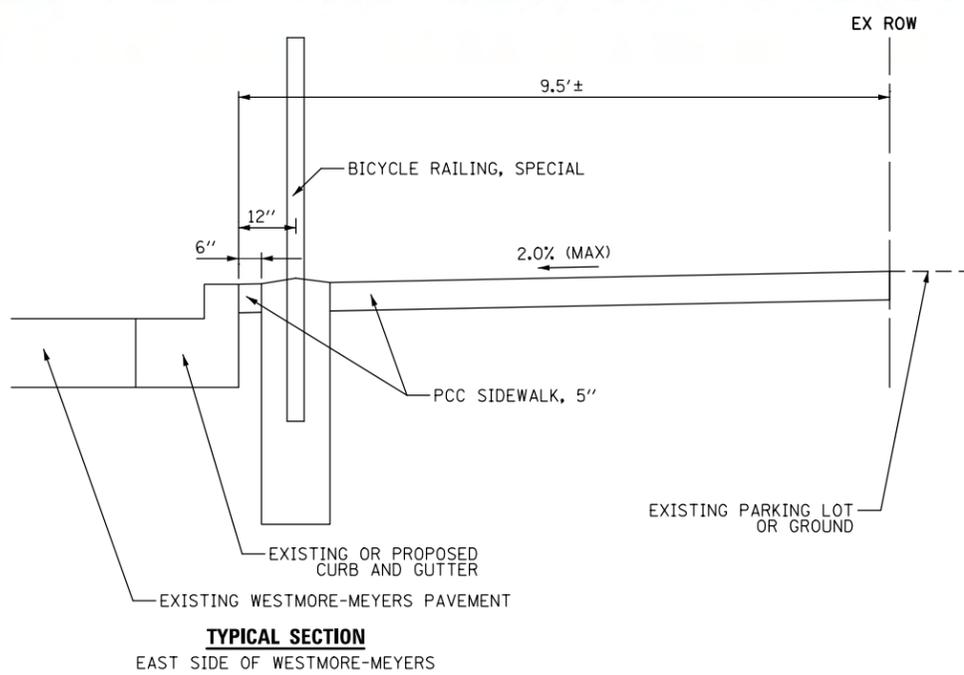
FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2638	17-00158-00-RS	DuPAGE	25	12
CONTRACT NO. 61E93			ILLINOIS FED. AID PROJECT	



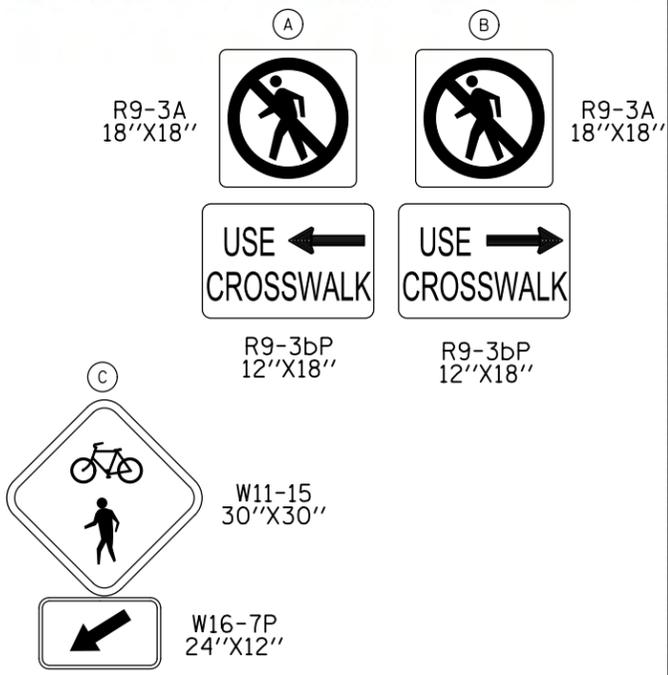
NOTES:
 SEE CONSTRUCTION DETAILS FOR BICYCLE RAILING, SPECIAL INFORMATION
 SEE ADA DETAILS FOR PROPOSED RADIUS
 RETURN INFORMATION



TYPICAL SECTION
 WEST SIDE OF WESTMORE-MEYERS



TYPICAL SECTION
 EAST SIDE OF WESTMORE-MEYERS



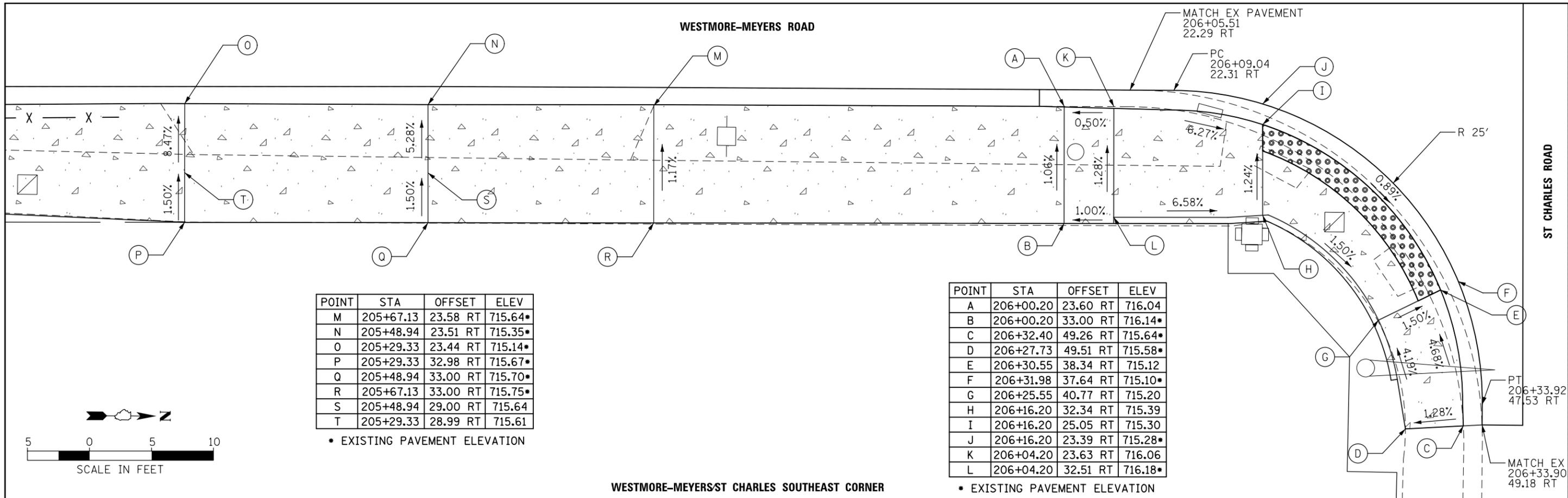
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	PLOT DATE = 5/24/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

VILLAGE OF LOMBARD
WESTMORE-MEYERS ROAD
PATH CROSSING DETAILS

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

FAU R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2638	17-00158-00-RS	DuPAGE	25	13
			CONTRACT NO. 61E93	
ILLINOIS FED. AID PROJECT				



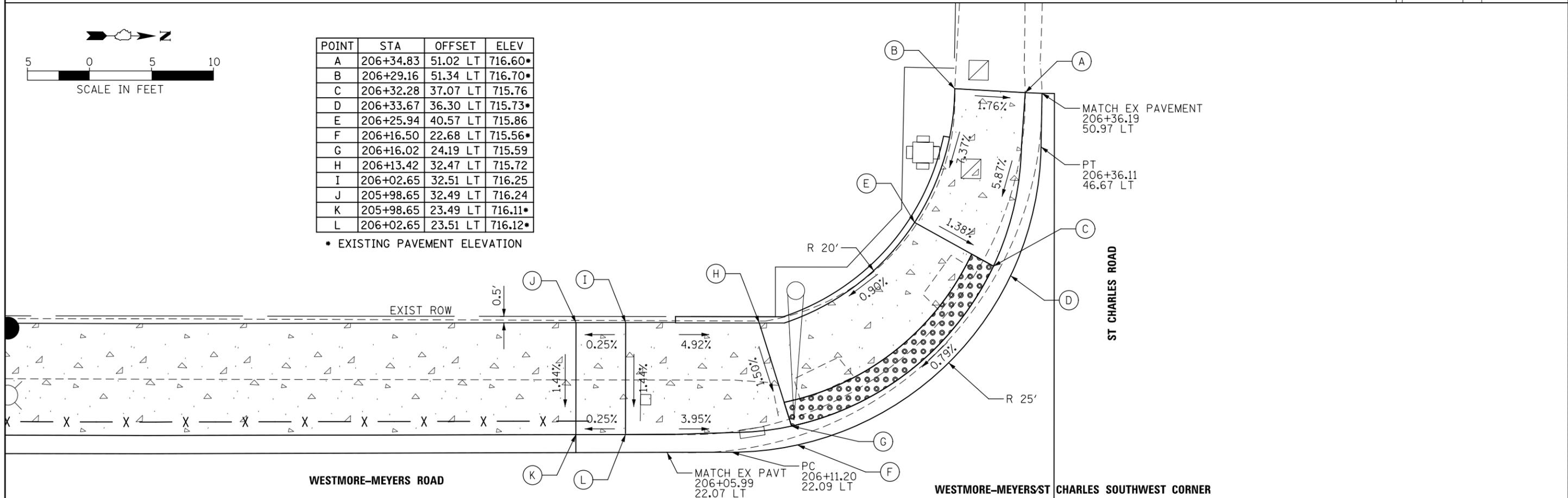
POINT	STA	OFFSET	ELEV
M	205+67.13	23.58 RT	715.64*
N	205+48.94	23.51 RT	715.35*
O	205+29.33	23.44 RT	715.14*
P	205+29.33	32.98 RT	715.67*
Q	205+48.94	33.00 RT	715.70*
R	205+67.13	33.00 RT	715.75*
S	205+48.94	29.00 RT	715.64
T	205+29.33	28.99 RT	715.61

* EXISTING PAVEMENT ELEVATION

POINT	STA	OFFSET	ELEV
A	206+00.20	23.60 RT	716.04
B	206+00.20	33.00 RT	716.14*
C	206+32.40	49.26 RT	715.64*
D	206+27.73	49.51 RT	715.58*
E	206+30.55	38.34 RT	715.12
F	206+31.98	37.64 RT	715.10*
G	206+25.55	40.77 RT	715.20
H	206+16.20	32.34 RT	715.39
I	206+16.20	25.05 RT	715.30
J	206+16.20	23.39 RT	715.28*
K	206+04.20	23.63 RT	716.06
L	206+04.20	32.51 RT	716.18*

* EXISTING PAVEMENT ELEVATION

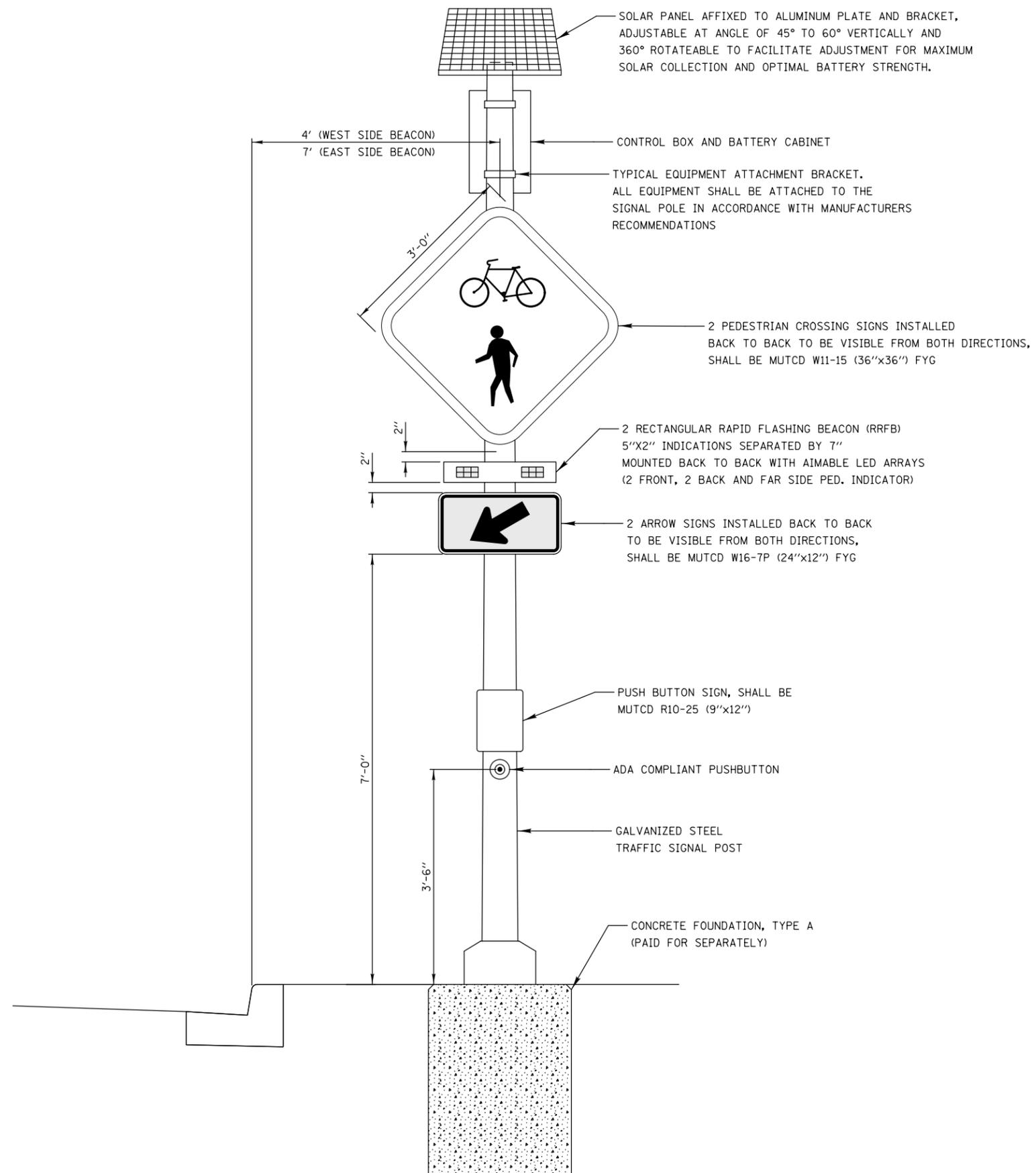
WESTMORE-MEYERS/ST CHARLES SOUTHEAST CORNER



POINT	STA	OFFSET	ELEV
A	206+34.83	51.02 LT	716.60*
B	206+29.16	51.34 LT	716.70*
C	206+32.28	37.07 LT	715.76
D	206+33.67	36.30 LT	715.73*
E	206+25.94	40.57 LT	715.86
F	206+16.50	22.68 LT	715.56*
G	206+16.02	24.19 LT	715.59
H	206+13.42	32.47 LT	715.72
I	206+02.65	32.51 LT	716.25
J	205+98.65	32.49 LT	716.24
K	205+98.65	23.49 LT	716.11*
L	206+02.65	23.51 LT	716.12*

* EXISTING PAVEMENT ELEVATION

WESTMORE-MEYERS/ST CHARLES SOUTHWEST CORNER



SOLAR-POWERED FLASHING BEACON ASSEMBLY (COMPLETE)

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	PLOT DATE = 5/24/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

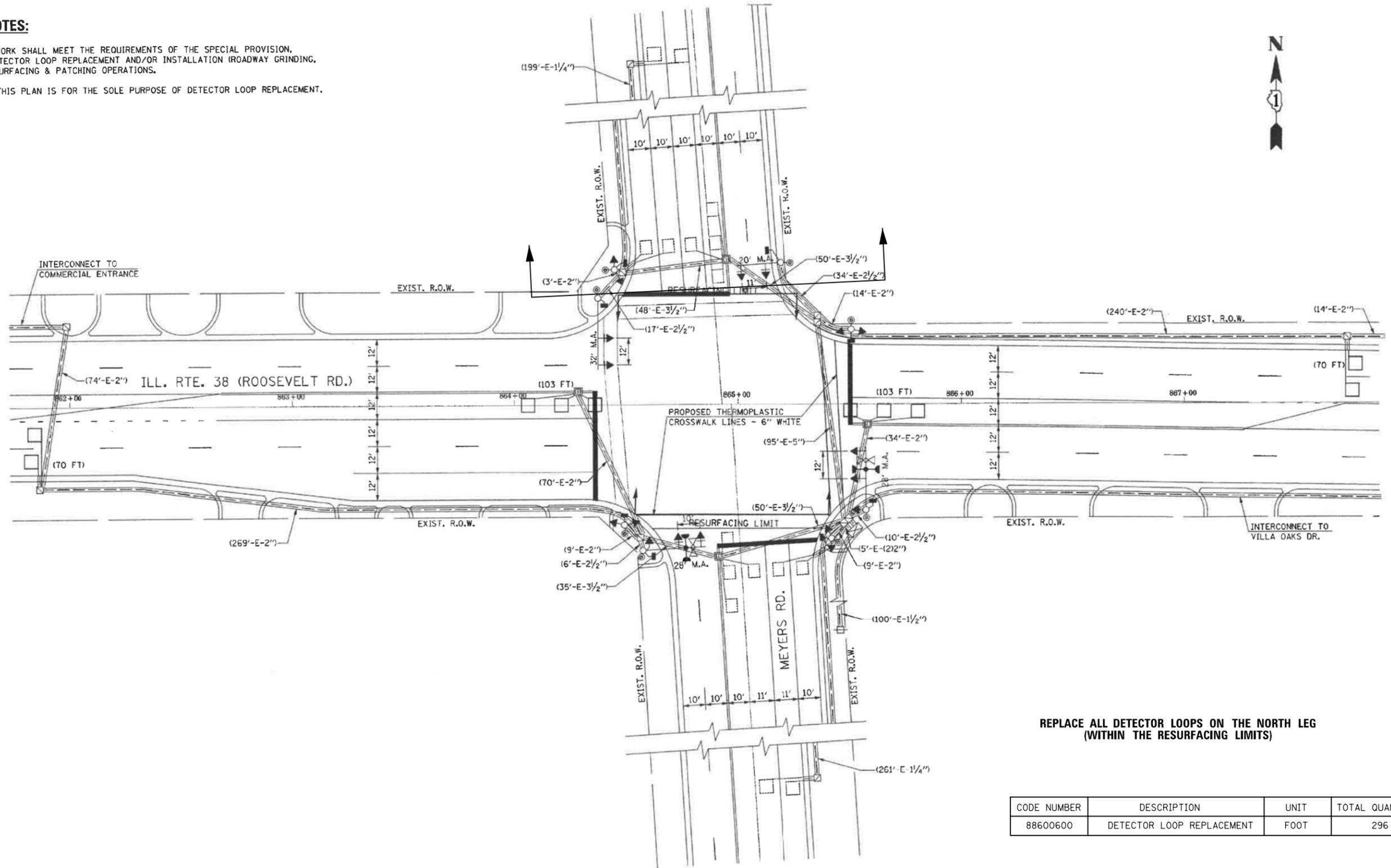
**VILLAGE OF LOMBARD
WESTMORE-MEYERS ROAD
CONSTRUCTION DETAILS**

SCALE: NTS SHEET 1 OF 1 SHEETS STA. TO STA.

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2638	17-00158-00-RS		25	16
CONTRACT NO. 61E93				
ILLINOIS FED. AID PROJECT				

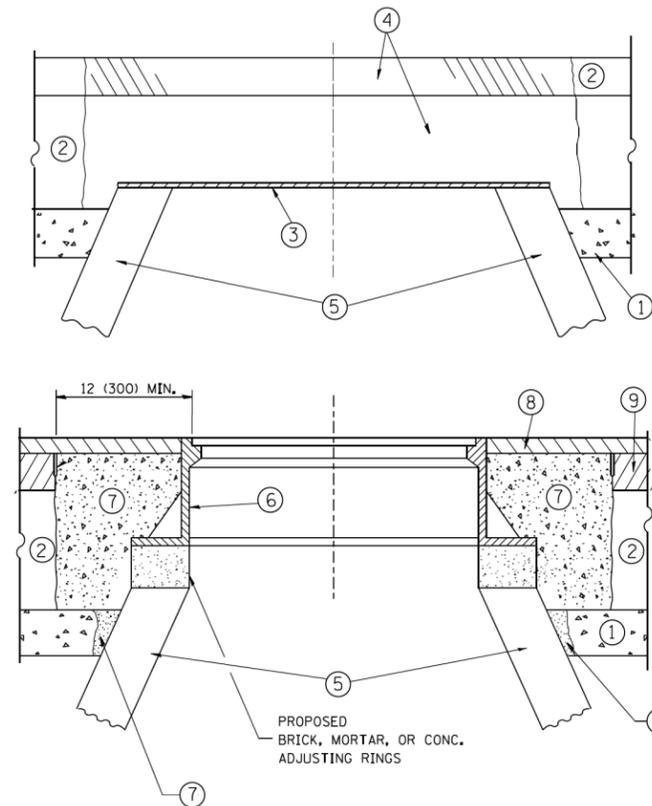
NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



REPLACE ALL DETECTOR LOOPS ON THE NORTH LEG (WITHIN THE RESURFACING LIMITS)

CODE NUMBER	DESCRIPTION	UNIT	TOTAL QUANTITY
88600600	DETECTOR LOOP REPLACEMENT	FOOT	296



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

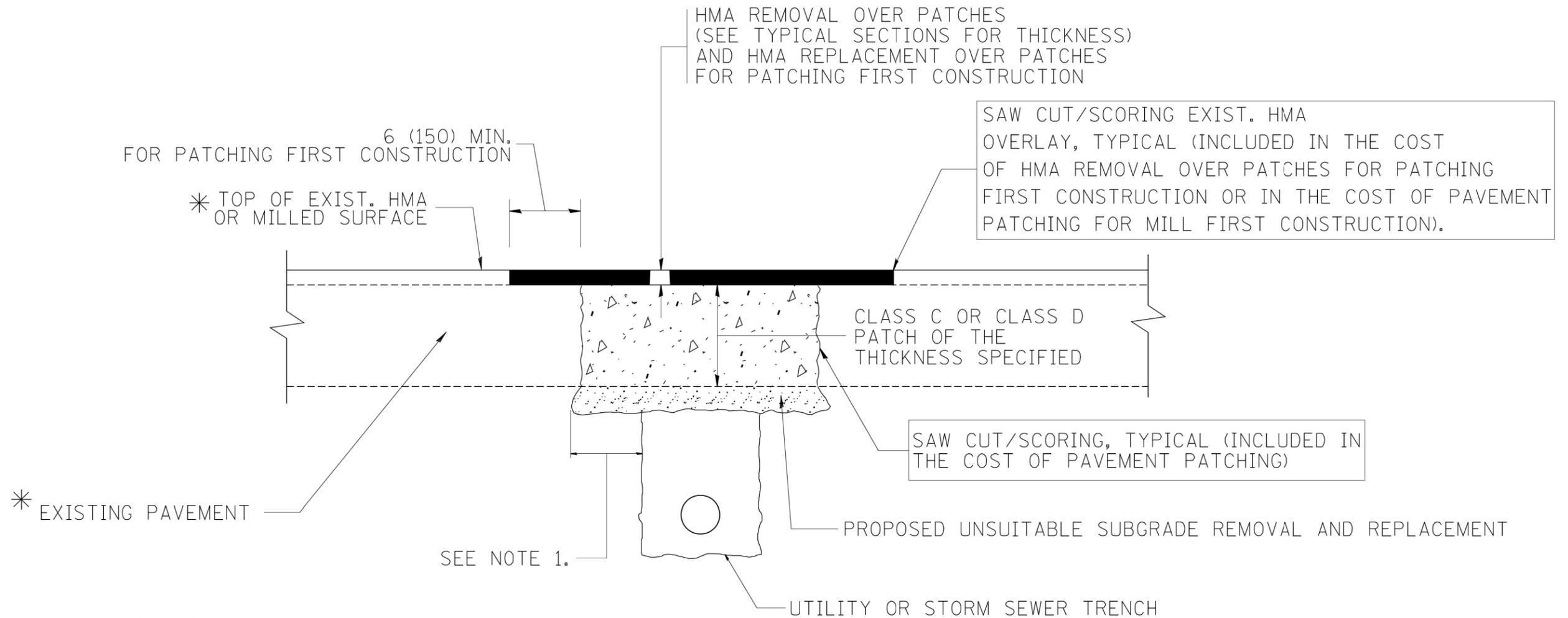
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ca:\pwork\pwork\baerd1\d0108315\bd08.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 1/68.5000 "/ m	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2638	17-00158-00-RS	DuPAGE	25	18
BD600-03 (BD-8)		CONTRACT NO. 61E93		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

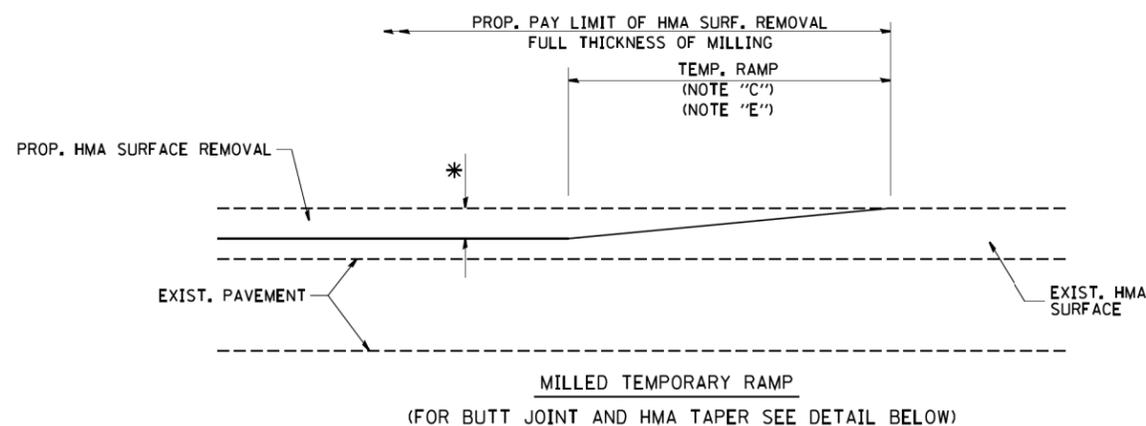
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

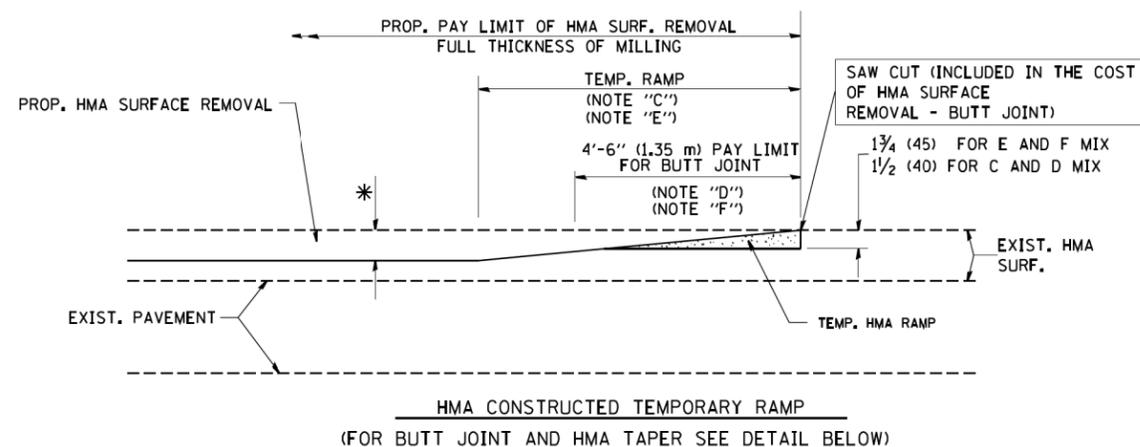
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			FAU RTE. 2638	SECTION 17-00158-00-RS	COUNTY DuPAGE	TOTAL SHEETS 25	SHEET NO. 19
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD400-04 (BD-22)			
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
CONTRACT NO. 61E93												

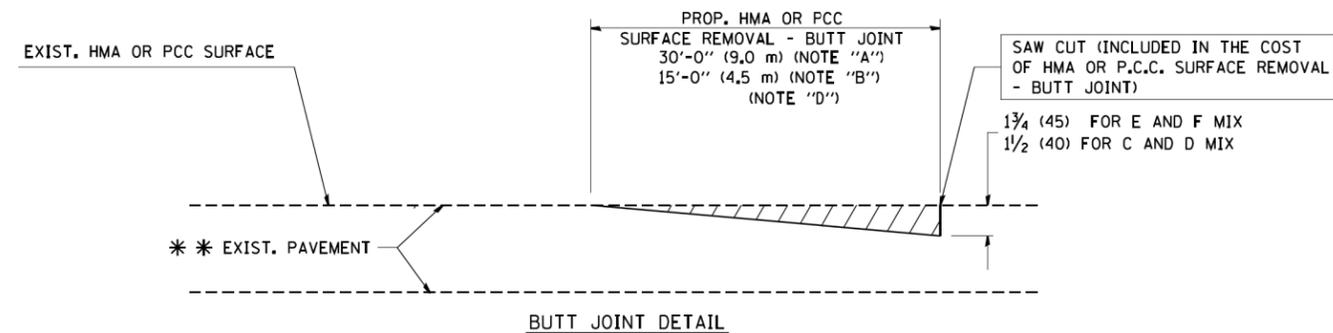


OPTION 1

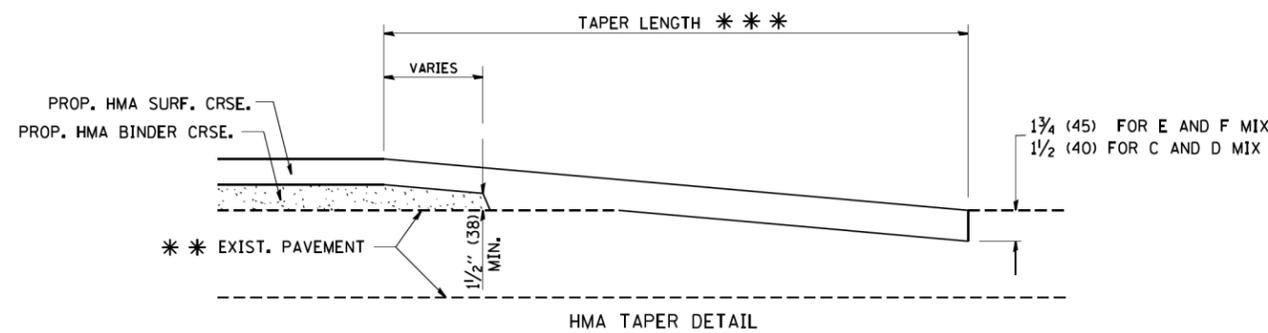


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

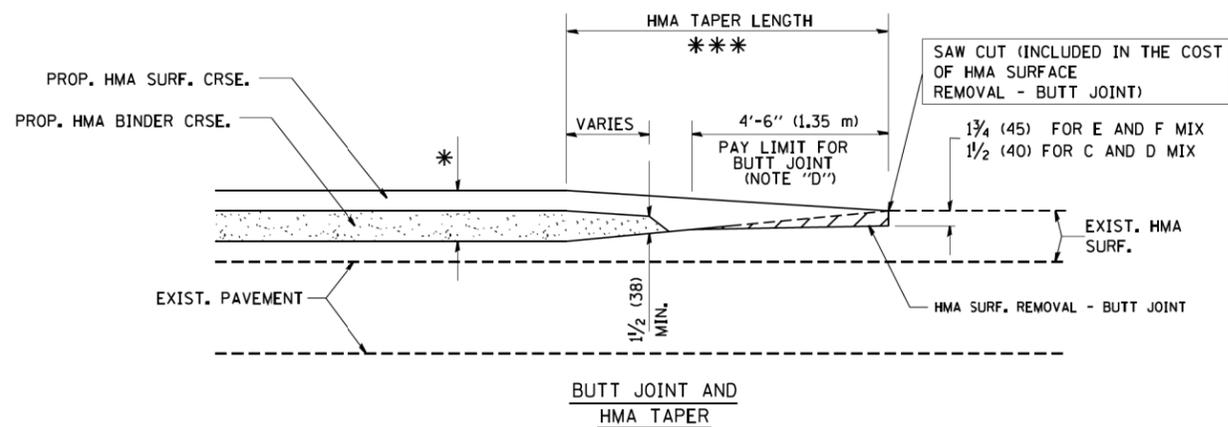
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME = W:\distata\22x34\bd32.dgn	USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

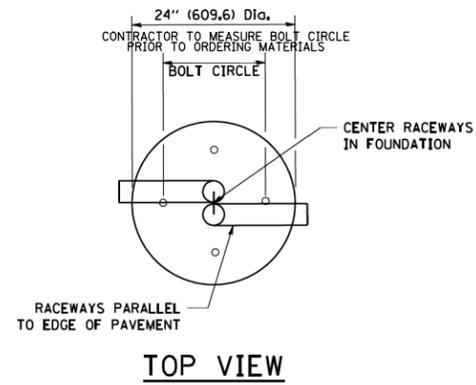
**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2638	17-00158-00-RS	DuPAGE	25	20
BD400-05 BD32			CONTRACT NO. 61E93	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LIGHT POLE FOUNDATION DEPTH TABLE
30 FT. (9.144 m) TO 35 FT. (10.668 m) MOUNTING HEIGHT

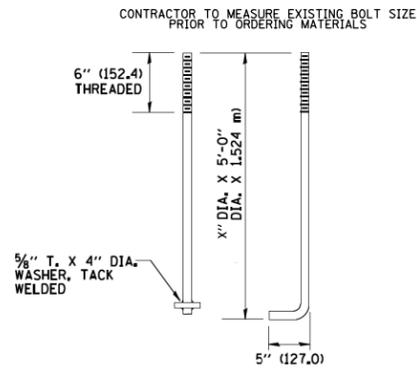
SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION	
	SINGLE ARM POLE	TWIN ARM POLE
SOFT CLAY Qu = 0.375 TON/SQ. FT.	11'-0" (3.35 m)	12'-8" (3.85 m)
MEDIUM CLAY Qu = 0.75 TON/SQ.FT	9'-0" (2.74 m)	14'-10" (4.52 m)
STIFF CLAY Qu = 1.50 TON/SO. FT.	7'-6" (2.29 m)	8'-7" (2.61 m)
LOOSE SAND φ = 34°	9'-6" (2.90 m)	10'-7" (3.22 m)
MEDIUM SAND φ = 37.5°	9'-0" (2.74 m)	9'-10" (2.99 m)
DENSE SAND φ = 40°	8'-3" (2.51 m)	9'-7" (2.91 m)



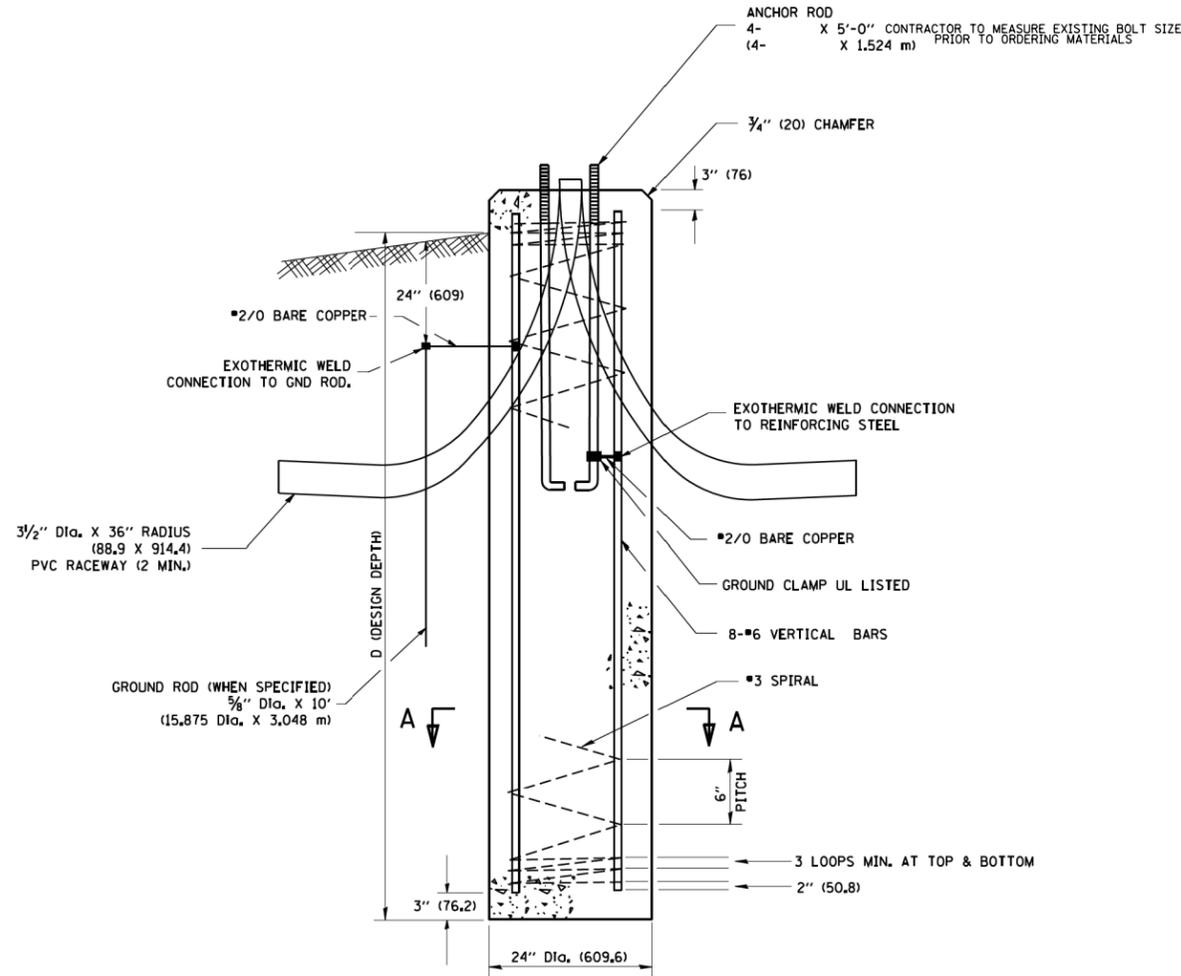
THIS DETAIL HAS BEEN MODIFIED FOR THIS PROJECT

NOTES

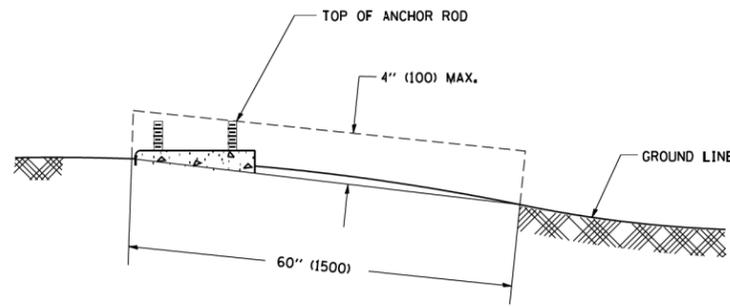
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 4 IN. (100 mm) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3/4-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS SI. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105), NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UM(6 MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.



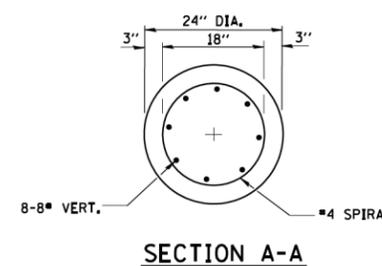
ANCHOR BOLT DETAIL



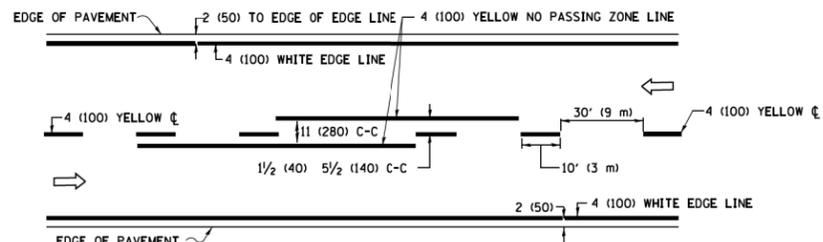
FOUNDATION DETAIL



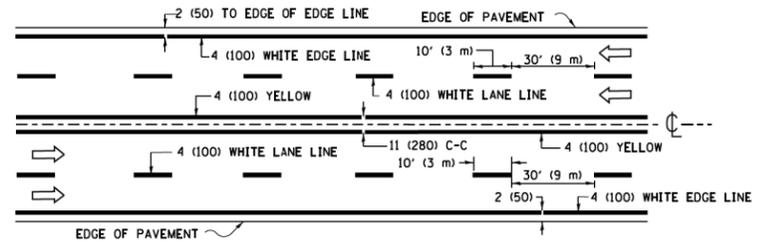
FOUNDATION EXTENSION DETAIL



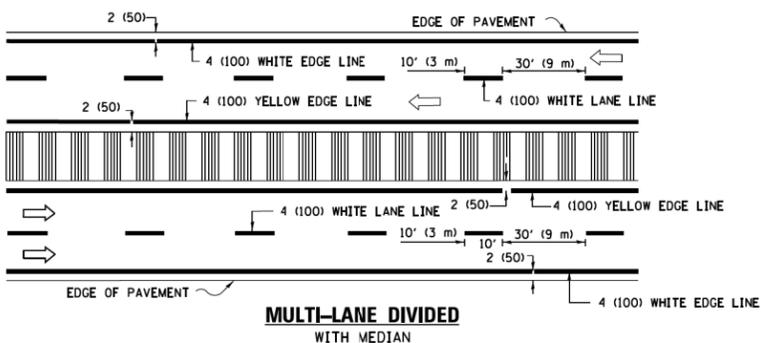
SECTION A-A



2-LANE ROADWAY

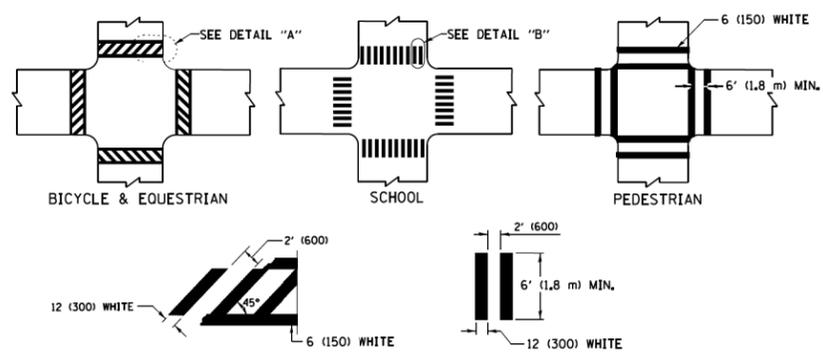


MULTI-LANE UNDIVIDED



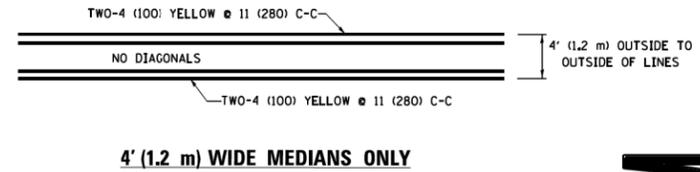
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

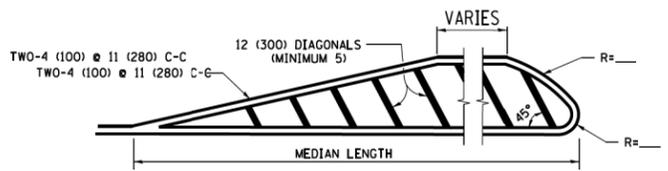


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

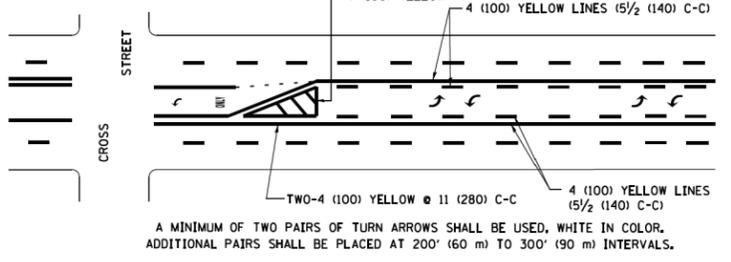


4' (1.2 m) WIDE MEDIANS ONLY

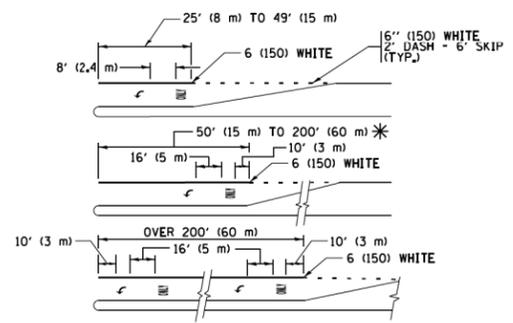


MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

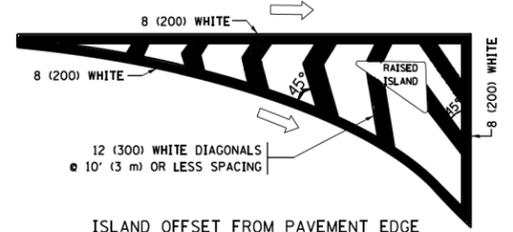


**MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING**

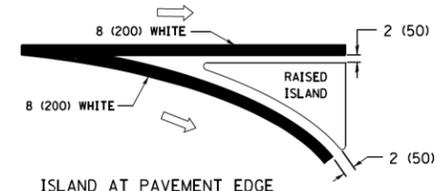


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

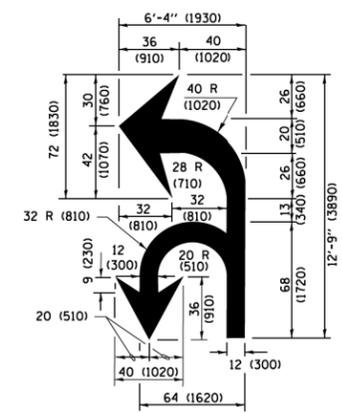
**TYPICAL LEFT (OR RIGHT) TURN LANE
TYPICAL TURN LANE MARKING**



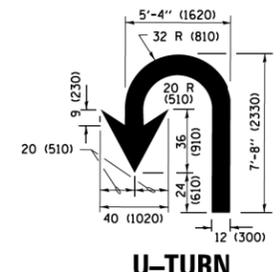
ISLAND OFFSET FROM PAVEMENT EDGE



**ISLAND AT PAVEMENT EDGE
TYPICAL ISLAND MARKING**



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS; 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

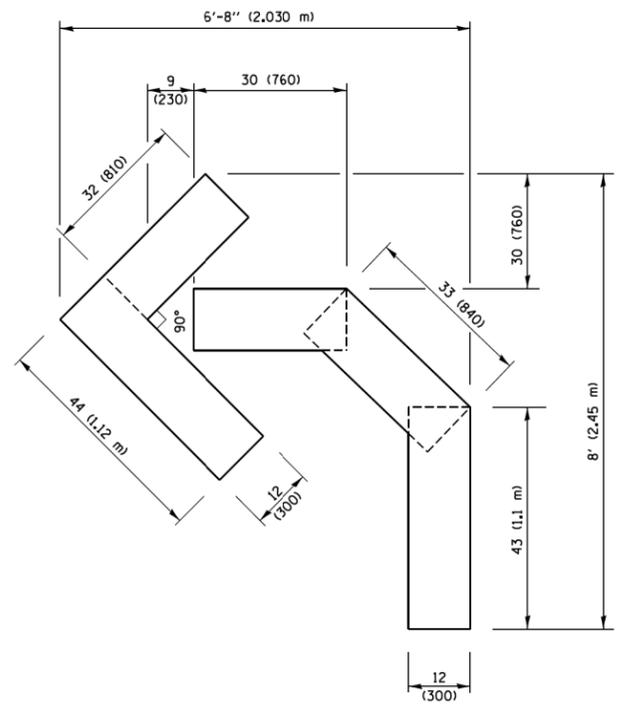
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Default	PLOT SCALE = 50.000' / 1" =	CHECKED -	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 6/23/2017	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

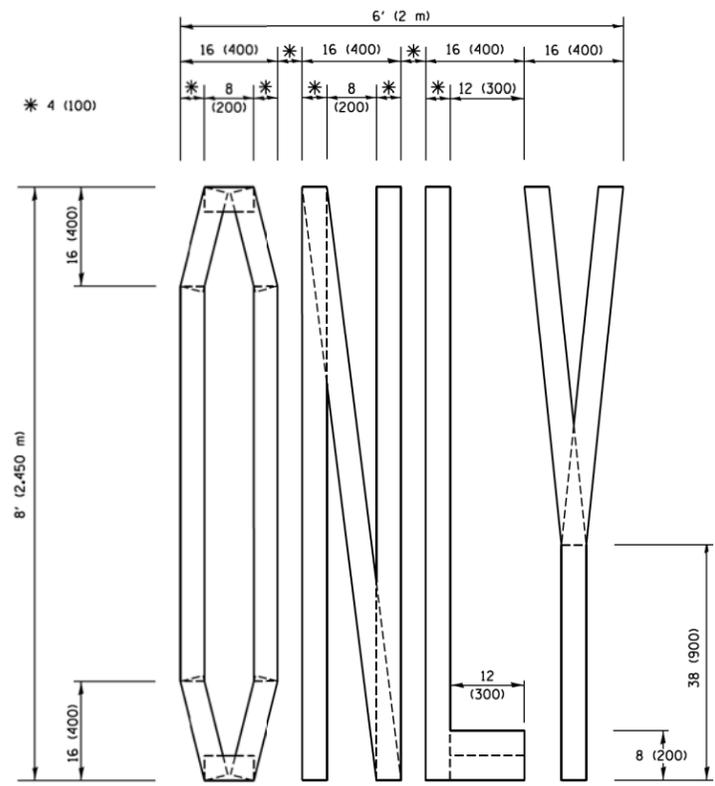
**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.
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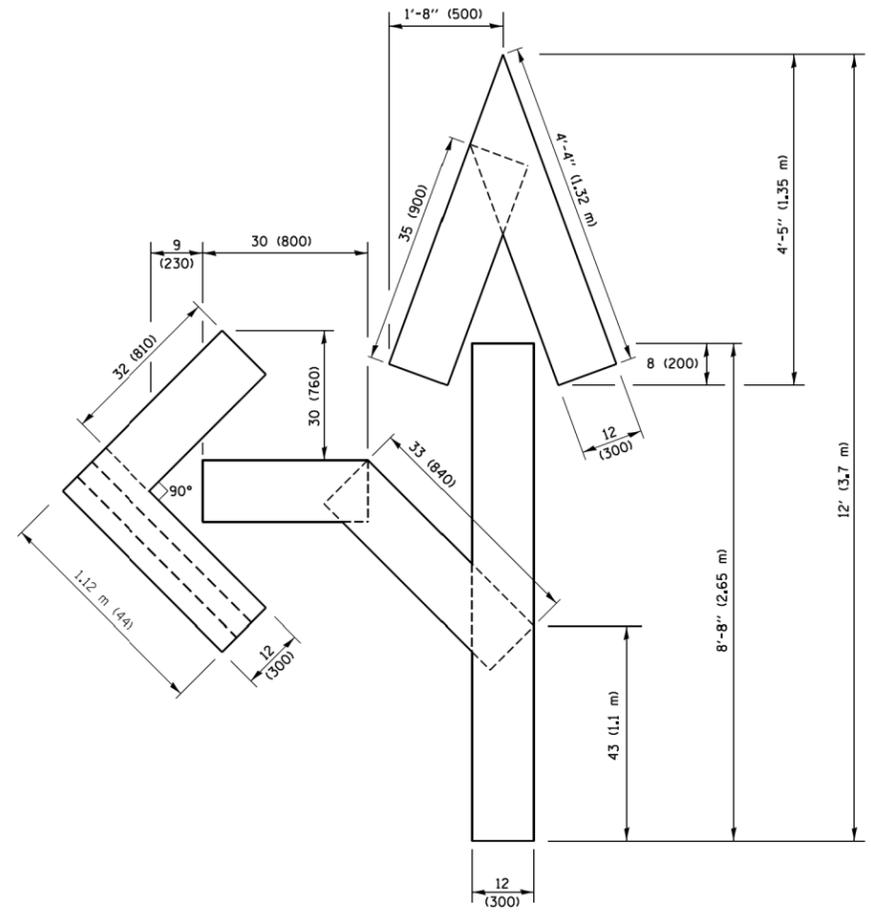
FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2638	17-00158-00-RS	DuPAGE	25	23
TC-13		CONTRACT NO.	61E93	
ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

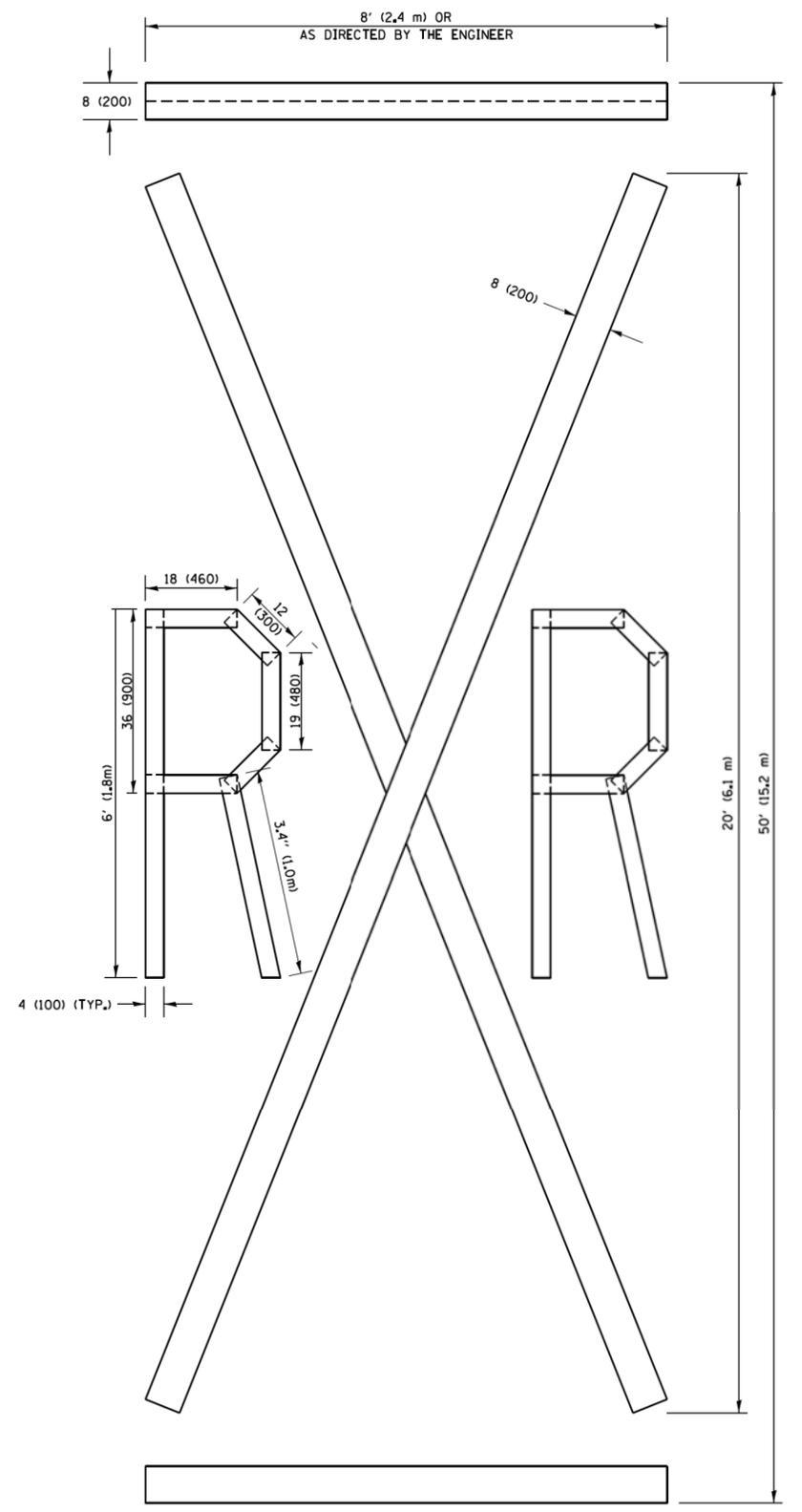


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

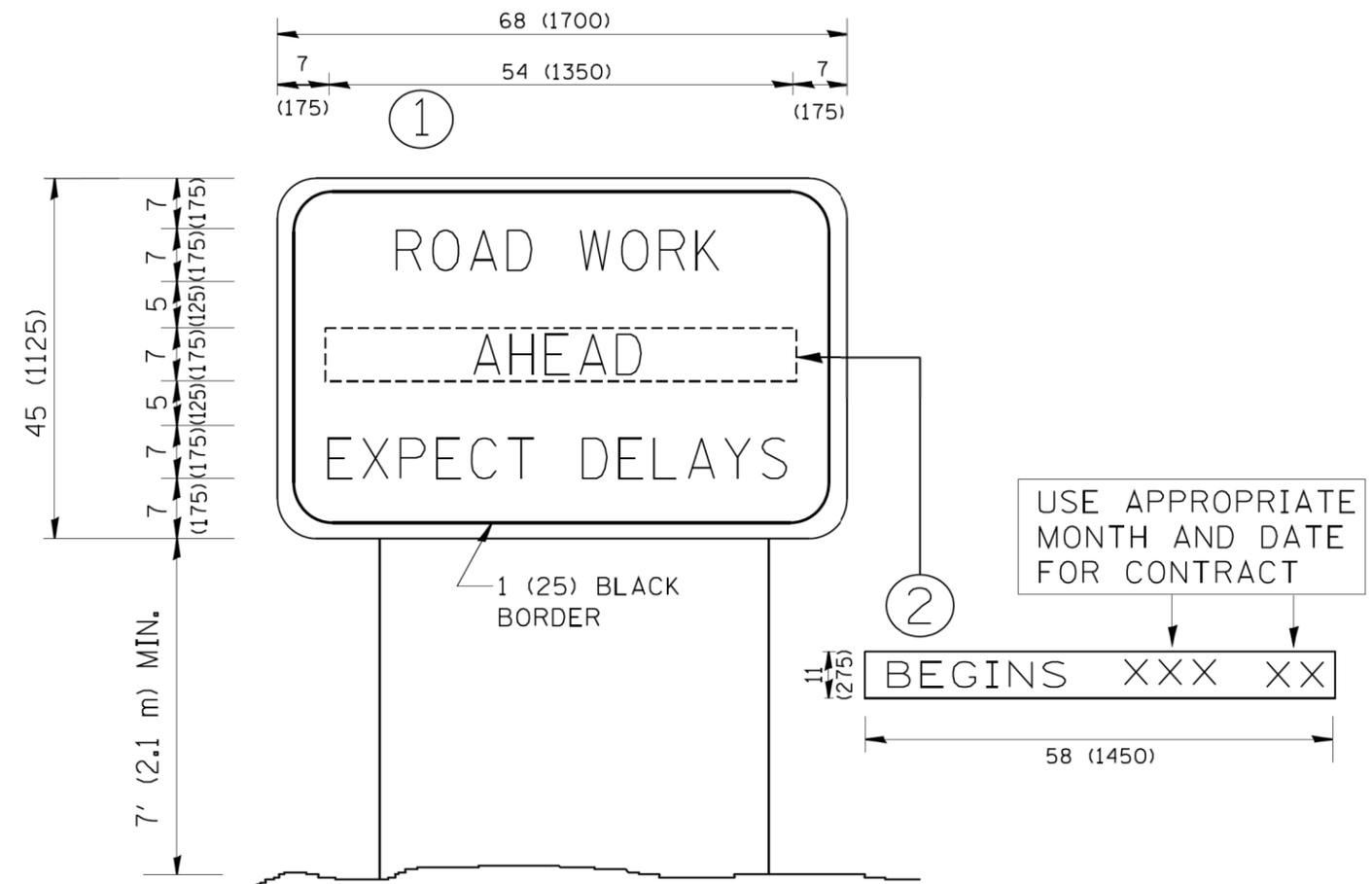
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = footemj	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
pw\1\084E81D\INTEG\illinois.gov\FWIDOT\Documents\DOT Offices\District 1\Projects\Dist 1\CADData\CADsheets\16.dgn		DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT SCALE = 50,0000 / 1 in.		CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 9/15/2016		DATE -	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2638	17-00158-00-RS	DuPAGE	25	24
TC-16		CONTRACT NO. 61E93		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = geglanoht	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

FAU RTE. 2638	SECTION 17-00158-00-RS	COUNTY DuPAGE	TOTAL SHEETS 25	SHEET NO. 25
TC-22			CONTRACT NO. 61E93	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				